



Public Notice is hereby given of a Regular Council Meeting for Public Hearings being held on:

Monday, November 16, 2015 – 7 p.m.

**Council Chambers, 1st Floor
Richmond City Hall
6911 No. 3 Road
Richmond, BC V6Y 2C1**

OPENING STATEMENT

Page

1. **RICHMOND ZONING BYLAW 8500, AMENDMENT BYLAW 9287 (RZ 14-670779)**
(File Ref. No. 12-8060-20-009287; RZ 14-670779) (REDMS No. 4691916)

PH-4

See Page PH-4 for full report

Location: 10551 No. 1 Road

Applicant: 0814948 BC Ltd.

Purpose: To rezone the subject property from “Single Detached (RS1/E)” to “Coach Houses (RCH1)”, to permit the property to be subdivided to create two (2) lots, each with a principal dwelling and an accessory coach house above a detached garage, with vehicle access from the existing rear lane.

First Reading: October 13, 2015

Order of Business:

1. Presentation from the applicant.
2. Acknowledgement of written submissions received by the City Clerk since first reading.
3. Submissions from the floor.

Page

Council Consideration:

1. Action on second and third readings of Richmond Zoning Bylaw 8500, Amendment Bylaw 9287.

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2A. **PROPOSED AMENDMENT TO SINGLE-FAMILY LOT SIZE POLICY 5463 (SECTION 13 BLOCK 4 NORTH RANGE 7 WEST)**

PH-24

See Page PH-24 for full report

Recommendation:

That Single-Family Lot Size Policy 5463 in Section 13-4-7, adopted by Council on February 19, 1996, be amended to exclude four (4) properties fronting Railway Avenue with existing rear lane access north of Linfield Gate from the Lot Size Policy.

2B. **RICHMOND ZONING BYLAW 8500, AMENDMENT BYLAW 9292 (RZ 15-691744)**

(File Ref. No. 12-8060-20-9292; RZ 15-691744) (REDMS No. 4737969)

Location: 7220 Railway Avenue

Applicant: Maryem Ahbib

Purpose: To rezone the subject property from “Single Detached (RS1/E)” to “Compact Single Detached (RC2)”, to permit a subdivision to create two (2) lots with vehicle access from the rear lane.

First Reading: October 26, 2015

Order of Business:

1. Presentation from the applicant.
2. Acknowledgement of written submissions received by the City Clerk since first reading.
3. Submissions from the floor.

Council Consideration:

1. Action on Single Family Lot Size Policy 5463 (Section 13 Block 4 North Range 7 West).

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2. Action on second and third readings of Richmond Zoning Bylaw 8500, Amendment Bylaw 9292.

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Page

3. **RICHMOND ZONING BYLAW 8500, AMENDMENT BYLAW 9487 (ZT 15-695231)**

(File Ref. No. 12-8060-20-009487; ZT 15-695231) (REDMS No. 4734828)

PH-48

See Page PH-48 for full report

Location: 6611, 6622, 6633*, 6655, 6688*, 6699*, 6811, 6877*, and 6899 Pearson Way (*Additional addresses assigned for future development)

Applicant: Oval 8 Holdings Ltd.

Purpose: To amend the “High Rise Apartment and Olympic Oval (ZMU4) – Oval Village (City Centre)” zone, a site-specific zone (the portion east of Hollybridge Way) applicable to the subject property, to permit:

- consolidation of three existing residential lots to create one large lot;
- consolidation and expansion of existing Public Rights of Passage right-of-way areas to create a larger central park;
- construction of a new “Temporary Public Open Space” and related dike and riverfront park improvements until the proposed central public open space is constructed; and
- transfer of a fee-simple lot to City ownership for the developer’s future construction of a City-owned affordable child care facility.

First Reading: October 26, 2015

Order of Business:

1. Presentation from the applicant.
2. Acknowledgement of written submissions received by the City Clerk since first reading.
3. Submissions from the floor.

Council Consideration:

1. Action on second and third readings of Richmond Zoning Bylaw 8500, Amendment Bylaw 9487.

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ADJOURNMENT

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City of Richmond

Report to Committee Planning and Development Division

To: Planning Committee
From: Wayne Craig
Director of Development

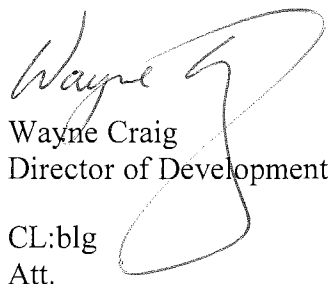
Date: September 24, 2015

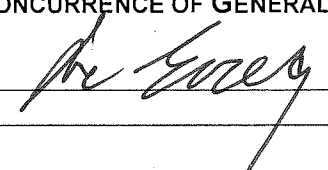
File: RZ 14-670779

Re: Application by 0814948 BC Ltd. for Rezoning at 10551 No. 1 Road from Single Detached (RS1/E) to Coach Houses (RCH1)

Staff Recommendation

That Richmond Zoning Bylaw 8500, Amendment Bylaw 9287, for the rezoning of 10551 No. 1 Road from "Single Detached (RS1/E)" to "Coach Houses (RCH1)", be introduced and given first reading.


Wayne Craig
Director of Development
CL:blg
Att.

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Affordable Housing	<input checked="checked" type="checkbox"/>	

Staff Report

Origin

0814948 BC Ltd. has applied to the City of Richmond for permission to rezone the property at 10551 No. 1 Road from the “Single Detached (RS1/E)” zone to the “Coach Houses (RCH1)” zone, to permit the property to be subdivided to create two (2) lots, each with a principal single-detached dwelling and an accessory coach house above a detached garage, with vehicle access from the existing rear lane (Attachment 1). A survey of the subject site showing the proposed subdivision plan is included in Attachment 2.

Findings of Fact

A Development Application Data Sheet providing details about the development proposal is attached (Attachment 3)

Surrounding Development

The subject site currently contains an older character single-family dwelling. Development immediately surrounding the subject site is as follows:

- To the North, are compact lots zoned “Coach Houses (RCH)”, each containing a principal single-family dwelling and an accessory coach house building.
- To the East, directly across No. 1 Road, are single-family dwellings on lots under Land Use Contract 148.
- To the South, is a single-family dwelling on a lot zoned “Single Detached (RS1/E)”.
- To the West, across the rear lane that parallels No. 1 Road, is a single-family dwelling on a lot zoned “Single Detached (RS1/E)” that fronts Sorrel Drive.

Related Policies & Studies

Official Community Plan (OCP) and Steveston Area Plan Designations

The OCP land use designation for the subject site is “Neighbourhood Residential”. The Steveston Area Plan designation for the subject site is “Single-Family” (Attachment 4). This redevelopment proposal is consistent with these designations.

Arterial Road Policy

The Arterial Road Policy identifies the subject site for redevelopment to compact lots or coach houses, with rear lane access.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on Title is required prior to final adoption of the rezoning bylaw.

Public Input

Staff has not received any comments from the public about the development proposal in response to the placement of the rezoning sign on the property.

Analysis**Site Planning, Transportation Requirements and Architectural Character**

The applicant's conceptual development plans are included in Attachment 5, which address the significant staff comments identified as part of the rezoning application review process.

The proposed site plan involves a principal dwelling on the east side and an accessory coach house above a detached garage on the west side of each lot proposed. Private open space is proposed in the rear yard in between the principal dwelling and the coach house on each lot proposed, which exceeds the minimum zoning requirement. Private open space for the exclusive use of the coach house is also proposed in the form of a balcony facing the existing rear lane.

Pedestrian access and circulation on-site is proposed to the main dwelling and coach house via a permeable pathway from No. 1 Road, as well as from the rear lane.

Vehicle access to the subject site is required to be from the rear lane only, with no access permitted to No. 1 Road in accordance with Bylaw 7222.

On-site parking is proposed in a garage in accordance with the Zoning Bylaw, and consists of two (2) parking spaces for the principal dwelling, provided in a tandem arrangement, along with one (1) parking space to the side for the coach house, with vehicle access to the site from the existing rear lane. Prior to final adoption of the rezoning bylaw, the applicant must register a restrictive covenant on title to prohibit the conversion of the parking area in the garage into habitable space.

The proposed architectural elevation plans include articulation of the coach house building, differentiation of exterior cladding materials, and appropriate window placement, thereby avoiding blank facades, providing some visual interest, and minimizing overlook onto adjacent properties.

On-site garbage and recycling is proposed to be located in the rear yard, and is setback a minimum of 1.5 m from the rear lot line, in accordance with the RCH1 zone. Screening of on-site garbage and recycling will be reviewed upon receipt of the required Landscape Plan for the site prior to final adoption of the rezoning bylaw.

Prior to final adoption of the rezoning bylaw, minor revisions will be made to the conceptual plans included in Attachment 5. Furthermore, the applicant must register restrictive covenants on title to ensure that:

- The coach house cannot be stratified; and
- The Building Permit application and ensuing development at the site is generally consistent with the plans included in Attachment 5. The Building Permit application

process includes coordination between Building Approvals and Planning staff to ensure that the covenant is adhered to.

Trees and Landscaping

A Certified Arborist's Report was submitted by the applicant, which identifies on and off-site tree species, assesses their structure and condition, and provides recommendations on tree retention and removal relative to the proposed development. The Report assesses:

- Five (5) bylaw-sized trees on the subject property; and,
- Three (3) trees located on the neighbouring property to the south at 10571 No. 1 Road.

The City's Tree Preservation Coordinator has reviewed the Arborist's Report, conducted visual tree assessment, and provides the following comments:

- Three (3) trees on-site have all been severely topped, are visibly dying, and should be removed and replaced (Trees # 999, 1000, 1001).
- Two (2) trees on-site are both in good condition, however, are located within the building envelopes for the coach houses (Trees # 997 and 998).
- Off-site Trees A and B on the adjacent property to the south (10571 No. 1 Road) are required to be retained and protected.
- Off-site Tree C on the adjacent property to the south (10571 No. 1 Road) is in marginal condition and is recommended for removal due to its proximity the building on the proposed south lot at the subject site, subject to neighbouring property owner authorization via a Tree Removal Permit. If the neighbouring property owner authorization is not obtained, Tree C must be protected.

Tree Retention

A total of three (3) off-site trees are to be retained and protected on the adjacent site to the south at 10571 No. 1 Road, and all five (5) trees on the subject site are proposed for removal. The proposed Tree Management Drawing is shown in Attachment 6.

To ensure protection of the off-site trees (Trees A, B, and C), the applicant must complete the following items prior to final adoption of the rezoning bylaw:

- Submit a contract with a Certified Arborist for supervision of all works conducted within close proximity to tree protection zones. The contract must include the scope of work, including the number of monitoring inspections at specified stages of construction, any special measures to ensure tree protection, and a provision for the Arborist to submit a post-construction impact assessment report to the City for review.

Prior to demolition of the existing dwelling on the subject site, the applicant is required to install tree protection fencing around the off-site trees to be retained. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin TREE-03 prior to any works being conducted on-site, and must remain in place until construction and landscaping on-site is completed.

Tree Replacement

All five (5) trees on-site are proposed to be removed and replaced. The OCP tree replacement ratio of 2:1 requires that 10 replacement trees be planted and maintained on the subject site. Due to the compact size of the proposed lots, staff recommends that a total of six (6) replacement trees be planted and maintained on the proposed lots at development stage, and that a cash-in-lieu contribution be submitted to the City's Tree Compensation Fund in the amount of \$2,000 for the balance of required replacement trees not planted (\$500/tree). The six (6) replacement trees must be of the following minimum sizes and be located as follows:

- Two (2) 6 cm deciduous or 3.5 m coniferous trees in the front yards.
- Two (2) 8 cm deciduous or 4 m high coniferous trees in the front yards.
- Two (2) 5 m high conifer trees in the rear yards (with a minimum value of \$3,000 each). Suitable trees for the rear yards, as recommended by the project Arborist and the City's Tree Protection Department staff are Serbian Spruce.

To ensure that the required replacement trees are planted and maintained, and that the front and rear yards of the subject site are enhanced, the applicant is required to submit a Landscape Plan prepared by a Registered Landscape Architect, along with a Security in the amount of 100% of a cost estimate for the works provided by the Landscape Architect. The Landscape Plan must respond to the guidelines of the Arterial Road Policy and must comply with the landscaping requirements of the RCH1 zone. The Landscape Plan, Cost Estimate, and Security are required to be submitted prior to final adoption of the rezoning bylaw. The Security will be reduced by 90% after construction and landscaping at the subject site is completed and a landscaping inspection by City staff has been passed. The City will retain 10% of the Security for a one-year maintenance period to ensure that the landscaping survives.

Existing Legal Encumbrances

There is an existing Statutory Right-of-Way (SRW) registered on Title of the subject site for utilities. The applicant is aware that no encroachment into the SRW is permitted.

Affordable Housing Strategy

The Affordable Housing Strategy requires a secondary suite or coach house on 50% of new lots, or a cash-in-lieu contribution of \$1.00/ft² of total building area toward the City's Affordable Housing Reserve Fund for single-family rezoning applications.

This proposal to permit a subdivision to create two (2) lots, each with a principal single detached dwelling and accessory coach house above a detached garage, conforms to the Affordable Housing Strategy.

Site Servicing and Frontage Improvements

There are no servicing concerns with the proposed rezoning. At subdivision and development stage, the applicant is required to:

- Pay Development Cost Charges (City and GVS & DD), School Site Acquisition Charge, Address Assignment Fees, and Servicing Costs.
- Submit a cash-in-lieu contribution for future lane improvements (e.g., full lane width asphalt pavement, lane drainage, roll curb and gutter, and lighting).
- Complete the required servicing works and frontage improvements as described in Attachment 7.

Financial Impact

This rezoning application results in an insignificant Operational Budget Impact (OBI) for off-site City infrastructure (such as roadworks, waterworks, storm sewers, sanitary sewers, street lights, street trees and traffic signals).

Conclusion

The purpose of this rezoning application is to rezone the property at 10551 No. 1 Road from the "Single Detached (RS1/E)" zone to the "Coach Houses (RCH1)" zone, to permit the property to be subdivided to create two (2) lots, each with a principal single-detached dwelling and accessory coach house over a detached garage, with vehicle access to the existing rear lane.

This rezoning application complies with the land use designations and applicable policies contained within the OCP for the subject site.

The list of rezoning considerations associated with this application is included in Attachment 7, which has been agreed to by the applicant (signed concurrence on file).

On this basis, it is recommended that Zoning Bylaw 8500, Amendment Bylaw 9287 be introduced and given first reading.



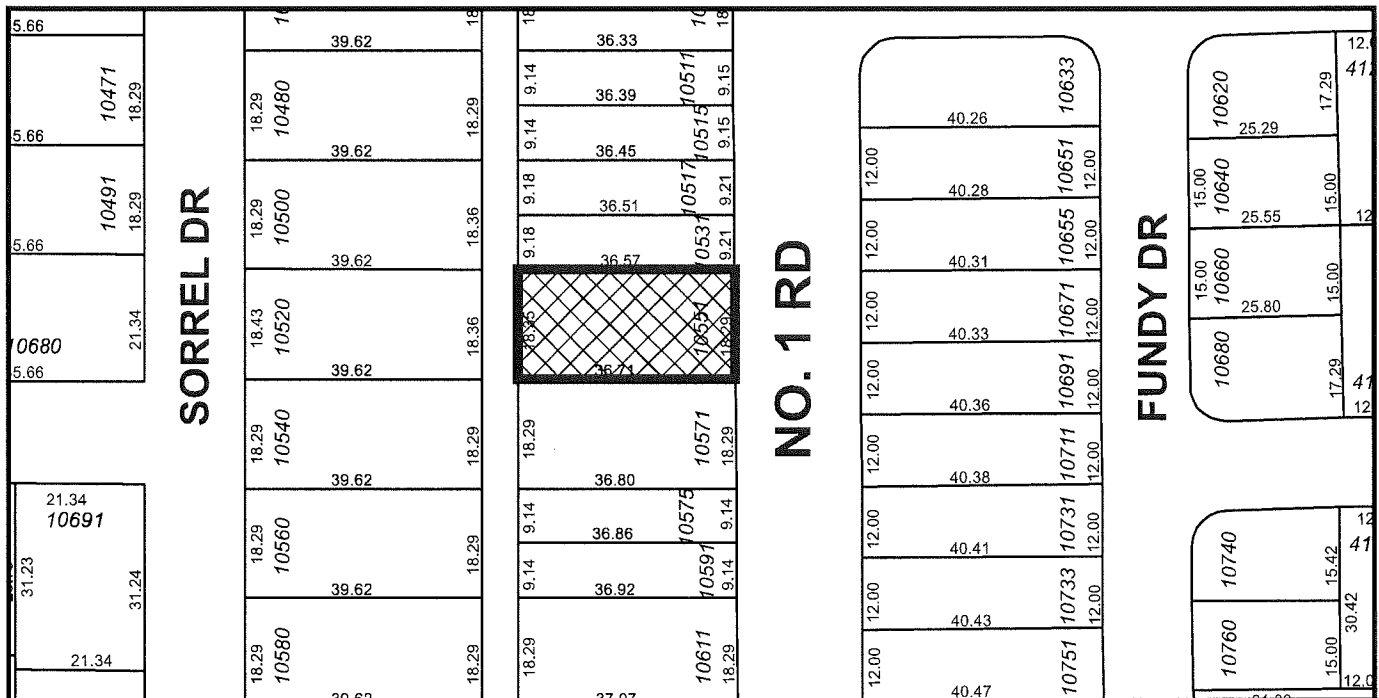
Cynthia Lussier
Planning Technician
(604-276-4108)

CL:blg

- Attachment 1: Location Map/Aerial Photo
- Attachment 2: Site Survey and Proposed Subdivision Plan
- Attachment 3: Development Application Data Sheet
- Attachment 4: Steveston Area Plan Land Use Map
- Attachment 5: Conceptual Development Plans
- Attachment 6: Proposed Tree Management Drawing
- Attachment 7: Rezoning Considerations



City of
Richmond



RZ 14-670779

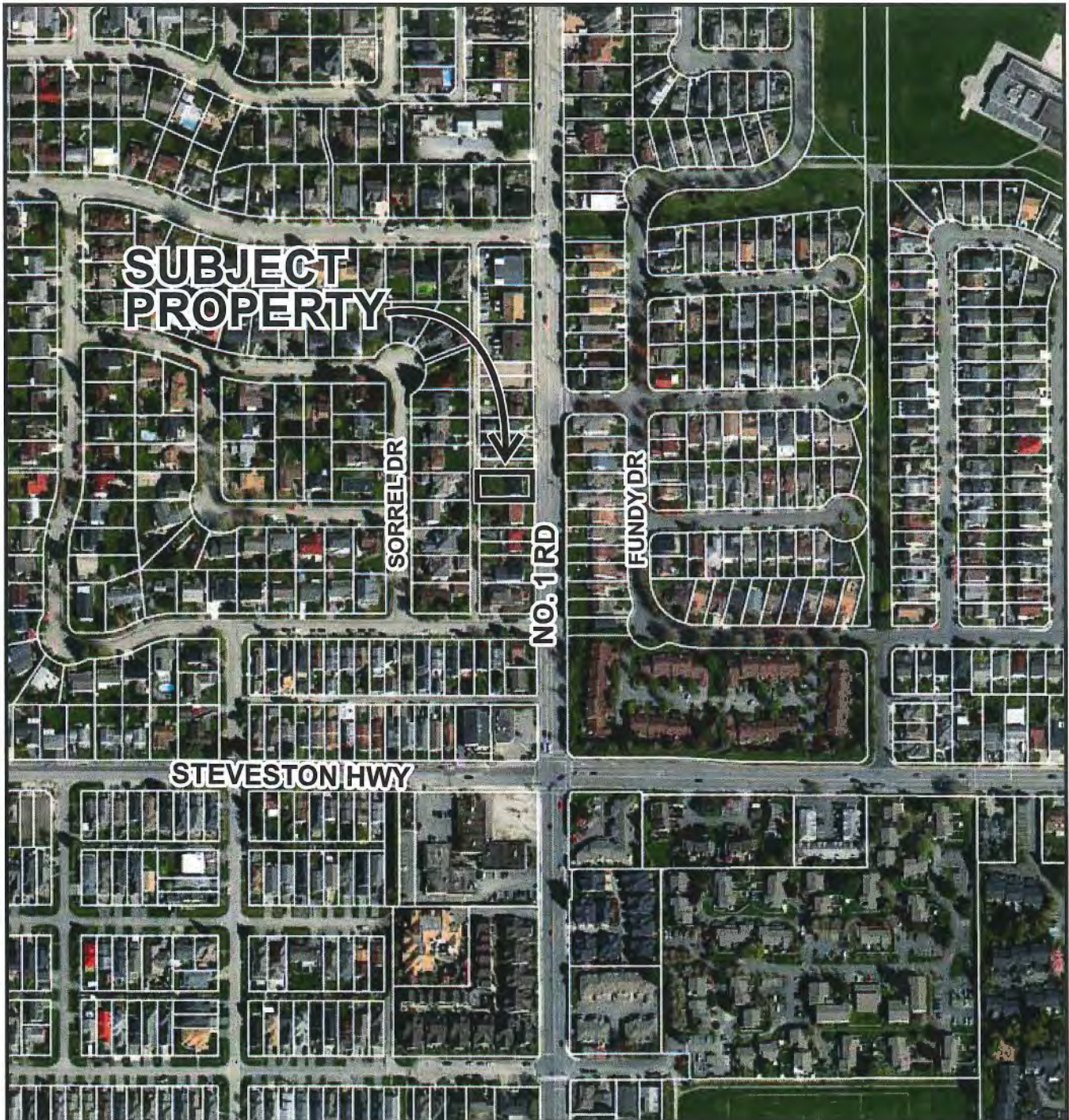
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Revision Date:

Note: Dimensions are in METRES



City of
Richmond



RZ 14-670779

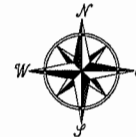
Original Date: 09/08/14

Revision Date:

Note: Dimensions are in METRES

TOPOGRAPHIC SURVEY AND PROPOSED SUBDIVISION OF LOT 593 SECTION 34 BLOCK 4 NORTH RANGE 7 WEST NEW WESTMINSTER DISTRICT PLAN 42890

#10551 NO. 1 ROAD,
RICHMOND, B.C.
P.I.D 008-594-298



SCALE: 1:200

0 5 10 15
ALL DISTANCES ARE IN METRES AND DECIMALS
THEREOF UNLESS OTHERWISE INDICATED

LEGEND:

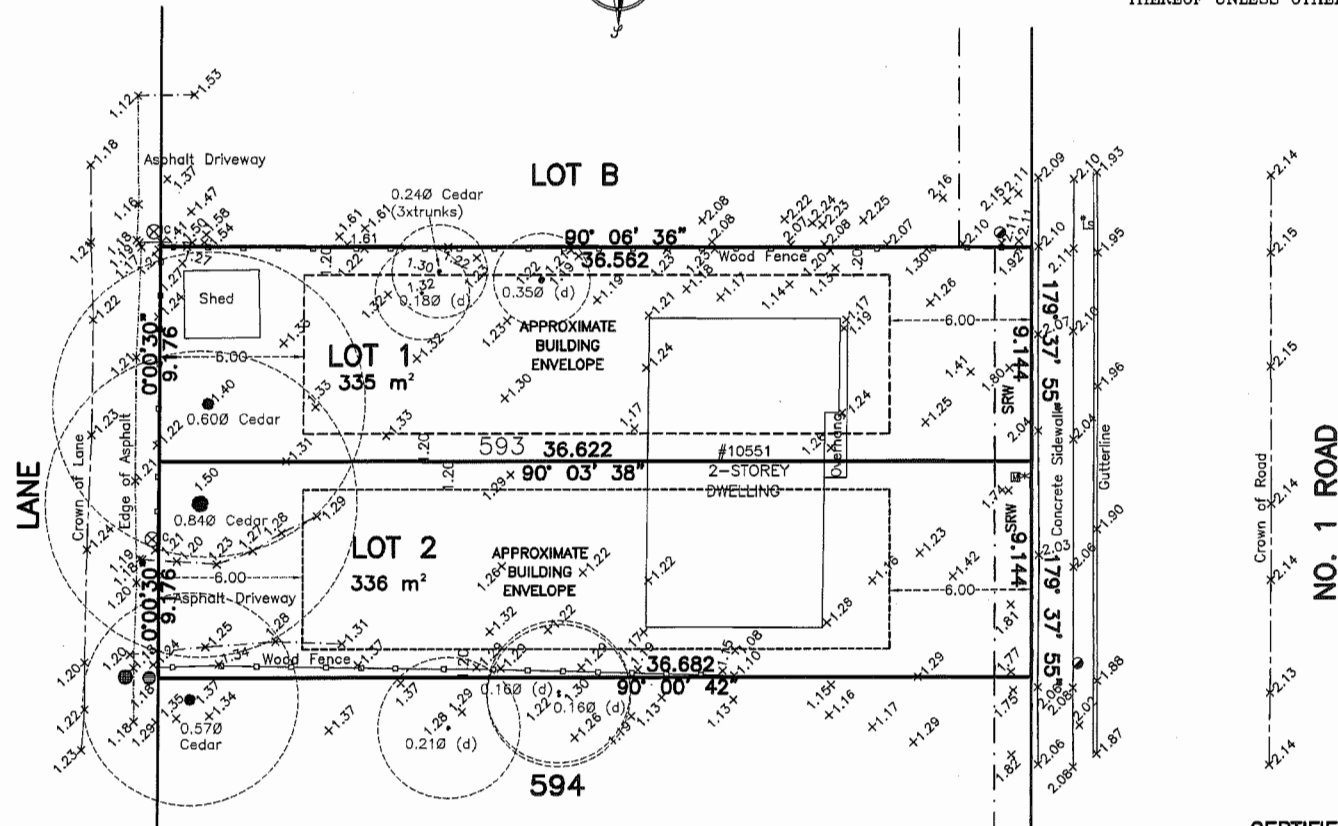
- (d) denotes deciduous
- denotes power pole
- denotes round catch basin
- ⊙ denotes manhole
- ⊗ denotes inspection chamber
- ⊞ denotes water meter
- ⊞ denotes water valve
- CO denotes cleanout
- LS denotes lamp standard

NOTE:

Elevations shown are based on City of
Richmond HPN Benchmark network.
Benchmark: HPN #231,
Control Monument 19713
Located at E side of Dyke, W side
lagoon opp. Mariner Park. Prot. By
4X4 posts
Elevation = 3.419 metres

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J. C. Tam and Associates
Canada and B.C. Land Surveyor
115 - 8833 Odlin Crescent
Richmond, B.C. V6X 3Z7
Telephone: 214-8928
Fax: 214-8929
E-mail: office@jctam.com
Website: www.jctam.com
Job No. 5681
FB-260 P112-113
Drawn By: TH

DWG No. 5681-TOPO



CERTIFIED CORRECT:
LOT DIMENSION ACCORDING TO
FIELD SURVEY.

