



City of Richmond

Agenda

Development Permit Panel

**Council Chambers, City Hall
6911 No. 3 Road**

**Wednesday, August 28, 2019
3:30 p.m.**

MINUTES

Motion to adopt the minutes of the Development Permit Panel meeting held on July 24, 2019.

1. DEVELOPMENT PERMIT 18-829241
(REDMS No. 6158291)

APPLICANT: 1076694 BC Ltd.

PROPERTY LOCATION: 10451, 10471 & 10491 No. 2 Road

Director's Recommendations

That a Development Permit be issued which would:

1. *Permit the construction of 12 townhouse units at 10451, 10471 & 10491 No. 2 Road on a site zoned "Low Density Townhouses (RTL4)"; and*
2. *Vary the provisions of Richmond Zoning Bylaw 8500 to:*
 - (a) *Reduce the minimum front yard setback from 6.0 m to 4.5 m; and*
 - (b) *Allow one small car parking stall in each of the side-by-side garages (12 small car stalls in total).*

Development Permit Panel – Wednesday, August 28, 2019

ITEM

2. **DEVELOPMENT PERMIT 19-858887**
HERITAGE ALTERATION PERMIT 19-858886
(REDMS No. 6220011 v. 2)

APPLICANT: Gordon Chan

PROPERTY LOCATION: 12551 No.1 Road

Director's Recommendations

1. *That a Development Permit be issued which would permit the replacement of existing wooden ramp, stairs and deck with new aluminum ramp, stairs, and deck around the existing building and the installation of a new aluminum ramp on the rear (south) elevation at 12551 No.1 Road; and*
2. *That a Heritage Alteration Permit be issued for 12551 No.1 Road in accordance with the Development Permit.*



3. **New Business**

4. **Date of Next Meeting:** September 11, 2019

ADJOURNMENT



City of Richmond

Minutes

Development Permit Panel Wednesday, July 24, 2019

Time: 3:30 p.m.

Place: Council Chambers
Richmond City Hall

Present: Joe Erceg, Chair
Cecilia Achiam, General Manager, Community Safety
Milton Chan, Acting Director, Engineering

The meeting was called to order at 3:30 p.m.

Minutes

It was moved and seconded

That the minutes of the meeting of the Development Permit Panel held on July 10, 2019 be adopted.

CARRIED

1. DEVELOPMENT PERMIT 17-775868

(REDMS No. 6203255)

APPLICANT: McDonald's Restaurant of Canada Ltd.

PROPERTY LOCATION: 8140 No. 2 Road

INTENT OF PERMIT:

1. Permit the construction of minor building additions, exterior renovations, and the installation of a dual drive-through lane system at the existing McDonald's restaurant at 8140 No. 2 Road on a site zoned "Community Commercial (CC)"; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to reduce the minimum interior (south) side yard from 6.0 m to 2.0 m for a detached accessory building containing garbage and recycling facilities.

1.

Development Permit Panel

Wednesday, July 24, 2019

Applicant's Comments

Andrea Scott, Lovick Scott Architects, Inc., with the aid of a visual presentation (attached to and forming part of these Minutes as Schedule 1) provided background information on the subject application, highlighting the following:

- the applicant is proposing a dual drive-through lane system on the south side of the existing McDonald's restaurant;
- the existing garbage and recycling facilities of the restaurant will be relocated from the southwest corner to the southeast corner of the restaurant and will be designed as a detached, fully enclosed and roofed building;
- a new wood fence will be installed along the south property line to provide screening and mitigate potential noise impact of the proposed dual drive-through lane system on the existing residential townhouse development to the south;
- pedestrian routes to the restaurant will be improved to enhance accessibility and safety of pedestrians;
- additional landscaping is proposed on-site including planting of a variety of shrubs;
- the existing restaurant building will be upgraded to the new McDonald's standards which include a square and linear building form and the use of high-quality metal cladding materials, among others;
- a white-coloured canopy wraps around the building to provide weather protection for pedestrians;
- the two drive-through windows will be redesigned using new materials and colours;
- a new meeting room for staff will be constructed in the southwest corner of the restaurant in lieu of the existing garbage and recycling facility which will be relocated; and
- the exterior renovations to the existing restaurant will be similar to the renovated McDonald's restaurants in Metro Vancouver and reflect the character of recent renovations to the neighbourhood shopping centre.

In reply to queries from the Panel, Ms. Scott acknowledged that (i) the existing chain link fence to the south of the restaurant building will be retained and a new wood fence along the south property line will be installed, (ii) there will be no changes to the existing rooftop mechanical equipment, (iii) the roof parapet will be redesigned but its existing height will be maintained, (iv) the volume of the new drive-through speakers will automatically adjust to the ambient noise levels, (v) the predicted noise levels of the new drive-through speakers comply with the City's Regulation Bylaw as indicated in the applicant's acoustical report, (vi) the ordering area in the drive-through will be expanded but maintained in its current location, (vi) no complaints have been reported regarding noise in the existing drive-through from residents of the adjacent residential development to the south, (vii) existing trees along the south property line will be retained to provide a buffer to the adjacent residential development to the south, and (viii) there will be no changes to lighting on the south side of the restaurant building.

Development Permit Panel

Wednesday, July 24, 2019

In response to a query from the Panel, Wayne Craig, Director, Development, confirmed that there is a drive aisle between the McDonald's restaurant building and the adjacent residential townhouse development to the south.

Staff Comments

Mr. Craig noted that (i) staff support the proposed variance for the required minimum south side yard setback for the accessory building for garbage and recycling as it improves upon the existing unenclosed and unroofed garbage facilities located in this same area, (ii) the accessory building will be located approximately 16 meters from the neighbouring townhouse development to the south, (iii) the applicant will provide a cash-in-lieu contribution to the City for upgrades to the existing traffic signal at No. 2 Road and at the entrance to the neighbourhood shopping centre, (iv) an Electric Vehicle (EV) charging station for two cars will be provided at the surface parking area of the restaurant, and (v) staff appreciate the retention of all existing trees on the site and the addition of new groundcover to supplement on-site planting areas.

Gallery Comments

None.

Panel Discussion

The Panel expressed support for the project, noting that the proposed side yard setback variance will improve the restaurant's existing garbage and recycling facilities. The Panel also expressed appreciation for the proposed improvements to on-site pedestrian circulation.

Panel Decision

It was moved and seconded

That a Development Permit be issued which would:

1. *permit the construction of minor building additions, exterior renovations, and the installation of a dual drive-through lane system at the existing McDonald's restaurant at 8140 No. 2 Road on a site zoned "Community Commercial (CC)"; and*
2. *vary the provisions of Richmond Zoning Bylaw 8500 to reduce the minimum interior (south) side yard from 6.0 m to 2.0 m for a detached accessory building containing garbage and recycling facilities.*

CARRIED

Development Permit Panel
Wednesday, July 24, 2019

2. New Business

It was moved and seconded

That the meeting of the Development Permit Panel scheduled on August 14, 2019 be cancelled.

CARRIED

3. Date of Next Meeting: August 28, 2019

4. Adjournment

It was moved and seconded

That the meeting be adjourned at 3:46 p.m.

CARRIED

Certified a true and correct copy of the Minutes of the meeting of the Development Permit Panel of the Council of the City of Richmond held on Wednesday, July 24, 2019.

Joe Erceg
Chair

Rustico Agawin
Committee Clerk

Schedule 1 to the Minutes of the
Development Permit Panel
meeting held on Wednesday,
July 24, 2019.

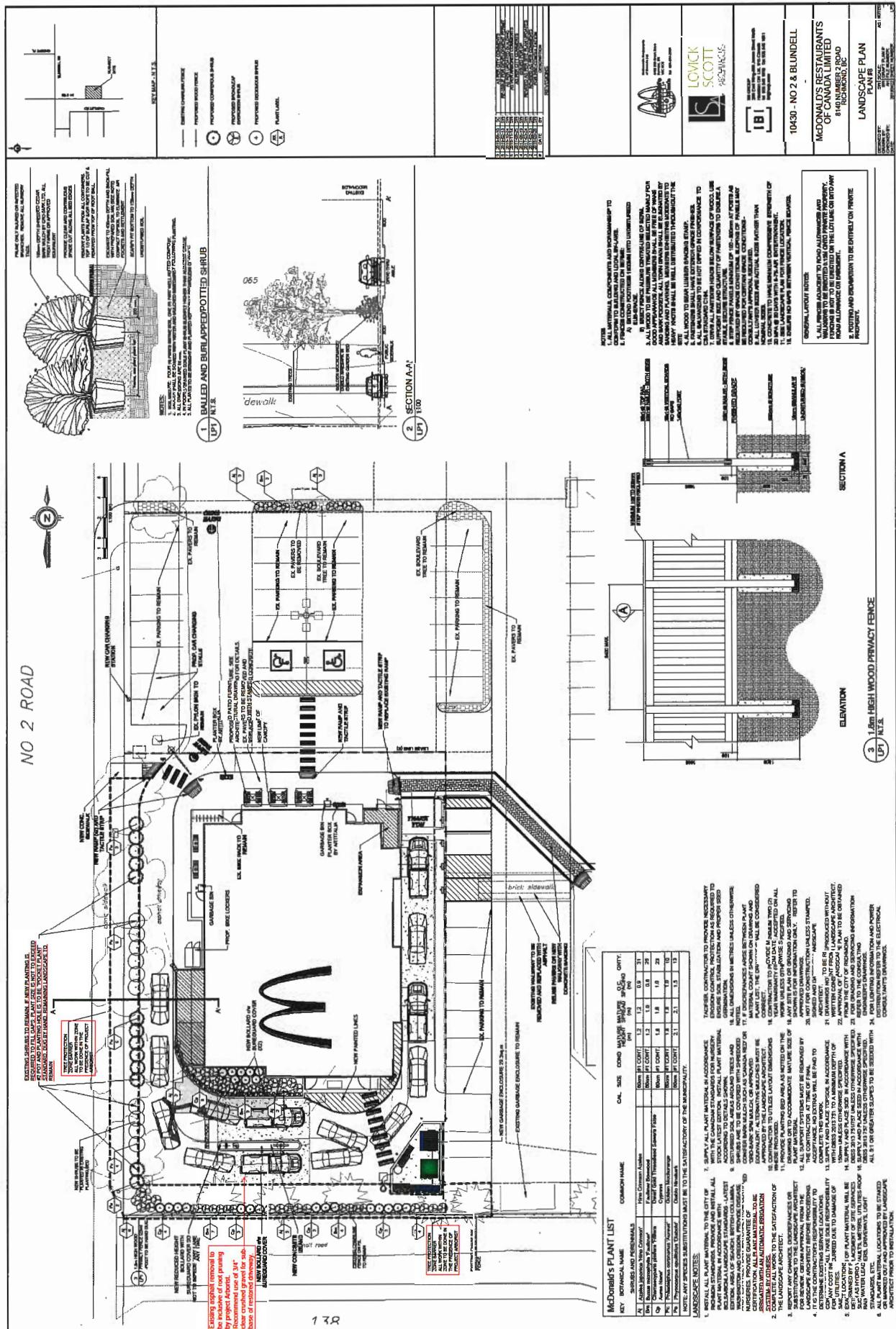


McDonald's - Blundell

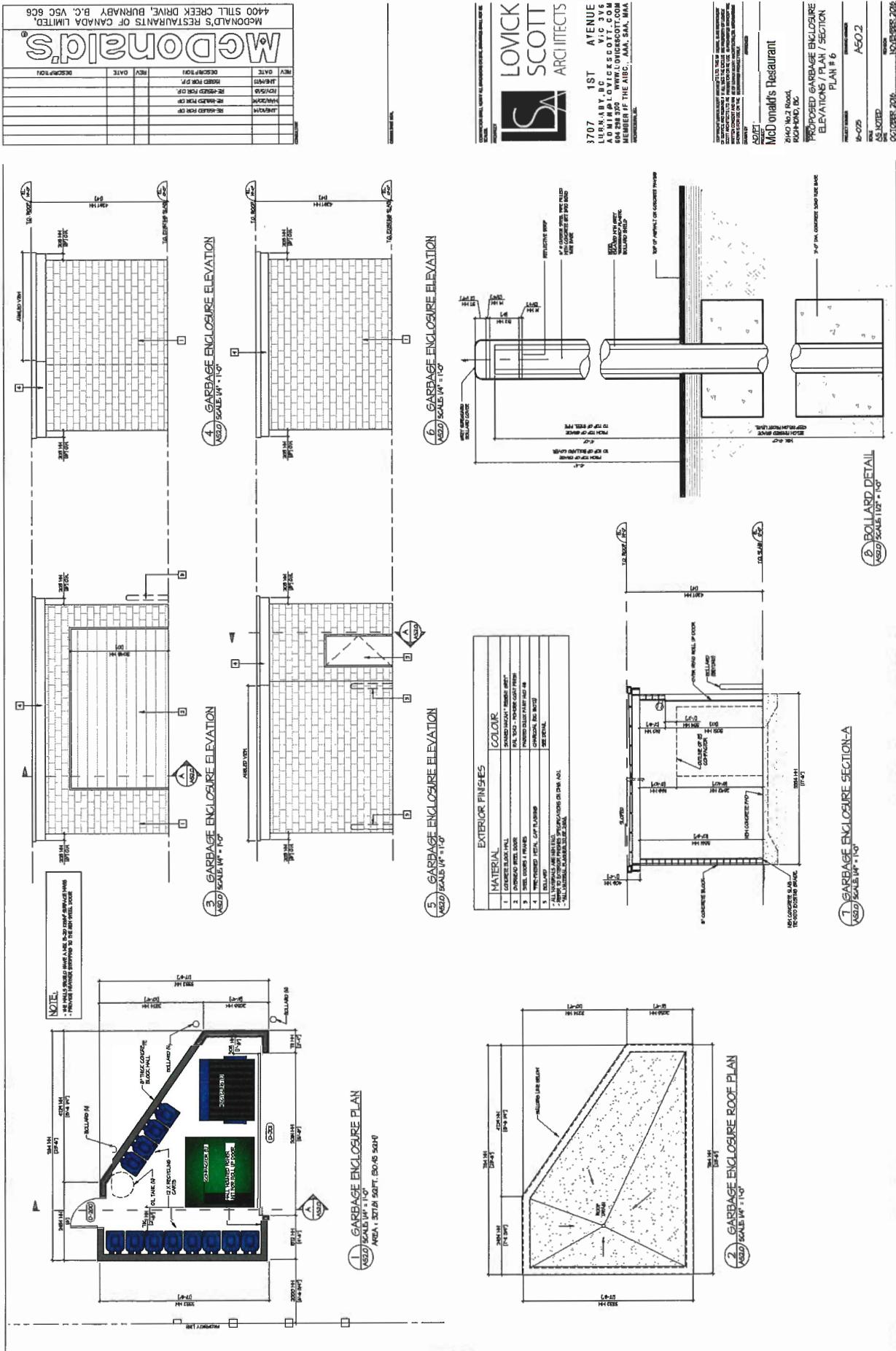
8140 No. 2 Road, Richmond, BC

Lovick Scott Architects

Site Plan



Landscape Plan



Garbage Details

Street Elevations



⑤ STREETSCAPE - NO. 2 ROAD



LOVICK
SCOTT
ARCHITECTS



MCDONALD'S BLUNDELL, RICHMOND, B.C.

KEY MAP



④ PROPOSED NORTH EAST ELEVATION



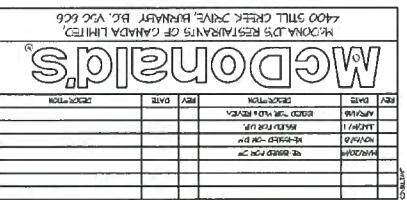
③ PROPOSED NORTH WEST ELEVATION

① VIEW OF EXISTING BUILDING - PARTIAL WEST SIDE ON NO. 2 ROAD



② VIEW OF EXISTING BUILDING - PARTIAL WEST SIDE ON NO. 2 ROAD





**LOVICK
SCOTT
ARCHITECTS**

3707 1ST AVENUE
BURNABY, BC V5C 3V6
D W H @ LOVICKSCOTT.COM
604 258 3760 WWW.LOVICKSCOTT.COM
MEMBER OF THE AIBC, AAA, SAA, MAAA & NWTA

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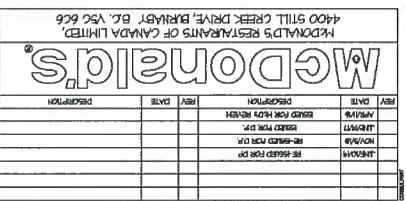
Proposed



Existing



Streetscapes



Colour Elevations

Examples of completed projects



McDonald's – Grandview Hwy.
2909 Grandview Hwy, Vancouver, B.C.



McDonald's – Grandview Hwy.

2909 Grandview Hwy, Vancouver, B.C.

View of dining area



McDonald's – Fleetwood

15574 Fraser Hwy, Surrey, B.C.

North West view



North view



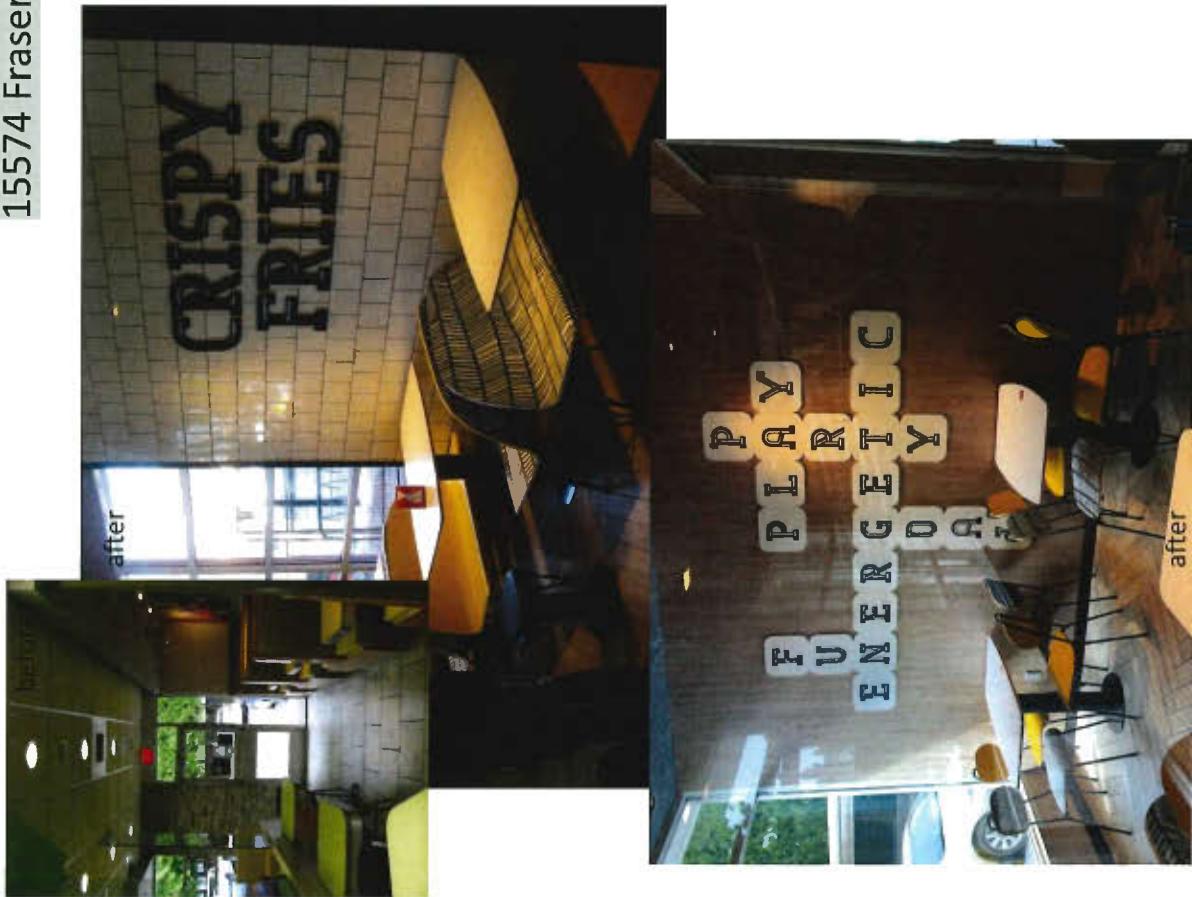
@drive-thru lane



McDonald's – Fleetwood

15574 Fraser Hwy, Surrey, B.C.

View of dining area



South East view



McDonald's – Pitt Meadows

12549 Harris Road, Pitt Meadows, B.C.



@drive-thru lane



North East view



McDonald's – Pitt Meadows

12549 Harris Road, Pitt Meadows, B.C.

View of dining area





**City of
Richmond**

Report to Development Permit Panel

To: Development Permit Panel

Date: August 1, 2019

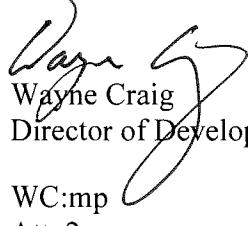
From: Wayne Craig
Director of Development

File: DP 19-858887
HA 19-858886

Re: **Application by Gordon Chan for a Development Permit and Heritage Alteration Permit at 12551 No.1 Road**

Staff Recommendation

1. That a Development Permit be issued which would permit the replacement of existing wooden ramp, stairs and deck with new aluminum ramp, stairs, and deck around the existing building and the installation of a new aluminum ramp on the rear (south) elevation at 12551 No.1 Road.
2. That a Heritage Alteration Permit be issued for 12551 No.1 Road in accordance with the Development Permit.


Wayne Craig
Director of Development

WC:mp
Att. 2

Staff Report

Origin

Gordon Chan has applied to the City of Richmond for permission to replace the existing wooden ramp, stairs and deck with new aluminum ramp, stairs and deck around the existing office building for Fisheries and Oceans Canada at 12551 No.1 Road. The applicant also proposes to install a new aluminum ramp on the rear (south) elevation.

The subject property is split-zoned: most of the site is zoned “Light Industrial (IL)”, and the small triangular portion along Bayview Street is zoned “Steveston Commercial (CS2)”.

The subject property is in the Steveston Village Heritage Conservation Area where a Heritage Alteration Permit is required for any exterior works or modification of land. Therefore, the Development Permit application is being considered in coordination with a Heritage Alteration Permit.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the north: Across Bayview Street is a vacant property at 12451 No.1 Road. The property is currently landscaped with trees and lawn. The property contains an identified heritage resource, the curved roadway pattern as a trace in the landscape of the original Canadian Pacific Railway track that extended along the waterfront to the Gulf of Georgia Cannery.

To the east: Immediately to the east is the south end of No.1 Road with a ferry terminal ramp and the No.1 Road South Discharge station, which is an identified heritage resource.

To the south: South Arm Fraser River

To the west: A one-storey, multi-tenant commercial building at 3900 Bayview Street.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the urban design issues and other staff comments identified as part of the review of the subject Development Permit and Heritage Alteration Permit applications.

Richmond Heritage Commission Comments

The applications were reviewed by the Richmond Heritage Commission (RHC) on June 5, 2019, and the RHC adopted the following resolution:

That the Richmond Heritage Commission support the Heritage Alteration Permit application (HA19-858886) and Development Permit application (DP19-858887) for 12551 No.1 Road subject to staff working with the Richmond Heritage Commission to ensure that the appearance of the proposed ramp, stairs and deck are in keeping with the heritage character of Steveston Village.

The applicant considered the Richmond Heritage Commission's comments and revised the proposed material to aluminum with matt finish. Information regarding the material along with an example photo was circulated to the Richmond Heritage Commission by email and was supported.

A copy of the relevant excerpt from the RHC minutes from June 5, 2019 is attached for reference (Attachment 2).

Advisory Design Panel Comments

The applications were not forwarded to the Advisory Design Panel as the proposed changes are minor in nature.

Analysis

Architectural Form and Character

- The existing office building is a one-storey structure with a flat roof. The building was constructed in the 1970s and is not one of the identified heritage resources within the Steveston Village Heritage Conservation Area.
- The applicant has indicated that the existing wooden stairs, ramp and deck around the building have deteriorated and they are in need of repair/replacement. Aluminum is chosen as it is more durable than wood.
- The building is currently painted with muted blue colour. The proposed aluminum material is proposed to be powder-coated with matt finish paint to match the existing colour of the building. The railing will be painted yellow for safety purposes.
- The proposed ramp on the front elevation will be slightly wider (i.e., 0.3 m) to increase accessibility and the stairs will be narrower to accommodate the wider ramp. The sizes of all other stairs and deck will remain unchanged.
- A pedestrian pathway will be clearly defined by means of pavement marking to provide safe pedestrian movement between the main building entrance and Bayview Street.
- The building is set back 7.6 m from Bayview Street and is also screened by the existing parking and vehicle circulation area at front. The affected areas on the front elevation are recessed and are not highly visible from Bayview Street.
- The proposed changes on the rear elevation will not be visible from Bayview Street.
- The proposed alteration does not impact other existing features of the property such as landscaping.

- The proposed changes are minor in nature and would not alter the existing character of the building.

