City of Richmond

Public Hearing Agenda

Public Notice is hereby given of a Regular Council Meeting for Public Hearings being held on:

Tuesday, May 19, 2020 – 7 p.m.

Council Chambers, 1st Floor
Richmond City Hall
6911 No. 3 Road
Richmond, BC V6Y 2C1

OPENING STATEMENT

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1. **OFFICIAL COMMUNITY PLAN BYLAW 7100, AMENDMENT BYLAW 10155 AND RICHMOND ZONING BYLAW 8500, AMENDMENT BYLAW 10156 (RZ 18-820669)**
(File Ref. No. 12-8060-20-010155/10156; RZ 18-820669) (REDMS No. 6282428 v.3; 6425700; 6426419)

PH-5 See Page **PH-5** for full report

Location:	10140, 10160 & 10180 No. 1 Road and 4051 & 4068 Cavendish Drive
Applicant:	Yamamoto Architecture Inc.
Purpose of OCP Designation Amendment:	To redesignate 4051 Cavendish Drive and a portion of 10140, 10160 & 10180 No. 1 Road from "Single-Family" to "Multiple-Family" in the Steveston Area Land Use Map to Schedule 2.4 of Official Community Plan Bylaw 7100 (Steveston Area Plan)

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Purpose of Zoning Amendment:

1. To rezone 4051 Cavendish Drive and the West Portions of 10140, 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Detached (RS1/E)" to a new site-specific zone entitled "Town Housing (ZT88) - No. 1 Road (Steveston)", to permit the development of 35 townhouses including six affordable housing units, with vehicle access from No. 1 Road; and
2. To rezone 4068 Cavendish Drive and the East Portions of 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Detached (RS1/E)" to "Single Detached (RS2/B)" to permit the development of two single-family lots with vehicle access from Cavendish Drive.

First Reading: April 14, 2020

Order of Business:

1. Presentation from the applicant.
2. Acknowledgement of written submissions received by the City Clerk since first reading.
3. Submissions from the floor.

Council Consideration:

1. Action on second and third readings of Official Community Plan Bylaw 7100, Amendment Bylaw 10155.

2. Action on second and third readings of Richmond Zoning Bylaw 8500, Amendment Bylaw 10156.

2. **RICHMOND ZONING BYLAW 8500, AMENDMENT BYLAW 10163 (RZ 18 843479)**
(File Ref. No. 12-8060-20-010163; RZ 18-843479) (REDMS No. 6426161; 6427581)

PH-183 See Page **PH-183** for full report

Location: 7100 & 7120 Ash Street

Applicant: Fougere Architecture Inc.

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Purpose: To rezone the subject properties from the "Single Detached (RS1/F)" zone to the "Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)" zone, in order to develop 17 two-storey townhouse units with vehicle access from Ash Street.

First Reading: April 14, 2020

Order of Business:

1. Presentation from the applicant.
2. Acknowledgement of written submissions received by the City Clerk since first reading.
3. Submissions from the floor.

Council Consideration:

1. Action on second and third readings of Richmond Zoning Bylaw 8500, Amendment Bylaw 10163.

3. **RICHMOND ZONING BYLAW NO. 8500, AMENDMENT BYLAW NO. 10122 (FENCE REGULATIONS)**
(File Ref. No. 08-4430-01; 12-8060-20-010122) (REDMS No. 6404835 v. 1B; 6360541)

PH-224 See Page **PH-224** for full report

Location: City wide

Applicant: City of Richmond

Purpose: To amend Richmond Zoning Bylaw No. 8500 to amend regulations as they relate to fences, including height and materials.

First Reading: April 14, 2020

Order of Business:

1. Presentation from the applicant.
2. Acknowledgement of written submissions received by the City Clerk since first reading.
3. Submissions from the floor.

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Council Consideration:

1. Action on second and third readings of Richmond Zoning Bylaw 8500, Amendment Bylaw No. 10122.

2. Adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw No. 10122.

3. Adoption of Building Regulation Bylaw No. 7230, Amendment Bylaw No. 10144.

ADJOURNMENT

City of Richmond

Report to Committee

To:	Planning Committee	**Date:**	March 16, 2020
From:	Wayne Craig Director, Development	**File:**	RZ 18-820669

Re: **Application by Yamamoto Architecture Inc. for the Rezoning of 4051 Cavendish Drive and the West Portions of 10140, 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Detached (RS1/E)" to "Town Housing (ZT88) - No. 1 Road (Steveston)"; and for the Rezoning of 4068 Cavendish Drive and the East Portions of 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Detached (RS1/E)" to "Single Detached (RS2/B)".**

Staff Recommendation

1. That Official Community Plan Amendment Bylaw 10155, to redesignate 4051 Cavendish Drive and a portion of 10140, 10160 & 10180 No. 1 Road from "Single-Family" to "Multiple-Family" in the Steveston Area Land Use Map to Schedule 2.4 of Official Community Plan Bylaw 7100 (Steveston Area Plan), be introduced and given first reading.
2. That Bylaw 10155, having been considered in conjunction with:
   - the City's Financial Plan and Capital Program;
   - the Greater Vancouver Regional District Solid Waste and Liquid Waste Management Plans;

   is hereby found to be consistent with said program and plans, in accordance with Section 477(3)(a) of the *Local Government Act*.
3. That Bylaw 10155, having been considered in accordance with OCP Bylaw Preparation Consultation Policy 5043, is hereby found not to require further consultation.
4. That Richmond Zoning Bylaw 8500, Amendment Bylaw 10156, to create the "Town Housing (ZT88) - No. 1 Road (Steveston)" zone, and to rezone 4051 Cavendish Drive and the West Portions of 10140, 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Detached (RS1/E)" to "Town Housing (ZT88) - No. 1 Road (Steveston)"; and to rezone 4068 Cavendish Drive and the East Portions of 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Detached (RS1/E)" to "Single Detached (RS2/B)", be introduced and given first reading.

Wayne Craig
Director, Development

WC:el

Att. 17

REPORT CONCURRENCE		
ROUTED TO:	**CONCURRENCE**	**CONCURRENCE OF GENERAL MANAGER**
Affordable Housing	☑	
Policy Planning	☑	
Transportation	☑	

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Staff Report

Origin

Yamamoto Architecture Inc. has applied to the City of Richmond for permission to rezone lands at 10140, 10160 & 10180 No. 1 Road and 4051 & 4068 Cavendish Drive (Attachment 1). The applicant is proposing to rezone 4051 Cavendish Drive and the West Portions of 10140, 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Dctached (RS1/E)" to a new site-specific zone entitled "Town Housing (ZT88) - No. 1 Road (Steveston)", to permit the development of 35 townhouses with vehicle access from No. 1 Road; and to rezone 4068 Cavendish Drive and the East Portions of 10160 & 10180 No. 1 Road from "Single Detached (RS1/B)" and "Single Detached (RS1/E)" to "Single Detached (RS2/B)" to permit the development of two single-family lots with vehicle access from Cavendish Drive. The townhouse development will include six affordable housing units and three secondary suites; and the single family lots will provide two secondary suites.

Project Description

The proposed development will extend Cavendish Drive through the site connecting the existing portions of Cavendish Drive, and create a townhouse site on the west side of Cavendish Drive and two single-family lots on the east side of Cavendish Drive (Attachment 2). The new Cavendish Drive road right of way area will be developed to function as an emergency access only. Bollards will be installed at each end to ensure no public vehicle access. The emergency access will also provide a pedestrian walkway between the northern and southern sections of the existing Cavendish Drive. A preliminary functional design of the new Cavendish Drive Connection emergency access/greenway can be found in Attachment 3.

35 townhouse units, including six Low-End Market Rental (LEMR) units, are proposed for the townhouse site on the west side of Cavendish Drive. Vehicle access is provided by a single driveway access to No. 1 Road. The site layout includes three two-storey units, five two-and-a-half-storey units, and 28 three-storey units in ten townhouse clusters. Three secondary suites and nine units designed to be convertible units are included in this proposal. The proposed density is 0.64 floor area ratio (FAR).

Two single family lots are proposed for the single family development site on the east side of Cavendish Drive. A separate Subdivision application will be required to create the two single family lots after the site is rezoned. Each proposed lot will have one vehicle access from the southern section of Cavendish Drive. Both proposed homes are 2-storeys with a side-by-side double car garage and each includes a two-bedroom secondary suite of approximately 64 m^2 (689 ft^2).

Findings of Fact

A Development Application Data Sheet providing details about the townhouse development proposal can be found in Attachment 4 and a Development Application Data Sheet providing details about the single family development proposal can be found in Attachment 5.

<u>Subject Site Existing Housing Profile</u>

There are three houses on the development site. The applicant has advised that there is no secondary suite in any of these houses, but the three houses are currently operated as rental units.

Surrounding Development

To the North: An existing single family dwelling on a lot zoned "Single Detached (RS1/E)" fronting No. 1 Road, which is identified for townhouse development under the Arterial Road Land Use Policy; and the Richmond Chinese Alliance Church on a lot zoned "Assembly (ASY)".

To the South: An existing 16-unit townhouse complex on a lot zoned "Low Density Townhouses (RTL3)" fronting No. 1 Road, and existing single family dwellings on a lots zoned "Single Detached (RS1/B)" fronting the southern section of Cavendish Drive.

To the East: Existing single family dwellings on a lots zoned "Single Detached (RS1/B)" fronting the northern section of Cavendish Drive.

To the West: Across No. 1 Road, existing single family dwellings on a lots zoned "Single Detached (RS1/B)" fronting No. 1 Road, which are identified for Arterial Road Compact Lot Single Detached development under the Arterial Road Land Use Policy; and an existing 11-unit townhouse complex on a lot zoned "Low Density Townhouses (RTL3)" fronting No. 1 Road.

Related Policies & Studies

Official Community Plan

The 2041 Official Community Plan (OCP) Land Use Map designation for the subject site is "Neighbourhood Residential". This redevelopment proposal for 35 townhouses and two single family lots is consistent with this designation. An amendment to the Steveston Area Plan is required as described below.

Steveston Area Plan

The Steveston Area Land Use Map designation for the western portion of the subject site (i.e., for the area approximately 45 m east of No. 1 Road) is "Multiple-Family", and the designation for the eastern portion of the subject site is "Single-Family" (Attachment 6). In order to allow the area between No. 1 Road and Cavendish Drive on the subject site to be redeveloped for townhouses, an OCP Amendment is required to redesignate a portion of the subject site from "Single-Family" to "Multiple-Family" in the Area Plan (see Attachment 7).

Arterial Road Policy

The Arterial Road Land Use Policy in the City's 2041 OCP (Bylaw 9000), directs appropriate townhouse development onto certain arterial roads outside the City Centre. The western portion of the subject site is identified for "Arterial Road Townhouse" on the Arterial Road Housing Development Map.

The eastern boundary of the "Arterial Road Townhouse" uses on the Arterial Road Housing Development Map on this block was determined based on the land use designation identified on the Steveston Area Land Use Map. Should the proposed OCP Amendment mentioned in the last section be approved by Council:

- the area between No. 1 Road and Cavendish Drive on the subject site will be consolidated into one development parcel and will be designated "Multiple-Family" on the Steveston Area Land Use Map;

- the development parcel fronting on No. 1 Road, west side of Cavendish Drive, will be allowed to be redeveloped into Arterial Road Townhouses under the Arterial Road Land Use Policy; and
- no amendment to the Arterial Road Housing Development Map is required according to the Arterial Road Land Use Policy.

Additional Density

The Arterial Road Land Use Policy allows additional density along arterial roads to be considered subject to provision of Low End Market Rental (LEMR) housing units, as per the below conditions:

- Bonus density is used to provide built LEMR units secured through a Housing Agreement;
- Built LEMR units comply with the City's Affordable Housing Strategy with respect to the housing unit sizes, tenant eligibility criteria and maximum monthly rental rates; and
- The overall design of the development complies with the Arterial Road Guidelines for Townhouses.

The proposed development under this application is generally consistent with the Arterial Road Policy.

Property to the North

The proposed site assembly will leave a residual development site to the north that will not meet the minimum 50 m site frontage requirement. The residual development site to the north at 10120 No. 1 Road, located between the subject site and the Richmond Chinese Alliance Church, has a frontage of approximately 20 m along No. 1 Road.

The applicant advised staff in writing that they have made attempts to acquire the adjacent property, but cannot reach an agreement with the owners. The applicant has requested that this application proceed without the acquisition of the adjacent property to the north.

While the proposed development would create an orphan site situation on the north side of the subject site, staff support the proposed development based on:

- the adjacent property owners are not interested in redeveloping their properties at this time;
- the developer has provided a development concept plan for the adjacent site to the north (on file);
- the developer has agreed to provide vehicle access to future townhouse development on the adjacent site to the north; a Public Rights-of-Passage (PROP) Statutory Right-of-Way (SRW) over the entry driveway on the subject site will be registered on Title of the subject site as a condition of rezoning to secure this arrangement.

Single Family Lot Size Policy 5426

The subject site is located within Single Family Lot Size Policy Area 5426 (Attachment 8), adopted by Council on December 18, 1989. The Single Family Lot Size Policy provides direction on the size of single family lots that may be created through rezoning and subdivision.

As per Section 2.3 of the Zoning Bylaw 8500, the proposed rezoning for the west portion of the site is not subjected to this Lot Size Policy 5426 since that portion of the site is located along an arterial road where the Lot Size Policy has been adopted more than five years ago, and is included/to be included

into an Area Plan which designates the site for "Multiple-Family" uses. Therefor, the townhouse portion of the development is consistent with the Policy.

The proposed rezoning for 4068 Cavendish Drive and the east portions of 10160 and 10180 No. 1 Road is subjected to this Lot Size Policy 5426 since a two lot subdivision for single family residential uses is being proposed. The Policy permits properties located within the policy area to be rezoned and subdivided as per "Single Detached (R2/B)" zone; where the minimum lot size is 360 m^2 and minimum lot width is 12.0 m (or 14.0 m in case of a corner lot). The proposed two lot single family subdivision is consistent with the Lot Size Policy. One lot will be approximately 360 m^2 in size and the other lot will be approximately 444 m^2 in size.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on Title is required prior to final adoption of the rezoning bylaw.

Public Consultation

A rezoning sign has been installed on the subject property. Staff have not received any comments from the public about the rezoning application in response to the placement of the rezoning sign on the property. However, staff have received comments from the public about the proposed development in response to the open houses held by the applicant.

Open Houses

The applicant conducted two public open houses for the rezoning application; the first one was held on June 20, 2018 and the second open house was held on June 26, 2019. Both open houses were held at the Richmond Chinese Alliance Church, which is located to the immediate north of the development site. For each of the two open houses, flyers were delivered by the applicant to approximately 107 properties in the immediate area (see Attachment 9 for the Notification Area). Staff attended the open houses to observe the meetings and answer policy or process-related questions.

June 20, 2018 Open House

Approximately 40 people attended the event. Comment sheets were provided to all the attendees. A total of 11 completed comment sheets were received after the meeting. Three independent emails from residents within the notification area were also received after the meeting. A copy of the Open House Summary prepared by the applicant, including the comment sheets and emails received, is included in Attachment 10.

Major concerns from the neighbourhood on the proposed development are summarized below with responses to each of the concerns identified in bold italics:

1. Security of the existing residences on Cavendish Drive

   Concerns were raised about the proposed road extensions to connect the northern and southern sections of the existing Cavendish Drive through the subject site, and the proposed public walkway between No. 1 Road and Cavendish Drive. Residents concern that the proposed improvements would attract more vehicle and foot traffic, parking, and transients on Cavendish Drive and result in increased noise, theft and undesirable activities.

In response to the concerns raised, Transportation staff has accepted an alternative proposal by the applicant to construct an emergency access/pedestrian walkway within the proposed road extension, instead of a through road, to minimize increases in traffic and parking on Cavendish Drive. Bollards fitted with locks will be installed at each end of the emergency access/walkway to allow for emergency vehicle access only and to ensure no public vehicle access.

Transportation staff advised that the proposed walkway between No. 1 Road and Cavendish Drive and the proposed emergency access/walkway connecting the two ends of Cavendish Drive would improve walkability and transit connectivity to the neighborhood, which includes Diefenbaker Elementary School. The proposed improvements would re-route the existing informal walkways through undeveloped lots onto paved and lit pathways where safety and security on the pedestrian route could be enhanced.

As part of the townhouse development proposal, pedestrian entry for the units proposed along the public walkway will be designed to face the walkway in order to activate the public walkway and add to passive surveillance. The public walkway will be designed in accordance with the Crime Prevention Through Environmental Design (CPTED) principles.

2. Tree Preservation

A desire was expressed to retain the existing tall trees on site. Based on the initial review of the tree inventory on site, two bylaw-sized trees are considered in good condition; a 140cm cal Sequoia tree and a 56cm cal Spruce tree are proposed to be protected and retained. In response to this concerns, the project arborist had reviewed the tree preservation strategy but is not able to recommend additional trees to be retained on site. However, the developer has revised the site plan of the townhouse development and incorporated the protected trees into the outdoor amenity space.

3. Form and Character

Concern was expressed over the fit of new building design to the existing single family residences on Cavendish Drive. Preliminary architectural plans for the proposed single family homes and townhouses have been developed. The proposed form and character of the proposed buildings seem to compliment with the existing/surrounding single family houses.

4. Site Grading

Concerns were raised regarding site grade and adjacency. The applicant advised that the floor slabs would be raised to meet the required minimum flood plain construction level, but all site grading will occur within the development site and no grade changes will occur along the property lines of adjacent properties. Staff will work with the applicant at the Development Permit stage to ensure no grade changes will occur along the common property lines.

5. Sidewalk Configuration

Concerns were expressed that the varying sidewalk configuration between the northern and southern sections of Cavendish Drive but the opinions were split on how best to improve this. Currently, the sidewalk on the northern section of Cavendish Drive is on the south/east side of the road; and the sidewalk on the southern section of Cavendish Drive is on the west/north side of the road. The proposed 6m emergency access/walkway will provide a seamless connection between the sidewalks on the two sections of Cavendish Drive.

6. Parking

   Concerns were expressed for the potential increase in neighbourhood parking to the area with the proposed townhouse development. All vehicle traffic to the townhouse development will be via No. 1 Road. All townhouse units fronting on to the new Cavendish Drive Connection will have access from the internal drive aisles/walkways within the development. The numbers of residential and visitor parking spaces proposed on the proposed townhouse site are in compliance with the zoning bylaw requirements.

7. Unit Height

   Concerns were raised about the proposed three-storey townhouse units. Townhouse units fronting onto Cavendish Drive have been reduced to a two-storey height from Cavendish Drive with a half storey in the roof space which will only be visible from within the townhouse site. This will ensure the character and form of the townhouses complement the existing single family homes on Cavendish Drive. In addition, townhouse units that have a side yard interface with existing adjacent single family homes on Cavendish Drive have been reduced to two storeys to address potential massing and shadowing concerns.

June 26, 2019 Open House

A second open house was held to provide area residents with information on the revised proposal and how the concerns raised in the first open house were addressed.

Approximately 20 people attended the event. Comment sheets were provided to all the attendees. A total of 7 completed comment sheets were received after the meeting. Two independent emails from residents within the notification area were also received after the meeting. A copy of the Open House Summary prepared by the applicant, including the comment sheets and emails received, is included in Attachment 11.

Concerns identified through the second open house are summarized below with responses to each of the concerns identified in bold italics:

1. Public Walkway Between No. 1 Road & Cavendish Drive

   Two residents were still concerned that the construction of the public walkway would result in an increase in crime and undesirable activities. Transportation staff have reviewed the requirements and feel that a public walkway between No. 1 Road and Cavendish Drive through this site is still warranted.

   Installation of a more direct pedestrian link from the surrounding neighborhood to No. 1 Road would make access to the transit stops on No. 1 Road more convenient for residents. This improvement facilitates walking, cycling and transit use; and a safe and accessible pathway with direct and connected links would support Richmond's mode shift targets in the Community Energy and Emissions Plan. Enhancements to support and encourage transit use is also consistent with the City's official Community Plan objectives.

   Staff will work with the applicant at the Development Permit stage to ensure that the design of the walkway incorporates Crime Prevention Through Environmental Design principles, including appropriate lighting, fencing and landscaping to enhance passive surveillance.

2. Single Family Lots

Two residents requested that the front yard setbacks of the proposed single family lots be reduced to provide larger rear yard; and that the side yard setbacks be increased to reduce shadowing (in order to provide a larger building separation from the new homes to the existing homes). The applicant has agreed to increase the setbacks outlined in the bylaw to the proposed single family lots:

Setbacks (m)	Bylaw Requirements	Proposed Lot A	Proposed Lot B
Internal Side Yard:	1.2 m	East side – 2.0 m	South side – 1.45 m
Rear Yard – 1st Floor:	Lot A: 6.0 m Lot B: 6.0 m	6.98 m	7.5 m
Rear Yard – 2nd Floor:	Lot A: 7.46 m Lot B: 6.0 m	10.81 m	7.5 m

These setbacks have been reflected on the proposed site plan. The applicant has agreed to register a legal agreement on Title, prior to final adoption of the rezoning bylaw, to ensure that future Building Permit applications will be consistent with these additional setbacks.

3. Tree Planting

Concerns were expressed regarding tree replacement. According to the Preliminary Landscape Plan provided by the applicant (Attachment 14), the developer is proposing to plant 62 new trees on-site. Tree size and species will be reviewed in detail through Development Permit and overall landscape design. Comments related to street tree planting have been forwarded to Parks Planning, Design & Construction staff and will be considered at the Servicing Agreement stage.

OCP Consultation Summary

Staff have reviewed the proposed OCP and zoning amendments, with respect to the *Local Government Act* and the City's OCP Consultation Policy No. 5043 requirements, and recommend that this report does not require referral to external stakeholders.

The table below clarifies this recommendation as it relates to the proposed OCP.

Stakeholder	Referral Comment (No Referral necessary)
BC Land Reserve Co.	No referral necessary.
Richmond School Board	No referral necessary.
The Board of Metro Vancouver	No referral necessary, as the proposed amendments are consistent with the Regional Growth Strategy.
The Councils of adjacent Municipalities	No referral necessary, as adjacent municipalities are not affected.
First Nations (e.g., Sto:lo, Tsawwassen, Musqueam)	No referral necessary.
TransLink	No referral necessary, as no transportation road network changes are proposed.
Port Authorities (Vancouver Port Authority and Steveston Harbour Authority)	No referral necessary.

Stakeholder	Referral Comment (No Referral necessary)
Vancouver International Airport Authority (VIAA) (Federal Government Agency)	No referral necessary.
Richmond Coastal Health Authority	No referral necessary.
Community Groups and Neighbours	No referral necessary.
All relevant Federal and Provincial Government Agencies	No referral necessary.

Should the Planning Committee endorse this application and Council grant 1st reading to the rezoning bylaw, the bylaws will be forwarded to a Public Hearing, where any area resident or interested party will have an opportunity to comment.

Public notification for the Public Hearing will be provided as per the *Local Government Act*.

Analysis

Built Form and Architectural Character

The applicant proposes to subdivide the five subject properties into three lots – one townhouse development site and two single family lots.

Single Family Subdivision

The proposal includes a two lot subdivision on the east side of Cavendish Drive. Each lot will contain a single family home with a two-bedroom secondary suite. Vehicular accesses to these two new single family lots will be from the southern portion of Cavendish Drive. Driveway locations will be co-ordinated with the proposed bollard locations at south end of the Cavendish Drive Connection emergency access/greenway.

To illustrate how the future lots and dwellings interface with the existing adjacent single family homes, the applicant has submitted preliminary site plans, landscape plans and building elevations for the two proposed single family lots (Attachment 12). The proposed single family subdivision and dwellings are designed to meet the "Single Detached (RS2/B)" zoning regulations. The designs of the proposed dwellings match the orientations of the existing adjacent single family homes and provide wider side yards and deeper rear yards to reduce shadowing to the existing neighbours. A shadow study for the single family development may be found in Attachment 13. The applicant has agreed to register a legal agreement on Title, prior to final adoption of the rezoning bylaw, to ensure that future Building Permit applications will be consistent with these designs.

Townhouse Development

The proposal also includes a 35 unit townhouse development west of Cavendish Drive. The proposed townhouse site, approximately 6,166 m^2, will be located between No. 1 Road and the new Cavendish Drive Connection. Vehicular access to this townhouse development will be from No. 1 Road only, at the north edge of the site's No. 1 Road frontage.

The townhouse development proposal consists of 35 townhouses, in a mix of two-storey, two-and-a-half-storey, and three-storey townhouse units in 10 clusters. Units will be oriented along No. 1 Road, the new Cavendish Drive Connection, and the proposed public walkway along the south property line. Three-storey units are proposed along No. 1 Road, along the north property line (adjacent to the neighbouring assembly site), and in the middle of the site. Building heights are reduced to two-storey

along the side yard and rear yard interfaces with existing adjacent single family homes in order to provide an adequate transition to the neighbouring residential developments. Units proposed along the new Cavendish Drive Connection will be two-and-a-half-storey. The top/half storey will be provided within the primary roof form of the building above the second floor, with no windows fronting onto Cavendish Drive, in order to create a form and character that complements with the single family homes on Cavendish Drive.

The outdoor amenity area will be situated in a central open courtyard along the main entry drive aisle. Preliminary site plan, landscape plan, building elevations, section plans, and a shadow study for the proposed townhouse development can be found in Attachment 14.

Three ground level secondary suites are proposed to be included in the development: the size of two secondary suites would be approximately 25 m^2 each and the size of the other secondary suite would be approximately 51 m^2. Each secondary suite contains a living area, a sleeping area, a kitchenette and a bathroom. No additional residential parking spaces will be assigned to the secondary suites since a side-by-side double car garage is proposed to be included in each of the townhouse units containing a secondary suite, consistent with the parking requirements of Zoning Bylaw 8500.

To ensure that these secondary suites will not be stratified or otherwise held under separate title, registration of a legal agreement on Title, or other measures restricting stratification, as determined to the satisfaction of the Director of Development, is required prior to final adoption of the rezoning bylaw.

To ensure that the secondary suites will be built, registration of a legal agreement on Title, stating that no final Building Permit inspection will be granted until the secondary suites are constructed to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw, is required prior to final adoption of the rezoning bylaw.

Consistent with the parking requirements in Richmond Zoning Bylaw 8500, a total of 64 resident vehicle parking spaces are proposed, of which 32 spaces (50%) are proposed in a tandem arrangement. Prior to rezoning approval, a restrictive covenant preventing the conversion of tandem parking area into storage or habitable space is required to be registered on title. Also consistent with the parking requirements, a total of seven visitor parking spaces are proposed on-site, one of which will be a handicapped visitor parking space. In addition, a total of 64 resident (Class 1) bicycle parking spaces (in excess of bylaw requirement) and seven visitor (Class 2) bicycle parking spaces are proposed.

Density for Townhouse Development

The Arterial Road Land Use Policy specifies a typical density of 0.60 FAR (Floor Area Ratio) for townhouse developments along arterial roads, subject to the applicant providing a cash-in-lieu contributions to the City's Affordable Housing Reserve Fund prior to Council approval of any rezoning application.

This policy further provides for the consideration of additional density for townhouse development if the proposal includes built affordable housing units, secured by the City's standard Housing Agreement. The applicant is proposing medium density townhouses with a maximum density of 0.65 FAR, including six affordable housing units with a combined floor area of not less than 14% of the total floor area. These units would be secured through a restrictive covenant and Housing Agreement registered on property title prior to Council approval of the rezoning.

Conceptual development plans are contained in Attachment 14. The six affordable housing units are proposed to be located in the northerly building fronting No. 1 Road. Private outdoor spaces are provided for each unit in the form of a yard at-grade and a balcony on the second floor. Consistent with the OCP policies to provide for a variety of housing, the proposed affordable housing units would be ground-oriented in design, and family-oriented in type and size as detailed below:

Number of Units	Unit Type	Minimum Unit Area as per Affordable Housing Strategy	Proposed Unit Size	Maximum Monthly Unit Rent**	Total Maximum Household Income**
5	2BR + den	69 m^2 (741 ft^2)	93.55 m^2 (1,007 ft^2)	$1,218	$46,800 or less
1	2BR + den	69 m^2 (741 ft^2)	97.27 m^2 (1,047 ft^2)	$1,218	$46,800 or less
Total: 6			Total: 565.02 m^2 (6,082 ft^2) (approx. 14.3% of total floor area proposed)		

** May be adjusted periodically as provided for under adopted City policy.

Staff note that 100% of the units are two-bedroom units. Staff also note that all units meet the minimum floor space requirements as outlined in the AHS. The Affordable Housing Strategy also targets 85% of LEMR units to meet Built Universal Housing (BUH) standards. Given that BUH standards are difficult to achieve in townhouse developments, the applicant is proposing to design five of the six LEMR units based on the convertible unit design standards.

Staff recommend that Council support this proposal as the community benefit is significant and the proposed form and massing of the townhouse cluster is generally consistent with the Arterial Road Land Use Policy.

New Site-Specific Zone

To accommodate the proposed development, a new site-specific zone "Town Housing (ZT88) - No. 1 Road (Steveston)" is proposed, with a maximum base density of 0.60 FAR and bonus density of 0.05 FAR, up to a total maximum of 0.65 FAR. The bonus density is conditional upon the provision of six affordable housing units with a combined net floor area of 14% of total net floor area. These units would be secured through a restrictive covenant and a Housing Agreement to be registered on title, prior to rezoning approval.

The ZT88 zone also reflects the applicant's proposal to allow a minimum 4.5 m setback along both No. 1 Road and Cavendish Drive. The proposed road setback is smaller than the required 6.0 m front yard setback in the standard townhouse zones. Staff support the proposed minimum 4.5 m road setback based on:

- the Arterial Road Guidelines for Townhouses in the OCP support reduced front yard setbacks with appropriate streetscape design;
- the resulting reduced front yard setback does not compromise tree preservation or tree planting opportunity along the site frontages;
- the proposed architectural design provides appropriate building articulation and interface with neighbouring properties;
- existing single family homes on Cavendish Drive typically have a road setback back less than 4.5 m;
- a 15.0 m wide road dedication through the site to facilitate the Cavendish Drive connection is required;

- a 0.4 m wide road dedication along No. 1 Road is required to accommodate the required frontage improvements;
- the proposed 4.5 m setback from No. 1 Road would only be applied to proposed Building No. 1; the resulting distance from the back of curb along No. 1 Road to the building face would be approximately 7.5 m;
- Building No. 2 will be set back approximately 5.37 m from No. 1 Road in order to provide a transition from Building No. 1 (at a 4.5 m setback) to the existing adjacent townhouse development to the south (at a 6.0 m setback); and
- the proposed development will be designed to meet the interior noise limits as per the CMHC standards in order to address the road traffic noise from No. 1 Road. A report from a certified acoustical engineer will be required prior to the Development Permit Application for this project being forwarded to the Development Permit Panel for consideration.

Development Permit

A Development Permit processed to a satisfactory level for the proposed townhouse development is a requirement of zoning approval. Through the Development Permit, the following issues are to be further examined:

- Compliance with Development Permit Guidelines for multiple-family projects in the 2041 Official Community Plan (OCP).
- Refinement of the site plan to ensure all the aboveground utility infrastructure improvements for this development proposal will be located at the appropriate location and screened from street view;
- Refinement of the proposed building form to achieve sufficient variety in design to create a desirable and interesting streetscape along No. 1 Road, to reduce visual massing of the three-storey units, and to address potential adjacency issues with adjacent residential uses.
- Refinement of the proposed site grading to ensure survival of all proposed protected trees, to provide appropriate transition between the proposed development and adjacent existing developments, and to ensure accessibility throughout the site including the public walkways.
- Refinement of the outdoor amenity area design, including the choice of play equipment, to create a safe and vibrant environment for children's play and social interaction.
- Review of size and species of on-site replacement trees to ensure bylaw compliance and to achieve an acceptable mix of conifer and deciduous trees on site.
- Opportunities to maximize planting areas along internal drive aisles, to maximize permeable surface areas, and to better articulate hard surface treatments on site.
- Review of aging-in-place features in all units and the designs of convertible units.
- Review of a sustainability strategy for the development proposal.

Additional issues may be identified as part of the Development Permit application review process.

Existing Legal Encumbrances

There is an existing utility Right-of-Ways (ROW) along the north property line of 4051 Cavendish Drive for existing sanitary sewer lines and connections. The developer is aware that no construction is permitted in these areas.

In addition, there is an existing tri-party utility right of way (City of Richmond, BC Hydro and Telus) on a portion of 4068 Cavendish Drive for the provision of utilities and services. In order to create the proposed two-lot single family subdivision at the southeast corner of the site, the developer is required to remove the existing inspection chamber, service connection and service lateral within the utility right of way; as well as to discharge the surplus portion (i.e., 5.0 m x 15.0 m) of the existing utility right of way located on 4068 Cavendish Drive prior to Subdivision Approval. The developer is responsible to coordinate with BC Hydro and Telus, as well as other private utility companies (i.e., Shaw and Fortis BC) to confirm that there are no existing private utilities within the utility right of way prior to the discharge.

Transportation and Site Access

Prior to final adoption of the rezoning bylaw, the developer is required to:

- provide a new road dedication, with a minimum width of 15.0 m, to link the two discontinuous ends of Cavendish Drive through the subject site, and to enter into a Servicing Agreement for the design and construction of a new emergency vehicle access/greenway within the road dedication. The exact road dedication and emergency vehicle access configurations are to be confirmed with survey information to be submitted by the applicant at Servicing Agreement stage;
- register a 6.0 m wide PROP (Property Right-of-Passage) SRW (Statutory Right-of-Way) on Title and enter into a Servicing Agreement for the design and construction of a new pedestrian access walkway along the south property line to provide legal means of public access between No.1 Road and Cavendish Drive;
- dedicate an approximately 0.4 m wide road across the entire No. 1 Road frontage to accommodate the required frontage improvements including a new sidewalk and grass and treed boulevard; and
- provide a vehicle access to the proposed townhouse development on No. 1 Road.

Tree Retention and Replacement

The applicant has submitted a Certified Arborist's Report; which identifies on-site and off-site tree species, assesses tree structure and condition, and provides recommendations on tree retention and removal relative to the proposed development. The Report assesses 61 bylaw-sized trees on the subject development site and seven trees on neighbouring properties.

The City's Tree Preservation Coordinator has reviewed the Arborist's Report and supports the Arborist's findings, with the following comments:

- A 140 cm caliper Sequoia tree (specifically tag# 33) and a 56 cm caliper Spruce tree (specifically tag# 34) located on the development site are in excellent condition and should be retained and protected.
- 59 trees (specifically tag# 1-32 & 35-61) located on the development site either dead, dying (sparse canopy foliage), have been previously topped or exhibit structural defects such as cavities at the main branch union and co-dominant stems with inclusions or are in conflict with the development. As a result, these trees are not good candidates for retention and should be replaced. A Tree Removal Permit (T2 19-875281) has already been issued for a dead (hazardous) Birch tree located on site.
- Seven trees located on neighbouring properties and city's property (specifically tag# OS1-OS3 on 10222 No. 1 Road, tag# OS4 on 4080 Cavendish Drive, tag# OS 5 on City's property, and

tag# OS6-OS7 on 4039 Cavendish Drive) are to be protected as per City of Richmond Tree Protection Information Bulletin Tree-03.

Tree Replacement

The applicant wishes to remove 59 on-site trees. The 2:1 replacement ratio would require a total of 118 replacement trees. According to the Preliminary Landscape Plan provided by the applicant (Attachment 14), the applicant proposes to plant 62 new trees on-site. The size and species of replacement trees will be reviewed in detail through Development Permit and overall landscape design. The applicant has agreed to provide a voluntary contribution of $42,000 ($750/tree) to the City's Tree Compensation Fund in lieu of planting the remaining 56 replacement trees should they not be accommodated on the site.

Tree Protection

Two trees on-site and seven trees on neighbouring properties are to be retained and protected. The applicant has submitted a tree protection plan showing the trees to be retained and the measures taken to protect them during development stage (Attachment 15). To ensure that the trees identified for retention are protected at development stage, the applicant is required to complete the following items:

- Prior to final adoption of the rezoning bylaw, submission to the City of a contract with a Certified Arborist for the supervision of all works conducted within or in close proximity to tree protection zones. The contract must include the scope of work required, the number of proposed monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the Arborist to submit a post-construction impact assessment to the City for review.
- Prior to final adoption of the rezoning bylaw, submission to the City of a Tree Survival Security in the amount of $20,000 to ensure that the140 cm caliper Sequoia tree (specifically tag# 33) and the 56 cm caliper Spruce tree (specifically tag# 34), both identified for retention, will be protected. No Tree Survival Security will be returned until the post-construction assessment report, confirming the protected trees survived the construction, prepared by the Arborist, is reviewed by staff.
- Prior to demolition of the existing dwellings on the subject development site, installation of tree protection fencing around all trees to be retained. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and remain in place until construction and landscaping on-site is completed.

Affordable Housing Strategy

Consistent with the City's Affordable Housing Strategy, the applicant has proposed to provide a secondary suite in each of the two single family dwellings proposed at the subject site, for a total of two suites. Each secondary suite will contain a two bedrooms, with minimum suite sizes of 64 m^2 (689 ft^2) each. Prior to final adoption of the rezoning bylaw, the applicant must register a legal agreement on Title to ensure that no final Building Permit inspection is granted until a two-bedroom secondary suite is constructed on both of the two future lots, to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw.

Public Art

In response to the City's Public Art Program (Policy 8703), the applicant will provide a voluntary contribution at a rate of $0.85 per buildable square foot (2018's rate) at the proposed townhouse development to the City's Public Art Reserve fund; for a total contribution in the amount of $36,669.58.

Energy Step Code

This development application is subject to the Energy Step Code. Applicants are expected to conduct energy modelling early on as part of their development plans to confirm that their proposed design is able to meet the requirements of BC Energy Step Code that will be in place at the time of their Building Permit application. Attached is a statement from the applicant acknowledging that the proposed townhouse development will comply with this requirement (Attachment 16).

Amenity Space

The applicant is proposing a cash contribution in-lieu of providing the required indoor amenity space on the townhouse site, as per the OCP. Based on the rate identified in the OCP (i.e., $1,600 per unit for the first 19 units, plus $3,200 per unit for the 20th to 35th unit), the total cash contribution required for the 35 unit townhouse development is $81,600.00.

Outdoor amenity space will be provided on the townhouse site. Based on the preliminary design, the total area of the proposed outdoor amenity spaces complies with the Official Community Plan (OCP) requirements (i.e., 6 m^2 of outdoor space per unit). Staff will work with the applicant at the Development Permit stage to ensure the configurations and designs of the outdoor amenity spaces meet the Development Permit Guidelines in the OCP, including provision of children's play equipment.

Site Servicing and Frontage Improvements

Prior to final adoption of the rezoning bylaw, the applicant is required to enter into the City's standard Servicing Agreement to design and construct a new public walkway along the south property line of the site between No. 1 Road and Cavendish Drive, a new emergency access/greenway to connect the two discontinuous ends of Cavendish Drive, frontage beautification works on the road frontages, as well as water, storm sewer and sanitary sewer upgrades and service connections along both No. 1 Road and Cavendish Drive (see Attachment 17 for details). All works are at the client's sole cost (i.e., no credits apply). The developer is also required to pay Development Cost Charges (DCC's) (City & GVS & DD), Translink DCC's, School Site Acquisition Charge and Address Assignment Fee.

Financial Impact or Economic Impact

As a result of the proposed development, the City will take ownership of developer contributed assets such as roadworks, waterworks, storm sewers, sanitary sewers, street lights, street trees and traffic signals. The anticipated operating budget impact for the ongoing maintenance of these assets is $5,500.00. This will be considered as part of the 2020 Operating budget.

Conclusion

The purpose of this application is to rezone the subject site to permit a 35 unit townhouse development on the west side of the new Cavendish Drive Connection and a two-lot single family residential subdivision on the east side of the new Cavendish Drive Connection. The proposal will provide a total of 42 residential units including six Low End Market Rental (LEMR) units, 29 townhouse units, two single family dwellings, and five secondary suites (two units as part of the single family development and three units as part of the townhouse development).

The proposal is consistent with the land use designation in the 2041 Official Community Plan (OCP) (i.e., "Neighbourhood Residential"). The proposed townhouse development is generally consistent with the Arterial Road Land Use Policy for townhouses. The conceptual development plans attached are generally consistent with all applicable OCP design guidelines, and would be further refined in the Development Application review process.

The application includes the significant benefit of six affordable housing units, which will be secured through a restrictive covenant and a Housing Agreement at the Development Permit stage.

The list of Rezoning Considerations, which must be completed by the applicant prior to adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 10156, is included in Attachment 17.

It is recommended that Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 10155 and Richmond Zoning Bylaw 8500, Amendment Bylaw 10156, be introduced and given First Reading.

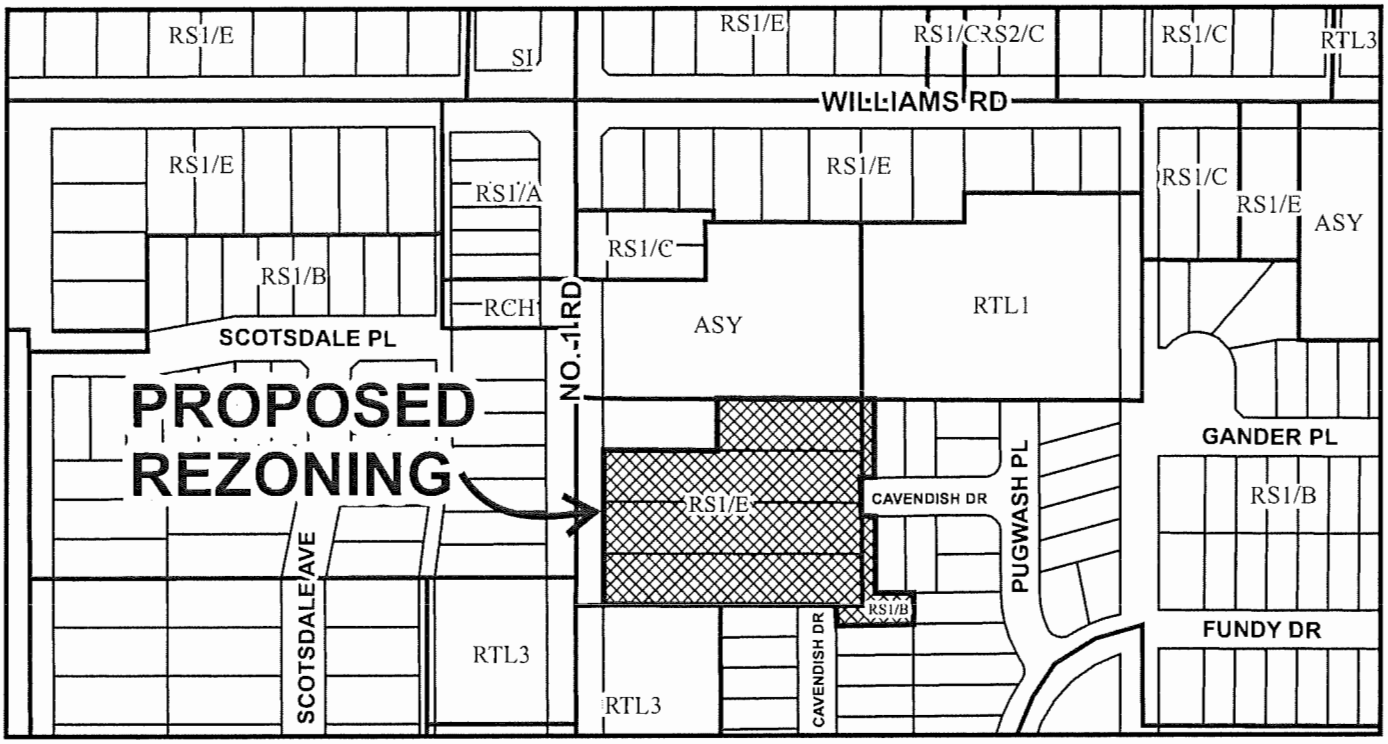
Edwin Lee
Planner 2

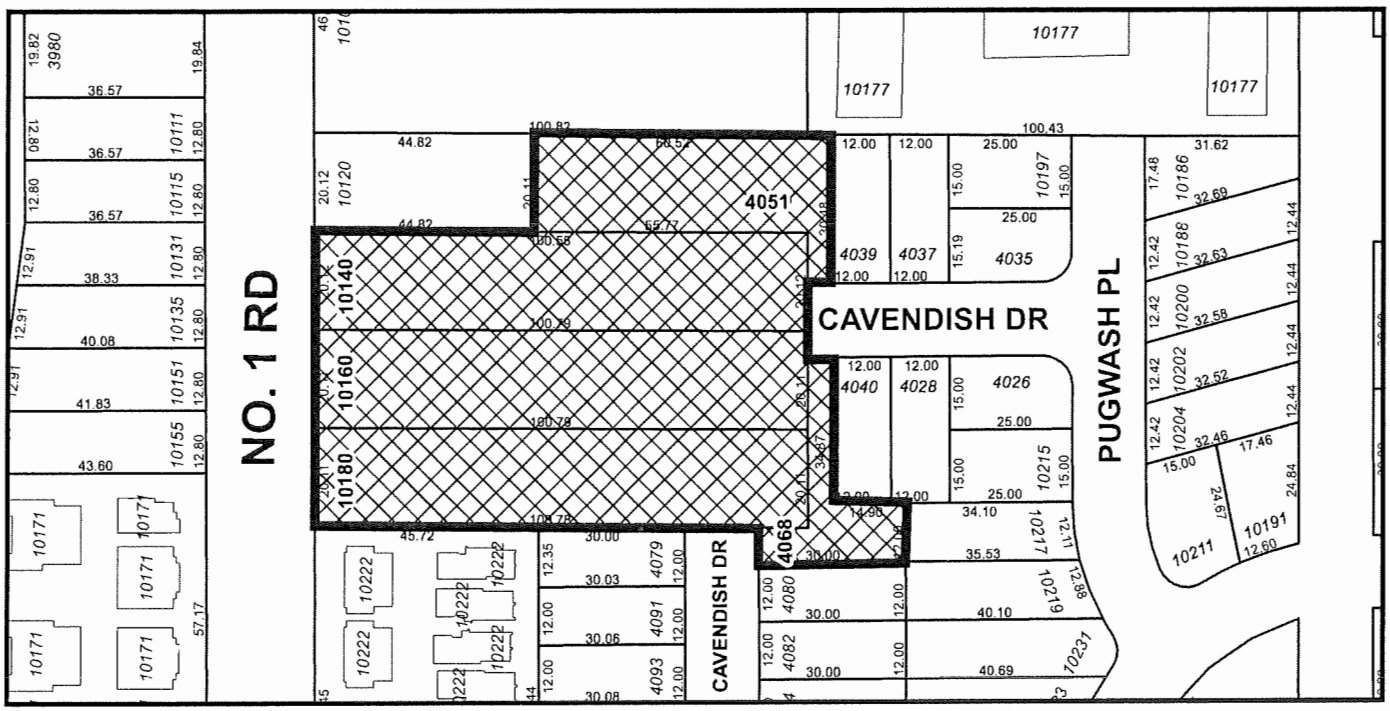
EL:cas

Attachment 1: Location Map
Attachment 2: Conceptual Development Plan
Attachment 3: Preliminary Design of Cavendish Drive Connection Emergency Access/Greenway
Attachment 4: Development Application Data Sheet – Proposed Townhouse Development
Attachment 5: Development Application Data Sheet – Proposed Single Family Subdivision
Attachment 6: Steveston Area Plan
Attachment 7: Proposed Land Use Designation Amendment
Attachment 8: Lot Size Policy 5426
Attachment 9: Notification Area – Open Houses
Attachment 10: Open House Summary – 2018
Attachment 11: Open House Summary – 2019
Attachment 12: Preliminary Development Plans - Proposed Single Family Subdivision
Attachment 13: Shadow Study - Proposed Single Family Subdivision
Attachment 14: Preliminary Development Plans - Proposed Townhouse Development
Attachment 15: Tree Management Plan
Attachment 16: Statement regarding BC Energy Step Code
Attachment 17: Rezoning Considerations

ATTACHMENT 1

City of Richmond





RZ 18-820669

Original Date: 05/11/18

Revision Date:

Note: Dimensions are in METRES

City of
Richmond

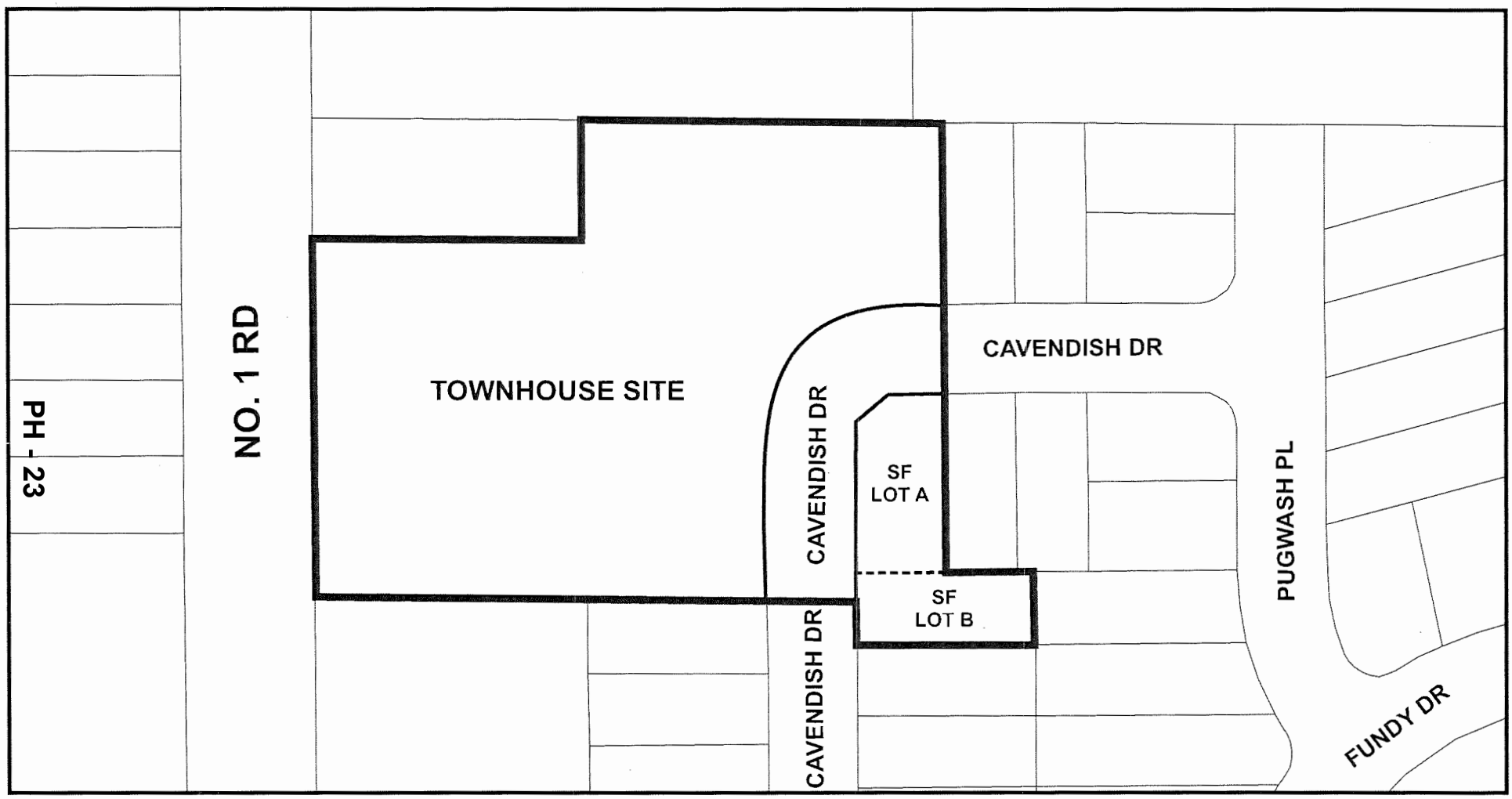
RZ 18-820669

Original Date: 05/11/18

Revision Date:

Note: Dimensions are in METRES

City of Richmond



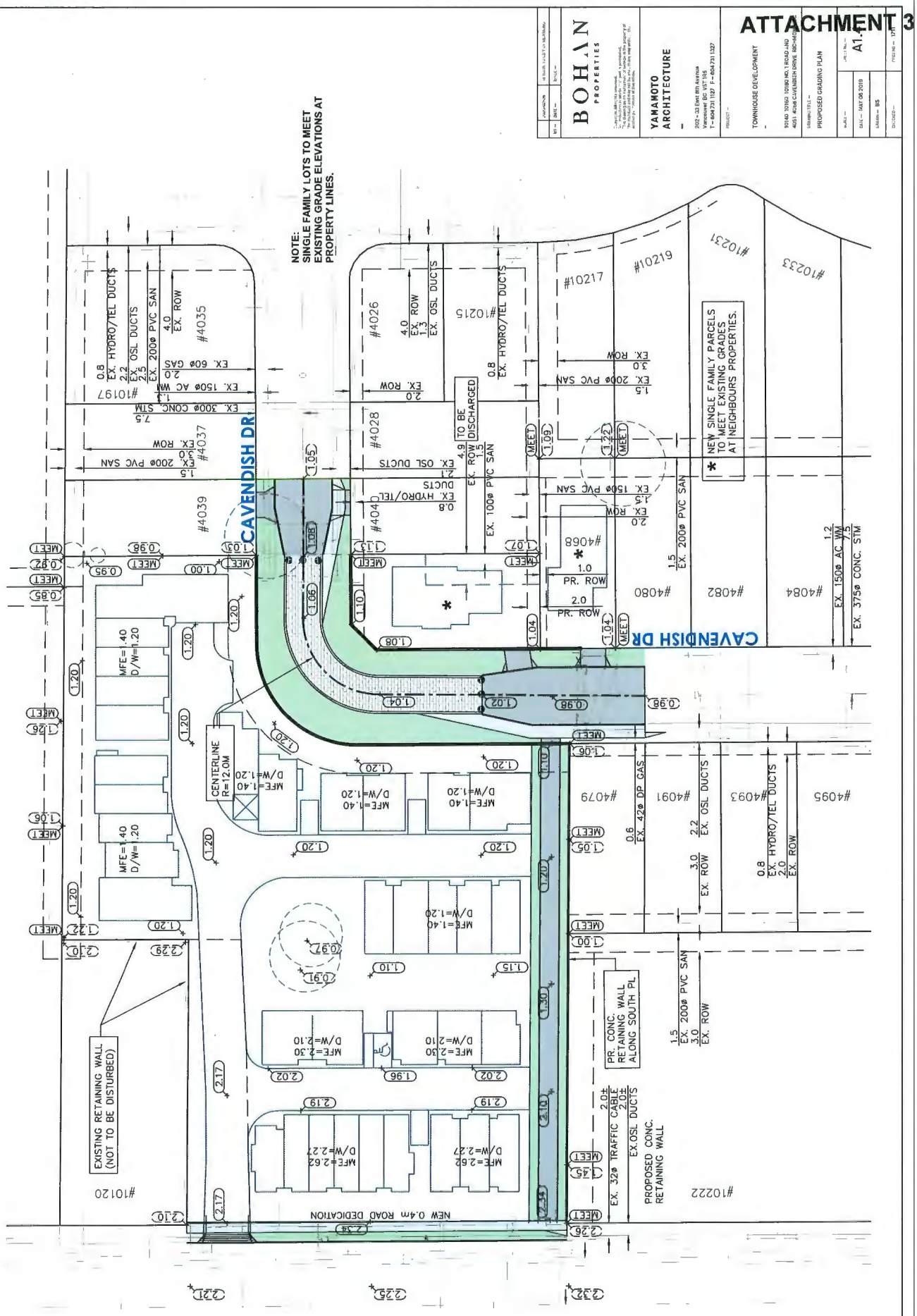
"Proposed Development at 10140, 10160 and 10180 No. 1 Road and 4051 and 4068 Cavendish Drive"

Original Date: 03/19/20

Revision Date:

Note: Dimensions are in METRES

ATTACHMENT 2


BOHAN PROPERTIES
YAMAMOTO ARCHITECTURE
TOWNHOUSE DEVELOPMENT
PROPOSED GRADING PLAN
A1.1
NOTE:
SINGLE FAMILY LOTS TO MEET
EXISTING GRADE ELEVATIONS AT
PROPERTY LINES.
CAVENDISH DR
NEW SINGLE FAMILY PARCELS
TO MEET EXISTING GRADES
AT NEIGHBOURS PROPERTIES.
EXISTING RETAINING WALL
(NOT TO BE DISTURBED)
PR. CONC.
RETAINING WALL
ALONG SOUTH PL
PROPOSED CONC.
RETAINING WALL
NEW 0.4m ROAD DEDICATION
CENTERLINE
R=12.0M
TO BE
DISCHARGED
#10197
#10120
#10222
#4035
#4037
#4039
#4026
#4028
#4040
#10215
#10217
#10219
#10231
#10233
#4068
#4080
#4082
#4084
#4079
#4091
#4093
#4095

City of Richmond

Development Application Data Sheet

Development Applications Department

RZ 18-820669 **Attachment 4**

Address: 4051 Cavendish Drive and the West Portions of 10140, 10160 & 10180 No. 1 Road

Applicant: Yamamoto Architecture Inc.

Planning Area(s): Steveston

	Existing	Proposed
Owner:	1050651 BC Ltd.	No Change
Site Size (m^2):	7,803 m^2 (Combined with SF site)	6,166 m^2
Land Uses:	Single Family Residential	Multiple-Family Residential
OCP Designation:	Low-Density Residential	No Change
Area Plan Designation:	Steveston Area Plan: Single Family / Multiple-Family	Steveston Area Plan: Multiple-Family
702 Policy Designation:	Policy 5426 – Single Detached (RS2/B) or (RS2/G)	No Change
Zoning:	Single Detached (RS1/B) & Single Detached (RS1/E)	Town Housing (ZT88) - No. 1 Road (Steveston)
Number of Units:	3	35
Other Designations:	N/A	No Change

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.65	0.65 Max.	none permitted
Lot Coverage – Building:	Max. 40%	40% Max.	none
Lot Coverage – Non-porous Surfaces:	Max. 65%	65% Max.	none
Lot Coverage – Landscaping:	Min. 25%	25% Min.	none
Setback – No. 1 Road (m):	Min. 4.5 m	4.5 m Min.	none
Setback – Cavendish Drive (m):	Min. 4.5 m	4.5 m Min.	none
Setback – North Side Yard (m):	Min. 3.0 m	3.0 m Min.	none
Setback – South Side Yard (m):	Min. 3.0 m	3.0 m Min.	none
Height (m):	Max. 12.0 m (3 storeys)	12.0 m (3 storeys) Max.	none
Lot Width:	Min. 50.0 m	60 m	none
Lot Depth:	Min. 35.0 m	106 m	none
Off-street Parking Spaces – Residential:	2 spaces per strata + 1 space per LEMR = 64	64	none

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Off-street Parking Spaces – Visitor:	0.2 spaces per unit = 7	7	none
Off-street Parking Spaces – Total:	71	71	none
Tandem Parking Spaces:	Max. 50% of proposed residential spaces in enclosed garages (64 x Max. 50% = 32)	32	none
Small Car Parking Spaces	Max. 50% when 31 or more spaces are provided on-site (71 x Max. 50% = 35)	2	none
Handicap Parking Spaces:	Min. 2% when 11 or more spaces are required (71 x 2% = 2 spaces)	2	none
Bicycle Parking Spaces – Class 1 / Class 2:	1.25 (Class 1) and 0.2 (Class 2) per unit	1.8 (Class 1) and 0.2 (Class 2) per unit	none
Off-street Parking Spaces – Total:	44 (Class 1) and 7 (Class 2)	64 (Class 1) and 7 (Class 2)	none
Amenity Space – Indoor:	Min. 70 m² or Cash-in-lieu	Cash-in-lieu	none
Amenity Space – Outdoor:	Min. 6 m² x 35 units = 210 m²	255 m²	none

Other: Tree replacement compensation required for removal of bylaw-sized trees.

City of Richmond

Development Application Data Sheet

Development Applications Department

RZ 18-820669 **Attachment 5**

Address: 4068 Cavendish Drive and the East Portions of 10160 & 10180 No. 1 Road

Applicant: Yamamoto Architecture Inc.

Planning Area(s): Steveston

	Existing	Proposed
Owner:	1050651 BC Ltd.	No Change
Site Size (m^2):	7,803 m^2 (Combined with TH site)	444 m^2 & 360 m^2
Land Uses:	Single Family Residential	No Change
OCP Designation:	Low-Density Residential	No Change
Area Plan Designation:	Steveston Area Plan: Single Family	No Change
702 Policy Designation:	Policy 5426 – Single Detached (RS2/B)	No Change
Zoning:	Single Detached (RS1/B) & Single Detached (RS1/E)	Single Detached (RS2/B)
Number of Units:	0	2
Other Designations:	N/A	No Change

On Future Subdivided Lots	Bylaw Requirement		Proposed		Variance
Floor Area Ratio:	Max. 0.55 for lot area up to 464.5 m^2		0.55		none permitted
Buildable Floor Area (m^2):*	Lot A: Max. 244 m^2 (2,628 ft^2) Lot B: Max. 198 m^2 (2,131 ft^2)		Lot A: Max. 221 m^2 (2,398 ft^2) Lot B: Max. 197 m^2 (2,131 ft^2)		none permitted
Lot Coverage (% of lot area):	Building: Max. 45% Non-porous: Max. 70% Landscaping: Max. 25%		Building: Max. 45% Non-porous: Max. 70% Landscaping: Max. 25%		none
Lot Size:	360 m^2		Lot A: 444 m^2 Lot B: 360 m^2		none
Lot Dimensions (m):	Lot A Width: 14.0 m Depth: 24.0 m	Lot B Width: 12.0 m Depth: 24.0 m	Lot A Width: 14.72 m Depth: 30.01 m	Lot B Width: 12.26 m Depth: 29.98 m	none
Setbacks (m):	Front: Min. 6.0 m Side: Min. 1.2 m Exterior Side: Min. 3.0 m Rear – 1st Floor: Lot A: Min. 6.0 m Lot B: Min. 6.0 m Rear – 2nd Floor: Lot A: Min. 7.46 m Lot B: Min. 6.0 m		Front: 6.0 m Min. Side: 1.2 m Min. Exterior Side: 3.0 m Min. Rear – 1st Floor: Lot A: 6.0 m Min. Lot B: 6.0 m Min. Rear – 2nd Floor: Lot A: 7.46 m Min. Lot B: 6.0 m Min.		none

6282428

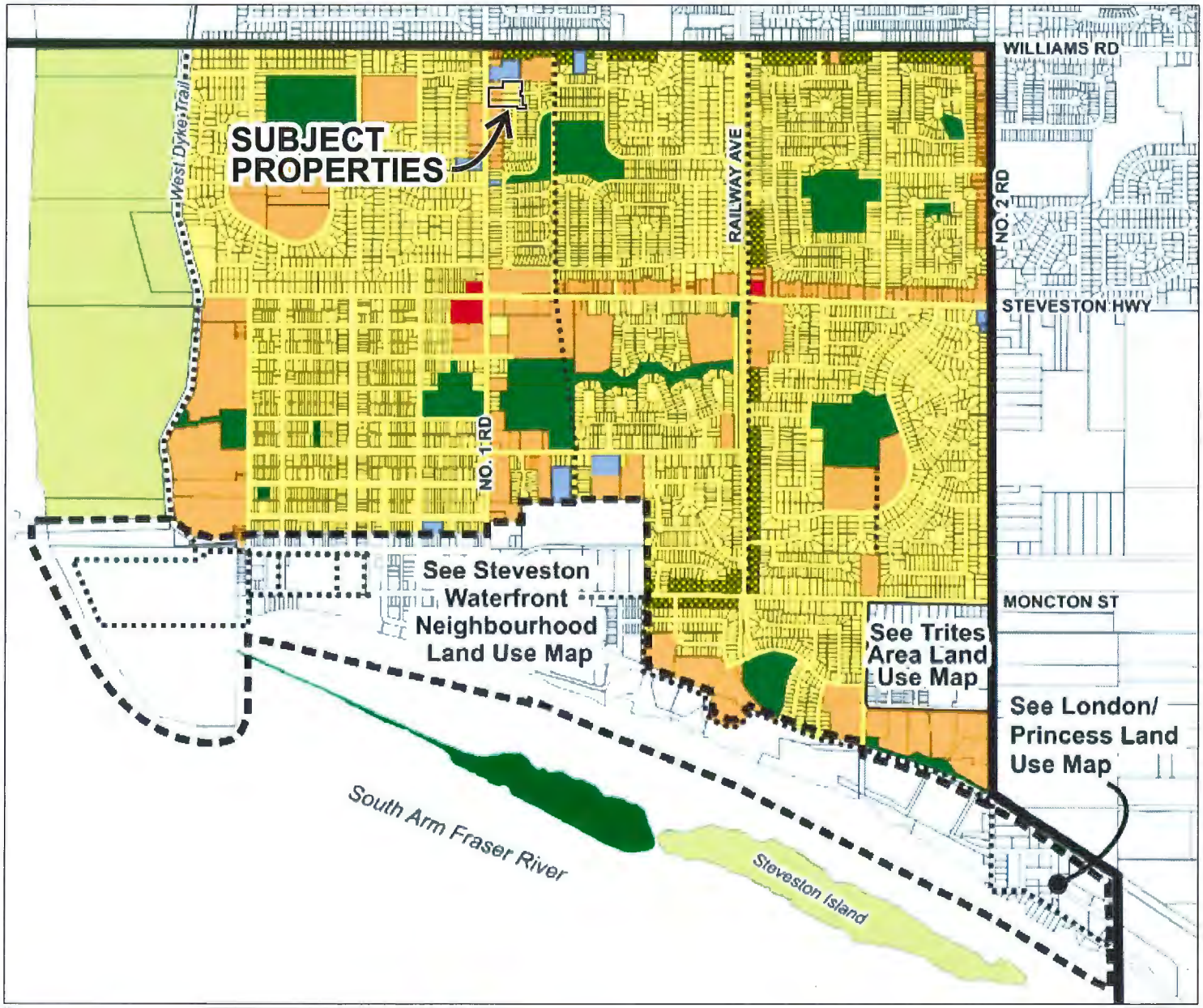
On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Height (m):	Max. 2 ½ Storeys	2 Storeys	none

Other: Tree replacement compensation required for removal of bylaw-sized trees.