Johnson C. Tam
JOHNSON C. TAM, B.C.L.S.
AUGUST 6th, 2014

ATTACHMENT 2



RZ 14-670779

Attachment 3

Address: 10551 No. 1 Road

Applicant: 0814948 BC Ltd.

Planning Area(s): Steveston

	Existing	Proposed
Owner:	Pami Vininder Singh Buttar Amandeep Amy Mander	To be determined
Site Size (m²):	671 m ²	North lot – 335 m ² South lot - 336 m ²
Land Uses:	Single detached dwelling	Two (2) residential lots
OCP Designation:	Neighbourhood Residential	No change
Area Plan Designation:	Single-Family	No change
Zoning:	Single Detached (RS1/E)	Coach Houses (RCH1)
Other Designations:	The Arterial Road Policy designates the subject site for redevelopment to coach houses with lane access.	No change

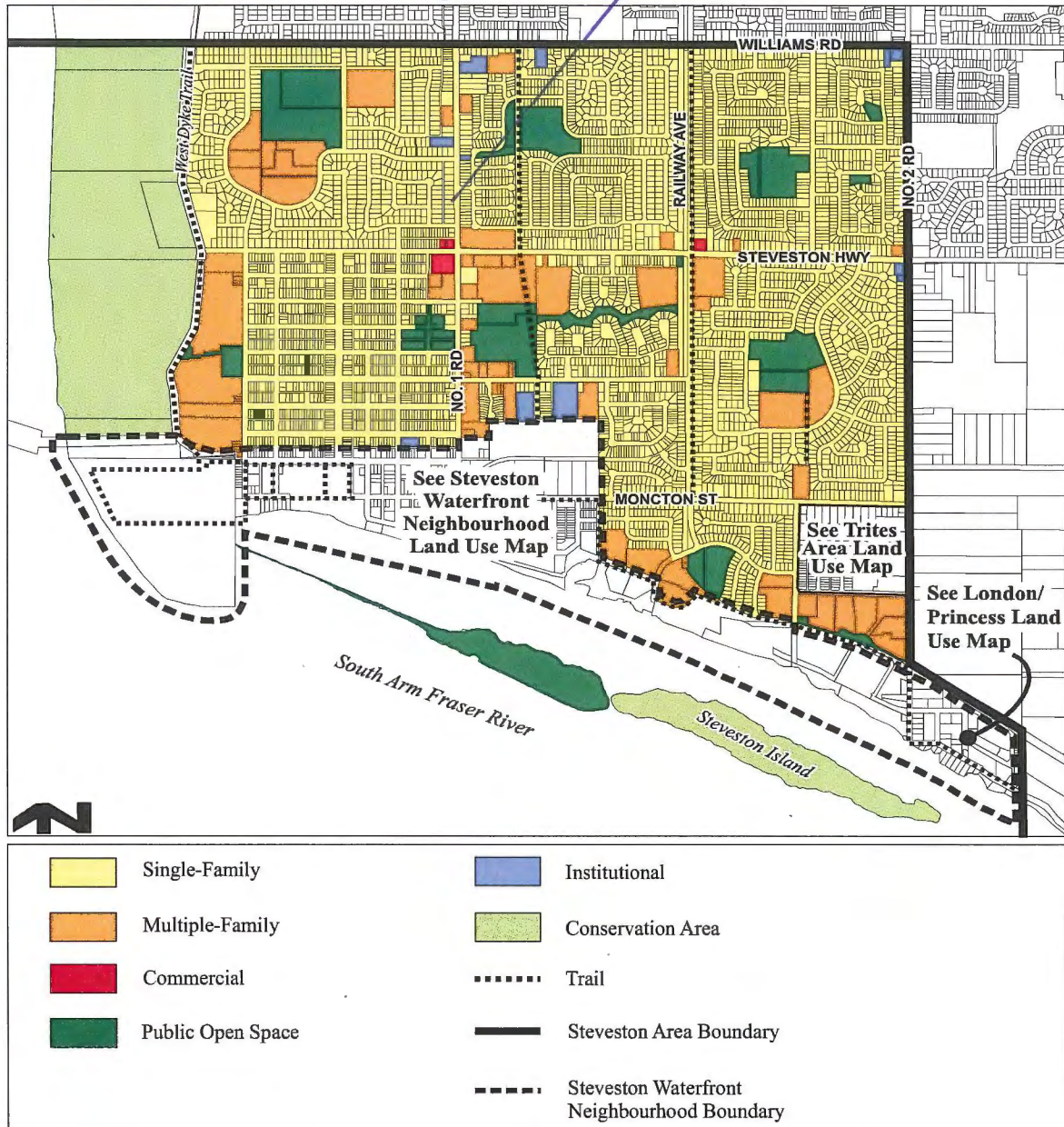
On Future Subdivided Lots	Bylaw Requirement		Proposed		Variance
Floor Area Ratio:	Max. 0.60		Max. 0.60		none permitted
Lot Coverage – Building:	Max. 45%		Max. 45%		none
Lot Coverage – Buildings, Structures, and Non-Porous Surfaces:	Max. 70%		Max. 70%		none
Lot Coverage – Live Plant Material:	Min. 20%		Min. 20%		none
Lot Size (min. dimensions):	315 m ²		North lot - 335 m ² South lot - 336 m ²		none
Principal Dwelling Setback – Front & Rear Yards (m):	Min. 6 m		Min. 6 m		none
Principal Dwelling Setback – Side Yards (m):	Min. 1.2 m		Min. 1.2 m		none
Coach House Building Setback – Rear Yard (m):	Min. 1.2 m		Min. 1.2 m		none
Coach House Building Setback – Side Yards (m):	Ground floor	Min. 0.6 & 1.8 m	Ground floor	Min. 0.6 & 1.8 m	none
	Second floor	Min. 1.2 m & 1.8 m	Second floor	Min. 1.2 m & 1.8 m	none
Principal Dwelling Height (m):	Max. 2 ½ storeys		Max. 2 ½ storeys		none

On Future Subdivided Lots	Bylaw Requirement		Proposed		Variance
Coach House Building Height (m):	Max. 2 storeys or 6.0 m, whichever is less, as measured from the highest elevation of the crown of the lane		Max. 2 storeys or 6.0 m, whichever is less, as measured from the highest elevation of the crown of the lane		none
On-Site Parking Spaces – Principal Dwelling:	2		2		none
On-Site Parking Spaces – Coach House:	1		1		none
Tandem Parking Spaces:	permitted		2 for Principal Dwelling		none
Amenity Space – Outdoor:	Principal Dwelling	30 m ²	Principal Dwelling	30 m ²	none
	Coach House	No minimum	Coach House	No minimum	none

Other: Tree replacement compensation required for loss of bylaw-sized trees.

City of Richmond

Steveston Area Land Use Map

 Bylaw 7783
2010/04/12




SCALE: 1/8" = 1'-0"


Crown of Road

www.dmanddesign.com

PRELIMINARY PROPOSAL

ROOF RIDGE EL.: 

ROOF MEAN EL.: 

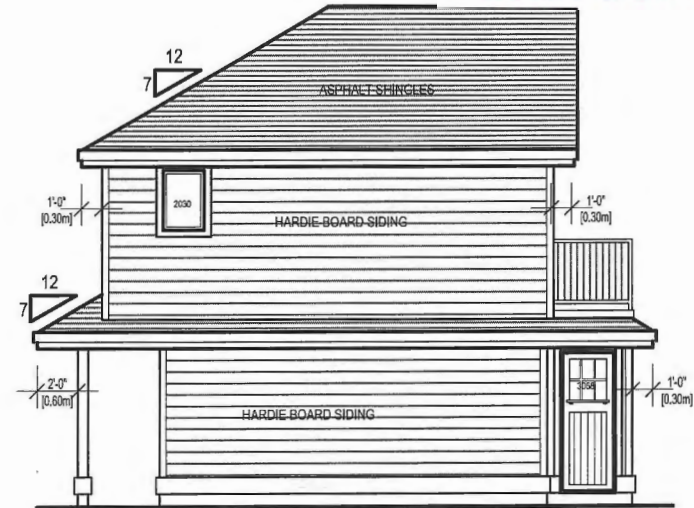
U/S 2nd FLR CEIL. EL.: 

T/O 2nd FLOOR EL.: 

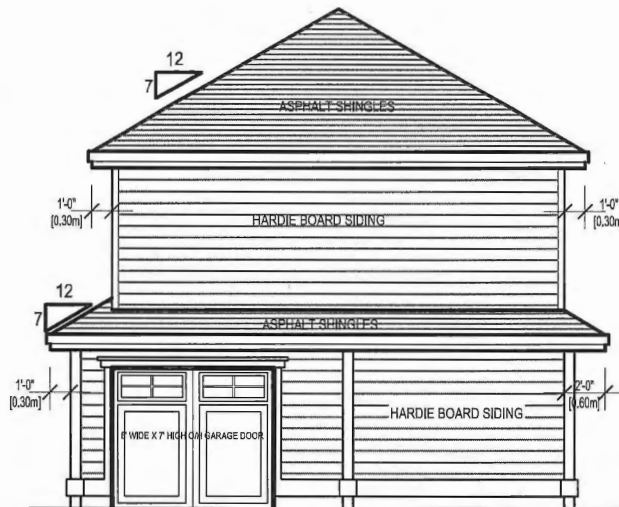
T/O MAIN FLOOR EL.: 



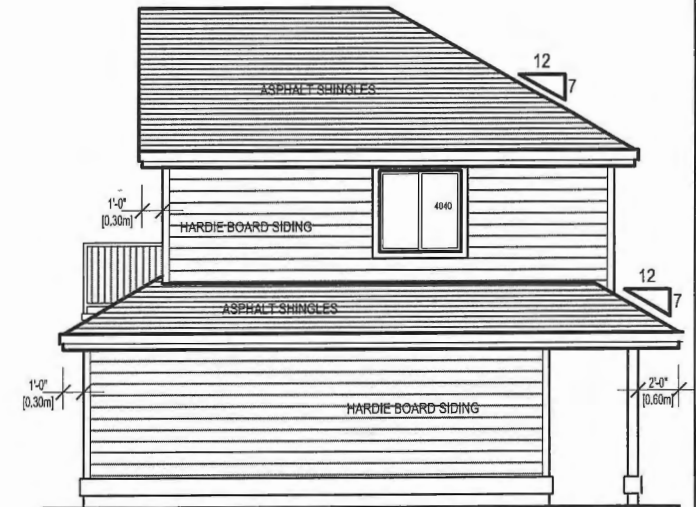
WEST ELEVATION



NORTH ELEVATION



EAST ELEVATION



SOUTH ELEVATION

COPYRIGHT D.MANO DESIGN AND DRAFTING SERVICES - ALL DESIGNS AND IDEAS
IN WHOLE OR IN PART MAY NOT BE REPRODUCED WITHOUT A WRITTEN AGREEMENT.

TREE INVENTORY AND ASSESSMENT LIST:

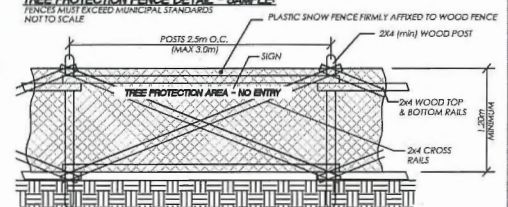
- Tag # denotes the tag affixed to the tree for reference in report and on drawings.
- Ht and Spr denote the height and spread (radius of crown) of the tree in metres as measured or estimated by the assessor if applicable. Height and Spread are not applicable for Grove or Forest Stand trees.
- Dbh denotes the diameter of the trunk measured at 1.4 m above grade or as per arboricultural standards (i.e. For multi stem trees).
- Cond denotes health and structural rating using Visual Tree Assessment (VTA) procedures. U denotes Unsuitable, M denotes Marginal, S denotes Suitable. See report for details.
- Action denotes the proposed treatment of the tree within the current development design. See report and drawing for details.

Tag #	Ht	Spr	Dbh (cm)	Tree Type	Cond	Observations	Action
997		6	74	Western redcedar	M	Two stems coalescent at base with long bark inclusion above. Small girdling roots over root crown. Leaders well limb-tied.	Remove
998		6	60	Western redcedar	M	Historically crown raised to 2m via heading cuts. Broken branches over lane. Limbs embedded in stem.	Remove
999	4	3	21	Apple	U	Historic partial failure at root mat and regenerated. Severe lean to south. Historically topped at 3m. In health decline.	Remove
1000	4	2	15+10+8	Sawara cypress	U	Historically topped at 2m. Replacement leaders weakly attached at topping site and high risk for failure.	Remove
1001	4	2	29+19	Cherry		Dead / Dying. Extensively decayed leaders. Historically topped at 4m and pruned via heading cuts. Large injury at base.	Remove
A			57	Western redcedar	M	Girdled at 1.5m with a wire supporting the fence. Stem is smaller diameter at girdle than above or below.	Protect
B	5	3	Approx. 25	Plum	U	Historically pruned via heading cuts and topped at 3m. Black-knot fungal disease observed throughout crown.	Refer - Protect
C	5	3	Approx. 16	Pear	M	Historically topped at 3m. Replacement leaders carry entire crown.	Protect

TREE PROTECTION GUIDELINES:

- TREE PROTECTION ZONE SETBACKS:**
The Tree Protection Zone (TPZ) alignments are established as directed by this office to provide a suitable setback to ensure adequate root protection to maintain tree health and tree stability. These alignments are based on site and tree conditions as determined by the project arborist, and the tree protection setbacks provided by others (including setbacks derived from municipal guidelines), and may be conditional to certain mitigation measures being undertaken (i.e. root pruning, compensatory treatment to remaining root zone). The tree protection guidelines also apply to the overhead portions of the tree (limbs, branches and foliage), even if those tree parts extend outside of the TPZ setback.
- TREE PROTECTION FENCES (BARRIERS):**
Barriers must be erected at the specified alignments and setbacks, maintained in good condition until the project reaches substantial completion, and the restrictions and guidelines implemented as detailed herein through to the completion of the landscaping phase. The materials and installation of the fence must meet or exceed the municipal standards. Signs stating TREE PROTECTION (ZONE - NO ENTRY) must be placed on the tree protection fence at a suitable frequency at the direction of the project arborist. Arborists will install signs in cases that we are retained to provide field services for compliance during construction. The owner, contractors, subcontractors and trades should be made aware of the restrictions therein, and consult with this office for any access. If required, tree protection fencing must be inspected and approved by the municipality and/or the project arborist prior to commencement of any demolition, site preparation or construction work.
- SURVEY OF FENCES:**
If any tree protection fences are aligned with or within close proximity to a restrictive covenant, a property line, and/or an environmentally sensitive or protected area, the contractor must undertake a survey of the location of those property lines such that the tree protection fence can be installed and inspected accurately.
- TREE PROTECTION AND LAND CLEARING OPERATIONS:**
If large scale land clearing areas contain a TPZ and/or there is whittling treatments required in new forest edge interfaces along the perimeter of the land clearing areas, and/or when certain trees within a TPZ are specified for removal, it is strongly recommended that the land clearing contractor should coordinate with the project arborist in advance to review their work plan and to identify retained trees and the protection measures for them, as well as during the clearing process to improve compliance and to assess new forest edges and provide tree failure risk mitigation prescriptions.
For tendering purposes, the proponents should be required to provide unit costs for the following treatments (including disposal of waste) in the forest interface zones:
 - Removal of defective trees in site categories of small (dbh up to 20 cm dbh), medium (dbh 21 cm to 45 cm), large (dbh 46 to 75 cm) and very large (76 cm dbh and greater), and
 - Pruning services (prunty).
 Note that approvals for leaving chips or large woody debris created by the waste of these operations on the site will be investigated with affected property owners upon request.
In certain cases, and subject to municipal approval, interim tree protection measures instead of standard tree protection fence installation may be acceptable, such as but not limited to:
 - Relinquishing the services of this project arborist to attend and direct the compliance to protection measures during the clearing process that is in proximity to the TPZ, or
 - Installing alternative demarcation of the TPZ such as survey stakes, pointing lines on the ground, and/or placing rope or flagging.
- TREE PROTECTION GUIDELINES:**
Any construction related work or activities within or directly adjacent to a TPZ requires advance approval and the on-site direction of the project arborist. The trunks, branches, foliage and roots of retained trees, as well as the soil within the tree protection zones, must not be damaged by construction activities. Except as approved and directed by the project arborist, activities within and access to the TPZ are restricted during the site preparation, construction and landscape installation phases of the project as follows:
 - Restricted low impact methods for the removal of trees and stumps within or adjacent to TPZ.
 - No soil disturbance, including trenching for underground services or utilities, shipping of organic soil for hard landscape installation, excavation for building foundations, fill placement, or trenching for irrigation or central installation.
 - No storage or transport of soil, spoil, construction materials, waste materials, etc.
 - No water or washing of concrete, slurry, dyke, paint, or other materials that may adversely impact the soil or roots.
 - No passage or operation of vehicles or equipment.
 - No placement of temporary structures or services.
 - No off-site lights, signs, cables or any other devices to retained trees.
 - No unauthorized pruning or cutting of retained trees. Any pruning or other treatment of a retained tree must be completed by a qualified arborist or tree service firm employing ISA Certified Arborists and in conformance with ANSI A300 Standards, and/or under the direction of the project arborist from this office, and.
 - Any excavation adjacent to the TPZ will require the attendance of the project arborist and root pruning to be undertaken as necessary.
 - The use of aerial lifts, cranes or other overhead equipment is restricted in proximity to retained trees and should be planned with the size and height of the crown of the tree accordingly.
 It is recognized that certain unpredictable construction conflicts with a TPZ may arise that could interfere with the protection of the selected trees. However any encroachment into a TPZ and/or changes to the tree retention scheme are subject to approval in advance by the project arborist and the municipality. Certain TPZ restrictions or guidelines noted herein may be waived if they are considered by the project arborist to be tolerable impacts, and/or if the impacts to the trees can be successfully mitigated by implementing special measures, protection systems, compensatory treatments, and/or follow-up work, as specified and directed by this office.
- LANDSCAPING:**
The landscaping phase is when retained trees can be severely damaged. The operation of equipment, the placement of growing medium, grading and sub-base preparation for hard landscape features (i.e. sidewalks and parking), site preparation for retaining walls and footings, excavation for fences, signs and other landscape features, digging of planting holes for new plants and trees, the digging of trenches for irrigation, drainage and lighting, and the placement of turf and other finishing works, all have a very high

TREE PROTECTION FENCE DETAIL - SAMPLE



2	JUL 10, 2015	ADDED UPSIZED REPLACEMENT TREES
0	AUG 25, 2014	INITIAL SUBMISSION
REV #	DATE	COMMENTS

actgroup.ca

ARBORTECH CONSULTING

a division of

HEAD OFFICE: SERVING METRO VANCOUVER

SUITE 200 - 3740 CHATHAM STREET, RICHMOND, BC CANADA V7E 2Z3

p 604.275.3484

FRASER VALLEY REGIONAL OFFICE

SUITE 107 - 1325 MCALLISTER ROAD, ABBOTSFORD, BC CANADA V2S 8A3

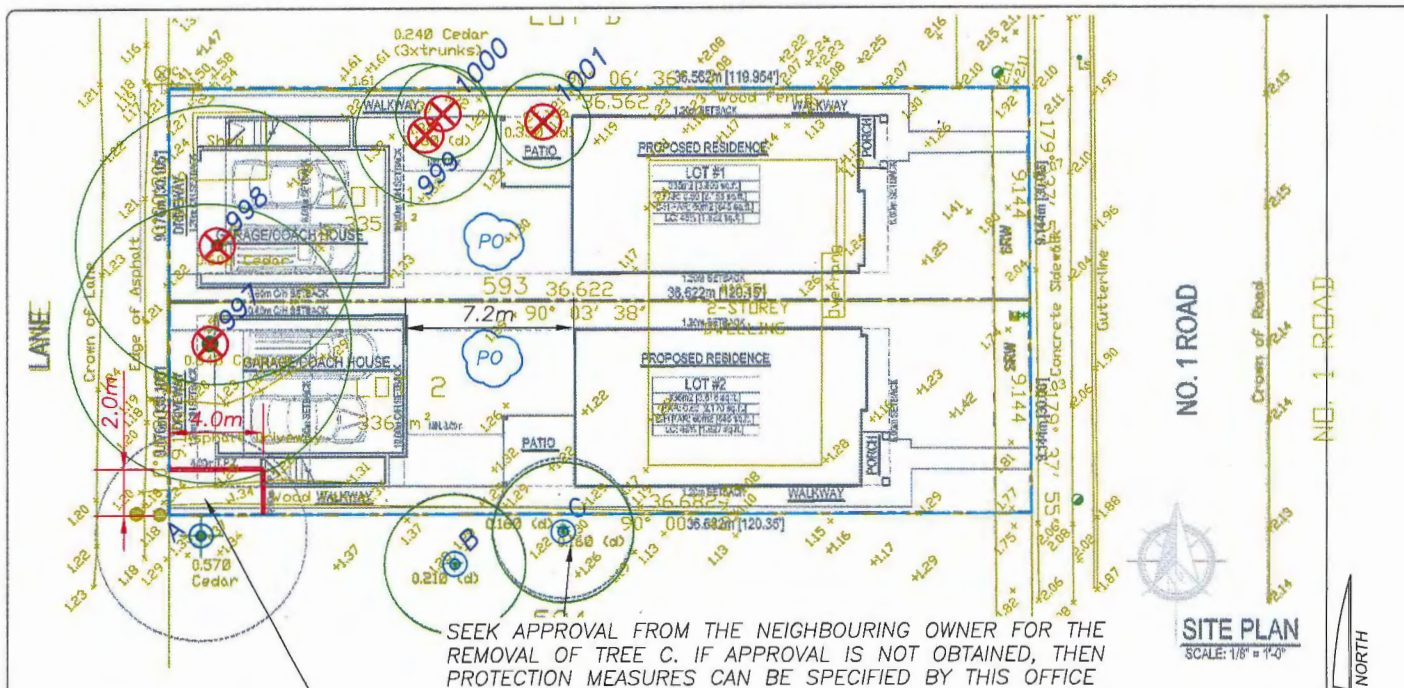
p 604.755.7132



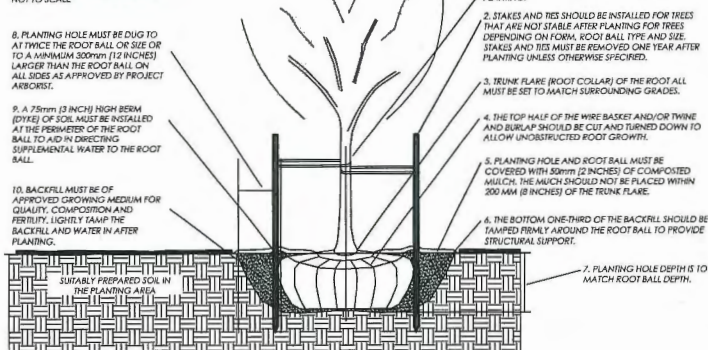
TREE MANAGEMENT DRAWING

PROJECT:	TWO LOT SUBDIVISION
ADDRESS:	10551 NO. 1 RD, RICHMOND
CLIENT:	DAVE MANDER
ACL FILE:	14289
SHEET:	1 OF 2

PLAN NOTES:
This plan is based on drawings supplied by the project Surveyor (SCL), Engineer (P ENG) and/or Design Professionals and is provided for context only as it relates to the planning and implementing the management of existing trees. This plan does not warrant or certify the accuracy of location of features or dimensions thereof. Refer to the original drawings from those professionals for those purposes.