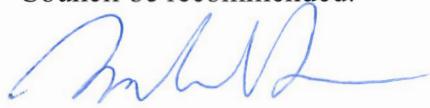
Conclusion

The applicant proposes to replace the existing wooden ramp, stairs and deck with new aluminum ramp, stairs and deck around the existing office building for Fisheries and Oceans Canada at 12551 No.1 Road. The applicant also proposes to install a new aluminum ramp on the rear (south) elevation.

The proposed changes are minor in nature and would not alter the existing character of the building.

A Heritage Alteration Permit is also required to be issued for any works permitted through the Development Permit as the subject site is also located in the Steveston Village Heritage Conservation Area.

Staff recommend that the Development Permit and Heritage Alteration Permit be endorsed, and issuance of the Development Permit and Heritage Alteration Permit for 12551 No.1 Road by Council be recommended.



Minhee Park
Planner 2

MP:lce

Attachment 1: Development Application Data Sheet

Attachment 2: Excerpt from the Richmond Heritage Commission June 5, 2019 Meeting Minutes



City of Richmond

Development Application Data Sheet

Development Applications Department

DP 19-858887

Attachment 1

Address: 12551 No.1 Road

Applicant: Gordon Chan Owner: Crown Federal

Planning Area(s): Steveston

	Existing	Proposed
Site Area:	2,503 m ²	No Change
Land Uses:	Office	No Change
OCP Designation:	Heritage Mixed Use (Commercial Industrial with Residential and Office Above)	No Change
Zoning:	Steveston Commercial (CS2) Light Industrial (IL)	No Change

	Bylaw Requirement	Proposed	Variance
Lot Coverage:	Max. 60 %	20.35 % (no change)	None
Setback – Front Yard:	Min. 3 m	7.6 m (no change)	None
Setback – West Side Yard:	Min. 0 m	10.4 m (no change)	None
Setback – East Side Yard:	Min. 0 m	5.6 m (no change)	None
Setback – Rear Yard:	Min. 0 m	16.3 m (no change)	None

Attachment 2

**Excerpt from the Draft Minutes
Richmond Heritage Commission Meeting**

**Wednesday, June 5, 2019 – 7:00 p.m.
Rm. M.2.002
Richmond City Hall**

3.1. Heritage Alteration Permit Application (HA19-858886) and Development Permit Application (DP19-858887) – 12551 No.1 Road

Staff provided an overview of the applications to replace existing wooden steps, platforms and ramp to the existing structure at 12551 No.1 Road. Since the property is located within the Steveston Village Heritage Conservation Area, a Heritage Alteration Permit is considered in coordination with a Development Permit.

In response to Commission's query, the applicant noted that aluminum is proposed as it is more durable than wood. Commission asked details of the proposed aluminum material and indicated that it might not paint well unless it is anodized aluminum. Commission also asked about the texture and noted that it should not be shiny material.

One member noted that he had no concerns regarding the proposed material.

In response to Commission's query, the applicant noted that anti-slip coating will be applied, and steps, platforms and ramps will be painted to match the existing building colour.

Commission requested further details be provided regarding the proposed aluminum material and staff work with the Richmond Heritage Commission to determine whether the proposed material is in keeping with the heritage character of Steveston Village.

It was moved and seconded:

That the Richmond Heritage Commission support the Heritage Alteration Permit application (HA19-858886) and Development Permit application (DP19-858887) for 12551 No.1 Road subject to staff working with the Richmond Heritage to ensure that the appearance of the proposed ramp, stairs and deck are in keeping with the heritage character of Steveston Village.

CARRIED



City of Richmond

Development Permit

No. DP 19-858887

To the Holder: Gordon Chan

Property Address: 12551 No.1 Road

Address: 201-33 East 8th Avenue, Vancouver BC V5T 1R5

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #6 attached hereto.
4. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
5. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

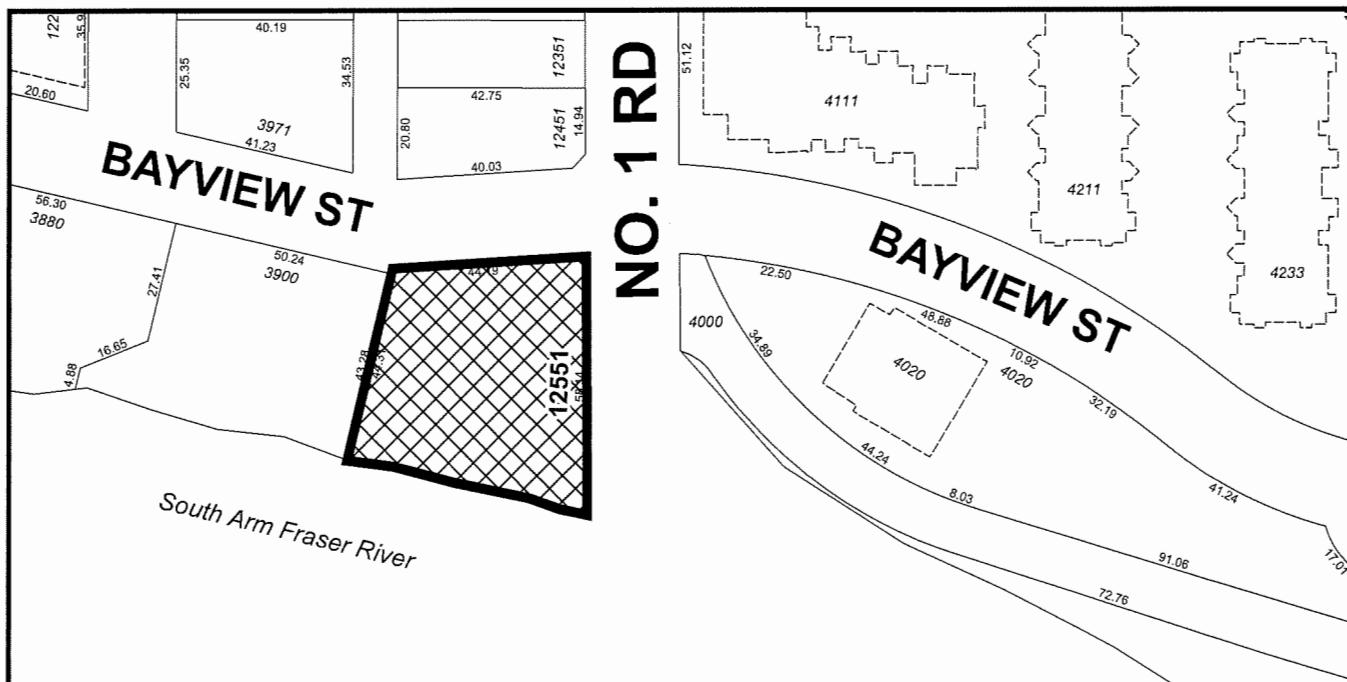
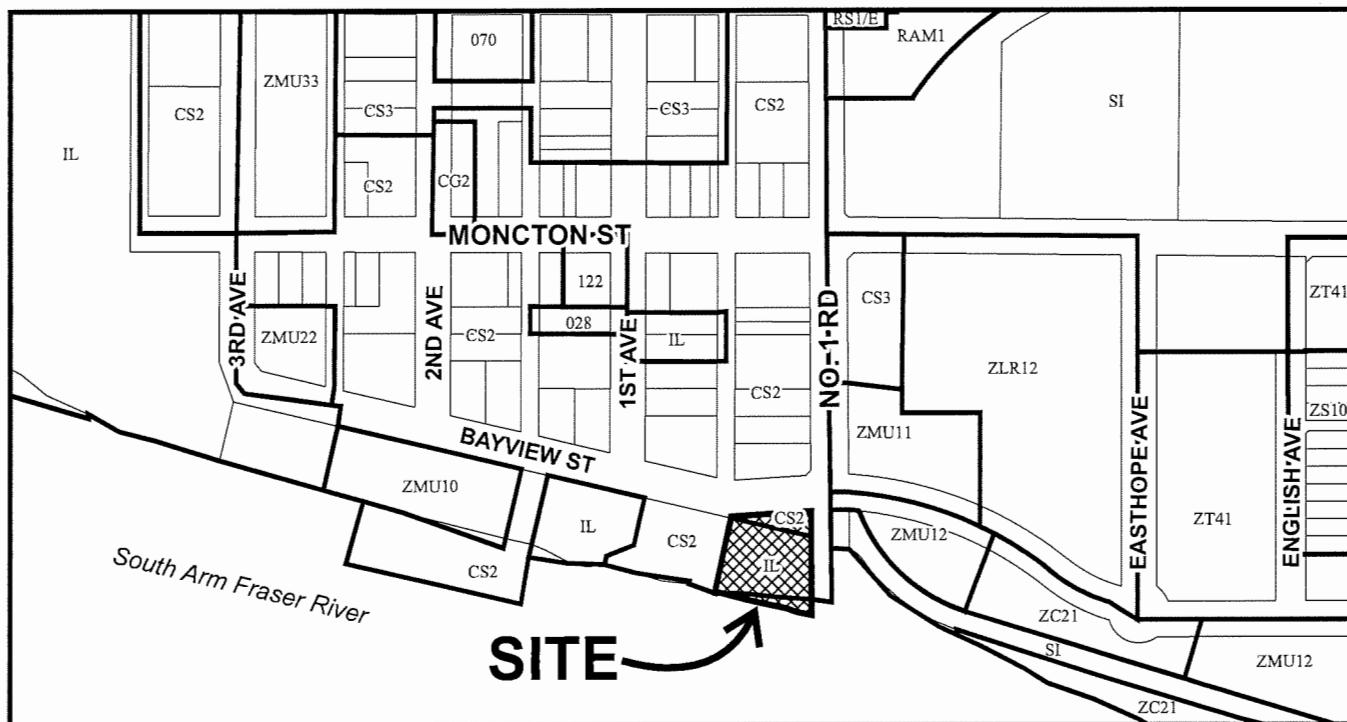
AUTHORIZING RESOLUTION NO. <Resolution No.> ISSUED BY THE COUNCIL THE DAY OF <Date>

DELIVERED THIS <Day> DAY OF <Month>, <Year>

MAYOR



City of Richmond

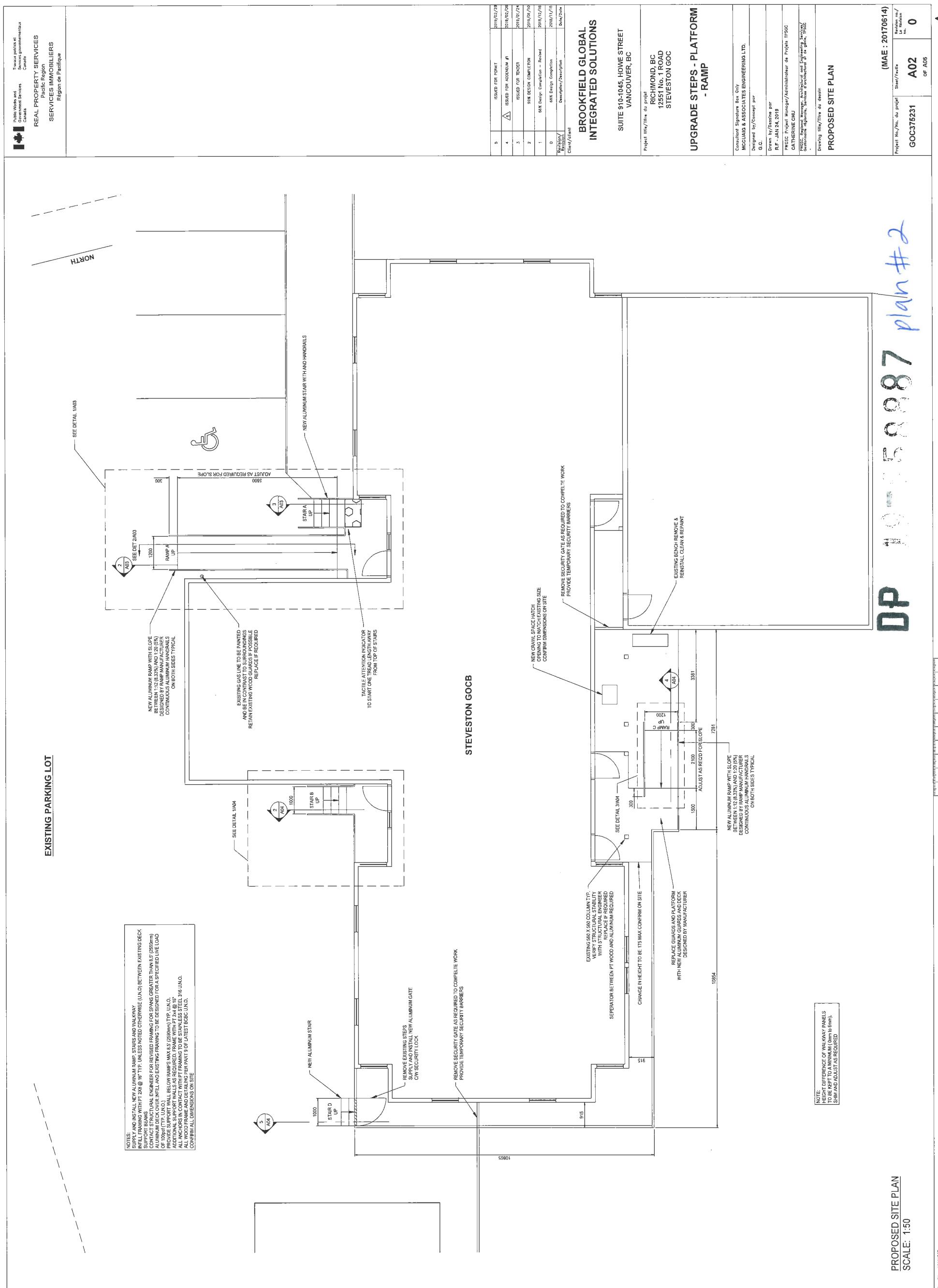


DP 19-858887

Original Date: 05/06/19

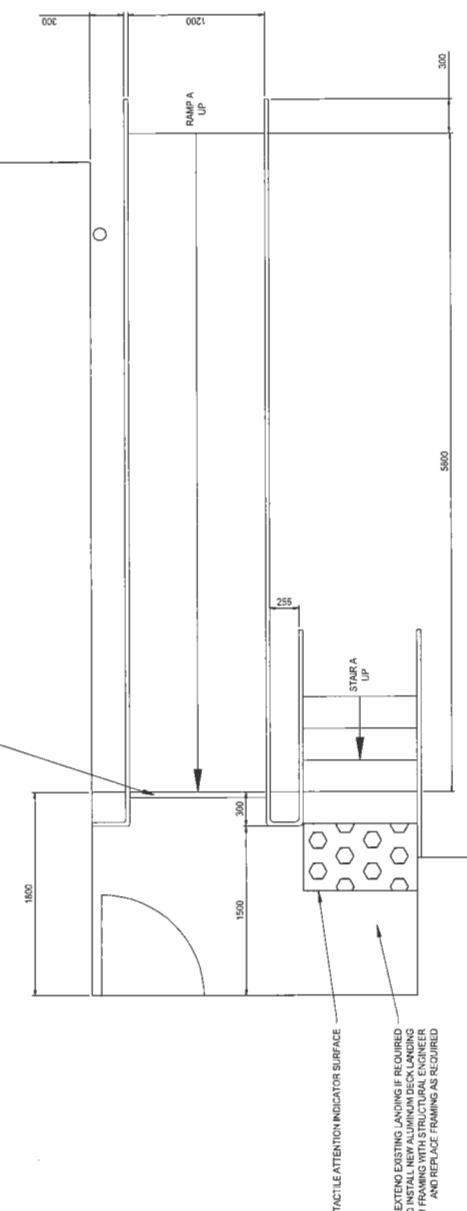
Revision Date: 05/07/19

Note: Dimensions are in METRES



PROPOSED SITE PLAN
SCALE: 1:50

✓ COLOUR CONTRAST STRIP AT TOP AND BOTTOM OF RAMP



RAMP A - PLAN VIEW
SCALE 1:25

**EXTEND EXISTING LANDING IF REQUIRED
AND INSTALL NEW ALUMINUM DECK LANDING
REVIEW FRAMING WITH STRUCTURAL ENGINEER
AND REPLACE FRAMING AS REQUIRED**

RAMP A - PLAN VIEW

SCALE 1:25

LANDING IF REQUIRED
LOCATE ON A PLATEFORM
BUILDING REINFORCING
PROVIDED AS REQUIRED

300

5800

1 A03

EASE OF WALL

Detailed description: This is an architectural plan view of Ramp A. The ramp itself is a single straight section sloping down from left to right. At the top left, there is a rectangular landing area with a small recessed step. A vertical dimension line indicates a height of 300 units from the ramp's surface to the top of the landing. A horizontal dimension line shows a total length of 5800 units for the ramp. On the right side, there is a vertical wall labeled 'EASE OF WALL' at the top. A railing system runs along the edge of the ramp. A circular callout in the bottom right corner contains the text '1 A03'. The drawing is oriented vertically on the page.

The diagram illustrates a transition from a concrete surface to a ramp. Key dimensions include:

- FACE OF WALL BEYOND: A dashed line indicating the wall's edge.
- 300 mm: The distance from the wall to the start of the ramp.
- 75 mm: The height of the ramp's top edge above the concrete surface.
- 76 mm: The height of the ramp's bottom edge above the concrete surface.
- 590 mm: The total height of the ramp.
- 175 mm: The height of the ramp's side slope.

Annotations provide specific requirements:

- PROTECTION REQUIRED MAX ABOVE SURFACE: Protection required at the maximum height above the surface.
- CONFIRM ON SITE: Confirmation required on site.
- ASPHALT GRADE DIFFERENCE IN HEIGHT OF GRADE AND BOTTOM RAMP BETWEEN 600 mm TO 1,200 mm AT 1:2 (30%) BEVEL IF OTHER 1:4 (30.5%)
- 600 mm TO 1,200 mm: The range of asphalt grade difference.
- 1:2 (30%): The bevel rate for asphalt grade difference.
- 1:4 (30.5%): The bevel rate if other than 1:2.

Text at the bottom left: ID IN MANUFACTURER'S STICKERS SURROUNDING THE DIAMETER OF RAILINGS IF BETWEEN 35 AND 45

RAMP A - ELEVATION

SCALE 1:25

RAMP A - ELEVATION

SCALE 1:25

The drawing shows a cross-section of a ramp with the following dimensions and features:

- Vertical dimensions:** The total height of the ramp is 6'0" (60 inches). The ramp section has a rise of 3'0" (36 inches) and a run of 12'0" (144 inches), resulting in a 1:4 slope.
- Handrail:** Handrails are designed by manufacturer. They are to be attached to walls where possible, kept 25 to 45 away from face of wall, colored to contrast surroundings, and have a height of 3'0" (36 inches).
- Step height:** Adjust step height as required, with a maximum height of 15" and a minimum of 12". The distance between nosings shall be at least 30".
- Surface:** The surface material is asphalt grade.
- Braille:** Braille is provided on the surface between 6'0" to 9'0".
- Tactile indicator:** A tactile attention indicator is located on the surface at 17'5".
- Curves:** The ramp has a 1:12 curve at the top and a 1:12 curve at the bottom.

BROOKFIELD GLOBAL
INTEGRATED SOLUTIONS

SUITE 910-1045, HOWE STREET
VANCOUVER, BC

RICHMOND BC

UPGRADE STEPS - PLATFORM

- RAMP

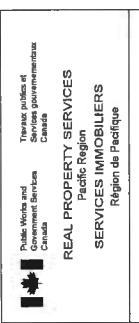
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MECQUIG & ASSOCIATES ENGINEERING LTD.
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JAN 24, 2019
 PHYSIC Project Manager/Administrateur de Projet TSSC
CATHERINE CHAU
 TSSC, Regional Manager, Aménagement et Développement, Services d'infrastructure et de gisement, TSSC
 Drawing title/Titre du dessin
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RAMP A - STAIR SECTION

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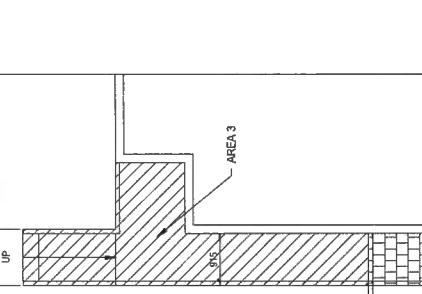
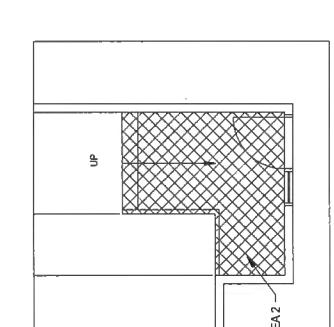
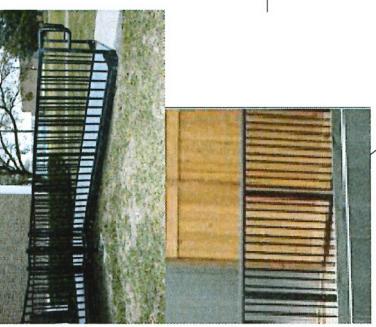
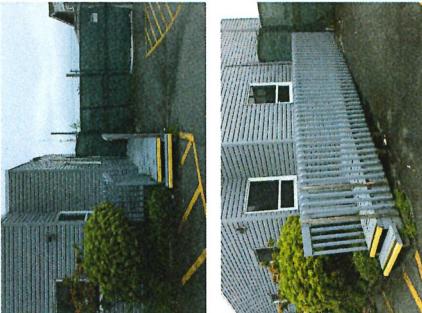
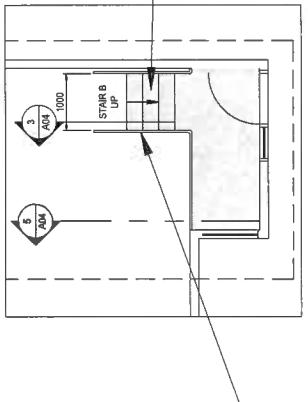
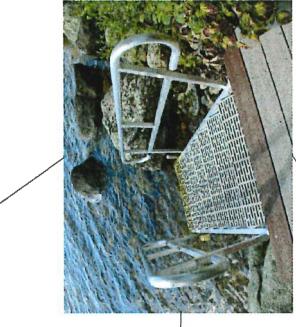
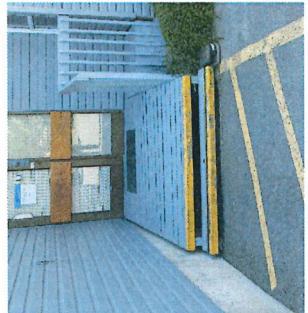
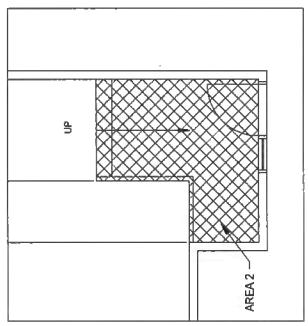
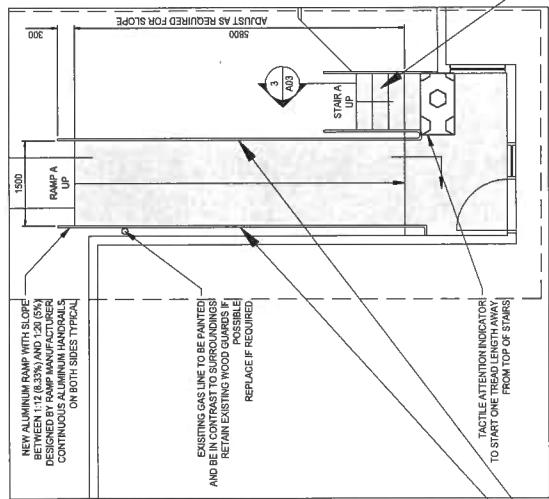
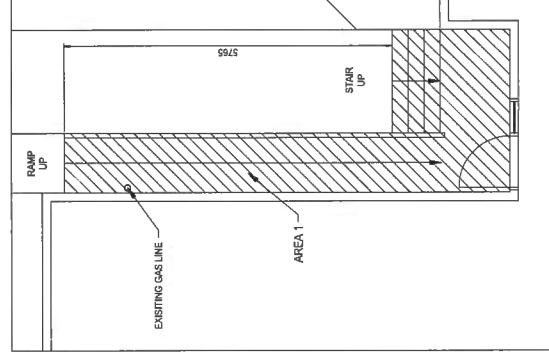
plan #3

(MAE-20170614)	
Project No./No. du projet	Street/Feuille
GOC375231	A03 or A06
	Barrière mo/ la barrière n° / la n°
	0



REAL PROPERTY SERVICES
Pacific Region
SERVICES IMMOBILIERS
Région de l'Atlantique