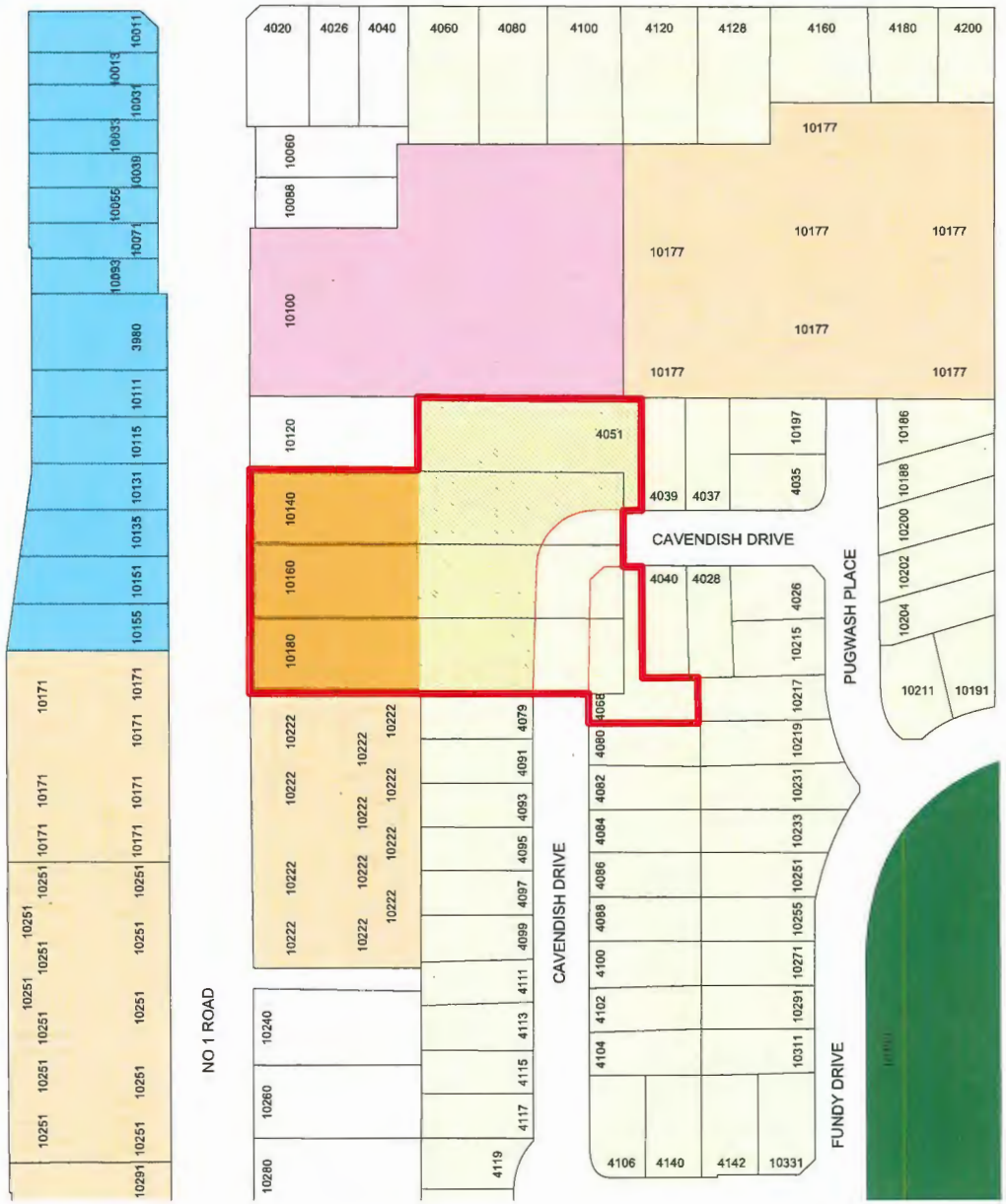
* Preliminary estimate; not inclusive of garage; exact building size to be determined through zoning bylaw compliance review at Building Permit stage.

Steveston Area Land Use Map

Bylaw 9813
2019/06/24



Single-Family	Institutional
Single-Detached/Duplex/Triplex	Conservation Area
Multiple-Family	Trail
Commercial	Steveston Area Boundary
Public Open Space	Steveston Waterfront Neighbourhood Boundary



BOHAN PROPERTIES

YAMAMOTO ARCHITECTURE

202 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO. 1 RD
4051 4068 CAVENDISH DRIVE

DRAWING TITLE –
OCP CONTEXT

SCALE – N.T.S.
DATE – MAY 06 2019
DRAWN – BS

ATTACHMENT 7

POLICY 5426:

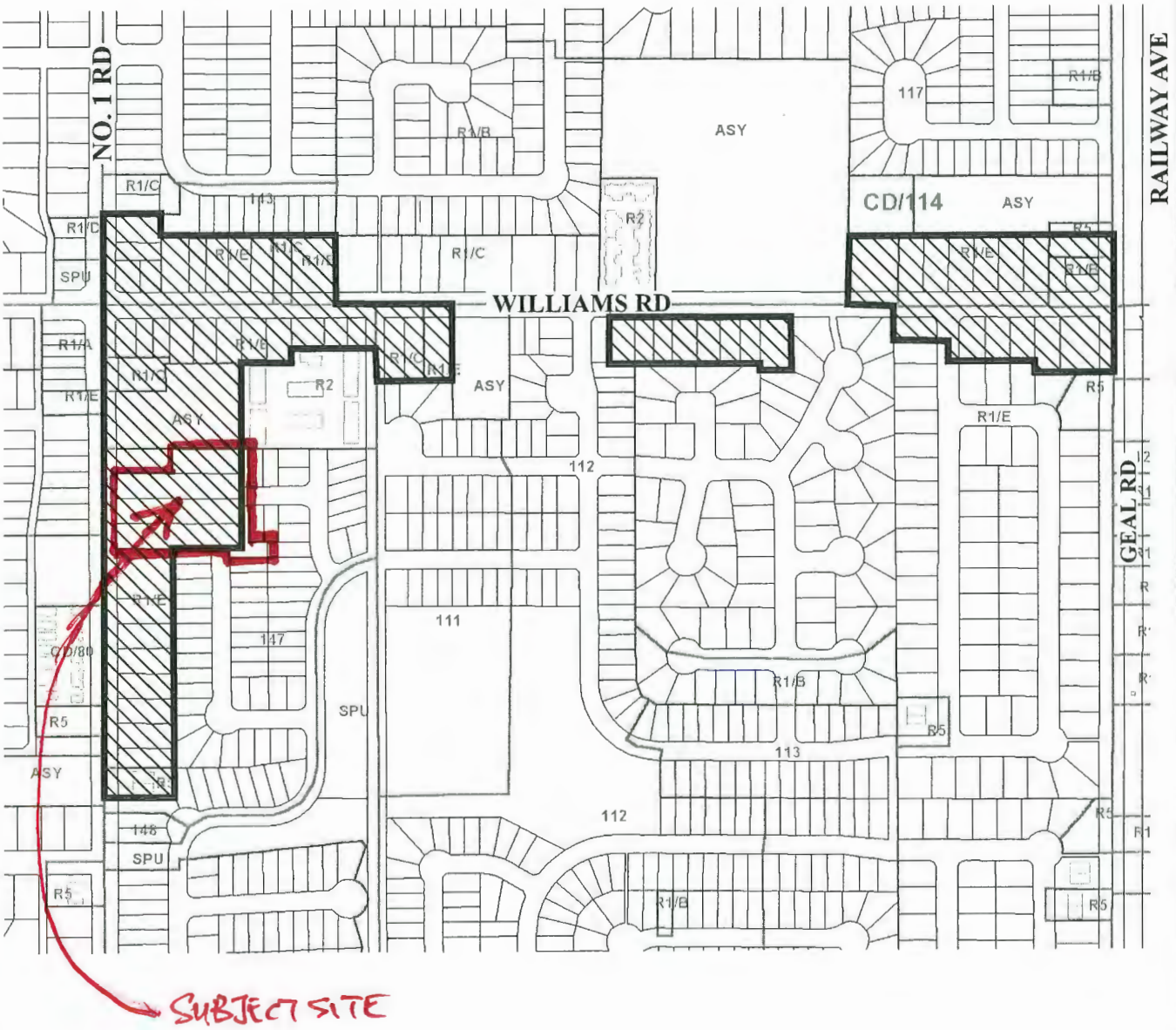
The following policy establishes lot sizes for properties within the area located on **Williams Road, No. 1 Road and Geal Road**, in a portion of Section 26-4-7/35-4-7:

That properties within the area located on Williams Road, No. 1 Road and Geal Road, in a portion of Section 26-4-7/35-4-7, be permitted to subdivide in accordance with the provisions of Single-Family Housing District (R1/B) in Zoning and Development Bylaw 5300, with the following provisions:

(a) If there is no lane or internal road access, then properties along No. 1 Road would be restricted to Single-Family Housing District (R1/E).

(b) Properties along Williams Road will be permitted Single-Family Housing District (R1/C) zoning unless there is lane or internal road access in which case Single-Family Housing District (R1/B) would be allowed.

and that this policy, as shown on the accompanying plan, be used to determine the disposition of future rezoning applications in this area, for a period of not less than five years, unless changed by the amending procedures contained in the Zoning and Development Bylaw.



Subdivision permitted as per **R1/B** except

1. Williams Road - **R1/C** unless there is a lane or internal road access then **R1/B**

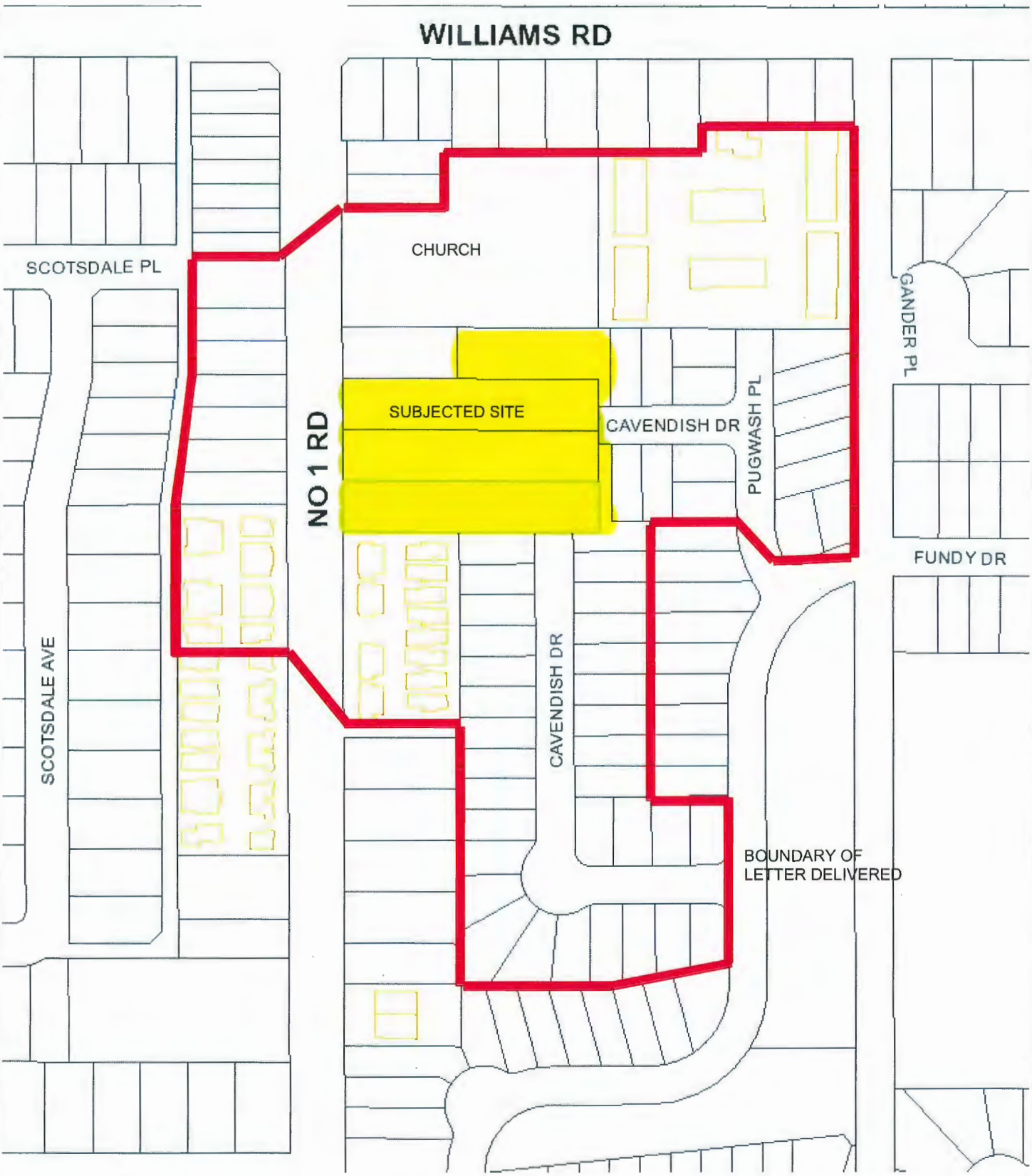
2. No. 1 Road - **R1/G** unless there is a lane or internal road access then **R1/B**.

POLICY 5426
SECTION 26 & 35, 4-7

Adopted Date: 12/18/89

Amended Date:

NOTIFICATION AREA



This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

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Core Concept
CONSULTING LTD.

#220 – 2639 Viking Way
Richmond, BC, V6V 3B7

Phone: 604.249.5040
Fax: 604.249.5041

City of Richmond
6911 No. 3 Road
Richmond, BC, V6Y 2C1

July 20, 2018

Attention: **Edwin Lee**

Reference: **Summary of Public Information Meeting**
10140 – 10180 No. 1 Road and 4051 and 4068 Cavendish Drive
City File: RZ 18-820669

Dear Edwin,

A Public Information Meeting for the proposed 35 unit townhouse and 2 single family lot development located at 10140 – 10180 No. 1 Road and 4051 and 4068 Cavendish Drive (City File RZ18-820669) was held between 5:00pm and 8:00pm on June 20, 2018 at the Richmond Chinese Alliance Church located at 10100 No. 1 Road.

Core Concept Consulting Ltd. prepared a Public Information Meeting invitation including a document outlining the synopsis of the proposed development. The invitation packages were hand-delivered by Core Concept Consulting staff to the residences in the vicinity of the proposed development during the period of June 5th and June 10th. Please refer to Appendix A for the Public Information Invitation Package and Appendix B for a map defining the notice distribution area.

There are 53 single family homes and 53 townhome residences and the church in the notice distribution area. 14 residences in the notice distribution area attended the Public Information Meeting (13%). There was one representative from the church who also attended the Public Information Meeting.

Attendees of the meeting were greeted upon entry and encouraged to sign the attendance sheet for the meeting. 32 attendees were formally recorded on the attendance sheet but several signatures represented households with multiple household members in attendance. We estimate a total turnout of 40 people in attendance during the course of the meeting – not counting City Staff, the Developer, or his consultants.

The Attendees were free to examine a series of presentation boards and Mr. Steven Yang (Developer), Taizo Yamamoto (Architect) and David Kozak and David Lu (Civil) were available to address any questions raised by the attendees in either small informal groups or one-on-one as preferred by the Attendee. Please refer to Appendix C for the Presentation Boards displayed.

Each participant was provided a feedback form that they could complete at the meeting or which they could take home and complete at their leisure. As

of July 11th we have compiled 11 feedback forms and 3 independent emails. A table summarizing each of the feedback forms received and our synopsis of the comments received is included in Appendix D. The synopses provided for each feedback form addresses what we interpret to be the key points raised by the Attendee. Not all points are necessarily addressed or identified. The reader should peruse each of the feedback forms to establish their own interpretation of the tone and content of the feedback forms supplied in Appendix E.

For the most part the attendees within the notice distribution area expressed concern over one or two issues that were of most concern to them. In general the responses tended to fall into the following categories (in no particular order).

- Security of the existing residences. Several residences expressed concern that the construction of a public walkway between No. 1 Road and Cavendish Drive will result in increased theft and undesirable activities.
- Increased Noise and Traffic: Several residences indicated a concern that the connection of the Cavendish Drive road ends will result in increased noise and traffic.
- Cavendish Drive Improvements to Pugwash: A couple residents would like the existing roadway and sidewalk improved.
- Tree Preservation: A few residences would like the existing tall trees to be retained.
- Form and Character: Several residences identified that they wanted the two single family homes and the townhomes to suit the existing single family residences on Cavendish Drive.
- Site Grading: A few residences indicated concern that the main floor of the new homes and site grading would be raised out of character with the existing neighbouring properties. A couple residences cited 10533 Fundy Drive as an example of their concerns.
- Sidewalk Configuration: Several residences indicated that they wanted the varying sidewalk configuration between the two sections of Cavendish Drive to be improved but the opinions were split on how best to achieve this.
- Parking: Several residences were concerned about the amount of street side parking for Cavendish Drive. The opinions were split between preferring no parking, to not having enough parking along Cavendish Drive.
- Unit Height: A couple residences objected to 3 story townhome units.

In the next two weeks the project team will be meeting to review the community feedback and determine if the development proposal can be adjusted to suit feedback.

Please contact me if you have any questions.

Core Concept
CONSULTING LTD.

Yours Truly,

Core Concept Consulting Ltd.

David R. Kozak
Senior Project Manager

Core Concept
CONSULTING LTD.

APPENDIX A

PUBLIC INFORMATION INVITATION PACKAGE

Core Concept
CONSULTING LTD.

#220 – 2639 Viking Way
Richmond, BC, V6V 3B7

Phone: 604.249.5040
Fax: 604.249.5041

June 5, 2018

To: **Owner/Occupant**

Subject: **Notice of Public Information Meeting for the Proposed Development of 10140-10180 No. 1 Road & 4051/4068 Cavendish Drive (Rezoning No. 18-820669)**

Dear Neighbour,

Bohan Properties, the owner of the above parcel would like to extend an invitation for you to attend a public information meeting related to a formal rezoning application to the City of Richmond for the above subject properties.

Public Information Meeting

Location:	Richmond Chinese Alliance Church 10100 No. 1 Road, Richmond
Date:	June 20th, 2018
Time:	5:00pm – 8:00pm

This letter summarizes the key aspects of the proposed development and the anticipated impact to the neighbourhood.

The site is located in between No 1 Road on the west, Cavendish Drive on the east, single-family homes on the south, a church and a single-family home on the north side as shown in Figure 1.

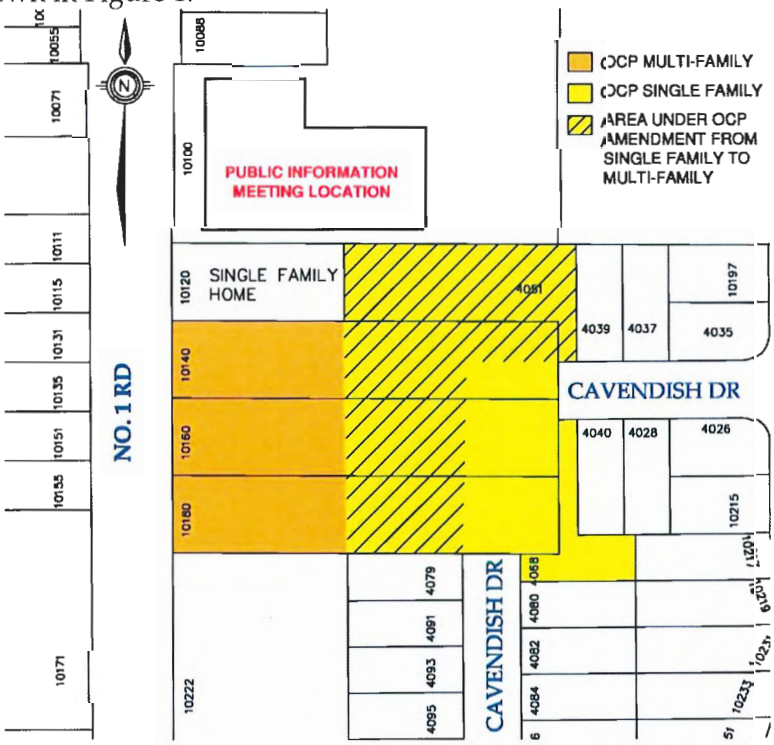


Figure 1. Siteplan & OCP Amendment

Core Concept
CONSULTING LTD.

The proposed development comprises 2 & 3 story townhouses north and west of Cavendish Drive as well as 2 single-family lots to complete the residential subdivision south and east of Cavendish Drive.

The proposed Cavendish Drive road layout will connect the two dead end sections of Cavendish Drive to complete the roadway (please see Figure 2 & 3 attached). This will provide several benefits to the neighbourhood:

- improved vehicular circulation and virtually eliminate the need for turn arounds in private driveways;
- improved fire truck, garbage, and recycling vehicle access and circulation;
- improved fire protection once the watermains in both sections of Cavendish Drive are connected;
- improved security and street lighting compared to the that present in the dead-end streets.

In addition, the development is proposing a public walkway along the south edge of the development between Cavendish Drive and No. 1 Road. We expect that this walkway will improve pedestrian circulation in the neighbourhood as well as access to public transit. The proposed architectural site plan is attached for your reference (please see Figure 4).

Please note that the developer intends for the townhouses of this proposed development to enter and exit the site through No. 1 Road with no vehicular access to Cavendish Drive. Therefore, we expect that the proposed development will not materially change the amount of vehicular traffic in Cavendish Drive, although some existing residents may change their driving patterns and exit north once the road is completed.

This application will proceed through the normal City of Richmond rezoning, subdivision application, and public consultation process. In addition, this application will also require an Official Community Plan (OCP) amendment from single family to multi-family zoning for a portion of the site (see Figure 1). We encourage you to attend the public information meeting where you can have the opportunity to ask questions and provide constructive feedback on a less formal setting. Should you be unable to attend the public information meeting, you can use the attached comments page and send them to the undersigned for consideration.

Please be advised that all comments received will be shared with the City of Richmond for consideration and will become public information. If you have any questions or concern, please feel free to contact Edwin Lee from the City of Richmond at (604) 276 4121 with reference to the Rezoning Number 18-820669.

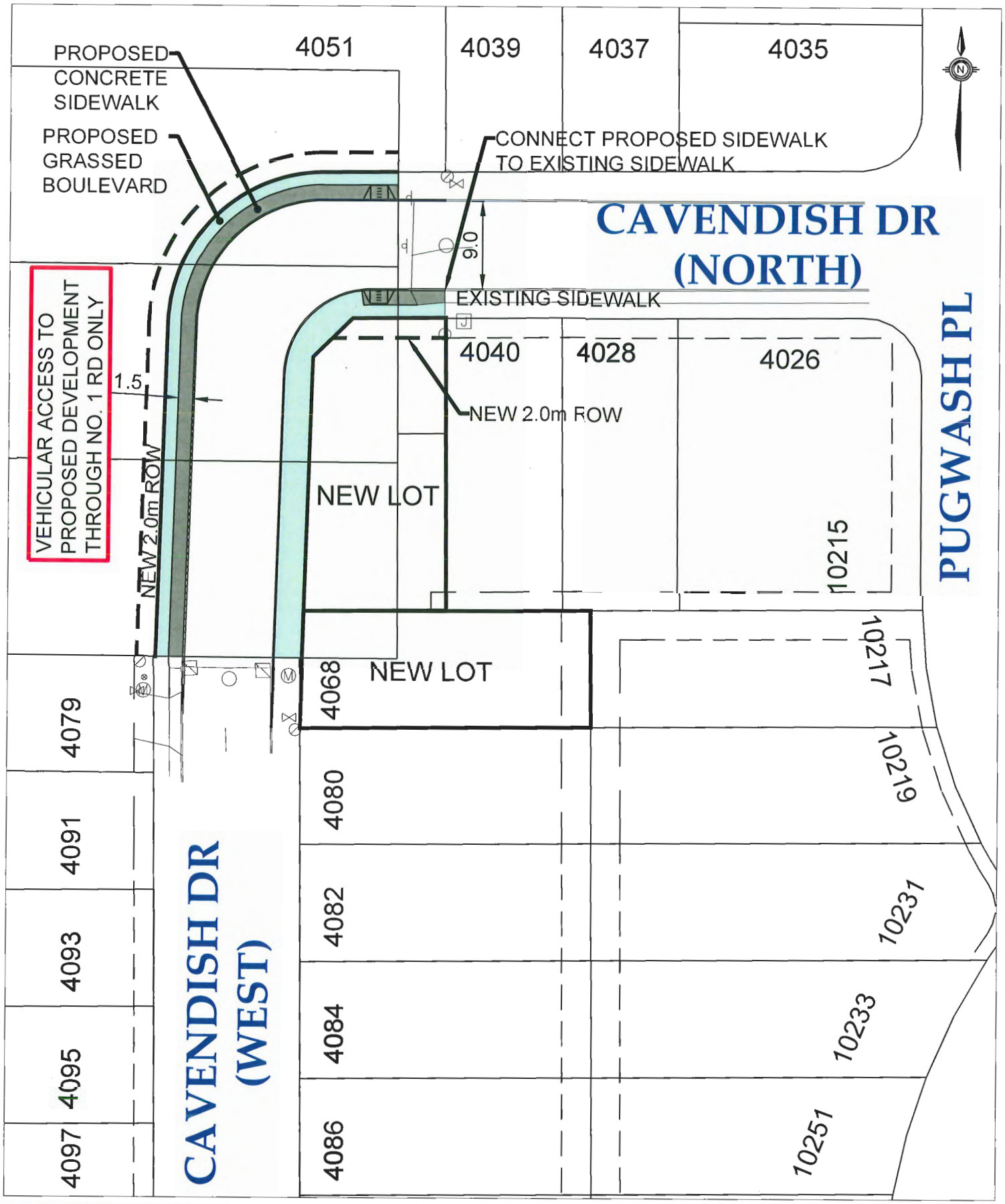
We look forward to seeing you at the public information meeting.

Core Concept
CONSULTING LTD.

Yours Truly,

Core Concept Consulting Ltd.

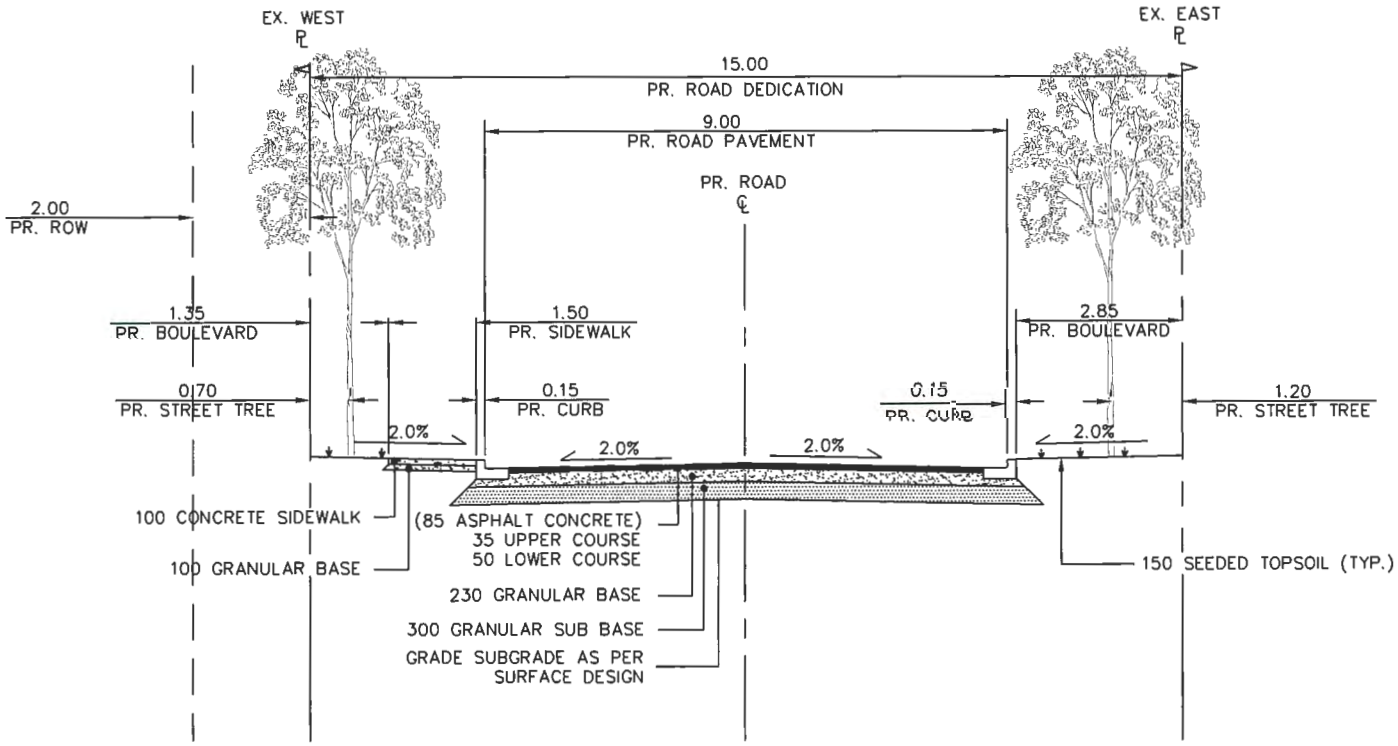
David R. Kozak
Senior Project Manager
Phone: (604) 249 5040 Fax: (604) 249 5041
Email: drkozak@coreconceptconsulting.com



CoreConcept CONSULTING LTD.
#220-2639 Viking Way, Richmond, BC, V6V 3B7
tel : 604.249.5040
fax : 604.249.5041

DATE: 05 JUN 2018
CCC# : 17101

FIGURE 2



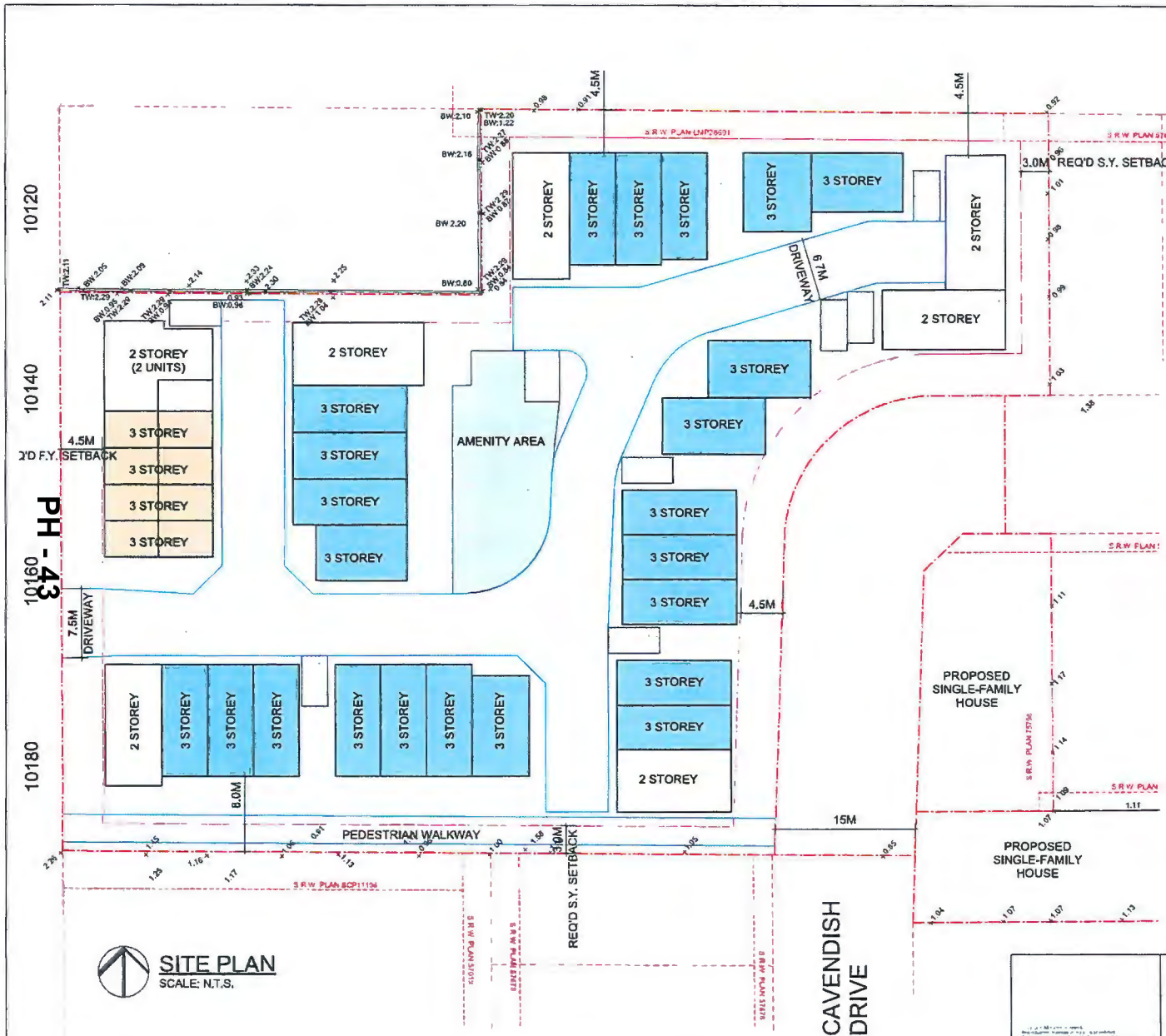
CAVENDISH DRIVE (WEST) PROPOSED TYPICAL SECTION - FACING NORTH

SCALE: 1:125 HOR., NTS. VERT.

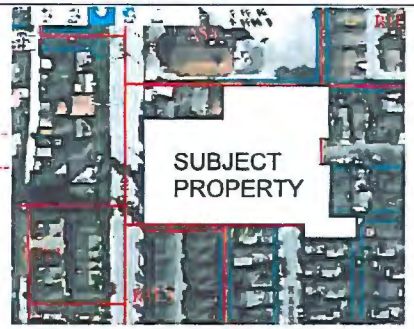
CoreConcept
CONSULTING LTD.
#220-2639 Viking Way, Richmond, BC, V6V 3B7
tel : 604.249.5040
fax : 604.249.5041

DATE: 05 JUN 2018
CCC# : 17101

FIGURE 3



SITE PLAN
SCALE: N.T.S.



CONTEXT PLAN
SCALE: N.T.S.

STATISTICS:

CIVIC ADDRESS: 10140, 10160, 10180 NO.1 ROAD AND 4051, 4068 CAVENDISH DRIVE, RICHMOND

PROPOSED ZONING:

LOT AREA BEFORE DEDICATIONS: 7803 SM (83,994 SQ.FT.)

LOT AREA AFTER DEDICATIONS: 6203 SM (66,780 SQ.FT.)
TWO SINGLE FAMILY LOTS:

PROPOSED DENSITY: 0.75 (50,085 SQ.FT.)
= 35 UNITS (6 LEMR AND 29 MARKET UNITS)

☐ LOW-END MARKET RENTAL UNIT (15% OF FAR - 7512 SQ.FT.)
= 6 UNITS @ 1222 SF AVERAGE

☐ MARKET UNITS
= 29 UNITS @ 1460 SF AVERAGE

PARKING:

REQUIRED :	2.0 SPACES x 29 UNITS	=	58	SPACES (RESIDENTS)
	1.0 SPACES x 6 UNITS	=	6	SPACES (LEMR)
	0.2 SPACES x 35 UNITS	=	7	SPACES (VISTORS)
	TOTAL	=	71	SPACES
PROVIDED :	2 CAR GARAGES x 29 UNITS	=	58	SPACES (RESIDENTS)
	1 CAR GARAGES x 6 UNITS	=	6	SPACES (LEMR)
	OPEN VISTORS PARKING	=	7	SPACES (VISITORS)
	TOTAL	=	71	SPACES

NOTE : 1 ACCESSIBLE PARKING SPACE IS PROVIDED.

REQUIRED BICYCLE	1.25 SPACES x 35 UNITS	=	43.75	SPACES (CLASS 1)
	0.2 SPACES x 35 UNITS	=	7	SPACES (CLASS 2)
	TOTAL	=	51	SPACES
PROVIDED BICYCLE	2 SPACES x 21 GARAGES	=	42	SPACES (CLASS 1)
	1 SPACE x 6 GARAGES	=	6	SPACES (CLASS 1)
	BICYCLE RACK	=	4	SPACES (CLASS 2)
	TOTAL	=	52	SPACES

AMENITY AREA

REQUIRED	INDOOR	MIN. 70 SQ.M. (753 SQ.FT.)
	OUTDOOR	210 SQ.M. (2260 SQ.FT.)
PROVIDED	INDOOR	PAYMENT IN LIEU
	OUTDOOR	210 SQ.M. (2260 SQ.FT.)

PRIVATE OUTDOOR SPACE: MIN. 30 SQ.M. PER UNIT

YAMAMOTO ARCHITECTURE

TOWNHOUSE DEVELOPMENT

SITE PLAN STATISTICS

FIGURE 4

A1.0

RZ18-820669 - DEVELOPMENT APPLICATION FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: _____

Address: _____

Phone (Optional): _____

Email (Optional): _____

Date: _____

Comments:

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

_____

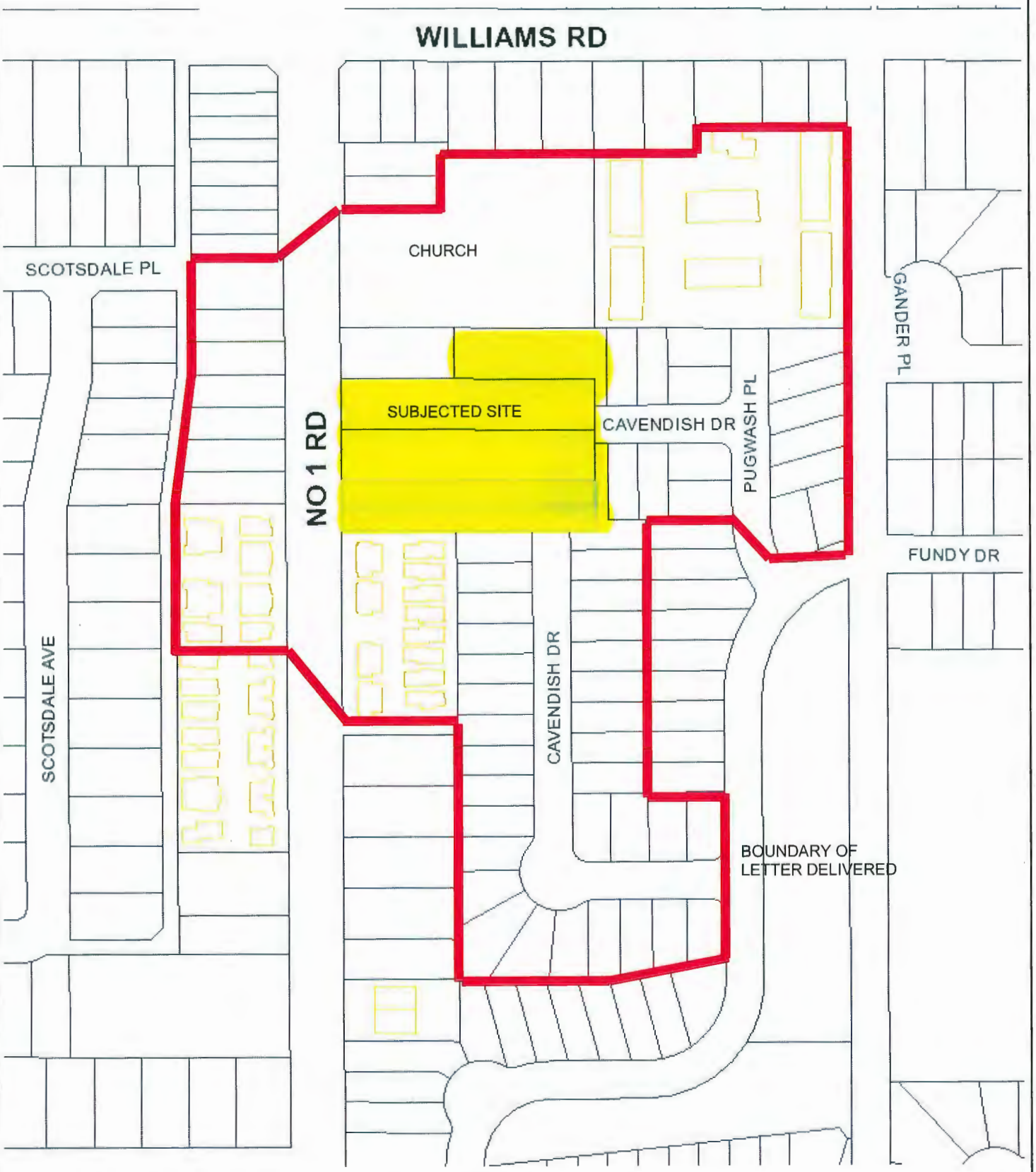
_____

Please Call Me to Discuss: ☐ **Yes (Time: _____)** ☐ **No**
(Please indicate above your preferred date and time)

Please note that a copy of this Feedback Form will be copied to the City

APPENDIX B
NOTICE DISTRIBUTION AREA

City of Richmond Interactive Map



This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

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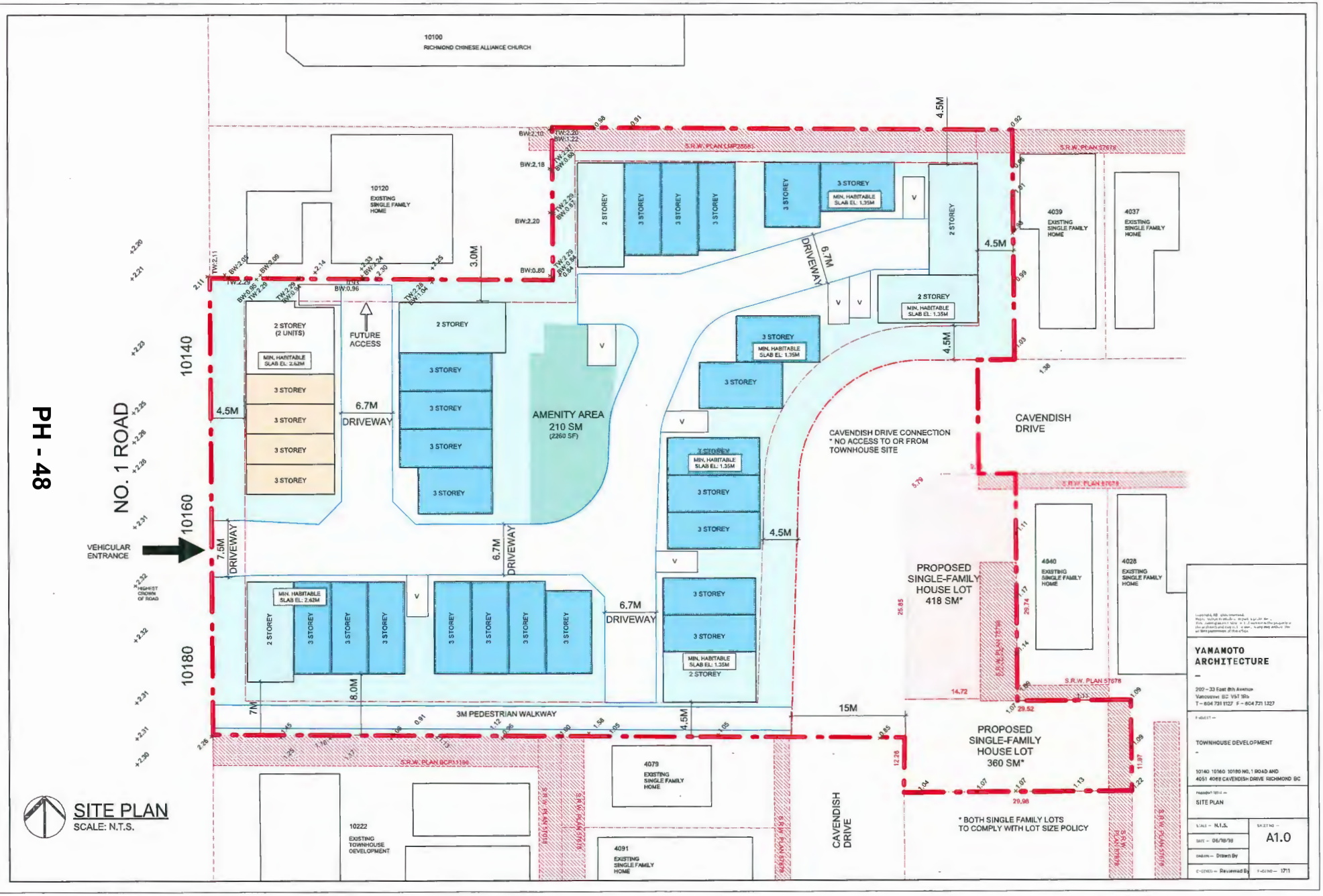
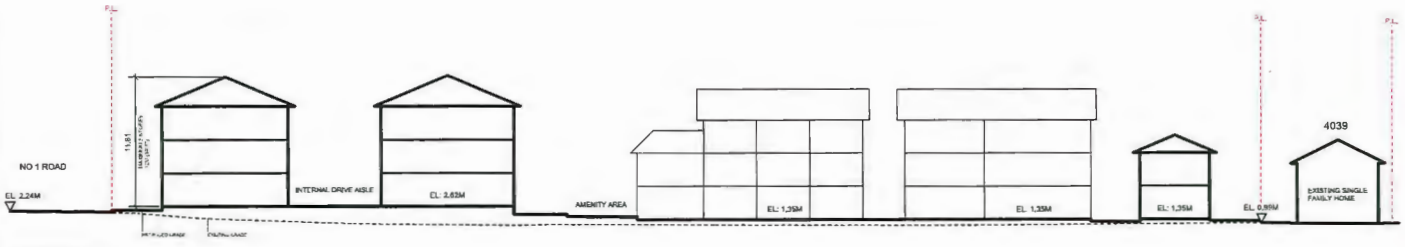
Core Concept
CONSULTING LTD.

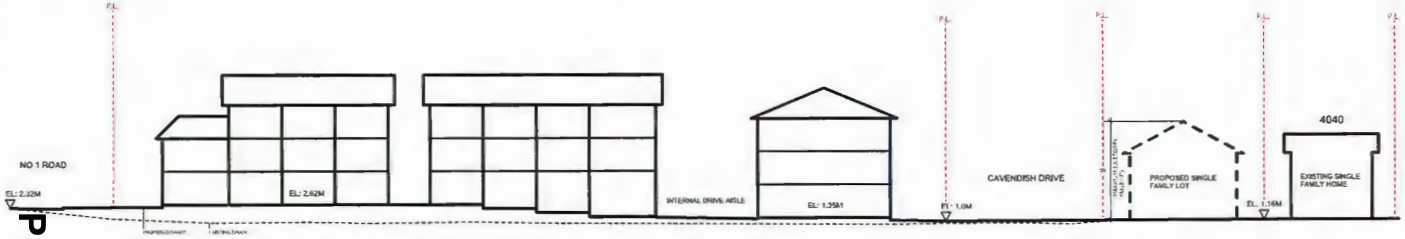
APPENDIX C
PRESENTATION BOARDS



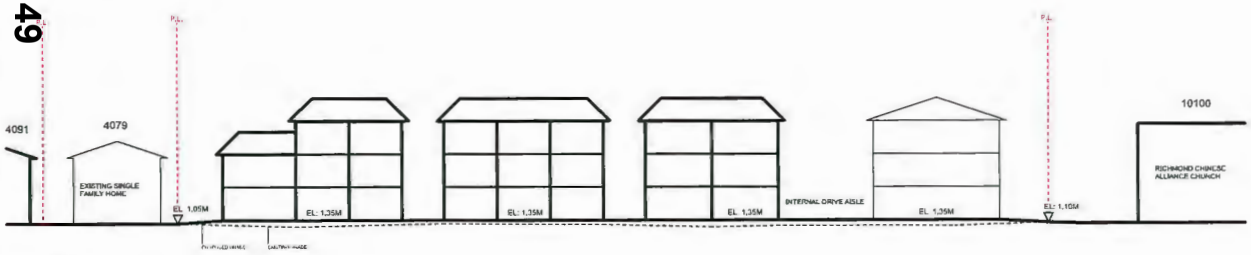
SITE PLAN
SCALE: N.T.S.



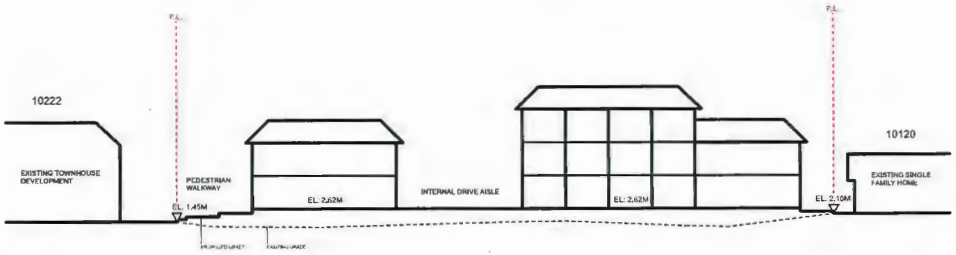
SITE SECTION A-A



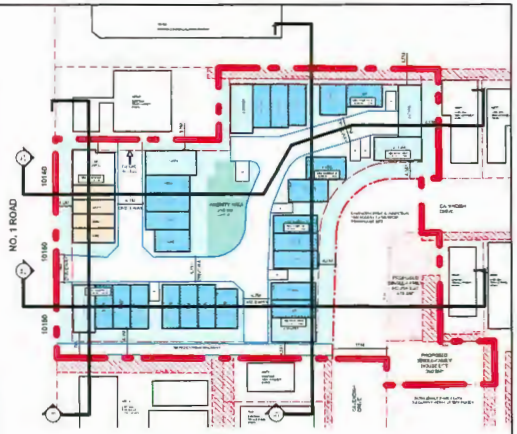
SITE SECTION B-B



SITE SECTION C-C



SITE SECTION D-D



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YAMAMOTO ARCHITECTURE
—
207 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

PROJECT –

TOWNHOUSE DEVELOPMENT
–
10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND BC

DRAWING TITLE –
SITE SECTIONS

SCALE – As.I.S.	SHEET NO. – A1.1
DATE – 06/18/16	
DRAWN – Drawn By	
CHECKED – Reviewed By	PROJECT NO. – 1711

NO 1 ROAD - FACING PROPERTY LINE BETWEEN CHURCH AND 10101

CAVENDISH DRIVE - FACING NORTHWEST

NO 1 ROAD - FACING PROPERTY LINE BETWEEN 10160 AND 10180

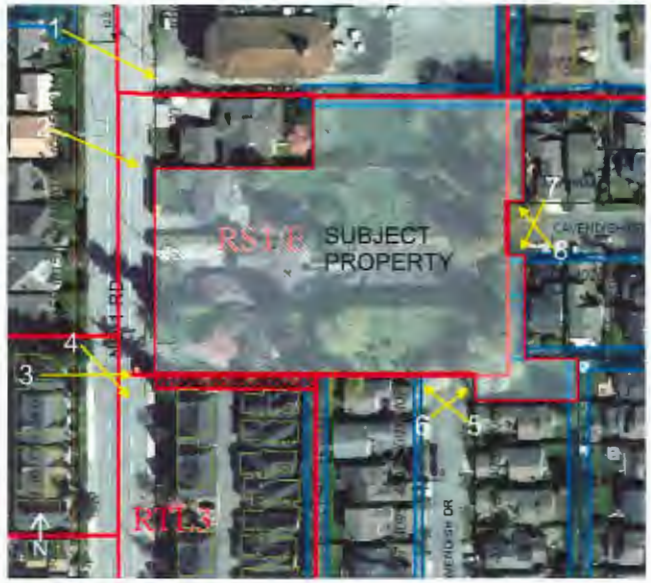
CAVENDISH DRIVE - FACING NORTHEAST

NO 1 ROAD - PROPERTY LINE BETWEEN 10140 AND 10222

CAVENDISH DRIVE - FACING SOUTHWEST

NO 1 ROAD - 10222

CAVENDISH DRIVE - FACING NORTHWEST



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YAMAMOTO ARCHITECTURE

202 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

PROJECT –

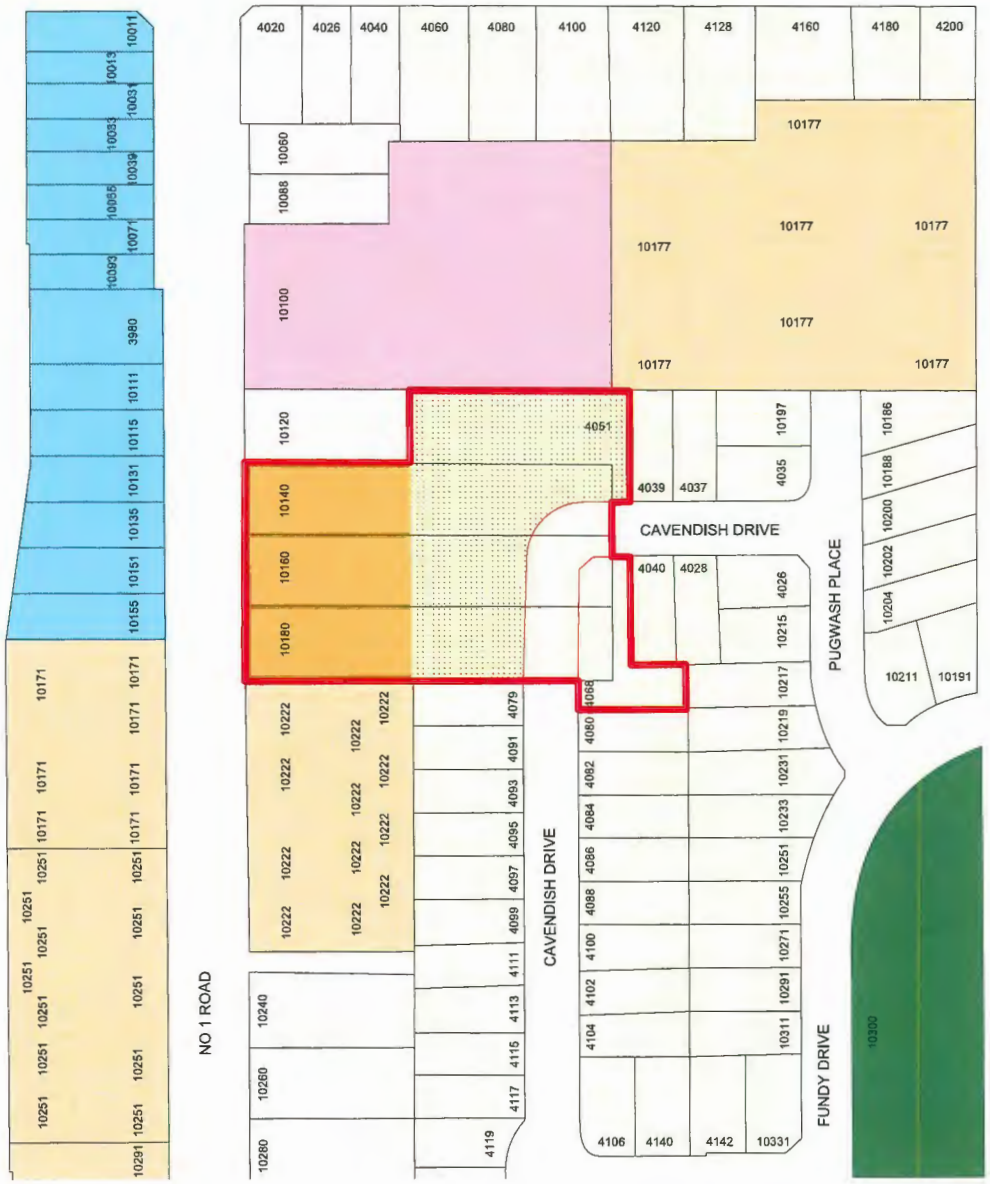
TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND BC

DRAWING TITLE –

SITE PHOTOS

SCALE –	SHEET NO. – A – P
DATE – 06/18/18	
DRAWN – Drawn By	
CHKD – Reviewed By	PROJ NO – 1711



OCP DESIGNATED MULTI- FAMILY - PROPOSED TOWNHOUSES

OCP DESIGNATED MULTI- FAMILY - CURRENT TOWNHOUSES

OCP DESIGNATED MULTI- FAMILY - CURRENT SINGLE FAMILY

OCP DESIGNATED SINGLE FAMILY - CURRENT SINGLE FAMILY

AREA UNDER OCP AMENDMENT FROM SINGLE FAMILY TO MULTI- FAMILY

COMMUNITY INSTITUTIONAL

PARK

ARTERIAL ROAD COMPACT LOT SINGLE DETACHED

OCP LAND USE PLAN

SCALE: N.T.S.

YAMAMOTO ARCHITECTURE

202 - 33 East 8th Avenue
Vancouver BC V5T 1R5
T - 604 731 1127 F - 604 731 1327

TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE, RICHMOND BC

OCP CONTEXT

SCALE — N.T.S.
DATE — 06/18/18
DRAWN — Drawn By
CHECKED — Reviewed By

A - OCP

PROJECT NO — 1711

APPLICABLE CITY POLICIES

RICHMOND 2041 OFFICIAL COMMUNITY PLAN

GOALS:

1. Welcoming and Diverse
   The City is inclusive and designed to support the needs of a diverse and changing population.
2. Connected and Accessilbe
   People are connected to and interact with each other. Places, buildings, and activities are connected and easily accessed by everyone.
3. Valued for its Special Places
4. Adaptable

ARTERIAL ROAD POLICY

"The City supports densification along its arterial roads. The purpose of this densification is to locate developments on arterial road properties in close proximity to commercial services, public amenities, schools, and transit service."

LOT SIZE POLICY

Minimum Lot Size: Subzone RS2/B
= 360 SM
= 3875 SF

2017 - 2027 AFFORDABLE HOUSING STRATEGY LOW END MARKET RENTAL POLICY (LEMR)

"LEMR units are secured as affordable in perpetuity through legal agreements on title, which restricts the maximum rents and tenant eligibility by income. This policy is intended to ensure the development of mixed income communities and provide rental homes for low-moderate income households."

PROJECT SUMMARY

- Connect and complete Cavendish Drive, eliminating dead ends and improving pedestrian and vehicular circulation.
- Improve circulation for emergency vehicles and garbage and recycling vehicles.
- Improve infrastructure by connecting watermains on either side of Cavendish Drive.
- Improve security and passive surveillance by re-routing current walkway through undeveloped lot at 4068 Cavendish Drive onto new, standard City sidewalk and street lighting.
- Improve the Number 1 Road frontage with new sidewalk.
- Create two new single family lots to complete the single family character of the block.
- Minimize increase in traffic along Cavendish by providing all vehicular access to townhouse site from No. 1 Road.
- Provide six Low-End Market Rental units to provide affordable housing options
- Reduce scale of buildings fronting Cavendish to duplexes and triplexes that relate to scale of existing single family homes
- Reduce building heights adjacent to existing single family homes
- Orient windows away from existing single family homes and yards to maximize privacy for current homeowners
- Create pedestrian linkage from Cavendish Drive to No.2 Road.
- Amend OCP to provide additional townhouses in the eastern portion of the site to offset the cost of Cavendish Drive dedication and construction.

YAMAMOTO ARCHITECTURE
–
202 – 33 East 8th Avenue
Vancouver, BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

TOWNHOUSE DEVELOPMENT
-
10140 10160 10180 NO. 1 ROAD AND
4061 4068 CAVENDISH DRIVE, RICHMOND, BC

PROJECT SUMMARY

N.T.S.
Date – 06/18/18
AS

PROJECT STATS

ADDRESS: 10140, 10160, 10180 NO.1 ROAD &
4051, 4068 CAVENDISH DRIVE

SITE AREA BEFORE ROADWAY DEDICATIONS:	= 83,994 SF
TOWNHOUSE SITE AREA AFTER DEDICATIONS:	= 66,780 SF
SINGLE FAMILY LOT 1 AREA:	= 4500 SF*
SINGLE FAMILY LOT 2 AREA:	= 3875 SF*

Both Single Family Lots to comply with Lot Size Policy

PROPOSED DENSITY TOWNHOUSE SITE:

- 29 MARKET TOWNHOUSES @ 1460 SF	= 42,340 SF
- 6 LOW-END MARKET RENTAL UNITS @ 1222 SF	= 7,332 SF

TOTAL:	35 UNITS
	49,672 SF
	0.74 FAR

AMENITY SPACE:

INDOOR AMENITY SPACE REQUIRED:	= 753 SF
OUTDOOR AMENITY SPACE REQUIRED:	= 2260 SF
INDOOR AMENITY SPACE PROVIDED	= CONTRIBUTION IN LIEU
OUTDOOR AMENITY SPACE PROVIDED	= 2260 SF

PARKING

PARKING REQUIRED LOW-END MARKET UNITS:	= 6 SPACES
PARKING REQUIRED TOWNHOUSE UNITS:	= 58 SPACES
PARKING REQUIRED VISITORS:	= 7 SPACES
TOTAL PARKING REQUIRED:	= 71 SPACES

PARKING PROVIDED LOW-END MARKET UNITS:	= 6 SPACES
PARKING PROVIDED TOWNHOUSE UNITS:	= 58 SPACES*
PARKING PROVIDED VISITORS:	= 7 SPACES
TOTAL PARKING PROVIDED:	= 71 SPACES

* MAXIMUM 50% OF TOTAL UNITS HAVE TANDEM PARKING

YAMAMOTO ARCHITECTURE
207 - 33 East 8th Avenue
Vancouver BC V5T 1R5
T - 604 731 1127 F - 604 731 1227

TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND BC

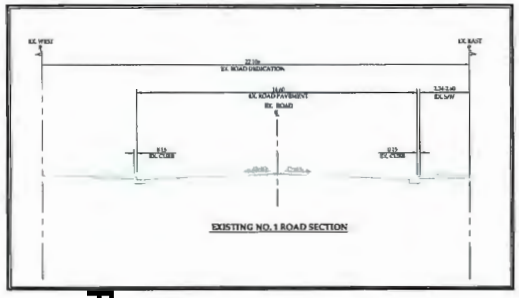
PROJECT STATS

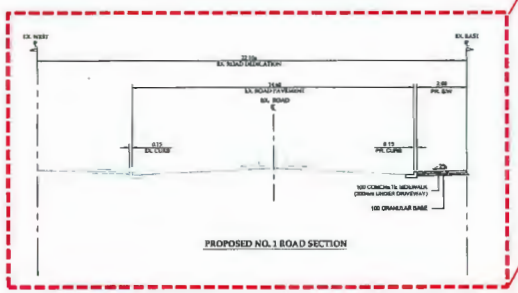
SCALE - N.T.S.
DATE - 06/18/18
DRAWN By
REVIEWED By
AS
DRAWING No. 371

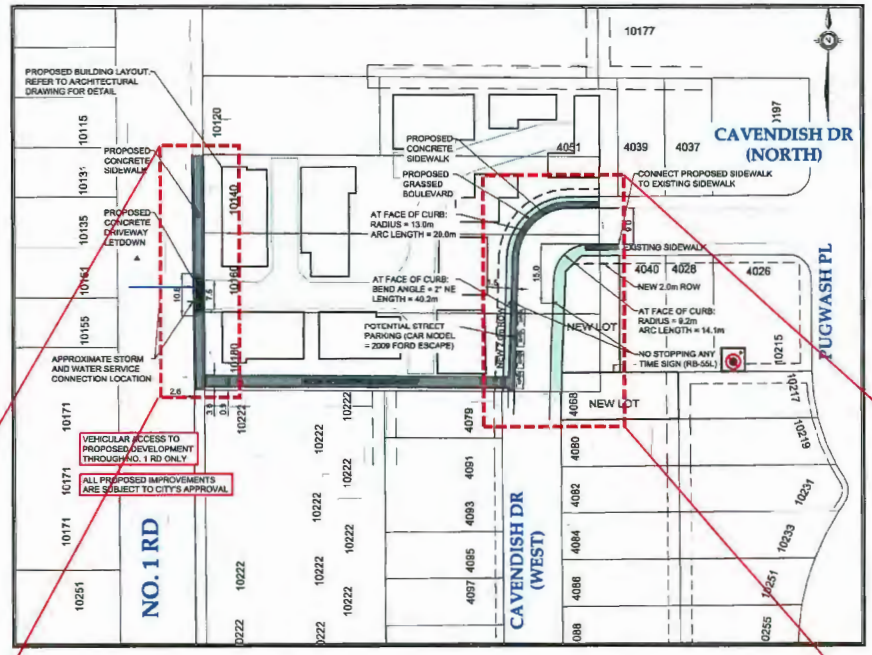
TOWNHOUSE DEVELOPMENT OF 10140-10180 NO. 1 ROAD & 4051/4068 CAVENDISH DRIVE, RICHMOND

PROPOSED SITE PLAN

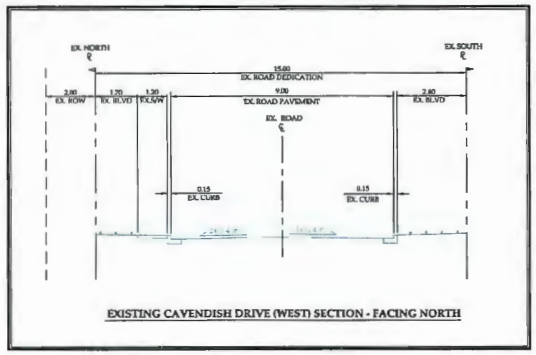
NO. 1 ROAD

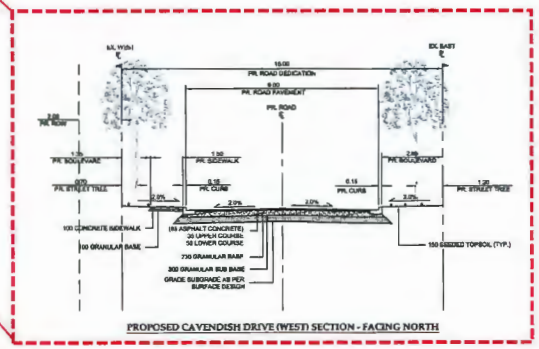






CAVENDISH DRIVE





BENEFITS:

- ✓ Improved vehicular circulation in Cavendish Drive
- ✓ Improved fire, garbage, and recycling vehicle access in Cavendish Drive
- ✓ Pedestrian walkway connecting Cavendish Drive to No. 1 Road
- ✓ Improved street lighting and visibility
- ✓ Dispose of dead ends

CoreConcept CONSULTING LTD.

TOWNHOUSE DEVELOPMENT OF 10140-10180 NO. 1 ROAD & 4051/4068 CAVENDISH DRIVE, RICHMOND

COMMUNITY FEATURE PLAN

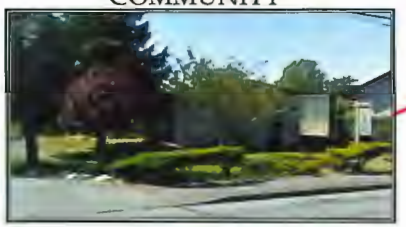
ALFRED B DIXON ELEMENTARY SCHOOL

Distance to development : 500m

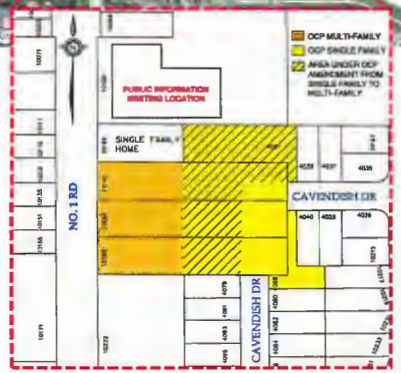
MANOAH STEVES PARK AND ELEMENTARY SCHOOL

Distance to development : 450m

EMMANUEL CHRISTIAN COMMUNITY



Distance to development : 175m



ST. JOSEPH THE WORKER PARISH AND MINISTRY

Distance to development : 430m

RICHMOND CHINESE ALLIANCE CHURCH

Distance to development : 20m

DIEFENBAKER ELEMENTARY SCHOOL

Distance to development : 265m

CoreConcept CONSULTING LTD.