TREE PLANTING DETAIL



PLAN NOTES:

This plan is based on drawings supplied by the project Surveyor (P.C.S.), Engineer (P.E.C.) and/or Design Professional, and is provided for reference only. It is not to be used for the planning and implementation of the management of existing trees. This plan does not warrant or certify the accuracy of locations of features or dimensions shown. Refer to the original drawings from these professionals for those purposes.

SUGGESTED PLANT LIST:

PLEASE USE BOTANICAL NAME WHEN ORDERING PLANT MATERIAL.
PLANT SITES MUST MEET MUNICIPAL REQUIREMENTS FOR MINIMUM SIZE AND SPECIES.
PLANTING LOCATIONS MUST MEET ARBORICULTURAL BEST MANAGEMENT PRACTICES AND BC S/LA/BCUNA SPECIFICATIONS FOR SELECTION, HANDLING, PLANTING, ESTABLISHMENT AND MAINTENANCE.

QTY	CODE	CAL/HT	BOTANICAL NAME	COMMON NAME
2	PD	5.0m HT	PICEA OMORICA	SERBIAN SPRUCE

LEGEND:

- ✕ denotes tree TAG NUMBER or ID REFERENCE.
- denotes DRIFLINE (spread of the branches and foliage) of the tree.
- denotes the trunk location of tree proposed to be **RETAINED** and protected.
- denotes the trunk location of tree proposed to be **REMOVED**.
- denotes trunk location of **HIGH RISK** tree for owner to obtain permit and **REMOVE** as soon as possible.
- denotes trunk location of **OFF-SITE** tree to be protected (unless owner/municipal approval for removal is obtained).
- denotes **NON-BY-LAW** underpass tree (measured by project arborist).
- denotes **SITE LIMITS**.
- denotes **TREE PROTECTION ZONE** setback alignments.

2	JUL 10, 2015	ADDED UPSIZED REPLACEMENT TREES
0	AUG 25, 2014	INITIAL SUBMISSION
REV #	DATE	COMMENTS

TREE MANAGEMENT DRAWING

PROJECT:	TWO LOT SUBDIVISION
ADDRESS:	10551 NO. 1 RD, RICHMOND
CLIENT:	DAVE MANDER
ACL FILE:	14289
SHEET:	2 OF 2

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ARBORTECH CONSULTING

HEAD OFFICE: SERVING METRO VANCOUVER
SUITE 200 - 3740 CHATHAM STREET, RICHMOND, BC CANADA V7E 2Z3 p 604.275.3484
FRASER VALLEY REGIONAL OFFICE
SUITE 107 - 1525 MCCALLUM ROAD, ABBOTSFORD, BC CANADA V2S 8A3 p 604.755.7132





Address: 10551 No. 1 Road

File No.: RZ 14-670779

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 9287, the following items are required to be completed:

1. Submission of a Landscape Plan, prepared by a Registered Landscape Architect, along with a Landscaping Security based on 100% of a cost estimate for the works provided by the Landscape Architect (including 10% contingency, fencing, required trees, all hard and soft landscaping, and installation). The Landscaping Security will be reduced by 90% after construction and landscaping on the subject site is completed and a landscaping inspection by City staff has been passed. The City will retain 10% of the Security for a one-year maintenance period to ensure that the landscaping survives. The Landscape Plan must respond to the guidelines of the Arterial Road Policy and comply with the landscaping requirements of the RCH1 zone, and must include the following six (6) replacement trees:
 - Two (2) 6 cm deciduous or 3.5 m coniferous trees in the front yards.
 - Two (2) 8 cm deciduous or 4 m high coniferous trees in the front yards.
 - Two (2) 5 m high conifer trees in the rear yards (with a minimum value of \$3,000 each). Suitable trees for the rear yards, as recommended by the project Arborist and the City's Tree Protection Department staff are Serbian Spruce.
2. Registration of a restrictive covenant on Title of the lot to ensure that the Building Permit application and ensuing development at the site is generally consistent with the plans included in Attachment 5 to this report.
3. City acceptance of the developer's offer to voluntarily contribute \$2,000 to the City's Tree Compensation Fund for the planting of replacement trees within the City.
4. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained (Trees A, B, and C on the adjacent site to the south at 10571 No. 1 Road). The Contract must include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
5. Registration of a flood indemnity covenant on Title.
6. Registration of a legal agreement on Title ensuring that the coach house cannot be stratified.
7. Registration of a legal agreement on title prohibiting the conversion of the parking area in the garage into habitable space.

Prior to Demolition Permit* issuance, the applicant must complete the following requirements:

- Installation of appropriate tree protection fencing on-site around the trees to be retained (Trees A, B and C on the adjacent property to the south at 10571 No. 1 Road). Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin TREE-03 prior to any works being conducted on-site, and must remain in place until construction and landscaping on-site is completed.

At Subdivision* and Building Permit* stage, the applicant must complete the following requirements:

- Pay Development Cost Charges (City and GVS & DD), School Site Acquisition Charge, Address Assignment Fees, and Servicing Costs.

- Submit a cash-in-lieu contribution for future lane improvements (e.g., full lane width asphalt pavement, lane drainage, roll curb and gutter, and lighting).
- Complete the following required servicing works and frontage improvements:

Water Works

- Using the OCP Model, there is 396 L/s of water available at a 20 psi residual at the No. 1 Road frontage. Based on your proposed development, your site requires a minimum fire flow of 95 L/s. Once you have confirmed your building design at the Building Permit stage, you must submit fire flow calculations signed and sealed by a professional engineer based on the Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) to confirm that there is adequate available flow.
- At the applicant's cost, the City is to disconnect the existing 20mm water connection, and install two (2) new 25mm diameter water connections complete with meter boxes in the existing right-of-way.

Storm Sewer Works

At the applicant's cost, the City is to:

- Cut and cap the existing storm connection at the southwest corner of the property.
- Install a new storm sewer inspection chamber and two new service connections at the proposed common property line and tie-in to the existing manhole STMH5748 in the rear lane.

Sanitary Sewer Works

- No sanitary sewer upgrade is required.
- The applicant is required to retain and tie-in to the existing sanitary service connections in the rear lane.

Frontage Improvements

- No lane upgrade is required.
- The applicant is required to submit a cash-in-lieu contribution at subdivision stage for future lane improvements (e.g., full lane width asphalt pavement, lane drainage, roll curb & gutter, and lighting).

General Items

- Proposed driveway locations must not conflict with existing street lights and/or utility poles. Requests to relocate street lights and/or utility poles will not be considered other than under exceptional circumstances.
- Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
- Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

(Signed original on file)

Signed

Date



**Richmond Zoning Bylaw 8500
Amendment Bylaw 9287 (RZ 14-670779)
10551 No. 1 Road**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation of the following area and by designating it **"COACH HOUSES (RCH1)"**.

P.I.D. 006-594-298

Lot 593 Section 34 Block 4 North Range 7 West New Westminster District Plan 42890

2. This Bylaw may be cited as **"Richmond Zoning Bylaw 8500, Amendment Bylaw 9287"**.

FIRST READING

A PUBLIC HEARING WAS HELD ON

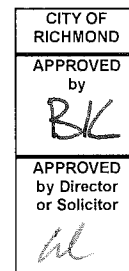
SECOND READING

THIRD READING

OTHER REQUIREMENTS SATISFIED

ADOPTED

OCT 13 2015



MAYOR

CORPORATE OFFICER



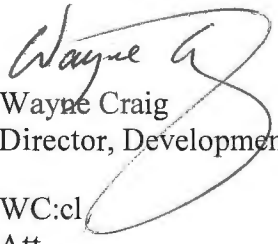
City of Richmond

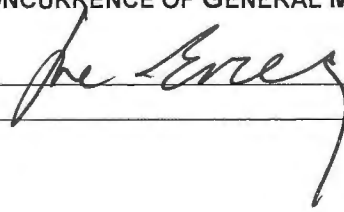
Report to Committee Planning and Development Division

To: Planning Committee
From: Wayne Craig
Director, Development
Date: September 23, 2015
File: RZ 15-691744
Re: Application by Maryem Ahbib for Rezoning at 7220 Railway Avenue from Single Detached (RS1/E) to Compact Single Detached (RC2)

Staff Recommendations:

1. That the following recommendation be forwarded to a Public Hearing:
 - a) That Single-Family Lot Size Policy 5463 for the area generally bounded by Railway Avenue, Blundell Road, and No. 2 Road, in a portion of Section 13 Block 4 North Range 7 West, be amended as shown in the proposed draft Single-Family Lot Size Policy 5463 (Attachment 5).
2. That Richmond Zoning Bylaw 8500, Amendment Bylaw 9292, for the rezoning of 7220 Railway Avenue from "Single Detached (RS1/E)" to "Compact Single Detached (RC2)", be introduced and given first reading.


Wayne Craig
Director, Development
WC:cl
Att.

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Affordable Housing	<input checked="" type="checkbox"/>	

Staff Report

Origin

Maryem Ahbib has applied to the City of Richmond for permission to rezone the property at 7220 Railway Avenue from the “Single Detached (RS1/E)” zone to the “Compact Single Detached (RC2)” zone, to permit the property to be subdivided to create two (2) lots with vehicle access to/from the existing operational rear lane (Attachment 1). A survey of the subject site showing the proposed subdivision plan is included in Attachment 2.

In order to consider this rezoning application, an amendment to Single-Family Lot Size Policy 5463 is required to remove the subject site from the Lot Size Policy, along with three (3) other properties fronting Railway Avenue north of Linfield Gate, which have existing lane access. Further discussion on the proposed amendment to Lot Size Policy 5463 is provided below.

Findings of Fact

A Development Application Data Sheet providing details about the development proposal is attached (Attachment 3).

Surrounding Development

The subject site contains an older character single-detached dwelling, which is proposed to be demolished. Existing development immediately surrounding the subject site is as follows:

To the North are two (2) lots zoned “Single Detached (RS1/E)” containing single-family dwellings, which are each the subject of an active rezoning application to the “Coach Houses (RCH1)” zone to permit subdivision to create small lots, each with a principal dwelling and accessory coach house above a detached garage with access from the rear lane (RZ 14-674043 and RZ 15-710175).

To the South is a lot zoned “Single Detached (RS1/E)” that contains an existing non-conforming duplex.

To the East, immediately across the rear lane is a lot zoned “Single Detached (RS1/E)” fronting Lindsay Road, which contains a single-family dwelling.

To the West, immediately across Railway Avenue, is the Railway Greenway trail on City-owned property.

Related Policies & Studies

Official Community Plan

The Official Community Plan (OCP) land use designation for the subject site is “Neighbourhood Residential”. This redevelopment proposal is consistent with this designation.

Arterial Road Policy

Since 2001, the City has encouraged redevelopment to compact lots along arterial roads where access is or can be made available to a rear lane. The Arterial Road Policy identifies the subject site for redevelopment to compact lots or coach house lots, with rear lane access.

Where such conditions exist on lots that are governed by a Lot Size Policy that is older than five (5) years, there is past precedent in place for amending the Lot Size Policy to exclude the properties fronting the arterial road.

It is on this basis that the proposed rezoning application and amendment to the Lot Size Policy are being considered.

Lot Size Policy 5463

The subject site is located within the area governed by Lot Size Policy 5463, adopted by Council on February 19, 1996 (Attachment 4). The Lot Size Policy permits those properties along Railway Avenue with rear lane access to rezone and subdivide in accordance with the "Single Detached (RS2/B)" zone (i.e., 12 m wide lots, 360 m² in area).

Consideration of the rezoning application at the subject site requires an amendment to Lot Size Policy 5463. The proposed amendment to the Lot Size Policy is to exclude four (4) properties fronting Railway Avenue with existing rear lane access north of Linfield Gate from the Lot Size Policy (i.e., 7180, 7200, 7220, and 7240 Railway Avenue). All other provisions of the Lot Size Policy would remain unchanged. The proposed amendment to Lot Size Policy 5463 is shown in Attachment 5.

A letter dated May 27, 2015 (Attachment 6), was sent to the owners and residents of all properties located within the area governed by Lot Size Policy 5463 to describe the proposed amendment to the Lot Size Policy and to advise them of the proposed rezoning application at the subject site. The letter indicated that any comments or concerns with either the proposed amendment to the Lot Size Policy or the proposed rezoning of 7220 Railway Avenue, were to be submitted to the City by June 26, 2015.

In response to the letter, the City received two (2) pieces of email correspondence (see Attachment 7):

- One (1) of which expressed support for the infill development application as it was an optimal location to increase density as the lot is located on an arterial road with a transit stop within a short walk; and
- One (1) of which expressed opposition to the proposal.

An amendment to the Lot Size Policy to enable the subject site and three (3) other lots along this block of Railway Avenue to redevelop for compact lots and coach houses is supported on the basis of: a) consistency with the Arterial Road Policy designation for this block in the OCP; b) locating infill development where there is existing access to transit, parks, community centres

etc.; and c) the ability to utilize the existing operational rear lane in keeping with the Residential Lot (Vehicular) Access Regulation Bylaw No. 7222.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on Title is required prior to final adoption of the rezoning bylaw.

Public Consultation

A rezoning sign has been installed on the subject property. Other than the public consultation process described above for the proposed Lot Size Policy amendment, staff have not received any comments from the public about the development proposal in response to the placement of the rezoning sign on the property.

Should the proposed amendment to Lot Size Policy 5463 be endorsed by City Council and the rezoning bylaw associated with this application be granted 1st reading, the rezoning bylaw would proceed to a Public Hearing for consideration, at which time further opportunity for public input into the proposal will be provided.

Analysis

Proposed Site Access

Access to the proposed lots is to be from the existing operational rear lane, with no access permitted to Railway Avenue, in accordance with Residential Lot (Vehicular) Access Regulation Bylaw No. 7222.

Trees & Landscaping

A survey has been submitted by the applicant, which shows that there are no bylaw-sized trees on the subject property (Attachment 2).

To ensure that the front yards of the proposed lot are enhanced at future development stage, the applicant is required to submit a Landscape Plan, prepared by a Registered Landscape Architect, along with a security in the amount of 100% of a cost estimate for the works provided by the Registered Landscape Architect (including 10% contingency, fencing, hard surfaces, trees, soft landscaping, and installation). The Landscape Plan must respond to the guidelines of the Arterial Road Policy, including the planting and maintenance of two (2) trees in the front yard of each lot proposed. The Landscape Plan, Cost Estimate, and Security are required to be submitted prior to final adoption of the rezoning bylaw. The Security will be reduced by 70% after construction and landscaping on the proposed lots is completed and a landscaping inspection has been passed by City staff. The City will retain 30% of the Security for a one (1) year maintenance period to ensure that the landscaping survives.

Affordable Housing Strategy

For single-family development proposals received prior to September 14, 2015, Richmond's Affordable Housing Strategy requires a secondary suite within a dwelling on 50% of new lots created through rezoning and subdivision, or a cash-in-lieu contribution of \$1.00/ft² of total buildable area towards the City's Affordable Housing Reserve Fund. This rezoning application was submitted on January 28, 2015, and is subject to these requirements.

The applicant proposes to provide a legal secondary suite on one (1) of the two (2) lots proposed at the subject site. To ensure that the secondary suite is built to the satisfaction of the City in accordance with the City's Affordable Housing Strategy, the applicant is required to enter into a legal agreement registered on title stating that no final Building Permit inspection will be granted until the secondary suite is constructed to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw. Registration of this legal agreement is required prior to final adoption of the rezoning bylaw. This agreement may be discharged from Title (at the initiation of the applicant) on the lot where the secondary suite is not required by the Affordable Housing Strategy after the requirements are satisfied.

Site Servicing and Frontage Improvements

Prior to final adoption of the rezoning bylaw, the applicant is required to enter into a Servicing Agreement for the design and construction of off-site improvements along Railway Avenue and the rear lane, as described in Attachment 8.

Financial Impact

This rezoning application results in an insignificant Operational Budget Impact (OBI) for off-site City infrastructure, such as roadworks, waterworks, storm sewers, sanitary sewers, street lights, street trees and traffic signals.

Conclusion

This proposal is to rezone the property at 7220 Railway Avenue from the "Single Detached (RS1/E)" zone to the "Compact Single Detached (RC2)" zone, to permit the property to be subdivided to create two (2) lots, with vehicle access to/from the existing rear lane. Concurrent with the rezoning application, the applicant requests that Council consider an amendment to Lot Size Policy 5463 to exclude the four (4) properties fronting Railway Avenue with existing rear lane access north of Linfield Gate from the Lot Size Policy.

This rezoning application complies with the applicable land use designations for the subject site that are contained within the OCP.

The list of Rezoning Considerations associated with this application is included in Attachment 8, which has been agreed to by the applicant (signed concurrence on file).

It is recommended that the proposed amendment to Lot Size Policy 5463 to exclude four (4) properties fronting Railway Avenue with existing rear lane access north of Linfield Gate from the Lot Size Policy be approved.

It is further recommended that Zoning Bylaw 8500, Amendment Bylaw 9292 be introduced and given first reading.



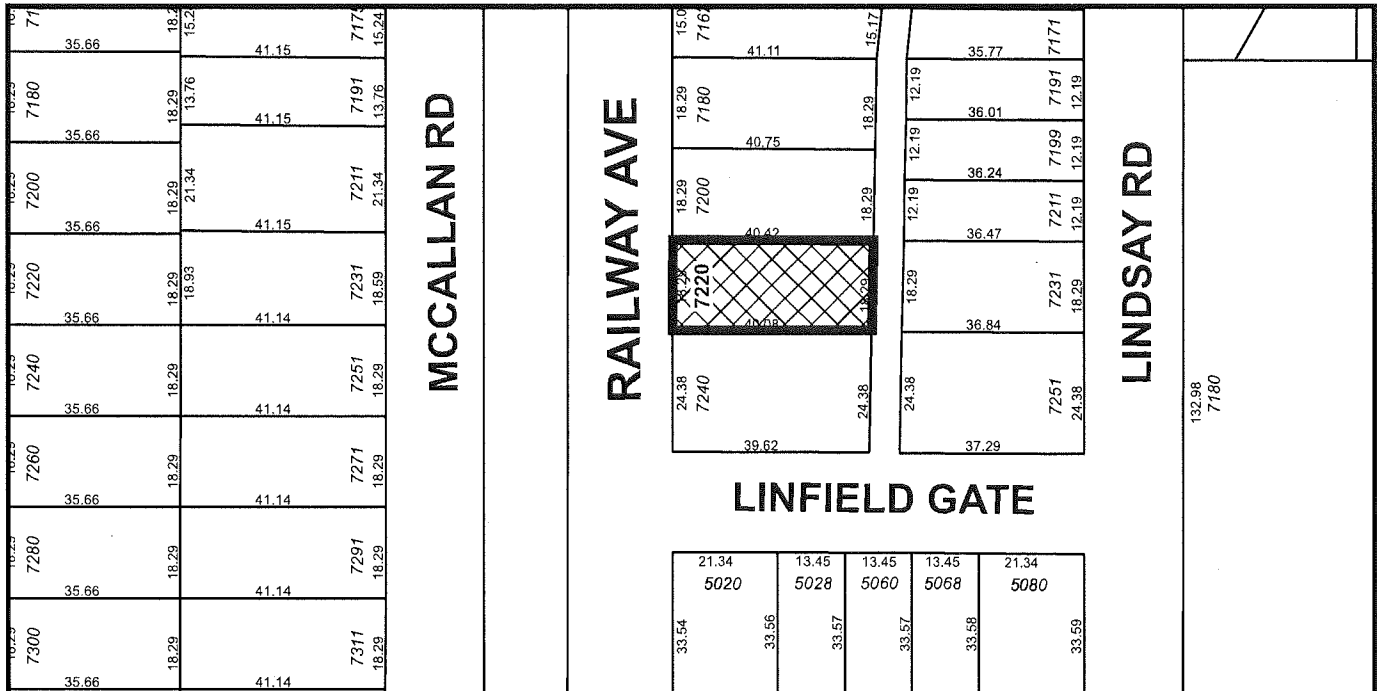
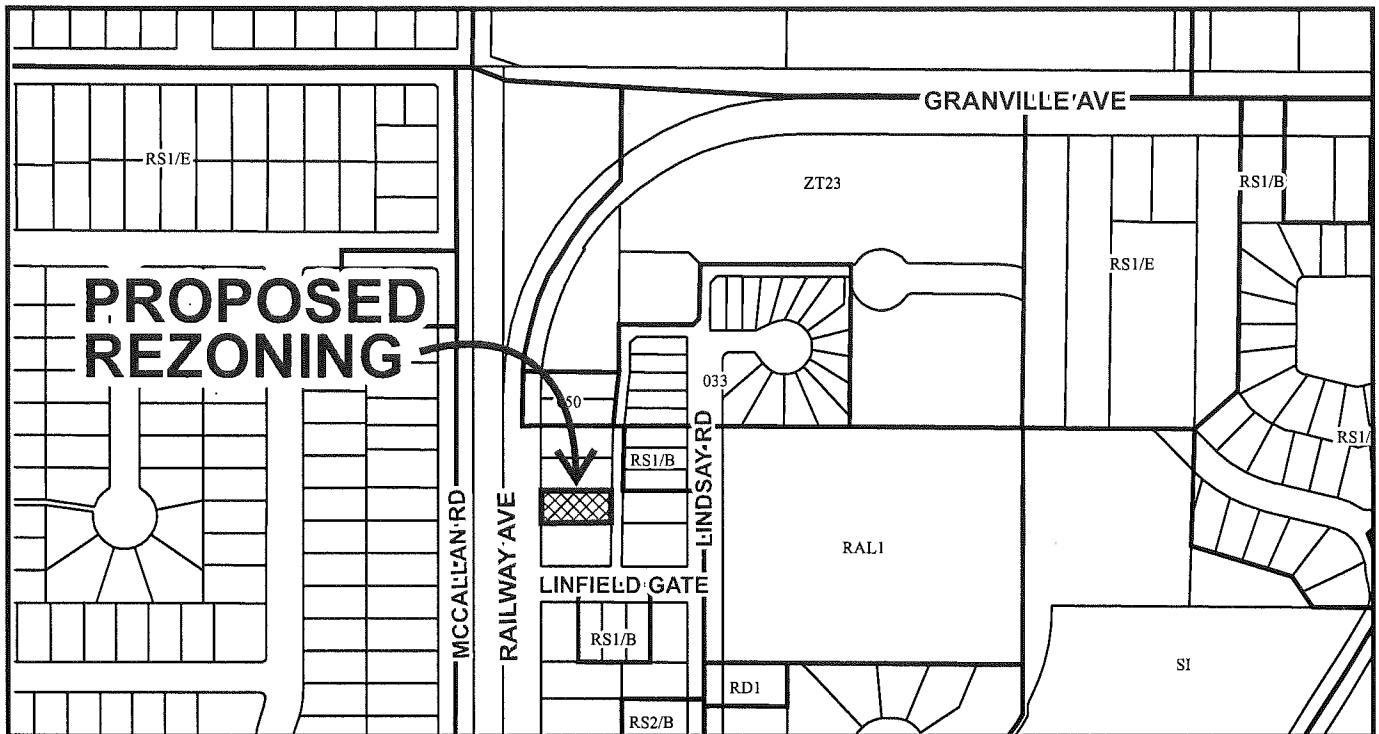
Cynthia Lussier
Planning Technician