PROPOSED



SITE PLAN - AREA OF WORK
SCALE: 1:100

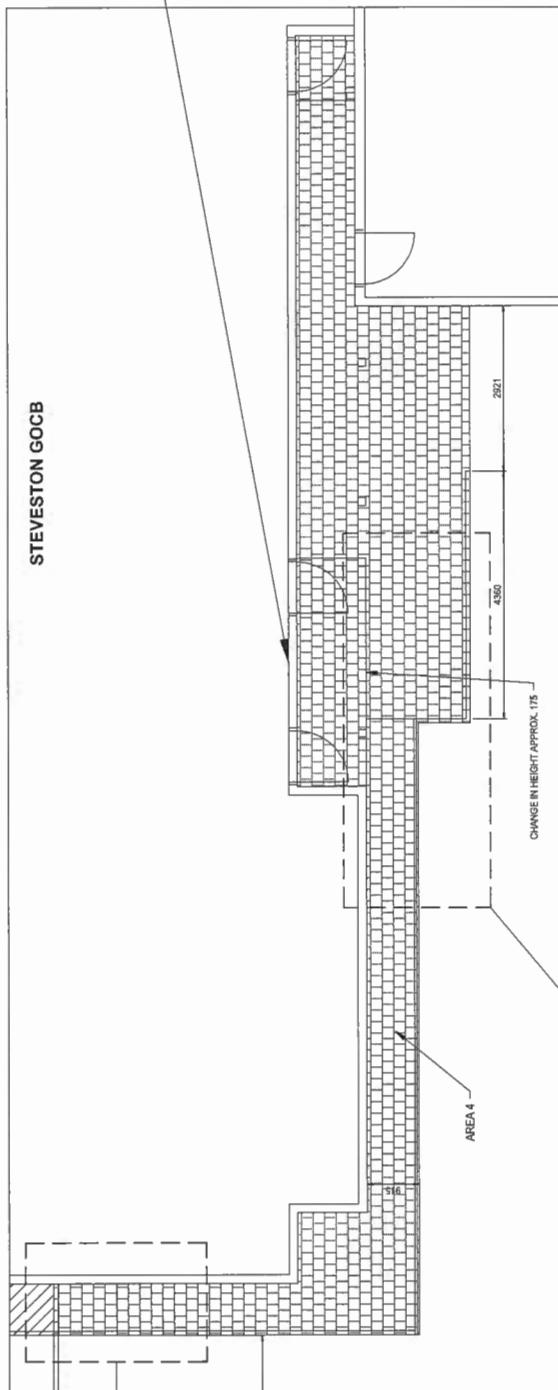
DP 10 85 8887 plan #5

Project No./No. du projet	Sheet/Feuille	Revision no./N° de révision
GOC375231	A05	0

(MAE - 20170614)



STEVESTON GOCB



5	HERITAGE AMPLIFICATION PERMIT	2019/05/29
4	ISSUED FOR PERMIT	2019/02/29
3	ISSUED FOR TENDER	2019/05/24
2	BIM DESIGN COMPLETION	2019/01/10
1	EDC Design Completion – Extended	2019/12/18
0	EDC Design Completion	2019/11/16
Reference/Description/ Client/Officer		Ref No/Date

BROOKFIELD GLOBAL
INTEGRATED SOLUTIONS

SUITE 910-1045, HOWE STREET
VANCOUVER, BC

UPGRADE STEPS - PLATFORM

UPGRADE STEPS - PLATFORM - RAMP

Construction Signature Box Only
IMOCUAG & ASSOCIATES ENGINEERING LTD.
Designed by/Conceived per
G.C.
Drawn by /Paschalis per
R.F.-JUN 15, 2019
PHYSIC Project Manager /Administrator de Projet TSGS500
CATHERINE GHOU
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Drawing title/Titre du dessin
PLAN VIEW - AREA A

(MAE : 20170614)
 Projekt-Nr./No. des projekts: GOC375231
 Status/Freizeit: A06
 Projekt-Info-/Projektinfo: OF A06
 Projekt-Info-/Projektinfo: 0

DMSXXXXX

plan #6

DP 19-858887

PLAN VIEW - AREA 4
SCALE: 1:100

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City of Richmond

Heritage Alteration Permit

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

File No.: HA19-858886

To the Holder: Gordon Chan

Property Address: 12551 No.1 Road

Legal Description: Lot 1 Section 10 Block 3 North Range 7 West New Westminster District Plan
BCP49814

(s.972, *Local Government Act*)

1. (Reason for Permit) Designated Heritage Property (s.967)
 Property Subject to Temporary Protection (s.965)
 Property Subject to Heritage Revitalization Agreement (s.972)
 Property in Heritage Conservation Area (s.971)
 Property Subject to s.219 Heritage Covenant
2. This Heritage Alteration Permit is issued in accordance with approved Development Permit DP 19-858887.
3. This Heritage Alteration Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto.
4. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse.

AUTHORIZING RESOLUTION NO. <Resolution No.> ISSUED BY THE COUNCIL THE DAY OF <Date>

DELIVERED THIS <Day> DAY OF <Month>, <Year>

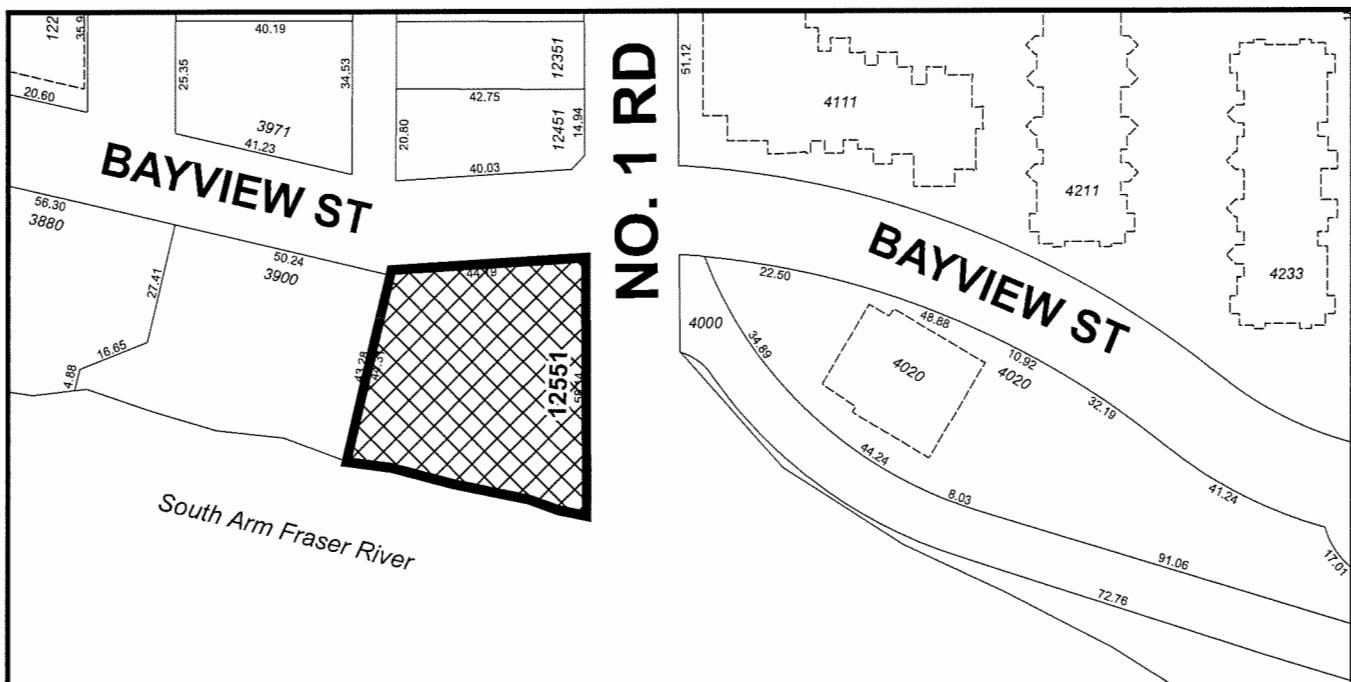
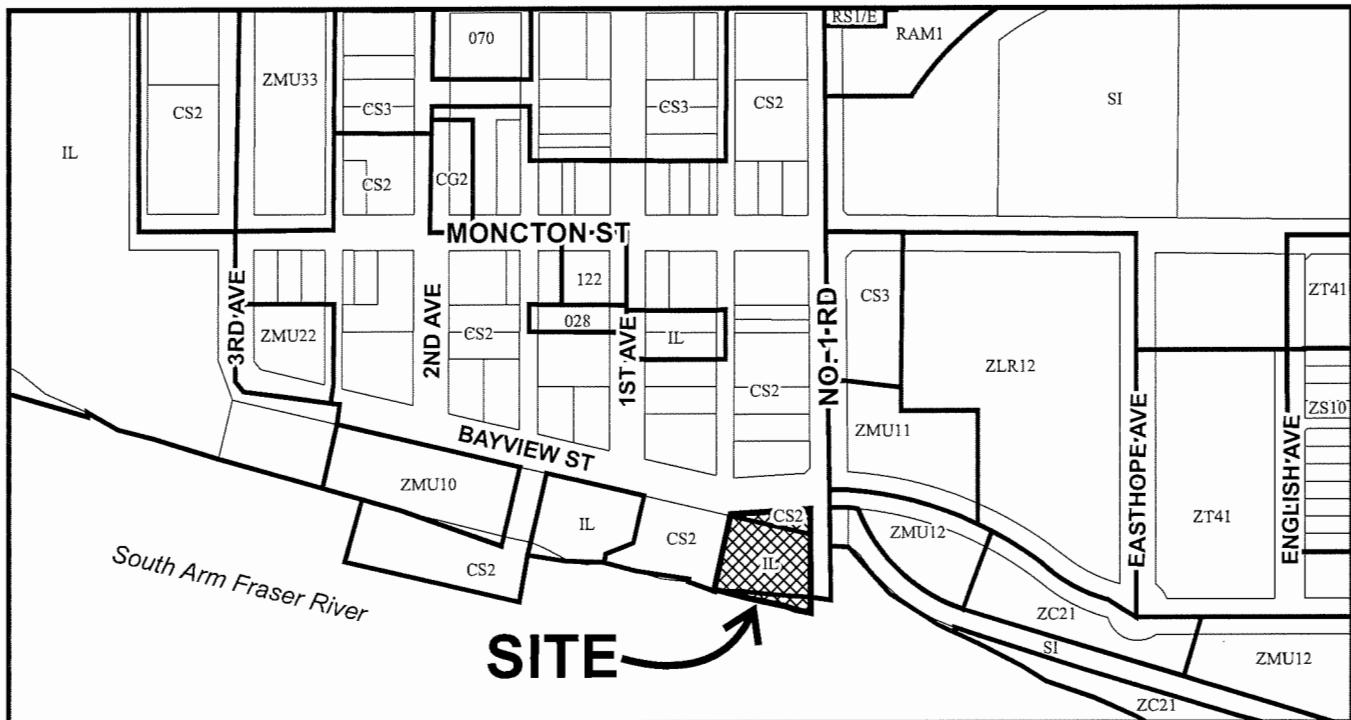
MAYOR

CORPORATE OFFICER

IT IS AN OFFENCE UNDER THE LOCAL GOVERNMENT ACT, PUNISHABLE BY A FINE OF UP TO \$50,000 IN THE CASE OF AN INDIVIDUAL AND \$1,000,000 IN THE CASE OF A CORPORATION, FOR THE HOLDER OF THIS PERMIT TO FAIL TO COMPLY WITH THE REQUIREMENTS AND CONDITIONS OF THE PERMIT.



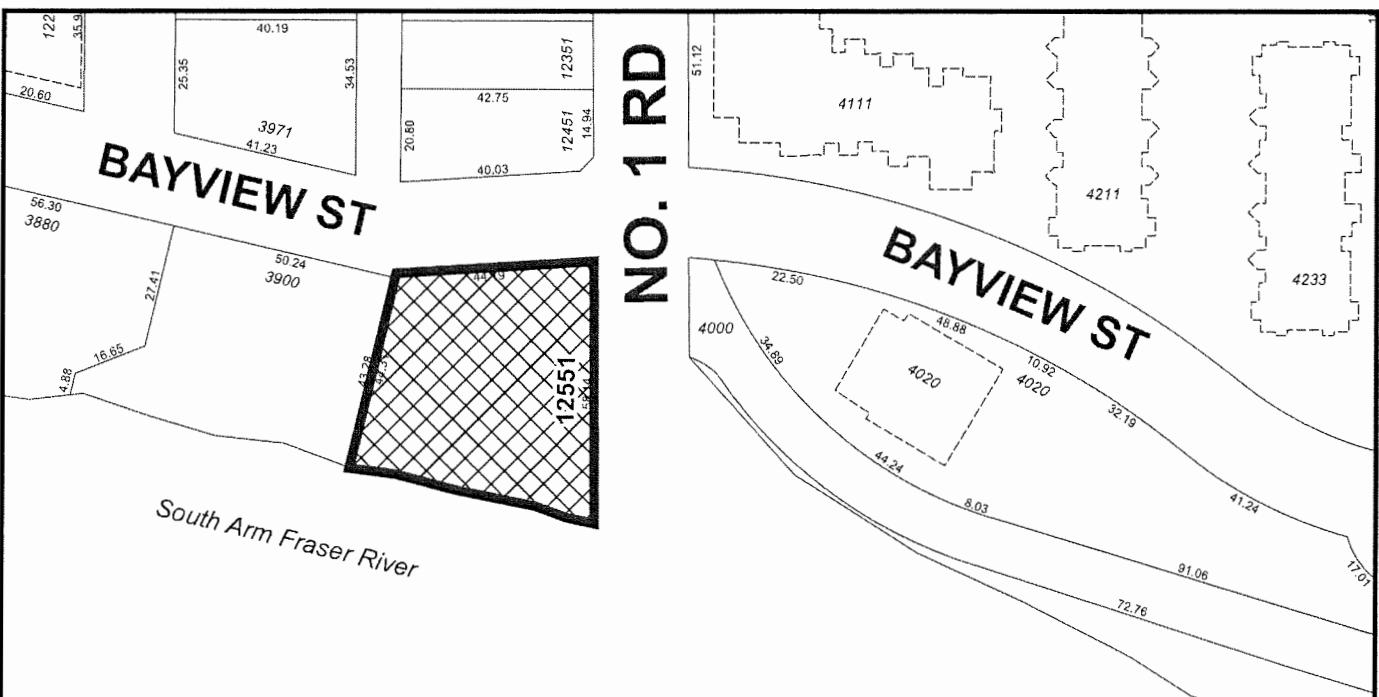
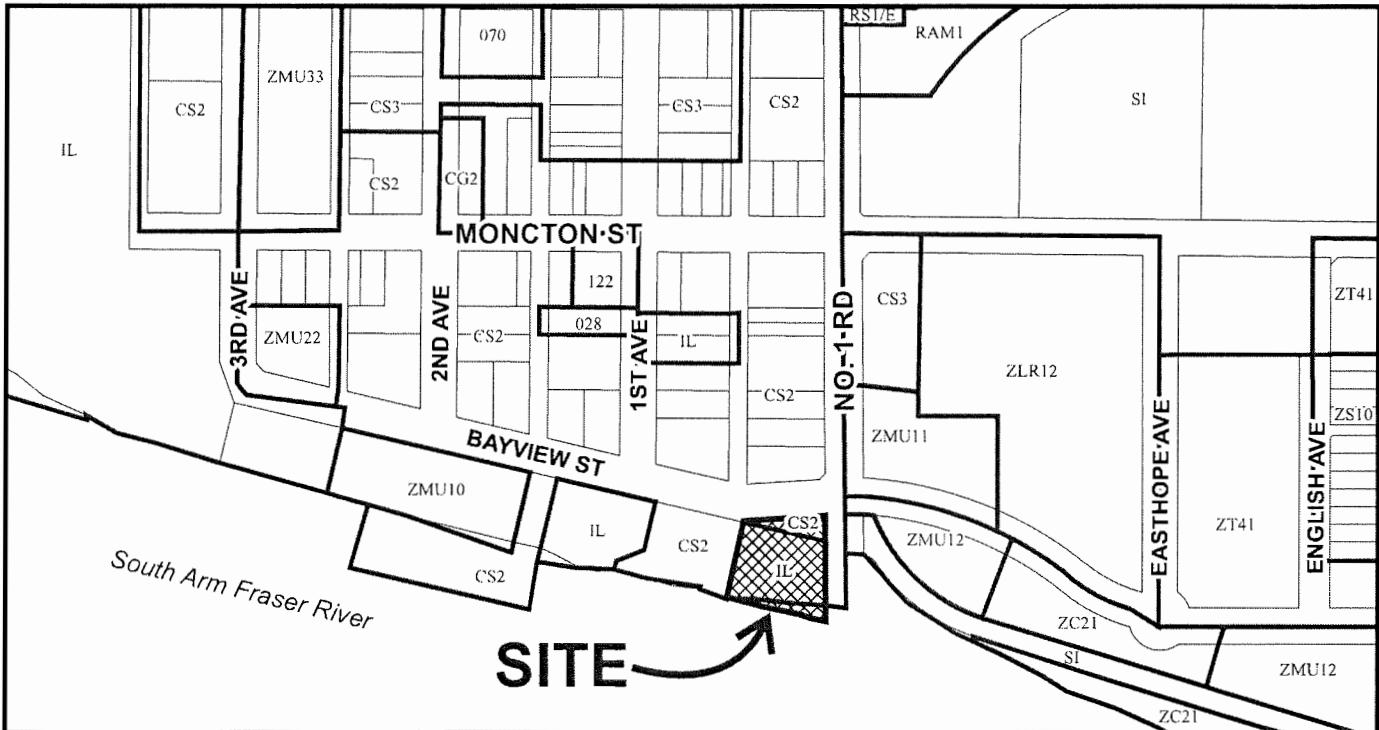
City of Richmond



	HA 19-858886	Original Date: 05/06/19 Revision Date: 05/07/19 Note: Dimensions are in METRES
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City of Richmond



	DP 19-858887	Original Date: 05/06/19 Revision Date: 05/07/19 Note: Dimensions are in METRES
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City of Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: August 6, 2019

From: Wayne Craig
Director of Development

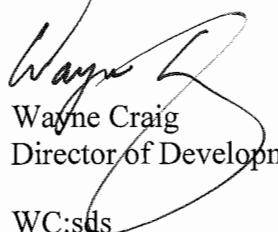
File: DP 18-829241

Re: Application by 1076694 BC Ltd. for a Development Permit at 10451, 10471 &
10491 No. 2 Road

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of 12 townhouse units at 10451, 10471 & 10491 No. 2 Road on a site zoned “Low Density Townhouses (RTL4)”; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - a) Reduce the minimum front yard setback from 6.0 m to 4.5 m; and
 - b) Allow one small car parking stall in each of the side-by-side garages (12 small car stalls in total).


Wayne Craig
Director of Development

WC:sds
Att. 4

Staff Report

Origin

1076694 BC Ltd. has applied to the City of Richmond for permission to develop 12 townhouse units at 10451, 10471 & 10491 No. 2 Road. The subject site is being rezoned from “Single Detached (RS1/E)” zone to “Low Density Townhouses (RTL4)” zone under Bylaw 9872 (RZ 17-778834), which received third reading following the Public Hearing on June 18, 2018. The subject site previously contained three single-family dwellings, which have been demolished.

A Servicing Agreement (SA 18-824442) is also associated with the proposal, which is a condition of Rezoning approval and includes, but is not limited to, the following improvements:

- new concrete sidewalk and grassed and treed boulevard along No. 2 Road frontage; and
- a new concrete bus pad at the existing southbound bus stop on No. 2 Road.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the North: Single-family dwelling on a lot zoned “Single Detached (RS1/E)” fronting No. 2 Road.

To the East: Across No. 2 Road, 133-unit townhouse development on a lot zoned “Town Housing (ZT72) – London/Steveston (No. 2 Road)”, a 9 m (30 ft.) wide public greenway connecting No. 2 Road to Steveston-London Park/School, and single-family dwellings on lots zoned “Single Detached (RS1/E)” fronting Dylan Place.

To the South: Single-family dwellings on lots zoned “Single Detached (RS1/E)” fronting No. 2 Road.

To the West: Single-family dwellings on lots zoned “Single Detached (RS1/B)” and a duplex zoned “Two-Unit Dwellings (RD1)” fronting Kozier Drive.

Rezoning and Public Hearing Results

The Public Hearing for the rezoning of this site was held on June 18, 2018. At the Public Hearing, the following concerns were expressed by the property owner of the neighbouring property at 10526 Kozier Drive:

- the existing retaining wall in place between the neighbouring property and 10491 No. 2 Road is in poor condition;
- construction at 10491 No. 2 Road by previous owners caused damage to the property owner home’s foundation;

- installation of perimeter fencing during demolition and construction; and
- unsightly premises with regard to tall grass on the subject site's lawn.

Staff worked with the applicant to address these issues in the following ways:

- the developer met with the property owner and agreed to replace the existing retaining wall through the development process at the Developer's cost. The City's Engineering department reviewed the proposed new retaining wall (shown on the plans) and has no concerns;
- the developer has provided a construction impact and management plan to address concerns of further foundation damage, including a pre and post construction assessment, and involvement of a geotechnical consultant, which was shared with the neighbours;
- the developer has noted perimeter fencing will be maintained on-site during all phases of construction; and
- the City's Community Bylaws Department initiated an unsightly premises investigation based on the neighbour's concerns, which the developer addressed by completing yard maintenance, contracted on a bi-weekly basis, to maintain the premises until demolition.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan (OCP) and is generally in compliance with the "Low Density Townhouse (RTL4)" zone except for the zoning variances noted below.

Zoning Compliance/Variances (staff comments in **bold)**

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Reduce the minimum front yard setback from 6.0 m to 4.5 m;
 - *The proposed variance is consistent with the Arterial Road Guidelines for Townhouses in the OCP and allows for a 6.0 m rear yard setback to both the ground and second floors of the rear units adjacent to single-family development. This provides an improved rear yard interface with the existing single-family dwellings to the west and enhances solar access to the rear yards.*
 - *The proposed front yard setback ranges from 4.5 m to 6.0 m in order to reflect the scale and articulation found in single-family areas and avoid long, continuous and unarticulated building frontages along the street. Approximately 25% of the front building face is setback 4.5 m from the front property line, with the remaining building face proposed to be setback 4.9 m, 5.2 m and 6.0 m. The varied front yard setback also provides an adequate transition to the existing single-family dwellings to the north and south.*
 - *The location of the existing curb along No. 2 Road will not change as there is no road widening required. New grassed/treed boulevard and concrete sidewalk will be provided between the existing curb and the front property line. The shortest distance*

between the proposed building face and the back of curb on No. 2 Road would be approximately 7.4 m.

- *The proposed location of the buildings along No. 2 Road frontage accommodates the proposed shared outdoor amenity space, which is located in the northeast corner in order to maximize tree protection opportunities.*
 - *The variance was identified at rezoning and no concerns were raised.*
- 2) Allow one small car parking stall in each of the side-by-side garages (12 small car stalls in total).
 - *Staff are supportive of the proposed variance as it enables the required resident vehicle parking spaces to be provided within the garages of each unit, in a side-by-side arrangement. The Zoning Bylaw allows a maximum 50% of the spaces provided to be small vehicle parking stalls for on-site parking areas which contain 31 or more spaces.*
 - *The variance was identified at rezoning and no concerns were raised.*

Advisory Design Panel Comments

The Advisory Design Panel (ADP) reviewed the proposal on January 8, 2019. A copy of the relevant excerpt from the Advisory Design Panel Minutes from January 8, 2019 is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '***bold italics***'.

Analysis

Conditions of Adjacency

- As per the Arterial Road Land Use Policy, the building height of the rear units is 9 m (two storeys) in response to the single-family rear yard adjacency to the west. In addition, a 6.0 m rear yard setback has been provided to both the ground and second floors of the rear units to provide an improved interface and increased sun exposure. Duplex units in three townhouse clusters are proposed along this interface, in order to break up the massing.
- The subject site is also bordered by single-family development to the north and south, which is identified for townhouse development in the Arterial Road Land Use Policy. The location of the shared outdoor amenity area in the northeast corner of the site, along with the retention of a significant hedge along the north property line, provides a buffer between the proposed townhouse units and the adjacent single-family development to the north. The three-storey townhouse units along No. 2 Road are reduced to two storeys within 7.5 m of the side property lines to provide an adequate transition to the north and south.
- The applicant has provided a shadow study to demonstrate how the proposed height and setbacks are articulated to address potential shadowing over adjacent properties to the north, south and west.
- A statutory right-of-way (SRW) allowing access to/from the adjacent future development sites through the subject site (over the entry driveway and internal drive aisle) has been secured at rezoning.
- Retaining walls will be provided along the west property line in order to provide usable yard space for the proposed units (ranging from 0.29 m to 0.87 m). A lower 1.2 m high timber

fence will be provided on top of the retaining wall to reduce the apparent height of the grade change, but also maintain privacy (cross-sections are provided in Plan #3.G).

- Perimeter drainage will be required as part of the Building Permit to ensure storm water is managed and addressed through the development and will not impact neighbouring properties.

