



Development Permit Panel

Council Chambers, City Hall
6911 No. 3 Road

Wednesday, July 16, 2014
3:30 p.m.

1. Minutes

*Motion to adopt the **minutes** of the Development Permit Panel meeting held on Wednesday, June 25, 2014.*



2. **Development Permit 14-662568**

(REDMS No. 4266509)

APPLICANT: 616147 BC Ltd.

PROPERTY LOCATION: 4220 Vanguard Road

Director's Recommendations

That a Development Permit be issued which would permit the construction of a 283 m² industrial storage warehouse building with mezzanine level at 4220 Vanguard Road on a site zoned "Industrial Retail (IR1)."



3. **Development Permit 14-657502**

(REDMS No. 4254762)

APPLICANT: Kasian Architecture Interior Design and Planning Ltd.

PROPERTY LOCATION: 11380 Steveston Hwy.

ITEM

Director's Recommendations

That a Development Permit be issued which would permit the construction of an 882 m² addition and exterior renovation to the building at 11380 Steveston Hwy on a site zoned "Industrial Community Commercial (ZC6) – Ironwood Area."



4. Development Variance 13-634940

(REDMS No. 4245844)

APPLICANT: Onni 7731 Alderbridge Holding Corp.

PROPERTY LOCATION: 5311 and 5399 Cedarbridge Way

Director's Recommendations

That a Development Variance Permit be issued which would further vary the provisions of Richmond Zoning Bylaw 8500 to further reduce the visitor parking requirement from 0.15 spaces/unit, as per Development Permit (DP 12-615424), to 0.10 spaces/unit for a portion of the development located at 5311 and 5399 Cedarbridge Way on a site zoned "High Density Low Rise Apartments (RAH2)."



5. New Business

6. Date of Next Meeting: Wednesday, July 30, 2014

7. Adjournment



Development Permit Panel Wednesday, June 25, 2014

Time: 3:30 p.m.
Place: Council Chambers
Richmond City Hall
Present: Joe Erceg, Chair
Robert Gonzalez, General Manager, Engineering and Public Works
Dave Semple, General Manager, Community Services

The meeting was called to order at 3:30 p.m.

1. Minutes

It was moved and seconded

That the minutes of the meeting of the Development Permit Panel held on Wednesday, June 11, 2014, be adopted.

CARRIED

2. Development Permit 13-650988

(File Ref. No.: DP 13-650988) (REDMS No. 4144693)

APPLICANT: First Richmond North Shopping Centres Ltd., (SmartCentres)

PROPERTY LOCATION: 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road

INTENT OF PERMIT:

1. To permit the construction of a neighbourhood commercial centre at 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road on a site zoned "Neighbourhood Commercial (ZC32) – West Cambie Area"; and
2. To vary the provisions of Richmond Zoning Bylaw 8500 to reduce the building setback for Building N on May Drive from 5.0 m to 1.5 m.

Development Permit Panel

Wednesday, June 25, 2014

Applicant's Comments

With the aid of a visual presentation, (attached to and forming part of these minutes as **Schedule 1**) Christopher Block, Architect, Chandler Associates Architecture Inc., and Mary Chan Yip, Landscape Architect, PMG Landscape Architects, gave a brief overview of the revisions made on the proposed development with respect to (i) urban design; (ii) architectural form and character; and (iii) landscaping and open space design.

Mr. Block spoke of the proposed design enhancements on buildings A, B and L. He commented on the added glazing, improved lighting elements and the upgraded materials that will be incorporated into the design.

Mr. Block then commented on the loading bay and noted of the proposed design enhancements such as increasing the screening wall height and trellis sizing to deflect noise and provide visual screening.

Discussion ensued with regard to the location of the District Energy Utility (DEU) infrastructure to be located on-site. Mr. Block noted that possible locations for the DEU structure could include the edge of the green deck, the roof of building B, or the roof of building C.

Discussion ensued regarding the elevation of the proposed development in relation to other developments in the area.

In reply to queries from the Panel, Mr. Block advised that the location of the DEU structure will be finalized in later stages of the design process.

Discussion ensued about the preferred location for the DEU structure within the proposed development.

Ms. Yip commented on the features of the proposed green deck above the surface parking area. She noted that the proposed green deck will replicate features of a shoreline and will incorporate elements such as logs and boulders. Also, she spoke of incorporating native plant species onto the green deck. She added that thorny plants will be planted along the perimeter of the green deck as a safety feature to encourage children to stay away from the edges.

Ms. Yip then commented on the different native species of plants incorporated in the proposed development and the further refinement of the proposed planting list since the last meeting.

Staff Comments

Wayne Craig, Director, Development, spoke of the potential sites for the DEU structure and noted that a general compliance may be needed in the future, pending on the proposed location and design.

Correspondence

None.

Development Permit Panel
Wednesday, June 25, 2014

Gallery Comments

None.

Panel Discussion

Panel expressed their appreciation for the revisions made on the proposed development.

Panel Decision

It was moved and seconded

That a Development Permit be issued which would:

1. *Permit the construction of a neighbourhood commercial centre at 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road on a site zoned "Neighbourhood Commercial (ZC32) – West Cambie Area"; and*
2. *Vary the provisions of Richmond Zoning Bylaw 8500 to reduce the building setback for Building N on May Drive from 5.0 m to 1.5 m.*

CARRIED

3. New Business

None.

4. Date Of Next Meeting: Wednesday, July 16, 2014

5. Adjournment

It was moved and seconded

That the meeting be adjourned at 3:51 p.m.

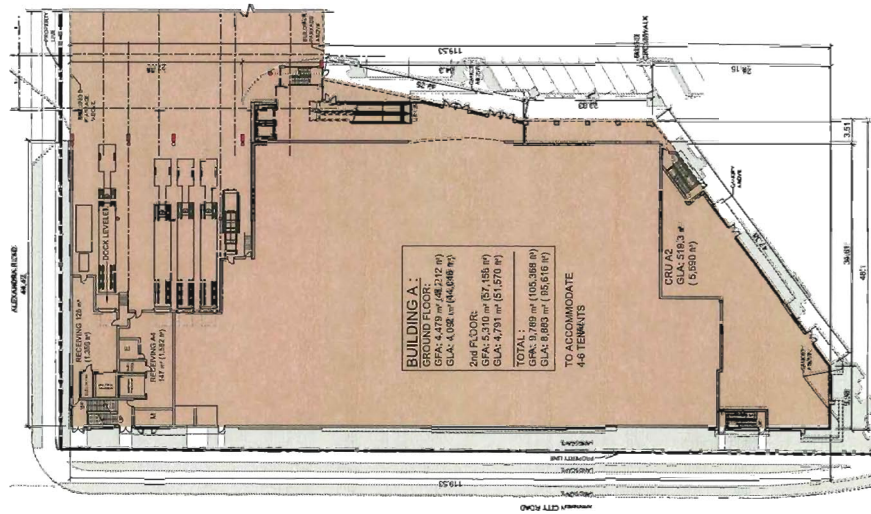
CARRIED

Development Permit Panel
Wednesday, June 25, 2014

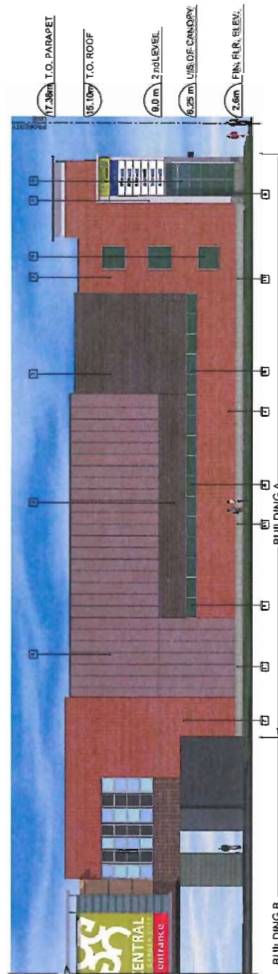
Certified a true and correct copy of the Minutes of the meeting of the Development Permit Panel of the Council of the City of Richmond held on Wednesday, June 25, 2014.

Joe Erceg
Chair

Evangel Biason
Auxiliary Committee Clerk



GROUND FLOOR PLAN
SCALE: 1:200



NORTH ELEVATION
SCALE: 1:150

PRELIMINARY SIGNAGE ANALYSIS

| EAST ELEVATION | | |
|----------------|---|---------------------------------|
| REQUIRED: | REMITTED SIGN/AREA/ LINEAR; FRONTAGE | TOTAL SIGN AREA PERMITTED |
| 52.20 m | 1 m² | 62.28 m² |

| PROPOSED : | 1.3 x 1.2 | 1.6 m ² |
|------------|-----------|---------------------|
| SIGNAGE 1 | 1.3 x 1.2 | 1.6 m ² |
| SIGNAGE 2 | 1.3 x 1.2 | 1.6 m ² |
| SIGNAGE 3 | 1.3 x 1.2 | 1.6 m ² |
| SIGNAGE 4 | 1.3 x 1.2 | 1.6 m ² |
| SIGNAGE 5 | 0.8 x 2.2 | 1.58 m ² |
| SIGNAGE 6 | 7/8 x 5.6 | 10.6 m ² |
| SIGNAGE 7 | 0.9 x 2.2 | 1.3 m ² |
| SIGNAGE 8 | 0.9 x 2.2 | 1.3 m ² |
| SIGNAGE 9 | 1.9 x 10 | 19.0 m ² |
| SIGNAGE 10 | 1.5 x 6.5 | 10.5 m ² |
| TOTAL | | 81.2 m ² |

| NORTH ELEVATION | | |
|-----------------|---------------------------------------|---------------------------|
| REQUIRED: | | |
| BLDG. WIDTH | PERMITTED SIGN AREA / LINEAR FRONTAGE | TOTAL SIGN AREA PERMITTED |
| 44.72 m | 1.0m* | 44.72 m² |

| PROPOSED: | |
|--------------|-------------------|
| SIGNAGE 1 | 3.2 x 4.0 12.8 m² |
| SIGNAGE 2 | 3.2 x 4.0 12.8 m² |
| SIGNAGE 3 | 3.2 x 4.0 12.8 m² |
| TOTAL | 38.4 m² |

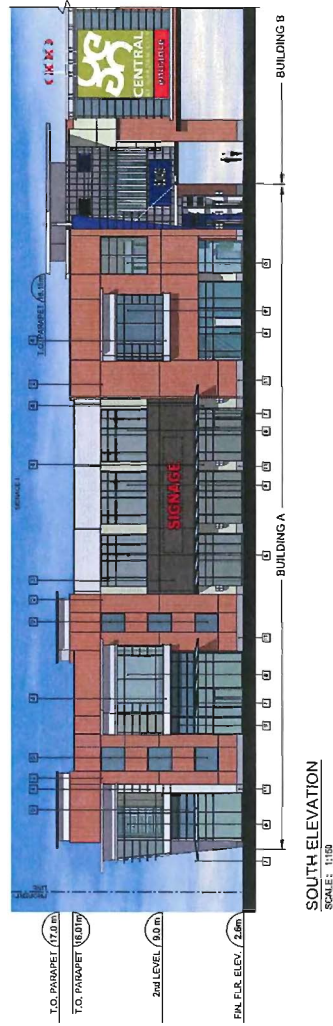
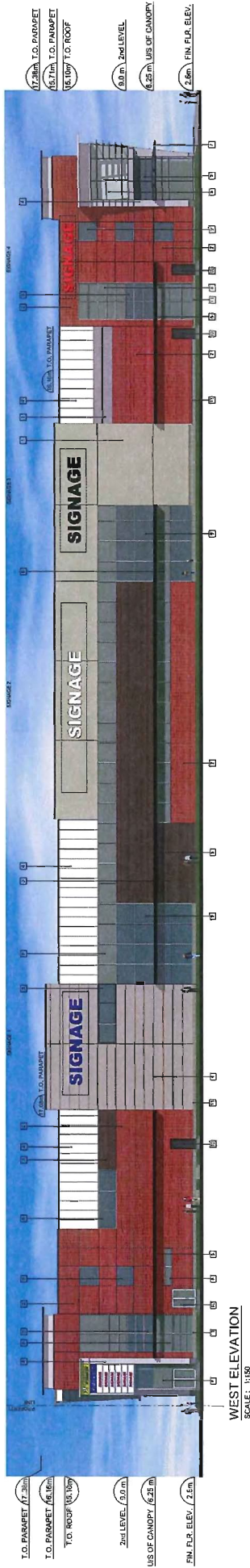


 KEYPLAN
NTS

BUILDING A PLANS AND ELEVATIONS

CENTRAL AT GARDEN CITY

RICHMOND, BC



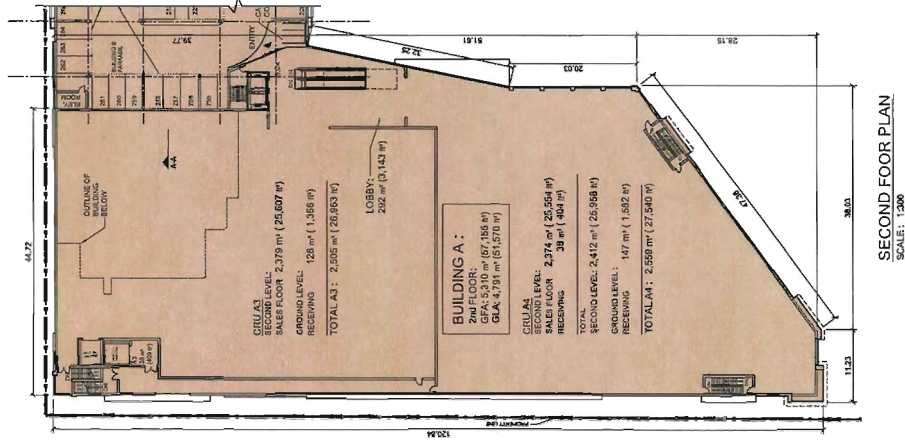
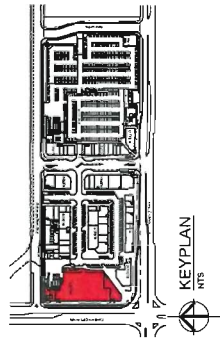
FINISH LEGEND AND COLOUR SCHEDULE:

- | | | |
|----------------------|---------------------------------------|-------------------------------------|
| 1 - EIFS | 5 - METAL EYEBROW | 9 - SPARKLE GLASS IN ALUMINUM FRAME |
| 2 - BRICK VENER | 6 - CANVAS AWNING | 10 - METAL DOOR |
| 3 - CORRUGATED METAL | 7 - METAL & GLASS CANOPY | 11 - CONCRETE UPS/AND |
| 4 - METAL PANELS | 8 - GLAZING IN BRUSHED ALUMINUM FRAME | 12 - PREFINISHED WOOD PANELS |

PRELIMINARY SIGNAGE ANALYSIS

| WEST ELEVATION | |
|--|--------------------------|
| REQUIRED | PERMITTED |
| BLDG. SIGN AREA / LENGTH | TOTAL SIGN AREA / LENGTH |
| 119.53 m ² / 1 m ² | 115.53 m ² |
| PROPOSED: | |
| SIGNAGE 1: 2.5 x 13 | 24 m ² |
| SIGNAGE 2: 2.5 x 18.4 | 46 m ² |
| SIGNAGE 3: 2.5 x 10 | 25 m ² |
| SIGNAGE 4: 1.5 x 8.0 | 12 m ² |
| TOTAL | 108 m ² |

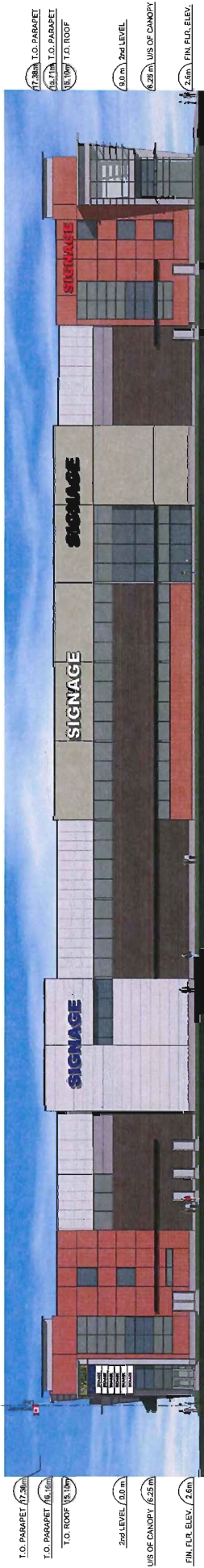
| SOUTH ELEVATION | |
|---|-------------------------|
| REQUIRED | PERMITTED |
| BLDG. SIGN AREA / WIDTH | TOTAL SIGN AREA / WIDTH |
| 47.28 m ² / 1 m ² | 47.28 m ² |
| PROPOSED: | |
| SIGNAGE 1: 1.5 x 9.0 | 13.5 m ² |
| TOTAL | 13.5 m ² |



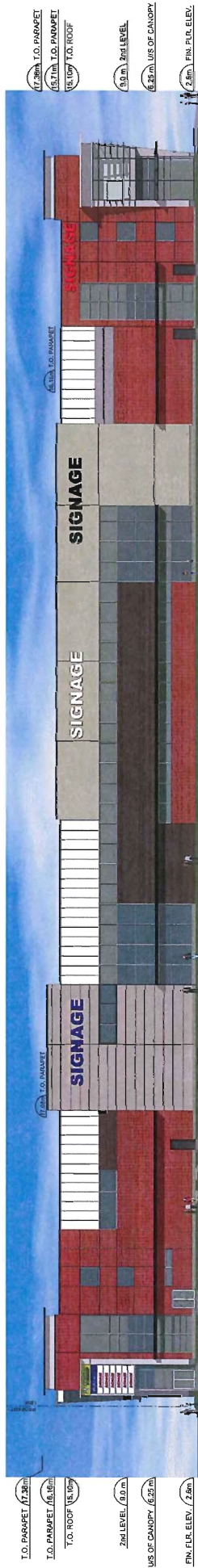
BUILDING A PLANS AND ELEVATIONS

CENTRAL AT GARDEN CITY

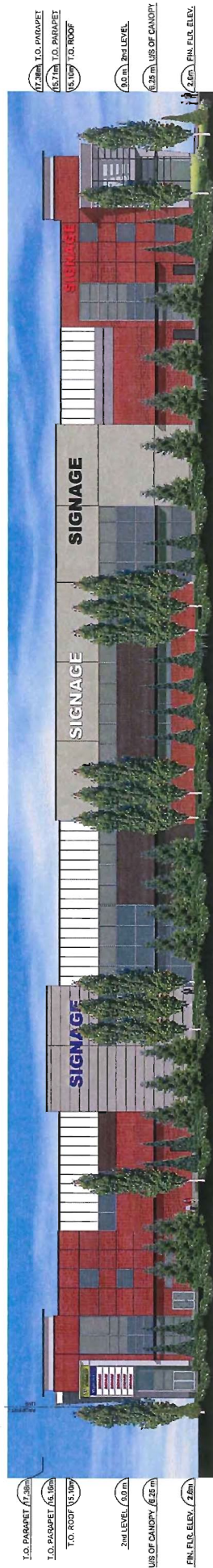
RICHMOND, BC



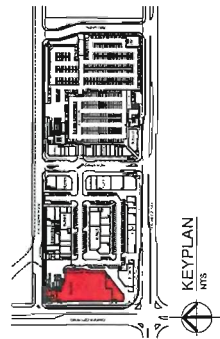
WEST ELEVATION - DP MARCH 24 2014
SCALE: 1/160

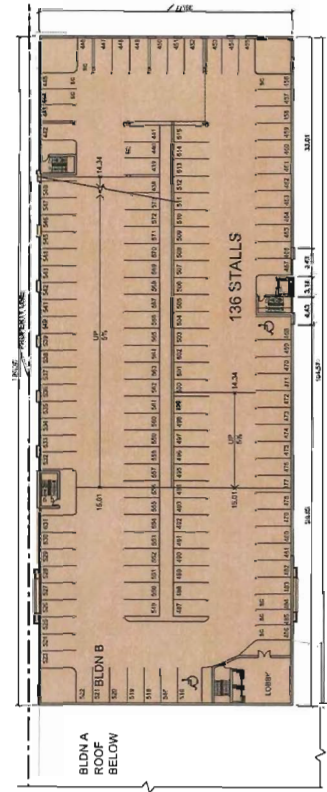
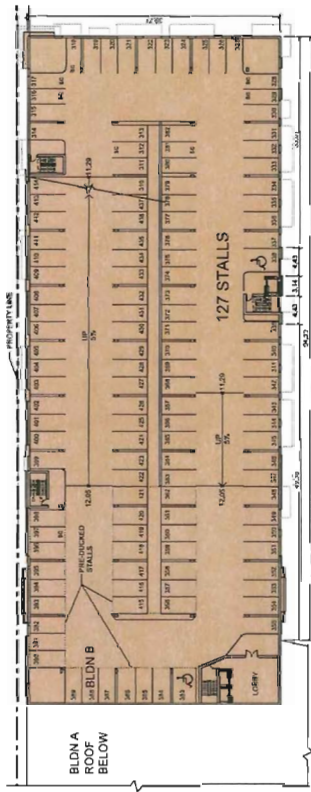
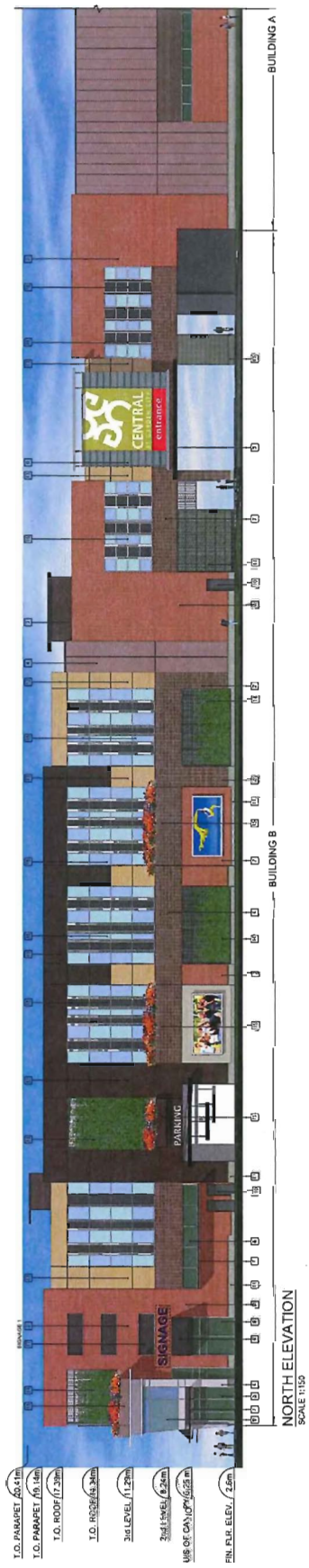