APPENDIX D
SUMMARY OF FEEDBACK FORMS

PUBLIC INFORMATION MEETING - SUMMARY OF ATTENDEE FEEDBACK

Date: Wednesday, June 20, 2018
Location: Richmond Chinese Alliance Church

City File: RZ18-820669
Development Location: 10140 – 10180 No. 1 Road & 4051 and 4068 Cavendish Drive

Name	Address	Site Distance	OCP Ammend. Support	Road Connect. Support	New S/F Lot Support	Cavendish Sidewalk Support	Cavendish to No. 1 Road Walkway	Building Height Support	Building Setback Support	Other Feedback
Belkin, Avital & Karmi	4026 Cavendish Drive, Richmond	90m	No	No	No	No	No	No	No	Against development. Fears increased noise and traffic. Is of opinion that they bought in cul-de-sac and neighbourhood should not change.
Michaels, Bard & Rick	4028 Cavendish Drive, Richmond	75m	-	-	-	-	-	-	-	Feedback by Letter (Refer to Appendix E). General summary is that the Michael's have concerns include building slab height, setbacks, and lot grading that should be consistent with the existing neighbours. Shadowing and privacy impact on neighbour. Would like sidewalk and road repairs on Cavendish Drive between the site and Pugwash place. Would like a contiguous sidewalk for the new Cavendish Drive. Architectural review of privacy and building massings requested. Would like a wider bulge in the Cavendish Drive bend to accomodate street side parking.
Sun, Ben & Theresa	4040 Cavendish Drive, Richmond	60m	Yes	Yes	Yes	Yes	Yes	Yes	Unsure	Most concerned that the proposed single family homes on Cavendish Drive should have similar height and site grading as the immediate neighbours. Would prefer sidewalk along the single family lots to reduce tha number of pedestrians crossing near the corner.
Randall, Craig	4082 Cavendish Drive, Richmond	80m	-	-	-	-	-	-	-	No feedback form completed
Melvin, Monica & Bryar	4084 Cavendish Drive, Richmond	90m	-	-	-	-	-	-	-	Feedback by Letter (Refer to Appendix E). The Melvin's concerns are mostly focused on the form and character of the single family homes and the townhomes. They want them to be consistent with the existing neighbourhood. 3 story townhomes should not be permitted. Would like the walkway to be removed from the development. Concerned about parking and traffic safety if Cavendish Drive is completed.
Smrekar, Winnie	4102 Cavendish Drive, Richmond	130m	-	-	-	-	-	-	-	No feedback form completed
Tretheway, Geneva	10177 Pugwash Place, Richmond	130m	-	-	-	-	-	-	-	No feedback form completed
Penson, Shawn	#7 - 10177 Pugwash Pl., Richmond	130m	-	-	-	-	-	-	-	No feedback form completed

PUBLIC INFORMATION MEETING - SUMMARY OF ATTENDEE FEEDBACK

Date: Wednesday, June 20, 2018
Location: Richmond Chinese Alliance Church

City File: RZ18-820669
Development Location: 10140 – 10180 No. 1 Road & 4051 and 4068 Cavendish Drive

Name	Address	Site Distance	OCP Ammend. Support	Road Connect. Support	New S/F Lot Support	Cavendish Sidewalk Support	Cavendish to No. 1 Road Walkway	Building Height Support	Building Setback Support	Other Feedback
Nielsen, M.	#8 - 10177 Pugwash Pl., Richmond	130m	No	Unsure	Unsure	Yes	Yes	Unsure	Unsure	Would like the form and character of Cavendish Drive to remain single family residential. Would like tall trees retained.
Siefke, Eleanor	#17 - 10177 Pugwash Pl., Richmond	130m	-	-	-	-	-	-	-	No feedback form completed
Mak, T.K.	10188 Pugwash Pl., Richmond	140m	No	No	No		No	Unsure	Unsure	Concerned that development will impede access to church. Would like existing Cavendish sidewalk to Pugwash moved to north side of street for church access, feels public walkway does not adequately compensate for current access.
Yan, Cha Feng & Xu	10215 Pugwash Pl., Richmond	95m	No	No	Unsure	No	No	No	No	Against development. Concerned that traffic, privacy, parking, child play safety, and property security will be compromised. Would like tall trees preserved. Opposed homes > than two floors.
Masson, Annelaure	10186 Pugwash Pl., Richmond	145m	Yes	No	No	No	Yes	Yes	Unsure	Primary concern is the preservation of existing trees and greenery
Liu, Zhe	10219 Pugwash Pl., Richmond	100m	No	No	Unsure	No	No	No	No	Safety, Environment, and Cavendish Drive Street side parking concerns. Does not support changes to Cavendish Drive. Would like to preserve all existing trees.
Tong, Joseph	10100 No. 1 Road, Richmond	85m	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No comments on form
Nguyen, Tia	3333 Corvette Way, Richmond	6,6000m	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Indicated Architectural preferences for modern aesthetics and open concept with natural plantings. Allowance for commercial suites on ground level
Yuen, Connor	10511 Palmberg Road, Richmond	7,600m	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No comments on form
Lee, Jason	#11 - 9079 Jones Road, Richmond	4,500m	-	-	-	-	-	-	-	No feedback form completed
Ma, Sharon	5686 Cornwall Place, Richmond	3,600m	-	-	-	-	-	-	-	No feedback form completed
Lu, Tommy	10880 Roselea Cres, Richmond	3,800m	-	-	-	-	-	-	-	No feedback form completed
Carter, Hugh	20780 Willoughby T.C. Dr., Langley	48,000m	-	-	-	-	-	-	-	No feedback form completed
No, Tommy	1351 Continental Str., Vancouver	18,000m	-	-	-	-	-	-	-	No feedback form completed
Momeni, Mehrdad	Partial Address, phone only	-	-	-	-	-	-	-	-	No feedback form completed
Chow, Connie	No Address, phone only	-	-	-	-	-	-	-	-	No feedback form completed

PUBLIC INFORMATION MEETING - SUMMARY OF ATTENDEE FEEDBACK

Date: Wednesday, June 20, 2018
Location: Richmond Chinese Alliance Church

City File: RZ18-820669
Development Location: 10140 – 10180 No. 1 Road & 4051 and 4068 Cavendish Drive

Name	Address	Site Distance	OCP Ammend. Support	Road Connect. Support	New S/F Lot Support	Cavendish Sidewalk Support	Cavendish to No. 1 Road Walkway	Building Height Support	Building Setback Support	Other Feedback
Chieng, Alex	No Address, phone only	-	-	-	-	-	-	-	-	No feedback form completed
Tsui, Leon	No Address, phone only	-	-	-	-	-	-	-	-	No feedback form completed
Tong, Vincent	No Address, phone only	-	-	-	-	-	-	-	-	No feedback form completed

FEEDBACK FROM OTHERS NOT ATTENDING THE PUBLIC INFORMATION MEETING

Lopez, Ernesto & Flora	4104 Cavendish Drive, Richmond	130m	-	-	-	-	-	-	-	Feedback by e-mail (Refer to Appendix E). Have concerns about safety and increased pedestrian and vehicular through traffic. Would like cameras installed in the walkway as well as high illuminance.

APPENDIX E
ORIGINAL FEEDBACK FORMS

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Ben & Theresa Sun

Address: 4040 Cavendish Drive

Phone (Optional): 604-274-5090

Email (Optional):

Date: June 25, 2018

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☒ No ☐ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

After connecting both ends of Cavendish pedestrian and car traffic will increase. Parked cars on both side of the street will block visibility of the traffic. Therefore we suggest to widen the street at the curve as shown on the plan for safty sake.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

Provided the grade and the height of these houses are the same as the existing homes.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

Build the sidewalk on the east side of the street (on the side of two single lots) connecting to the existing sidewalk on the south side of Cavendish. This will increase the safty of the pedestrian traffic as one does not need to cross the street at the curve as proposed in the plan.

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

Do you support the proposed building heights?

Yes ☒ No ☐ Unsure ☐

Comments:

The plan do not show any buildings on the two single lots adjacent to 4040 and 4080 Cavendish Dr. It would be neighborly if the heights of these building limit to the same height as the neighbors'.

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ No ☐ Unsure ☒

Comments:

The plan do not show any buildings on the lots adjacent to 4040 and 4080 Cavendish Dr. It would be neighborly to have the setback as the neighboring property.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Avital Belkin

Address: 4026 Cavendish Dr

Phone (Optional): 604-5518564

Email (Optional): avitalbel@hotmail.com

Date: June 20/18

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ No ☑ Unsure ☐

Comments:
I am against! That would affect the price of our property negatively as well as make the area noisy and make traffic

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☐ No ☑ Unsure ☐

Comments:
We don't want traffic in front of our house.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☑ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☐ **No** ☑ **Unsure** ☐

Comments:

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ **No** ☑ **Unsure** ☐

Comments:

Do you support the proposed building heights?

Yes ☐ **No** ☑ **Unsure** ☐

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ **No** ☑ **Unsure** ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

We bought the property in quiet neighborhood in cultesacc. You purpose to turn this area into something completely different. That will change this area bring noise more people more traffic more garbage pollution and bring the price of our property down! I hope you never do it in this area!

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Karmi Belkin

Address: 4026 Cavendish Drive

Phone (Optional):

Email (Optional):

Date: June 20, 2018

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ No ☒ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☐ **No** ☒ **Unsure** ☐

Comments:

_____

_____

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ **No** ☒ **Unsure** ☐

Comments:

_____

_____

Do you support the proposed building heights?

Yes ☐ **No** ☒ **Unsure** ☐

Comments:

_____

_____

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ **No** ☒ **Unsure** ☐

Comments:

_____

_____

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Zhe Li

Address: 10219 Pugwash Pl.

Phone (Optional): 604-600-1874

Email (Optional): el08ms@gmail.com

Date: June. 20th. 2018.

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ No ☒ Unsure ☐

Comments:

For our safety and environment concern, I strongly do not support this amendment.

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

More than 30 units will be building here. if this amendment can be submitted. Which mean; Quite a lot vehical may be park on Cavendish Drive then the project be finished.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☐ Unsure ☒

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

I can not support any proposal relate with rebuilding or change Cavendish Drive.

Do you support the proposed building heights?

Yes ☐ No ☒ Unsure ☐

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ No ☒ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

This proposal doesn't talk about the greens. Lots of trees is growed in the area under OCP amendment. Do not kill these trees for economic benefits or interests.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: M. NIELSEN

Address: 8 - 10177 PUGWASH PLACE

Phone (Optional): 604 272 1748

Email (Optional): markl.nielsen@hotmail.ca

Date: 18 06 20

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ No ☑ Unsure ☐

Comments:

PREFER TO MAINTAIN SINGLE FAMILY DWELLINGS FRONTING CAVENDISH, IN KEEPING WITH CURRENT CHARACTER OF THE DRIVE

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☑ No ☐ Unsure ☑

Comments:

OK - BUT WORRY ABOUT INCREASED TRAFFIC AS A RESULT

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☐ Unsure ☑

Comments:

I AM CONCERNED ABOUT THE POTENTIAL LOSS OF THE TALL TREES IN THE AREA, A NEIGHBOURHOOD LANDMARK

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

_____

_____

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

_____

_____

Do you support the proposed building heights?

Yes ☐ **No** ☐ **Unsure** ☑

Comments:

_____

_____

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ **No** ☐ **Unsure** ☑

Comments:

_____

_____

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

TRADITIONAL, BLENDING WITH NEIGHBOURING SITES

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: T.K. Mak

Address: 10188 Pugwash PL

Phone (Optional): 604-448-8871

Email (Optional):

Date:

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ No ☒ Unsure ☐

Comments:

Difficult to access the church (North side).

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

children like to play at dead end road. more cars can park for going to the church.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☑ Unsure ☐

Comments:

No, because the proposed plan will block accessing to the church in the North.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☐ No ☐ Unsure ☐

Comments:

support the side walk moving to the north for accessing the church

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

the South side is very bad idea for accessing the church.

Do you support the proposed building heights?

Yes ☐ No ☐ Unsure ☒

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ No ☐ Unsure ☒

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

No change with Cavendish.

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Chaofeng Yan & Yan Xu

Address: 10215 Pugwash Place

Phone (Optional): 604-271-4158, 778-688-9853

Email (Optional): kcfyan@yahoo.com, yanx_98@yahoo.com

Date: June 20, 2018

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ No ☒ Unsure ☐

Comments:

Building Multi-family houses will negatively impact the current neighborhood traffic, privacy, parking and safety. All tall trees will be cut which the green environment will be disappear. ~~Increasing density will heavily negatively impact privacy, safety and cause other issues. Security~~ ~~much concern~~. Kids in neighborhood will lose their play land and security will be a big concern.

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

if Both ends are connected, our neighbor hood will become big parking lot. The developer only designed 7 visitor parks which is way under the need of the families in the multi-house area. The parking on No.1 road is also limited and there are ~~several~~ churches schools and community areas around. After building multi-family house, no doubt that visitors and ~~the~~ will use our neighbor-hood as parking ~~lot~~ area.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☐ Unsure ☒

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal? ~~This~~

Yes ☐ No ☑ Unsure ☐

Comments:

~~It will give~~ All though we will have convienent access to No.1. it also ~~increase~~ provide the convienient access to thieves and other ~~crime~~ criminal behavior. We ~~to~~ prefer safety and security over convienience.

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ No ☑ Unsure ☐

Comments:

Do you support the proposed building heights?

Yes ☐ No ☑ Unsure ☐

Comments:

Privacy concern. most of current houses are two-floor and lower. ~~Building~~ the proposed building heights are higher. ~~which and it will~~

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ No ☑ Unsure ☐

Comments:

Prefer ~~big~~ longer.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

fit for our current neighborhood style.

Hate to see monster house or ~~that~~ houses like tall towers.

We want no change to our neighborhood.

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Annelaure MASSON

Address: 10186 PUGWASH RL

Phone (Optional):

Email (Optional):

Date: June 20 /2018

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☒ No ☐ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☒ No ☒ Unsure ☐

Comments:

Connecting the streets does not provide any benefits - Concern on trees being removed.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

Do you support the proposed building heights?

Yes ☒ No ☐ Unsure ☐

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ No ☐ Unsure ☒

Comments:

Concern on Trees being removed. Loss of Greenery is an issue

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

keep trees (very mature and Bird friendly)

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: JOSEPH TONG.

Address: 10100 No. 1 ROAD.

Phone (Optional):

Email (Optional): tongjelly@gmail.com

Date: 2018-06-20

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☑ No ☐ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

Do you support the proposed building heights?

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Tia Nguyen

Address: 3333 corvette way

Phone (Optional): 604-908-8978

Email (Optional): Hanguyen@outlook.com

Date: June. 20, 2018

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☑ No ☐ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

_____

_____

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

_____

_____

Do you support the proposed building heights?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

_____

_____

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

_____

_____

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

- modern aesthetics
- open - concept
- community - feel for families
- plantations (nature)
- commercial suites on ground level

minimal
sleek

Other recommendations or suggestion:

central air condition

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: CONNOR QUEN

Address: 3333 CORVETTE WAY

Phone (Optional): 778 999 3353

Email (Optional):

Date: JUN 20 2018

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☑ No ☐ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on constructing a new, public sidewalk along the new Cavendish Drive. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

Do you support the proposed building heights?

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☑ No ☐ Unsure ☐

Comments:

_____

_____

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

David Kozak

From: Ernesto & Flora Lopez <ernie_flora@hotmail.com>
Sent: Monday, July 09, 2018 2:58 PM
To: David Kozak
Subject: Development Application Feedback

Dear Mr. Kozak,

RE: RZ18-820669 - DEVELOPMENT APPLICATION FEEDBACK

Site address: 10140-10180 No. 1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Ernesto & Flora Lopez
Address: 4104 Cavendish Dr., Richmond
Date: July 9, 2018

Comments:
Hello, we were unable to attend the public information meeting that occurred on June 20, however we would still like to provide our feedback. We have lived in this cul-de-sac for 16 years and have enjoyed a quiet, safe, no-through road neighbourhood. Our concerns now are regarding more traffic flow (pedestrians/cars) and safety. Should this proposal be approved we wish to see the following recommendations take effect and be provided by our tax dollars. To provide safety measures regarding more people coming in and out of our neighbourhood we strongly recommend having numerous bright lamp-posts throughout the pathways and possibly cameras. This is to deter and discourage any illegal/dangerous activities as sometimes there are people that drug-deal or break and enter in neighbourhoods that are really dark at night, and have an easy way to get in/out. Having bright lights and a couple of cameras throughout the pathways and streets we feel strongly would discourage such activities at night. This is a relatively safe neighbourhood full of children and we hope to keep it this way for many more years. We would appreciate a copy of this email be sent to Edwin Lee from the City of Richmond.

Sincerely,
Ernesto & Flora Lopez

RZ18-820669 - DEVELOPMENT APPLICATION FEEDBACK
Site Address: 10140 - 10180 No.1 Road & 4051/4068 Cavendish Drive Richmond
Name: Monica Melvin
Address: 4084 Cavendish Drive
Date: June 20, 2018

I am concerned about proposed development and rezoning application put forth by Core Concept Consulting Ltd. for these reasons:

1) There is very limited information on the handout as to how the plot of land will be developed, there needs to be further drawings and explanations about the style and type of houses and townhouses. Will the architecture match what is currently in the neighbourhood?
2) If a developer is building into an existing neighbourhood they should be aware of the surroundings and build homes that will be harmonious to the neighbourhood. All the houses on Cavendish West, Cavendish North and Pugwash are **2 story homes**. Building 3 story townhouses will not fit in or be harmonious to the street. **There should only be 2 story homes and townhouses.**
3) The pedestrian walkway should not be included, it will bring vagrants and allow people to wander though our neighbourhood which might increase the crime rate. Right now, we have a very safe and private street due to the dead end. With this development, our privacy will be lost.
4) If the road of Cavendish Drive is joined, then the traffic will increase. The parishioners from the Chinese Alliance Church and people in the neighbourhood will use the parking on the extended Cavendish Road and due to the curve this will not be safe as cars will be parked on both sides. Drivers will not be able to see who is coming around the curved corner. This could cause an increase in accidents.

David Kozak

From: Rick Michaels <RickMichaels@Shaw.ca>
Sent: Sunday, June 10, 2018 8:08 PM
To: Lee,Edwin
Subject: Rezoning 18-820669 - No1 Rd & Cavendish Dr.

Hi Edwin, my name is Rick Michaels and I live at 4028 Cavendish Dr. We received a redevelopment package for this rezoning together with an invite to a public information meeting on June 20th. The plans are too preliminary to provide meaningful feedback at this time. Information in these meetings can be quite varied in level of detail and not complete to the degree necessary to properly evaluate its impact. Hence writing to you now before the meeting to request specific pieces of information that will assist me in formulating a proper opinion. Do you provide electronic access to rezoning applications and plans as is done in Vancouver? If not can you please arrange for electronic access to these plans for public viewing either thru the City or the applicant? The items I wish to gain a proper understanding of may take longer to figure out than provided for in a crowded noisy information meeting.

The information I am most interested in at the moment is the following:

4068 Cavendish Drive and the new lot west of 4040 Cavendish

1) Will the site grading be raised above that existing or will the current grading which is compatible with neighbouring sites be the maximum permitted?

2) Will the building form massing and design including setbacks, height in feet and storeys be required to be the same as the neighbouring sites?

3) Will the drawings at the information meeting clearly show site grading, the maximum permitted envelope, setbacks and design criteria including adequate design details to evaluate shadowing and privacy/ overlook into neighbouring residences?

4) What will be the extent of the sidewalk and road repairs on Cavendish Drive between the development site and Pugwash Place. Tree root damage at the west end of the current street is significant and posing tripping hazards and some drainage issues.

5) What will be the degree of boulevard improvement/change in front of the existing houses of 4039, 4037, 4028 and 4040.

6) I recall on your service maps that the current east-west sanitary line in the rear yards of 4040 and 4028 and 10215 Pugwash turns north-south immediately west of 4040. Will this north-south leg and what appears to be a manhole (in plan) be relocated or will it remain with an easement required for a portion of the east side yard of the new north-south lot?

4 TOWNHOUSE BLOCKS ON THE WEST SIDE OF NEW CAVENDISH EXTENSION

1) What will be the finish grade of the site in this part of the development site. I appreciate that the west side buildings on No 1 will be on a raised grade to meet No. 1 Rd but happens thereafter and at Cavendish Dr.?

2) What will be the maximum permitted height of the three storeys.

3) Similar to 3 above. Form, massing, site planning, design criteria?

4) Same as 5 what exactly is being proposed biting that boulevard treatment between the two Cavendish Drives are not the same. They were at one time until City had to to remove the majority of the trees because of the significant root damage to driveways, sidewalks and curbs (Qualico which built this subdivision in the early 80's planted maples in undersized landscape pockets between buildings and other hardscape. The longer Cavendish landscape is not the same as the shorter one and now we will have this new middle section. How will this potentially eclectic circumstance be handled? Digressing for a moment - I have never understood the logic of sidewalks not being continuous from street end to street end? This will be the case here – what is the science and logic for switching sidewalks midblock from one side of the street to the other?

5) The most northerly 2 two storey townhouses have significant facades directly facing the the only open spaces and some of the major windows of 4039. A 3m setback is shown. However even without the detailed plans this seems to be a severe impact to that existing residence and its open spaces and some of its windows and rooms. The sketch graphics show the proposed building to be set back from the 3m setback by about another 1.5 m. If this is the case then why not increase the setback to the setback shown on the sketch. Again it would be helpful to have detailed information on those buildings for the 4039 property owners to evaluate impact to their site.

My major concern at the moment is that the site grading along Cavendish is not raised to any new higher standard and matches that on both existing portions of Cavendish Dr. The house siting and design criteria for the two new lots be consistent with the existing built forms on Cavendish with due respect to shadowing, privacy/overlook. The street and boulevard treatments of the three sections of Cavendish be blended and harmonious not three eclectic compositions from three eras of landscape thinking. The townhouse form along Cavendish be neighbourly with and compatible and respectful interfaces with existing development.

Figure 2 of the package delivered is missing a property line between 4026 and 10215. One more question, the road alignment in figure 2 shows that the new piece of Cavendish will be skewed to the east and not aligned in the typical fashion with the other two sections of Cavendish – why? Why not have a wider bulge at the turn noting there will be more cars on the street and it isn't an atypical quiet street with the church traffic. The Church traffic and parking will probably increase with a fully developed road and proper pedestrian access to No. 1 RD. Have no issues with the church traffic and parking, they are great neighbours; however lets make this as safe as possible and easy for two way traffic to manage the corner. Lets not after the fact have to lose street parking to manage atypical traffic on this street. Visitors to our future new neighbours plus some of the new residents will also make use of the street frontage for their parking, and rightly so. They are entitled just like anyone else. So how about maxing out the number of spaces available plus increase the safety margin accordingly. The current schematic of the street seems to fall short in regards to these considerations? A wider turn similar to that at the other end of Cavendish (maybe not to the same extreme) might help or some other street geometry?

Thank you for time and patience.
Rick Michaels

YAMAMOTO ARCHITECTURE

DATE –
September 16th, 2019

TO –
City of Richmond
6911 No. 3 Road
Richmond, BC, V6Y 2C1

PROJECT –
10140 – 10180 No. 1 Road &
4051 & 4068 Cavendish Drive
Richmond, BC

ATTN –
Edwin Lee

PROJECT NO –
RZ 18-820669

Summary of Public Information Meeting – Number Two
10140 – 10180 No. 1 Road and 4051 and 4068 Cavendish Drive
City File: RZ 18-820669

Dear Edwin,

A second Public Information Meeting for the proposed 35 unit townhouse and 2 single family lot development located at 10140 – 10180 No. 1 Road and 4051 and 4068 Cavendish Drive (City File RZ 18-820669) was held between 5.00pm and 8.00pm on June 26, 2019 at the Richmond Chinese Alliance Church located at 10100 No. 1 Road.

Core Concepts Consulting Ltd. Prepared a Public Information Meeting invitation including a document outlining the synopsis of the proposed development. The invitation packages were hand- delivered by Bohan Developments staff to the residences in the vicinity of the proposed development on June 12th, 2019. Please refer to Appendix A for the Public Information Invitation Package and Appendix B for the map defining the notice distribution area.

There are 53 single family homes and 53 residences and the church in the notice distribution area. 12 residences in the notice distribution area attended the second Public Information Meeting (11%). Two residents attended the second Public Information Meeting who had not attended the first one.

Attendees of the meeting were greeted upon entry and encouraged to sign the attendance sheet. 12 attendees were formally recorded on the attendance sheet but often one signature actually represented households with multiple household members in attendance. We estimate a total turn out of 20 people.

The attendees were free to examine a series of presentation boards (20 boards in total) and Mr Steven Yang (Bohan Developments) and Brian Sheehan (Yamamoto Architecture) were available to address any questions raised by the attendees in either small informal groups or one-on-one as preferred by the attendee. Refer to Appendix C for a reduced copy of the Presentation Boards displayed.

Each participant was provided a feedback form that they complete at the meeting or which they could take home and complete at their leisure. As part of the second Public Information Meeting the presentation boards and feedback were made available to attendees on the Bohan Development website.

As of September 6th, we compiled 7 feedback forms and 2 independent emails. A table summarizing each of the feedback forms received and our synopsis of the comments received is included in Appendix D. The synopses provided for each feedback form addresses what we interpret to be the key points raised by the Attendee. Not all points are necessarily addressed or identified. The reader should peruse each of the feedback forms to establish their own interpretation of the tone and content of the feedback forms supplied in Appendix E.

Overall a majority of the attendees were in support of the design changes that were made and felt that their feedback / concerns from the first Public Information Meeting were addressed. There were still one or two issues that were of most concern to them.

Public Walkway Between No. 1 Road & Cavendish Drive

Majority of attendees supported the proposal for the walkway connection between No.1 Road and Cavendish Drive after reviewing the design and landscape drawings. There two residence that still maintained their concerns that the construction of the public walkway would result in an increase in crime and undesirable activities.

Single Family Lots

Feedback from the attendees with regards to the single family lots were side yard setbacks to be increased to reduce shadowing and reducing the front yard setback to provide larger rear yard spaces to each single-family lot.

Trees Preservation / Planting

Attendees were able to review the landscape / tree management boards and understand the existing mature trees which are being retained and rationale for a large number of trees being removed. Attendees requested for street trees to be planted along the Cavendish Drive connection with the species matching the current city trees. Attendees would also prefer slow growing planting and non-invasive trees with wide spread root bases.

The community feedback has been reviewed and the proposal has been adjusted to suit.

Please contact me if you have any questions

Best Regards,

Brian Sheehan

APPENDIX D

SUMMARY OF FEEDBACK FORMS

APPENDIX A

PUBLIC INFORMATION INVITATION PACKAGE

Core Concept
CONSULTING LTD.

#220 – 2639 Viking Way
Richmond, BC, V6V 3B7

Phone: 604.249.5040
Fax: 604.249.5041

BOHAN
PROPERTIES

To: **Owner/Occupant** June 10, 2019

Subject: **Notice of 2nd Public Information Meeting for the Proposed Development of 10140-10180 No. 1 Road & 4051/4068 Cavendish Drive (Rezoning No. 18-820669)**

Dear Neighbour,

Bohan Properties, the owner of the above parcel would like to extend an invitation for you to attend a 2nd Public Information Meeting (PIM) related to the proposed 2 & 3 story townhomes and 2 single family residential lots for the above subject properties. The purpose of this meeting is to allow the public an opportunity to learn more about the revised project.

<u>Public Information Meeting</u>

Location:	**Richmond Alliance Church**
	10100 No. 1 Road, Richmond, BC
Date:	**June 26th, 2019**
Time:	**5:00pm to 8:00pm (Open house format)**

The site is located in between No 1 Road on the west, Cavendish Drive on the east, single-family homes on the south, a church and a single-family home on the north side as shown in Figure 1.

The proposed development requires a formal rezoning and Official Community Plan (OCP) amendment application to the City of Richmond

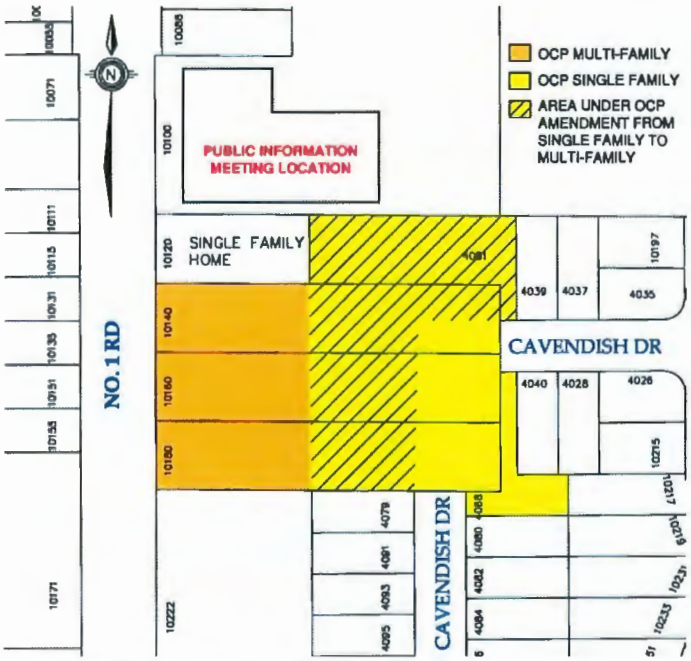


Figure 1. Proposed Parcels for Rezoning & OCP Amendment

Core Concept CONSULTING LTD.

BOHAN PROPERTIES

The 1st Public Information Meeting was held for this application on June 20th, 2018 where we listened to community feedback. The application has been revised to incorporate this feedback.

Summary of Key Changes since the June 20th PIM:

- The internal road and unit layouts have been revised to preserve mature and healthy trees.
- Cavendish Drive has been reconfigured to permit only through pedestrian access (and emergency vehicle access through locked steel bollards). A Traffic Impact Assessment (TIA) report confirmed this configuration was favorable and had minimal impact on the neighbourhood. We believe this will address neighbourhood concerns regarding noise, traffic, and parking.
- The TIA validated the proposed site entry from No. 1 Road.
- The form and character of the townhomes fronting Cavendish will suit the character of the neighbourhood. The height of these units has been reduced from 3 stories to 2 and 2.5 stories.
- The form and character of the two residential lots will suit the other homes in the neighbourhood. The main floor elevation of the lots will be 0.3m above the centerline of the fronting roadway in accordance with City Bylaw 8204.

We welcome your attendance anytime between 5:00pm and 8:00pm and look forward to your feedback on this project. The presentation materials may be viewed online on June 27th, 2019 or later at www.bohan.ca/cavendishrezoning.

If you cannot attend the meeting you may contact the City or the Developer to obtain more information or to provide feedback. They may be reached at:

Bohan Properties
Steven Yang, Managing Partner
Phone: (604) 341 7777
Email: steven@bohan.ca

City of Richmond Planning
Edwin Lee, Planner I
Phone: (604) 276 4121
Email: elee@richmond.ca

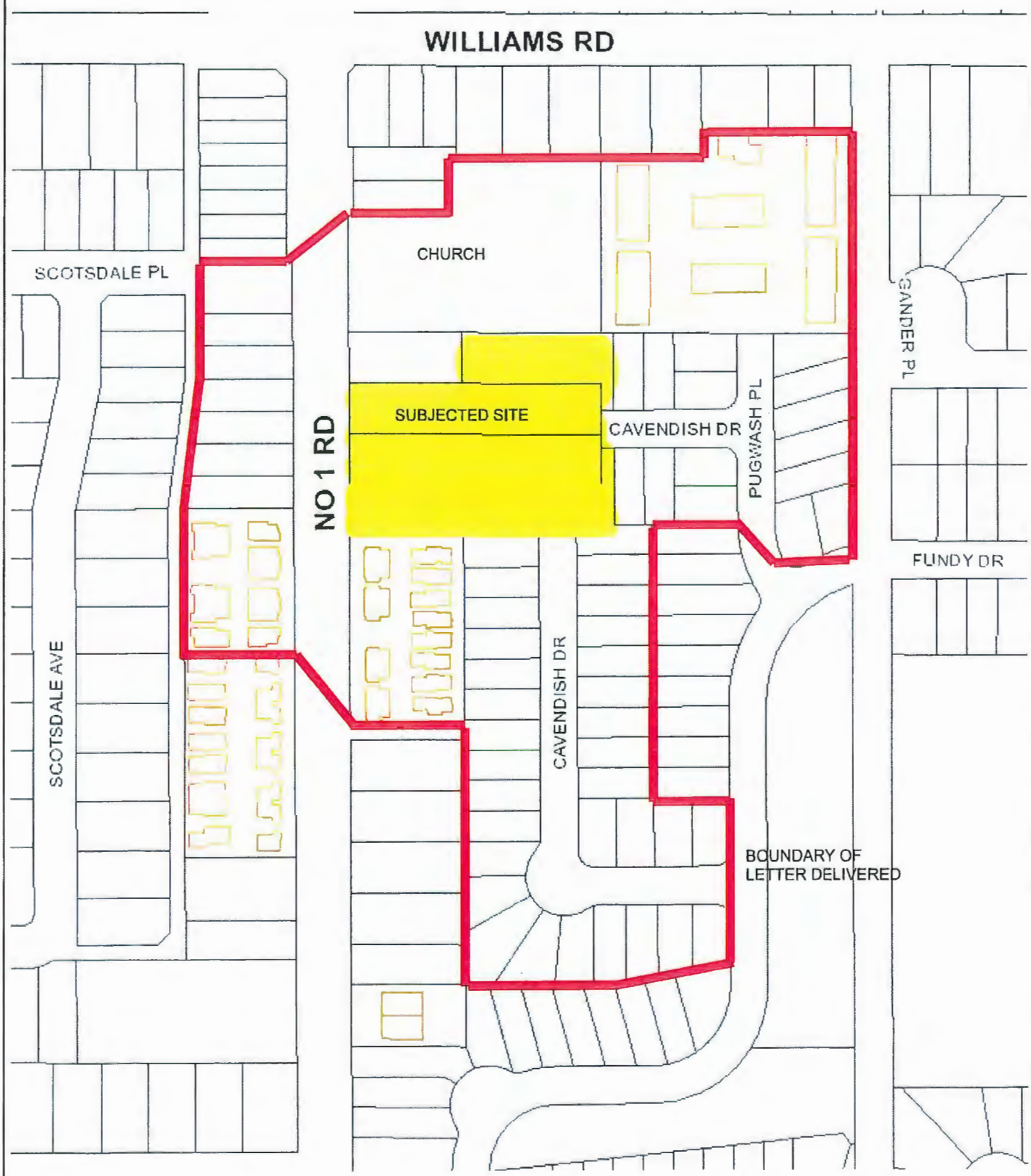
We look forward to seeing you at the public information meeting.

Yours Truly,
Core Concept Consulting Ltd.

David R. Kozak

David R. Kozak
Senior Project Manager
Phone: (604) 249 5040
Email: drkozak@coreconceptconsulting.com

City of Richmond Interactive Map



This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

© City of Richmond

APPENDIX B

NOTICE DISTRIBUTION AREA

PROJECT STATISTICS

ADDRESS: 10140, 10160, 10180 NO.1 ROAD &
4051, 4068 CAVENDISH DRIVE.

SITE AREA BEFORE ROADWAY DEDICATIONS	83,990 SQFT (7,803 SM)
TOWNHOUSE SITE AREA AFTER DEDICATIONS	66,368 SQFT (6,166 SM)

PROPOSED DENSITY TOWNHOUSE SITE:

-35 TOWNHOUSES WHICH INCLUDE:

-29 MARKET TOWNHOUSES (INCL 4 CONVERTIBLE UNITS & 1 SECONDARY SUITE)

-6 LOW-END MARKET RENTAL UNITS (LEMR UNITS)

TOTAL: 35 UNITS
43,589 SQFT
0.65 FAR

PROPOSED AMENITY SPACE

INDOOR AMENITY SPACE REQUIRED:	775 SQFT (72 SM)
OUTDOOR AMENITY SPACE REQUIRED:	2,235 SQFT (216 SM)
INDOOR AMENITY SPACE PROVIDED:	CONTRIBUTION IN LIEU
OUTDOOR AMENITY SPACE PROVIDED:	3,592 SQFT (333.7 SM)

PROPOSED SINGLE FAMILY LOTS

PROPOSED SINGLE FAMILY LOT A AREA:	4,779 SQFT (444 SM)
PROPOSED SINGLE FAMILY LOT B AREA:	3,875 SQFT (360 SM)

*Both Single Family Lots to comply with Lot Size Policy
*Each Single Family dwelling will include a secondary suite

PARKING

PARKING REQUIRED LOW-END MARKET UNITS:	6 SPACES
PARKING REQUIRED TOWNHOUSE UNITS:	58 SPACES
PARKING REQUIRED VISITORS:	7 SPACES
TOTAL PARKING REQUIRED:	71 SPACES
PARKING PROVIDED LOW-END MARKET UNITS:	6 SPACES
PARKING PROVIDED TOWNHOUSE UNITS:	58 SPACES
PARKING PROVIDED VISITORS:	7 SPACES
TOTAL PARKING PROVIDED:	71 SPACES

*40% OF TOTAL UNITS HAVE TANDEM PARKING, MAXIMUM 50%

BOHAN PROPERTIES

YAMAMOTO ARCHITECTURE

TOWNHOUSE DEVELOPMENT

PROJECT SUMMARY

A1.0

APPLICABLE CITY POLICIES

RICHMOND 2041 OFFICIAL COMMUNITY PLAN

GOALS:

1. Welcome and Diverse
   The City is inclusive and designed to support the needs of a diverse and changing population.
2. Connected and Accessible
   People are connected to and interact with each other. Places, buildings, and activities are connected and easily accessed by everyone.
3. Valued for its Special Places
4. Adaptive.

ARTERIAL ROAD POLICY

"The City supports densification along its arterial roads. The purpose of this densification is to locate developments on arterial road properties in close proximity to commercial services, public amenities, schools, and transit service."

LOT SIZE POLICY

Minimum Lot Size: Subzone RS2/B
=360SM (3,875 SQFT)

2017 - 2027 AFFORDABLE HOUSING STRATEGY

LOW END MARKET RENTAL POLICY (LEMR)

"LEMR units are secured as affordable in perpetuity through legal agreements on title, which restricts the maximum rents and tenant eligibility by income. This policy is intended to ensure the development of mixed income communities and provide rental homes for low-moderate income households."

PROJECT SUMMARY

Development Application:
Formal Rezoning & Official Community Plan (OCP) Amendment Application.

Connect Cavendish Drive, eliminating dead ends and improving pedestrian circulation. Bollards to be installed at each end to ensure no public vehicle access.

Improve infrastructure by connecting watermains on either side of Cavendish Drive.

Improve security and passive surveillance by re-routing current walkway through undeveloped lot at 4068 Cavendish Drive onto new, standard City sidewalk and street lighting.

Improve the No. 1 Road frontage with new 1.8m Boulevard & 1.5m sidewalk.

Create a pedestrian walkway between No1 Road and Cavendish Drive. Increasing neighbourhood access to Cavendish Drive, schools, transit and neighbourhood circulation.

Create two new single family lots to complete the single family character of the block. Driveway access to each lot from teh south end of Cavendish Drive.

Proposed single family lots to include a secondary suite within the proposed dwelling.

Minimize increase in traffic along Cavendish by providing all vehicular access to townhouse site from No. 1 Road.

Within the 35 unit development there will be provide six Low-End Market Rental units (LEMR)to provide affordable housing options and 4 Convertible units to provide options for aging in place and provide housing opportunities for different needs.

Orient windows away from existing single family homes and yards to maximize privacy for current homeowners.

Amend OCP to provide additional townhouses in the eastern portion of the site to offset the cost of Cavendish Drive dedication and construction.

BOHAN PROPERTIES

YAMAMOTO ARCHITECTURE

TOWNHOUSE DEVELOPMENT

PROJECT SUMMARY

A1

PREVIOUS PUBLIC CONCERNS

Below are public concerns on the previous proposal that came out of the first Public Information Meeting (PIM).

We have taken these concerns on board and addressed them in the new proposal.

REMOVAL OF EXISTING TREES

- mature tree being removed
- privacy concerns

BUILDING HEIGHT

- facing Cavendish Drive
- facing existing single family
- window locations

CAVENDISH DRIVE CONNECTION

- increased traffic flow
- Townhouse access

HOW HAVE THESE BEEN ADDRESSED

EXISTING TREES

Interior drive aisle and townhouse layout revised to preserve mature and healthy trees.

Existing mature sequoia & spruce trees to be retained and incorporated into the proposed outdoor amenity space. Trees of good health and structure will be retained along the south, east and west property line. Existing trees will be integrated into the overall landscape design.

BUILDING HEIGHT

Townhouses fronting Cavendish Drive have been reduced from 3 storey to 2.5 storey. This will help relate to the scale of the existing single family homes within the neighbourhod.

Townhouses fronting onto the public walkway along the south property line have been reduced from 3 storey to 2 storey. With the public walkway and proposed landscaping this will reduce the exposure and limit the building massing.

Townhouses adjacent to existing single family lots have been reduced from 3 storey to 2 storey.

Townhouse windows will be orientated away from existing single family homes and yards to maximize privacy for current homeowners.

CAVENDISH DRIVE CONNECTION

Proposed Cavendish Drive connection will improve pedestrian circulation to No 1 Road, transit and neighbourhood circulation. Bollards to be installed at each end to ensure no public vehicle access.

Bollards will be fitted with locks to allow for emergency vehicle access only.

The Cavendish Drive road width has been reduced from 15m to 6m width. The reduced road width provides an increased landscaped boulevard between single family lots and townhouse development.

Cavendish Drive connection provides a closed off "open space" for neighbourhood activities.

Improve security and passive surveillance by re-routing current walkway through undeveloped lot at 4068 Cavendish Drive onto new, standard City sidewalk and street lighting.

Improve infrastructure by connecting watermains on either side of Cavendish Drive.

NO 1 ROAD FRONTAGE & PUBLIC WALKWAY TO CAVENDISH DRIVE

Improve the No. 1 Road frontage with new 1.8m Boulevard & 1.5m sidewalk with landscaped buffer. The wider 1.5m sidewalk will provide a safer walkway for pedestrian flow and will be set back from No 1 Road.

New paved pedestrian walkway between No1 Road and Cavendish Drive will increasing neighbourhood access to Cavendish Drive, schools, transit and neighbourhood circulation.

The pedestrian walkway will be lit with street lights to provide a secure walkway for pedestrians and neighbouring houses.

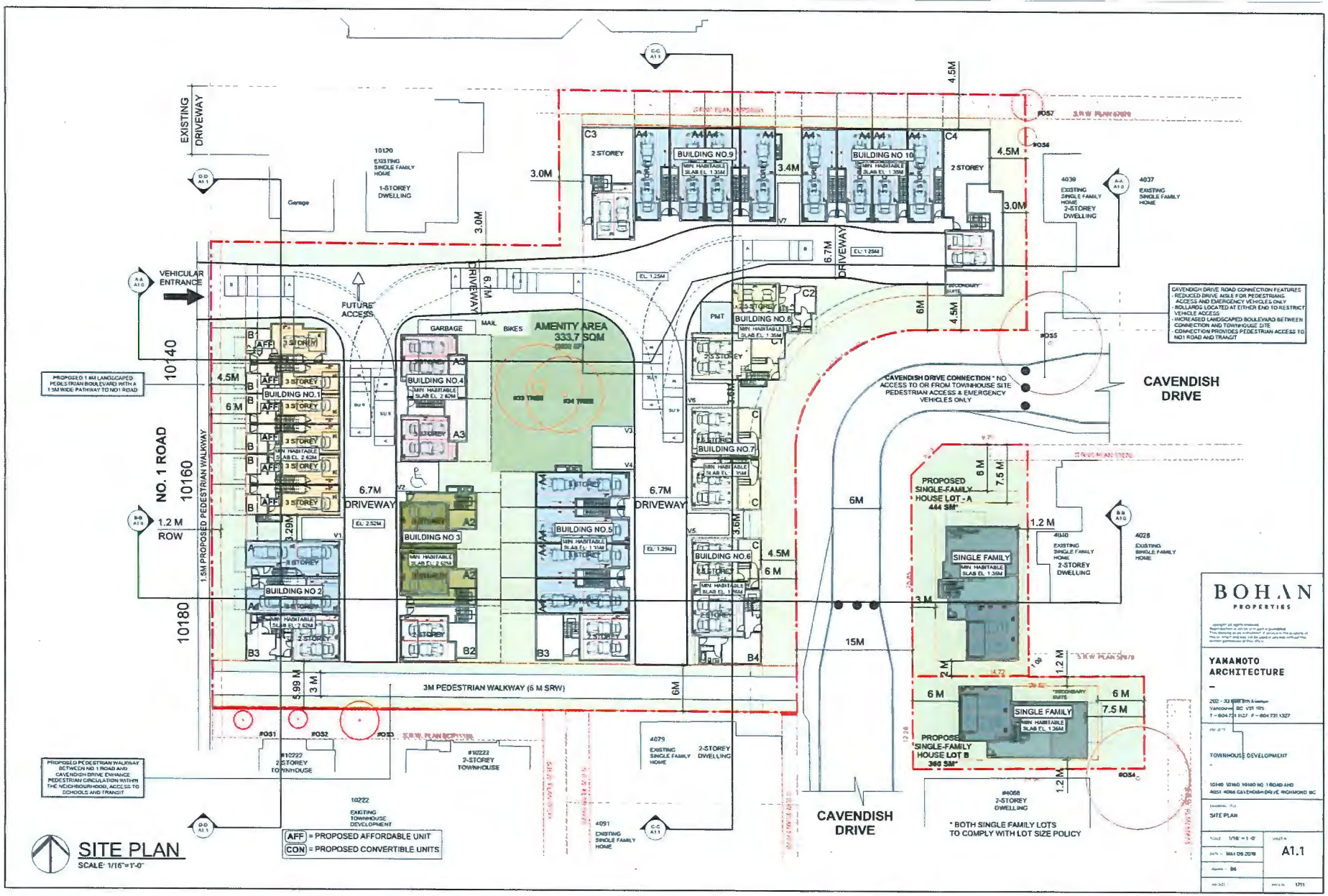
BOHAN PROPERTIES

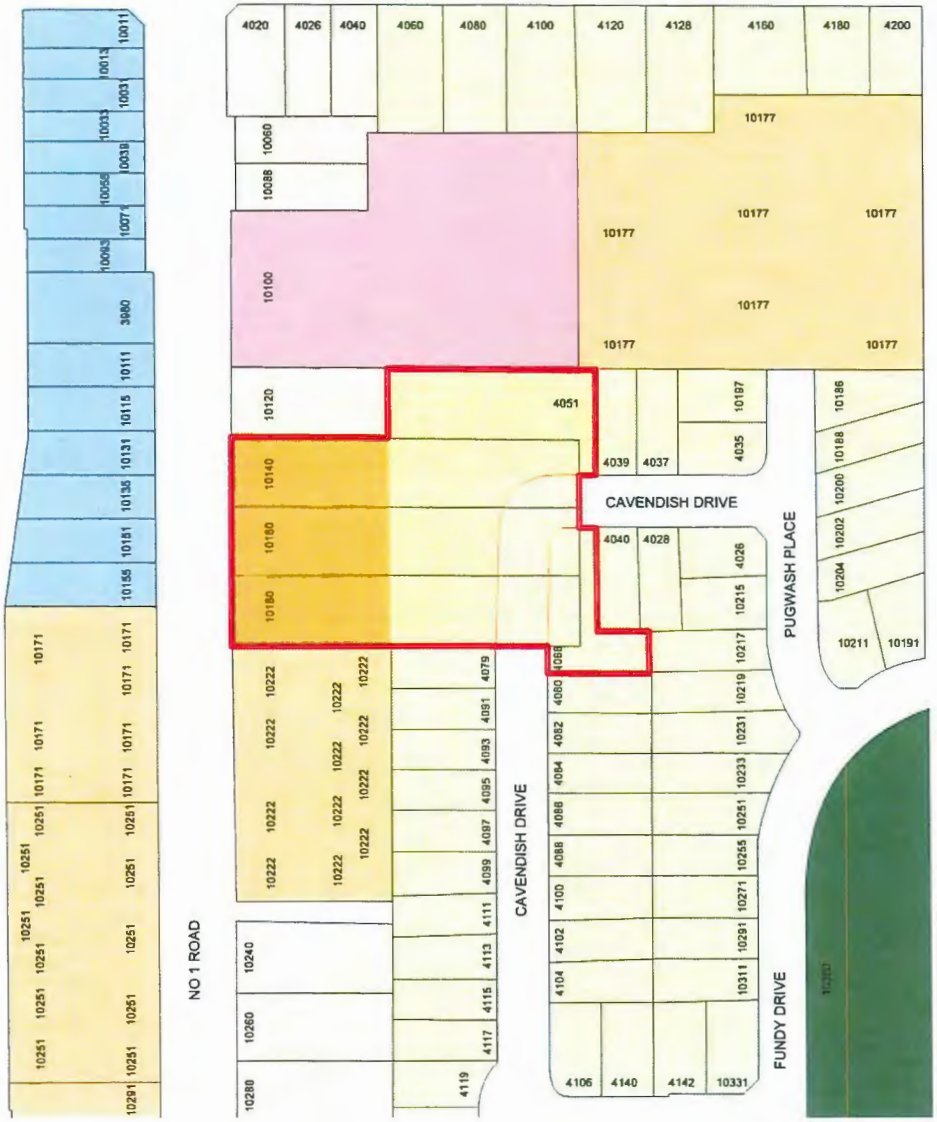
YAMAMOTO ARCHITECTURE

TOWNHOUSE DEVELOPMENT

PUBLIC CONCERNS

A


SITE PLAN
SCALE: 1/16"=1'-0"
EXISTING DRIVEWAY
VEHICULAR ENTRANCE
FUTURE ACCESS
NO. 1 ROAD
10140
10160
10180
1.2 M ROW
1.5M PROPOSED PEDESTRIAN WALKWAY
PROPOSED 1.8M LANDSCAPED PEDESTRIAN BOULEVARD WITH A 1.5M WIDE PATHWAY TO NO 1 ROAD
PROPOSED PEDESTRIAN WALKWAY BETWEEN NO 1 ROAD AND CAVENDISH DRIVE ENHANCE PEDESTRIAN CIRCULATION WITHIN THE NEIGHBOURHOOD, ACCESS TO SCHOOLS AND TRANSIT
6.7M DRIVEWAY
GARBAGE
MAIL
BIKES
AMENITY AREA 333.7 SQM
BUILDING NO.1
BUILDING NO.2
BUILDING NO.3
BUILDING NO.4
BUILDING NO.5
BUILDING NO.6
BUILDING NO.7
BUILDING NO.8
BUILDING NO.9
BUILDING NO 10
3M PEDESTRIAN WALKWAY (6 M SRW)
CAVENDISH DRIVE
CAVENDISH DRIVE CONNECTION * NO ACCESS TO OR FROM TOWNHOUSE SITE PEDESTRIAN ACCESS & EMERGENCY VEHICLES ONLY
CAVENDISH DRIVE ROAD CONNECTION FEATURES
· REDUCED DRIVE AISLE FOR PEDESTRIANS, ACCESS AND EMERGENCY VEHICLES ONLY
· BOLLARDS LOCATED AT EITHER END TO RESTRICT VEHICLE ACCESS
· INCREASED LANDSCAPED BOULEVARD BETWEEN CONNECTION AND TOWNHOUSE SITE
· CONNECTION PROVIDES PEDESTRIAN ACCESS TO NO1 ROAD AND TRANSIT
PROPOSED SINGLE-FAMILY HOUSE LOT - A 444 SM*
PROPOSED SINGLE-FAMILY HOUSE LOT B 360 SM*
SINGLE FAMILY
* BOTH SINGLE FAMILY LOTS TO COMPLY WITH LOT SIZE POLICY
AFF = PROPOSED AFFORDABLE UNIT
CON = PROPOSED CONVERTIBLE UNITS
BOHAN PROPERTIES
YAMAMOTO ARCHITECTURE
TOWNHOUSE DEVELOPMENT
SITE PLAN
A1.1


CAVENDISH DRIVE
PUGWASH PLACE
FUNDY DRIVE
NO 1 ROAD
4051
4039
4037
4035
4040
4028
4026
10177
10177
10177
10177
10177
10177
10100
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10280
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4128
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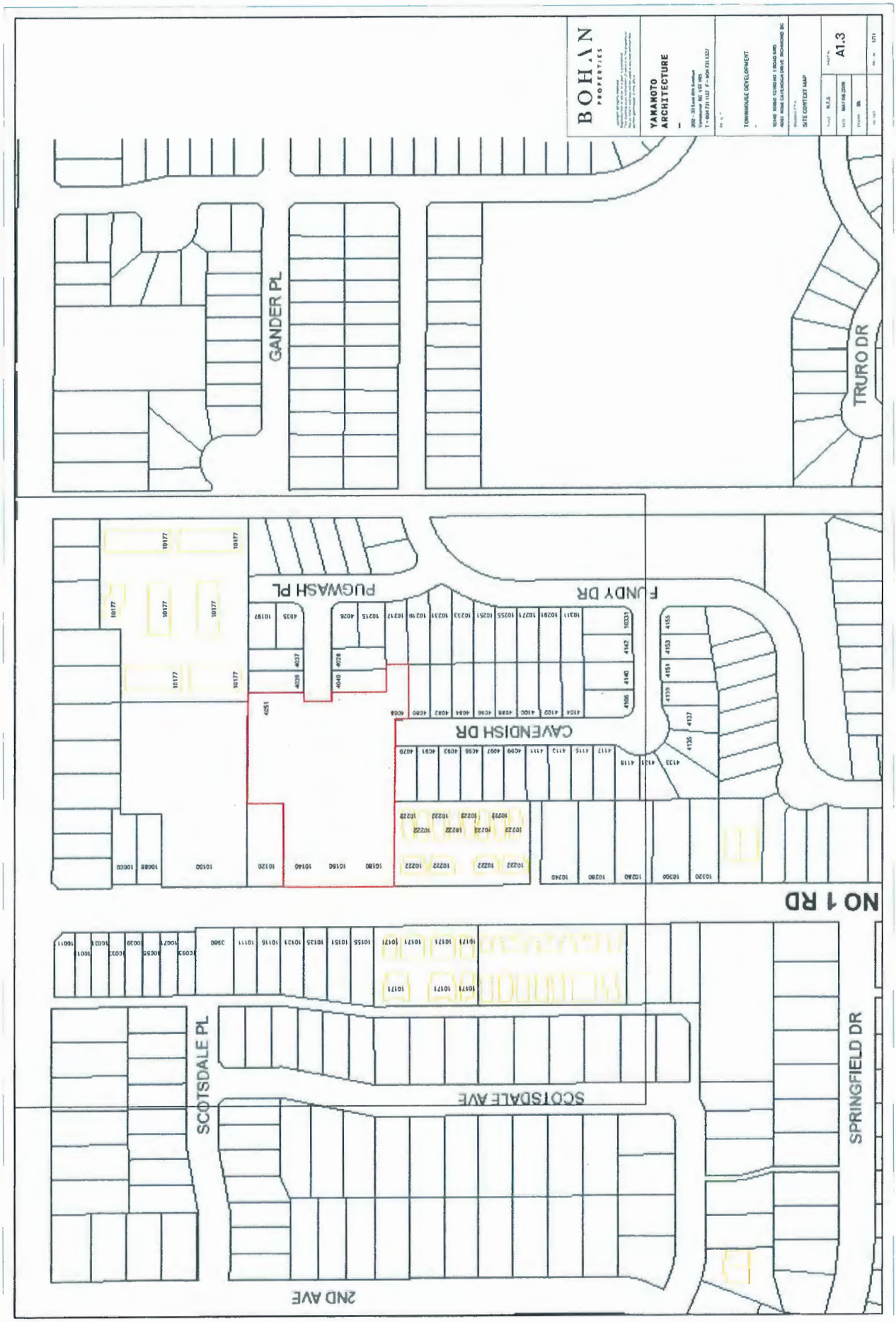
10291
10155
10181
10135
10131
10115
10111
3980
10095
10071
10055
10039
10033
10031
10013
10011
OCP DESIGNATED MULTI- FAMILY - PROPOSED TOWNHOUSES
OCP DESIGNATED MULTI- FAMILY - CURRENT TOWNHOUSES
OCP DESIGNATED MULTI- FAMILY - CURRENT SINGLE FAMILY
OCP DESIGNATED SINGLE FAMILY - CURRENT SINGLE FAMILY
AREA UNDER OCP AMENDMENT FROM SINGLE FAMILY TO MULTI- FAMILY
COMMUNITY INSTITUTIONAL
PARK
ARTERIAL ROAD COMPACT LOT SINGLE DETACHED
BOHAN PROPERTIES
YAMAMOTO ARCHITECTURE
TOWNHOUSE DEVELOPMENT
OCP CONTEXT
N.T.S.
A1.2


BOHAN
PROPERTIES
YAMAMOTO
ARCHITECTURE
TOWNHOUSE DEVELOPMENT
SITE CONTEXT MAP
A1.3
GANDER PL
TRURO DR
PUGWASH PL
FUNDY DR
CAVENDISH DR
NO 1 RD
SCOTSDALE PL
SCOTSDALE AVE
SPRINGFIELD DR
2ND AVE

NO 1 ROAD - FACING PROPERTY LINE BETWEEN CHURCH AND FIRST

CAVENDISH DRIVE - FACING NORTHWEST

NO 1 ROAD - FACING PROPERTY LINE BETWEEN FIRST AND THIRD

CAVENDISH DRIVE - FACING NORTHEAST

NO 1 ROAD - PROPERTY LINE BETWEEN THIRD AND FIRST

CAVENDISH DRIVE - FACING SOUTHWEST

NO 1 ROAD - FIRST

CAVENDISH DRIVE - FACING NORTHWEST

BOHAN PROPERTIES

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YAMAMOTO ARCHITECTURE

202 - 33 East 8th Avenue
Vancouver BC V5T 1R5
T - 604 731 1127 F - 604 731 1327

PROJECT

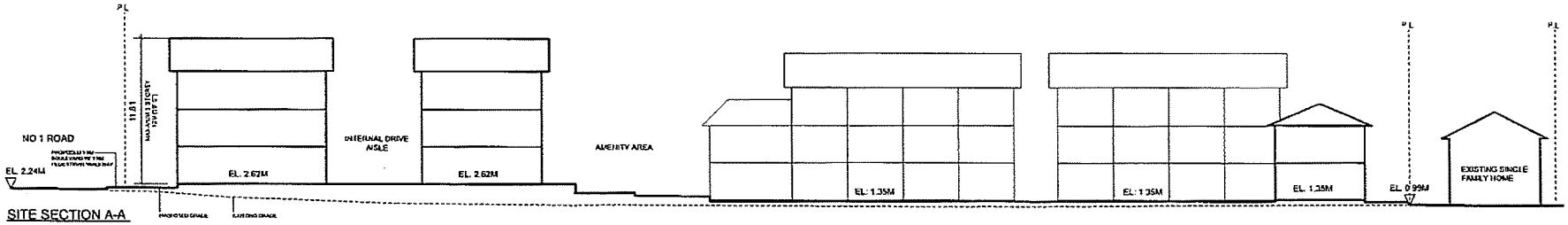
TOWNHOUSE DEVELOPMENT

10160 10180 10180 NO 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND BC

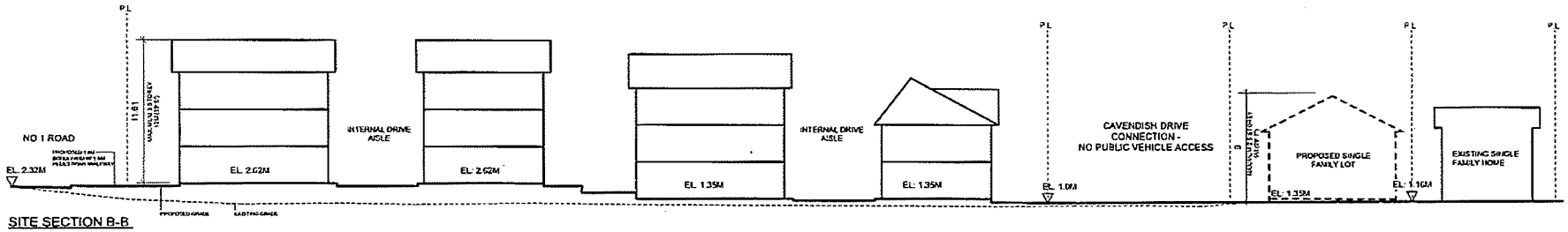
DRAWING TITLE

SITE PHOTOS

DATE MAY 06 2019	SHEET A1.4
DRAWN BR	
PROJECT NO. 1791	



SITE SECTION A-A



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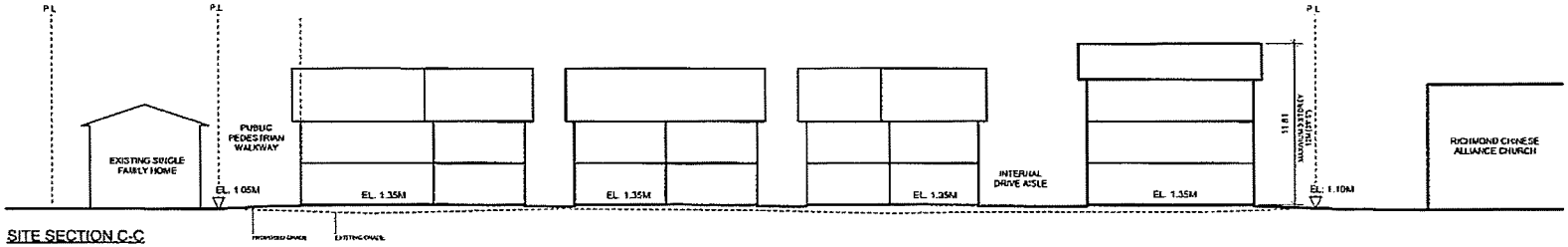
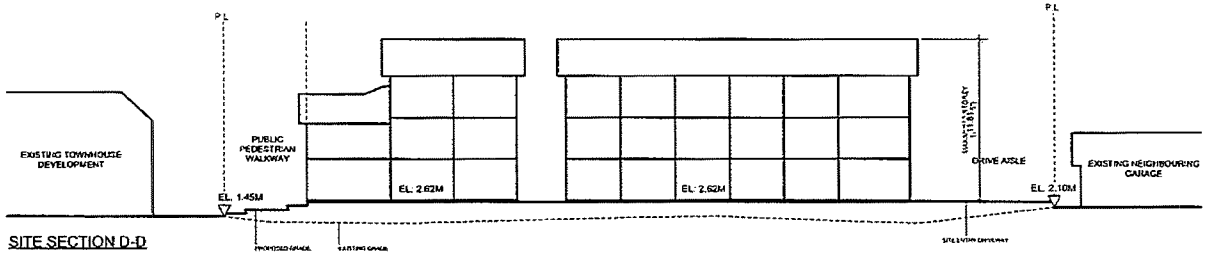


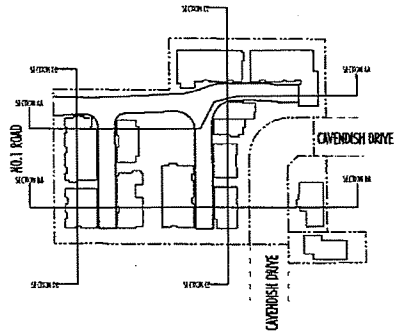
SITE SECTION C-C



SITE SECTION D-D

SITE SECTIONS

SCALE: 1:150



KEY PLAN

BOHAN PROPERTIES

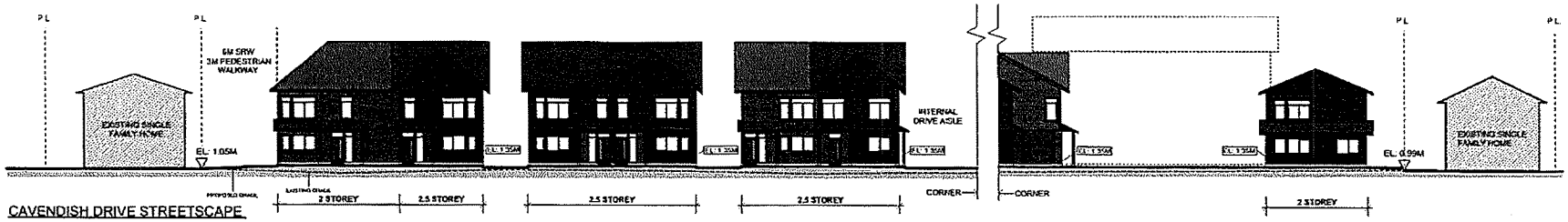
YAMAMOTO ARCHITECTURE

TOWNHOUSE DEVELOPMENT

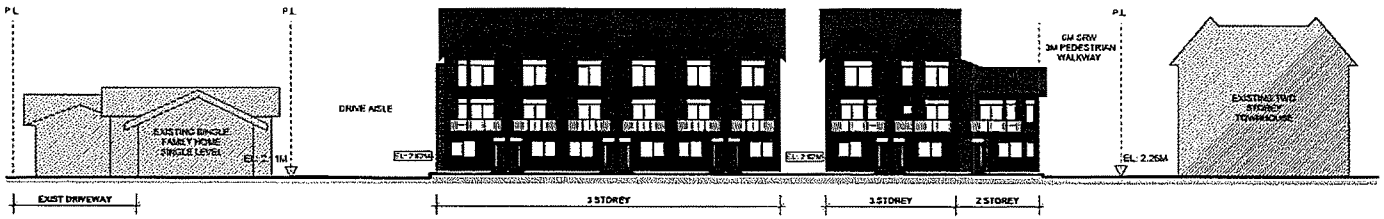
SITE SECTIONS

A1.5

PH - 114



CAVENDISH DRIVE STREETSCAPE



NO. 1 ROAD STREETSCAPE

STREETSCAPES

SCALE: 1:150

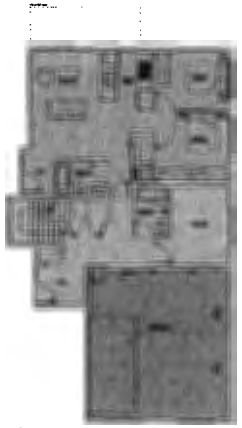
BOHAN
PROPERTIES

YAMAMOTO
ARCHITECTURE

TOWNHOUSE DEVELOPMENT

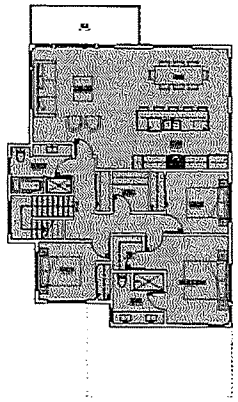
NO 1 ROAD & CAVENDISH DRIVE
STREETSCAPES

A1.6

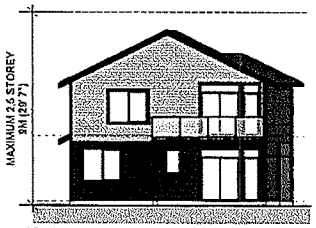


LOT A

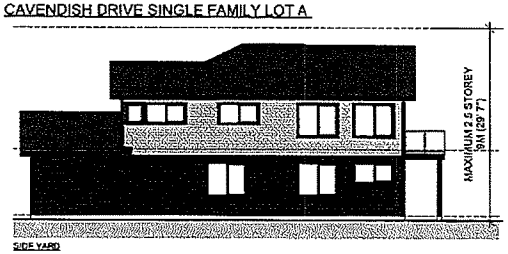
MAIN FLOOR: 1049 SQ.FT.
GARAGE: 530.9 SQ.FT.

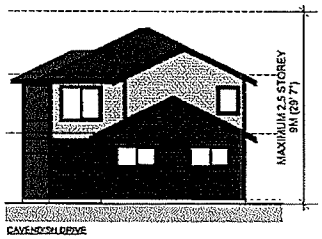


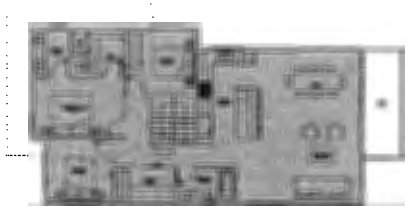
SECOND FLOOR: 1334 SQ.FT.



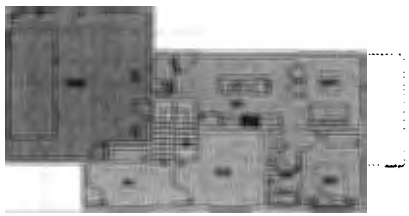
CAVENDISH DRIVE SINGLE FAMILY LOT A





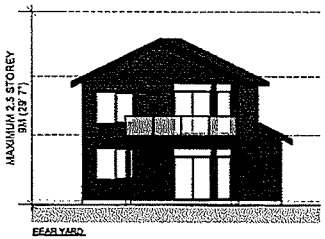


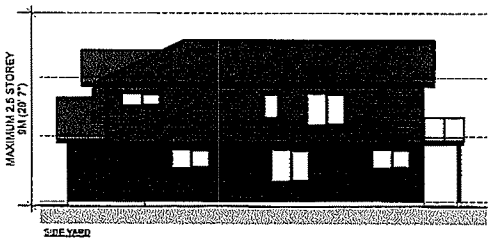
SECOND FLOOR: 1267 SQ.FT.

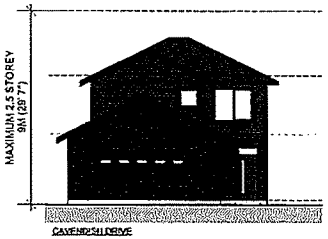


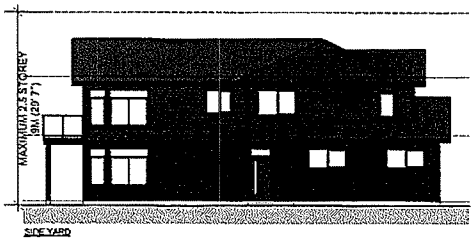
LOT B

MAIN FLOOR: 864 SQ.FT.
GARAGE: 500 SQ.FT.