Urban Design and Site Planning

- Vehicular access to the proposed development is to be from No. 2 Road through a new driveway in the middle of the site, with future connections to the neighbouring properties to the north and south, secured by a SRW.
- The layout of the townhouse buildings is oriented around a north-south internal maneuvering drive-aisle, providing access to the unit garages.
- Units along No. 2 Road will have direct pedestrian access from the street and the units along the rear will have access from the internal drive aisle.
- All townhouse units will have two vehicle parking spaces in side-by-side arrangement. A variance is requested for one small vehicle parking space in each garage, for a total of 12 small spaces. No tandem parking spaces are proposed.
- A total of three visitor parking spaces will be provided, including an accessible parking space. The number of parking spaces proposed is in compliance with the minimum Zoning Bylaw requirement.
- Both internal and external bicycle parking spaces have been incorporated into the proposal and are in compliance with Zoning Bylaw requirements.
- The shared outdoor amenity area is located in the northeast corner of the site, along No. 2 Road, in order to maximize tree retention opportunities. This location also offers casual surveillance from the street and adjacent units. The size of the outdoor amenity area is consistent with the OCP requirement (6.0 m^2 per unit).
- Cash-in-lieu of indoor amenity space was secured through rezoning (\$12,000) consistent with OCP policy.
- The proposal will utilize door-to-door pickup and has accommodated space for garbage and recycling containers within the individual garages and in front of the units without obstructing the drive-aisle. The City's Environmental Programs department reviewed the arrangement and has no concerns. The mail kiosk has been incorporated into the design of the buildings to minimize visual impact.

Architectural Form and Character

- The buildings were designed as a modern interpretation of the French Provincial style.
- Hipped roofs with steep pitches and bell roof detailing, balanced symmetrical proportions, tall second storey windows and stucco finish are distinctive characteristics of this architectural style.
- The street fronting units will have individual canopies. A pedestrian scale is generally achieved along the public streets and internal drive-aisle through the inclusion of variation in building form, projections, recesses, materials/colours and landscape features.
- Neutral tones and materials provide a contrast to the dark roof. Proposed building materials include predominantly stucco and stucco detailing and moulding.
- Juliette balconies, ornate trims, round dormers and oval windows are also provided to enhance the character.

Landscape Design and Open Space Design

- Tree preservation was reviewed at rezoning stage where five on-site trees were identified for removal. Based on a 2:1 tree replacement ratio stated in the OCP, 10 replacement trees on-site are required. The applicant is proposing to plant 16 replacement trees as per the size requirements based on the City's Tree Protection Bylaw.
- Four on-site trees (tag# 25, 26, 28, 30) located on the subject property are proposed to be protected. Two trees (tag# 25 & 26) are located in the shared outdoor amenity area in the northeast corner and two trees (tag# 28 & 30) are located in southwest corner in the rear yard. The shared outdoor amenity area was located in the northeast corner to maximize tree retention opportunities. A contract with a Certified Arborist and a tree survival security of \$35,000 were secured through rezoning for the retention of these trees.
- The shared outdoor amenity area is designed at multiple grades to ensure the retention of the significant trees, while still being accessible. A circular wood bench is proposed around one of the significant trees to retained, additional seating in the upper area and significant planting are provided to activate the space and provide opportunities for social interaction. The two areas are linked by an accessible ramp along the north side of the area and stepping stones down into the tree protection area.
- The proposal provides a pedestrian-oriented streetscape fronting No. 2 Road with a landscaped edge treatment including a new tree in each front yard, lawn and patio areas, low transparent fencing, and a gate to each street fronting unit.
- All units along the rear will have a private yard with a patio and a lawn area. Various hedges, shrubs and ground covers have been selected to ensure the landscape treatment remains interesting throughout the year.
- Pockets of landscaping will be provided along the drive-aisle between entry doors/garages to soften the internal streetscape.
- An irrigation system will be provided for the proposed landscaping on-site.
- A children's play area is proposed in the shared outdoor amenity area, and play equipment has been chosen to provide different play opportunities (i.e. climbing, social, imagination, balance, motor skills) that can be used by different age groups and for multiple purposes. Seating is provided nearby for caregivers.
- The drive-aisle will be treated with pavers along with a concrete pathway for pedestrians. The surface parking spaces, patios and entry pathways will be treated with permeable pavers for better water infiltration and variety in paving surfaces. Coloured concrete circles are provided to indicate the transition from the drive-aisle to the shared outdoor amenity area.
- In order to ensure the proposed landscaping works are completed, the applicant is required to provide a landscape security of \$129,494.75 prior to issuance of the Development Permit.

Crime Prevention Through Environmental Design

- Windows are located to increase the visual presence and surveillance along No. 2 Road, the amenity area, as well as the internal drive aisle. All entrances are visible and overlooked by pedestrians or by neighbour's windows.
- Site lighting and clear site lines provide unobstructed views of surrounding area.
- Low permeable fencing and low landscaping are provided to maximize clear unobstructed views and casual surveillance opportunities.
- Landscaping and fencing is provided along the street edge to distinguish the public realm and private space.

- Good neighbour fencing is provided throughout the site and is 1.2 m high to encourage interaction between neighbours and foster a sense of community. The proposed fencing was discussed with the existing neighbours and had no concerns.
- Security lighting will be installed including wall mounted lighting for the drive-aisle and lighting at entries to provide adequate outdoor security illumination.

Sustainability

- As the application was submitted prior to the introduction of the Step Code requirement, the applicant has provided a report produced by a Certified Energy Advisor to confirm that the proposed townhouse units will be designed to achieve an EnerGuide rating of 82. The report prepared by the Energy Advisor is on file and will be utilized through the Building Permit review process to ensure these measures are incorporated in the Building Permit drawings.
- An air source heat pump system will be used for this development. The applicant has provided an acoustical report from an acoustical consultant indicating the proposed units, with consideration of the proposed location, will comply with the City's Noise Bylaw.
- The architect advised that the following design/features are incorporated into the proposal:
 - Energy star windows with low E-glazing to alleviate heating and cooling energy consumption;
 - Climate-based automatic irrigation controls to minimize use of water on site;
 - The use of low-flow water fixtures, low-energy lighting and high energy efficiency appliances;
 - Durable and long-lasting materials that can reduce building maintenance;
 - Permeable ground cover and planting to absorb rainwater runoff and reduce load on municipal sewers; and
 - Interior paint with low VOC content.

Accessible Housing

- The proposed development includes one convertible unit that is designed with the potential to be easily renovated to accommodate a future resident in a wheelchair. The potential conversion of these units will require installation of a chair lift in the staircase of Unit 11E.
- All of the proposed units incorporate aging in place features to accommodate mobility constraints associated with aging. These features include:
 - stairwell hand rails;
 - lever-type handles for plumbing fixtures and door handles; and
 - solid blocking in washroom walls to facilitate future grab bar installation beside toilets, bathtubs and showers.

Conclusions

As the proposed development would meet applicable policies and Development Permit Guidelines, staff recommend that the Development Permit be endorsed, and issuance by Council be recommended.

The list of Development Permit Considerations is included in Attachment 3, which has been agreed to by the applicant (signed concurrence on file).

August 6, 2019

- 8 -

DP 18-829241



Steven De Sousa
Planner 1

SDS:cas

Attachment 1: Development Application Data Sheet

Attachment 2: Excerpt from the Meeting Minutes of the ADP (January 8, 2019)

Attachment 3: Development Permit Considerations



City of Richmond

Development Application Data Sheet

Development Applications Department

DP 18-829241

Attachment 1

Address: 10451, 10471 & 10491 No. 2 Road

Applicant: 1076694 BC Ltd. Owner: 1076694 BC Ltd.

Planning Area(s): Steveston

Floor Area Gross: 2,376.5 m² (25,580 ft²) Floor Area Net: 1,701.3 m² (18,312 ft²)

	Existing	Proposed
Site Area:	2,833.7 m ² (30,502 ft ²)	No change
Land Uses:	Single-family residential	Multiple-family residential
OCP Designation:	Neighbourhood Residential (NRES)	No change
Zoning:	Single Detached (RS1/E)	Low Density Townhouses (RTL4)
Number of Units:	3	12

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.6	0.6	None permitted
Lot Coverage – Buildings:	Max. 40%	40%	None
Lot Coverage – Non-porous:	Max. 65%	65%	None
Lot Coverage – Live Landscaping:	Min. 25%	26%	None
Setback – Front Yard:	Min. 6.0 m	4.5 m	Variance requested
Setback – Side Yard:	Min. 3.0 m	3.0 m	None
Setback – Rear Yard:	Min. 3.0 m	6.0 m	None
Height:	Max. 12.0 m (3 storeys)	Front buildings: 12.0 m (3 storeys) Rear buildings: 9.0 m (2 storeys)	None
Lot Width:	Min. 50.0 m	65.3 m	None
Lot Depth:	Min. 35.0 m	42.3 m	None
Lot Size:	N/A	2,833.7 m ²	None
Off-street Parking Spaces – Regular (R) / Visitor (V):	2 (R) and 0.2 (V) per unit	2 (R) and 0.2 (V)	None

Off-street Parking Spaces – Accessible:	Min 2% when three or more visitor stalls required = 1	1	None
Total off-street Spaces:	24 (R) and 3 (V)	24 (R) and 3 (V)	None
Tandem Parking Spaces:	Permitted – Max 50% of required spaces	0	None
Small Car Parking Spaces:	None when fewer than 31 spaces are provided on-site	12	Variance requested
Bicycle Parking Spaces – Class 1:	1.25 per dwelling unit	1.3 per dwelling unit	None
Bicycle Parking Spaces – Class 2:	0.2 per dwelling unit	0.2 per dwelling unit	None
Bicycle Parking Spaces – Total:	15 Class 1 and 3 Class 2	16 Class 1 and 3 Class 2	None
Amenity Space – Indoor:	Min. 50 m ² or cash-in-lieu	Cash-in-lieu	None
Amenity Space – Outdoor:	Min. 6 m ² per unit = 72 m ²	77 m ²	None

**Excerpt from the Minutes from
The Design Panel Meeting**

**Tuesday, January 8, 2019 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall**

DP 18-829241 – 12-UNIT TOWNHOUSE DEVELOPMENT

ARCHITECT: Fougere Architecture Inc.
LANDSCAPE ARCHITECT: Van der Zalm and Associates
PROPERTY LOCATION: 10451, 10471, and 10491 No. 2 Road

Applicant's Presentation

Wayne Fougere, Fougere Architecture Ltd., and Stephen Heller, Van der Zalm and Associates, presented the project and answered queries from the Panel on behalf of the applicant.

Panel Discussion

Comments from Panel members were as follows:

- investigate opportunities for introducing a gap at the centre of the rear buildings and having two-storey buildings flank the gap to extend the visibility at the entry driveway through the site;

The OCP does not support 3-plexes along the rear yard. Introducing a gap would only be possible at the expanse of a unit which is not acceptable.

Also, one of the rear neighbours previously expressed concern about being exposed to No. 2 Road during construction (when buildings are demolished), we therefore believe our neighbours appreciate a buffer between them and the Arterial Road.

- consider increasing the gap between the two-storey rear buildings;
We provide 2.8 m in between buildings (at the tightest part) and increase the gap to 3.4m in between the entries columns. Furthermore the setback entries visually open up the space between buildings. We believe the gap is adequate and in line with what is typically provided in townhouse developments.

- look at the upper storeys on the three-storey buildings which step back on No. 2 Road but overhang the internal driveway at the back; review the scale to avoid becoming overbearing;

If we were to reduce the overhang on the internal drive, we would need to increase the 3rd floor massing on No. 2 Road. We believe it's better to maintain the dormer look on the front as the overhang at the drive aisle is minimal. 3rd storey is already reduced in size- it is less than 1/3 of the total building width (total width: 21.9 m; width of 3rd storey: 6.3 m).

- consider introducing a chimney element on the roof of the centre building at the rear to help focus on this character expression in the front buildings;

The maximum height limit of 9.0m prevents adding chimneys to the rear buildings.

- investigate opportunities for increasing the separation between the rear duplex buildings (especially Building 4) and the three-storey buildings fronting No. 2 Road through articulation of the rear duplex buildings along the internal drive aisle;

The rear buildings are articulated along the internal drive aisle by stepping back at the entries. Any more articulation (setbacks) would result in a decrease of habitable area and an increase of drive aisle area which is, in our opinion, not a good trade off.

- appreciate the limited materials and colour palette, helps pull the project concept together;

Noted.

- consider slightly varying the colours of the individual unit doors to provide more identity to the residential units;

The previous comment noted that the limited colour palette is beneficial to the design. We agree with this sentiment and don't think that adding different colours to doors would enhance the look.

- the project is straightforward;

Noted.

- understand that the City's Arterial Road Guidelines for Townhouses provide guidance regarding arrangement and scale of buildings, in particular the rear units of arterial road developments; however, would appreciate if an exception could be made for the subject development to improve its proposed site lay-out;

It's our opinion that the site layout is conforming to regulations and acceptable to our neighbours and the City.

- understand the site constraints and appreciate the applicant's intent to save the existing trees, however the proposed site for the outdoor amenity area is not appropriate as it is (i) disconnected from other buildings in the development due to its corner location, (ii) located in the part of the property that receives the least amount of sun exposure, and (iii) limited in terms of accessibility and size of the flat area;
 - i) Due to the small size of the development the maximum distance from any unit to the open space is very limited. Also, the location at the end of the internal road could be seen as a bonus since it removes the play area from any vehicular traffic.*
 - ii) The current location was agreed upon by both the City and the developer in order to save the trees. In the Arterial Road Design Guidelines the only other acceptable location would be in between the rear buildings. This location would have the same issues in terms of sun exposure.*
 - iii) See response to the following comments for a description of the improvements to the amenity area.*
- Consider utilizing the space south of the existing trees area in the outdoor amenity area for access to minimize shadowing from Building 1 (Unit 3D) and to enhance the accessibility of the outdoor amenity area from the internal drive aisle;

Relocating the access point wouldn't have an impact on the shadowing. The access at the end of the road is more practical as the road slopes down, reducing the height difference between road and existing grade. An access south of the trees would require a very long ramp, which would conflict with the tree protection area.
- consider eliminating the proposed play equipment in the outdoor amenity area and take advantage of the topography of the area to design the whole amenity area as a play space;

Previous play house has been eliminated as requested. A new playhouse has been added as requested. Topography proposes terraces, as with the previous concept. Refer to Section 3, sheet 3F. Amenity area re-designed as play space with seating.
- consider a more accessible surface on the outdoor amenity area; also consider eliminating the concrete stepping stones on the outdoor amenity area as they appear discouraging and not "visually accessible" from an accessibility point of view;

Concrete stepping stones have been removed. Upper play area is accessible. The transition from the lane to the mulch area will be a disappearing concrete edge. Wheelchairs could still roll into the lower area and move around with assistance.

- the outdoor amenity space is limited and the play area appears stark due to the provision of just a single play element; the applicant could introduce something more creative;

Formal play equipment now accompanied by natural play elements.

- review the design of the outdoor amenity area to make it more visually appealing and stimulating for children;

The revised circular design is more visually appealing than the previous design.

- concerned about the proposed location of the exterior air handling units between the rear buildings due to potential noise issues; consider relocating the outdoor units to the rear yards;

A review by our Acoustical Engineer determined that the noise level emitted by the HVAC units is within the permitted sound level of the Noise Regulation By-law 8856.

- appreciate the proposed sustainability features of the project; the project is on the right track from a sustainability point of view;

Noted.

- the checklist for achieving EnerGuide 82 rating was not included in the package provided to the Panel;

Noted. It was provided at DP Resubmission on November 27, 2018.

- notice the 4/12 slope for the roof of the rear units; the applicant is reminded of the requirement to provide adequate ventilation for the attic space;

Noted.

- consider relocating the outdoor amenity area to the south end of the site;

The current location was agreed upon by both the City and the developer in order to save the trees.

- consider eliminating the proposed play equipment and redesigning the play area using natural materials, e.g. logs and boulders, to provide more interest and play opportunities for children; would also be more economical to build;

The play area has been re-designed to include natural materials. The formal play structure remains.

- consider installing a deck adjacent to the retained trees on the rear yards of rear units to increase the size of the rear yards;

The wood deck in the rear yards will not be reinstated due to the neighbour's concerns.

- concerned on the cross access easement; buyers should be informed on this arrangement;

While cross access easement is a common occurrence on No. 2 Road projects, future buyers will be informed about this.

- appreciate the project; the applicant has done well considering the site constraints;

Noted.

- the rear units appear inferior to the front units in terms of the level of development, materiality and detailing; the steep roof slopes for the front units are successful; however, the proposed roof forms for the rear units are not.

The form and slope of these roofs were designed in order to minimize the impact on the neighbours to the back. Other than that, the elevations were designed with the same materials and the same amount of detail as the front units (Juliette balconies, ornate trims, dormers).

- difficult to tell that the buildings consist of 12 individual units due to their design, e.g. window placements, access points, doorways, and detailing; design development is needed to provide more identity to individual units without being too prescriptive;

The clearly visible front doors and address sign which will be placed on the fence column should make it clear that there are multiple units within one building.

- not clear whether the large windows at the west elevation of rear buildings are for individual units or shared by two units; review the size of these windows as they are more appropriate for large houses;

The large windows are located in the living room which has a 5m room height. It is true that these types of large openings are commonly only found in single family houses. However, it was our and our client's intent to create townhomes that feel larger and more luxurious than the typical townhouse and providing these types of features is part of this design target.

- appreciate the limited materials and colour palette; however, concerned about the long-term maintenance of Exterior Insulation and Finish Systems (EIFS) materials considering the City's climate;

EIFS is only used for details like the quoins or window frames. The majority is regular stucco.

- not clear about the types of materials for gates, fencing and pilasters; note that the rear units use inexpensive wood; review whether the proposed materials tie together;

Refer to detail drawings for complete materials palette. Higher quality aluminum fencing wraps the corners of the development and extends to the middle of Building 5 on the South and terminates at the existing retaining wall on the North. The south condition is typical for a side yard residential setting. Fencing along the North PL may be revisited if/when the neighboring property is redeveloped and the proposed lane is extended.

- the whole internal drive aisle could be designed both as a “people” space and for vehicular circulation; could be used as a play space for children and social/gathering area for residents; some soft landscape suggested;

the design of the internal road was inspired by the woonerf principle (one common space rather than dedicated vehicular and pedestrian lanes) and intended exactly as described above- as multifunctional space, not only as vehicular circulation.

The north laneway ending was revised to create a playful environment that could be used by tenants as a social gathering space.

- appreciate the applicant contributing to the City’s Public Art Fund; the applicant is encouraged to incorporate public art in the project as it will enhance the public realm and benefit the development, residents and visitors;

Due to the small size of this project and limited space for an art installation, we feel a contribution to the Art Fund will be more useful.

- support the proposed paving treatment for the internal drive aisle and the defined pedestrian pathway as they will enhance the pedestrian experience throughout the development;

Noted.

- consider slightly varying the heights of canopies above the unit doors to provide variation between the entrances to individual units;

We tried some variations on the design of the entry roofs. Unfortunately these changes were not very successful as it only added complication to the roof construction without enhancing the design.

- garbage and recycling bins should not be permanently located along the internal drive aisle; and

Garbage and recycling will be stored in each unit’s garage. Locations shown on drive aisle are only temporary locations on pick-up days.

- consider making the whole outdoor amenity area one level and using natural materials to enhance its usability and provide more play opportunities for children.

This conflicts with a previous comment to ‘take advantage of the topography’. We have not made the amenity area one level, as this makes the eastern portion of the amenity area ~ 3 feet lower than the sidewalk and the neighbouring site. Natural play elements have been added.

Panel Decision

It was moved and seconded

That DP 18-829241 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Advisory Design Panel.

CARRIED



Development Permit Considerations

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 10451, 10471 & 10491 No. 2 Road

File No.: DP 18-829241

Prior to forwarding the application to Council for approval, the developer is required to complete the following:

1. Final Adoption of Zoning Amendment Bylaw 9872.
2. Receipt of a Letter-of-Credit for landscaping in the amount of \$129,494.75. The letter-of-credit will not be returned until the Letter of Assurance, confirming the landscaping is installed as per the Development Permit, prepared by the Landscape Architect, is reviewed by staff.

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin TREE-03, and must remain in place until construction and landscaping on-site is completed.
2. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
3. Incorporation of energy efficiency, CPTED, sustainability and accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
4. If applicable, payment of latecomer agreement charges, plus applicable interest associated with eligible latecomer works.
5. If applicable, obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

Initial: _____

- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

[Signed copy on file]

Signed

Date



City of Richmond

Development Permit

No. DP 18-829241

To the Holder: 1076694 BC Ltd.

Property Address: 10451, 10471 & 10491 No. 2 Road

Address: c/o Allen Zu
155 – 3757 Jacombs Road
Richmond, BC V6V 2R3

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - a) Reduce the minimum front yard setback from 6.0 m to 4.5 m; and
 - b) Allow one small car parking stall in each of the side-by-side garages (12 small car stalls in total).
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #3 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$129,494.75 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit
No. DP 18-829241

To the Holder: 1076694 BC Ltd.
Property Address: 10451, 10471 & 10491 No. 2 Road
Address: c/o Allen Zu
155 – 3757 Jacombs Road
Richmond, BC V6V 2R3

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF ,

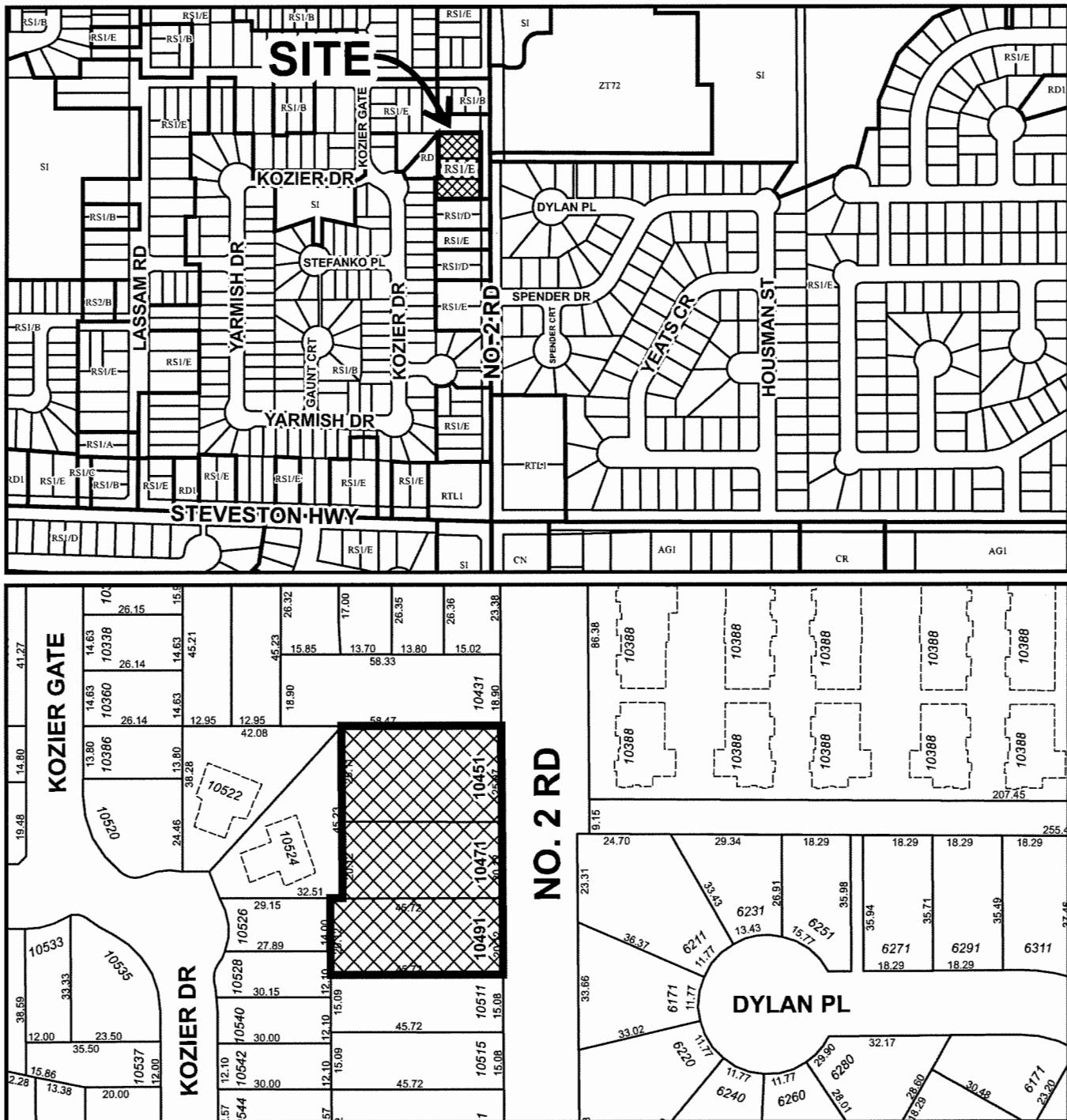
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF ,

MAYOR



City of Richmond



**DP 18-829241
SCHEDULE "A"**

Original Date: 08/21/18

Revision Date:

Note: Dimensions are in METRES

DP 18-829241 PLAN 1B

AUG 06 2019

FUGERE
architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
202-422 Queen Street
Vancouver BC V7T 4L6
fugerearchitecture.ca