CL:rg

- Attachment 1: Location Map/Aerial Photo
- Attachment 2: Survey showing proposed subdivision plan
- Attachment 3: Development Application Data Sheet
- Attachment 4: Lot Size Policy 5463
- Attachment 5: Proposed amendment to Lot Size Policy 5463
- Attachment 6: City's letter dated May 27, 2015
- Attachment 7: Correspondence received from residents
- Attachment 8: Rezoning Considerations



City of
Richmond



RZ 15-691744

Original Date: 02/10/15

Revision Date:

Note: Dimensions are in METRES



City of
Richmond



RZ 15-691744

Original Date: 02/10/15

Revision Date

Note: Dimensions are in METRES



RZ 15-691744

Attachment 3

Address: 7220 Railway Avenue

Applicant: Maryem Ahbib

Planning Area(s): Blundell

	Existing	Proposed
Owner:	Madan Jhim Poonam Mehay	To be determined
Site Size (m²):	735.7 m ² (7,919 ft ²)	Proposed north lot – 368.6 m ² Proposed south lot – 367.1 m ²
Land Uses:	Single-family residential	No change
OCP Designation:	Neighbourhood Residential	No change
Lot Size Policy Designation:	Lots along Railway Avenue with lane access are permitted to rezone and subdivide in accordance with RS2/B	Proposed amendment to remove the four (4) existing lots fronting Railway Avenue with rear lane access north of Linfield Drive to be excluded from the Lot Size Policy
Zoning:	Single Detached (RS1/E)	Compact Single Detached (RC2)
Other Designations:	The Arterial Road Policy designates the subject site for redevelopment to compact lots and coach houses	No change

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.60	Max. 0.60	none permitted
Lot Coverage – Buildings:	Max. 50%	Max. 50%	none
Lot Coverage – Non-Porous Surfaces:	Max. 70%	Max. 70%	none
Lot Coverage – Live plant material:	Min. 20%	Min. 20%	none
Lot Size (min. dimensions):	270 m ²	Proposed north lot – 368.6 m ² Proposed south lot – 367.1 m ²	none
Setback – Front & Rear Yards (m):	Min. 6 m	Min. 6 m	none
Setback – Side Yard (m):	Min. 1.2 m	Min. 1.2 m	none
Height (m):	2 ½ storeys	2 ½ storeys	none



City of Richmond

Policy Manual

Page 1 of 2	Adopted by Council: February 19, 1996	POLICY 5463
File Ref: 4045-00	SINGLE-FAMILY LOT SIZE POLICY IN QUARTER-SECTION 13-4-7	

POLICY 5463:

The following policy establishes lot sizes for properties within the area generally bounded by **Railway Avenue, Blundell Road and No. 2 Road**, in a portion of Section 13-4-7 as shown on the attached map:

That properties within the area generally bounded by Railway Avenue, Blundell Road and No. 2 Road, in a portion of Section 13-4-7, be permitted to rezone in accordance with the provisions of Single-Family Housing District, Subdivision Area H (R1/H) in Zoning and Development Bylaw 5300, with the exception that:

1. Single-Family Housing District, Subdivision Area E (R1/E) applies to lots with frontage on No. 2 Road and Blundell Road that do not have a lane or internal road access;
2. Single-Family Housing District, Subdivision Area B (R1/B) applies to properties with duplexes on them with the exception that Single-Family Housing District, Subdivision Area E (R1/E) applies to those properties with frontage on No. 2 Road and Blundell Road that do not have lane or internal road access;
3. Single-Family Housing District, Subdivision Area B (R1/B) applies to properties generally fronting Lindsay Road and Linfield Gate in the western portion of Section 13-4-7; and

That this policy be used to determine the disposition of future single-family rezoning applications in this area, for a period of not less than five years, unless amended according to Bylaw No. 5300.



City of Richmond

Policy Manual

Page 1 of 2	Adopted by Council:	DRAFT PROPOSED POLICY 5463
File Ref: 4045-00	SINGLE-FAMILY LOT SIZE POLICY IN QUARTER-SECTION 13-4-7	

POLICY 5463:

The following policy establishes lot sizes for properties within the area generally bounded by **Railway Avenue, Blundell Road and No. 2 Road**, in a portion of Section 13-4-7 as shown on the attached map:

That properties within the area generally bounded by Railway Avenue, Blundell Road and No. 2 Road, in a portion of Section 13-4-7, be permitted to rezone and subdivide in accordance with the provisions of the "Single Detached (RS2/H)" zone in Richmond Zoning Bylaw 8500, with the exception that:

1. The "Single Detached (RS2/E)" zone applies to lots with frontage on No. 2 Road and Blundell Road that do not have a lane or internal road access;
2. The "Single Detached (RS2/B)" zone applies to properties with duplexes on them with the exception that the "Single Detached (RS2/E)" zone applies to those properties with frontage on No. 2 Road and Blundell Road that do not have lane or internal road access;
3. The "Single Detached (RS2/B)" zone applies to properties generally fronting Lindsay Road and Linfield Gate in the western portion of Section 13-4-7; and

That this policy be used to determine the disposition of future single-family rezoning applications in this area, for a period of not less than five years, unless amended according to Richmond Zoning Bylaw 8500.



City of Richmond

6911 No. 3 Road,
Richmond, BC V6Y 2C1
www.richmond.ca

May 27, 2015

File: RZ 14-674043
RZ 15-691744

Planning and Development Department
Development Applications
Fax: 604-276-4052

Dear Owner/Resident:

Re: Proposed amendment to Single-Family Lot Size Policy 5463 and proposed rezoning applications at 7180 Railway Avenue and 7220 Railway Avenue

This is to advise you that the City of Richmond has received two (2) rezoning applications for properties in your neighbourhood at 7180 Railway Avenue and 7220 Railway Avenue. These applications also propose an amendment to Single-Family Lot Size Policy 5463 that is established for your neighbourhood. Details on these applications are provided below:

- **Rezoning Application at 7180 Railway Avenue:** Landcraft Homes Ltd. has applied to the City of Richmond for permission to rezone 7180 Railway Avenue from "Single Detached (RS1/E)" to "Coach Houses (RCH1)" to permit subdivision into two (2) lots, each with a principal dwelling and detached coach house with vehicle access to and from the existing rear lane. The application is being processed under City file RZ 14-674043. A location map and proposed subdivision plan of the subject site is included in **Attachment 1**.
- **Rezoning Application at 7220 Railway Avenue:** Maryem Ahbib has applied to the City of Richmond for permission to rezone 7220 Railway Avenue from "Single Detached (RS1/E)" to "Compact Single Detached (RC2)" to permit subdivision into two (2) compact lots with vehicle access to and from the existing rear lane. The application is being processed under City File RZ 15-691744. A location map and proposed subdivision plan of the subject site is included in **Attachment 2**.

Single-Family Lot Size Policy 5463

In 1996, City Council adopted Lot Size Policy 5463 to establish the lot sizes that would be considered on properties generally bounded by Railway Avenue, Blundell Road, and No. 2 Road (see **Attachment 3**). The Lot Size Policy provides the following direction:

- Properties along No. 2 Road, Blundell Road, and Railway Avenue are restricted to the "Single Detached (RS1/E)" zone (i.e., 18 m wide lots, 550 m² in area), with the exception that those lots with lane or internal road access may be permitted to rezone and subdivide in accordance with the "Single Detached (RS2/B)" zone (i.e., 12 m wide lots, 360 m² in area);
- Properties with duplexes in the neighbourhood and along on arterial roads with lane or internal road access may be permitted to rezone and subdivide in accordance with the "Single Detached (RS2/B)" zone;
- Properties fronting Lindsay Road and Linfield Gate may be permitted to rezone and subdivide in accordance with the "Single Detached (RS2/B)" zone; and

- Other remaining lots in the neighbourhood (as shown on the attached map), may be permitted to rezone and subdivide in accordance with the “Single Detached (RS1/H)” zone (i.e., 16.5 m wide lots, 360 m² in area).

Proposed Amendment to Single-Family Lot Size Policy 5463

The Lot Size Policy currently permits the two (2) subject properties (7180 and 7220 Railway Avenue) to rezone and subdivide in accordance with the “Single Detached (RS1/B)” zone.

Consistent with the Arterial Road Policy in Richmond’s Official Community Plan, which allows for compact lot and coach house development at a higher density on designated properties along arterial roads with lane access, the applicants at 7180 and 7220 Railway Avenue are requesting permission to amend Lot Size Policy 5463 and to rezone the subject properties to permit a subdivision to create two (2) smaller lots with vehicle access to/from the existing rear lane (note: vehicle access to Railway Avenue is not permitted).

The proposed amendment to Lot Size Policy 5463 is to exclude the four (4) properties fronting Railway Avenue with existing rear lane access north of Linfield Gate from the Lot Size Policy (i.e., 7180, 7200, 7220, and 7240 Railway Avenue). All other provisions of Lot Size Policy 5463 would remain unchanged. The proposed amendment to Lot Size Policy 5463 is shown in **Attachment 4**.

The minimum lot dimensions, area, and density of the zones proposed for 7180 and 7220 Railway Avenue are listed below:

Site Address	Proposed Zone	Min. Width	Min. Depth	Min. Area	Max. FAR	Purpose
7180 Railway Avenue	“Coach Houses (RCH1”	9.0 m (29.5 ft)	35.0 m (114.8 ft)	315.0 m ² (3,390.6 ft ²)	0.6	Single-detached housing and a detached coach house
7220 Railway Avenue	“Compact Single Detached (RC2)”	9.0 m (29.5 ft)	24.0 m (78.7 ft)	270.0 m ² (2,906.3 ft ²)	0.6 applied to a max. of 464.5 m ² of lot area, together with 0.30 applied to the balance of lot area in excess of 464.5 m ²	Single-Detached housing

Process

Please review the accompanying materials. Please forward any comments or concerns you may have about the proposed amendment to Lot Size Policy 5463, and/or the redevelopment proposals at 7180 Railway Avenue and 7220 Railway Avenue, to my attention at the following address by **Friday June 26, 2015**:

Cynthia Lussier, Planning Technician – Design
Development Applications Department
City of Richmond
6911 No. 3 Road
Richmond BC V6Y 2C1

Staff will complete reports to Planning Committee on the proposed Lot Size Policy amendment and rezoning applications, and will incorporate your feedback as part of the reports. If the applications are supported by the Planning Committee, both applications would then be subsequently considered by Richmond City Council at a Council meeting and a Public Hearing. You will be provided with the opportunity to address Council directly if the proposed amendment to Lot Size Policy 5463 and the rezoning applications at 7180 Railway Avenue and 7220 Railway Avenue proceed to a Public Hearing.

It is emphasized that the proposed amendment to Lot Size Policy 5463 and proposed rezoning applications at 7180 Railway Avenue and 7220 Railway Avenue does not change the zoning permitted on other properties in the area. Any future rezoning applications on other properties within the Lot Size Policy area must undergo the standard rezoning application review process, which involves a Public Hearing process.

If you have any questions or require further explanation, please contact me by phone at 604-276-4108, or via email at clussier@richmond.ca.

Sincerely,



Cynthia Lussier
Planning Technician - Design

AY/CL:cl

Attachments:

- Attachment 1: Location Map and Proposed Subdivision Plan of Rezoning Application at 7180 Railway Avenue (RZ 14-674043)
- Attachment 2: Location Map and Proposed Subdivision Plan of Rezoning Application at 7220 Railway Avenue (RZ 15-691744)
- Attachment 3: Existing Single-Family Lot Size Policy 5463
- Attachment 4: Draft Proposed Amendment to Single-Family Lot Size Policy 5463

Lussier, Cynthia

Subject: FW: street address entered twice in database

From: Mike Davison [<mailto:mazzyfan@live.com>]
Sent: Monday, 01 June 2015 5:02 PM
To: Lussier, Cynthia
Subject: RE: street address entered twice in database

Hi Cynthia,

There were 2 Files:

RZ 14-674043
RZ 15-691744

Regarding 7180 & 7220 Railway Ave.

One proposal doubles the # of dwellings on the lot while the other quadruples it but the lots are on an arterial road with a transit stop within a ~2 walk so as optimal a location to increase density as there is.

Regards,
Mike

From: CLussier@richmond.ca
To: mazzyfan@live.com
Subject: RE: street address entered twice in database
Date: Mon, 1 Jun 2015 19:47:01 +0000

Hi Mike
Thank you for your email.
I'm not familiar with which development application your email is associated.

It is my assumption that the envelopes you received contained information about a development proposal in your area.
If so, what is the address for the development site?

Once I have an address, I can look into whether there were any other problems with the mailout.

In the meantime, do you have any concerns about the development application?

Please let me know if you do.

Cynthia Lussier
Planning Technician
Development Applications Division
City of Richmond
Tel: 604-276-4108

Email: clussier@richmond.ca
www.richmond.ca

From: Mike Davison [<mailto:mazzyfan@live.com>]
Sent: Friday, 29 May 2015 8:46 PM
To: Lussier, Cynthia
Subject: street address entered twice in database

Hello Cynthia,

Two envelopes with the exact same planning information were delivered to my address and so one should be deleted. Below I give the exact info (including punctuation and whether in upper or lower case) that was on the mailing labels:

OCCUPANT
5111 BLUNDELL RD
RICHMOND, BC V7C 1H3

OCCUPANT,
5111 Blundell Rd
Richmond, BC, V7C 1H3

Thanks,
Mike

Lussier, Cynthia

From: Kerry Starchuk [kerrystarchuk@hotmail.com]
Sent: Wednesday, 24 June 2015 8:31 AM
To: Lussier, Cynthia
Subject: Re Letter May 23, 2015

Re: RZ14-674043
RZ15 -691744

To Whom this may concern,

I received a letter about zoning on Railway Avenue. Why waste my time reading this crap and sending it out to all the residents?

The city is going to do what they want and really couldn't care less about what the residents want. There was a petition on

Railway about development of town houses and it was ignored.

Greed has taken over this city and there will be long term consequences.

**For the City of Richmond to be the most
appealing, livable, and well-managed
community in Canada.**

Have a nice day!

Regards,

Kerry Starchuk

cc: Carol Day - Councillor



Address: 7220 Railway Avenue

File No.: RZ 15-691744

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 9292, the developer is required to complete the following:

1. Submission of a Landscape Plan, prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development, and deposit of a Landscaping Security based on 100% of the cost estimate provided by the Landscape Architect (including 10% contingency, fencing, hard surfaces, trees, soft landscaping, and installation costs). The Landscape Plan should:
 - comply with the guidelines of the OCP's Arterial Road Policy and should not include hedges along the front property line;
 - include a mix of coniferous and deciduous trees;
2. Registration of a flood indemnity covenant on title.
3. Registration of a legal agreement on Title to ensure that no final Building Permit inspection is granted until a secondary suite is constructed on one (1) of the two (2) future lots, to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw.
4. Enter into a Servicing Agreement* for the design and construction of off-site improvements along Railway Avenue and the rear lane. The scope of works is to include (but is not limited to) the following:
 - regrading of the rear lane to create a center swale and installation of a 200 mm storm sewer complete with manholes and inspection chambers from the subject site's north property line tying into the existing drainage system on Linfield Gate. The City will fund approximately 31 m of this work, subject to funding approval.
 - upgrading of the existing storm sewer service connection and inspection chamber at the subject site's south corner along the Railway Avenue frontage to City of Richmond standards.
 - installation of a new storm service connection complete with inspection chamber along the Railway Avenue frontage at the subject site's northwest corner.
 - construction of a new 1.5 m wide concrete sidewalk at the property line along Railway Avenue, with connections to the existing sidewalk north and south of the subject site.
 - removal of the existing sidewalk next to the curb and backfilling of the area between the new sidewalk and curb with a grassed boulevard to include trees, lighting, and other utility requirements as determined through the Servicing Agreement review process (note: the sidewalk and boulevard works must not affect the existing bus stop pad and bus shelter located in the development frontage).
 - reconstruction of the existing rear lane along the entire length of the east frontage of the site to the current City lane design standards (5.4 m wide pavement and 0.3 m wide rollover curb on both sides of the lane, along with lane lighting).
 - The Servicing Agreement design is to include the design of the following required water, storm, and sanitary service connection works:

Water Works

- Using the OCP Model, there is 55.4 L/s of water available at a 20 psi residual at the Railway Avenue east frontage and 421.7 L/s of water available at a 20 psi residual at the Railway Avenue west frontage. Based on your proposed development, your site requires a minimum fire flow of 95.0 L/s.
- The developer is required to submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm the development has adequate fire flow for onsite fire

protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage and Building designs.

- At the developer's cost, the City is to a) cut and cap all existing water service connections at the watermain, along Railway Avenue frontage; and b) install two (2) new 25 mm water service connections complete with meters and meter boxes along the Railway Avenue frontage.

Sanitary Sewer Works

- At the developer's cost, the City is to: a) upgrade the existing sanitary service connection and inspection chamber at the site's southeast corner along the rear lane frontage to City of Richmond standards, to service the proposed south lot; and b) install one (1) new sanitary service connection complete with new inspection chamber (approximately 5 m south from the north property line) along the rear lane frontage to service the proposed north lot.
- General Items:
The developer is to coordinate with BC Hydro, Telus and other private communication service providers:
 - for their servicing requirements;
 - to underground proposed Hydro service lines;
 - when relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
 - To determine if above-ground structures are required and to coordinate their locations (e.g. Vista, PMT, LPT, Shaw cabinets, Telus Kiosks, etc).

Notes:

Further details on the scope of work associated with the Servicing Agreement to be confirmed during the Servicing Agreement design and review process.

Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

At Subdivision* stage, the developer must complete the following requirements:

- Payment of Development Cost Charges (City and GVS & DD), School Site Acquisition Charge, Address Assignment Fees, and Servicing Costs.

At Building Permit* stage, the developer must complete the following requirements:

- Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. The Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
- Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the

Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed

Date



**Richmond Zoning Bylaw 8500
Amendment Bylaw 9292 (RZ 15-691744)
7220 Railway Avenue**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation of the following area and by designating it **"COMPACT SINGLE DETACHED (RC2)"**.

P.I.D. 004-506-472

Lot 215 Section 13 Block 4 North Range 7 West New Westminster District Plan 40948

2. This Bylaw may be cited as **"Richmond Zoning Bylaw 8500, Amendment Bylaw 9292"**.

FIRST READING

OCT 26 2015

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

OTHER REQUIREMENTS SATISFIED

ADOPTED

CITY OF RICHMOND
APPROVED by BIL
APPROVED by Director or Solicitor <i>hl</i>

MAYOR

CORPORATE OFFICER



City of Richmond

Report to Committee Planning and Development Division

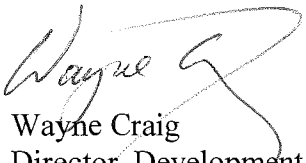
To: Planning Committee
From: Wayne Craig
Director, Development

Date: October 14, 2015
File: ZT 15-695231

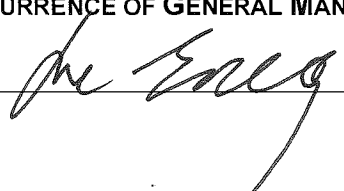
Re: Application by Oval 8 Holdings Ltd. for a Zoning Text Amendment to the "High Rise Apartment and Olympic Oval (ZMU4) - Oval Village (City Centre)" Zone at 6611, 6622, 6633, 6655, 6688, 6699, 6811, 6877, and 6899 Pearson Way

Staff Recommendation

That Richmond Zoning Bylaw 8500, Amendment Bylaw 9487, for a Zoning Text Amendment to the "High Rise Apartment and Olympic Oval (ZMU4) – Oval Village (City Centre)" zone, a site-specific zone applicable at 6611, 6622, 6633, 6655, 6688, 6699, 6811, 6877, and 6899 Pearson Way, to permit changes to the approved subdivision plan, be introduced and given first reading.


Wayne Craig
Director, Development

WC:spc
Att. 9

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Community Social Development	<input checked="" type="checkbox"/>	
Parks Services	<input checked="" type="checkbox"/>	
Real Estate Services	<input checked="" type="checkbox"/>	
Engineering	<input checked="" type="checkbox"/>	

Staff Report

Origin

Oval 8 Holdings Ltd. (Aspac Developments) has applied to the City of Richmond for a Zoning Text Amendment to the “High Rise Apartment and Olympic Oval (ZMU4) – Oval Village (City Centre)” zone, the site-specific zone applicable to “River Green”, a multi-phase, high density, mixed use development along the Middle Arm of the Fraser River between the No. 2 Road and Dinsmore Bridges. More specifically, the purpose of the proposed Zoning Text Amendment is to change the subdivision boundaries of the portion of the ZMU4 zone situated east of Hollybridge Way (i.e. between the Richmond Olympic Oval and Gilbert Road) (Attachments 1, 2, and 3) to:

- Consolidate three (3) residential lots into one to improve site planning (Attachments 4 and Attachment 7, Schedule A);
- Consolidate and expand Public Rights of Passage right-of-way areas secured prior to rezoning adoption for walkways and related uses on the three (3) affected lots to provide for a larger, central, riverfront park space (Attachments 5 and 6);
- Construct, at the developer’s sole cost, a new “Temporary Public Open Space” and related City dike/park improvements, to be completed prior to occupancy of the developer’s first phase east of the Richmond Olympic Oval (i.e. Lot 9, currently under construction) for public use until the proposed central public open space is constructed by the developer in its ultimate form in the developer’s fourth/final phase east of the Oval (Attachment 7); and
- Create a fee-simple lot and transfer it to City ownership (at no cost to the City), which lot shall be the future site of a City-owned, stand-alone, turnkey, affordable, child care facility to be constructed by the developer (at the developer’s sole cost) as per legal agreements registered on title prior to rezoning. (Attachment 9, Schedule D)

Findings of Fact

To date, development approvals in the subject area of “River Green” (i.e. east of the Richmond Olympic Oval) include:

- Rezoning (RZ 09-460962): October 2011 / adopted
- Zoning Text Amendment:
 - i. Affordable Housing Value Transfer (ZT 12-610289) September 2013 / adopted
- Development Permits:
 - i. Lot 9 @ riverfront (DP 11-587954) July 2013 / issued
 - ii. Lot 12 @ River Road (DP 11-587896) July 2013 / issued
- Building Permit:
 - i. Lot 9 @ riverfront (BP 13-651592) February 2015 / issued

A Development Application Data Sheet, providing details about the subject development proposal, is attached. (Attachment 8)

Surrounding Development

The subject site is located in the City Centre's Oval Village, an emerging high-rise, high density, mixed use community focussed around the Richmond Olympic Oval, the river, and an array of pedestrian-oriented retail and recreational amenities. Development near the subject site includes:

To the North: The Middle Arm of the Fraser River, dike, and related public amenities and park.

To the East: A City-owned, heritage-designated woodlot at 6900 River Road (the restoration and interpretation of which is the responsibility of the "River Green" developer, as per RZ 09-460962), beyond which is the Gilbert Road approach to the Dinsmore Bridge and light-industrial lands designated for future park use.

