



**Planning Committee
Electronic Meeting**

**Council Chambers, City Hall
6911 No. 3 Road**

**Tuesday, March 8, 2022
4:00 p.m.**

Pg. # ITEM

MINUTES

PLN-5 *Motion to adopt the **minutes** of the meeting of the Planning Committee held on February 23, 2022.*



NEXT COMMITTEE MEETING DATE

March 22, 2022, (tentative date) at 4:00 p.m. in the Council Chambers

PLANNING AND DEVELOPMENT DIVISION

1. **APPLICATION BY ART HOMES LTD. FOR REZONING AT 4400, 4420, AND 4440 SMITH CRESCENT FROM THE “SINGLE DETACHED (RS1/F)” ZONE TO THE “HIGH DENSITY TOWNHOUSES (RTH1)” ZONE**

(File Ref. No. RZ 18-802860; 12-8060-20-009877) (REDMS No. 6817810)

PLN-11

See Page PLN-11 for full report

Designated Speakers: Wayne Craig and Jordan Rockerbie

STAFF RECOMMENDATION

That Richmond Zoning Bylaw 8500, Amendment Bylaw 9877, for the rezoning of 4400, 4420, and 4440 Smith Crescent from the “Single Detached (RS1/F)” zone to the “High Density Townhouses (RTH1)” zone, be introduced and given first reading.



2. **APPLICATION BY TERRA SPIRES DEVELOPMENT LP FOR REZONING AT 8740, 8760, 8780 AND 8800 SPIRES ROAD, AND THE SURPLUS PORTION OF THE SPIRES ROAD ROAD ALLOWANCE FROM SINGLE DETACHED (RS1/E) TO PARKING STRUCTURE TOWNHOMES (RTP4)**

(File Ref. No. RZ 19-870807; 12-8060-20-010357) (REDMS No. 6790846)

PLN-55

See Page PLN-55 for full report

Designated Speakers: Wayne Craig and Edwin Lee

STAFF RECOMMENDATION

That Richmond Zoning Bylaw 8500, Amendment Bylaw 10357, for the rezoning of 8740, 8760, 8780 and 8800 Spires Road, and the surplus portion of the Spires Road road allowance from “Single Detached (RS1/E)” to “Parking Structure Townhomes (RTP4)”, be introduced and given first reading.



3. **APPLICATION BY PAKLAND PROPERTIES FOR REZONING AT 11720 WILLIAMS ROAD FROM THE “SINGLE DETACHED (RS1/E)” ZONE TO THE “COMPACT SINGLE DETACHED (RC2)” ZONE**

(File Ref. No. RZ 21-936290; 12-8060-20-010359) (REDMS No. 6839252)

PLN-121

See Page PLN-121 for full report

Designated Speakers: Wayne Craig and Cynthia Lussier

STAFF RECOMMENDATION

That Richmond Zoning Bylaw 8500, Amendment Bylaw 10359, for the rezoning of 11720 Williams Road from the “Single Detached (RS1/E)” zone to the “Compact Single Detached (RC2)” zone, be introduced and given first reading.



4. **APPLICATION BY THE CITY OF RICHMOND FOR A TEMPORARY COMMERCIAL USE PERMIT AT 8620 AND 8660 BECKWITH ROAD**

(File Ref. No. TU 22-005410) (REDMS No. 6837489)

PLN-140

See Page PLN-140 for full report

Designated Speakers: Wayne Craig and Jordan Rockerbie

STAFF RECOMMENDATION

- (1) *That the application by the City of Richmond for an extension to Temporary Commercial Use Permit TU 18-841880 for the properties at 8620 and 8660 Beckwith Road to permit a “Parking, non-accessory” use be considered for three years from the date of issuance; and*
- (2) *That this application be forwarded to the April 19, 2022 Public Hearing at 7:00 p.m. in the Council Chambers of Richmond City Hall.*



5. **RICHMOND HERITAGE COMMISSION 2021 ANNUAL REPORT & 2022 WORK PROGRAM AND BUDGET ALLOCATION**

(File Ref. No. 01-0100-30-HCOM1-01) (REDMS No. 6836286)

PLN-153

See Page PLN-153 for full report

Designated Speaker: John Hopkins

STAFF RECOMMENDATION

- (1) *That the Richmond Heritage Commission 2021 Annual Report, as presented in the staff report titled “Richmond Heritage Commission 2021 Annual Report & 2022 Work Program and Budget Allocation”, dated February 4, 2022, from the Director, Policy Planning, be received for information;*

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Pg. # ITEM

- (2) *That the Richmond Heritage Commission 2022 Work Program, as presented in the staff report titled “Richmond Heritage Commission 2021 Annual Report & 2022 Work Program and Budget Allocation”, dated February 4, 2022, from the Director, Policy Planning, be approved; and*
- (3) *That the Richmond Heritage Commission 2022 Budget Allocation, as presented in the staff report titled “Richmond Heritage Commission 2021 Annual Report & 2022 Work Program and Budget Allocation”, dated February 4, 2022, from the Director, Policy Planning, be approved.*

6. MANAGER’S REPORT

ADJOURNMENT



Planning Committee

Date: Wednesday, February 23, 2022

Place: Council Chambers
Richmond City Hall

Present: Councillor Bill McNulty, Chair
Councillor Alexa Loo (by teleconference)
Councillor Chak Au
Councillor Carol Day
Councillor Andy Hobbs (by teleconference)
Councillor Harold Steves (by teleconference)

Also Present: Councillor Michael Wolfe (by teleconference)

Call to Order: The Chair called the meeting to order at 4:31 p.m.

MINUTES

It was moved and seconded
*That the minutes of the meeting of the Planning Committee held on
February 8, 2022, be adopted as circulated.*

CARRIED

Planning Committee
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PLANNING AND DEVELOPMENT DIVISION

1. **APPLICATION BY 0853803 BC LTD. AND 1121648 BC LTD. FOR REZONING AT 6571 AND 6591 NO. 1 ROAD FROM THE “SINGLE DETACHED (RS1/F)” ZONE TO THE “LOW DENSITY TOWNHOUSES (RTL4)” ZONE**

(File Ref. No. RZ 16-731275 12-8060-20-010288) (REDMS No. 6675660)

It was moved and seconded

That Richmond Zoning Bylaw 8500, Amendment Bylaw 10288, for the rezoning of 6571 and 6591 No. 1 Road from the “Single Detached (RS1/F)” zone to the “Low Density Townhouses (RTL4)” zone, be introduced and given first reading.

CARRIED

2. **COMMUNITY POVERTY REDUCTION AND PREVENTION TABLE -DRAFT TERMS OF REFERENCE**

(File Ref. No. 07-3190-01/) (REDMS No. 6799850)

This item was removed and added to the Tuesday, February 22, 2022 General Purposes Committee Meeting Agenda.

3. **ESTABLISHMENT OF UNDERLYING ZONING FOR PROPERTIES DEVELOPED UNDER LAND USE CONTRACTS 004, 005, 024, 026, 028, 029, 045, 047, 056, 070, 075, 087, 092, 122, 126, AND 128 IN THE BLUNDELL, SEAFAIR, AND STEVESTON AREAS AND IN THE NORTH PORTION OF CITY CENTRE**

(File Ref. No. 08-4430-03-09) (REDMS No. 6781543)

Staff briefed Committee on the establishment of underlying zoning for Land Use Contracts, noting that the Local Government Act requires the city to adopt all underlying zoning bylaws by June 30, 2022 and that if Council adopts the 15 bylaws proposed in this report the process of establishing underlying zoning for land use contracts in the City will be complete.

It was moved and seconded

- (1) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10314, to establish underlying zoning for the property developed under Land Use Contract 004, be introduced and given first reading;*
- (2) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10315, to establish underlying zoning for the property developed under Land Use Contract 005, be introduced and given first reading;*

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- (3) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10316, to establish underlying zoning for the property developed under Land Use Contract 024, be introduced and given first reading;*
- (4) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10317, to establish underlying zoning for the property developed under Land Use Contract 026, be introduced and given first reading;*
- (5) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10318, to establish underlying zoning for the property developed under Land Use Contract 028, be introduced and given first reading;*
- (6) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10319, to establish underlying zoning for the property developed under Land Use Contract 029, be introduced and given first reading;*
- (7) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10320, to establish underlying zoning for the property developed under Land Use Contract 045, be introduced and given first reading;*
- (8) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10321, to establish underlying zoning for the property developed under Land Use Contracts 047 and 075, be introduced and given first reading;*
- (9) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10322, to establish underlying zoning for the property developed under Land Use Contract 056, be introduced and given first reading;*
- (10) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10323, to establish underlying zoning for the property developed under Land Use Contract 070, be introduced and given first reading;*
- (11) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10324, to establish underlying zoning for the property developed under Land Use Contract 087, be introduced and given first reading;*
- (12) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10325, to establish underlying zoning for the property developed under Land Use Contract 092, be introduced and given first reading;*
- (13) *That Richmond Zoning Bylaw 8500, Amendment Bylaw 10326, to establish underlying zoning for the property developed under Land Use Contract 122, be introduced and given first reading;*
- (14) *That,*
 - (a) *Richmond Zoning Bylaw 8500, Amendment Bylaw 10351, to establish underlying zoning for properties developed under Land Use Contract 126 and to create the “Commercial (ZC51) – Bridgeport Road and Sea Island Way (City Centre)” zone, be introduced and given first reading; and*

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- (b) *Upon adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 10351, the Richmond Zoning Bylaw 8500, Amendment Bylaw 9629, which is at third reading, be understood to:*
 - (i) *rezone the subject properties (8320, 8340, 8360, 8440 Bridgeport Road and 8311, 8351 Sea Island Way) from “Auto-Oriented Commercial (CA)”, “Land Use Contract 126” and “Commercial (ZC51) – Bridgeport Road and Sea Island Way (City Centre)”, to the new “High Rise Commercial (ZC29) – Bridgeport Gateway” zone; and*
 - (ii) *discharge “Land Use Contract 126”;*
- (15) *That,*
 - (a) *Richmond Zoning Bylaw 8500, Amendment Bylaw 10352, to establish underlying zoning for the property developed under Land Use Contract 128 and to create the “Neighbourhood Pub (ZC52) – Blundell Road (Blundell)” zone”, be introduced and given first reading; and*
 - (b) *Upon adoption of Amendment Bylaw 10352, the Richmond Zoning Bylaw 8500, Amendment Bylaw 9891, which is at third reading, be understood to:*
 - (i) *rezone the subject property (6031 Blundell Road) from “Land Use Contract 128” and “Neighbourhood Pub (ZC52) – Blundell Road (Blundell)”, to the new “Community Commercial (CC)” zone; and*
 - (ii) *discharge “Land Use Contract 128”.*

CARRIED

4. OFFICIAL COMMUNITY PLAN TARGETED UPDATE – PROPOSED SCOPE AND PROCESS

(File Ref. No. 08-4045-01) (REDMS No. 6757895)

Staff provided a brief summary highlighting that over the last 5 years the Official Community Plan (OCP) has had several amendments. Staff are proposing a review and update to the OCP targeting six specific areas. There will be a major focus on housing affordability that will include an examination of Regional and National housing affordability models and their applicability to Richmond. The outcome of the OCP review will be a series of policies and program options for Council’s consideration. The OCP update will include extensive online public consultation through Let’s Talk Richmond, engagement with Advisory Committees, Community Groups and the Development industry. The update is proposed to be complete by Mid 2024.

4.

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Discussion ensued regarding (i) the research and review of the six specific areas will be dealt with simultaneously; (ii) public consultation will be broken into focused targeted areas, (iii) examples of different types of developments will be provided to residents so that they can respond to options that are presented to them, (iv) much of the review will be research based focusing on affordability of housing, (v) housing affordability will be the top priority and the largest portion of the OCP review. (vi) new development potential will be targeted at being affordable and tied to median income for ownership and rentals.

Further discussion ensued regarding (i) the time line of the OCP review, (ii) continuation of updates to the OCP generated from referrals during the review process, (iii) repurposing of already built housing into multiple units, and (iv) managing new development potential in a different way so that it doesn't become just as unaffordable as the existing development capacity in the plan.

In response to queries from Committee staff advised that this report gives an idea of the scope of the review which can change in consultation with Council and the community and that this OCP will be a made in Richmond OCP. Staff are not looking at creating a generic policy but looking for a real policy that produces results.

It was moved and seconded

That Council endorse the proposed scope and process to update the Official Community Plan as outlined in the staff report dated February 7, 2022 from the Director, Policy Planning titled, "Official Community Plan Targeted Update - Proposed Scope and Process".

CARRIED

5. MANAGER'S REPORT

(i) Community Dialogue Project

Staff advised Committee that the Community Dialogue Project sessions will begin in March with individuals with lived experiences of homelessness and service providers. Dialogue sessions for the general public will begin in May and June. Communication regarding the sessions will go out in April through social media, the City's website and key stakeholders.

(ii) New Policy Planning Staff

Babak Behnia was introduced as the new Planner 2. He will be involved in managing the Environmental portfolio in Policy Planning and assisting with other land use policy matters.

5.

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(iii) Update on Agritech

Staff are reviewing the new Agricultural Land Reserve regulations and will provide a memo to Council regarding the new regulations.

ADJOURNMENT

It was moved and seconded
That the meeting adjourn (5:23 p.m.).

CARRIED

Certified a true and correct copy of the Minutes of the meeting of the Planning Committee of the Council of the City of Richmond held on Wednesday, February 23, 2022.

Councillor Bill McNulty
Chair

Raman Grewal
Legislative Services Associate



To: Planning Committee
From: Wayne Craig
Director, Development

Date: February 23, 2022
File: RZ 18-802860

**Re: Application by Art Homes Ltd. for Rezoning at 4400, 4420, and
4440 Smith Crescent from the "Single Detached (RS1/F)" Zone to the "High
Density Townhouses (RTH1)" Zone**

Staff Recommendation

That Richmond Zoning Bylaw 8500, Amendment Bylaw 9877, for the rezoning of 4400, 4420, and 4440 Smith Crescent from the "Single Detached (RS1/F)" zone to the "High Density Townhouses (RTH1)" zone, be introduced and given First Reading.

Wayne Craig
Director, Development
(604-247-4625)

WC:jr
Att. 6

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Affordable Housing	<input checked="" type="checkbox"/>	

Staff Report

Origin

Art Homes Ltd. (Director: Buta S. Dosamjh), on behalf of South Rich Homes JV Ltd. (Director: Buta S. Dosamjh), has applied to the City of Richmond to rezone 4400, 4420, and 4440 Smith Crescent from the “Single Detached (RS1/F)” zone to the “High Density Townhouses (RTH1)” zone, to permit the development of 34 townhouse units. A location map and aerial photo are provided in Attachment 1.

Findings of Fact

A Development Application Data Sheet providing details about the development proposal is provided in Attachment 2.

Subject Site Existing Housing Profile

There are three single detached dwellings on the subject site, which would be demolished. None of the dwellings contain a secondary suite. The dwellings were previously owner-occupied and are currently vacant.

Surrounding Development

Development immediately surrounding the subject site is as follows:

- To the North: A single detached dwelling on a property zoned “Single Detached (RS1/F)”.
- To the South: A single detached dwelling on a property zoned “Single Detached (RS1/F)”.
- To the East: The Highway 91A road allowance, which includes a pedestrian overpass connecting the pathway behind the subject site to Hamilton Highway Park and Thompson Road.
- To the West, across Smith Crescent: Hamilton VLA Park, which includes a playground, sport courts, and open field.

Related Policies & Studies

Official Community Plan/Hamilton Area Plan

The subject site is located in the Hamilton planning area, and is designated “Neighbourhood Residential” on the Official Community Plan (OCP) land use map and “Neighbourhood Residential (Townhouse 0.75 FAR)” on the Hamilton Area Plan land use map (Attachment 3). The FAR referenced in the Hamilton Area Plan land use designation is the intended maximum density, inclusive of bonus density associated with the Affordable Housing Strategy and Hamilton Area Plan amenity contributions. The proposed rezoning is consistent with these designations.

The Hamilton Area Plan requires a cash contribution for neighbourhood amenity space at a rate of \$6.55 per buildable square foot for townhouse development. Prior to final adoption of the rezoning bylaw, the applicant made a \$323,714.10 contribution to the Hamilton Amenity Fund.

The Hamilton Area Plan includes objectives to improve non-motorized transportation options in the neighbourhood to better connect residents with parks and amenities. The proposed development would provide a new public pathway linking Smith Crescent to the pathway in the highway right-of-way (ROW) behind the subject site. This would allow more direct access to and from the pedestrian overpass crossing Highway 91A.

Affordable Housing Strategy

Residential rezoning applications involving townhouse developments are required to provide a cash-in-lieu contribution towards the City's Affordable Housing Reserve Fund. As per the City's Affordable Housing Strategy, townhouse rezoning applications received prior to November 15, 2021 are required to provide a cash-in-lieu contribution of \$8.50 per buildable square foot. Consistent with the strategy, a \$420,087.00 contribution is required prior to final adoption of the rezoning bylaw.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on title is required prior to final adoption of the rezoning bylaw.

The Flood Construction Level in Hamilton is 3.5 m GSC. Structural elevation of the floor systems containing habitable space is proposed to meet the Flood Construction Level. No indoor habitable space is proposed at grade. The storage areas in the garages cannot be used as habitable space. Prior to final adoption of the rezoning bylaw, the applicant must register a legal agreement on title restricting conversion of any garage or storage area to habitable space.

This proposal is consistent with Richmond Flood Plain Designation and Protection Bylaw 8204 and the form of development is typical in the Hamilton area.

Ministry of Transportation and Infrastructure Approval

As the subject site abuts a Provincially-controlled highway, this redevelopment proposal has been referred to the Ministry of Transportation and Infrastructure (MOTI). Preliminary approval has been received from MOTI, and formal approval is required prior to final adoption of the rezoning bylaw.

Public Consultation

A rezoning sign has been installed on the subject property. Staff have not received any comments from the public about the rezoning application in response to the placement of the rezoning sign on the property.

Should the Planning Committee endorse this application and Council grant First Reading to the rezoning bylaw, the bylaw will be forwarded to a Public Hearing, where any area resident or interested party will have an opportunity to comment. Public notification for the Public Hearing will be provided as per the *Local Government Act*.

Analysis

Urban Design and Site Planning

The proposed development consists of six buildings containing a total of 34 townhouse units. The site plan and massing are generally consistent with the Development Permit Guidelines contained in the OCP and Hamilton Area Plan. Conceptual development plans are provided in Attachment 4.

All of the units would be three storeys, with living space located on the second and third storey. Living space is not permitted on the ground floor due to the flood construction level in the Hamilton neighbourhood.

The site plan is structured around a pedestrian circulation network. Building 1 fronts Smith Crescent, Buildings 2 and 3 front an interior walkway, and Buildings 5 and 6 front the public walkway. Building 4 fronts the drive aisle, due to the challenge of adding accessible footpaths through the cluster of mature trees between this building and the off-site pathway behind the site.

All of the units have private outdoor space at grade and at least one balcony. Units in Buildings 1, 2, 3, 5, and 6 feature two projecting balconies, which are stacked to provide weather protection to the balcony or porch below. Units in Building 4 have a single recessed balcony, but have a larger outdoor space at grade than units in other buildings.

Building 6 is set back 2.91 m from the rear property line, which abuts the Highway 91A road allowance. The applicant has provided an acoustical report, which includes recommendations for building upgrades to achieve CHMH indoor noise standards in Buildings 4 and 6. These recommendations will be further reviewed and secured through the Development Permit process.

The proposed rear yard setback is consistent with Richmond Zoning Bylaw 8500, but encroaches into the 4.5 m setback to highways required by the Ministry of Transportation and Infrastructure (MOTI). Prior to advancing the Development Permit application to the Development Permit Panel the applicant is required to receive MOTI approval for a setback permit exception.

The shared outdoor amenity area is proposed in the southwest corner of the site, and is specifically located to facilitate retention of two large trees. The current concept includes a large play structure, bench seating, picnic table, and landscaping around the two retained trees. Detailed design and programming of the private and shared outdoor amenity spaces will be reviewed through the Development Permit process.

A garbage and recycling room is proposed near the middle of the site flanking the central drive aisle. Vehicles servicing the site would be able to complete a three-point turn in the drive aisle to enter and exit the site in a forward motion.

Housing Type and Tenure

The proposed development consists of ground-oriented townhouses that would be stratified and sold upon completion. Consistent with OCP policy respecting townhouse and multiple family housing development projects, and in order to maximize potential rental and housing opportunities throughout the City, the applicant has agreed to register a restrictive covenant on title prior to rezoning bylaw adoption, prohibiting: (a) the imposition of any strata bylaw that would prohibit any residential dwelling unit from being rented; and (b) the imposition of any strata bylaw that would place age-based restrictions on occupants of any residential dwelling unit.

Transportation and Site Access

Vehicle access is proposed from a single driveway to Smith Crescent located in the approximate centre of the frontage. Vehicle and bicycle parking for residents and visitors are provided consistent with Richmond Zoning Bylaw 8500, except for a variance to the maximum percentage of tandem parking spaces for residents. The proposed development would require 68 parking spaces for residents, of which a maximum of 50% can be provided in a tandem arrangement. The proposed development has 59% of the parking spaces (i.e., 40 spaces) in a tandem arrangement.

Staff support the requested variance as it is a direct response to the Flood Construction Level and has been accepted in similar developments in the Hamilton neighbourhood. In addition, the applicant is providing voluntary Transportation Demand Management (TDM) measures to reduce future residents' vehicle dependence. The proposed TDM measures include:

- 60% increase over the minimum Class 1 bicycle parking requirements for residents.
- 75% increase over the minimum Class 2 bicycle parking requirements for visitors.
- Significant cycling and pedestrian upgrades as required by the Hamilton Area Plan, including off-street pathways and road upgrades. Additional details are provided below.

The proposed pedestrian pathway system would connect Smith Crescent with the existing pedestrian path adjacent to Highway 91A, which is consistent with the Hamilton Area Plan. This public pathway will be secured through a 6.0 m wide Statutory Right-of-Way (SRW) along the south property line and will include a 3.0 m wide paved surface, lighting, and landscaping. Future development to the south would be required to provide an additional 6.0 m wide Statutory Right-of-Way (SRW) and increase the width of the pathway. Improvements to the pathway in the highway right-of-way (ROW) behind the subject site will also be required, consisting of pavement widening to 3.0 m, lighting, and landscaping. Detailed design of the new and upgraded pathways will be through the Servicing Agreement process.

Tree Retention and Replacement

The applicant has submitted a Certified Arborist's Report; which identifies on-site and off-site tree species, assesses tree structure and condition, and provides recommendations on tree retention and removal relative to the proposed development. The Report assesses 57 bylaw-sized trees on the subject property, three trees on neighbouring properties, and four street trees on City property.

The City's Tree Preservation Coordinator has reviewed the Arborist's Report and supports the Arborist's findings, with the following comments:

- Two Maple trees (Tag # 24 and 25) located in the front yard of 4420 Smith Crescent are in excellent condition and should be retained and protected. Provide a 4.5 m tree protection zone from the base of each tree. A \$10,000.00 Tree Survival Security is required for each tree.
- One Cherry and one Maple tree (Tag # 33 and 34) located in the rear yard of 4420 Smith Crescent site are proposed to be retained. Provide a 4.5 m tree protection zone from the base of each tree. A \$5,000.00 Tree Survival Security is required for each tree.
- Seven Western Red Cedar trees (Cluster noted as Tag # 55) located in the rear yard of 4400 Smith Crescent are proposed to be retained. Provide a 4.5 m tree protection zone from the base of each tree. A \$5,000.00 Tree Survival Security is required for each tree.
- One Ash tree (Tag # 54) located on the neighbouring property to the north and two trees (Cluster noted as Tag # 55) located on the neighbouring properties to the north and east to be protected as per City of Richmond Tree Protection Information Bulletin Tree-03.
- One tree (Cluster noted as Tag # 55) located in the rear yard of 4400 Smith Crescent is in conflict with Building 4. Remove and replace.
- Three Western Red Cedar trees (Tag # 21, 22, and 23) located in the side yard of 4420 Smith Crescent ranging in size from 30 cm to 42 cm caliper are in good condition but are in conflict with the proposed driveway and Building 5. Several revised site plans were considered in an effort to retain these trees, but resulted in impacts to urban design objectives or the retention of other trees on the development site. These trees are growing as a hedge and are not suitable for relocation. Remove and replace.
- 42 trees (Tag # 1-20, 26-32, 35-38, 41-50, and 52) located on the development site are either dead, dying (sparse canopy foliage), are infected with Fungal Blight or exhibit structural defects such as cavities at the main branch union and co-dominant stems with inclusions. As a result, these trees are not good candidates for retention. Remove and replace.
- One hedgerow (Tag # 40) located in the front yard between 4400 and 4420 Smith Crescent is in poor condition and should be removed.
- Replacement trees should be specified at 2:1 ratio as per the OCP.

The four trees on City property have been reviewed by Parks Department staff, with the following comments:

- Tree # 39 – Tree appears to be in fair health good condition. It is showing signs of drought stress with the leaves browning on the edges but otherwise looks to be in fair health. Size of the tree makes it a good candidate to relocate. This tree should be reviewed as part of the Servicing Agreement for possible relocation. Final determination should be made prior to the Development Permit application proceeding to Development Permit Panel. A \$5,000.00 survival security is required prior to final adoption of the rezoning bylaw if it is retained.

- Tree # 53 – Tree is in good health fair condition. It has been topped historically but appears to be pruned regularly so there are no major defects. Tree will be close to the frontage improvements but all efforts to design around this tree must be made moving forward. A \$5,000.00 survival security is required prior to final adoption of the rezoning bylaw.
- Tree # 51 – Tree is in good health but poor condition. It has approximately 40 stems which would make it hard to work around for the frontage improvements. It will also be an issue for sight lines moving forward due to the form of the tree. Recommend removal. \$1,500.00 is required in compensation prior to final adoption of the rezoning bylaw.
- Tree # 56 – Tree is in poor health and condition. Main limbs have died back with decay in the main stem. All the growth appears to be sucker growth. This tree is not a good candidate to retain or relocate. Recommend removal. \$1,500.00 required in compensation prior to final adoption of the rezoning bylaw.

Tree Protection

Eleven trees (Tag # 24, 25, 34, 35, and 55) on the subject site, three trees (Tag # 54 and 55) on neighbouring properties, and two trees (Tag # 39 and 53) on City property are to be retained and protected. The applicant has submitted a tree protection plan showing the trees to be retained and the measures taken to protect them during development stage (Attachment 5). To ensure that the trees identified for retention are protected at development stage, the applicant is required to complete the following items:

- Prior to final adoption of the rezoning bylaw, submission to the City of a contract with a Certified Arborist for the supervision of all works conducted within or in close proximity to tree protection zones. The contract must include the scope of work required, the number of proposed monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the arborist to submit a post-construction impact assessment to the City for review.
- Prior to final adoption of the rezoning bylaw, submission to the City of a \$75,000.00 Tree Survival Security for the 11 on-site trees and two City-trees to be retained and/or relocated.
- Prior to demolition of the existing dwelling on the subject site, installation of tree protection fencing around all trees to be retained. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and remain in place until construction and landscaping on-site is completed.

Tree Replacement

The applicant wishes to remove 46 on-site trees. The 2:1 replacement ratio would require a total of 92 replacement trees. The applicant has agreed to plant 41 trees in the development. The required replacement trees are to be a minimum of 8 cm caliper deciduous or 4 m tall coniferous species, as per Tree Protection Bylaw No. 8057.

To satisfy the 2:1 replacement ratio established in the OCP, the applicant will contribute \$38,250.00 to the City's Tree Compensation Fund in lieu of the remaining 51 trees that cannot be accommodated on the subject property after redevelopment.

Variance Requested

The proposed development is generally consistent with the “High Density Townhouses (RTH1)” zone, except for the following requested variances (staff comments in ***bold italics***).

1. Increase permitted tandem parking from 50% to 59% of spaces (i.e., 40 of 68 spaces).

Richmond Zoning Bylaw 8500 permits up to 50% of the required residential parking spaces to be provided in a tandem arrangement. In the case of this development, up to 34 spaces could be provided in a tandem arrangement.

Staff support the requested variance as it is a direct response to the high flood construction level in Hamilton, is consistent with other approved developments in the Hamilton neighbourhood, results in a marginal increase to the permitted tandem parking, and is off set by voluntary TDM measures. Prior to final adoption of the rezoning bylaw the applicant is required to register a legal agreement on title restricting conversion of any garage or storage area into habitable space.

Townhouse Energy Efficiency and Renewable Energy

The proposed development consists of townhouses that staff anticipate would be designed and built in accordance with Part 9 of the BC Building Code. As such, this development would be required to achieve Step 3 of the BC Energy Step Code for Part 9 construction (Climate Zone 4) or Step 2 with a low carbon energy system. As part of the Development Permit application, the applicant will be required to identify the Step Code target and provide a report prepared by a Certified Energy Advisor which demonstrates that the proposed design and construction will meet or exceed the applicable standard.

Amenity Space

The applicant proposes a cash contribution in-lieu of providing indoor amenity space on-site. Prior to final adoption of the rezoning bylaw, the applicant must provide an \$88,460.00 contribution to the City-wide amenity fund.

Outdoor amenity space is provided on site. Based on the preliminary design, the size of the proposed outdoor amenity space is consistent with the OCP minimum requirement of 6 m² per unit. Staff will work with the applicant at the Development Permit stage to ensure the design of the outdoor amenity space meets the Development Permit guidelines contained in the OCP.

Public Art

Based on the maximum buildable floor area of approximately 49,422 ft² residential floor area, the recommended Public Art contribution based on administrative guidelines of \$0.93 (2022 rate) is approximately \$45,962.46.

As the project will generate a recommended Public Art contribution close to \$40,000.00 and there are limited opportunities to locate Public Art on-site, as per Policy it is recommended that the Public Art contribution be directed to the Public Art Reserve for city-wide projects on City lands. The contribution is required prior to final adoption of the rezoning bylaw.

Site Servicing and Frontage Improvements

Prior to final adoption of the rezoning bylaw, the applicant is required to enter in to a Servicing Agreement for the design and construction of the required site servicing and frontage works described in Attachment 6, including, but not limited to:

- Filling of the Smith Crescent drainage ditch and replacement with a piped drainage system.
- A new pedestrian pathway along the south property line, consisting of a 3.0 m wide pathway, lighting, and landscaping.
- Improvements to the pathway behind the subject site, consisting of a 3.0 m wide pathway, lighting, and landscaping.
- Frontage improvements on both sides of Smith Crescent, generally consisting of new sidewalks, landscaped boulevards, street parking, and a two-way bike path.

Development Permit Application

Prior to final adoption of the rezoning bylaw, a Development Permit application is required to further examine the following issues:

- Compliance with the Development Permit guidelines for the form and character of multi-family projects contained in the OCP and Hamilton Area Plan.
- Review of the size and species of proposed new trees to ensure bylaw compliance and achieve an acceptable mix of deciduous and coniferous species.
- Refinement of the shared outdoor amenity design and programming, including the choice of play equipment, to create a safe and inviting environment for children's play and social interaction.
- Review of the relevant accessibility features, including the provision of three convertible units and aging-in-place features for all units.
- Review of a sustainability strategy for the development.
- Review of any necessary noise mitigation strategies related to potential traffic noise from Highway 91A. The applicant has provided an acoustical report, which includes recommendations for upgrades to Buildings 4 and 6.
- Review of the proposed setback to Highway 91A, which requires approval from the Ministry of Transportation and Infrastructure.

Financial Impact or Economic Impact

This rezoning application results in an Operational Budget Impact (OBI) of \$5,000.00 for off-site City infrastructure (such as roadworks, waterworks, storm sewers, sanitary sewers, street lights, street trees and traffic signals).

Conclusion

The purpose of this application is to rezone 4400, 4420, and 4440 Smith Crescent from the “Single Detached (RS1/F)” zone to the “High Density Townhouses (RTH1)” zone, to permit the development of 34 townhouse units.

The proposed rezoning is generally consistent with the plans and policies affecting the subject site.

The list of rezoning considerations is provided in Attachment 6, which has been agreed to by the applicant (signed concurrence on file).

It is recommended that Richmond Zoning Bylaw 8500, Amendment Bylaw 9877 be introduced and given First Reading.



Jordan Rockerbie
Planner 1
(604-276-4092)

JR:js

Attachments:

- Attachment 1: Location Map and Aerial Photo
- Attachment 2: Development Application Data Sheet
- Attachment 3: Hamilton Area Plan Land Use Map
- Attachment 4: Conceptual Development Plans
- Attachment 5: Tree Retention Plan
- Attachment 6: Rezoning Considerations



City of
Richmond



**SUBJECT
PROPERTIES**

4440
4420



RZ 18-802860

Original Date: 02/14/18

Revision Date: 01/11/22

Note: Dimensions are in METRES



RZ 18-802860

Attachment 2

Address: 4400, 4420, and 4440 Smith Crescent

Applicant: Art Homes Ltd.

Planning Area(s): Hamilton

	Existing	Proposed
Owner:	South Rich Homes JV Ltd. (Director: Buta S. Dosamjh)	To be determined
Site Size (m²):	6,122 m ²	No change
Land Uses:	Single detached	Townhouses
OCP Designation:	Neighbourhood Residential	No change
Area Plan Designation:	Neighbourhood Residential (Townhouse 0.75 FAR)	No change
Zoning:	Single Detached (RS1/F)	High Density Townhouses (RTH1)
Number of Units:	3 single detached dwellings	34 townhouse dwellings

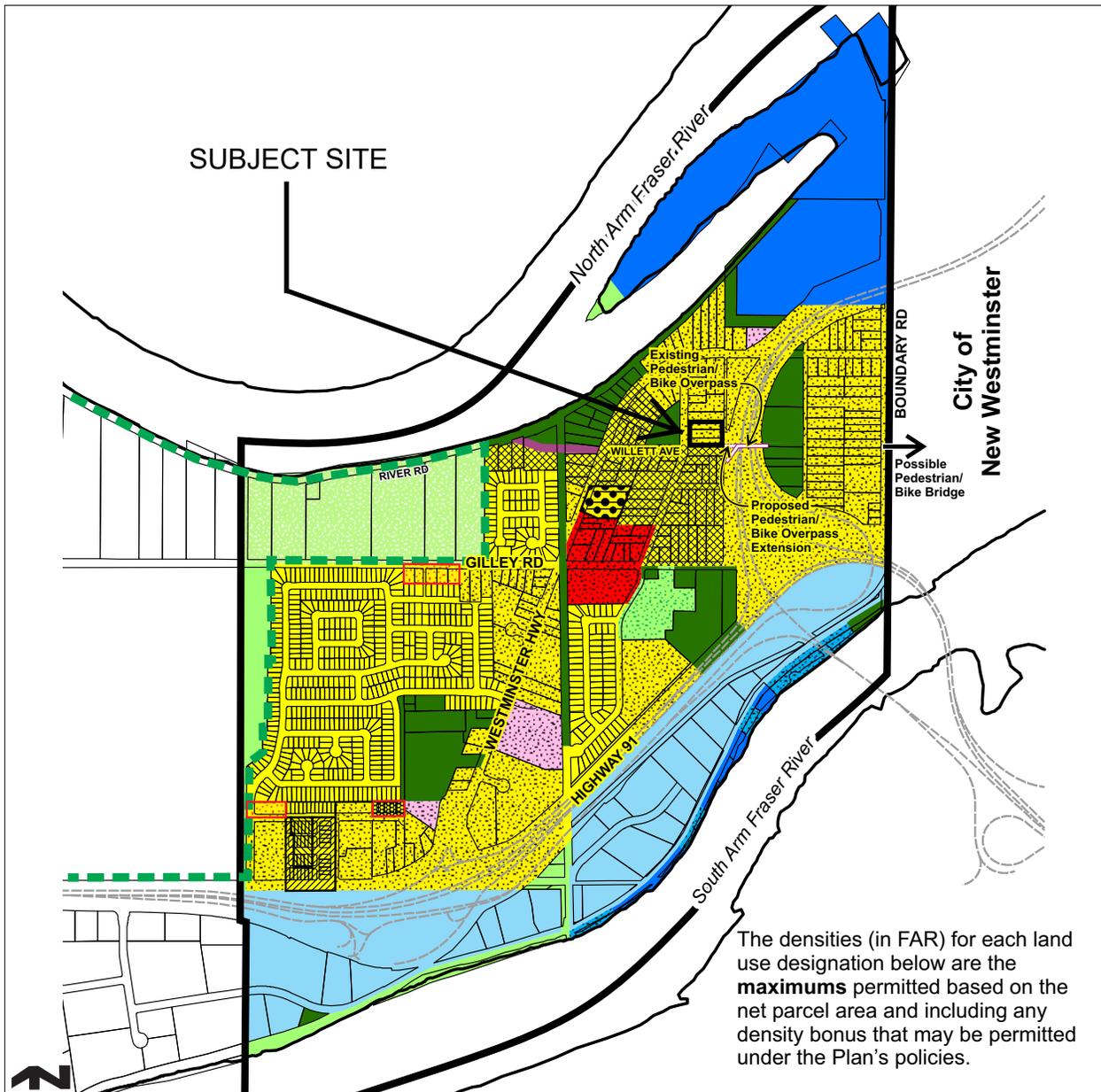
On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.75 FAR	0.71 FAR	None permitted
Buildable Floor Area (m ²):*	Max. 4,591.5 m ² (49,422 ft ²)	4,346.6 m ² (47,091 ft ²)	None permitted
Lot Coverage (% of lot area):	Building: Max. 45% Non-porous Surfaces: Max. 70% Live Landscaping: Min. 20%	Building: Max. 45% Non-porous Surfaces: Max. 70% Live Landscaping: Min. 20%	None
Lot Size:	1,800 m ²	6,122 m ²	None
Lot Dimensions (m):	Width: 40.0 m Depth: 30.0 m	Width: 67.06 m Depth: 91.44 m	None
Setbacks (m):	Front: Min. 4.5 m Rear: Min. 2.0 m North Side: Min. 2.0 m South Side: Min. 2.0 m	Front: 4.81 m Rear: Min. 2.91 m North Side: 3.0 m South Side: 10.6 m	None
Height (m):	Max. 12.0 m	11.0 m	None
Off-street Parking Spaces – Resident (R) / Visitor (V):	2 (R) and 0.2 (V) per unit	2 (R) and 0.2 (V) per unit	None
Off-street Parking Spaces – Total:	68 (R) and 7 (V)	68 (R) and 7 (V)	None
Off-street Parking Spaces – Accessible:	2% of visitor spaces (i.e. 1 space)	1 space	None

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Tandem Parking Spaces:	Permitted – Maximum of 50% of required spaces	59% (i.e. 40 spaces)	Vary by 6 spaces
Bicycle Parking Spaces – Class 1:	1.25 spaces per unit (i.e. 43 spaces)	68 spaces	None
Bicycle Parking Spaces – Class 2:	0.2 spaces per unit (i.e. 7 spaces)	12 spaces	None
Amenity Space – Indoor:	70 m ² or cash-in-lieu	Cash-in-lieu	None
Amenity Space – Outdoor:	204 m ² (6 m ² per unit)	215 m ²	None

Other: Tree replacement compensation required for loss of significant trees.

* Preliminary estimate; not inclusive of garage; exact building size to be determined through zoning bylaw compliance review at Building Permit stage.

Land Use Map Bylaw 9260
2017/06/12



The densities (in FAR) for each land use designation below are the **maximums** permitted based on the net parcel area and including any density bonus that may be permitted under the Plan's policies.

Area Plan Boundary	Neighbourhood Residential (Single Family or Duplex 0.75 FAR)
ALR Boundary	Neighbourhood Residential (Townhouse 0.55 FAR)
Agriculture	Neighbourhood Residential (Townhouse 0.75 FAR)
Community Institutional	Neighbourhood Residential (Stacked Townhouse 1.00 FAR)
Conservation Area	Neighbourhood Village Centre (Residential 4 Storey 1.50 FAR)
Industrial	Neighbourhood Village Centre (Retail and Office with Residential above 4 Storey 1.50 FAR)
Marine Residential/Industrial	Park and Major Trail/Greenway Corridors
Mixed Employment	Proposed Streets
Neighbourhood Residential (Single Family 0.55 FAR)	School
Neighbourhood Residential (Single Family 0.60 FAR)	
Neighbourhood Residential (Single Family with Coach Houses 0.60 FAR)	

PROJECT DATA

PROJECT WORK:
CONSTRUCT NEW THREE STOREY TOWNHOUSE RESIDENTIAL BUILDING.

CIVIC ADDRESS
4400 4420 4440 SMITH CRESCENT, RICHMOND

LOT AREA
65896 SQFT

PROPOSED ZONING
RTH1

SETBACK
SOUTH 6M + 2M
NORTH 6 M + 2M
EAST & WEST 4.5M

STOREYS
3 STOREYS

FLOOR AREA RATIO
MAXIMUM ALLOWED:
PROPOSED:
0.75 (49,422 SF)
0.71 (47,691 SF)

UNITS FLOOR AREAS
UNIT A1 1488 SQFT (FSR)
UNIT A2 1626 SQFT (FSR)
UNIT B 1246 SQFT (FSR)
UNIT B2 653 SQFT (NON-FSR)

NUMBER OF UNIT A1 11
NUMBER OF UNIT A2 3
NUMBER OF UNIT B 15
NUMBER OF UNIT B2 9

TOTAL FLOOR AREA: 67146 SQFT
TOTAL FSR FLOOR AREA: 47091 SQFT

FSR EXCLUSIONS
GARAGE STORAGE & ENTRY LOBBY 21287 SQFT
GARAGE ROOM 210 SQFT

SITE COVERAGE
FIRST FLOOR AREA 22658 SQFT
SITE COVERAGE 34.4%

UNIT NUMBER
PROPOSED:
34 UNITS WITH TANDEM GARAGE)
48% OF PROPOSED PARKING ARE TANDEM PARKING
4% RESIDENTIAL PARKING SPACES HAVE LEVEL 2 EV CHARGING

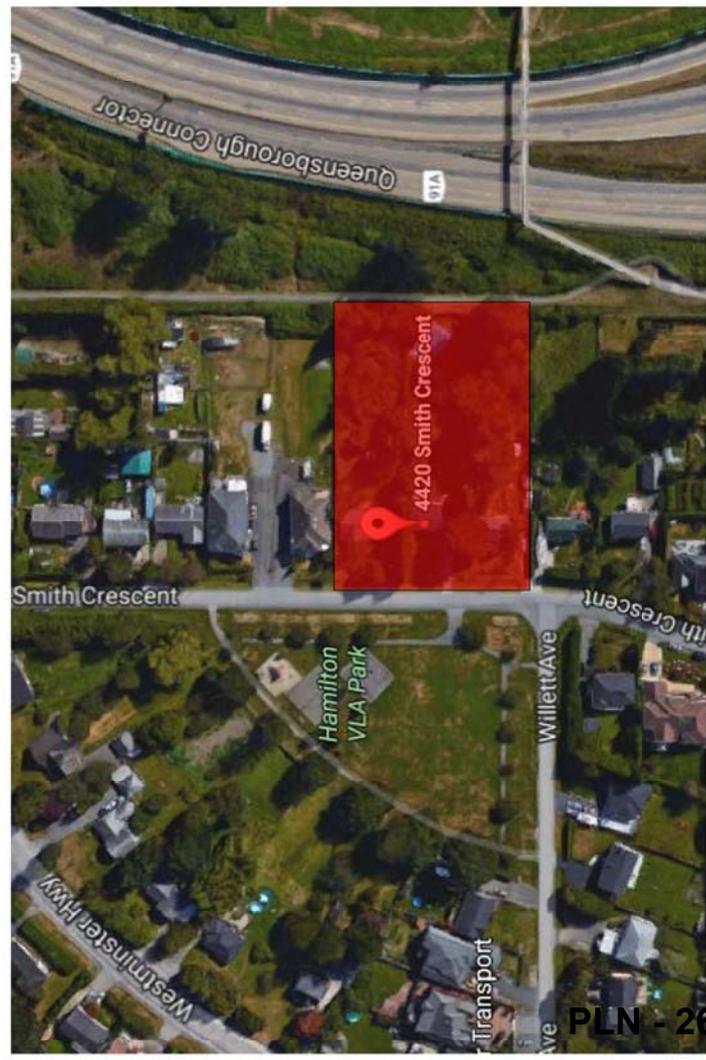
COMMON OUTDOOR AMENITY SPACE
PROPOSED 204 SM (6M / UNIT)
REQUIRED 215 SM

OFF STREET VISITOR PARKING FOR ALL 34 UNITS
REQUIRED 7 (0.2 PER UNIT)
PROPOSED 7 REGULAR

BIKE STORAGE
CLASS 1 34
CLASS 2 12

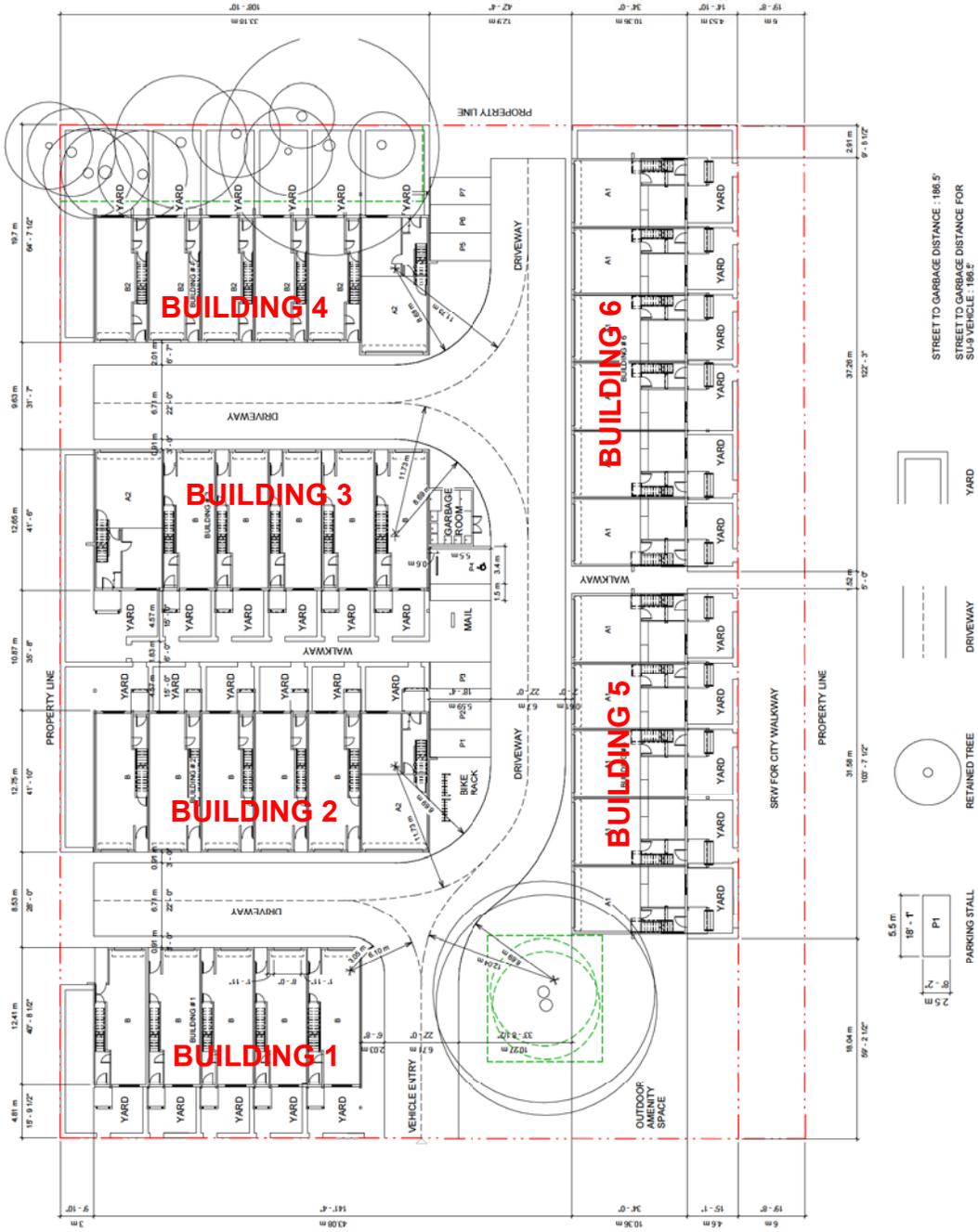
SCHEDULE DRAWINGS LIST

- A100 PROJECT DATA
- A101 FIRE ACCESS PLAN
- A200 SITE PLAN - 1ST, 2ND & 3RD FLOOR
- A202 PLANS & ROOF
- A203 BUILDING 2 - 1ST, 2ND & 3RD FLOOR PLANS & ROOF
- A204 BUILDING 3 - 1ST, 2ND & 3RD FLOOR PLANS & ROOF
- A205 BUILDING 4 - 1ST, 2ND & 3RD FLOOR PLANS & ROOF
- A206 BUILDING 5 - 1ST, 2ND & 3RD FLOOR PLANS & ROOF
- A207 BUILDING 6 - 1ST, 2ND & 3RD FLOOR PLANS & ROOF
- A208 BUILDING 7 - 1ST, 2ND & 3RD FLOOR PLANS & ROOF
- A211 UNIT A1 FLOOR PLANS
- A212 UNIT A2 FLOOR PLANS
- A213 UNIT B2 FLOOR PLANS
- A214 BUILDING #1 ELEVATIONS
- A301 BUILDING #2 ELEVATIONS
- A302 BUILDING #3 ELEVATIONS
- A303 BUILDING #4 ELEVATIONS
- A304 BUILDING #5 ELEVATIONS
- A305 BUILDING #6 ELEVATIONS
- A401 SITE SECTIONS
- A501 3D NORTH EAST
- A502 3D NORTH WEST
- A503 3D INTERFACE FRONTBACK
- A504 3D INTERFACE SIDES
- A100 CONTEXT