CAVENDISH DRIVE SINGLE FAMILY LOT B

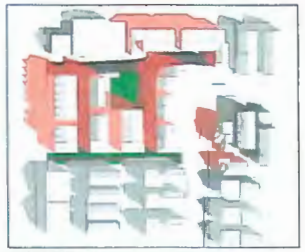
BOHAN
PROPERTIES

YAMAMOTO
ARCHITECTURE

TOWNHOUSE DEVELOPMENT

SINGLE FAMILY FLOORPLANS
AND EXTERIOR ELEVATIONS

A1.7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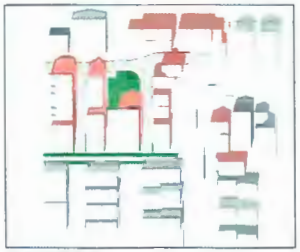
21 MARCH 12PM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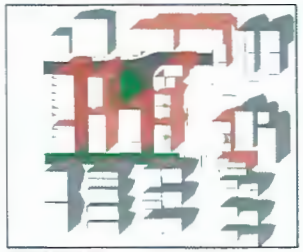
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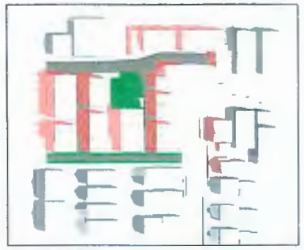
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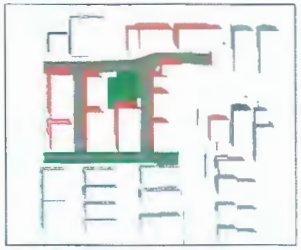
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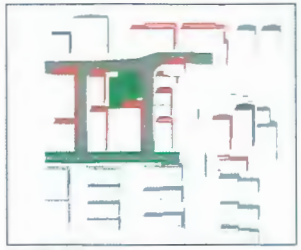
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21 JUNE 4PM

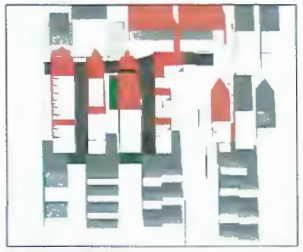
21 SEPTEMBER 10AM

21 SEPTEMBER 12PM

21 SEPTEMBER 2PM

21 SEPTEMBER 4PM

21 DECEMBER 10AM


21 DECEMBER 12PM

21 DECEMBER 2PM

21 DECEMBER 4PM

BOHAN
PROPERTIES

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ARCHITECTURE
—
302 - 33 East 8th Avenue
Vancouver BC V5T 1R8
T - 604 731 1127 F - 604 731 1327

TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO 1 ROAD AND
4051 4066 CAVENDISH DRIVE, RICHMOND BC

PROJECT SHADOW STUDY

DATE MAY 06 2019	A8
DRAWN BS	
REV. No. 1711	

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pmg LANDSCAPE ARCHITECTS

Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6G9
p 604 294-0011 f 604 294-0022

SCALE

NO	DATE	REVISION DESCRIPTION	DR
1	18 JAN 29	TREE MANAGEMENT PLAN - JAN 29	RC
2	19 JAN 31	REV TYP PLAN	RC
3	19 MAR 21	TREE MANAGEMENT PLAN - MAR 21	RC
4	19 MAY 14	PRELIMINARY	RC
5	19 JUL 24	REVISED PLAN	RC

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

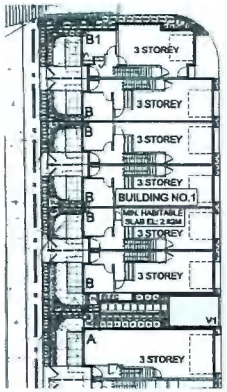
10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

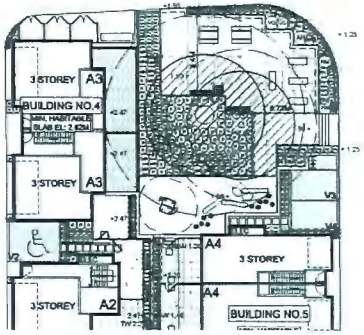
LANDSCAPE PLAN

DATE	19 MAR 31	DRAWING NUMBER
SCALE	1/16"=1'-0"	**L1**
DRAWN	RC	
DESIGN	RC	
CHKD	PCM	OF 8

PMG PROJECT NUMBER 19-013



1 SHRUB PLAN ENLARGEMENT 1



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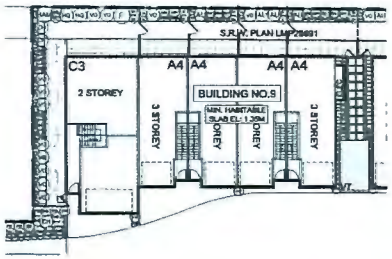

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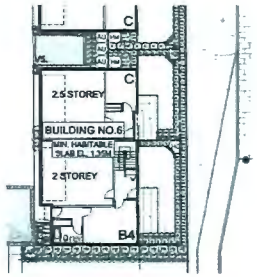

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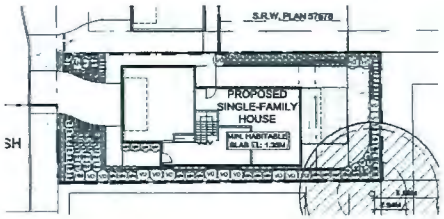


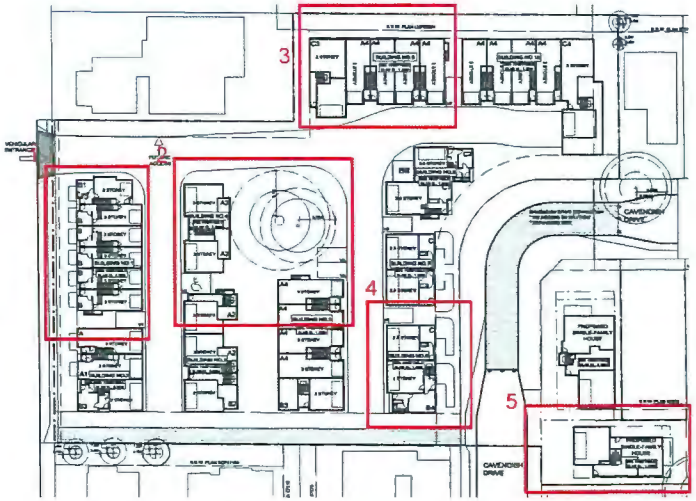
5 SHRUB PLAN ENLARGEMENT 5

PLANT SCHEDULE

PMG PROJECT NUMBER: 19-013

KEY	QTY.	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
SHRUB				
		ARBUTUS UNEDO 'COMPACTA'	COMPACT STRAWBERRY BUSH	#3 POT 60CM
		AUCUBA JAPONICA 'SERRATIFOLIA'	SAWTOOTHED JAPANESE AUCUBA	#3 POT 50CM
		BUXUS MICROPHYLLA 'WINTER GEM'	LITTLE LEAF BOX	#5 POT 40CM
		CARYOPTERIS x CLANDONENSIS 'DARK KNIGHT'	BLUEBEARD	#2 POT 40CM
		CHOISYA TERNATA 'SUNDANCE'	MEXICAN MOCK ORANGE	#3 POT 50CM
		FATSIA JAPONICA	JAPANESE ARALIA	#3 POT
		HAMAMELIS MOLLIS	CHINESE WITCH HAZEL	#3 POT 80CM
		HYDRANGEA MACROPHYLLA 'NIKKO BLUE'	BLUEAF HYDRANGEA (BLUE)	#3 POT 60CM
		HYDRANGEA QUERCIFOLIA	OAKLEAF HYDRANGEA	#3 POT 60CM
		KALMIA LATIFOLIA 'ELF'	DWARF MOUNTAIN LAUREL	#3 POT 50CM
		MAHONIA NERVOSA	LONGLEAF MAHONIA	#2 POT 40CM
		NANDINA DOMESTICA 'FIREPOWER'	FIREPOWER HEAVENLY BAMBOO	#2 POT 40CM
		PIERIS JAPONICA 'FOREST FLAME'	PIERIS WHITE BLOOMS	#5 POT 50CM
		PRUNUS LAUROCERASUS 'OTTO LUYKEN'	OTTO LUYKEN LAUREL	#3 POT 50CM
		RHODODENDRON 'BOW BELLS'	RHODODENDRON	#5 POT 50CM
		RHODODENDRON 'P J M'	RHODODENDRON LIGHT PURPLE E MAY	#3 POT 50CM
		ROSA MEDILAND 'BONICA'	BONICA ROSE	#2 POT 40CM
		ROSA MEDILAND 'RED'	MEDILAND ROSE RED	#2 POT 40CM
		SKIMMIA JAPONICA (NON MALE)	JAPANESE SKIMMIA	#2 POT 30CM
		SPIRAEA JAPONICA 'LITTLE PRINCESS'	LITTLE PRINCESS SPIRAEA PINK	#2 POT 40CM
		TAXUS x MEDIA 'HILLI'	HILLI YEW	1.2M BAR
		VACCINIUM OVATUM 'THUNDERBIRD'	EVERGREEN HUCKLEBERRY	#3 POT 60CM
		WIGGELA FLORIDA 'PURPUREA'	PURPLE WIGGELA	#3 POT 60CM
GRASS				
		CAREX OSHIMENSIS 'EVERGOLD'	EVERGOLD JAPANESE SEDGE	#1 POT
		FESTUCA OVINA 'GLAUCA'	BLUE FESCUE	#1 POT
		MISCANTHUS SINENSIS 'ADAGIO'	ADAGIO DWARF JAP SILVER GRASS	#2 POT
		MISCANTHUS SINENSIS 'GRACILLIMUS'	MAIDEN GRASS	#2 POT
		PENNISETUM ALOPECUROIDES 'HAMELIN'	DWARF FOUNTAIN GRASS	#1 POT
		PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#1 POT
		STIPA TENUISSIMA	MEXICAN FEATHER GRASS	#1 POT
PERENNIAL				
		ECHINACEA PURPUREA	PURPLE CONEFLOWER	19CM POT
		GERANIUM SANGUINEUM 'JOHN ELLIOTT'	GERANIUM MAGENTA	19CM POT
		HEMEROCALLIS 'WHITE TEMPATION'	DAYLILY WHITE	#1 POT 20CM
		HOSTA 'PATRIOT'	HOSTA GREEN AND WHITE VARIEGATED	#1 POT 1 EYE
		LAVENDULA ANGUSTIFOLIA 'MUNSTEAD'	ENGLISH LAVENDER COMPACT VIOLET-BLUE	#1 POT
		RUDBECKIA FULGIDA VAR SULLIVANTII 'GOLDSTURM'	RUDBECKIA YELLOW	19CM POT
		STACHYS BYZANTINA 'SILVER CARPET'	LAMBS EAR	19CM POT
		DRYOPTERIS ERYTHROSORA 'BRILLIANCE'	BRILLIANCE AUTUMN FERN	#2 POT 45CM
		ERICA CARNEA 'SPRINGWOOD PINK'	WINTER HEATH PINK	#1 POT
		GAULTHERIA SHALLON	SALAL	#1 POT 20CM
		PACHYSANDRA TERMINALIS	JAPANESE SPUROE	#1 POT 15CM
		POLYSTICHUM MUNITUM	WESTERN SWORD FERN	#1 POT 25CM

NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER CNLA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND FRASER VALLEY. * SUBSTITUTIONS: OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS. TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD. DEFINITION OF CONDITIONS OF AVAILABILITY. * ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. * ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. * BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.



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SEAL

NO	DATE	REVISION DESCRIPTION	DR
5	21 JUN 21	REV Townhouse Site Plan UPDATED	RJ
4	21 JUN 21	New Site Plan	RJ
3	21 MAY 24	REV Townhouse Site Plan UPDATED	RJ
2	21 MAR 18	Issued for DP	RJ
1	19 JAN 30	Issued for DP	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

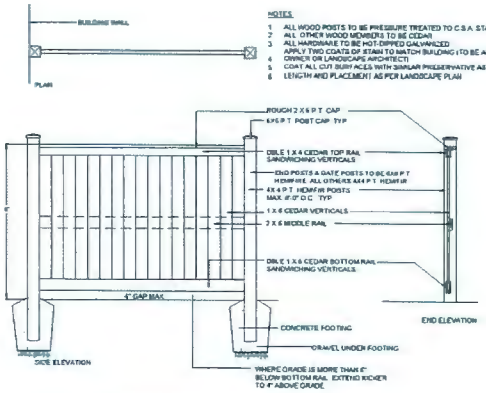
10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

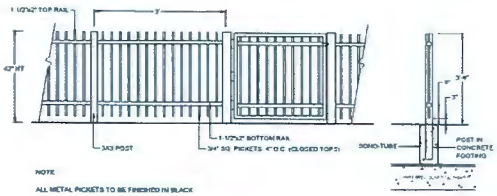
SHRUB ENLARGEMENT

DATE	19 JAN 31	DRAWING NUMBER
SCALE	1/16"=1'-0"	L2
DRAWN	RJ	
DESIGN	RJ	
CHK'D	PCM	OF 8

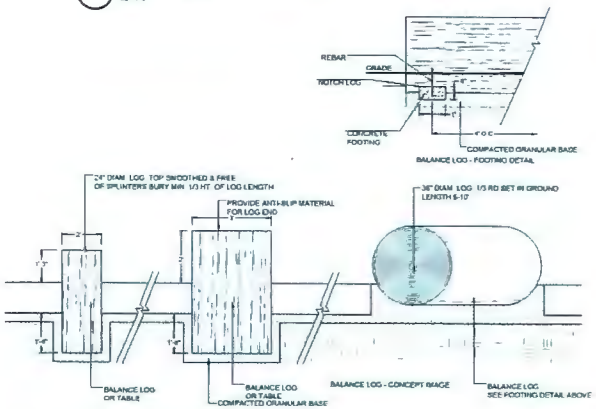
19013-4 PMG PROJECT NUMBER 19-013



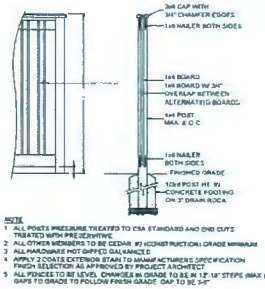
1 6' HT. WOOD PRIVACY FENCE
1/2"=1'-0"



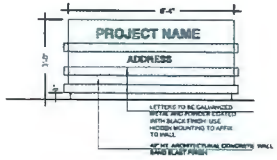
2 42" HT. METAL FENCE AND GATE
1/2"=1'-0"



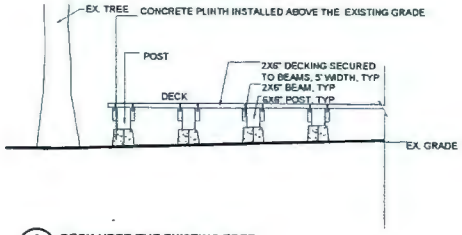
3 BALANCE LOGS
1/2"=1'-0"



4 6' HEIGHT SOLID WOOD FENCE
1/2"=1'-0"



5 DEVELOPMENT SIGNAGE
1/2"=1'-0"

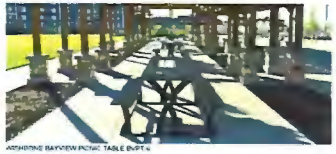


6 DECK UDER THE EXISTING TREE
1/2"=1'-0"

MACLIN BIKE RACK 350-4-S

WISHBONE BAYVIEW BENCH

WISHBONE BAYVIEW TABLE


WISHBONE BAYVIEW PICNIC TABLE 6'-0" L

KOMFAN M79949P RED HOUSE

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SEAL

NO	DATE	REVISION DESCRIPTION	DR
5	19.JAN.21	PMG MINIMALIST RE PLAN UPDATED	RJ
4	20.JAN.21	NEW SITE PLAN	RJ
3	14.MAY.20	PMG MINIMALIST RE PLAN UPDATED	RJ
2	15.MAY.19	PLAN & PLANT UP	RJ
1	29.APR.19	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

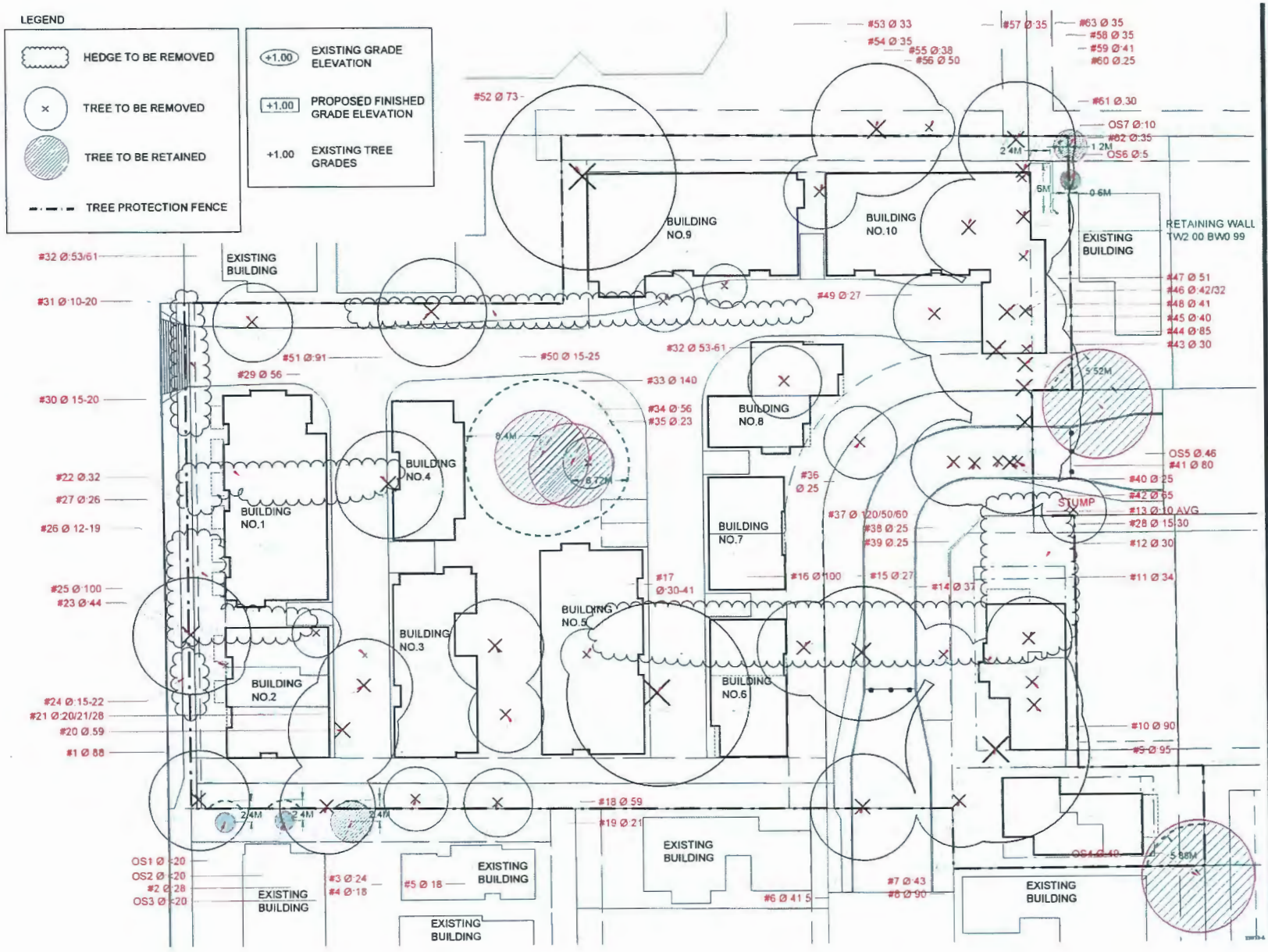
10140, 10160, 10180 NO. 1 ROAD
AND 4051, 4068 CAVENDISH DRIVE
RICHMOND, BC

DRAWING TITLE

LANDSCAPE DETAILS

DATE	19.JAN.21	DRAWING NUMBER
SCALE	AS SHOWN	L3
DRAWN	RJ	
DESIGN	RJ	
CHK'D	PCM	OF 8

PMG PROJECT NUMBER 19-013



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SEAL

NO	DATE	REVISION DESCRIPTION	DR
5	19.MAR.29	TREE MANAGEMENT PLAN REVISED	NI
4	19.MAR.21	NEW SITE PLAN	NI
3	19.MAR.21	TREE MANAGEMENT PLAN REVISED	NI
2	19.MAR.14	PLANTING PLAN	NI
1	19.JAN.24	NEW SITE PLAN	NI

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE MANAGEMENT PLAN

DATE	13.JAN.11	DRAWING NUMBER
SCALE	1/16"=1'-0"	L4
DRAWN	BJ	
DESIGN	BJ	
CHK'D	PCM	OF 8
PMG PROJECT NUMBER		19-013

TREE INFORMATION TABLE

Tree #	Species	DBH (cm)	Health & Condition	CRZ (Min) (m)	CRZ (preferred) (m)	Comments/Recommendations
1	Fir	88	Poor	5.28	10.56	• Previously topped • hydro pruned • heavily covered in ivy **Recommend:** • Remove.
2	Walnut	28	Good-Fair	1.68	3.36	• Previously topped/pruned. **Recommend:** • Consider for removal.
3	Pine	24	Poor	1.44	2.88	• Previously topped. **Recommend:** • Remove
4	Mtn Ash	18	Poor	1.08	2.16	**Recommend:** • Remove
5	Apple	18	Poor	1.08	2.16	• Extensive decay. **Recommend:** • Remove
6	Cherry	41.5	Fair	2.49	4.98	**Recommend:** • Consider for removal.
7	Fir	43	Poor	2.58	5.16	• 1-sided canopy. **Recommend:** • Remove
8	Poplar	90	Poor	5.4	10.8	• Large limbs prev. Removed • Visible decay. **Recommend:** • Remove
9	Lombardy Poplar	95	Fair	5.7	11.4	• Overmature • Frequent shedding of limbs likely. **Recommend:** • Consider for removal.
10	Lombardy Poplar	90	Fair	5.4	10.8	• Overmature • Frequent shedding of limbs likely. **Recommend:** • Consider for removal.
11	Fir	34	Poor	2.04	4.08	• Lean towards east. **Recommend:** • Remove
12	Spruce	30	Good	1.8	3.6	**Recommend:** • Consider for removal.
13	Hazelnut	10 avg	Fair	0.6	1.2	**Recommend:** • Consider for removal.
14	Apple	37	Poor	2.22	4.44	• 4 main stems • heavily leaning towards property. • heaving roots • extensive decay. **Recommend:** • Remove
15	Walnut	27	Fair	1.62	3.24	**Recommend:** • Consider for removal.
16	Willow	100	Poor	6.0	12.0	• Deadwood & decay throughout canopy. **Recommend:** • Remove
17	Golden cedar	30-41	Good-Fair	2.46	4.92	• Hedge row, 3 stems prev. topped. **Recommend:** • Consider for removal.
18	Spruce	59	Poor	3.54	7.08	• Previously topped • high heeled roots. **Recommend:** • Remove
19	Magnolia	21	Poor	1.26	2.52	• Poor pruning history/canopy structure. **Recommend:** • Remove
20	Cherry	59	Poor	3.54	7.08	• Multi-stem • visible decay. **Recommend:** • Remove
21	Birch	20/ 21/ 28	Poor	1.68	3.36	• Multi-ste • previously topped • visible decay. **Recommend:** • Remove
22	Spruce	32	Poor	1.92	3.84	• Previously topped. **Recommend:** • Remove
23	Cedar	44	Good-Fair	2.64	5.28	• Slight lean towards property. **Recommend:** • Consider for removal.

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SEAL

NO	DATE	REVISION DESCRIPTION	DR.
5	21 JAN 21	[illegible]	AJ
4	20 JAN 21	[illegible]	AJ
3	14 FEB 20	[illegible]	AJ
2	14 FEB 20	[illegible]	AJ
1	20 SEP 19	[illegible]	AJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE INFORMATION TABLE

DATE 19 JAN 21
SCALE
DRAWN AJ
DESIGN AJ
CHKD PCM

DRAWING NUMBER **L5** OF 8

PMG PROJECT NUMBER 19-013

24	Holly x 4	15-22	Poor	1.32	2.64	• Hedge row. **Recommend:** • Remove
25	Fir	100	Poor	6.0	12.0	• Previously topped. • hydro pruned • heavily covered in ivy. **Recommend:** • Remove
26	Excelsa cedar	12-19	Fair-Poor	1.14	2.28	• Hedge row. • Previously topped • Hard pruned. **Recommend:** • Consider for removal.
27	Birch	26	Very poor	1.56	3.12	**Recommend:** • Remove
28	Excelsa cedar	15-30	Fair	1.8	3.6	• Hedge row. • Previously topped. **Recommend:** • Consider for removal.
29	Pine	56	Poor	3.36	6.72	• Previously topped to hedge height. **Recommend:** • Remove
30	Excelsa / Emerald cedar	15-20	Fair-Poor	1.2	2.4	• Previously topped. **Recommend:** • Consider for removal.
31	Excelsa cedar	10-20	Fair-Poor	1.2	2.4	• Hedge row. • Previously topped. **Recommend:** • Consider for removal.
32	Deodar cedar	53 / 61	Poor	3.66	7.32	• Twin stem • previously topped. **Recommend:** • Remove
33	Sequoia	140	Fair	8.4	16.8	• Species is rapid grower **Recommend:** • Retain per City request; install tree protection fencing • Arborist supervision required during excavations for adjacent building.
34	Spruce	56	Poor	3.36	6.72	**Recommend:** • Retain per City request; install tree protection fencing
35	Spruce	23	Poor	1.38	2.76	• Conflict with proposed amenity area **Recommend:** • Remove
36	Willow	25	Poor	1.5	3.0	**Recommend:** • Remove
37	Poplar x 3	120 / 50 / 60	Poor	7.2	14.4	• Row of 3 trees. **Recommend:** • Remove
38	Cedar x 4	25	Poor	1.5	3.0	• Row of 4 trees. **Recommend:** • Remove
39	Dogwood	25	Poor	1.5	3.0	**Recommend:** • Remove
40	Cedar	25	Poor	1.5	3.0	**Recommend:** • Remove
41	Poplar	80	Fair-Poor	4.8	9.6	**Recommend:** • Consider for removal.
42	Polar	65	Fair-Poor	3.9	7.8	**Recommend:** • Consider for removal.
43	Pine	30	Poor	1.8	3.6	**Recommend:** • Remove
44	Poplar	85	Fair-Poor	5.1	10.2	**Recommend:** • Consider for removal.
45	Cedar	40	Poor	2.4	4.8	**Recommend:** • Remove
46	Pine / Maple	42/ 32	Poor	1.92	3.84	• Codominant trees. **Recommend:** • Remove
47	Fir	51	Dead	3.06	6.12	**Recommend:** • Remove
48	Willow	41	Poor	2.46	4.92	• Previously topped. **Recommend:** • Remove
49	Plum	27	Very poor	1.62	3.24	• Actively failing • broken limbs • decay. **Recommend:** • Remove
50	Holly/ Excelsa cedar	15-25	Poor	1.5	3.0	• Hedge row. **Recommend:** • Remove
51	Pine	91	Poor	5.46	10.92	• Previously topped at several heights • twin stem • included bark. **Recommend:** • Remove
52	Willow	73	Very Poor	4.38	8.76	• Extensive decay. **Recommend:** • Remove

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SEAL

NO	DATE	REVISION DESCRIPTION	DR.
5	[illegible]	[illegible]	[illegible]
4	[illegible]	[illegible]	[illegible]
3	[illegible]	[illegible]	[illegible]
2	[illegible]	[illegible]	[illegible]
1	[illegible]	[illegible]	[illegible]

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BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE INFORMATION TABLE

DATE [illegible] DRAWING NUMBER
SCALE
DRAWN BJ **L6**
DESIGN BJ
CHKD FDM OF 8

19013-6 PMG PROJECT NUMBER 19-013

53	Fir	33	Poor	1.98	3.96	• Previously topped • decay. **Recommend:** • Remove
54	Plum	35	Dying	2.16	4.32	• Failed limbs • decay. **Recommend:** • Remove
55	Willow	38	Very poor	2.28	4.56	• Multiple stems. **Recommend:** • Remove
56	Willow	50	Very poor	3.0	6.0	• Multiple stems • Decay. **Recommend:** • Remove
57	Willow	35	Very poor	2.1	4.2	• Multiple stems • Decay. **Recommend:** • Remove
58	Willow	35	Very poor	2.1	4.2	• Multiple stems • Decay. **Recommend:** • Remove
59	Birch	41	Poor	2.46	4.92	• Twin stems. **Recommend:** • Remove
60	Willow	25	Very poor	1.5	3.0	• Multiple stems • Decay. **Recommend:** • Remove
61	Willow	30	Very poor	1.8	3.6	• Multiple stems • Decay. **Recommend:** • Remove
62	Willow	35	Very poor	2.16	4.32	• Multiple stems • Decay. **Recommend:** • Remove
63	Willow	35	Very poor	2.16	4.32	• Multiple stems • Decay. **Recommend:** • Remove
OS1	Excelsa	<20	Good	1.2	2.4	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line

OS2	Excelsa	<20	Good	1.2	2.4	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line
OS3	Fir	<20	Good	1.2	2.4	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line
OS4	Deodar cedar	49	Good	2.94	5.88	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing • **Arborist supervision is recommended during excavations required for proposed single family house**
OS5	Maple	46	Good	2.76	5.52	• Blvd./Neighbour's property. **Recommend:** • Retain; install tree protection fencing
OS6	Walnut	5	Good	0.3	0.6	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line
OS7	Lilac	10	Good	0.6	1.2	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line

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SEAL

NO	DATE	REVISION DESCRIPTION	DR

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

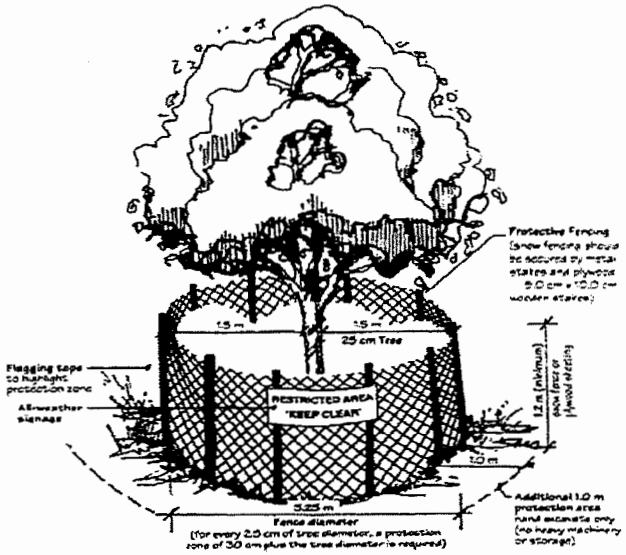
DRAWING TITLE

TREE INFORMATION TABLE

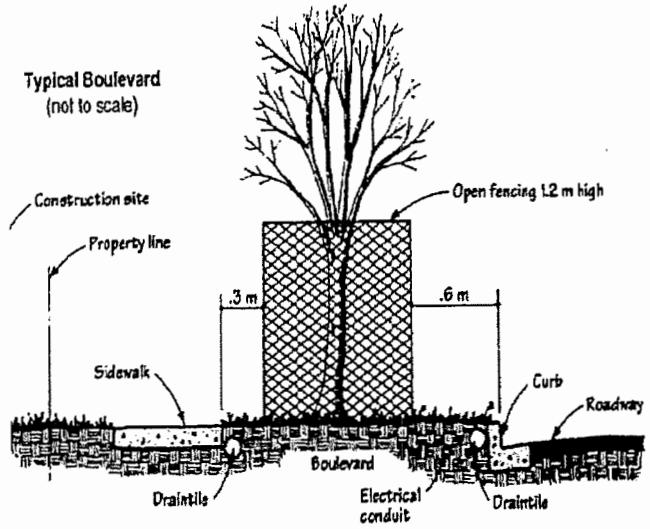
DATE 19.JAN.14 DRAWING NUMBER
SCALE
DRAWN RJ **L7**
DESIGN RJ
CHKD RCM OF 8

PMG PROJECT NUMBER 19-013

SUITABLE REPLACEMENT TREES	
SPECIES	
COMMON NAME	BOTANICAL NAME
ACER PALMATUM	JAPANESE MAPLE
ACER RUBRUM 'RED SUNSET'	RED SUNSET MAPLE
AMELANCHIER x GRANDIFLORA 'AUTUMN BRILLIANCE'	AUTUMN BRILLIANCE SERVICEBERRY
CERCIDIPHYLLUM JAPONICUM	KATSURA TREE
CORNUS KOUSA 'CHINENSIS'	CHINESE KOUSA DOGWOOD
LIQUIDAMBAR STYRACIFLUA 'SLENDER SILHOUETTE'	SLENDER SILHOUETTE SWEETGUM
LIQUIDAMBAR STYRACIFLUA 'WORPLESDON'	WORPLESDON SWEET GUM
PINUS NIGRA 'ARNOLD SENTINEL'	ARNOLD SENTINEL AUSTRIAN BLACK PINE
PSEUDOTSUGA MENZIESII	DOUGLAS FIR
PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR
QUERCUS PALUSTRIS 'GREEN PILLAR'	GREEN PILLAR PIN AK



1 TREE PROTECTION BARRIER



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SEAL

NO.	DATE	REVISION DESCRIPTION	DR
4	19 JAN 21	[illegible]	N
3	19 JAN 21	[illegible]	N
2	19 NOV 20	[illegible]	N
1	13 MAR 20	[illegible]	N
0	24 APR 19	[illegible]	N

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

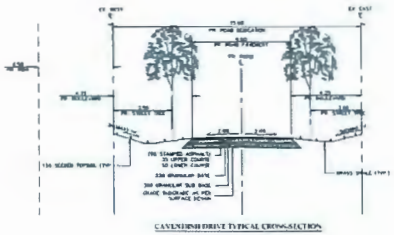
10140, 10160, 10180 NO. 1 ROAD AND 4051, 4063 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE MANAGEMENT DETAIL

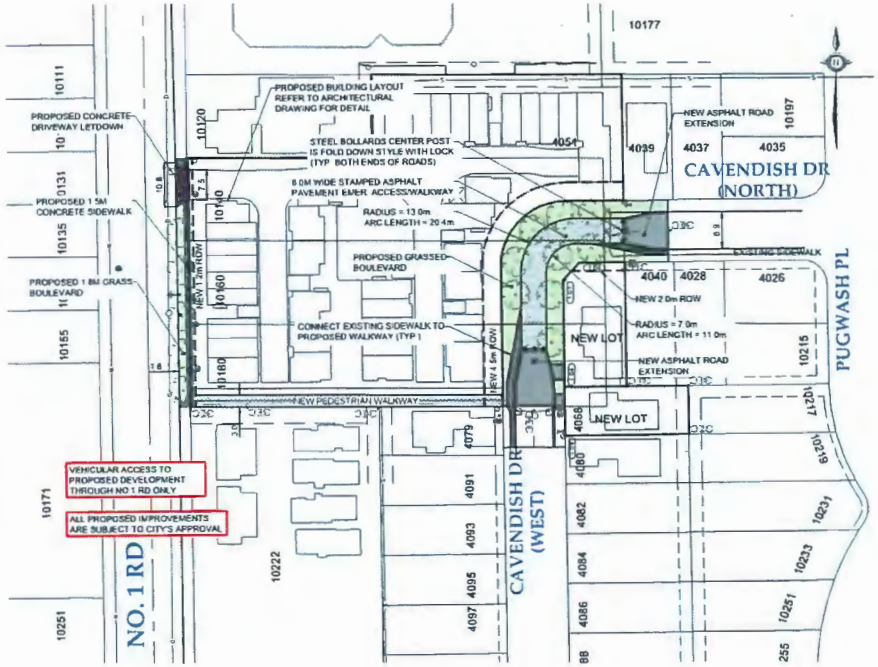
DATE	19 JAN 21	DRAWING NUMBER
SCALE		L8
DRAWN	RJ	
DESIGN	RJ	
CHKD	FCM	OF 8

PMG PROJECT NUMBER 19-013



FOLD DOWN LOCKABLE STEEL BOLLARD
N.T.S.

THIS IS A TYPICAL FOLD DOWN LOCKABLE BOLLARD. FINAL BOLLARD STYLE MAY NOT MATCH THAT SHOWN.



PLAN
SCALE: 1:500

PLAN SUBJECT TO CITY OF RICHMOND APPROVAL

NOTES

ALL ELEVATIONS ARE TO GEODETIC DATUM AND REFER TO RICHMOND BENCHMARK NUMBER

ELEVATION

ALL MATERIALS AND CONSTRUCTION TO BE IN ACCORDANCE WITH THE CURRENT MASTER MUNICIPAL CONSTRUCTION DOCUMENTS (MMCD) AND THE CURRENT CITY OF RICHMOND SUPPLEMENTARY SPECIFICATIONS AND DETAIL DRAWINGS, UNLESS OTHERWISE NOTED.

CoreConcept CONSULTING LTD
#220-2639 Viking Way, Richmond, BC, V6V 3B7
tel 604 249 5040
fax 604 249 5041

BOHAN PROPERTIES

Core Concept Consulting Project No. 17101
DWG. 1 OF 1

REV'N	DATE	BY	CK	DESCRIPTION
1	20 JUN 2018	BRh	BRh	REVISED AS PER CITY AND PUBLIC FEEDBACK
0	29 JUN 2018	BL	BRh	ISSUE FOR PUBLIC INFORMATION MEETING

REVISIONS

29 JUN 2018

City of Richmond
6911 NO. 3 ROAD RICHMOND B.C. V6Y 2C1

TITLE
PRELIMINARY ROAD LAYOUT
10140-10180 NO 1 ROAD
CITY FILE: —

DESIGN	BL		
DRAWN	BL	DWG. No.	
CHECKED	BRh	SCALE 1:500	DATE JUN 2018
ENGINEER	AV	SLC No. 35-4-7	SHT No. 1 OF 1

APPENDIX C

PRESENTATION BOARDS

PUBLIC INFORMATION MEETING - SUMMARY OF ATTENDEE FEEDBACK

Date: Wednesday, June 26, 2019
Location: Richmond Chinese Alliance Church
Summary Revised September 16th, 2019

RZ18-820669
10140 – 10180 No. 1 Road &
4051 and 4068 Cavendish Drive

Name	Address	Site Distance	OCP Ammend. Support	Road Connect. Support	New S/F Lot Support	Cavendish to No. 1 Road Walkway	Building Height Support	Building Setback Support	Other Feedback
Michaels, Rick	4028 Cavendish Drive, Richmond	75m	-	-	-	-	-	-	Feedback by Email (Refer to Appendix E). General summary is that Rick had concerns with the siting of the Single Family lots. Rick's suggests equalizing the side yards to 2m, reducing the front yard setback to provide a larger rear yard and reviewing options for reducing the separation between the second floor layouts between houses.
Sun, Ben & Theresa	4040 Cavendish Drive, Richmond	60m	-	-	-	-	-	-	Most concerned that the proposed single family homes on Cavendish Drive allow for overlook into their rear yard. Suggestions are provided to move the single family houses forward on the site (reducing the front yard setback) and aligning them with existing neighbouring houses.
Kozij, Grant	4080 Cavendish Drive, Richmond	60m	Unsure	Yes	No	No	Unsure	Unsure	Concerns that the walkway form No.1 Road to Cavendish Drive will increase crime in the area and does not link to buses.
Randall, Craig	4082 Cavendish Drive, Richmond	80m	-	-	-	-	-	-	No feedback form completed
Melvin, Monica & Bryan	4084 Cavendish Drive, Richmond	90m	-	-	-	-	-	-	Would like the proposed street trees along the Cavendish Drive connection to match the existing street trees. Proposed planting should also be slow growing and minimal root depth. No Fir or Maple trees.
Mah, Paul	4095 Cavendish Drive, Richmond	80m	Yes	Yes	Yes	No	No	Unsure	Does not want to have the Cavendish Drive Townhouses to have access to the Cavendish Drive connection due to concerns of increased pedestrian and vehicle traffic and parking. Would like to see a maritime architectural style incorporated into the building exterior detailing. Does not support the walkway between No.1 Road and Cavendish Road due to potential increase in crime. Would like to see non-invasive trees planted as part of the development and provided an alternative for the walkway location.
Danny,	4113 Cavendish Drive, Richmond	130m	Yes	No	No	No	No	Yes	REV. 190916 Form recevied from Edwin Lee on the 6th September. Does not want Cavendish Road connection, single family lots and would prefer two level buildings.
Masson, Anne & Neil	10186 Pugwash Pl., Richmond	145m	Yes	Yes	Yes	Yes	Unsure	Yes	Primary comment to plant more trees, Current trees are mature and provide a beautiful addition to the area and wildlife.

PUBLIC INFORMATION MEETING - SUMMARY OF ATTENDEE FEEDBACK

Date: Wednesday, June 26, 2019
Location: Richmond Chinese Alliance Church
Summary Revised September 16th, 2019

RZ18-820669
10140 – 10180 No. 1 Road &
4051 and 4068 Cavendish Drive

Name	Address	Site Distance	OCP Ammend. Support	Road Connect. Support	New S/F Lot Support	Cavendish to No. 1 Road Walkway	Building Height Support	Building Setback Support	Other Feedback
Nathan	No Address, phone only		Yes	Yes	Yes	Yes	Yes	Yes	Overall support of the development, No. 1 Road / Cavendish Drive walkway and Cavendish Drive connection.
Pearl	10222 No. 1 Road, Richmond	250m	-	-	-	-	-	-	No feedback form completed
Curtis	Partial Name, phone only	-	Yes	Yes	Yes	Yes	Yes	Yes	In support of the overall development.
Marco	Partial Name, phone only	-	-	-	-	-	-	-	No feedback form completed

APPENDIX E

ORIGINAL FEEDBACK FORMS

Cavendish Public Information Meeting, Sign-In Sheet

Location: Richmond Chinese Alliance Church

Address: 10100 No. 1 Road, Richmond

Date: June 26, 2019

Time: 5pm to 8pm

	Name	Phone Number	Address
1	KANDALL	604 2785578	4082 CAVENDISH
2	Melvin	604-275-2274	4084 Cavendish Dr.
3	Nathan	604-764-8374	
4	PAUL	778 297 1715	4095 CAVENDISH
5	Grant	604-241-4366	4080 Cavendish
6	Curtis	778.939.9884	
7	Marco	604.771.2449.	
8	Anne	604 2720537	PUGWASH PL.
9	NEIL	604 2720537	10186 PUGWASH PLACE
10	Ben	604 274 5090	4040 CAVENDISH
11	Danny	778-840-1204	4113 Cavendish
12	Pearl	778-688 4420.	10222 No1 Rd
13			
14			
15			
16			
17			
18			
19			
20			

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Nathan [redacted]

Address: [redacted]

Phone (Optional): 604-764-8574

Email (Optional): nathan.jylchan@gmail.com

Date: 06/26/2019

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☒ No ☐ Unsure ☐

Comments:

This provides more potential housing for Richmond citizens. This will increase efficiency of the usage of land.

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

This reduces the chance of potential congestion from non-emergency vehicles while opening up the dead ends.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

Adding to the community, this is a nice touch. Most developments focus on one-type of home. Rather, two new single-family lots increases the diversity of the neighbourhood (property-wise).

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☒ **No** ☐ **Unsure** ☐

Comments:

~~[illegible]~~ with the aforementioned drive being for emergency vehicles only, the public walkway will allow residents to move between these areas without congesting the emergency vehicle area.

Do you support the proposed building heights?

Yes ☒ **No** ☐ **Unsure** ☐

Comments:

Heights are not too tall which is great. This will allow the development to better amalgamate into the other properties in the area.

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☒ **No** ☐ **Unsure** ☐

Comments:

N/A

What kind of Architectural style would you like to see on this site?

Ideally, something more architecturally pleasing in line with Vancouver's newer styles. The development can take the lead in changing the architecture in the area.

Other recommendations or suggestion:

The plant/tree removal, of nothing ones, is excellent. The lot only allows so many amenities due to the limited space. However, it all makes sense and addresses existing community issues (ie cutting trees).

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Anne Masson

Address: Dunwarth Dr

Phone (Optional):

Email (Optional):

Date: June 26

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☒ No ☐ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☒ No ☐ Unsure ☐

Comments:

Do you support the proposed building heights?

Yes ☐ No ☐ Unsure ☒

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☒ No ☐ Unsure ☐

Comments:

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

Plant more trees - Current trees are mature and provide a beautiful addition to the area and houses wildlife.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Monica Melvin

Address: 4084 Cavendish Dr.

Phone (Optional): 604-275-2274

Email (Optional):

Date: June 26/19

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ No ☐ Unsure ☐

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☐ No ☐ Unsure ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ No ☐ Unsure ☐

Comments:

On Other Page.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ No ☐ Unsure ☐

Comments:

Do you support the proposed building heights?

Yes ☐ No ☐ Unsure ☐

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ No ☐ Unsure ☐

Comments:

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

*on Landscape Drawing Code 14. "Street Tree" *

Re: Trees being replaced on the Pedistrian Access at the end of the Cavendish Drive. Please plant trees or shrubs that will grow slowly and not create deep spreading roots which damage drive ways. If you can match the trees that are already on Cavendish Drive that would be nice. It would blend in better. They are slow growing and not so large with shallower roots. Please no firs or maples.

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Curtis Eaves

Address:

Phone (Optional):

Email (Optional):

Date: June 26/2019

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☑ No ☐ Unsure ☐

Comments:

100% support this. This area needs more multi family and there is demand for it

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

Will improve pedestrian access

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☑ No ☐ Unsure ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

Yes will be a great solution to the multi family development

Do you support the proposed building heights?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

What kind of Architectural style would you like to see on this site?

~~Modern~~ ~~but~~ Modena and effective use of all space

Other recommendations or suggestion:

GREATER FSR

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: _____

Address: _____

Phone (Optional): _____

Email (Optional): _____

Date: _____

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ **No** ☐ **Unsure** ☐

Comments:

_____

_____

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☐ **No** ☐ **Unsure** ☐

Comments:

_____

_____

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ **No** ☐ **Unsure** ☐

Comments:

_____

_____

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ **No** ☐ **Unsure** ☐

Comments:

_____

_____

Do you support the proposed building heights?

Yes ☐ **No** ☐ **Unsure** ☐

Comments:

_____

_____

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ **No** ☐ **Unsure** ☐

Comments:

_____

_____

What kind of Architectural style would you like to see on this site?

_____

_____

_____

Other recommendations or suggestion:

_____

_____

_____

_____

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Grant Kozij

Address: 4080 Cavendish Dr.

Phone (Optional):

Email (Optional): gkozij@shaw.ca

Date: July 3/19

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ **No** ☐ **Unsure** ☒

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☒ **No** ☐ **Unsure** ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ **No** ☒ **Unsure** ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Grant Kozij

Address: 4080 Cavendish Dr.

Phone (Optional):

Email (Optional): gkozij@shaw.ca

Date: July 3/19

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☐ **No** ☐ **Unsure** ☒

Comments:

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☒ **No** ☐ **Unsure** ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ **No** ☒ **Unsure** ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ **No** ☒ **Unsure** ☐

Comments:

Allows people to access neighbour hood, does not link to buses, could increase crime in area

Do you support the proposed building heights?

Yes ☐ **No** ☐ **Unsure** ☒

Comments:

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ **No** ☐ **Unsure** ☒

Comments:

What kind of Architectural style would you like to see on this site?

Other recommendations or suggestion:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: Paul Mah

Address: 4095 Cavendish Drive

Phone (Optional):

Email (Optional):

Date: July 2, 2019

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes [X] **No** [ ] **Unsure** [ ]

Comments:

We agree as long as there is a fence border with landscaping separating the townhouses from the Cavendish neighbourhood. We do not want gated access along this fence border between the townhomes and Cavendish Drive.

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes [X] **No** [ ] **Unsure** [ ]

Comments:

We wish to ensure that there is adequate street lighting along the new connector. However, we do not want the front of the townhomes to face the new Cavendish connector because this will increase car traffic and parking congestion from the townhome owners/visitors entering Cavendish Drive.

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes [X] **No** [ ] **Unsure** [ ]

Comments:

We support the plan for the two new single family homes. However, we do have a concern about the home on lot A due to the amount of shadow and lack of sunlight it will receive throughout the year. Can this be addressed in some way to improve this issue for the prospective home owner?

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ No ☒ Unsure ☐

Comments:

We prefer not to have a public walkway connecting No. 1 Rd to Cavendish. It will make our quiet street busier and give criminals easier entry/exit which is not good for our neighbourhood in particular because it is unique in that the design of our homes do not allow clear front view of the street. We have a history of thefts and property intrusions. We have strong concerns that a Cavendish walkway will only make crime worse for our neighbourhood. If the City deems the walkway a requirement, please reconsider its location (see comments below).

Do you support the proposed building heights?

Yes ☐ No ☒ Unsure ☐

Comments:

We would prefer the new townhouse development be consistent with existing 2 story townhouse height behind us on No. 1 Rd. We do not like the idea of 3 story townhomes over looking our neighbourhood for privacy reasons.

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☐ No ☐ Unsure ☒

Comments:

What kind of Architectural style would you like to see on this site?

We prefer the style to be consistent with Steveston / the Maritimes (hence our street names), as well as with our current neighbourhood house design and colour schemes. We do not wish to have big mansion style homes here.

Other recommendations or suggestion:

Do not plant trees with invasive roots that could damage property. Ensure adequate property line drainage between the new development (ie. single family homes / townhomes) and existing Cavendish homes to prevent their flooding. Keep plan for low end market rental units to be situated closer to No. 1 Rd and further away from Cavendish side. If a new pedestrian walkway becomes a City requirement, we prefer the new walkway be located north of the townhouse development and just south of the church connecting No. 1 Rd to Pugwash Place, thereby allowing the walkway to remain straight and without any angles.
Building a walkway here makes the most sense because it would be nearby a major community gathering space, that is the church, and would therefore meet the City's goal for communities to be connected and accessible.

Please note that a copy of this Feedback Form will be copied to the City

From: **Steven Yang** steven@bohan.ca
Subject: Fwd: Cavendish Project
Date: July 17, 2019 at 4:49 PM
To: Brian Sheehan bsheehan@yamamotoarchitecture.com

SY

One of the feedbacks received FYI. Another to come.

---------- Forwarded message ---------
From: **ben sun** <ben_sun_bc@yahoo.ca>
Date: Wed, Jul 10, 2019 at 3:23 PM
Subject: Cavendish Project
To: steven@bohan.ca <steven@bohan.ca>, Edwin Lee <elee@richmond.ca>
Cc: Rick Michaels <rickmichaels@shaw.ca>

Hi, as a owner and resident of 4040 Cavendish I thank you for the opportunity to view your proposal. I like your green space idea with emergency vehicle access on this stretch of the street.

I have some concerns about the two new houses adjacent to me. They are overlooking my backyard and intruding my privacy. They also blocking western sunlight which my vegetable garden need. If the house on lot B moved forward toward west such that the front of the house align with its neighbouring site, it will provide me with more privacy. It will provide a bigger backyard which the two units can share.

Regarding the house on lot A, if the house is shift to the north and west will provide me better privacy and sunlight for my backyard. By shifting west by 4 feet (a total of 2.4m between my fence and side of the house) will provide ease of maintenance. By shifting north the new house will align with the neighbouring sites. I wonder if the garage should facing north instead of west.

Of course the green space and the bollard placements need to be adjusted accordingly.

Thank you
Ben

--
Steven Yang
Managing Partner | BOHAN Properties
D: 6043417777
E: steven@bohan.ca

From: **Steven Yang** steven@bohan.ca
Subject: Fwd: Cavendish Project
Date: July 17, 2019 at 4:50 PM
To: Brian Sheehan bsheehan@yamamotoarchitecture.com

SY

Feedback from Rick Michaels.

---------- Forwarded message ---------
From: **Rick Michaels** <RickMichaels@shaw.ca>
Date: Thu, Jun 27, 2019 at 11:30 AM
Subject: Cavendish Project
To: Edwin Lee <ELee@richmond.ca>, Steven Yang <steven@bohan.ca>

Hi , thank you for the opportunity to view your proposal! I commend and congratulate you on making excellent improvements for the development and its neighbours.

I have attached a marked up plan for the two Cavendish Street houses that hopefully benefit those two houses and gain more alignment with neighbouring sites without one iota of change to the building designs and floor plans. Equalizing the side yards for the house west of me gives more separation to my neighbour but also gives the new house more side yard for mtce purposes. A side yard of 2m is all the difference in the world for ladder placements and the like. Pinching towards the greenway is no harm done and actually brings eyes closer to the street.

As for the house south of me; its neighbour is one storey at the front and rear. Decreasing the driveway by moving the house forward to align better with the existing house would create more useable rear yard for the development site This would also better align the two storey portions of both buildings. Yes, all cards on the table this also serves me as more rear yard on the development site means less overlook into my house from the second storey windows and deck. The second storey in the new houses are primary living spaces so far more active than our seconds storeys of bedrooms only. So a little more separation and less driveway would be helpful.

Shifting the house forward in creating the larger rear yard would pull the deck aback and perhaps a stair can be added to the deck. A larger rear yard would lend itself to creating private yard space for the secondary suite and the upper floor unit. The driveway and entry taking up so much space at the front when all other house are punched forward is questionable.

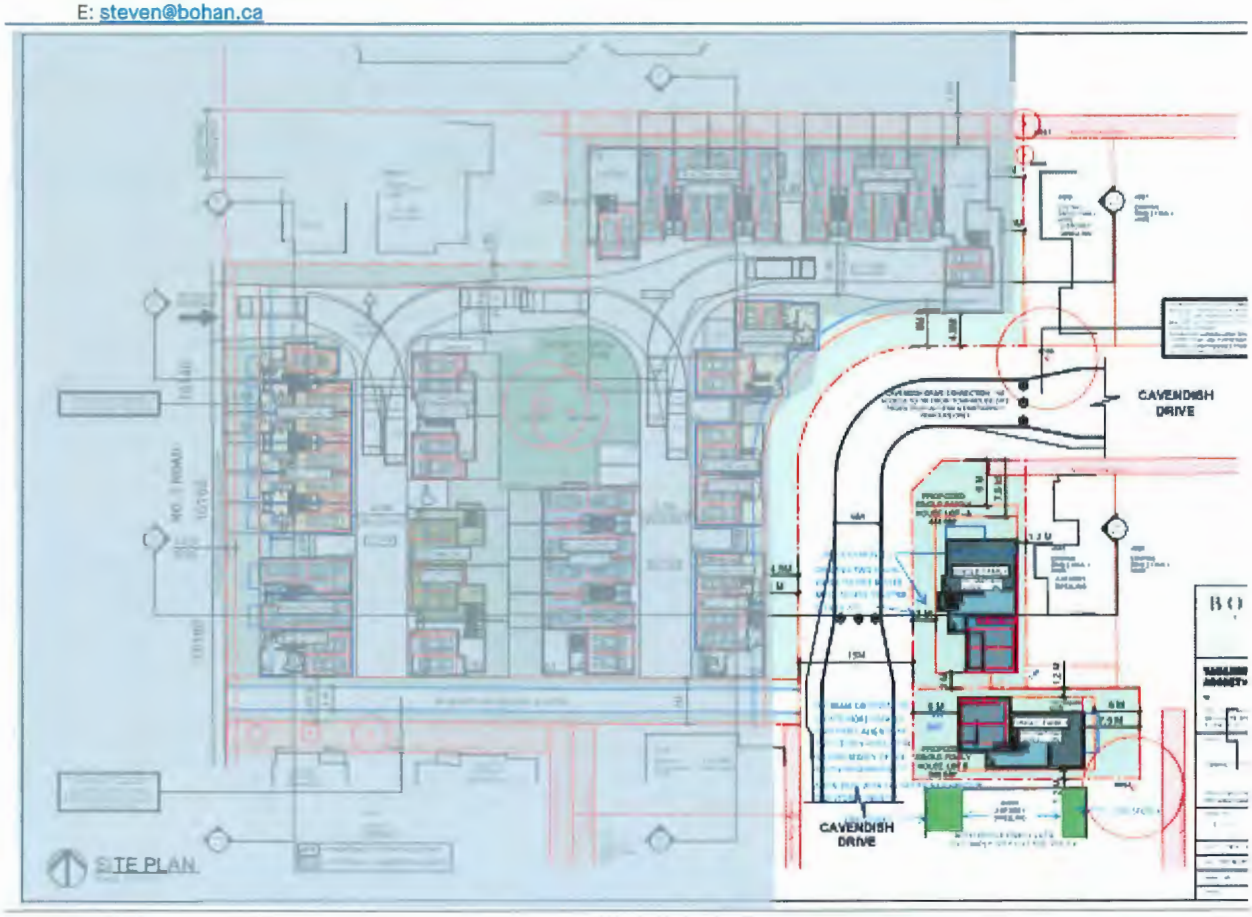
The only comment I would offer on the west house is would reversing the upper floor plan give the upper floor unit an improved greener distant outlook and more sun from the west? The impact to accommodate the stair change appears to be minor – both units might then gain benefit of a more distant outlook from key rooms? This might reduce the overlook into Ben's courtyard which is the substantive "outward"view window for his living room. Just a thought.

Hope this helps and looking forward to new neighbours !!!!

Thanks,
Rick

--
Steven Yang

Managing Partner | BOHAN Properties
D: 6043417777
E: steven@bohan.ca



RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

Site Address: 10140-10180 No.1 Road & 4051/4068 Cavendish Drive, Richmond, BC

Name: DANNY YU

Address: 4113 CAVENDISH DR.

Phone (Optional):

Email (Optional):

Date: 8/19/2019

The proposed development will involve amending the City of Richmond Official Community Plan for a portion of the parcel from Single Family to Multi-Family to allow townhouses along the eastern portion of the property. Do you support this amendment?

Yes ☑ **No** ☐ **Unsure** ☐

Comments:

Only because the architect is Yamamoto. If changed to another architect then "no"

The proposed development will involve connecting both dead ends of Cavendish Drive with a Pedestrian and Emergency Access connector (no through traffic permitted). Do you support this proposal?

Yes ☐ **No** ☑ **Unsure** ☐

Comments:

The proposed development will involve the addition of 2 new single-family lots. Do you support this proposal?

Yes ☐ **No** ☑ **Unsure** ☐

Comments:

Please note that a copy of this Feedback Form will be copied to the City

RZ18-820669 – PUBLIC INFORMATION MEETING FEEDBACK

The proposed development is planning on construction a new, public walkway that connects Cavendish Drive to No. 1 Road. Do you support this proposal?

Yes ☐ No ☑ Unsure ☐

Comments:

Do you support the proposed building heights?

Yes ☐ No ☑ Unsure ☐

Comments:

2-level building height is better

Do you support the proposed setbacks? (Distance between building and property line).

Yes ☑ No ☐ Unsure ☐

Comments:

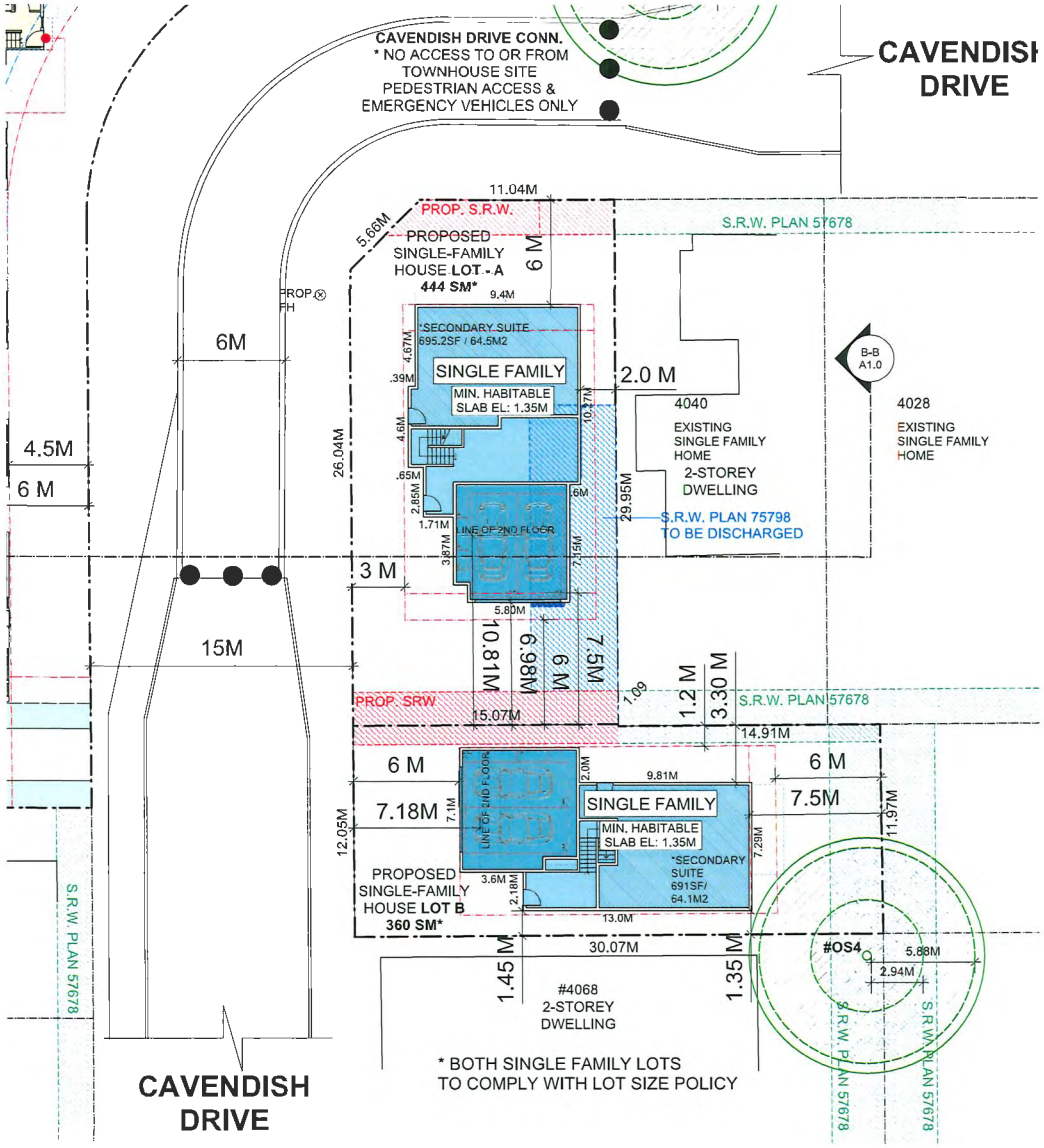
What kind of Architectural style would you like to see on this site?

Light color exterior wall with modern look.

Other recommendations or suggestion:

N/A.

Please note that a copy of this Feedback Form will be copied to the City



STATISTICS: SINGLE FAMILY LOT A

ZONING :	RS2/B		
SITE AREA	4,779 SQ.FT.	(444 SQ.M.)	
LOT COVERAGE PERMITTED:	4,779 SQ.FT.	X 45% =	2,151 SQ.FT.
LOT COVERAGE PROPOSED:	1,558 SQ.FT.		=33%
COMBINED AREA PERMITTED:	4,779 SQ.FT.	X 70% =	3,345 SQ.FT.
COMBINED AREA PROPOSED:	2,355 SQ.FT.		=49%
FLOOR AREA RATIO PERMITTED:	2,628 SQ.FT.	(0.55 FAR)	
FLOOR AREA RATIO PROPOSED:	2,355 SQ.FT.		
GROSS FLOOR AREA PROPOSED:	2,841 SQ.FT.		INCL GARAGE AREA

STATISTICS: SINGLE FAMILY LOT B

ZONING :	RS2/B		
SITE AREA	3,875 SQ.FT.	(360 SQ.M.)	
LOT COVERAGE PERMITTED:	3,875 SQ.FT.	X 45% =	1,744 SQ.FT.
LOT COVERAGE PROPOSED:	1,364 SQ.FT.		=35%
COMBINED AREA PERMITTED:	3,875 SQ.FT.	X 70% =	2,713 SQ.FT.
COMBINED AREA PROPOSED:	2,131 SQ.FT.		=55%
FLOOR AREA RATIO PERMITTED:	2,131 SQ.FT.	(0.55 FAR)	
FLOOR AREA RATIO PROPOSED:	2,131 SQ.FT.		
GROSS FLOOR AREA PROPOSED:	2,631 SQ.FT.		INCL GARAGE AREA

BOHAN PROPERTIES

YAMAMOTO ARCHITECTURE

700 - 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

TOWNHOUSE DEVELOPMENT

SITE PLAN

SCALE: 3/32" = 1'-0"

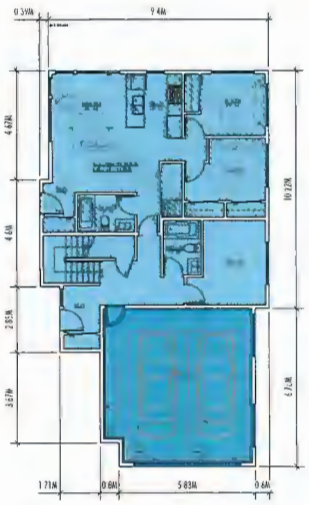
DATE – MAY 06 2010

DRAWN – BS

SITE PLAN - SINGLE FAMILY LOTS

SCALE: 3/32"=1'-0"

ATTACHMENT 12



MAIN FLOOR: 1072 SQ.FT.
GARAGE: 486 SQ.FT

SECOND FLOOR: 1283 SQ.FT.

CAVENDISH DRIVE

REAR YARD

CAVENDISH DRIVE SINGLE FAMILY LOT A

SIDE YARD

CAVENDISH DRIVE

PH - 153

BOHAN
PROPERTIES

YAMAMOTO ARCHITECTURE

202 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

PROJECT –

TOWNHOUSE DEVELOPMENT

10149 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE, RICHMOND BC

DRAWING TITLE –

SINGLE FAMILY FLOORPLANS
& EXTERIOR ELEVATIONS (LOT A)

SCALE – 1/8" = 1'-0"

DATE – MAY 06 2019

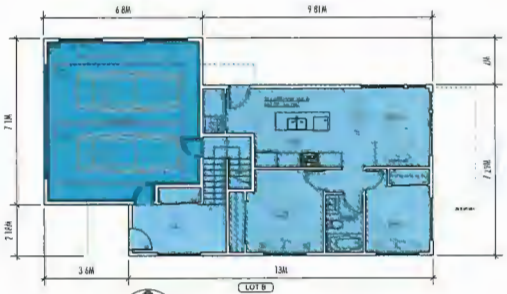
DRAWN – BS

CHECKED –

DRAWING NO. – A2.2

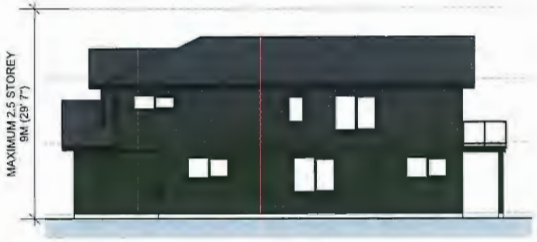
PROJ NO – 1711

SECOND FLOOR: 1267 SQ.FT.