To the South: River Road, beyond which lies several sites that are currently under development with high-rise, high density, residential and mixed use buildings, similar in scale and character to the subject "River Green" development.

To the West: Hollybridge Way and canal, beyond which is the Richmond Olympic Oval and the balance of "River Green" (ZMU4-zoned) lands, including Aspac's sales centre and its first phase of residential development (which is occupied).

Related Policies & Studies

Official Community Plan / City Centre Area Plan (Schedule 2.10)

The subject development proposal is in conformance with City Centre Area Plan (CCAP) policies, including:

- Specific Land Use Map: Oval Village (2031), which designates "River Green" for mid- and high-rise, mixed use development (i.e. "Urban Centre T5" and "Village Centre Bonus") with pedestrian-oriented retail and related uses along River Road and a maximum density of 3.0 floor area ratio (FAR);
- Park and public open space linkages, which direct that development of the subject site includes open space features designed to enhance public access to and enjoyment of the City Centre's riverfront; and
- "Village Centre (commercial) Bonus", which requires that at least 5% of bonus floor area is constructed as a City-owned, affordable child care (to a turnkey level of finish, at the developer's sole cost).

As per legal agreements registered on title with respect to RZ 09-460962, in the third phase of development (east of the Richmond Olympic Oval), the developer must construct a 464.5 m² (5,000 ft²) City-owned, affordable child care facility and related outdoor program space and required parking on the subject site, to a turnkey level of finish, at the developer's sole cost.

Public Consultation

Informational signage is posted on the subject site to notify the public of the subject application. At the time of writing this report, no public comment had been received. The statutory Public Hearing will provide neighbours and other interested parties with an opportunity to provide comment.

Analysis

The subject portion of “River Green” (i.e. situated east of the Richmond Olympic Oval) is comprised of five lots zoned for high-density development, including:

- Three (3) waterfront lots zoned for residential uses only; and
- Two (2) lots that front River Road and are zoned for mixed residential/commercial uses, together with (on the east lot only) an affordable child care facility to be constructed by the developer (as per density bonus provisions in “River Green’s” ZMU4 zone).

In addition, through the rezoning of the subject portion of “River Green”, Public Rights of Passage right-of-ways were registered on title to secure 5,473.9 m² (1.35 ac) of the three (3) waterfront lots for public open space purposes, generally in the form of public walkways linking Pearson Way with the City dike and waterfront park.

The developer has requested the subject Zoning Text Amendment on the basis of the following:

- The Middle Arm waterfront is an increasingly popular destination and would benefit from better public access, but planned linkages in the vicinity of “River Green” are limited to Hollybridge Way and the meandering public walkway right-of-ways secured through the site’s original rezoning;
- The consolidation of “River Green’s” three (3) waterfront lots would facilitate the consolidation and expansion of two (2) of the development’s public walkway right-of-ways for use as a larger, central open space that could enhance public access and views to the City dike/waterfront park and accommodate a greater range of informal recreation activities (e.g., children’s play);
- In addition, lot consolidation and subdivision would facilitate the creation of a fee simple lot, to be transferred to the City, for a stand-alone, affordable, turnkey child care (which facility would be constructed by the developer, at the developer’s sole cost, as per density bonus provisions in “River Green’s” ZMU4 zone); and
- The form of development on the consolidated residential lot would be more efficient.

Staff are supportive of the developer’s proposal on the basis that:

- The developer proposes to increase “River Green’s” total area of publicly-accessible open space (east of the Oval) from 1.35 ac to 1.5 ac (5,473.9 m² to 6,076.2 m²);
- Consolidation and expansion of “River Green’s” public open space will enhance its amenity without any cost to the City because the developer will be solely responsible for construction and maintenance (as per legal agreements to be registered on title);
- The developer proposes to accelerate the delivery of public open space amenities within “River Green” with a “Temporary Public Open Space”, which space shall be constructed prior to occupancy of the developer’s first phase east of the Oval (i.e. Lot 9) and maintained for public use until it is replaced by the permanent central public open space in the developer’s final phase (i.e. new Lot 17), all at the developer’s sole cost;
- Transfer of a fee simple lot to the City for child care and community amenity purposes provides the City with a valuable asset and greatly enhances the ability of the City to ensure the affordable and effective operation of the child care facility;

- Proximity of the proposed child care lot to the existing City-owned (heritage woodlot) park at 6900 River Road will enhance the child care experience and visually expand the park's frontage in a key City Centre "gateway" location;
- Low-rise development on the proposed City-owned child care lot will reduce potential development impacts on significant trees and landscape features within the City-owned (heritage woodlot) park, as per the environmental assessment submitted by a registered environmental consultant and arborist on behalf of the developer;
- The development efficiencies achieved by the proposed residential lot consolidation provides for:
 - i. Reduced lot coverage (i.e. 37% versus 45%);
 - ii. Increased tower separation from adjacent lots on and off "River Green" (i.e. previous 24 m minimum separation is increased to 35 m or more);
 - iii. Broad, landscaped areas adjacent to the dike that will visually expand and enhance the public's waterfront park experience
 - iv. Improved views across/around the "River Green" development, both at grade and from surrounding towers;
 - v. Reduced shadowing of on-site public open space and the existing City-owned (heritage woodlot) park at 6900 River Road; and
 - vi. No impacts on parking, vehicle access, residential amenity space, streetscape character or other City Centre Area Plan objectives or related requirements; and
- The developer is not requesting any increase in density or relaxation in development requirements approved by Council via "River Green's" original rezoning (RZ 09-460962).

Sustainability

The developer shall be required to design and construct the subject development to satisfy the following standards:

- Residential and Mixed Use Buildings: LEED Silver (equivalent) standards and City District Energy Utility hook-up (for 70% of space heating); and
- City-Owned Child Care Facility: As for residential and mixed use buildings or, as determined by the City, alternative standards (e.g., Passive House, Net Zero Energy Building) with the aim of achieving greater energy efficiency without increased cost to the developer.

Zoning Bylaw

To facilitate the developer's proposed subdivision changes, amendments to "River Green's" site-specific ZMU4 zone are required (as per Zoning Amendment Bylaw 9487), including:

- Permitted Uses: Addition of "community amenity space" on the proposed City-owned child care lot and changes to the reference map to reflect the proposed subdivision;
- Permitted Density: Public open space density bonus requirements and maximum buildable floor areas for residential and non-residential uses are clarified;
- Permitted Lot Coverage: Requirements for the City-owned child care lot (45% max.) and consolidated residential lot (37% max.);
- Permitted Heights: Requirements for the City-owned child care lot (18 m max.); and
- Subdivision Provisions/Minimum Lot Size: Requirements for the City-owned child care lot and consolidated residential lot.

Site Servicing and Frontage Improvements

All off-site Engineering, Transportation, and Parks requirements with respect to “River Green” were identified and secured via the original rezoning (RZ 09-460962). Legal agreements are registered on title requiring that necessary improvements are designed and constructed, as the developer’s sole cost, on a phase-by-phase basis via the City’s standard Servicing Agreement and/or Development Permit processes. Prior to adoption of the subject Zoning Text Amendment bylaw, minor adjustments to the phasing of works arising from the developer’s proposed subdivision change will be addressed via revisions to legal agreements registered on title.

Existing Legal Encumbrances

Various existing legal agreements registered on title must be revised to facilitate “River Green’s” proposed subdivision changes, transfer of the proposed child care lot to the City, development and public open space phasing, and related requirements. These revisions are itemized in the attached Zoning Text Amendment Considerations (Attachment 9).

Financial Impact or Economic Impact

As a result of the subject application, it is proposed that the City will take ownership of developer-contributed assets related to the child care facility, including building and land improvements. The anticipated operating budget impact for the ongoing maintenance of these assets is estimated to be \$40,000 (upon completion of the facility, projected for 2018), which cost will be considered as part of the 2018 Operating budget. Note that operating costs for the child care were anticipated at the time of the original rezoning (RZ 09-460962).

Conclusion

Staff recommend support for the subject Zoning Text Amendment on the basis that it will facilitate a change in “River Green’s” existing subdivision, which will benefit the developer, the general public, and the City through improved site design, enhanced public open space, and the transfer of a fee simple lot to the City for child care purposes.

It is recommended that Zoning Bylaw 8500, Amendment Bylaw 9487 be introduced and given first reading.

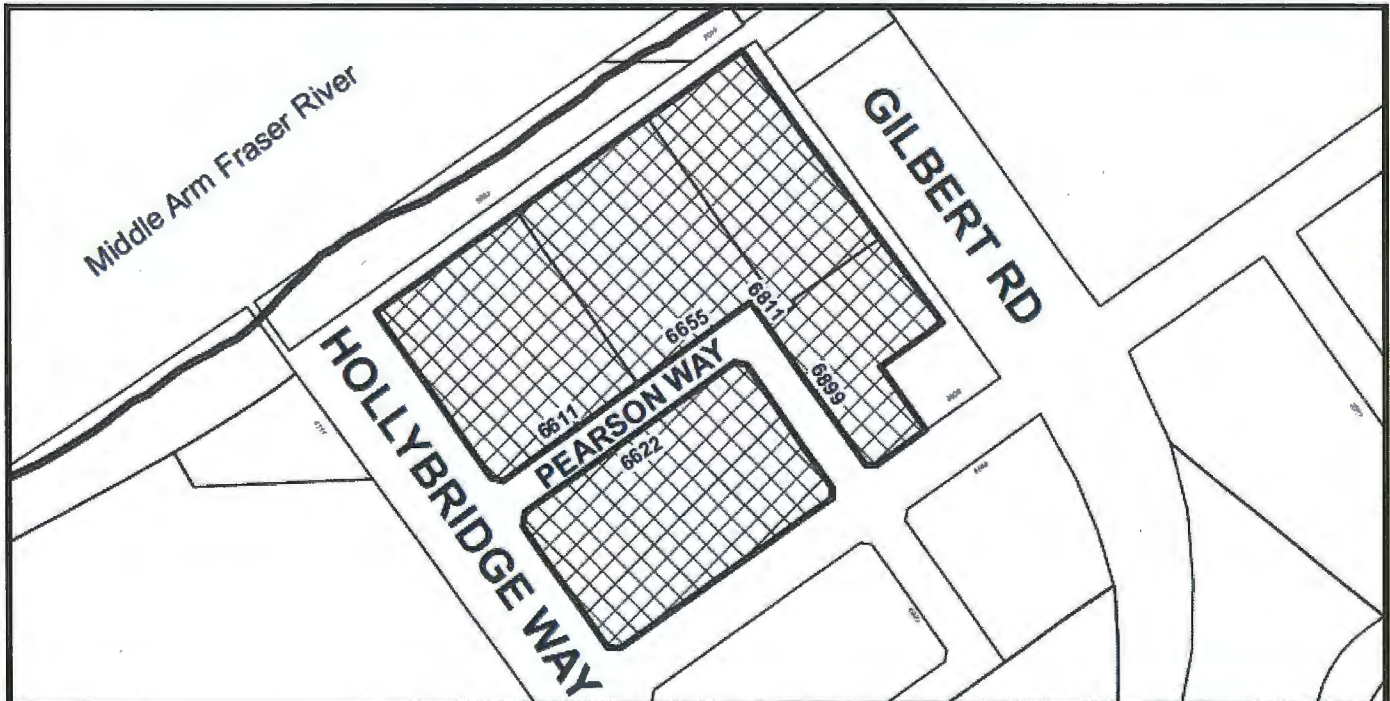
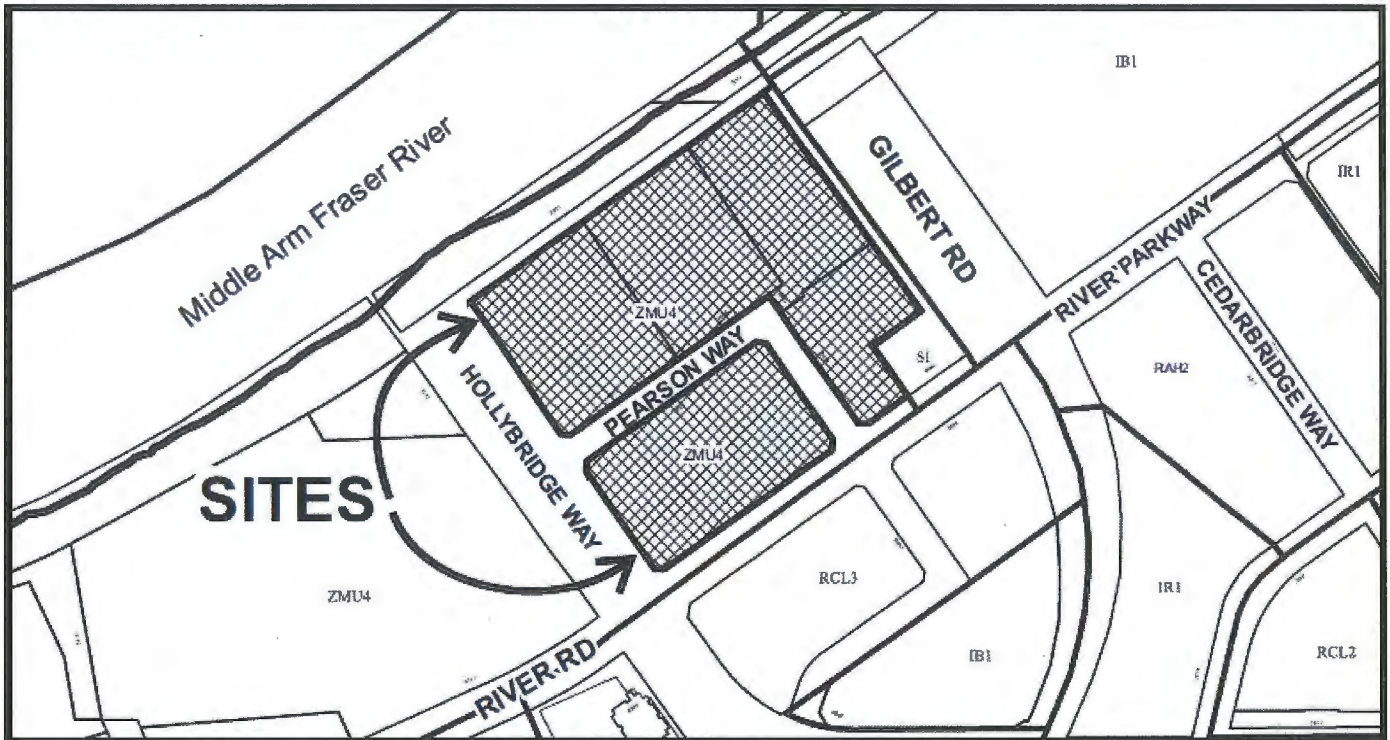


Suzanne Carter-Huffman
Senior Planner/Urban Design

SPC:spc

- Attachment 1: Location Map
- Attachment 2: Aerial Photograph
- Attachment 3: “River Green” Location Map
- Attachment 4: Proposed Site Planning Change
- Attachment 5: Proposed Public Open Space Change
- Attachment 6: Proposed “Permanent” Public Open Space @ New Lot 17
- Attachment 7: Proposed “Temporary” Public Open Space @ New Lot 17
- Attachment 8: Development Application Data Sheet
- Attachment 9: Zoning Text Amendment Considerations