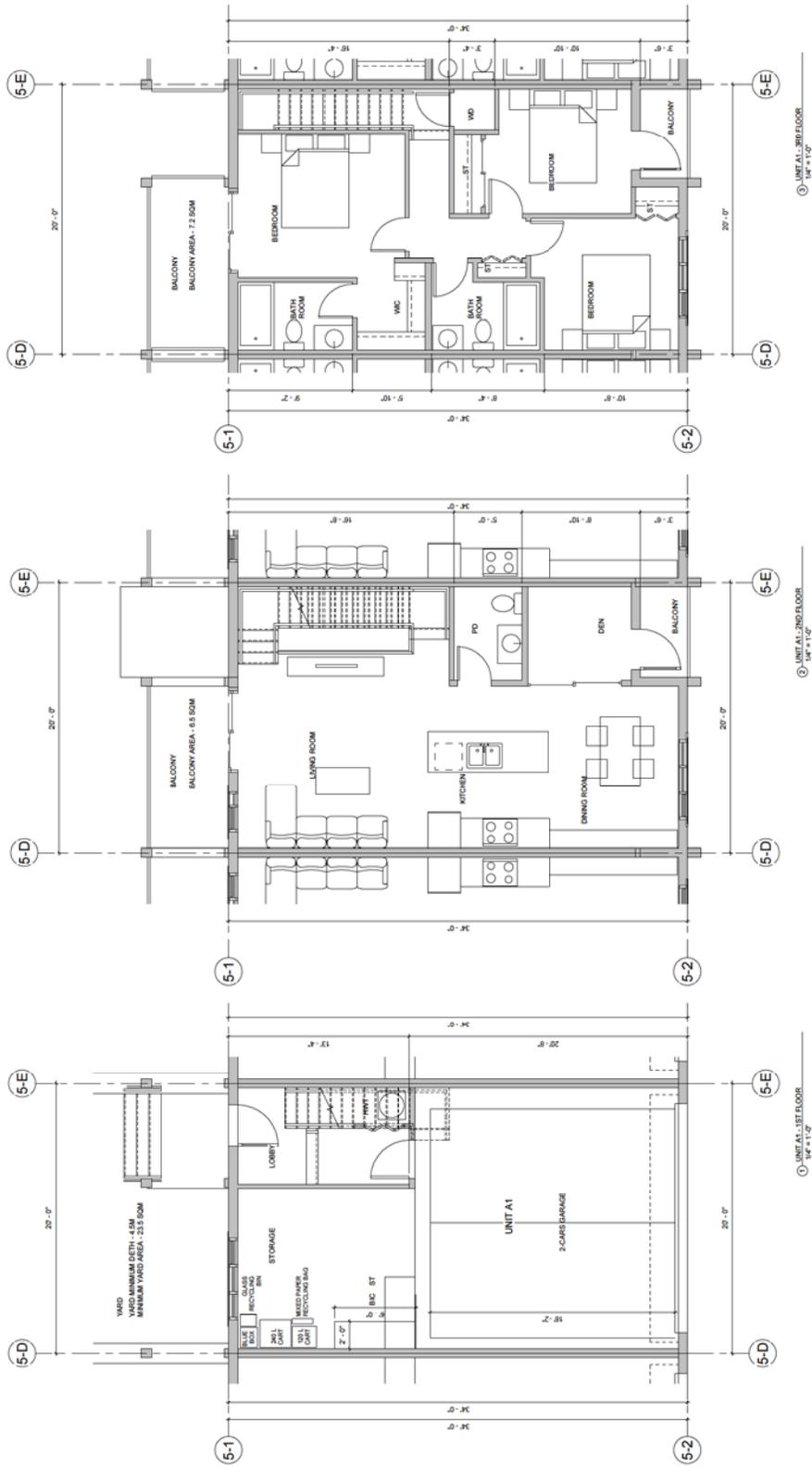


PLN - 26





SMITH CRESCENT
REFER TO CIVIL DRAWINGS



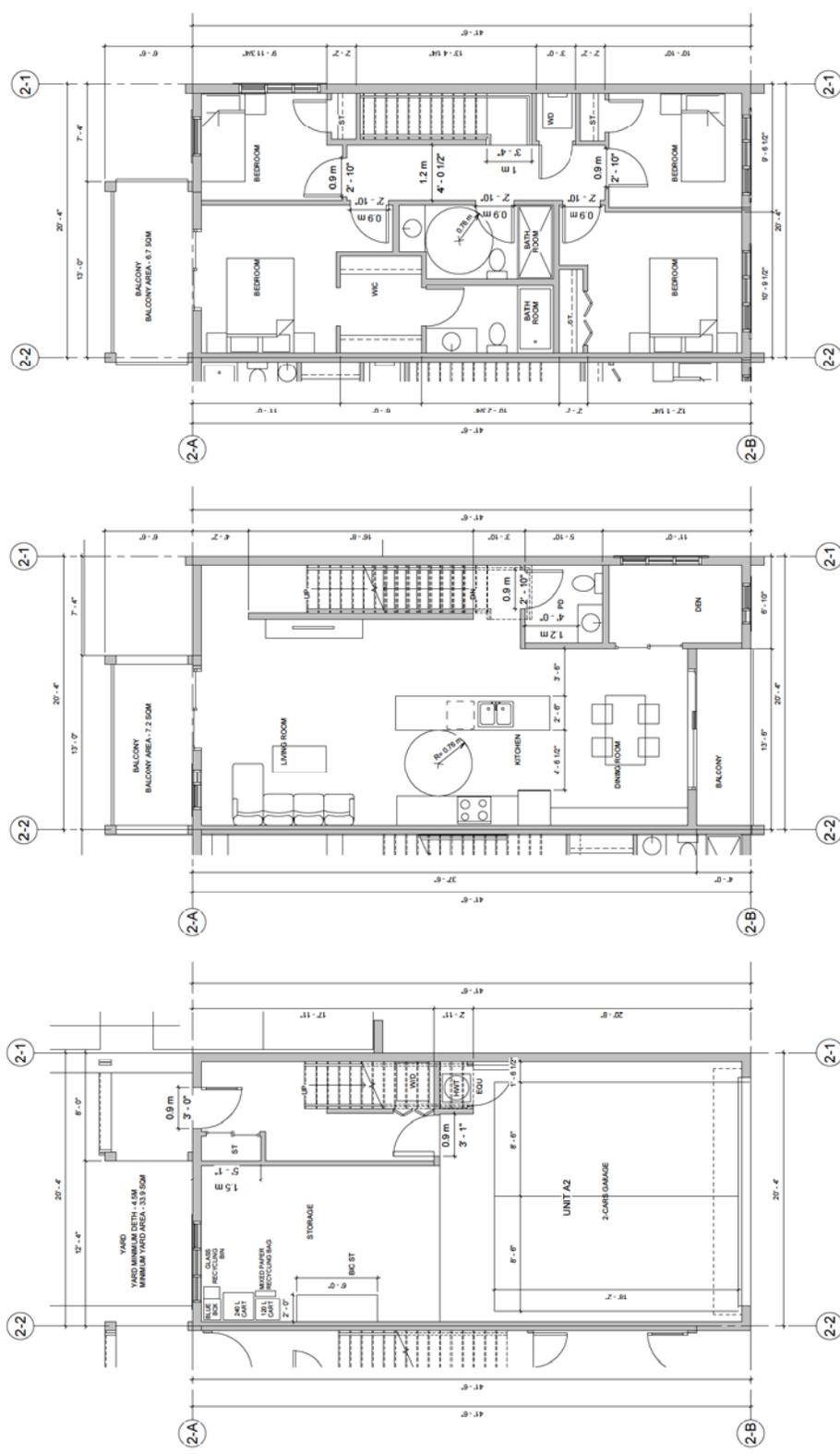
GROSS AREA 637 SQFT
FSE EXCLUSIONS 21 SQFT
GARAGE AREA 21 SQFT
TOTAL AREA 658 SQFT

GROSS AREA: 667 SQ FT
FOR AREA: 657 SQ FT

GROSS AREA 660 SQFT
FOR EXCLUSIONS 24 SQFT
- GARAGE 24 SQFT
- WASH/DRY 210 SQFT

PRIVATE OFFICE SPACE (WARD DEPTH 6.4 8.9M)
*23.5 SQM * 1.5 SQM * 7.2 SQM = 37.2 SQM

UNIT A1 FLOOR PLAN
3 BEDROOM + DEN + WORKSHOP
AREA = 1468 SQFT (FSR) 618 SQFT (NON-FSR)



① UNIT A2 - 1ST FLOOR
1st fl + 1.0'

② UNIT A2 - 2ND FLOOR
1st fl + 1.0'

③ UNIT A2 - 3RD FLOOR
1st fl + 1.0'

GROSS AREA: 844 SQ FT
FOR EXCLUSIONS: 48 SQ FT
FOR AREA: 796 SQ FT

GROSS AREA: 739 SQ FT
FOR AREA: 191 SQ FT

GROSS AREA: 844 SQ FT
FOR EXCLUSIONS: 48 SQ FT
FOR AREA: 796 SQ FT

Dimensions

Item	Unit	Value
1. Overall Depth (Exterior)	mm	8440
2. Overall Depth (Interior)	mm	8060
3. Overall Depth (Net Area)	mm	8060
4. Overall Depth (Net Area)	mm	8060
5. Overall Depth (Net Area)	mm	8060
6. Overall Depth (Net Area)	mm	8060
7. Overall Depth (Net Area)	mm	8060
8. Overall Depth (Net Area)	mm	8060
9. Overall Depth (Net Area)	mm	8060
10. Overall Depth (Net Area)	mm	8060

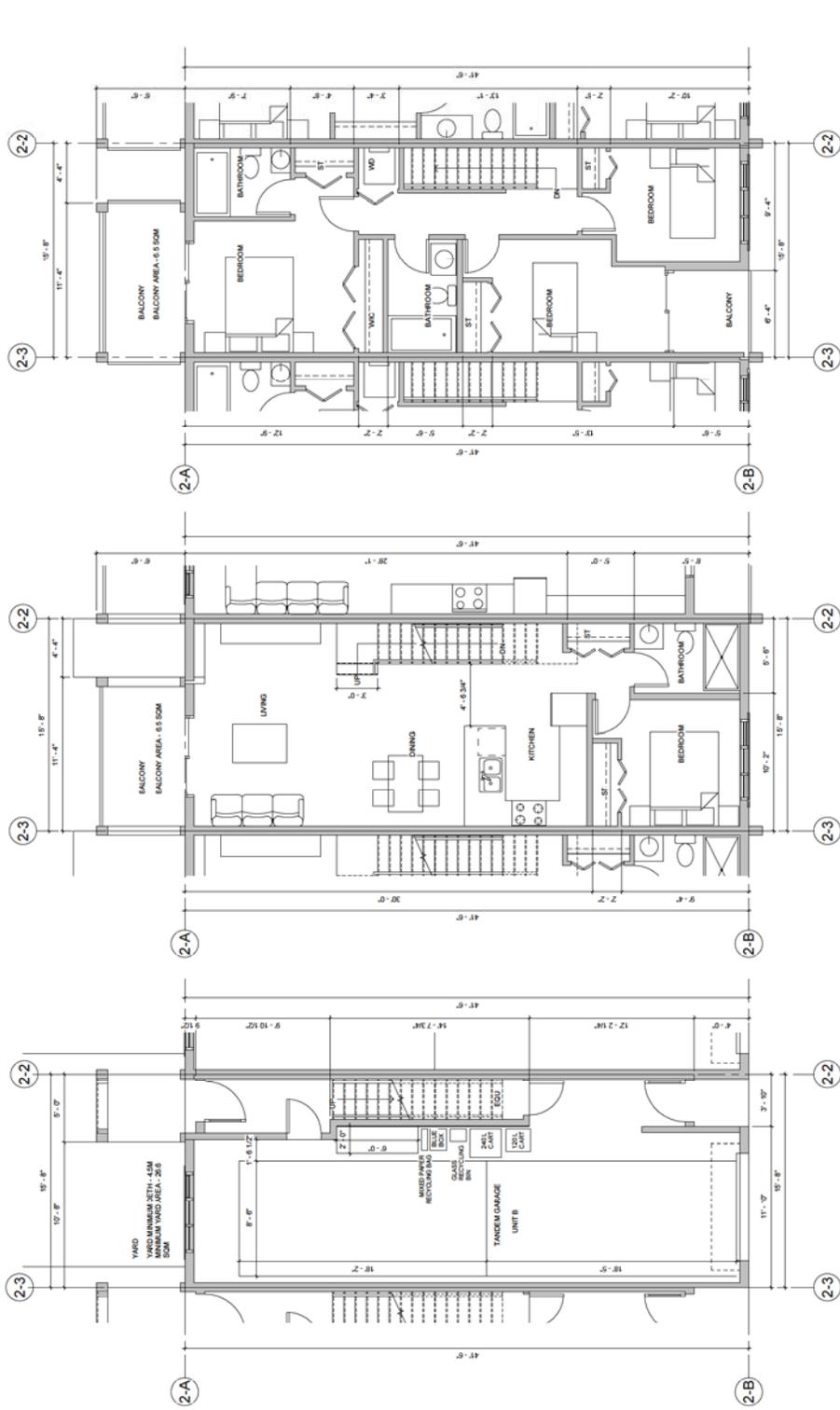


ACCESSIBILITY & AGING PLACE
1. STAIRS IN PLACE (STAIRS ARE INSTALLED IN ALL UNITS)
2. ALL THREE UNITS ARE CONVERTIBLE UNITS.
PRIVATE OUTDOOR SPACE (YARD DEPTH IS 4.9M)
- 33.9 SQM + 7.250M + 6.9 SQM
4 BEDROOM + DEN + WORKSHOP

UNIT A2 FLOOR PLAN

AREA = 844 SQ FT (844 SQ FT NOMINAL)
4 BEDROOM + DEN + WORKSHOP

STAR LINE DIMENSION IMAGE FROM H20/1909/101/08



① UNIT 8 - 1ST FLOOR
1/4" = 1'-0"

GROSS AREA	617	SQFT
FOR EXCLUSIONS	38	SQFT
NET AREA	579	SQFT

② UNIT 8 - 2ND FLOOR
1/4" = 1'-0"

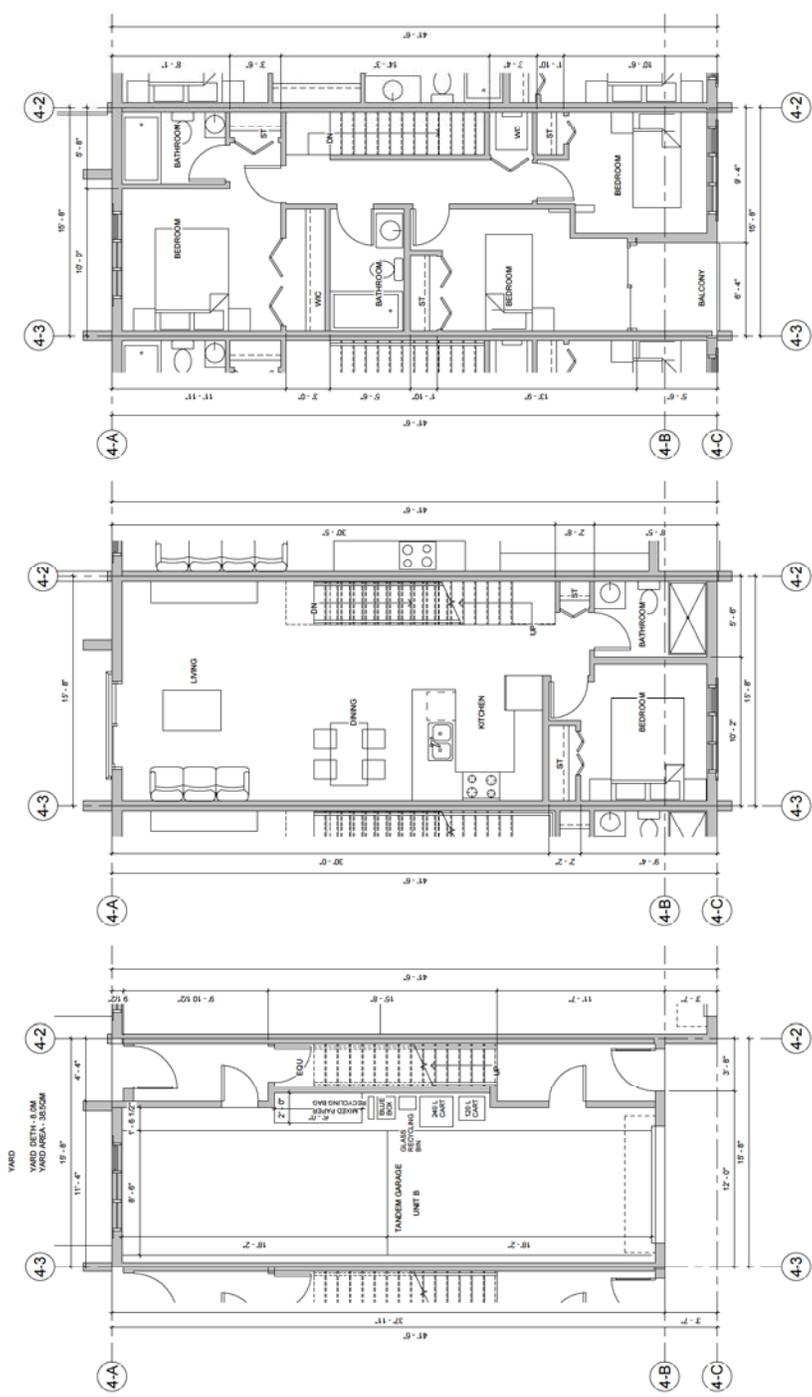
GROSS AREA	660	SQFT
FOR EXCLUSIONS	606	SQFT

③ UNIT 8 - 3RD FLOOR
1/4" = 1'-0"

GROSS AREA	632	SQFT
FOR EXCLUSIONS	137	SQFT
- ENTRY STAIRS	78	SQFT
FOR AREA	17	SQFT

PRIVATE OUTDOOR SPACE (VARD DECK @ 4.9M)
-25.50M + 5.50M = 5.50M + 35.5 5.0M

UNIT 8 FLOOR PLAN
3 BEDROOM + WORKSHOP
AREA = 1246 SQFT (F SR), 653 SQFT (NON-F SR)



① UNIT B1 - 1ST FLOOR
14' x 11' 0"

GROSS AREA	617	SOFT
EXCLUSIONS	38	SOFT
FSR AREA	579	SOFT

② UNIT B2 - 2ND FLOOR
14' x 11' 0"

GROSS AREA	600	SOFT
EXCLUSIONS	60	SOFT

③ UNIT B3 - 3RD FLOOR
14' x 11' 0"

GROSS AREA	617	SOFT
EXCLUSIONS	486	SOFT (FACT AREA - 50 SQM)
FSR AREA	22	SOFT

UNIT B2 FLOOR PLAN

3 BEDROOM + WORKSHOP

AREA	181	SOFT (FSR)	62	SOFT (NON FSR)
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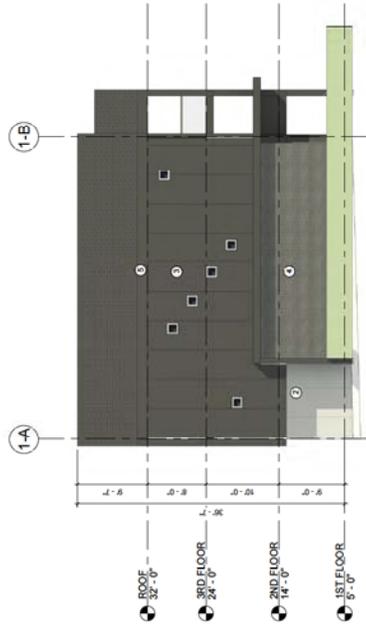
PRIVATE OUTDOOR SPACE (YARD DEPTH IS 1.5M)
~38.55 SQM



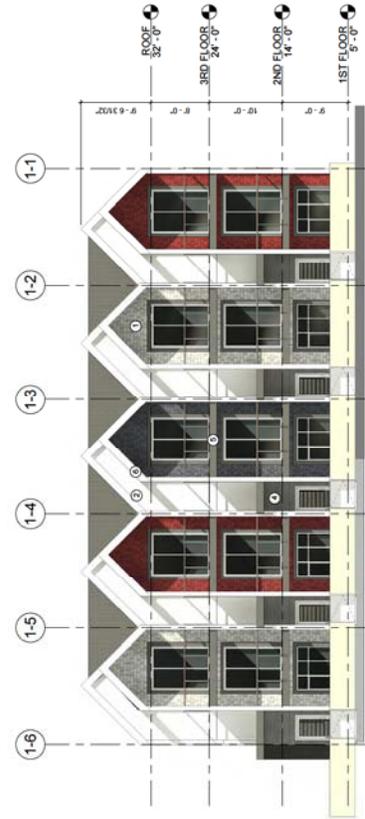
① BUILDING #1 - EAST ELEVATION
1/8" = 1'-0"

MATERIAL LEGEND

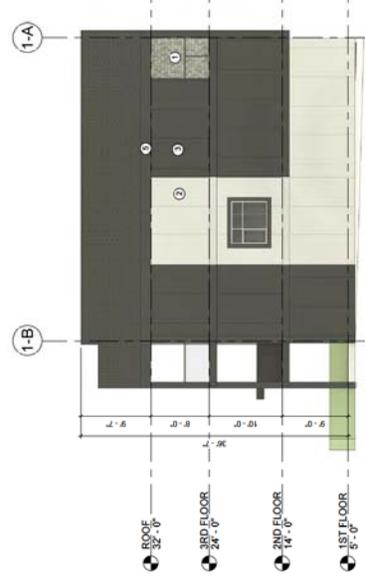
1. FIBER CEMENT SHINGLES - VARYING COLORS
2. FIBER CEMENT SIDING - SPANISH
3. FIBER CEMENT SIDING - IRON GREY
4. LEDGESTONE
5. DARK TRIM BOARD (MATCH MAT. #3)
6. WHITE TRIM BOARD (MATCH MAT. #2)



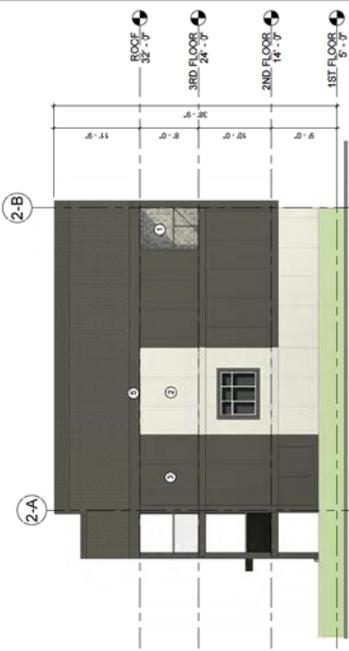
② BUILDING #1 - NORTH ELEVATION
1/8" = 1'-0"



③ BUILDING #1 - WEST ELEVATION
1/8" = 1'-0"

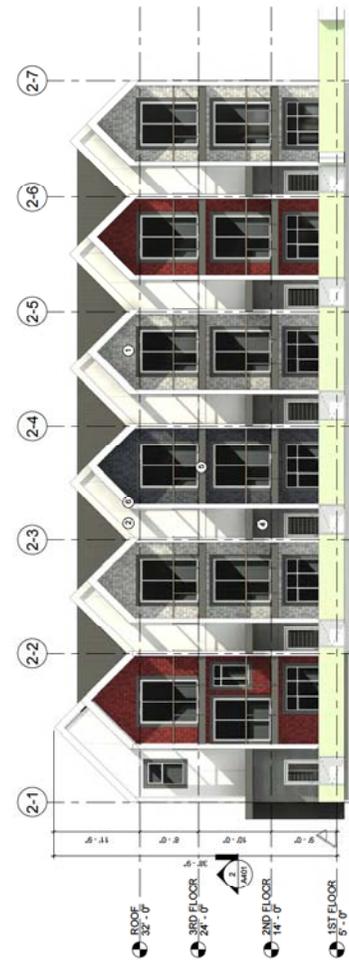


④ BUILDING #1 - SOUTH ELEVATION
1/8" = 1'-0"

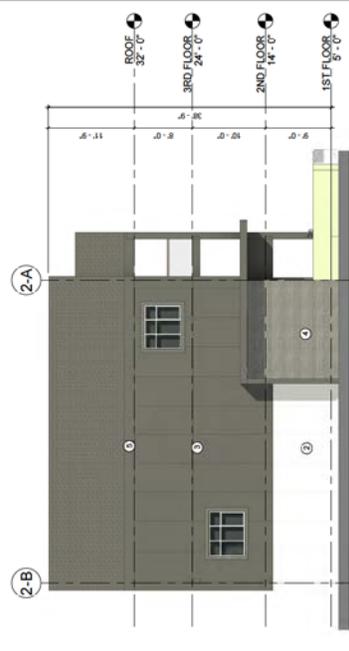


① BUILDING #1 - NORTH ELEVATION
1/8" = 1'-0"

- MATERIAL LEGEND.**
1. FIBER CEMENT SHINGLES - VARYING COLORS
 2. FIBER CEMENT BOARDS - IRON GREY
 3. FIBER CEMENT BOARDS - IRON GREY
 4. LEDGESTONE
 5. HARD MATCH (MATCH MAT. #3)
 6. WHITE TRIM BOARD (MATCH MAT. #2)



② BUILDING #2 - EAST ELEVATION
1/8" = 1'-0"



③ BUILDING #2 - SOUTH ELEVATION
1/8" = 1'-0"



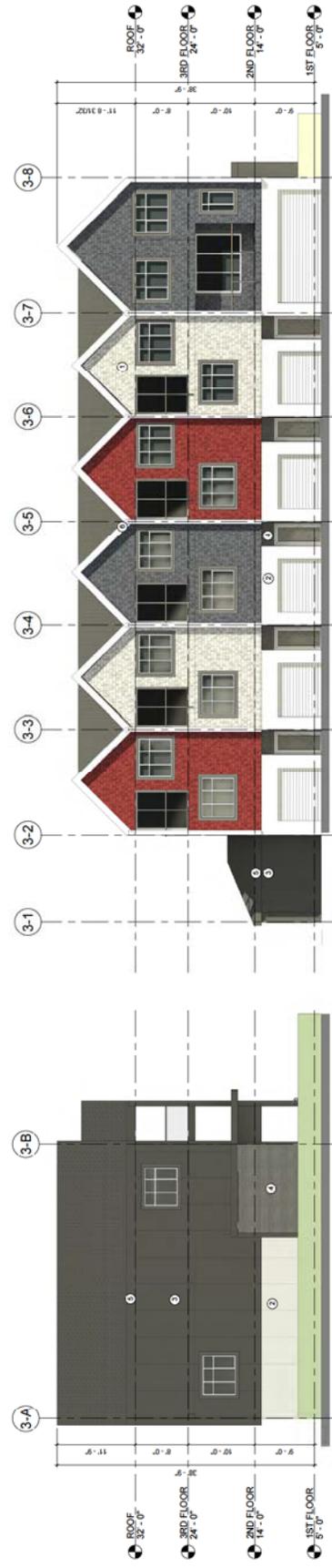
④ BUILDING #2 - WEST ELEVATION
1/8" = 1'-0"



③ BUILDING #3...WEST ELEVATION
1/8" = 1'-0"

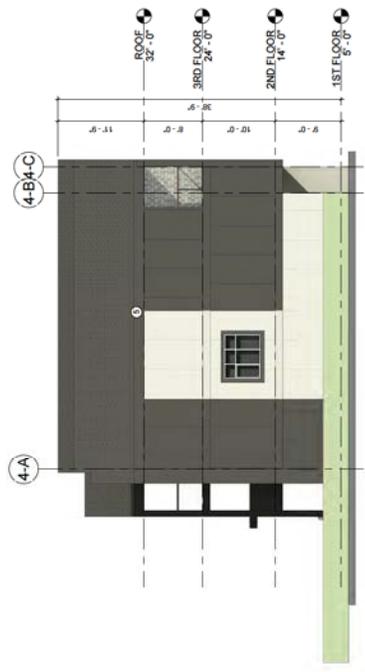
③ BUILDING #3...SOUTH ELEVATION
1/8" = 1'-0"

- MATERIAL LEGEND.**
1. FIBER CEMENT SHINGLES - VARIOUS COLORS
 2. FIBER CEMENT BOARDS - OFF-WHITE
 3. FIBER CEMENT BOARDS - IRON GREY
 4. FIBER CEMENT BOARDS - MATCH MAT. #3
 5. DARK TRIM BOARD (MATCH MAT. #3)
 6. WHITE TRIM BOARD (MATCH MAT. #2)



① BUILDING #8...EAST ELEVATION
1/8" = 1'-0"

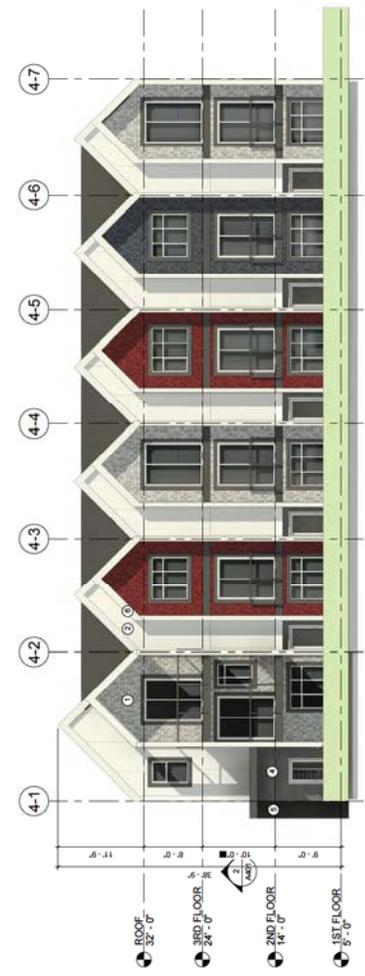
② BUILDING #8...NORTH ELEVATION
1/8" = 1'-0"



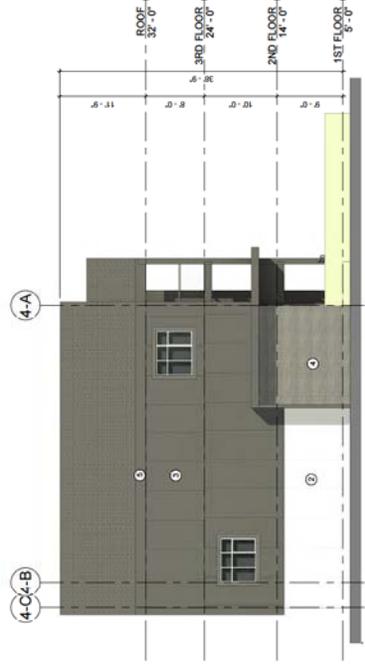
① BUILDING #4 - NORTH ELEVATION
10' x 11'

MATERIAL LEGEND.

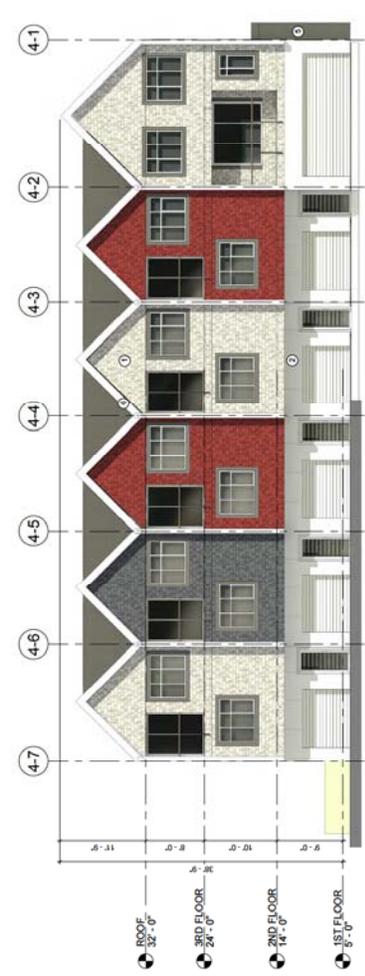
1. FIBER CEMENT SHINGLES - VARIING COLORS
2. FIBER CEMENT BOARDS - OFF-WHITE
3. FIBER CEMENT BOARDS - IRON GREY
4. LEDGESTONE
5. DARK TRIM BOARD (MATCH MAT. #3)
6. WHITE TRIM BOARD (MATCH MAT. #2)



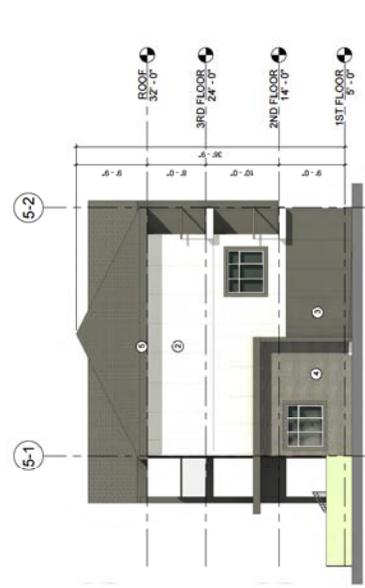
① BUILDING #4 - EAST ELEVATION
10' x 11'



② BUILDING #4 - SOUTH ELEVATION
10' x 11'

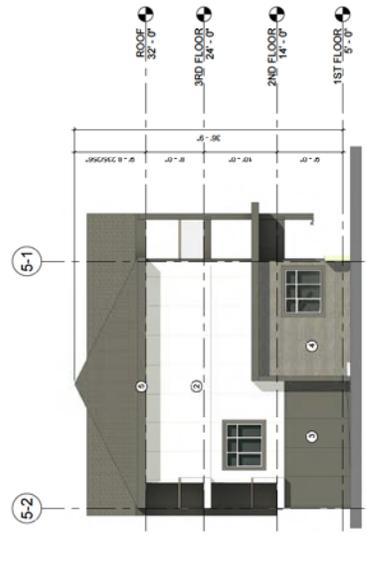


③ BUILDING #4 - WEST ELEVATION
10' x 11'

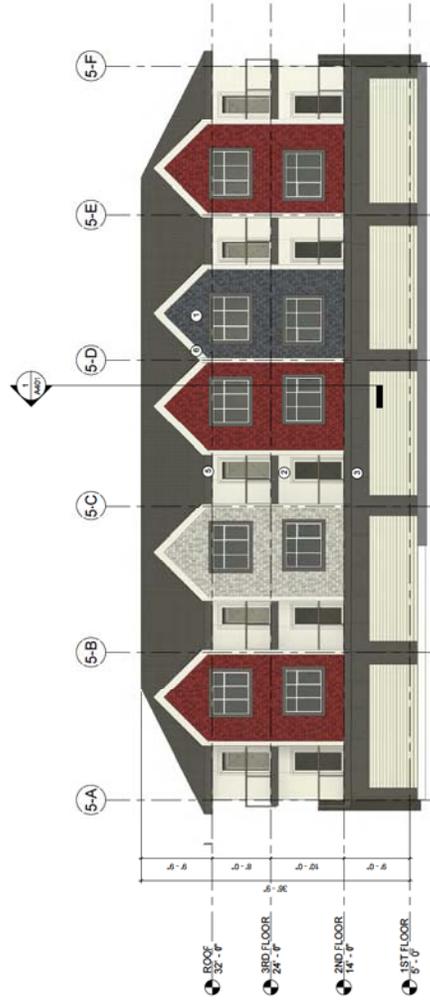


⑤ BUILDING #6 - EAST ELEVATION
1/8" = 1'-0"

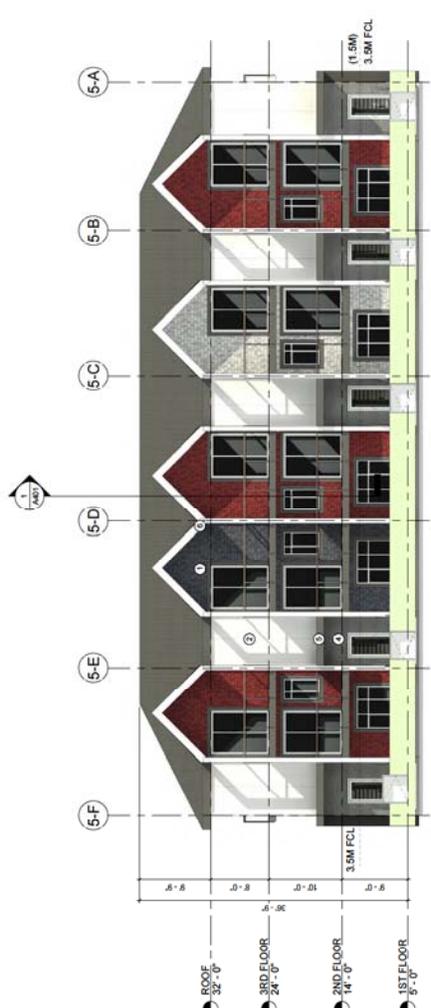
- MATERIAL LEGEND:**
1. FIBER CEMENT SHINGLES - VARYING COLORS
 2. FIBER CEMENT BOARDS - OFF-WHITE
 3. FIBER CEMENT BOARDS - IRON GREY
 4. LEDGESTONE
 5. DARK TRIM BOARD (MATCH MAT #3)
 6. WHITE TRIM BOARD (MATCH MAT #2)



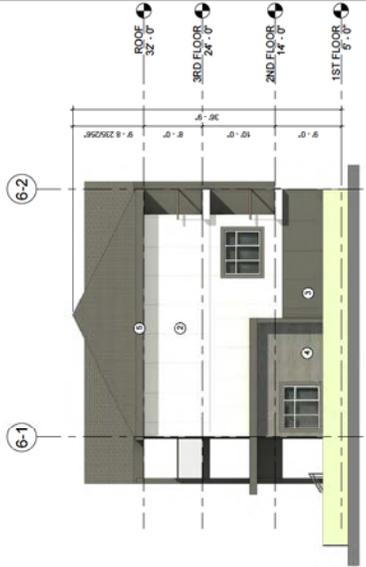
⑥ BUILDING #6 - WEST ELEVATION
1/8" = 1'-0"



⑦ BUILDING #6 - NORTH ELEVATION
1/8" = 1'-0"

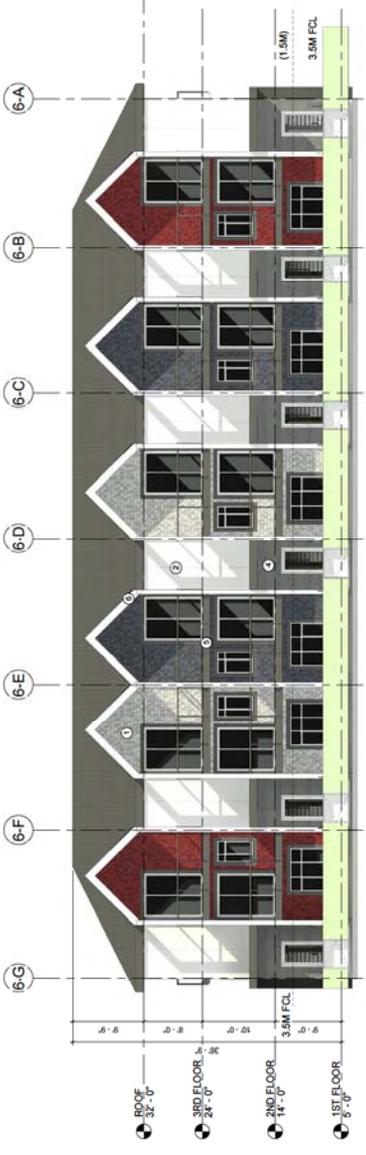


⑧ BUILDING #6 - SOUTH ELEVATION
1/8" = 1'-0"

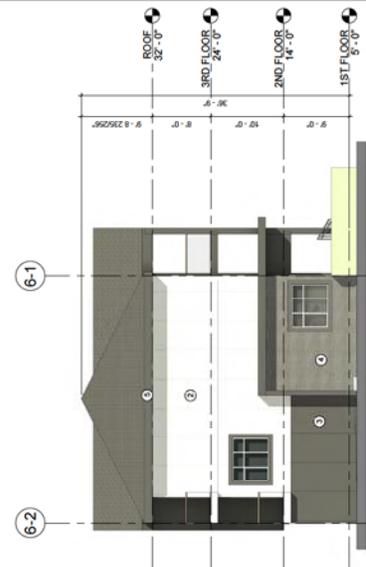


① BUILDING #6 MERRIDOL - EAST ELEVATION
1/8" = 1'-0"

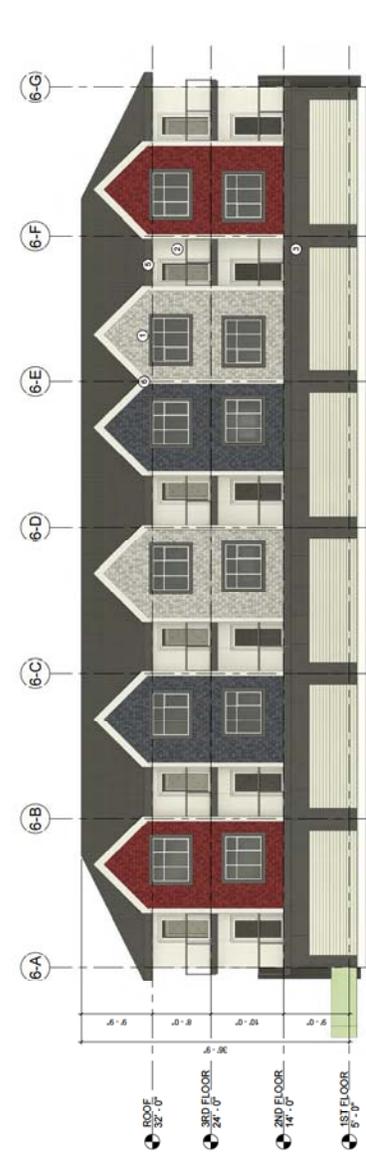
- MATERIAL LEGEND**
1. FIBER CEMENT SHINGLES - VARIING COLORS
 2. FIBER CEMENT SHINGLES - IRON GREY
 3. FIBER CEMENT BOARDS - IRON GREY
 4. LEDGESTONE
 5. DARK TRIM BOARDS (MATCH MAT. #8)
 6. WHITE TRIM BOARD (MATCH MAT. #8)



② BUILDING #6 MERRIDOL - SOUTH ELEVATION
1/8" = 1'-0"



③ BUILDING #6 MERRIDOL - WEST ELEVATION
1/8" = 1'-0"



④ BUILDING #6 MERRIDOL - NORTH ELEVATION
1/8" = 1'-0"





① - INTERFACE - FRONT/WEST



② - INTERFACE - BACK/EAST



① INTERFACE SIDE 1 (NORTH)



② INTERFACE SIDE 2 (SOUTH)

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SEAL



MATERIALS PLAN

HARDSCAPE LEGEND

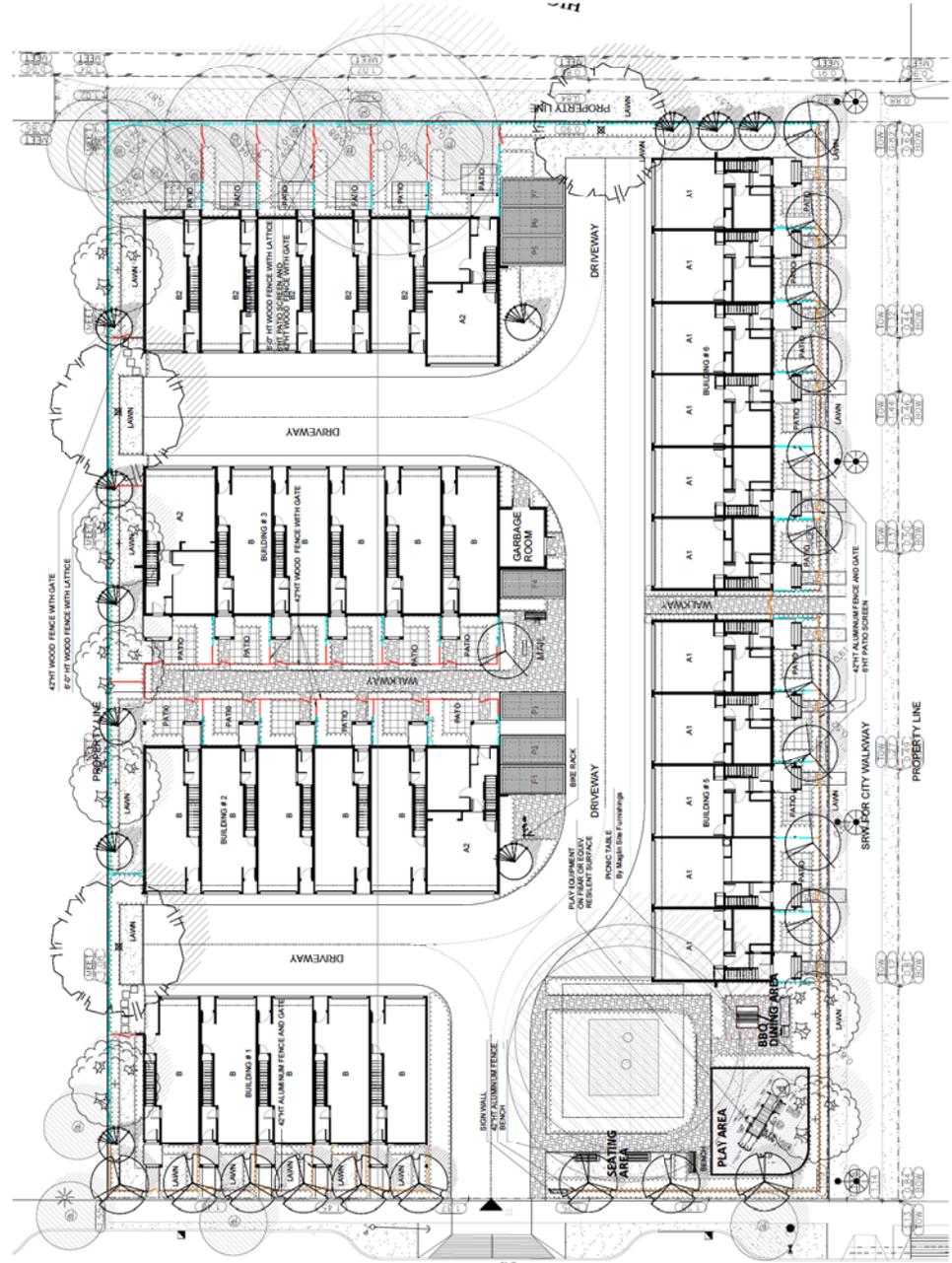
- A HOLLOW BLOCK PAVING BY NATURAL MATERIALS WITH A RANDOM LAYOUT PATTERN
- B ECO-CONCRETE PERMEABLE PAVING BY NATURAL MATERIALS WITH A RANDOM LAYOUT PATTERN
- C HARVEST BRICK PAVING BY NATURAL MATERIALS WITH A RANDOM LAYOUT PATTERN
- D 2X2 CONCRETE SLAB, MANUFACTURED BY [SPECIFICATION]
- E 2X2 STEPPING STONE
- F FIRM ON PLAYGROUND
- G MULCH IN T2Z

FENCE LEGEND

- 4" FT ALUMINUM FENCE
- 4" FT WOOD FENCE
- 4" FT WOOD FENCE

LIGHTING LEGEND

- SYMBOL: STYLE
- 1 BOLLARD 30" HT
- 2 MOUNTED STEP LIGHT BY OTHERS
- 3 SLOPE



PLANT SCHEDULE

PLANT	SYMBOL	COMMON NAME	PLANTED SIZE / REMARKS
3	(Symbol)	RED SUNSET MAPLE	9CM CAL. 2M STD. B&B
4	(Symbol)	DAWKOP PURPLE BEECH	8CM CAL. 1.8M STD. B&B
5	(Symbol)	DAWKOP PURPLE BEECH	4M FT. B&B
6	(Symbol)	SLANGER SERBIAN SPRUCE	4M FT. B&B
7	(Symbol)	JAPANESE SNOWBELL	7CM CAL. 2M STD. B&B

NOTES : PLANT SIZE IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD. LATEST DESIGN. CONTAINER SIZES SPECIFIED AS PER CANADA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. SEARCH AND REVIEW MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT THE TIME OF PLANTING. ALL PLANT MATERIALS MUST BE PROVIDED FROM A CERTIFIED NURSERY. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD. DEFINITION OF CONDITIONS OF PLANT MATERIAL MUST BE PROVIDED FROM A CERTIFIED DISEASE FREE NURSERY. SUBSTITUTIONS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.

NOTE:
- All soft landscape areas to be irrigated with automatically installed to I.I.A.B.C. Standards, latest edition.
- For grading information please refer to civil and architectural plans.

PLN - 42

PROJECT:
34 UNIT TOWNHOUSE DEVELOPMENT

NO. DATE REVISION DESCRIPTION DR

CLIENT: JAMAL CONSTRUCTION WITH: WILSON CHANG ARCHITECT INC.