MAIN FLOOR: 864 SQ.FT.
GARAGE: 500 SQ.FT

REAR YARD



SIDE YARD

CAVENDISH DRIVE

SIDE YARD

CAVENDISH DRIVE SINGLE FAMILY LOT B

PH - 154

BOHAN PROPERTIES

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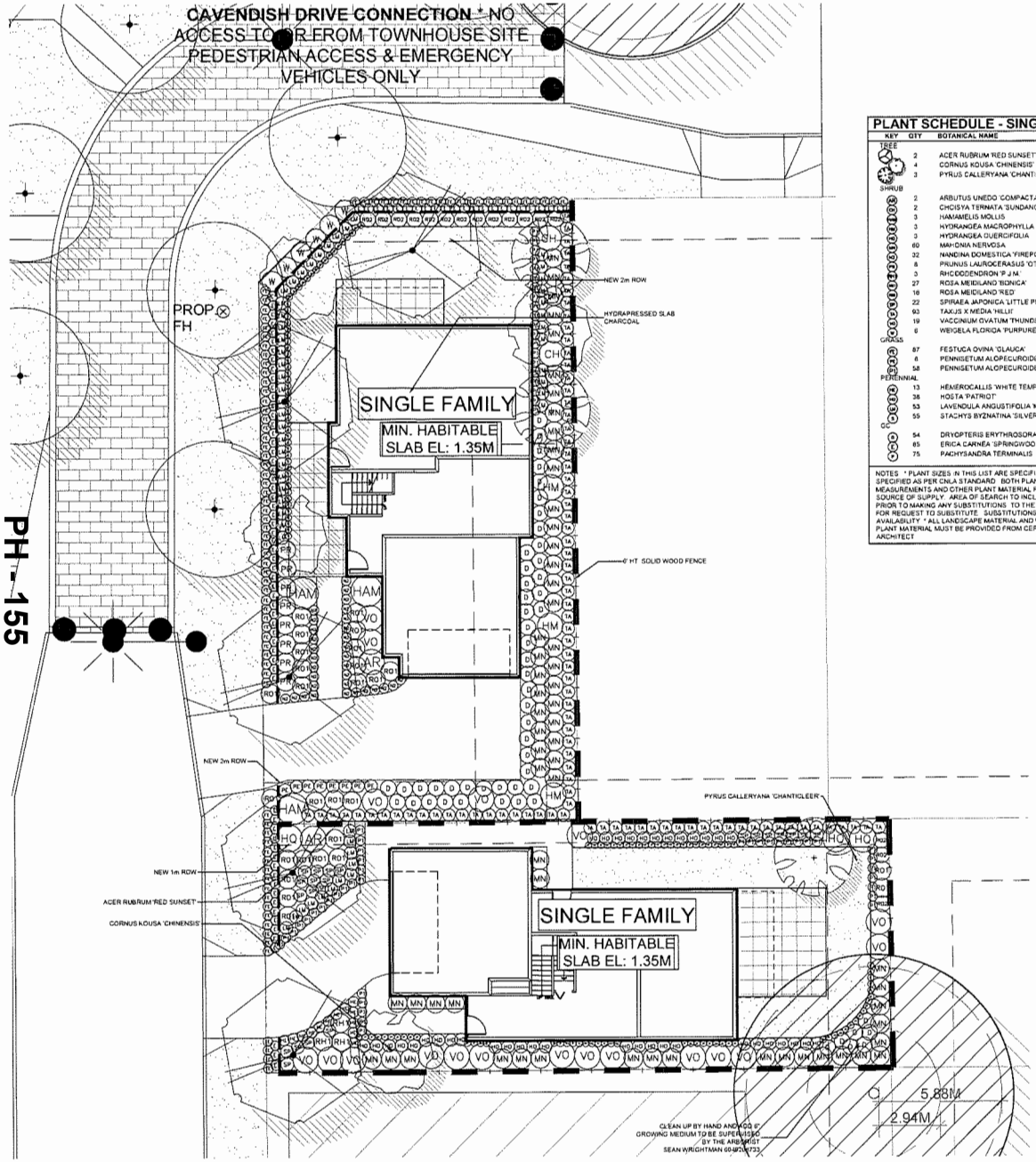
YAMAMOTO ARCHITECTURE
202 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

PROJECT –
TOWNHOUSE DEVELOPMENT
10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND BC

DRAWING TITLE –
SINGLE FAMILY FLOORPLANS
& EXTERIOR ELEVATIONS (LOT B)

SCALE – 1/8" = 1'-0"
DATE – MAY 06 2019
DRAWN – BS
CHECKED –
SHEET NO. – A2.3
PROJ NO – 1711



PH - 155

PLANT SCHEDULE - SINGLE FAMILY

PMG PROJECT NUMBER: 19-013

KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
TREE				
AR	2	ACER RUBRUM 'RED SUNSET'	RED SUNSET MAPLE	10CM CAL; 2M STD; B&B
CK	4	CORNUS KOUSA 'CHINENSIS'	CHINESE KOUSA DOGWOOD	3.5M HT; BUSH FORM; B&B
PC	3	PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	8CM CAL; 1.5M STD; B&B
SHRUB				
AU	2	ARBUTUS UNEDO 'COMPACTA'	COMPACT STRAWBERRY BUSH	#3 POT; 60CM
CT	2	CHOISYA TERNATA 'SUNDANCE'	MEXICAN MOCK ORANGE	#3 POT; 50CM
HM	3	HAMAMELIS MOLLIS	CHINESE WITCH HAZEL	#3 POT; 60CM
HN	3	HYDRANGEA MACROPHYLLA 'NIKKO BLUE'	BIGLEAF HYDRANGEA (BLUE)	#3 POT; 60CM
HQ	3	HYDRANGEA QUERCIFOLIA	OAKLEAF HYDRANGEA	#3 POT; 60CM
MN	60	MAHONIA NERVOSA	LONGLEAF MAHONIA	#2 POT; 40CM
ND	32	NANDINA DOMESTICA 'FIREPOWER'	FIREPOWER HEAVENLY BAMBOO	#2 POT; 40CM
PL	8	PRUNUS LAUROCERASUS 'OTTO LUYKEN'	OTTO LUYKEN LAUREL	#3 POT; 50CM
RH	3	RHODODENDRON 'P.J.M.'	RHODODENDRON; LIGHT PURPLE; E. MAY	#3 POT; 50CM
RO	27	ROSA MEIDILAND 'BONICA'	BONICA ROSE	#2 POT; 40CM
RR	16	ROSA MEIDILAND 'RED'	MEIDILAND ROSE, RED	#2 POT; 40CM
SJ	22	SPIRAEA JAPONICA 'LITTLE PRINCESS'	LITTLE PRINCESS SPIRAEA, PINK	#2 POT; 40CM
TH	93	TAXUS X MEDIA 'HILLII'	HILLII YEW	1.2M B&B
VO	19	VACCINIUM OVATUM 'THUNDERBIRD'	EVERGREEN HUCKLEBERRY	#3 POT; 60CM
WF	6	WEIGELA FLORIDA 'PURPUREA'	PURPLE WEIGELA	#3 POT; 60CM
GRASS				
FO	87	FESTUCA OVINA 'GLAUCA'	BLUE FESCUE	#1 POT
PH	6	PENNISETUM ALOPECUROIDES 'HAMELIN'	DWARF FOUNTAIN GRASS	#1 POT
PB	58	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#1 POT
PERENNIAL				
HC	13	HEMEROCALLIS 'WHITE TEMPATION'	DAYLILY, WHITE	#1 POT; 20CM
HA	38	HOSTA 'PATRIOT'	HOSTA, GREEN AND WHITE VARIEGATED	#1 POT; 1 EYE
LA	53	LAVENDULA ANGUSTIFOLIA 'MUNSTEAD'	ENGLISH LAVENDER, COMPACT, VIOLET-BLUE	#1 POT
SB	55	STACHYS BYZNATINA 'SILVER CARPET'	LAMB'S EAR	15CM POT
GC				
DA	54	DRYOPTERIS ERYTHROSORA 'BRILLIANCE'	BRILLIANCE AUTUMN FERN	#2 POT; 45CM
EC	65	ERICA CARNEA 'SPRINGWOOD PINK'	WINTER HEATH, PINK	#1 POT
PT	75	PACHYSANDRA TERMINALIS	JAPANESE SPURGE	#1 POT; 15CM

NOTES * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION CONTAINER SIZES SPECIFIED AS PER CNLA STANDARD BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS * SEARCH AND REVIEW MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY * SUBSTITUTIONS OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL UNAPPROVED SUBSTITUTIONS WILL BE REJECTED ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD - DEFINITION OF CONDITIONS OF AVAILABILITY * ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION * ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY * BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT

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pmg LANDSCAPE ARCHITECTS
Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6G9
p 604 294-0011 f 604 294-0022

SEAL

NO	DATE	REVISION DESCRIPTION	DR
10	20 FEB 19	NEW SITE PLAN/CITY COMMENTS	RJ
9	20 JAN 22	FIRE HYDRANT REQUIRED	RJ
8	19 NOV 26	NEW SITE PLAN/CITY COMMENTS	RJ
7	19 NOV 07	NEW SITE PLAN/CITY COMMENTS	RJ
6	19 SEP 06	NEW SITE PLAN/CITY COMMENTS	RJ
5	19 JUN 24	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	19 MAR 24	TREE MANAGEMENT PLAN UPDATED	RJ
2	19 MAR 14	COORDINATION MEETING	RJ
1	19 APR 26	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

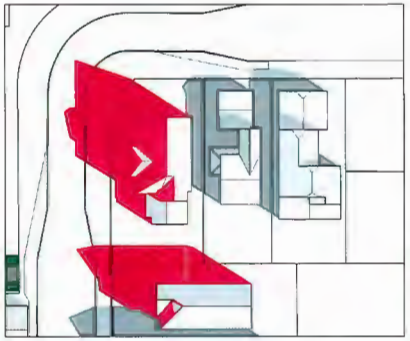
DRAWING TITLE

SINGLE FAMILY ENLARGEMENT

DATE	19.JAN.31	DRAWING NUMBER
SCALE	1/8"=1'-0"	L3
DRAWN	RJ	
DESIGN	RJ	OF 9
CHK'D	PCM	

19013-10

PMG PROJECT NUMBER 19-013



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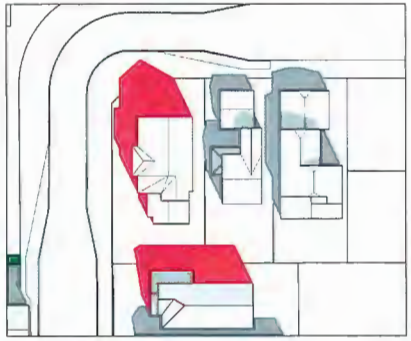

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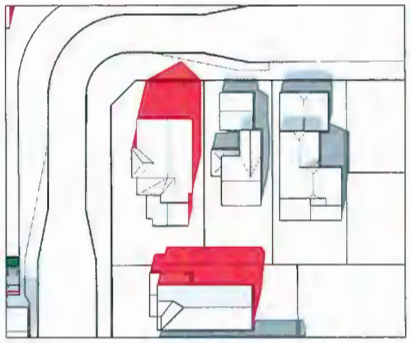

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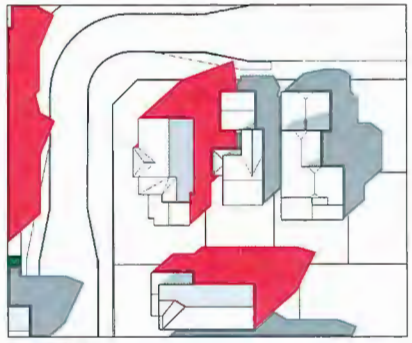

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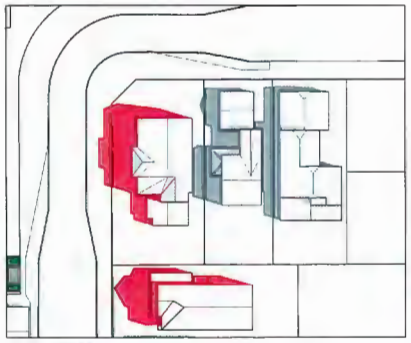

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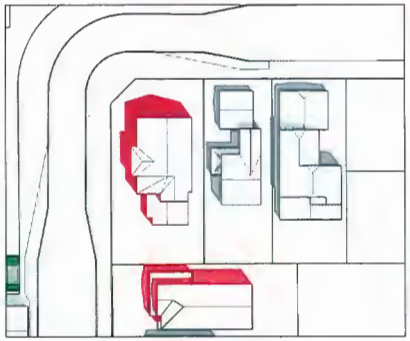

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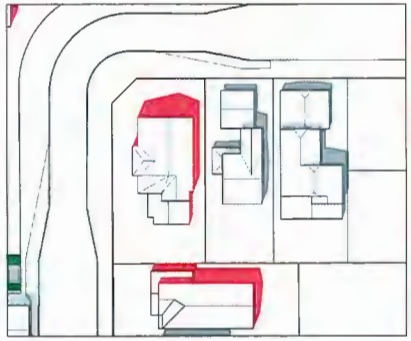

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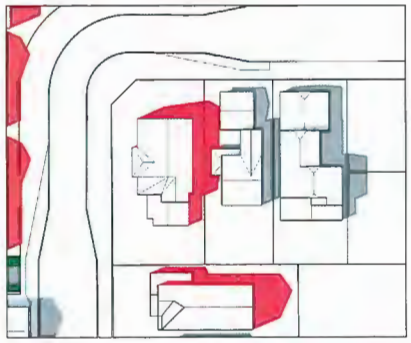

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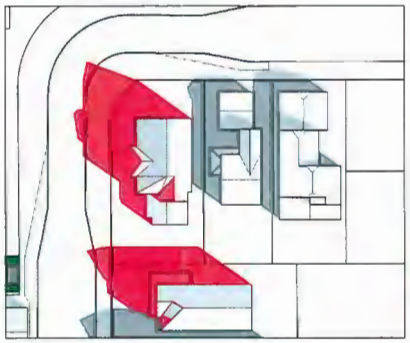

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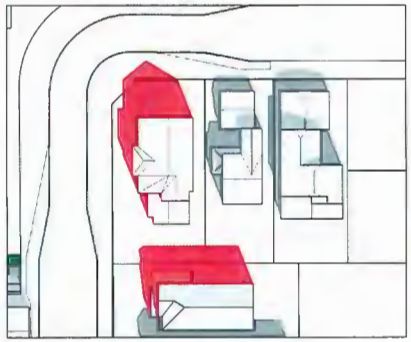

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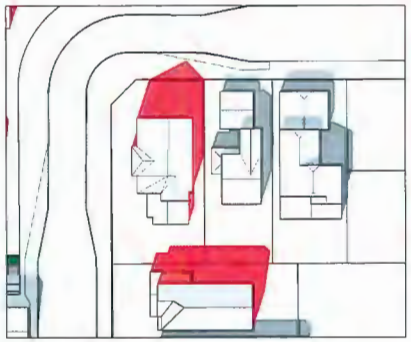

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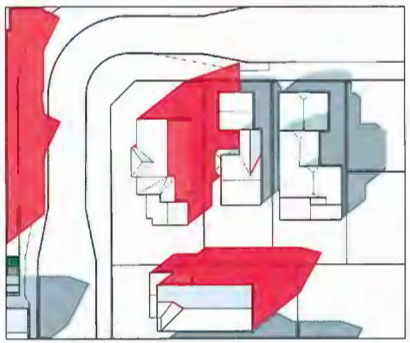

21 SEPTEMBER 4PM

BOHAN
PROPERTIES

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YAMAMOTO
ARCHITECTURE

202 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

PROJECT –

TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND

DRAWING TITLE –

PROJECT SHADOW STUDY
SINGLE FAMILY LOT A & B

SCALE –

DATE – MAY 06 2019

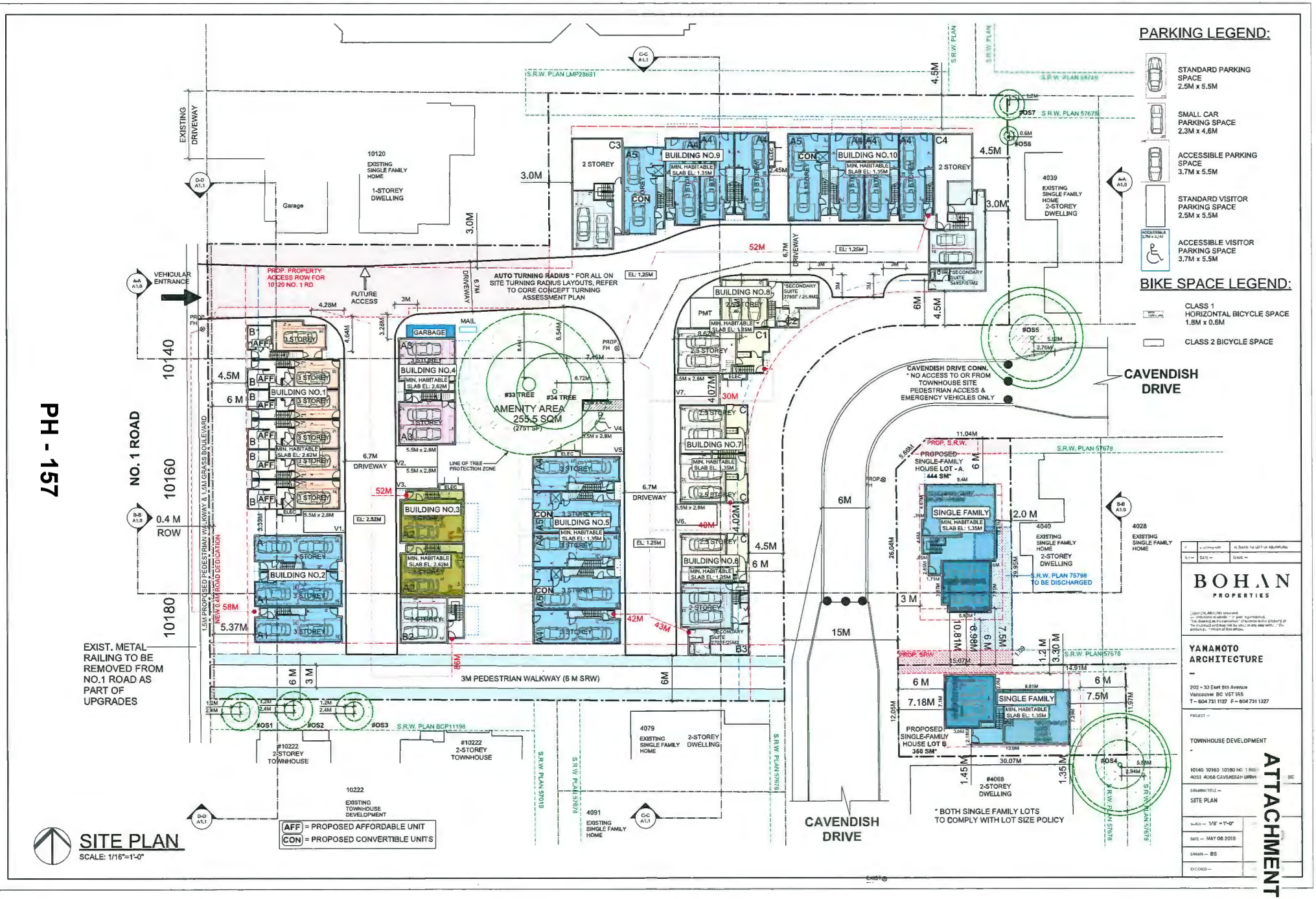
DRAWN – BS

CHECKED –

JOB NO –

A3

ATTACHMENT 13



ATTACHMENT 14

PLANT SCHEDULE - TOWNHOUSE, DUPLEX AND SINGLE -FAMILY

PMG PROJECT NUMBER: 19-013

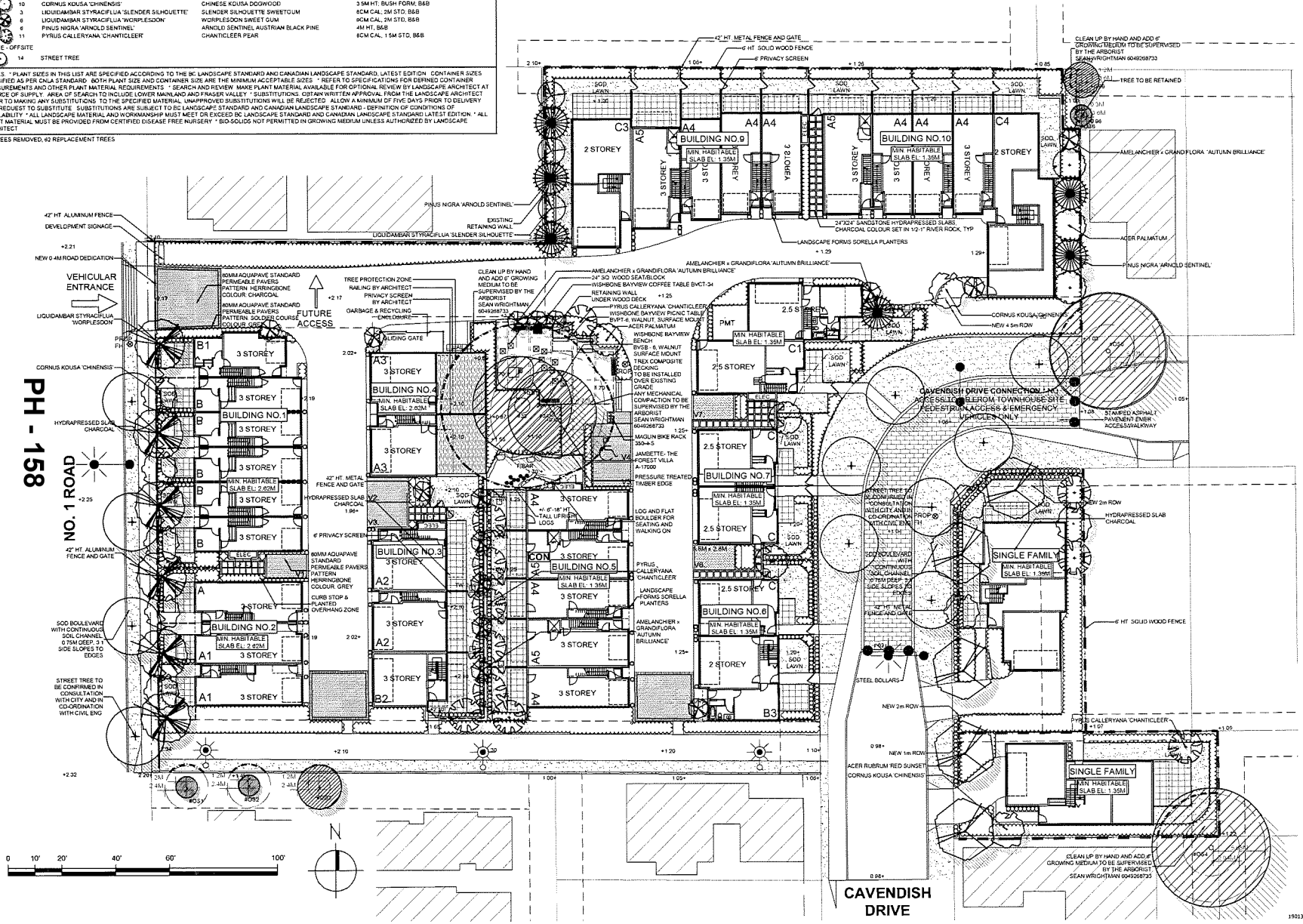
KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
TREE - ON SITE				
	6	ACER PALMATUM	JAPANESE MAPLE	6CM CAL, 1M STD, B&B
	5	ACER RUBRUM 'RED SUNSET'	RED SUNSET MAPLE	16CM CAL, 2M STD, B&B
	15	AMELANCHIER x GRANDIFLORA 'AUTUMN BRILLIANCE'	AUTUMN BRILLIANCE SERVICEBERRY	6CM CAL, B&B, 3 STEM CLUMP
	10	CORNUS KOUSA 'CHINENSIS'	CHINESE KOUSA DOGWOOD	3.5M HT, BUSH FORM, B&B
	3	LIQUIDAMBAR STYRACIFLUA 'SLENDER SILHOUETTE'	SLENDER SILHOUETTE SWEETGUM	8CM CAL, 2M STD, B&B
	8	LIQUIDAMBAR STYRACIFLUA 'WORPLESDON'	WORPLESDON SWEET GUM	8CM CAL, 2M STD, B&B
	6	PINUS NIGRA 'ARNOLD SENTINEL'	ARNOLD SENTINEL AUSTRIAN BLACK PINE	4M HT, B&B
	11	PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	8CM CAL, 1.5M STD, B&B
TREE - OFFSITE				
	14	STREET TREE		

NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION CONTAINER SIZES SPECIFIED AS PER CNLA STANDARD BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS * SEARCH AND REVIEW MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY * AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY * SUBSTITUTIONS OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL UNAPPROVED SUBSTITUTIONS WILL BE REJECTED ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD - DEFINITION OF CONDITIONS OF AVAILABILITY * ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. * ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY * BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT

63 TREES REMOVED, 62 REPLACEMENT TREES

TREE TO BE RETAINED

KIDS PLAY AREA: 106.3



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Suite C100 – 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6G9
p. 604 294-0011 . f. 604 294-0022

SEAL

ND	DATE	REVISION DESCRIPTION	DR
10	20 FEB 19	NEW SITE PLAN/DTP COMMENTS	RJ
9	20 MAY 27	FAC HOUSE REUPDATED	RJ
8	19 NOV 26	NEW SITE PLAN/DTP COMMENTS	RJ
7	19 NOV 07	NEW SITE PLAN/DTP COMMENTS	RJ
6	19 OCT 06	NEW SITE PLAN/DTP COMMENTS	RJ
5	19 JUN 24	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	19 MAY 24	TREE MANAGEMENT PLAN UPDATED	RJ
2	19 MAY 14	ISSUED FOR REVIEW	RJ
1	19 APR 26	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

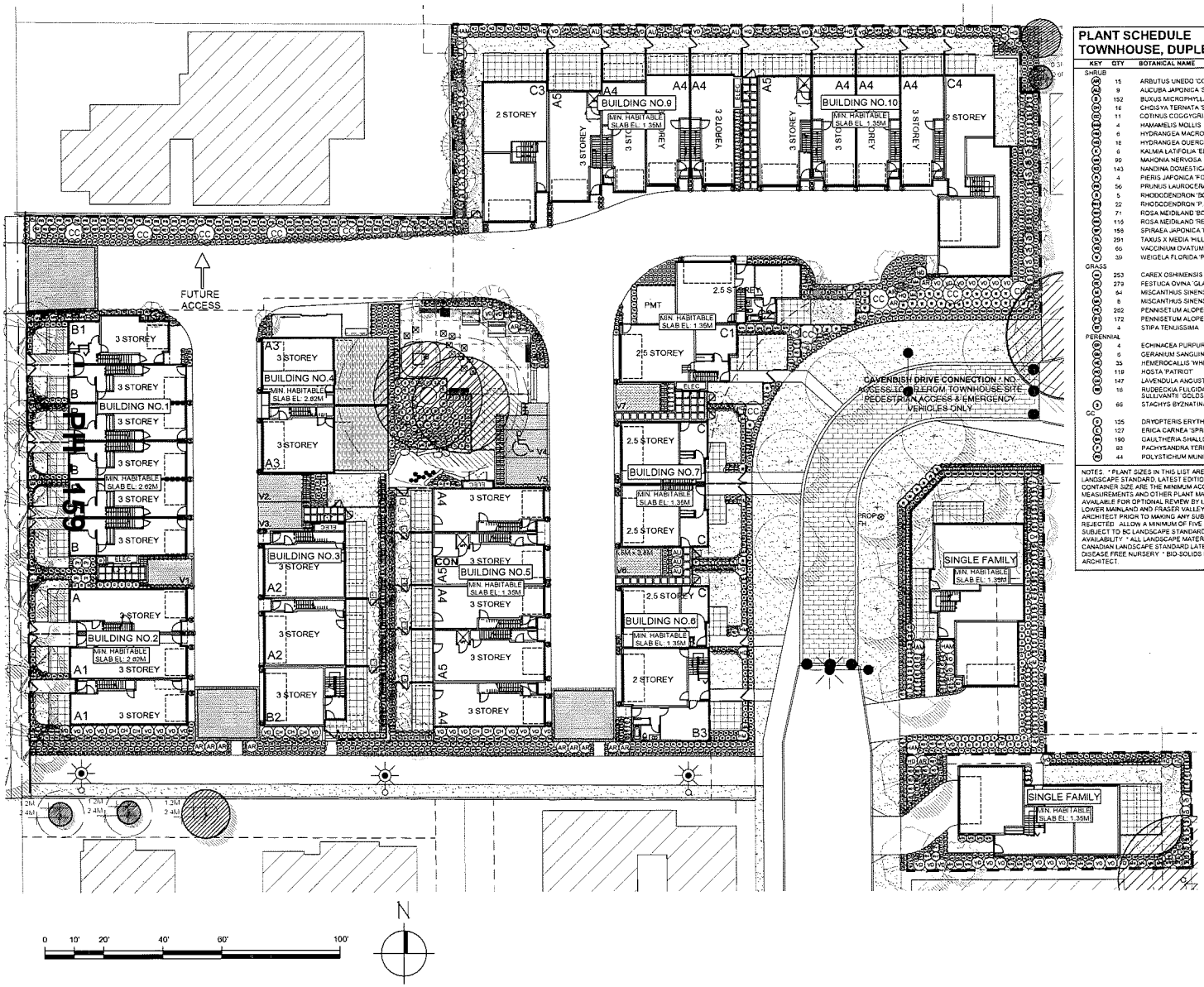
10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

LANDSCAPE PLAN

DATE	19.JAN.31	DRAWING NUMBER	
SCALE	1/16"=1'-0"		L1
DRAWN	RJ		
DESIGN	RJ		
CHK'D	PCM		OF 9

PMG PROJECT NUMBER 19-013



PLANT SCHEDULE
TOWNHOUSE, DUPLEX AND SINGLE -FAMILY

PMG PROJECT NUMBER: 19-013

KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
SHRUB				
AU	15	ARBUTUS UNEDO 'COMPACTA'	COMPACT STRAWBERRY BUSH	#3 POT, 60CM
AJ	9	AUCUBA JAPONICA 'SERRATIFOLIA'	SAWTOOTHED JAPANESE AUCUBA	#3 POT, 50CM
BM	152	BUXUS MICROPHYLLA 'WINTER GEM'	LITTLE-LEAF BOX	#3 POT, 40CM
CT	16	CHOISYA TERNATA 'SUNDANCE'	MEXICAN MOCK ORANGE	#3 POT, 50CM
CG	11	COTINUS COGGYGRIA 'GOLDEN SPIRIT'	GOLDEN SMOKE BUSH	#3 POT, 60CM
HM	4	HAMAMELIS MOLLIS	CHINESE WITCH HAZEL	#3 POT, 80CM
HB	6	HYDRANGEA MACROPHYLLA 'NIKKO BLUE'	BIGLEAF HYDRANGEA (BLUE)	#3 POT, 60CM
HQ	18	HYDRANGEA QUERCIFOLIA	OAKLEAF HYDRANGEA	#3 POT, 60CM
KL	6	KALMIA LATIFOLIA 'ELF'	DWARF MOUNTAIN LAUREL	#3 POT, 50CM
MN	90	MAHONIA NERVOSA	LONGLEAF MAHONIA	#2 POT, 40CM
ND	143	NANDINA DOMESTICA 'FIREPOWER'	FIREPOWER HEAVENLY BAMBOO	#2 POT, 40CM
PJ	4	PIERIS JAPONICA 'FOREST FLAME'	PIERIS, WHITE BLOOMS	#3 POT, 50CM
PL	56	PRUNUS LAUROCERASUS 'OTTO LUYKEN'	OTTO LUYKEN LAUREL	#3 POT, 50CM
RB	5	RHODODENDRON 'BOW BELLS'	RHODODENDRON	#2 POT, 50CM
RP	22	RHODODENDRON 'P.J.M.'	RHODODENDRON, LIGHT PURPLE, E. MAY	#3 POT, 50CM
RG	71	ROSA MEIDILAND 'BONICA'	BONICA ROSE	#2 POT, 40CM
RM	116	ROSA MEIDILAND 'RED'	MEIDILAND ROSE, RED	#2 POT, 40CM
SJ	159	SPIRAEA JAPONICA 'LITTLE PRINCESS'	LITTLE PRINCESS SPIRAEA, PINK	#2 POT, 40CM
TM	291	TAXUS X MEDIA 'HILLII'	HILLII YEW	1.2M B&B
VO	66	VACCINIUM OVATUM 'THUNDERBIRD'	EVERGREEN HUCKLEBERRY	#3 POT, 60CM
WF	39	WEIGELA FLORIDA 'PURPUREA'	PURPLE WEIGELA	#3 POT, 60CM
GRASS				
CO	253	CAREX OSHIMENSIS 'EVERGOLD'	EVERGOLD JAPANESE SEDGE	#1 POT
FO	279	FESTUCA OVINA 'GLAUCA'	BLUE FESCUE	#1 POT
MS	64	MISCANTHUS SINENSIS 'ADAGIO'	ADAGIO DWARF JAP. SILVER GRASS	#2 POT
MS	8	MISCANTHUS SINENSIS 'GRACILLIMUS'	MAIDEN GRASS	#2 POT
PA	262	PENNISETUM ALOPECUROIDES 'HAMELIN'	DWARF FOUNTAIN GRASS	#1 POT
PA	172	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#1 POT
ST	4	STIPA TENUISSIMA	MEXICAN FEATHER GRASS	#1 POT
PERENNIAL				
EP	4	ECHINACEA PURPUREA	PURPLE CONEFLOWER	15CM POT
GS	6	GERANIUM SANGUINEUM 'JOHN ELSLEY'	GERANIUM, MAGENTA	15CM POT
HC	35	HEMEROCALLIS 'WHITE TEMPATION'	DAYLILY, WHITE	#1 POT, 20CM
HP	119	HOSTA 'PATRIOT'	HOSTA, GREEN AND WHITE VARIEGATED	#1 POT, 1 EYE
LA	147	LAVENDULA ANGUSTIFOLIA 'MUNSTEAD'	ENGLISH LAVENDER, COMPACT, VIOLET-BLUE	#1 POT
RF	18	RUDBECKIA FULGIDA VAR SULLIVANTII 'GOLDSTURM'	RUDBECKIA, YELLOW	15CM POT
SB	66	STACHYS BYZNATINA 'SILVER CARPET'	LAMB'S EAR	15CM POT
CC				
BA	135	DRYOPTERIS ERYTHROSORA 'BRILLIANCE'	BRILLIANCE AUTUMN FERN	#2 POT, 45CM
EC	127	ERICA CARNEA 'SPRINGWOOD PINK'	WINTER HEATH, PINK	#1 POT
GS	190	GAULTHERIA SHALLON	SALAL	#1 POT, 20CM
PM	93	PACHYSANDRA TERMINALIS	JAPANESE SPURGE	#1 POT, 15CM
PM	44	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	#1 POT, 25CM

NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER CNLA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW, MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. * SUBSTITUTIONS: OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD - DEFINITION OF CONDITIONS OF AVAILABILITY. * ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. * ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. * BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.

19013-10

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Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6C9
p 604 294-0011 . f 604 294-0022

SEAL

NO	DATE	REVISION DESCRIPTION	DR
10	20 OCT 19	NEW SITE PLAN/CITY COMMENTS	RJ
9	20 JAN 22	FIRE HYDRANTS LOCATED	RJ
8	19 NOV 21	NEW SITE PLAN/CITY COMMENTS	RJ
7	19 NOV 07	NEW SITE PLAN/CITY COMMENTS	RJ
6	19 SEP 06	NEW SITE PLAN/CITY COMMENTS	RJ
5	19 JUN 24	TMT MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	19 MAY 24	TMT MANAGEMENT PLAN UPDATED	RJ
2	19 MAY 14	ISSUED FOR REZONING	RJ
1	19 APR 26	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

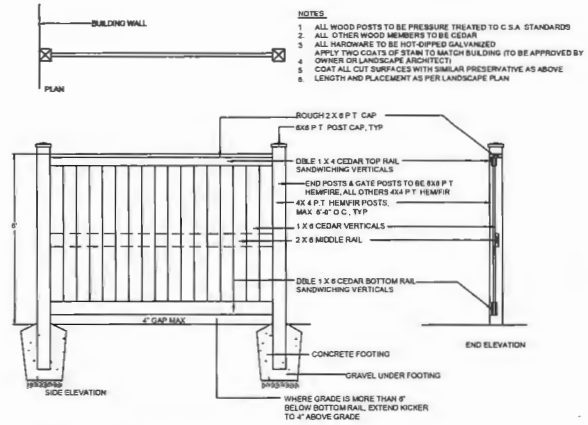
10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

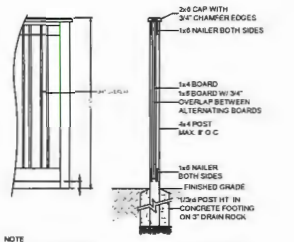
SHRUB PLAN

DATE:	19 JAN 31	DRAWING NUMBER:
SCALE:	1/16"=1'-0"	**L2**
DRAWN:	RJ	
DESIGN:	RJ	
CHK'D:	POM	OF 9

PMG PROJECT NUMBER **19-013**

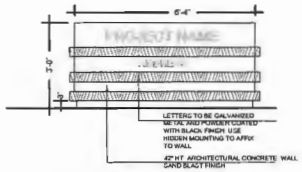


1 6' HT. WOOD PRIVACY FENCE
1/2"=1'-0"



NOTE
1. ALL POSTS PRESSURE TREATED TO CSA STANDARD AND END CUTS TREATED WITH PRESERVITIVE
2. ALL OTHER MEMBERS TO BE CEDAR, #2 (CONSTRUCTION) GRADE MINIMUM
3. ALL HARDWARE HOT DIPPED GALVANIZED
4. APPLY 2 COATS EXTERIOR STAIN TO MANUFACTURERS SPECIFICATION FINISH SELECTION AS APPROVED BY PROJECT ARCHITECT
5. ALL FENCES TO BE LEVEL, CHANGES IN GRADE TO BE IN 12"-18" STEPS (MAX.) GAPS TO GRADE TO FOLLOW FINISH GRADE. GAP TO BE 3-6"

2 6' HEIGHT SOLID WOOD FENCE
1/2"=1'-0"



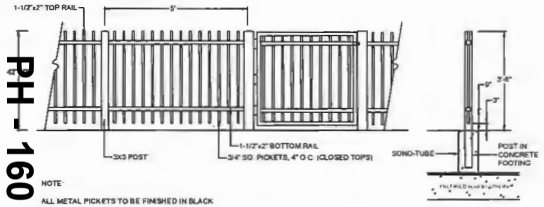
3 DEVELOPMENT SIGNAGE
1/2"=1'-0"

JAMBETTE THE FOREST VILLA

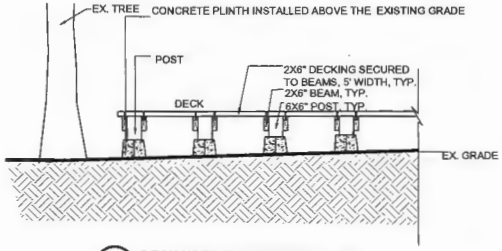
MAGLIN BIKE RACK 550-4-S

WISHBONE BAYVIEW BENCH

WISHBONE BAYVIEW TABLE



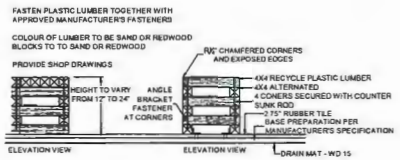
4 42" HT. METAL FENCE AND GATE
1/2"=1'-0"



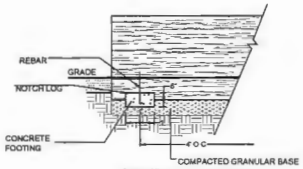
5 DECK UDER THE EXISTING TREE
1/2"=1'-0"

LANDSCAPE FORMS SORELLA PLANTERS

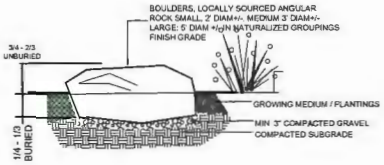
WISHBONE BAYVIEW PICNIC TABLE BVPT-6



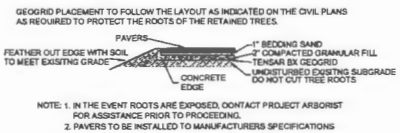
6 WOOD SEAT/BLOCK
1/2"=1'-0"



BALANCE LOG - FOOTING DETAIL



7 BALANCE LOGS
1/2"=1'-0"



8 PAVERS THROUGH ROOT ZONE
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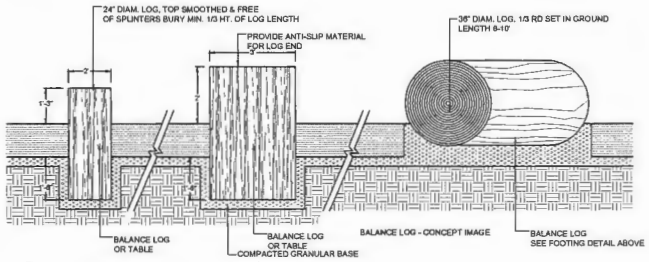
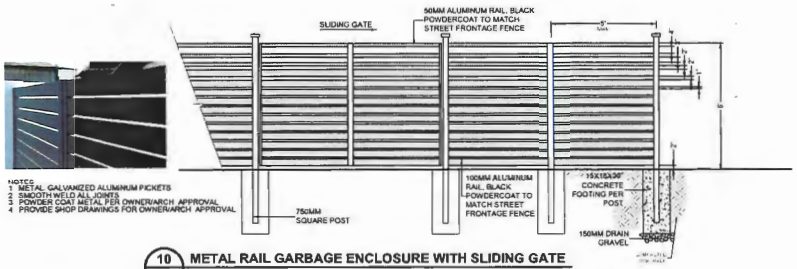


9 BALANCE LOGS
1/2"=1'-0"



10 METAL RAIL GARBAGE ENCLOSURE WITH SLIDING GATE
3/8"=1'-0"

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Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6G9
p 604 294-0011 , f 604 294-0022

SEAL

NO	DATE	REVISION DESCRIPTION	DR
10	20 FEB 19	NEW SITE PLAN/DTP COMMENTS	RJ
9	20 JAN 22	TREE MGMNT RE-ISSUED	RJ
8	19 NOV 21	NEW SITE PLAN/DTP COMMENTS	RJ
7	18 NOV 07	NEW SITE PLAN/DTP COMMENTS	RJ
6	15 SEP 06	NEW SITE PLAN/DTP COMMENTS	RJ
5	18 JUN 24	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	18 MAY 24	TREE MANAGEMENT PLAN UPDATED	RJ
2	18 MAY 14	ISSUED FOR REZONING	RJ
1	29 APR 14	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

LANDSCAPE DETAILS

DATE	19.JAN.21	DRAWING NUMBER
SCALE	AS SHOWN	L4
DRAWN	RJ	
DESIGN	RJ	
CHK'D	PCM	OF 9

19013-10 PMG PROJECT NUMBER: 19-013

CAVENDISH DRIVE STREETSCAPE

NO. 1 ROAD STREETSCAPE

STREETSCAPES

SCALE: 1:150

BOHAN
PROPERTIES

YAMAMOTO
ARCHITECTURE

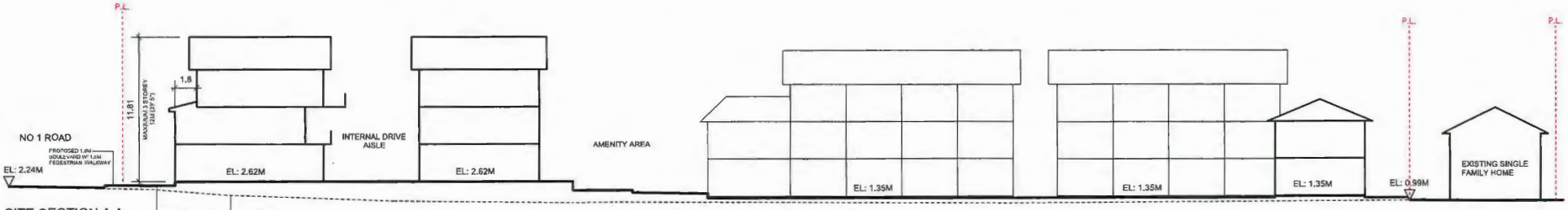
700 - 33 East 8th Avenue
Vancouver BC V5T 1R5
T - 604 731 1122 F - 604 731 1327

TOWNHOUSE DEVELOPMENT

10140 10160, 10180 NO. 1 ROAD AND
4051 4061 CAVENDISH DRIVE RICHMOND BC

NO 1 ROAD & CAVENDISH DRIVE
STREETSCAPES

1/8 = 1'-0"
MAY 06 2019
BS
A2.4
1711



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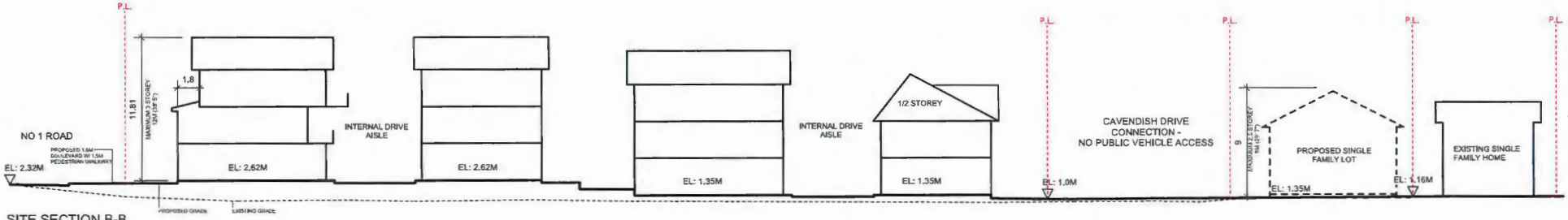

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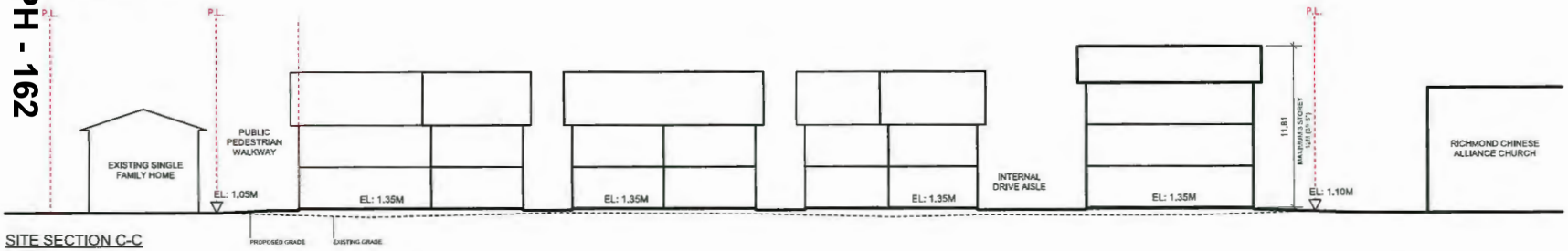
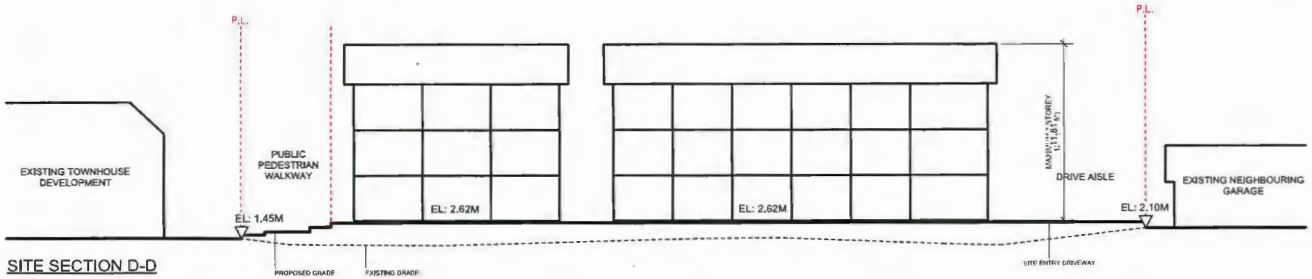


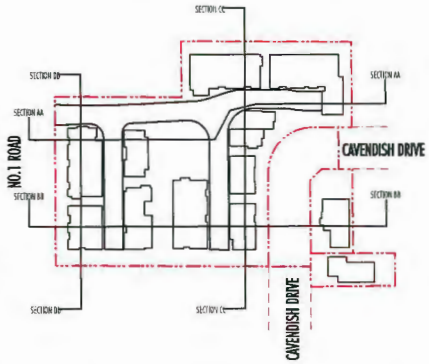
SITE SECTION C-C



SITE SECTION D-D

SITE SECTIONS

SCALE: 1:150



KEY PLAN

BOHAN PROPERTIES

YAMAMOTO ARCHITECTURE

202 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

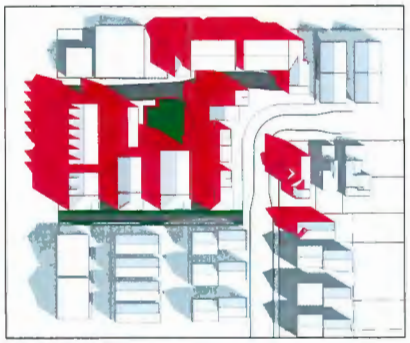
PROJECT –

TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND BC

DRAWING TITLE –
SITE SECTIONS

SCALE – 1:150
DATE – MAY 08 2019
DRAWN – BS
CHECKED –
DRAWING No – A2.5
PROJECT No – 1711



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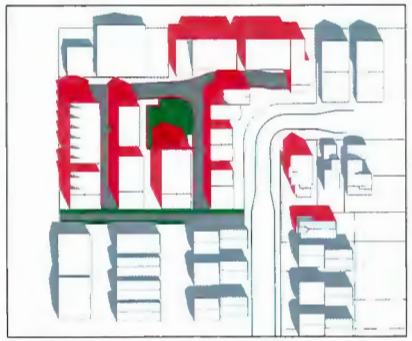

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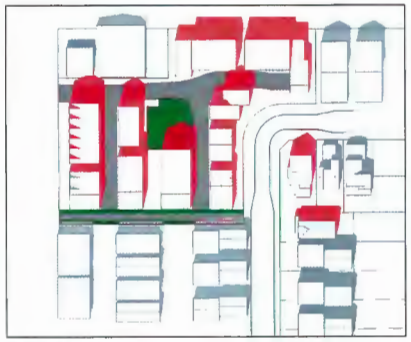

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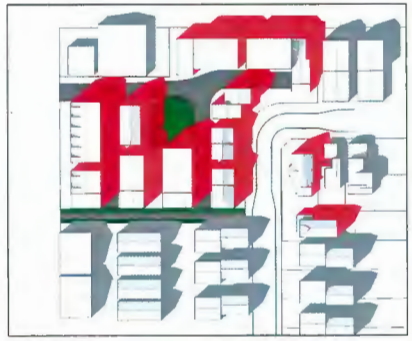

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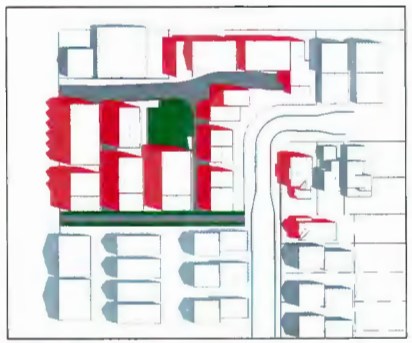
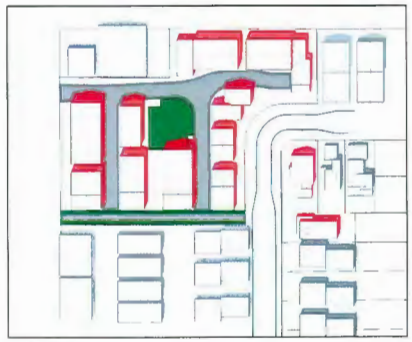


21 JUNE 10AM

21 JUNE 12PM



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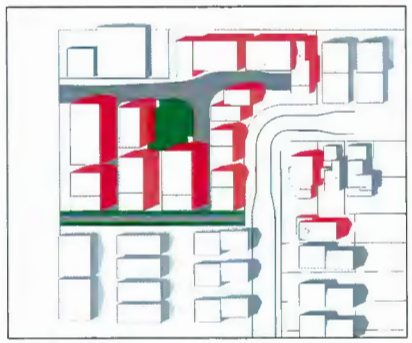

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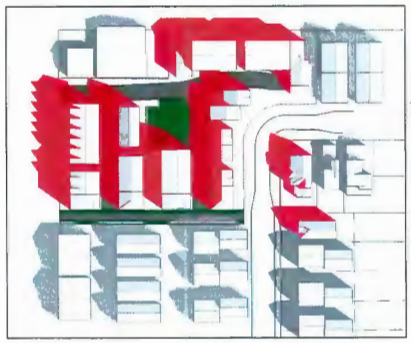

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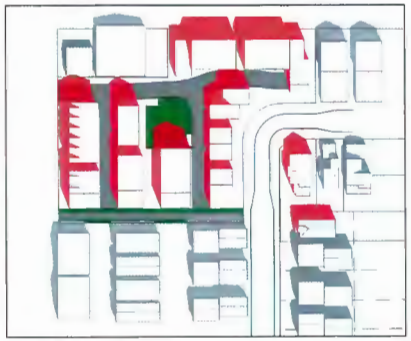

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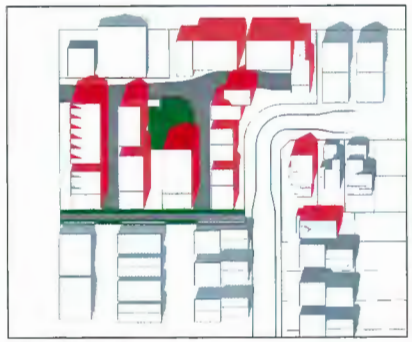

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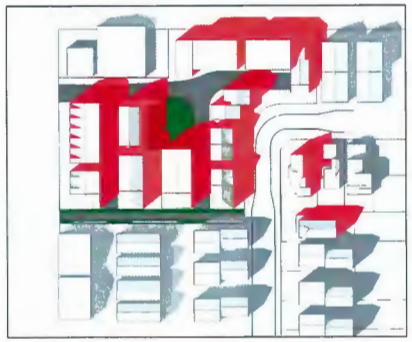

21 SEPTEMBER 4PM

BOHAN
PROPERTIES

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[illegible]

YAMAMOTO
ARCHITECTURE

202 – 33 East 8th Avenue
Vancouver BC V5T 1R5
T – 604 731 1127 F – 604 731 1327

PROJECT –

TOWNHOUSE DEVELOPMENT

10140 10160 10180 NO. 1 ROAD AND
4051 4068 CAVENDISH DRIVE RICHMOND BC

DRAWING TITLE –

PROJECT SHADOW STUDY
TOWNHOUSE DEVELOPMENT

SCALE –

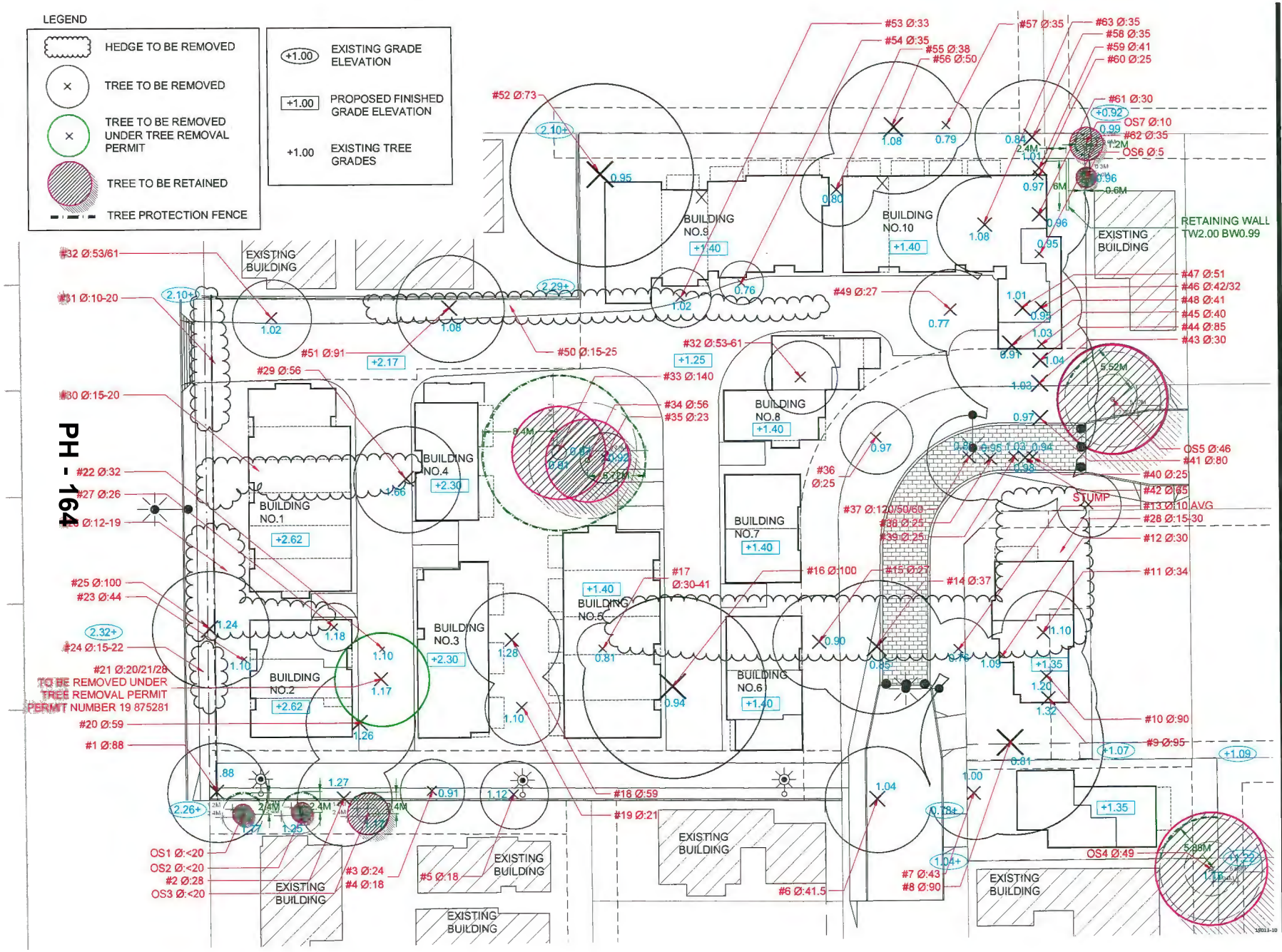
DATE – MAY 08 2019

DRAWN – BS

CHECKED –

DRAWING NO. – A3.0

PROJECT NO. – 1711



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Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6G9
p. 604 294-0011 ; f. 604 294-0022

SEAL

NO	DATE	REVISION DESCRIPTION	DR
10	20 FEB 19	NEW SITE PLAN/CITY COMMENTS	RJ
9	20 JAN 22	FIRE HYDRANT RELOCATED	RJ
8	19 NOV 26	NEW SITE PLAN/CITY COMMENTS	RJ
7	19 NOV 07	NEW SITE PLAN/CITY COMMENTS	RJ
6	19 SEP 06	NEW SITE PLAN/CITY COMMENTS	RJ
5	19 JUN 24	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	19 MAY 24	TREE MANAGEMENT PLAN UPDATED	RJ
2	19 MAY 14	ISSUED FOR REVIEW	RJ
1	19 APR 26	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE MANAGEMENT PLAN

DATE	19.JAN.31
SCALE	1/16"=1'-0"
DRAWN	RJ
DESIGN	RJ
CHK'D	PCM

DRAWING NUMBER L5

PMG PROJECT NUMBER 19-0

ATTACHMENT 15

PH - 164

TREE INFORMATION TABLE

Tree #	Species	DBH (cm)	Health & Condition	CRZ (Min) (m)	CRZ (preferred) (m)	Comments/Recommendations
1	Fir	88	Poor	5.28	10.56	• Previously topped • hydro pruned • heavily covered in ivy. **Recommend:** • Remove.
2	Walnut	28	Good-Fair	1.68	3.36	• Previously topped/pruned. **Recommend:** • Consider for removal.
3	Pine	24	Poor	1.44	2.88	• Previously topped. **Recommend:** • Remove
4	Mtn Ash	18	Poor	1.08	2.16	**Recommend:** • Remove
5	Apple	18	Poor	1.08	2.16	• Extensive decay. **Recommend:** • Remove
6	Cherry	41.5	Fair	2.49	4.98	**Recommend:** • Consider for removal.
7	Fir	43	Poor	2.58	5.16	• 1-sided canopy. **Recommend:** • Remove
8	Poplar	90	Poor	5.4	10.8	• Large limbs prev. Removed • Visible decay. **Recommend:** • Remove
9	Lombardy Poplar	95	Fair	5.7	11.4	• Overmature • Frequent shedding of limbs likely. **Recommend:** • Consider for removal.
10	Lombardy Poplar	90	Fair	5.4	10.8	• Overmature • Frequent shedding of limbs likely. **Recommend:** • Consider for removal.
11	Fir	34	Poor	2.04	4.08	• Lean towards east. **Recommend:** • Remove
12	Spruce	30	Good	1.8	3.6	**Recommend:** • Consider for removal.
13	Hazelnut	10 avg	Fair	0.6	1.2	**Recommend:** • Consider for removal.
14	Apple	37	Poor	2.22	4.44	• 4 main stems • heavily leaning towards property, • heaving roots • extensive decay. **Recommend:** • Remove
15	Walnut	27	Fair	1.62	3.24	**Recommend:** • Consider for removal.
16	Willow	100	Poor	6.0	12.0	• Deadwood & decay throughout canopy. **Recommend:** • Remove
17	Golden cedar	30-41	Good-Fair	2.46	4.92	• Hedge row, 3 stems prev. topped. **Recommend:** • Consider for removal.
18	Spruce	59	Poor	3.54	7.08	• Previously topped • high heeled roots. **Recommend:** • Remove
19	Magnolia	21	Poor	1.26	2.52	• Poor pruning history/canopy structure. **Recommend:** • Remove
20	Cherry	59	Poor	3.54	7.08	• Multi-stem • visible decay. **Recommend:** • Remove
21	Birch	20/ 21/ 28	Poor	1.68	3.36	• Multi-ste • previously topped • visible decay. **Recommend:** • Remove
22	Spruce	32	Poor	1.92	3.84	• Previously topped. **Recommend:** • Remove
23	Cedar	44	Good-Fair	2.64	5.28	• Slight lean towards property. **Recommend:** • Consider for removal.