DP 18-829241
APRIL 15, 2019

PARKING PLAN

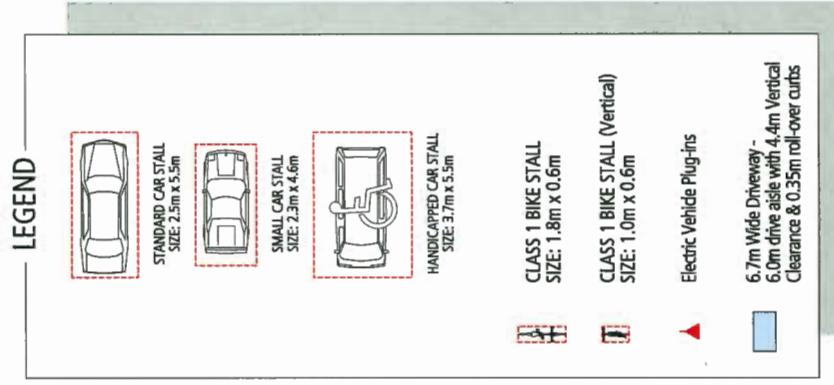
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10451-10491 NO. 2 ROAD

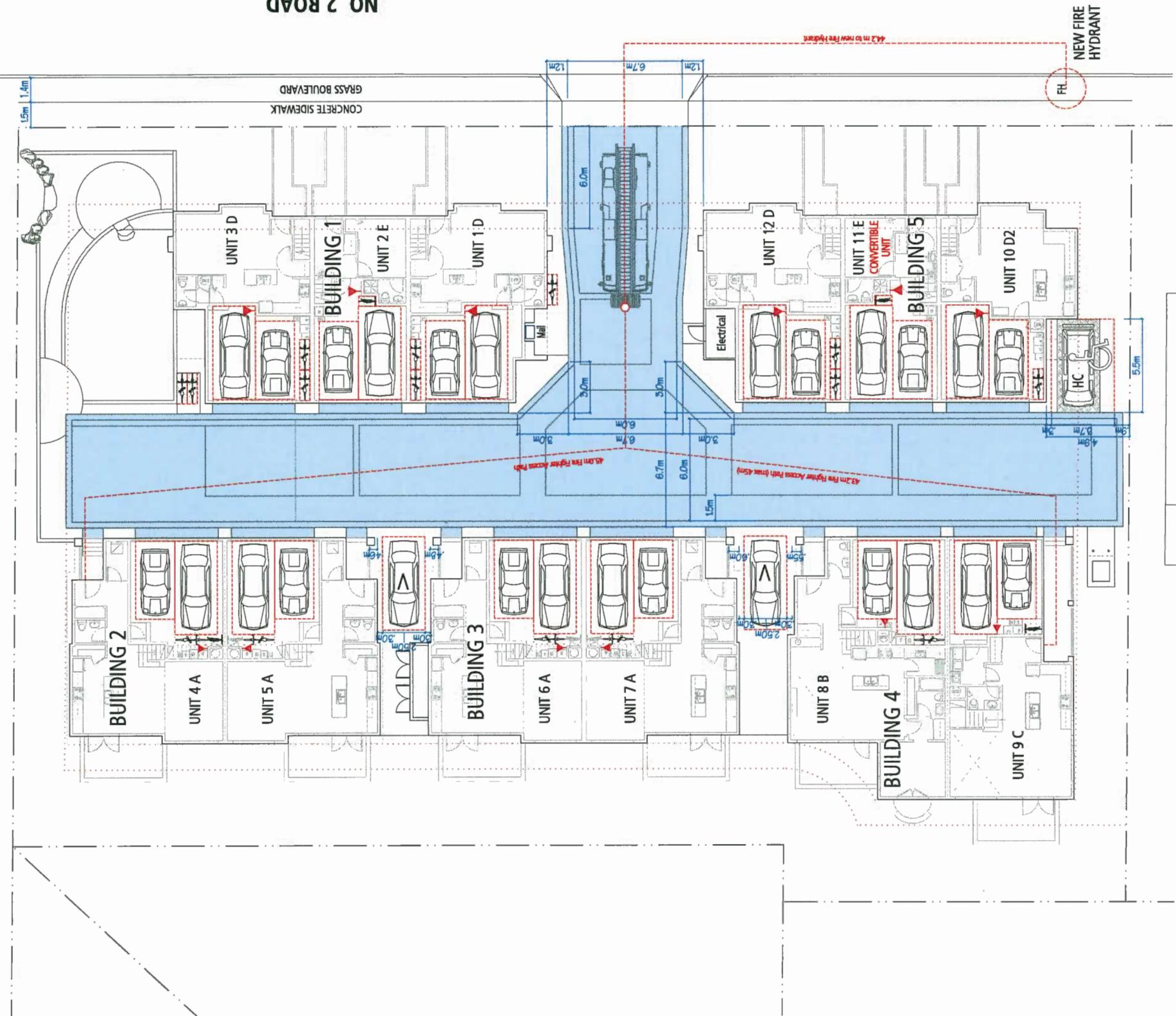
for
Woodward Williams
Limited Partnership

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NO. 2 ROAD

CROWN OF ROAD



PLAN 18-829241
AUG 06 2019

DP 18-829241
APRIL 15, 2019



DP 18-829241
APRIL 15, 2019

SITE PLAN SECOND FLOOR

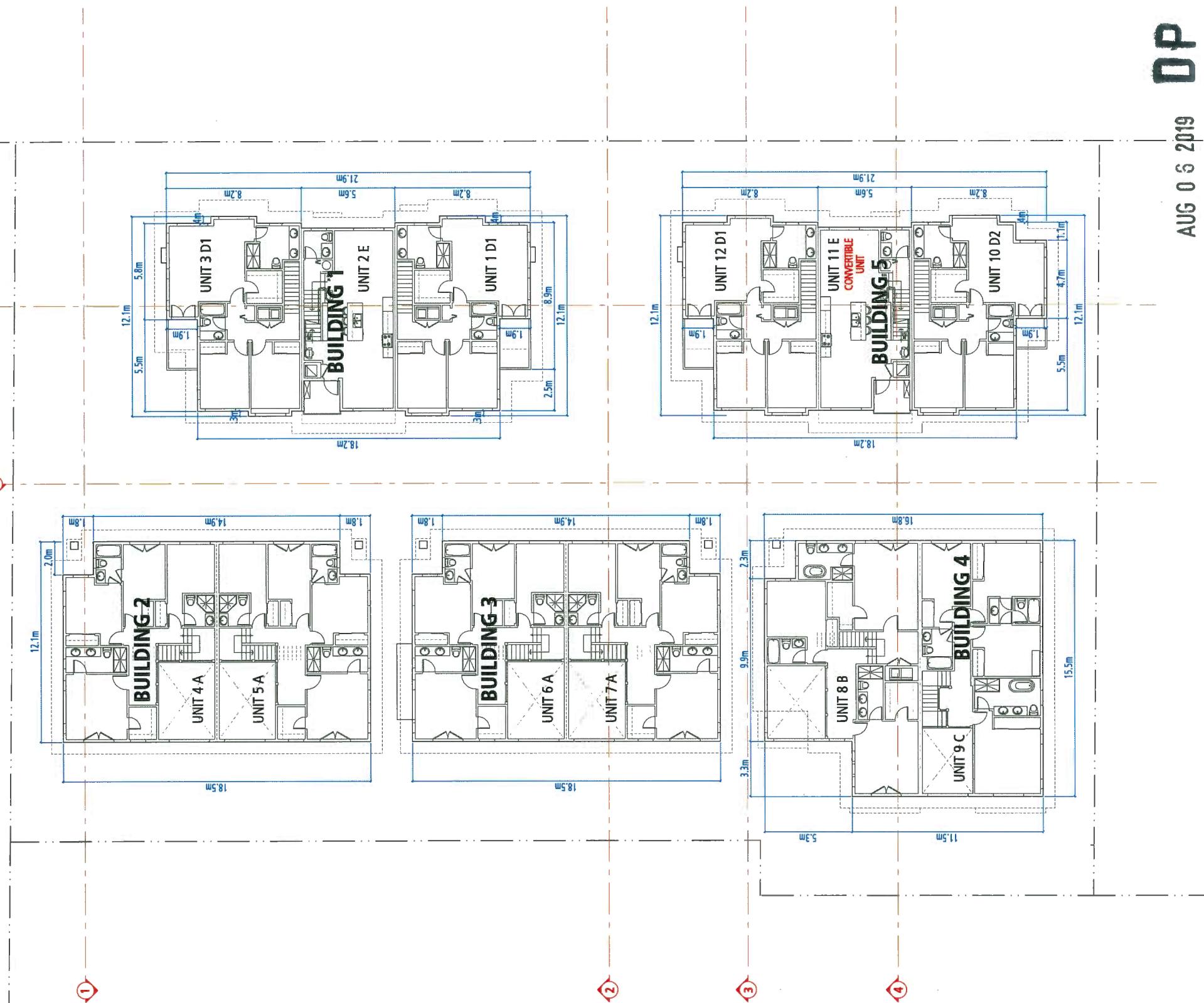
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10451-10491
NO. 2 ROAD

for
Woodward Williams
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DP 18-829241 PLAN ID
AUG 06 2019



DP 18-829241
APRIL 15, 2019

FOUGERE
architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
200-4420 Quebec Street
Vancouver, BC V5T 4L5
fougerrearchitecture.ca

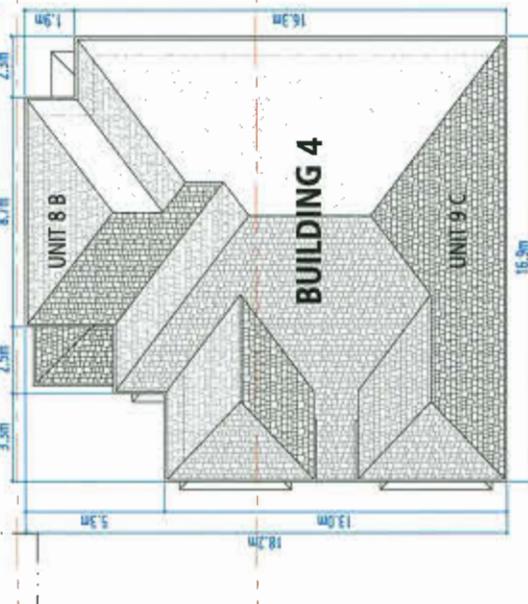
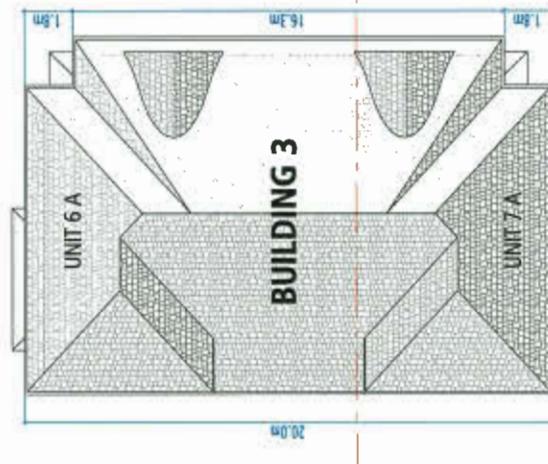
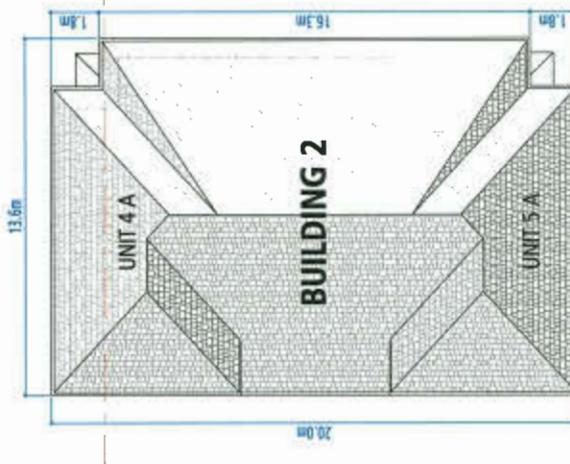
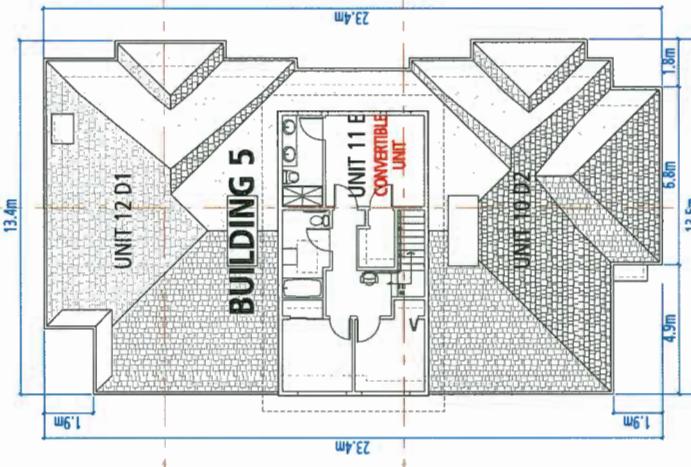
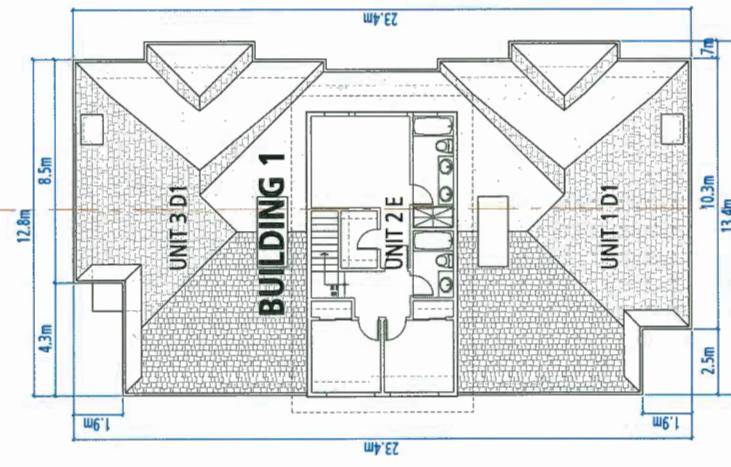
SITE PLAN THIRD FLOOR

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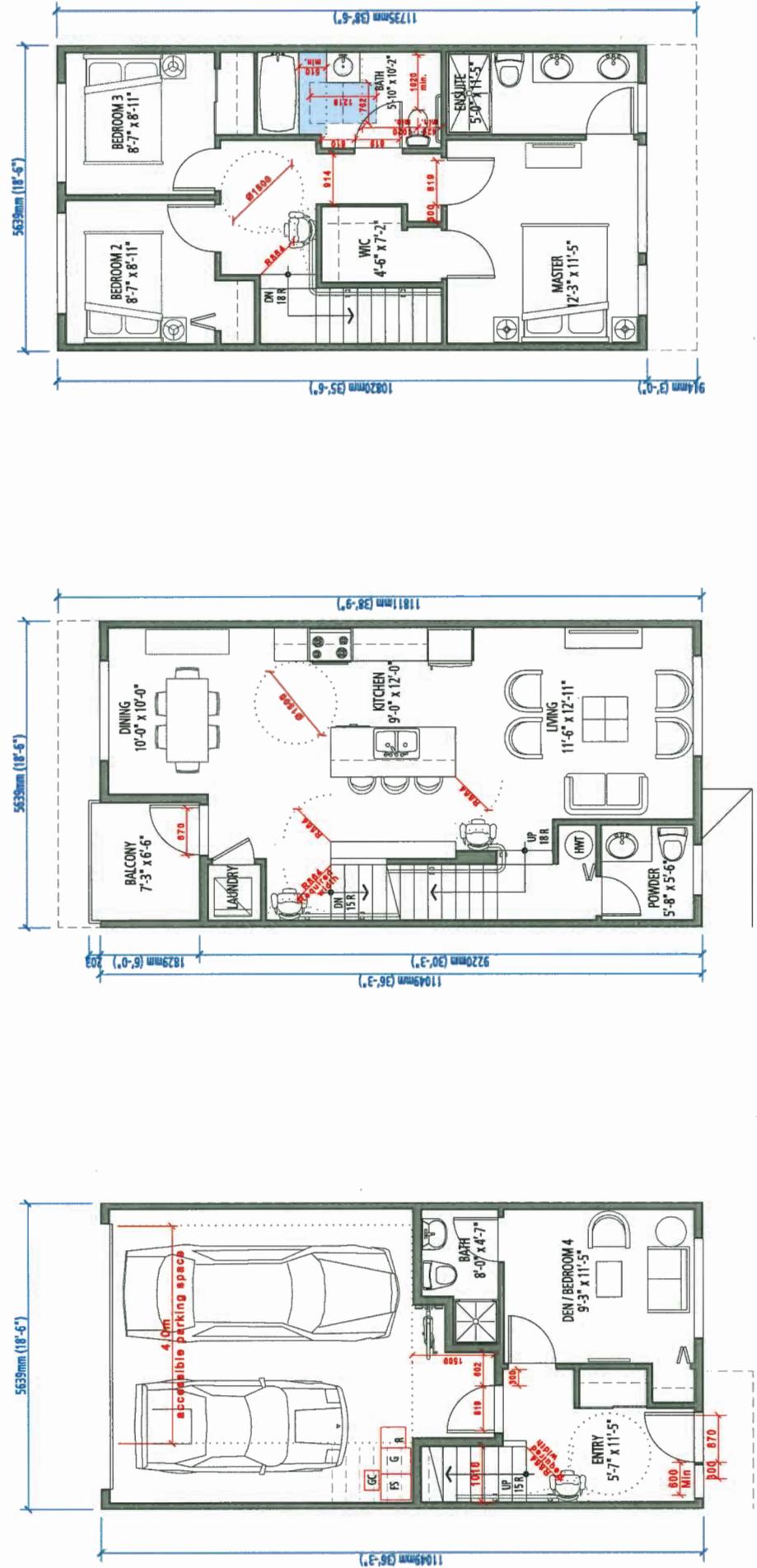
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CONVERTIBLE UNIT



Summary of the Convertible Unit Features Checklist as Submitted

- Entry doors min. 863mm clear opening (3'-0" swinging door spec.)
- Entry door clear exterior floor space min. 1220 mm depth by door width plus 600 mm on latch side.
- Interior doors to entry & main living areas, min. 800 mm clear opening.
- Wall blocking for future installation of grab-bars (toilet, tub and shower)
- Clear area needed under future work space. Plumbing and gas pipes (in-wall and in-floor) located clear of under counter area of future work space (stove, sink & min. 810 mm wide counter)
- 1500 mm turning diameter or turning path diagram
- One window that can be opened with a single hand in the living room
- One window that can be opened with a single hand in one bedroom

10451-10491 NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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FOR FOUGERE ARCHITECTURE INC.
BY: [Signature]

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DP 18-829241
APRIL 15, 2019

FOUGERE
architecture inc.
BRITISH COLUMBIA - ALBERTA - WASHINGTON
200 - 402 Queen Street
Vancouver, BC V7T 4L6
fougererarch.com.ca

AUG 06 2019 DP 18-829241 PLAN 1E

DP 18-829241
AUG 06 2019

BUILDING 1, 5 ELEVATIONS

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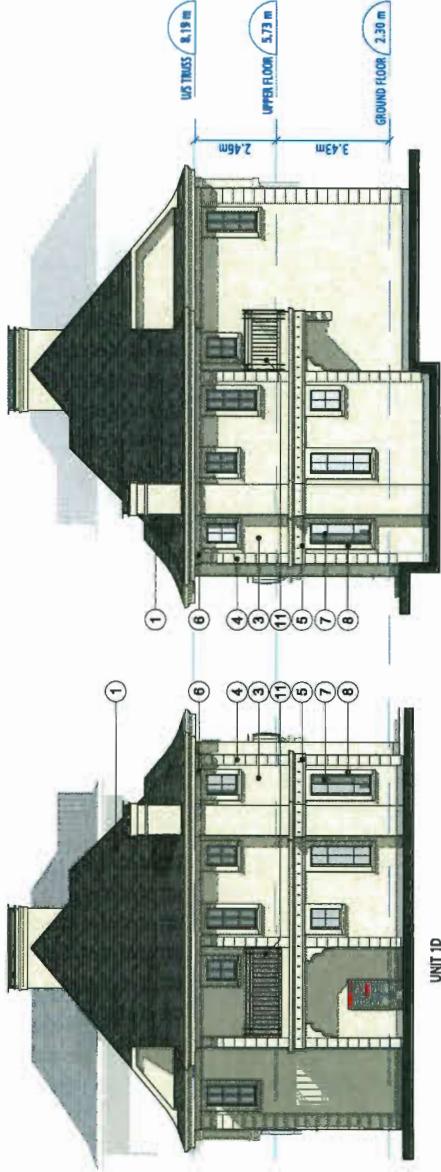
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for
Woodward Williams
Limited Partnership

BUILDING 1

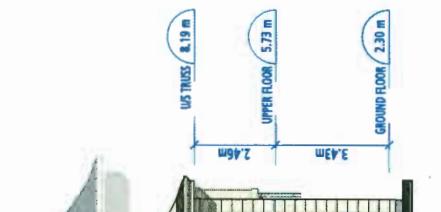
NORTH ELEVATION

UNIT 1D



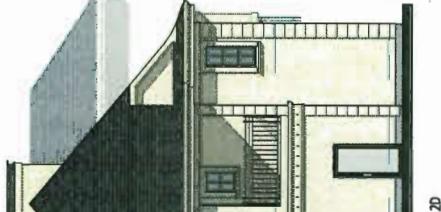
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UNIT 1D



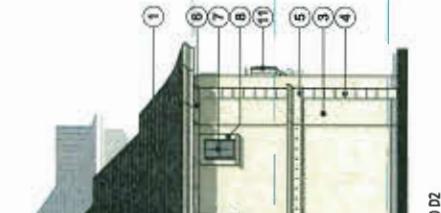
SOUTH ELEVATION

UNIT 1D



SOUTH ELEVATION

UNIT 1D



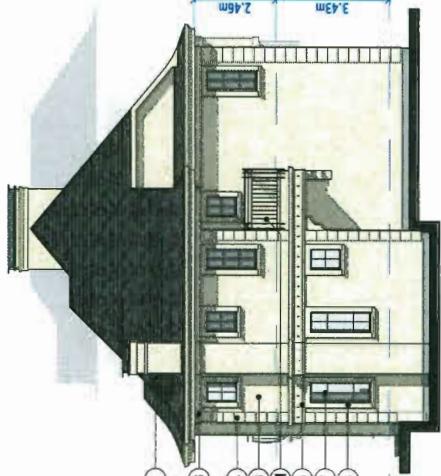
EXTERIOR FINISHES SCHEDULE

SYMBOL NUMBER	MATERIAL	COLOUR
1	ASPHALT SHINGLES	IKO Cambridge - Dual Black
2	METAL ROOF	Sherwin Williams 7069 Iron Ore
3	STUCCO	Benjamin Moore OC6 Niveous
4	EPS STUCCO DETAILS	Benjamin Moore OC40 Alabescent
5	EPS MOULDING	Benjamin Moore OC40 Alabescent
6	EPS MOULDING	Benjamin Moore OC40 Alabescent
7	VINYL WINDOWS	Black
8	EPS STUCCO WINDOW TRIMS	Benjamin Moore OC40 Alabescent
9	EXTENDED ENTRY DOOR	Sherwin Williams 6590 Cavalier
10	GARAGE DOOR	Sherwin Williams 6590 Cavalier
11	METAL RAILING	Black

EXTERIOR FINISHES SCHEDULE

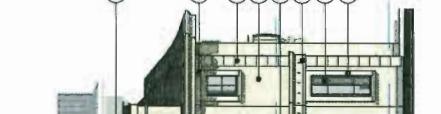
NORTH ELEVATION

UNIT 1D



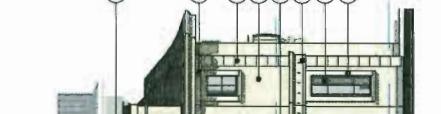
WEST ELEVATION

UNIT 1D



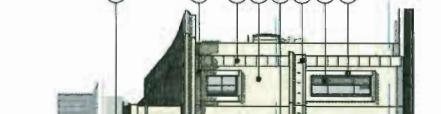
SOUTH ELEVATION

UNIT 1D



WEST ELEVATION

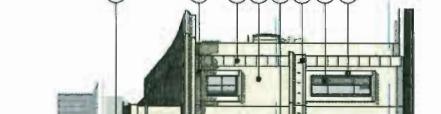
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EXTERIOR FINISHES SCHEDULE

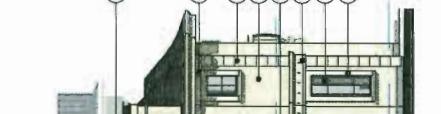
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UNIT 1D



WEST ELEVATION

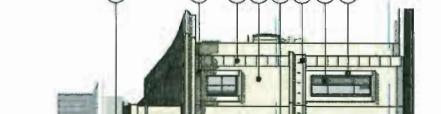
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EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

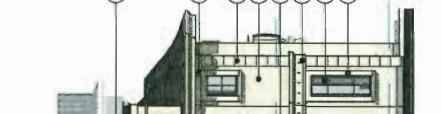
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EXTERIOR FINISHES SCHEDULE

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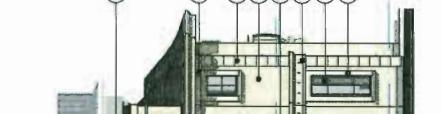
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EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