DRAWING TITLE:
LANDSCAPE PLAN

DATE: JUL 20, 2021
SCALE: 1/4"=1'-0"
DRAWN: DD
DESIGN: DD
CHKD: PCM
PMG PROJECT NUMBER: 21-560

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SCALE

NO.	DATE	REVISION DESCRIPTION	DR
1	21 AUGUST	ISSUANCE FOR PERMIT COMMENTS	DR

CLIENT: JAMAIN CONSTRUCTION
WITH: WILSON CHANG ARCHITECT INC.

PROJECT:
34 UNIT TOWNHOUSE DEVELOPMENT
4800 - 4440 SMITH CRESCENT
RICHMOND

DRAWING TITLE:
LANDSCAPE DETAILS

DATE: JUL 20, 2021
SCALE:
DRAWN: DD
DESIGN: DD
CHKD: PCM

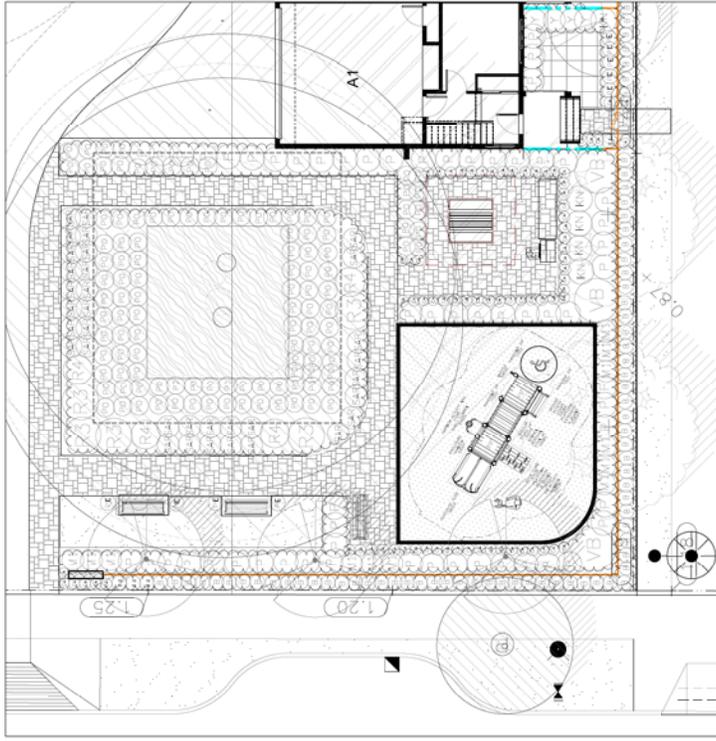
DRAWING NUMBER:

L2

OF 3

21-560

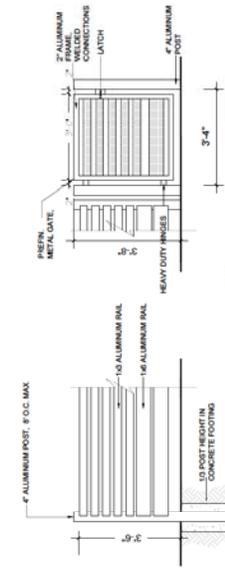
PMG PROJECT NUMBER:
23660.12P



LANDSCAPE ENLARGEMENT

1/8" = 1'-0"

B 72	BUXUS MICROPHYLLA 'WINTER GEM'
B 71	FRAXINUS JAPONICA 'WILEY FIRE'
EA 3	ERGONIA MARGINALIS 'MAGNIFICENT'
KN 3	ROSA 'KNOCKOUT RADRAZZ'
RA 4	SARGOCOCOA 'HOOKERANA RUSCIFOLIA'
SAB4	TAXUS X MEDIA 'HICKSII'
Y 4	VIBURNUM X 'BURKWOODII'
VB 3	MISCANTHUS SINENSIS 'LITTLE KITEN'
CA 78	PENNETIUM ALOPECUROIDES 'HADELIN'
MI 12	RUDBECKIA FULGIDA 'VAR SULLIVANTI' 'GOLDLOCKS'
PE 3	POLYSTICHUM MUNITUM
RU 4	
POT18	



UNIT GATES

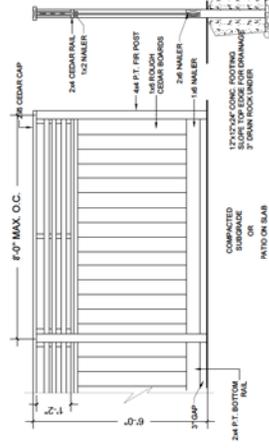
1/2" = 1'-0"

UNIT FENCES

1 FENCE TO HAVE 2 COATS OF POWDER COAT IN BLACK.

2 GATE TO HAVE 2 COATS OF POWDER COAT IN BLACK.

- NOTES:
1. ALL POSTS PRESURE TREATED TO CAN STANDARD AND END CUTS TREATED WITH PRESERVATIVE.
 2. ALL OTHER NUMBERS TO BE CEDAR. (42" CONSTRUCTION GRADE MINIMUM)
 3. ALL HARDWARE NOT DIMENSIONED.
 4. FINISH SELECTIONS TO BE APPROVED BY PROJECT ARCHITECT.
 5. ALL FENCES TO BE LEVEL. CHANGES IN GRADES TO BE IN 1/4" STEPS (MAX).
 6. At site to match the conditions shown of the Heritage House and New House drawings.



6'-0" HT. WOOD FENCE WITH LATTICE

1/2" = 1'-0"

4'-8"

PLAN VIEW

ARCHITECTURAL CONCRETE WALL

LETTERS GRAPHS BY CHINK WITH FLOATING EFFECT

PROJECT NAME ADDRESS

FRONT VIEW

SECTION

1/2" = 1'-0"

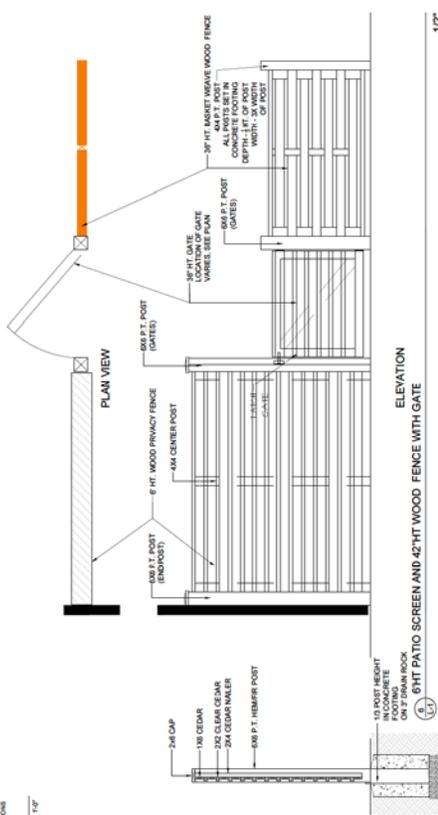
MICHAEL J MILLS CONSULTING

NOTE: ALL DIMENSIONS ARE EXPRESSED. CONTACT PROJECT ARCHITECT FOR ASSISTANCE PRIOR TO PROCEEDING.

PAVERS TO BE INSTALLED TO MANUFACTURER'S SPECIFICATIONS.

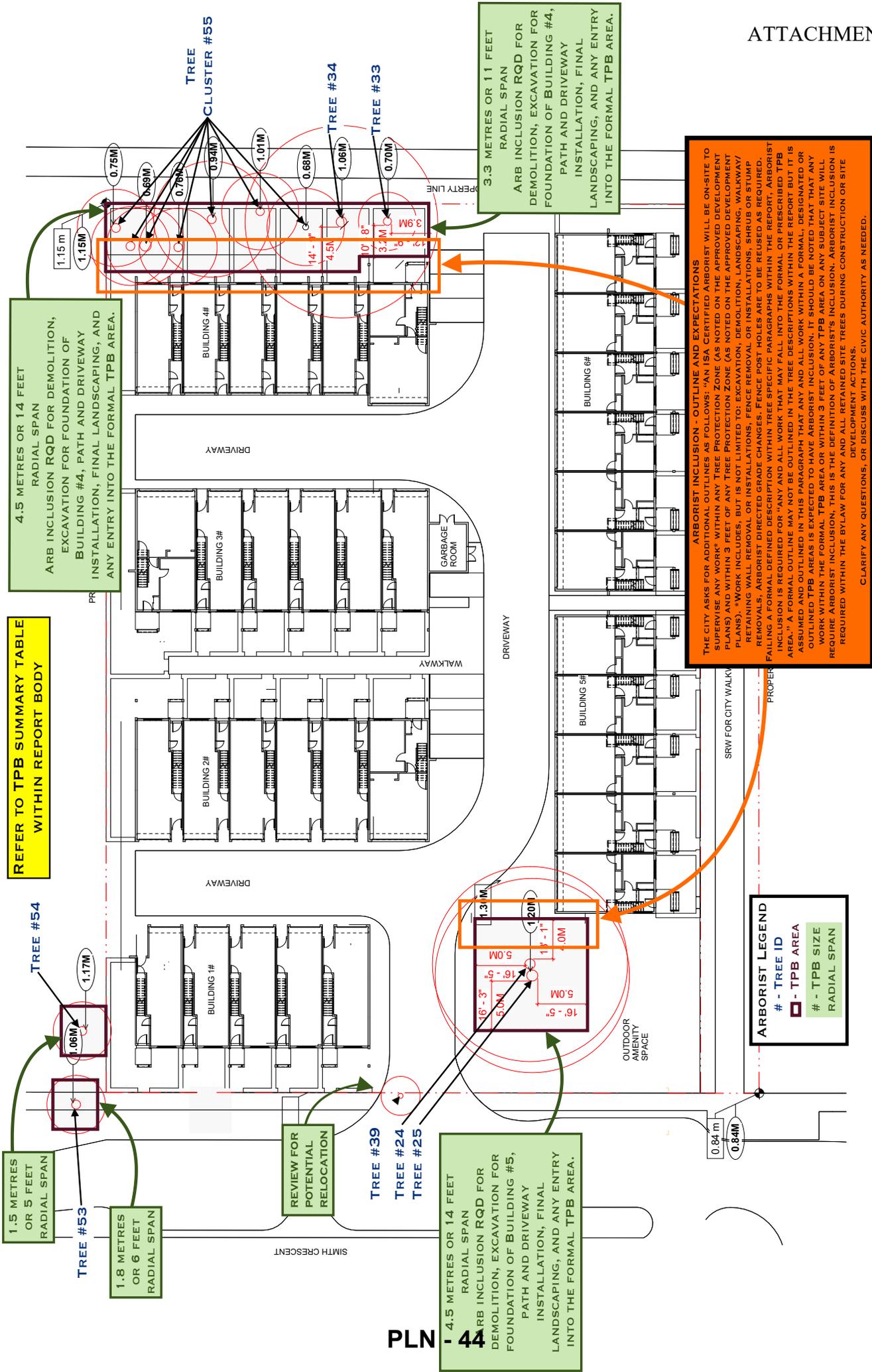
3/4" x 1'-0"

PAVERS THROUGH ROOT ZONE



ELEVATION

1/2" = 1'-0"



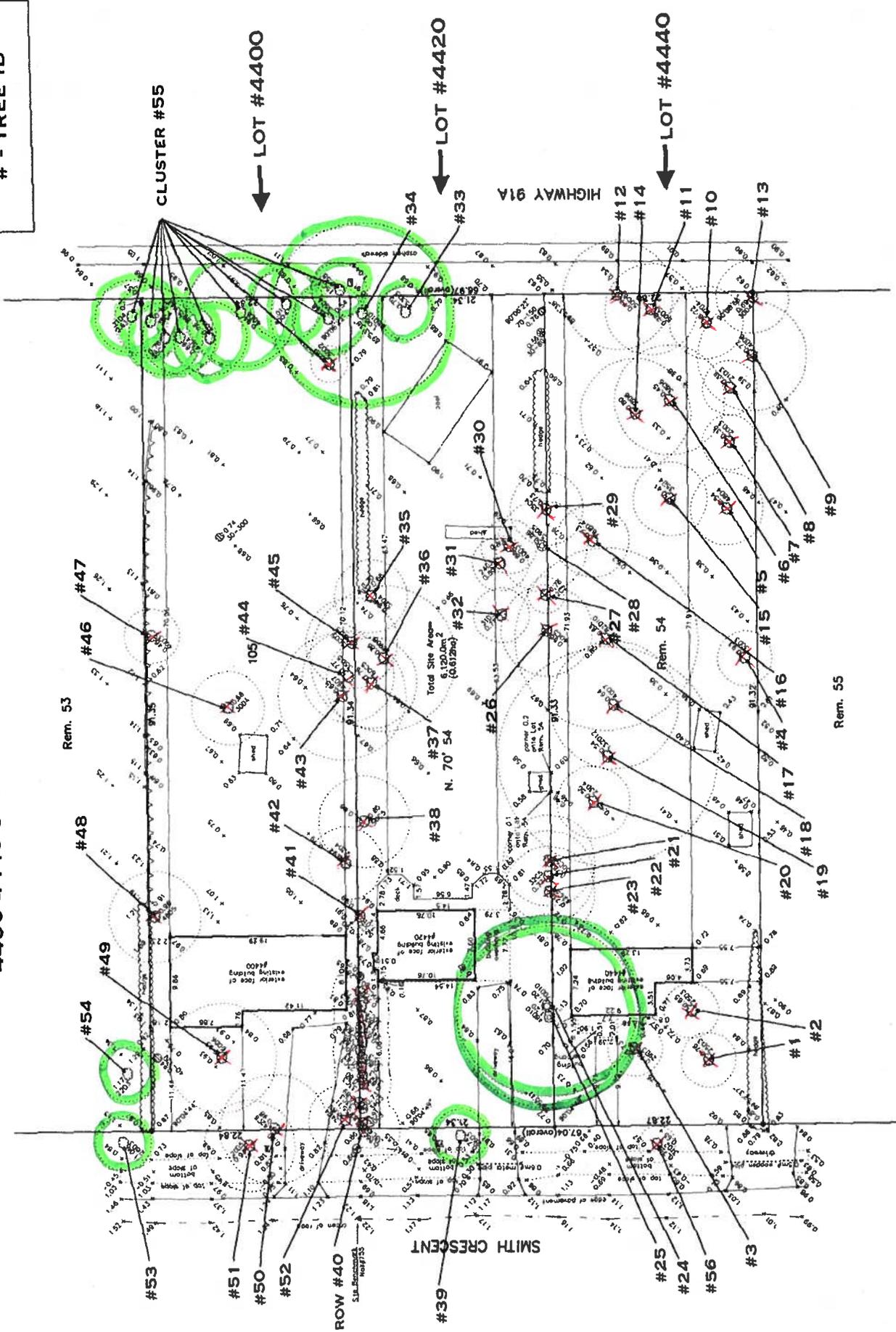
ARBORIST INCLUSION - OUTLINE AND EXPECTATIONS

THE CITY ASKS FOR ADDITIONAL OUTLINES AS FOLLOWS: "AN ISA CERTIFIED ARBORIST WILL BE ON-SITE TO SUPERVISE ANY WORK* WITHIN ANY TREE PROTECTION ZONE (AS NOTED ON THE APPROVED DEVELOPMENT PLANS) AND WITHIN 3 FEET OF ANY TREE PROTECTION ZONE (AS NOTED ON THE APPROVED DEVELOPMENT PLANS). *WORK INCLUDES, BUT IS NOT LIMITED TO: EXCAVATION, DEMOLITION, LANDSCAPING, WALKWAY/RETAINING WALL REMOVAL OR INSTALLATIONS, FENCE REMOVAL OR INSTALLATIONS, SHRUB OR STUMP REMOVALS, ARBORIST DIRECTED GRADE CHANGES, FENCE POST HOLES ARE TO BE REUSED AS REQUIRED. FAILING A FORMAL DEFINED DESCRIPTION WITHIN TREE SPECIFIC PARAGRAPHS WITHIN THE REPORT, ARBORIST INCLUSION IS REQUIRED FOR "ANY AND ALL WORK THAT MAY FALL INTO THE FORMAL OR PRESCRIBED TPB AREA." A FORMAL OUTLINE MAY NOT BE OUTLINED IN THE TREE DESCRIPTIONS WITHIN THE REPORT BUT IT IS ASSUMED AND OUTLINED IN THIS PARAGRAPH THAT ANY AND ALL WORK WITHIN A FORMAL, DESIGNATED OR OUTLINED TPB AREA IS EXPECTED TO HAVE ARBORIST INCLUSION. IT SHOULD BE NOTED THAT ANY WORK WITHIN THE FORMAL TPB AREA OR WITHIN 3 FEET OF ANY TPB AREA ON ANY SUBJECT SITE WILL REQUIRE ARBORIST INCLUSION, THIS IS THE DEFINITION OF ARBORIST'S INCLUSION. ARBORIST INCLUSION IS REQUIRED WITHIN THE BYLAW FOR ANY AND ALL RETAINED SITE TREES DURING CONSTRUCTION OR SITE DEVELOPMENT ACTIONS.

CLARIFY ANY QUESTIONS, OR DISCUSS WITH THE CIVIC AUTHORITY AS NEEDED.

ARBORIST LEGEND
- TREE ID

4400-4440 SMITH CRESCENT, RICHMOND





Address: 4400, 4420, and 4440 Smith Crescent

File No.: RZ 18-802860

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 9877, the developer is required to complete the following:

1. Provincial Ministry of Transportation & Infrastructure Approval.
2. Consolidation of all the lots into one development parcel (which will require the demolition of the existing dwellings).
3. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
4. City acceptance of the developer's offer to voluntarily contribute \$38,250 to the City's Tree Compensation Fund for the planting of replacement trees within the City, for the removal of trees on the subject site.
5. Submission of a \$3,000 contribution to the City's Tree Compensation Fund for the removal of two City-owned trees in the development frontage (Note: payment is required prior to removal of the trees or final adoption of the rezoning bylaw, whichever is earlier).
6. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
7. Submission of a Tree Survival Security to the City in the amount of \$75,000 for the 11 on-site trees and two City trees to be retained, including: \$10,000 for each of Tag # 24 and 25, and \$5,000 for each of Tag # 34, 35, 39, 53 and 55 (consisting of 7 trees). Up to 90% of the security will be returned upon receipt of a Post Construction Impact Assessment from the Certified Arborist, with the remainder held for one year to ensure that the trees survive.
8. Granting of a 6.0 m wide statutory right-of-way along the south property line for a pedestrian pathway, landscaping, and lighting, with the developer and owner being responsible for liability, construction and maintenance. The design is to be included in the Servicing Agreement (SA) in accordance with City specifications and standards.
9. Registration of a flood plain covenant on title identifying a minimum habitable elevation of 3.5 m GSC.
10. Registration of a restrictive covenant prohibiting (a) the imposition of any strata bylaw that would prohibit any residential dwelling unit from being rented; and (b) the imposition of any strata bylaw that would place age-based restrictions on occupants of any residential dwelling unit.
11. The submission and processing of a Development Permit* completed to a level deemed acceptable by the Director of Development.
12. City acceptance of the developer's offer to voluntarily contribute \$0.93 per buildable square foot (e.g. \$45,962.46) to the City's public art fund.
13. City acceptance of the developer's offer to voluntarily contribute \$6.55 per buildable square foot (e.g. \$323,714.10) to the City's Hamilton Area Plan Amenity Reserve Fund.
14. Contribution of \$88,460 in-lieu of on-site indoor amenity space to go towards development of the City facilities (2021 rates: \$1,805 per unit up to 19; \$3,611 per additional unit up to 39).
15. City acceptance of the developer's offer to voluntarily contribute \$8.50 per buildable square foot (e.g. \$420,087.00) to the City's affordable housing fund.
16. Registration of a legal agreement on title prohibiting the conversion of the garage and storage areas into habitable space.

17. Enter into a Servicing Agreement* for the design and construction of engineering infrastructure improvements. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to:

Water Works:

- a. Using the OCP Model, there is 123 L/s of water available at a 20 psi residual at the Smith Crescent frontage. Based on your proposed development, your site requires a minimum fire flow of 220 L/s.
- b. At the developer's cost, the developer is required to:
 - i) At building permit stage, submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm the development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit designs at Building Permit stage.
 - ii) Upgrade the existing 150mm watermain to 300mm diameter (complete with fire hydrants at the east side of the road that are spaced as per City standards) approximately 213 m from the intersection of Smith Crescent and Willett Avenue, north to Westminster Highway. Tie-in shall be to the existing 300mm diameter watermain on Westminster Highway at the North. Valves should be installed to meet City standards. Exact alignment in the roadway shall be determined via the Servicing Agreement process.
 - iii) As the clearance between the new watermain and the new sanitary main will be less than the minimum required in the City's Engineering Specifications (i.e., 3m minimum), the joints shall be wrapped in accordance with the latest version of ANSI/AWWA Standards C214, C209, C217 as indicated in the City's Engineering specifications.
 - iv) Install a new water service connection at the Smith Crescent frontage, complete with meter and meter chamber in a right-of-way which will be provided by the developer. The dimensions and location of the right of way shall be finalized at the servicing agreement process.
 - v) Re-connect all existing water service connections on Smith Crescent, complete with meter and meter chambers.
 - vi) Install a new fire hydrant along Development frontage to meet City spacing requirements.
 - vii) Re-connect existing fire hydrants north of the development site to the new watermain.
 - viii) Remove and legally dispose offsite the existing 150 mm AC mains along Smith Crescent (Willet Avenue intersection to Westminster Hwy at the North) upon successful tie-in of the new watermain to the system.
- c. At the developer's cost, the City is to:
 - i) Complete all proposed water main tie-ins.
 - ii) Cut and cap at main all existing water service connections.

Storm Sewer Works:

- a. At the developer's cost, the developer is required to:
- i) Install a new 600mm diameter storm sewer in the roadway on the east side with an approximate length of 93 meters from the north property line of 4400 Smith Crescent (Manhole STMH572) to the intersection of Willett Ave and Smith Crescent. Exact alignment in the roadway shall be determined via the Servicing Agreement process.
 - ii) New manholes are required at the north and south end of the new storm sewer. The placement of the manhole at the south end shall match the alignment of the ultimate storm sewer along Willett Avenue, which shall be approximately 14 meters south of the southern property line of the Hamilton VLA Park.
 - iii) Tie-in to the north shall be:
 - Via a headwall to connect to the existing ditches to the north.
 - Via a new 600mm diameter pipe from the new northern manhole to a new manhole that will replace STMH572 at the west side of Smith Crescent.
 - iv) Tie-in to the south shall be:
 - Via a headwall to connect to the existing ditches to the south.
 - Via a new 600mm diameter pipe from the new southern manhole to a new manhole which will be tied-in to the existing storm sewer that crosses Willett Avenue at the west side of Smith Crescent.
 - v) Infill the existing ditch along the east side of Smith Crescent to accommodate the following:
 - the required frontage improvements
 - the required tie-in of the widened road to the existing edge of asphalt
 - the required tie-ins of the new storm sewers to the existing ditches that front 4380 and 4460 Smith Crescent.
 - vi) Remove and dispose offsite the existing storm sewers along the west side of Smith Crescent between STMH571 and STMH572.
 - vii) Install a storm sewer service connection, complete with a 1050mm diameter manhole in a 3m X 3m utility right of way and tie-in to the proposed storm sewer at Smith Crescent frontage.
- b. At the Developers cost, the City is to:
- i) Complete all proposed storm sewer tie-ins.

Sanitary Sewer Works:

- a. At the Developers cost, the Developer is required to:
- i) Install sanitary sewers with an approximate length of 25 meters (complete with manholes that are spaced as per City standards) from the north property line to the proposed pump station at the east side of Hamilton VLA Park. If the sanitary gravity line south of the proposed pump station is not complete, the Developer is required to construct additional sanitary main along property frontage from the proposed pump station manhole to the southern property line of the proposed development.

- ii) All flows are to be directed towards the sanitary pump station, connect to the pump station via the proposed manhole fronting the station. Provide a manhole at the north and end of the new sanitary line for future connections. If the portion of sanitary main south of the pump station is constructed by the Developer, a manhole will be required at the south end of the main.
- iii) Install a sanitary sewer service connection, complete with a 1050mm diameter manhole in a 3m x 3m utility right of way and tie-in to the proposed manhole fronting the pump station.
- iv) Pay, in keeping with the Subdivision and Development Bylaw No. 8751, a \$13,172.64 cash-in-lieu contribution towards the Hamilton Area Sanitary Pump Station.

Frontage Improvements:

- a. At the Developers cost, the Developer is required to:
 - ii) Upgrade Smith Crescent as per the cross section in the Hamilton Area Plan, which includes from west to east:
 - 2.0 m wide concrete sidewalk;
 - 3.2 m wide asphalt off-road cycle path;
 - 0.6 m wide buffer strip;
 - 2.4 m wide area for landscaped/tree boulevard and on-street parking (equal length of the frontage should be provided for each);
 - 0.15 m wide concrete curb/gutter
 - 6.8 m wide driving surface
 - 0.15 m wide concrete curb/gutter
 - 2.4 m wide area for landscaped/tree boulevard and on-street parking (equal length of the frontage should be provided for each);
 - 0.6 m wide buffer strip; and
 - 2.0 m wide concrete sidewalk.
 - (Note: additional roadwork would be necessary outside the development frontage in order to provide a proper transition of the above-noted cross-section to existing road)
 - iii) Upgrade the pedestrian pathway behind the subject site to include:
 - 3.0 m wide asphalt pathway;
 - Pedestrian-scale lighting; and
 - Landscaping.
 - (Note: works within MOTI lands require Provincial approval)
 - iv) Construct a new pedestrian pathway along the south property line to include (from north to south):
 - 2.5 m wide landscape strip with pedestrian-scale lighting;
 - 3.0 m wide pathway; and
 - 0.5 m wide temporary landscape buffer (provide low shrubs or groundcovers that are easily removed).
 - (Note: there is an existing fence and hedges on 4460 Smith Crescent. Works along the property line should be coordinated with the adjacent property owner to minimize off-site impacts.)
 - v) Coordinate with Parks on all proposed landscape treatments on Smith Crescent and both pedestrian pathways.

- vi) Provide street lighting along Smith Crescent frontage.
- vii) Remove existing BC Hydro service connection support pole and anchors on property frontage.
- viii) Put underground the existing private utility overhead lines (e.g., BC Hydro, Telus and Shaw) along Smith Crescent frontage of the proposed site. The developer is required to coordinate with the private utility companies regarding the extent of the undergrounding works.
- ix) Coordinate with private utility companies:
 - When relocating/modifying any of the existing power poles and/or guy wires.
 - To determine if above ground structures are required now or in the future and coordinate their on-site locations (e.g. Vista, PMT, LPT, Shaw cabinets, Telus Kiosks, etc).
 - To provide rights-of-ways to accommodate equipment and future under-grounding of the overhead lines.
- x) Locate all above ground utility cabinets and kiosks required to service the proposed development within the developments site (see list below for examples). A site plan showing conceptual locations for such infrastructure shall be included in the development process design review. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the right of way requirements and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of SRWs that shall be shown in the functional plan and registered prior to SA design approval:
 - BC Hydro Vista - Confirm SRW dimensions with BC Hydro
 - BC Hydro PMT – Approximately 4mW X 5m (deep) – Confirm SRW dimensions with BC Hydro
 - BC Hydro LPT – Approximately 3.5mW X 3.5m (deep) – Confirm SRW dimensions with BC Hydro
 - Street light kiosk – Approximately 2mW X 1.5m (deep)
 - Traffic signal controller cabinet – Approximately 3.2mW X 1.8m (deep)
 - Traffic signal UPS cabinet – Approximately 1.8mW X 2.2m (deep)
 - Shaw cable kiosk – Approximately 1mW X 1m (deep) – show possible location in functional plan. Confirm SRW dimensions with Shaw
 - Telus FDH cabinet - Approximately 1.1mW X 1m (deep) – show possible location in functional plan. Confirm SRW dimensions with Telus
- xi) Provide arborist assessment of the existing trees (e.g., City and privately owned) along Smith Crescent that may be impacted by the potential road raising, underground utility installations and road widening. (Note: Review Trees # 39 and 53 for retention and/or relocation opportunities.)
- xii) If required, coordinate with property owners and address the impact of the road widening to the existing single family properties along Smith Crescent. The developer shall coordinate with the owner(s) of the affected properties the extent of works required in private properties. The developer shall get written consent or permission to work in private property from the owner(s) of the affected lots. Coordination works shall be at the developer's cost and may include but not be limited to the following:
 - Host community meetings and provide written notices to the individual property owners.

- Provide design/drawings showing the required works inside each property affected by the road widening that may include but not limited to the following:
- Removal and reinstatement of existing driveways that may require construction of a retaining wall on each side of the reinstated driveways on private property.
- Landscaping repairs and / or replacement.
- Community notices and design drawings shall be reviewed and approved by City staff prior to sending to the affected properties.
- Provide to the City copies of design drawings for each lot (affected by the road widening) signed by the lot owner indicating their acceptance to complete the proposed works. Sign off by the owners of the affected properties is required prior to Servicing Agreement design approval.

General Items:

a. At the developers cost, the Developer is required to:

- ii) Provide, within the first SA submission, a geotechnical assessment of preload and soil preparation impacts on the existing utilities (e.g., AC watermain at Smith Crescent etc.) fronting or within the development site and provide mitigation recommendations.
- iii) Coordinate the SA design for this development with the servicing agreement(s) for adjacent development(s), both existing and in-stream. The developer's civil engineer shall submit a signed and sealed letter with each servicing agreement submission confirming that they have coordinated with civil engineer(s) of the adjacent project(s) and that the servicing agreement designs are consistent. The City will not accept the first submission if it is not coordinated with the adjacent developments. The coordination letter should cover, but not be limited to, the following:
 - Corridors for City utilities (existing and proposed water, storm sewer, and sanitary) and private utilities.
 - Pipe sizes, material and slopes.
 - Location of manholes and fire hydrants.
 - Road grades, high points and low points.
 - Alignment of ultimate and interim curbs.
 - Proposed street lights design.
- iv) Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

Prior to a Development Permit* being forwarded to the Development Permit Panel for consideration, the developer is required to:

1. Submission of a Landscape Plan and cost estimate, prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development. The Landscape Plan should:
 - comply with the guidelines of the OCP and Hamilton Area Plan;
 - include a mix of coniferous and deciduous trees;
 - include the dimensions of tree protection fencing as illustrated on the Tree Retention Plan attached to this report;and

- include the 41 proposed replacement trees with the following minimum sizes:

No. of Replacement Trees	Minimum Caliper of Deciduous Tree	or	Minimum Height of Coniferous Tree
41	8 cm		4 m

If required replacement trees cannot be accommodated on-site, a cash-in-lieu contribution in the amount of \$750/tree to the City’s Tree Compensation Fund for off-site planting is required.

- Incorporation of the acoustical report recommendations into the Development Permit drawings, to ensure maximum interior noise levels (decibels) within the dwelling units achieve CMHC standards follows:

Portions of Dwelling Units	Noise Levels (decibels)
Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

- Complete a proposed townhouse energy efficiency report and recommendations prepared by a Certified Energy Advisor which demonstrates how the proposed construction will meet or exceed the required townhouse energy efficiency standards in compliance with Richmond Building Regulation Bylaw 7230.
- Apply to the Ministry of Transportation and Infrastructure (MOTI) for a setback permit for any buildings located within the minimum 4.5 m setback to a provincial highway. If MOTI declines to issue/approve a setback permit, the buildings must be redesigned to comply with the minimum setback requirement.
- Incorporate the agreed upon TDM measures into the Development Permit drawings, including:
 - Class 1 bicycle parking at a rate of 2 spaces per dwelling unit;
 - Class 2 bicycle parking at a rate of 0.35 spaces per dwelling unit; and
 - Design of the two pedestrian pathways along the south side of the site and behind the site as detailed in the Servicing Agreement requirements.

Prior to Development Permit* issuance, the development must complete the following requirements:

- Submission of a Landscape Security to the City based on 100% of the cost estimate provided by the Landscape Architect plus a 10% contingency. Up to 90% of the security will be returned after a City inspection, with the remainder held for one year to ensure that the agreed upon landscaping survives.

Prior to Building Permit* issuance, the developer must complete the following requirements:

- Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
- Incorporation of accessibility, sustainability, and noise mitigation measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
- Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed

Date



Richmond Zoning Bylaw 8500
Amendment Bylaw 9877 (RZ 18-802860)
4400, 4420, and 4440 Smith Crescent

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

- 1. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation of the following area and by designating it "HIGH DENSITY TOWNHOUSES (RTH1)".

P.I.D. 001-094-068

Lot 105 Section 36 Block 5 North Range 4 West New Westminster District Plan 38115

P.I.D. 007-554-630

North 70 Feet Lot 54 Section 36 Block 5 North Range 4 West New Westminster District Plan 8421

P.I.D. 007-555-903

Lot 54 Except: the North 70 Feet; Section 36 Block 5 North Range 4 West New Westminster District Plan 8421

- 2. This Bylaw may be cited as "Richmond Zoning Bylaw 8500, Amendment Bylaw 9877".

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

OTHER CONDITIONS SATISFIED

MINISTRY OF TRANSPORTATION AND INFRASTRUCTURE APPROVAL

ADOPTED

Series of horizontal lines for recording dates and signatures.



MAYOR

CORPORATE OFFICER



City of Richmond

Report to Committee

To: Planning Committee

Date: February 14, 2022

From: Wayne Craig
Director, Development

File: RZ 19-870807

Re: Application by Terra Spires Development LP for Rezoning at 8740, 8760, 8780 and 8800 Spires Road, and the Surplus Portion of the Spires Road road allowance from Single Detached (RS1/E) to Parking Structure Townhomes (RTP4)

Staff Recommendation

That Richmond Zoning Bylaw 8500, Amendment Bylaw 10357, for the rezoning of 8740, 8760, 8780 and 8800 Spires Road, and the surplus portion of the Spires Road road allowance from "Single Detached (RS1/E)" to "Parking Structure Townhomes (RTP4)", be introduced and given first reading.

Wayne Craig
Director, Development

WC:el
Att. 8

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Real Estate Services	<input checked="" type="checkbox"/>	
Affordable Housing	<input checked="" type="checkbox"/>	
Engineering	<input checked="" type="checkbox"/>	
Policy Planning	<input checked="" type="checkbox"/>	
Transportation	<input checked="" type="checkbox"/>	

Staff Report

Origin

Terra Spires Development LP, on the behalf of 1219002 BC Ltd. (Incorporation number: BC1219002; Directors: Kai-Shen (John) Hsiung and Yi-Jen (Claire) Wang), has applied to the City of Richmond for permission to rezone 8740, 8760, 8780 and 8800 Spires Road, and the surplus portion of the Spires Road road allowance (Attachment 1) from the “Single Detached (RS1/E)” zone to the “Parking Structure Townhouses (RTP4)” zone in order to permit the development of 36 townhouse units and three secondary suites with a common parking structure accesses from Spires Road. A preliminary site plan, building elevations, and landscape plan are contained in Attachment 2.

A Servicing Agreement will be required for this development to design and construct frontage beautification along the site frontages (including ditch infill), road widening, City Centre standard new concrete sidewalk and landscaped boulevard, new fire hydrants, public walkways on-site, upgrades to the storm sewer and sanitary sewer, as well as service connections.

Findings of Fact

A Development Application Data Sheet providing details about the development proposal is attached (Attachment 3).

Subject Site Existing Housing Profile

The site currently contains four single-family homes, which will be demolished. The applicant has advised that all of the four houses on-site are rented out; and there are no suites in the houses.

Surrounding Development

The Spires Road Neighbourhood is identified in the City Centre Area Plan (CCAP) as an area intended to transition from a predominately single-family neighbourhood toward a higher density neighbourhood through the development of townhouse buildings over parking structures.

- To the North: Across Spires Road, single-family homes on lots zoned “Single Detached (RS1/E)” and a recently approved 22 unit high density townhouse development (RZ 17-790301 & DP 19-875398) on a lot zoned “Parking Structure Townhouses (RTP4)”. This townhouse development at 8699 Spires Road is currently under construction.
- To the South: A 12-unit townhouse development on a lot zoned Low Density Townhouses (RTL1) at 8551/8571 Cook Road, and two vacant lots at 8591 and 8611 Cook Road. A rezoning application to develop 39 townhouse units at 8591 and 8611 Cook Road (RZ 17-769352) has been received and design development is required prior to this project being forwarded to Planning Committee for consideration.
- To the East: Across Cook Gate, single-family homes on lots zoned “Single Detached (RS1/E)”, which are designated for high density townhouses under the City Centre Area Plan.

- To the West: A recently approved 64 unit high density townhouse development (RZ 17-766525 & DP 18-829140) on a lot zoned “Parking Structure Townhouses (RTP4)”. This townhouse development at 8888 Spires Road is currently under construction.

Related Policies & Studies

Official Community Plan/City Centre Area Plan

The 2041 Official Community Plan (OCP) Land Use Map designation for the subject development site is “Neighbourhood Residential”. This redevelopment proposal is consistent with this designation.

The subject development site is located within the Brighthouse Village of the City Centre Area Plan (CCAP), Schedule 2.10 of the Official Community Plan (OCP) Bylaw No. 7100 (Attachment 4). The site is in “Sub-Area B.1: Mixed Use – Low-Rise Residential & Limited Commercial”, which is intended for grade-oriented housing in the form of higher-density townhouses (built over common parking structures) in areas north of Granville Avenue within the city centre. The preliminary design of the proposal featuring high density townhouses over a common parking structure generally complies with the CCAP Guidelines in terms of land use, density, and overall neighbourhood character. Further consideration of the Development Guidelines and form and character will take place at the Development Permit stage of the process.

The subject development site is surrounded by properties with development potential subject to the CCAP. Registration of a legal agreement on title is required before final adoption of the rezoning bylaw, stipulating that the residential development is subject to potential impacts due to other development that may be approved within the City Centre and requiring this information be provided through signage in the sales centre and through the disclosure statement to all initial purchasers.

The proposed rezoning is subject to a community planning implementation strategy contribution for future community planning initiatives. The applicant proposes to make a cash contribution at the current rate of \$0.31 per buildable square foot (2021-2023 rate), for a total contribution of \$13,052.36 prior to final adoption of the rezoning bylaw.

Council Referral

Planning Committee passed a referral motion on October 20, 2021 to direct staff to consider density bonusing in the Spires Road Area to facilitate new multiple family developments that include a mix of strata housing, market rental and income controlled rental. Staff is undertaking an economic analysis of density bonusing and inclusionary zoning to determine the implications of the requirements for market rental and income controlled rental units, as well as development viability. A public consultation process is also underway to seek inputs from the area owners and residents regarding their concerns and expectations for future developments within the neighbourhood. Staff are working towards bringing forward a staff report in response to the referral in May 2022.

When the referral motion was discussed at Planning Committee, there was recognition that there were in-stream rezoning applications under review in the area and applications consistent with the current OCP and CCAP designations would be brought forward to Council for consideration. The applicant is aware of the ongoing planning study for the area and the potential that additional density may be considered if rental units are included in their proposal. Given the advanced stage of design for the subject proposal, the developers has elected to proceed in accordance with the current OCP and CCAP designation. As the subject application is not requesting a density bonus, and is consistent with the current OCP and CCAP, this application may be considered by Council in advance of this referral.

OCP Aircraft Noise Sensitive Development (ANSD) Policy

The subject development site is located within Area 4 (Aircraft Noise Notification Area) on the OCP Aircraft Noise Sensitive Development Map. While all aircraft noise sensitive land uses (including residential uses) maybe considered, registration of an Aircraft Noise Sensitive Use Covenant on title to address aircraft noise mitigation and public awareness is required prior to final adoption of the rezoning bylaw. At the Development Permit stage, submission of an acoustic report, prepared by a qualified professional, is required to address indoor sound level mitigation criteria as set out in the OCP and identify how noise mitigation measures will be incorporated into the building design.

Energy Step Code

The developer has committed to design the subject development to meet the City's Step Code requirements. Under current requirements, the development would be expected to achieve Step 3 of the Energy Step Code for Part 3 construction. A commitment letter is presented as Attachment 5. Details on how all units are to be built and maintained to this commitment will be reviewed at Building Permit stage.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on title is required prior to final adoption of the rezoning bylaw.

Affordable Housing Strategy

In addition to the provision of three secondary suites on site, the applicant proposes to make a cash contribution to the Affordable Housing Reserve Fund in accordance with the City's Affordable Housing Strategy. As per the Strategy, townhouse rezoning applications received prior to November 15, 2021 are required to provide a cash-in-lieu contribution of \$8.50 per buildable square foot. Consistent with this direction, a contribution of \$357,887.40 is required prior to final adoption of the rezoning bylaw.

Public Art Program Policy

In response to the City's Public Art Program (Policy 8703), the applicant will provide a voluntary contribution at a rate of \$0.87 per buildable square foot (2019 rate) to the City's Public Art Reserve fund; for a total contribution in the amount of \$36,630.83.

Public Consultation

Two rezoning signs have been installed on the subject property (one on each road frontage). Staff have not received any comments from the public about the rezoning application in response to the placement of the rezoning sign on the property.

Should the Planning Committee endorse this application and Council grant 1st reading to the rezoning bylaw, the bylaw will be forwarded to a Public Hearing, where any area resident or interested party will have an opportunity to comment.

Public notification for the Public Hearing will be provided as per the *Local Government Act*.

Analysis

Road Closure

A new narrower 16.0 m wide road cross-section for the Spires Road Neighbourhood has been established for the area to better support the development envisioned for this area in the City Centre Area Plan. This new road cross-section has already been applied to three high density townhouse development projects along Spires Road since 2019.

Based on the new road cross-section and the preliminary functional road design reviewed and accepted by Engineering and Transportation Departments, 2.05 m of the existing Spires Road road allowance adjacent to the frontage of the subject development site has been identified for road closure (Attachment 6). The area, which is approx. 170.8 m² (1,834.5 ft²), is surplus to Engineering and Transportation needs.

Prior to rezoning bylaw adoption, the applicant is required to enter into a purchase and sales agreement with the City for the purchase of the lands, which is to be based on the business terms approved by Council. The primary business terms of the purchase and sales agreement will be brought forward to Council with the road closure bylaw, in a separate report from the Director, Real Estate Services.

Road Dedication

An approximately 0.865 m wide road dedication along the site's Cook Gate frontage is required to accommodate frontage improvement works. The developer is also required to provide an approximately 1.219 m wide lane dedication along the south property line of 8740 Spires Road to accommodate the future east-west lane parallel to Cook Road. In addition, a 4.0 m x 4.0 m corner cut road dedication and a 3.0 m x 3.0 m corner cut road dedication are required at the northeast and southeast corners of the subject site, respectively.

Existing Legal Encumbrances

There is an existing tri-party utility right of way (City of Richmond, BC Hydro and Telus) along the common property line of 8760 and 8780 Spires Road for the provision of utilities and services. The right of way contains a sanitary gravity line that conveys flows from the northern areas of the Spires neighborhood. In order to accommodate the proposed development, the existing sanitary line must be decommissioned and the utility right of way must be discharged.

Engineering Department will support the decommissioning of the existing sanitary line and discharge of this utility right of way after the existing sanitary main that conveys flows from the north is connected to the new sanitary mains at Spires Road and Cook Gate, where these sanitary mains are connected to the new Eckersley B sanitary pump station. The developers will be responsible for these works and the scope of works will be included in the Servicing Agreement.

The developers understand that no site preparation works (e.g., preload, soil densification, etc.) shall be commenced until the new sanitary lines in Spires Road and Cook Gate are operational and connected to the new Eckersley B pump station.

The developer is also responsible to coordinate with BC Hydro and Telus to remove all utility infrastructures within the right of way and to negotiate for the discharge of the right of way.

Tree Retention and Replacement

The applicant has submitted a Certified Arborist's Report; which identifies on-site and off-site tree species, assesses tree structure and condition, and provides recommendations on tree retention and removal relative to the proposed development. The Report assesses 22 bylaw-sized trees on the subject property, two trees on neighbouring properties, and eight street trees on City property.

The City's Tree Preservation Coordinator and Park Services staff have reviewed the Arborist's Report and supports the Arborist's findings, with the following comments:

On-site Trees:

- A 37 cm caliper English Walnut tree (specifically tag# 300), located on the development site along the Spires Road frontage is in good condition and should be retained and protected a minimum 4 m out from the base of the tree. A \$10,000 survival security is required for this tree to be retained.
- 21 trees (specifically tag# 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 301, 302, 303, 304, 305, 306 and 307) located on site are either dead, dying (sparse canopy foliage), have been previously topped or exhibit structural defects such as cavities at the main branch union and co-dominant stems with inclusions. As a result, these trees are not good candidates for retention and should be replaced.

City Trees:

- An 81 cm caliper Sawara Cypress tree and an 80 cm caliper Sawara Cypress tree (specifically tag# C03 & C04), located along the Spires Road frontage of the site, are in good/fair health and condition and should be retained and protected. A \$20,000 survival security is required for these two City trees to be retained.
- An 8 cm caliper Japanese Maple Tree and an Eastern White Cedar hedge (specifically tag# C07 & C08), located along the north side of Spires Road, are to be retained and protected. Ditch in-fill work will need to be supervised by project Arborist.
- Four trees (specifically tag# C01, C02, C05, C06) and untagged hedges, located along Spires Road frontage of the site, are in poor health and condition. They are also in conflict with the proposed frontage improvement works. A \$7,000 tree compensation is required for the removal of these City trees.

Off-site Trees:

- An 80 cm caliper Douglas Fir tree (specifically tag# N01) located on the neighbouring property to the south at 8611 Cook Road and a 40 cm caliper Norway Spruce tree (specifically tag# N02) located on the property across the street at 8751 Spires Road, are identified to be retained and protected. Provide tree protection as per City of Richmond Tree Protection Information Bulletin Tree-03.
- Replacement trees should be specified at 2:1 ratio as per the OCP.

Tree Replacement

The applicant wishes to remove 21 on-site trees; a Tree Management Plan is included in this report (Attachment 7). The 2:1 replacement ratio would require a total of 42 replacement trees for the removal of 21 trees. According to the Preliminary Landscape Plan provided by the applicant (Attachment 2), the applicant proposes to plant 19 new trees on-site and provide cash-in-lieu for the remaining trees. Staff will work with the applicant to explore the opportunity to include additional replacement trees on site at the Development Permit stage. The size and species of replacement trees will also be reviewed in detail through Development Permit and overall landscape design. The applicant has agreed to provide a voluntary contribution of \$17,250 (\$750/tree) to the City's Tree Compensation Fund in lieu of planting the remaining 23 replacement trees should they not be accommodated on the site.

Tree Protection

A 37 cm caliper English Walnut tree (specifically tag# 300), four city trees (specifically tag# C03, C04, C07 & C08) and two trees on neighbouring properties (specifically tag# N01 & N02) are to be retained and protected. The applicant has submitted a tree protection plan showing the trees to be retained and the measures taken to protect them during development stage (Attachment 7). To ensure that the trees identified for retention are protected at development stage, the applicant is required to complete the following items:

- Prior to final adoption of the rezoning bylaw, submission to the City of a contract with a Certified Arborist for the supervision of all works conducted within or in close proximity to tree protection zones. The contract must include the scope of work required, the number of proposed monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the arborist to submit a post-construction impact assessment to the City for review.
- Prior to final adoption of the rezoning bylaw, submission to the City of a Tree Survival Security in the amount of \$10,000 to ensure that the 37 cm caliper English Walnut tree (specifically tag# 300), identified for retention, will be protected. No Tree Survival Security will be returned until the post-construction assessment report, confirming the protected trees survived the construction, prepared by the Arborist, is reviewed by staff.
- Prior to demolition of the existing dwelling on the subject site, installation of tree protection fencing around all trees to be retained. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and remain in place until construction and landscaping on-site is completed.

Built Form and Architectural Character

The applicant proposes to consolidate the four properties and the surplus road frontage of these properties into one development parcel, with a total net site area of 3,260 m² (35,087 ft²). The proposal is to build a high density, ground-oriented, four-storey townhouse project on the consolidated lot at a density of 1.2 floor area ratio (FAR).

The development will contain 36 units including:

- three single-level units and three Basic Universal Housing (BUH) units;
- 27 three-level units on the podium; all of these units will have their main unit entry located on the podium level; and
- three four-level units each with a secondary suite.

Dwelling sizes are ranging from 64 m² (698 ft²) to 160 m² (1,724 ft²). All of the units will have private outdoor areas at grade, on the elevated podium overtop the parking structure, and/or on the top floor oriented towards the internal courtyard. All single-level units, BUH units, and secondary suites will have street level entry with direct pedestrian access to Spires Road or Cook Gate. All single-level units and BUH units will also have direct access to the parking area.

The sizes of the proposed secondary suites (studios) range from approximately 33.7 m² (363 ft²) to 36.0 m² (388 ft²). No additional parking stall is required for the proposed secondary units since this site is not located on an arterial road. To ensure that the secondary suite will not be stratified or otherwise held under separate title, registration of a legal agreement on title is required prior to final adoption of the rezoning bylaw. To ensure that the secondary suite is built, registration of a legal agreement on title, stating that no Building Permit inspection granting occupancy will be completed until the secondary suites are constructed to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw, is required prior to final adoption of the rezoning bylaw.

Amenity Space

The conceptual development plans include 70 m² (753 ft²) of indoor amenity, which meets the minimum requirements in the Official Community Plan (OCP).