* NOTE: TREE #21 TO BE REMOVED UNDER TREE REMOVAL PERMIT.
PERMIT NUMBER 19 875281

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p. 604 294-0011 , f. 604 294-0022

SEAL:

NO	DATE	REVISION DESCRIPTION	DR
10	20 FEB 19	NEW SITE PLAN/CITY COMMENTS	RJ
9	20 JAN 22	FIRE HYDRANT RELOCATED	RJ
8	19 NOV 26	NEW SITE PLAN/CITY COMMENTS	RJ
7	19 NOV 07	NEW SITE PLAN/CITY COMMENTS	RJ
6	19 SEP 06	NEW SITE PLAN/CITY COMMENTS	RJ
5	19 JUN 24	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	19 MAY 24	TREE MANAGEMENT PLAN UPDATED	RJ
2	19 MAY 14	ISSUED FOR REVIEWING	RJ
1	19 APR 26	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE INFORMATION TABLE

DATE	19.JAN.31	DRAWING NUMBER
SCALE:		**L6**
DRAWN	RJ	
DESIGN	RJ	
CHKD	PCM	OF 9

19013-10

PMG PROJECT NUMBER **19-013**

24	Holly x 4	15-22	Poor	1.32	2.64	• Hedge row. **Recommend:** • Remove
25	Fir	100	Poor	6.0	12.0	• Previously topped, • hydro pruned • heavily covered in ivy. **Recommend:** • Remove
26	Excelsa cedar	12-19	Fair-Poor	1.14	2.28	• Hedge row. • Previously topped • Hard pruned. **Recommend:** • Consider for removal.
27	Birch	26	Very poor	1.56	3.12	**Recommend:** • Remove
28	Excelsa cedar	15-30	Fair	1.8	3.6	• Hedge row. • Previously topped. **Recommend:** • Consider for removal.
29	Pine	56	Poor	3.36	6.72	• Previously topped to hedge height. **Recommend:** • Remove
30	Excelsa / Emerald cedar	15-20	Fair-Poor	1.2	2.4	• Previously topped. **Recommend:** • Consider for removal.
31	Excelsa cedar	10-20	Fair-Poor	1.2	2.4	• Hedge row. • Previously topped. **Recommend:** • Consider for removal.
32	Deodar cedar	53 / 61	Poor	3.66	7.32	• Twin stem • previously topped. **Recommend:** • Remove
33	Sequoia	140	Fair	8.4	16.8	• Species is rapid grower **Recommend:** • Retain per City request; install tree protection fencing • Arborist supervision required during excavations for adjacent building.
34	Spruce	56	Poor	3.36	6.72	**Recommend:** • Retain per City request; install tree protection fencing
35	Spruce	23	Poor	1.38	2.76	• Conflict with proposed amenity area **Recommend:** • Remove
36	Willow	25	Poor	1.5	3.0	**Recommend:** • Remove

37	Poplar x 3	120 / 50 / 60	Poor	7.2	14.4	• Row of 3 trees. **Recommend:** • Remove
38	Cedar x 4	25	Poor	1.5	3.0	• Row of 4 trees. **Recommend:** • Remove
39	Dogwood	25	Poor	1.5	3.0	**Recommend:** • Remove
40	Cedar	25	Poor	1.5	3.0	**Recommend:** • Remove
41	Poplar	80	Fair-Poor	4.8	9.6	**Recommend:** • Consider for removal.
42	Polar	65	Fair-Poor	3.9	7.8	**Recommend:** • Consider for removal.
43	Pine	30	Poor	1.8	3.6	**Recommend:** • Remove
44	Poplar	85	Fair-Poor	5.1	10.2	**Recommend:** • Consider for removal.
45	Cedar	40	Poor	2.4	4.8	**Recommend:** • Remove
46	Pine / Maple	42/ 32	Poor	1.92	3.84	• Codominant trees. **Recommend:** • Remove
47	Fir	51	Dead	3.06	6.12	**Recommend:** • Remove
48	Willow	41	Poor	2.46	4.92	• Previously topped. **Recommend:** • Remove
49	Plum	27	Very poor	1.62	3.24	• Actively failing • broken limbs • decay. **Recommend:** • Remove
50	Holly/ Excelsa cedar	15-25	Poor	1.5	3.0	• Hedge row. **Recommend:** • Remove
51	Pine	91	Poor	5.46	10.92	• Previously topped at several heights • twin stem • included bark. **Recommend:** • Remove
52	Willow	73	Very Poor	4.38	8.76	• Extensive decay. **Recommend:** • Remove

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SEAL

NO	DATE	REVISION DESCRIPTION	DR.
10	20 FEB 19	NEW SITE PLAN/CITY COMMENTS	RJ
9	22 JAN 19	FIRE HYDRANT RELOCATED	RJ
8	19 NOV 24	NEW SITE PLAN/CITY COMMENTS	RJ
7	19 NOV 09	NEW SITE PLAN/CITY COMMENTS	RJ
6	19 SEP 06	NEW SITE PLAN/CITY COMMENTS	RJ
5	19 JUN 24	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	19 MAY 24	TREE MANAGEMENT PLAN UPDATED	RJ
2	19 MAY 14	ISSUED FOR REVIEWING	RJ
1	19 APR 26	NEW SITE PLAN	RJ

CLIENT

BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE INFORMATION TABLE

DATE 19.JAN.31
SCALE
DRAWN RJ
DESIGN RJ
CHKD PCM

DRAWING NUMBER: **L7** OF 9

19013-10

PMG PROJECT NUMBER **19-013**

53	Fir	33	Poor	1.98	3.96	• Previously topped • decay. **Recommend:** • Remove
54	Plum	35	Dying	2.16	4.32	• Failed limbs • decay. **Recommend:** • Remove
55	Willow	38	Very poor	2.28	4.56	• Multiple stems. **Recommend:** • Remove
56	Willow	50	Very poor	3.0	6.0	• Multiple stems • Decay. **Recommend:** • Remove
57	Willow	35	Very poor	2.1	4.2	• Multiple stems • Decay. **Recommend:** • Remove
58	Willow	35	Very poor	2.1	4.2	• Multiple stems • Decay. **Recommend:** • Remove
59	Birch	41	Poor	2.46	4.92	• Twin stems. **Recommend:** • Remove
60	Willow	25	Very poor	1.5	3.0	• Multiple stems • Decay. **Recommend:** • Remove
61	Willow	30	Very poor	1.8	3.6	• Multiple stems • Decay. **Recommend:** • Remove
62	Willow	35	Very poor	2.16	4.32	• Multiple stems • Decay. **Recommend:** • Remove
63	Willow	35	Very poor	2.16	4.32	• Multiple stems • Decay. **Recommend:** • Remove
OS1	Excelsa	<20	Good	1.2	2.4	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line
OS2	Excelsa	<20	Good	1.2	2.4	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line
OS3	Fir	<20	Good	1.2	2.4	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line
OS4	Deodar cedar	49	Good	2.94	5.88	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing • **Arborist supervision is recommended during excavations required for proposed single family house**
OS5	Maple	46	Good	2.76	5.52	• Blvd./Neighbour's property. **Recommend:** • Retain; install tree protection fencing
OS6	Walnut	5	Good	0.3	0.6	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line
OS7	Lilac	10	Good	0.6	1.2	• Neighbour's property. **Recommend:** • Retain; install tree protection fencing at property line

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SEAL

NO	DATE	REVISION DESCRIPTION	DR.
10	20 FEB 19	NEW SITE PLAN/CITY COMMENTS	RJ
9	20 JAN 22	FIRE HYDRANT RELOCATED	RJ
8	19 NOV 21	NEW SITE PLAN/CITY COMMENTS	RJ
7	19 NOV 21	NEW SITE PLAN/CITY COMMENTS	RJ
6	19 SEP 21	NEW SITE PLAN/CITY COMMENTS	RJ
5	19 JUN 21	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 21	NEW SITE PLAN	RJ
3	19 MAY 21	TREE MANAGEMENT PLAN UPDATED	RJ
2	19 MAY 19	ISSUED FOR REZONING	RJ
1	19 APR 19	NEW SITE PLAN	RJ

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BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE INFORMATION TABLE

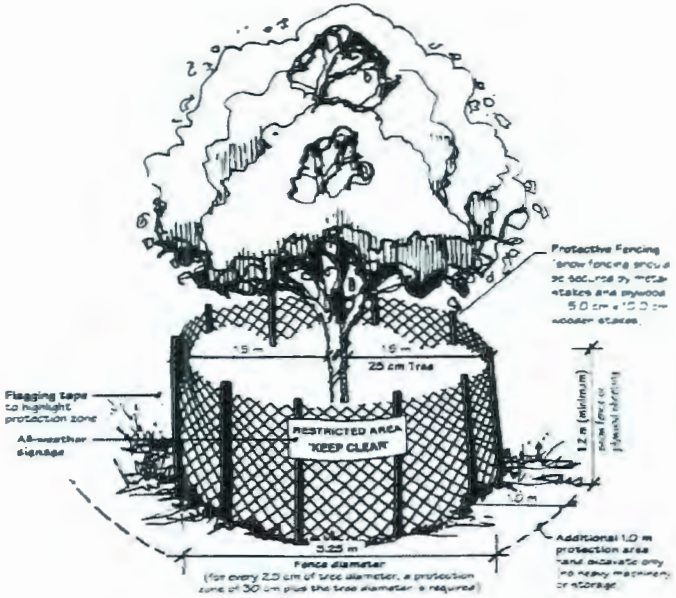
DATE 19.JAN.31
SCALE
DRAWN RJ
DESIGN RJ
CHKD PCM

DRAWING NUMBER **L8** OF 9

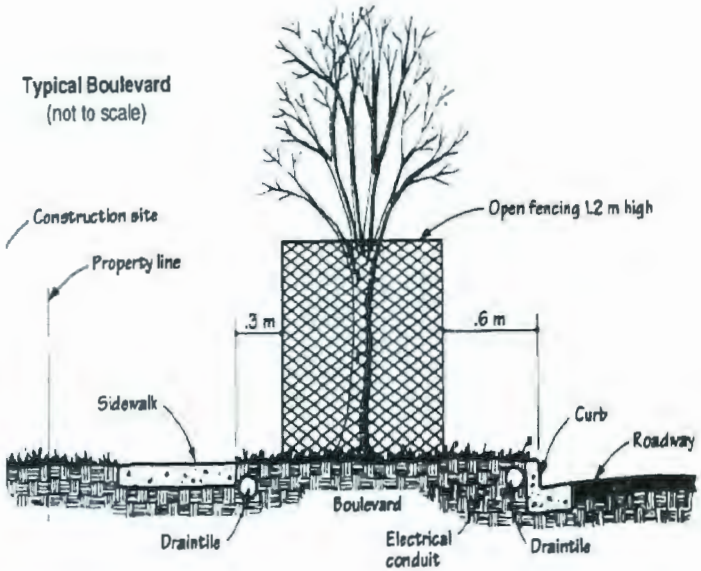
19013-10

PMG PROJECT NUMBER **19-013**

SUITABLE REPLACEMENT TREES	
SPECIES	
COMMON NAME	BOTANICAL NAME
ACER PALMATUM	JAPANESE MAPLE
ACER RUBRUM 'RED SUNSET'	RED SUNSET MAPLE
AMELANCHIER x GRANDIFLORA 'AUTUMN BRILLIANCE'	AUTUMN BRILLIANCE SERVICEBERRY
CORNUS KOUSA 'CHINENSIS'	CHINESE KOUSA DOGWOOD
LIQUIDAMBAR STYRACIFLUA 'SLENDER SILHOUETTE'	SLENDER SILHOUETTE SWEETGUM
LIQUIDAMBAR STYRACIFLUA 'WORPLESDON'	WORPLESDON SWEET GUM
PINUS NIGRA 'ARNOLD SENTINEL'	ARNOLD SENTINEL AUSTRIAN BLACK PINE
PSEUDOTSUGA MENZIESII	DOUGLAS FIR
PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR



1 TREE PROTECTION BARRIER



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SEAL

NO	DATE	REVISION DESCRIPTION	DR
10	20 FEB 19	NEW SITE PLAN/CITY COMMENTS	RJ
9	30 JAN 17	TREE REMOVAL REQUESTED	RJ
8	19 NOV 16	NEW SITE PLAN/CITY COMMENTS	RJ
7	19 NOV 07	NEW SITE PLAN/CITY COMMENTS	RJ
6	19 SEP 06	NEW SITE PLAN/CITY COMMENTS	RJ
5	19 JUN 04	TREE MANAGEMENT PLAN UPDATED	RJ
4	19 JUN 31	NEW SITE PLAN	RJ
3	19 MAY 04	TREE MANAGEMENT PLAN UPDATED	RJ
2	19 MAY 14	ISSUED FOR REZON. NO	RJ
1	19 APR 16	NEW SITE PLAN	RJ

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BOHAN PROPERTIES

PROJECT

TOWNHOUSE DEVELOPMENT

10140, 10160, 10180 NO. 1 ROAD AND 4051, 4068 CAVENDISH DRIVE RICHMOND, BC

DRAWING TITLE

TREE MANAGEMENT DETAIL

DATE	19.JAN.31	DRAWING NUMBER
SCALE		L9
DRAWN	RJ	
DESIGN	RJ	
CHKD	PCM	OF 9

19013-10 PMG PROJECT NUMBER 19-013

YAMAMOTO ARCHITECTURE

DATE —
February 27, 2020

TO —
City of Richmond Planning Dept.
6911 No. 3 Rd.
Richmond, B.C V6Y 2C1

DEVELOPER —
Bohan Properties

REFERENCE —
RZ 18-820669

ATTN —
Edwin Lee

PROJECT —
10140, 10160 No. 1 Road & 4051 & 4068 Cavendish Drive, Richmond, BC

Dear Edwin,

RE: **Letter of Commitment for Energy Step Code Requirements for Rezoning**

Project Address: 10140, 10160 & 10180 No. 1 Rd and 4051 & 4068 Cavendish Drive, Richmond, BC

Please accept this letter as confirmation that the townhouse development comprised of the following addresses 10140, 10160 & 10180 No. 1 Road and 4051 & 4068 Cavendish Drive, Richmond, BC will comply with the requirements of the Energy Step Code Part 9 Policy for the Rezoning Stage.

At this stage the preferred pathway for compliance has not yet been determined for this project.

Regards,

Taizo Yamamoto, AIBC

Professional Seal

Signed: _____

Name: Taizo Yamamoto, Architect AIBC

202 - 33 East 8th Avenue Vancouver, BC V5T 1R5 — T 604 731 1127 F 604 731 1327 — yamamotoarchitecture.com

City of Richmond

Rezoning Considerations

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 10140, 10160 & 10180 No. 1 Road and 4051 & 4068 Cavendish Drive **File No.:** RZ 18-820669

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 10156, the developer is required to complete the following:

1. Final Adoption of OCP Amendment Bylaw 10155.
2. A minimum width of 15.0 m road dedication to link the two discontinuous ends of Cavendish Drive through the subject site. Exact width is to be confirmed with survey information to be submitted by the applicant.
3. An approximately 0.4 m wide road dedication across the entire No. 1 Road frontage to accommodate the required frontage improvements including a new sidewalk and grass and treed boulevard. Exact width is to be confirmed with survey information to be submitted by the applicant.
4. Granting of a 6.0 m wide statutory right-of-way along the south property line of the townhouse development site for the purposes of public access between No. 1 Road and Cavendish Drive. Any works essential for public access within the required statutory right-of-way (SRW) are to be included in the Servicing Agreement (SA). The design must be prepared in accordance with City specifications & standards and the construction of the works will be inspected by the City concurrently with all other SA related works. The property owner/strata are responsible for all maintenance of improvements, including but not limited to the public walkways/sidewalks, street furniture, lighting and landscaping within the SRW, and are responsible for all liability of SRW area.
5. Registration of a statutory right-of-way (SRW), and/or other legal agreements or measures; as determined to the satisfaction of the Director of Development, over the entire area of the proposed entry driveway on the townhouse site from No. 1 Road, in favour of future residential developments to the north, including the installation of way-finding and other appropriate signage on the subject property, and requiring a covenant that the owner provide written notification of this through the disclosure statement to all initial purchasers, provide an acknowledgement of the same in all purchase and sale agreements, and erect signage in the initial sales centre advising purchasers of the potential for these impacts. Language should be included in the SRW document that the City will not be responsible for maintenance or liability within the SRW and that utility SRW under the drive aisle is required.
6. Registration of a flood indemnity covenant on title.
7. Subdivision of the existing properties into two development parcels: one townhouse development site on the west side of Cavendish Drive and one single family development site on the east side of Cavendish Drive. (Note: demolition of the existing dwellings on site will be required).
8. Registration of a legal agreement on Title to ensure that no development on the single family development site on the east side of Cavendish Drive is permitted until the entire site is rezoned to "Single Detached (RS2/B)" and is further subdivided into two single family lots as per the "Single Detached (RS2/B)" zone.
9. Registration of a legal agreement on title to ensure that, at future development stages (i.e., Subdivision and Building Permit), the developments in the proposed single family subdivision are generally consistent with the preliminary site plans (including proposed setbacks), landscape plans and building elevations included as Attachment 13 to this report.
10. Registration of a legal agreements on Title or other measures, as determined to the satisfaction of the Director of Development, to ensure that:
    a) No final Building Permit inspection is granted until three secondary suites are constructed on the townhouse development site, to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw.
    b) The secondary suites cannot be stratified or otherwise held under separate title.
11. Registration of a legal agreement on title prohibiting the conversion of the tandem parking area into habitable space

Initial: _____

12. Registration of the City's standard Housing Agreement to secure six affordable housing units, the combined habitable floor area of which shall comprise no less than 14% of the subject development's total residential building area on the townhouse development site. Occupants of the affordable housing units subject to the Housing Agreement shall enjoy full and unlimited access to and use of all on-site indoor and outdoor amenity spaces. The terms of the Housing Agreements shall indicate that they apply in perpetuity and provide for the following:

Unit Type	Number of Units	Minimum Unit Area	Maximum Monthly Unit Rent**	Total Maximum Household Income**
2 Bdrm + Den	6	69 m^2 (741 ft^2)	$1,218	$46,800 or less

* Unit mix in the above table may be adjusted through the Development Permit Process provided that the total area comprises at least 10% of the subject development's total residential building area.

** May be adjusted periodically as provided for under adopted City policy.

13. City acceptance of the developer's offer to voluntarily contribute $42,000 to the City's Tree Compensation Fund for the planting of replacement trees within the City.
14. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
15. Submission of a Tree Survival Security to the City in the amount of $20,000.00 for the140 cm caliper Sequoia tree (specifically tag# 33) and the 56 cm caliper Spruce tree (specifically tag# 34) to be retained.
16. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
17. Registration of a legal agreement on Title to ensure that no final Building Permit inspection is granted until a two-bedroom secondary suite is constructed on both of the two future single family residential lots, to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw.
18. City acceptance of the developer's offer to voluntarily contribute $0.85 per buildable square foot (e.g. $36,669.58) to the City's public art fund.
19. Contribution of $81,600.00 in-lieu of on-site indoor amenity space.
20. Submission of a Landscape Plan for the proposed single family subdivision, prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development, and deposit of a Landscaping Security based on 100% of the cost estimate provided by the Landscape Architect, including installation costs. The Landscape Plan should:
    - include a mix of coniferous and deciduous trees; and
    - include the dimensions of tree protection fencing as illustrated on the Tree Retention Plan attached to this report.
21. Registration of a legal agreement on Title identifying that the proposed development must be designed and constructed in a manner that mitigates potential traffic noise from No. 1 Road to the proposed dwelling units. Dwelling units must be designed and constructed to achieve:
    a) CMHC guidelines for interior noise levels as indicated in the chart below:

Portions of Dwelling Units	Noise Levels (decibels)
Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

    b) The ASHRAE 55-2004 "Thermal Environmental Conditions for Human Occupancy" standard for interior living spaces.
22. The submission and processing of a Development Permit* completed to a level deemed acceptable by the Director of Development.
23. Enter into a Servicing Agreement* for the design and construction of a new public walkway along the south property line of the site between No. 1 Road and Cavendish Drive, a new emergency access/greenway to connect the two discontinuous ends of Cavendish Drive, frontage beautification works on the road frontages, as well as water, storm sewer and sanitary sewer upgrades and service connections along both No. 1 Road and Cavendish Drive. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to,

Initial: _____

Water Works:

- Using the OCP Model, there is 368 L/s and 103 L/s of water available at a 20 psi residual at the hydrants located along No. 1 Road and Cavendish Drive respectively. Based on your proposed development, your townhouse development requires a minimum fire flow of 220 L/s and your single family home development requires a minimum fire flow of 95 L/s.
- At the Developer's cost, the Developer is required to:
  - Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm the development has adequate fire flow for onsite fire protection at the Building Permit stage. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit designs.
  - Coordinate with the City's Fire Department to determine whether an onsite fire hydrant is required to service the townhouse development.
  - Upgrade approx. 67m of watermain along Cavendish Drive (east-west) from 150 AC to 200 PVC. The tie in to the east shall be to the existing watermain along Pughwash Place.
  - Install approx. 51m of 200mm PVC watermain from the south property line of 10180 No. 1 Road towards north along the new Cavendish Road. Continue the new watermain approx. 13m towards the east. Tie in to the south shall be to the ex. watermain. Tie in to the east shall be to the upgraded. watermain.
  - Install a new service connection for each of the two single family units off of the new water main on Cavendish Dr., complete with water meter assembly.
  - Provide an adequately sized utility SRW for a new water mater and its chamber that shall be placed inside the proposed townhouse development. A plan showing the location and size of the required utility SRW shall be submitted to the City for review and approval at the servicing agreement stage.
- At the Developer's cost, the City will:
  - Cut and cap at main all existing water service connections to the developing property.
  - Reconnect all existing water service connection on Cavendish Drive to the new or upgraded watermain.
  - Install a new service connection for the townhouse development off of the existing 300mm watermain along No. 1 Road frontage, complete with water meter placed inside the development with in an adequate City utility ROW.

Storm Sewer Works:

- At the Developer's cost, the Developer is required to:
  - Install approximately 44m of new 600mm storm sewer from the existing manhole STMH5319 northwards and approximately 24m towards east and shall tie into ex. manhole STMH5328. Upgrade STMH5328 and STMH5319 to a 1200mm diameter manhole. The manhole at the intersection of the two storm sewers shall be the highpoint of the system.
  - Install approx. 90 m of 300mm storm sewer for road drainage for the proposed pedestrian walk way, Complete with manholes and CBs.
  - Install a service lateral off of the new storm sewer on Cavendish Road at the adjoining property line of the two single family homes, complete with one new IC and 2 service connections.
  - Appropriately sized manholes and catch basins are required for the new storm sewer, spaced as per City standard.
- At the Developer's cost, the City will:
  - Install a new storm service connection for the townhouse development off of existing box culvert located along the No. 1 Road frontage.
  - Cut and cap at main all existing storm service connections.
  - Remove all existing inspection chambers and storm service leads and dispose offsite.
  - Complete all tie-ins, cutting, and capping of all proposed works to the existing city infrastructure.

Initial: _____

Sanitary Sewer Works:

- At the Developer's cost, the Developer is required to not start onsite excavation and/or foundation works until the City has completed the proposed rear yard sanitary connections. Also indicate this as a note on the site plan and SA design plans.
- At the Developer's cost, the City will:
  - Install a new sanitary service connection for the townhouse development off of the existing sanitary manhole SMH4217.
  - Remove the existing IC SIC7205, the service connection to 10160 No. 1 Road, the service lateral extending northwards from SIC15873.
  - Upgrade the existing inspection chamber SIC15873 and install service connection to the new single family development west of 4040 Cavendish Dr. off of the new IC. Reconnect the connection to 4040 Cavendish Dr.
  - Confirm that inspection chamber SIC9376 is up to City standard. If confirmed, retain the existing IC and service connection to service the new single family lot north of 4080 Cavendish Road. If SIC9376 is not up to City Standard, upgrade it with a new IC and reconnect all connections.
  - Provide a 15m x 3m sanitary right of way along the north property line of the new single family development north of 4080 Cavendish Road, measuring 15m from the east property line.
  - Discharge 5m x 15m sanitary right of way located on 4068 Cavendish Drive.
  - Cut and cap at main all existing sanitary service connections to the proposed site.
  - Remove all existing inspection chambers and sanitary leads connected to the proposed site and dispose offsite.
  - Complete all proposed sanitary sewer service connections and tie-ins.

Frontage Improvements:

- At the Developer's cost, the Developer is required to:
  - provide frontage improvements:

    No. 1 Road Frontage

    i. Sidewalk, boulevard and curb/gutter:
       - Remove and replace the existing concrete curb and gutter.
       - Remove the existing sidewalk and railing, and construct a new 1.5m-wide concrete sidewalk next to the new west property line of the subject site. The functional plan does not show the s/w at PL and needs to be updated.
       - Construct a new minimum 1.5m-wide grass boulevard between the new sidewalk and the new curb.
       - The new sidewalk and boulevard are to transition to meet the existing frontage treatments to the north and south of the subject site.

    ii. All existing driveways along the No. 1 Road development frontage are to be closed permanently. The Developer is responsible for the removal of the existing driveway let-downs and the replacement with barrier curb/gutter, concrete sidewalk, and grass boulevard as described under Item i above. Vehicle access to the proposed townhouse development is to be provided by a single driveway located on No. 1 Road.

    iii. Provide a standard City of Richmond lighting system and other utility requirements along No. 1 Road. There are utility poles that may need to be relocated.

    Cavendish Drive Frontage Improvements

    iv. Construct a new greenway linking the discontinuous sections of Cavendish Drive to the east and south of the subject site, with a road cross-section consisting of:
       - A stamped asphalt walkway with a minimum pavement width of 6.0 m.
       - Concrete edge banding along the outside edge of the asphalt walkway.

Initial: _____

- Grass / tree boulevards over the remaining width between the walkway and the new property lines of Cavendish drive.
- Removable steel bollards to allow emergency vehicle access at the transitions to Cavendish Drive south and east of the subject site.
- Lighting is required as part of servicing agreement works.

v. Construct new smooth transitions between the existing cross-sections of Cavendish Drive south and east of the subject site, and the new greenway described in Item iv, including:

- Barrier curb and gutter on both sides of Cavendish Drive.
- New driveways for 4068, 4040, and 4039 Cavendish Drive, as well as the new single family lot at the corner of Cavendish Drive.
- 2.0m-wide concrete sidewalks along the back-of-curb to link the new greenway to the existing sidewalks on Cavendish Drive, as well as the new pedestrian walkway along the south of the subject site.
- Grass/tree boulevards over the remaining width between the new curbs/sidewalks and the property lines of Cavendish Road.
- The new sidewalk and boulevard are to transition to meet the existing frontage treatments to the east and south of the subject site.

vi. Remove and replace the full existing cross section of Cavendish Drive at the dead ends south and east of the subject site, including the existing pavement, curb and gutter, and sidewalk as described in Item iv. The precise extent of this work is to be determined at the City's sole discretion as part of the Servicing Agreement for the proposed development; however, it shall extend to the east property line of 4039 Cavendish Drive and the south property line of 4079 Cavendish Drive at a minimum.

vii. Provide a standard City of Richmond lighting system and other utility requirements along Cavendish Drive. There are utility poles that may need to be relocated.

<u>Pedestrian Access Walkway</u>

viii. Construct a new pedestrian access walkway along the south property line of the subject site to connect No. 1 Road and Cavendish Drive, with a cross-section consisting of:

- A 3.0m-wide asphalt pedestrian walkway
- A 1.5m-wide grass buffer strip on either side of the walkway
- Wayfinding signage that clearly identifies the walkway as a public access route.
- Pedestrian scale lighting to be included as part of servicing agreement works.

ix. Provide required pedestrian lighting and other utility requirements for the proposed walkway along the south property line.

- Luminaires are to be LED and are to match the roadway lighting within the surrounding area.
- Locate all above ground utility cabinets and kiosks required to service the proposed development within the developments site (see list below for examples). A functional plan showing conceptual locations for such infrastructure shall be included in the development process design review. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the right of way requirements and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of SRWs that shall be shown in the functional plan and registered prior to SA design approval:
  - BC Hydro Vista - Confirm SRW dimensions with BC Hydro
  - BC Hydro PMT – Approximately 4mW X 5m (deep) – Confirm SRW dimensions with BC Hydro
  - BC Hydro LPT – Approximately 3.5mW X 3.5m (deep) – Confirm SRW dimensions with BC Hydro
  - Street light kiosk – Approximately 2mW X 1.5m (deep)
  - Traffic signal controller cabinet – Approximately 3.2mW X 1.8m (deep)
  - Traffic signal UPS cabinet – Approximately 1.3mW X 2.2m (deep)

Initial: _____

- Shaw cable kiosk – Approximately 1mW X 1m (deep) – show possible location in functional plan. Confirm SRW dimensions with Shaw
- Telus FDH cabinet - Approximately 1.1mW X 1m (deep) – show possible location in functional plan. Confirm SRW dimensions with Telus

General Items:

- The Developer is required to:
  - Provide, within the building permit application, a geotechnical assessment of preload and soil preparation impacts on the existing utilities fronting the development site) and provide mitigation recommendations.
  - Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
  - Provide Private utility companies rights-of-ways to accommodate their equipment (i.e. above ground private utility kiosks, vista, transformers, etc. shall be designed to minimize the impacts on public space); the developer is required contact the private utility companies to learn of their requirements.
  - Pre-duct for future hydro, telephone and cable utilities along all property frontages.

Prior to a Development Permit* being forwarded to the Development Permit Panel for consideration, the developer is required to:

1. Complete an acoustical and thermal report and recommendations prepared by an appropriate registered professional, which demonstrates that the interior noise levels and noise mitigation standards comply with the City's Official Community Plan and Noise Bylaw requirements. The standard required for air conditioning systems and their alternatives (e.g. ground source heat pumps, heat exchangers and acoustic ducting) is the ASHRAE 55-2004 "Thermal Environmental Conditions for Human Occupancy" standard and subsequent updates as they may occur. Maximum interior noise levels (decibels) within the dwelling units must achieve CMHC standards follows:

Portions of Dwelling Units	Noise Levels (decibels)
Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

Prior to Development Permit* issuance, the following must be completed:

1. Submission of a Landscaping Security based on 100% of the cost estimate provided by the landscape architect.

At Subdivision* stage, the developer must complete the following requirements:

1. Payment of property taxes up to the current year, Development Cost Charges (City, Metro Vancouver and Translink), School Site Acquisition Charge, Address Assignment Fees, and the costs associated with the completion of the required servicing works and frontage improvements.

Prior to Demolition Permit Issuance, the developer must complete the following requirements:

1. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.

   Note: Should the applicant wish to begin site preparation work after third reading of the rezoning bylaw, but prior to final adoption of the rezoning bylaw and issuance of the Development Permit, the applicant will be required to obtain a Tree Permit and submit landscaping security (i.e. $88,500 in total) to ensure the replacement planting will be provided.

Initial: _____

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
2. Incorporation of energy efficiency, CPTED, sustainability, and accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
3. If applicable, payment of latecomer agreement charges, plus applicable interest associated with eligible latecomer works.
4. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

* This requires a separate application.

- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

  All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

  The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed _____ Date _____

City of Richmond

Bylaw 10155

Richmond Official Community Plan Bylaw 7100 Amendment Bylaw 10155 (RZ 18-820669) 10140, 10160 & 10180 No. 1 Road and 4051 & 4068 Cavendish Drive

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Official Community Plan Bylaw 7100, Schedule 2.4 (Steveston Area Plan), is amended by replacing the Steveston Area Land Use Map with "Schedule A attached to and forming part of Bylaw 10155".

2. This Bylaw may be cited as **"Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 10155".**

FIRST READING	APR 14 2020
PUBLIC HEARING	
SECOND READING	
THIRD READING	
OTHER CONDITIONS SATISFIED	
ADOPTED	

CITY OF RICHMOND
APPROVED by
E.L.
APPROVED by Manager or Solicitor

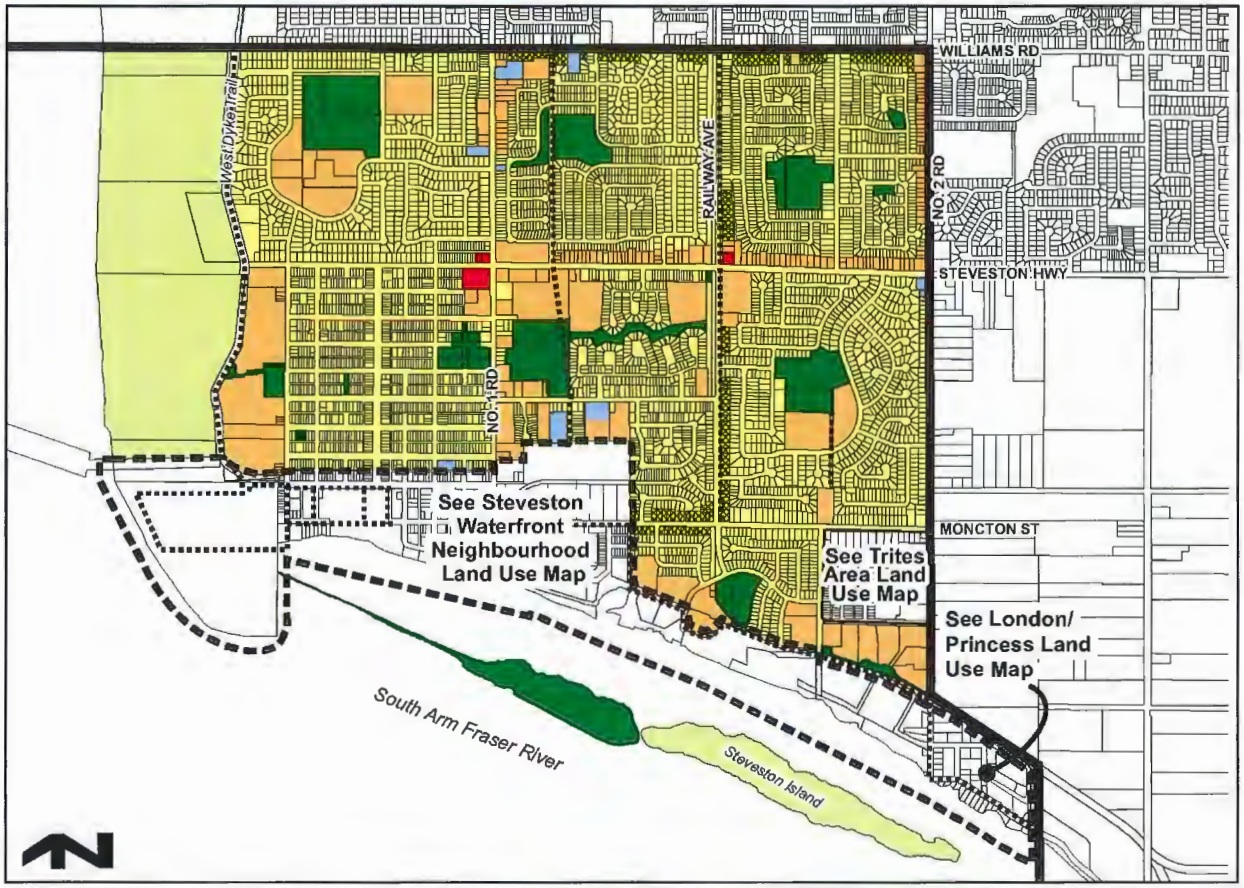
MAYOR

CORPORATE OFFICER

"Schedule A" attached to and forming part of Bylaw 10155

Schedule A

Steveston Area Land Use Map



Single-Family	Institutional
Single-Detached/Duplex/Triplex	Conservation Area
Multiple-Family	Trail
Commercial	Steveston Area Boundary
Public Open Space	Steveston Waterfront Neighbourhood Boundary

City of Richmond

Bylaw 10156

Richmond Zoning Bylaw 8500
Amendment Bylaw 10156 (RZ 18-820669)
10140, 10160 & 10180 No. 1 Road and 4051 & 4068 Cavendish Drive

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by:

   a. Inserting the following into the end of the table contained in Section 5.15.1c regarding affordable housing density bonusing provisions:

Zone	Sum Per Buildable Square Foot of Permitted **Principal Building**
"ZT88	$8.50"

   b. Inserting as Section 17.88 thereof the following:

" **17.88 Town Housing (ZT88) – No. 1 Road (Steveston)**

17.88.1 Purpose

The **zone** provides for **town housing** and other compatible **uses**.

17.88.2 Permitted Uses

- **child care**
- **housing, town**

Secondary Uses

- **boarding and lodging**
- **home business**
- **community care facility, minor**

17.88.3 Permitted Density

1. The maximum **floor area ratio** (FAR) is 0.40, together with an additional 0.1 **floor area ratio** provided that it is entirely used to accommodate **amenity space**.

2. Notwithstanding Section 17.88.3.1, the reference to "0.4" is increased to a higher **density** of "0.60" if the **owner**, at the time **Council** adopts a zoning amendment bylaw to include the **owner's lot** in the ZT88 **zone**, pays into the **affordable housing reserve** the sum specified in Section 5.15 of this bylaw.

3. Notwithstanding Section 17.88.3.1, the reference to "0.4" is increased to a higher **density** of "0.65", if the **owner**, at the time **Council** adopts a zoning amendment bylaw to include the **owner's lot** in the ZT88 zone, and provided that prior to the first occupancy of the **building** the **owner**:

   a) provides in the **building** not less than 6 **affordable housing units** and the combined **habitable space** of the total number of **affordable housing units** comprises not less than 14% of total **floor area** that is **habitable space**; and

   b) enters into a **housing agreement** with respect to the **affordable housing units** and registers the **housing agreement** against the title to the **lot**.

17.88.4 Permitted Lot Coverage

1. The maximum **lot coverage** is 40% for **buildings**.

2. No more than 65% of the **lot** may be occupied by **buildings**, **structures** and **non-porous surfaces**.

3. 25% of the **lot area** is restricted to **landscaping** with live plant material.

17.88.5 Yards & Setbacks

1. The minimum **road setback** is 4.5 m from No. 1 Road and from Cavendish Drive.

2. Notwithstanding Section 4.9 of this bylaw, no **building** projection including fireplaces and chimneys, **bay windows** and **hutches**, **balconies** and **porches**, shall be permitted in the minimum **road setback**.

3. The minimum **side yard** and **rear yard** is 3.0 m.

17.88.6 Permitted Heights

1. The maximum **height** for **buildings** is 12.0 m, but containing no more than 3 **storeys**.

2. The maximum **height** for **accessory buildings** is 5.0 m.

3. The maximum **height** for **accessory structures** is 9.0 m.

17.88.7 Subdivision Provisions/Minimum Lot Size

1. The minimum **lot width** on major **arterial roads** is 50.0 m.

2. The minimum **lot depth** is 35.0 m.

3. There is no minimum **lot area**.

17.88.8 Landscaping & Screening

1. **Landscaping** and **screening** shall be provided in accordance with the provisions of Section 6.0.

17.88.9 On-Site Parking and Loading

1. On-site **vehicle** and bicycle parking and loading shall be provided according to the standards set out in Section 7.0.

17.88.10 Other Regulations

1. In addition to the regulations listed above, the General Development Regulations of Section 4.0 and the Specific Use Regulations of Section 5.0 apply. "

2. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by designating that portion outlined in bold and shown as Area "A" on "Schedule A" attached to and forming part of this bylaw as **"TOWN HOUSING (ZT88) – No. 1 Road (Steveston)".**

3. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by designating that portion outlined in bold and shown as Area "B" on "Schedule A" attached to and forming part of this bylaw as **"SINGLE DETACHED (RS2/B)".**

4. This Bylaw may be cited as **"Richmond Zoning Bylaw 8500, Amendment Bylaw 10156".**

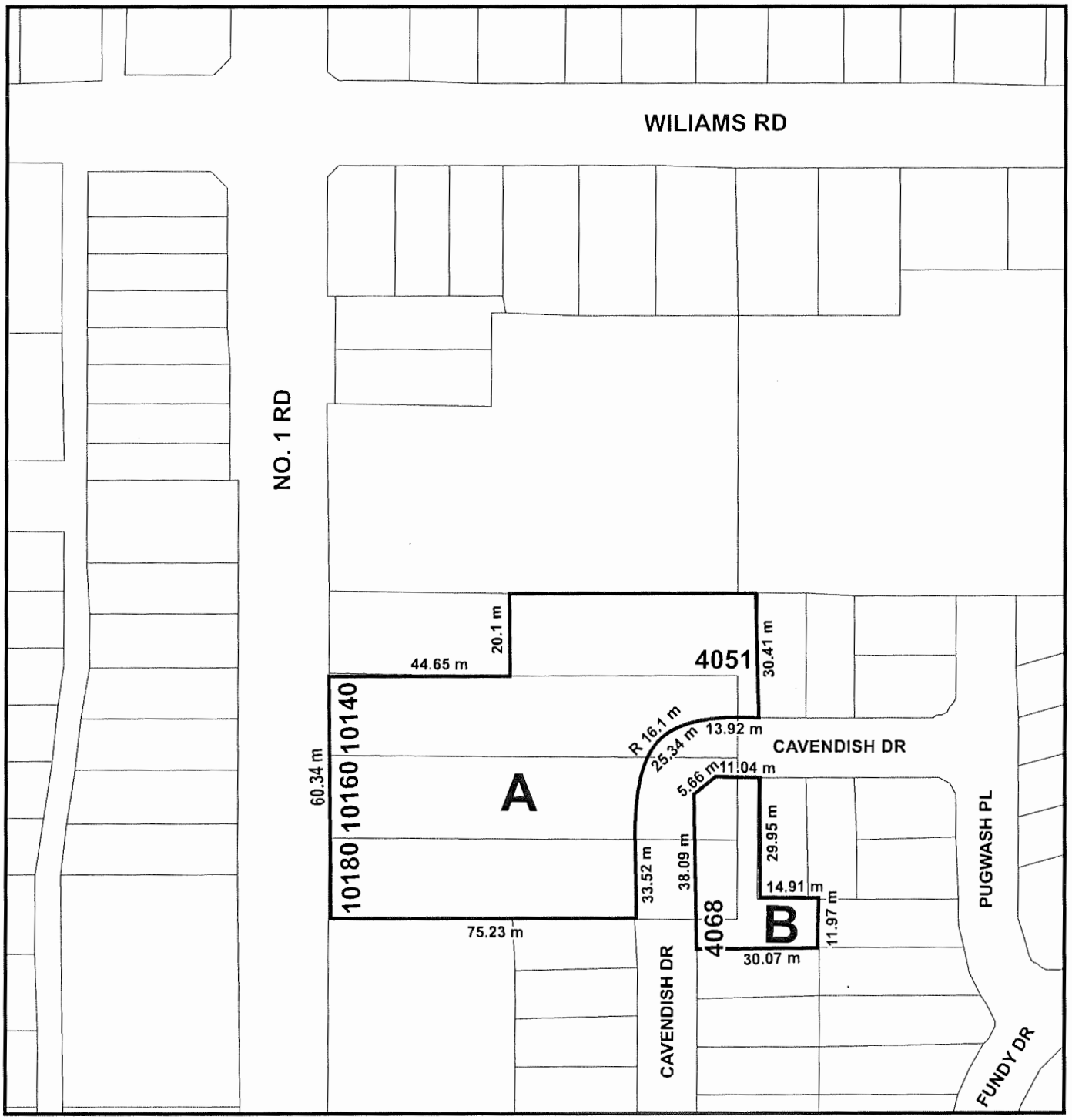
FIRST READING	APR 14 2020
A PUBLIC HEARING WAS HELD ON	
SECOND READING	
THIRD READING	
OTHER CONDITIONS SATISFIED	
ADOPTED	

CITY OF RICHMOND
APPROVED by
APPROVED by Director or Solicitor
BRB

MAYOR

CORPORATE OFFICER

City of Richmond



"Richmond Zoning Bylaw 8500 Amendment Bylaw 10156"
"Schedule A"

Original Date: 03/10/20

Revision Date:

Note: Dimensions are in METRES

City of Richmond

Report to Committee

To:	Planning Committee	**Date:**	March 23, 2020
From:	Wayne Craig Director, Development	**File:**	RZ 18-843479
Re:	**Application by Fougere Architecture Inc. for Rezoning at 7100 and 7120 Ash Street from the "Single Detached (RS1/F)" Zone to the "Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)" Zone**		

Staff Recommendation

That Richmond Zoning Bylaw 8500, Amendment Bylaw 10163, for the rezoning of 7100 and 7120 Ash Street from the "Single Detached (RS1/F)" zone to the "Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)" zone, be introduced and given first reading.

Wayne Craig
Director, Development
(604-247-4625)

WC:jr
Att. 7

REPORT CONCURRENCE		
ROUTED TO:	**CONCURRENCE**	**CONCURRENCE OF GENERAL MANAGER**
Affordable Housing	☑	

6426161

Staff Report

Origin

Fougere Architecture Inc. has applied to the City of Richmond for permission to rezone 7100 and 7120 Ash Street from the "Single Detached (RS1/F)" zone, to the "Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)" zone, in order to develop 17 two-storey townhouse units with vehicle access from Ash Street. A location map is provided in Attachment 1.

Findings of Fact

A Development Application Data Sheet providing details about the development proposal is provided in Attachment 2.

Subject Site Existing Housing Profile

The subject site consists of two lots, each containing a single-family dwelling. The applicant has indicated that one of the dwellings was previously owner-occupied, and one was rented. Neither dwelling contains a secondary suite. Both dwellings would be demolished.

Surrounding Development

Development immediately surrounding the subject site is as follows:

- To the North: Two-storey townhouses on a property zoned "Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)", with access from Ash Street.
- To the South: A single-family dwelling on a property zoned "Single Detached (RS1/F)"," with access from Ash Street.
- To the East: Single-family dwellings on properties zoned "Single Detached (ZS14) – South McLennan (City Centre)", with access from Sills Avenue.
- To the West: Across Ash Street, single-family dwellings on properties zoned "Single Detached (RS1/F)", with access from Ash Street.

Related Policies & Studies

Official Community Plan/McLennan South Sub-Area Plan

The subject site is located in the City Centre planning area, and is designated "Neighbourhood Residential" in the Official Community Plan (OCP). The proposed rezoning is consistent with this designation.

The subject site in located in the area governed by the McLennan South Sub-Area plan, and is designated "Residential, 2 ½ Storeys", which permits development in triplex, duplex, and single-family forms (Attachment 3). Townhouses with units arranged in duplex or triplex

clusters are considered an appropriate development form in this area. The proposed rezoning is consistent with this designation.

The McLennan South Sub-Area Plan includes requirements for implementation of a new ring-road system, which would provide for greater vehicle and pedestrian circulation through the neighbourhood (Attachment 4). The northern part of this ring-road, Sills Avenue, has been partially constructed to the east and west of the subject site. A straight alignment of Sills Avenue would require dedication and construction of the new road through a portion of the subject site.

Staff propose an off-set alignment of Sills road further south. The City owns property between Ash Street and Armstrong Street which would facilitate construction of this road in coordination with the redevelopment of adjacent properties.

The McLennan South Sub-Area Plan allows new roads to deviate from the circulation map, and an amendment to the map is not required, provided the proposed deviation:

- Does not result in significant traffic impacts on or compromise access to adjacent properties.
- Does not result in a significant net increase in the amount of new road envisioned under the circulation map.
- Results in a coherent pattern that maintains the intended pedestrian scale of the area's blocks consistent with the neighbourhood's residential character.
- Provides a recognizable benefit to the area.

Transportation staff have reviewed the application and support the proposed scheme, as it supports the Sub-Area Plan objectives of breaking up large blocks and increasing pedestrian connectivity. Furthermore, the offset alignment of Sills Avenue will provide natural traffic calming and reinforce Granville Avenue and General Currie Road as the dominant east-west traffic routes.

To break up the block and provide an additional pedestrian connection, a mixed-use pathway is provided along the south property line of subject site. This pathway would be expanded through redevelopment of the properties to the south.

Affordable Housing Strategy

As per the City's Affordable Housing Strategy, townhouse rezoning applications are required to provide a cash-in-lieu contribution of $8.50 per buildable square foot towards the City's Affordable Housing Reserve Fund. The applicant proposes to make a cash-in-lieu contribution of $190,468, which is consistent with the Affordable Housing Strategy.

Public Art Program Policy

The applicant will be participating in the City's Public Art Program by making a voluntary contribution to the City's Public Art Reserve Fund for City-wide projects on City lands. Since

this rezoning was received in 2018, the applicable rate for the contribution is $0.85 per buildable square foot, for a total contribution in the amount of $19,046.80.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on title is required prior to final adoption of the rezoning bylaw.

Public Consultation

A rezoning sign has been installed on the subject property. Staff have not received any comments from the public about the rezoning application in response to the placement of the rezoning sign on the property.

Should the Planning Committee endorse this application and Council grant first reading to the rezoning bylaw, the bylaw will be forwarded to a Public Hearing, where any area resident or interested party will have an opportunity to comment. Public notification for the Public Hearing will be provided as per the *Local Government Act*.

Analysis

Site Planning

The applicant proposes 17 two-storey townhouse units with a central drive aisle. Conceptual development plans are included in Attachment 5.

The proposed site layout consists of seven duplexes and one triplex, all of which contain two-storey units. Two convertible units are proposed, and would be located on either side of the common outdoor amenity area. Garbage and recycling is located in two enclosures in the front yard of the site, allowing for easy access by service vehicles.

Each unit has a private outdoor space at grade off of the living room, and a second storey balcony off of the master bedroom.

Amenity Space

The applicant is proposing a cash contribution in-lieu of providing indoor amenity space on-site, at a rate of $1,769 per unit as per the OCP. The total contribution for this 17-unit townhouse development would be $30,073.

A common outdoor amenity space is proposed on the south side of the drive aisle in the middle of the site. This location is centrally located, will enjoy good solar exposure, and provides an additional connection between the drive aisle and the proposed pedestrian pathway. The proposed design includes landscaping, a concrete pathway for universal access, picnic table, sandbox, playhouse, and stepping logs in order to provide for a variety of social and play opportunities for children and adults.

Further refinement of the site plan, architectural character, outdoor amenity space, and convertible unit features of the proposed development will occur through the Development Permit process.

Transportation and Site Access

Vehicle access to the subject site is provided from Ash Street. A small vehicle turnaround is provided after the first triplex, which would allow passenger vehicles and small vans to access the site, perform a three-point turn, and exit the site in a forward motion.

Each unit has a garage with two parking spaces in a side-by-side arrangement, which exceeds the minimum 1.4 spaces per unit required by Richmond Zoning Bylaw 8500 for multi-family developments in the City Centre planning area. Four visitor parking spaces are proposed on site, including one van accessible parking space. Level 2 EV charging is provided in each garage as per Zoning Bylaw requirements. Staff support the proposed number of resident and visitor parking spaces, as it reduces the demand for street parking.

Pedestrian access is provided from multiple locations. The two end units fronting Ash Street have direct access to the sidewalk. Units on the north side of the site have access from the drive aisle, and units on the south side of the site have access from both the drive aisle and a proposed pedestrian walkway.

The pedestrian pathway would connect Ash Street to the intersection of Sills Avenue and Armstrong Street, improving pedestrian circulation through the neighbourhood in a manner generally consistent with the McLennan South Sub-Area Plan. This pathway would be expanded through the future redevelopment of 7140 and 7160 Ash Street. In its interim condition, the proposed pathway would consist of a 2.0 m wide pathway, 0.5 m wide landscaped strip, and pedestrian-scale lighting. The ultimate condition would consist of a 2.5 m wide pathway, 0.75 m wide landscaped strip on both sides, and pedestrian-scale lighting on the north side only. Construction of the pathway would be through the Servicing Agreement.

Prior to final adoption of the rezoning bylaw, the applicant must provide a 2.5 m wide Statutory Right-of-Way (SRW) for Public Rights-of-Passage (PROP) for the proposed pedestrian pathway.

An approximately 8.1 m by 8.1 m road dedication is required at the southeast corner of the subject site for the intersection of Sills Avenue and Armstrong Street. Design and construction of the road works will be through a Servicing Agreement.

Tree Retention and Replacement

The applicant has submitted a Certified Arborist's Report; which identifies on-site and off-site tree species, assesses tree structure and condition, and provides recommendations on tree retention and removal relative to the proposed development. The Report assesses 23 bylaw-sized trees on the subject property and 13 trees on neighbouring properties.

The City's Tree Preservation Coordinator has reviewed the Arborist's Report and supports the Arborist's findings, with the following comments:

- Four Douglas Fir trees (Tag # 364, 365, 366, 367) located on the development site are in good condition and should be retained and protected if possible.
- Two Silver Maple trees (Tag # 358, 369) located on the development site are in poor condition and should be removed and replaced.
- Five Yellow Cedar trees (Tag # 359, 360, 361, 362, 363) located on the development site are in poor condition and should be removed and replaced.
- Ten Birch trees (Tag # 368, 370, 371, 373, 374, 375, 376, 377, 378, 379) located on the development site are in poor condition and should be removed and replaced.
- One Black Pine tree (Tag # 372) located on the development site is in poor condition and should be removed and replaced.
- One Black Pine tree (Tag # 380) is located within the future Armstrong Road right-of-way (ROW) and is in conflict with the required road works. Replacement is not required for trees within a required roadway.
- 13 trees (Tag # OS1-OS13) located on adjacent neighbouring properties are identified to be retained. Provide tree protection as per City of Richmond Tree Protection Information Bulletin Tree-03.
- Replacement trees should be specified at a 2:1 ratio as per the OCP.

Tree Replacement

The applicant wishes to remove 18 on-site trees (Tag # 358-363 and 368-379) and one tree within the future roadway (Tag # 380). The 2:1 replacement ratio would require a total of 36 replacement trees. The required replacement trees are to be of the following minimum sizes, based on the size of the trees being removed as per Tree Protection Bylaw No. 8057.

No. of Replacement Trees	Minimum Caliper of Deciduous Replacement Tree	Minimum Height of Coniferous Replacement Tree
8	11 cm	6 m
4	10 cm	5.5 m
10	8 cm	4 m
14	6 cm	3.5 m

To satisfy the 2:1 replacement ratio established in the OCP, the applicant proposes to plant a minimum of 36 replacement trees in the development. If required replacement trees cannot be accommodated on-site, a cash-in-lieu contribution in the amount of $750/tree to the City's Tree Compensation Fund for off-site planting is required.

Tree Protection

Four trees (Tag # 364-367) on the subject site and 13 trees (Tag # OS1-OS13) on neighbouring properties are to be retained and protected. The applicant has submitted a tree protection plan showing the trees to be retained and the measures taken to protect them during development

stage (Attachment 6). To ensure that the trees identified for retention are protected at development stage, the applicant is required to complete the following items:

- Prior to final adoption of the rezoning bylaw, submission to the City of a contract with a Certified Arborist for the supervision of all works conducted within or in close proximity to tree protection zones. The contract must include the scope of work required, the number of proposed monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the arborist to submit a post-construction impact assessment to the City for review.
- Prior to final adoption of the rezoning bylaw, submission to the City of a $40,000 survival security for the four on-site trees to be retained.
- Prior to demolition of the existing dwellings on the subject site, installation of tree protection fencing around all trees to be retained. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and remain in place until construction and landscaping on-site is completed.

Variance Requested

The proposed development is generally in compliance with the "Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)" zone in Richmond Zoning Bylaw 8500, with the exception of the following variances:

1. Reduce the minimum front yard setback from 6.0 m to 1.21 m for accessory structures only.

   Staff are supportive of the proposal as it allows the garbage and recycling enclosures to be located near the entrance to the site, reducing the distance required for collection vehicles to back up out of the site. The proposal is consistent with existing development to the north of the site. Review of the design and screening of the proposed enclosures would be through the Development Permit process.

2. Reduce the minimum setback to Sills Avenue/Armstrong Street from 6.0 m to 2.53 m.

   The reduced setback results from road dedication for the intersection of Sills Avenue and Armstrong Street. Staff are supportive of the proposal as it is a similar setback to what is proposed between the building and the edge of the pedestrian pathway. The building would be approximately 4.03 m from the edge of the sidewalk, which is greater than the 3.6 m setback from the building to the pedestrian pathway.

   Additionally, the proposed setback is similar to with what would be achieved by the future redevelopment of the properties to the south. Single-family lots with a north-south orientation would result in the corner lot having a 3.0 m exterior side yard setback to Armstrong Street.

BC Energy Step Code

On July 26, 2018, Council adopted BC Energy Step Code requirements for all new residential developments. The proposed development consists of townhouses that staff anticipates would be designed and built in accordance with Part 9 of the BC Building Code. As such, this development would be expected to achieve Step 3 of the BC Energy Step Code for Part 9 construction (Climate Zone 4).

Site Servicing and Frontage Improvements

The applicant is required to enter into a Servicing Agreement at Building Permit stage for the design and construction of the required site servicing and frontage works, as described in Attachment 7.

Frontage improvements on Ash Street include, but may not be limited to, the following:

- Road widening to a total width of 8.5 m.
- 1.75 m wide sidewalk at the property line, approx. 2.5 m wide landscaped boulevard, and 0.15 m curb and gutter.
- Removal of the two existing driveways and replacement with a new driveway crossing and frontage works as described above,
- Relocation or undergrounding of the hydro utility pole located within the proposed new driveway.

Frontage improvements on Sills Avenue include, but may not be limited to, the following:

- 1.5 m wide sidewalk, landscaped boulevard, and 0.15 m curb and gutter along the curved alignment of the intersection of Sills Avenue and Armstrong Street.

The on-site pedestrian pathway connecting Ash Street to Sills Avenue will include, but may not be limited to:

- 0.5 m landscaped strip with lighting; and
- 2.0 m wide paved surface, or other treatment as deemed appropriate in order to retain off-site trees.

Development Permit Application

A Development Permit application is required to be processed to a satisfactory level prior to final adoption of the rezoning bylaw. Further refinements to architectural, landscape, and urban design will be completed as part of the Development Permit application review process, including, but not limited to, the following:

- Compliance with the Development Permit Guidelines for multi-family developments in the OCP and McLennan South Sub-Area Plan.

- Review of the size and species of on-site trees to ensure bylaw compliance and to achieve a mix of coniferous and deciduous species.

- Review and refinement of tree retention measures to ensure the survival of the four Douglas Fir trees located on the north side of the site, as well as the trees located on the neighbouring property to the south in close proximity to the proposed pedestrian pathway.

- Design of the common outdoor amenity space, including choice of play equipment and other features to ensure a safe and vibrant environment for children's play and social interaction.

- Design of the proposed waste enclosures within the front yard setback, to ensure an attractive streetscape and adequate screening.

- Review of accessibility features, including aging-in-place features in all units, and the provision of two convertible units.

- Review of a sustainability strategy for the development proposal, including steps to achieve Step 3 of the Energy Step Code for Part 9 construction (Climate Zone 4).

Financial Impact

The rezoning application results in an insignificant Operational Budget Impact (OBI) for off-site City infrastructure (such as roadworks, waterworks, storm sewers, sanitary sewers, street lights, street trees and traffic signals).

Conclusion

The purpose of this application is to rezone 7100 and 7120 Ash Street from the "Single Detached (RS1/F)" zone, to the "Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)" zone, in order to develop 17 two-storey townhouse units with vehicle access from Ash Street.

The rezoning application complies with the land use designations and applicable policies contained within the OCP for the subject site. Further review of the project design will be completed as part of the Development Permit application review process.

The list of rezoning considerations is included in Attachment 7, which has been agreed to by the applicant (signed concurrence on file).

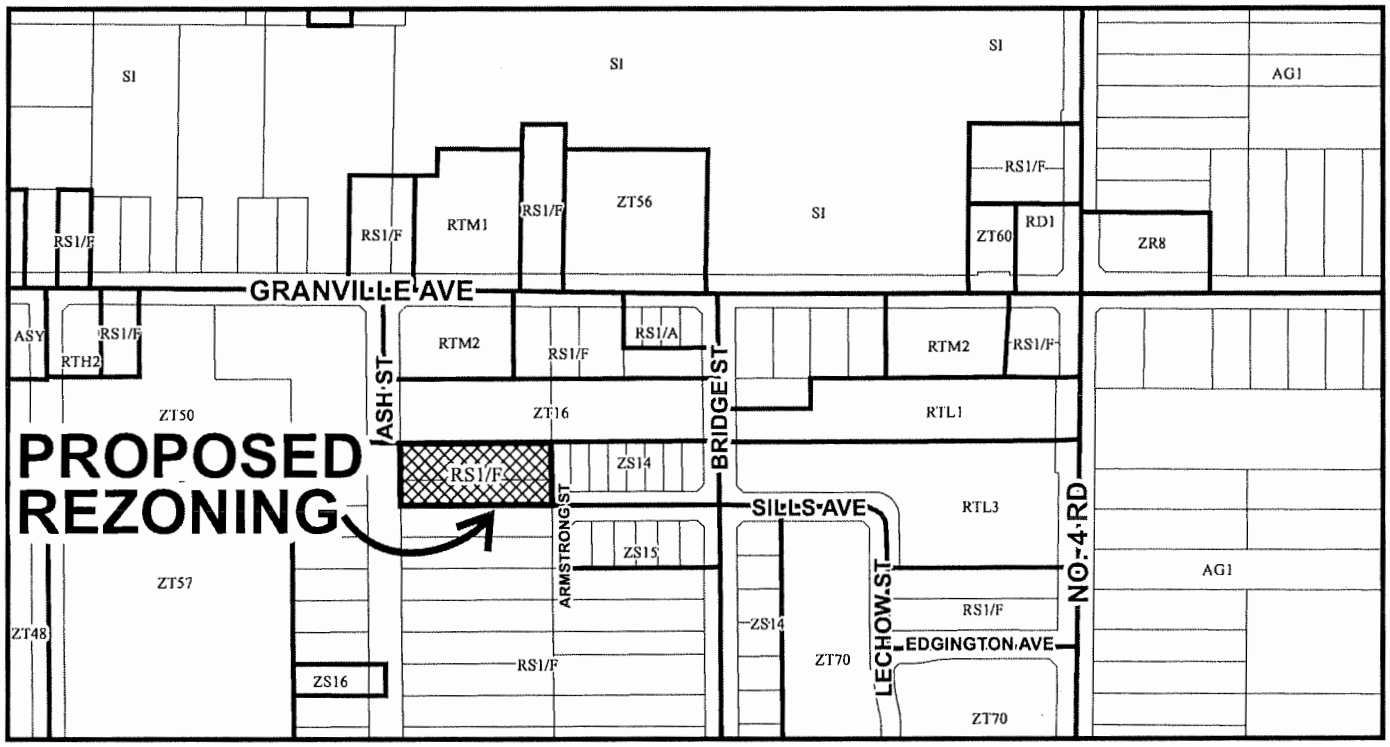
It is recommended that Richmond Zoning Bylaw 8500, Amendment Bylaw 10163 be introduced and given first reading.

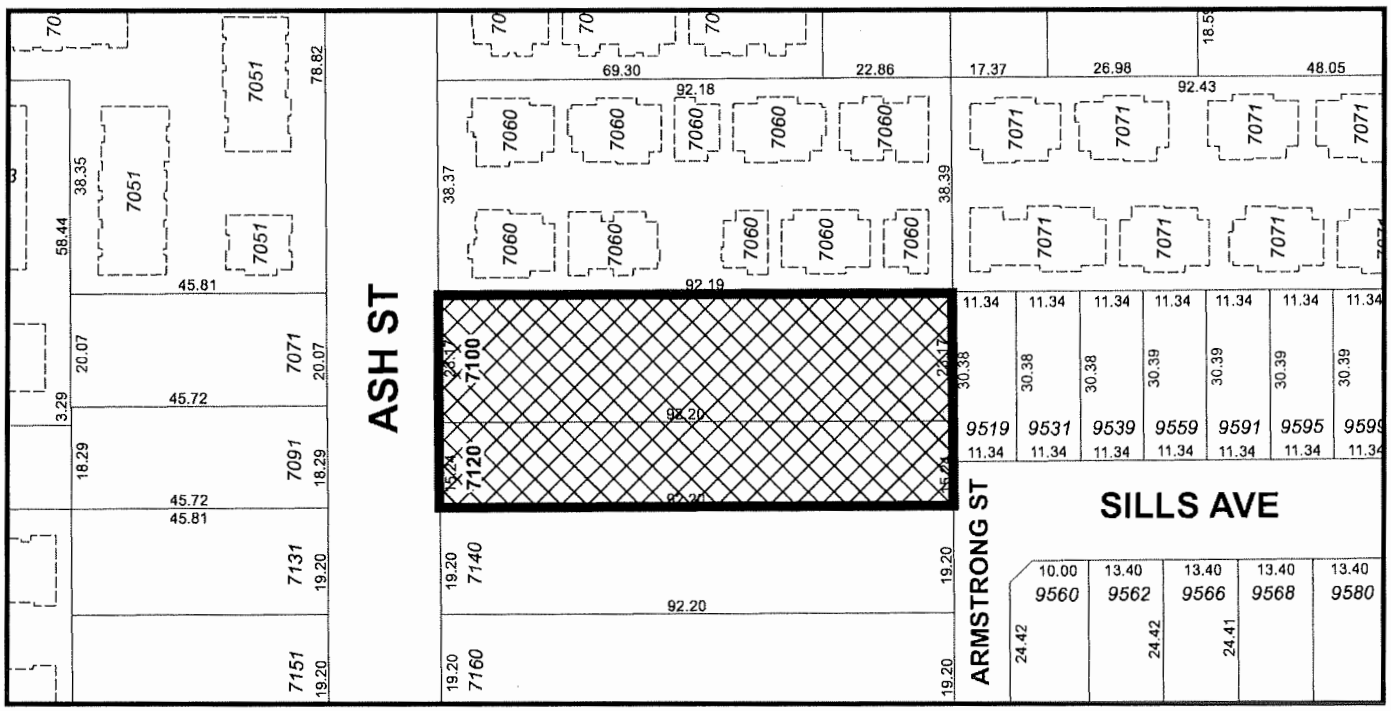
Jordan Rockerbie
Planner 1
(604-276-4092)

JR:blg

Attachment 1: Location Map and Aerial Photo
Attachment 2: Development Application Data Sheet
Attachment 3: McLennan South Sub-Area Plan Land Use Map
Attachment 4: McLennan South Sub-Area Plan Circulation Map
Attachment 5: Conceptual Development Plans
Attachment 6: Tree Retention Plan
Attachment 7: Rezoning Considerations

City of Richmond





RZ 18-843479

Original Date: 01/09/19

Revision Date:

Note: Dimensions are in METRES

City of
Richmond

RZ 18-843479

Original Date: 01/09/19

Revision Date:

Note: Dimensions are in METRES

City of Richmond

Development Application Data Sheet

Development Applications Department

RZ 18-843479 **Attachment 2**

Address: 7100 & 7120 Ash Street

Applicant: Fougere Architecture Inc.

Planning Area(s): City Centre – McLennan South

	Existing	Proposed
Owner:	1199445 BC LTD	To be determined
Site Size (m^2):	3,535.3 m^2	3,469.7 m^2
Land Uses:	Single-family	Townhouses
OCP Designation:	Neighbourhood Residential	No change
Area Plan Designation:	Residential, 2 ½ Storeys	No change
Zoning:	Single Detached (RS1/F)	Town Housing (ZT16) - South McLennan and St. Albans Sub-Area (City Centre)
Number of Units:	Two single-family dwellings	17 townhouse dwellings

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.60	Max. 0.60	none permitted
Buildable Floor Area (m^2):*	Max. 2,081.8 m^2 (22,408 ft^2)	2,081.7 m^2 (22,407 ft^2)	none permitted
Lot Coverage (% of lot area):	Building: Max. 40%	Building: Max. 40%	none
Lot Size:	No minimum	3,469.7 m^2	none
Lot Dimensions (m):	Width: No minimum Depth: No minimum	Width: 38.38 m Depth: 92.13 m	none
Setbacks (m):	Front: Min. 6.0 m Rear: Min. 3.0 m Side: Min. 3.0 m To Sills Ave: Min. 6.0 m	Front: 6.0 m for buildings, 1.21 m for accessory structures Rear: 3.02 m Side: 3.6 m To Sills Ave: 2.53 m	**Front: vary by 4.79 m for accessory structures only** **To Sills Ave: vary by 3.47 m**
Height (m):	Max. 11.0 m	Max. 11.0 m	none
Off-street Parking Spaces – Regular (R) / Visitor (V):	1.4 (R) and 0.2 (V) per unit	2 (R) and 0.24 (V) per unit	none

6426161

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Off-street Parking Spaces – Total:	24 (R) and 4 (V)	34 (R) and 4 (V)	none
Tandem Parking Spaces:	Permitted – Maximum of 50% of required spaces	0%	none
Small Car Parking Spaces:	Permitted – Maximum of 50% of required spaces	45% (17 spaces)	none
Accessible Parking Spaces:	1	1	none
Amenity Space – Indoor:	50 m^2 or cash-in-lieu	Cash-in-lieu	none
Amenity Space – Outdoor:	6 m^2 per unit (i.e. 102 m^2)	102 m^2	none

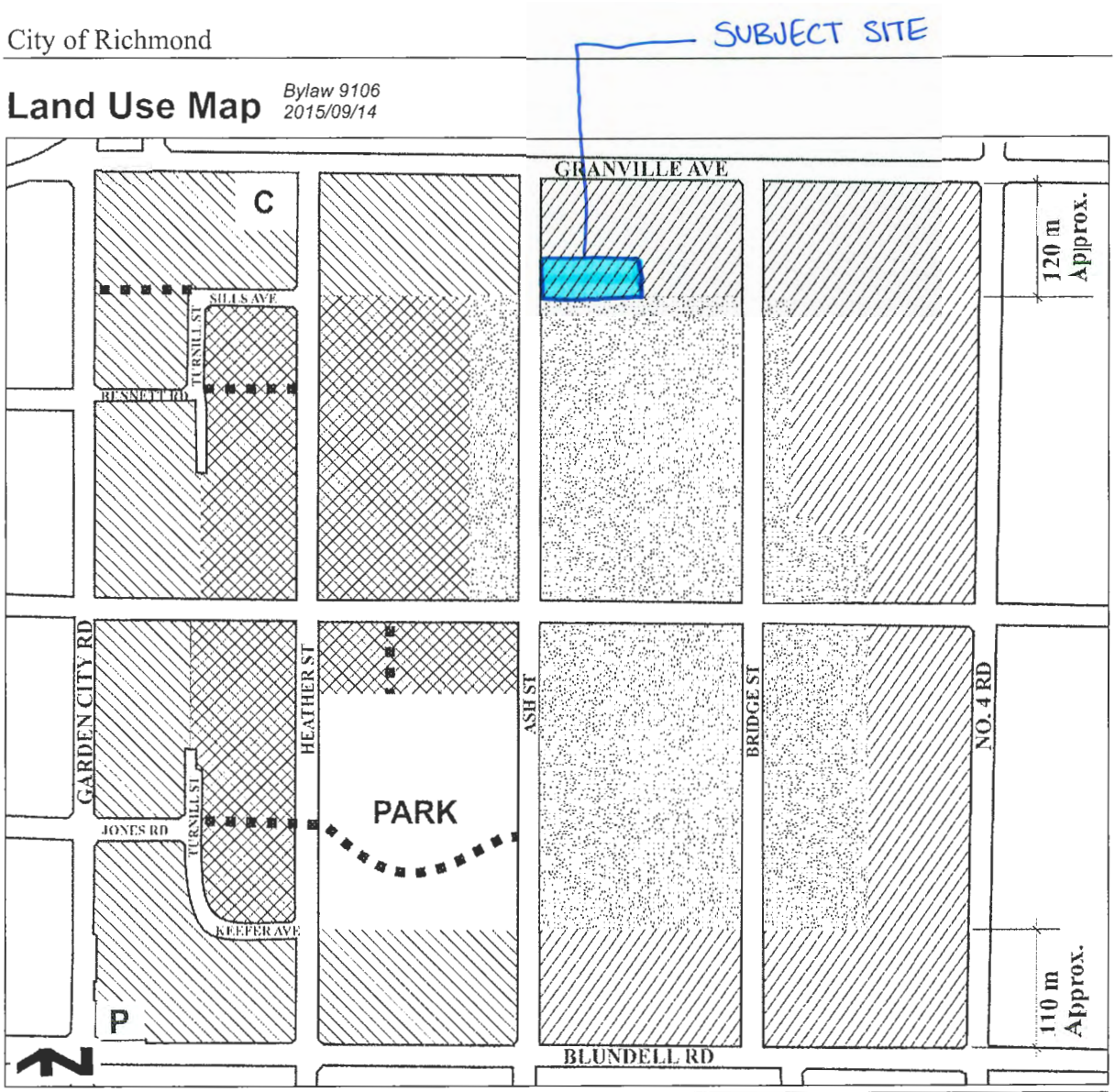
Other: Tree replacement compensation required for loss of significant trees.

* Preliminary estimate; not inclusive of garage; exact building size to be determined through zoning bylaw compliance review at Building Permit stage.

City of Richmond

Land Use Map

Bylaw 9106
2015/09/14



Residential, Townhouse up to **3 storeys** over 1 parking level, Triplex, Duplex, Single-Family **0.75 base F.A.R.**

Residential, 2 ½ storeys typical (3 storeys maximum) Townhouse, Triplex, Duplex, Single-Family **0.60 base F.A.R.**

Residential, 2 ½ storeys typical (3 storeys maximum), predominantly Triplex, Duplex, Single-Family **0.55 base F.A.R.**

Residential, Historic Single-Family, **2 ½ storeys** maximum **0.55 base F.A.R.**, Lot size along Bridge and Ash Streets:

- Large-sized lots (e.g. 18 m/59 ft. min. frontage and 550 m^2/ 5,920 ft^2 min. area)

Elsewhere:

- Medium-sized lots (e.g. 11.3 m/ 37 ft. min. frontage and 320 m^2/ 3,444 ft^2 min. area), with access from new roads and General Currie Road;

Provided that the corner lot shall be considered to front the shorter of its two boundaries regardless of the orientation of the dwelling.