PROPOSED WEST ELEVATION
SCALE: 1/160



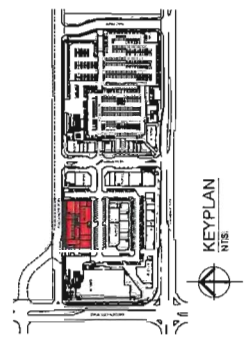
PROPOSED WEST ELEVATION WITH LANDSCAPE
SCALE: 1/160





FINISH LEGEND AND COLOUR SCHEDULE:

| | | | |
|----------------------|---------------------------------------|--------------------------------------|---|
| 1 - EPS | 13 - METAL EYEBROW | 9 - SPANDREL GLASS IN ALUMINUM FRAME | 15 - PAINTED CONCRETE |
| 2 - BRICK VENEER | 6 - CANVAS AWNING | 10 - METAL DOOR | 16 - METAL SCREEN |
| 3 - CORRUGATED METAL | 7 - METAL & GLASS CANOPY | 11 - CONCRETE UPSTAND | 17 - PLASTER |
| 4 - METAL PANELS | 8 - GLAZING IN BRUSHED ALUMINUM FRAME | 12 - PRESFINISHED WOOD PANELS | 18 - TINTED GLASS / INTEGRATED MESH DECORATIVE SCREEN |
| | | | 19 - PERFORATED MESH SCREEN |

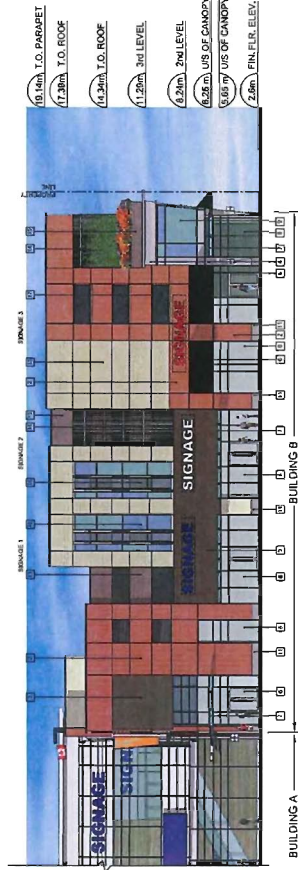


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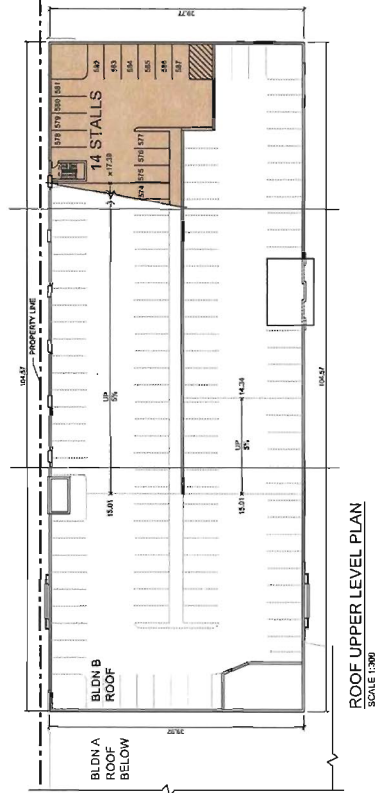
BUILDING B PLANS AND ELEVATIONS CENTRAL AT GARDEN CITY RICHMOND, BC



DATE: JUNE 18, 2014
 SCALE: AS SHOWN
 PROJECT #: A-3.4



EAST ELEVATION
SCALE 1:150



ROOF UPPER LEVEL PLAN
SCALE 1:300

PRELIMINARY SIGNAGE ANALYSIS

EAST ELEVATION

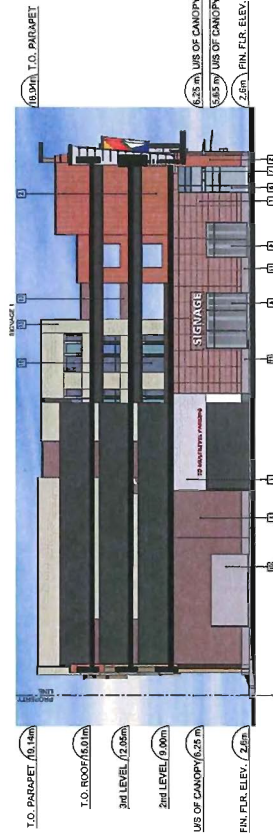
| REQUIRED: | PERMITTED | TOTAL SIGN |
|-------------|----------------------|---------------------|
| BLDG. WIDTH | 29.77 m | 1 m ² |
| FRONTAGE | 1 m ² | 1 m ² |
| TOTAL | 29.77 m ² | 18.9 m ² |

| PROPOSED: | SIGNAGE 1 | SIGNAGE 2 | TOTAL SIGN |
|-------------|-----------|-----------|---------------------|
| BLDG. WIDTH | 1.0 x 6.0 | 1.0 x 6.0 | 6.0 m ² |
| FRONTAGE | 1.0 x 6.0 | 1.0 x 6.0 | 6.0 m ² |
| TOTAL | 1.0 x 6.0 | 1.0 x 6.0 | 18.9 m ² |

WEST ELEVATION

| REQUIRED: | PERMITTED | TOTAL SIGN |
|---------------------|----------------------|----------------------|
| BLDG. LINEAR LENGTH | 30.77 m | 1 m ² |
| FRONTAGE | 1 m ² | 1 m ² |
| TOTAL | 30.77 m ² | 29.77 m ² |

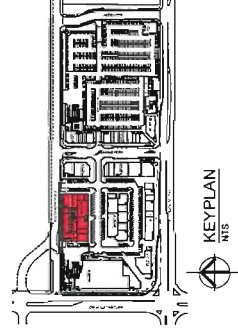
| PROPOSED: | SIGNAGE 1 | SIGNAGE 2 | TOTAL SIGN |
|---------------------|------------------|--------------------|--------------------|
| BLDG. LINEAR LENGTH | 1 x 4.5 | 4.5 m ² | 4.5 m ² |
| FRONTAGE | 1 m ² | 1 m ² | 1 m ² |
| TOTAL | 1 x 4.5 | 4.5 m ² | 4.5 m ² |



WEST ELEVATION
SCALE 1:150

FINISH LEGEND AND COLOUR SCHEDULE:

| | | | |
|----------------------|--|---------------------------------------|---|
| 1 - EPS | 5 - METAL EYEBROW | 9 - SPANDREL GLASS IN ALUMINIUM FRAME | 13 - PAINTED CONCRETE |
| 2 - BRICK VENEER | 8 - CANVAS AWNING | 10 - METAL DOOR | 14 - METAL SCREEN |
| 3 - CORRUGATED METAL | 7 - METAL & GLASS CANOPY | 11 - CONCRETE UPSTAND | 15 - PLANTER |
| 4 - METAL PANELS | 6 - GLAZING IN BRUSHED ALUMINIUM FRAME | 12 - PREFINISHED WOOD PANELS | 16 - TINTED GLASS / PERFORATED MESH DECORATIVE SCREEN |
| | | | 17 - PERFORATED MESH SCREEN |



KEY PLAN
N/E

BUILDING B PLANS AND ELEVATIONS CENTRAL AT GARDEN CITY

RICHMOND, BC





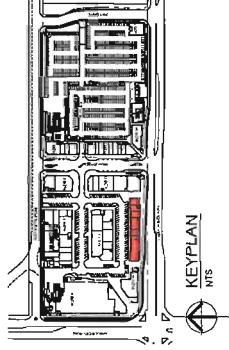
SOUTH ELEVATION - DP MARCH 24 2014
SCALE 1:100



PROPOSED SOUTH ELEVATION
SCALE 1:100



PROPOSED SOUTH ELEVATION WITH LANDSCAPE
SCALE 1:100

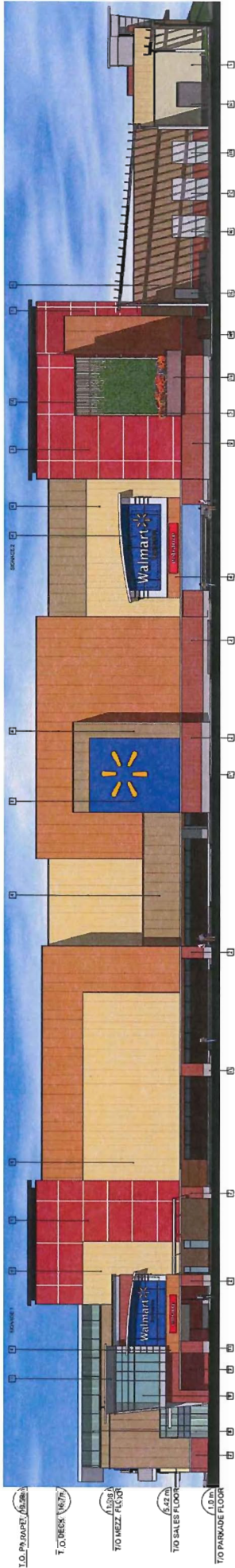


Chandler Associates
Graphic Architects
Architectural Inc.
The Building
1000 West 10th Avenue
Vancouver BC V6H 1G1
Tel: 604.681.1234
Fax: 604.681.1235
office@can-architects.com

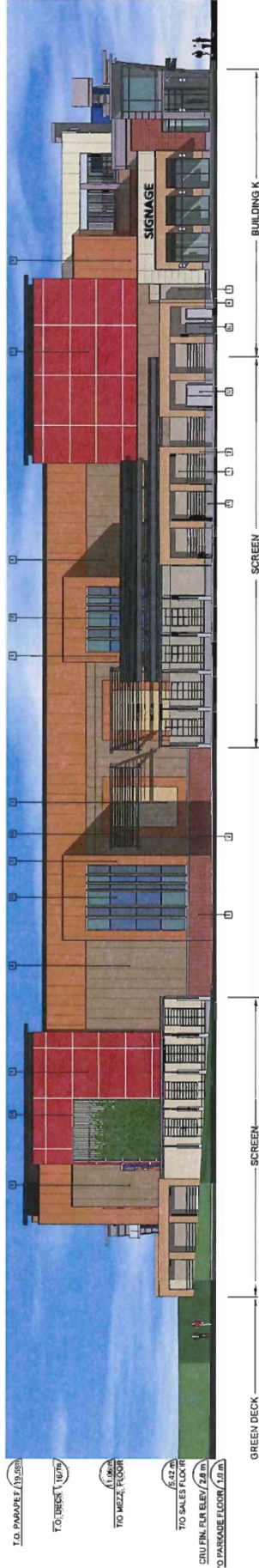
DATE: JUNE 18, 2014
SCALE: AS NOTED
PROJECT # 407
A-3.19sup

BUILDING L PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC





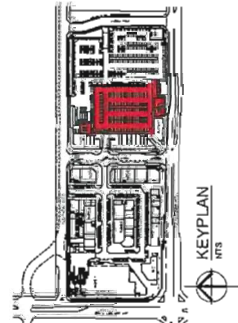
EAST ELEVATION
SCALE 1:150



NORTH ELEVATION
SCALE 1:150

FINISH LEGEND AND COLOUR SCHEDULE:

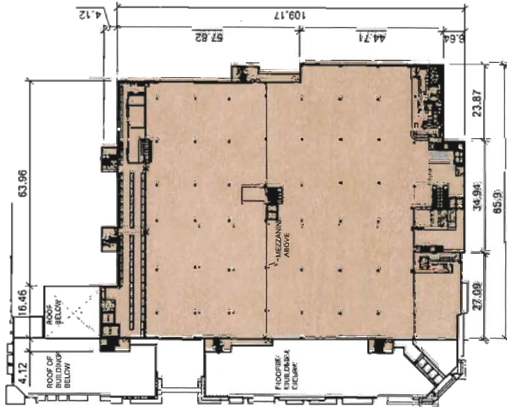
| | | | | | | | |
|---|--------------------|----|-----------------------------------|----|------------------------------------|----|--------------------|
| 1 | - BRICK VENEER | 8 | - METAL EYEBROW | 9 | - SPANDREL GLASS IN ALUMINUM FRAME | 10 | - PAINTED CONCRETE |
| 2 | - CORRUGATED METAL | 9 | - CANVAS AWNING | 10 | - METAL DOOR | 11 | - METAL SCREEN |
| 3 | - METAL PANELS | 10 | - METAL & GLASS CANOPY | 11 | - CONCRETE UPSTAND | 12 | - METAL PLANTER |
| 4 | - METAL PANELS | 11 | - GLASS IN BRUSHED ALUMINUM FRAME | 12 | - BRUSHED WOOD PANELS | 13 | - METAL SPANDREL |



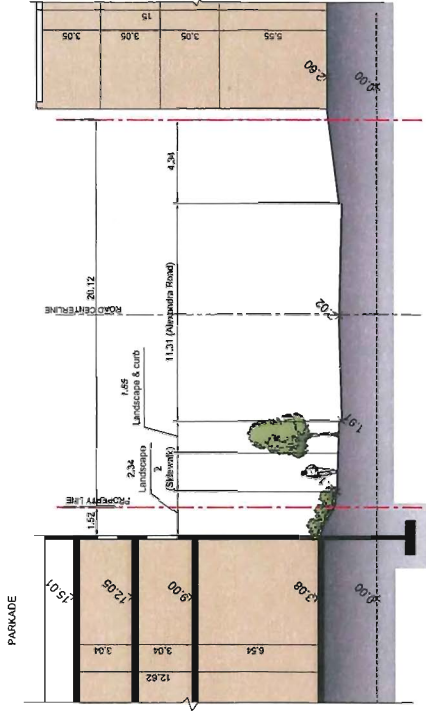
KEY PLAN
SCALE 1:150

PRELIMINARY SIGNAGE ANALYSIS

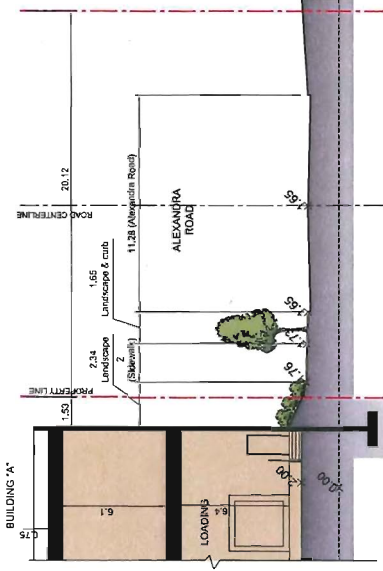
| REQUIRED: | | PROPOSED: | |
|-----------|---------|-----------|-----------|
| BUILDING | 115.8 m | SIGNAGE 1 | 7.5 x 2.5 |
| FRONTAGE | 1.0 m | SIGNAGE 2 | 7.5 x 2.5 |
| | | TOTAL | 37.6 m |



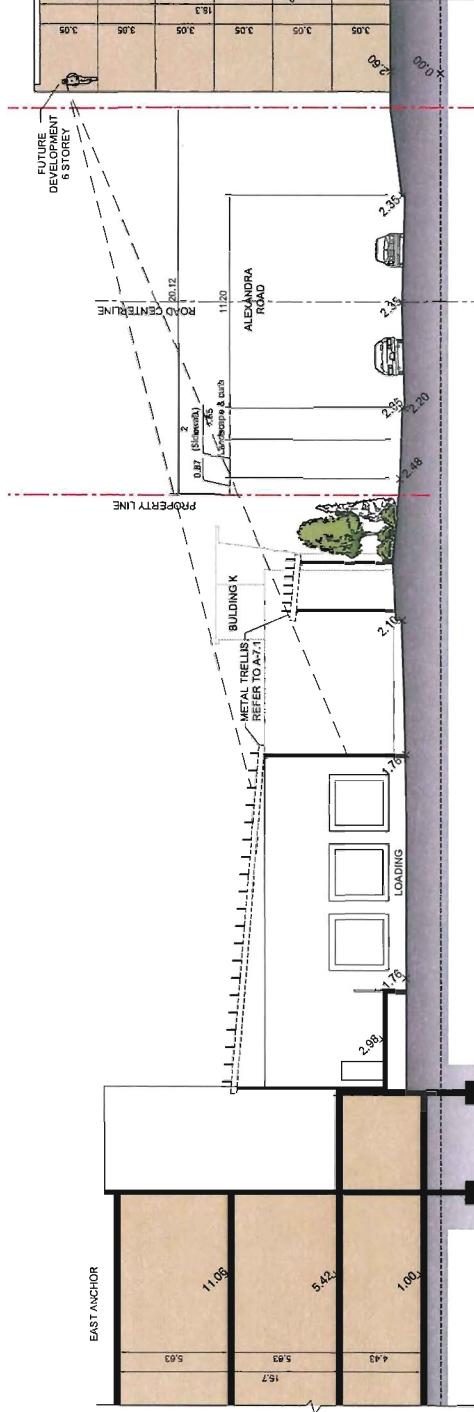
SALES LEVEL PLAN
SCALE 1:150



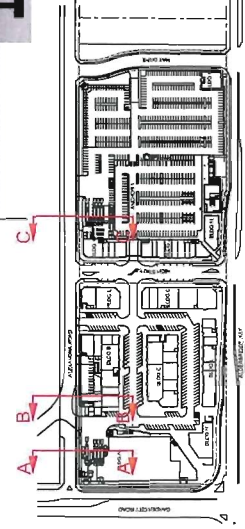
SECTION B-B
SCALE 1:100



SECTION A-A
SCALE 1:100



SECTION C-C
SCALE 1:100



KEY PLAN
SCALE 1:100



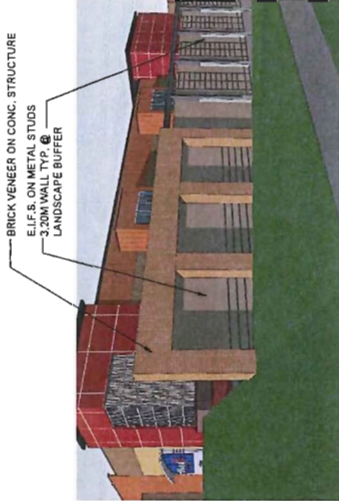
3 - PARKADE SCREENS
(NTS)



2 - PARKADE SCREENS
(NTS)



4b - WALL TRELLIS - 3D VIEW
(NTS)



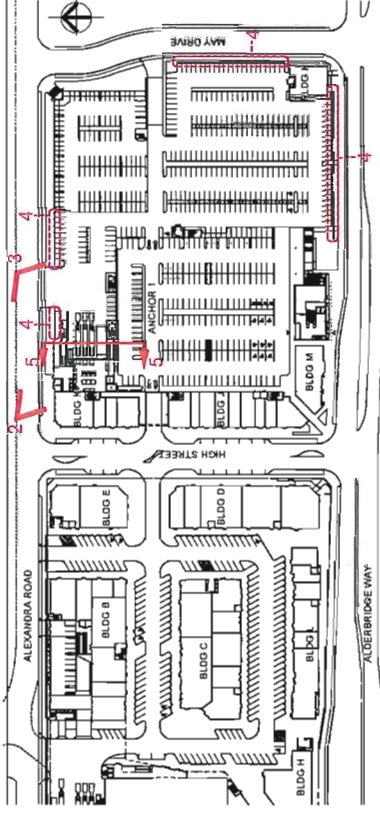
4a - WALL TRELLIS - 3D VIEW
(NTS)



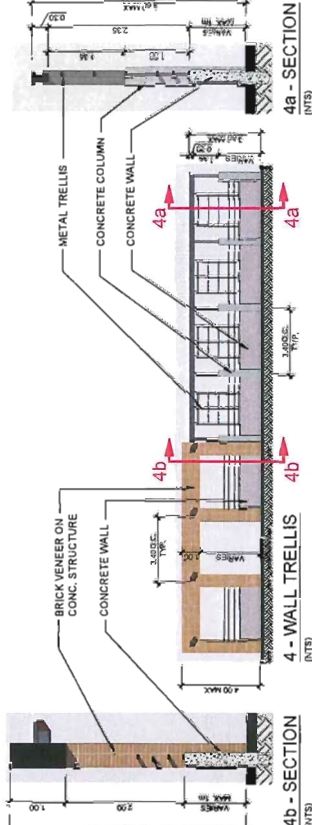
5c - TRELLIS AT LOADING DOCK - 3D VIEW
(NTS)