Outdoor amenity spaces will also be provided on-site. Based on the preliminary design, the total area of the proposed outdoor amenity spaces at 569 m² (6,126 ft²), which exceeds the minimum requirements under the Official Community Plan (OCP). Staff will work with the applicant at the Development Permit stage to ensure the configurations and designs of the outdoor amenity spaces meet the Development Permit Guidelines in the OCP.

Transportation and Site Access

Vehicle access to the development will be from a new entry driveway off Spires Road, providing access to the parking structure proposed on-site. One loading area is proposed on-site at the entry driveway.

To enhance pedestrian circulation within the Spires Road Neighbourhood, the following walkways will be secured prior to final adoption:

- A 1.5 m wide statutory right-of-way (SRW) along the entire west property line to provide pedestrian connection between Spires Road and the future back lane. Together with a similar 1.5 m wide SRW registered on the neighbouring site to the west, the Developer is required to construct an asphalt concrete walkway over the total 3.0 m wide SRW. The cross-section of the walkway is to consist of a 1.5 m wide asphalt concrete surface with a 0.75 m wide swale for drainage along both edges of the walkway.
- A 1.5 m wide SRW along the entire south property line for pedestrian circulation along the future back lane. The Developer is required to build a new 1.5 m wide concrete sidewalk across the entire south property line of the subject site.

Vehicle and Bicycle Parking On-site

The proposal will feature 36 units with a total of 54 resident parking spaces and eight visitor parking spaces, which exceed the minimum bylaw requirements for this neighbourhood. Prior to final adoption, a restrictive covenant is required to be registered on title to ensure that:

- the number of visitor parking stalls per zoning bylaw requirements will be maintained in perpetuity;
- selling, leasing, assigning, or designating any of the visitor parking spaces to individual unit owners/renters/occupants or any other persons by the developers/applicants/owners and future strata councils is prohibited; and
- the required visitor parking stalls are available for the common use of visitors to this development and are accessible to visitors at all times.

The proposal will feature 18 resident parking spaces in a tandem arrangement (41% of total required residential parking spaces provided), which is consistent with the maximum 50% of tandem parking provision of Richmond Zoning Bylaw 8500. Prior to final adoption, the applicants are required to enter into a number of legal agreements to ensure that:

- where two parking spaces are provided in a tandem arrangement, both parking spaces must be assigned to the same dwelling unit; and
- conversion of any of the tandem parking areas into habitable space is prohibited.

In addition, the proposal will feature three Basic Universal Housing units; an accessible parking stall will be provided for each of these units. A restrictive covenant to reflect this arrangement is required prior to final adoption.

Furthermore, the proposal will feature a total of 45 Class-1 and eight Class-2 (visitor) bicycle parking spaces on-site, which meets the bylaw requirements. All visitor bicycle parking spaces will be provided by the lobby area and within the outdoor amenity space at grade. All residential bicycle parking spaces will be provided within bike storage rooms located within the parking structure. Prior to final adoption, a restrictive covenant is required to be registered on title to ensure that:

- conversion of the proposed bike storage rooms in this development into habitable space or general storage area is prohibited; and
- the bike storage room must remain available for shared common use and for the sole purpose of bicycle storage.

Site Servicing and Frontage Improvements

Prior to final adoption of the rezoning bylaw, the applicant is required to:

- Provide a cash-in-lieu contribution in the amount of \$156,936.00 for the construction of a new 600mm storm sewer via the capital project works that will front the development. This is the cost required for the construction of the storm main fronting the development's property (approximately 79 m) and is a portion of the total cost of the system from the bend at Spires Road to Cook Gate (i.e., 189 m).
- Enter into the City's Standard Servicing Agreement to design and construct frontage beautification along the site frontages (including ditch infill), road widening, City Centre standard new concrete sidewalk and landscaped boulevard, new fire hydrants, public walkways on-site, upgrades to the storm sewer and sanitary sewer, as well as service connections (see Attachment 8 for details). All works are at the client's sole cost (i.e., no credits apply).

The applicant is also required to pay Development Cost Charges (DCC's) (City & GVS & DD), School Site Acquisition Charge and Address Assignment Fee at Building Permit stage.

Design Review and Future Development Permit Considerations

A Development Permit processed to a satisfactory level is a requirement of zoning approval. Through the Development Permit, the following issues are to be further examined:

- Compliance with Development Permit Guidelines for multiple family projects in the 2041 Official Community Plan and the City Centre Area Plan.
- Refinement of the site plan to ensure all the aboveground private utility infrastructure improvements required as part of this development will be located on site and screened from street view.
- Refinement of the site plan and tree management scheme to ensure protection of retained trees; design review is required to confirm:
 - the proposed Working Space Setback between the root protection zone and the proposed building foundation is adequate for required infrastructure (i.e. drainage, frost protection, grading, etc.) without resulting in encroachment within the root protection zone; and
 - excavation within the Working Space Setback for the new building foundation is acceptable, subject to low impact measures during construction.

Notes: arborist to confirm tree protection measures can be accommodated during construction; encroachment within the root protection zone for site preparation, installation of drainage, frost protection, fill or other disturbances will not be supported.

- Refinement of the proposed building elevations that will be visible from the fronting streets and future lane to provide additional articulations and design consistence; detailed review of façade materials and colors.
- Refinement of the proposed site plan and site grading to ensure appropriate transition between the proposed development and adjacent existing developments.
- Refinement of the tree replacement scheme to provide additional replacement trees on-site.
- Refinement of landscape design, including the size and configuration of the outdoor amenity spaces and choice of play equipment, to create a safe and vibrant environment for children's play and social interaction.
- Review of aging-in-place features in all units and the provision of Basic Universal Housing units.
- Review of the sustainability strategy for the development proposal.

Additional issues may be identified as part of the Development Permit application review process.

Financial Impact or Economic Impact

As a result of the proposed development, the City will take ownership of developer contributed assets such as roadworks, waterworks, storm sewers, sanitary sewers, street lights, street trees and traffic signals. The anticipated operating budget impact for the ongoing maintenance of these assets is \$5,000.00. This will be considered as part of the 2022 Operating budget.

To facilitate the narrowing of Spires Road and Cook Crescent as well as the subject rezoning application proposal, the applicant proposes to purchase a portion of the Spires Road road allowance for inclusion in the applicant's development site. The total approximate area of City lands proposed to be sold and included in the development site is 170.8 m² (1,834.5 ft²). As identified in the attached rezoning considerations (Attachment 8), the applicants are required to enter into a purchase and sales agreement with the City for the purchase of the lands, which is to be based on the business terms subject to Council approval.

Conclusion

The proposed 36-unit townhouse development is consistent with the Official Community Plan (OCP) and the City Centre Area Plan (CCAP). Further review of the project design is required to ensure a high quality project and design consistency with the existing neighbourhood context, and this will be completed as part of the Development Permit application review process. The list of rezoning considerations is included as Attachment 8; which has been agreed to by the applicant (signed concurrence on file). On this basis, staff recommend support of the application.

It is recommended that Richmond Zoning Bylaw 8500, Amendment Bylaw 10357 be introduced and given first reading.



Edwin Lee
Planner 2

EL:cas

- Attachment 1: Location Map
- Attachment 2: Conceptual Development Plans
- Attachment 3: Development Application Data Sheet
- Attachment 4: Specific Land Use Map: Brighthouse Village (2031)
- Attachment 5: Letter from Developer regarding Step Code Requirements
- Attachment 6: Proposed Road Closure Plan
- Attachment 7: Tree Management Plan
- Attachment 8: Rezoning Considerations



Richmond Zoning Bylaw 8500
Amendment Bylaw 10357 (RZ 19-870807)
8740, 8760, 8780, 8800 Spires Road, and the Surplus Portion of the Spires Road Road Allowance

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

- 1. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation of the following area and by designating it "PARKING STRUCTURE TOWNHOUSES (RTP4)".

P.I.D. 101-472-801
Lot 60 Section 9 Block 10 North Range 4 West New Westminster District Plan 21489

P.I.D. 101-472-819
Lot 61 Section 9 Block 10 North Range 4 West New Westminster District Plan 21489

P.I.D. 003-690-962
Lot 62 Section 9 Block 10 North Range 4 West New Westminster District Plan 21489

P.I.D. 101-472-827
Lot 63 Section 9 Block 10 North Range 4 West New Westminster District Plan 21489

and a closed portion of Spire Road dedicated by Plan 21489, Sections 9 and 10 , Block 4 North Range 6 West New Westminster District as shown in Reference Plan EPP 115232.

- 2. This Bylaw may be cited as "Richmond Zoning Bylaw 8500, Amendment Bylaw 10357".

FIRST READING
A PUBLIC HEARING WAS HELD ON
SECOND READING
THIRD READING
OTHER CONDITIONS SATISFIED
ADOPTED

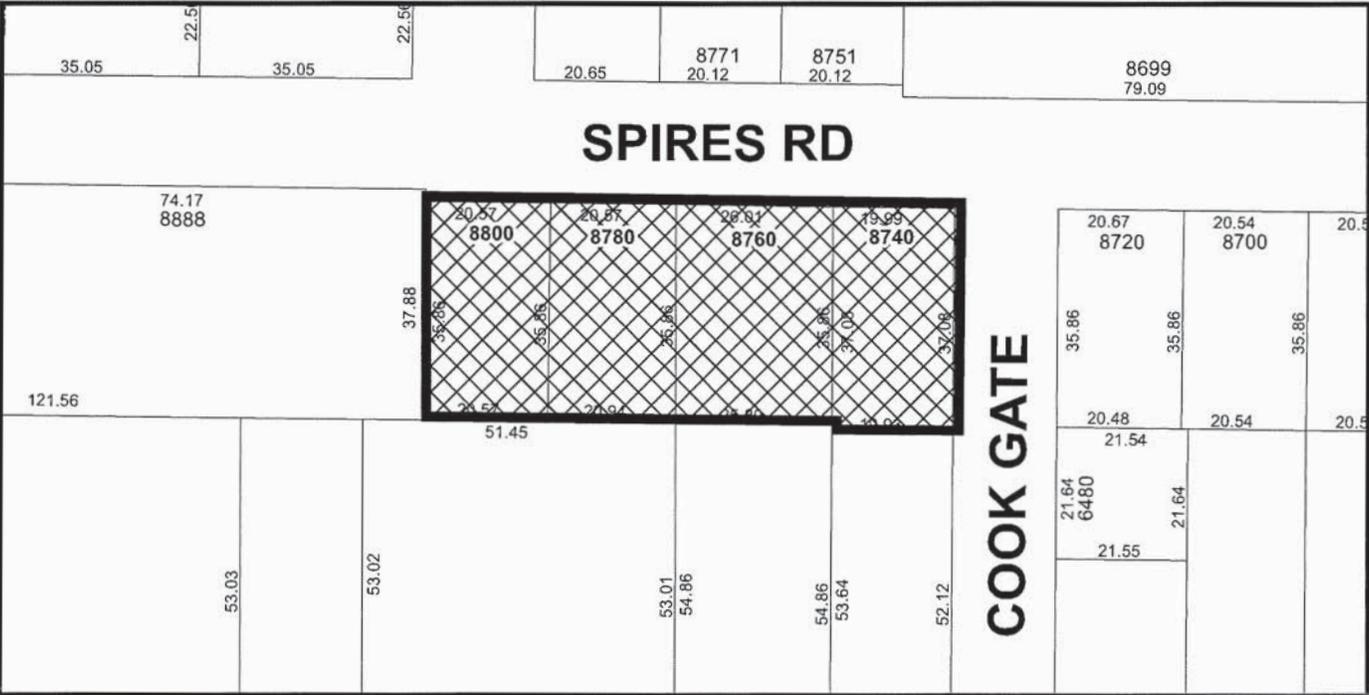
CITY OF RICHMOND
APPROVED by
E.L.
APPROVED by Director or Solicitor

MAYOR

CORPORATE OFFICER



City of
Richmond



	<p>RZ 19-870807</p>	<p>Original Date: 09/10/19 Revision Date: 02/07/22 Note: Dimensions are in METRES</p>
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City of
Richmond



RZ 19-870708

Original Date: 09/11/19

Revision Date: 02/07/22

Note: Dimensions are in METRES

ATTACHMENT 2

This plan and design one and all three...
 Architectural Inc. and cannot be used or...
 Formwerks Architectural Inc. without...
 dimensions shall have precedence over...
 conditions on the job. Formwerks...
 variations from the dimensions and...
 conditions on the drawing.

REVISIONS

NOV 11, 2021	ISSUED FOR PERMITTING
NOV 11, 2021	ISSUED FOR PERMITTING
MAR 8, 2021	ISSUED FOR PERMITTING
SEP 24, 2021	ISSUED FOR PERMITTING
NOV 3, 2021	ISSUED FOR PERMITTING
NOV 3, 2021	ISSUED FOR PERMITTING
NOV 25, 2021	ISSUED FOR PERMITTING

TERA

FORMWERKS ARCHITECTURAL

162 West 4th Ave., Vancouver, BC V6C 1G6
 TEL: 604.681.8200 FAX: 604.681.8201

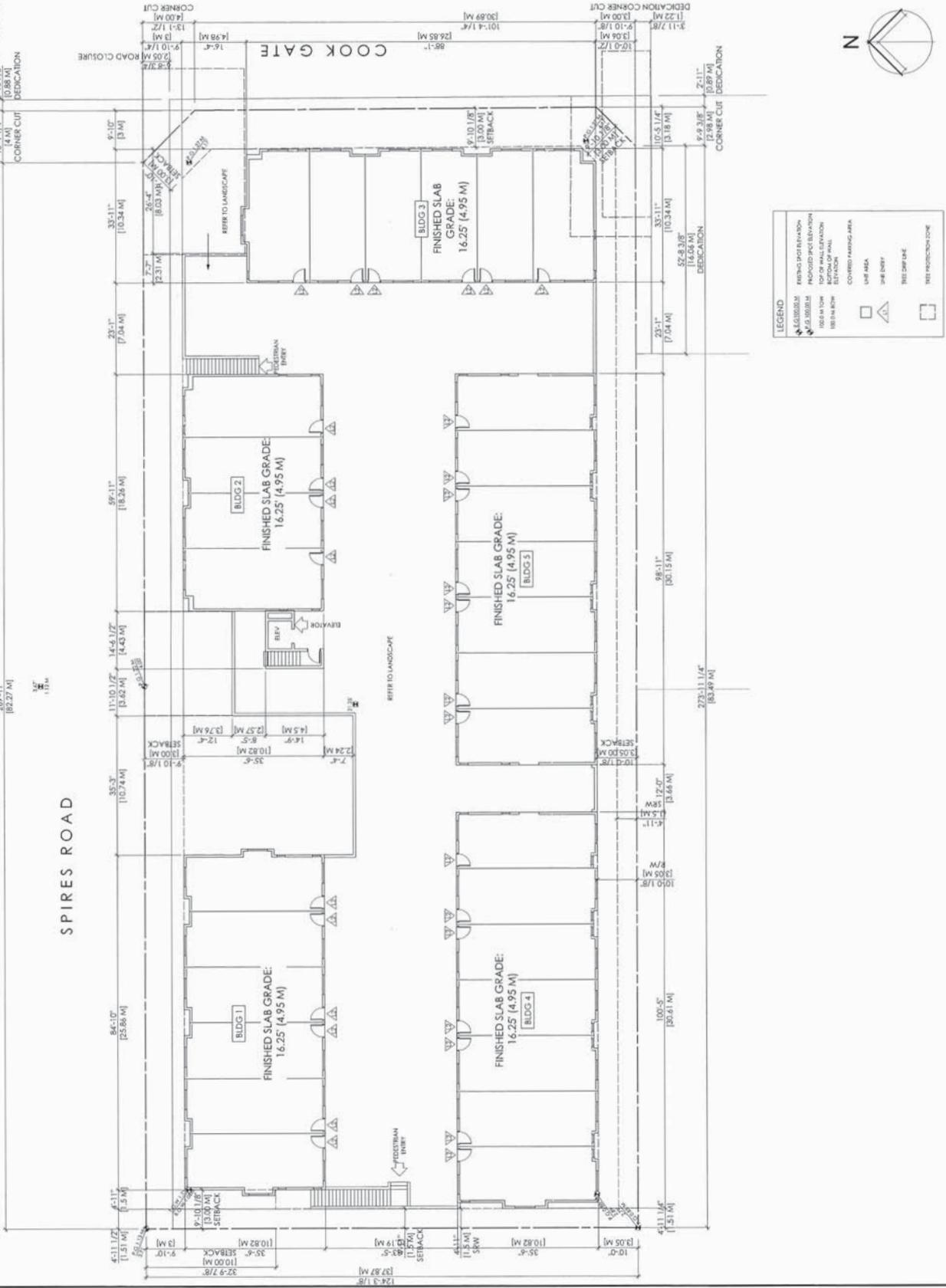
PROJECT: 8800 SPIRES ROAD
 8740-8800 SPIRES ROAD
 RICHMOND, BC

DRAWING: SITE PLAN LEVEL 2

SCALE: 1/32" = 1'-0"

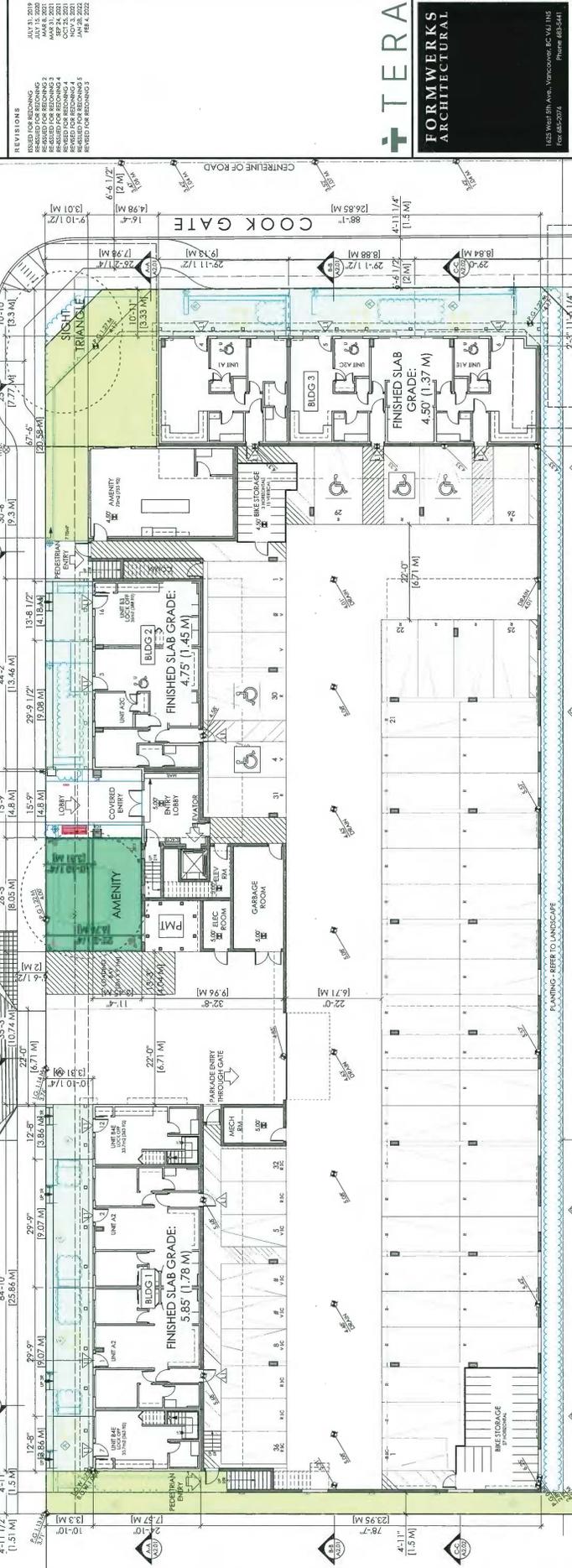
DATE: 2021 JAN 18

SHEET: A1.03



ATTACHMENT 2

SPIRES ROAD
 CLASS 2 BICYCLESTALLS
 ADDRESS MONUMENT
 MANBOXXE IN LOBBY
 SITES
 CENTRELINE OF ROAD
 COOK GATE
 FINISHED SLAB GRADE: 5.95' (1.78 M)
 FINISHED SLAB GRADE: 4.75' (1.45 M)
 FINISHED SLAB GRADE: 4.50' (1.37 M)



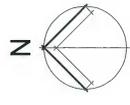
PROJECT
8800 SPIRES ROAD
 8740-8800 SPIRES ROAD
 RICHMOND, BC

DRAWING
GROUND LEVEL PLAN

SCALE
 3/32" = 1'-0"
 DATE
 2022 FEB 4
 SHEET
A1.04

TERA
FORMWERKS ARCHITECTURAL
 1125 West 5th Ave., Vancouver, BC V6J 1N6
 Tel: 604-270-7171 Fax: 604-270-7172
 Phone: 604-541-1125

REVISIONS
 JUL 11, 2021
 JUL 13, 2021
 AUG 31, 2021
 OCT 24, 2021
 NOV 3, 2021
 FEB 4, 2022



LEGEND

CONVERTIBLE UNIT	SMALL CAR PARKING	SMALL CAR PARKING NET TO FENCE OR STRUCTURE	HORIZONTAL BICYCLE STORAGE
ENTRY	SMALL CAR PARKING NET TO FENCE OR STRUCTURE ON NON-EDGE	SMALL CAR PARKING NET TO FENCE OR STRUCTURE ON NON-EDGE	VERTICAL BICYCLE STORAGE
EXISTING FOOT ELEVATION	STANDARD PARKING	PARKING WITH ONE WALL LATERAL CLEARANCE (83 M)	
PROPOSED GRADE	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE	
EXISTING FLOOR ELEVATION	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
PROPOSED FLOOR ELEVATION	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
PRIVATE OUTDOOR AREA	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
AMENITY OUTDOOR AREA	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
CHEST PLAY AREA	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
ADDITIONAL OUTDOOR	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
TRUCK REAR SIDE LOADING BUFFER	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
ROLL-OVER CURB	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	
GRASS CURB	STANDARD PARKING	STANDARD PARKING NET TO FENCE OR STRUCTURE FOR SEES	

ATTACHMENT 2

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REVISIONS	DATE
REVISION FOR RECORDING	JUL 23, 2019
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017
REVISION FOR RECORDING	MAR 8, 2017



FORMWERKS ARCHITECTURAL

162 West 5th Ave., Vancouver, BC V6B 1H3
Tel: 604.681.8888
www.formwerks.com

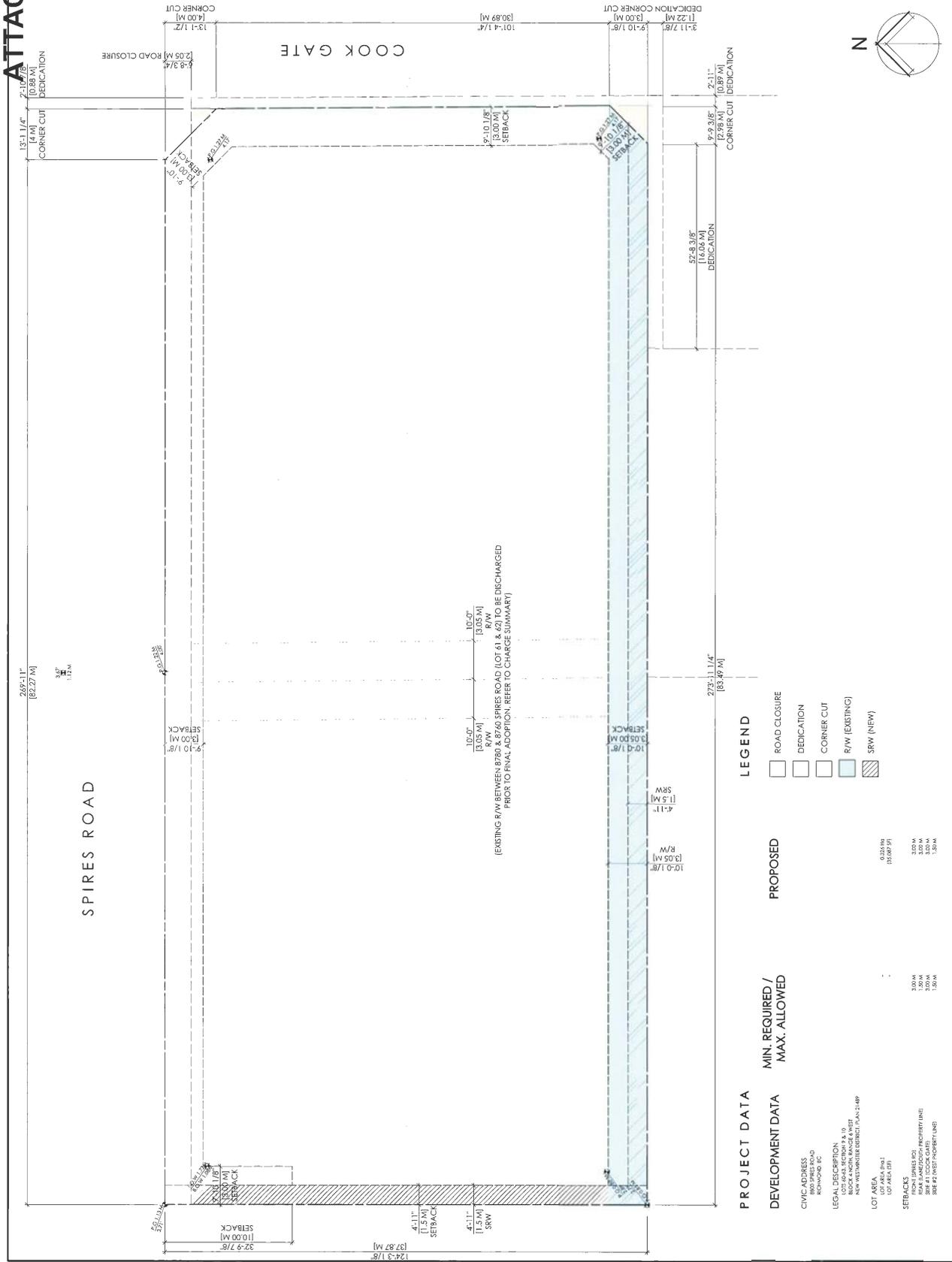
PROJECT

8800 SPIRES ROAD
8740-8800 SPIRES ROAD
RICHMOND, B.C.

DRAWING

SITE AREA PLAN

SCALE	3/32" = 1'-0"
DATE	2022.06.27
SHEET	A1.04C



ATTACHMENT 2

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REVISIONS

REVISED FOR RECORD	JUL 31 2019
REVISED FOR RECORD	MAR 8 2019
REVISED FOR RECORD	MAR 8 2019
REVISED FOR RECORD	APR 24 2011
REVISED FOR RECORD	NOV 3 2011
REVISED FOR RECORD	NOV 3 2011
REVISED FOR RECORD	JAN 28 2012

FORMWERKS ARCHITECTURAL

162 West 59th Ave., Vancouver, BC V6J 1N5
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PROJECT

8800 SPIRES ROAD

8740-8800 SPIRES ROAD
RICHMOND, BC

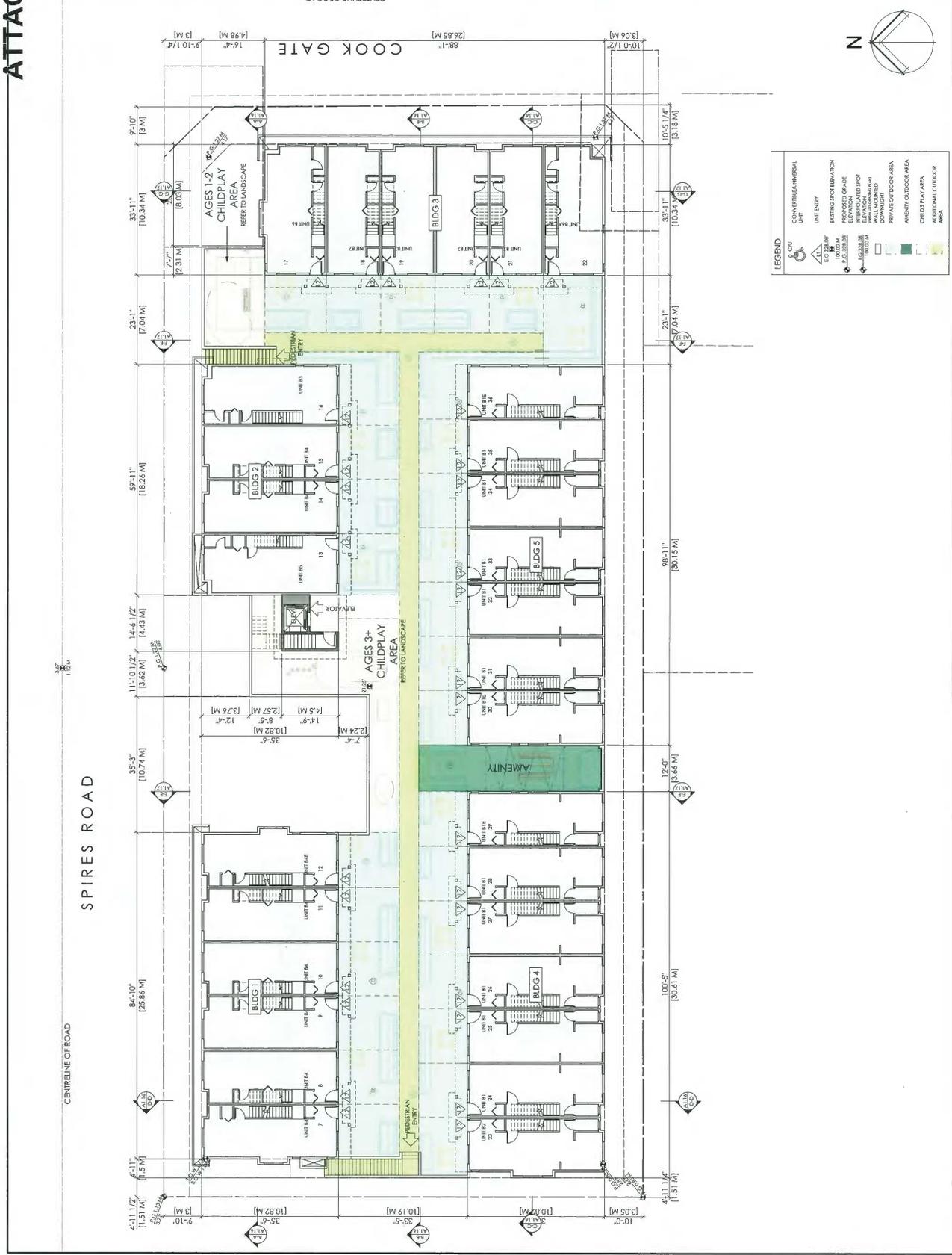
DRAWING

LEVEL 2 PLAN

SCALE 3/32" = 1'-0"

DATE 2022 JUN 28

SHEET **A1.05**



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REVISIONS

REVISED FOR SETTING	AUG 11 2020
REVISED FOR SETTING	AUG 11 2020
REVISED FOR SETTING	AUG 11 2020
REVISED FOR SETTING	AUG 11 2020
REVISED FOR SETTING	AUG 11 2020
REVISED FOR SETTING	AUG 11 2020
REVISED FOR SETTING	AUG 11 2020
REVISED FOR SETTING	AUG 11 2020

FORMWERKS ARCHITECTURAL

1425 West 9th Ave., Vancouver, BC V6H 1Y6
Tel: 604.278.2222

PROJECT

8800 SPIRES ROAD

8740-8800 SPIRES ROAD
RICHMOND, B.C.

DRAWING

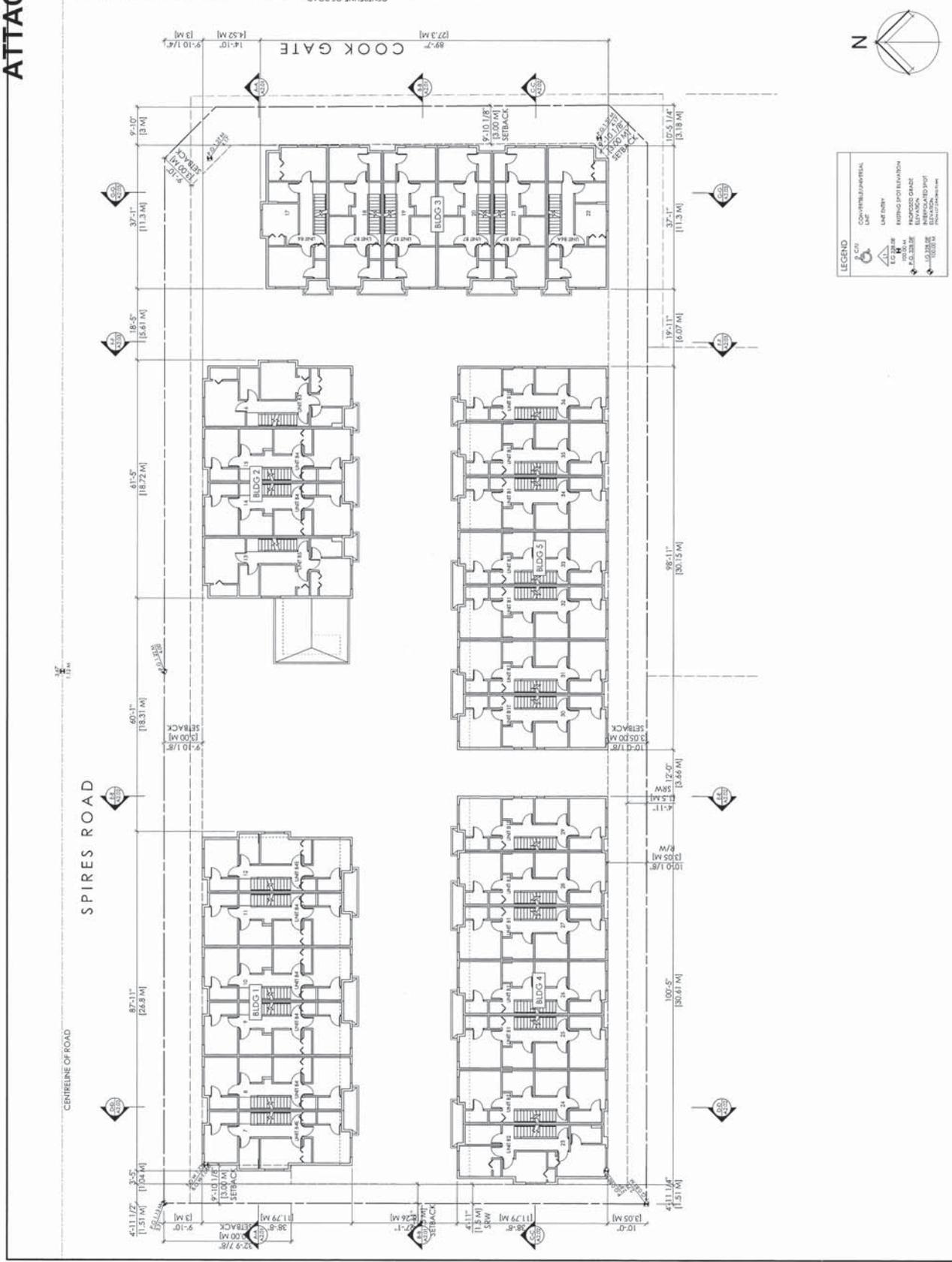
LEVEL 3 PLAN

SHEET

SCALE: 3/32" = 1'-0"

DATE: 2023.02.28

A1.06



ATTACHMENT 2

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REVISIONS

NO. 1	DATE	DESCRIPTION
1	07.11.2023	ISSUED FOR PERMITS
2	07.11.2023	ISSUED FOR PERMITS
3	07.11.2023	ISSUED FOR PERMITS
4	07.24.2023	ISSUED FOR PERMITS
5	08.13.2023	ISSUED FOR PERMITS
6	08.13.2023	ISSUED FOR PERMITS

TERA

FORMWERKS ARCHITECTURAL

1521 North Ave. Vancouver, BC V6L 1T6
 Tel: 604.273.3333
 Email: info@formwerks.com

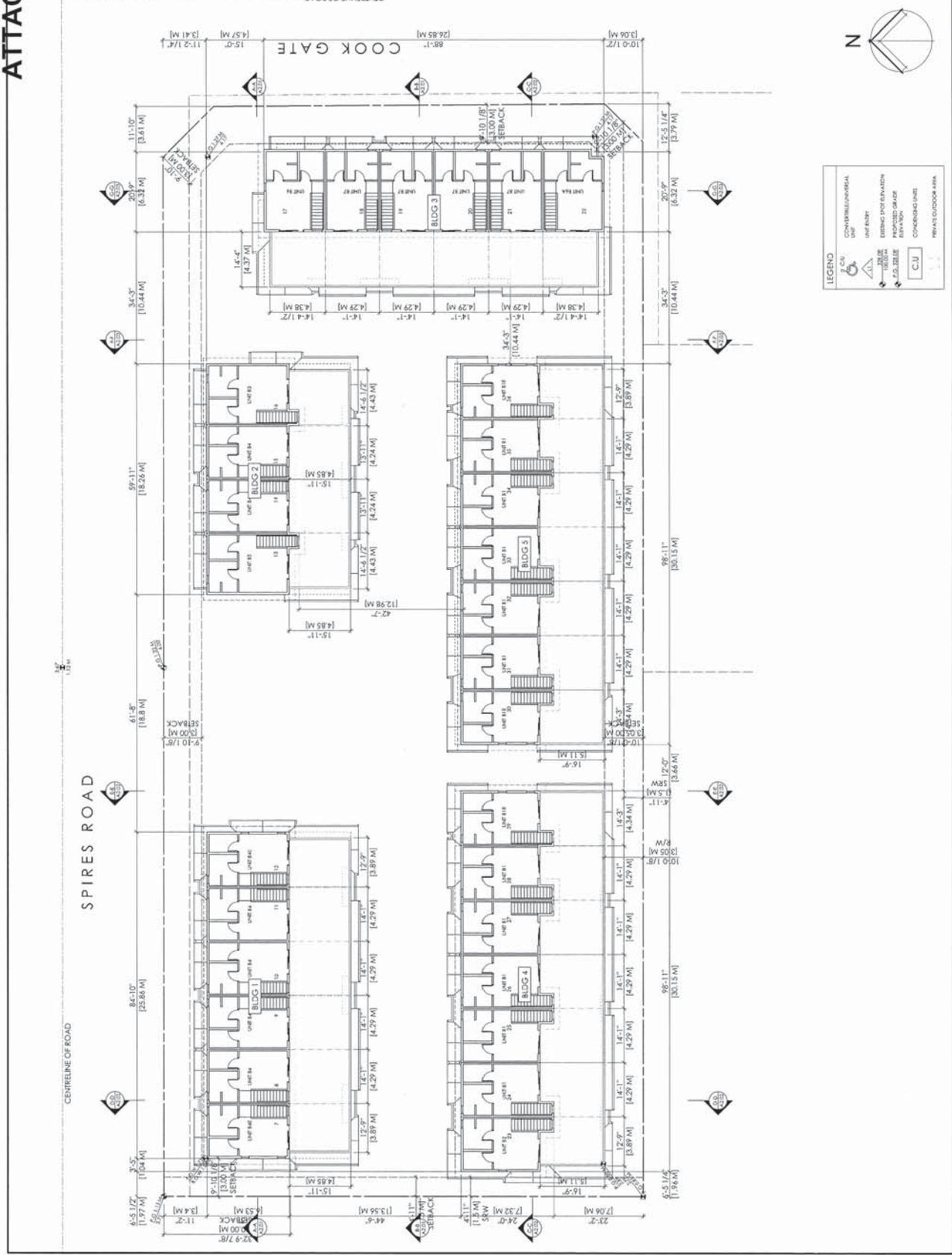
PROJECT

8800 SPIRES ROAD

8740-8800 SPIRES ROAD
 RICHMOND, BC

LEVEL 4 PLAN

SCALE	3/32" = 1'-0"
GATE	2023 JUN 28
SHEET	A1.07



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REVISIONS

NOV 15 2021	REVISION FOR RECORDING 1
NOV 15 2021	REVISION FOR RECORDING 2
NOV 15 2021	REVISION FOR RECORDING 3
NOV 15 2021	REVISION FOR RECORDING 4
NOV 15 2021	REVISION FOR RECORDING 5
NOV 15 2021	REVISION FOR RECORDING 6
NOV 15 2021	REVISION FOR RECORDING 7
NOV 15 2021	REVISION FOR RECORDING 8
NOV 15 2021	REVISION FOR RECORDING 9
NOV 15 2021	REVISION FOR RECORDING 10

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FORMWERKS ARCHITECTURAL

1125 West 10th Ave., Vancouver, BC V6H 1G9
 TEL: 604-278-8800
 WWW.FORMWERKS.COM

PROJECT

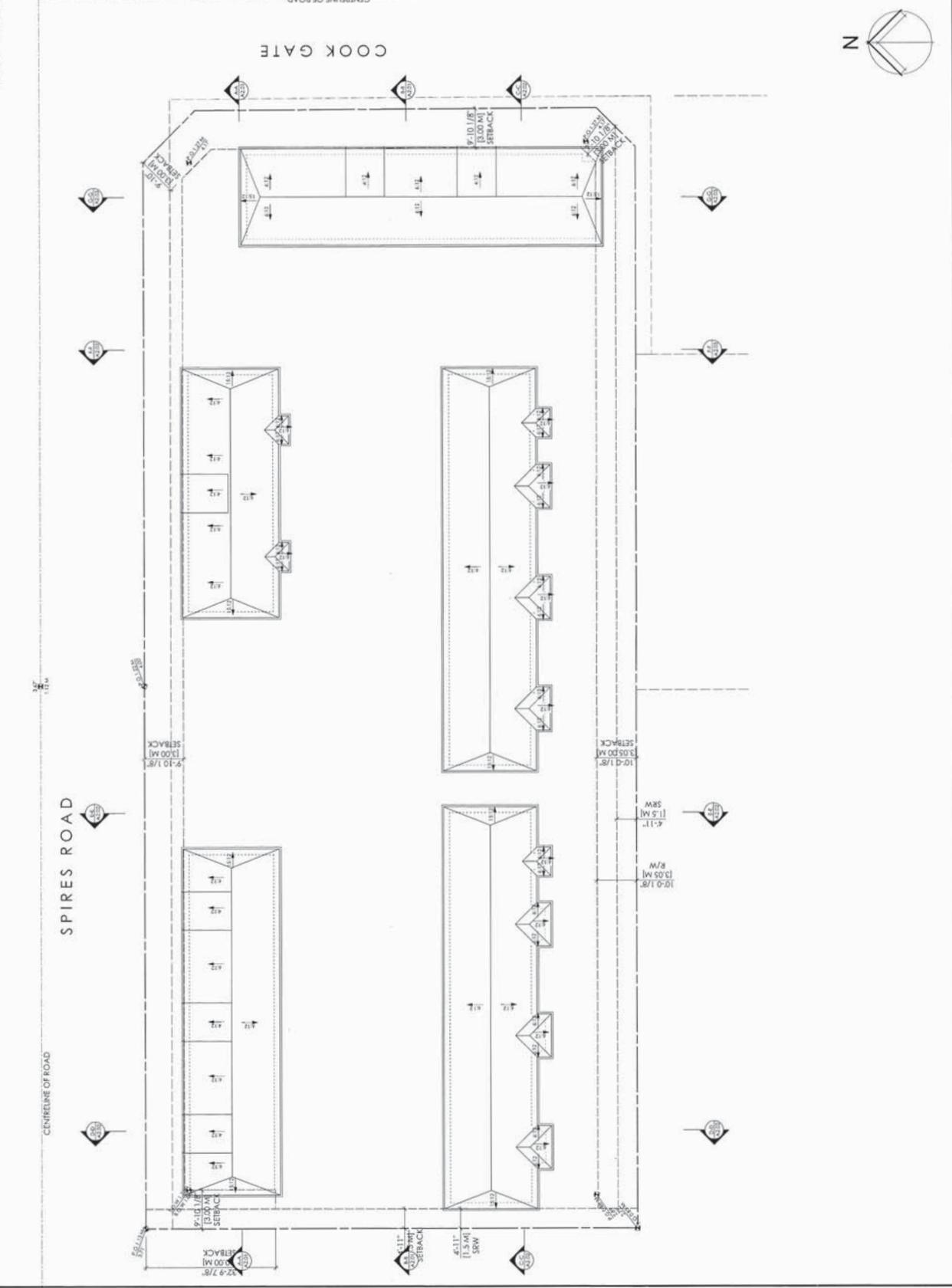
880 SPIRES ROAD

8740-8800 SPIRES ROAD
 RICHMOND, BC

DRAWING

ROOF PLAN

SCALE	SHEET
3/32" = 1'-0"	A1.08
DATE	2023-09-18



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REVISIONS

REVISED FOR REVISIONS 1	AUG 11, 2019
REVISED FOR REVISIONS 2	AUG 14, 2019
REVISED FOR REVISIONS 3	SEP 10, 2019
REVISED FOR REVISIONS 4	MAR 31, 2021
REVISED FOR REVISIONS 5	OCT 25, 2021
REVISED FOR REVISIONS 6	FEB 10, 2022
REVISED FOR REVISIONS 7	JAN 23, 2022



FORMWERKS ARCHITECTURAL

1425 West 99 Ave., Vancouver, BC V6J 1N5
Tel: 604-276-7676
Fax: 604-276-7676

PROJECT

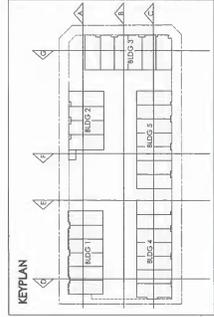
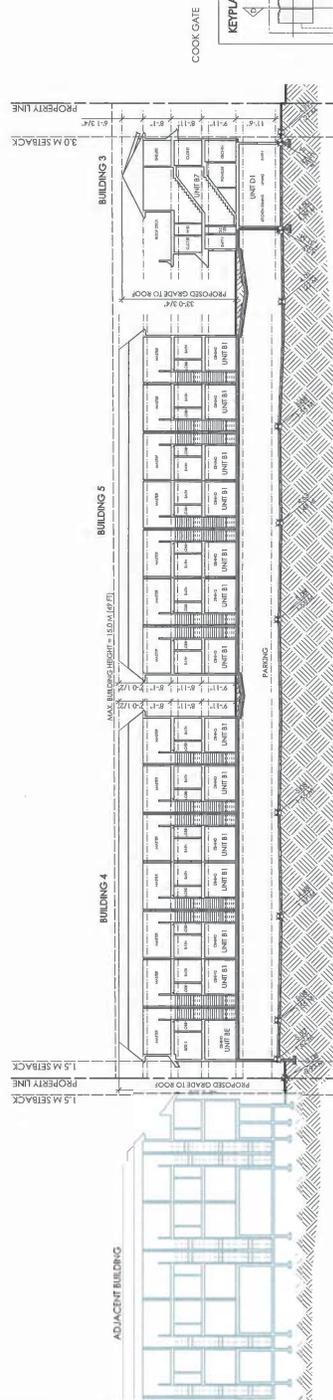
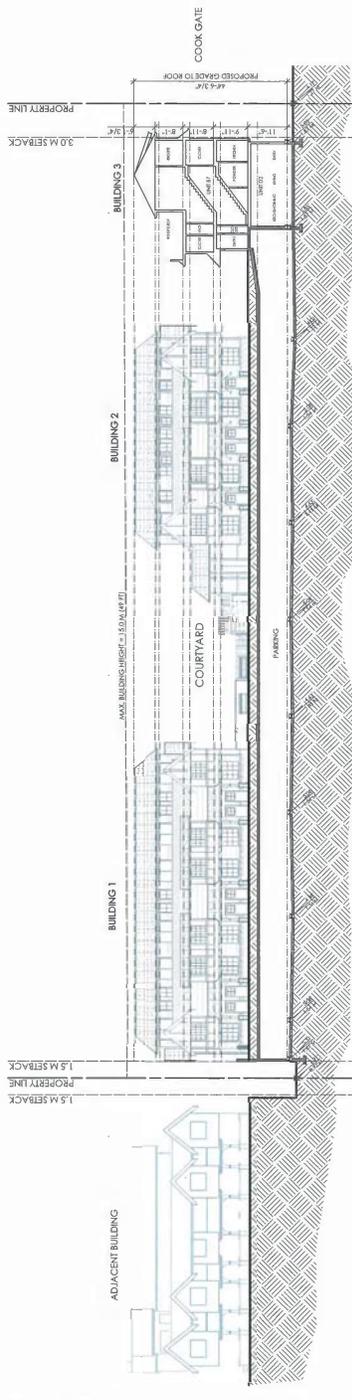
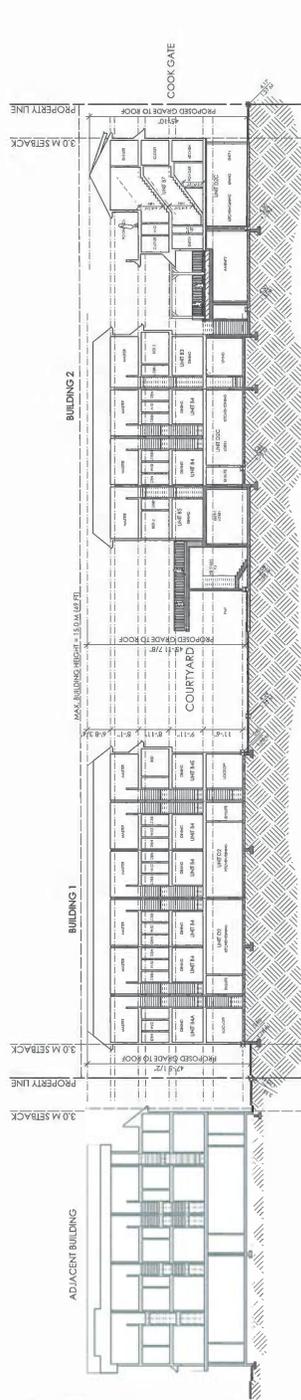
8800 SPIRES ROAD