Trail/Walkway

C Church

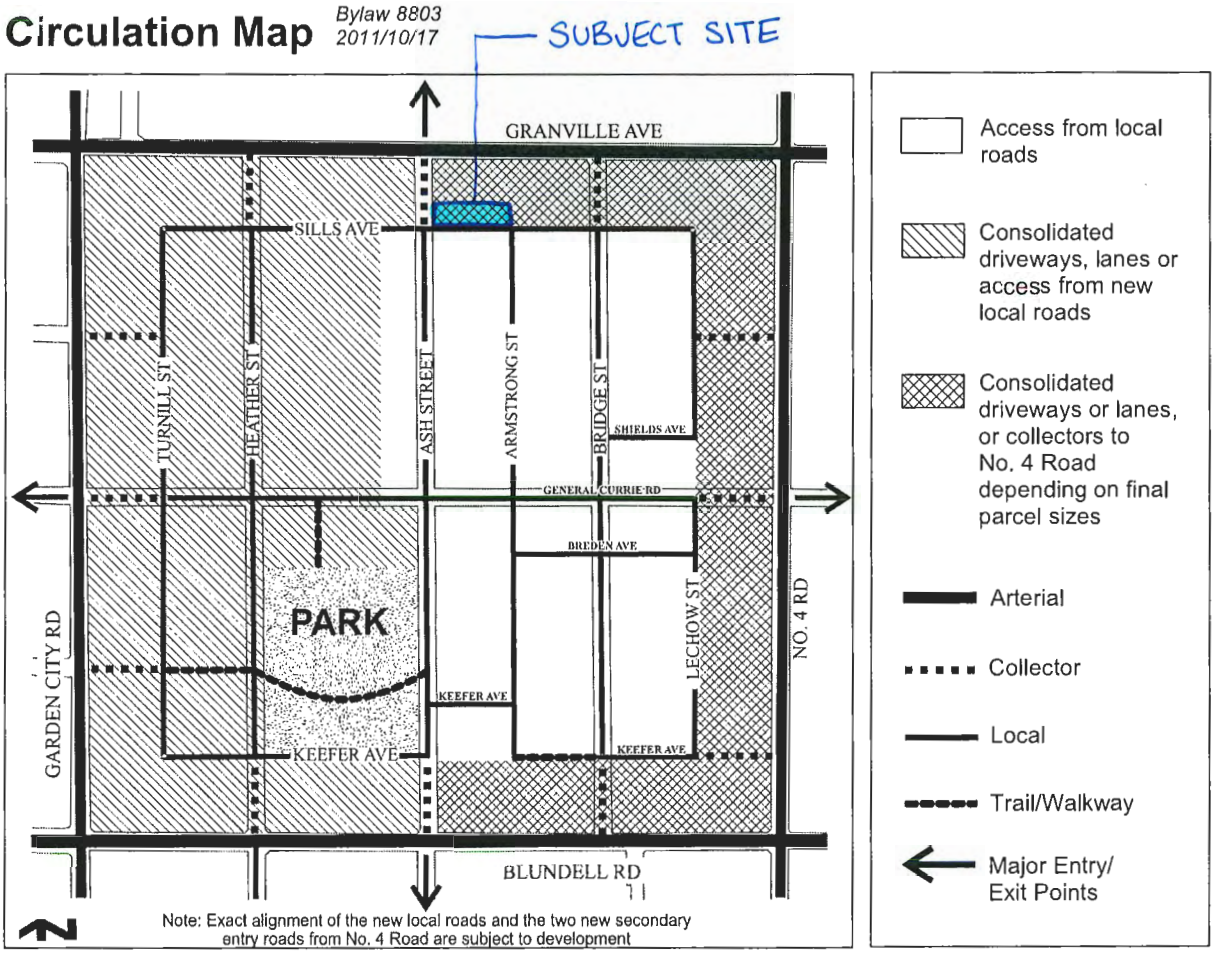
P Neighbourhood Pub

Note: Sills Avenue, Le Chow Street, Keefer Avenue, and Turnill Street are commonly referred to as the "ring road".

l) Encourage cycling as a means of travel by calming automobile traffic within McLennan South and supporting the City Centre policies and programs for bicycles.

Circulation Map

Bylaw 8803
2011/10/17



ATTACHMENT 5

FEBRUARY 26, 2020

7100 ASH STREET

FOUGERE
architecture inc.
BRITISH COLUMBIA • ALBERTA • WASHINGTON
202 – 2425 Quebec Street 604.873.2907
Vancouver, BC V5T 4L6 fougerearch@telus.net

PH - 200

7100 ASH STREET

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REGISTERED ARCHITECT
WEI-LI FOUGERE
February 26, 2020
BRITISH COLUMBIA

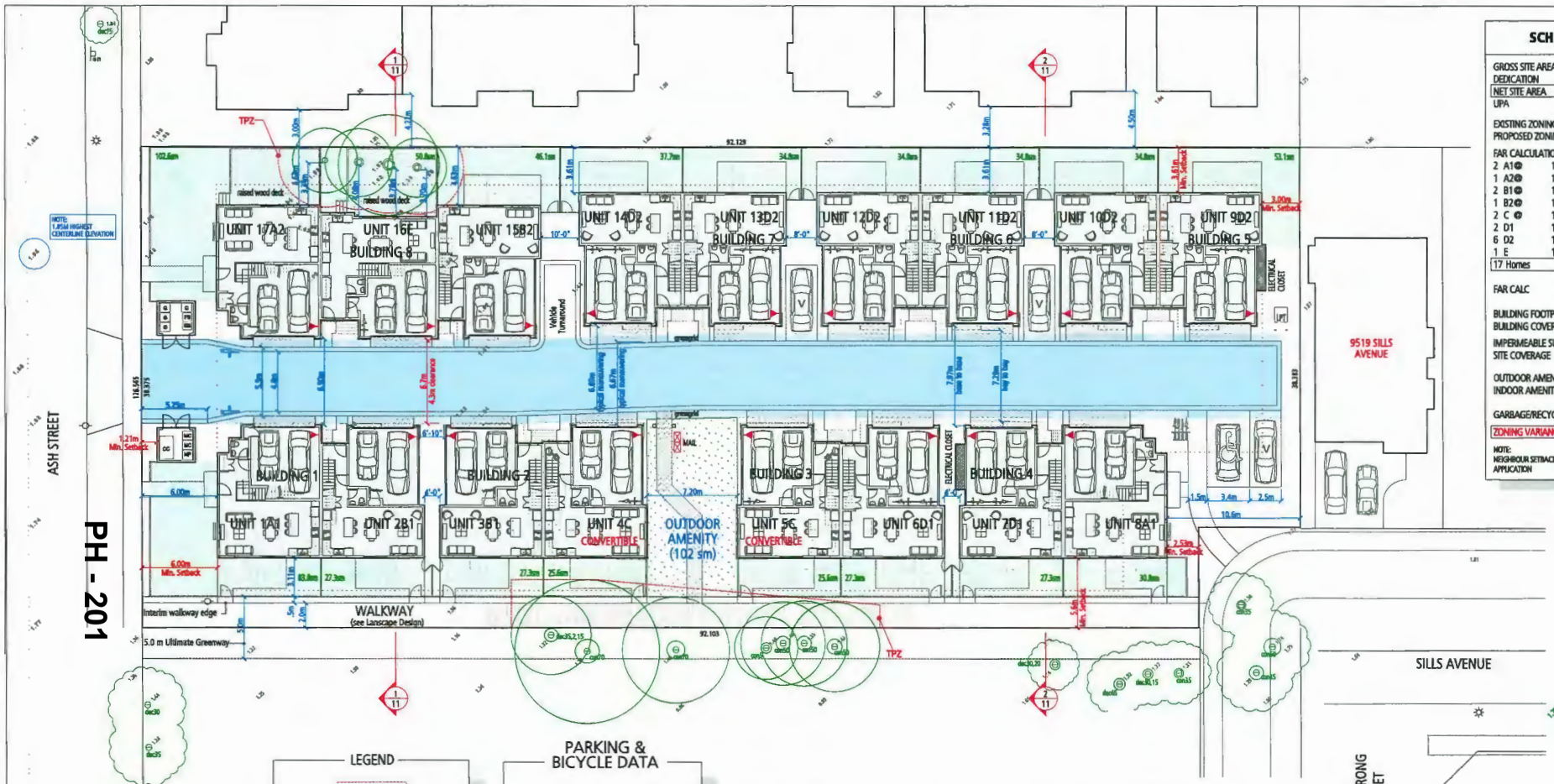
CONTEXT PLAN

SCALE: 1:300

0 10' 30' 60'

FEBRUARY 26, 2020

FOUGERE
architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
202 – 2425 Quebec Street 604.873.2907
Vancouver, BC V5T 4L6 fougerearchitecture.ca



SCHEMATIC SITE DATA

GROSS SITE AREA		38,054 sf (0.874 Acres)
DEDICATION		706 sf (0.017 Acres)
NET SITE AREA		37,348 sf (0.857 Acres)
UPA		19.8
EXISTING ZONING		RS1/F
PROPOSED ZONING		ZT16
FAR CALCULATION		
2 A1	1,340 sf	2,680 sf
1 A2	1,340 sf	1,340 sf
2 B1	1,294 sf	2,588 sf
1 B2	1,294 sf	1,294 sf
2 C	1,345 sf	2,690 sf
2 D1	1,305 sf	2,610 sf
6 D2	1,305 sf	7,830 sf
1 E	1,375 sf	1,375 sf
17 Homes		22,407 sf
FAR CALC	22,407/37,348	0.60 FAR
BUILDING FOOTPRINT		15,111 sf
BUILDING COVERAGE		40 %
IMPERMEABLE SUFACES		22,237 sf
SITE COVERAGE		60 %
OUTDOOR AMENITY		102 sm (6sm x 17 homes)
INDOOR AMENITY		CASH IN LIEU
GARBAGE/RECYCLING		CENTRALIZED
ZONING VARIANCE		2.53m Setback - Sills Avenue

NOTE:
NEIGHBOUR SETBACKS FROM RIM MAP & REZONING APPLICATION

7100 ASH STREET

LEGEND

- STANDARD CAR STALL SIZE: 2.5m² x 5.5m
- SMALL CAR STALL SIZE: 2.2m² x 4.6m
- ACCESSIBLE CAR STALL SIZE: 1.5m ISLE + 3.4m x 5.5m
  * +0.3m IF BESIDE WALL
- CLASS 1 BIKE STALL SIZE: 1.8m x 0.6m
- CLASS 1 BIKE STALL (Vertical) SIZE: 1.0m x 0.6m
- LEVEL 2 Electric Vehicle Plug-ins one for each parking space
- 6.7m swept path with 4.3m vertical clearance 5.5m paved drive aisle & 0.35m roll-over curbs
- PRIVATE OPEN SPACE

PARKING & BICYCLE DATA

Residential 2 cars/unit	34
Visitor 0.2 cars/unit	4
Total Required	38
Parking Provided	
Standard Stall	17
Small Stall	17
Visitor Provided	4
Total Provided	38

On-site Bicycle Parking Requirements	
Class 1: 1.25 stalls/unit	22
max. 33% Vertical Stalls	7
Class 2: 0.2 stall/unit	4
Total Class 1 Stalls Required	22
Total Class 1 Stalls Provided	25
Total Class 2 Stalls Required	4
Total Class 2 Stalls Provided	4
Horizontal Stalls Provided	19
Vertical Stalls Provided	6

CODE ANALYSIS - PART 9

Project: #1812 Ash

BCBC REF. Division A 1.3.3.3.(1)(a)	CODE INFORMATION
Applicable code	British Columbia Building Code 2018
Major occupancy	Group C
Building area (Permitted 600 m²)	(Proposed) 254.2 m²
Building height	2 Storeys
Streets faced	1 Street
Floor FRR*	N/A
Roof FRR	N/A
Load bearing wall FRR	N/A
Sprinkler system required	N/A
Construction type (Permitted: combustible/noncombustible)	(Proposed) Combustible
Fire alarm required	N/A
Standpipe required	N/A
Seperation of residential suites	1 hr
Fire separation between dwelling and garage	N/A
Gas air barrier system required between unit and garage	Yes

* Fire Resistance Rating

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REGISTERED ARCHITECT
BRIAN A. FOUGERE
February 26, 2020
BRITISH COLUMBIA

SITE PLAN

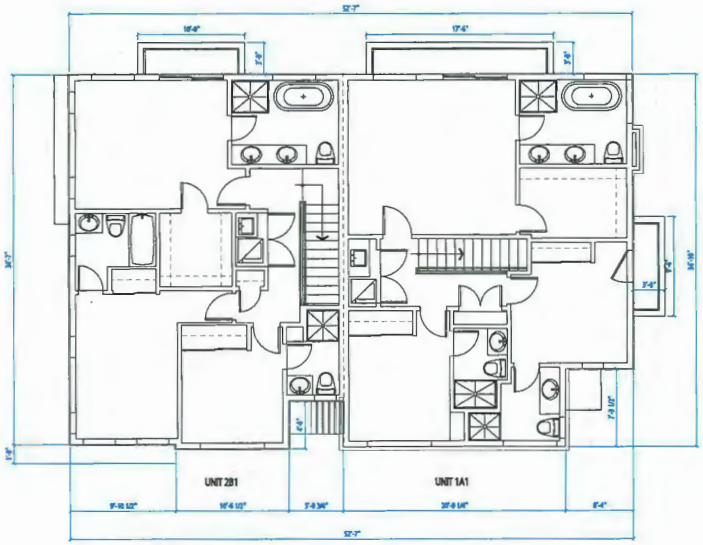
SCALE: 1:150

0 5' 15' 30'

FEBRUARY 26, 2020

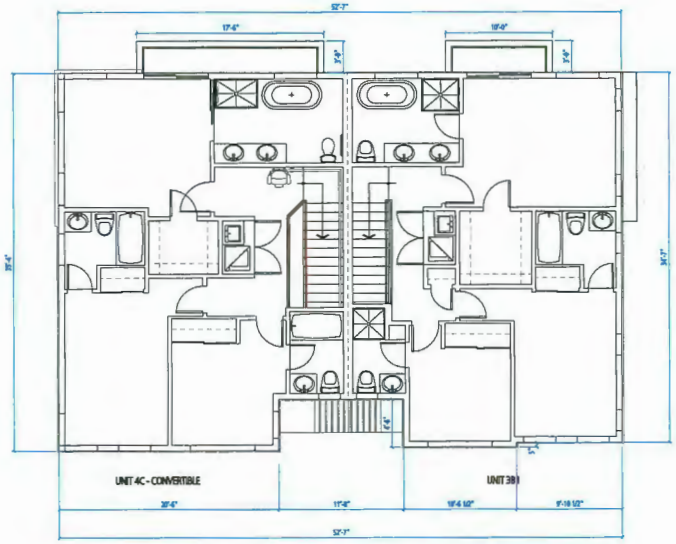
FOUGERE architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
202 – 2425 Quebec Street Vancouver, BC V5T 4L6
604.873.2907 fougerearch@telus.net

BUILDING 1



SECOND FLOOR

BUILDING 2



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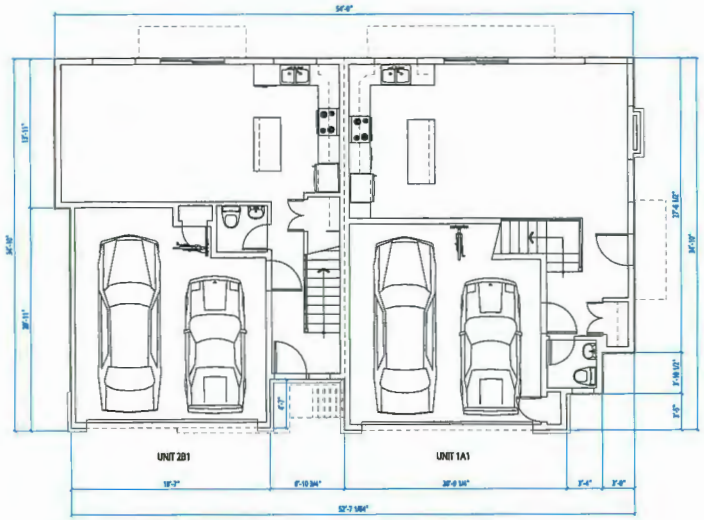
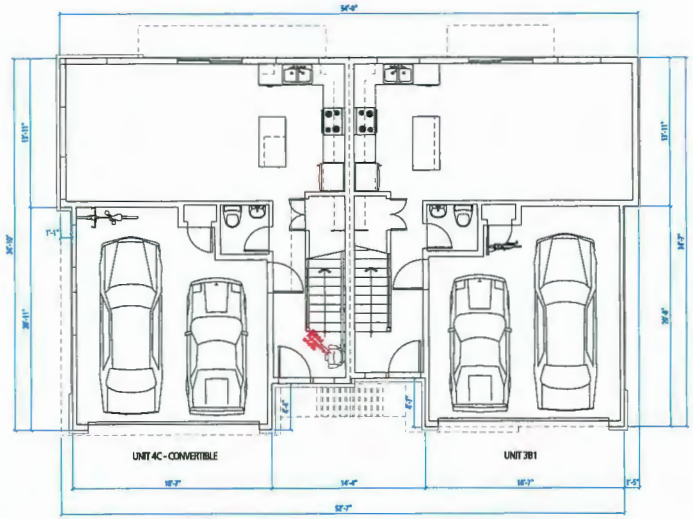


GROUND FLOOR



GROUND FLOOR

7100 ASH STREET

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REGISTERED ARCHITECT
WAYNE A. FOUGERE
BRITISH COLUMBIA

BLOCK PLANS

BUILDINGS 1 & 2

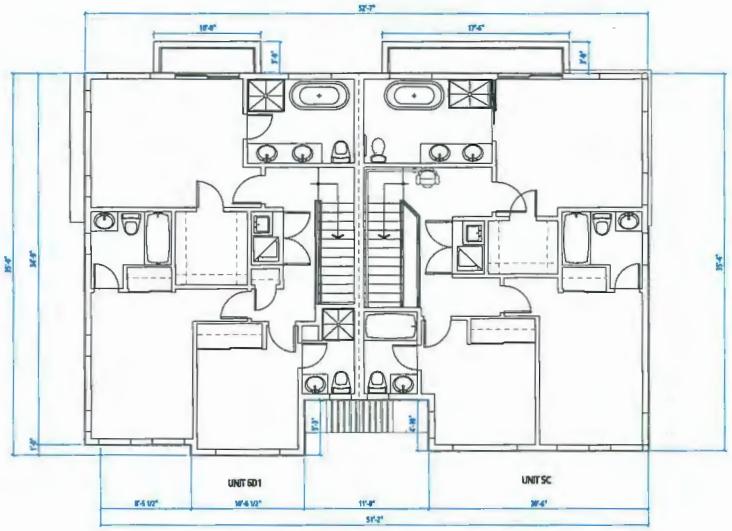
SCALE: 3/16" = 1'-0"

0 5' 10' 20'

FEBRUARY 11, 2020

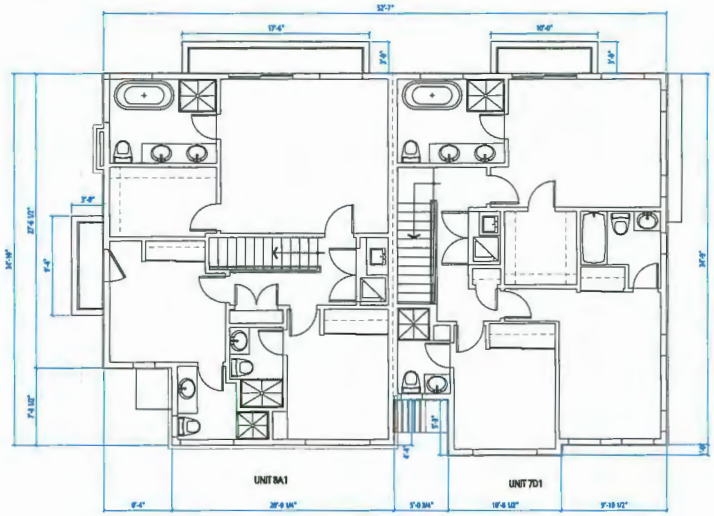
FOUGERE
architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
202 – 2425 Quebec Street Vancouver, BC V5T 4L6
604.873.2997
fougerearch@telus.net

BUILDING 3



SECOND FLOOR

BUILDING 4



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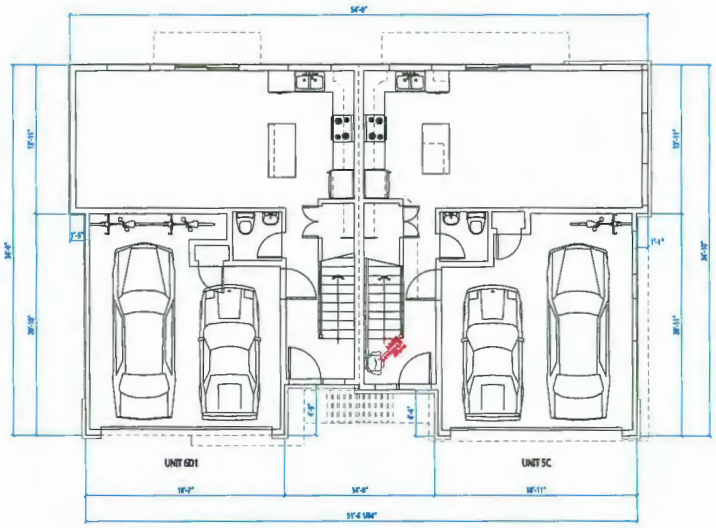

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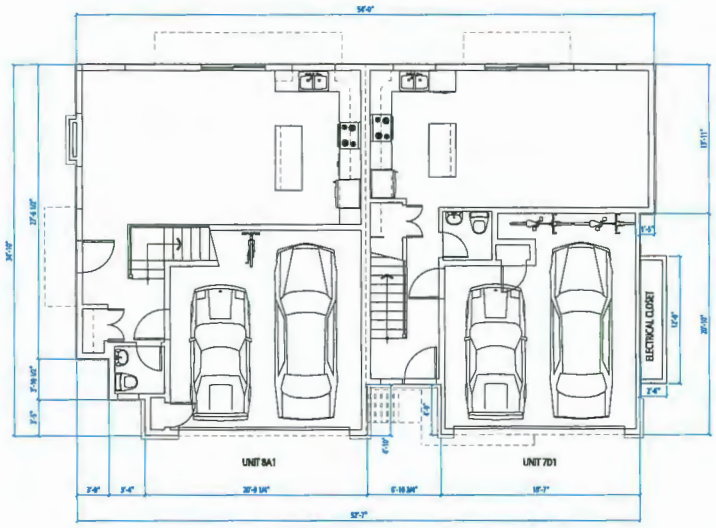

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REGISTERED ARCHITECT
BRITISH COLUMBIA

BLOCK PLANS

BUILDINGS 3 & 4

SCALE: 3/16" = 1'-0"

0 5' 10' 20'

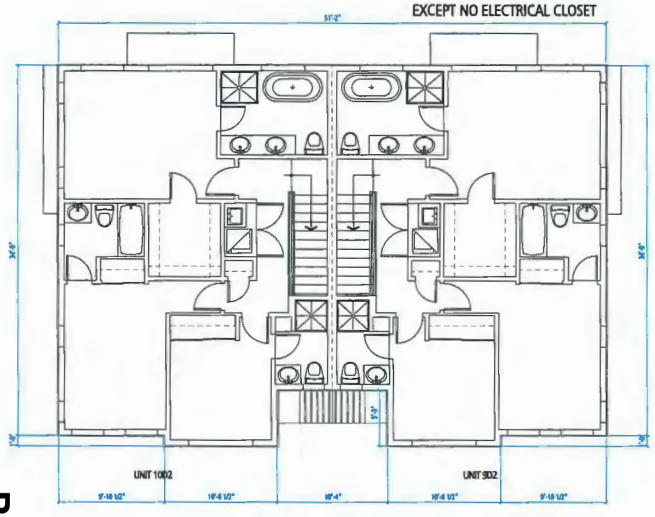
FEBRUARY 11, 2020

FOUGERE
architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
202 – 2425 Quebec Street Vancouver, BC V5T 4L6
604.873.2907
fougerearchitecture.ca

6

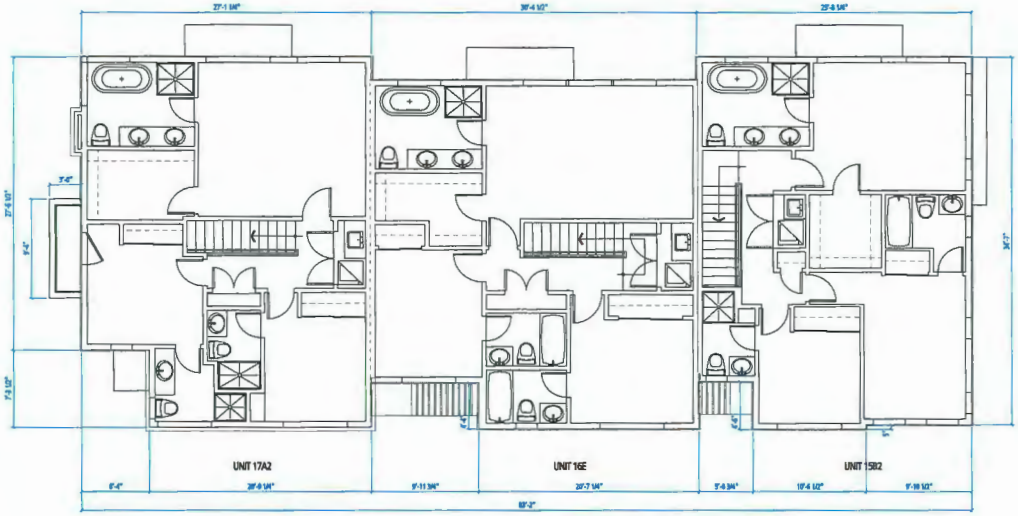
BUILDING 5

BUILDING 6 & 7 SIMILAR
EXCEPT NO ELECTRICAL CLOSET



SECOND FLOOR

BUILDING 8



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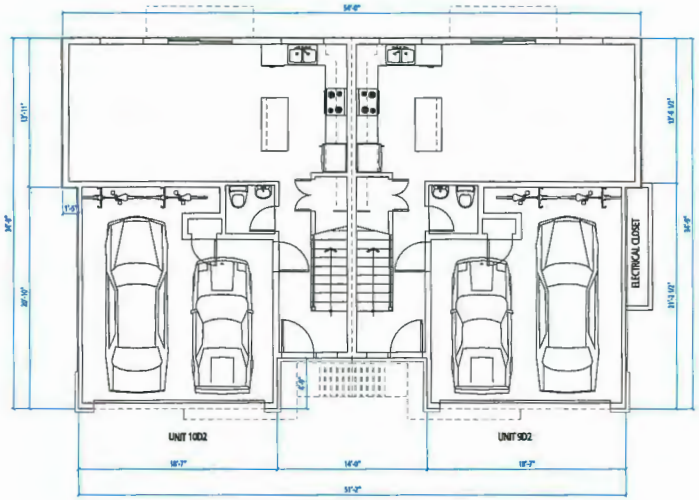

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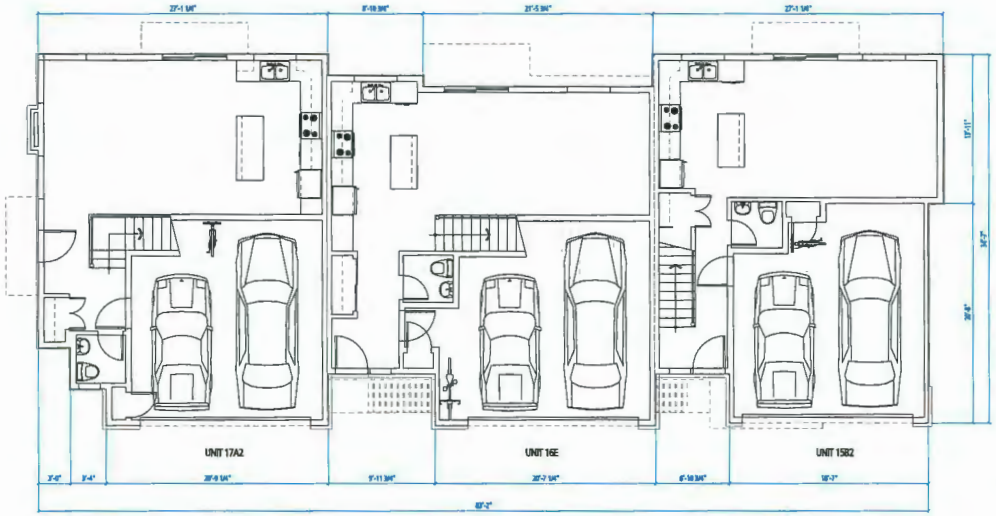

GROUND FLOOR

PH - 204

7100 ASH STREET

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REGISTERED ARCHITECT
BRIAN A. FOUGERE
February 24, 2020
BRITISH COLUMBIA

BLOCK PLANS

BUILDINGS 5 - 8

SCALE: 3/16" = 1'-0"

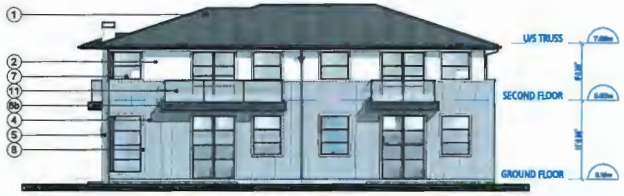
0 5' 10' 20'

FEBRUARY 11, 2020

FOUGERE
architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
202 – 2425 Quebec Street 604.879.2907
Vancouver, BC V5T 4L6 fougerearchitecture.ca

NORTH ELEVATION UNIT B1 UNIT A1

NORTH ELEVATION UNIT C UNIT B1

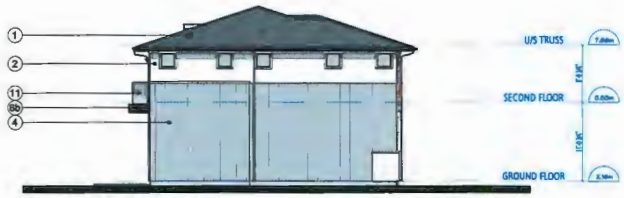


SOUTH ELEVATION UNIT A1 UNIT B1

SOUTH ELEVATION UNIT B1 UNIT C

WEST ELEVATION UNIT A1

WEST ELEVATION UNIT B1



EAST ELEVATION UNIT B1

BUILDING 1

EAST ELEVATION UNIT C

BUILDING 2

ACCENT (DOOR) COLOURS

a	Sherwin Williams- SW 6701 Thermal Springs
b	Sherwin Williams- SW 6753 Jargon Jade
c	Sherwin Williams- SW 6790 Ashwin Sea
d	Sherwin Williams- SW 6802 Danube

EXTERIOR FINISHES SCHEDULE

1	ASPHALT SHINGLES	IKO- Weathered Wood
2	EIFS	SHERWIN WILLIAMS- SW 7005 Toque White
3	HARDIE PLANK LAP SIDING	FIBRIER COATING – Cedarform
4	BOARD AND BATTEN	SHERWIN WILLIAMS- SW 7015 Repose Gray
5	BRICK	MUTURAL MATERIAL-CASTLE GRAY
6a	TRIM	SHERWIN WILLIAMS- SW 7005 Toque White
6b	TRIM	SHERWIN WILLIAMS- SW 6991 Black Magic
7	VINYL WINDOWS	SHERWIN WILLIAMS- SW 6991 Black Magic
8	WINDOW & DOOR TRIMS	SHERWIN WILLIAMS- SW 7005 Toque White
9a-d	EXTERIOR ENTRY DOOR	varies (see accent colours)
10	GARAGE DOOR	SHERWIN WILLIAMS- SW 6991 Black Magic
11	GLASS RAILING	GLASS AND METAL FRAME
12	DOWN SPOUT	SHERWIN WILLIAMS- SW 6991 Black Magic

7100 ASH STREET

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ELEVATIONS

SCALE: 1/8" = 1'-0"

0 5' 10' 15'

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NORTH ELEVATION UNIT D1 UNIT C

SOUTH ELEVATION UNIT C UNIT D1

WEST ELEVATION UNIT C

EAST ELEVATION UNIT D1

BUILDING 3

SOUTH ELEVATION UNIT A1 UNIT D1

NORTH ELEVATION UNIT D1 UNIT A1

WEST ELEVATION UNIT A1

EAST ELEVATION UNIT D1

BUILDING 4

ACCENT (DOOR) COLOURS

a	Sherwin Williams- SW 6731 Thermal Springs
b	Sherwin Williams- SW 6753 Jargon Jade
c	Sherwin Williams- SW 6750 Adriatic Sea
d	Sherwin Williams- SW 9005 Dapples

EXTERIOR FINISHES SCHEDULE

1	ASPHALT SHINGLES	IKO- Weathered Wood
2	EIFS	SHERWIN WILLIAMS- SW 7003 Toque White
3	HARDIE PLANK LAP SIDING	FISHER COATING - Cedarlance
4	BOARD AND BATTEN	SHERWIN WILLIAMS- SW 7015 Repose Gray
5	BRICK	MUTURAL MATERIAL-CASTLE GRAY
6a	TRIM	SHERWIN WILLIAMS- SW 7003 Toque White
6b	TRIM	SHERWIN WILLIAMS- SW 6991 Black Magic
7	VINYL WINDOWS	SHERWIN WILLIAMS- SW 6991 Black Magic
8	WINDOW & DOOR TRIMS	SHERWIN WILLIAMS- SW 7003 Toque White
9a-d	EXTERIOR ENTRY DOOR	varies (see accent colours)
10	GARAGE DOOR	SHERWIN WILLIAMS- SW 6991 Black Magic
11	GLASS RAILING	GLASS AND METAL FRAME
12	DOWN SPOUT	SHERWIN WILLIAMS- SW 6991 Black Magic

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ELEVATIONS

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0 5' 10' 15'

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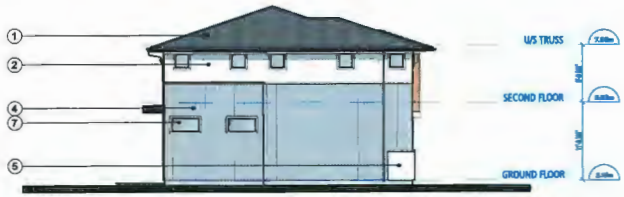
ACCENT DOOR COLOR	
BUILDING 5	9a & 9b
BUILDING 6	9d & 9c
BUILDING 7	9a & 9c

SOUTH ELEVATION — UNIT D2 — UNIT D2

NORTH ELEVATION — UNIT A2 — UNIT E — UNIT B2

NORTH ELEVATION — UNIT D2 — UNIT D2

SOUTH ELEVATION — UNIT B2 — UNIT E — UNIT A2



WEST ELEVATION — UNIT D2

WEST ELEVATION — UNIT A2

EAST ELEVATION — UNIT D2

BUILDINGS 5,6,7

EAST ELEVATION — UNIT B2

BUILDING 8

ACCENT (DOOR) COLOURS	
a	Sherwin Williams- SW 6701 Thermal Springs
b	Sherwin Williams- SW 6763 Jargon Jade
c	Sherwin Williams- SW 6790 Adriatic Sea
d	Sherwin Williams- SW 6828 Danube

EXTERIOR FINISHES SCHEDULE		
1	ASPHALT SHINGLES	SKD- Weathered Wood
2	EIFS	SHERWIN WILLIAMS- SW 7008 Toque White
3	HARDIE PLANK LAP SIDING	FINISH COATING - Cedarture
4	BOARD AND BATTEN	SHERWIN WILLIAMS- SW 7015 Repose Gray
5	BRICK	NUTURAL MATERIAL-CASTLE GRAY
6a	TRIM	SHERWIN WILLIAMS- SW 7008 Toque White
6b	TRIM	SHERWIN WILLIAMS- SW 6991 Black Magic
7	VINYL WINDOWS	SHERWIN WILLIAMS- SW 6991 Black Magic
8	WINDOW & DOOR TRIMS	SHERWIN WILLIAMS- SW 7008 Toque White
9a-d	EXTERIOR ENTRY DOOR	varies (see accent colours)
10	GARAGE DOOR	SHERWIN WILLIAMS- SW 6991 Black Magic
11	GLASS RAILING	GLASS AND METAL FRAME
12	DOWN SPOUT	SHERWIN WILLIAMS- SW 6991 Black Magic

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ELEVATIONS

SCALE: 1/8" = 1'-0"

0 5' 10' 15'

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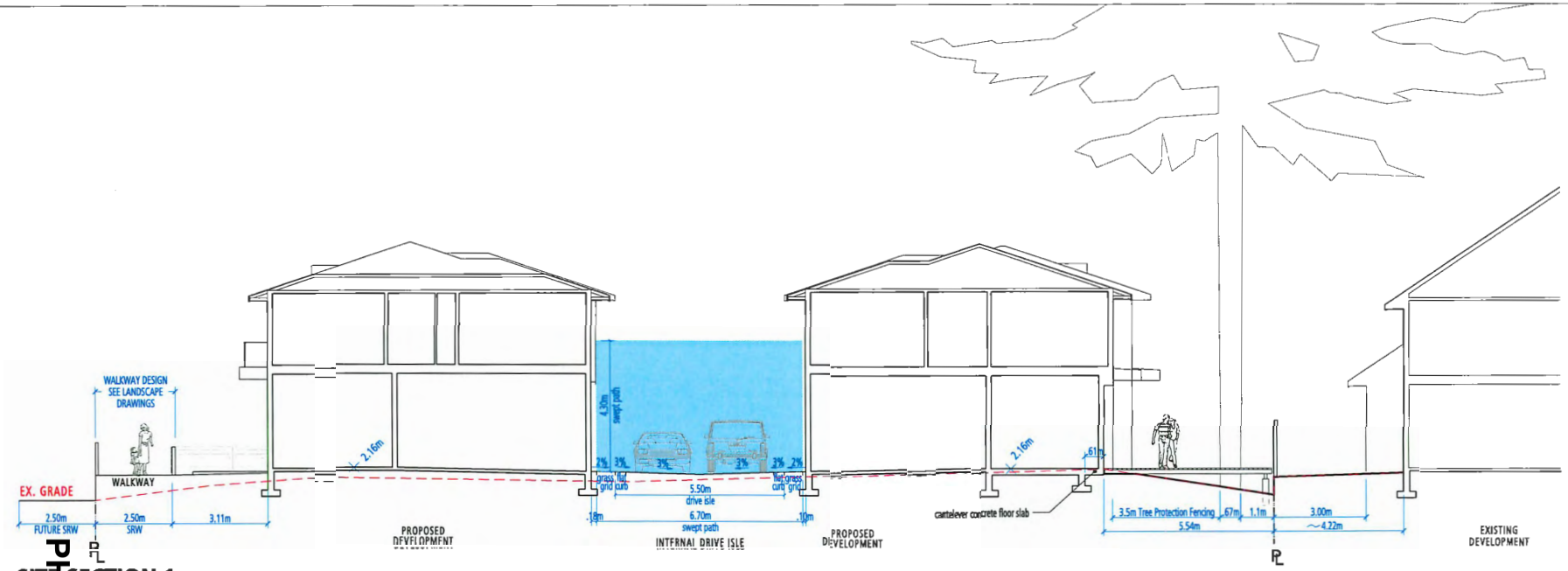
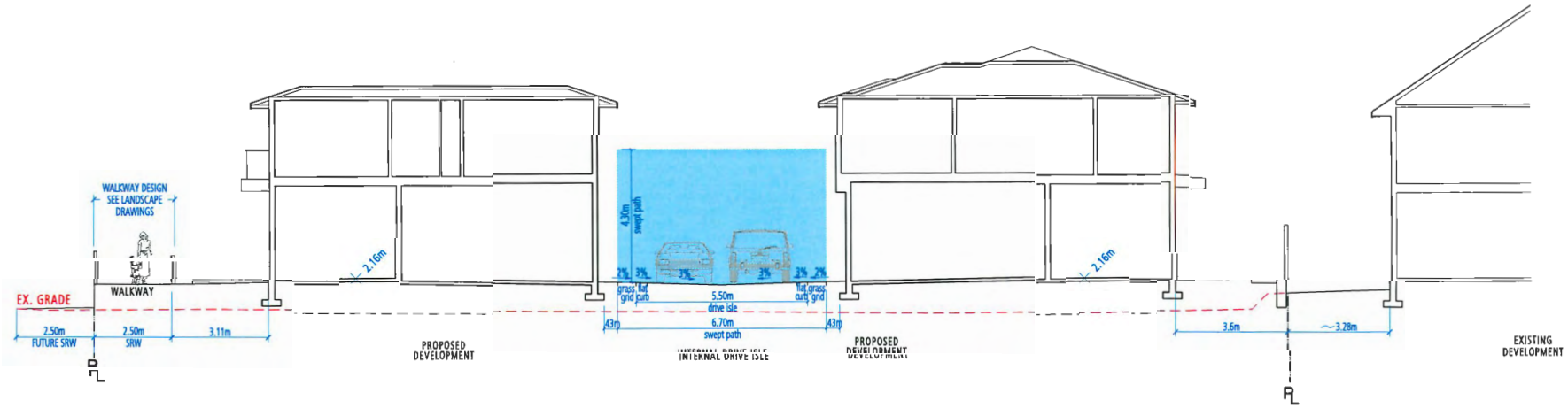


SITE SECTION 1



7100
ASH STREET

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REGISTERED ARCHITECT
DANIEL A. FOUGERE
February 24, 2020
BRITISH COLUMBIA

SITE SECTIONS

SCALE: 3/16" = 1'-0" UNO

0 5' 10' 15'

FEBRUARY 11, 2020

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VIEW FROM WEST- ASH STREET

VIEW FROM SOUTH WEST- ASH STREET

7100 ASH STREET

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REGISTERED ARCHITECT
BRIAN A. FUGERE
February 26, 2020
BRITISH COLUMBIA

PERSPECTIVES

FEBRUARY 26, 2020

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INTERIOR VIEW FROM EAST

VIEW FROM SOUTH EAST

7100 ASH STREET

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REGISTERED ARCHITECT
WEIHI A. FUNG
BRITISH COLUMBIA

PERSPECTIVES

FEBRUARY 26, 2020

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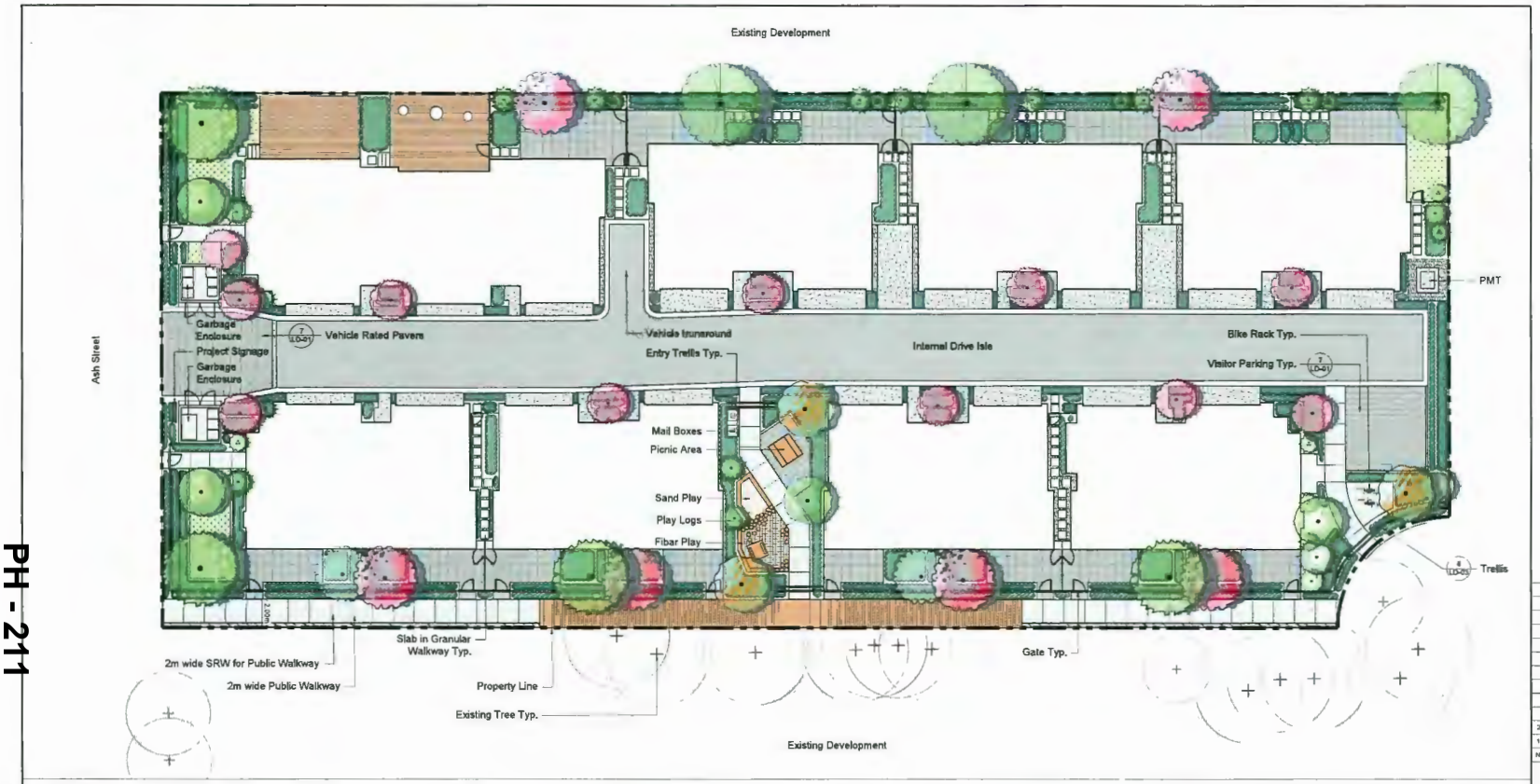

LEGEND

KEY	REF.	DESCRIPTION
	REFER TO OTHERS	CONCRETE SLAB (VEHICULAR) REFER TO OTHERS
	5 LD-01	CONCRETE & SCORELINES (PEDESTRIAN)
	4 LD-02	HYDRAPRESSED SLAB Pattern: Stack Bond Colour: Natural Manufacturer: Abbotsford Concrete
	6 LD-01	SLABS IN GRANULAR PAVING Pattern: Stack Bond Colour: Natural Manufacturer: Abbotsford Concrete
	7 LD-01	VEHICLE RATED PAVERS Pattern: Herringbone Bond Colour: Brown Manufacturer: Abbotsford Concrete
		GRAVEL
	2 LD-03	GRASS PAVE
		SOD See Critical Landscape Notes for Specifications
	3 LD-01	SHRUB / HEDGE PLANTING

	2 LD-02	SAND
	8 LD-01	ENGINEERED WOOD FIBAR
	1 LD-03	CEDAR PRIVACY FENCE
	3 LD-02	3FT PICKET FENCE
	5 LD-03	PICNIC TABLE
	6 LD-03	BIKE RACK
REFER TO TREE SCHEDULE		PROPOSED TREE

PLANT SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer griseum / Paperbark Maple	B&B	6cm		3
	Acer palmatum 'Sango Kaku' / Coral Bark Maple	B&B	6cm		4
	Amelanchier canadensis / Canadian Serviceberry	B&B	5cm		2
	Cercidiphyllum japonicum / Katsura Tree	B&B	6cm		4
	Chamaecyparis obtusa 'Gracilis' / Slender Hinoki Cypress	B&B		1.2m std	11
	Cornus florida rubra / Pink Flowering Dogwood	B&B	6cm		6
	Magnolia kobus / Kobus Magnolia	B&B	6cm		3
	Malus x 'Prairifire' / Prairifire Crab Apple	B&B	6cm		11
	Picea glauca pendula / Weeping White Spruce	2.0m ht.			2
	Picea pungens 'Fat Albert' / Colorado Spruce	B&B		2.0m ht	14
	Picea pungens 'Hoopsii' / Hoopsii Blue Spruce	B&B		2.0m ht	2

1:150

Frontage Fence Perspective

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No.	By	Description	Date
2.	TS	Issued for Rezoning	Feb. 25, 2020
1.	TS	Issued for Rezoning	Mar. 22, 2019

REVISIONS TABLE FOR DRAWINGS

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REVISIONS TABLE FOR SHEET

Project:
7100 & 7120 Ash Street

Location:
7100 & 7120 Ash Street, Richmond BC

Drawn: TS
Checked: TM
Approved: DJ
Scale: 1:150
Stamp
Original Sheet Size: 24"x36"

Drawing Title: OVERALL SITE PLAN

NORTH

VDZ Project #: DP2019-04

Drawing #: L-02

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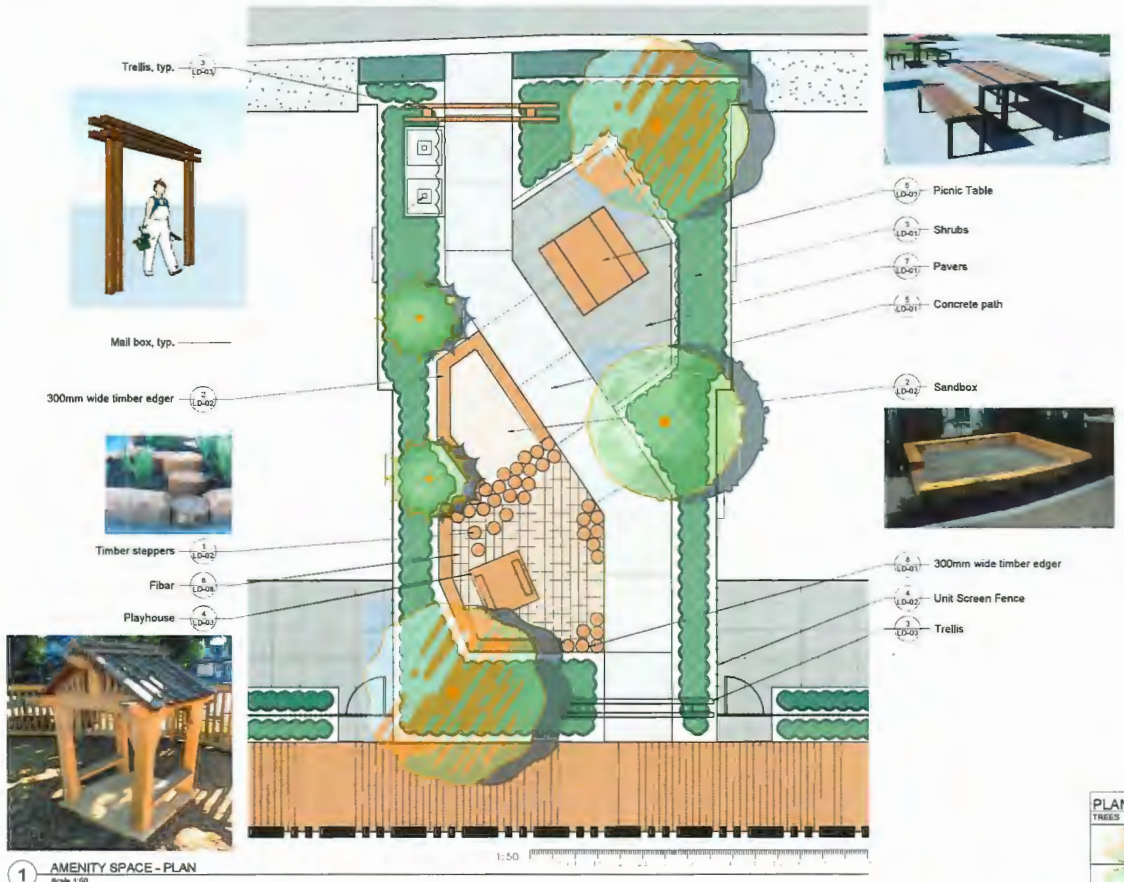
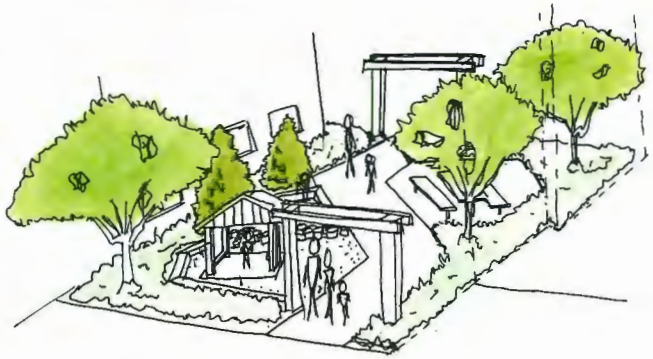


1 AMENITY SPACE - PLAN
Scale 1:50



LEGEND

KEY	REF.	DESCRIPTION
	REFER TO OTHERS	CONCRETE SLAB (VEHICULAR) REFER TO OTHERS
	5 LD-01	CONCRETE & SCORELINES (PEDESTRIAN)
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REFER TO TREE SCHEDULE		PROPOSED TREE

PLANT SCHEDULE

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	Amelanchier canadensis / Canadian Serviceberry	B&B	5cm		2
	Cercidiphyllum japonicum / Katsura Tree	B&B	6cm		4
	Chamaecyparis obtusa 'Gracilis' / Slender Hinoki Cypress	B&B		1.2m std	11
	Cornus florida rubra / Pink Flowering Dogwood	B&B	6cm		8
	Magnolia kobus / Kobus Magnolia	B&B	6cm		3
	Malus x 'Prairifire' / Prairifire Crab Apple	B&B	6cm		11
	Picea glauca pendula / Weeping White Spruce	2.0m ht.			2
	Picea pungens 'Fat Albert' / Colorado Spruce	B&B		2.0m ht	14
	Picea pungens 'Hoopsii' / Hoopsii Blue Spruce	B&B		2.0m ht	2

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Key Map (NTS)

No.	By	Description	Date
2.	TS	Issued for Rezoning	Feb. 25, 2020
1.	TS	Issued for Rezoning	Mar. 22, 2019

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No.	By	Description	Date

REVISIONS TABLE FOR SHEET

Project:
7100 & 7120 Ash Street

Location:
7100 & 7120 Ash Street, Richmond BC

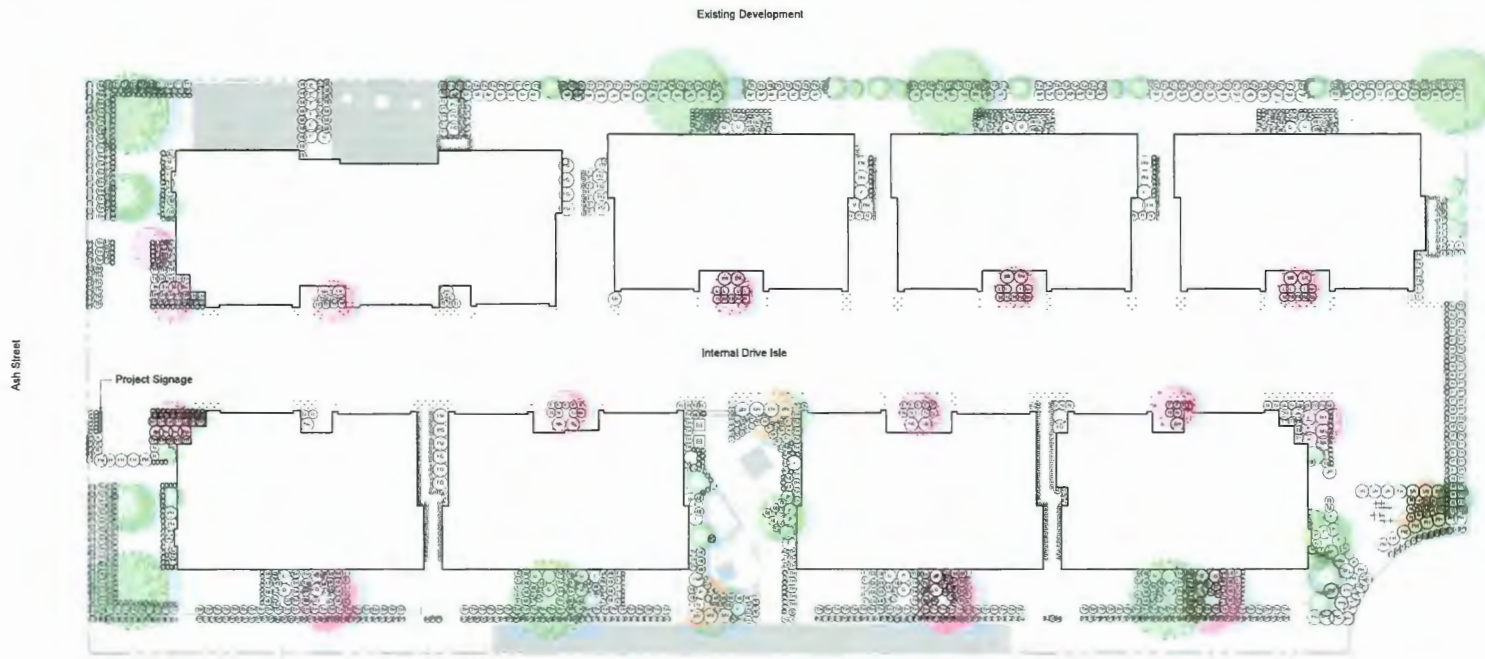
Drawn TS	Stamp
Checked TM	
Approved OJ	Original Sheet Size 24"x36"
Scale 1:50	

Drawing Title: AMENITY SITE PLAN

NORTH

VDZ Project #: DP2019-04

Drawing #: L-03



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PLANT SCHEDULE

SHRUBS	BOTANICAL / COMMON NAME	CONT	SPACING	QTY
AC	Asarum caudatum / British Columbia Wild Ginger	#2	0,50m	108
AF	Aster divaricatus 'Wood's Pink' / Wood's Pink Aster	#2	0,60m	36
BN	Berberis nervosa / Longleaf Barberry	#3	1m	19
BI	Blechnum spicant / Deer Fern	#2	0,60m	120
BS	Buxus sempervirens / American Boxwood	#2	0,45m	298
C	Cardamine trifolia / Three-leaf Cardamine	#1	0,30m	143
K	Cornus sericea 'Kelseyi' / Kelseyi Dogwood	#2	0,60m	6
L	Daphne x 'Lawrence Crocker' / Lawrence Crocker Daphne	#2	0,60m	18
D	Dicentra spectabilis 'Queen of Hearts' / Bleeding Heart	#1	0,35m	263
DE	Dryopteris erythrosora / Autumn Fern	#2	0,60m	16
E	Erica carnea 'Springwood Pink' / Pink Spring Heath	#2	0,50m	76
EO	Erythronium oregonum / Fawn Lily	bulb	0,50m	16
G	Gaultheria shallon / Salal	#2	0,90m	28
LH	Lavandula angustifolia 'Hidcote Blue' / Hidcote Blue Lavender	#2	0,60m	12

SHRUBS	BOTANICAL / COMMON NAME	CONT	SPACING	QTY
N	Nandina domestica 'Gulf Stream' TM / Heavenly Bamboo	#2	0,60m	56
PJ	Pachysandra terminalis 'Green Carpet' / Japanese Spurge	#2	1m	21
PM	Pennisetum alopecuroides 'Hamelin' / Hamelin Dwarf Fountain Grass	#2	0,90m	27
PJ	Pieris japonica 'Mountain Fire' / Mountain Fire Pieris	#3	1m	4
P	Polystichum munitum / Western Sword Fern	#2	0,90m	21
P	Polytrichum Commune / Hair Cap Moss	#1	0,30m	255
R	Rhododendron x 'Ramapo' / Ramapo Rhododendron	#2	0,60m	62
S	Sarcococca hookeriana humilis / Sweet Box	#2	0,45m	31
SJ	Skimmia japonica 'Rubella' / Skimmia	#2	0,90m	63
TH	Taxus x media 'H.M. Eddie' / Eddie Yew	#2	0,60m	216
TS	Thuja occidentalis 'Smaragd' / Emerald Green Arborvitae	#2	0,60m	146
V	Vaccinium ovatum / Evergreen Huckleberry	#3	1m	13

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No.	By	Description	Date
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REVISIONS TABLE FOR SHEET

Project:

7100 & 7120 Ash Street

Location:

7100 & 7120 Ash Street, Richmond BC

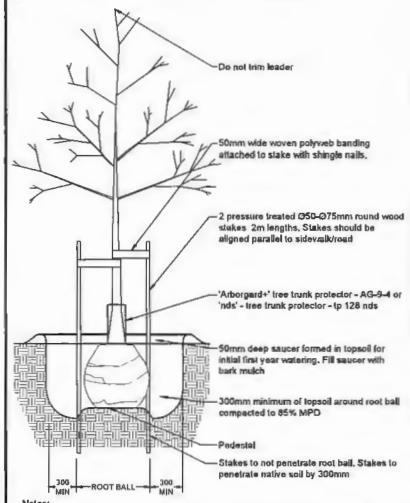
Drawn TS	Stamp
Checked TM	
Approved DJ	Original Sheet Size 24"x36"
Scale 1:150	

Drawing Title: PLANTING PLAN

NORTH

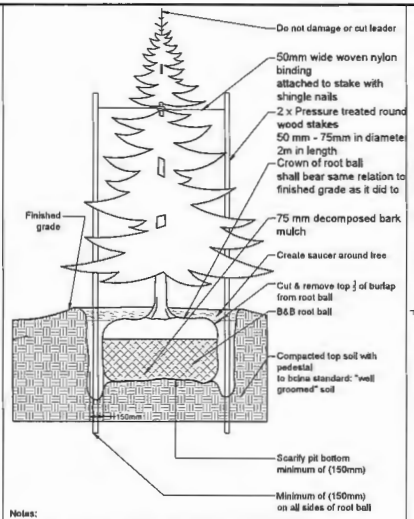
VDZ Project #: DP2019-04

Drawing #: L-04



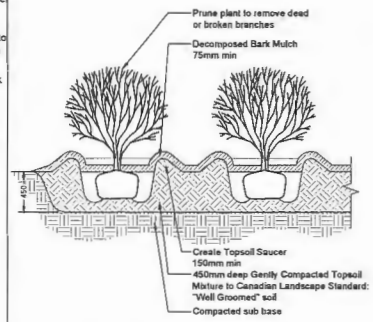
Notes:
1. Sacking/burlap to be loosened and dropped to the bottom of the planting hole. all string, twine, etc.to be removed.
2. All wire baskets shall have the top 1/3 of the wire removed prior to planting.
3. All trees shall be single stem

1 DECIDIOUS TREE
Scale 1:25



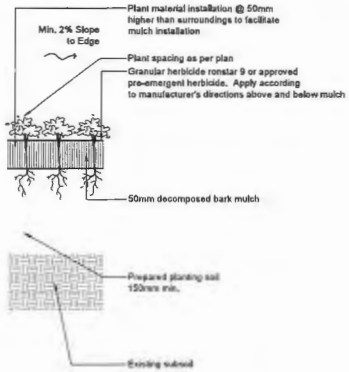
Notes:
1. Do not damage main roots or destroy root ball when installing tree stake.
2. Water thoroughly after installation
3. Remove tree rings and stakes two years after installations
4. Provide drainage for planting pit in impermeable soil

2 CONIFEROUS TREE
Scale 1:25

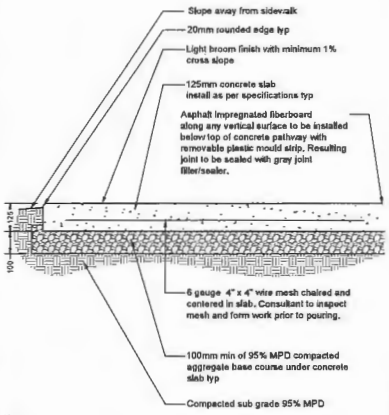


NOTES:
1. Remove all string, twine, pots, tags from plant.
2. All soil, mulch and plant material to meet latest Canadian Landscape Standard.
3. Topsoil saucer diameter to be 1.5X diameter of plant container.
4. O.C. spacing per planting plan.

3 SHRUB PLANTING
Scale 1:25



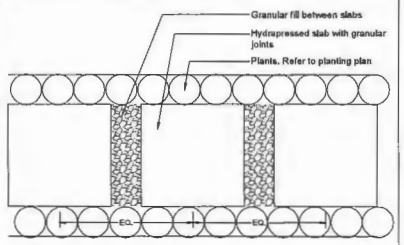
4 GROUND COVER PLANTING
Scale 1:10



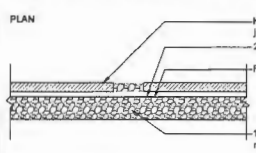
Notes:
1. Contractor to provide expansion joints where concrete meets all vertical structures
2. Horizontal scoreline 1500mm o.c. center scoreline on 150mm smooth finish or to match existing concrete pathway

5 CONCRETE SLAB
Scale 1:10

Notes:
1. Slabs to be spaced equally between adjacent surfaces.



PLAN

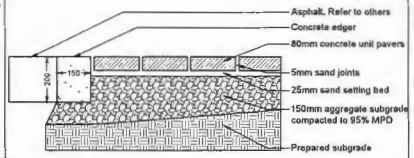


SECTION

Notes:
1. Install all components as per manufacturer's specifications.
2. Supplier: Abbotsford Concrete or approved Equal
3. Type: Terrada
4. Sizes: 610mm x 610mm, 457mm x 457mm
5. Color: Charcoal

6 SLABS IN GRANULAR PAVING
Scale 1:10

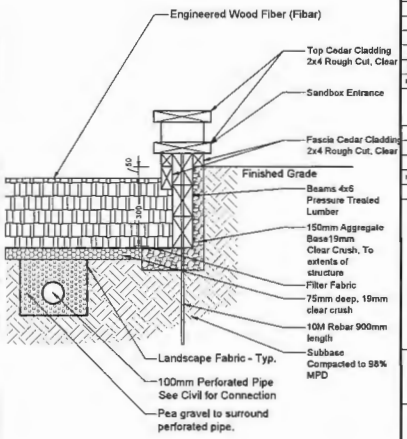
STRETCHER BOND PATTERN



SECTION

Notes:
1. Install all components as per manufacturer's specifications.
2. Stretcher bond pattern
3. Abbotsford Concrete: Classic Series 3:1 Standard, Colour: Brown

7 VEHICULAR UNIT PAVERS
Scale 1:10



8 PLAY AREA FIBAR AND EDGER
Scale 1:10

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Fort Langley, BC | Vancouver, BC
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No.	By	Description	Date
2.	TS	Issued for Rezoning	Feb. 25, 2020
1.	TS	Issued for Rezoning	Mar. 22, 2019

REVISIONS TABLE FOR DRAWINGS

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No.	By	Description	Date

REVISIONS TABLE FOR SHEET

Project:
7100 & 7120 Ash Street

Location:
7100 & 7120 Ash Street, Richmond BC

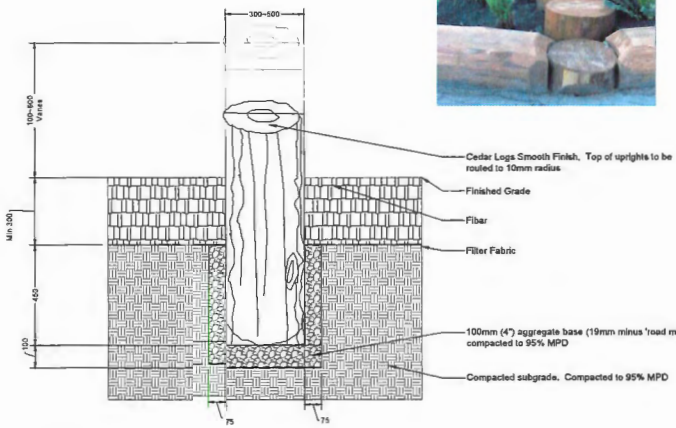
Drawn: TS
Checked: TM
Approved: DJ
Stamp
Original Sheet Size: 24"x36"
Scale: AS SHOWN

Drawing Title: DETAILS
VDZ Project #: DP2019-04
Drawing #: LD-01

PH - 214

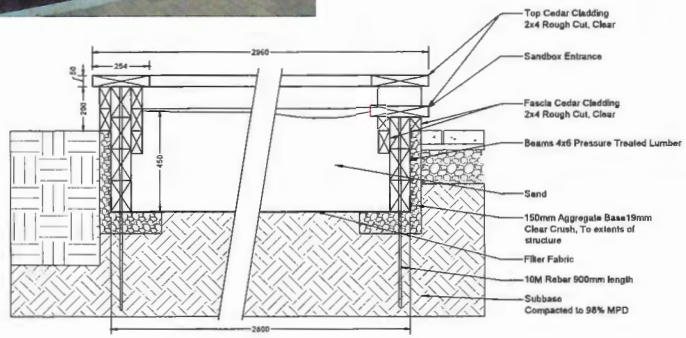
Z:\PROJECTS\DEVELOPMENT REMOVED\DP\7100 AND 7120 ASH STREET\DP\SHEET DJ\DWI DETAILS.DWG

Notes:
1. Logs to be stained with Sikkens Cetol SRD - Colour 077-Cedar or approved equal. End sealer to be used on all areas below grade. Apply as per manufacturers specifications.
2. Size of log uprights as per plan.
3. All branches and trunk splits to be cut flush, routed and sealed. Bark to remain intact.