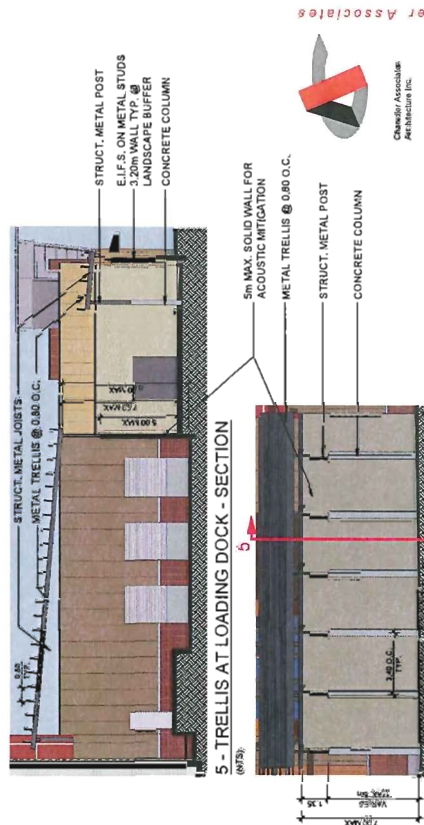
5b - TRELLIS AT LOADING DOCK - 3D VIEW
(NTS)



1 - KEY PLAN
(NTS)



4b - SECTION
(NTS)

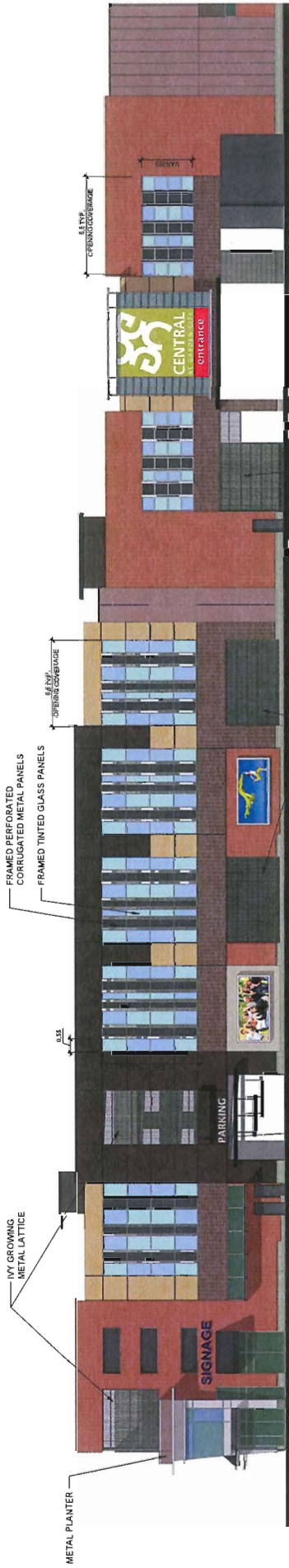


5a - TRELLIS AT LOADING DOCK - ELEVATION
(NTS)

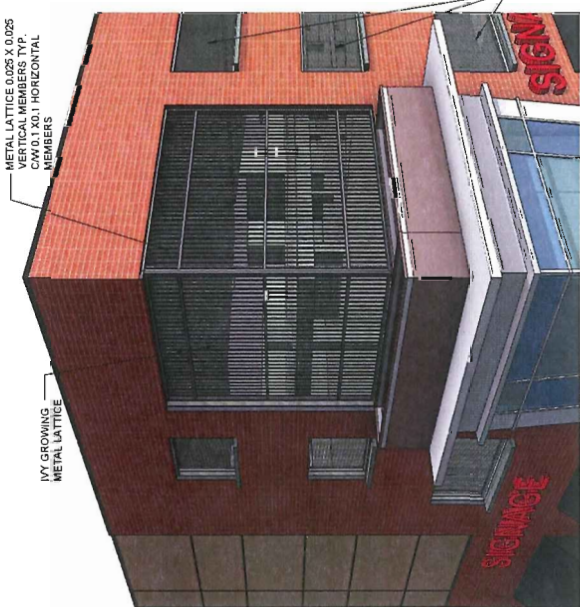
TRELLIS ELEVATIONS, PERSPECTIVES AND DETAILS

CENTRAL AT GARDEN CITY

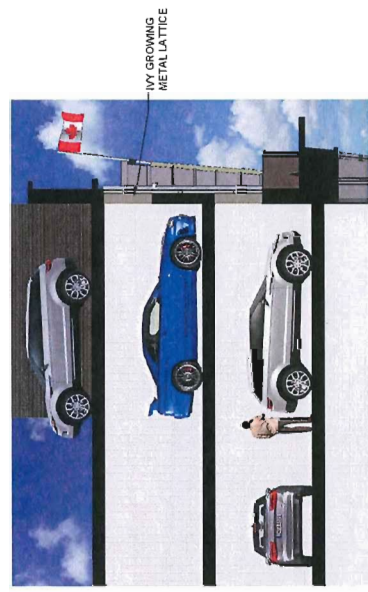
RICHMOND, BC



BUILDING 'B' NORTH ELEVATION
SCALE 1:125



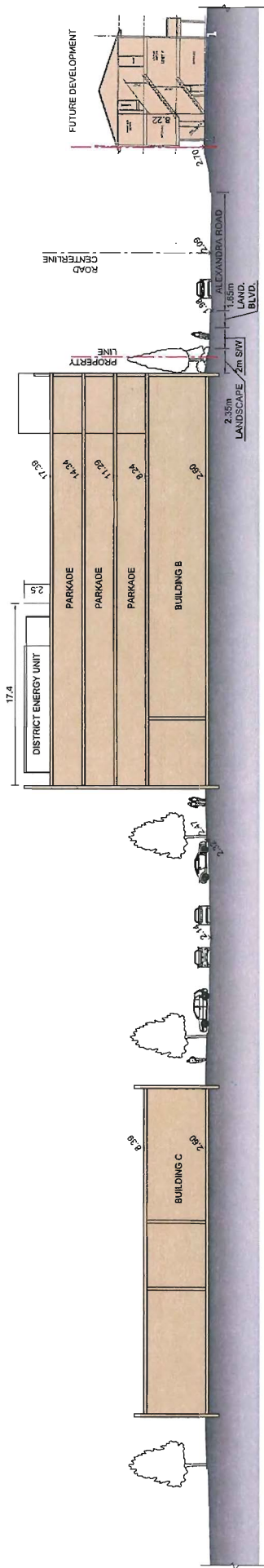
PERSPECTIVE AT TYPICAL METAL LATTICE (PARKADE SCREEN)
SCALE 1:125



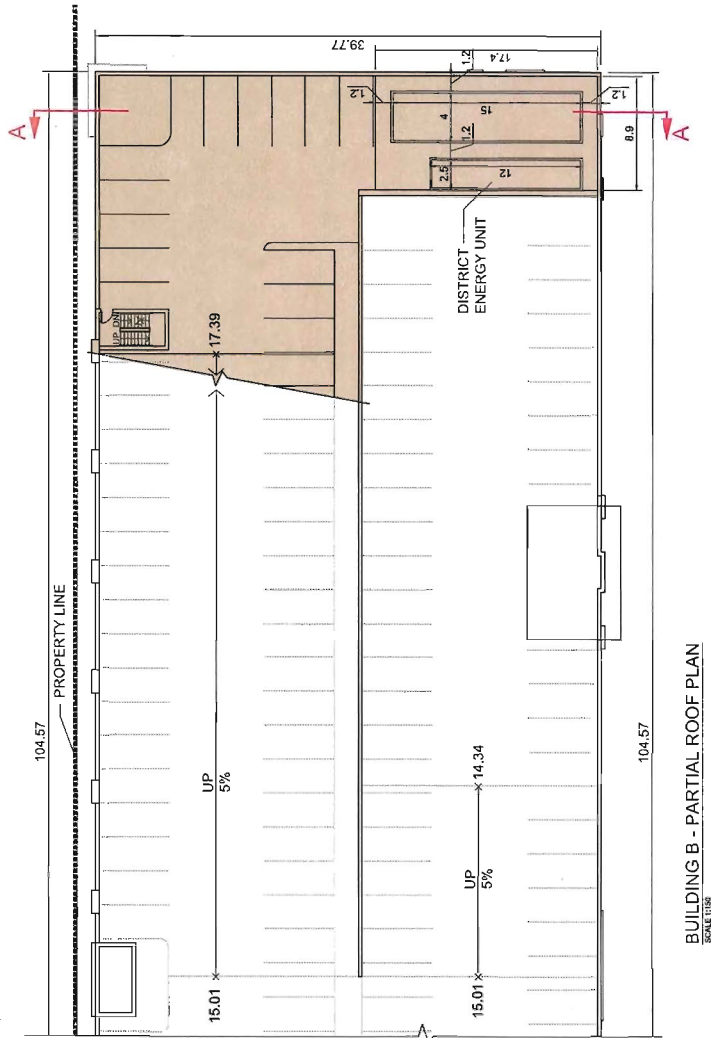
SECTION THROUGH TYPICAL METAL LATTICE
SCALE 1:125



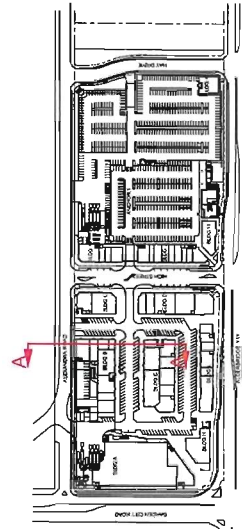
TYPICAL DECORATIVE PARKADE SCREENS
SCALE 1:125

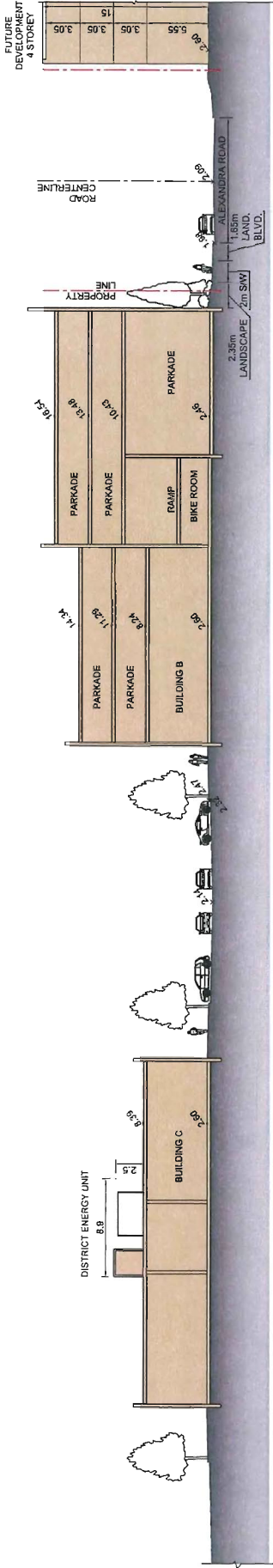


SECTION A-A
SCALE 1:100

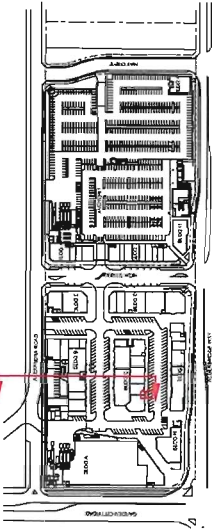
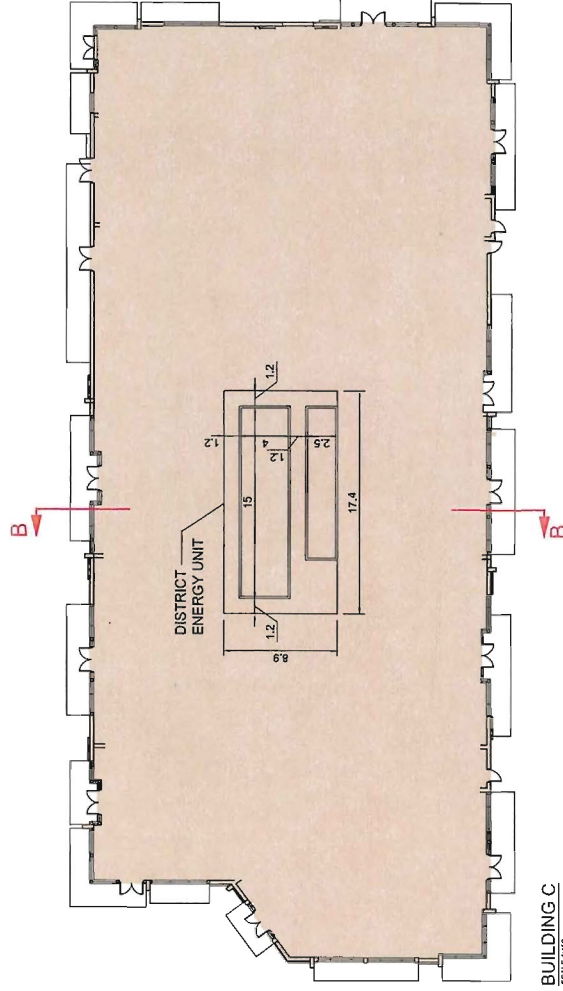


BUILDING B - PARTIAL ROOF PLAN
SCALE 1:100





SECTION B-B
SCALE 1:150



CENTRAL AT GARDEN CITY

RICHMOND, BC

SECTION & PLAN - PROPOSED DEU LOCATIONS

Chandler Associates
Architectural Inc.
3750 - 101st Ave, Coquitlam, BC
V3C 6H7
Tel: 604 681 2325
Fax: 604 681 2325
info@chandlerassociates.com

DATE: JUNE 18, 2014
SCALE: AS NOTED
PROJECT #:

DEU-2



City of Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: June 24, 2014

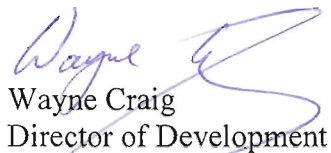
From: Wayne Craig
Director of Development

File: DP 14-662568

Re: Application by 616147 BC Ltd. for a Development Permit at 4220 Vanguard Road

Staff Recommendation

That a Development Permit be issued which would permit the construction of a 283 m² industrial storage warehouse building with mezzanine level at 4220 Vanguard Road on a site zoned "Industrial Retail (IR1)".


Wayne Craig
Director of Development

WC:sb
Att.

Staff Report

Origin

616147 BC Ltd. has applied to the City of Richmond for permission to develop an industrial warehouse building at 4220 Vanguard Road on a site zoned “Industrial Retail (IR1)”.

A Development Permit is required for this proposal because the industrial property is in close proximity to residential properties to the east. Industrial development does not typically require a Development Permit.

There is no associated rezoning or Servicing Agreement application. The site currently contains surface parking and accessory structures.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the north, fronting onto Vanguard Road are a two-storey building and single-storey auto repair building on properties zoned “Industrial Retail (IR1)”.

To the east, fronting onto Danforth Drive are residential homes on properties zoned Single Detached (RS1/E).

To the south, fronting onto Vanguard Road is a two-storey building with warehouse, accessory structures and surface parking on two (2) additional properties under the same ownership as the subject property and the same “Industrial Retail (IR1)” zoning.

To the west, across Vanguard Road are a number of two-storey buildings, warehouses and surface parking on properties zoned “Industrial Retail (IR1)”.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan and is in compliance with the “Industrial Retail (IR1)” zone.

Advisory Design Panel Comments

The proposal was not presented to the Advisory Design Panel as it is modest accessory storage warehouse building for a larger industrial complex in an industrial subdivision.

Analysis

Floodplain Management

- In accordance with the City's Flood Plain Designation and Protection Bylaw 8204, the owner has agreed to register a floodplain covenant as a consideration of the Development Permit specifying a minimum habitable elevation of no lower than 0.3 m above the highest elevation of the crown of the adjacent road (Vanguard Road).

OCP Aircraft Noise Sensitive Development (ANSD) Policy

- The site is located within Area 3 (Moderate Aircraft Noise Area) of the ANSD map. Area 3 allows for the proposed industrial use and all aircraft noise sensitive land uses. The policy also requires the registration of a restrictive covenant on title to address public awareness and aircraft noise mitigation for residential uses. In accordance with the policy, the owner has agreed to register an aircraft noise sensitive use restrictive covenant as a consideration of the Development Permit.

Conditions of Adjacency

- The subject site is located in the East Cambie planning area, in an existing industrial subdivision that is located between Shell Road, Highway 99, Cambie Road and an existing residential subdivision to the east. The area is intended and zoned for industrial uses.
- The subject site backs onto the rear yards of the residential properties to the east. The proposal mitigates the impacts of industrial development on these properties by supplementing the approximately 9 m (30 ft) tall cedar hedge on the neighbouring residential property with the addition of a new 2 m (6.5 ft) tall cedar hedge on the subject property, by locating the 7.8 m (25.6 ft) tall building at the rear of the site between the residential properties and operational activities on the site and by not having any window or door openings on the rear facade.
- The proposal building is not anticipated to result in any noise concerns as it is intended only as a storage warehouse, not as an industrial production facility and will have only small heating equipment inside the building. No external HVAC equipment is proposed for the building. The owner is required to comply with the City's noise regulation requirements both during construction and for the industrial operations.
- The subject site is bordered by an industrial property to the north. Existing fencing and hedging will be retained along the interface.
- The proposed development on the subject site will supplement an existing industrial operation on the two (2) properties to the south and therefore no buffer or separation is proposed along the south edge of the subject site.
- Frontage improvements along Vanguard Road at the west edge of the subject site are not proposed as part of the subject Development Permit application. The existing gravel shoulder, grass and ditch will be retained and may be improved in the future with redevelopment of the larger three-lot parcel.

- Frontage improvements are not sought along Vanguard Road as part of the subject Development Permit application on a relatively small site in the middle of the block for a relatively small building to complement a larger industrial operation. The existing industrial business operates on three adjacent properties, with the larger main building on the middle property and the proposed smaller storage warehouse on the northern property.

Urban Design and Site Planning

- The subject site is one (1) of three (3) properties owned by the owner for the operation of the industrial business (4220, 4260 & 4300 Vanguard Road). The subject site is accessed by an existing driveway on the adjacent property at 4260 Vanguard Road and the owner has agreed to legally secure this arrangement by registering a cross-access easement as a consideration of the Development Permit.
- The proposed warehouse building has been located at the rear of the property, providing a buffer between adjacent residential properties and operational activities on the subject site such as materials and equipment movement, car and truck activity as well as lighting in the surface manoeuvring and parking area.

Architectural Form and Character

- A simple modest storage warehouse building is proposed, mostly finished with corrugated aluminum siding. Smaller areas of horizontal fibre cement board siding, anodized aluminum double pane low E windows, metal overhead door and vinyl fabric canopies over the door ways are proposed.

Landscape Design and Open Space Design

- The landscape design includes improving the streetscape with a proposed 3 m wide landscaping bed along Vanguard Road on the subject site and also on the adjacent main business site at 7260 Vanguard Road. The design for the landscaping buffer includes red oak trees, coniferous mugo pine shrubs and deciduous shrub and perennial planting.
- Two (2) two deciduous red oak trees are proposed to be planted on the site, which currently has no existing trees.
- The existing 3 m (10 ft) tall cedar hedge at the front northwest corner of the site will be protected and retained.
- The neighbour's existing 9 m (30 ft) tall cedar hedge across the rear of the site will be protected for retention and supplemented with additional cedar hedge planting along the rear property line on the subject site. The existing hedge is protected by an existing 3 m (10 ft) wide utilities right-of-way, restricting ground disturbance and fill in this area.
- The existing fencing along the north edge and rear of the subject site will be retained.

Conclusions

The project will provide a modest storage facility for an existing industrial business that is designed to buffer the neighbouring residential home from the view and noise of industrial uses. It also proposes to provide tree and shrub planting to improve the Vanguard Road streetscape. Based on the proposal's design response to the objectives of the OCP and general compliance with the site's existing IR1 zoning, staff support the proposed development.



Sara Badyal
Planner 2

SB:rg

The following are to be met prior to forwarding this application to Council for approval:

- Registration of a cross-access easement on Title of 4260 Vanguard Road ensuring access to/from Vanguard Road in favour of 4220 Vanguard Road;
- Registration of a flood covenant (Area A);
- Registration of an aircraft noise sensitive use restrictive covenant (Area 3); and
- Receipt of a Letter-of-Credit for landscaping in the amount of \$13,018.01.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).
- If applicable, payment of latecomer agreement charges associated with eligible latecomer works.

Attachment 1: Development Application Data Sheet



DP 14-662568

Attachment 1

Address: 4220 Vanguard Road

Applicant: 616147 BC Ltd.

Owner: 616147 BC Ltd.