8740-8800 SPIRES ROAD
RICHMOND, BC

DRAWING

SITE SECTIONS

SCALE 1/16" = 1'-0"
DATE 2022 JUN 28
SHEET
A1.11



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REVISIONS	DATE
ISSUED FOR PERMITTING	JULY 13, 2020
ISSUED FOR PERMITTING	JULY 13, 2020
ISSUED FOR PERMITTING	NOV 13, 2020
ISSUED FOR PERMITTING	NOV 13, 2020
ISSUED FOR PERMITTING	NOV 13, 2020
ISSUED FOR PERMITTING	NOV 13, 2020
ISSUED FOR PERMITTING	NOV 13, 2020
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ISSUED FOR PERMITTING	NOV 13, 2020

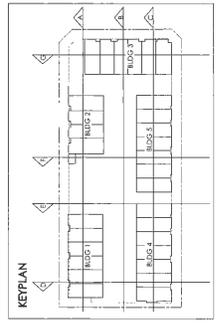
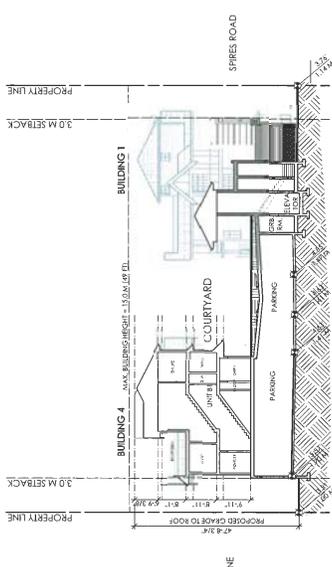
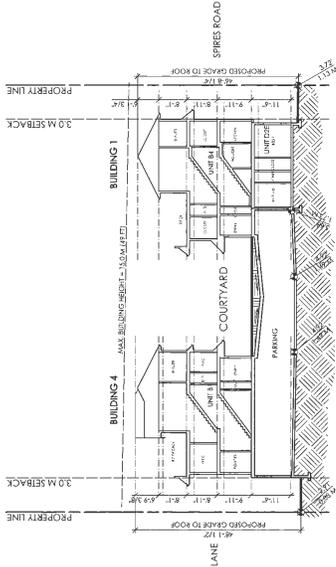
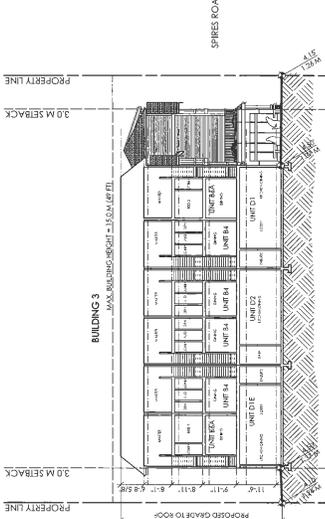
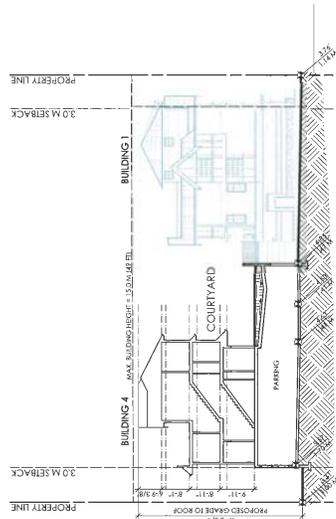
TERA

FORMWERKS ARCHITECTURAL

1425 West 5th Ave., Vancouver, BC, V6J 1T5
Tel: 604-275-2776

PROJECT: **8800 SPIRES ROAD**
8740-8800 SPIRES ROAD
RICHMOND, BC

DRAWING	
SCALE	1:118" = 1'-0"
DATE	2022.09.03
A1.12	



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REVISIONS

REVISED FOR RECORDS	JULY 11, 2019
REVISED FOR RECORDS	JULY 15, 2019
REVISED FOR RECORDS	SEP 11, 2019
REVISED FOR RECORDS	APR 31, 2021
REVISED FOR RECORDS	APR 30, 2021
REVISED FOR RECORDS	OCT 25, 2021
REVISED FOR RECORDS	NOV 15, 2021
REVISED FOR RECORDS	JAN 2, 2022

TERA

FORMWERKS ARCHITECTURAL

1425 West 5th Ave., Vancouver, BC V6J 1N5
 (604) 685-2076
 Phone: 483-5441

PROJECT

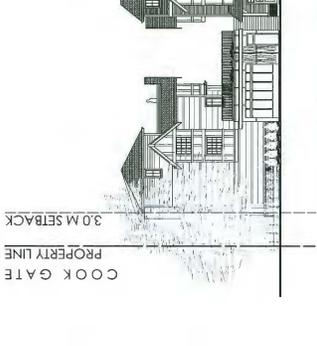
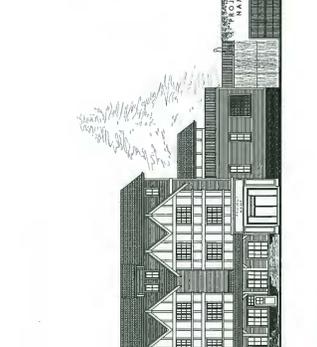
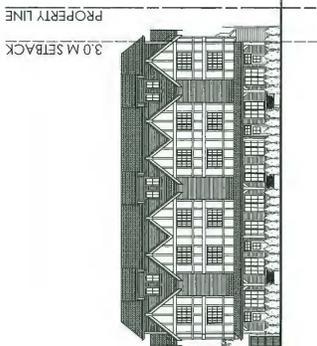
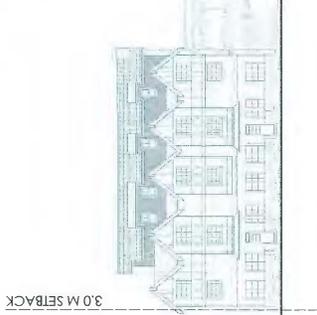
8800 SPIRES ROAD

8740-8800 SPIRES ROAD
 RICHMOND, BC

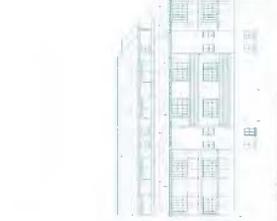
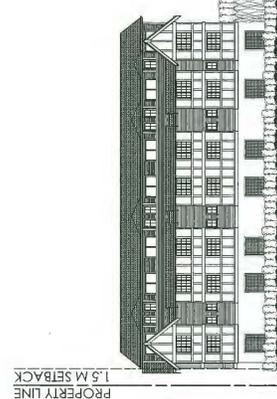
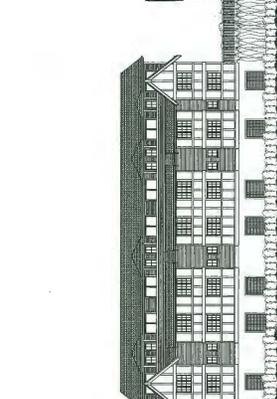
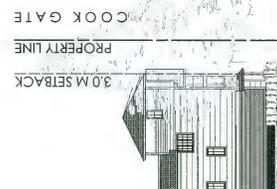
DRAWING

STREETSCAPES

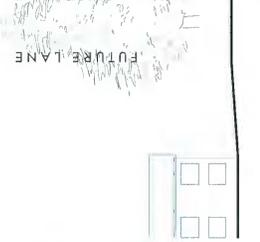
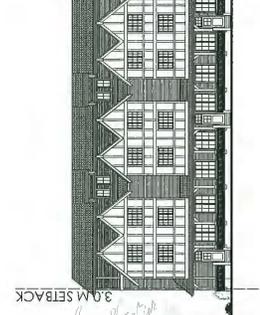
SCALE	1/16" = 1'-0"
DATE	2022.09.19
SHEET	A1.13



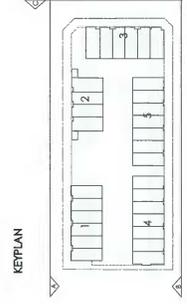
A - NORTH STREETSCAPE (SPIRES RD)



B - SOUTH STREETSCAPE (LANE)



C - EAST STREETSCAPE (COOK GATE)



ATTACHMENT 2

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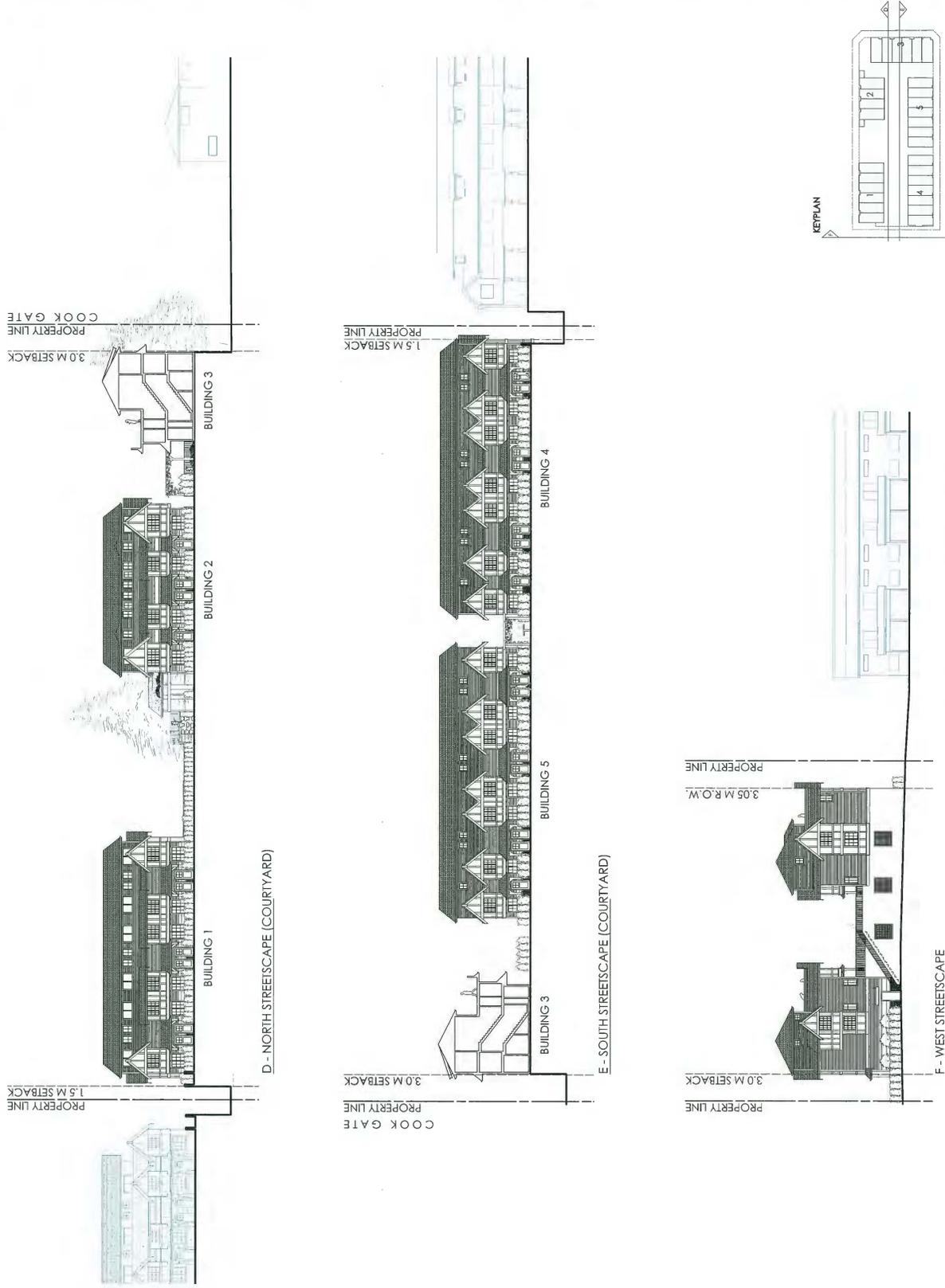
REVISIONS
 JUL 11, 2020
 ISSUED FOR PERMITS
 JUL 15, 2020
 REVISED FOR RECORDS
 APR 31, 2021
 REVISED FOR RECORDS
 OCT 28, 2021
 REVISED FOR RECORDS
 JUN 28, 2022
 REVISED FOR RECORDS
 JUN 28, 2022

TERA
FORMWERKS ARCHITECTURAL
 1670 West 7th Ave., Vancouver, B.C. V6J 1N5
 Form: 6852079 Phone: 685-5441

PROJECT
8800 SPIRES ROAD
 8740-8800 SPIRES ROAD
 RICHMOND, B.C.

DRAWING
STREETSCAPES

SCALE 1/16" = 1'-0"
DATE 2022.06.29
SHEET A1.14



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REVISIONS

NO. 1	DATE	DESCRIPTION
1	JUL 11, 2022	ISSUED FOR PERMITTING
2	AUG 8, 2022	ISSUED FOR PERMITTING
3	JUL 24, 2022	ISSUED FOR PERMITTING
4	AUG 11, 2022	ISSUED FOR PERMITTING
5	NOV 2, 2022	ISSUED FOR PERMITTING
6	FEB 4, 2022	ISSUED FOR PERMITTING

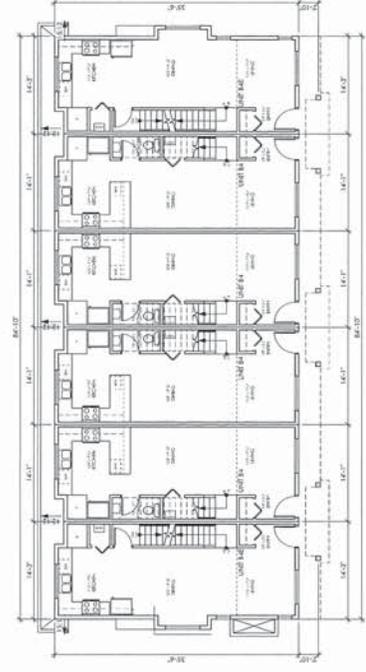


FORMWERKS ARCHITECTURAL
 1525 West 4th Ave., Vancouver, BC V6J 1H5
 Tel: 604.273.2070

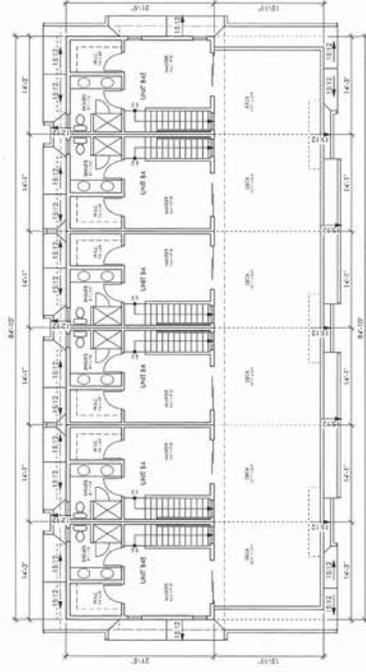
PROJECT
 8800 SPIRES ROAD
 8740-8800 SPIRES ROAD
 RICHMOND, BC

DRAWING
 BUILDING 1
 PLANS

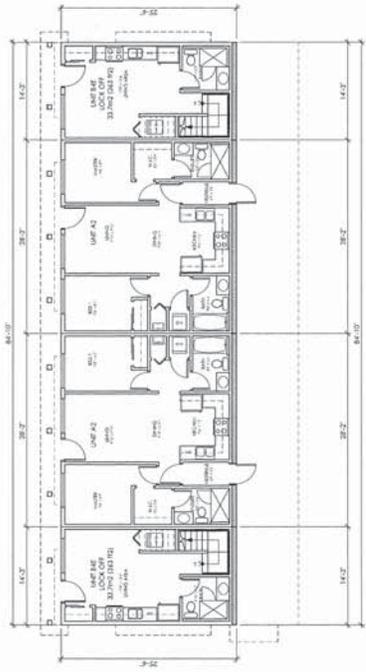
SCALE	1/8" = 1'-0"	SHEET	A2.00
DATE	2022/06/4		



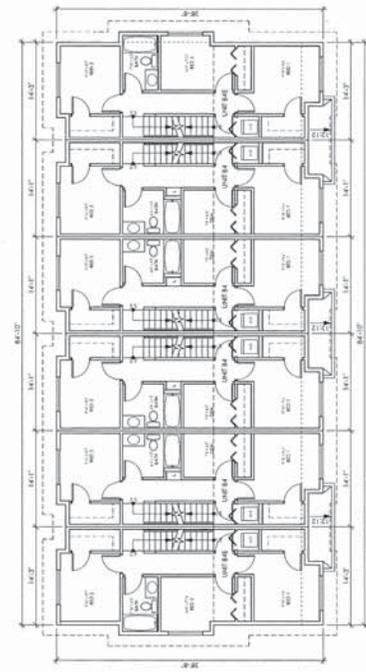
LEVEL 2 PLAN
BUILDING 1



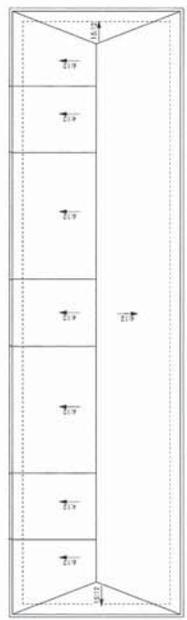
LEVEL 4 PLAN
BUILDING 1



LEVEL 1 PLAN
BUILDING 1



LEVEL 3 PLAN
BUILDING 1



ROOF PLAN
BUILDING 1



ATTACHMENT 2

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REVISIONS

NOV 11, 2021	ISSUED FOR PERMITS
AUG 11, 2021	ISSUED FOR PERMITS
MAR 8, 2021	ISSUED FOR PERMITS
SEP 24, 2021	ISSUED FOR PERMITS
NOV 2, 2021	ISSUED FOR PERMITS
SEP 20, 2021	ISSUED FOR PERMITS



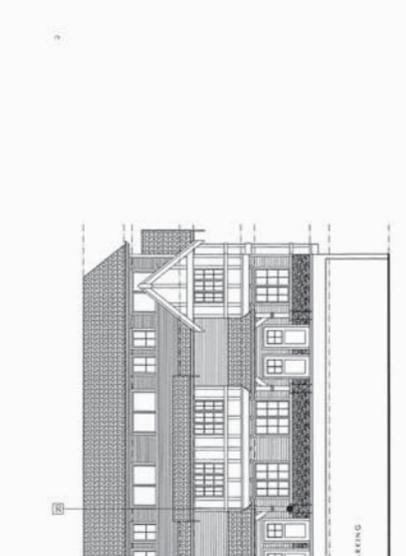
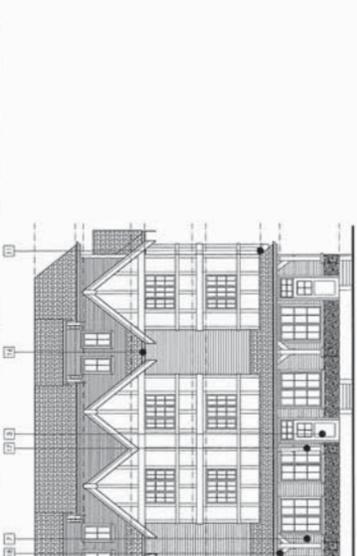
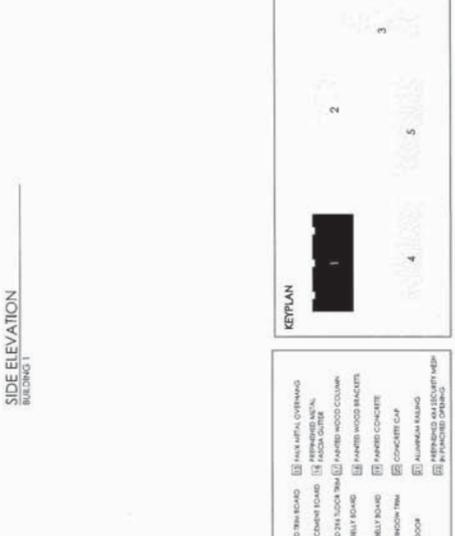
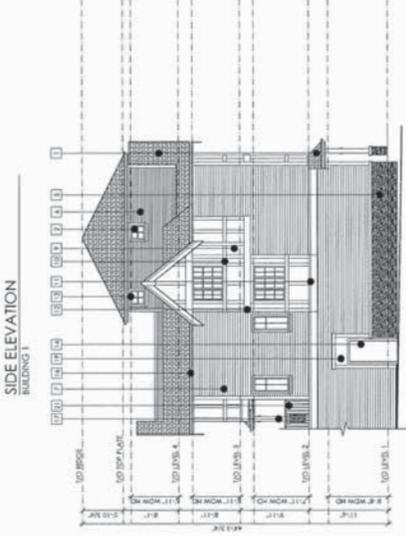
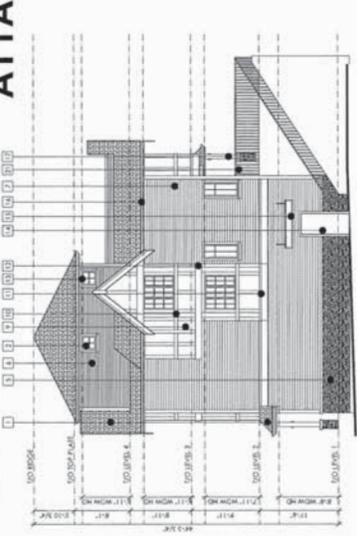
FORMWERKS ARCHITECTURAL

1021 West 29 Ave, Vancouver, BC V6L 1R6
 (604) 683-2020

PROJECT
 880 SPIRES ROAD
 8740-8800 SPIRES ROAD
 RICHMOND, BC

DRAWING
 BUILDING 1
 ELEVATIONS

SHEET
 SCALE 1/8" = 1'-0"
 DATE 2021 09 18
A2.01



- LEGEND**
- | | | | |
|---|-------------------------|----|---------------------|
| 1 | PAINTED WOOD TRIM BOARD | 10 | PAINTED WOOD SHAKES |
| 2 | PAINTED WOOD TRIM BOARD | 11 | PAINTED WOOD SHAKES |
| 3 | PAINTED WOOD TRIM BOARD | 12 | PAINTED WOOD SHAKES |
| 4 | PAINTED WOOD TRIM BOARD | 13 | PAINTED WOOD SHAKES |
| 5 | PAINTED WOOD TRIM BOARD | 14 | PAINTED WOOD SHAKES |
| 6 | PAINTED WOOD TRIM BOARD | 15 | PAINTED WOOD SHAKES |
| 7 | PAINTED WOOD TRIM BOARD | 16 | PAINTED WOOD SHAKES |
| 8 | PAINTED WOOD TRIM BOARD | 17 | PAINTED WOOD SHAKES |
| 9 | PAINTED WOOD TRIM BOARD | 18 | PAINTED WOOD SHAKES |



ATTACHMENT 2

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REVISIONS	
NOV 21 2018	ISSUED FOR RECORDING 1
NOV 21 2018	ISSUED FOR RECORDING 2
NOV 21 2018	ISSUED FOR RECORDING 3
NOV 21 2018	ISSUED FOR RECORDING 4
NOV 21 2018	ISSUED FOR RECORDING 5
NOV 21 2018	ISSUED FOR RECORDING 6
NOV 21 2018	ISSUED FOR RECORDING 7
NOV 21 2018	ISSUED FOR RECORDING 8
NOV 21 2018	ISSUED FOR RECORDING 9
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NOV 21 2018	ISSUED FOR RECORDING 11
NOV 21 2018	ISSUED FOR RECORDING 12
NOV 21 2018	ISSUED FOR RECORDING 13
NOV 21 2018	ISSUED FOR RECORDING 14
NOV 21 2018	ISSUED FOR RECORDING 15
NOV 21 2018	ISSUED FOR RECORDING 16
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NOV 21 2018	ISSUED FOR RECORDING 21
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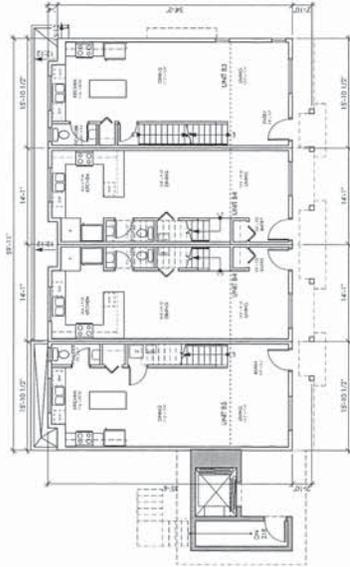


FORMWERKS ARCHITECTURAL
 1425 West 24th Ave., Vancouver, BC V6J 1T8
 Tel: 604-681-8888 Fax: 604-681-8889

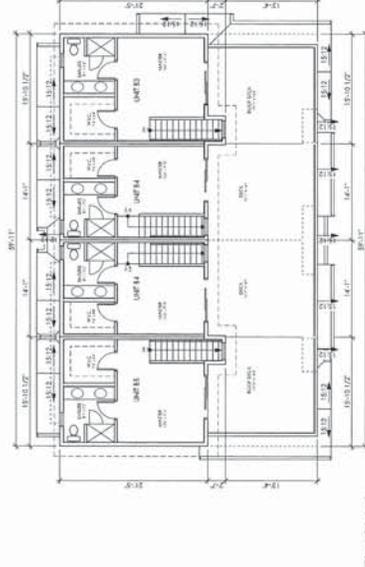
PROJECT
8800 SPIRES ROAD
 8740-8800 SPIRES ROAD
 RICHMOND, BC

DRAWING
BUILDING 2 PLANS

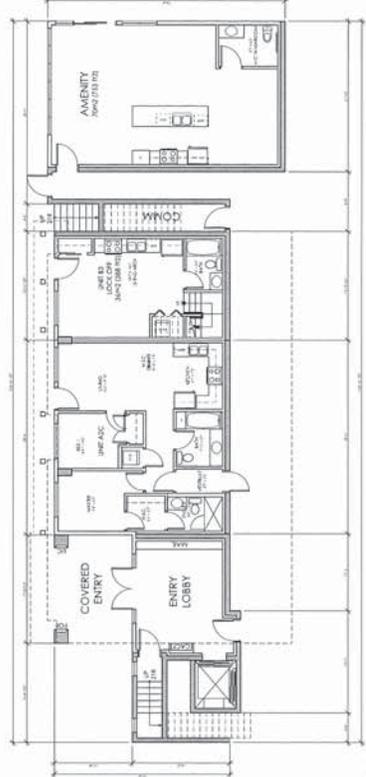
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DATE	2022 Nov 4		



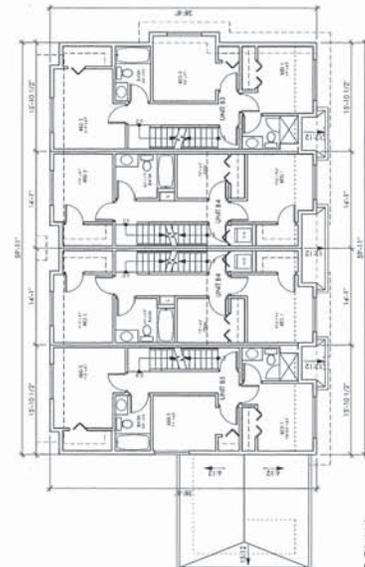
LEVEL 2 PLAN
BUILDING 2



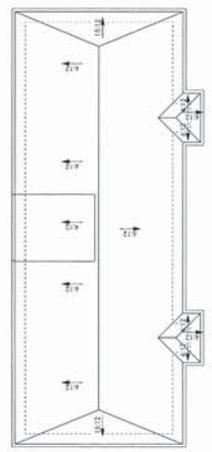
LEVEL 4 PLAN
BUILDING 2



LEVEL 1 PLAN
BUILDING 2



LEVEL 3 PLAN
BUILDING 2



ROOF PLAN
BUILDING 2



ATTACHMENT 2

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REVISIONS

NOV 11 2021	REVISED FOR PERMITS
AUG 11 2021	REVISED FOR PERMITS
MAY 18 2021	REVISED FOR PERMITS
JUL 24 2021	REVISED FOR PERMITS
NOV 3 2021	REVISED FOR PERMITS
JAN 20 2022	REVISED FOR PERMITS

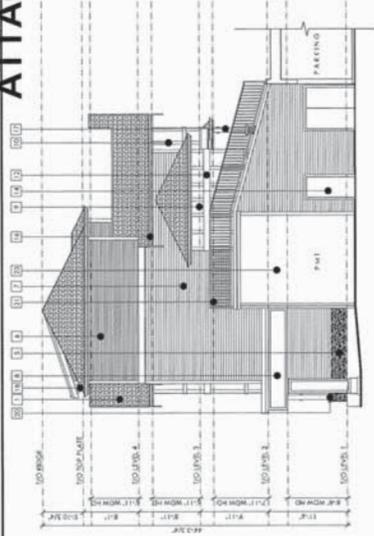


1021 West 4th Ave, Richmond, BC V6V 1K6
Tel: (604) 270-1111
www.formwerks.com

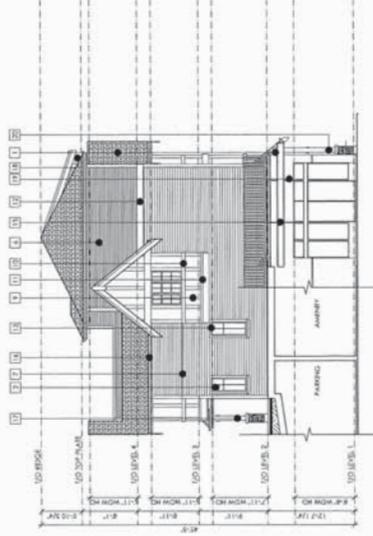
PROJECT
8800 SPIRES ROAD
8740-8800 SPIRES ROAD
RICHMOND, BC

DRAWING
BUILDING 2
ELEVATIONS

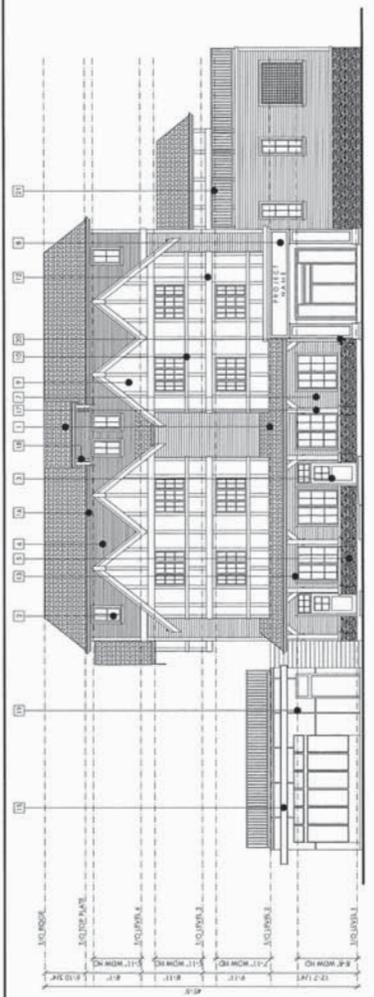
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DATE: 2022 JUN 28
SHEET: A2.03



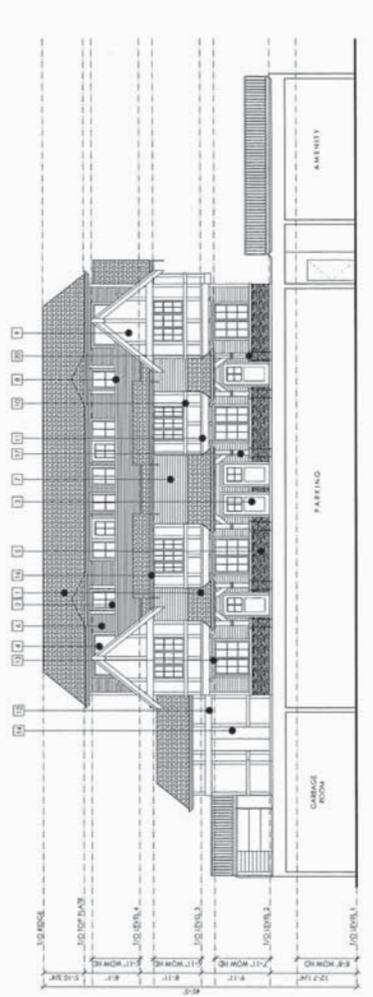
FRONT ELEVATION
BUILDING 2



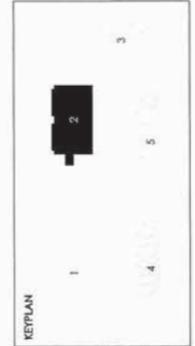
SIDE ELEVATION
BUILDING 2



REAR ELEVATION
BUILDING 2



SIDE ELEVATION
BUILDING 2



LEGEND

1 PAINTED WOOD TRIM BOARD	16 METAL OVERMOUNT
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3 PAINTED WOOD TRIM BOARD	18 PAINTED WOOD TRIM BOARD
4 PAINTED WOOD TRIM BOARD	19 PAINTED WOOD TRIM BOARD
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13 PAINTED WOOD TRIM BOARD	28 PAINTED WOOD TRIM BOARD
14 PAINTED WOOD TRIM BOARD	29 PAINTED WOOD TRIM BOARD
15 PAINTED WOOD TRIM BOARD	30 PAINTED WOOD TRIM BOARD

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REVISIONS

REVISED FOR RECORDS	JULY 11, 2023
REVISED FOR RECORDS	MAY 8, 2023
REVISED FOR RECORDS	MAY 24, 2023
REVISED FOR RECORDS	MAY 24, 2023
REVISED FOR RECORDS	MAY 24, 2023
REVISED FOR RECORDS	JULY 28, 2023



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(604) 681-2028

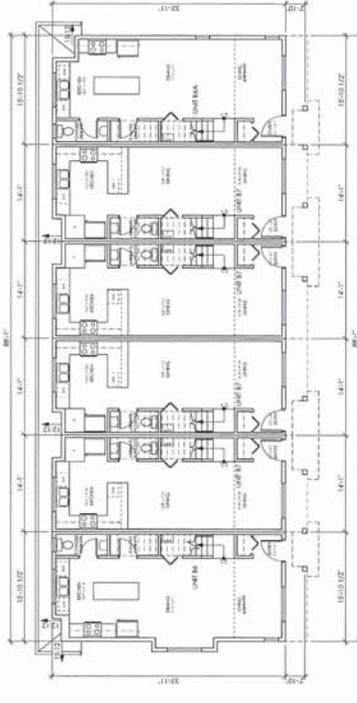
PROJECT

8800 SPIRES ROAD
8740-8800 SPIRES ROAD
RICHMOND, BC

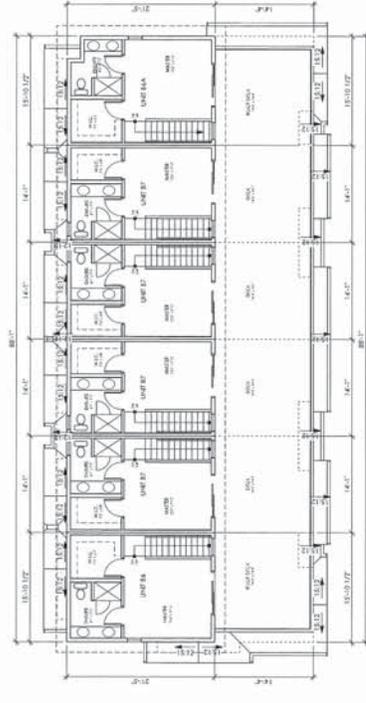
DRAWING

BUILDING 3 PLANS

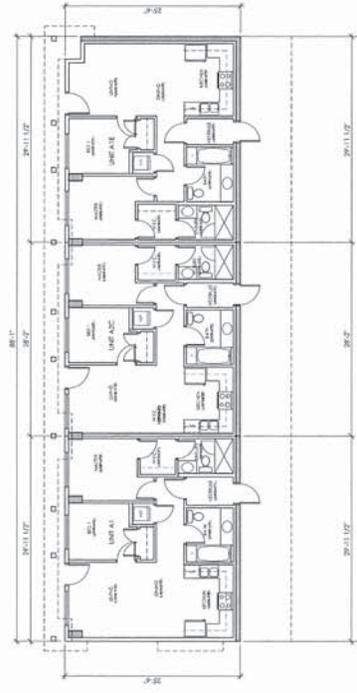
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DATE	2023.06.28		



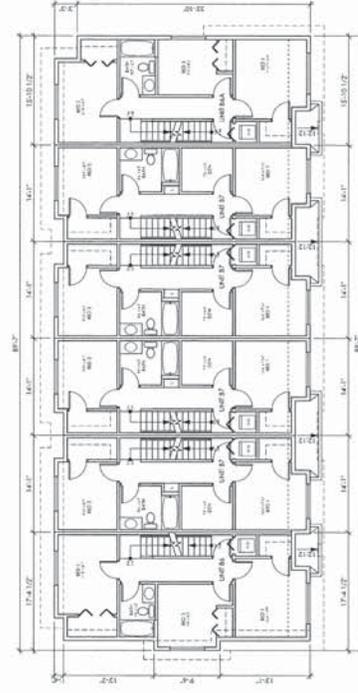
LEVEL 2 PLAN
BUILDING 3



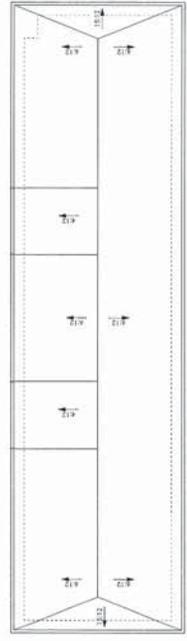
LEVEL 4 PLAN
BUILDING 3



LEVEL 1 PLAN
BUILDING 3



LEVEL 3 PLAN
BUILDING 3



ROOF PLAN
BUILDING 3



ATTACHMENT 2

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REVISIONS	DATE
REVISED FOR 800 SPIRES	MAY 13, 2020
REVISED FOR 800 SPIRES	MAY 13, 2020
REVISED FOR 800 SPIRES	MAY 13, 2020
REVISED FOR 800 SPIRES	MAY 13, 2020
REVISED FOR 800 SPIRES	MAY 13, 2020
REVISED FOR 800 SPIRES	MAY 13, 2020
REVISED FOR 800 SPIRES	MAY 13, 2020
REVISED FOR 800 SPIRES	MAY 13, 2020

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1421 West 15th Ave., Richmond, VA 23220
 Phone: 804-541-1100
 Fax: 804-209-1100

PROJECT

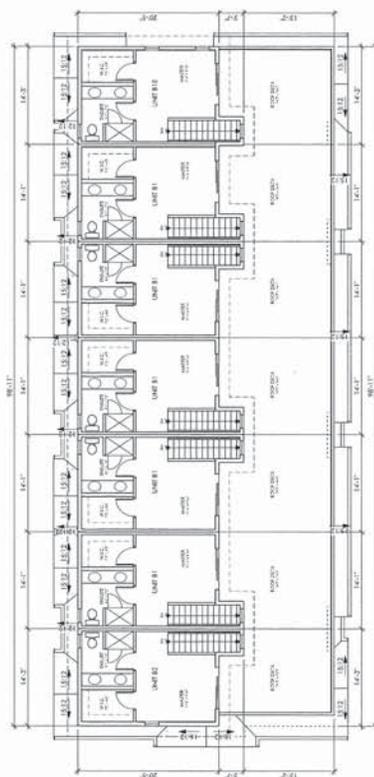
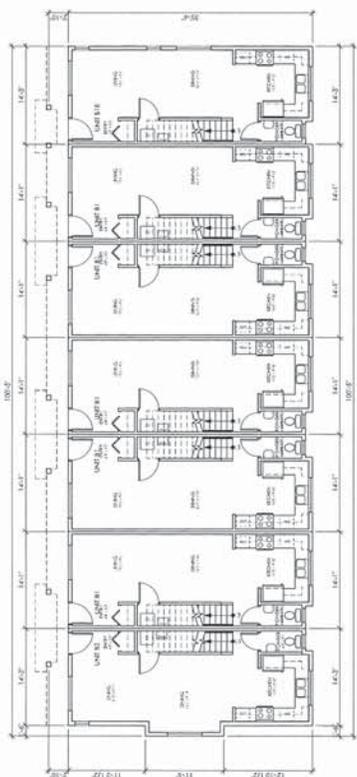
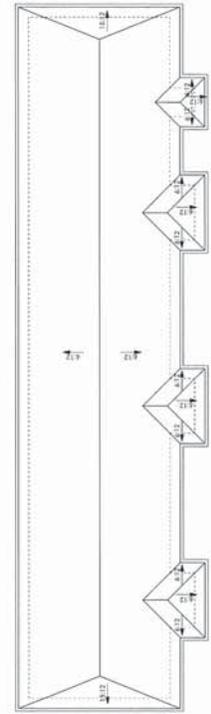
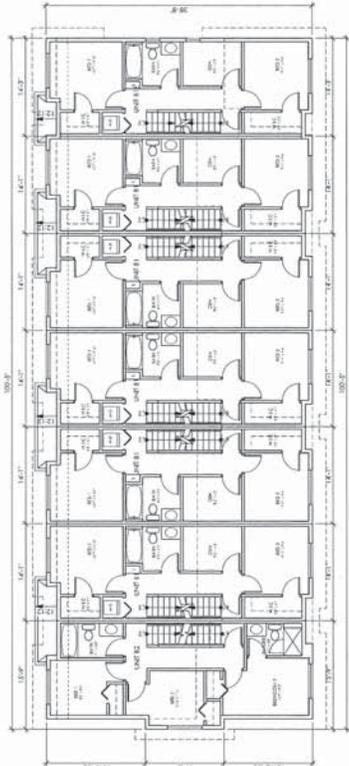
8800 SPIRES ROAD

8740-8800 SPIRES ROAD
 RICHMOND, BC

DRAWING

BUILDING 4
PLANS

SCALE	1/8" = 1'-0"	SHEET	A2.06
DATE			



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REVISIONS

NO. 1	DATE	DESCRIPTION
1	JUL 15, 2021	ISSUED FOR PERMITS
2	MAY 11, 2021	ISSUED FOR PERMITS
3	MAY 11, 2021	ISSUED FOR PERMITS
4	MAY 11, 2021	ISSUED FOR PERMITS
5	MAY 11, 2021	ISSUED FOR PERMITS
6	MAY 11, 2021	ISSUED FOR PERMITS

TERA

FORMWERKS ARCHITECTURAL

1221 West 7th Ave., Richmond, VA 23218
Tel: 432-2076 Fax: 432-2076

PROJECT

8800 SPIRES ROAD

8740-8800 SPIRES ROAD
RICHMOND, BC

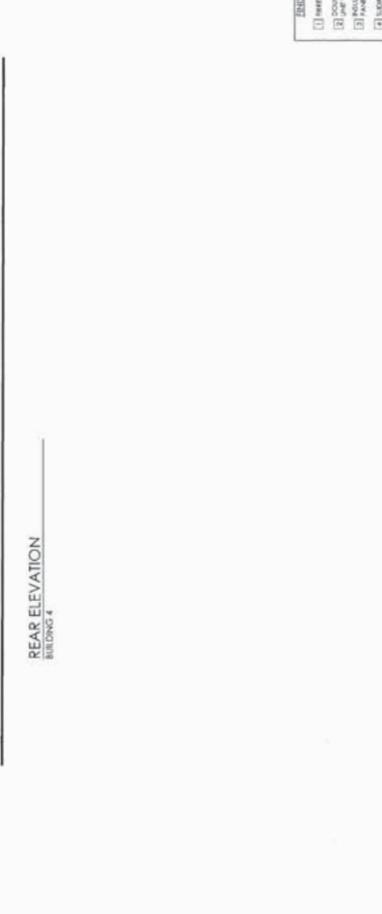
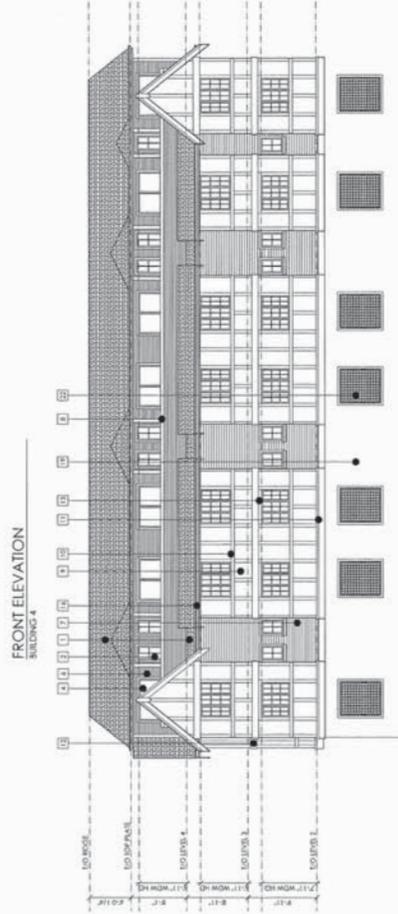
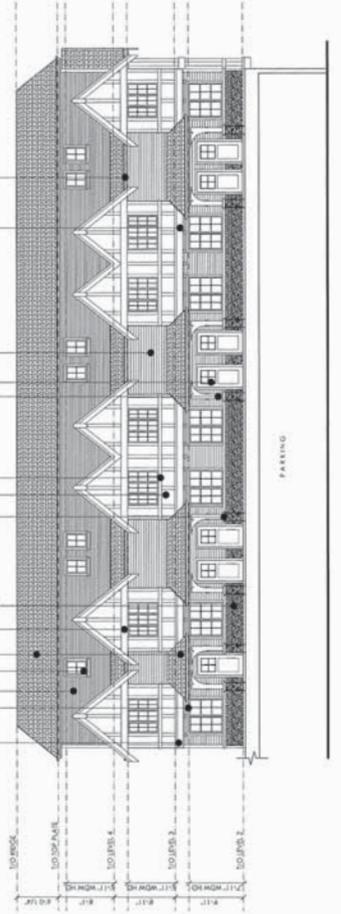
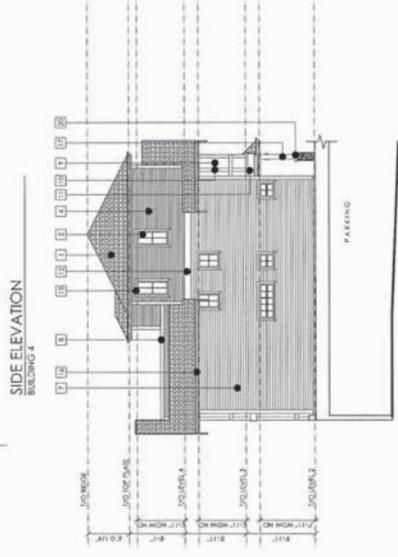
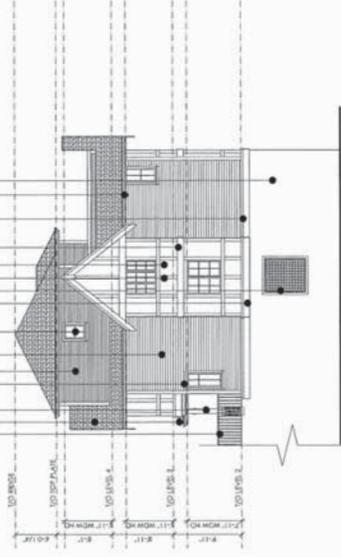
DRAWING

BUILDING 4 ELEVATIONS

SCALE 1/8" = 1'-0"

DATE 2021 APR 14

SHEET A2.07



FINISH SCHEDULE

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2	PAINTED WOOD TRIM BOARD	2	PAINTED WOOD TRIM BOARD	2	PAINTED METAL
3	PAINTED WOOD TRIM BOARD	3	PAINTED WOOD TRIM BOARD	3	PAINTED METAL
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7	PAINTED WOOD TRIM BOARD	7	PAINTED WOOD TRIM BOARD	7	PAINTED METAL
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11	PAINTED WOOD TRIM BOARD	11	PAINTED WOOD TRIM BOARD	11	PAINTED METAL
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14	PAINTED WOOD TRIM BOARD	14	PAINTED WOOD TRIM BOARD	14	PAINTED METAL



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REVISIONS

ISSUED FOR RECORD	AUG 21, 2008
ISSUED FOR RECORD	MAY 15, 2007
ISSUED FOR RECORD	MAY 15, 2007
ISSUED FOR RECORD	MAY 15, 2007
ISSUED FOR RECORD	OCT 25, 2007
ISSUED FOR RECORD	JAN 28, 2007

TERA

FORMWERKS ARCHITECTURAL

1422 Royal Pk. Ave., Richmond, BC V6V 1H5
 Formwerks@telus.net Phone: 483-5441

PROJECT

8800 SPIRES ROAD

8740-8800 SPIRES ROAD
 RICHMOND, B.C.