Attachment 1
Location Map



ZT 15-695231

Original Date: 04/16/15

Revision Date: 10/13/15

Note: Dimensions are in METRES

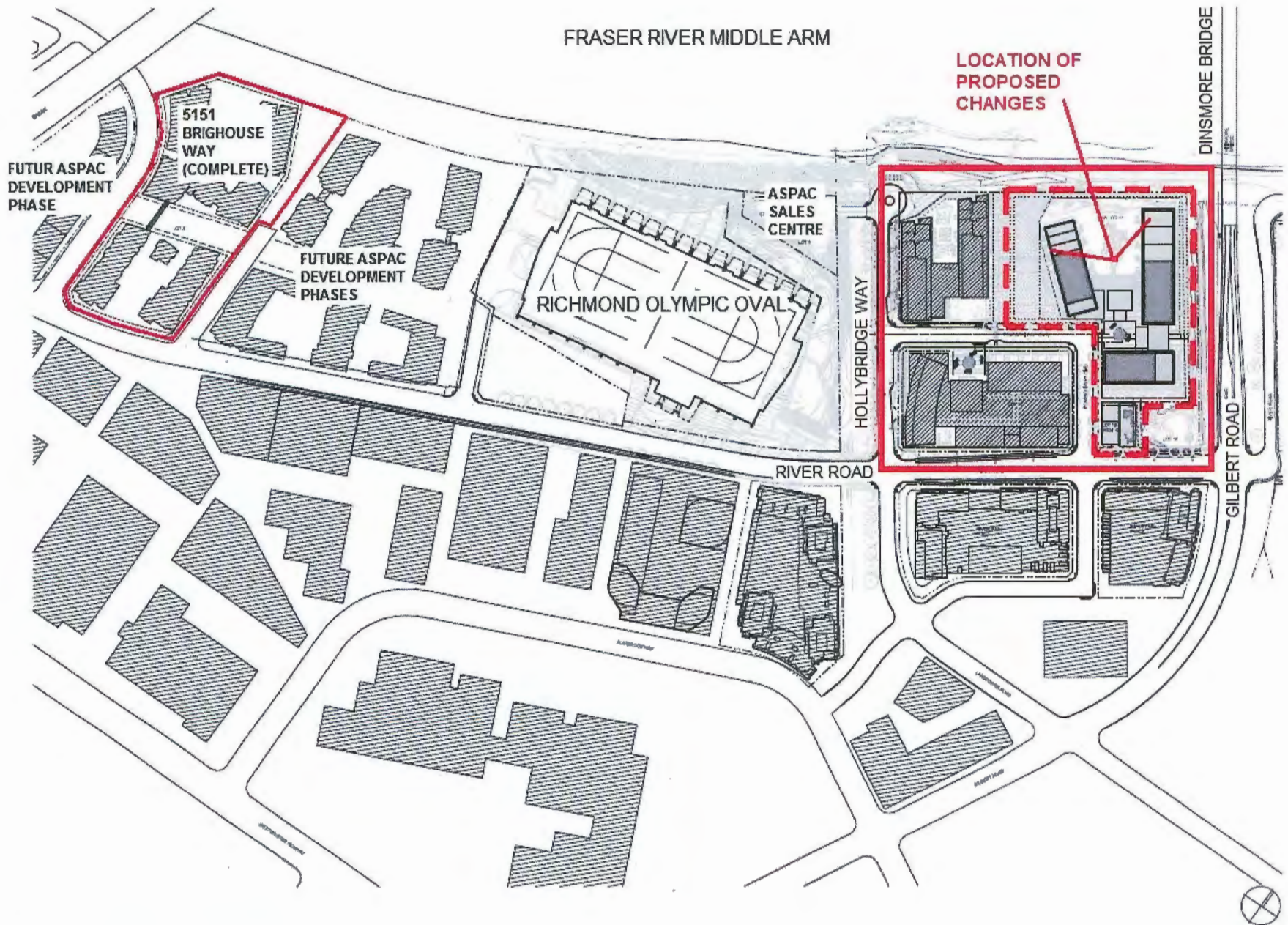


ZT 15-695231

Original Date: 04/16/15

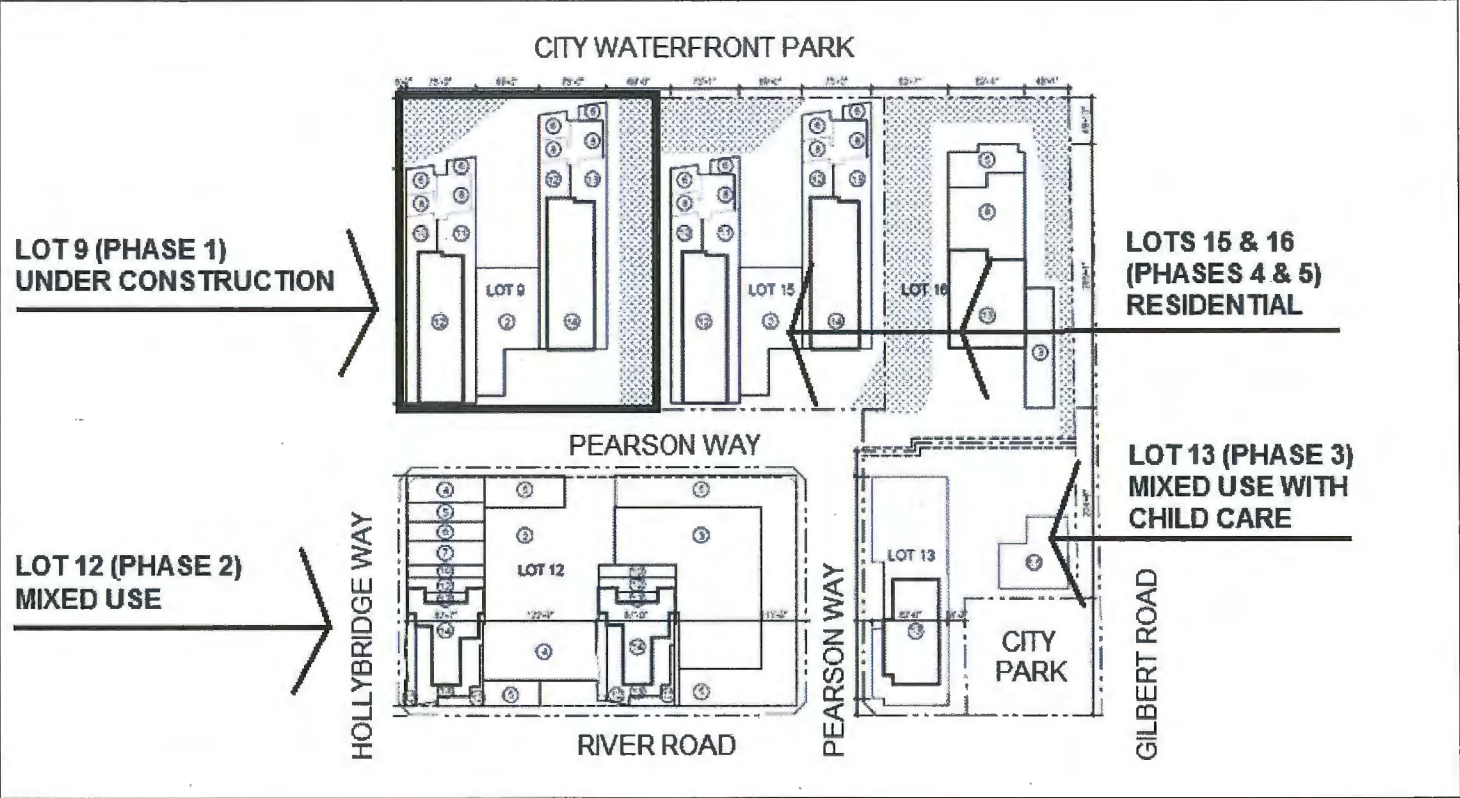
Revision Date: 10/13/15

Note: Dimensions are in METRES

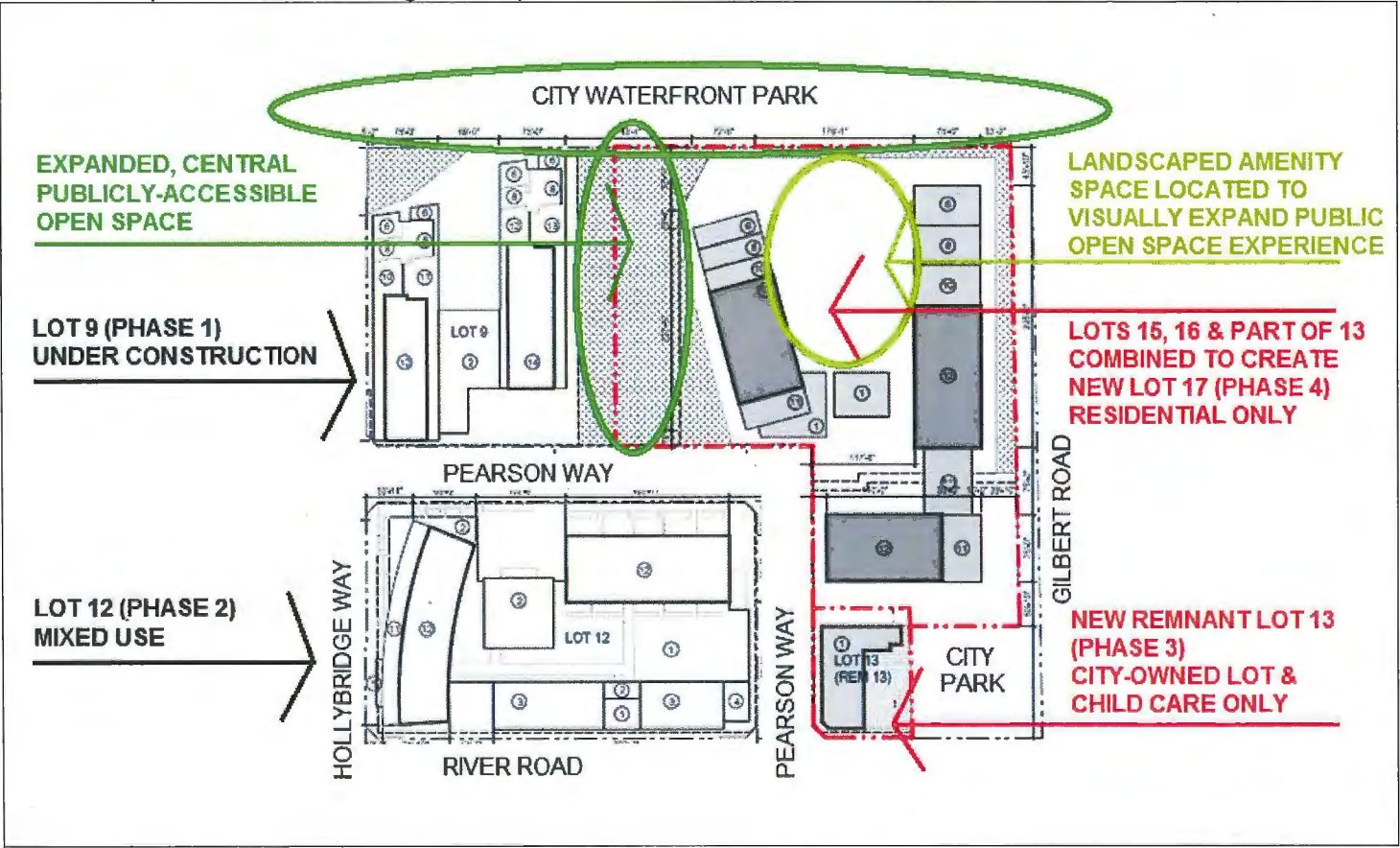


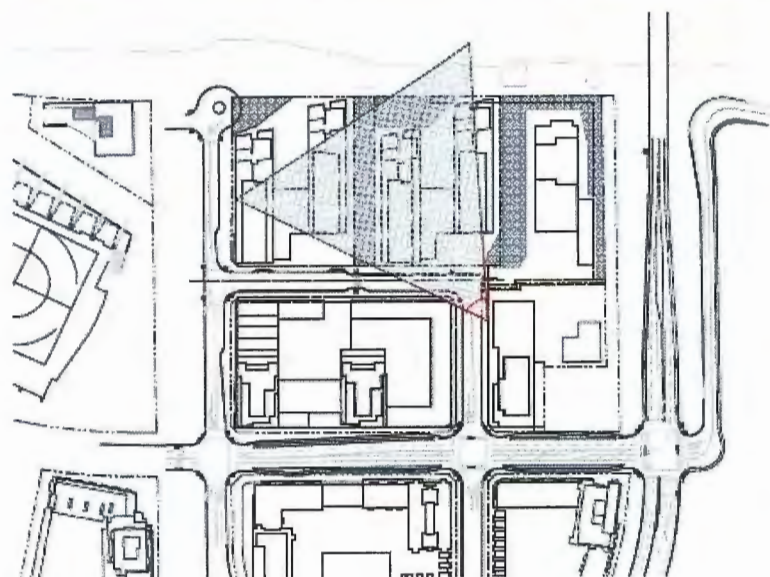
Attachment 3
 "River Green" Location Map

Original Site Planning Concept



Proposed Site Planning Concept

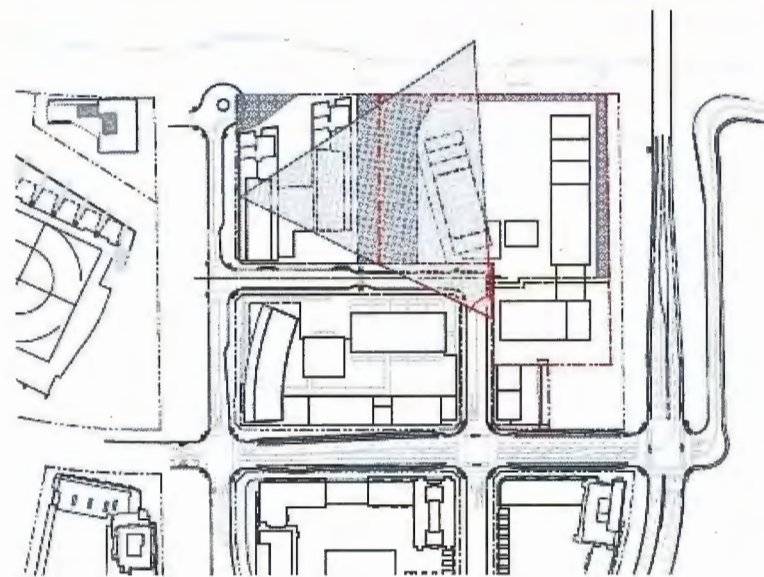




Original Master Plan View from Pearson Way (N-S)
 • Two narrow pedestrian mews
 • Minimum views to waterfront

ORIGINAL DEVELOPMENT CONCEPT

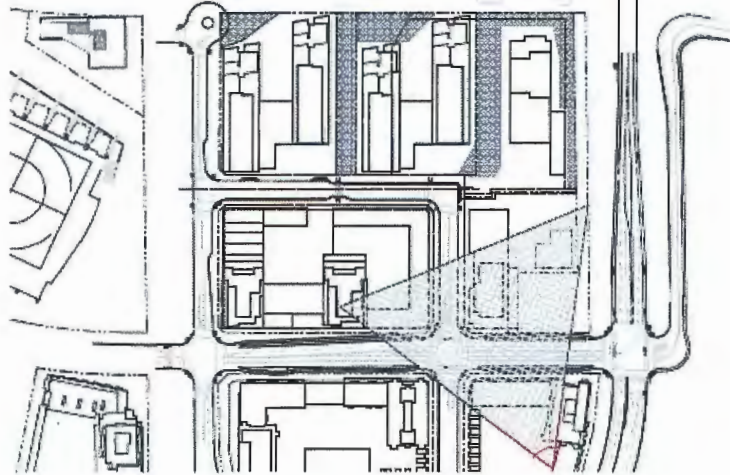
Street-level view looking north along Pearson Way opens on to buildings.



Proposed Master Plan View from Pearson Way (N-S)
 • One large integrated open space
 • Major views to waterfront

PROPOSED DEVELOPMENT CONCEPT

The same street-level view will open on to the expanded public open space & access to the City waterfront park

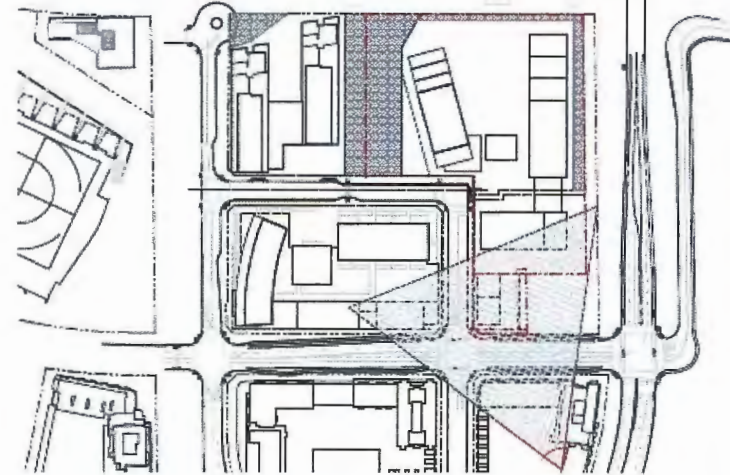


Original Master Plan Bird's Eye View from Southeast
 - Massing of 5 buildings on waterfront
 - Limit public views and open space configuration

ORIGINAL DEVELOPMENT CONCEPT

High-level view across the subject site towards the river.

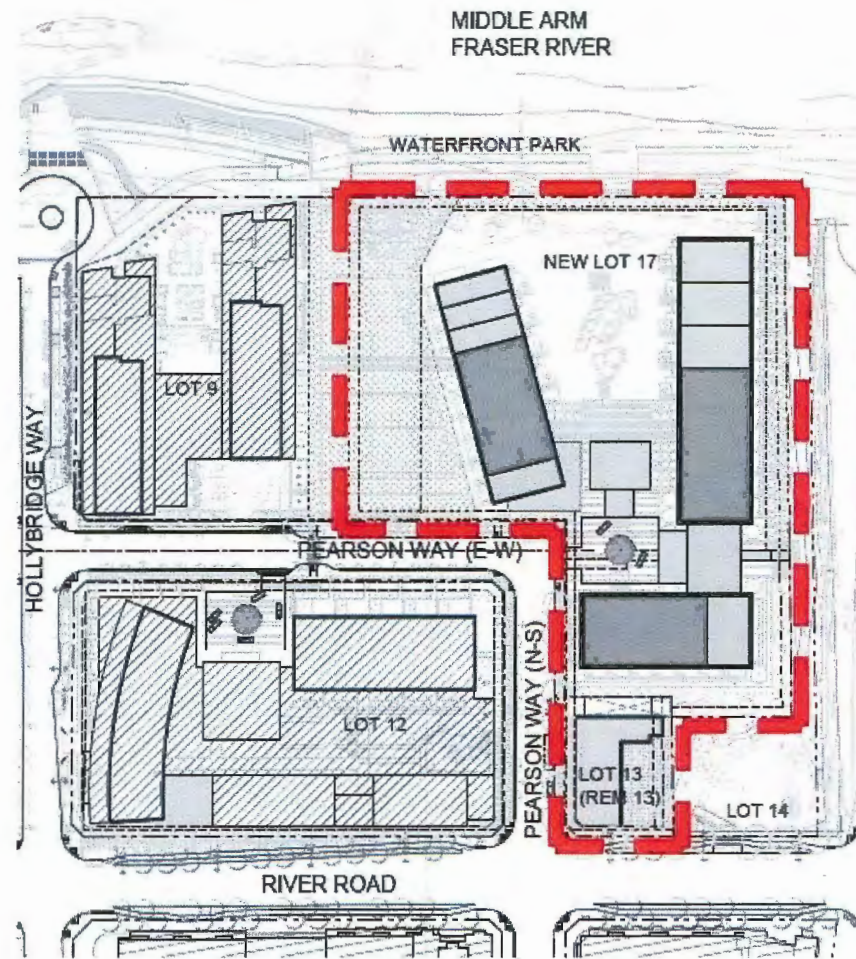
RIVER GREEN ZONING TEXT AMENDMENT
 BIRD'S EYE VIEW - OVERVIEW OF RIVER GREEN SITE



Proposed Master Plan Bird's Eye View from southeast
 - Massing of 4 buildings on waterfront
 - Major public views and one large integrated open space on waterfront

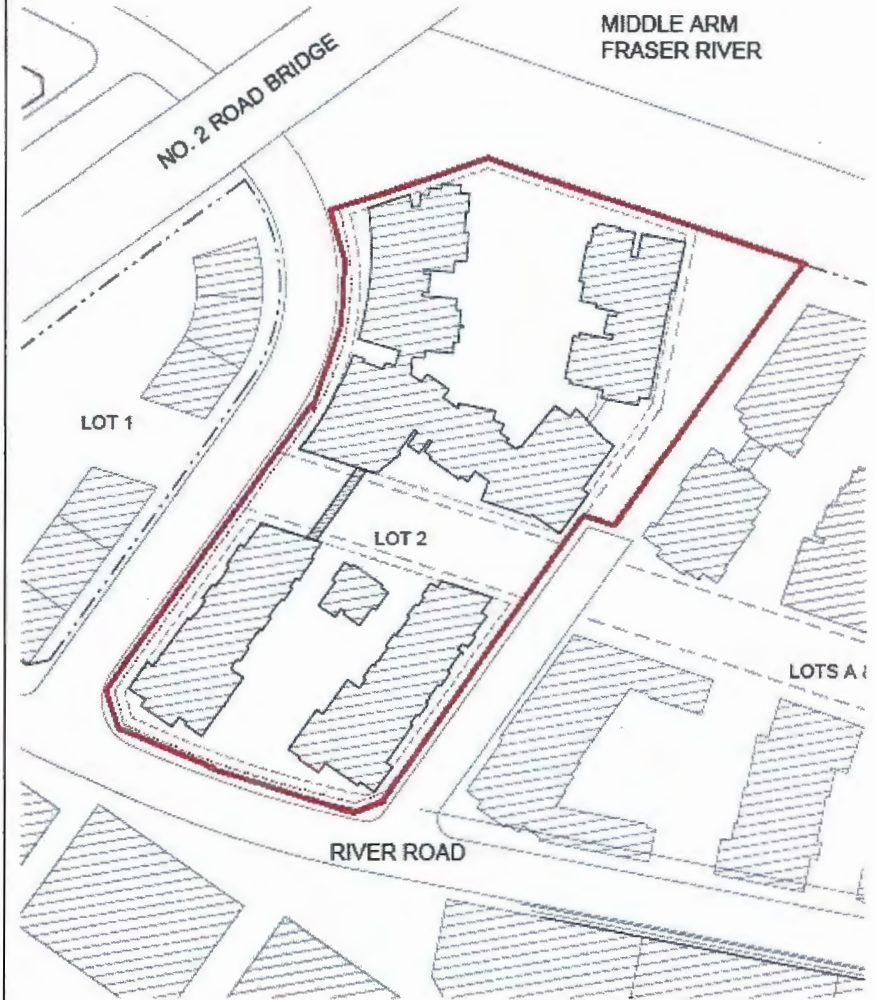
PROPOSED DEVELOPMENT CONCEPT

The same high-level view will be enhanced by reduced building height on New Remanant Lot 13 (City-owned, 1-storey child care), greater building separation between New Lot 17 & Lot 9 (under construction) & expanded landscaped areas.



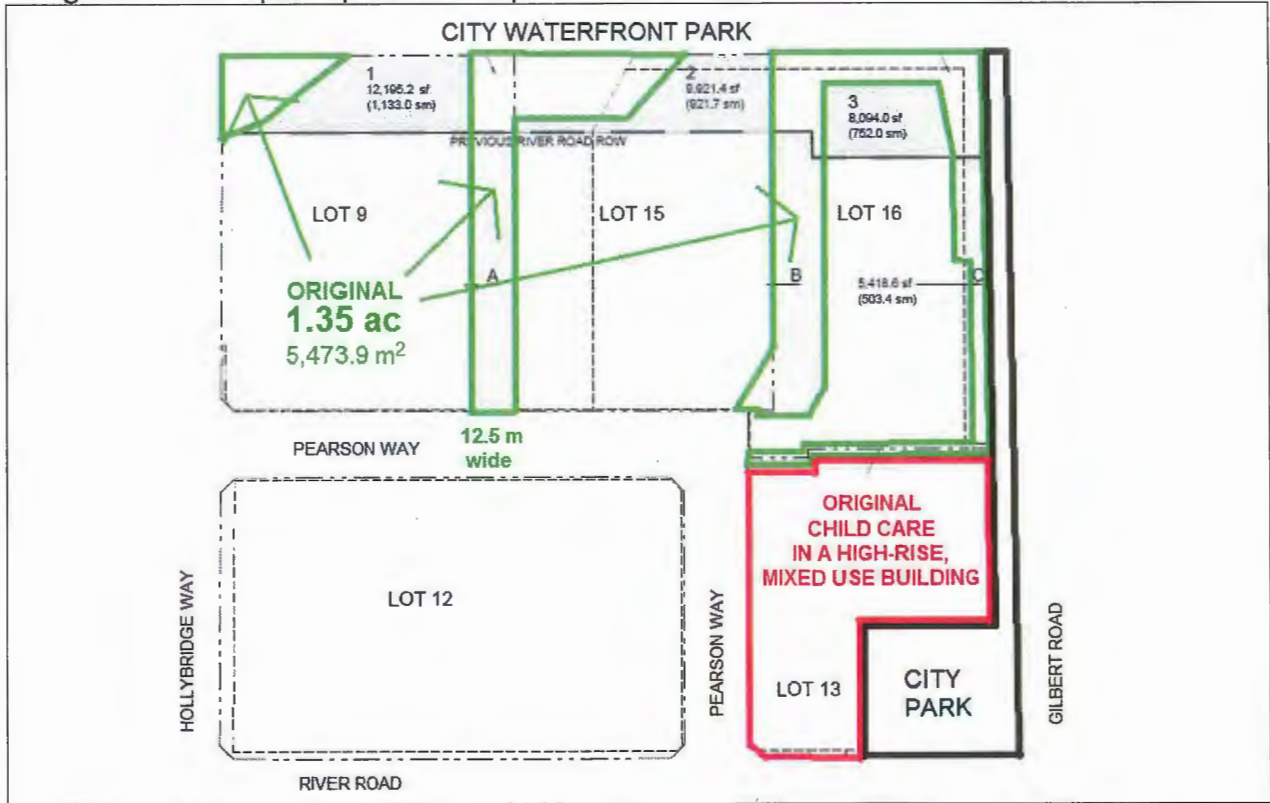
PROPOSED

The scale of building on New Lot 17 will be similar to that of the developer's recently completed buildings located west of the Richmond Olympic Oval; however, the separation between New Lot 17 towers and surrounding future towers will be greater.

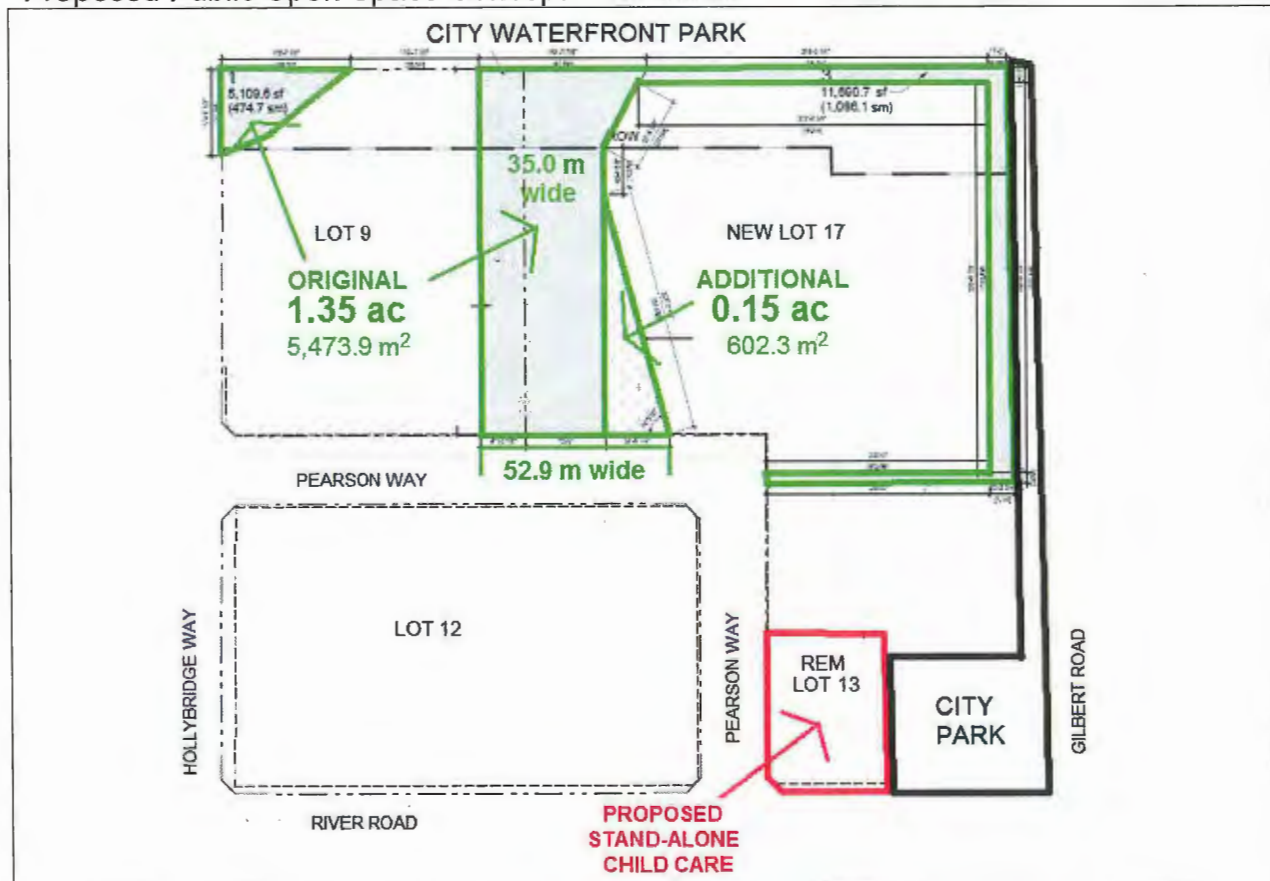


COMPLETED DEVELOPMENT @ 5151 BRIGHOUSE WAY

Original Public Open Space Concept – 1.35 acres



Proposed Public Open Space Concept – 1.5 acres



Waterfront Park / Dike Phase 2 (To be built with the construction of New Lot 17)



Phase 2 Legend Permanent Public Open Space / Park Construction of New Lot 17 Temporary Condition - Temp Dike Trail

Attachment 6
Proposed "Permanent" Public Open Space @ New Lot 17

4734828 **Waterfront Park / Dike Phase 1 Expanded** (To be built with the construction of Lot 9)



Phase 1 Expanded Legend Permanent Park Lot 9 Temporary Condition - Interim Public Open Space / Temp Dike Trail

Attachment 7
Proposed "Temporary" Public Open Space @ New Lot 17
(to be completed prior to occupancy of Lot 9)



Development Application Data Sheet
Development Applications Department

ZT 15-695231

Address: 6611, 6622, 6633, 6655, 6688, 6699, 6811, 6877, and 6899 Pearson Way

Applicant: Oval 8 Holdings Ltd. (Aspac Developments Ltd.)