Planning Area(s): East Cambie

| | Existing | Proposed |
|------------------------|---------------------------|--|
| Site Area: | 1226.1 m ² | No Change |
| Land Uses: | Vacant | Industrial |
| OCP Designation: | Mixed Employment | Complies |
| Area Plan Designation: | Industrial | Complies |
| Zoning: | "Industrial Retail (IR1)" | Complies |
| Number of Units: | 0 | 283 m ² (3,050 ft ²) Warehouse with Mezzanine |

| | Bylaw Requirement | Proposed | Variance |
|----------------------------|-------------------|----------|----------------|
| Floor Area Ratio | Max 1.0 | 0.23 | None Permitted |
| Lot Coverage | Max. 60% | 18% | None |
| Setbacks: | | | |
| Vanguard Road | Min. 3 m | 42.7 m | |
| Side Yard (North) | Min. 0 m | 2.4 m | None |
| Side Yard (South) | Min. 0 m | 3 m | |
| Rear Yard | Min. 0 m | 3 m | |
| Height (m): | Max. 12 m | 7.8 m | None |
| Off-street Parking Spaces: | | | |
| Industrial | 3 | 3 | |
| Accessible | (1) | (1) | None |
| Total | 3 | 3 | |
| Tandem Parking Spaces | Not permitted | 0 | None |



City of Richmond

Development Permit

No. DP 14-662568

To the Holder: 616147 BC LTD.
Property Address: 4220 VANGUARD ROAD
Address: C/O WENDY ANDREWS
THE ANDREWS ARCHITECTS INC.
9171 CHAPMOND CRESCENT
RICHMOND, BC V7E 1M6

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #6 attached hereto.
4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$13,018.01 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit

No. DP 14-662568

To the Holder: 616147 BC LTD.
Property Address: 4220 VANGUARD ROAD
Address: C/O WENDY ANDREWS
THE ANDREWS ARCHITECTS INC.
9171 CHAPMOND CRESCENT
RICHMOND, BC V7E 1M6

7. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

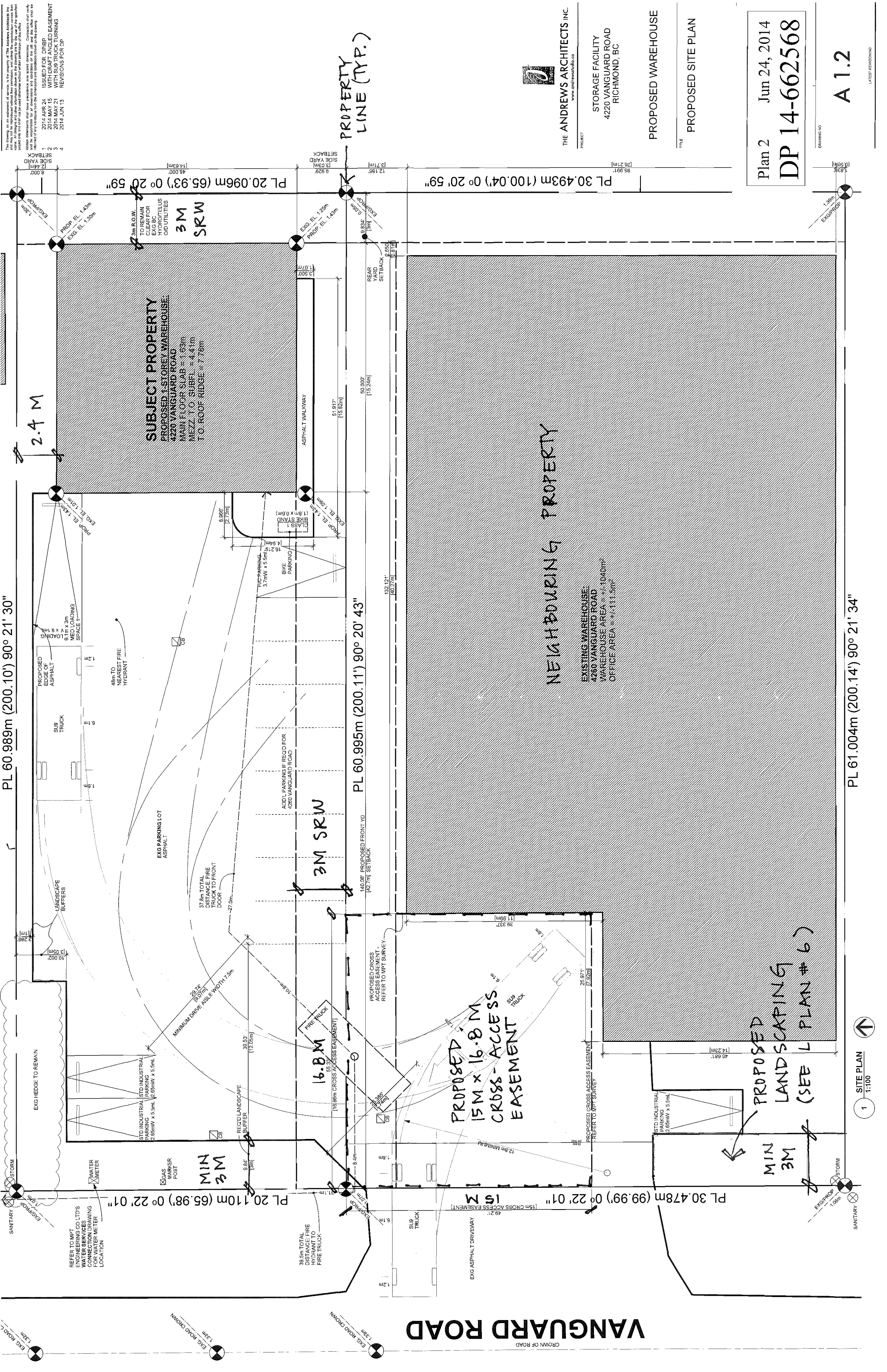
This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



The drawing is an extension of services to the property of The Andrews Architects Inc. and may not be reproduced without their permission, and unless the reproduction carries their name, it shall not be used otherwise without written permission of this office.

Noted: Dimensions shall have precedence over scaled dimensions. Contractors shall verify all dimensions and locations of all structures and utilities on the site prior to construction. If any variations from the dimensions and conditions shown on the drawing are required, they shall be referred to the architect for approval.

1 2014 APR 24 ISSUED FOR DP/DP
2 2014 MAY 15 WITH DRAFT ANGLED EASEMENT
3 2014 MAY 21 WITH SUB TRUCK TURNING
4 2014 JUN 13 REVISIONS FOR DP

PROJECT

THE ANDREWS ARCHITECTS INC.
www.andrewsarchitects.ca

STORAGE FACILITY
4220 VANGUARD ROAD
RICHMOND, BC

PROPOSED WAREHOUSE

TITLE

PROPOSED SITE PLAN

Plan 2 Jun 24, 2014
DP 14-662568

DRAWING NO.

A 1.2

LATEST REVISION NO.

| | | |
|---|-------------|----------------------------|
| 1 | 2014 APR 24 | ISSUED FOR DP/BP |
| 2 | 2014 MAY 15 | WITH DRAFT ANGLED EASEMENT |
| 3 | 2014 MAY 21 | WITH SUG TRUCK TURNING |
| 4 | 2014 JUN 13 | REVISIONS FOR DP |

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|---|-------------|----------------------------|
| 1 | 2014 APR 24 | ISSUED FOR DP/BP |
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| 3 | 2014 MAY 21 | WITH SUG TRUCK TURNING |
| 4 | 2014 JUN 13 | REVISIONS FOR DP |

PROJECT

TITLE

DRAWING NO.

LATEST REVISION NO.

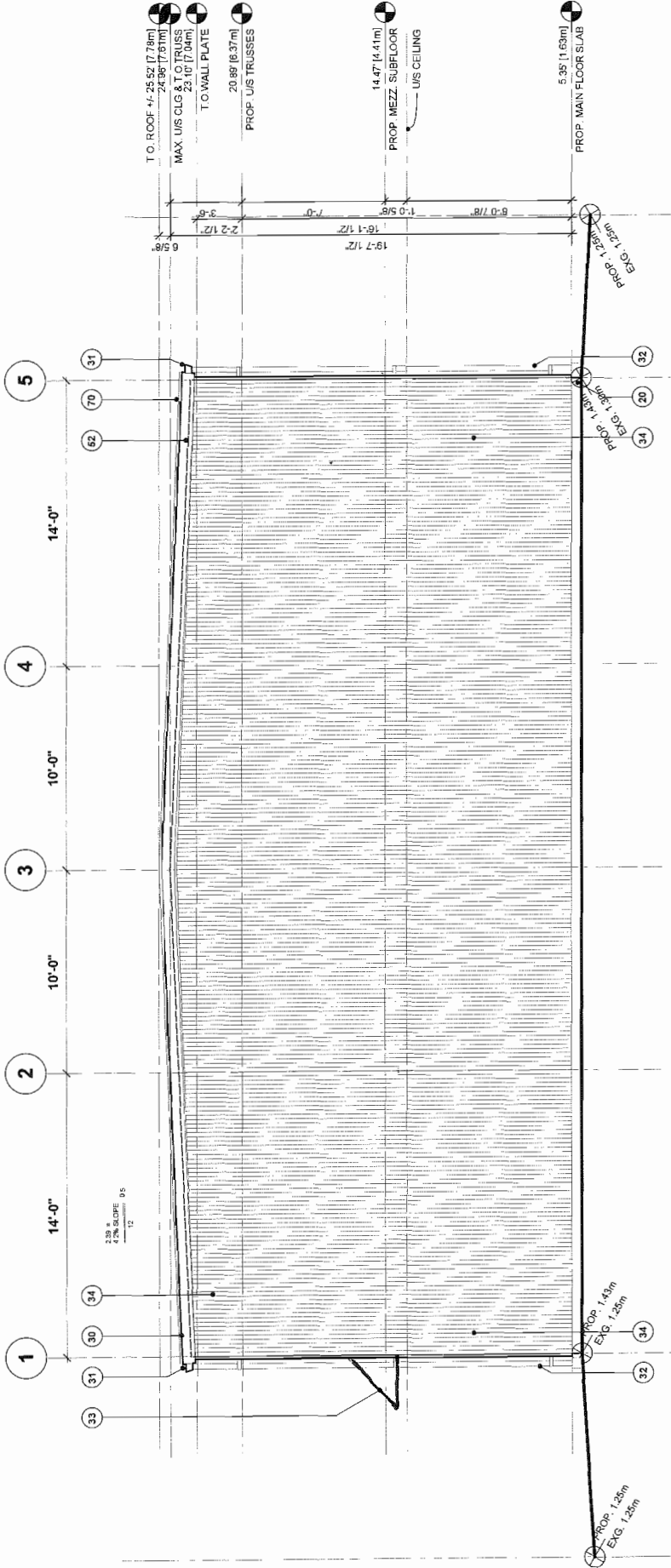
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- 1
- 2014 APR 24
- ISSUED FOR DBBP

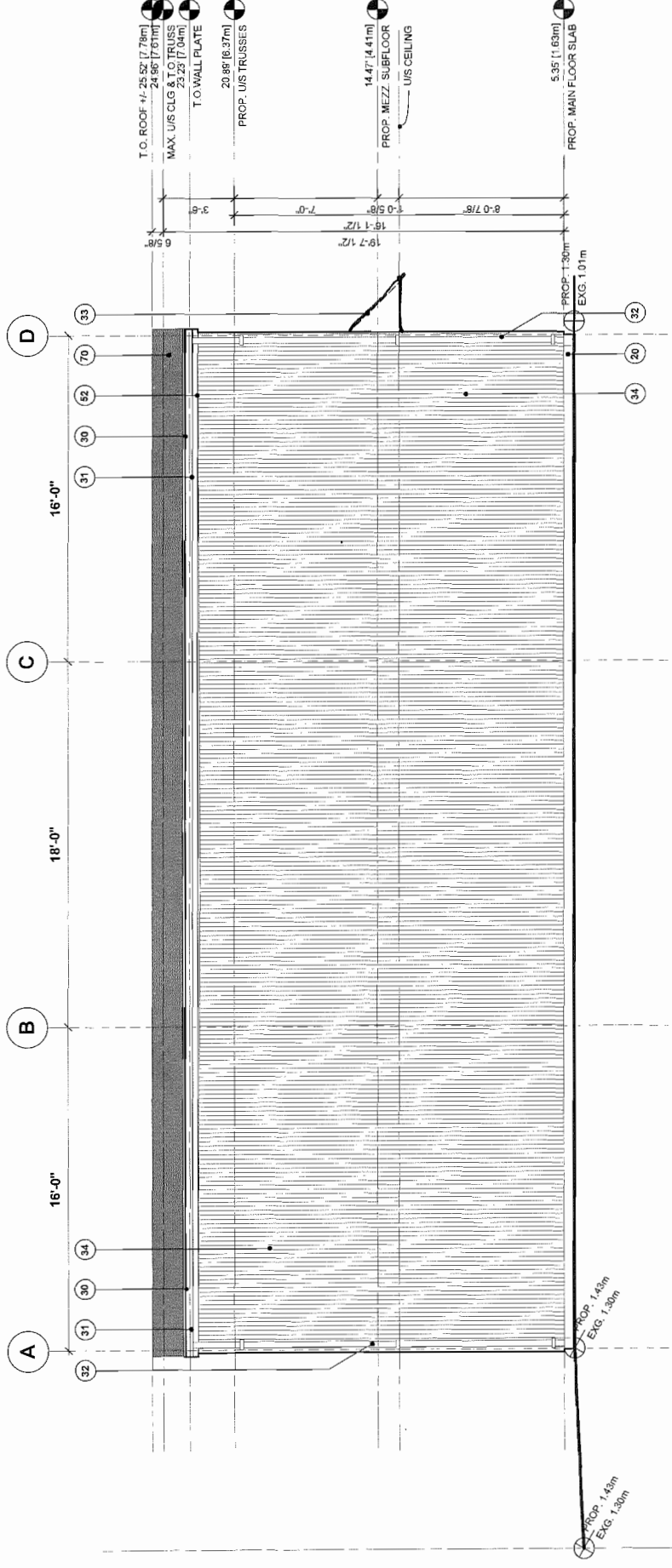
- 2
- 2014 MAY 13
- WITH PROPOSED EASEMENT

- 3
- 2014 MAY 21
- WITH SUB TRUCK TURNING

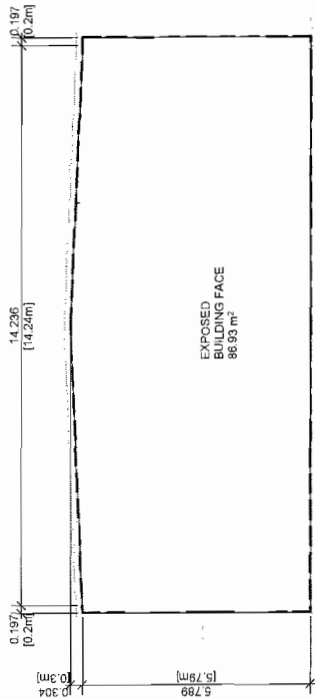
- 4
- 2014 JUN 13
- REVISIONS FOR DP



1 REAR (EAST) ELEVATION
1/4" = 1'-0"



2 NORTH ELEVATION
1/4" = 1'-0"

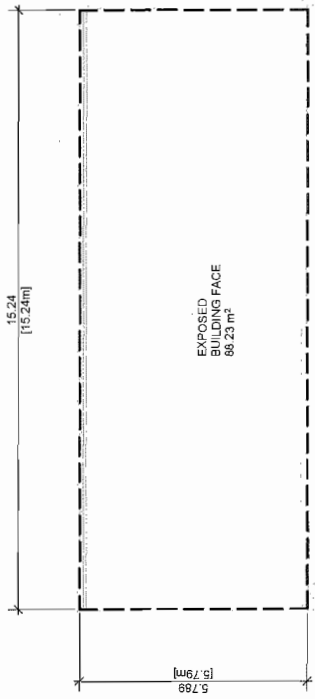


LIMITING DISTANCE: 3.05 m
EXPOSING BLDG FACE: 86.83 m²
UNPROTECTED OPENINGS: 0.00 m²
PERCENTAGE: 0.00%

3 EAST ELEVATION EXPOSING BLDG FACE
1/8" = 1'-0"

EXTERIOR MATERIALS LEGEND:

| | | | |
|----|---|----|---|
| 20 | NEW CONCRETE FOUNDATION | 50 | HORIZ. REVEL SIDING WITH 4" EXPOSURE - PREFINISHED (COLOUR TBC) - HARDIE BD OR APPROVED ALTERNATE |
| 30 | PREFINISHED CAP FLASHING | 60 | NEW EXTERIOR GLAZED DOOR - THERMALLY BROKEN, LOW E, SAFETY GLASS FIBREGLASS |
| 31 | PREFINISHED ALUM. GUTTER | 61 | NEW EXTERIOR SOLID CORE METAL DOOR |
| 32 | PREFINISHED ROUND ALUMINUM DOWNSPOUT CANOPY ON ALUM. FRAME SHOP DVGS REQ'D | 62 | METAL WINDOW - INSULATED ALUMINUM, 320T SERIES OR APPROVED ALTERNATE SHOP DVGS REQ'D |
| 33 | DOWNSPOUT CANOPY ON ALUM. FRAME SHOP DVGS REQ'D | 70 | 2 PLY SBS ROOFING |
| 34 | CORRUGATED ALUMINUM SIDING - PREFINISHED (COLOUR TBC) - 7/8" THK PBC TYP. METAL WALL - APPROVED ALTERNATE | | |
| 35 | METAL FLASHING FLASHING / TRIM | | |
| 40 | DOUBLE GLAZED THERMALLY BROKEN LOWE, ANGLICIZED ALUMINUM WINDOW - METRO ALUMINUM, 320T SERIES OR APPROVED ALTERNATE SHOP DVGS REQ'D | | |



LIMITING DISTANCE: 2.44 m
EXPOSING BLDG FACE: 86.23 m²
UNPROTECTED OPENINGS: 0.00 m²
PERCENTAGE: 0.00%

4 NORTH ELEVATION EXPOSING BLDG FACE
1/8" = 1'-0"



THE ANDREWS ARCHITECTS INC.
www.andrewsarchitects.ca

PROJECT

STORAGE FACILITY
4220 VANGUARD ROAD
RICHMOND, BC

PROPOSED WAREHOUSE

TITLE
EAST ELEVATION
NORTH ELEVATION

Plan 5 Jun 24, 2014
DP 14-662568

1-11-14

DRAWING NO

A 3.2

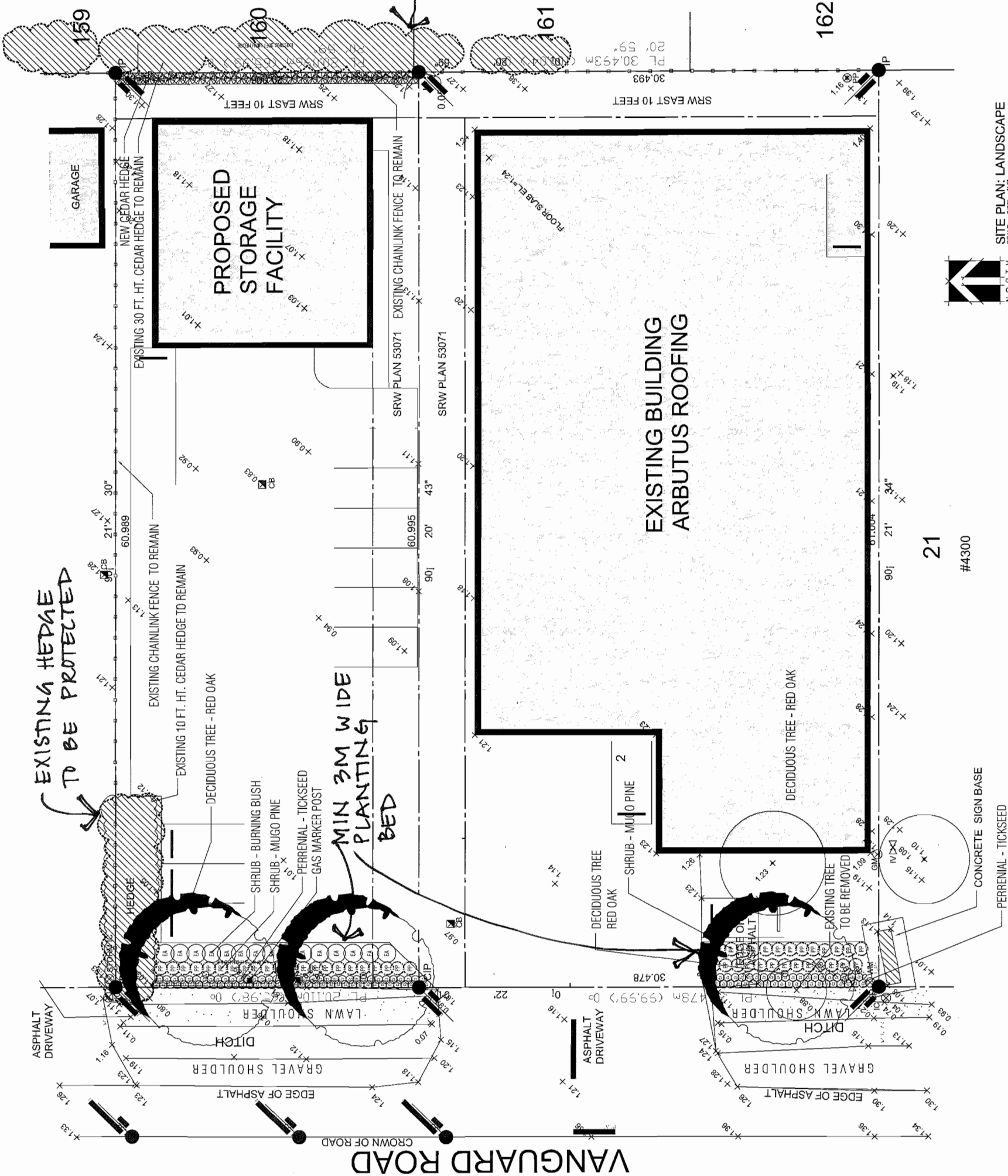
LATEST REVISION NO

- GENERAL LANDSCAPE NOTES:
1. ALL LANDSCAPE MATERIAL SHALL MEET DEFENDED STANDARDS REQUIRED BY DONTA OR BICLA GUIDELINES.
 2. TOPSOIL SHALL BE 12" (300MM) DEEP AND 100% COMPOST. ALL AREAS WHERE TOPSOIL WILL BE REQUIRED AT THE CONTRACTORS EXPENSE. SUBMIT TO LANDSCAPE CONSULTANT FOR APPROVAL. TOPSOIL SHALL BE REMOVED OFF SITE IMMEDIATELY AT THE LANDSCAPE CONTRACTORS EXPENSE.
 3. BE REMOVED OFF SITE IMMEDIATELY AT THE LANDSCAPE CONTRACTORS EXPENSE.
 4. TOPSOIL DEPTHS FOR PLANTING AS FOLLOWS:
 - A. GRASSED AREAS: 12" (300MM)
 - B. GRASS COVERS: 12" (300MM)
 - C. SHRUBS: 18" (450MM)
 - D. TREE PITS: 36" (900MM) WITH 12" (300MM) BELOW FOOT BALL.
 5. LAWN AREAS SHALL BE SODDED WITH #1 PREMIUM RESIDENTIAL SOD NON-MESHED.
 6. 2" DEPTH OF 1" MINUS COMPOST MULCH TO BE INSTALLED IN ALL SHRUB PLANTING AREAS.
 9. ROAD GRADING AND OVERALL SITE GRADING BY CIVIL ENGINEERING/ON ARCHITECTURAL.