DRAWING

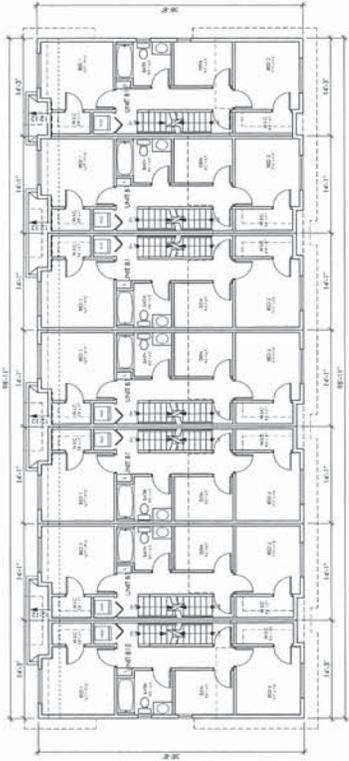
BUILDING 5 PLANS

SHEET

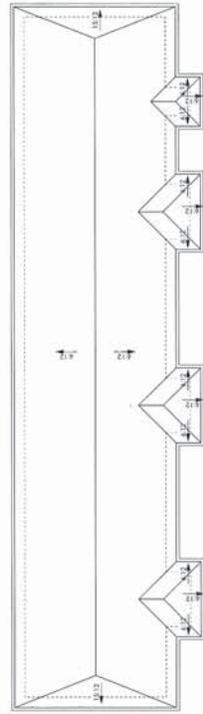
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DATE 2007.08.19

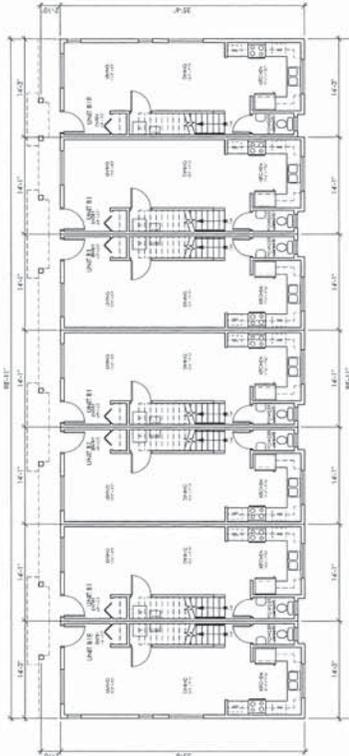
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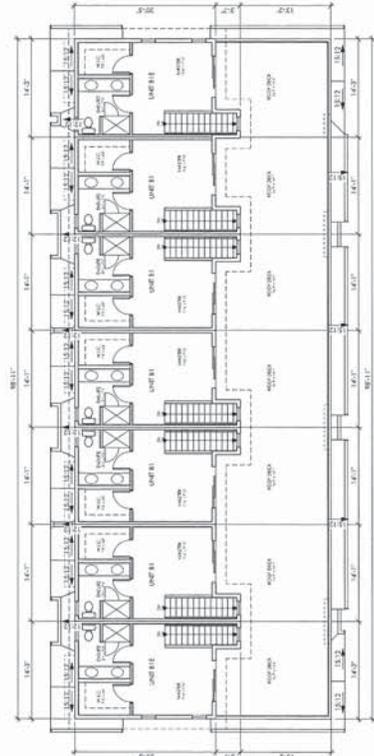
LEVEL 3 PLAN
BUILDING 5



ROOF PLAN
BUILDING 5



LEVEL 2 PLAN
BUILDING 5



LEVEL 4 PLAN
BUILDING 5



SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
1	12.14.16	ISSUED FOR PERMIT	DR
2	12.14.16	ISSUED FOR PERMIT	DR
3	12.14.16	ISSUED FOR PERMIT	DR
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20	12.14.16	ISSUED FOR PERMIT	DR

PROJECT:

**SPIRES II 38 UNIT
TOWNHOUSE DEVELOPMENT
8800, 8780, 8760 SPIRES GATE
AND 8740 SPIRES ROAD
RICHMOND**

DRAWING TITLE:

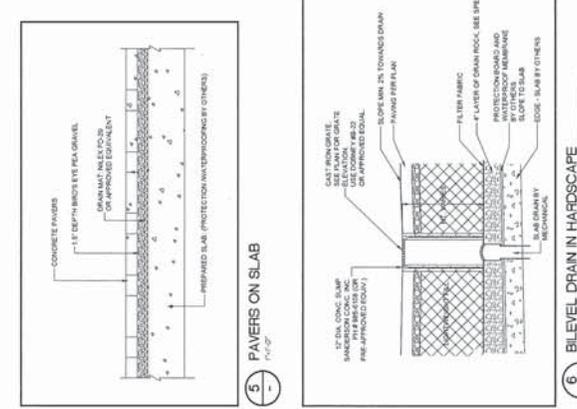
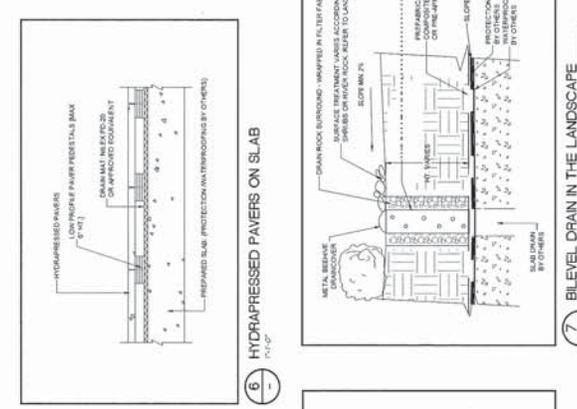
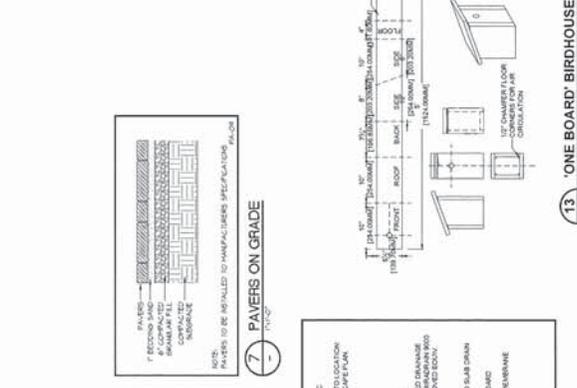
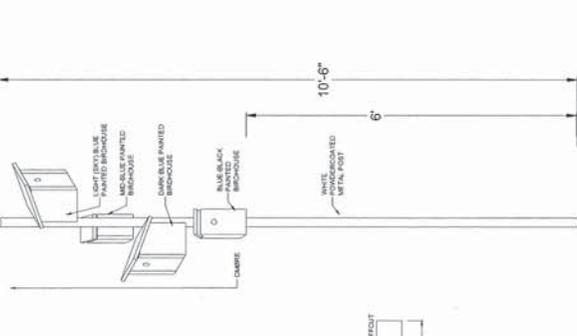
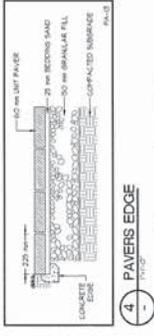
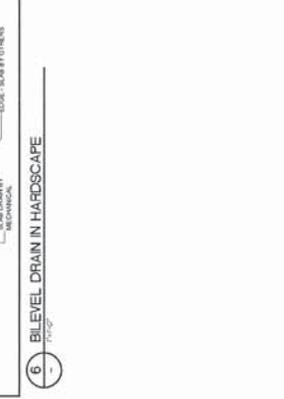
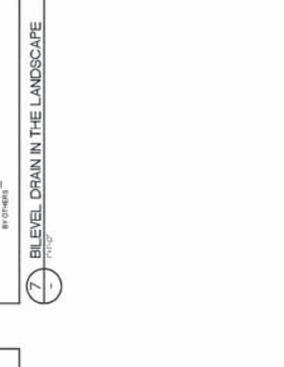
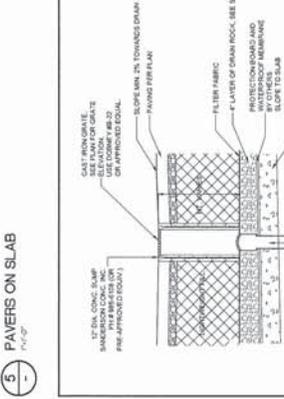
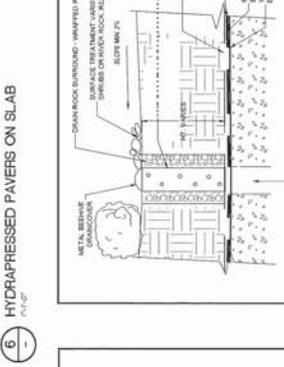
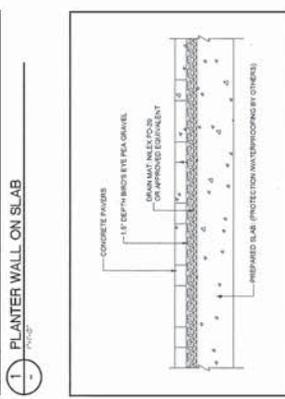
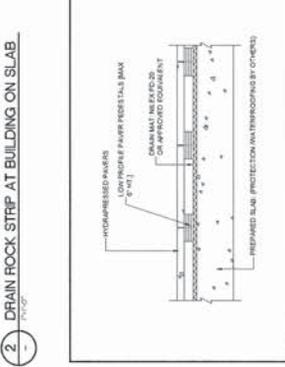
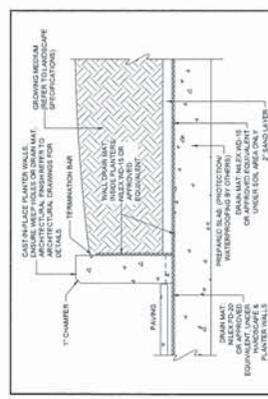
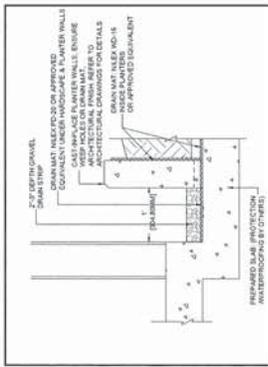
**LANDSCAPE
DETAILS**

DATE: 23.06.14
SCALE: AS SHOWN
DRAWN: MC
DESIGN: MC
CHECKED: JC
PROJECT NUMBER: 21-176

L6

OF 7

2376-02-01





RZ 19-870807

Attachment 3

Address: 8740, 8760, 8780 and 8800 Spires Road, and the surplus portion of the Spires Road road allowance

Applicant: Terra Spires Development LP

Planning Area(s): City Centre

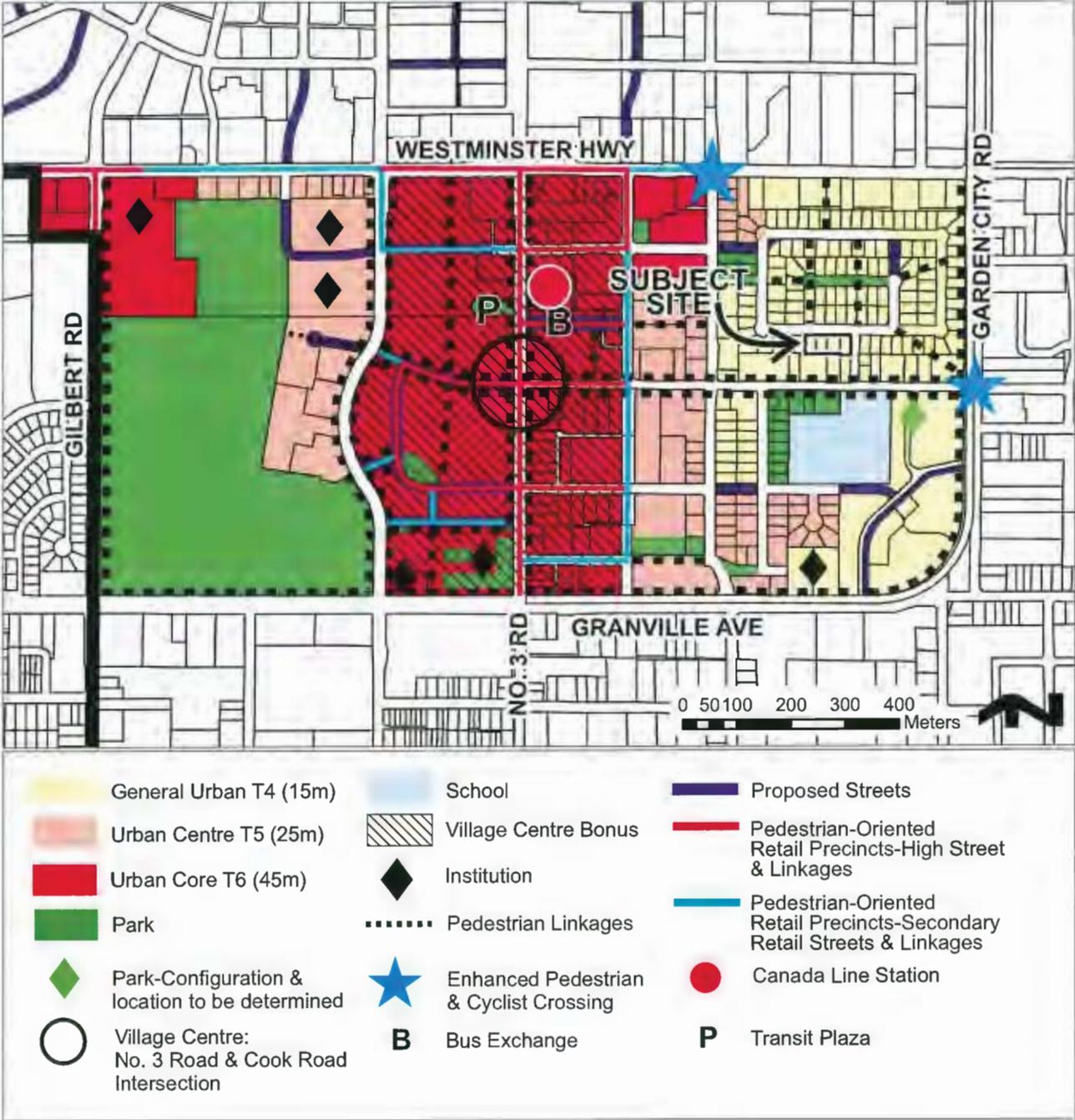
	Existing	Proposed
Owner:	1219002 BC Ltd.	No Change
Site Size (m²):	3,430 m ² (36,925 ft ²)	3,260 m ² (35,087 ft ²)
Land Uses:	Single-Family Residential	Multiple-Family Residential
OCP Designation:	Low-Density Residential	No Change
Area Plan Designation:	City Centre Area Plan: General Urban T4 Sub-Area B.1: Mixed Use – Low-Rise Residential & Limited Commercial	No Change
702 Policy Designation:	N/A	No Change
Zoning:	Single Detached (RS1/E)	Parking Structure Townhouses (RTP4)
Number of Units:	4	36
Other Designations:	N/A	No Change

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 1.20	1.20	none permitted
Lot Coverage – Building:	Max. 50%	50% Max.	none
Lot Coverage – Non-porous Surfaces:	Max. 80%	80.0% Max.	none
Lot Coverage – Landscaping:	Min. 20%	20% Min.	none
Setback – Front Yard - Cook Gate (m):	Min. 3.0 m	3.0 m	none
Setback – Exterior Side Yard – North – Spires Road (m):	Min. 3.0 m	3.0 m	none
Setback – Interior Side Yard - South (future lane) (m):	Min. 1.5 m	1.5 m	none
Setback – Rear - West (m):	Min. 1.5 m	1.5 m	none
Height (m):	Max. 15.0 m (4 storeys)	15.0 m Max.	none

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Lot Depth:	Min. 30.0 m	86.27 m	none
Site Area:	Min. 2,400 m ²	3,260 m ²	none
Off-street Parking Spaces – Regular (R) / Visitor (V):	1.2 (R) and 0.2 (V) per unit	1.5 (R) and 0.2 (V) per unit	none
Off-street Parking Spaces – Total:	44 (R) and 8 (V)	54 (R) and 8 (V)	none
Tandem Parking Spaces:	Max. 50% of required residential spaces (44 x Max. 50% = 22)	18	none
Small Car Parking Spaces	Max. 50% when 31 or more spaces are provided on-site (62 x Max. 50% = 31)	6	none
Handicap Parking Spaces:	Min. 2% when 11 or more spaces are required (44 x 2% = 1 spaces)	4	none
Bicycle Parking Spaces – Class 1 / Class 2:	1.25 (Class 1) and 0.20 (Class 2) per unit	1.25 (Class 1) and 0.2 (Class 2) per unit	none
Off-street Parking Spaces – Total:	45 (Class 1) and 8 (Class 2)	45 (Class 1) and 8 (Class 2)	none
Amenity Space – Indoor:	Min. 70 m ² or Cash-in-lieu	Min. 70 m ²	none

Other: Tree replacement compensation required for removal of bylaw-sized trees.

Specific Land Use Map: Brighthouse Village (2031)



Maximum building height may be subject to established Airport Zoning Regulations in certain areas.

FORMWERKS ARCHITECTURAL

March 10, 2021

City of Richmond

6911 No. 3 Road,
Richmond, BC
V6Y 2C1

Re: Letter of Confirmation

**8800 Spires Rd., Richmond, BC
RZ-17-766525**

This letter confirms that the design of the proposed development will meet the energy efficiency requirements expected at the time of the proposed project's Building Permit Application. This also confirms that the undersigned is aware of the Step Code requirements for this project.

If you have any questions, please do not hesitate to contact me.

Your Truly,

A blue ink signature is written over a red circular professional seal. The seal contains the text "REGISTERED ARCHITECT" at the top, "JAMES F. BUSSEY" in the center, and "BRITISH COLUMBIA" at the bottom.

James F. Bussey, Architect, AIBC
Formwerks Architectural Inc.

ATTACHMENT 6

**REFERENCE PLAN TO ACCOMPANY THE CITY OF RICHMOND
ROAD CLOSING AND REMOVAL OF ROAD DEDICATION BYLAW NO. _____
OF A PORTION OF ROAD DEDICATED ON PLAN 21489
SECTIONS 9 AND 10, BLOCK 4 NORTH, RANGE 6 WEST, NEW WESTMINSTER DISTRICT**

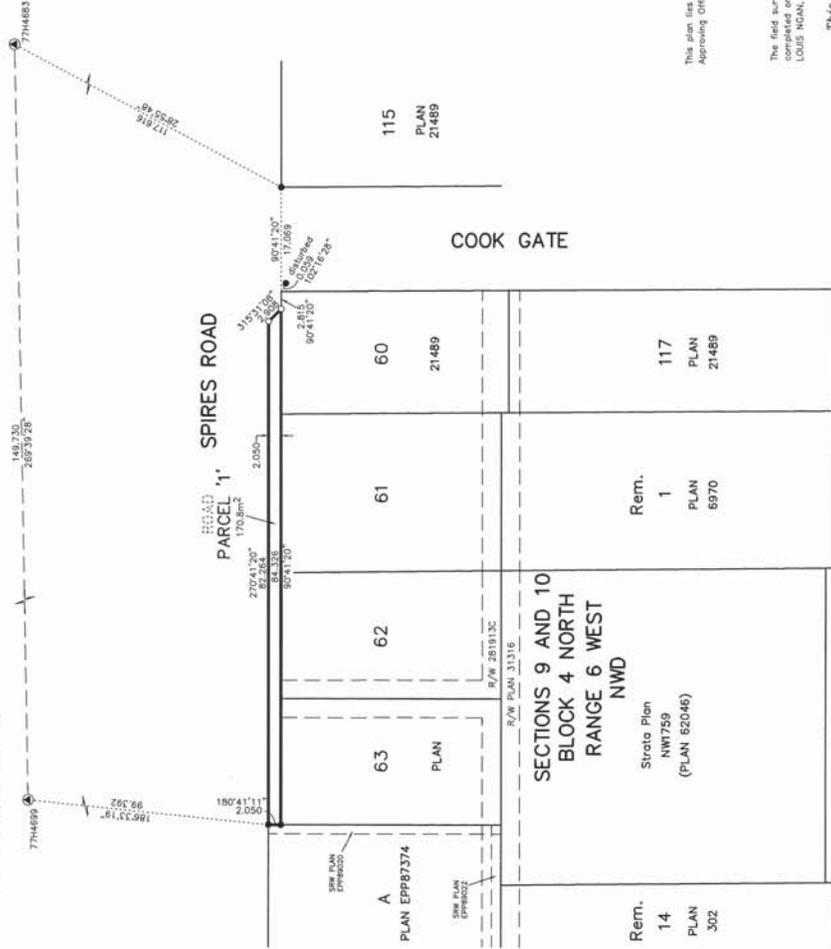
Pursuant to Section 120, Land Title Act and Section 40, Community Charter

BCGS 926.015

SCALE 1 : 500



All distances are in metres.
The intended plot size of this plan is
560mm in width by 432mm in height (C base)
when plotted at a scale of 1 : 500.



UTM Zone 10 Coordinates	
Datum: NAD83(CRS)4,0,0,BC1,MWD	
Point Identifier: 77H4683	77H4689
UTM Northing: 5446188.50	5446187.61
UTM Easting: 480754.61	490604.94
Estimated horizontal positional accuracy: 0.01m	0.01m

LEGEND:
 Integrated survey area No. 18, Richmond, NAD83(CRS)4,0,0,BC1,MWD
 Grid bearings are derived from observations between
 geodetic control monuments 77H4683 and 77H4689.
 The accuracy of the grid bearings and the accuracy
 achieved are derived from the MASCOI published coordinates
 for geodetic control monuments 77H4683 and 77H4689.
 This plan shows horizontal ground-level distances unless
 otherwise noted. Distances are shown in metres, multiply
 of 0.099804, which has been derived from geodetic
 control monuments 77H4683 and 77H4689.
 ● denotes control monument found.
 ○ denotes standard from past found.
 ○ denotes standard from past plotted.

This plan lies within the jurisdiction of the
 Approving Officer for City of Richmond.

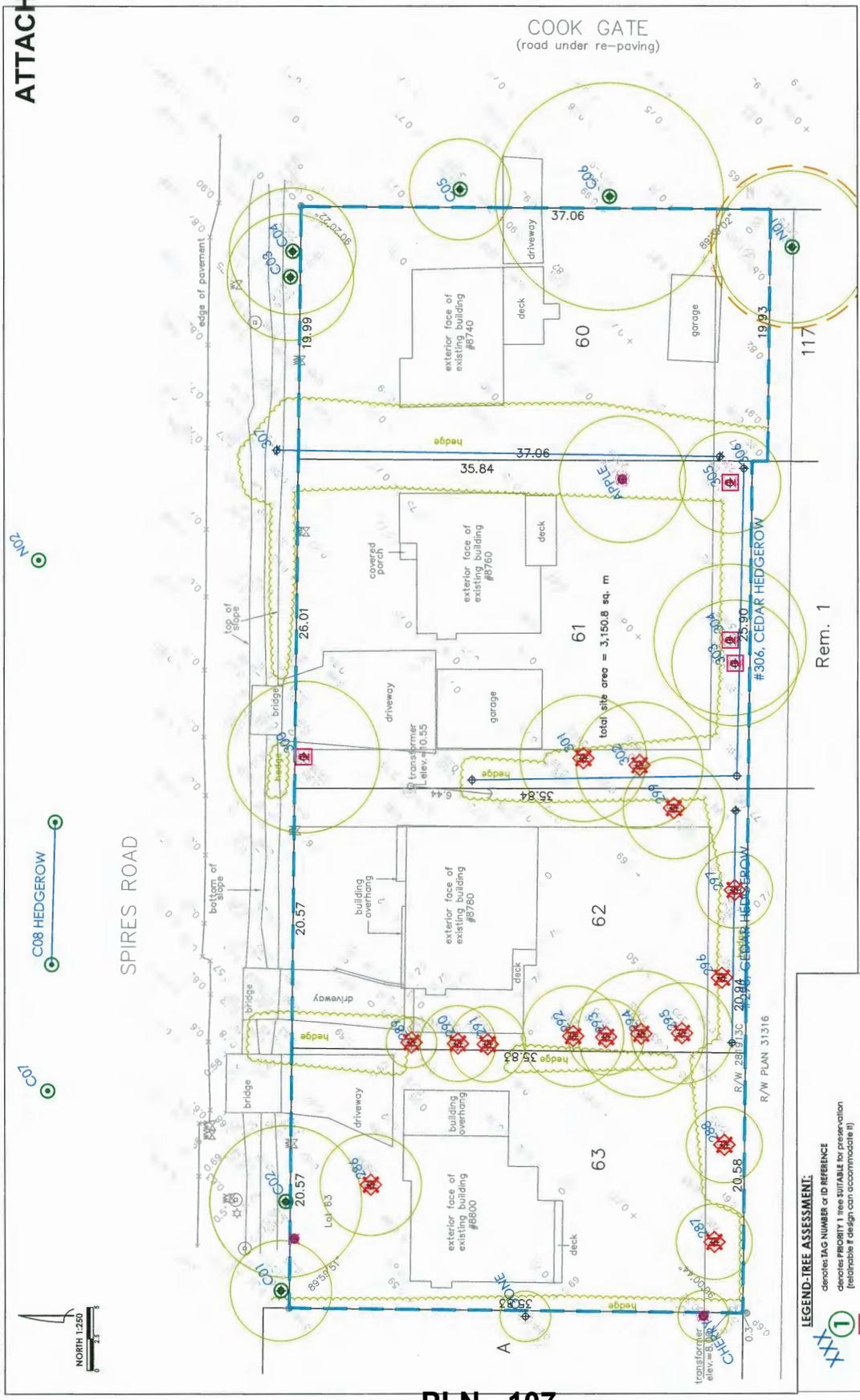
The field survey represented by this plan was
 conducted by the City of Richmond, 2021
 LOUIS NGAN, BOLS #743

This plan lies within the
 Metro Vancouver Regional District

L N L S METRO VANCOUVER
 LAND SURVEYORS
 1528 KINGSWAY, VANCOUVER, BC, V5N 2P9
 T 604.327.1535 WEB WWW.LNLS.CA

FILE:18860_04REF

ATTACHMENT 7



LEGEND - TREE ASSESSMENT:

- ① denotes TAG NUMBER or ID REFERENCE
- ① denotes PRIORITY 1 tree SUITABLE for preservation (retainable if design can accommodate it)
- ② denotes PRIORITY 2 tree MARGINAL for preservation, (possible candidate subject to design and other conditions) (not visible)
- ✗ denotes PRIORITY 3 tree UNSUITABLE for preservation (not visible)
- ③ denotes UNDERSIZE TREE that is smaller than bylaw defined size (consult municipality to determine if it is a bylaw protected tree)
- ④ denotes OFF-SITE tree within influencing distance (neighbour or city to be advised or consulted)
- ⑤ denotes PRELIMINARY TREE PROTECTION SETBACK (for planning and design consideration)

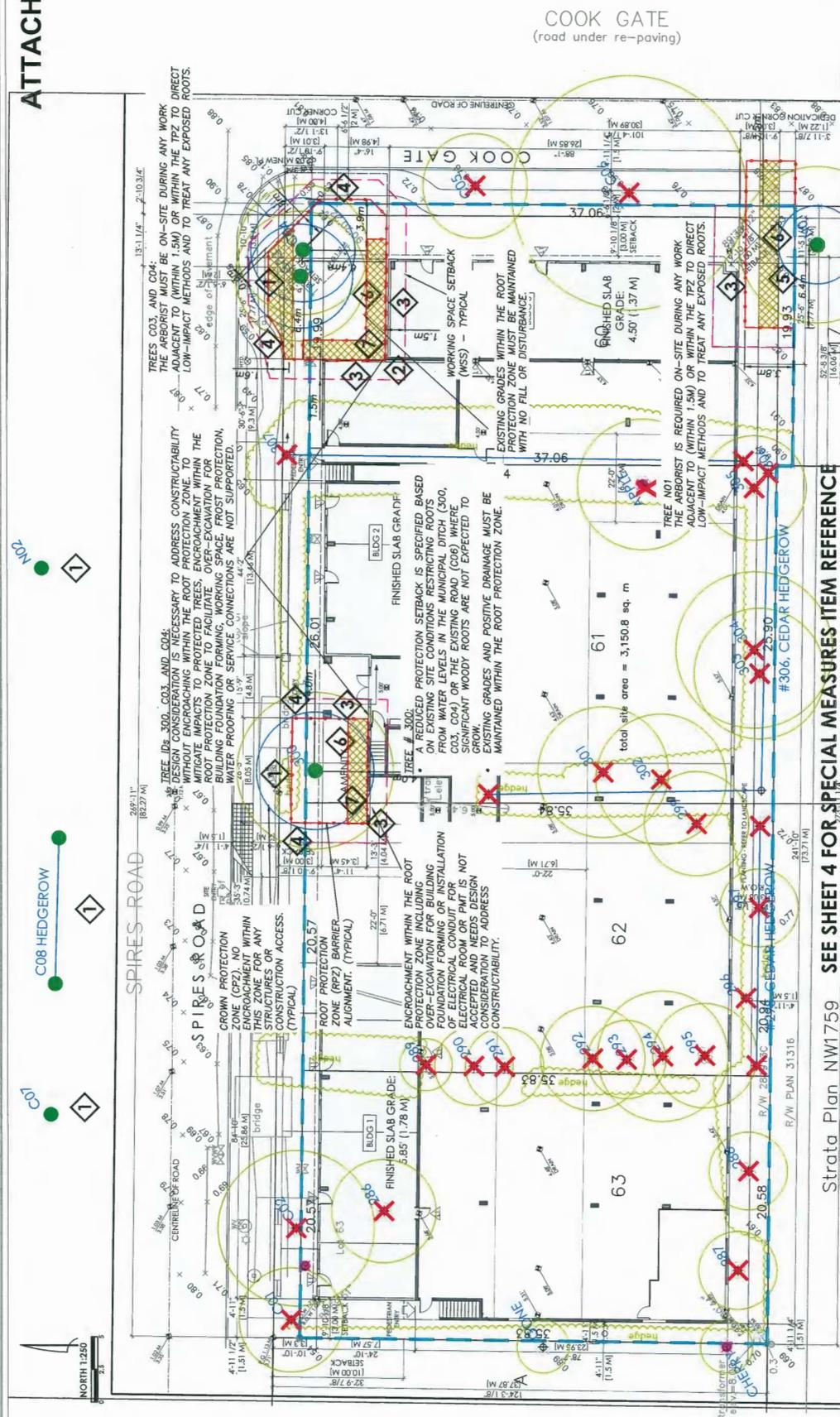
TREE MANAGEMENT DRAWING - CITY LETTER SIZE SHEET 1/4

PROJECT:	PROPOSED TOWNHOUSE DEVELOPMENT
ADDRESS:	8740-8800 SPIRES ROAD
CLIENT:	TERRA WEST PROPERTIES
CITY REF:	ACL FILE: 18331
PLOT SIZE:	11"X17" REV #: 7 DATE: FEB 4, 2022

oclgroup.ca

ARBORTECH CONSULTING
 A C I G R O U P
 SUITE 145-12051 HORSESHOE WAY, RICHMOND, BC V7A 4V4 604.275.9484

ATTACHMENT 7



COOK GATE
(road under re-paving)

Strata Plan NW1759 SEE SHEET 4 FOR SPECIAL MEASURES ITEM REFERENCE

LEGEND - TREE MANAGEMENT IN DEVELOPMENT AREA:

- See arborist report for further details.
- denotes TAG NUMBER or ID REFERENCE (see tree inventory and assessment list)
- denotes RETENTION tree (protection measures as specified)
- denotes INVESTIGATE TREE (not to be removed, retain, or remove, consult this office for protection measures. If to be removed, a permit may be required from municipality.)
- denotes REMOVAL tree (TAGGED TREE) (municipal permit or approvals may be required)
- denotes HIGH RISK tree to be REMOVED or MODIFIED (see tree inventory and report - permit or approvals required)

LEGEND - TREE PROTECTION:

- denotes CROWN PROTECTION ZONE - CPZ or original (furthest extent of foliage). Exclusion zone - no aerial encroachment of buildings, cranes, equipment, etc
- denotes ROOT PROTECTION ZONE - RPT as specified by project arborist. This is the alignment for TREE PROTECTION BARRIERS (dimensions to outside of trunk)
- denotes WORKING SPACE SETBACK - WSS (dimensions to outside of trunk) requires approval and on-site supervision by the Project Arborist
- denotes SPECIAL MEASURES required
- denotes REFERENCE # to tree protection specifications
- See REPORT or LOU for further details. Project Arborist to direct or implement.

TREES CO3, AND CO4: THE ARBORIST MUST BE ON-SITE DURING ANY WORK ADJACENT TO (WITHIN 1.5M) OF THE TREE TO DIRECT LOW-IMPACT METHODS AND TO TREAT ANY EXPOSED ROOTS.

TREE ID# 300, CO3, AND CO4: NECESSARY TO ADDRESS CONSTRUCTIBILITY DESIGN NOT ENCROACHING WITHIN THE ROOT PROTECTION ZONE TO MITIGATE IMPACTS TO PROTECTED TREES. ENCROACHMENT WITHIN THE ROOT PROTECTION ZONE TO FACILITATE OVER-EXCAVATION FOR BUILDING FOUNDATION FORMING, WORKING SPACE, FROST PROTECTION, WATER PROOFING OR SERVICE CONNECTIONS ARE NOT SUPPORTED.

FINISHED SLAB GRADE: A REDUCED PROTECTION SETBACK IS SPECIFIED BASED ON EXISTING SITE CONDITIONS RESTRICTING ROOTS FROM WATER LEVELS IN THE MUNICIPAL DITCH (300, CO3, CO4) OR THE EXISTING ROAD (CO8) WHERE PROMINENT WOODY ROOTS ARE NOT EXPECTED TO GROW. EXISTING GRADES AND POSITIVE DRAINAGE MUST BE MAINTAINED WITHIN THE ROOT PROTECTION ZONE.

EXISTING GRADES WITHIN THE ROOT PROTECTION ZONE MUST BE MAINTAINED WITH NO FILL OR DISTURBANCE.

TREE NOT TO BE REMOVED: THE ARBORIST IS REQUIRED ON-SITE DURING ANY WORK ADJACENT TO (WITHIN 1.5M) OR WITHIN THE RPT TO DIRECT LOW-IMPACT METHODS AND TO TREAT ANY EXPOSED ROOTS.

PROJECT:	PROPOSED TOWNHOUSE DEVELOPMENT
ADDRESS:	8740-8800 SPIRES ROAD
CLIENT:	TERRA WEST PROPERTIES
CITY REF:	JACL FILE: 18331
PLAT SIZE:	11'X17' REV # 7 DATE: FEB 4, 2022

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ARBORTECH CONSULTING
ACI GROUP
SUITE 145 - 12051 HORSESHOE WAY, RICHMOND, BC V7A 4V4 604.275.3484

TREE PROTECTION STANDARD MEASURES

The project arborist must be called to attend and review, approve, direct and/or supervise certain works from time to time during the demolition, site preparation, construction and landscaping, at critical milestones or activities. To schedule reviews or site visits as described above, we require a minimum of 3 business days advance notice.

- A. **Tree Protection Barrier Installation:**
Prior to site works commencing, to direct and inspect the installation of tree protection barriers. We will sign off once they are approved, so that city approvals can be enabled.
- B. **Tree Health Management Treatments:**
Prior to construction, the project arborist will undertake or direct the installation of soil protection and enhancement treatments where deemed necessary or appropriate, such as but not limited to:
 - a. Soil amendment may be applied within TPZ (i.e. 10mm-minus well composted bark mulch, Nutri+Mulch, or equivalent) to a depth of 100 mm (or as directed by the Project Arborist).
 - b. Interim watering program and system (i.e. manual sprinkler on a timer, temporary irrigation, or truck delivery) by developer/owner or by the project arborist. Note that log books of the watering may be required. The watering shall achieve even coverage within the RPZ to deliver the equivalent of up to 5 cm (2 inches) depth on a twice monthly schedule in April through June and September, and to a weekly schedule for July and August. Watering events will be exempted by the project arborist when natural rainfall for a period is sufficient to sustain the tree.
- C. **Access within TPZ:**
Whenever access into the tree protection zone (TPZ) is contemplated or desired for any reason.
- D. **Work within TPZ:**
Whenever any grading, trenching, excavation or landscape work occurs within a TPZ, including the root protection zone (RPZ) and the working space setback (WSS) offset from a RPZ as specified by the project arborist.
- E. **Pruning:**
Certain retained trees may require pruning as recommended by the project arborist to treat one or more of the following: restoration of form, aesthetics, mitigation of defects, building clearance, sight lines, crown raising clearance for vehicles or pedestrians and/or construction access. All tree pruning work is to be carried under the direction of the project arborist from this office and by an ISA Certified Arborist employed by a qualified tree service firm working in conformance with applicable ANSI standards (A300 and Z1.33), and meeting the city, contractor and/or developer/owner insurance and licensing requirements.
- F. **Low Impact Tree or Stump Removal:**
For any tree removal or stump removal from within a RPZ or WSS.
- G. **Landscape Finishing:**
All landscaping activities must be reviewed by this office in advance of commencing and on-site direction and guidance from the project arborist planned accordingly, such as but not limited to:
 - a. Preparation works and construction of landscape finishing works including but not limited to: sidewalks, paths, patios, decks, retaining walls, fencing, irrigation, conduit, benches, patio pavers, soil placement, grass or turf installation, planting or other landscape items.
 - b. Turf within TPZ's is discouraged, however if desired we may support it as long as a suitable mulch zone setback is implemented around the base of the tree.
 - c. Certain landscape features may be excluded or will require specific materials and methods to be utilized that meet tree root protection compliance requirements.
 - d. Note that the planting of any plants, shrubs or hedges within the TPZ is restricted to small pot sizes (i.e. #1 or #2 depending on species) and using "pocket planting" standards. Planting holes are minimized in size, dug into existing grade to avoid damage to woody roots, and backfilled with minimal addition of growing medium.

TREE PROTECTION SPECIAL MEASURES:

The following items within a TPZ require project arborist direction, treatment or supervisory monitoring. See the Tree Management Drawing for additional references to locations where special measures are required.

1. **Ditch Infill Works:**
Stripping and excavating for storm pipe and manhole installations, as well as backfilling the ditch, will require low impact methods to protect the roots growing along the slope of the ditch and otherwise within the TPZ. Supervision from the project arborist is required. The upper soil strata within the ditch is to be backfilled using growing medium meeting the specifications of the project arborist so that root regeneration is enabled. Upon completion of this work, tree protection barriers are to be expanded to protect the new growing space.
2. **Building Site Preparation:**
The project arborist must be on site concurrently with the excavation (site stripping) activities adjacent to the tree protection zones to identify tree roots, provide root protection measures and/or undertake root pruning treatments as necessary. Also when pre-load or fill placement is planned, the project arborist must be contacted to review any measures, including temporary retaining walls and/or drainage.
3. **Root Pruning for Site Excavation:**
The project arborist must be on site concurrently with any excavation adjacent to the tree protection zone to identify tree roots, provide root protection measures and/or undertake root pruning treatments as necessary. Over-excavation for shoring, forming of building foundations and installation of drainage or other infrastructure are not supported by this office. Special measures are required for construction (i.e., shot-crete, blind forming), to accept a proposed building aligned with the root protection zone.
4. **Root Pruning for Services Adjacent to TPZ:**
The project arborist must approve the method of excavation (i.e. excavator, hydro-vac, air-vac, air spade etc) and also must be on site concurrent with trenching to identify tree roots, provide root protection measures and/or undertake root pruning treatments as necessary.
5. **Fence Construction:**
The digging of fence posts and construction of a fence must be reviewed in advance by the project arborist. Low impact digging methods and siting of fence posts may require adjustment depending on the scope of root impacts.
6. **Landscape Works:**
Coordination with this office is required to review any proposed works (i.e. planting, installation of any growing medium or materials, pruning, excavation of any scope, installation of any hardscape features including irrigation, fencing and retaining walls), within and directly adjacent to the tree protection zone(s) to approve those works and to provide low impact method and material recommendations. Any hardscape feature proposed within root protection zones will require a no-excavation and no-compaction of existing soils for preparation and installation.
7. **Worker Access Within Root Protection Zone for Construction:**
Temporary soil armoring measures are required for worker access within the root zone during construction to mitigate soil compaction. An elevated boardwalk is required to be installed and maintained to accept limited and controlled worker access and egress within the root protection zone directly adjacent to the building. All other root protection zone specifications apply. Machine operation within root protection zones remains restricted.

ATTACHMENT 7

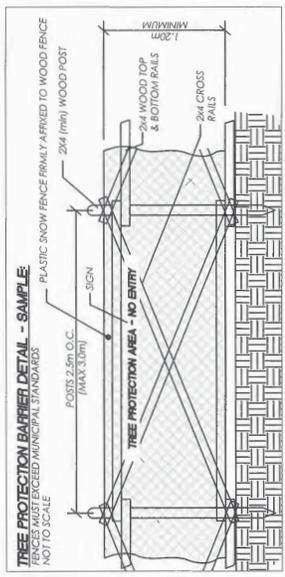
TREE MANAGEMENT DRAWING - CITY LETTER SIZE SHEET 4/4

PROJECT:	PROPOSED TOWNHOUSE DEVELOPMENT
ADDRESS:	8740-8800 SPIRES ROAD
CLIENT:	TERRA WEST PROPERTIES
CITY REF:	ACL FILE: 18331
PLOT SIZE:	11'X17' REV #: 7 DATE: FEB 4, 2022

arborgroup.ca



SUITE 145 - 12051 HORSESHOE WAY, RICHMOND, BC V7A 4V4 604.275.9494





Address: 8740, 8760, 8780 and 8800 Spires Road, and the surplus portion of the Spires Road road allowance **File No.:** RZ 19-870807

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 10357, the developer is required to complete the following:

1. Council approval of the road closure bylaw for a portion of Spires Road. The developer shall be required to enter into a purchase and sales agreement with the City for the purchase of the Land, which is to be based on the business terms approved by Council. The primary business terms of the purchase and sales agreement will be brought forward for consideration by Council in a separate report from the Manager, Real Estate Services. All costs associated with the purchase and sales agreement shall be borne by the developer.
2. Consolidation of all the lots into one development parcel (which will require the demolition of the existing dwellings).
3. Approximately 0.865 m road dedication along the entire Cook Gate frontage
4. Approximately 1.219 m lane dedication along the south property line of 8740 Spires Road.
5. A 4.0 m x 4.0 m corner cut road dedication at the northeast corner of the subject site (southwest corner of the Cook Gate / Spires Road intersection).
6. A 3.0 m x 3.0 m corner cut road dedication at the southeast corner of the subject site (northwest corner of the Cook Gate / east-west lane intersection).
7. Granting of a 1.5 m wide statutory right-of-way across the site's lane frontage (south property line) for the construction of a concrete/asphalt sidewalk/walkway. Any works essential for public access within the required statutory right-of-way (SRW) are to be included in the Servicing Agreement (SA). The design must be prepared in accordance with City specifications & standards and the construction of the works will be inspected by the City concurrently with all other SA related works. No fence is allowed to be installed within or along this SRW area except permitted by the City. The owners are responsible for all maintenance of improvements, including but not limited to the concrete/asphalt sidewalk/walkway, landscaping, signage and lighting installed within the SRW, and are responsible for all liability of SRW areas.
8. Granting of a 1.5 m wide statutory right-of-way along the entire west property line for the construction of a walkway. Any works essential for public access within the required statutory right-of-way (SRW) are to be included in the Servicing Agreement (SA). The design must be prepared in accordance with good engineering practice with the objective to optimize public safety and after completion of the works, the Owner is required to provide a certificate of inspection for the works, prepared and sealed by the Owner's Engineer in a form and content acceptable to the City, certifying that the works have been constructed and completed in accordance with the accepted design. No fence is allowed to be installed within or along this SRW area except permitted by the City. The owners are responsible for all maintenance of improvements, including but not limited to the walkway, landscaping, signage and lighting installed within the SRW, and are responsible for all liability of SRW areas.
9. Registration of a legal agreement on title stipulating that the development is subject to potential impacts due to other development that may be approved within the City Centre including without limitation, loss of views in any direction, increased shading, increased overlook and reduced privacy, increased ambient noise and increased levels of night-time ambient light, and requiring that the owner provide written notification of this through the disclosure statement to all initial purchasers, and erect signage in the initial sales centre advising purchasers of the potential for these impacts.
10. Registration of a restrictive covenant prohibiting (a) the imposition of any strata bylaw that would prohibit any residential dwelling unit from being rented; and (b) the imposition of any strata bylaw that would place age-based restrictions on occupants of any residential dwelling unit.
11. Registration of an aircraft noise sensitive use covenant on title.
12. Registration of a flood indemnity covenant on title.

13. Registration of a legal agreements on Title ensuring that:
 - a) No final Building Permit inspection is granted until three secondary suites are constructed on site, to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw; and
 - b) The secondary suites cannot be stratified or otherwise held under separate title.
14. Registration of a legal agreement on title ensuring that:
 - a) the number of visitor parking stalls per zoning bylaw requirements will be maintained in perpetuity;
 - b) selling, leasing, assigning, or designating any of the visitor parking spaces to individual unit owners/renters/occupants or any other persons by the developers/applicants/owners and future strata councils is prohibited; and
 - c) the required visitor parking stalls are available for the common use of visitors to this development and are accessible to visitors at all times.
15. Registration of a legal agreement on title:
 - a) ensuring that where two parking spaces are provided in a tandem arrangement both parking spaces must be assigned to the same dwelling unit; and
 - b) prohibiting the conversion of the tandem parking area into habitable space.
16. Registration of a legal agreement on title ensuring that a parking stall, with dimensions and arrangements/configurations in accordance to the accessible space requirements in Richmond Zoning Bylaw 8500, will be assigned to each of the basic universal housing units contained within the proposed townhouse development. No accessible parking signage or pavement markings will be required on these parking spaces.
17. Registration of a legal agreement on title ensuring that:
 - a) conversion of any of the bicycle storage areas/rooms within the parking structure into habitable space or general storage area is prohibited; and
 - b) all of the bicycle parking areas are available for shared common use and for the sole purpose of bicycle storage.
18. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site and off-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
19. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
20. City acceptance of the developer's voluntary contribution in the amount of \$13,052.36 (i.e. \$0.31/ft² of buildable area, excluding affordable housing) to future City community planning studies, as set out in the City Centre Area Plan.
21. City acceptance of the developer's offer to voluntarily contribute \$8.50 per buildable square foot (e.g. \$357,887.40) to the City's affordable housing fund.
22. City acceptance of the developer's offer to voluntarily contribute \$0.87 per buildable square foot (e.g. \$36,630.83) to the City's public art fund.
23. City acceptance of the developer's offer to voluntarily contribute \$7,000 to Parks Division's Tree Compensation Fund for the removal of four trees (specifically tag# C01, C02, C05, C06) and untagged hedges, located along Spires Road frontage of the site.
 Note: Developer/contractor must contact the Parks Division (604-244-1208 ext. 1342) four (4) business days prior to the removal to allow proper signage to be posted. All costs of removal and compensation are the responsibility borne by the applicants.
24. City acceptance of the developer's offer to voluntarily contribute \$17,250 to the City's Tree Compensation Fund for the planting of 23 replacement trees within the City. If additional replacement trees (over and beyond the 19 replacement trees as proposed at the rezoning stage) could be accommodated on-site (as determined at Development Permit stage), the above cash-in-lieu contribution may be reduced in the rate of \$750 per additional replacement trees to be planted on-site.
25. Submission of a Tree Survival Security to the City in the amount of \$10,000 for the 37 cm caliper English Walnut trees to be retained.

26. City acceptance of the developer's offer to voluntarily contribute \$156,936.00 towards the construction of a new 600mm storm sewer via the capital project works that front the development.
27. The submission and processing of a Development Permit* completed to a level deemed acceptable by the Director of Development.
28. Enter into a Servicing Agreement* for the design and construction of frontage improvement works along the site frontages. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to, Water Works:

- a. Using the OCP Model, there is currently 254 L/s of water available at a 20 psi residual at the Spires Road frontage. Based on your proposed development, your site requires a minimum fire flow of 220 L/s.
- b. At the developer's cost, the City is to:
 - Install one new water service connection off of the existing 200mm water main along Spires Road complete with meter and meter box in a right of way which will be provided by the developer. The dimensions and location of the right of way shall be finalized through the servicing agreement process.
 - Remove all existing water service connections.
 - Provide new hydrants to meet City Engineering specifications or relocate existing fire hydrant as required to accommodate the proposed frontage improvements. Fire department approval is required for all fire hydrant relocations.

Storm Sewer Works:

- a. At the developer's costs, the developer is required to:
 - Install a new 750mm diameter storm sewer at the north-south aligned Spires Road from the north property line of 8931 Spires Road to 22 meters south. Tie-in to the south shall be to the new manhole at the bend at Spires Road that will be built through the City's Capital project. An appropriately sized manhole is required at the north end. Exact alignment of the new storm sewer in the roadway shall be determined via the Servicing Agreement process.
 - Install a new 750mm diameter storm sewer from the new manhole in Spires Road at the north property line of 8931 Spires Road then tie-in to the existing ditch fronting 8951 Spires Road. Tie-in to the existing ditch shall be via a headwall.
 - Install a new 600mm diameter storm sewer at the Cook Gate frontage from the south property line of 8740 Spires Road to 51 meters north. The south end of the new storm sewer shall have a manhole and tie-in to the north shall be to the existing storm sewer in Spires Road through a new manhole. Exact alignment of the new storm sewer in Cook Gate shall be determined via the Servicing Agreement process.
 - Install lane drainage (200mm diameter) along the entire south property line complete with manholes as per Engineering specifications then tie-in to the new manhole at Cook Gate.
- b. At the Developers cost, the City is to:
 - Install a new storm service connection complete with inspection chamber connecting to the new 750mm storm sewer along Spires Road

Sanitary Sewer Works:

- a. At the developer's costs, the developer is required to address the following:
 - If the new sanitary sewers in Spires Road and Cook Gate are still not connected to the new Eckersley sanitary pump station at the time site preparations (e.g., preload, soil densification, etc.) or sanitary connection is required by the development, the developer shall construct temporary sanitary mains and manholes along Spires Road and Cook Gate then tie-in to the existing sanitary manhole that is located mid-block at Cook Gate. The temporary sanitary mains and manholes are intended to divert to the existing sanitary system the sanitary flows from the northern portion of the Spires neighbourhood that flow through the proposed site. This requirement is subject to the existing sanitary system having adequate capacity to accommodate the proposed site which will be determined through a capacity analysis. If required, the developer shall provide cash contribution to the City for the filling and abandonment of the temporary sanitary line which will be

done when the ultimate sanitary mains in the streets are operational. The cash contribution, if required, shall be determined through the servicing agreement process.