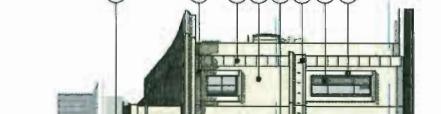
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

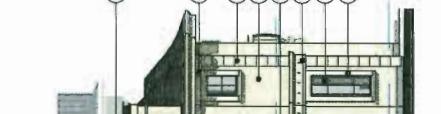
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

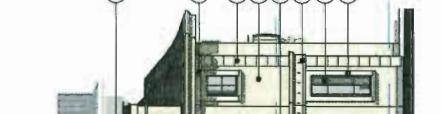
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

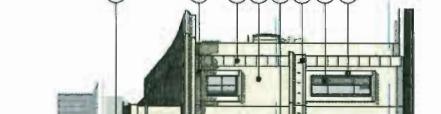
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

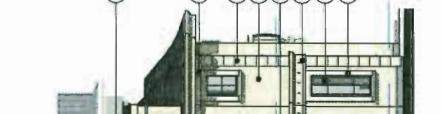
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

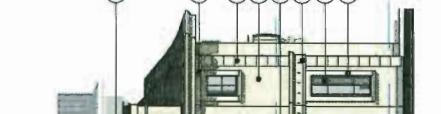
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

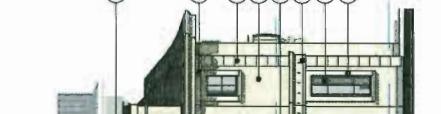
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EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

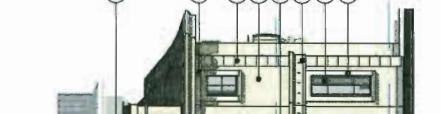
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

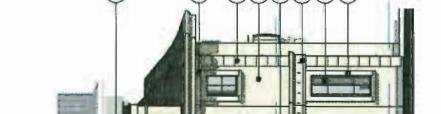
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

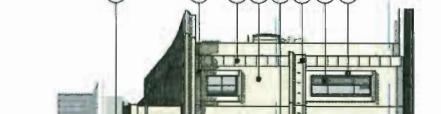
UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

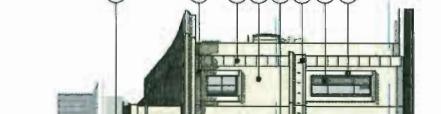
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EXTERIOR FINISHES SCHEDULE

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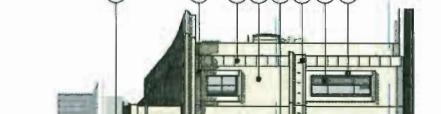
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EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

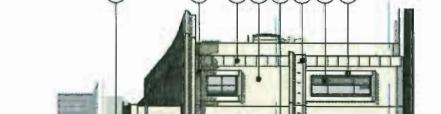
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EXTERIOR FINISHES SCHEDULE

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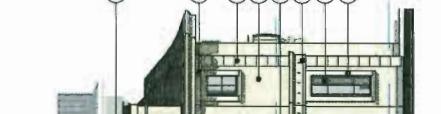
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EXTERIOR FINISHES SCHEDULE

WEST ELEVATION

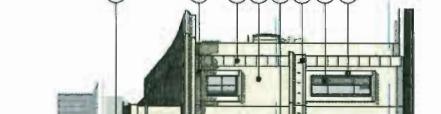
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EXTERIOR FINISHES SCHEDULE

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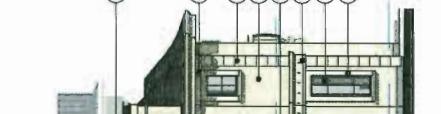
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EXTERIOR FINISHES SCHEDULE

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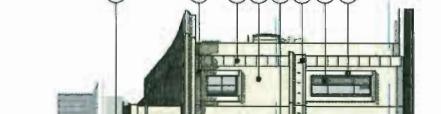
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EXTERIOR FINISHES SCHEDULE

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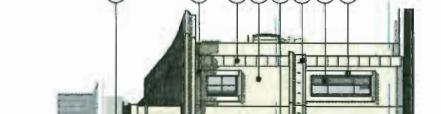
UNIT 1D



EXTERIOR FINISHES SCHEDULE

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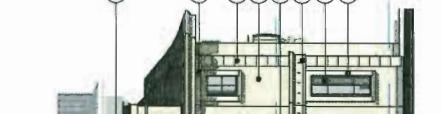
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EXTERIOR FINISHES SCHEDULE

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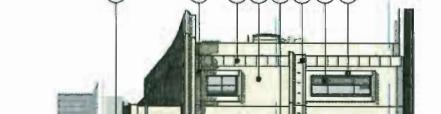
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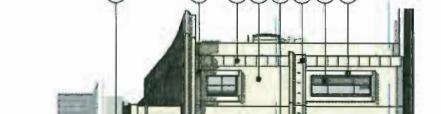
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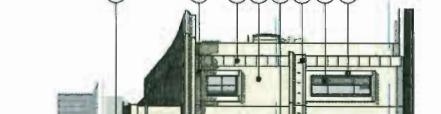
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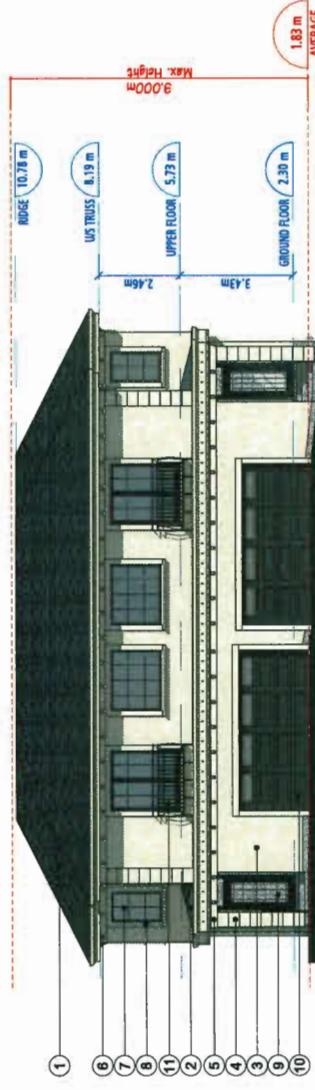
WEST ELEVATION

UNIT 1D



EXTERIOR FINISHES SCHEDULE

WEST ELEVATION</h4



EAST ELEVATION



EAST ELEVATION



WEST ELEVATION



UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



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UNIT 7A



UNIT 6A



UNIT 7A



UNIT 6A



UNIT 7A

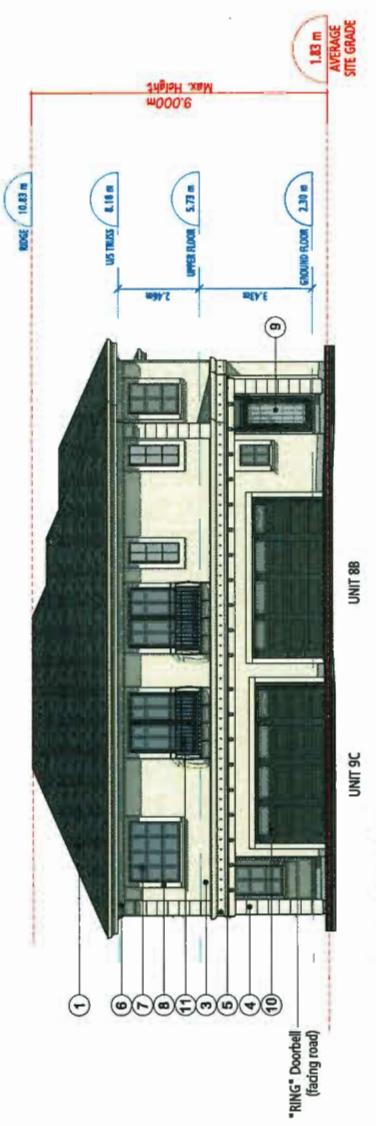


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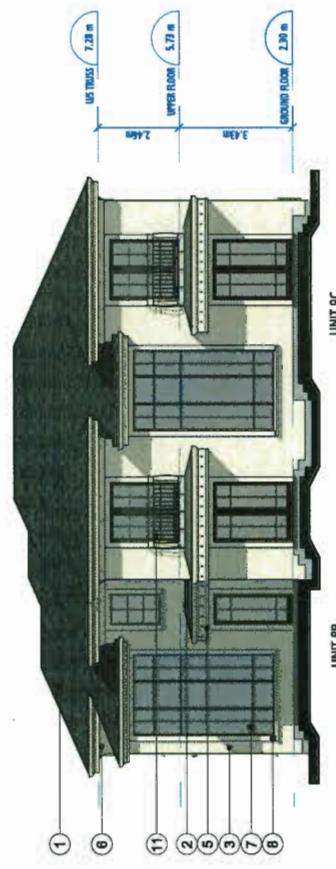


UNIT 7A

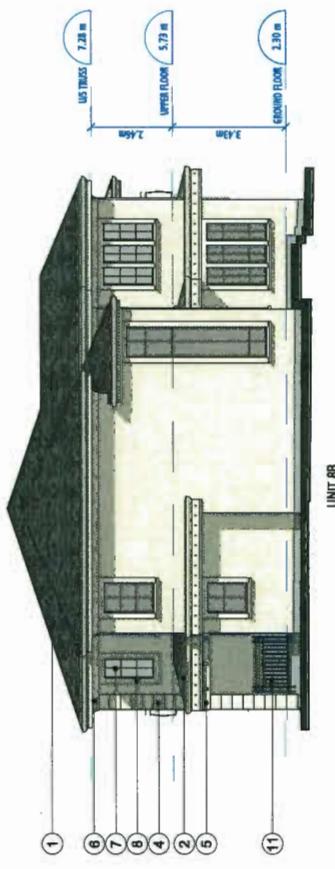




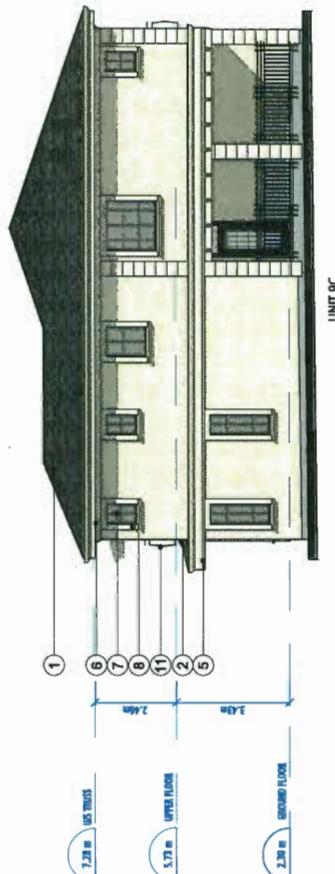
EAST ELEVATION



WEST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION

10451-10491 NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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DP 18-829241
JULY 29, 2019

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architecture inc.
BRITISH COLUMBIA • ALBERTA • MONTANA
202 - 4400 Quebec Street
Vancouver, BC V5T 4L6
fougerearchitecture.ca

BUILDING 4 ELEVATIONS

SCALE: 1:100
0 2 4 6

EXTERIOR FINISHES SCHEDULE		
SYMBOL NUMBER	MATERIAL	COLOUR
1	ASPHALT SHINGLES	IKO Cambridge - Dual Black
2	METAL ROOF	Shireen Williams 7068 Iron Ore
3	STUCCO	Benjamin Moore OC36 Niveous
4	EFS STUCCO DETAILS	Benjamin Moore OC40 Albescent
5	EFS STUCCO MOULDING	Benjamin Moore OC40 Albescent
6	EFS STUCCO MOULDING	Benjamin Moore OC40 Albescent
7	VINYL WINDOWS	Black
8	EFS STUCCO WINDOW TRIMS	Benjamin Moore OC40 Albescent
9	EXTERIOR ENTRY DOOR	Shireen Williams 6990 Cavalier
10	GARAGE DOOR	Shireen Williams 6990 Cavalier
11	METAL RAILING	Black

BUILDING 4

DP 10451-10491

AUG 06 2019

10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

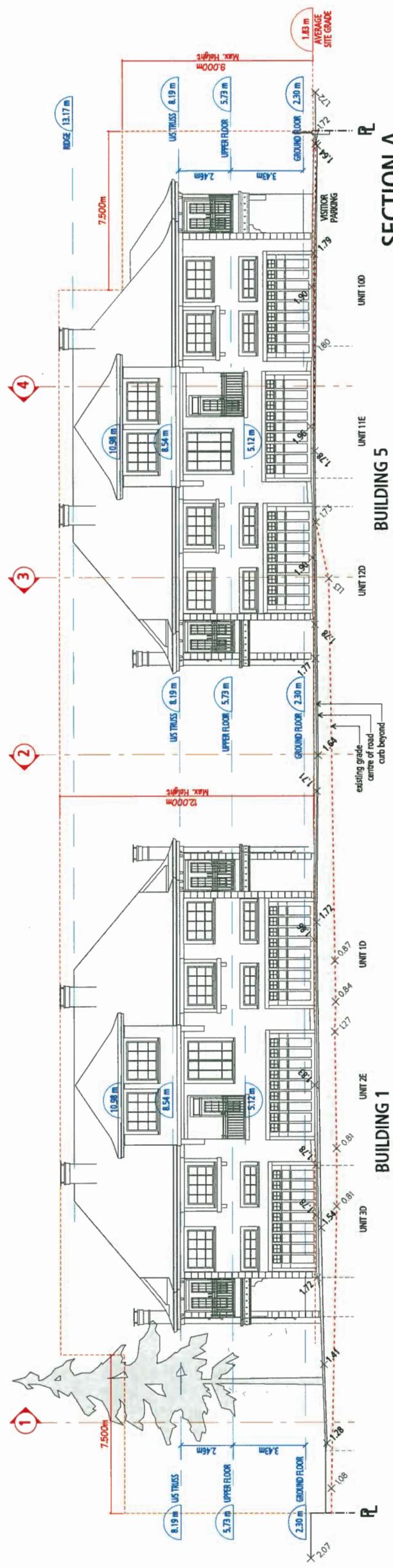
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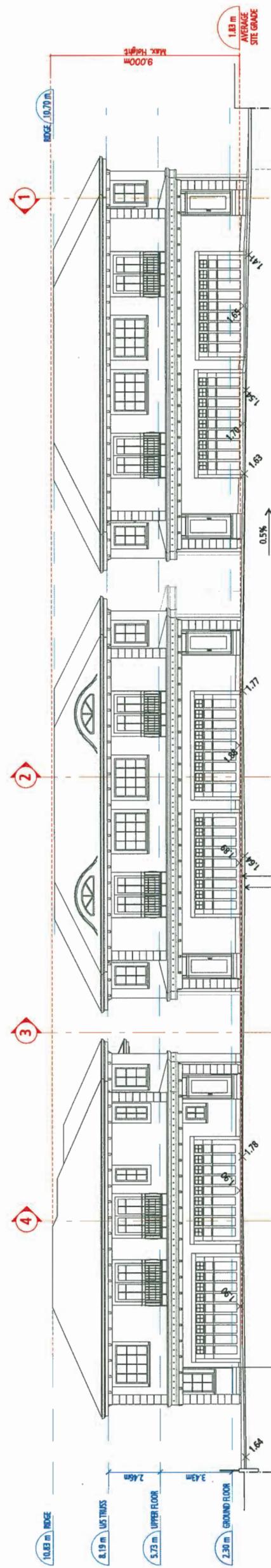
SECTIONS

SCALE: 1:100

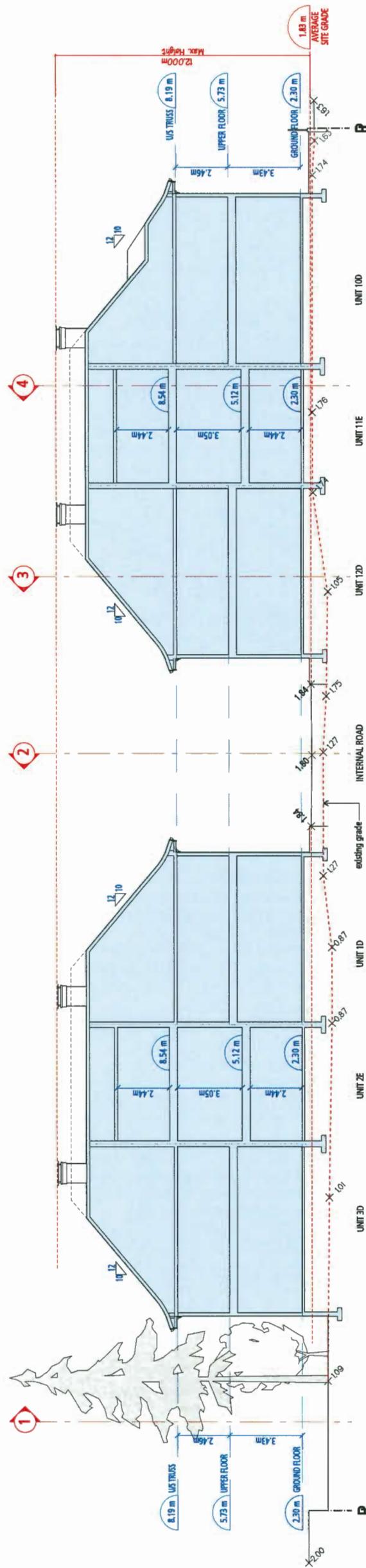
FOUGERE
architecture inc.
BRITISH COLUMBIA • ALBERTA • WASHINGTON
604.873.7307
fougerer.ca



SECTION A



SECTION B



SECTION C

- 829241

AUG 06 2019

PLAN 2E

DP

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18-829241

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BRITISH COLUMBIA - ALBERTA - WASHINGTON
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DP 18-829241
AUGUST 1, 2019

SECTIONS

SCALE: 1:100
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**10451-10491
NO. 2 ROAD**

for
Woodward Williams
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SECTION 2

BUILDING 1

BUILDING 3

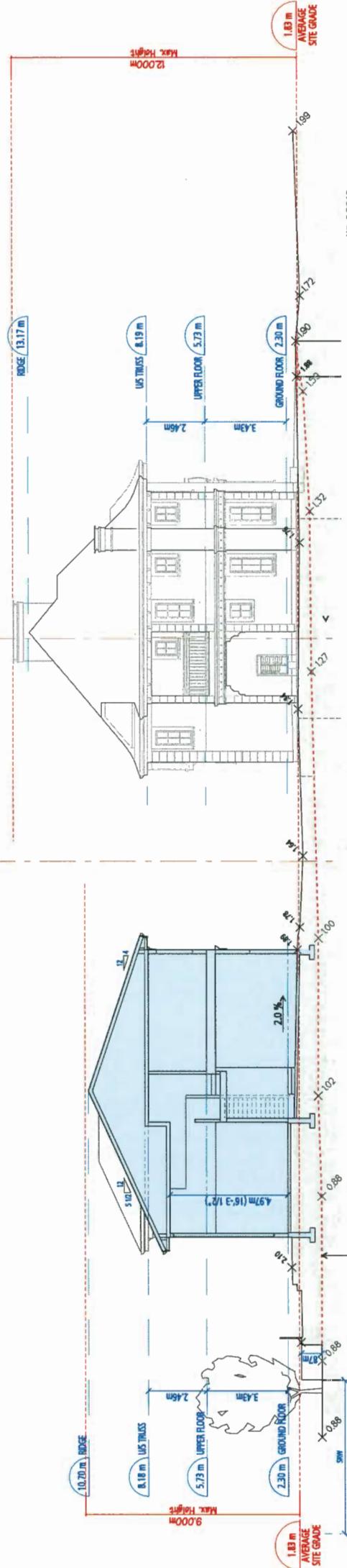
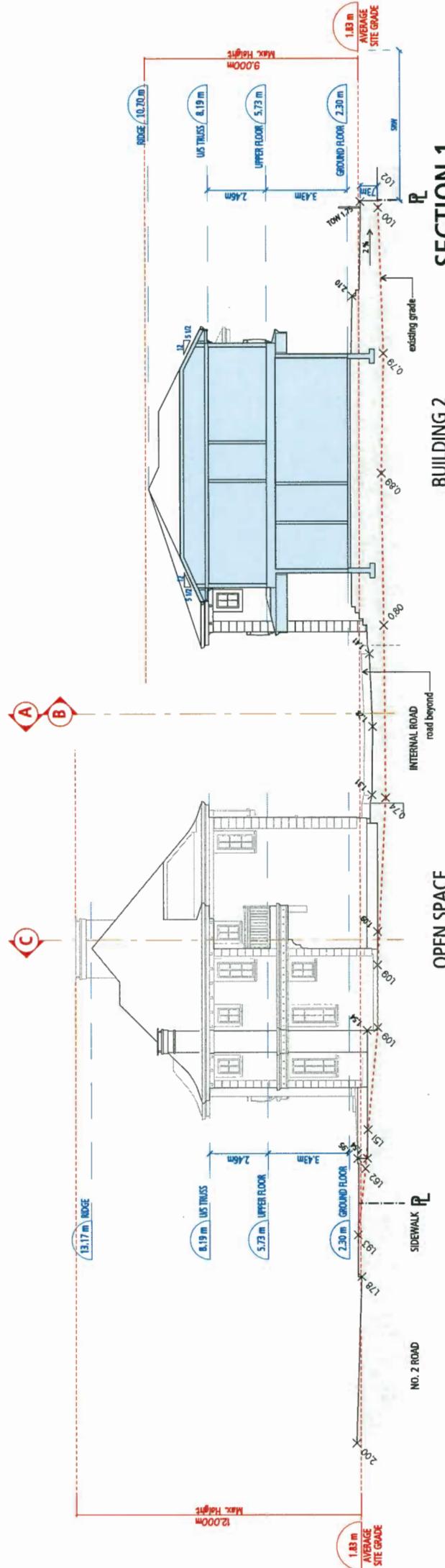
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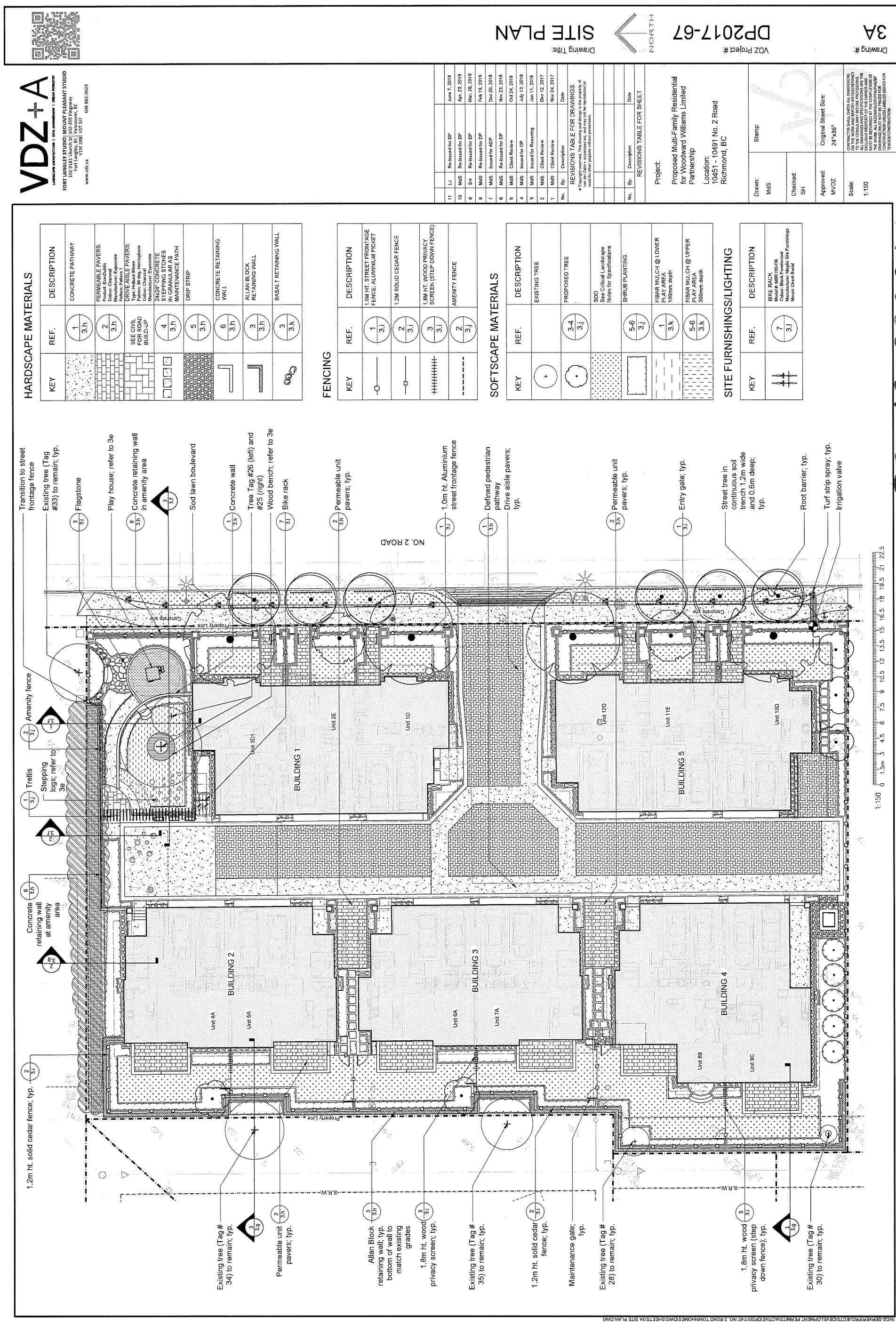
BUILDING 2