PLANT LIST:

| KEY | QTY | BOTANICAL NAME | COMMON NAME | SIZE | COND. |
|-----|-----|-------------------------------|---------------------|-----------|-----------|
| OPD | 3 | QUERCUS RUBRA | RED OAK | 50 CM CAL | B & B |
| TDC | 30 | THILIA OCCIDENTALIS SMARAGO | EMERALD GREEN CEDAR | 2.0 M HT. | B & B |
| EA | 15 | EUDYMYAS ALATUS COMPACTUS | DWARF BURNING BUSH | #3 | CONTAINER |
| PP | 46 | PNUS MUGO PAMULO | DWARF MUGO PINE | #3 | CONTAINER |
| CZ | 118 | CORYDORUS VERTICILLATA ZAGREB | ZAGREB TICKSEED | #1 | CONTAINER |

NEIGHBORING
HEDGE TO BE
PROTECTED



SITE PLAN: LANDSCAPE
SCALE: 3/32" = 1'-0"

21

#4300



PROJECT

STORAGE FACILITY

4220 VANGUARD ROAD, RICHMOND, B.C.

ARCHITECT: THE ANDREWS ARCHITECTS INC.

RSD MANUYAMA & ASSOCIATES 680 C. LEO IN BOAT SQUARE, VANCOUVER, B.C. V6Z 4B4. PH: (604) 474-4997 FX: (604) 474-9931 E: info@manuyama.com

REVISIONS/ISSUED
1. JUNE 27/2014 REVISION FOR LOT 490.
2. JUNE 27/2014 REVISION FOR LOT 490.

SHEET TITLE

LANDSCAPE PLAN

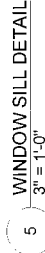
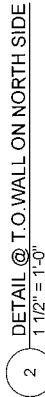
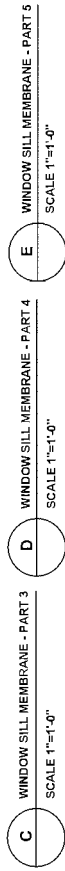
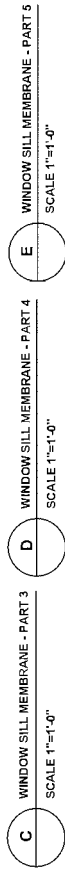
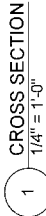
SHEET NO.

L-1

Plan 6 Jun 24, 2014
DP 14-662568

| | | |
|---|-------------|----------------------------|
| 1 | 2014 APR 24 | ISSUED FOR DP/BP |
| 2 | 2014 MAY 15 | WITH DRAFT ANGLED EASEMENT |
| 3 | 2014 MAY 21 | WITH SU9 TRUCK TURNING |
| 4 | 2014 JUN 13 | REVISIONS FOR DP |

- | | | |
|---|-------------|----------------------------|
| 1 | 2014 APR 24 | ISSUED FOR DP/BP |
| 2 | 2014 MAY 15 | WITH DRAFT ANGLED EASEMENT |
| 3 | 2014 MAY 21 | WITH SU9 TRUCK TURNING |
| 4 | 2014 JUN 13 | REVISIONS FOR DP |



PROJECT

STORAGE FACILITY
4220 VANGUARD ROAD
RICHMOND, BC

PROPOSED WAREHOUSE

TITLE

87-117

Reference Plan Jun 24, 2014
DP 14-662568

A.4.1

LATEST REVISION NO.



City of Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: June 20, 2014

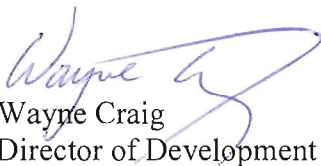
From: Wayne Craig
Director of Development

File: DP 14-657502

Re: **Application by Kasian Architecture Interior Design and Planning Ltd. for a
Development Permit at 11380 Steveston Hwy**

Staff Recommendation

That a Development Permit be issued which would permit the construction of an 882 m² addition and exterior renovation to the building at 11380 Steveston Hwy on a site zoned "Industrial Community Commercial (ZC6) – Ironwood Area;"


Wayne Craig
Director of Development

WC:cl
Att.

Staff Report

Origin

Kasian Architecture Interior Design and Planning Ltd has applied, on behalf of Westbank/Coppersmith Corner Shopping Centre Inc, to the City of Richmond for permission to construct an 882 m² addition to and exterior renovation of an existing building located in the southwest corner of the site at 11380 Steveston Hwy (see key map in Attachment 1). The site is zoned "Industrial Community Commercial (ZC6) – Ironwood Area." There is currently a neighbourhood shopping centre on the site (constructed in 2000), which consists of five (5) buildings containing a variety of commercial retail shops and services that meet the needs of surrounding residents (e.g. Canadian Tire, Mark's Work Wearhouse, TD Bank, Tim Horton's, Coppersmith Dental etc.).

The work proposed as part of this Development Permit application includes:

- renovation of and addition to the southernmost commercial retail unit (CRU 1A) to provide additional space for the on-site relocation of an existing mall tenant (Mark's Work Wearhouse);
- renovation of the remainder of the building facades of CRUs 1 & 2 to introduce a more contemporary design typology that will both begin the process of updating the mall and blend with the new addition; and
- upgrading of the site landscaping and public realm adjacent to this building to enhance the public experience.

The proposed work is intended to be undertaken in two (2) phases:

- Phase 1 (2014-2015) - the addition of building "CRU 1B," interior renovation to a portion of the existing building ("CRU 1A"), implementation of improved site landscaping adjacent to the proposed expansion, and development of a pedestrian plaza north of the proposed expansion in the southwest corner of the site; and
- Phase 2 (2015-2016) – facade upgrades to the remaining portion of the existing building "CRU 1 & 2."

The applicant has also provided a plan for implementing the proposed contemporary design typology on the remaining four (4) building on the site, as well as improvements to the remainder of the site landscaping and public realm for the remainder of the site area. This would be undertaken as part of future Development Permit applications in 2016 to 2019 (Attachment 2).

Development Information

Please refer to attached Development Application Data Sheet (Attachment 3) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

The subject site is located in an established key commercial area that supports a wide range of employment, daily shopping, and personal services to meet the needs of surrounding residents. Specifically, development surrounding the subject site is as follows:

- To the north, immediately across Steveston Hwy, is a mix of single-family housing and townhomes on sites zoned “Single Detached (RS1/E) and “Low-Density Townhouses (RTL3)”;
- To the east, immediately across Coppersmith Place, is an existing neighbourhood shopping centre at 11688 Steveston Hwy, which contains a Save-On-Foods, London Drugs, Ironwood Branch of the Richmond Public Library etc., on a site that is split-zoned “Community Commercial (CC)” and “Industrial Business Park (IB1);”
- To the south, is an existing mixed commercial/office building at 11331 Coppersmith Way on a site zoned “Industrial Community Commercial (ZC6);” and
- To the west, is an existing Translink bus operations yard fronting Coppersmith Way, on a site that is split-zoned “Light Industrial (IL)” and “Industrial Business Park (IB1).”

Public Input

In response to the notification sign being installed on the subject property, staff received two (2) phone calls and (1) piece of written correspondence about the development proposal (Attachment 4):

- One (1) caller was an existing tenant of the mall, who expressed concerns with how the proposed changes would impact available on-site parking.

As discussed further below, the proposal includes a reconfiguration of the existing drive-aisle and parking layout on-site resulting in an overall increase of one (1) vehicle parking space. The proposed number of vehicle parking spaces on-site complies with the requirements of the Richmond Zoning Bylaw 8500.

- The other call and piece of written correspondence was received from a representative of the adjacent property owner to the south at 11331 Coppersmith Way. The nature of concerns expressed by the adjacent property owner is that the design of the building addition, as proposed:
 - will severely compromise the existing connection between the two properties as well as the visibility to their building and tenants
 - deviates from the existing animated retail corner that currently faces towards their property.
 - results in their property having to face a relatively blank facade and loading services along the south building elevation.

The applicant has reviewed the correspondence and has met with the adjacent property owner to discuss their concerns. The applicant has submitted a written statement indicating how they have revised their design in an attempt to address the concerns expressed (Attachment 5).

Staff has discussed the proposed revisions to the Development Permit application with the adjacent property owner. While the adjacent property owner appreciates the positive changes made to the proposal in an attempt to address the concerns raised, the adjacent property owner would prefer the applicant to move the proposed building addition to the west.

Staff are supportive of the applicant's revised design for the following reasons:

- access is maintained between the two properties, consistent with the design guidelines in the Ironwood Sub-Area Plan.
- it provides improvements to the south and east elevation of the proposed building addition to animate and draw people toward the southwest corner of the site.
- it provides improved landscaping and screening around the proposed garbage and recycling enclosure.

Staff Comments

The proposed scheme attached to this report (Plan # 1.a to # 4.b) has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, the proposal complies with the intent of the applicable sections of the Official Community Plan (OCP) and Shellmont (Ironwood Sub-Area) Plan, and is in compliance with the "Industrial Community Commercial (ZC6) – Ironwood Area" zoning, with no variances requested.

Sign Permit applications will be required for the work proposed, which must comply with the Sign Bylaw No. 5560, and be consistent with the design guidelines for signage in the OCP.

Advisory Design Panel Comments

Because of its relatively minor nature, this application was not presented to the Advisory Design Panel (ADP).

Analysis

Conditions of Adjacency

- Directly to the south of the proposed addition, on an adjacent property, is a retail and office building at 11331 Coppersmith Way that is linked to the subject site through pedestrian and vehicular access. The proposed addition has been sized and located to maintain some visibility toward this building and the proposed parking configuration maintains pedestrian and vehicular access along the south property line. As there are commercial retail units that face from this building toward the subject site, the application has also proposed to animate the new east and south-facing facades with display glass windows, a variety of materials and colours, as well as columnar trees and other soft landscape materials.

Urban Design and Site Planning

Changes to the existing site plan are limited to the area around the proposed addition to building "CRU 1" in the southwest corner of site, as illustrated on Plans # 1.a and 1.b (Phase 1).

Proposed changes include:

- The creation of additional floor area at the south end of the existing building, resulting in an L-shaped form;
- The creation of a pedestrian plaza at the point where the existing building meets the proposed building addition;
- The reconfiguration of the on-site drive-aisle and vehicle parking layout, as well as the removal of two (2) treed planting islands within the surface parking area.

Pedestrian Circulation and Accessibility

Measures are proposed by the applicant as part of this application to promote pedestrian circulation and to make the pathways accessible to persons in a wheelchair. These include:

- A pedestrian pathway along the north and east of the proposed building addition, which connects to the north/south pathway that runs along the storefronts of the existing building (Phase 1). The proposed pathway is consistent with the cross-section guidelines for pedestrian pathways on commercial sites contained in the OCP, which specifies: a minimum 1.5 m wide boulevard with trees in grates next to the curb and a minimum clear 2 m wide patterned paved walkway extending to the face of the building.
- Providing curb letdowns at the ends of the proposed pathways to ensure accessibility (Phase 1).
- Providing a defined pedestrian path from the subject site to the south property line to further enhance the connection to the adjacent property to the south at 11331 Coppersmith Way, consistent with the design guidelines in the Ironwood Sub-Area Plan.
- Adding canopy structures over the proposed new storefront entrance and over a portion of the pathway proposed along the east side of the building addition to provide weather protection for pedestrians (Phase 1).
- Replacing or renovating various sections of the existing canopy and facade over the storefront entrances along the existing portion of the building ("CRU 1 & 2") to modernize the facade expression while maintaining weather protection (Phase 2).

Drive-aisle Reconfiguration, Parking, and Loading

This development proposal results in changes to the existing drive-aisle and parking configuration at the site.

Portions of existing drive-aisles and two (2) treed planting islands within the surface parking area around the expansion area are to be removed to accommodate the proposed building addition and the pedestrian plaza (details on tree replacement are discussed below).

With the proposed site plan, access is maintained between the subject site and the adjacent site to the south at 11331 Coppersmith Way, consistent with the design guideline in the Ironwood Sub-Area Plan.

With the current condition, the Zoning Bylaw requires 397 vehicle parking spaces on-site. Currently, the number of vehicle parking spaces at the subject site exceeds the number of spaces required by the Zoning Bylaw by 54 spaces, for a total of 451 vehicle parking spaces.

With the proposed building addition, the Zoning Bylaw requires 432 vehicle parking spaces on-site. To accommodate the proposed building addition, 44 standard vehicle parking spaces are proposed to be removed. After the existing surplus spaces on-site are accounted for, an additional 25 parking spaces are required to be provided on-site to comply with the Zoning Bylaw.

To achieve compliance with the Zoning Bylaw, the applicant proposes to convert a number of existing standard vehicle parking spaces into small car parking spaces (consistent with the small car parking space provisions in the Zoning Bylaw), to locate additional vehicle parking spaces for employee use along the south side of the Canadian Tire building, and to make minor modifications to some existing conditions on-site. The resulting parking plan includes 432 vehicle parking spaces, which meets the Zoning Bylaw requirements (Plan # 2).

Consistent with the design guidelines in the OCP, the applicant has proposed one (1) standard vehicle parking space equipped with a 240 volt electrical vehicle charging station. Prior to Development Permit issuance, a legal agreement to secure the electrical vehicle charging station is required to be registered on title.

The proposal includes the addition of a loading space to the rear and side of the proposed building addition at the southwest corner of the subject site.

Consistent with the Zoning Bylaw, the applicant has proposed a secured bike storage room within the proposed building addition to accommodate the three (3) required Class 1 bicycle parking spaces, as well as a bike rack to accommodate the four (4) required Class 2 visitor bicycle parking spaces to the northwest of the storefront entrance to the proposed building addition.

The City's Transportation department staff has reviewed the proposed parking plan and indicate that it is acceptable.

Garbage and Recycling

The applicant has proposed to relocate one (1) of the existing garbage and recycling enclosures on-site from its current location at the south end of building "CRU 1 & 2," to the southwest corner of the subject site where pedestrian and vehicle traffic is lowest.

The garbage and recycling enclosure is proposed to be screened with Cedar wood stain fencing, hedging, and plant material along the east and south sides to further conceal it from view.

The proposed garbage and recycling enclosure is sized appropriately to contain the required number of containers (i.e., one 4 cubic yard garbage bin on wheels, one 4 cubic yard cardboard bin on wheels, two 95 gal paper recycling carts, two 95 gal beverage container recycling carts, and one 95 gal organics container).

Architectural Form and Character (Plan # 4.a and # 4.b)

- Proposed changes to the facades of the subject building are intended to begin the process of updating the overall mall with a more contemporary expression. The applicant has suggested undertaking these updates in phases to address the challenges of evolving a variety of existing buildings with multiple existing tenants in a continuously operating developing. Using this approach, there will be contrasting facade vocabularies until all the updating is completed. However, by starting the upgrades the building furthest from the two street frontages, as well as requiring that distinct buildings be upgraded as a whole, the visual impact of the contrast will be minimized.
- The proposed design of the building addition and the facade upgrades will retain the existing pedestrian-scaled retail frontage.
- The proposed height of the building addition is similar to the existing building height as well as the heights of other building in the development. The applicant has provided height variations, consistent with the design guidelines in the OCP, as part of the facade renovations in order to create visual interest and to break up the building massing.
- A variety of cladding materials are proposed for the building addition (Phase 1), including: contrasting metal panels, light and dark charcoal stone, and storefront glazing with charcoal-finished aluminum frame. The use of the varied cladding materials will aid in providing visual interest and in breaking down the facade of the proposed building addition into smaller components.
- As mentioned previously, the south and east elevation of the proposed building addition also includes display glass windows to animate and draw people toward the southwest corner of the site.
- The proposed upgrades to the existing building (Phase 2) aim to maintain building articulation and break up the building into smaller components through:
 - the use of unique aluminum panels that are perforated, folded, tilted, and illuminated for the upper half of the facade, which provide a dynamic effect that will break down the appearance of the facade; and;
 - the removal of the masonry around the columns under the existing canopy to further expose the retail frontage, increase visual interest, and increase the perceived depth of the facade.
- The proposed canopy for the building addition (Phase 1) will feature stained wood veneer soffits to enhance the pedestrian environment while the upgraded metal and glass canopy proposed for the existing building (Phase 2) will feel lighter and enhance visibility.

Landscape Design and Open Space Design

- To accommodate the proposed building addition, two (2) bylaw-sized trees and eight (8) undersized trees located in planting islands within the surface parking area are proposed to be removed (Trees # 2, 4, 5, 6, 7, 8, 10, 11, 12, 13). The City's Tree Preservation Coordinator conducted an on-site visual tree assessment and concurs with the proposal to remove the trees due to their location within the building envelope, and to retain the seven (7) remaining trees

located near the expansion area (Trees # 1, 3, 9, 14, 15, 16, 17). The proposed Tree Retention Plan is included in Attachment 6.

- To ensure survival of the trees to be retained on-site, the applicant is required to:
 - Install tree protection fencing around the driplines of protected trees, in accordance with the City's Tree Protection Bulletin (TREE-03) prior to demolition of the existing structures on-site;
 - Submit a contract with a Certified Arborist prior to issuance of the Development Permit for on-site supervision of all works proposed within tree protection zones. The contract is to include the scope of work to be undertaken, including the proposed number of site monitoring inspections, and provision for the Arborist to submit a post-construction impact assessment report to the City for review;
 - Submit a security in the amount of \$2,000 for Tree # 14. Following completion of construction and landscaping on the subject site, the security will be released subject to receipt of the post-construction impact assessment report prepared by the Arborist and a City landscaping inspection to verify tree survival.
- Consistent with the 2:1 tree replacement ratio guideline in the OCP, the proposed Landscape Plan (Plan # 3.a to #3.e) includes four (4) replacement trees near the proposed building addition to compensate for the removal of the two (2) bylaw-sized trees. The Landscape Plan also provides for an additional eight (8) trees to be planted on-site near the proposed building addition. The proposed tree species list is consistent with the species guidelines contained in the OCP (discussed further below).
- Consistent with the design guidelines in the OCP, the applicant proposes a new 372 m² (4,000 ft²) pedestrian plaza with three (3) long seating elements at the intersection of the existing building and proposed building addition. This plaza is intended to provide a passive outdoor amenity that can be used by both employees and customers of the shopping centre. The plaza is also situated to better define the existing main pedestrian circulation route, which provides a connection between the existing building "CRU 1 & 2" and the Canadian Tire building to the east.
- As mentioned above, the applicant proposes to plant one (1) replacement tree in structural soil in the southwest corner of the plaza, as well as one (1) replacement tree in structural soil and a variety of taller ornamental grasses in the planting area along the north side of the plaza to act as a screen between a portion of the plaza and the adjacent surface parking area (i.e., Skyline Honey Locust tree, a large Upright English Oak tree, Feather Reed Grass, and Dwarf Fountain Grass).
- The surface of the pedestrian plaza is proposed to be treated with charcoal-coloured concrete pavers to provide visual interest and texture, as well as to relate to the colour scheme proposed for the exterior building materials.
- The surface of the pedestrian pathways around the proposed building addition will feature broom-brushed concrete paving to match the pattern of the pathway located alongside the existing building.
- The boulevard next to the pedestrian pathway that runs alongside the east side of the proposed building addition is to contain a row of three (3) replacement trees complete with

decorative tree grates and structural soil (i.e., Columnar Red Maple trees), as well as four (4) small custom benches to match those proposed within the pedestrian plaza.