Planning Area(s): City Centre (Oval Village)

	Existing	Proposed
Owner	Oval 8 Holdings Inc. No. 0805724	No change
Lot Sizes Affected by Proposed Subdivision Change	Lot 13: 4,814 m2 Lot 15: 8,066 m2 Lot 16: 6,814 m2 TOTAL: 19,694 m2	New Remnant Lot 13 (child care): 1,486 m2 New Lot 17 (residential): 18,208 m2 TOTAL: 19,694 m2
Land Uses	Lot 13: Vacant Lot 15: Vacant Lot 16: Office (mid-2016 demolition)	New Remnant Lot 13: City-owned child care New Lot 17: High-rise residential & public open space
OCP Designation	Mixed Use Park	No change
City Centre Area Plan	Urban Centre T5 Village Centre Bonus Pedestrian Linkages	No change
Zoning	High Rise Apartment and Olympic Oval (ZMU4) – Oval Village (City Centre)	No change; HOWEVER: - Zoning amendments are proposed to facilitate the requested subdivision change
Heritage	Neighbouring City park @ 6900 River Road (Lot 14) designated as a Heritage (landscape) Area	No change

	Existing ZMU4 Requirements	Proposed	Variance
Floor Area Ratio	East of Richmond Oval: 2.9 FAR	No change	None permitted
Lot Coverage	Lot 13: 90% Lot 15: 45% Lot 16: 45%	New Remnant Lot 13: 45% New Lot 17: 37%	None
Lot Size (Minimum)	Lot 13: 4,700 m2 Lot 15: 7,900 m2 Lot 16: 6,700 m2	New Remnant Lot 13: 1,485 m2 New Lot 17: 18,000 m2	None
Setback	River Road: 3.0 m Pearson Way: 3.0 m	No change	None
Height (Maximum)	Within 50.0 m of the dike: 25.0 m, but may be increased to 47.0 m based on approved design Elsewhere: 47.0 m	No change, EXCEPT: - New Remnant Lot 13 reduced to 18.0 m	None
Off-Street Parking Spaces	As per Richmond Zoning Bylaw, EXCEPT a portion of visitor parking may be located off-site	No change (i.e. maximum off-site visitor parking spaces is unchanged)	None



Zoning Text Amendment Considerations

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 6611, 6622, 6633, 6655, 6688, 6699, 6811, 6877 & 6899 Pearson Way

File No.: ZT 15-695231

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 9487, the developer is required to complete the following:

1. Registration of a Subdivision Plan, generally as indicated by the attached Preliminary Subdivision Plan (Schedule A), to consolidate and subdivide existing Lots 13, 15, and 16 to create two (2) new fee simple lots:
 - 1.1. Lot 17 (i.e. existing Lot 15, Lot 16, and north portion of Lot 13), fronting Pearson Way and measuring approximately 18,208.1 m² in size, which lot shall be used by the developer for multiple-family residential and public open space purposes; and
 - 1.2. Remnant Lot 13 (i.e. south portion of existing Lot 13), fronting River Road and Pearson Way and measuring at least 1,485.8 m² in size, which lot shall be used to satisfy the developer's City-owned affordable child care facility requirements.
2. Enter into legal agreement(s), to the satisfaction of the City, to secure transfer of ownership of new Remnant Lot 13, measuring at least 1,485.8 m² in size (Schedule A), together with the child care facility required to be constructed by the developer, at the developer's sole cost, on new Remnant Lot 13 (as per legal agreements registered on title prior to adoption of RZ 09-460962 and updated via the subject bylaw adoption processes for ZT 15-695231), to the City for \$10 as a fee simple tenure, free and clear of encumbrances, for community amenity (e.g., child care) and related purposes.
 - 2.1. The primary business terms of the required transfer shall be to the satisfaction of the Manager, Real Estate Services, the City Solicitor, and the Director of Development and may include, but may not be limited to, requirements that:
 - 2.1.1. The lot and developer-constructed child care facility are not transferred to the City before the child care facility receives final Building Permit inspection granting occupancy; and
 - 2.1.2. All costs associated with the transfer shall be borne by the developer.
 - 2.2. Development phasing requirements with respect to the transfer of new Remnant Lot 13 and the child care facility to the City shall include, but not be limited to:
 - 2.2.1. "No development" shall be permitted on new Lot 17, restricting Development Permit* issuance for new Lot 17, in whole or in part, until the developer designs and secures construction of the required child care facility on new Remnant Lot 13 to the satisfaction of the Director of Development, Manager of Community Social Development, Senior Manager of Parks, and Director of Engineering as per an approved Development Permit* for new Remnant Lot 13;
 - 2.2.2. "No building" shall be permitted on new Lot 17 (except for City-approved improvements within the "Temporary Public Open Space" SRW area), restricting Building Permit* issuance for new Lot 17, in whole or in part, until the required child care facility is included in a Building Permit* for new Remnant Lot 13 to the satisfaction of the Director of Development, Manager of Community Social Development, Senior Manager of Parks, and Director of Engineering as per an approved Building Permit* for new Remnant Lot 13; and
 - 2.2.3. "No occupancy" shall be permitted on new Lot 17, restricting final Building Permit inspection granting occupancy for new Lot 17, in whole or in part, until the entirety of the required child care facility on new Remnant Lot 13 receives final Building Permit* inspection granting occupancy.

3. Granting of a Public Rights of Passage statutory right-of-way (SRW) for public open space and related purposes on new Lot 17, generally as indicated by the attached Preliminary Permanent Public Open Space SRW Plan (Schedule B).
 - 3.1. The size of the SRW area shall be at least 4,325.1 m², which shall be comprised of the area of:
 - 3.1.1. The original 5,256.9 m² “adjacent to the waterfront park” SRW area registered on Lots 9, 10, and 11, as per RZ 09-460962 (BB1991702 to BB1991705 / Plan BCP49386) and amended upon the creation of Lot 15 (formerly Lot 10) and 16 (formerly Lot 11), *excluding* the 1,751.1 m² SRW area subsequently registered on Lot 9 via DP 11-587954, as shown in Schedule C (CA3218553 to CA3218556, CA3218557 to CA3218560, and CA3218561 to CA3218564), *which “adjacent to the waterfront park” SRW shall be discharged and replaced prior to adoption of the subject bylaw: 3,505.8 m², which area includes:*
 - a) Existing Lot 15 SRW area (as shown in Schedule C): 1,733.1 m²;
 - b) Existing Lot 16 SRW area (as shown in Schedule C): 1,645.3 m²; and
 - c) Remnant SRW area NOT accommodated on Lot 9 via DP 11-587954: 127.4 m²;
 - 3.1.2. The existing “pedestrian walkway and utilities” SRW along the common property line of existing Lots 13 and 16 (BB1991712 & BB1991713 / Plan BCP49388), *which “pedestrian walkway and utilities” SRW shall be discharged and replaced prior to adoption of the subject bylaw: 217.0 m² (i.e. 108.5 m² on each lot); and*
 - 3.1.3. The developer’s proposed additional public open space SRW area on new Lot 17 (i.e. over and above the total “adjacent to the waterfront park” and “pedestrian walkway and utilities” SRW areas required via RZ 09-460962): 602.3 m².
 - 3.2. The SRW area shall provide for the same uses, encroachments, and related activities and features as generally provided for under the existing “adjacent to the waterfront park” and “pedestrian walkway and utilities” SRW areas, as determined to the satisfaction of the City, which shall include, but may not be limited to:
 - 3.2.1. 24-hour-a-day, universally accessible, pedestrian, bicycle, and emergency and service vehicle access, together with related uses, features, utilities, and bylaw enforcement;
 - 3.2.2. Building encroachments into the SRW area in the form of parking structures and related features, provided that such encroachments are located entirely below finished grade and do not compromise the City’s intended public use or enjoyment, design quality, or landscaping (e.g., tree planting) of the SRW area, as determined to the satisfaction of the Director of Development and Senior Manager, Parks via the City’s standard Development Permit* and Servicing Agreement* processes;
 - 3.2.3. Revisions to the boundary of the SRW area, as determined to the satisfaction of the Director of Development and Senior Manager, Parks via the City’s standard Development Permit* processes, provided that the intended public use of the SRW area is not compromised and the minimum required size of the SRW area is not reduced (i.e. at least 4,325.1 m²); and
 - 3.2.4. The design, construction, and maintenance of the right-of-way and all associated liability to be the responsibility of the owner, as determined to the satisfaction of the City.

Note: The developer shall not be eligible for Development Cost Charge credits with respect to public open space or related improvements within the SRW area.
 - 3.3. In addition to Section 3.2, the SRW shall expressly provide for its development, in part, as a Temporary Public Open Space, which shall include, but may not be limited to:
 - 3.3.1. A strip of land for public use/access (within the SRW area) measuring at least 20.0 m wide along the entire west property line of new Lot 17, together with additional space (not intended for general public uses/access) to accommodate a landscape buffer (e.g., hedge, fence), slope, retaining walls, and/or other features necessary to safely and attractively demise the publicly-accessible portion of the Temporary Public Open Space from the remainder of new Lot 17 and address the differential in finished grade between the two areas;

- 3.3.2. Revisions to the boundary of the Temporary Public Open Space SRW area, as determined to the satisfaction of the Director of Development and Senior Manager, Parks via the City's standard Servicing Agreement* processes, provided that the total size of the publicly-accessible portion of the Temporary Public Open Space is not reduced;
- 3.3.3. 24-hour-a-day, universally accessible, pedestrian, bicycle, and emergency and service vehicle access, together with related uses, features, utilities, and bylaw enforcement;
- 3.3.4. No building encroachments within the publicly-accessible portion of the Temporary Public Open Space;
- 3.3.5. Raising the finished grade of the publicly-accessible portion of the Temporary Public Open Space to that of the adjacent City-owned dike/park;
- 3.3.6. The design, construction, and maintenance of the affected SRW area and all associated liability to be the responsibility of the owner, as determined to the satisfaction of the City; and
- 3.3.7. Maintenance of the Temporary Public Open Space for uninterrupted public use until such time as the City agrees, in writing, that it must be removed, in whole or in part, to facilitate the development of new Lot 17 and construction of permanent public open space features within the SRW area in compliance with an approved Development Permit*.

Note: The developer shall not be eligible for Development Cost Charge credits with respect to the Temporary Public Open Space.

- 3.4. Legal agreements shall be registered on title with respect to the owner's development of public open space works within the SRW area as follows:
 - 3.4.1. Prior to adoption of the subject Zoning Amendment Bylaw 9487, the developer shall expand the scope of the developer's approved Servicing Agreement for dike and waterfront park works (SA 13-633917 and SA 13-633918), as determined to the satisfaction of the City, to include:
 - a) The design and construction of the Temporary Public Open Space, including raising the finished grade of the publicly-accessible portion of the park to that of the adjacent City-owned dike/park (e.g., fill, lawn, gravel path, landscape buffer with fence, grade-change treatment);
 - b) The design and construction of related improvements within the City-owned dike and waterfront park along the north side of new Lot 17; and
 - c) Letter of Credit, as required, for the expanded scope of works.
 - 3.4.2. For Lot 9, no final Building Permit inspection shall be granted, restricting occupancy of Lot 9 in whole or in part, until the Temporary Public Open Space on new Lot 17 and related improvements within the City-owned dike/waterfront park area are completed to the satisfaction of the City, as per SA 13-633918.
 - 3.4.3. For new Lot 17:
 - a) "No development" shall be permitted, restricting Development Permit* issuance for new Lot 17, in whole or in part, until the developer designs and secures construction to the City's satisfaction (as per an approved Development Permit* and Landscape Letter of Credit) of the public open space area occupying the:
 - Entirety of the new Lot 17 SRW area; and
 - "Remnant portion" of Lot 9's contiguous Public Rights of Passage SRW area.
(See "Note" below.)
 - b) "No building" shall be permitted, restricting Building Permit* issuance for new Lot 17, in whole or in part, until the design of the entirety of the new Lot 17 SRW area and "remnant portion" of Lot 9's contiguous Public Rights of Passage SRW area (see "Note" below) are included in the Building Permit* drawings to the satisfaction of the Director of Development and Senior Manager, Parks.
 - c) "No occupancy" shall be permitted, restricting final Building Permit* inspection granting occupancy for new Lot 17, in whole or in part, until the entirety of the new Lot 17 SRW area and "remnant portion" of Lot 9's contiguous Public Rights of Passage SRW area (see "Note" below) are constructed to the satisfaction of the City.

Note: The City-approved Development Permit for Lot 9, DP 11-587954, includes landscape within two Public Rights of Passage SRWs that together encompass a space measuring 12.5 m wide and extending along the entire length of the lot's east property line. This Lot 9 SRW area will be contiguous to the subject SRW area on new Lot 17 (as shown in Schedules B and C). It is the intent of the City that the Lot 9 and new Lot 17 SRW areas are designed, constructed, maintained, and operated as one public open space area when new Lot 17's Temporary Public Open Space is completed and, subsequently, when the new Lot 17 SRW area is completed in its final form. In light of this, it is the intent of the City with respect to the "remnant" portion of the Lot 9 SRW area (i.e. an approximately 9.0 m wide strip of land situated east of the approved 3.5 m wide public walkway, to be constructed over Lot 9's parking structure) that:

- *Prior to occupancy of Lot 9, this "remnant" Lot 9 SRW area is developed as lawn, without decorative paving, furnishings, or other features (secured via the existing Landscape Letter of Credit for DP 11-587954); and*
- *Via the Development Permit*, Building Permit*, and occupancy approval processes for new Lot 17, this "remnant" Lot 9 SRW area is comprehensively designed and constructed with the SRW area located on new Lot 17.*

4. Updating of the "Child Care Terms of Reference", attached as Schedule H to the Rezoning Considerations for RZ 09-460962 and referenced in legal documents registered on title, to:
 - 4.1. Reflect the proposed change in form of development (i.e. stand-alone, one-storey building over one level of below-grade parking on a fee-simple, City-owned lot, with loading and garbage/recycling pick-up on-street/curb-side on Pearson Way);
 - 4.2. Include the "Child Care - Illustrative Concept Drawings" attached to these Zoning Text Amendment Considerations as Schedule D (which "concept drawing" shall be understood to illustrate general intent without constraining design development, as determined to the satisfaction of the Director of Development, Manager of Community Social Development, and Director of Engineering); and
 - 4.3. Require the developer to design and construct the child care facility to satisfy the following requirements, to the satisfaction of the Director of Engineering and Director of Development:
 - 4.3.1. LEED Silver (equivalent) and City District Energy Utility service/hook-up (for 70% of space heating); or
 - 4.3.2. Alternative requirements (e.g., Passive House, Net Zero Energy Building), as determined to the sole satisfaction of the City, with the aim of achieving greater energy efficiency while generally being cost-neutral to the developer (i.e. generally equal in cost to the developer's LEED and DEU requirements).
5. Registration of changes to or replacement or discharge of various existing legal agreements registered on title, which may include, but may not be limited to, the following.

Note: "TS" numbers refer to items listed in the Master Summary of Title Encumbrances submitted by the applicant on September 11, 2015 and saved as REDMS #4746201.

- 5.1. (TS2) CA2355841 – Extend the notation of ESA DP 11-593370 across the entirety of new Lot 17.
- 5.2. (TS4) AD147604 – Extend the notation of DVP No. 89-367 across the entirety new Lot 17.
- 5.3. (TS5) AD217827 – Extend the notation of DVP No. 89-367 across the entirety new Lot 17.
- 5.4. (TS6) BB1991763 – Discharge of the temporary vehicle access easement because the affected areas are consolidated.
- 5.5. (TS7) BB1991764 – Discharge of the temporary vehicle access easement because the affected areas are consolidated.
- 5.6. (TS8) BB1991771 – Discharge of the temporary vehicle access easement because the affected areas are consolidated.
- 5.7. (TS9) CA3289620 – Partial release. (Coordinate with TS35.)
- 5.8. (TS10 & TS55) BB1991772 – Discharge of the temporary vehicle access easement because the affected areas are consolidated.

- 5.9. (TS13) CA3217702 – Discharge of the visitor parking easement benefitting new Remnant Lot 13 over Lot 12. (*Note: The easement shall remain in effect for the benefit of new Lot 17 over Lot 12.*)
- 5.10. (TS14 & TS64) CA4510746 & CA4510747 – Extend the crane swing easement over the entirety of new Lot 17.
- 5.11. (TS18) BP171973 (Plan LMP46822) – Discharge of sanitary sewer agreement (i.e. sewer abandoned). (*Note: Approval of Richmond Engineering Department required.*)
- 5.12. (TS21) BB1991699 – Discharge of “no separate sale” covenant over new Remnant Lot 13. (*Note: The covenant shall remain in effect over other lots.*)
- 5.13. (TS22 & TS23) BB1991702 & BB1991703 (Plan BCP49386) – Discharge and replace the “adjacent to the waterfront park” covenant and SRW as per Zoning Text Amendment Consideration 3 (above).
- 5.14. (TS24 & TS25) BB1991712 & BB1991713 (Plan BCP49388) – Discharge and replace “pedestrian walkway and utilities” covenant and SRW on new Lot 17 as per Zoning Text Amendment Consideration 3 (above).
- 5.15. (TS28) BB1991722 – Discharge from new Lot 17 because it has no frontage along new River Road. (Remains on new Remnant Lot 13.)
- 5.16. (TS29) BB1991723 (Plan BCP49390) - Discharge from new Lot 17 because it has no frontage along new River Road. (Remains on new Remnant Lot 13.)
- 5.17. (TS30, TS31, TS32 & TS33) BB1991727 (Plan BCP49391), CA3289616, BB1991728 & BB1991729 – Discharge and replace with new agreement on new Lot 17.
- 5.18. (TS34, TS35, TS36, TS37, TS38, TS39 & TS40) BB1991739, CA3289618, CA3377759, BB1991741, CA3377761, BB1991745 & CA3377763 – Discharge and replace with separate “no development” covenants on new Lot 17 and new Remnant Lot 13, and make corresponding revisions to ensure consistency between the locations of the new lots and various development requirements and features. In addition, clarification shall be provided with respect to:
 - 5.18.1. Phasing – The order of phasing shall be Lot 9, Lot 12, new Remnant Lot 13 (City child care), and new Lot 17.
 - 5.18.2. District Energy Utility (DEU) – The developer is required to use DEU for a minimum of 70% of space heating;
 - 5.18.3. Park – Replace the meaning of “Pedestrian Walkway” with that portion of the proposed Public Rights of Passage SRW on new Lot 17 linking Lot 14 (City park) with Pearson Way and make the design and construction of the SRW area the sole responsibility of the developer of new Lot 17 (as per Zoning Text Amendment Consideration 3, above).
 - 5.18.4. Child Care – The “Child Care Terms of Reference” attached as Schedule H to the Rezoning Considerations for RZ 09-460962 shall be updated as per Zoning Text Amendment Consideration 4 (above).
- 5.19. (TS43, TS44, TS62 & TS63) BB1991751, BB1991752, CA3289625 & CA3289626 – Discharge and replace Airport Noise Covenants.
- 5.20. (TS49, TS50, TS51, TS52, TS53, TS54 & TS55) BB1991763, BB1991764, BB1991765, BB1991766, BB1991771, BB1991772 & CA3289621 (Plan BCP49391) – Discharge easements and related covenants because lots are to be consolidated.
- 5.21. (TS56) BB1991773 - Discharge because lots are to be consolidated.
- 5.22. (TS57) BB1991774 - Discharge because lots are to be consolidated.
- 5.23. (TS59) BB1350876 – Extend over the portion of Lot 13 within new Lot 17.
- 5.24. (TS61) CA3217706 & CA3217707 – Discharge from new Remnant Lot 13 (City lot).

NOTE:

* This requires a separate application.

- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial Wildlife Act and Federal Migratory Birds Convention Act, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

SIGNED COPY ON FILE (REDMS #4761012)

Signed _____

Date _____

SCHEDULE A
Preliminary Subdivision Plan (REDMS #4743975)

**PROPOSED SUBDIVISION PLAN OF PARTS OF SECTIONS 5 AND 6
BLOCK 4 NORTH RANGE 6 WEST AND SECTION 31
BLOCK 5 NORTH RANGE 6 WEST NWD**

PLAN EPP _____

LANDS SUBDIVIDED

LOT 15 Sec 6 Bk4N R6W
NWD PLAN EPP30101

LOT 16 Sec 5 AND 6 Bk4N R6W AND Sec 31
Bk5N R6W NWD PLAN EPP30101

PART OF LOT 13 Sec 5 AND 6 Bk4N R6W
NWD PLAN BCP49385

DRAFT

SCALE 1:1000

20 0 20 40 60 METRES

ALL DISTANCES ARE IN METRES.

MIDDLE ARM FRASER RIVER

ORIGINAL BOUNDARY ACCORDING TO CROWN GRANT
AND PLAN 23626 AND PLAN 79376

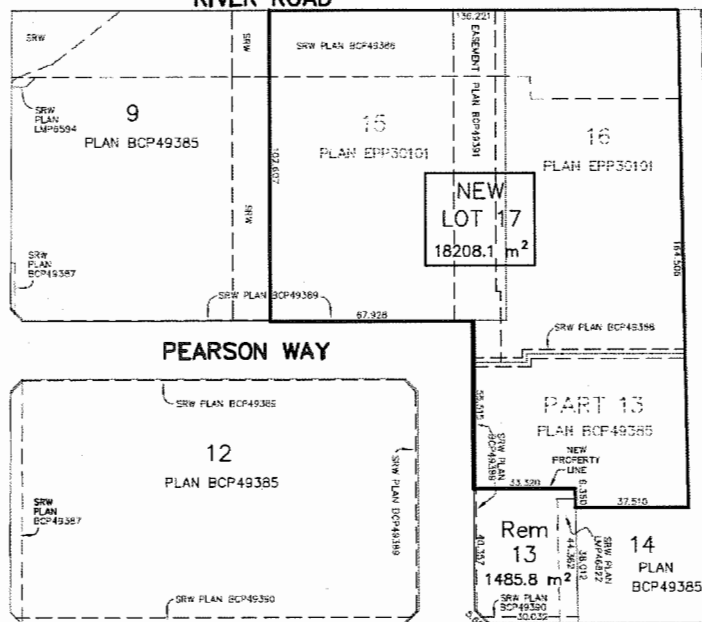
P
PLAN 23626

RIVER ROAD

(DINSMORE BRIDGE)

GILBERT ROAD
PLAN 39196

HOLLYBRIDGE WAY



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MATSON PECK & TOPLISS
SURVEYORS & ENGINEERS

#210 - 6171 COOK ROAD
RICHMOND, B.C.