SECTION 1

BUILDING 2

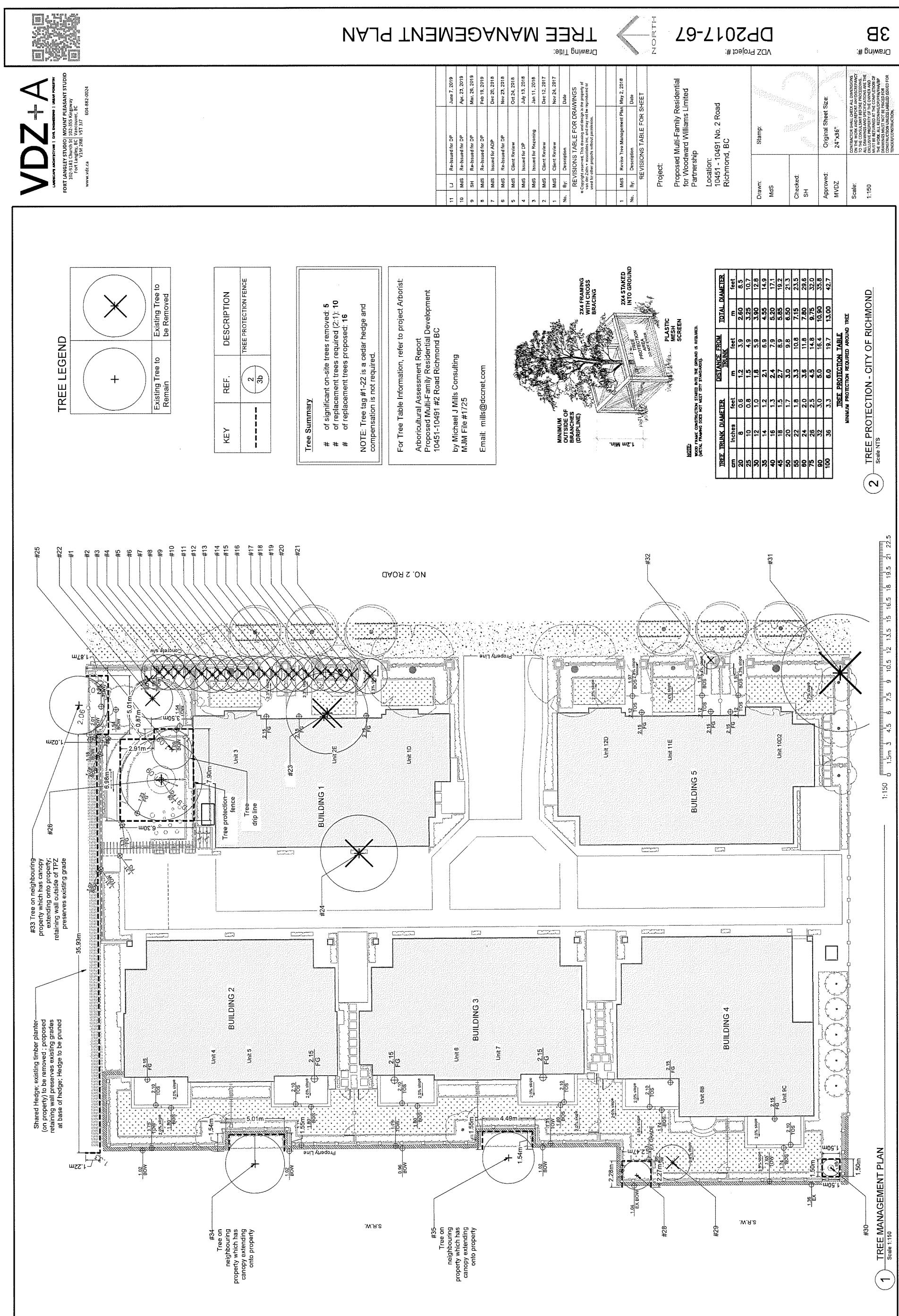
OPEN SPACE





PLAN 3A

AUG 06 2019 DP 18-829241



WZD-SERVERPROJECTS DEVELOPMENT PERMIT APPLICATION NO. 2 ROAD TOWNSHOMES OWNERSHIP SHEET 38 THREE MANAGEMENT PLAN DWG



Landscape Architecture | ONE Dimensions | URBAN PLANNING
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PLANTING PLAN



DP2017-67

Vdz Project #:

3C

#

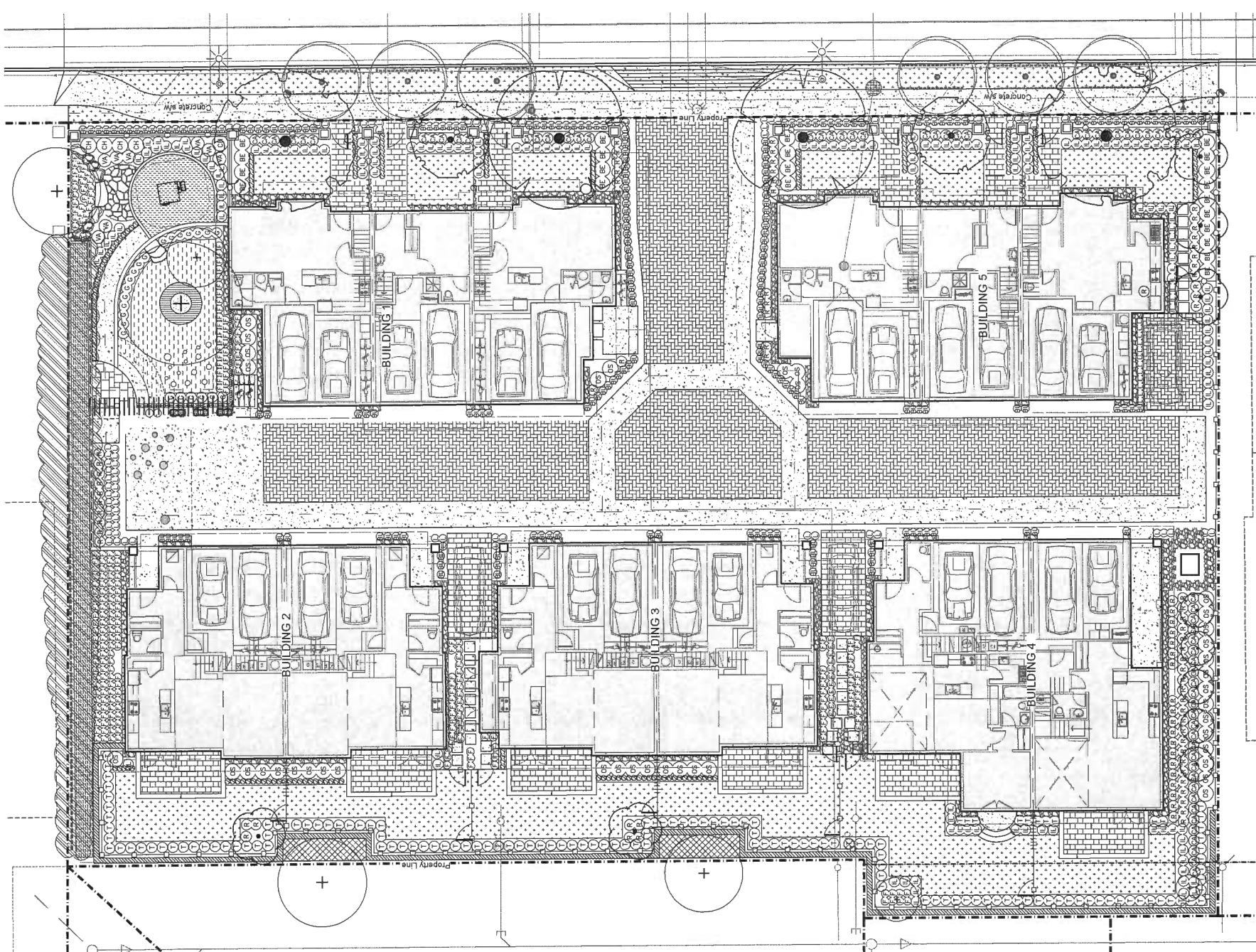
TREE SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT.	CAL.	SIZE	QTY
	<i>Magnolia x 'Vulcan'</i> / Magnolia Tree Form	B&B	6cm	1.2m std.	5
	<i>Pinus nigra 'Arnold Sentinel'</i> / Arnold Sentinel Austrian Black Pine	B & B	-	3m ht.	5
	<i>Stereodendron pseudocamellia</i> / Japanese Stereodendron	B & B	6cm cal.	1.8 m std.	2
	<i>Tilia tomentosa 'Siegling'</i> / Siegling Silver Linden	B&B	12cm cal.	2	
	<i>Tilia tomentosa 'Silver Linings'</i> / Silver Linden	B&B	10cm cal.	2	
STREET TREES	BOTANICAL / COMMON NAME	CONT.	CAL.	SIZE	QTY
	<i>Catalpa bignonioides 'Aurea'</i> / Hybrid Catalpa	B & B	7cm cal.	1.8 m std.	6

PLANT SCHEDULE ONSITE	BOTANICAL NAME / COMMON NAME	CONT.	SPACING	QTY
SHRUBS	<i>Berberis x glauca 'William Penn'</i> / William Penn Barberry	#3	0.90m	22
BE	<i>Buxus 'Green Velvet'</i> / Boxwood	#2	0.45m	221
BU	<i>Carpinetria x chindensis 'Dark Knight'</i> / Blue Mist Shrub	#3	0.60m	69
CA	<i>Choisya ternata 'Aztec Pearl'</i> / Mexican Orange	#3	0.90m	5
CH	<i>Gaultheria shallon 'Salal'</i> / Soft Touch Japanese Holly	#3	0.60m	17
G	<i>Ilex crenata 'Soft Touch'</i> / Soft Touch Japanese Holly	#3	0.60m	154
IL	<i>Ceanothus x buckwoodii 'Buckwood Osmanthus'</i>	#3	0.90m	40
OS	<i>Polystichum munitum / Western Sword Fern</i>	#1	0.45m	248
F	<i>Rose 'Medallion White'</i> / Medallion White Rose	#2	0.60m	90
R	<i>Taxus x media 'Hicksii'</i> / Hicks Yew	#1.2m ht.	0.60m	142
T	<i>Vaccinium corymbosum / Blueberry bush</i>	#2	0.75m	8
VA				
ANNUAL/SUPERPERENNIALS	BOTANICAL NAME / COMMON NAME	CONT.	SPACING	QTY
2	<i>Heuchera 'Temperevita' / Candyfloss</i>	10cm pot	0.30m	175
L	<i>Lavandula angustifolia 'Hidcote'</i> / Hidcote Lavender	#1	0.45m	98
GROUND COVERS	BOTANICAL NAME / COMMON NAME	CONT.	SPACING	QTY
	<i>Mahonia nervosa / Dull Oregon Grape</i>	#1	450mm	60

- ALL PLANT MATERIAL SHALL MEET OR EXCEED STANDARDS REQUIRED BY BCINA OR BCSA GUIDELINES.
- TOPSOIL DEPTHS FOR PLANTING AS FOLLOWS
- A. GROUND COVERS: 300mm
- B. SHRUBS: 450mm
- C. TREE PTS.: MOUND SOIL TO MINIMUM 900MM DEPTH AROUND ROOT BALLS
- D. 100-150MM DEPTHS OF MULCH TO BE INSTALLED IN ALL SHRUB PLANTING AREAS.
- E. 27 MM DEPTHS OF MULCH TO BE INSTALLED IN ALL PLANTING AREAS TO BE IRRIGATED.
- F. ALL PLANTING AREAS TO BE IRRIGATED.

NO. 2 ROAD



STREET FRONTAGE PLANTING



Project:

Proposed Multi-Family Residential Partnership

Location:
10451 - 10491 No. 2 Road
Richmond, BC

Stamp:

MS

Checked:
SHApproved:
WVDZOriginal Sheet Size:
24" x 36"Scale:
1:150

1:150 0 1.5m 3 4.5 6 7.5 9 10.5 12 13.5 15 16.5 18 19.5 21 22.5

AUG 06 2019 DP 18-829241 PLAN 3C



VDZ + A

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Fort Langley, BC Vancouver, BC
V1M 2R8 V5T 3J7 604-882-0024
www.yds.ca

GRADING AND DRAINAGE

KEY	DESCRIPTION	
-165 BOS	PROPOSED ELEVATION	
	PROPOSED SLOPE:	
	PROPOSED (RECTANGULAR) AND EXISTING (ELLISE) GRADES BY ARCHITECTURE	
	PROPOSED SLOPE BY ARCHITECTURE	
	EXISTING ELEVATION	
	CONCRETE RETAINING WALL	
	ALLAN BLOCK RETAINING WALL	

GRADING PLAN

Drawing Title:

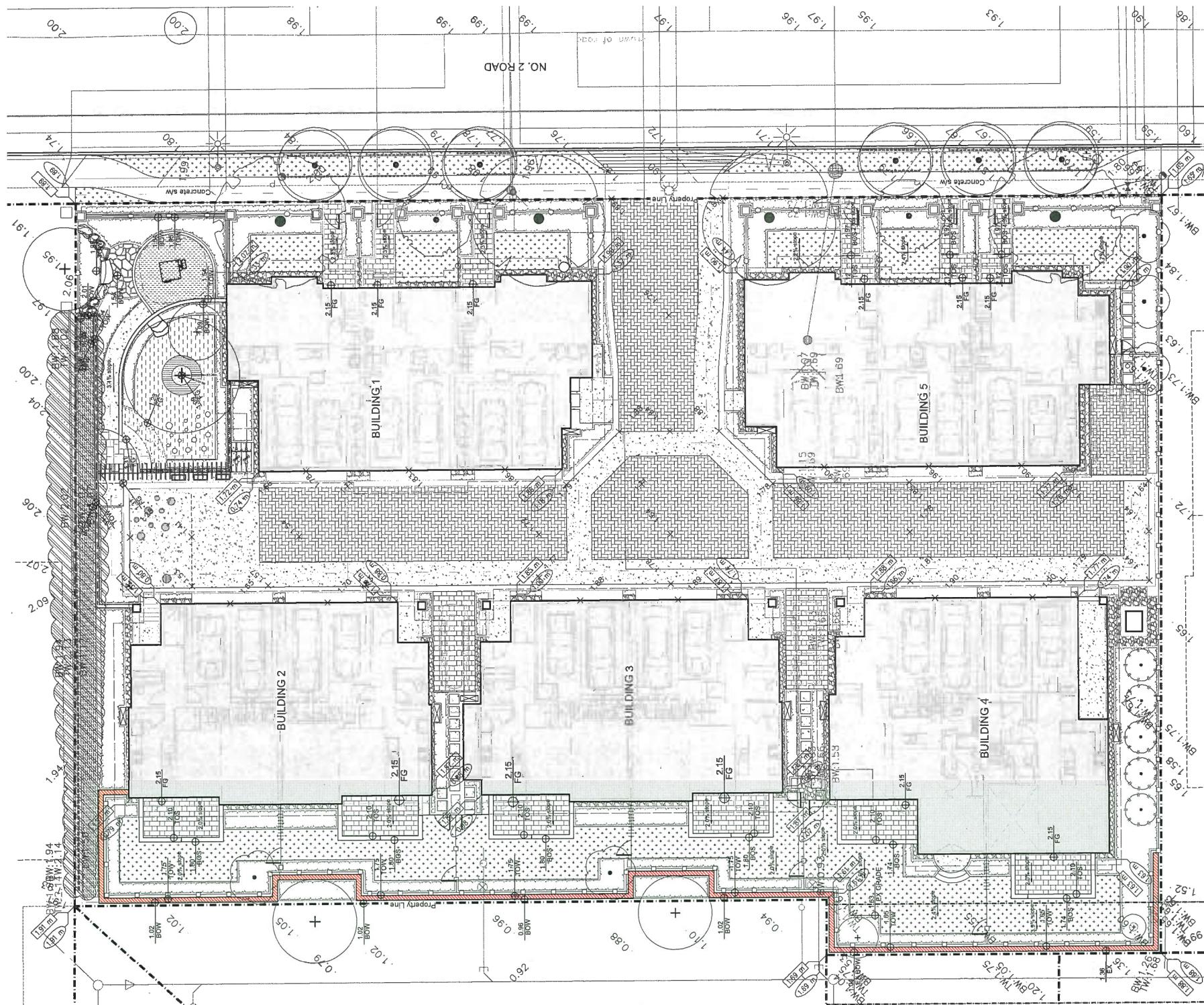
DP2017-67



ADZ Project #:

3D

Drawing #:



W02-SERVERPROJECTS DEVELOPMENT PERMITS ACTIVE ID: P2017-67 NO. 2 ROAD TOWNSHOMES DWG/SHEET 33RD GRADING PLAN DWG



VDZ+A
LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN PLANNING
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Fort Langley, BC V4A 1G2
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Fort Langley, BC | Vancouver, BC
V1M 2B8 | V5T 3J7
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AMENITY PLAN

Drawing Title:

DP2017-67

3E



Project: Proposed Multi-Family Residential
for Woodward Williams Limited
Partnership
Location: 10451 - 10491 No. 2 Road

WDZ-SERVE-PROJ-CTS-DEV-ELDPMNT PERMITSGATIVEDP2017-67 NO. 2 ROAD TOWNHOMESDWG(SHEET 1 OF 1) AMENDT PLN DWG

THEIR CONSTRUCTION.

PLAN 3F

AUG 06 2019

DP 18-829241

VDZ+A

LAWSON ARCHITECTURE | CIVIL ENGINEERING | LANDSCAPE
PORT LANGLEY STUDIO | MOUNTAIN REASANT STUDIO
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Port Langley, BC | Vancouver, BC
V1M 2B8 | V1T 3J7
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LANDSCAPE SECTIONS

DP2017-67

VDZ Project #:

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3F

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Scale: 1:50

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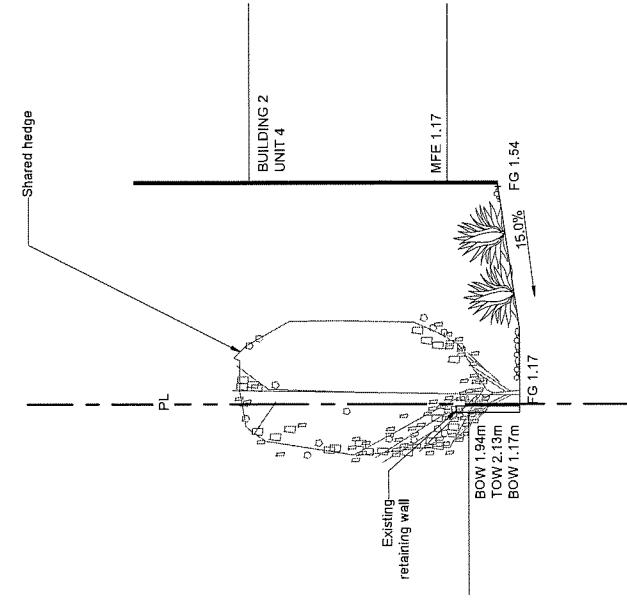


LANDSCAPE SECTIONS

DP2017-67

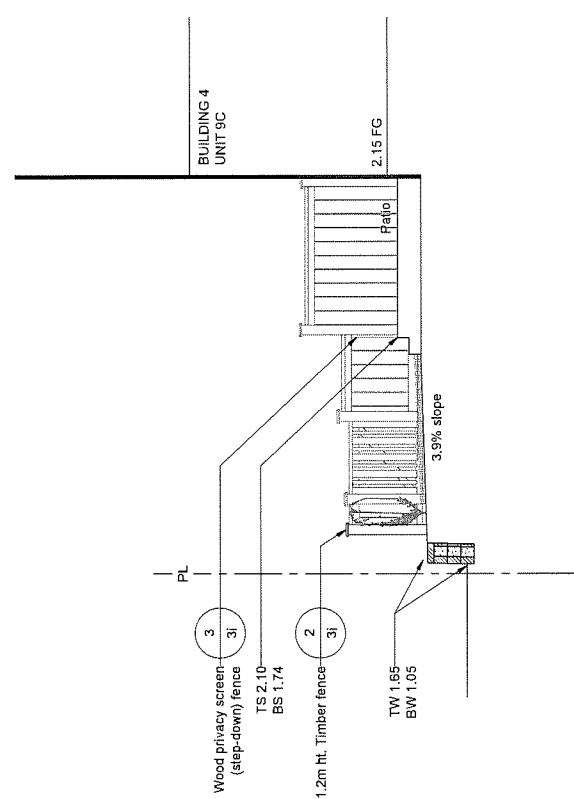
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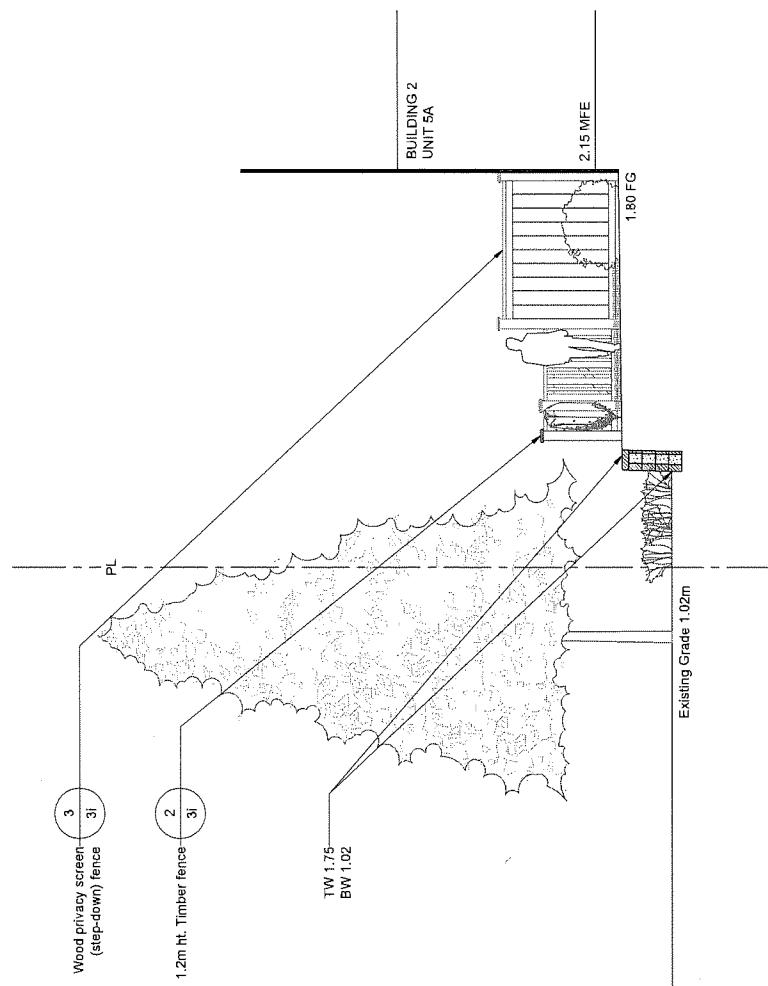
SECTION / ELEVATION AT BUILDING 2 UNIT 4, LOOKING EAST

Scale 1:50



SECTION / ELEVATION AT BUILDING 4 UNIT 9C, LOOKING NORTH

Scale 1:50



SECTION / ELEVATION AT BUILDING 2 UNIT 5A, LOOKING NORTH

Scale 1:50

WDZ-SEPERATE DEVELOPMENT PERMIT 2017-67 NO. 2 ROAD TOWNSHOES CLOWS SHEET 5 LANDSCAPE SECTIONS DWS

No.	By:	Description	Date
1	MWS	Client Review	Nov 24, 2017
2	MWS	Client Review	Dec 12, 2017
3	MWS	Issued for Rezoning	Jan 11, 2018
4	MWS	Issued for OP	July 13, 2018
5	MWS	Client Review	Oct 24, 2018
6	MWS	Re-issued for OP	Nov 23, 2018
7	MWS	Re-issued for ADP	Dec 20, 2018
8	MWS	Re-issued for OP	Feb 19, 2019
9	SH	Re-issued for OP	Mar. 26, 2019
10	MWS	Re-issued for OP	Apr. 23, 2019
11	PL	Re-issued for OP	June 7, 2019

Drawing Title:

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11	PL	Re-issued for OP	June 7, 2019

Drawing Title:

REVISIONS TABLE FOR SHEET

Project:

Proposed Multi-Family Residential
 Partnership
 Location:
 10451 - 10491 No. 2 Road
 Richmond, BC

Drawn:
 MWS
 MWS
 SH

Stamp:

SH

Approved:

MWDZ

Checked:

SH

Original Sheet Size:

24" x 36"

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Scale:

1:50

1:50

PLAN 361

AUG 06 2019

DP 18-829241



LANDSCAPE DETAILS

DP2017-67

3H

DRAWING TITLE:

VDZ Project #:

Original Sheet Size:

Scale: 24" x 36"

Comments:

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Product: AB® Collection
Manufacturer: Allan Block
Colour: (S) GREY

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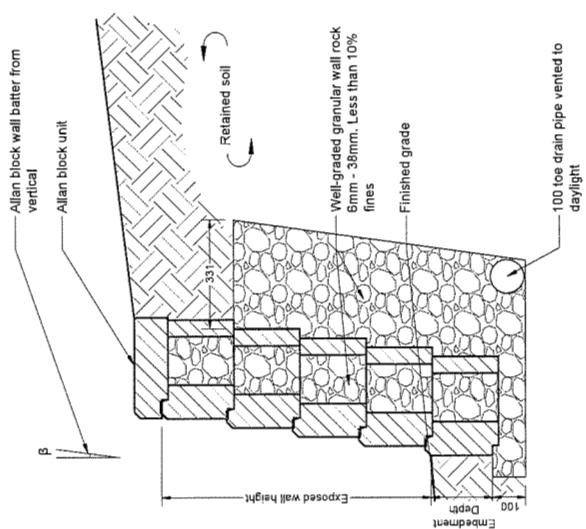
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VIM 2B# V5T 3J7

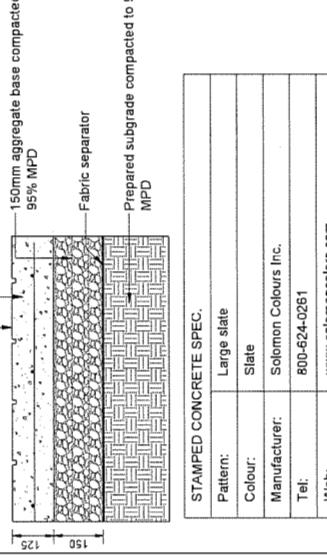
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3 MODULAR RETAINING WALL

Scale 1:10



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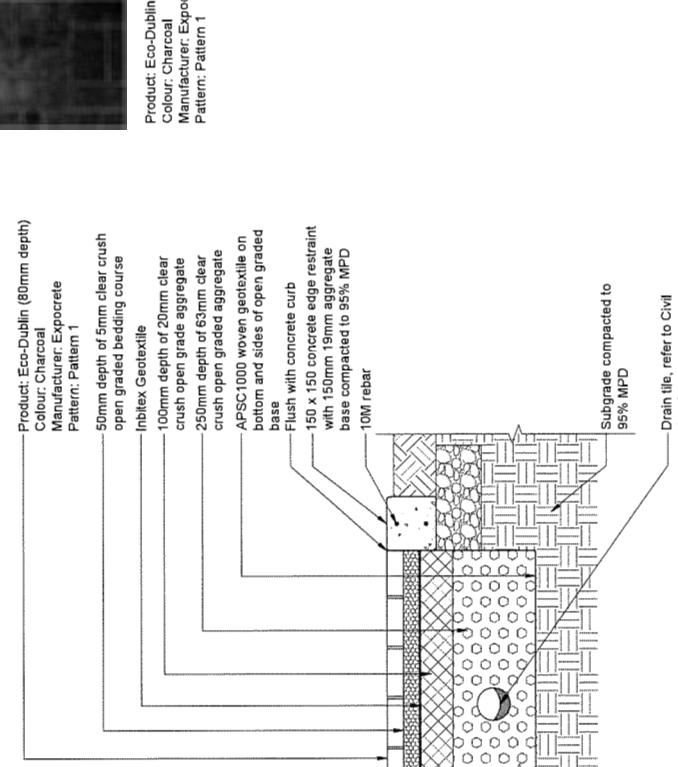
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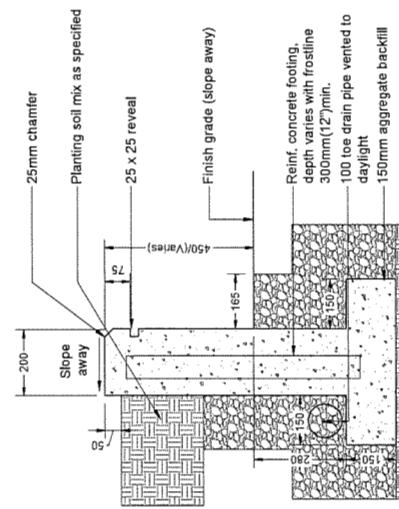


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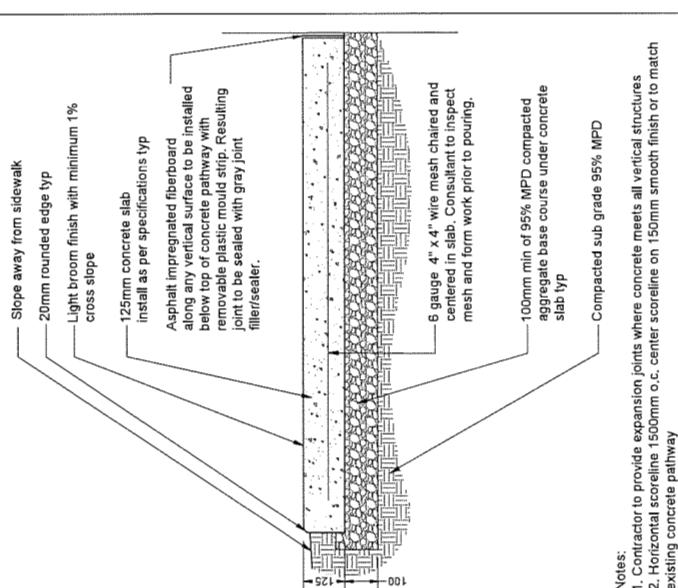
1. Install all components as per manufacturer's specifications.
2. Aqua-rave - Part Extraction System
3. Running bond pattern.
4. Concrete curb control joint every 3m.

2 PERMEABLE UNIT PAVERS

Scale 1:10



Note:
Concrete wall detail see
structural engineer drawings

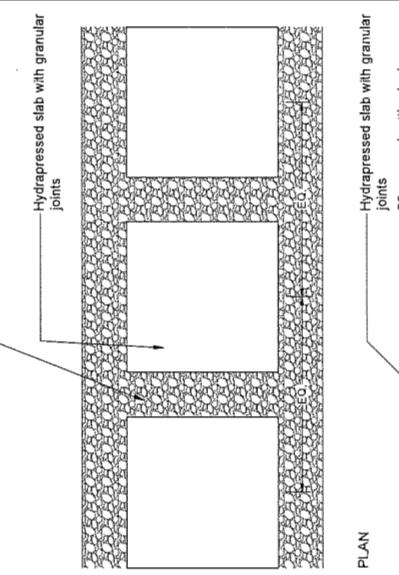


Notes:

1. Slabs to be spaced equally between adjacent surfaces.
2. Horizontal scoreline 150mm o.c. center scoreline on 150mm smooth finish or to match existing concrete pathway

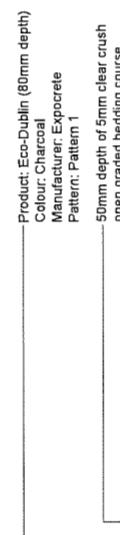
1 CONCRETE PATHWAY

Scale 1:10



Notes:

1. Install all components as per manufacturer's specifications.
2. Supplier: Abbotsford Concrete or approved Equal
3. Type: Teada
4. Sizes: 610mm x 610mm, 457mm x 457mm
5. Color: Charcoal



Product: Eco-Dublin
Colour: Charcoal
Manufacturer: Exocrete
Pattern: Pattern 1

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Project:

Proposed Multi-Family Residential Partnership

Location:

10451-10491 No. 2 Road

Richmond, BC

Drawn:

MS DRAWN2

Checked:

SH

Approved:

WDZ

Scale:

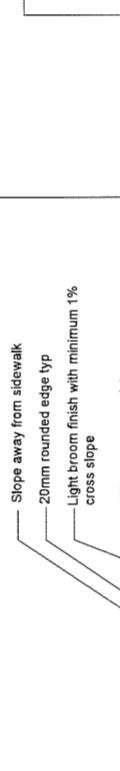
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Product: Eco-Dublin
Colour: Charcoal
Manufacturer: Exocrete
Pattern: Pattern 1

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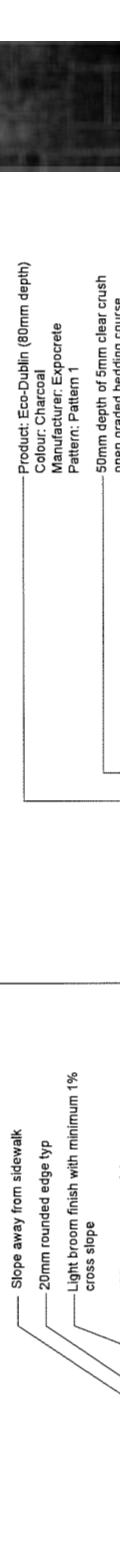
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Colour: Charcoal
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Proposed Multi-Family Residential Partnership

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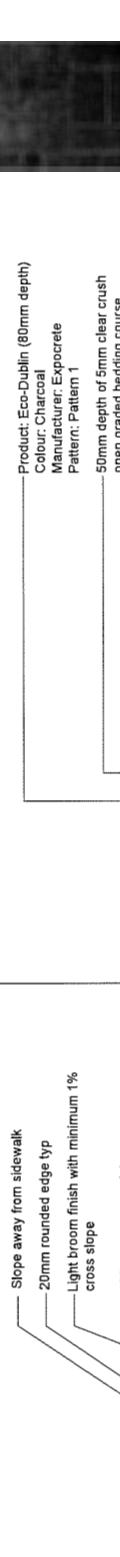
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Colour: Charcoal
Manufacturer: Exocrete
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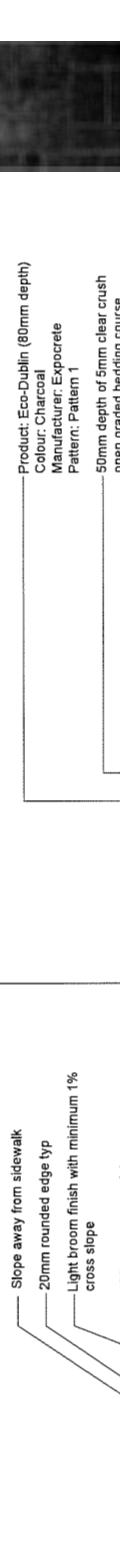
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Product: Eco-Dublin
Colour: Charcoal
Manufacturer: Exocrete
Pattern: Pattern 1

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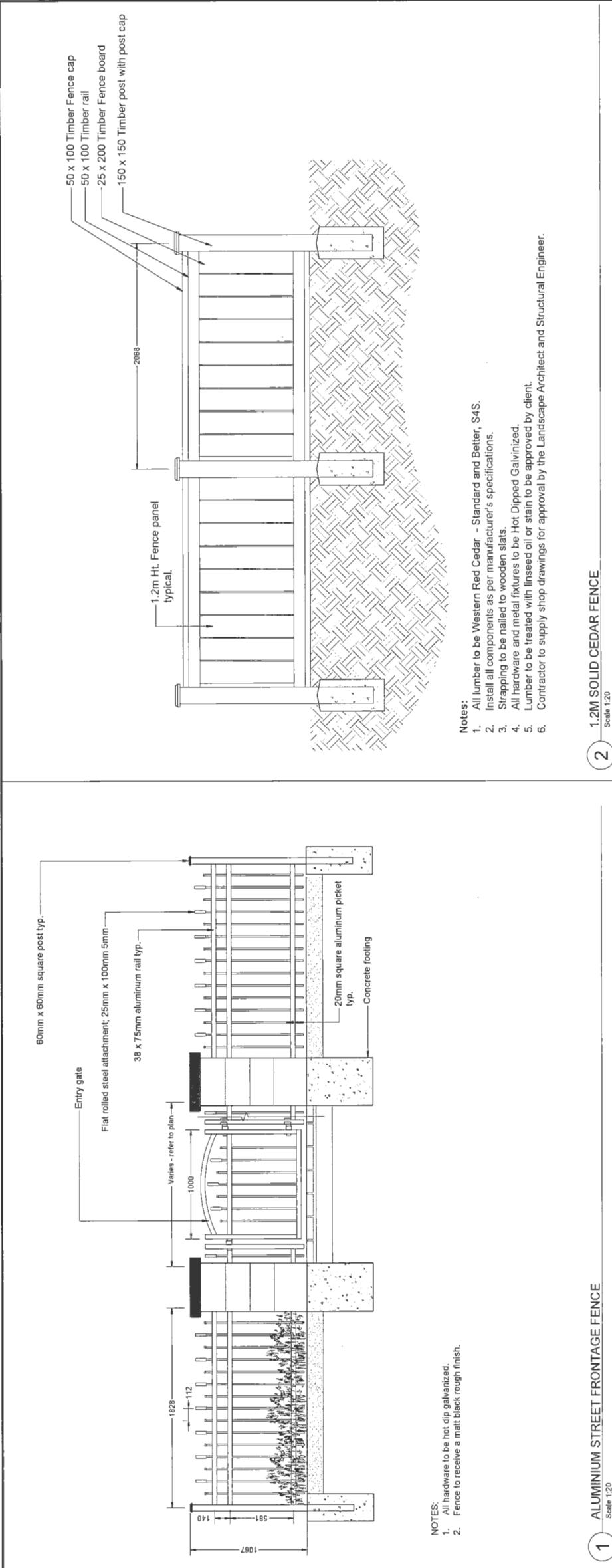
LANDSCAPE DETAILS

DP2017-67

VDZ Project #: 3I

Drawing Title:

Drawing #:



Notes:

- All lumber to be Western Red Cedar - Standard and Better, S4S.
- Install all components as per manufacturer's specifications.
- Strapping to be nailed to wooden slats.
- All hardware and metal fixtures to be Hot Dipped Galvanized.
- Lumber to be treated with linseed oil or stain to be approved by client.
- Contractor to supply shop drawings for approval by the Landscape Architect and Structural Engineer.
- 1836mm fence panel directly adjacent to unit between units where no privacy screen provided by Architecture.

3 WOOD PRIVACY SCREEN (STEP DOWN) FENCE

Scale 1:20

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24"x36"
Scale:
AS SHOWN

3K

Drawing #:

DP2017-67

V2D Project #:

V2D

Original Sheet Size:

24" x 36"

Scale:

AS SHOWN

Approved:

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Drawn:

Stamp:

Ms.

Comments:

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DP2017-67

V2D

Drawing #:

LANDSCAPE DETAILS

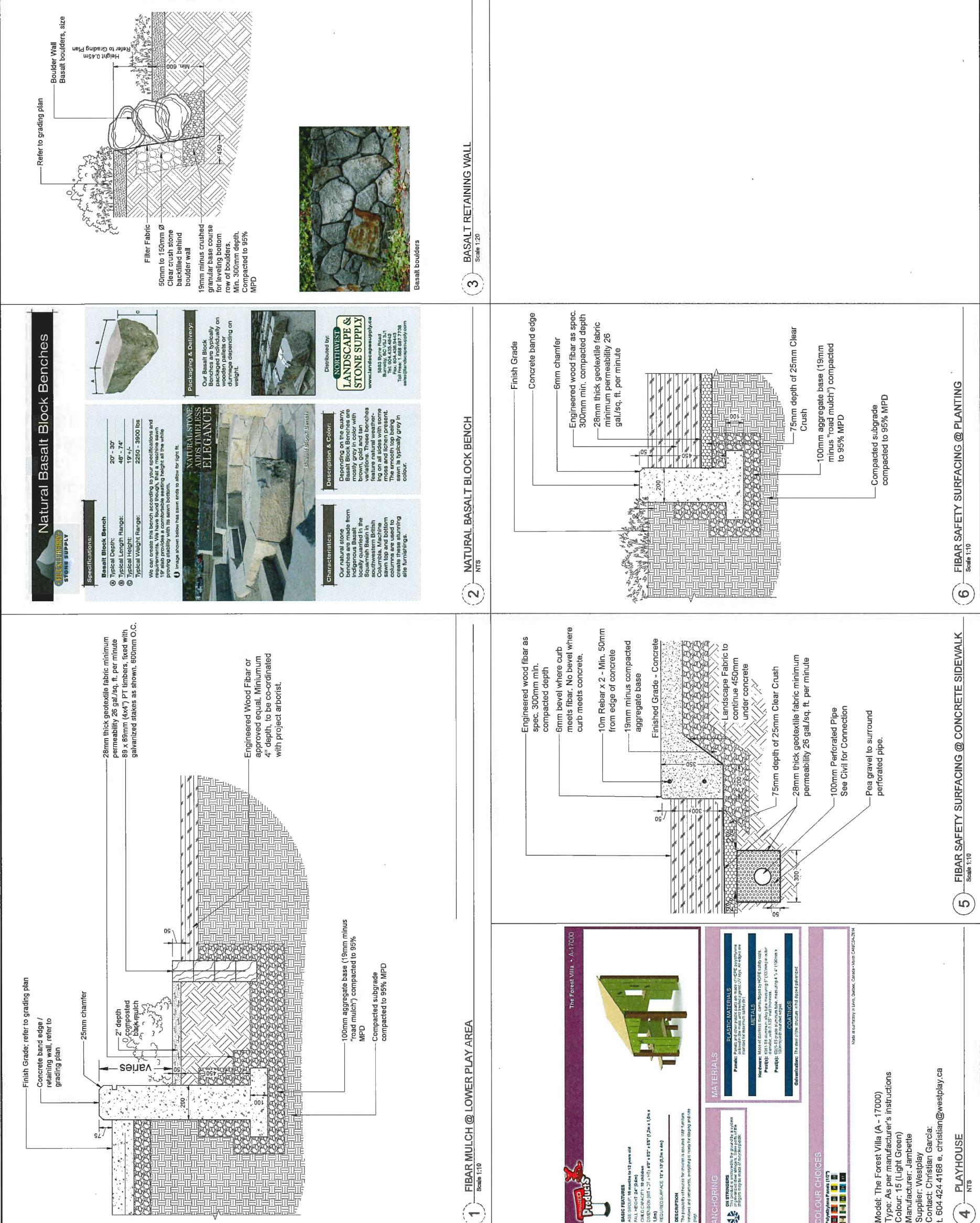
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Fort Langley BC V2M 1B6 VST 337
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LANDSCAPE DETAILS

Drawing #:

LANDSCAPE DETAILS





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Port Langley, BC Vancouver, BC
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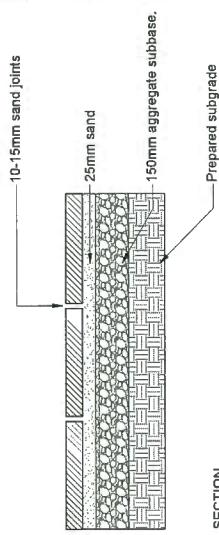
LANDSCAPE DETAILS

DP2017-67

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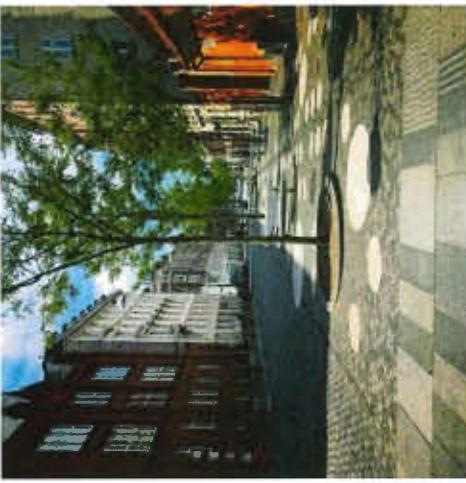
3L

DZ Project #:



SECTION

FLAGSTONE
Stone: Trout Creek 2"x3" Patio Flagstone
Thickness: 25mm
Width: 300mm
Length: 600mm
Common Uses: Stair risers and stair treads
Supplier: Northwest Landscape Supply



Acrylic concrete paint shapes

Concrete paint to be Star spectrum
acrylic based vibrancy
coloured specially coating (or
approved substitute).
Colours: Sky Blue, Cool Grey

3 COLOURED CONCRETE CIRCLES @ DRIVE AISLE

4 FLAGSTONE

Scale 1:10

No.	By:	Description	Date
11	Li	Re-issued for DP	June 7, 2019
10	MAS	Re-issued for DP	Apr. 23, 2019
9	SH	Re-issued for DP	Mar. 26, 2019
8	MAS	Re-issued for DP	Feb 19, 2019
7	MAS	Issued for ADDP	Dec 20, 2018
6	MAS	Re-issued for DP	Nov 23, 2018
5	MAS	Client Review	Oct 24, 2018
4	MAS	Issued for DP	July 13, 2018
3	MAS	Issued for Revising	Jan 11, 2018
2	MAS	Client Review	Dec 12, 2017
1	MAS	Client Review	Nov 24, 2017

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DP 18-829241 REFERENCE PLAN

AUG 06 2019

CONTEXT PLAN

SCALE: 1:300

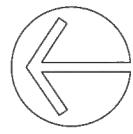
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102 - 2425 Quebec Street
Vancouver, BC V5T 4L6
604-873-2907
fougmarchitects.ca



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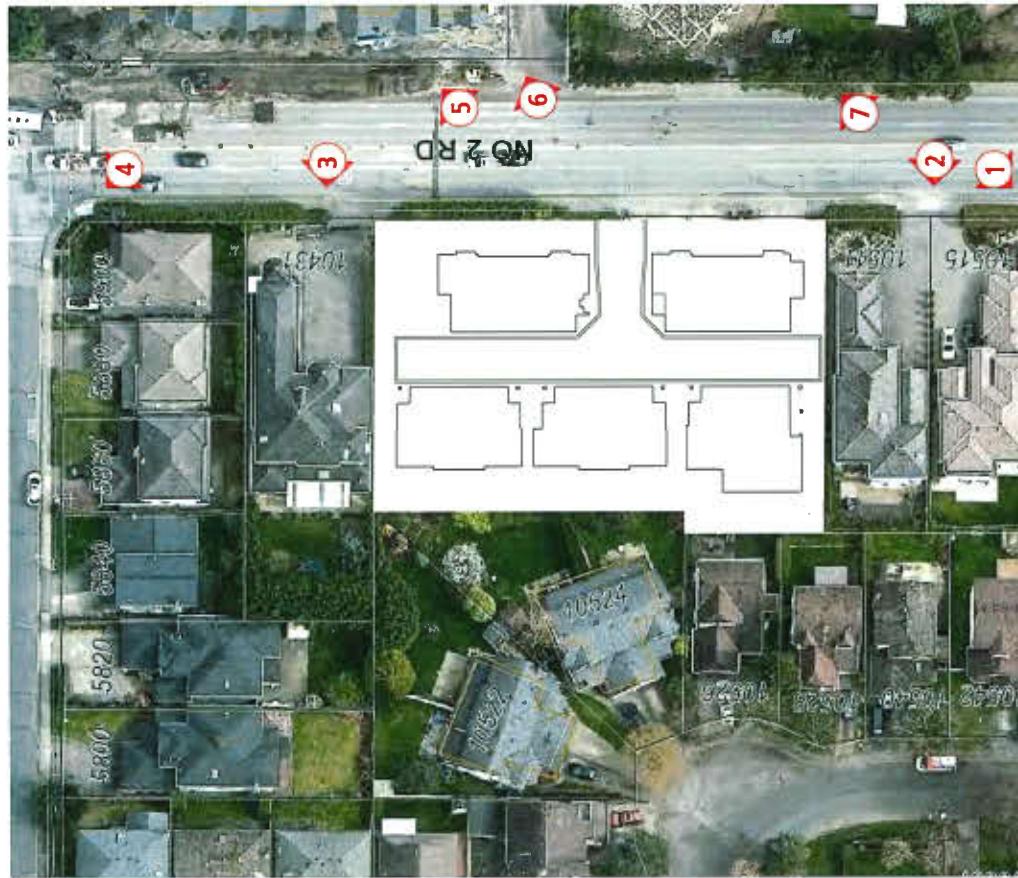
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STREETSCAPE

SCALE: 1:150
0 3 6 9



R 10431 No 2 Road

PROPOSED DEVELOPMENT

10515 No 2 Road R

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NO. 2 ROAD

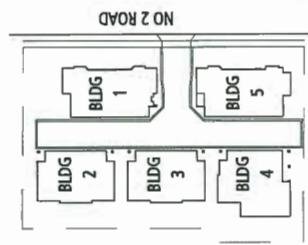
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PERSPECTIVES

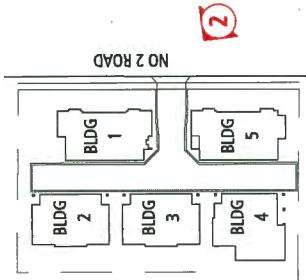


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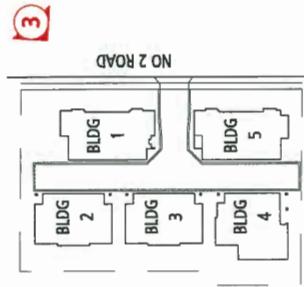


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