- The applicant proposes to plant five (5) trees and additional plant material along the south elevation of the proposed building addition to soften the interface between that elevation and the adjacent mixed commercial-office building on the property to the south (i.e., Columnar Red Maple trees, Nootka Rose, and Dwarf Oregon Grape).
- All existing planting islands within the surface parking area surrounding the proposed building addition are proposed to be enhanced with additional plant materials to reinstate the design intent of the landscaping approved with the original Development Permit.

Crime Prevention Through Environmental Design

- The design of the proposed building addition and upgrades to the existing building will improve the visibility around the storefront canopies, making the space feel safer for pedestrians;
- Downlight fixtures are proposed to illuminate pedestrian pathways along the east elevation of the building addition, as well as along the south elevation next to additional surface parking and the existing mixed commercial-office building on the adjacent property to the south.

Sustainability Features

- Consistent with the design guidelines in the OCP, the applicant has proposed a 240 volt electrical vehicle outlet for one (1) standard vehicle parking space located next to the proposed building addition;
- The applicant has confirmed that the proposed building addition is designed to be LEED Silver equivalent, as demonstrated through the LEED Project Checklist in Attachment 7. At Building Permit stage, the applicant is required to provide a letter of assurance from a registered professional that the LEED checklist will be followed.

Conclusion

Kasian Architecture Interior Design and Planning Ltd has applied for a Development Permit to construct an 882 m² addition and exterior renovation to an existing building located in the southwest corner of the subject site at 11380 Steveston Hwy. Proposed alterations are intended to be undertaken in two (2) phases to provide: additional space for an existing tenant on the site (Mark's Work Wearhouse), to introduce a modern design typology for the site, and to upgrade the site landscaping and public realm environment adjacent to the building expansion area.

The applicant has addressed the significant urban design issues and other staff comments identified as part of the application review, including meeting the architectural form and character guidelines contained within the OCP, and providing landscaping and public amenity enhancements to the site to achieve the objectives of the design guidelines of the Ironwood Sub-Area Plan.

The proposal complies with the "Industrial Commercial (ZC6) – Ironwood Area" zoning, with no variances requested.

On this basis, staff recommends support for issuance of this Development Permit application.



Cynthia Lussier
Planning Technician
CL:rg

- Attachment 1: Key Plan
- Attachment 2: Letter of Intent/Implementation Plan
- Attachment 3: Development Application Data Sheet
- Attachment 4: Written correspondence from property owner at 11331 Coppersmith Way
- Attachment 5: Applicant's written response to concerns expressed by property owner at 11331 Coppersmith Way
- Attachment 6: Proposed Tree Retention Plan
- Attachment 7: LEED Project Checklist

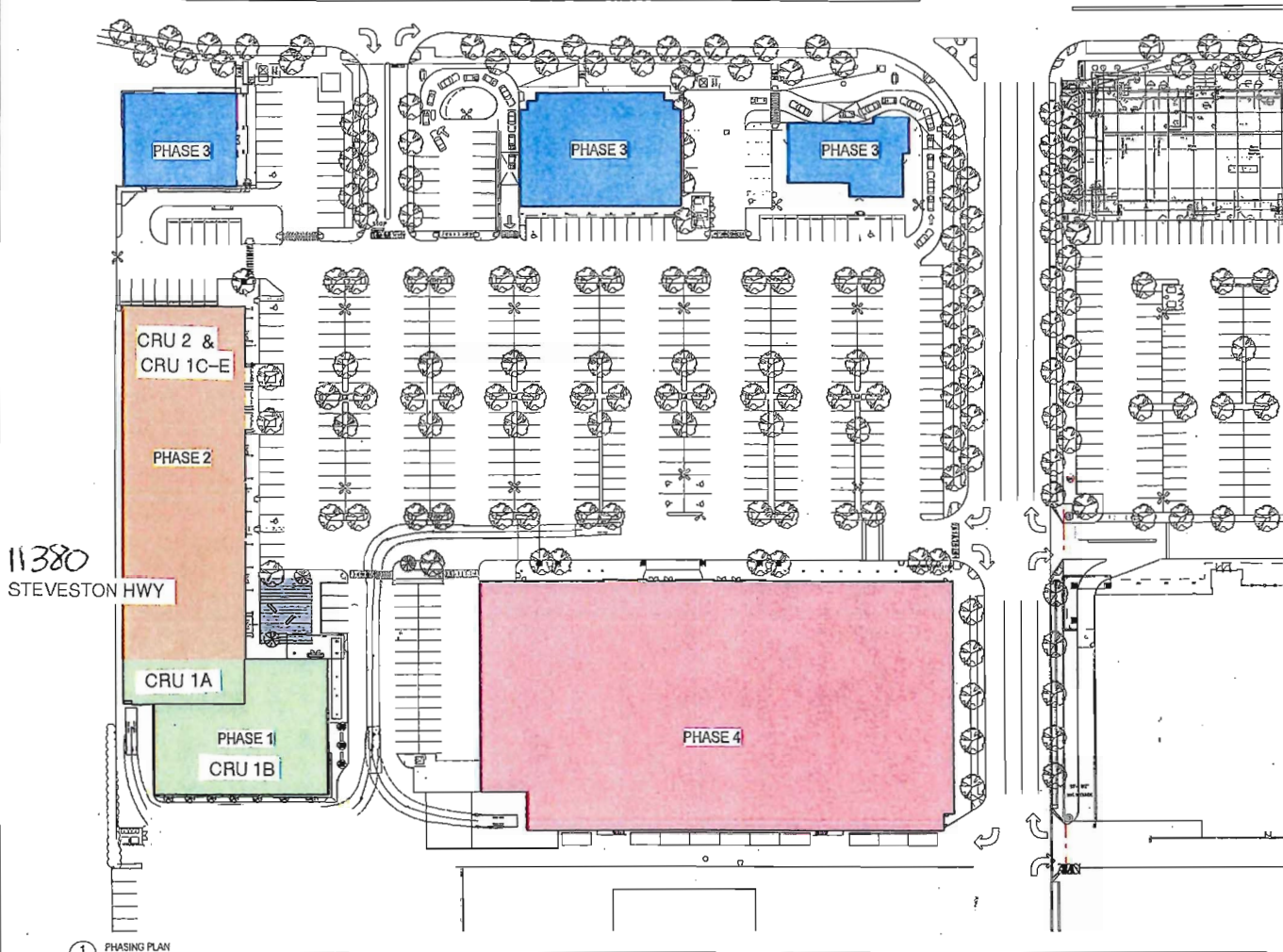
The following is to be met prior to forwarding this application to Council for approval:

1. Receipt of a landscaping security in the amount of \$112,801.92, equal to the cost estimate provided by the Registered Landscape Architect.
2. Receipt of a survey of the entire property legally described as Lot A Section 1 Block 3 North Range 6 West NWD Plan LMP45518, prepared by a registered BC Land Surveyor, in accordance with the City's Survey Guidelines Bulletin (DEVAPPS-01).
3. Submission of a contract with a Certified Arborist for on-site supervision of all works proposed within Tree Protection Zones of Trees # 1, 3, 9, 14, 15, 16, 17. The contract is to include the scope of work to be undertaken, including the proposed number of site monitoring inspections, and provision for the Arborist to submit a post-construction impact assessment report to the City for review.
4. Submission of a security in the amount of \$2,000 for Tree # 14. Following completion of construction and landscaping on the subject site, the security will be released subject to receipt of the post-construction impact assessment report prepared by the Arborist and a City landscaping inspection to verify tree survival.
5. Registration of a legal agreement to secure the proposed electrical vehicle charging station on the subject site.

Prior to future Building Permit issuance, the developer is required to:

- Provide a letter of assurance from a registered professional confirming that the LEED Checklist will be followed.
- Obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submit a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).

SITE ADDRESS 11380 STEVESTON HWY



1 PHASING PLAN
SCALE: 1:400

westbank

SCALE: 1:400

REISSUED FOR DEVELOPMENT
PERMIT

PHASING PLAN
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-03
JUNE 24, 2014
PROJECT 160046

westbank

Ms. Cynthia Lussier
Planning Technician, Development Applications Division
City of Richmond
6911 No. 3 Road
Richmond BC V6Y 2C1

VIA EMAIL: clussier@richmond.ca

Re: Development Permit Application - 11380 Stevenston Highway (Coppersmith Corner Shopping Centre)

Dear Cynthia,

Following our meeting on June 6, 2014, the purpose of this letter is to provide clarity on Westbank's overall vision and timeline for the Coppersmith Corner Shopping Centre upgrade.

As noted in the development permit application, the proposed retail development is an approximate 880 sq.m addition and an approximate 250 sq.m renovation to an existing commercial retail unit located at the southwest portion of the centre. The new addition is designed to accommodate the growing needs of the community and the space requirements of our anchor tenant, Mark's Work Warehouse. Westbank also considers the potential expansion as an opportunity to introduce a new, unified modern design typology for the existing buildings, and to upgrade the site landscaping and public realm environment.

The proposed upgrade is envisioned in four phases, which will be implemented over a period of approximately five years:

Phase 1 (2014 – 2015, to be undertaken through the proposed DP 14-657502)

- Expansion/renovation to accommodate Mark's
- Implementation of improved site landscaping, pedestrian connections and weather protection adjacent to the expansion
- Development of a pedestrian plaza in front of Mark's

Phase 2 (2015 – 2016, to be undertaken through the proposed DP 14-657502)

- Façade upgrades to remaining frontage of CRU Building 1 and CRU Building 2
- Implementation of improved site landscaping, pedestrian connections and weather

Westbank Projects Corp.
501 - 1067 West Cordova Street
Vancouver, Canada V6C 1C7

604 685 8986 general
604 893 1708 fax
www.westbankcorp.com

westbank

protection adjacent to the buildings

Phase 3 (2016 – 2017, to be undertaken as part of a future DP application)

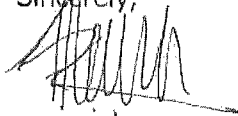
- Façade upgrades to CRU Building 3, CRU Building 4 and CRU Building 5
- Implementation of improved site landscaping, new plaza adjacent to a restaurant space, improved pedestrian connections and new weather protection adjacent to the buildings

Phase 4 (2018 – 2019, to be undertaken as part of a future DP application)

- Façade upgrade to Canadian Tire building
- Implementation of improved site landscaping, pedestrian connections and weather protection adjacent to the building
- Implementation of improved site landscaping to the surface parking lot

Should you have any questions or concerns, please do not hesitate to contact me.

Sincerely,



Farouk Babul
WESTBANK PROJECTS CORP.



DP 14-657502

Attachment 3

Address: 11380 Steveston Hwy

Kasian Architecture Interior

Applicant: Design and Planning Ltd.

Coppersmith Corner Shopping

Owner: Centre Inc.

Planning Area(s): Shellmont

Floor Area Gross: 10,682 m²

| | Existing | Proposed |
|-------------------------|---|-----------------------|
| Site Area: | 31,538 m ² | 31,538 m ² |
| Land Uses: | Neighbourhood shopping centre | No change |
| OCP Designation: | <ul style="list-style-type: none"> • 2041 OCP Land Use Map designation - Mixed Employment • Ironwood Sub-Area Plan Land Use Map designation – Area A (Commercial Development) | No change |
| Zoning: | Industrial Community Commercial (ZC6) – Ironwood Area | No change |

| | Bylaw Requirement | Proposed | Variance |
|--|-------------------|---|----------------|
| Floor Area Ratio: | 60% | 33.87% | none permitted |
| Lot Coverage: | Max. 50% | 31% | none |
| Setback – Front Yard (north): | Min. 6.0 m | Existing – 6.0 m Proposed - no change | none |
| Setback – Exterior Side Yard (east): | Min. 6.0 m | Existing – 6.0 m Proposed - no change | none |
| Setback – Interior Side Yard (west): | Min. 0 m | Existing - approx 0 m Proposed - no change | none |
| Setback – Rear Yard (south): | Min. 0 m | Existing - approx 6.0 m Proposed - no change | none |
| Height (m): | Max. 12.0 m | Parapet – 7.7 m | none |
| Minimum Lot Size: | N/A | N/A | none |
| On-Site Vehicle Parking Spaces: | | | |
| Standard size (50%) | 216 | 131 | none |
| Small size (50%) | 216 | 294 | |
| Accessible | 9 | 10 | |
| Total: | 432 | 435 | none |
| On-Site Bicycle Parking Spaces: | | | |
| Class 1 | 3 | 3 | none |
| Class 2 | 4 | 4 | |



June 17, 2014

Cynthia Lussier
City of Richmond
Planning & Development
6911 No.3 Road
Richmond, BC
V6Y 2C1

Porte Realty Ltd.
380 -1665 West Broadway
Vancouver BC V6J 1X1
t 604.732.7651
f 604.732.4673
porte.ca

Dear Ms. Lussier:

Re: Westbank
File No: DP14-657502

We are the owners of 11331 Coppersmith Way. When we purchased the property 10 years ago, the expectation and agreement was always that our property and the adjacent centre would function together as one.

The purpose of the cross access was to permit the flow of traffic, both vehicular and pedestrian, between the two properties and between Coppersmith Way and Steveston Highway.

The location, height, siting and bulk of the proposed addition to the shopping centre severely compromises the physical access, the parking and the visibility/connection between the properties and centres. Furthermore, it will result in a blank side of the building, with garbage and loading at the location of the cross access, facing our building.

As originally designed and built, the retail space "turned the corner", providing an animated retail corner facing towards our building.

We are supportive of an expansion of the centre and increasing the customer traffic at the south end of the centre.

We are opposed to the design as presented and urge Planning to work with the Developer to prepare a pedestrian friendly design which animates the south end of their centre and, more importantly, lives up to the intent and spirit of the agreement that was in place with the original development of the shopping centre and our retail/office building.

Sincerely,

PORTE REALTY LTD.

A handwritten signature in black ink, appearing to read "David Porte", is written over a horizontal line.

per: David Porte

DP:el

RECEIVED
JUN 23 2014



July 3rd, 2014

Cynthia Lussier,
Development Applications Division
City of Richmond

Dear Cynthia:

**Re: Development Permit Application with respect to property located at Unit 11380
Steveston Hwy (Marks Renovation & Addition)**

In response to the letter we received on 24th June 2014 from David Porte of Porte Reality Ltd, Farouk Babel of Westbank and Andrew Gordon of Kasian met with Daniel Bar-Dayana and David Porte at Porte Reality Ltd on the 25th June 2014 to further discuss their concerns.

Primary concerns appear to relate to the location of the proposed building on the site, the vehicular circulation through 11,380 Steveston Hwy to 11,331 Coppersmith Way and the animation of the south elevation.

We considered moving the building to the west and discovered this would remove the opportunity to create a pedestrian plaza to the north of the store. We have explored the possibility of relocating the existing drive aisle between 11,380 Steveston Hwy and 11,331 Coppersmith Way, to the east edge of the proposed Mark Work Warehouse building. However transport engineers at the City of Richmond have deemed the access to the loading bay would be unsatisfactory.

We propose:

- Providing a pedestrian walk way to the south property line that will enhance the connection to 11,331 Coppersmith Way as recommended in the OCP.
- Providing display glass windows to the east and south elevation to animate this corner of the site.

- Rotate the proposed garbage enclosure so it is less visible and plant a 6' hedge along the east and south edge of the enclosure. This will entirely screen the garbage.
- Although not included as part of the DP drawing submission, we are requesting permission to install an additional pylon sign on Steveston Hwy adjacent to the existing TD Bank. Should the City be amenable to this request, we would offer one space for use by the neighbouring site for their signage.

The addition of these items to what we feel is a carefully considered addition to Coppersmith Mall, will be to the benefit of the local community.

Kind Regards,

Andrew Gordon
EDAC, BArch (Hons), BSc

Project Manager
T (604) 631-4535

nk

LEED Canada-CS 2009 Project Checklist

Ironwood CRUs 1 & 2, Richmond BC

| Yes | ? | No | Project Totals (pre-certification estimates) | | 110 Possible Points |
|-----|----|----|---|--|---------------------|
| 56 | 14 | 40 | Certified 40-49 points Silver 50-59 points Gold 60-79 points Platinum 80 points and above | | |
| | | | | | |
| Yes | ? | No | Sustainable Sites | | 28 Points |
| ✓ | | | Prereq 1 | Construction Activity Pollution Prevention | Required |
| 1 | | | Credit 1 | Site Selection | 1 |
| 3 | 2 | | Credit 2 | Development Density and Community Connectivity | 3.5 |
| | 1 | | Credit 3 | Brownfield Redevelopment | 1 |
| 6 | | | Credit 4.1 | Alternative Transportation: Public Transportation Access | 3.6 |
| | 2 | | Credit 4.2 | Alternative Transportation: Bicycle Storage & Changing Rooms | 2 |
| 3 | | | Credit 4.3 | Alternative Transportation: Low-Emitting & Fuel-Efficient Vehicles | 3 |
| | 2 | | Credit 4.4 | Alternative Transportation: Parking Capacity | 2 |
| | 1 | | Credit 5.1 | Site Development: Protect and Restore habitat | 1 |
| | 1 | | Credit 5.2 | Site Development: Maximize Open Space | 1 |
| 1 | | | Credit 6.1 | Stormwater Design: Quantity Control | 1 |
| | 1 | | Credit 6.2 | Stormwater Design: Quality Control | 1 |
| | 1 | | Credit 7.1 | Heat Island Effect: Non-Roof | 1 |
| 1 | | | Credit 7.2 | Heat Island Effect: Roof | 1 |
| | 1 | | Credit 8 | Light Pollution Reduction | 1 |
| | 1 | | Credit 9 | Tenant Design and Construction Guidelines | 1 |
| | | | | | |
| Yes | ? | No | Water Efficiency | | 10 Points |
| ✓ | | | Prereq 1 | Water Use Reduction | Required |
| 2 | 2 | | Credit 1 | Water Efficient Landscaping | 2.4 |
| 2 | | | Credit 2 | Innovative Wastewater Technologies | 2 |
| | 2 | | Credit 3 | Water Use Reduction | 2-4 |
| | | | | | |
| Yes | ? | No | Energy & Atmosphere | | 37 Points |
| ✓ | | | Prereq 1 | Fundamental Commissioning of Building Energy Systems | Required |
| ✓ | | | Prereq 2 | Minimum Energy Performance | Required |
| ✓ | | | Prereq 3 | Fundamental Refrigerant Management | Required |
| 6 | 1 | 14 | Credit 1 | Optimize Energy Performance | 3-21 |
| | 4 | | Credit 2 | On-Site Renewable Energy | 2.4 |
| | 2 | | Credit 3 | Enhanced Commissioning | 2 |
| | 2 | | Credit 4 | Enhanced Refrigerant Management | 2 |
| 3 | | | Credit 5.1 | Measurement and Verification: Base Building | 3 |
| 3 | | | Credit 5.2 | Measurement and Verification: Tenant Submetering | 3 |
| | 2 | | Credit 6 | Green Power | 2 |
| | | | | | |
| Yes | ? | No | Materials & Resources | | 13 Points |
| ✓ | | | Prereq 1 | Storage and Collection of Recyclables | Required |
| 5 | | | Credit 1 | Building Reuse: Maintain Existing Walls, Floors, and Roof | 1-5 |
| 2 | | | Credit 2 | Construction Waste Management | 1-2 |
| 1 | | | Credit 3 | Materials Reuse | 1 |
| 1 | 1 | | Credit 4 | Recycled Content | 1-2 |
| 1 | 1 | | Credit 5 | Regional Materials | 1-2 |
| 1 | | | Credit 6 | Certified Wood | 1 |
| | | | | | |
| Yes | ? | No | Indoor Environmental Quality | | 12 Points |
| ✓ | | | Prereq 1 | Minimum Indoor Air Quality Performance | Required |
| ✓ | | | Prereq 2 | Environmental Tobacco Smoke (ETS) Control | Required |
| 1 | | | Credit 1 | Outdoor Air Delivery Monitoring | 1 |
| 1 | | | Credit 2 | Increased Ventilation | 1 |
| 1 | | | Credit 3 | Construction IAQ Management Plan: During Construction | 1 |
| 1 | | | Credit 4.1 | Low-Emitting Materials: Adhesives and Sealants | 1 |
| 1 | | | Credit 4.2 | Low-Emitting Materials: Paints and Coatings | 1 |
| 1 | | | Credit 4.3 | Low-Emitting Materials: Flooring Systems | 1 |
| 1 | | | Credit 4.4 | Low-Emitting Materials: Composite Wood and Agrifibre Products | 1 |
| 1 | | | Credit 5 | Indoor Chemical and Pollutant Source Control | 1 |
| 1 | | | Credit 6 | Controllability of System: Thermal Comfort | 1 |
| 1 | | | Credit 7 | Thermal Comfort: Design | 1 |
| | 1 | | Credit 8.1 | Daylight and Views: Daylight | 1 |
| 1 | | | Credit 8.2 | Daylight and Views: Views | 1 |
| | | | | | |
| Yes | ? | No | Innovation in Design | | 6 Points |
| 1 | 0 | 5 | Credit 1.1 | Innovation in Design | 1 |
| | 1 | | Credit 1.2 | Innovation in Design | 1 |
| | 1 | | Credit 1.3 | Innovation in Design | 1 |
| | 1 | | Credit 1.4 | Innovation in Design | 1 |
| | 1 | | Credit 1.5 | Innovation in Design | 1 |
| 1 | | | Credit 2 | LEED® Accredited Professional | 1 |
| | | | | | |
| Yes | ? | No | Regional Priority | | 4 Points |
| | 1 | | Credit 1 | Durable Building | 1 |
| 1 | | | Credit 2.1 | Regional Priority Credit - SSc2 | 1 |
| 1 | | | Credit 2.2 | Regional Priority Credit - MRc2 | 1 |
| | 1 | | Credit 2.3 | Regional Priority Credit | 1 |



No. DP 14-657502

To the Holder: KASIAN ARCHITECTURE INTERIOR DESIGN
AND PLANNING LTD.