- If the existing sanitary system does not have adequate capacity as determined through the capacity analysis, the developer may have the option to complete the Capital Project works (e.g., construction of the Eckersley sanitary pump station and the last section of sanitary main that ties-in to the new pump station) that are required to service their development and enter into a latecomer agreement.
 - Connect the existing sanitary main that conveys flows from the northern portion of the Spires neighbourhood to the new manhole in front of 8760 and 8780 Spires Road when the new sanitary lines in Spires Road and Cook Gate are connected to the new Eckersley B pump station.
 - Discharge the existing utility right of way along the common property line of 8760 and 8780 Spires Road after the new sanitary mains along Spires Road and Cook Gate are operational and the new sanitary mains are connected to the new Eckersley B sanitary pump station.
 - The developer shall not start with the site preparation works (e.g., preload, soil densification, etc.) until the new sanitary lines in Spires Road and Cook Gate are operational and connected to the new Eckersley B pump station or the temporary sanitary lines to divert flows from the northern portion of Spires neighborhood to the existing sanitary system are completed. If the Developer wishes to proceed with the proposed development's site preparation works prior to completion of the Capital Project works (e.g., construction of the Eckersley sanitary pump station and the last section of sanitary main that ties-in to the new pump station), the developer may have the option to complete the Capital Project and enter into a latecomer agreement or build the temporary sanitary lines first. The option to utilize temporary sanitary lines prior to preload installation is subject to Council approving the 2020 Capital program.
 - Design the proposed development to accommodate future access, maintenance, repair or replacement of the existing sanitary sewer along the entire south property line of the proposed development without impact to the development site, to the satisfaction of the City.
 - Provide a pre and post pre-load and construction surveys and CCTV of the existing sanitary sewer along the entire south property line. Any damage to be repaired and any required replacement shall be done at the Developer's sole cost.
 - Ensure that the existing sanitary sewer along the entire south property line of the proposed development remains operational during any preload and/or construction phase (the sewer will remain active despite new works proposed for Spires Road). If the existing sanitary line is impacted during site preparation or construction of the proposed development then the developer shall be responsible to make the damaged sanitary system operational in the duration of the onsite works (i.e., temporary bypass via pumping, etc.). The damaged sanitary system shall be replaced at the same alignment through the servicing agreement, at the developer's costs, after completion of the site preparation and/or building construction works.
 - Ensure no soil fill or building encroaches into the existing sanitary right of way along the south property line of the proposed development.
 - Provide a signed and sealed geotechnical assessment, complete with recommendations to ensure the following conditions are met. The assessment and mitigation recommendations shall be included in the rezoning staff report and the development process design review.
 - That the City be able to construct, maintain, operate, repair, or remove City utilities/infrastructures (i.e. sanitary main along the south property line) without impact to the onsite works. The building edge shall be set based on the required clearance between the building edge and the edge of the existing sanitary main as recommended by a professional geotechnical engineer.
 - That the on-site works (e.g. soil densification, preload, foundation works, etc.), or the construction/maintenance of the proposed building, not cause damage to the existing sanitary main along the south property line. Impact of the site preparation works (e.g., soil densification, pre-load, foundation excavation, dewatering, etc.) to the existing sanitary main needs to be determined by the Geotechnical Engineer. If the existing sanitary main will be significantly impacted, the works required to mitigate the impact or the replacement of the affected existing infrastructures need to be done prior to start of the site preparation works at developer's cost.
- b. At the developers cost, the City is to:

- Cut and cap at main all existing connections and remove inspection chambers along the south property line.
- Install a new sanitary service connection c/w inspection chamber. Ultimate tie-in shall be to the existing sanitary manhole fronting the common property line of 8760 and 8780 Spires Road.

Frontage Improvements:

a. Spires Road Development Frontage Improvements

- Road works and behind-the-curb frontage improvements - Spires Road along the entire frontage of the subject site is to be widened to provide 8.7 m wide pavement (one parking lane and two traffic lanes). The following are the road and behind-the-curb frontage improvement cross-section elements (measured from south to north) to be designed and constructed by the Developer. The existing ditches are to be filled to accommodate these frontage improvements.
 - Existing south property line of the road right-of-way along the development frontage.
 - 2.05 m wide landscaped boulevard with street trees. (The Spires Road right-of-way is planned to be reduced from a width of 20.1 m to 16.0 m. The 2.05 m wide boulevard may become surplus City land and available for disposition).
 - 2.0 m wide concrete sidewalk.
 - 1.5 m wide landscaped boulevard with street trees.
 - 0.15 m wide curb.
 - 8.7 m wide pavement.
 - (Note: For the edge of pavement treatment along the north side of the subject site's frontage, refer to the "Road widening considerations" section below for details).
- Road widening considerations
 - Scope of work - The existing pavement is to be widened to 8.7 m to provide two traffic lanes and one parking lane.
 - Design standards - The Developer is required to design the complete road cross-section of the fronting road, between the property lines of the road right-of-way, per TAC and City Engineering Design Specifications.
 - Edge of pavement and frontage treatments (north side of Spires Road) - The design is to include the edge of pavement and other frontage treatments along the north side of the subject site's Spires Road frontage. The design must show that the widened pavement can be supported structurally. Pavement support solutions may include in-filling existing ditches. Concrete barriers or other physical above-ground protection elements are not considered appropriate solutions.
 - Neighbourhood consultation - The Developer is to consult area residents along the north side of the subject site's frontage regarding changes to their driveways and other frontage treatments.
 - Road Functional design - The road functional design plans prepared as part of the rezoning application is considered preliminary and may have to be revised and finalized to account for design issues identified through the SA detailed design process. Refer to the "Road Functional Design Plan" section below for details regarding functional design requirements.
 - Approval - All road design and any required design changes are to be approved by Transportation and Engineering staff.
- Pavement transition works - Spires Road to the west of the subject site, as part of the redevelopment of 8820 to 8931 Spires Road, will be widened. In the event that the timing of the subject site is in advance of the westerly neighbouring site, the road works described above will need to include tie-in taper sections (20:1) to transition to meet the existing pavement to the west. At the east end of the subject site, similar tie-in taper sections are required across the Spires Road/Cook Gate intersection.
- Existing driveways along the Spires Road site frontage - All existing driveways along the Spires Road site frontage are to be closed permanently. The Developer is responsible for removing the existing driveways and the replacement with barrier curb/gutter, boulevards and concrete sidewalk per standards described under Section B (1) above.
- Existing driveways along the north side of the Spires Road site frontage - All existing driveways on the opposite side of the Spires Road development frontage are to be maintained during and post construction.

Consultation and co-ordination with adjacent property owners is required if their driveways are affected as part of the proposed road works.

- New development driveway
- Construct a new driveway to the site at the Spires Road site frontage. These design standards are to be followed: 6.7 m wide pavement at the property line, with 0.9 m flares at the curb and 45° offsets to meet existing grade of sidewalk/boulevard. The full 6.7 m wide driveway is to be maintained for a distance of 10.0 m (depth of the on-site loading area) measured from the fronting property line. The site plan is to show the driveway configuration with dimensions (in metric).
- Truck access to on-site loading area - A separate on-site loading area and truck access, located immediately next to the proposed driveway, is to be provided. These design standards are to be followed:
 - Minimum 3.25 m long rollover curb at the access to the loading area measured from the edge of the nearest driveway flare;
 - 10.0 m minimum depth measured from the property line;
 - Grass rete driving surface over the entire footprint of the loading area (3.25 m x 10 m); and
 - Back-in truck access only with on-site signage to prohibit back-out movements.
 - (Note: Confirm with Waste Management if these standards are adequate for garbage/recycle pick-up).
- Parks consultation - Consult Parks on the requirements for tree protection/placement including tree species and spacing as part of the frontage works.
- Engineering consultation - Consult Engineering on lighting and other utility requirements as part of the frontage works. The tree planting works will need to be coordinated with Engineering to ensure there are no conflicts with any above ground or underground utilities.

b. Cook Gate Development Frontage Improvements

- Road works and behind-the-curb frontage improvement - Cook Gate along the entire site frontage is to be widened to provide a 5.75 m wide pavement, measured from the existing center line of the road to the west. The road works are also to include a tie-in taper (20:1) to the existing section of Cook Gate to the south of the site. The following are the road and behind-the-curb frontage improvement. Any existing ditches are to be filled to accommodate these frontage improvements.
 - New west property line of the road right-of-way along development frontage.
 - 2.0 m wide concrete sidewalk.
 - 1.5 m wide landscaped boulevard with street trees.
 - 0.15 m wide curb.
 - 5.75 m wide pavement.
 - Existing center line of road.
 - (Note #1: The existing Cook Gate road right-of-way (17.07 m) is not adequate to support the required frontage improvements. A 0.865 m wide road dedication, along the entire Cook Gate frontage of the subject site is required).
 - (Note #2: 8611 Cook Road is undergoing rezoning, the Developer is required to confirm and ensure that the road dedications and the frontage improvement elements across the two properties fronting the west side of Cook Gate are consistent).
- Frontage improvement transition works - The road works are to include any required tie-in sections to existing pavement and behind-the-curb treatments to the south.
- Existing driveways along the Spires Gate frontage - All existing driveways along the Cook Gate site frontage are to be closed permanently. The Developer is responsible for removing the existing driveways and the replacement with barrier curb/gutter, boulevards and concrete sidewalk per standards described under Section C(1) above.
- Existing driveways on the opposite side of the Cook Gate site frontage - All existing driveways on the opposite side of the Cook Gate development frontage are to be kept during and post construction. Consultation and co-ordination with adjacent property owners is required if their driveways are affected as part of the proposed road works.
- Parks consultation - Consult Parks on the requirements for tree protection/placement including tree species and spacing as part of the frontage works.

- Engineering consultation - Consult Engineering on lighting and other utility requirements as part of the frontage works. The tree planting works will need to be coordinated with Engineering to ensure there are no conflicts with any above ground or underground utilities.
- c. Road Works - Spires Road/Cook Gate Intersection
- The road works at the southwest corner of the Spires Road/Cook Gate intersection are to include:
 - Intersection widening - Complete the pavement widening and curb/gutter works around the southwest corner of the intersection to meet the new pavement and curb/gutter to the west and south.
 - Transition works - The road widening of Spires Road at the site frontage is to include a transition section (20:1 taper) at the east side of the Spires Road/Cook Gate intersection.
 - Curb return radius - The minimum intersection corner curb return radius is to be set at 5.5 m. The final design corner return radius is to be determined through the road functional plan exercise taking into consideration emergency vehicle and truck movements.
 - Wheelchair access - Wheelchair ramps at the southwest corner of the intersection are to be provided.
- d. Peripheral Sidewalk/Walkway
- Two peripheral sidewalk/walkway are required as part of the frontage improvements of this site.
 - South sidewalk - The Developer is required to build a new 1.5 m wide concrete sidewalk across the entire south property line of the subject site. The new sidewalk is part of the future rear lane cross-section and is accommodated within an existing 3.0 m wide SRW. The elevation of the sidewalk is to take into consideration the elevation of the future lane and is to be confirmed with Engineering.
 - West walkway - The Developer is required to register a 1.5 m wide SRW along the west property line of the site. Together with a similar 1.5 m wide SRW registered on the neighbouring site to the west, the Developer is required to construct an asphalt concrete walkway over the total 3.0 m wide SRW. The cross-section of the walkway is to consist of a 1.5 m wide asphalt concrete surface with a 0.75 m wide swale for drainage along both edges of the walkway.
- e. Road Functional Design Plans
- The Developer is required to submit road functional design plans to show the road works and behind-the-curb frontage improvements described under Sections B to E above. The functional plans are to be approved by Transportation and Engineering.
 - Road functional design considerations
 - All road works are to be designed to meet City Engineering Design Specifications and TAC standards.
 - Road functional plans are required for all development frontages including Spires Road, Cook Gate, Spires Road/Cook Gate intersection, and peripheral sidewalk/walkway.
 - Full road and frontage improvement cross-sections (interim and ultimate) including edge of pavement treatments along the opposite side of the subject site's fronting roads. In particular, refer to the "Road widening considerations" section above for details on edge of pavement and other frontage treatments along the north side of the subject site's Spires Road frontage.
 - Provide 20:1 taper sections to tie-in the widened section of Spires Road and Cook Gate to the existing roadways as required.
 - The maximum cross slope for the boulevard and sidewalk is to be set at 2%.
 - The center line of the widened pavement is to follow the existing Spires Road and Cook Gate center lines.
 - (Note: 8611 Cook Road is undergoing rezoning, the Developer is required to confirm and ensure that the road dedications and frontage improvements along the two properties fronting the west side of Cook Gate are consistent).
 - Engineering consultation - As part of the review and approval process of the functional plan, Engineering is to be consulted on the following design issues, among other requirements.
 - Vertical alignment - The elevation of the centre line of Spires Road and Cook Gate along the development frontage is to take into considerations drainage requirements and to ensure there is no conflict with district energy equipment and other underground utilities.
 - Horizontal alignment - Consult Engineering to confirm that all underground utilities can be accommodated within the proposed road cross-sections.

- Approval - The road functional design plans prepared as part of the rezoning application is considered preliminary and may have to be revised and finalized to account for design issues identified through the SA detailed design process. All road design and any required design changes are to be approved by Transportation and Engineering
- f. At the developer's costs, the developer is required to coordinate with BC Hydro, Telus and other private communication service providers to undertake the following:
- To provide underground service lines and private utility lines for the proposed development along Spires Road and Cook Gate.
 - To coordinate the removal or relocation of the existing overhead lines along the south property line.
 - To provide the private utility companies (e.g., BC Hydro, Telus and Shaw) rights of ways in the proposed site to facilitate transition from the existing rear yard overhead private utility service to an underground service at Spires Road and Cook Gate frontages. The private utility servicing (i.e., transition from rear yard overhead service to underground service at the fronting streets) shall be coordinated with the private utility companies and the servicing plan showing such transition shall be included in the development process design review. The purpose of this is to ensure that all private utility above ground cabinets that are required to facilitate the transition from rear yard overhead system to underground system at the fronting streets are determined and placed onsite and the required rights of ways are secured via the Development Permit process.
 - To maintain BC Hydro and private communication services to the neighbouring properties that are connected to the existing rear yard overhead system if the rear yard overhead system is going to be removed when the new underground services are provided along the fronting streets.
 - To pre-duct for future hydro, telephone and cable utilities along all road frontages at Developer's cost.
 - When relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
 - To locate all above ground utility cabinets and kiosks required to service the proposed development within the developments site (see list below for examples). A functional plan showing conceptual locations for such infrastructure shall be included in the staff report and the development process design review. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements and the locations for the above ground structures. If a private utility company does not require an above ground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of SRWs that shall be shown in the functional plan and registered prior to SA design approval:
 - BC Hydro PMT – 4mW X 5m (deep)
 - BC Hydro LPT – 3.5mW X 3.5m (deep)
 - Street light kiosk – 1.5mW X 1.5m (deep)
 - Traffic signal kiosk – 2mW X 1.5m (deep)
 - Traffic signal UPS – 1mW X 1m (deep)
 - Shaw cable kiosk – 1mW X 1m (deep) – show possible location in functional plan
 - Telus FDH cabinet - 1.1mW X 1m (deep) – show possible location in functional plan
- g. Review the existing street lighting levels along Spires Road and Cook Gate frontages and upgrade lighting along the developments frontage.
- h. Complete other frontage improvements as per Transportation's requirements. The existing ditches along the north and south sides of Spires Road are to be filled to accommodate the road works and frontage improvements.
- i. A geotechnical assessment (complete with recommendations) is required to confirm that the existing road base structures are adequate to support the required road upgrades at Spires Road frontage.

General Items:

- a. At the developers cost, the Developer is required to:
- Not encroach (e.g., footing foundations, trees, patios, etc.) into the rear yard sanitary right of way. Please note fence along south property line should be a standard wooden fence.

- Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

Prior to a Development Permit* being forwarded to the Development Permit Panel for consideration, the developer is required to:

1. Complete an acoustical and thermal report with recommendations prepared by an appropriate registered professional, which demonstrates that the interior noise levels and noise mitigation standards comply with the City's Official Community Plan and Noise Bylaw requirements. The standard required for air conditioning systems and their alternatives (e.g. ground source heat pumps, heat exchangers and acoustic ducting) is the ASHRAE 55-2004 "Thermal Environmental Conditions for Human Occupancy" standard and subsequent updates as they may occur. Maximum interior noise levels (decibels) within the dwelling units must achieve CMHC standards follows:

Portions of Dwelling Units	Noise Levels (decibels)
Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

Prior to a Development Permit* issuance, the developer is required to complete the following:

1. Submission of a Landscaping Security based on 100% of the cost estimate provided by the landscape architect.

Prior to Demolition Permit Issuance, the developer must complete the following requirements:

1. Installation of appropriate tree protection fencing on site around all trees to be retained on adjacent properties prior to any construction activities, including building demolition, occurring on-site.

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
2. Incorporation of accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
3. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed

Date



To: Planning Committee
From: Wayne Craig
Director, Development

Date: February 18, 2022
File: RZ 21-936290

Re: **Application by Pakland Properties for Rezoning at 11720 Williams Road from the “Single Detached (RS1/E)” Zone to the “Compact Single Detached (RC2)” Zone**

Staff Recommendation

That Richmond Zoning Bylaw 8500, Amendment Bylaw 10359, for the rezoning of 11720 Williams Road from the “Single Detached (RS1/E)” zone to the “Compact Single Detached (RC2)” zone, be introduced and given first reading.

Wayne Craig
Director, Development
(604-247-4625)

WC/CL:blg
Att. 6

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Affordable Housing	<input checked="" type="checkbox"/>	

Staff Report

Origin

Pakland Properties (Khalid Hasan) on behalf of the property owners (Pavitar Sahota, Harpal Sahota, Hirdepal Sahota) has applied to the City of Richmond for permission to rezone 11720 Williams Road from the “Single Detached (RS1/E)” zone to the “Compact Single Detached (RC2)” zone, to permit a subdivision to create two lots with vehicle access from the rear lane. A location map and aerial photo are provided in Attachment 1.

A Development Application Data Sheet providing details about the development proposal is provided in Attachment 2.

Existing Site Condition and Context

A survey of the subject site is included in Attachment 3. The subject site is located on the south side of Williams Road, between Seacote Road and No. 5 Road. The subject site is currently accessed via a driveway crossing on Williams Road and there is also vehicle access to/from the existing rear lane.

Subject Site Existing Housing Profile

The subject site consists of a large lot containing a single-family dwelling that is occupied by the property owners. There are no secondary suites in the dwelling. The existing dwelling is proposed to be demolished at future development stage.

Surrounding Development

Existing development immediately surrounding the subject site is as follows:

- To the north, immediately across Williams Road are compact lots zoned “Compact Single Detached (RC1)”.
- To the south, immediately across a lane, is a large lot zoned “Single Detached (RS1/E)” fronting Seabrook Crescent.
- To the east, are compact lots zoned “Compact Single Detached (RC2)”, which were rezoned and subdivided in 2018 (RZ 16-740422/SD 16-740424).
- To the west, is a large lot zoned “Single Detached (RS1/E)”, which has rezoning and subdivision potential as it is designated for “Arterial Road Compact Lot Single Detached” in the Arterial Road Land Use Policy.

Existing Legal Encumbrances

There are existing statutory right-of-ways (SRWs) registered on title of the property for storm sewer, telecommunication and hydro infrastructure located along the east property line. The applicant has been advised that encroachment into the SRWs is not permitted.

Related Policies & Studies

Official Community Plan

The Official Community Plan (OCP) land use designation for the subject site is “Neighbourhood Residential”. This redevelopment proposal is consistent with this designation.

Single-Family Lot Size Policy 5434

The subject site is located within the area governed by Single-Family Lot Size Policy 5434, which was adopted by Council on February 19, 1990, and subsequently amended in 1991 and 2006 (Attachment 4). The Policy permits properties fronting Williams Road to rezone and subdivide in accordance with the “Compact Single Detached (RC2)” zone with vehicle access to the rear lane only.

This redevelopment proposal is consistent with the Lot Size Policy, as it would allow for the creation of two compact lots with vehicle access to the rear lane only.

Arterial Road Land Use Policy

The Arterial Road Land Use Policy identifies the subject site for “Arterial Road Compact Lot Single Detached”, which allows single detached housing on lots greater than 9.0 m wide provided that there is rear lane access. This redevelopment proposal is consistent with the Arterial Road Land Use Policy designation.

Affordable Housing Strategy

Consistent with the Affordable Housing Strategy, the applicant has proposed to construct a one-bedroom secondary suite in each of the new dwellings. Prior to final adoption of the rezoning bylaw, the applicant is required to register a legal agreement on title stating that no final Building Permit inspection will be granted until the secondary suites are constructed to the satisfaction of the City in accordance with the BC Building Code and the City’s Zoning Bylaw.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on title is required prior to final adoption of the rezoning bylaw.

Public Consultation

A rezoning sign has been installed on the subject property. Staff have not received any comments from the public about the rezoning application in response to the placement of the rezoning sign on the property.

Should the Planning Committee endorse this application and Council grant first reading to the rezoning bylaw, the bylaw will be forwarded to a Public Hearing, where any area resident or interested party will have an opportunity to comment.

Public notification for the Public Hearing will be provided as per the *Local Government Act*.

Analysis

Site Access

The subject site currently has a driveway crossing to/from Williams Road, which is required to be removed and re-instated with concrete sidewalk as part of the proposed development. In accordance with Residential Lot (Vehicular) Access Regulation Bylaw No. 7222, vehicle access from the proposed lots to Williams Road is not permitted, and vehicle access to/from the proposed lots is required to be from the rear lane.

Tree Retention and Replacement

There are no bylaw-sized trees on the subject property. The applicant has submitted a Certified Arborist's Report, which identifies off-site trees in close proximity to the subject site, assesses tree structure and condition, and provides recommendations on tree retention and removal relative to the proposed development. The Report assesses one bylaw-sized street tree in the boulevard on Williams Road on City-owned property and one undersized tree on the neighbouring property to the east at 11726 Williams Road.

The City's Tree Preservation Coordinator and Parks Department arboriculture staff have reviewed the Arborist's Report and supports the Arborist's findings, with the following comments:

- Tree tag # A located on City-owned property is in good health and fair condition. This tree has been pruned by BC Hydro for line clearance but has good vigour. The roots on the south side of the tree have been pruned for a previous City sidewalk replacement. This tree is to be retained and protected as per the Arborist's Report recommendations.
- Tree tag # B located on the neighbouring property to the east is to be retained protected as per the Arborist's Report recommendations.

The applicant has submitted a Tree Retention Plan showing the trees to be retained and the measures to be taken to protect them during development stage (Attachment 5). To ensure that the trees identified for retention are protected at development stage, the applicant is required to complete the following items:

- Prior to final adoption of the rezoning bylaw, submission to the City of:
 - A contract with a Certified Arborist for the supervision of all works conducted within or in close proximity to tree protection zones (Tree tags # A and B), including installation or removal of servicing infrastructure. The contract must include the scope of work required, the number of proposed monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the Arborist to submit a post-construction impact assessment to the City for review.
 - A tree survival security in the amount of \$10,000.00 for Tree tag # A. The security will be held until construction and landscaping on the subject site is completed and a landscape inspection has been passed by City staff.

The City may retain a portion of the security for a one-year maintenance period from the date of landscape inspection to ensure that the tree survives. To accompany the tree survival security, a legal agreement that sets the terms for release of the security must be entered into between the applicant and the City.

- Prior to demolition of the existing dwelling on the subject site, installation of tree protection fencing around all trees to be retained (Tree tags # A and B). Tree protection fencing must be installed to City standard in accordance with the Arborist's Report recommendations and the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and must remain in place until construction and landscaping on-site is completed.

Consistent with Zoning Bylaw regulations and the landscape guidelines for compact lots in the Arterial Road Land Use Policy, the applicant must plant and maintain two new trees per lot (a total of four trees). The City's Tree Preservation Coordinator has identified that the new trees must be a minimum 8 cm caliper for deciduous trees or 4 m high for coniferous trees.

To ensure that the two new required trees are planted and maintained on each lot proposed and that the front yards of the proposed lots are enhanced, the applicant is required to complete the following prior to final adoption of the rezoning bylaw:

- Submit a Landscaping Security and in the amount of \$3,000.00 (\$750.00/tree).
- Submit a Landscape Plan for the front yards prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development. The Landscape Plan must comply with the guidelines of the Arterial Road Land Use Policy in the OCP.
- Submit a Landscaping Security based on 100% of a cost estimate for the proposed Landscape Plan works provided by the Landscape Architect (including materials, installation, and a 10% contingency). The security will be held until construction and landscaping on-site is completed and a site inspection is conducted. The City may retain a portion of the security for a one-year maintenance period to ensure that the landscaping survives. To accompany the landscaping security, a legal agreement that sets the terms for release of the security must be entered into between the applicant and the City.

Site Servicing

At Subdivision stage, the applicant is required to pay:

- \$25,498.97 for cost recovery of rear lane upgrades constructed as part of a City Capital Works program (i.e., \$1,168.07 per metre of frontage) in accordance with Works and Services Cost Recovery Bylaw 8752.
- Cash to the City for future lane improvements in-lieu of upgrading the lane along the subject site's lane frontage in accordance with Subdivision and Development Bylaw 8751.
- Development Cost Charges (City and GVS & DD and TransLink), School Site Acquisition Charge, Address Assignment Fees, and other costs associated with completion of the water, storm, and sanitary servicing works as described in Attachment 6.

As previously identified, the existing driveway crossing to Williams Road must be removed and re-instated with concrete sidewalk as part of the proposed development. This work is to be undertaken via a City work order.

Financial Impact

This rezoning application results in an insignificant Operational Budget Impact (OBI) for off-site City infrastructure (such as roadworks, waterworks, storm sewers, sanitary sewers, street lights, street trees, and traffic signals).

Conclusion

This application is to rezone the property at 11720 Williams Road from the “Single Detached (RS1/E)” zone to the “Compact Single Detached (RC2)” zone to permit the property to be subdivided to create two lots, with vehicle access from the rear lane.

This rezoning application complies with the land use designations and applicable policies for the subject site that are contained within the OCP and it complies with Single-Family Lot Size Policy 5434.

The list of rezoning considerations is included in Attachment 6, which has been agreed to by the applicant (signed concurrence on file).

It is recommended that Richmond Zoning Bylaw 8500, Amendment Bylaw 10359 be introduced and given first reading.



Cynthia Lussier
Planner 2
(604-276-4108)

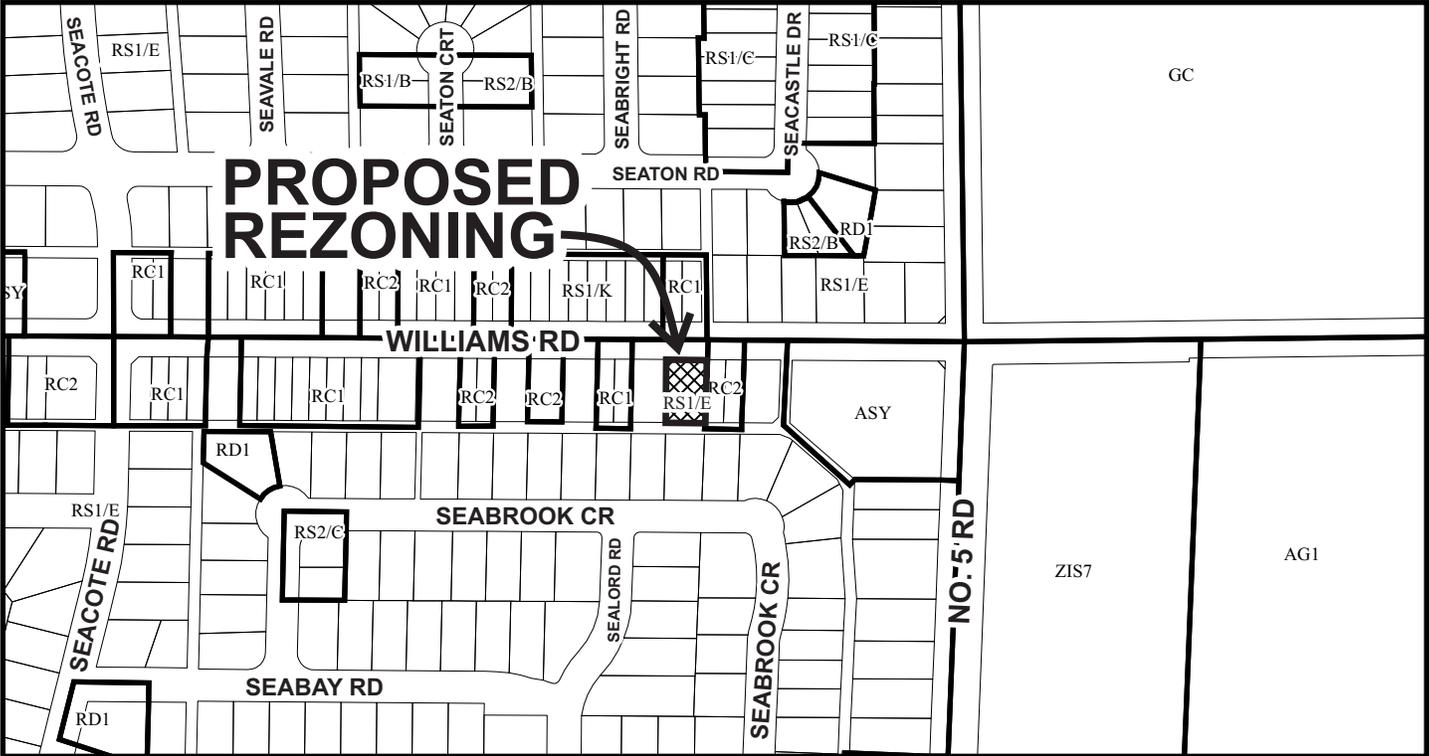
CL:blg

Attachments:

- Attachment 1: Location Map/Aerial Photo
- Attachment 2: Development Application Data Sheet
- Attachment 3: Site Survey
- Attachment 4: Single-Family Lot Size Policy 5434
- Attachment 5: Tree Retention Plan
- Attachment 6: Rezoning Considerations

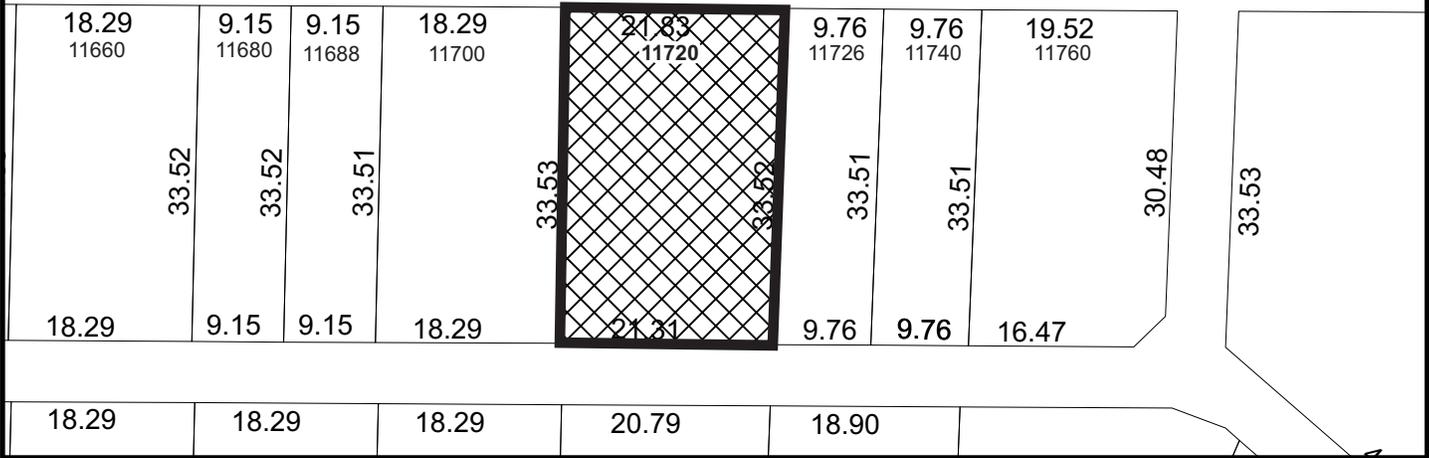


City of
Richmond



07	11633 10.07	11651 10.07	11653 10.07	11671 10.07	11677 10.07	11691 10.06	11699 10.07	11711 18.29	11731 18.29	11751 18.29
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WILLIAMS RD



RZ 21-936290

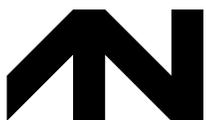
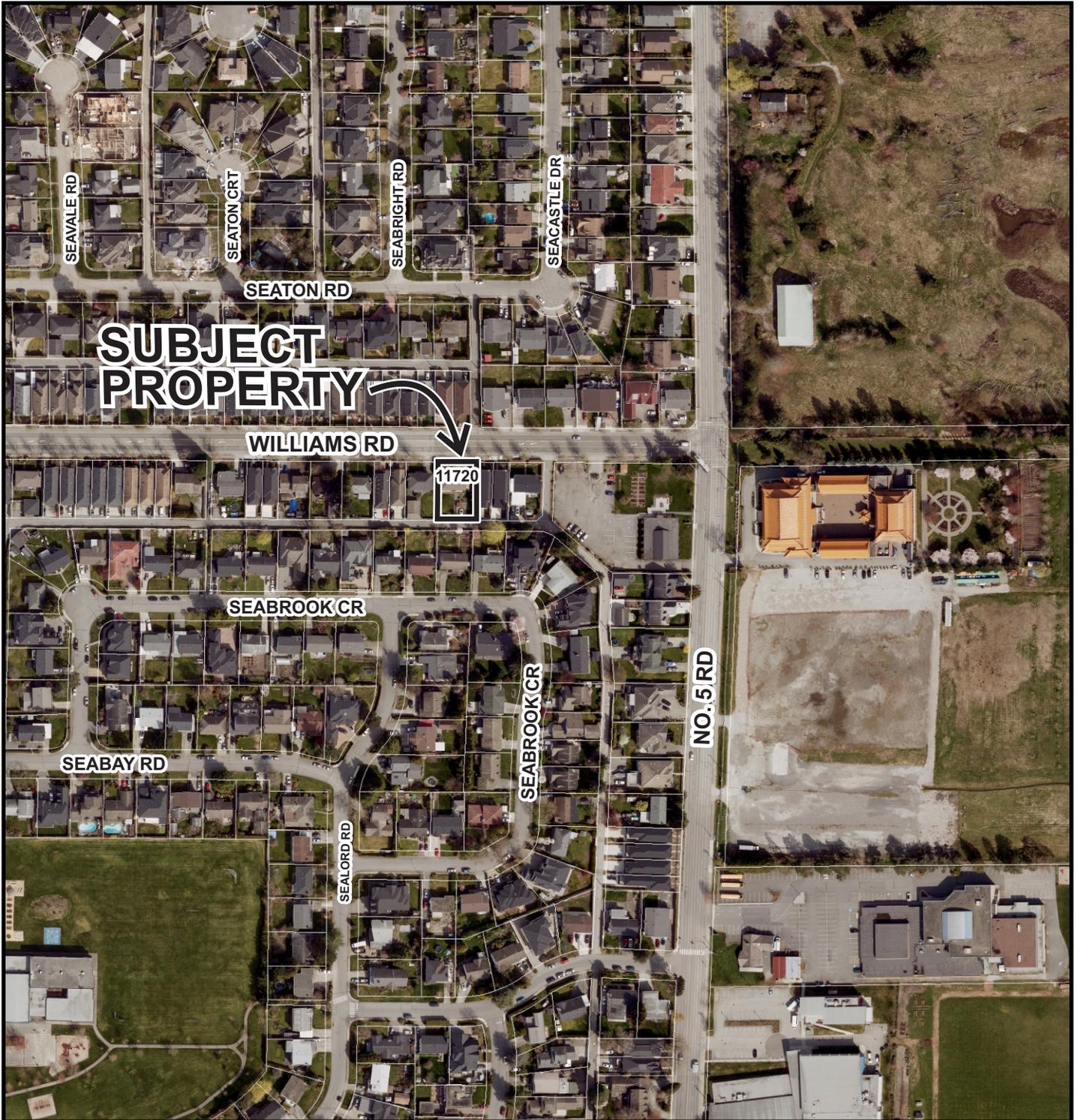
Original Date: 09/08/21

Revision Date:

Note: Dimensions are in METRES



City of
Richmond



RZ 21-936290

Original Date: 09/08/21

Revision Date:

Note: Dimensions are in METRES



RZ 21-936290

Address: 11720 Williams Road

Applicant: Pakland Properties

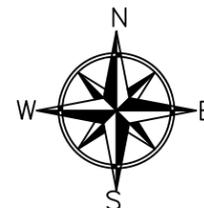
Planning Area(s): Shellmont

	Existing	Proposed
Owner:	Pavitar Sahota, Harpal Sahota, Hirdepal Sahota	To be determined
Site Size (m²):	722.5 m ² (7,776 ft ²)	West lot – 340.8 m ² (3,668 ft ²) East lot – 381.7 m ² (4,108 ft ²)
Land Uses:	Single-family dwelling	Two residential lots
OCP Designation:	Neighbourhood Residential	No change
Single-Family Lot Size Policy Designation:	Compact Single Detached (RC2) with rear lane access	No change
Zoning:	Single Detached (RS1/E)	Compact Single Detached (RC2)
Other Designations:	The Arterial Road Land Use Policy designates the subject site for redevelopment to “Arterial Road Compact Lot Single Detached”	No change

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.60	Max. 0.60	none permitted
Buildable Floor Area (m ²)*:	West lot: Max. 204.48 m ² (2,201 ft ²) East lot: Max. 228.96 m ² (2,464 ft ²)	West lot: Max. 204.48 m ² (2,201 ft ²) East lot: Max. 228.96 m ² (2,464 ft ²)	none permitted
Lot Coverage (% of lot area):	Building: Max. 50% Non-porous Surfaces: Max. 70% Live landscaping: Min. 20%	Building: Max. 50% Non-porous Surfaces: Max. 70% Live landscaping: Min. 20%	none
Min. Lot Size:	270 m ²	West lot – 340.8 m ² East lot – 381.7 m ²	none
Min. Lot Dimensions (m):	Width: 9.0 m	West lot – avg. 10.17 m East lot – avg. 11.39 m	none
	Depth: 24.0 m	33.5 m	
Setbacks (m):	Front: Min. 6.0 m Rear: Min. 6.0 m Side: Min. 1.2 m	Front: Min. 6.0 m Rear: Min. 6.0 m Side: Min. 1.2 m	none
Height (m):	2 ½ storeys	2 storeys	none
Parking Spaces	Principal dwelling	2 spaces	none
	Secondary suite	1 space	

* Preliminary estimate; not inclusive of garage; exact building size to be determined through zoning bylaw compliance review at Building Permit stage.

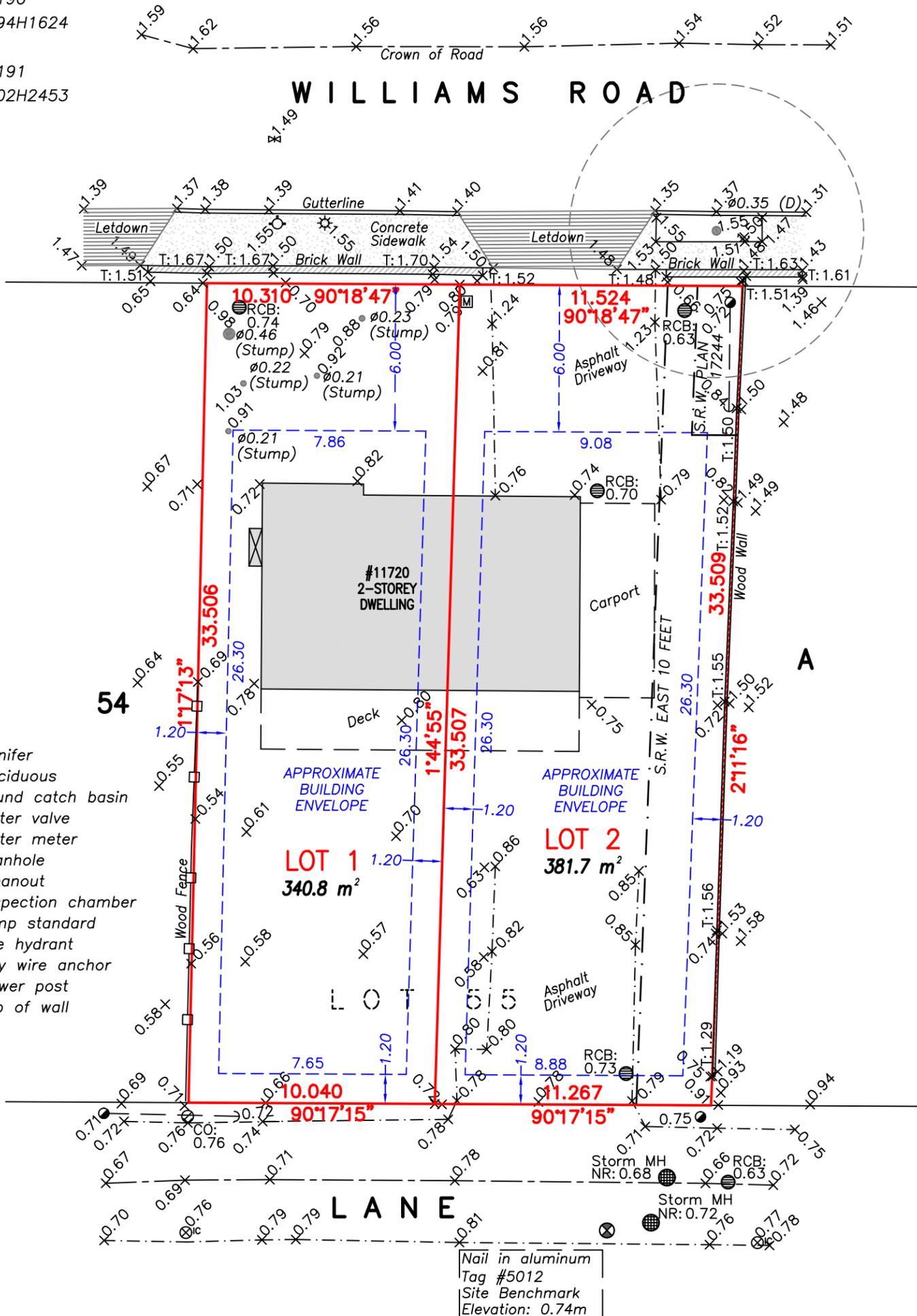
**TOPOGRAPHIC SURVEY AND PROPOSED SUBDIVISION OF
LOT 55 SECTION 36 BLOCK 4 NORTH RANGE 6 WEST
NEW WESTMINSTER DISTRICT PLAN 28788**



#11720 WILLIAMS ROAD,
RICHMOND, B.C.
P.I.D. 003-666-689

NOTE:

Elevations shown are based on
City of Richmond HPN
Benchmark network.
Benchmark: HPN #190
Control Monument 94H1624
Elevation: 2.353m
Benchmark: HPN #191
Control Monument 02H2453
Elevation: 1.664m



LEGEND:

- (C) denotes conifer
- (D) denotes deciduous
- denotes round catch basin
- ⊗ denotes water valve
- ⊠ denotes water meter
- ⊙ denotes manhole
- ⊕ denotes cleanout
- ⊗^c denotes inspection chamber
- ⊛ denotes lamp standard
- ⊕ denotes fire hydrant
- denotes guy wire anchor
- denotes power post
- T: denotes top of wall

© copyright
J. C. Tam and Associates
Canada and B.C. Land Surveyor
115 - 8833 Odlin Crescent
Richmond, B.C. V6X 3Z7
Telephone: (604) 214-8928
Fax: (604) 214-8929
E-mail: office@jctam.com
Website: www.jctam.com
Job No. 7621
FB-398 P19-20
Drawn By: WK

SCALE: 1:200



ALL DISTANCES ARE IN METRES AND DECIMALS
THEREOF UNLESS OTHERWISE INDICATED

NOTE:

Use site Benchmark Tag #5012 for
construction elevation control.

CERTIFIED CORRECT:
LOT DIMENSION ACCORDING TO
FIELD SURVEY.

JOHNSON C. TAM, B.C.L.S., C.L.S.
January 8th, 2021.

DWG No. 7621-Topo-02



City of Richmond

Policy Manual

Page 1 of 2

Adopted by Council: February 19, 1990
 Amended by Council: November 18, 1991
 Amended by Council: October 16, 2006

POLICY 5434

File Ref:

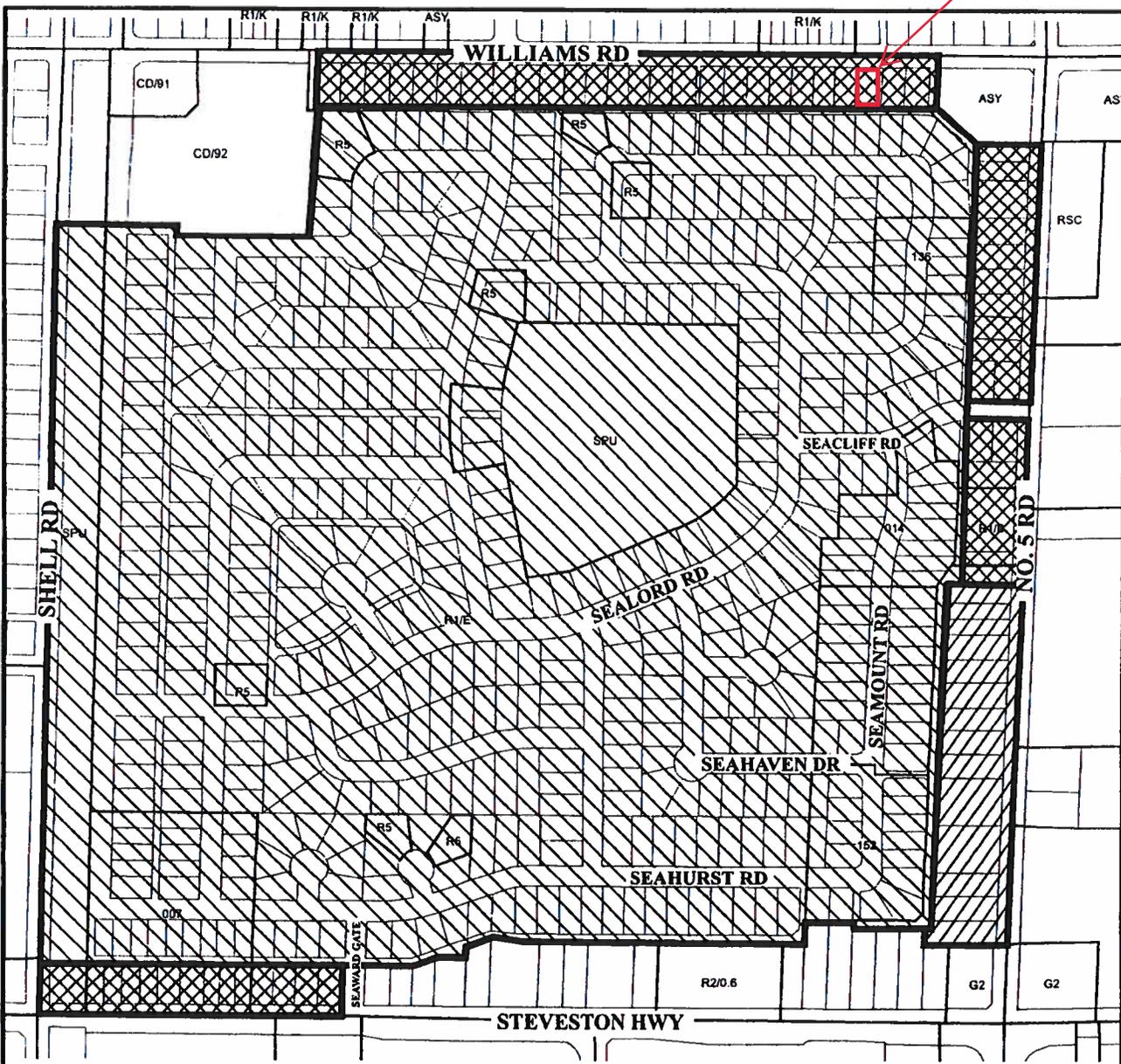
SINGLE-FAMILY LOT SIZE POLICY IN QUARTER-SECTION 36-4-6

POLICY 5434:

The following policy establishes lot sizes in a portion of Section 36-4-6, within the area bounded by **Steveston Highway, Shell Road, No. 5 Road, and Williams Road**:

1. That properties within the area bounded by Shell Road, Williams Road, No. 5 Road, and Steveston Highway, in a portion of Section 36-4-6, be permitted to subdivide in accordance with the provisions of Single-Family Housing District (R1/E), with the exception that:
 - a) Properties fronting on Williams Road from Shell Road to No. 5 Road, properties fronting on Steveston Highway from Seaward Gate to Shell Road, and properties fronting on No. 5 Road from Williams Road to approximately 135 m south of Seacliff Road to rezone and subdivide in accordance with the provisions of Single-Family Housing District (R1-0.6) or Coach House District (R/9) provided that vehicle accesses are to the existing rear laneway only. Multiple-family residential development shall not be permitted in these areas.
 - b) Properties fronting on No. 5 Road from Steveston Highway to approximately 135 m south of Seacliff Road be permitted to subdivide in accordance with the provisions of Single-Family Housing District, Subdivision Area B (R1/B) provided that vehicle accesses are to the existing rear laneway only.
2. This policy, as shown on the accompanying plan, is to be used to determine the disposition of future rezoning applications in this area, for a period of not less than five years, unless changed by the amending procedures contained in the Zoning and Development Bylaw.