V8Y 3T8

PH: 604.270.9331

FX: 604.270.4137

CADFILE: 15711-SEPT 24-PRO-SUB.DWG

R-15-15711-SUB

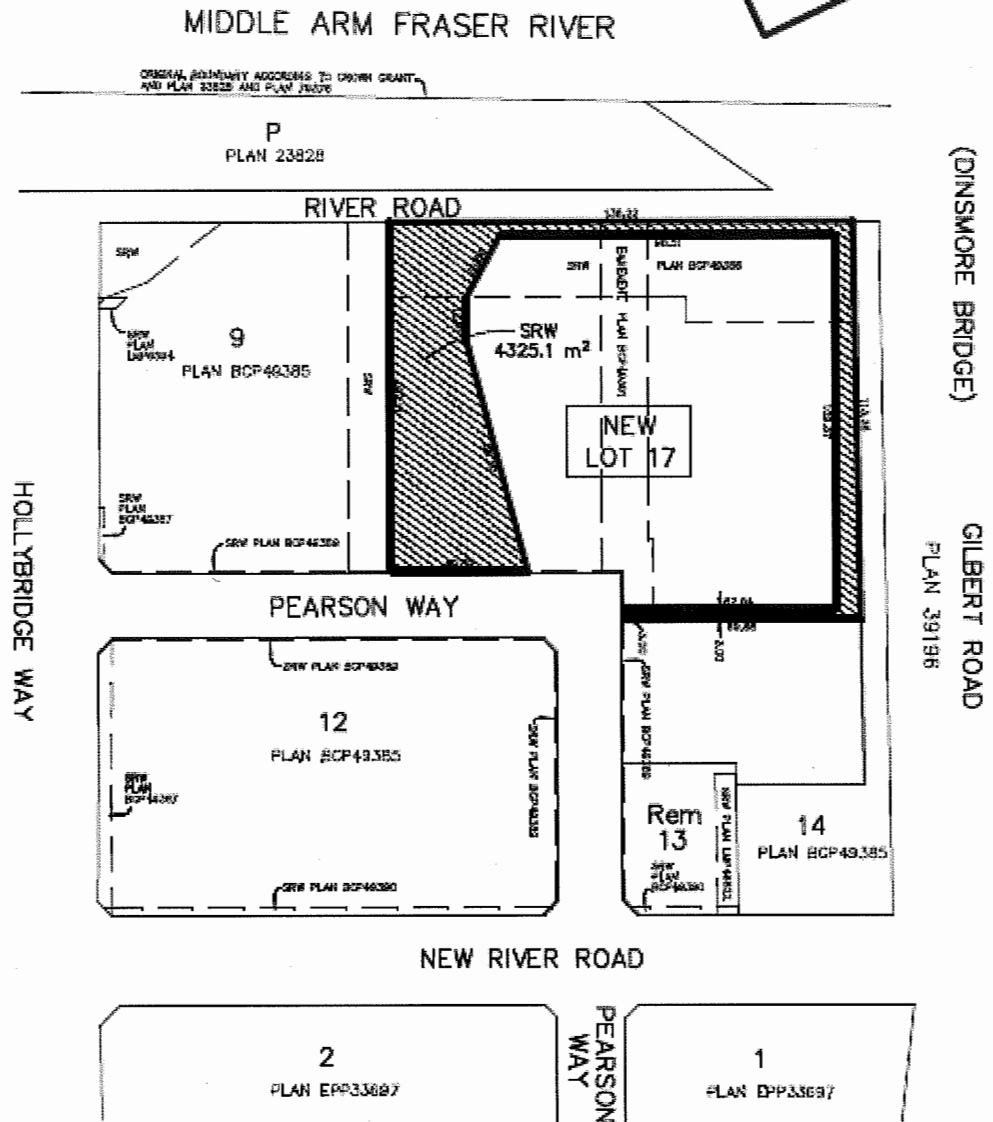
PH - 71

DATE: SEPTEMBER 24, 2015

SKETCH PLAN OF PROPOSED STATUTORY RIGHT OF WAYS
OVER NEW LOT 17 SECTIONS 5 AND 6 BLOCK 4 NORTH
RANGE 6 WEST AND SECTION 31 BLOCK 5 NORTH RANGE 6 WEST
NEW WESTMINSTER DISTRICT

FOR THE PURPOSE OF PUBLIC OPEN SPACE

DRAFT



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SURVEYORS & ENGINEERS

#320 - 11120 HORSESHOE WAY

RICHMOND, B.C., V7A 5H7

PH: 604-270-9331

FAX: 604-270-4137

CADFILE: 17941-2015-OCT 5-PRO-SRW.DWG

R-15-17941-PRO-SRW

LEGEND
SCALE 1:2000



ALL DISTANCES ARE IN METRES

DATE: OCTOBER 5, 2015

SKETCH PLAN OF EXISTING STATUTORY RIGHT OF WAYS
OVER PART OF SECTIONS 5 AND 6 BLOCK 4 NORTH
RANGE 6 WEST AND SECTION 31 BLOCK 5 NORTH RANGE 6 WEST
NEW WESTMINSTER DISTRICT

DRAFT



MIDDLE ARM FRASER RIVER

ORIGINAL AGREEMENT ADDENDUM TO 2009 GRANT
SRW PLAN 33828 AND PLAN 26576

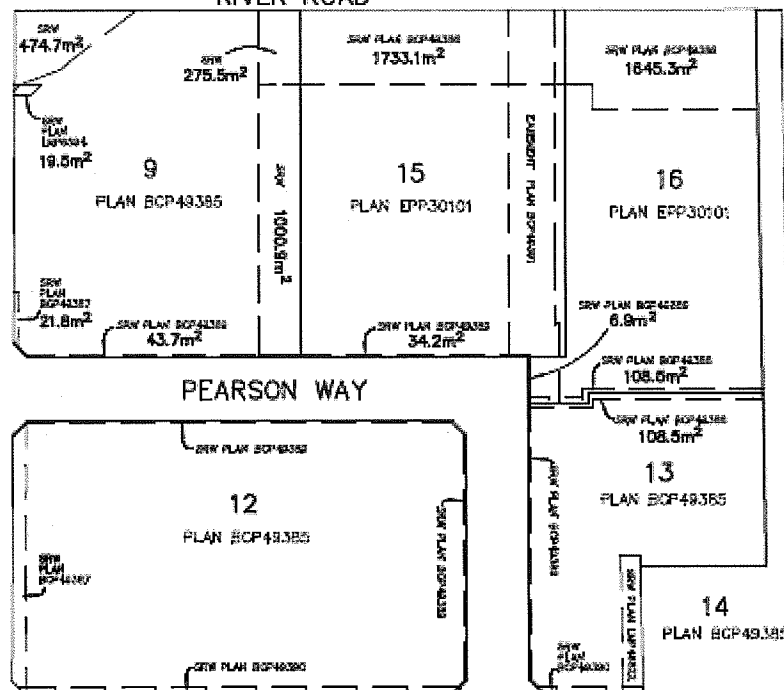
P
PLAN 23828

RIVER ROAD

(DINSMORE BRIDGE)

GILBERT ROAD
PLAN 39186

HOLLYBRIDGE WAY



PEARSON WAY

NEW RIVER ROAD

PEARSON
WAY

2
PLAN EPP33887

1
PLAN EPP33887

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SURVEYORS & ENGINEERS

#320 - 11120 HORSESHOE WAY

RICHMOND, B.C., V7A 5H7

PH: 604-270-9331

FAX: 604-270-4137

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LEGEND
SCALE 1:2000

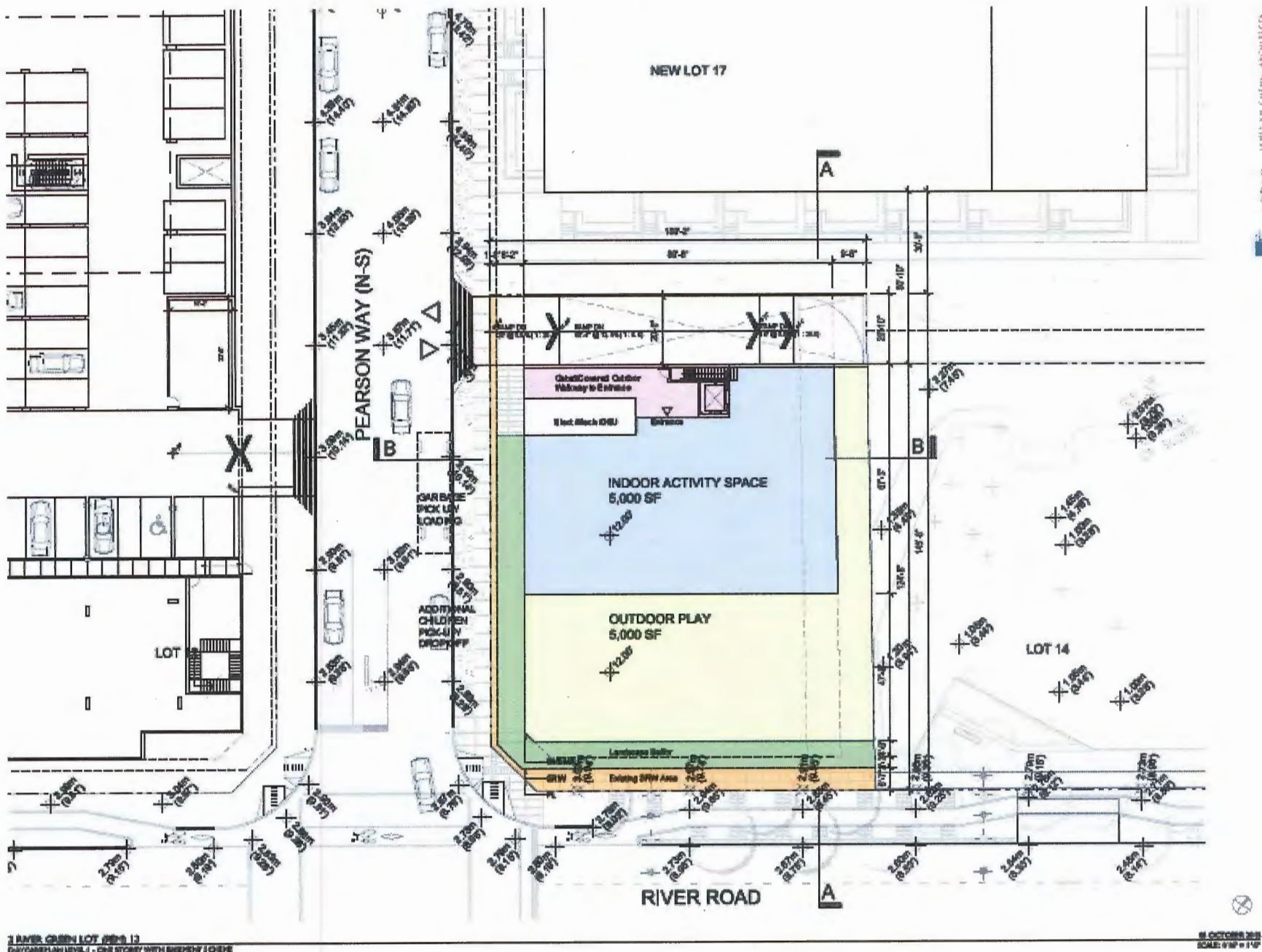


ALL DISTANCES ARE IN METRES

R-15-17941-EX-SRW

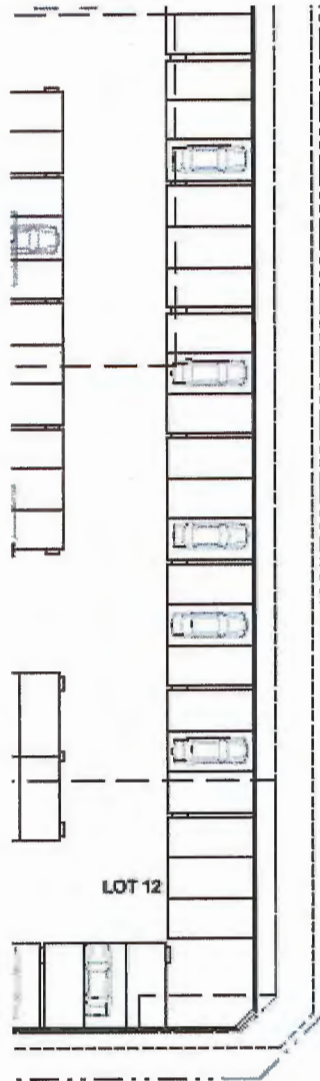
DATE: OCTOBER 6, 2015

SCHEDULE D
Child Care – Illustrative Concept Drawings (REDMS #4750570)



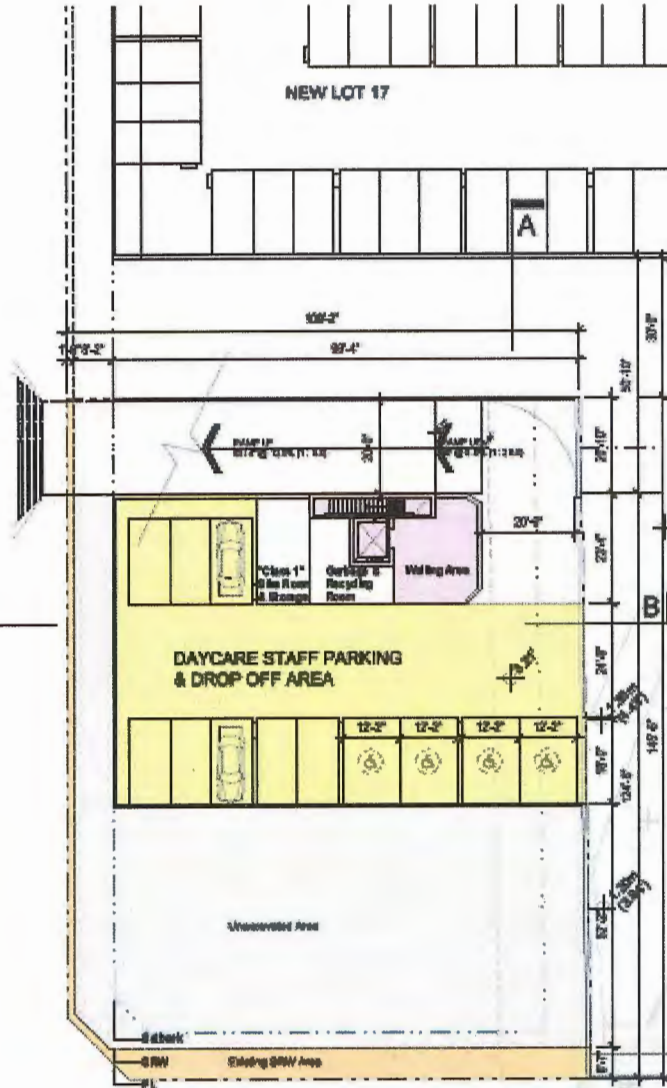
1 RIVER GLEN LOT 1000 12
DAYCARE PLAN LEVEL - ONE STORY WITH SHEDS & DRIVE

11 OCTOBER 2011
SCALE: 1/8" = 1'-0"



PEARSON WAY (N-S)

B



RIVER ROAD

A

Site Statistics

Original Site Area:	13,712 SF
Setbacks Area:	2,176 SF
West and South East:	1,138 SF
Net Buildable Area:	10,412 SF
(with East, West, & South Setbacks)	
Net Buildable Area:	11,542 SF
(with West, & South Setbacks)	
Proposed Site Area:	10,000 SF
Proposed Additional Setback:	
East:	205 SF
West:	205 SF
Proposed Net Buildable Area:	12,458 SF
(with East, West, & South Setbacks)	
Proposed Net Buildable Area:	13,818 SF
(with West, & South Setbacks)	

Parking Requirement

7.2.2.3 0.75 space per employee;
plus 1 space for each 10 children in care

9 Parking Stalls

Bicycle Requirement

7.14.9.1 Class 1: 0.27 spaces per each 1,076 SF
Class 2: 0.4 spaces per each 1,076 SF

2 Bike Stalls
2 Bike Racks**Garbage Requirement**

- 1 Cardboard Bin - 4 cu yd
- 1 Garbage Bin - 4 cu yd
- 1 Black Garbage Cart - 65 gallon
- 1 Paper Blue Recycling Cart - 65 gallon
- 1 Containers Blue Recycling Cart - 65 gallon
- 1 Food Scraps Green Cart - 65 gallon
- 1 Returnable Bottles Recycling Cart - 65 gallon

2 RIVER GREEN LOT (REDMS #4750570)
DAYCARE PLAN LEVEL 1 - ONE-STORY WITH BASEMENT

18 OCTOBER 2011
SCALE: 1/8" = 1'-0"

ASAPAC
ARCHITECTS
P.C.

SCHEDULE D
Child Care - Illustrative Concept Drawings (REDMS #4750570)

SCHEDULE D
 Child Care – Illustrative Concept Drawings (REDMS #4750570)

5. THELMA SHING & ASSOCIATES



2 RIVER GREEN LOT (REDMS) 13
 DAYCARE SECTION A-A - AT INDOOR AND OUTDOOR AREAS AND UNDERGROUND PARKING

15 OCTOBER 2018
 SCALE: 1/8" = 1'-0"

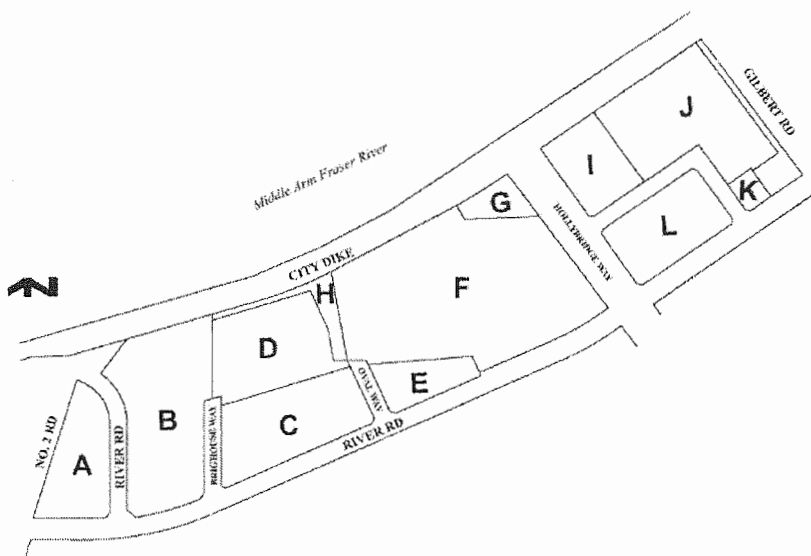




**Richmond Zoning Bylaw 8500
Amendment Bylaw 9487 (15-695231)
6611, 6622, 6633, 6655, 6688, 6699, 6811, 6877, and 6899 Pearson Way**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by:
 - 1.1. In Section 20.4.2 Permitted Uses:
 - 1.1.1. Inserting “**amenity space, community**” as a Permitted Use; and
 - 1.1.2. Repealing Diagram 1 and replacing it with following:



- 1.2. In Section 20.4.4 Permitted Density:
 - 1.2.1. In Sub-Section 1, repealing ““L”, and “M”” and replacing it with “and “L””;
 - 1.2.2. In Sub-Section 2(d), repealing ““L”, and “M”” and replacing it with “and “L””;

1.2.3. Repealing Sub-Section 3 and replacing it with the following:

“Notwithstanding Sections 20.4.4.2d, the reference to “1.2” is increased to a higher **density** of “2.9” provided that:

- a) for the area identified as “I”, “J”, “K”, and “L” in Diagram 1, Section 20.4.2, prior to first occupancy of the **building**, the **owner**:
 - i) provides a **community amenity** contribution of \$1 million to the **City** for the Oval Village waterfront;
 - ii) pays or secures to the satisfaction of the **City** a monetary contribution of \$6,791,769 to the **City’s** capital Affordable Housing Reserve Fund established pursuant to Reserve Fund Establishment Bylaw No. 7812;
 - iii) grants to the **City**, via a statutory **right-of-way** or as otherwise determined to the satisfaction of the **City**, rights of public use over a suitably landscaped area of the **site**, at least 6,076.2 m² in size, for **park** and related purposes (but excluding sidewalk widening) within the areas identified as “I” and “J” in Diagram 1, Section 20.4.2, including:
 - for “I”: an area of at least 1,751.1 m²; and
 - for “J”: an area of at least 4,325.1 m²; and
 - iv) enters into legal agreement(s) with the **City**, registered against the title to the **lot** and secured via Letter(s) of Credit, at the sole cost of the **owner** and in an amount to be determined to the satisfaction of the **City**, for **child care**; and
- b) prior to first occupancy of the **building** within the area identified as “J” and “K” in Diagram 1, Section 20.4.2, the **owner**, within the area identified as “K” in Diagram 1, Section 20.4.2:
 - i) provides a **child care** facility, the **habitable space** of which shall be at least 464.5 m², excluding **floor area** not intended for the exclusive **use** of the **child care** and **floor area** not included in the calculation of **floor area ratio**; and
 - ii) transfers the **child care** facility and not less than 1,485.0 m² of land to the **City** as fee simple for **child care** or **community amenity space** purposes.”

1.2.4. Repealing Sub-Section 4 and replacing it with the following:

“Notwithstanding Sections 20.4.4.1, 20.4.4.2, and 20.4.4.3, for the area identified as “A”, “B”, “C”, “D”, “E”, “F”, “G”, “H”, “I”, “J”, and “L” in Diagram 1, Section 20.4.2:

- a) the maximum total combined **floor area**, regardless of subdivision, shall not exceed 454,013.2 m², of which the **floor area** of residential

uses shall not exceed 292,929.6 m² and the **floor area** of other **uses** shall not exceed 161,083.6 m²; and

- b) the maximum **floor area** for the areas indicated as “A”, “B”, “C”, “D”, “E”, “F”, “G”, “H”, “I”, “J”, and “L” shall not exceed:
 - i) for “A”, “C”, and “D”: 116,572.1 m², total combined **floor area** regardless of subdivision, for residential **uses** and nil for other **uses**;
 - ii) for “B”: 65,480.0 m² for residential **uses** and nil for other **uses**;
 - iii) for “E”, “F”, and “H”: , total combined **floor area** regardless of subdivision, nil for residential **uses** and 155,456.0 m² for other **uses**;
 - iv) for “G”: nil for residential **uses** and 2,365.7 m² for other **uses**;
 - v) for “I”: 27,650.0 m² for residential **uses** and nil for other **uses**; and
 - vi) for “J” and “L”: , total combined **floor area** regardless of subdivision, 83,227.5 m² for residential **uses** and 3,261.9 m² for other **uses** (provided that all non-residential **uses** are located on “L”).”; and

1.2.5. In Sub-Section 6, repealing “**child care purposes**” and replacing it with “**child care or community amenity space purposes** within the area identified as “K” in Diagram 1, Section 20.4.2”;

1.3. In Section 20.4.5 Permitted Lot Coverage:

1.3.1. In Sub-Section 2, repealing ““L”, and “M”” and replacing it with “and “L””;

1.3.2. In Sub-Section 5, repealing ““I”, “J”, and “K”” and replacing it with ““I” and “K””; and

1.3.3. Following Sub-Section 5, inserting a new Sub-Section 6 as follows:

“For the area identified as “J” in Diagram 1, Section 20.4.2, the maximum **lot coverage** shall be 37% (which for the purposes of this bylaw for area “J” only shall mean the percentage of the total horizontal **lot area** that is covered by **buildings** and all enclosed and/or supported **structures**, including landscaped roofs over **parking spaces** where such roofs are situated above **finished site grade**, but excluding **eaves**, **balconies**, unroofed patios and raised decks, and landscaped roofs over **parking spaces** where such landscaped roofs are situated at or below **finished site grade**). A minimum of 40% of the **lot** shall be covered by a combination of trees, shrubs, native and ornamental plants or other landscape material specified in a Development Permit approved by the **City**.”

- 1.4. In Section 20.4.6 Yards & Setbacks:
 - 1.4.1. In Sub-Section 1(e), following “Notwithstanding Section 20.4.6.1:a.ii,” inserting “in the area identified as “L” in Diagram 1, Section 20.4.2,”;
 - 1.4.2. In Sub-Section 2(a), repealing ““L”, “M”” and replacing it with “and “L””; and
 - 1.4.3. In Sub-Section 2(b), repealing “less then 3.0 m” and replacing it with “less than 3.0 m”;
- 1.5. In Section 20.4.7 Permitted Heights:
 - 1.5.1. In Sub-Section 4, repealing ““G” and H” and replacing it with ““G”, “H”, and “K””;
 - 1.5.2. In Sub-Section 6, in the opening phrase, repealing “and “K”” and replacing it with “and “L””; and
 - 1.5.3. In Sub-Section 6(c), repealing “Section 20.4.7.5.b” and replacing it with “Section 20.4.7.6.b”;
- 1.6. In Section 20.4.8 Subdivision Provisions/Minimum Lot Size:
 - 1.6.1. In Sub-Section 2(i), repealing “7,900.0 m²” and replacing it with “18,000.0 m²”;
 - 1.6.2. In Sub-Section 2(j), repealing “6,700.0 m²” and replacing it with “1,485.0 m²” and, at the end of the Sub-Section, inserting “and”;
 - 1.6.3. In Sub-Section 2(k), at the end of the Sub-Section, repealing “and”; and
 - 1.6.4. Repealing Sub-Section 2(l);
- 1.7. In Section 20.4.10 On-Site Parking and Loading:
 - 1.7.1. In Sub-Section 2(c), in the opening phrase, repealing ““I”, “J”, “K”, and “M” and replacing it with ““I” and “J””;
 - 1.7.2. In Sub-Section 2(c)(i), repealing ““K”, “L”, and “M” and replacing it with “and “L””;
 - 1.7.3. Repealing Sub-Section 2(c)(iii) and replacing it with the following:

“a minimum of 12 residential visitor **parking spaces** are provided on area “I” and a minimum of 36 residential visitor **parking spaces** are provided on area “J”.”; and
 - 1.7.4. Repealing Sub-Section 2(c)(iv);

- 1.8. In Section 20.4.11 Other Regulations:
 - 1.8.1. Repealing Sub-Section 1;
 - 1.8.2. Repealing Sub-Section 2;
 - 1.8.3. Repealing Sub-Section 4;
 - 1.8.4. Repealing Sub-Section 5 and replacing it with the following:

“The following **uses** are permitted within the area identified as “K” in Diagram 1, Section 20.4.2:

 - a) **amenity space, community**; and
 - b) **child care.**”;
 - 1.8.5. Repealing Sub-Section 6; and
 - 1.8.6. In Sub-Section 7, repealing ““L”, and “M”” and replacing it with “and “L””; and
 - 1.9. Making various text and graphic amendments to ensure consistency throughout the Richmond Zoning Bylaw 8500 as amended.
2. This Bylaw may be cited as “**Richmond Zoning Bylaw 8500, Amendment Bylaw 9487**”.

FIRST READING

PUBLIC HEARING

SECOND READING

THIRD READING

OTHER CONDITIONS SATISFIED

ADOPTED

OCT 26 2015

CITY OF RICHMOND
APPROVED by <i>hu</i>
APPROVED by Director or Solicitor <i>al</i>

MAYOR_____
CORPORATE OFFICER