Property Address: 11380 STEVESTON HWY (Parent: 11388 Steveston Hwy)

Address: SUITE 1685- 1500 WEST GEORGIA ST
VANCOUVER, BC V6G 2Z6

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans # 1.a to # 4.b attached hereto.
4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$112,801.92. to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
7. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

Development Permit

No. DP 14-657502

To the Holder: KASIAN ARCHITECTURE INTERIOR DESIGN
AND PLANNING LTD.

Property Address: 11380 STEVESTON HWY (Parent: 11388 Steveston Hwy)

Address: SUITE 1685- 1500 WEST GEORGIA ST
VANCOUVER, BC V6G 2Z6

AUTHORIZING RESOLUTION NO.
DAY OF , .

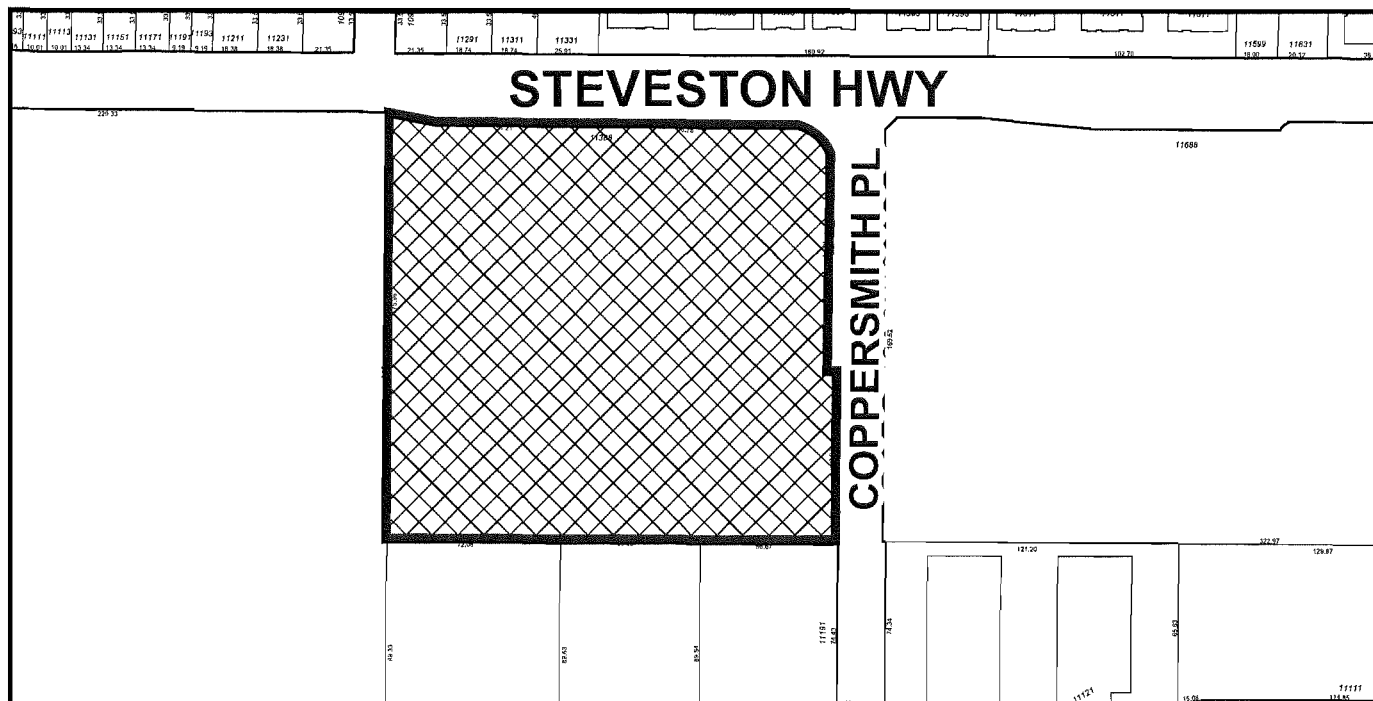
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond



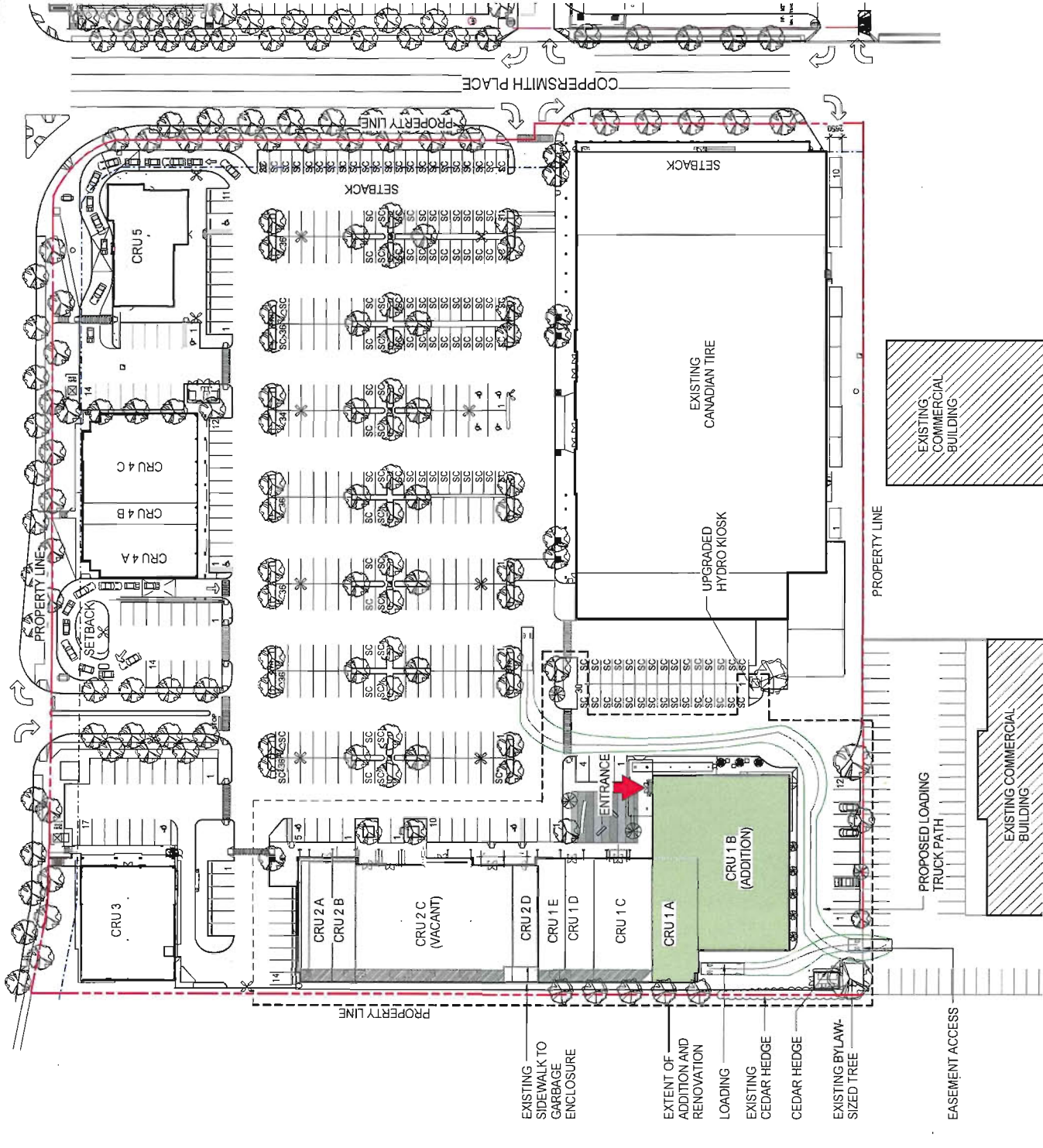
DP 14-657502
SCHEDULE "A"

Original Date: 03/07/14

Revision Date:

Note: Dimensions are in METRES

STEVESTON HIGHWAY



1 OVERALL SITE PLAN
DP-02
SCALE: 1:500

westbank

SCALE: 1 : 500

REISSUED FOR DEVELOPMENT
PERMIT

OVERALL SITE PLAN & PROJECT DATA
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-02
JULY 2, 2014
PROJECT 160046

Project Data
Civic Address: 11380 Steveston Highway, Richmond, BC
Legal Address: A SEC 1 BLK3N RG6W PL LMP45518
Zoning: ZC6

| Site Area | Bylaw Maximum | Existing 31538 sq. m. | Proposed 31538 sq. m. |
|---|------------------|--------------------------|--------------------------|
| Gross Leasable Floor Area REFER TO SHEET DP-04 FOR BREAKDOWN | 6900 sq.m | 5904 sq.m | 6786 sq.m |
| Lot Coverage | Max. 50% | 30% | 31% |
| Setbacks | | | |
| | NORTH | 6m | 6m |
| | EAST | 6m | 6m |
| | WEST | 0m | 0m |
| | SOUTH | 0m | 0m |
| Building Height | 12.0m | 9.5m | 9.5m |

| | |
|--------------|----------------|
| Parking Data | Refer to DP-04 |
|--------------|----------------|

| | | | |
|--|----------|--------|--------|
| Building Area Data (for F.A.R. Calculations) | Max. 60% | 31.09% | 33.87% |
|--|----------|--------|--------|

| | | | | |
|------------------------------------|---------|---|-----|---|
| Bicycle Storage Spaces (CRU 1 A&B) | Class 1 | 3 | N/A | 3 |
| | Class 2 | 4 | N/A | 4 |

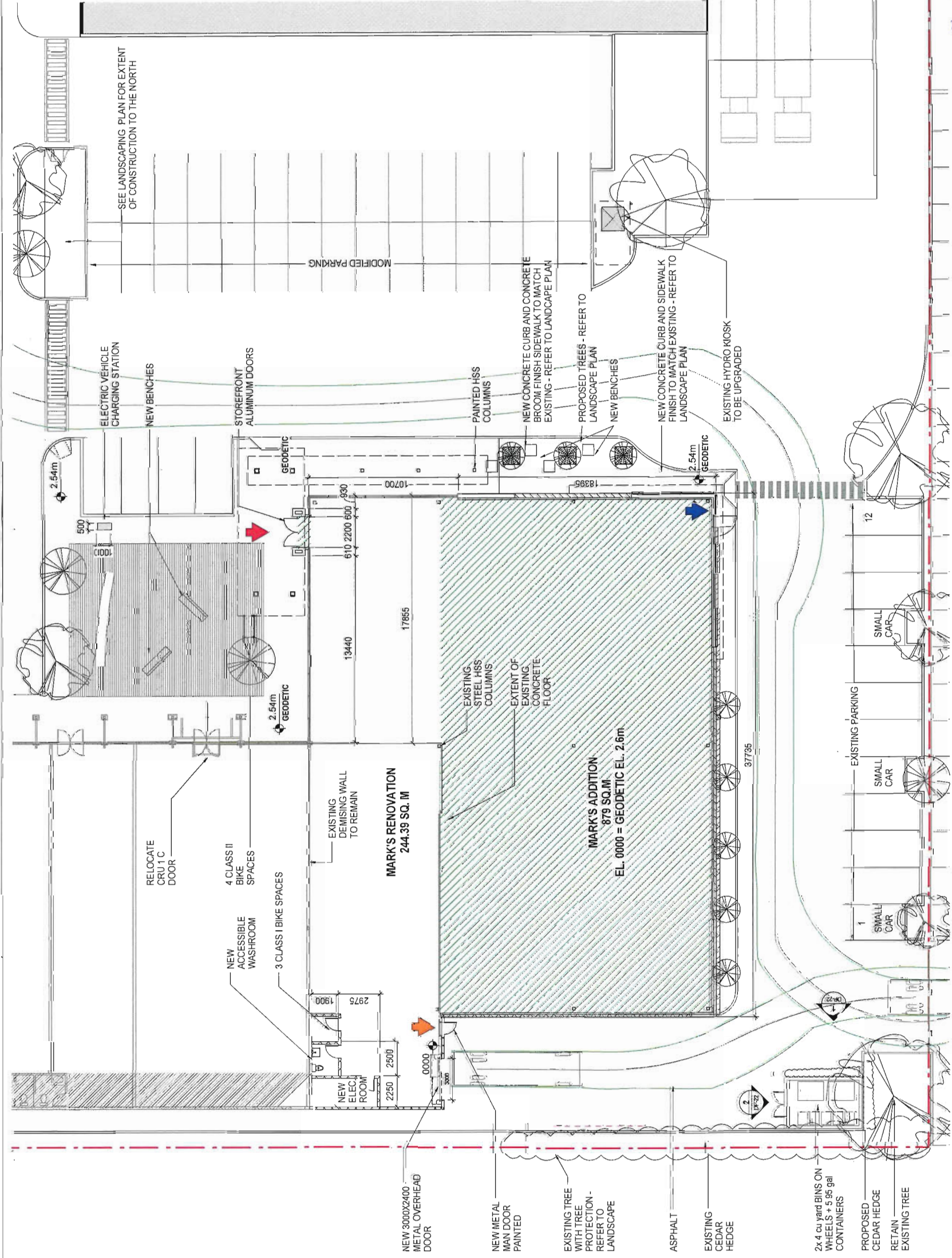
NOTE: REFER TO DP-09 FOR ENLARGED SITE PLAN
OF EXTENT OF ADDITION & RENOVATION

DP 14-657502



Plan# 1.a.





1
DP-09
PROPOSED SITE/LANDSCAPE PLAN & GROUND FLOOR PLAN
SCALE: 1:125

SCALE: As indicated

REISSUED FOR DEVELOPMENT PERMIT

PROPOSED SITE & GROUND FLOOR PLAN
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-09
JULY 2, 2014
PROJECT 160046



DP 14-657502
Plan #1.b.

Existing Areas

| Building # | Building Area (sf) | Building Area (m2) |
|--------------------------------|--------------------|--------------------|
| CRU 1A | 2630 | 244.3 |
| CRU 1B | 9461 | 878.9 |
| CRU 1C | 3488 | 324.0 |
| CRU 1D | 1684 | 156.4 |
| CRU 1E | 1805 | 167.7 |
| CRU 2A | 2048 | 190.3 |
| CRU 2B | 2095 | 194.6 |
| CRU 2C | 8371 | 777.7 |
| CRU 2D | 1786 | 119.5 |
| CRU 1&2 Subtotal Retail | 17871 | 1660.2 |
| CRU 1&2 Subtotal Food Catering | 5536 | 514.3 |
| CRU 3 | 5248 | 487.5 |
| CRU 4A | 2859 | 265.6 |
| CRU 4B | 1154 | 107.2 |
| CRU 4C | 4698 | 436.4 |
| CRU 5 | 3925 | 364.6 |
| CT Retail | 40016 | 3,717.5 |
| CT Service | 5191 | 482.2 |
| CT Warehouse | 10757 | 999.3 |
| CT Office | 456 | 14.5 |
| CT Total Area | 56120 | 5,213.5 |

Proposed Areas

| Building # | Building Area (sf) | Building Area (m2) |
|--------------------------------|--------------------|--------------------|
| CRU 1A | 2630 | 244.3 |
| CRU 1B | 9461 | 878.9 |
| CRU 1C | 3488 | 324.0 |
| CRU 1D | 1684 | 156.4 |
| CRU 1E | 1805 | 167.7 |
| CRU 2A | 2048 | 190.3 |
| CRU 2B | 2095 | 194.6 |
| CRU 2C | 8371 | 777.7 |
| CRU 2D | 1786 | 119.5 |
| CRU 1&2 Subtotal Retail | 27332 | 2,539.1 |
| CRU 1&2 Subtotal Food Catering | 5536 | 514.3 |
| CRU 3 | 5248 | 487.5 |
| CRU 4A | 2859 | 265.6 |
| CRU 4B | 1154 | 107.2 |
| CRU 4C | 4698 | 436.4 |
| CRU 5 | 3925 | 364.6 |
| CT Retail | 40016 | 3,717.5 |
| CT Service | 5191 | 482.2 |
| CT Warehouse | 10757 | 999.3 |
| CT Office | 156 | 14.5 |
| CT Total Area | 56120 | |

Existing Bylaw Required Parking

Typical Calculation (350/100) x 3 = 10.5

| Building Type | Building Area (sf) | Building Area (m2) | Bylaw Requirements | Req. Parking |
|------------------------|--------------------|--------------------|--------------------------------|--------------|
| Retail | 62585 | 5,814.1 | 3 stalls/100 sqm up to 350 sqm | 10.5 |
| CT Service | 5191 | 482.2 | 4 stalls/100 sqm additional | 218.5 |
| CT Warehouse | 10757 | 999.3 | 3 stalls/100 sqm up to 350 sqm | 10.5 |
| CT Office | 156 | 14.5 | 4 stalls/100 sqm additional | 5.3 |
| Food Catering | 6690 | 621.5 | 3 stalls/100 sqm up to 350 sqm | 10.5 |
| Restaurant, Drive Thru | 6784 | 630.2 | 4 stalls/100 sqm additional | 25.9 |
| Financial Service | 5248 | 487.5 | 8 stalls/100 sqm up to 350 sqm | 0.4 |
| | | | 10 stalls/100sqm additional | 28 |
| | | | 7 stalls/100 sqm up to 350 sqm | 27.1 |
| | | | 7 stalls/100 sqm up to 350 sqm | 24.5 |
| | | | 3 stalls/100 sqm up to 350 sqm | 19.6 |
| | | | 4 stalls/100 sqm additional | 10.5 |
| | | | | 397 |

Proposed Bylaw Required Parking

Typical Calculation (350/100) x 3 = 10.5

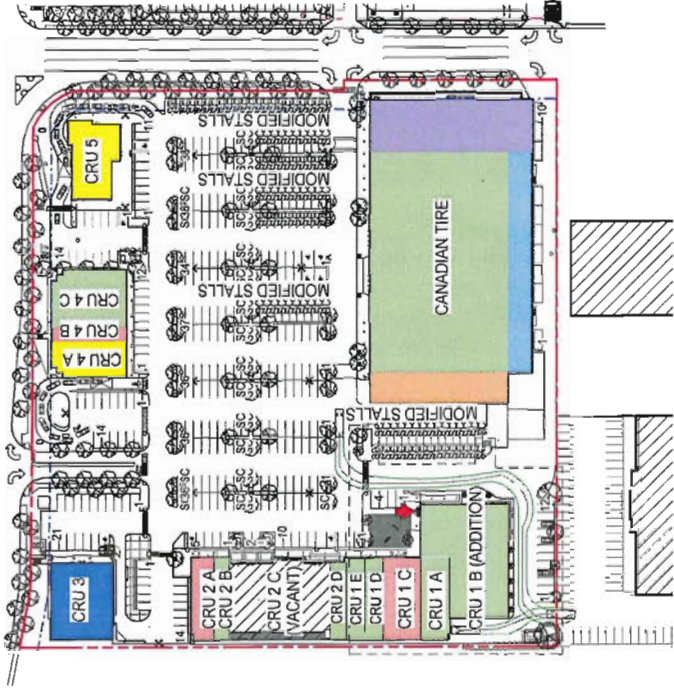
| Building Type | Building Area (sf) | Building Area (m2) | Bylaw Requirements | Req. Parking |
|------------------------|--------------------|--------------------|--------------------------------|--------------|
| Retail | 72046 | 6,693.1 | 3 stalls/100 sqm up to 350 sqm | 10.5 |
| CT Service | 5191 | 482.2 | 4 stalls/100 sqm additional | 253.7 |
| CT Warehouse | 10757 | 999.3 | 3 stalls/100 sqm up to 350 sqm | 10.5 |
| CT Office | 156 | 14.5 | 4 stalls/100 sqm additional | 5.3 |
| Food Catering | 6690 | 621.5 | 3 stalls/100 sqm up to 350 sqm | 10.5 |
| Restaurant, Drive Thru | 6784 | 630.2 | 8 stalls/100 sqm up to 350 sqm | 25.9 |
| Financial Service | 5248 | 487.5 | 10 stalls/100sqm additional | 0.4 |
| | | | 7 stalls/100 sqm up to 350 sqm | 28 |
| | | | 7 stalls/100 sqm up to 350 sqm | 27.1 |
| | | | 3 stalls/100 sqm up to 350 sqm | 24.5 |
| | | | 3 stalls/100 sqm up to 350 sqm | 19.6 |
| | | | 4 stalls/100 sqm additional | 10.5 |
| | | | | 3.5 |
| | | | | 432 |