Subject Site



-  Subdivision permitted as per **R1/E** (18 m wide lots)
-  Subdivision permitted as per **R1-0.6 or R/9**
(access to lane only) (No Multiple-family residential development is permitted.)
-  Subdivision permitted as per **R1/B**



Policy 5434
Section 36-4-6

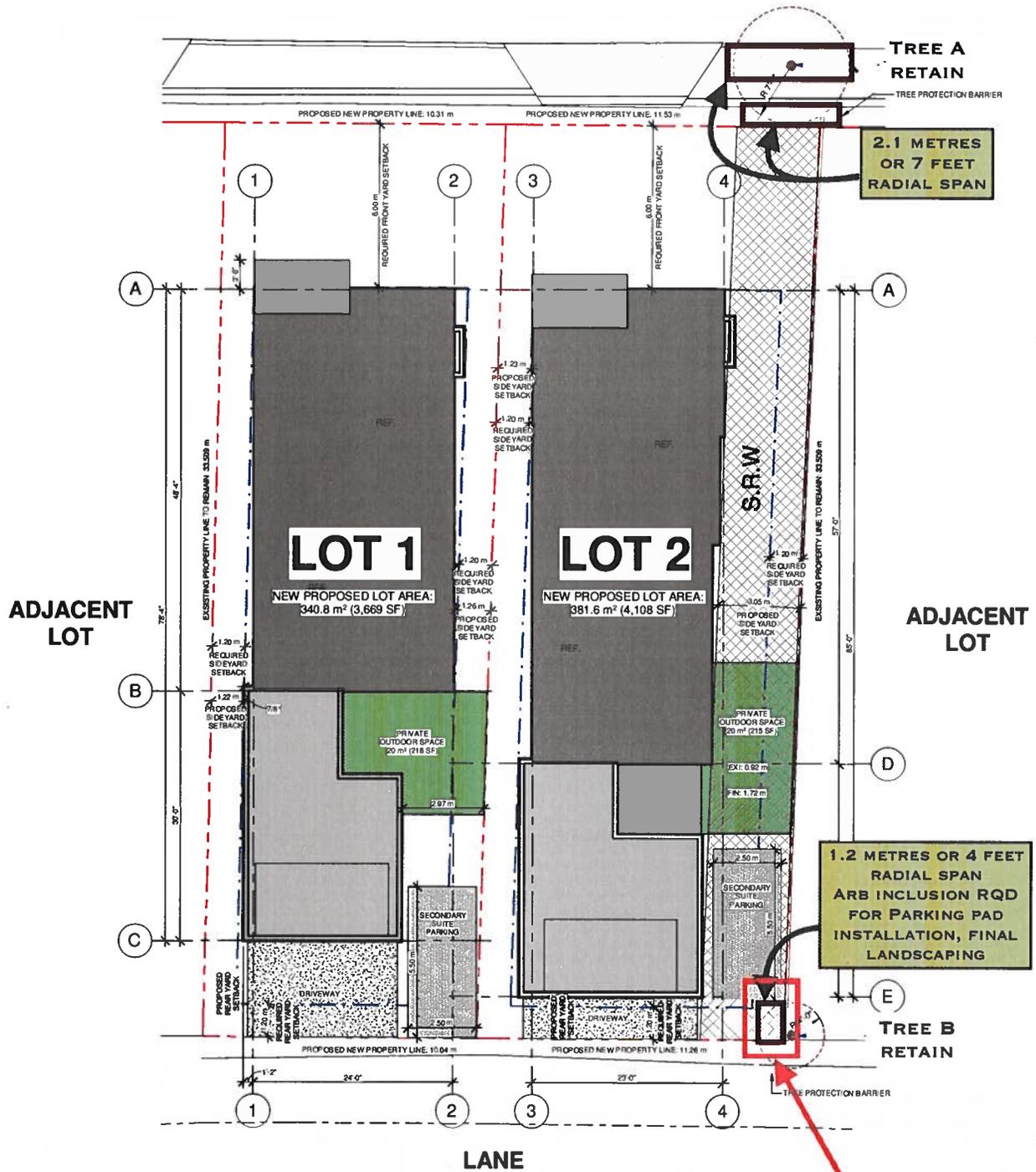
Adopted Date: 02/19/1990
Amended Date: 11/18/1991
10/16/2006

ARBORIST LEGEND

- # - TREE ID
- - TPB AREA
- # - TPB RADIAL ARB INCLUSION

REFER TO TPB AND TREE IMPACT SUMMARY TABLE WITHIN REPORT BODY

WILLIAMS ROAD



1 SITE PLAN TREE SURVEY
1/8" = 1'-0"

Note: This is a Tree Retention Plan only. The Site Plan/Floor Plan shown is to be revised and reviewed as part of the Building Permit application process.



Address: 11720 Williams Road

File No.: RZ 21-936290

Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 10359, the developer is required to complete the following:

1. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained (Tree tag # A and B), including (but not limited to) the installation or removal of servicing infrastructure. The Contract should include the scope of work to be undertaken, the proposed number of site monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the Arborist to submit a post-construction impact assessment report to the City for review.
2. Submission of a Tree Survival Security to the City in the amount of \$10,000 for Tree tag # A trees to be retained. To accompany the tree survival security, a legal agreement that sets the terms for release of the security must be entered into between the Applicant and the City.
3. Submission of a Landscaping Security in the amount of \$3,000 (\$750/tree) to ensure that a total of two trees are planted and maintained on each lot proposed (for a total of 4 trees); minimum 8 cm deciduous caliper or 4.0 m high conifers. To accompany the landscaping security, a legal agreement that sets the terms for release of the security must be entered into between the Applicant and the City.
4. Submission of a Landscape Plan, prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development, and deposit of a Landscaping Security based on 100% of the cost estimate provided by the Landscape Architect (including materials, installation, and a 10% contingency). The Landscape Plan should:
 - comply with the guidelines of the OCP's Arterial Road Policy and should not include hedges along the front property line;
 - include a mix of coniferous and deciduous trees;
 - include the dimensions of tree protection fencing as illustrated on the Tree Retention Plan attached to this report; and
 - include the 2 required trees (minimum 8 cm deciduous caliper or 4.0 m high conifers)To accompany the landscaping security, a legal agreement that sets the terms for release of the security must be entered into between the Applicant and the City.
5. Registration of a flood indemnity covenant on title.
6. Registration of a legal agreement on Title to ensure that no final Building Permit inspection is granted until a minimum one-bedroom secondary suite is constructed on each of the two lots proposed, to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw.

At Subdivision* stage, the following must be completed:

- pay \$25,498.97 for cost recovery of rear lane drainage upgrades constructed as part of a City Capital Works program (i.e., \$1,168.07 per metre of frontage) in accordance with Works and Services Cost Recovery Bylaw 8752.
- pay cash to the City for future lane improvements (e.g., 5.1 m wide pavement, roll curbs/gutters, and lane lighting) in-lieu of upgrading the lane along the subject site's lane frontage (consistent with the Subdivision and Development Bylaw 8751).
- pay Development Cost Charges (City and GVS & DD), School Site Acquisition Charge, Address Assignment Fees, and the costs associated with the completion of the required frontage works, and water, storm, and sanitary service connections. The works include, but are not limited to:

Water Works:

- Using the OCP Model, there is 677.0 L/s of water available at a 20 psi residual at the Williams Rd frontage. Based on your proposed development, your site requires a minimum fire flow of 95 L/s.
- The Applicant is required to coordinate with Richmond Fire Rescue to confirm whether fire hydrants are required along the proposed development's frontage. If required by Richmond Fire Rescue, the necessary water main and hydrant installations shall be reviewed by Engineering and added to the scope of water works.
- At the Applicant's cost, the Applicant is required to:
 - Cut and cap the existing water service connection at Williams Road;
 - Install a new water service connection complete with water meter on Williams Rd to service the proposed west lot.
 - Install a new water service connection complete with water meter on Williams Rd to service the proposed east lot. Place the water service connection and meter on the east lot where appropriate in order to accommodate protection of the City tree along the Williams Road frontage.
 - Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage building designs.
 - Review hydrant spacing on all road frontages and install new fire hydrants as required to meet City spacing requirements for the proposed land use.
 - Provide a statutory right-of-way (SRW) for the water meter. Minimum SRW dimensions to be the size of the meter box (from the City of Richmond supplementary specifications) plus any appurtenances (for example, the bypass on W2o-SD) + 0.5 m on all sides. Exact SRW dimensions to be finalized during the service connection works design.
- At the Applicant's cost, the City will complete all tie-ins for the proposed works to existing City infrastructure.

Storm Sewer Works:

- At the Applicant's cost, the Applicant is required to:
 - Provide an erosion and sediment control plan for all on-site and off-site works, to be reviewed as part of the service connection works design.
 - Cut and cap the existing storm service connections STCN28255, STCN28253, and STLAT100634, and STCN28254 along the north property line.
 - Remove Inspection chambers STIC60598, STIC48253, and STIC100543
 - Install a new 100 mm storm service connection at the adjoining property line of the proposed two properties, complete with inspection chamber and dual service leads.
- At the Applicant's cost, the City will complete all tie-ins for the proposed works to existing City infrastructure.

Sanitary Sewer Works:

- At the Applicant's cost, the Applicant is required to:
 - Not start onsite excavation or foundation construction until completion of rear-yard sanitary works by City crews.
 - Cut and cap existing sanitary line SCON30409. The existing inspection chambers SIC10341 shall be retained to service 11700 Williams Rd.
 - Install a new 100 mm sanitary service connection at the adjoining property line of the proposed two properties, complete with inspection chamber and dual service leads.
- At the Applicant's cost, the City will complete all tie-ins for the proposed works to existing City infrastructure.

Frontage Works:

- At the Applicant's cost, the Applicant is required to remove the existing driveway crossing to Williams Road, to re-instate the frontage with concrete sidewalk, and to repair any damaged/uneven sidewalk panels as necessary. This work is to be undertaken via a City work order.

Street Lighting:

- At the Applicant's cost, the Applicant is required to review street lighting levels along all road and lane frontages, and upgrade as required.

General Items:

- At the Applicant's cost, the Applicant is required to:
 - o Coordinate with BC Hydro, Telus and other private communication service providers:
 - To pre-duct for future hydro, telephone and cable utilities along all road frontages.
 - Before relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
 - To underground overhead service lines.
 - o Locate/relocate all aboveground utility cabinets and kiosks required to service the proposed development and proposed undergrounding works, and all aboveground utility cabinets and kiosks located along the development's frontages, within the development's site (see list below for examples). A functional plan showing conceptual locations for such infrastructure shall be included in the development design review process. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements (e.g., SRW dimensions) and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of SRWs that shall be shown on the architectural plans/functional plan as part of the service connection works design:
 - BC Hydro PMT – 4.0 x 5.0 m
 - BC Hydro LPT – 3.5 x 3.5 m
 - Street light kiosk – 1.5 x 1.5 m
 - Traffic signal kiosk – 2.0 x 1.5 m
 - Traffic signal UPS – 1.0 x 1.0 m
 - Shaw cable kiosk – 1.0 x 1.0 m
 - Telus FDH cabinet – 1.1 x 1.0 m
 - o Provide, prior to start of site preparation works a preload plan and geotechnical assessment of preload, dewatering, and soil preparation impacts on the existing utilities fronting the development site and provide mitigation recommendations.
 - o Provide a video inspection report of the existing utilities along the road frontages prior to start of site preparation works. A follow-up video inspection, complete with a civil engineer's signed and sealed recommendation letter, is required after site preparation works are complete (i.e. pre-load removal, completion of dewatering, etc.) to assess the condition of the existing utilities and provide recommendations to retain, replace, or repair. Any utilities damaged by the pre-load, de-watering, or other ground preparation shall be replaced or repaired at the Applicant's cost.
 - o Conduct pre- and post-preload elevation surveys of all surrounding roads, utilities, and structures. Any damage, nuisance, or other impact to be repaired at the developer's cost. The post-preload elevation survey shall be incorporated within the service connection works design.
 - o Monitor the settlement at the adjacent utilities and structures during pre-loading, dewatering, and soil preparation works per a geotechnical engineer's recommendations, and report the settlement amounts to the City for approval.

- Submit a proposed strategy at the Building Permit stage for managing excavation de-watering. Note that the City's preference is to manage groundwater onsite or by removing and disposing at an appropriate facility. If this is not feasible due to volume of de-watering, the Applicant will be required to apply to Metro Vancouver for a permit to discharge into the sanitary sewer system. If the sanitary sewer does not have adequate capacity to receive the volume of groundwater, the Applicant will be required to enter into a de-watering agreement with the City wherein the developer will be required to treat the groundwater before discharging it to the City's storm sewer system.
- Not encroach into City rights-of-ways with any proposed trees, retaining walls, or other non-removable structures. Retaining walls proposed to encroach into rights-of-ways must be reviewed by the City's Engineering Department.
- Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement.

Prior to Demolition Permit* issuance, the following must be completed:

- Installation of tree protection fencing around all trees to be retained (Tree tags # A and B). Tree protection fencing must be installed to City standard in accordance with the Arborist's Report recommendations and the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and must remain in place until construction and landscaping on-site is completed.

Prior to Building Permit* issuance, the following must be completed:

- Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. The Management Plan shall include location for parking for services, deliveries, workers, loading, application for any traffic lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
- If applicable, payment of latecomer agreement charges, plus applicable interest associated with eligible latecomer works.
- Obtain a Building Permit for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

* This requires a separate application.

- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

(signed concurrence on file)

Signed

Date



**Richmond Zoning Bylaw 8500
Amendment Bylaw 10359 (RZ 21-936290)
11720 Williams Road**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation of the following area and by designating it **“COMPACT SINGLE DETACHED (RC2)”**.

P.I.D. 003-666-689

Lot 55 Section 36 Block 4 North Range 6 West New Westminster District Plan 28788

2. This Bylaw may be cited as **“Richmond Zoning Bylaw 8500, Amendment Bylaw 10359”**.

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

OTHER CONDITIONS SATISFIED

ADOPTED

CITY OF RICHMOND
APPROVED by

APPROVED by Director or Solicitor


MAYOR

CORPORATE OFFICER



To: Planning Committee

Date: February 22, 2022

From: Wayne Craig
Director, Development

File: TU 22-005410

Re: Application by the City of Richmond for a Temporary Commercial Use Permit at 8620 and 8660 Beckwith Road

Staff Recommendation

- (1) That the application by the City of Richmond for an extension to Temporary Commercial Use Permit TU 18-841880 for the properties at 8620 and 8660 Beckwith Road to permit a “Parking, non-accessory” use be considered for three years from the date of issuance; and
- (2) That this application be forwarded to the April 19, 2022 Public Hearing at 7:00 p.m. in the Council Chambers of Richmond City Hall.

Wayne Craig
Director, Development
(604-276-4654)

WC/JR:blg
Att. 2

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Community Bylaws	<input checked="" type="checkbox"/>	
Finance	<input checked="" type="checkbox"/>	
Transportation	<input checked="" type="checkbox"/>	

Staff Report

Origin

The City of Richmond has applied for an extension to an existing Temporary Commercial Use Permit (TCUP) allowing non-accessory parking as a permitted use at 8620 and 8660 Beckwith Road on properties zoned “Light Industrial (IL)”. A location map and aerial photo are provided in Attachment 1.

Council issued the original TCUP (TU 18-841880) on May 21, 2019 for a three-year term expiring on May 21, 2022. The proposed TCUP extension would permit the City to continue operating a pay parking lot on the subject site for a period of three years from the date of issuance.

There is no proposed change to the layout or number of parking stalls permitted by the existing TCUP.

Findings of Fact

A Development Application Data Sheet providing details about the proposal is provided in Attachment 2.

Surrounding Development

The subject site is located in a transitioning area within the Bridgeport Village area of the City Centre. The property at 8620 Beckwith Road contains both parking associated with the automotive business at 2700 No. 3 Road and a portion of the City-operated pay parking lot, and the property at 8660 Beckwith Road was vacant before being used as a pay parking lot. These three properties are all owned by the City. Development immediately surrounding the subject site is as follows:

- To the north, across Beckwith Road: Industrial buildings on two properties zoned “Light Industrial (IL)”, and several vacant properties zoned “Light Industrial (IL)”.
- To the east, across Sexsmith Road: Industrial and commercial buildings on two properties zoned “Light Industrial (IL)”. One of the properties is included in an active rezoning application (RZ 16-740020). 8771, 8831, 8851 and 8811 Douglas Street are proposed to be rezoned for a hotel and office building. The application is currently under review, and a staff report will be submitted to the Planning Committee following completion of the staff review process.
- To the south: An industrial building at 2700 No. 3 Road on a property zoned “Light Industrial (IL)”, and a vacant property zoned “Light Industrial (IL)”. The vacant property is a former road parcel owned by the City of Richmond, which is currently used to access the rear lane. There are open drainage ditches on both sides of the lane through this property.

- To the west, across No. 3 Road: An industrial building on a property zoned “Light Industrial (IL)”, and a commercial building on a property zoned “Auto-Oriented Commercial (CA)”.

Related Policies & Studies

Official Community Plan/City Centre Area Plan – Bridgeport Village

The subject site is located in the Bridgeport Village area of the City Centre Area Plan (CCAP) and is designated as “Commercial” in the Official Community Plan (OCP). The site is also designated as “Urban Centre T5 (45 m)” on the Bridgeport Village Specific Land Use Map contained in the CCAP, which provides for a variety of commercial and institutional uses.

The OCP allows Temporary Commercial Use Permits to be considered in areas designated “Industrial”, “Mixed Employment”, “Commercial”, “Neighbourhood Shopping Centre”, “Mixed Use”, “Limited Mixed Use”, and “Agricultural” (outside of the Agricultural Land Reserve), where deemed appropriate by Council and subject to conditions suitable to the proposed use and surrounding area.

The proposed temporary use of the site for non-accessory parking is consistent with the land use designations and applicable policies in the OCP.

Richmond Zoning Bylaw 8500

The subject site is zoned “Light Industrial (IL)”, which permits a range of general industrial uses. The site was previously used for “commercial vehicle parking and storage”, which is a land use permitted in the zone. The proposed “parking, non-accessory” land use is not permitted in the zone, but is generally compatible with the surrounding land uses and the previous use of the property on an interim basis. “Non-accessory parking” describes parking that is not associated with a permitted use of the property.

Aircraft Noise Sensitive Development Policy

The subject site is located within “Area 1A – Restricted Area” of the Aircraft Noise Sensitive Development Policy, where new aircraft noise sensitive land uses are prohibited. Non-accessory parking is not an aircraft noise sensitive land use, and may be considered within this area.

Local Government Act

The *Local Government Act* states that Temporary Commercial Use Permits are valid for a period of up to three years from the date of issuance. An application for an extension to the Permit may be made and issued by Council for up to three additional years. Following this one time extension, a new TCUP application would be required.

Public Consultation

Should the Planning Committee and Council endorse the staff recommendation, the application will be forwarded to a Public Hearing on April 19, 2022, where any area resident or interested party will have an opportunity to comment. Public notification for the Public Hearing will be provided as per the *Local Government Act*.

Analysis

Operations

Council issued the original TCUP on May 21, 2019 for a period of three years. The construction and opening of the parking lot were paused due to COVID-19, as the demand for commuter long-term parking declined. The parking lot opened in August, 2021 and is managed by Community Bylaws as a paid parking lot for public use. A kiosk was installed on the site to accept payment for both long-term parking on the subject site and short-term parking on Beckwith Road and Sexsmith Road. Parking is regulated by Richmond Traffic Bylaw No. 5870 and Richmond Parking (Off-Street) Regulation Bylaw No. 7403.

The City has not received any complaints related to the operation of the parking lot.

Currently, the parking lot occupies 8660 Beckwith Road and the northeast corner of 8620 Beckwith Road. The remainder of 8620 Beckwith Road is used by the tenant at 2700 No. 3 Road, another City-owned property. The City may choose to expand the pay parking lot to a greater portion of 8620 Beckwith Road should demand for long-term parking increase.

Landscaping

The site preparation works for the parking lot included modest landscape improvements and removal of invasive species from the ditch. Nine new trees were planted in the Sexsmith Road frontage and wooden bollards surround the site. Concrete wheel stops indicate the location of parking stalls. No new landscaping is proposed through this application. Staff have visited the site and can confirm that the landscaping is in good condition.

Financial Impact

The annual Operating Budget Impact (OBI) cost for maintenance as included in the existing operating budget is estimated at \$5,500 per annum, and will be covered on an ongoing basis from the gross revenue generated by the parking lot cost shared between Community Bylaws and Real Estate Services.

Conclusion

It is recommended that the attached Temporary Commercial Use Permit be re-issued to the City of Richmond as an extension to TU 18-841880 to allow non-accessory parking at 8620 and 8660 Beckwith Road on a temporary basis for a period of three years.

A handwritten signature in black ink, appearing to read "J. Rockerbie". The signature is written in a cursive, flowing style.

Jordan Rockerbie
Planner 1
(604-276-4092)

JR:blg

Attachments:

Attachment 1: Location Map and Aerial Photo

Attachment 2: Development Application Data Sheet



City of Richmond



TU 22-005410

Original Date: 02/03/22

Revision Date:

Note: Dimensions are in METRES



TU 22-005410

Attachment 2

Address: 8620 and 8660 Beckwith Road

Applicant: The City of Richmond

Planning Area(s): City Centre – Bridgeport Village

	Existing	Proposed
Owner:	City of Richmond	No change
Site Size (m²):	2,268 m ² (24,412 ft ²)	No change
Land Uses:	Parking, non-accessory (permitted until May 21, 2022)	Parking, non-accessory
OCP Designation:	Commercial	No change
Area Plan Designation:	Urban Centre T5 (45 m)	No change
Zoning:	Light Industrial (IL)	No change

	Bylaw Requirement	Proposed	Variance
Off-street Parking Spaces – Total:	N/A	44	none
Off-street Parking Spaces – Standard:	Min. 50% (i.e. 22 spaces)	30	none
Off-street Parking Spaces – Small:	N/A	13	none
Off-street Parking Spaces – Accessible:	Min. 2% (i.e. 1 space)	1	none



No. TU 22-005410

To the Holder: CITY OF RICHMOND
Property Address: 8620 AND 8660 BECKWITH ROAD
Address: C/O 6911 NO. 3 ROAD
RICHMOND, BC V6Y 2C1

This Temporary Commercial Use Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.

1. This Temporary Commercial Use Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
2. The subject property may be used for the following temporary Commercial uses:
"Parking, non-accessory;" for a maximum of 44 spaces in accordance with Schedule "B"
3. Any temporary buildings, structures and signs shall be demolished or removed and the site and adjacent roads shall be maintained and restored to a condition satisfactory to the City of Richmond, upon the expiration of this permit or cessation of the use, whichever is sooner.
4. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached as Schedules "B" and "C" to this Permit which shall form a part hereof.
5. Any temporary buildings, structures and signs shall be demolished or removed and the site and adjacent roads shall be maintained and restored to a condition satisfactory to the City of Richmond, upon the expiration of this permit or cessation of the use, whichever is sooner.
6. This Permit is valid for a maximum of three years from the date of issuance.
7. This Permit is not a Building Permit.

To the Holder: CITY OF RICHMOND
Property Address: 8620 AND 8660 BECKWITH ROAD
Address: C/O 6911 NO. 3 ROAD
RICHMOND, BC V6Y 2C1

AUTHORIZING RESOLUTION NO. ISSUED BY THE COUNCIL THE
DAY OF , .

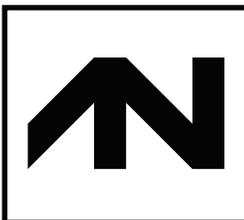
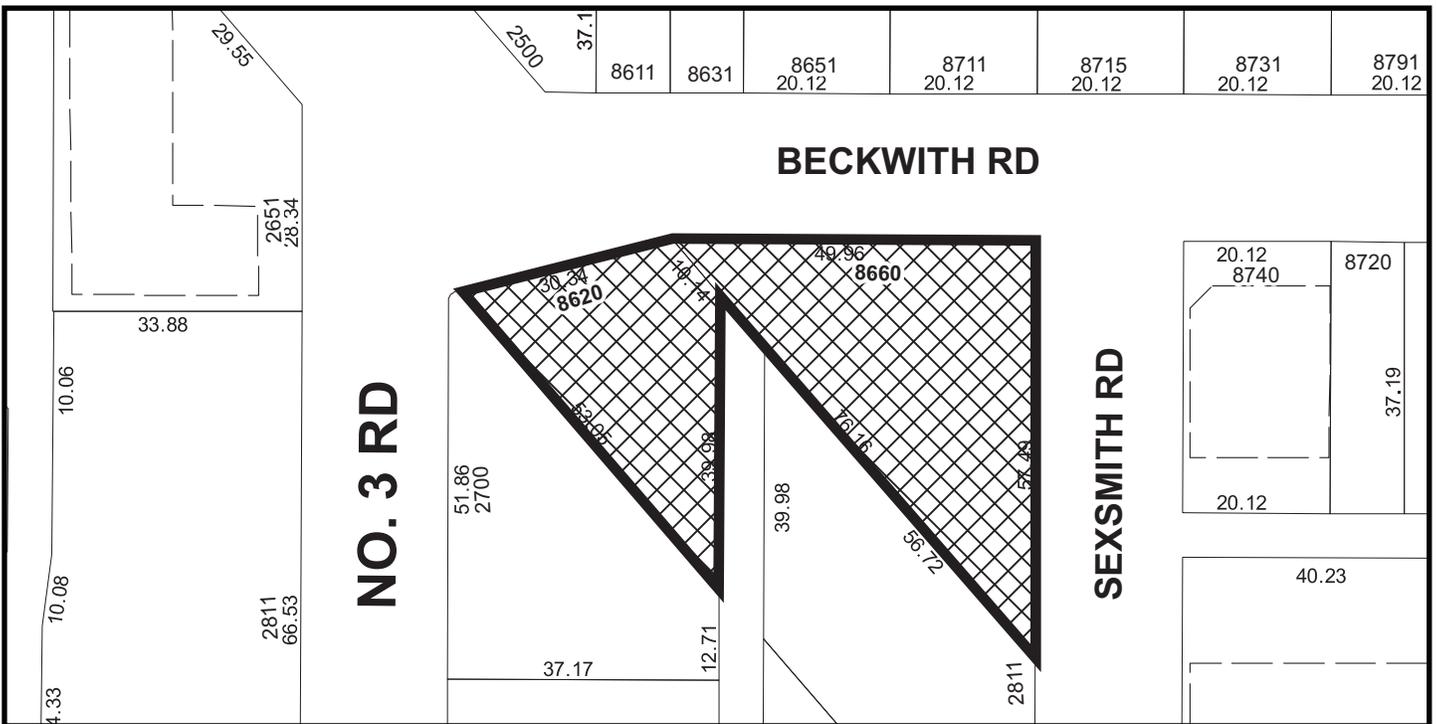
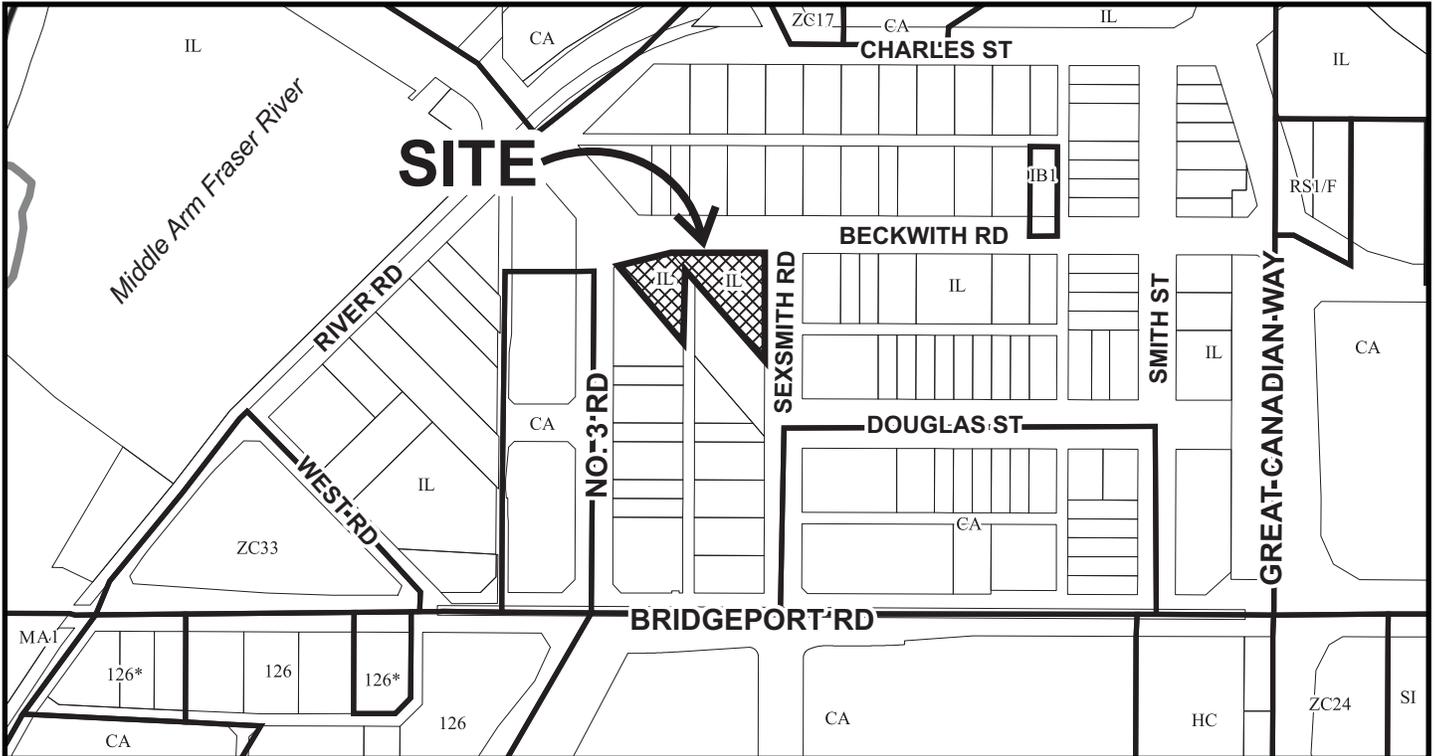
DELIVERED THIS DAY OF , .

MAYOR

CORPORATE OFFICER



City of
Richmond



TU 22-005410

Original Date: 02/03/22
 Revision Date:
 Note: Dimensions are in METRES

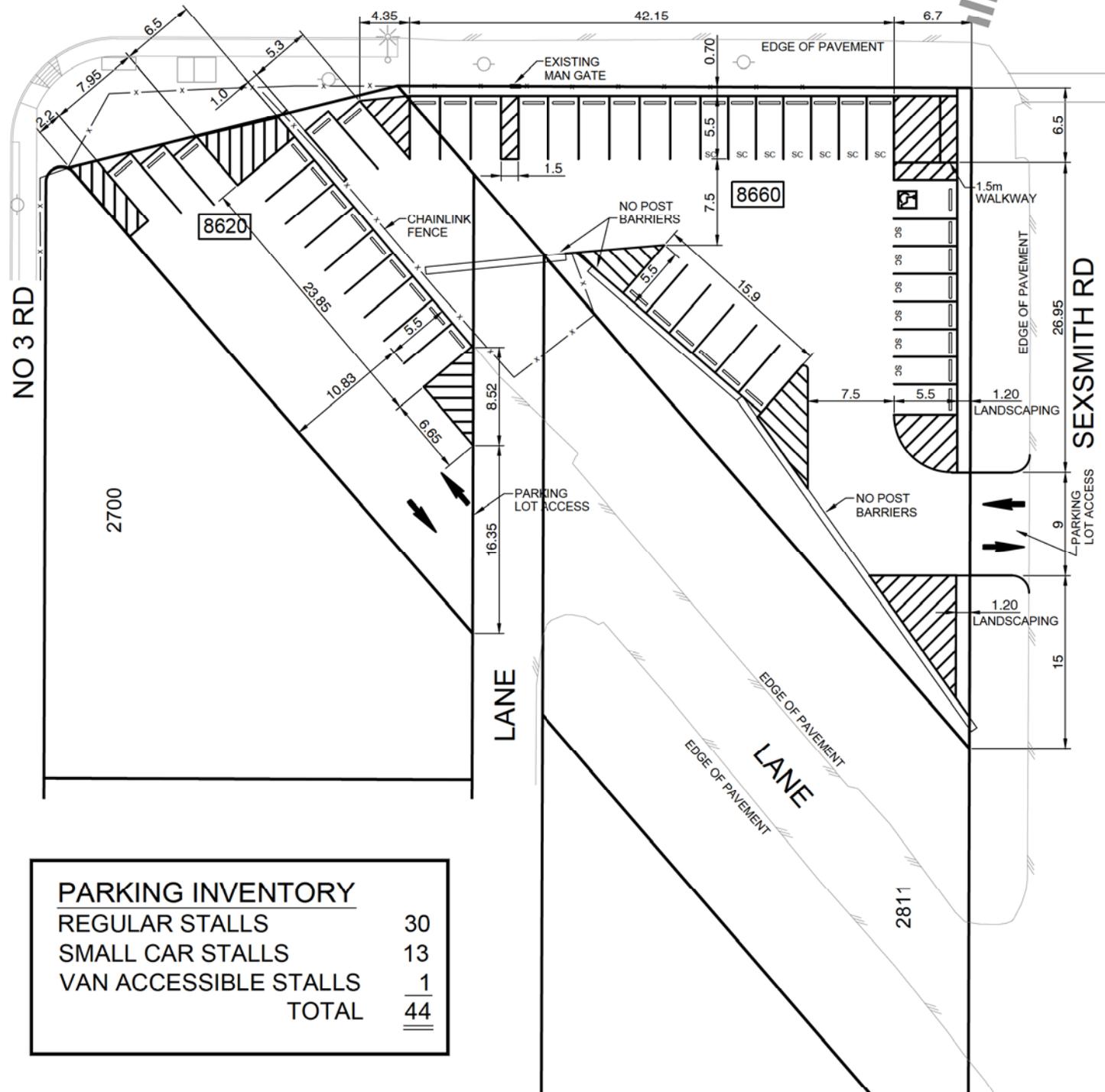
8611 8631 8651 8711



BECKWITH RD

TO BE CONSTRUCTED
UNDER SEPARATE
CAPITAL WORKS PROJECT

NEW MULTI-
USE PATH

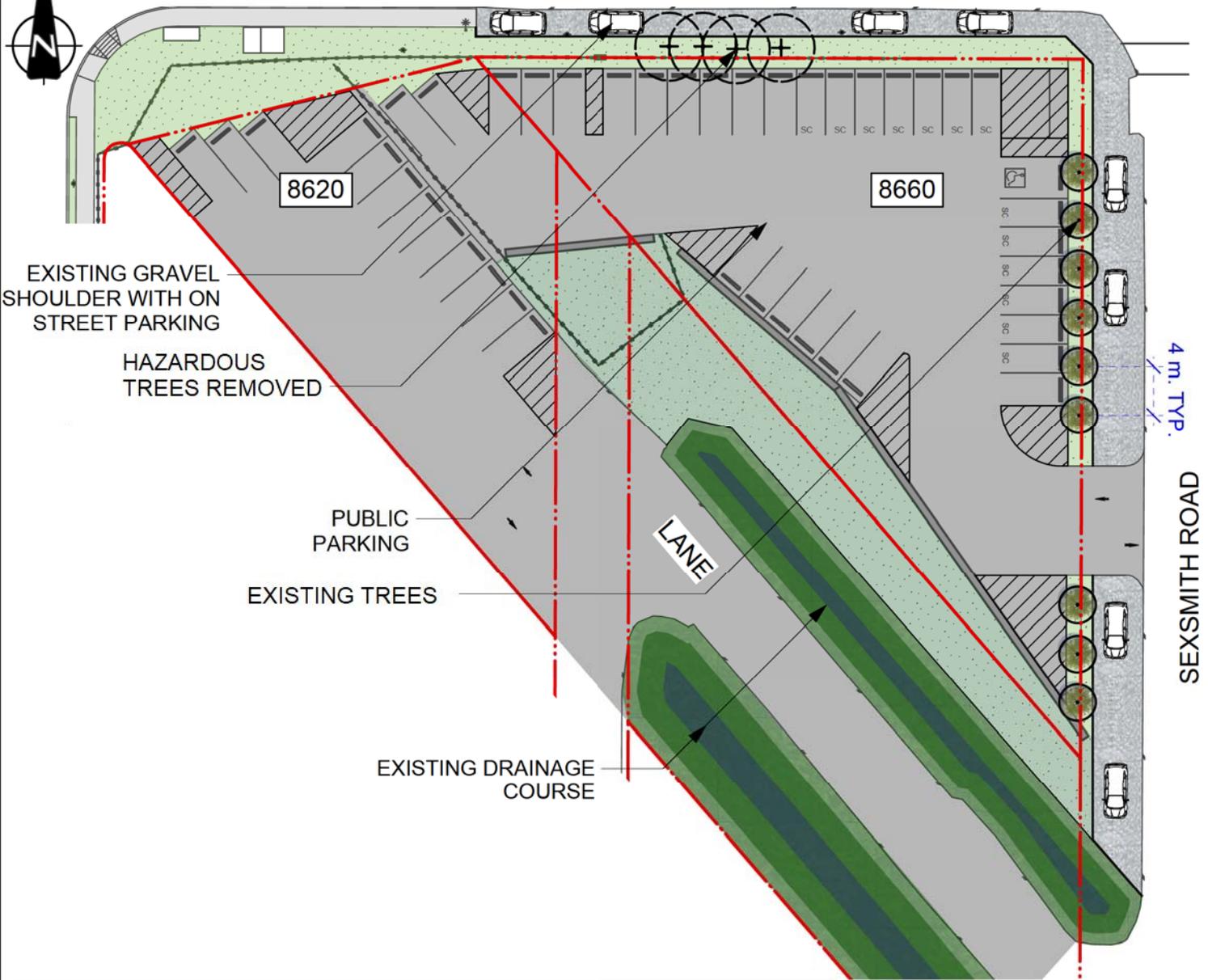


PARKING INVENTORY	
REGULAR STALLS	30
SMALL CAR STALLS	13
VAN ACCESSIBLE STALLS	1
TOTAL	44



TITLE: PROPOSED PUBLIC PARKING LOTS AT 8620/8660 BECKWITH ROAD			
DESIGN: T.S.	DWG. No:		
DRAWN: T.S.	SCALE: N.T.S.	DATE: MAR. 2019	
CHECKED: S.H.	SEC. No:	SHT. No: 1 OF 1	
ENGINEER: S.H.	PLN - 151		

BECKWITH ROAD



LANDSCAPE LEGEND

8620 + 8660 BECKWITH ROAD

	DECIDUOUS TREE <i>Populus tremuloides</i> (Trembling Aspen)
	GRASS SEED: 85% tall fescue, 15% creeping red fescue
	ESA NATIVE GRASS BLEND: 21% Spike Bent Grass, 15% Alaska Brome, 22% Tufted Hair Grass, 32% Native Red Fescue, 10% Meadow Barley

MINIMUM SOIL DEPTHS
TREES: 900mm
GRASS: 800mm

NOTE: ALL LANDSCAPE WORKS TO CONFORM TO CITY OF RICHMOND SUPPLEMENTARY SPECIFICATIONS AND DETAIL DRAWINGS UNLESS OTHERWISE NOTED

6911 No.3 Road Richmond BC V6Y 2C1

TITLE: PROPOSED PUBLIC PARKING LOTS AT 8620/8660 BECKWITH ROAD			
DESIGN: MP	DWG. No: LANDSCAPE PLAN		
DRAWN: MP	SCALE: 1:500	DATE: MAR. 2019	
CHECKED: MP	SEC. No:	SHT. No: 1 OF 1	
LANDSCAPE ARCHITECT: PLN - 152			



To: Planning Committee **Date:** February 4, 2022
From: John Hopkins **File:** 01-0100-30-HCOM1-01/2022-Vol 01
 Director, Policy Planning
Re: **Richmond Heritage Commission 2021 Annual Report & 2022 Work Program and Budget Allocation**

Staff Recommendation

1. That the Richmond Heritage Commission 2021 Annual Report, as presented in the staff report titled “Richmond Heritage Commission 2021 Annual Report & 2022 Work Program and Budget Allocation”, dated February 4, 2022, from the Director, Policy Planning, be received for information;
2. That the Richmond Heritage Commission 2022 Work Program, as presented in the staff report titled “Richmond Heritage Commission 2021 Annual Report & 2022 Work Program and Budget Allocation”, dated February 4, 2022, from the Director, Policy Planning, be approved; and
3. That the Richmond Heritage Commission 2022 Budget Allocation, as presented in the staff report titled “Richmond Heritage Commission 2021 Annual Report & 2022 Work Program and Budget Allocation”, dated February 4, 2022, from the Director, Policy Planning, be approved.

John Hopkins
Director, Policy Planning

JH:mp
Att. 3

REPORT CONCURRENCE		
ROUTED TO: Arts, Culture & Heritage	CONCURRENCE <input checked="" type="checkbox"/>	CONCURRENCE OF GENERAL MANAGER
SENIOR STAFF REPORT REVIEW	INITIALS: 	APPROVED BY CAO

Staff Report

Origin

The Richmond Heritage Commission was established on May 9, 2005, upon City Council approval of Richmond Heritage Commission Bylaw No. 7906. A primary role of the Commission is to provide advice to Council on heritage conservation and promotion matters, and undertake and provide support for activities that benefit and advance heritage in the City.

On July 12, 2021, Richmond Heritage Commission Bylaw No. 7906 was amended to clarify and strengthen the Commission's role in reviewing development applications city-wide and in Steveston Village, and reduce the size of the Commission from nine members to six members, three of which must have demonstrated professional experience in heritage conservation planning or designing buildings in a heritage area. All six positions are currently filled.

This report:

- Summarizes the activities of the Commission in 2021; and
- Recommends 2022 Work Program and Budget Allocation for approval by Council.

The Richmond Heritage Commission reviewed and endorsed the proposed Work Program at its meeting held on February 2, 2022.

This report supports Council's Strategic Plan 2018-2022 Strategy # 6 Strategic and Well-Planned Growth:

Leadership in effective and sustainable growth that supports Richmond's physical and social needs.

6.4 Recognize Richmond's history and heritage through preservation, protection and interpretation.

This report supports Council's Strategic Plan 2018-2022 Strategy #8 An Engaged and Informed Community:

Ensure that the citizenry of Richmond is well-informed and engaged about City business and decision-making.

8.1 Increased opportunities for public engagement.

2021 Annual Report

The detailed 2021 Annual Report of the Richmond Heritage Commission is contained in Attachment 1, and includes the following highlights:

- Reviewed and provided comments on two development applications involving alterations on a heritage-designated property and a property within the Steveston Village Heritage Conservation Area;
- Received information and regular updates on various City policies and initiatives;

- Received information and provided comments on the updated Heritage Inventory report from the consultant;
- Received information and provided comments on the Steveston Village Heritage Conservation Grant Program referral; and
- Provided sponsorship to the annual Richmond Regional Heritage Fair and Doors Open Richmond events, and to the Oral Histories project.

The Richmond Heritage Awards was not held in 2021 due to the Covid-19 pandemic.

2022 Work Program

The detailed 2022 Work Program of the Richmond Heritage Commission is contained in Attachment 2, and includes the following highlights:

- Continue to review and provide recommendations on relevant development applications or other initiatives that may have an impact on the character of heritage resources city-wide and in the Steveston Village early on in the process;
- Continue to participate as a stakeholder in the ongoing Heritage Inventory update project;
- Receive nominations, and select and recognize the winners for the 2022 Richmond Heritage Awards;
- Continue to provide sponsorship to the Richmond Heritage Fair and Doors Open Richmond events, and the Oral Histories project; and
- Continue to explore options and develop a specific proposal/program to raise the profile of heritage in the City and to expand Commission members' knowledge and expertise related to heritage conservation.

The work program will be revised as necessary, based on emerging issues and future Council priorities.

2022 Budget Allocation

The total operating budget of the Richmond Heritage Commission is \$5,000 and the detailed 2022 Budget Allocation is contained in Attachment 3.

Financial Impact

None.

Conclusion

The Richmond Heritage Commission's mandate is to advise City Council on heritage conservation and promotion matters, and to undertake and provide support for activities that benefit and advance heritage in Richmond. The 2021 Annual Report for the Richmond Heritage Commission is submitted for information, and the 2022 Work Program and Budget Allocation are recommended for Council's approval.



Minhee Park
Planner 2

MP:cas

Attachment 1: Richmond Heritage Commission 2021 Annual Report

Attachment 2: Richmond Heritage Commission 2022 Work Program

Attachment 3: Richmond Heritage Commission 2022 Budget Allocation

**RICHMOND HERITAGE COMMISSION
2021 ANNUAL REPORT**

Richmond Heritage Commission 2021 Accomplishments		
Projects	Achieved Outcomes	Accomplishments and Comments
Development Proposals	Provided heritage perspective and advice to Council and staff	<ul style="list-style-type: none"> Reviewed and provided comments on a total of two development applications forwarded by staff.
Heritage Policy		<ul style="list-style-type: none"> Reviewed and provided comments on updated Heritage Inventory report from the consultant. Received information and provided comments on the Steveston Village Heritage Conservation Grant Program referral.
City of Richmond Museum and Heritage Services	Received information and helped support and promote the City's services and sites	<ul style="list-style-type: none"> Received information from staff on programs, initiatives and projects related to City-owned historic places and museums, and provided feedback.
Richmond Heritage Awards		<ul style="list-style-type: none"> Not held in 2021 due to the pandemic.
Community Projects	Sponsored and supported community initiatives	<ul style="list-style-type: none"> Provided \$1,000 in sponsorship to the Richmond Heritage Fair. Provided \$1,000 in sponsorship to the Doors Open Richmond. Provided \$350 in sponsorship to the Richmond Museum's Oral Histories project.

List of Applications Reviewed in 2021		
Application No.	Address of property	Application Purpose
HA 20-896761/ DP20-896760	Unit 100 - 12211 1 st Avenue	To construct a new storefront entry with sidelight glazing, complete with interior ramp to improve accessibility.
HA21-936123	6511 Dyke Road	To undertake minor maintenance work to the London Farmhouse and improve the landscaping and interpretation in the lands at London Heritage Farm.

**RICHMOND HERITAGE COMMISSION
PROPOSED 2022 WORK PROGRAM**

Projects	Results Expected	Comments
Development Proposals	Heritage perspective and advice to Council	<ul style="list-style-type: none"> Review and provide recommendations on relevant development applications or other initiatives that may have an impact on the character of heritage resources city-wide and in the Steveston Village early on in the process.
Heritage Policy		<ul style="list-style-type: none"> Continue to participate as a stakeholder in the Heritage Inventory Update.
Richmond Heritage Awards	Receive nominations and select recipients	<ul style="list-style-type: none"> Receive award nominations, and select and honour the winners.
City of Richmond Museum and Heritage Services	Receive information and help support and promote the City's services and sites	<ul style="list-style-type: none"> Receive information from staff on programs, initiatives and projects related to City-owned historic places and museums and provide feedback.
Community Heritage Partners and Projects	Sponsor and support community initiatives	<ul style="list-style-type: none"> Provide sponsorship to the Richmond Heritage Fair, Doors Open Richmond event, and the Oral Histories project of the Richmond Museum Society.
Capacity Building	Raise profile of Richmond Heritage and enhance knowledge	<ul style="list-style-type: none"> Recommend options and develop a specific proposal/program to raise public awareness of Richmond heritage. Expand and enhance knowledge and expertise related to heritage and pursue other educational opportunities.

ATTACHMENT 3**RICHMOND HERITAGE COMMISSION
PROPOSED 2022 BUDGET ALLOCATION**

Work Item	2022 Budget Allocation
Annual Sponsorship - Heritage Fair	\$1,000
Annual Sponsorship - Doors Open	\$1,000
Annual Sponsorship - Oral Histories	\$350
Heritage Awards (Advertising, Promotion, Framing of Awards)	\$1,300
Education	\$350
Meeting Refreshments	\$0
Raise profile of Richmond's heritage	\$1,000
Contingency	N/A
Total	\$5,000