1 STEPPING STOME STUMPS
Scale 1:10

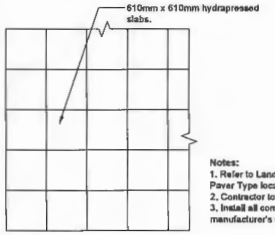
Notes:
1. All exposed decking and fascia to be sanded smooth.
2. Apply Sikkens Cetol SRD to all Cedar elements. Colour: Oak.
3. All members to be fastened with hot dipped galvanized hardware.



2 SANDBOX AND EDGER
Scale 1:10

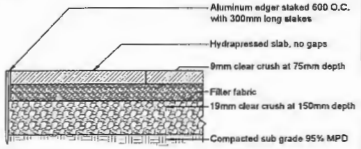
3ft Picket Fence

3 RAIL FENCE
NTS



Notes:
1. Refer to Landscape Drawings for Paver Type locations
2. Contractor to provide submittals
3. Install all components as per manufacturer's specifications

PLAN VIEW
Scale 1:25



SECTION ON GRADE

Hydrapressed Slab Details:
Type: Texada
Size: 610mm x 610mm x 60mm
Colours: Refer to Legend
Pattern: Stack Bond
Manufacturer: Abbotsford Concrete Products
www.pavingstones.com 1-800-563-4091

4 PERMEABLE HYDRAPRESSED SLAB AT GRADE
Scale 1:10

Square Trex deck with trees

5 SQUARE TREX DECK BUILT AORUND EXISTING TREES
NTS

Trex boardwalk with kick plate

6 SQUARE BOARDWALK WITH KICK PLATE
NTS

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No.	By	Description	Date
2.	TS	Issued for Rezoning	Feb. 25, 2020
1.	TS	Issued for Rezoning	Mar. 22, 2019

REVISIONS TABLE FOR DRAWINGS

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7100 & 7120 Ash Street

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7100 & 7120 Ash Street, Richmond BC

Drawn TS	Stamp
Checked TM	
Approved DJ	Original Sheet Size 24"x36"
Scale AS SHOWN	

Drawing Title: DETAILS

VDZ Project #: DP2019-04

Drawing #: LD-02

PH - 215

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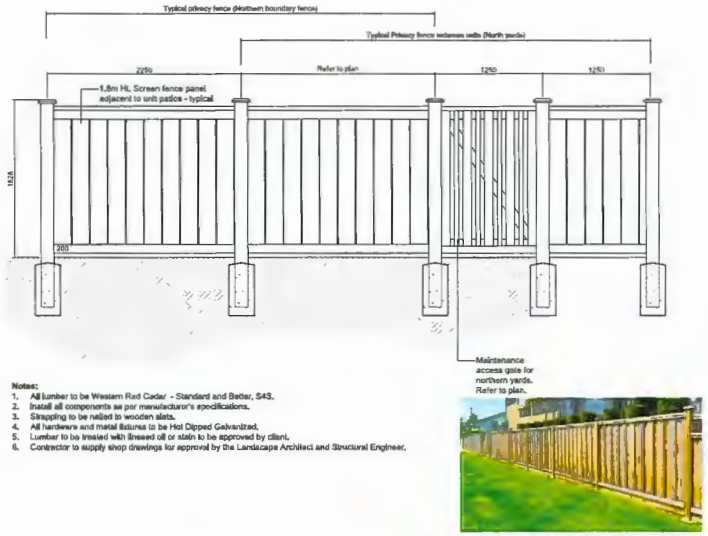
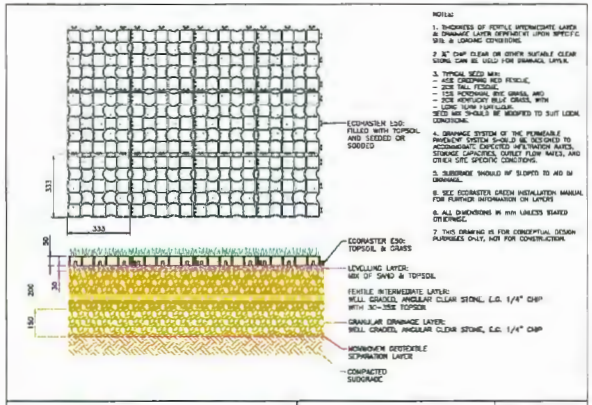


Notes:
1. All lumber to be Western Red Cedar - Standard and Better, S4S.
2. Install all components as per manufacturer's specifications.
3. Strapping to be nailed to wooden slats.
4. All hardware and metal fixtures to be Hot Dipped Galvanized.
5. Lumber to be treated with linseed oil or stain to be approved by client.
6. Contractor to supply shop drawings for approval by the Landscape Architect and Structural Engineer.

1 PRIVACY FENCE
Scale 1:20



NOTES:
1. THICKNESS OF FERTILE INTERMEDIATE LAYER & DRAINAGE LAYER DEPENDENT UPON SPECIFIC SITE & LOADING CONDITIONS.
2. 3/4" CHIP CLEAR OR OTHER SUITABLE CLEAR STONE CAN BE USED FOR DRAINAGE LAYER.
3. TYPICAL SEED MIX:
- 40% CREEPING RED FESCUE,
- 20% TALL FESCUE,
- 15% PERENNIAL RYE GRASS, AND
- 20% KENTUCKY BLUE GRASS, WITH
- LONG TERM FERTILIZER.
SEED MIX SHOULD BE MODIFIED TO SUIT LOCAL CONDITIONS.
4. DRAINAGE SYSTEM OF THE PERMEABLE PAVEMENT SYSTEM SHOULD BE DESIGNED TO ACCOMMODATE EXPECTED INFILTRATION RATES, STORAGE CAPACITIES, OUTLET FLOW RATES, AND OTHER SITE SPECIFIC CONDITIONS.
5. SUBGRADE SHOULD BE SLOPED TO AID IN DRAINAGE.
8. SEE ECORASTER GREEN INSTALLATION MANUAL FOR FURTHER INFORMATION ON LAYERS
6. ALL DIMENSIONS IN mm UNLESS STATED OTHERWISE.
7. THIS DRAWING IS FOR CONCEPTUAL DESIGN PURPOSES ONLY, NOT FOR CONSTRUCTION.

PURUS ECORASTER®

ECORASTER E50
PERMEABLE PAVING SYSTEM
GENERAL, GRASSED

DATE:	MAR. 2016
SCALE:	NTS
SHEET:	1 OF 1
REVISION:	4

2 GRASS PAVE (WITHIN FIRE ACCESS PATH)
NTS

Material: To match rail fence

3 TRELLIS
NTS

Supplier: Habitat Systems
Model name: Playhouse
Material: Yellow cedar
Contact: https://www.habitat-systems.com/product/playhouses-2/

4 PLAY HOUSE
NTS

Supplier: Maglin Site Furniture
Model Number: MLPT720
Finish: Black powder coat (fine textured matte finish)
Mount: Surface mount

5 PICNIC TABLE
NTS

Supplier: Maglin Site Furniture
Model Number:SCBR1600
Finish: Black powder coat (fine textured matte finish)
Mount: Surface mount

6 BIKE RACK
NTS

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No.	By:	Description	Date
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7100 & 7120 Ash Street

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7100 & 7120 Ash Street, Richmond BC

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Checked: TM	
Approved: TM	Original Sheet Size: 24"x36"
Scale: AS SHOWN	CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING WITH WORK. ALL MATERIAL OR EQUIPMENT SUPPLIED BY THE CONTRACTOR, PROPERTY OF THE OWNER AND MUST BE INSTALLED AT THE COMPLETION OF THE WORK. ALL RECORDING OF PAYMENT FOR WORK OR MATERIALS MUST NOT BE PRESUMED TO MEAN ACCEPTANCE OF WORK OR MATERIALS.

Drawing Title: DETAILS

VDZ Project #: DP2019-04

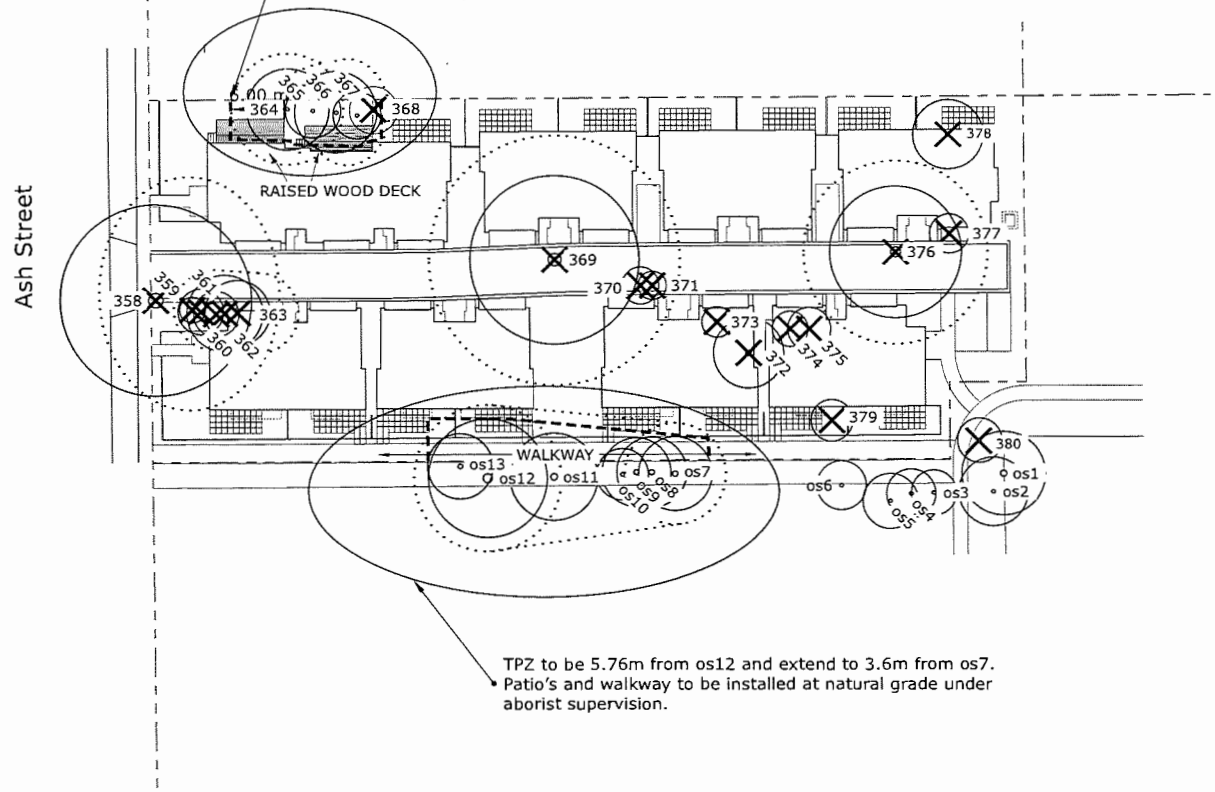
Drawing #: LD-03

Z:\PROJECTS\DEVELOPMENT\PROJECTS\DP2019-04 7100 AND 7120 ASH STREET\DWG\SHEETS\LD-03 DETAILS.DWG

Tree Management Plan for Development at
7100 & 7120 Ash Street
Richmond, BC

Date: August 31, 2018
Updated: August 9, 2019

Tree Protection zone fence to extend 6m West of tree #364. TPZ fence to extend 3.5 to building. TPZ fence to extend 2.5m East of tree #367.
-Architectural design shows cantilevered floor and deck posts which provide 4.3m footing setback from the trees.
— All ground works to be supervised by arborist. Decking to be installed at early stage to enable construction access but protect ground. Existing grade in the TPZ is to remain at pre construction grade.
— Clearance pruning required (less than 25%) is to be performed to ISA standards and under arborist supervision.



TPZ to be 5.76m from os12 and extend to 3.6m from os7.
Patio's and walkway to be installed at natural grade under aborist supervision.

Tree Inventory

Tag	Species	DBH (cm)	TPZ (m)
358	Silver maple	155	9.30
359	Yellow cedar	21	1.26
360	Yellow cedar	37	2.22
361	Yellow cedar	37	2.22
362	Yellow cedar	62	3.72
363	Yellow cedar	53	3.18
364	Douglas fir	94	4.5
365	Douglas fir	45	2.70
366	Douglas fir	67	4.02
367	Douglas fir	39	2.34
368	Birch	-	-
369	Silver maple	137	8.22
370	Birch	30	1.80
371	Birch	23	1.38
372	Black pine	57	3.42
373	Birch	25	1.50
374	Birch	29	1.74
375	Birch	35	2.10
376	Birch	107	6.42
377	Birch	32	1.92
378	Birch	-	-
379	Birch	34	2.04
380	Black pine	36	2.16
os1	Douglas fir	68	4.08
os2	Douglas fir	56	3.36
os3	Black pine	37	2.22
os4	Birch	40	2.40
os5	Birch	45	2.70
os6	Birch	40	2.40
os7	Douglas fir	60	3.60
os8	Douglas fir	57	3.42
os9	Douglas fir	56	3.36
os10	Douglas fir	42	2.52
os11	Douglas fir	71	4.26
os12	Douglas fir	96	5.76
os13	Birch	53	3.18

Legend

N

dripline

x = remove tree

tree barrier

1m 1:500 10m

ATTACHMENT 6

City of Richmond

ATTACHMENT 7

Rezoning Considerations

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 7100 & 7120 Ash Street **File No.:** RZ 18-843479

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 10163, the developer is required to complete the following:

1. 8.1 m by 8.1 m road dedication at the southeast corner of the site, for the extension of Sills Avenue.
2. Consolidation of all the lots into one development parcel (which will require the demolition of the existing dwellings).
3. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
4. Submission of a Tree Survival Security to the City in the amount of $40,000 for the four on-site trees to be retained (Tag # 364-367).
5. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
6. Granting of an approximately 2.5 m wide statutory right-of-way for public rights-of-passage across the entire south property line for the construction of a pedestrian pathway. Design is to include 2.0 m wide pathway and 0.5 m wide landscaped shoulder with pedestrian-scale lighting.

   Any works essential for public access within the required statutory right-of-way (SRW) are to be included in the Servicing Agreement (SA) and the maintenance & liability responsibility is to be clearly noted. The design must be prepared in accordance with good engineering practice with the objective to optimize public safety and after completion of the works, the Owner is required to provide a certificate of inspection for the works, prepared and sealed by the Owner's Engineer in a form and content acceptable to the City, certifying that the works have been constructed and completed in accordance with the accepted design. Works to be secured via DP (for multi-family, commercial or industrial (only those industrial sites within the City Centre Area Plan or otherwise required by the OCP DP Guidelines), or via Rezoning for single-family sites.
7. Registration of a flood indemnity covenant on title.
8. City acceptance of the developer's offer to voluntarily contribute $$0.85 per buildable square foot (e.g. $19,046.80) to the City's Public Art Fund.
9. Contribution of $1,769 per dwelling unit (e.g. $30,073) in-lieu of on-site indoor amenity space to go towards development of City facilities.
10. City acceptance of the developer's offer to voluntarily contribute $8.50 per buildable square foot (e.g. $190,468) to the City's Affordable Housing Fund.
11. The submission and processing of a Development Permit* completed to a level deemed acceptable by the Director of Development.

Prior to a Development Permit* being forwarded to the Development Permit Panel for consideration, the developer is required to:

1. Submission of a Landscape Plan, prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development. The Landscape Plan should:
   - comply with the guidelines of the OCP's Arterial Road Policy and should not include hedges along the front property line;
   - include a mix of coniferous and deciduous trees;
   - include the dimensions of tree protection fencing as illustrated on the Tree Retention Plan attached to this report; and

Initial: _____

- include the 36 required replacement trees with the following minimum sizes:

No. of Replacement Trees	Minimum Caliper of Deciduous Replacement Tree	Minimum Height of Coniferous Replacement Tree
8	11 cm	6 m
4	10 cm	5.5 m
10	8 cm	4 m
14	6 cm	3.5 m

If required replacement trees cannot be accommodated on-site, a cash-in-lieu contribution in the amount of $750/tree to the City's Tree Compensation Fund for off-site planting is required.

2. Complete a proposed townhouse energy efficiency report and recommendations prepared by a Certified Energy Advisor which demonstrates how the proposed construction will meet or exceed the required townhouse energy efficiency standards (BC Energy Step Code Step 3 or better).

Prior to Development Permit Issuance, the developer must complete the following requirements:

1. Submission of a Landscape Security based on 100% of the cost estimate provided by the Landscape Architect, including installation costs.

Prior to Demolition Permit Issuance, the developer must complete the following requirements:

1. Installation of appropriate tree protection fencing around all trees to be maintained as part of the development prior to any construction activities, including building demolition, occurring on-site. Should the developer with to begin site preparation work after third reading of the rezoning bylaw, but prior to final adoption of the rezoning bylaw and issuance of the Development Permit, the applicant will be required to obtain a Tree Removal Permit (Rezoning in Process – T3).

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
2. Incorporation of energy efficiency and accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
3. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.
4. Enter into a Servicing Agreement* for the design and construction of engineering infrastructure improvements. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to, the following:

   Water Works:

   a) Using the OCP Model, there is 452 L/s of water available at a 20 psi residual at the Ash Street frontage. Based on your proposed development, your site requires a minimum fire flow of 220 L/s.
   b) At Developer's cost, the Developer is required to:
      i) Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage building designs.
      ii) Review hydrant spacing on all road frontages and install new fire hydrants as required to meet City spacing requirements for the proposed land use.
      iii) Provide a right-of-way for the water meter. Minimum right-of-way dimensions to be the size of the meter box (from the City of Richmond supplementary specifications) + any appurtenances (for example, the bypass on

Initial: _____

W2o-SD) + 0.5 m on all sides. Exact right-of-way dimensions to be finalized via the servicing agreement process.

c) At Developer's cost, the City will:
   i) Cut, cap, and remove all existing water connections and meters serving the development site.
   ii) Install one new water connection to serve the proposed development. The water meter and meter box shall be located onsite in a right-of-way, as described above.

Storm Sewer Works:

a) At Developer's cost, the Developer is required to:
   i) Provide an erosion and sediment control plan for all on-site and off-site works, to be reviewed as part of the servicing agreement design.
   ii) Upgrade approximately 40 m of storm sewer along the Ash Street frontage to minimum 600 mm diameter, from the north property line to the south property line, complete with new manholes at the tie-in points.
   iii) Reconnect all existing services to the proposed storm sewer.
   iv) Perform a capacity analysis to size the proposed storm sewer. The analysis shall be included in the servicing agreement drawing set.
   v) Confirm the locations of inspection chambers STIC 43549 and STIC 43554. If the inspection chambers are located within the development site, the developer shall provide rights-of-ways to accommodate the inspection chambers. Alternatively, the developer can obtain written consent from the adjacent property owners for access to their properties to re-align the storm connections so that the inspection chambers can be relocated out of the development site. Prior to seeking consent from the adjacent property owners, the developer is required to coordinate with the City to ensure the form and content of the communication will satisfy the City's requirements.

b) At Developer's cost, the City will:
   i) Cut, cap, and remove all existing storm connections serving the development site. The connections at the north and south property lines of the development site shall be capped at the inspection chamber and the inspection chambers retained, to keep service to the adjacent properties as described above.
   ii) Complete all tie-ins for the proposed works to existing City infrastructure.

Sanitary Sewer Works:

c) At Developer's cost, the City will:
   i) Cut, cap, and remove all existing sanitary connections and inspection chambers serving the development site.
   ii) Install one new sanitary connection, complete with inspection chamber, to serve the proposed development.

Frontage Improvements:

d) At Developer's cost, the Developer is required to:
   i) Coordinate with BC Hydro, Telus and other private communication service providers:
      (1) To relocate or underground the hydro pole at the common property line of 7100 & 7120 Ash Street, as required to facilitate construction of the proposed driveway and sidewalk.
      (2) To pre-duct for future hydro, telephone and cable utilities along all road frontages.
      (3) Before relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
      (4) To underground overhead service lines.
   ii) Locate/relocate all above ground utility cabinets and kiosks required to service the proposed development, and all above ground utility cabinets and kiosks located along the development's frontages, within the developments site (see list below for examples). A functional plan showing conceptual locations for such infrastructure shall be included in the development design review process. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements (e.g., statutory right-of-way dimensions) and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter

Initial: _____

to be submitted to the City. The following are examples of statutory right-of-ways that shall be shown on the architectural plans/functional plan, the servicing agreement drawings, and registered prior to SA design approval:

(1) BC Hydro PMT – 4.0 x 5.0 m
(2) BC Hydro LPT – 3.5 x 3.5 m
(3) Street light kiosk – 1.5 x 1.5 m
(4) Traffic signal kiosk – 2.0 x 1.5 m
(5) Traffic signal UPS – 1.0 x 1.0 m
(6) Shaw cable kiosk – 1.0 x 1.0 m
(7) Telus FDH cabinet – 1.1 x 1.0 m

iii) Provide street lighting along Ash Street.

iv) Complete frontage improvements on Ash Street including:
(1) Road widening to a total width if 8.5 m, which will require shifting the crown of the road east to match the existing road cross-section north of the subject site;
(2) New 1.75 m wide concrete sidewalk next to the property line; and
(3) New 2.5 m wide boulevard, including grass, trees, and road lighting;

v) Complete frontage improvements on Sills Avenue including:
(1) Construct new curb and gutter along the curved alignment of the intersection of Sills Avenue and Armstrong Street;
(2) Construct a new 1.5 m wide concrete sidewalk along the curved alignment of the intersection of Sills Avenue and Armstrong Street, within the new road dedication at the southeast of the subject site;
(3) Construct a new grass/tree boulevard of varying width between the new sidewalk and new north curb line of Sills Avenue, and infill the area between the new sidewalk and the property lines of the subject site and 9515 Sills Avenue with a new grass/tree boulevard; and
(4) Reconstruct the existing driveway to 9515 Sills Avenue to City design standards to suit the new curb and sidewalk alignment described above.

vi) Construct a pedestrian pathway along the south property line, to include:
(1) 0.5 m landscaped shoulder with lighting; and
(2) 2.0 m paved pathway, or other surface treatment to the satisfaction of the City

General Items:

e) At Developer's cost, the Developer is required to:

i) Provide, prior to start of site preparation works or within the first servicing agreement submission, whichever comes first, a preload plan and geotechnical assessment of preload, dewatering, and soil preparation impacts on the existing utilities fronting the development site and provide mitigation recommendations. Based on the City's review of the geotechnical report, at the City's discretion, some or all of the following may be required:
(1) Provide a video inspection report of the existing storm and sanitary sewers fronting the development site prior to start of site preparation works or within the first servicing agreement submission, whichever comes first. A follow-up video inspection, complete with a civil engineer's signed and sealed recommendation letter, is required after site preparation works are complete (i.e. pre-load removal, completion of dewatering, etc.) to assess the condition of the existing utilities and provide recommendations to retain, replace, or repair. Any utilities damaged by the pre-load, de-watering, or other ground preparation shall be replaced or repaired at the Developer's cost.
(2) Conduct pre- and post-preload elevation surveys of all surrounding roads, utilities, and structures. Any damage, nuisance, or other impact to be repaired at the developer's cost. The post-preload elevation survey shall be incorporated within the servicing agreement design.
(3) Monitor the settlement at the adjacent utilities and structures during pre-loading, dewatering, and soil preparation works per a geotechnical engineer's recommendations, and report the settlement amounts to the City for approval.

ii) Coordinate the servicing agreement design for this development with the servicing agreement(s) for the adjacent development(s), both existing and in-stream. The developer's civil engineer shall submit a signed and sealed letter with each servicing agreement submission confirming that they have coordinated with civil engineer(s) of the adjacent project(s) and that the servicing agreement designs are consistent. The City will

Initial: _____

not accept the 1st submission if it is not coordinated with the adjacent developments. The coordination letter should cover, but not be limited to, the following:

(1) Corridors for City utilities (existing and proposed water, storm sewer, sanitary and DEU) and private utilities.
(2) Pipe sizes, material and slopes.
(3) Location of manholes and fire hydrants.
(4) Road grades, high points and low points.
(5) Alignment of ultimate and interim curbs.
(6) Proposed street lights design.

iii) Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

Note:

* This requires a separate application.

- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

  All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

  The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

[signed concurrence on file]

Signed | Date

City of Richmond

Bylaw 10163

Richmond Zoning Bylaw 8500
Amendment Bylaw 10163 (RZ 18-843479)
7100 & 7120 Ash Street

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation of the following area and by designating it **"TOWN HOUSING (ZT16) - SOUTH MCLENNAN AND ST. ALBANS SUB-AREA (CITY CENTRE)".**

   P.I.D. 000-788-597
   The North 76 Feet of Lot 3 Block "C" of Section 15 Block 4 North Range 6 West New Westminster District Plan 1207

   P.I.D. 000-751-359
   Lot 3 Except: the North 76 Feet; Block "C" Section 15 Block 4 North Range 6 West New Westminster District Plan 1207

2. This Bylaw may be cited as **"Richmond Zoning Bylaw 8500, Amendment Bylaw 10163".**

FIRST READING	APR 14 2020
A PUBLIC HEARING WAS HELD ON	
SECOND READING	
THIRD READING	
OTHER CONDITIONS SATISFIED	
ADOPTED	

CITY OF RICHMOND
APPROVED by
APPROVED by Director or Solicitor

MAYOR

CORPORATE OFFICER

City of Richmond

Report to Committee

To:	Planning Committee	**Date:**	March 5, 2020
From:	James Cooper, Architect AIBC Director, Building Approvals Barry Konkin Director, Policy Planning	**File:**	08-4430-01/2020-Vol 01
Re:	**Fence Regulations Addressing Height and Materials**		

Staff Recommendation

1. That Richmond Zoning Bylaw No. 8500, Amendment Bylaw No. 10122, respecting changes to fence regulations, be introduced and given first reading, and

2. That Building Regulation Bylaw No. 7230, Amendment Bylaw No. 10144, requiring a permit for fences constructed with concrete foundations, be introduced and given first, second and third readings.

James Cooper, Architect AIBC
Director, Building Approvals
(604-247-4606)

Barry Konkin
Director, Policy Planning
(604-276-4139)

Att. 4

REPORT CONCURRENCE		
ROUTED TO:	**CONCURRENCE**	**CONCURRENCE OF GENERAL MANAGER**
Law	☑	
Finance	☑	
SENIOR STAFF REPORT REVIEW	**INITIALS:** CJ	**APPROVED BY CAO**

Staff Report

Origin

At the November 5, 2019 Planning Committee meeting, the following referral motion was passed:

> *That staff review Richmond Zoning Bylaw No. 8500 to examine:*
>
> *1) regulations for building fences and walls, including the definition of a fence and a wall;*
>
> *2) materials that can be used, including the possible elimination of masonry and iron; and*
>
> *3) tree planting restrictions;*
>
> *and report back.*

This report supports Council's Strategic Plan 2018-2022 Strategy #6 Strategic and Well-Planned Growth:

> *Leadership in effective and sustainable growth that supports Richmond's physical and social needs.*
>
> *6.1 Ensure an effective OCP and ensure development aligns with it.*

The referral was a result of public concerns regarding unpermitted construction of a concrete planter along the 181 m (594 ft) frontage of a property on No. 2 Road, which is zoned "Agriculture (AG1)." This report responds to the referral by providing information on current fence regulations in the City of Richmond and presents a bylaw for Council's consideration which would amend current fence regulations.

After investigating provisions to regulate tree planting, staff have determined that there are legal issues regarding imposition of regulations for fencing in the Agricultural Land Reserve (ALR) that are more appropriately addressed by the City Solicitor in a separate memorandum to Council offering legal advice on the matter.

Findings of Fact

<u>Current Fence Regulations</u>

Fences and walls are different types of structures. Richmond Zoning Bylaw No. 8500 and Building Regulation Bylaw No. 7230 contains existing interpretations and regulations for fences. Currently, both Richmond Zoning Bylaw No. 8500 and Building Regulation Bylaw No. 7230 provide a definition of 'fence,' but not 'wall.'

Bylaw No. 8500 Section 3.4 defines a fence:

> *"Fence means a structure used as an enclosure or for screening purposes around all or part of a lot."*

Bylaw No. 7230 Section 3.4 defines a fence:

> *"Fence means a structure bounding an area of land designed to limit access to or from the area or to screen the area from view."*

Fence regulations are provided in Section 6 of Richmond Zoning Bylaw No. 8500 and limit fence heights along arterial roads and in residential and non-residential zones. Barbed wire, razor wire and barbed tape obstacle, and electrified wire are prohibited in residential zones and permitted in other zones under certain conditions. Other materials, including masonry and iron (ornamental metal), are not currently regulated. See Attachment 1 for an excerpt of Richmond Zoning Bylaw No. 8500 current fence regulations.

In addition, in a report to committee dated June 6, 2017, staff noted that it is unlawful for the City to prohibit front yard fences or gates, but as per the *Local Government Act*, Council is able to regulate these structures, including siting, height, materials and setbacks.

Fence Regulation Research

In examining Richmond's fence regulations, staff have conducted an environmental scan of fence requirements in other municipalities in and around Metro Vancouver (Attachment 2). The results of the scan indicate the following:

- Some municipalities do not define 'fence'; however, some definitions of 'structure' include fences.
- Most municipalities do not define 'wall.'
- All municipalities limit fence height in residential zones.
- Some municipalities limit fence height in agricultural zones.
- Most municipalities do not regulate fence material with the exception of Coquitlam which has prohibited unadorned cast in place concrete which is termed "wall" and not fence.

Following the environmental scan, staff examined the City's current regulations and identified a series of recommended bylaw amendments for Council's consideration. The proposed amendments are included in Bylaw No. 10122.

Analysis

The public and Council recently raised concerns regarding concrete supported structures on agriculturally zoned properties and how such structures are regulated by existing zoning definitions. In order to address the November 5, 2019 Planning Committee referral, staff have examined existing fencing regulations and related definitions in Richmond Zoning Bylaw No. 8500, to identify areas where these regulations could be improved.

Based on the analysis, it is recommended that regulations pertaining to fence construction in agriculture zones be amended to achieve the intended agrarian character of these areas. Ornate or masonry style fences will be prohibited in agricultural zones outside of the street frontage associated with the principal dwelling. Fencing materials outside of the street frontage shall be agrarian in character consisting of materials and dimensions as defined in this report. This report also proposes amendments to clarify how the vertical height of fences is measured. Proposed Bulletins 43 and 44 (Attachments 3 and 4) have been created to clarify this information for the public.

Amendments to Richmond Zoning Bylaw No. 8500

Staff have identified opportunities to improve regulations to provide more clarity regarding fencing. The following amendments to Richmond Zoning Bylaw No. 8500 are recommended:

Amendments to Section 3.4 – Use and Term Definitions (Applicable to All Zones)

Proposed Amendments		Comments
Height, fence	Means the vertical distance between the average finished site grade measured at a point 1.0 m from both sides of the property line to the top of the fence.	- Replacement of 'average landscape grade' with 'average finished site grade.' Finished site grade is consistent with the language in the zoning bylaw and is defined. - Replacement of 'both sides of the fence' to 'both sides of the property line.' This accounts for fences that may be built 1.0 m or more from the property line. - This amendment will be applicable in all zones.
Agrarian Materials, fence	The following are suitable materials and design for construction of agrarian fencing in the agriculture zones. 1. Wood Post and Rail, minimum spacing between horizontal members shall be 0.3 m; a. Diagonal cross bracing permitted if bracing between posts; 2. Metal post and rail, minimum 0.3 m spacing between horizontal members; 3. Wood Post and welded wire mesh; 4. Steel Post and welded wire mesh; 5. Wood pickets, 8 cm minimum distance between pickets.	- No current definition exists.

Amendments to Section 6.8 – Fence Limitations in Residential Zones

Proposed Amendments		Comments
6.8.3	Fence height shall be measured at the average finished site grade between points measured 1.0 m from both sides of the property line to the top of the fence.	- Replacement of 'measured at the point at which the fence intersects the ground' to reflect the same fence height measurements as prescribed in the definition of 'height, fence.'

Amendments to Section 6.9 – Fence Limitations in All Other Zones

Proposed Amendments		Comments
6.9.1	No fence constructed in the agricultural zones and site specific zones that govern farm businesses shall exceed 2.4 m in height, with the following exceptions: a) Fence height shall not exceed 2.0 m where the fence is located in the side yard of a single detached housing unit; b) Fence height shall not exceed 1.2 m where the fence is located in the front yard (or yard fronting a public way) of a single detached housing unit.	- Amend the height limitations from 2.0 m to 1.2 m in the front yard of a single detached housing unit on agricultural properties, to create consistency of height in the front yard.
6.9.3	Fence height shall be measured at the average finished site grade 1.0 m from both sides of the property line to the top of the fence.	- Addition of the same fence height provision in Section 6.8.3 to regulate fence height in non-residential zones as well.
6.9.4	The following are suitable materials and design for construction of agrarian fencing in the agriculture zones. a) Wood Post and Rail, minimum spacing between horizontal members shall be 0.3 m; i. Diagonal cross bracing permitted if bracing between posts; b) Metal post and rail, minimum 0.3 m spacing between horizontal members; c) Wood Post and welded wire mesh; d) Steel Post and welded wire mesh; e) Wood pickets, 8 cm minimum distance between pickets.	- No current definition exists.

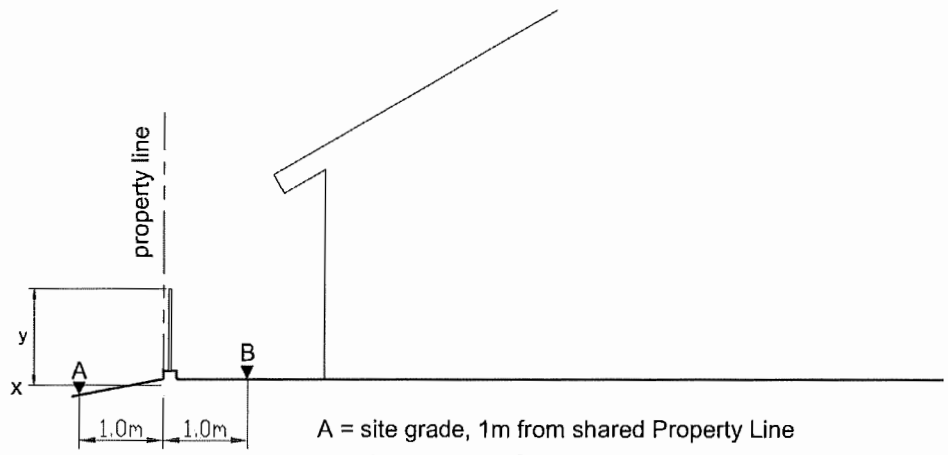
6.9.5	In agricultural zones, a) The fence shall be constructed of materials limited to fence agrarian materials, to the satisfaction of the Director, Building Approvals. b) Any gate providing farm access (even when such gate also provides access to a single detached housing unit) is required to comply with the agrarian materials. c) Masonry fences shall only be permitted along property lines fronting a public road. i. No masonry fence or its above grade components shall exceed 1.2 m in height and 0.3 m in width. Height to include an additional 0.15 m appurtenance allowance for piers spaced no closer than 3.65 m edge to edge. ii. No masonry fence below grade components shall exceed 0.43 m in width of fence footing and 0.8 m square for pier footings. iii. Total masonry fence length shall be further limited to the width of the single detached dwelling fronting the public road plus 6 m.	- Addition of provisions to regulate the materials, height, width, and location of fences in agricultural zones.

Amendments to Section 4 – General Development Regulations (4.12 Projections into Yards in All Zones)

Proposed Amendments		Comments
4.12.1	No building, structure, feature or portion thereof shall be developed, used, occupied, constructed, erected, modified, converted, enlarged, reconstructed, altered, placed, maintained or added to within any required yard except as follows, provided that they meet the provisions of the British Columbia Building Code. The exceptions below do not apply to the 4 m side yard setback in properties with an AG1 agricultural zone when that same setback is used to accommodate farm access."	- Amend the projections into side yards such that they do not apply to farm access roads that are 4 m or less.

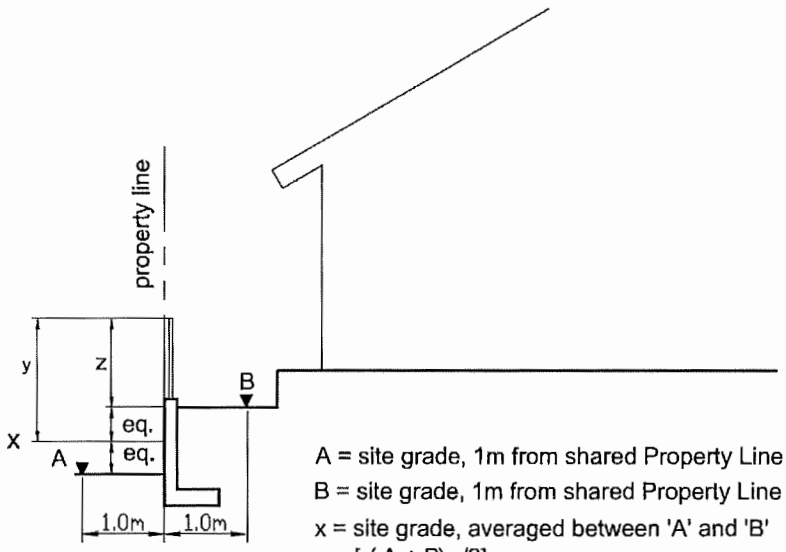
In order to provide clarity, Staff have created the following diagrams to illustrate some aspects of the Amendments. These illustrations will be contained in proposed Bulletins 43 & 44.

Illustrations clarifying the Amendments:



A = site grade, 1m from shared Property Line
B = site grade, 1m from shared Property Line
y = maximum height of fence measured from average grade [(A + B) /2] to the top of the fence

MAXIMUM FENCE HEIGHT SHALL BE MEASURED FROM THE AVERAGE OF GRADES MEASURED 1.0M FROM EACH SIDE OF THE SHARED PROPERTY LINE



A = site grade, 1m from shared Property Line
B = site grade, 1m from shared Property Line
x = site grade, averaged between 'A' and 'B' [(A + B) /2]
y = maximum height of fence, as measuerd from 'x'
z = 1.1 m (42") minimum when B-A is greater than 0.60 m (24")

ADJACENT GRADE ('B') SHALL BE ADJUSTED TO ACCOMMODATE THE MAXIMUM FENCE HEIGHT ('Y') AS PRESCRIBED IN THE ZONING BYLAW

Amendments to Building Regulation Bylaw No. 7230

Current Building Regulation Bylaw No. 7230 does not require a permit for a fence. In order to enforce proposed limitations on the footing sizes as recommended in Richmond Zoning Bylaw No. 8500, and encourage applicants to limit the use of concrete, the following amendment to Building Regulation Bylaw No. 7230 is recommended to require that a building permit be secured for fencing with a concrete foundation.

Amendments to Part SIXTEEN – INTERPRETATION

Proposed Amendments		Changes to Existing Provisions
Structure	Means all or part of a construction, whether fixed to, supported by, sunk into, or located in land, water or airspace, and includes freestanding sign structures over 3.0 m in height and supporting structures for such signs, and includes a sewage holding tank, but excludes landscaping, paving, a fence without concrete foundations, or a retaining wall under 1.0m in height.	- Clarifying that a fence with a concrete foundation requires a permit.

Financial Impact

None.

Conclusion

This report responds to a Council referral to examine regulations for fences and fence materials, particularly masonry. Staff recommend regulating fence heights and materials in agricultural zones. It is recommended that Richmond Zoning Bylaw No. 8500, Amendment Bylaw No. 10122 be introduced and given first reading and that Richmond Building Bylaw No. 7230, Amendment Bylaw No. 10144 be introduced and given first, second and third readings.

Serena Trachta
Manager, Plan Review
(604-204-8515)

John Hopkins
Senior Policy Coordinator
(604-276-4279)

ST:aa

Attachment 1: Excerpt from Richmond Zoning Bylaw No. 8500 of Current Fence Regulations
Attachment 2: Summary Table of Environmental Scan
Attachment 3: Building Bulletin 43 Residential Zones: Fence Heights
Attachment 4: Building Bulletin 44 Agricultural Zones: Fence Heights and Materials

Current Fence Regulations in Richmond Zoning Bylaw No. 8500

Section 3.4 – Use and Term Definitions

Fence Means a structure used as an enclosure or for screening purposes around all or part of a lot.

Height, fence Means the vertical distance between the average landscape grade 1.0 m from both sides of the fence to the top of the fence.

Screen Means a continuous wall, fence, compact evergreen hedge or combination thereof, supplemented with landscape planting, which would effectively screen from view the area that it encloses.

Structure Means a construction of any kind whether fixed to or supported by or sunk into land or water including towers, flag poles, swimming pools, docks, signs and tanks, but does not include areas of hard-surfacing.

Section 6 – Landscaping and Screening

6.2 General

6.2.9 For a lot fronting onto a local arterial road or a major arterial road, a solid masonry or brick fence up to a maximum fence height of 1.2 m is permitted within the required front yard setback area, but any mechanical or manual gate must be located at least 6.0 m from the front lot line.

6.8 Fence Limitations in Residential Zones

6.8.1 No fence constructed in residential zones and site specific zones that include residential uses shall exceed 2.0 m in height. Furthermore, a fence located in the front yard, or any part of a yard between the principal building and the front lot line, shall not exceed 1.2 m in height.

6.8.2 Where a fence is located along a lot line that abuts:

a) a zone other than a residential zone; or

b) a site specific zone that governs residential uses;

the maximum fence height shall be 2.4 m along that lot line only.

6.8.3 Fence height shall be measured at the point at which the fence intersects the ground.

6.8.4 An outdoor play space provided on a property zoned for residential child care use shall be enclosed by a solid fence of a minimum height of 1.2 m but not exceeding a maximum height of 2.0 m. The minimum and maximum heights apply to all fences enclosing the outdoor play space, including fences located in the front yard of the zoned property, notwithstanding Section 6.8.1.

6.8.5 The use of barbed wire, electrified wire, razor wire and barbed tape obstacles as fencing material is prohibited in all the residential zones or site specific zones that govern single detached housing.

6.9 Fence Limitations in All Other Zones

6.9.1 No fence constructed in the agricultural zones and site specific zones that govern farm businesses shall exceed 2.4 m in height. Furthermore, a fence shall not exceed 2.0 m in height where:

a) the fence is located in the front yard and side yard of a single detached housing unit;

b) the fence extends in the front of the foremost portion or portions of the single detached housing unit; and

c) the single detached housing unit is situated on a lot that is used as a farm business, and the lot is assessed as a "farm" under the *Assessment Act*.

6.9.2 No fence constructed in all the other zones shall exceed a maximum height of 2.4 m.

6.9.3 The use of electrified wire as a fencing material is prohibited except where it is used to confine domestic farm animals.

6.9.4 Barbed wire, razor wire and barbed tape obstacle, and electrified wire may only be used as a fencing material:

a) where it is used to confine domestic farm animals; or

b) the purpose of the fence is to limit access to a lawful commercial, industrial, community or institutional use of land, provided that the wire component of the fence is no closer to the ground than 2.0 m.

Current Fence Regulations in Building Regulation Bylaw No. 7230

Part Sixteen: Interpretation

Fence means a structure bounding an area of land designed to limit access to or from the area or to screen the area from view.

Structure means all or part of a construction, whether fixed to, supported by, sunk into, or located in, land, water or airspace, and includes freestanding sign structures over 3.0 m in height and supporting structures for such signs, and includes a sewage holding tank, but excludes landscaping, paving, a fence, or a retaining wall under 1.0 m in height.

Summary Table of Environmental Scan

Municipality	Definition of 'Fence'	Definition of 'Wall'	General Height Limitations in Residential Zones		General Height Limitations in Agricultural Zones		Regulated Fence Materials Other than Wire (Barbed, Razor, Electrified)
			Front Yards	All Other Locations	Front Yards	All Other Locations	
Richmond	A structure used as an enclosure or for screening purposes around all or part of a lot	N/A	1.2 m	2.0 m 2.4 m on lot lines abutting non-residential zones	2.0 m	2.4 m	N/A
Richmond Proposed	No change	No change	No change	No change	1.2 m	2.0 m	Agrarian (Rural Farm) materials required in AG zones.
Burnaby	N/A	N/A	1.07 m	1.8 m	N/A	N/A	Open mesh / chain link
Coquitlam	A structure, not being a building, intended for the purpose of total or partial physical and/or visual separation or enclosure of a property or portion thereof; includes a wall, not being part of a building, intended for the purpose of total or partial physical and/or visual separation or enclosure of a property, does not include retaining wall. Materials used to construct a fence are limited to wood, masonry materials (excluding poured concrete), metal, pre-cast manufactured perforated or decorative concrete blocks or panels, and any combination thereof.	Any building element with a slope of 60 degrees or more to the horizontal; does not include a wall which is utilized as a fence, or a retaining wall.	1.3 m	1.8 m	N/A	3.1 m	See definition of fence.
Delta	A structure used as an enclosure or for screening purposes, and includes gates and walls, but excludes retaining walls and arbors.	N/A	1.2 m	1.8 m 2.4 m on lot lines abutting non-residential zones	1.2 m	2.4 m	Chain link Concrete blocks for retaining walls
City of Langley	N/A	N/A	N/A	2.0 m	N/A	N/A	N/A
Township of Langley	N/A	N/A	1.0 m	2.0 m	N/A	N/A	N/A
Maple Ridge	A structure constructed of materials including wood, masonry, concrete, or metal, intended for the purpose of total or partial physical and/or visual separation or enclosure of a property or portion thereof, and includes chain link fences, however	N/A	1.2 m	2.0 m	1.2 m 3.6 m	2.0 m 3.6 m	N/A

Municipality	Definition of 'Fence'	Definition of 'Wall'	General Height Limitations in Residential Zones		General Height Limitations in Agricultural Zones		Regulated Fence Materials Other than Wire (Barbed, Razor, Electrified)
			Front Yards	All Other Locations	Front Yards	All Other Locations	
	does not include retaining walls.						
New Westminster	*Closed fence*: one that has more than fifty percent (50%) of its area closed.	N/A	1.22 m	1.83 m	N/A	N/A	Open mesh / chain link
City of North Vancouver	*Structure, accessory*: a structure used for an accessory use, including fences, radio and television antennae and satellite dishes.	N/A	1.22 m	1.829 m	-	-	N/A
District of North Vancouver	N/A	N/A	1.8 m	2.4 m	-	-	N/A
Pitt Meadows	A structure used as an enclosure or as a visual barrier around all or part of a lot	N/A	1.2 m	1.8 m 2.4 m on lot lines abutting agricultural or industrial zones	1.8 m	2.4 m	N/A
Port Coquitlam	A vertical structure used for enclosure or screening where the thickness is equal to or less than 8 cm (0.26 ft) excluding top and bottom rails and posts.	A vertical structure used for enclosure, screening or soil retention constructed of brick, masonry, stone, or timbers or any other material where the thickness of the wall is greater than 8 cm (0.26 ft).	1.2 m	2.5 m	1.2 m	N/A	See definition of wall.
Surrey	N/A	N/A	1.2 m	1.8 m	N/A	N/A	For properties on railway land, stones, cement, bricks, similar durable materials, chain link, or combination thereof.
Vancouver	Includes arbors, archways, boundary fences, gates, pergolas, screens, trellises, walls and similar structures.	N/A	1.2 m	1.9 m	N/A	N/A	Permitted: wood, brick, concrete block, metal
West Vancouver	A vertical structure used as an enclosure or a screen of all or part of a site, not exceeding 0.08 metre in thickness, excluding posts and rails, but shall not include a garden wall.	A vertical structure used as an enclosure or screening about all or part of a site constructed of concrete masonry, timbers, rock, or any other material where the thickness is more than 0.08 metre.	1.2 m 1.8 m	1.8 m 2.4 m	-	-	See definition of wall.

City of Richmond

Bulletin
Building Approvals Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

www.richmond.ca

This information is provided for clarification purposes only and is not in substitution of any applicable City Bylaws or Provincial or Federal Codes or laws. In the case of any contradictions, legislative Codes, laws or Bylaws take precedence. You must satisfy yourself that any existing or proposed construction or other works complies with such Bylaws, Codes or other laws.

Residential Zones: Fence Heights	No.: BUILDING-43 Last Revised: 2020/02/06 Date Created: 2020/02/06

This bulletin is to inform Owners and Builders of the height regulations for fences in residential zones recently adopted in Richmond Zoning Bylaw No. 8500, Amendment Bylaw No. 10122 and Building Regulation Bylaw No. 7230, Amendment Bylaw No. 10144.

Summary

- Definition of fence height has been clarified to identify measurement parameters.
- Maximum fence heights in residential zones have been clarified.

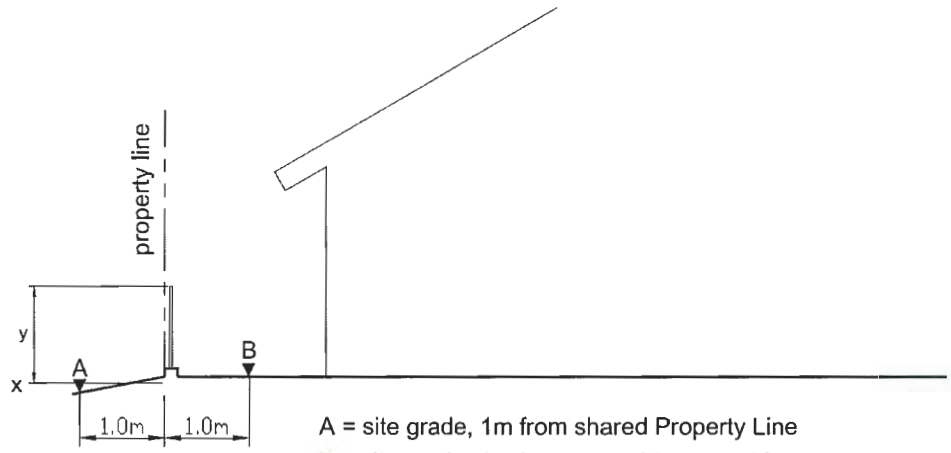
Fence Height Requirements

- The maximum fence height of 2.0 metres (m) is permitted for fences constructed in residential zones and site specific zones that include residential uses. (Richmond Zoning Bylaw No.8500:6.8.1)
    - A maximum fence height of 1.2 m is permitted for fences located in the front yard or between the principal dwelling unit and the front property line or public road.
    - A maximum fence height of 1.83 m is permitted for fences when located elsewhere within a required yard. (Richmond Zoning Bylaw No. 8500:Residential Zones)
- The use of barbed wire, electrified wire, razor wire, and barbed tape obstacles as fencing material is prohibited in all residential zones and in site specific zones that govern single detached housing. (Richmond Zoning Bylaw No. 8500:6.8.5)
- A building permit is required for any fence construction with concrete foundations. (Building Regulation Bylaw No. 7230:16.1)

Measuring Fence Height

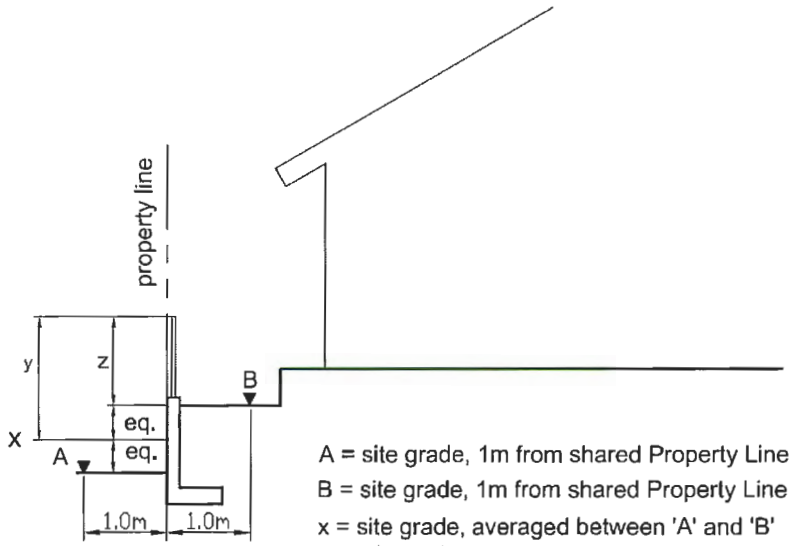
- Fence height is determined by measuring the vertical distance between the average finished site grade, measured 1.0 m from both sides of the property line, to the top of the fence. (Richmond Zoning Bylaw No. 8500:6.8.3)
- Grading must be strategically managed to avoid impact with the maximum fence height limit shown.
- Please refer to the diagrams attached.

Should you have any questions, comments, or suggestions concerning this bulletin, please reference the Bulletin number and email building@richmond.ca or call the Building Approvals General Inquiries line at **604-276-4118**.



A = site grade, 1m from shared Property Line
B = site grade, 1m from shared Property Line
y = maximum height of fence measured from average grade [(A + B) /2] to the top of the fence

MAXIMUM FENCE HEIGHT SHALL BE MEASURED FROM THE AVERAGE OF GRADES MEASURED 1.0M FROM EACH SIDE OF THE SHARED PROPERTY LINE



A = site grade, 1m from shared Property Line
B = site grade, 1m from shared Property Line
x = site grade, averaged between 'A' and 'B' [(A + B) /2]
y = maximum height of fence, as measuerd from 'x'
z = 1.1 m (42") minimum when B-A is greater than 0.60 m (24")

ADJACENT GRADE ('B') SHALL BE ADJUSTED TO ACCOMMODATE THE MAXIMUM FENCE HEIGHT ('Y') AS PRESCRIBED IN THE ZONING BYLAW

References

Please see Bulletin BUILDING-44 for regulations regarding fences in agricultural zones.

City of Richmond Zoning Bylaw, Landscaping and Screening:
https://www.richmond.ca/__shared/assets/LandscapingScreening24225.pdf

Should you have any questions, comments, or suggestions concerning this bulletin, please reference the Bulletin number and email building@richmond.ca or call the Building Approvals General Inquiries line at **604-276-4118**.

City of Richmond

Bulletin
Building Approvals Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

www.richmond.ca

This information is provided for clarification purposes only and is not in substitution of any applicable City Bylaws or Provincial or Federal Codes or laws. In the case of any contradictions, legislative Codes, laws or Bylaws take precedence. You must satisfy yourself that any existing or proposed construction or other works complies with such Bylaws, Codes or other laws.

Agricultural Zones: Fence Heights and Materials	No.: BUILDING-44 Last Revised: 2020/02/06 Date Created: 2020/02/06

This bulletin is to inform Owners and Builders of the fence height and material regulations in agricultural zones recently adopted in Richmond Zoning Bylaw No. 8500, Amendment Bylaw No. 10122 and Building Regulations Bylaw No. 7230, and Amendment Bylaw No. 10144.

Summary

- Definition of fence height has been clarified to identify measurement parameters.
- Maximum fence heights in agricultural zones have been clarified.
- Acceptable materials for use in agricultural zones have been clarified in order to promote and maintain the agrarian character.

General Requirements

- The maximum fence height of 2.4 metres(m) is permitted for fences constructed in in agricultural zones and site specific zones that govern farm businesses. (Richmond Zoning Bylaw No. 8500:6.9.1)
    - A maximum fence height of 1.2 m is permitted for fences located in the front yard or between the single detached housing unit and the front property line or public road.
    - A maximum fence height of 2.0 m is permitted for fences located in the side yard or between the single detached housing unit and the side property line.
- The use of barbed wire, electrified wire, razor wire, and barbed tape obstacles as fencing material is prohibited in all residential zones and in site specific zones that govern single detached housing. (Richmond Zoning Bylaw No. 8500: 6.8.5)
- A building permit is required for any fence construction with concrete foundations. (Building Regulation Bylaw No. 7230:16.1)

Measuring Fence Height

- Fence height is determined by measuring the vertical distance between the average finished site grade, measured 1.0 m from both sides of the property line, to the top of the fence. (Richmond Zoning Bylaw No. 8500:6.9.3)
- Grading must be strategically managed to avoid impact with the maximum fence height limit shown. See Building Bulletin-43 for additional information.

Material Regulations

- The following are suitable materials and design for construction of agrarian fencing in the Agriculture zones. (Richmond Zoning Bylaw No. 8500:6.9.4)
    - Wood Post and Rail, minimum spacing between horizontal members shall be 0.3 m;

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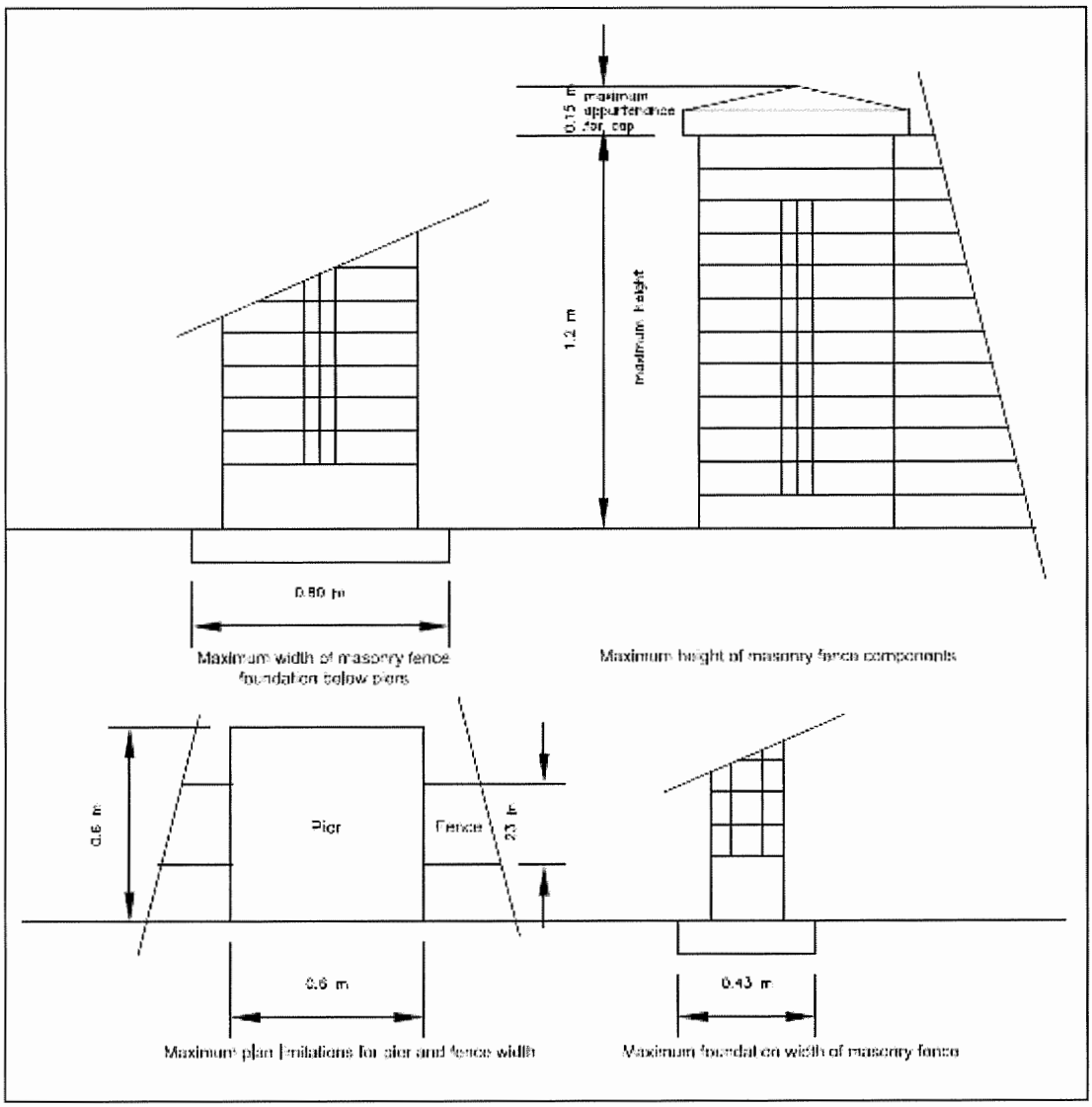
        - Diagonal cross bracing permitted if bracing between posts;
    - Metal post and rail, minimum 0.3 m spacing between horizontal members;
    - Wood Post and welded wire mesh;
    - Steel Post and welded wire mesh; and/or
    - Wood pickets, 8 cm minimum distance between pickets.
- Fences in agriculture zones shall be constructed of materials limited to fence agrarian materials, except as noted below (Zoning Bylaw 8500:6.9.5):
    - Masonry and concrete fences shall only be permitted along property lines fronting a public road.
        - Masonry and concrete fences are defined as fences composed either partially or entirely of stone, brick, concrete, concrete block, or other similar building materials.
    - No masonry or concrete fence or its components shall exceed 1.2 m in height.
        - An appurtenance allowance of 0.15 m for pier caps is permitted provided the piers are spaced no closer than 0.365 m edge to edge.
        - The width of the masonry fence shall not exceed 0.3 m in width.
        - Footings shall limited as shown in the attached diagrams.
    - Total masonry fence length shall be further limited to the width of the single detached dwelling fronting the public road plus 6 m.
        - Beyond that length, fences shall be constructed of materials limited to agrarian materials.
- Please refer to the diagrams attached for additional information.



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City of Richmond

Bylaw 10122

Richmond Zoning Bylaw No. 8500
Amendment Bylaw No. 10122 (Fence Regulations)

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw No. 8500, as amended, is further amended at Section 3.4 [Use and Term Definitions] by deleting the definitions of "Height, fence" in its entirety and replacing it with the following:

   "**Height, fence** means the vertical distance between the average **finished site grade** measured at a point 1.0 m from both sides of the **property line** to the top of the **fence.**"

2. Richmond Zoning Bylaw No. 8500, as amended, is further amended at Section 3.4 [Use and Term Definitions] by inserting the following definition in alphabetical order:

   "**Agrarian materials, fence** The following are suitable materials and design for the construction of agrarian fencing in **agriculture zones**.

   1. Wood Post and Rail, minimum spacing between horizontal members shall be 0.3 m.

      a. Diagonal cross bracing permitted if bracing between posts.

   2. Metal post and rail, minimum 0.3 m spacing between horizontal members.

   3. Wood Post and welded wire mesh.

   4. Steel Post and welded wire mesh.

   5. Wood pickets, 8 cm minimum distance between pickets."

3. Richmond Zoning Bylaw No. 8500, as amended, is further amended at Section 6.8 [Fence Limitations in Residential Zones] by deleting Section 6.8.3 in its entirety and replacing it with the following:

   "6.8.3 **Fence height** shall be measured at the average **finished site grade** measured at a point 1.0 m from both sides of the **property line** to the top of the **fence**."

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4. Richmond Zoning Bylaw No. 8500, as amended, is further amended at Section 6.9 [Fence Limitations in All Other Zones] by deleting Section 6.9.1 in its entirety and replacing it with the following:

"6.9.1 No **fence** constructed in the **agricultural zones** and **site specific zones** that govern **farm businesses** shall exceed 2.4 m in **height**. Furthermore, a fence shall not:

a) exceed 2.0 m in **height** where the **fence** is located in the **exterior side yards** of a **single detached housing unit**; or

b) exceed 1.2 m in **height** where the **fence** is located in the **front yard** (or yard fronting a public street) of a **single detached housing unit**."

5. Richmond Zoning Bylaw No. 8500, as amended, is further amended at Section 6.9 [Fence Limitations in Residential Zones] by deleting Section 6.9.3 in its entirety and replacing it with the following:

"6.9.3 **Fence height** shall be measured at the average **finished site grade** measured at a point 1.0 m from both sides of the **property line** to the top of the **fence**."

6. Richmond Zoning Bylaw No. 8500, as amended, is further amended at Section 6.9 [Fence Limitations in All Other Zones] by inserting the following, in numerical order, and adjusting the numbers thereafter:

"6.9.4 The following are suitable **fence agrarian materials** for the design and construction of fencing in **agriculture zones**.

a) Wood Post and Rail, minimum spacing between horizontal members shall be 0.3 m.

i. Diagonal cross bracing permitted if bracing between posts.

b) Metal post and rail, minimum 0.3 m spacing between horizontal members.

c) Wood Post and welded wire mesh.

d) Steel Post and welded wire mesh.

e) Wood pickets, 8 cm minimum distance between pickets.

6.9.5 In **agricultural zones**:

a) **Fences** shall be constructed of materials limited to farm **agrarian materials** for fencing to the satisfaction of the Director, Building Approvals.

b) Any gate providing farm **access** (even if also serving the **single detached housing unit**) is required to comply with the **agrarian materials**.

c) Masonry **fences** shall only be permitted along **property lines** fronting a public road.

a. No masonry **fence** or its above **grade** components shall exceed 1.2 m in **height** and 0.3 m in width. **Height** may increase an additional 0.15 m as an appurtenance allowance for piers spaced no closer than 3.65 m edge to edge.

b. No masonry **fence** below **grade** components shall exceed 0.43 m in width for **fence** footing and 0.8 m square for pier footings.

c. Total masonry **fence** length shall be further limited to the width of the house fronting the public road plus 6 m."

7. Richmond Zoning Bylaw No. 8500, as amended, is further amended as Section 4.12.1 [Projections into Yards in All Zones] by deleting Section 4.12.1 in its entirety and replacing it with the following:

"4.12.1

a) No **building**, **structure**, feature or portion thereof shall be developed, used, occupied, constructed, erected, modified, converted, enlarged, reconstructed, altered, placed, maintained or added to within any required **yard** except as follows, provided that they meet the provisions of the British Columbia Building Code. The exceptions below do not apply to the 4 m **side yard setback** in properties with an AG1 **agricultural zone** when that same **setback** is used to accommodate farm **access**."

8. This Bylaw is cited as **"Richmond Zoning Bylaw No. 8500, Amendment Bylaw No. 10122".**

FIRST READING	APR 14 2020
PUBLIC HEARING	
SECOND READING	
THIRD READING	
ADOPTED	

CITY OF RICHMOND
APPROVED by
APPROVED by Director or Solicitor

MAYOR

CORPORATE OFFICER

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City of Richmond

Bylaw 10144

Building Regulation Bylaw No. 7230, Amendment Bylaw No. 10144

The Council of the City of Richmond enacts as follows:

1. Building Regulation Bylaw No. 7230, as amended, is further amended at Section 16.1 by deleting the definition of Structure and replacing it with the following:

   "**Structure** means all or part of a **construction**, whether fixed to, supported by, sunk into, or located in, land, water or airspace, and includes freestanding sign structures over 3.0 m in height and supporting structures for such signs, and includes a **sewage holding tank**, but excludes landscaping, paving, a **fence** without concrete foundations, or a retaining wall under 1.0 m in height.".

2. This Bylaw is cited as **"Building Regulation Bylaw No. 7230, Amendment Bylaw No. 10144"**.

FIRST READING	APR 14 2020
SECOND READING	APR 14 2020
THIRD READING	APR 14 2020
ADOPTED	

CITY OF RICHMOND
APPROVED for content by originating Division
SG
APPROVED for legality by Solicitor
BRB

MAYOR

CORPORATE OFFICER

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