Parking Summary

| | Total | Small Car | Regular | Disabled | Occupied Stalls | Demolished Stalls | Practical Total | Difference |
|--|-------|-----------|---------|----------|-----------------|-------------------|-----------------|------------|
| Existing Bylaw Required Parking Stalls | 397 | 198* | 198** | 8 | 0 | 0 | 397 | |
| Existing Provided Parking Stalls | 451 | 49 | 392 | 10 | 20 | 0 | 431 | 34 |
| Proposed Bylaw Required Parking Stalls | 432 | 216* | 216** | 9 | 0 | 44 | 432 | |
| Proposed Provided Parking Stalls | 432 | 131 | 291 | 10 | 0 | 44 | 432 | 0 |

*Small car required stalls based on 50% max. of required ratio

**Regular car required stalls based on 50% min. of required ratio



1 OVERALL SITE PLAN - FLOOR AREA TYPE
CRU/S SCALE: 1:1000

SCALE: AS INDICATED

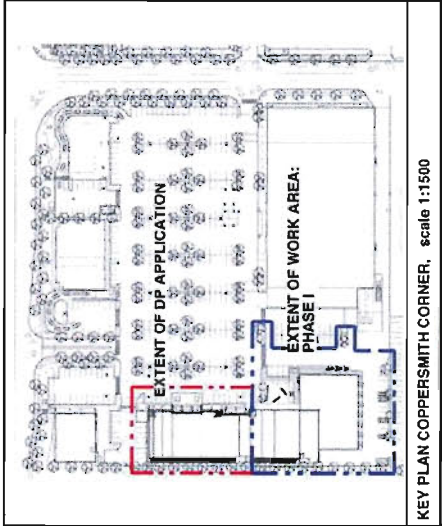
REISSUED FOR DEVELOPMENT PERMIT

PARKING DATA
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-05
JULY 2, 2014
PROJECT 160046

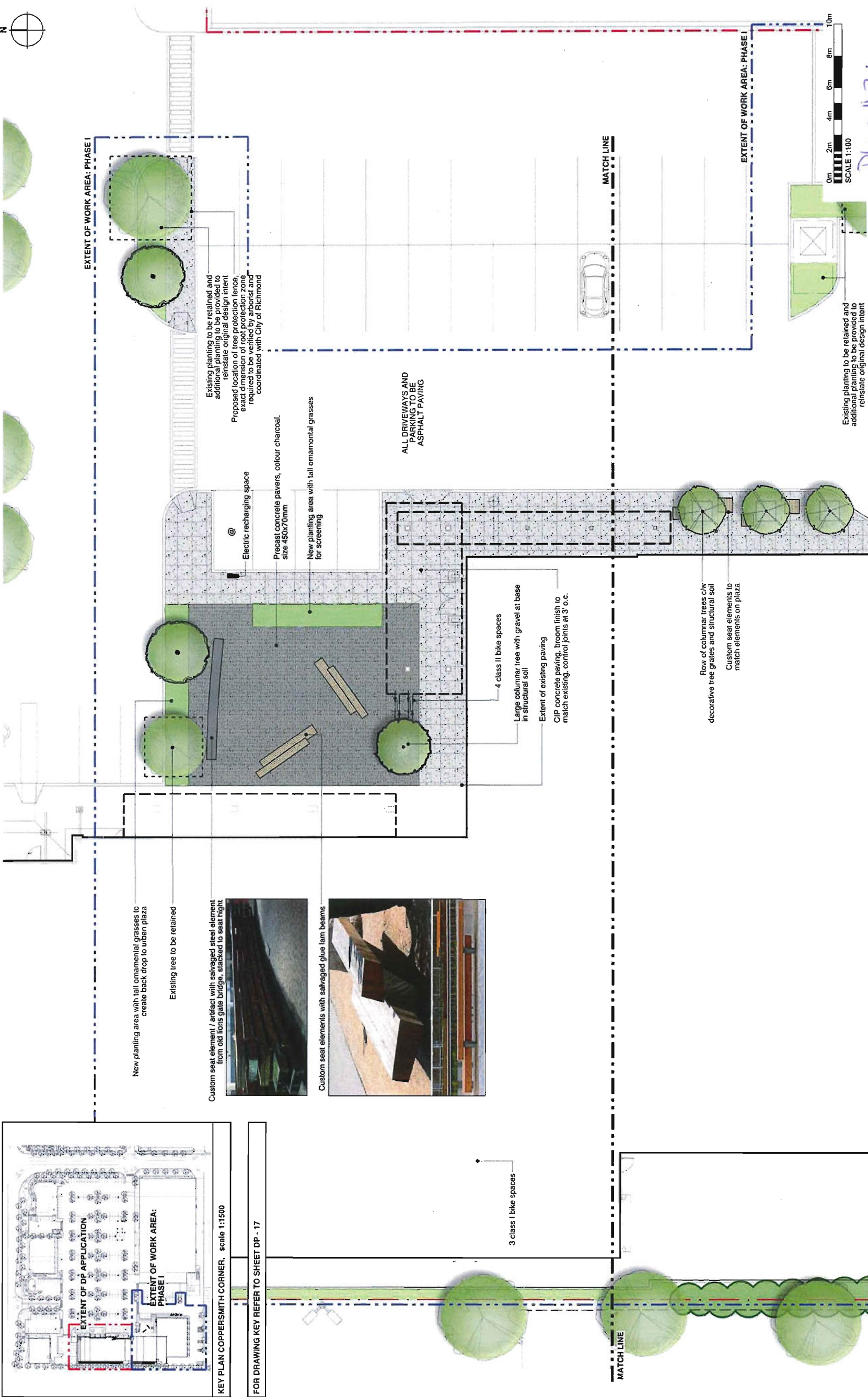


DP 14-657502
Plan#2



KEY PLAN COPPERSMITH CORNER, scale 1:1500

FOR DRAWING KEY REFER TO SHEET DP - 17



SCALE: 1:100

REISSUED FOR DEVELOPMENT PERMIT

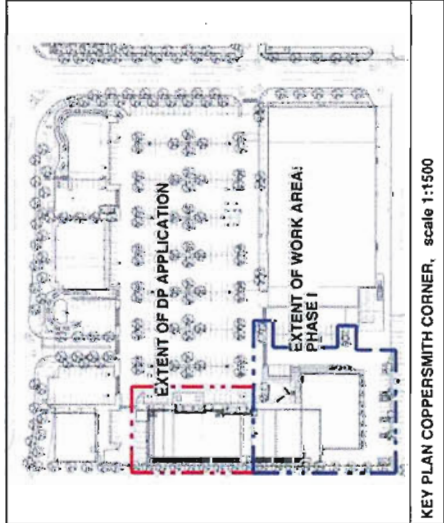
LANDSCAPE CONCEPT PLAN - NORTH
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-19
JULY 3, 2014
PROJECT 160046

Plan#3.b.
kasian
Durante Kieck Ltd.
102 - 1637 W. 5th Avenue
Vancouver BC V6J 1N5
t: 604 684 4611
f: 604 684 0577
www.dki.bc.ca

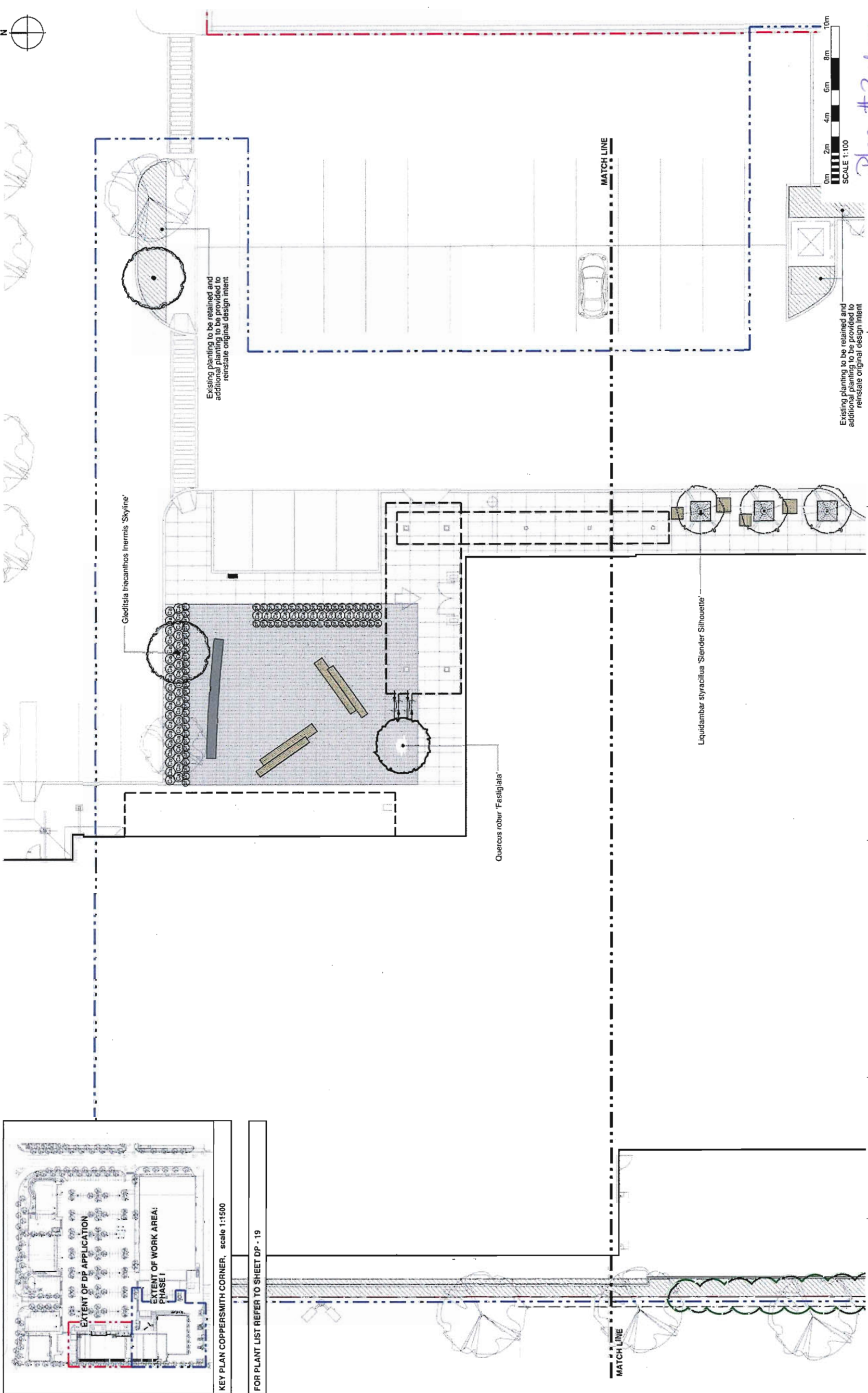
westbank

DP 14-657502



KEY PLAN COPPERSMITH CORNER, scale 1:1500

FOR PLANT LIST REFER TO SHEET DP - 19



SCALE: 1:100

REISSUED FOR DEVELOPMENT
PERMIT

LANDSCAPE PLANTING PLAN - NORTH
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-21
JULY 3, 2014
PROJECT 160046

Plan #3.d.
kasian
Durante Kreuk Ltd.
102 - 1637 W. 5th Avenue
Vancouver BC V6J 1N5
t: 604 684 4611
f: 604 684 0577
www.dki.bc.ca

Existing planting to be retained and additional planting to be provided to reinstate original design intent

Existing planting to be retained and additional planting to be provided to reinstate original design intent

Liquidambar styraciflua 'Slender Silhouette'

Quercus robur 'Fastigiata'

MATCH LINE

MATCH LINE

0m 2m 4m 6m 8m 10m
SCALE 1:100

westbank

DP 14-657502



1 EAST ELEVATION - GARBAGE ENCLOSURE
DP-22 SCALE: 1:20



2 NORTH ELEVATION - GARBAGE ENCLOSURE
DP-22 SCALE: 1:20

DP 14-657502
Plan #3.e.



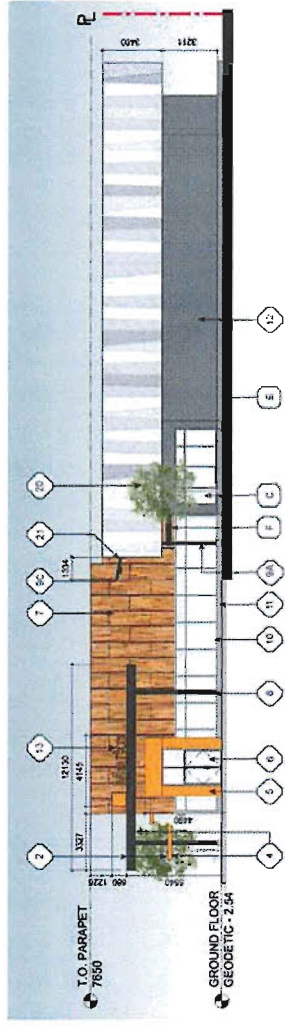
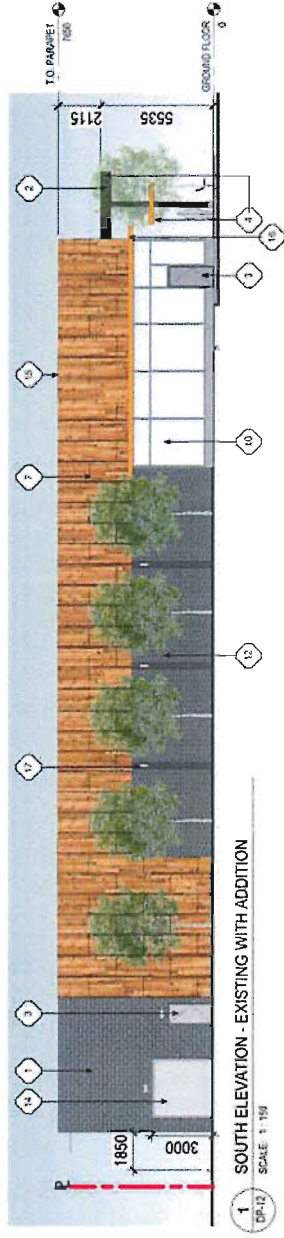
DP-22
JULY 3, 2014
PROJECT 160046

GARBAGE/RECYCLING ENCLOSURE ELEVATION
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

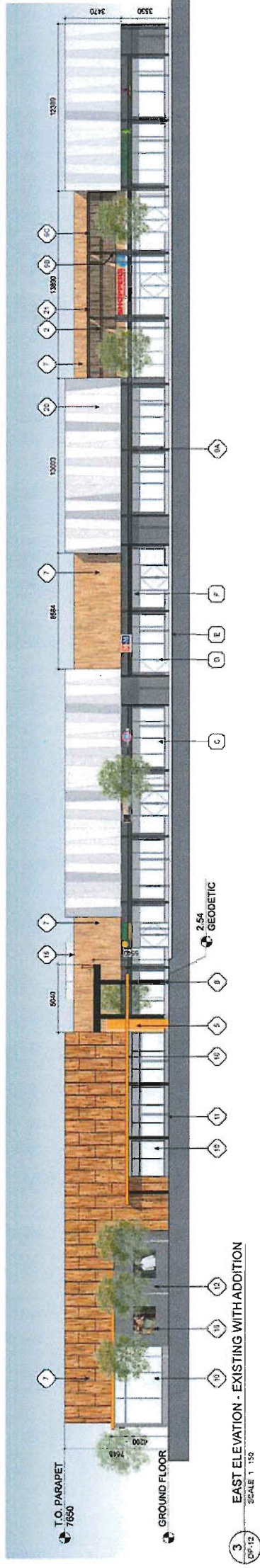
REISSUED FOR DEVELOPMENT
PERMIT

SCALE:

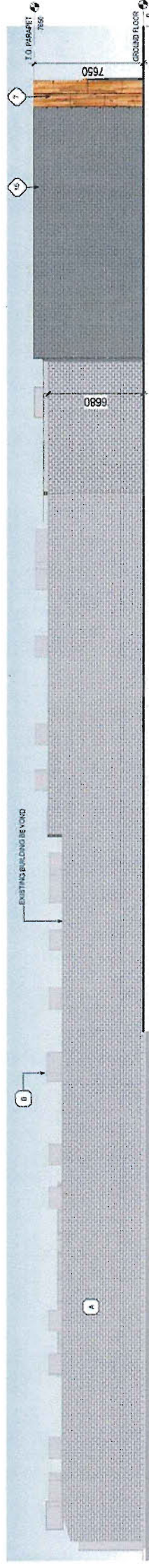
westbank



2 NORTH ELEVATION - EXISTING WITH ADDITION
SCALE 1:150



3 EAST ELEVATION - EXISTING WITH ADDITION
SCALE 1:150



4 WEST ELEVATION - EXISTING WITH ADDITION
SCALE 1:150

PROPOSED EXTERIOR MATERIAL LEGEND

- 1 SPLIT FACE CONCRETE BLOCK WALL
- LIGHT CHARCOAL COLOUR
- 2 PREFINISHED METAL FASCIA
- CHARCOAL FINISH
- 3 METAL MANDOOK AND FRAME
- PAINTED CHARCOAL
- 4 WOOD VENEER PANEL SOFFIT
- 5 ALUMINUM PANEL
PORTAL ORANGE FINISH
- 6 ALUMINUM STOREFRONT DOORS
CHARCOAL FINISH
- 7 METAL PANEL CLADDING
-VERTICAL WOOD GRAIN SCREEN PRINT
- 8 PAINTED STEEL COLUMNS
- CHARCOAL FINISH
- 9A EXISTING STEEL COLUMNS REPAINTED
- CHARCOAL FINISH
- 9B PAINTED STEEL COLUMNS
EXTENSION TO EXISTING COLUMN
- CHARCOAL FINISH
- 9C PAINTED STEEL STRUCTURE NEW CANOPY
- ALUMINUM STOREFRONT
- CHARCOAL FINISH
- 11 CAST IN PLACE CONCRETE UPSTAND
- SACK FINISH
- 12 STONE VENEER
- LIGHT CHARCOAL COLOUR
- 13 BACK-LIT ILLUMINATED SIGNAGE
- 14 PRE-FINISHED METAL OVERHEAD DOOR
- CHARCOAL FINISH
- 15 MECHANICAL ROOF TOP UNITS
- 16 PREFINISHED METAL FASCIA
- ORANGE FINISH
- 17 DOWNLIGHT FIXTURE
- 18 ADVERTISING PANEL
- 19 STEEL CANOPY STRUCTURE
- ORANGE FINISH
- 20 ALUMINUM PANEL, FOLDED AND PERFORATED
- WHITE FINISH
- 21 GLASS TOP FOR NEW CANOPY

EXISTING EXTERIOR MATERIAL LEGEND

- A EXISTING CONCRETE BLOCK WALL
- B EXISTING MECHANICAL ROOF TOP UNIT
- C EXISTING ALUMINUM STOREFRONT GLAZING
- D EXISTING ALUMINUM STOREFRONT DOOR
- E EXISTING CONCRETE BASE
- F EXISTING STEEL SIGN BAND

DP 14-657502
Plan # 4.a.



westbank

SCALE:

REISSUED FOR DEVELOPMENT
PERMIT

PROPOSED BUILDING ELEVATIONS
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-12
JULY 2, 2014
PROJECT 160046



PRECEDENT I
H&M SUNSET STRIP, LA, CA
UNIVERSAL DESIGN STUDIO



DAY TIME VIEW



NIGHT TIME VIEW

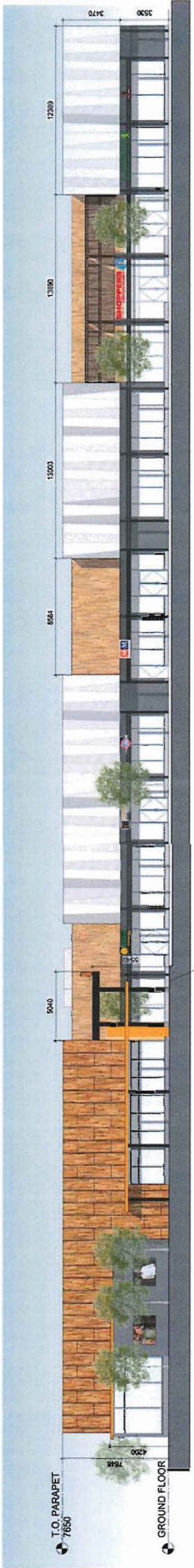


CORNER VIEW

PRECEDENT II
CLASSEN CURVE, OKLAHOMA CITY, OK
ELLIOTT ASSOCIATES ARCHITECTS



STEEL CANOPY

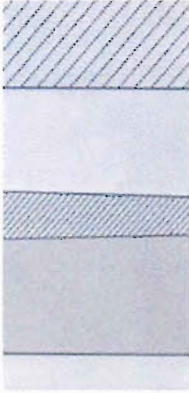


EAST ELEVATION (NTS)

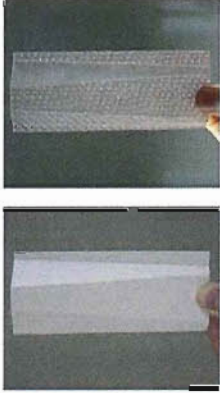
MATERIALS PALETTE



FOLDED AND PERFORATED METAL PANEL
- WHITE FINISH



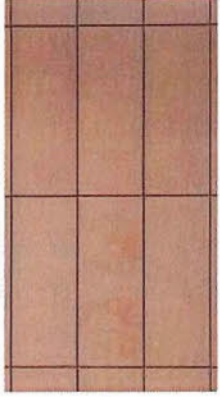
EXAMPLE OF PERFORATION PATTERN IN
WHITE FINISH METAL PANEL



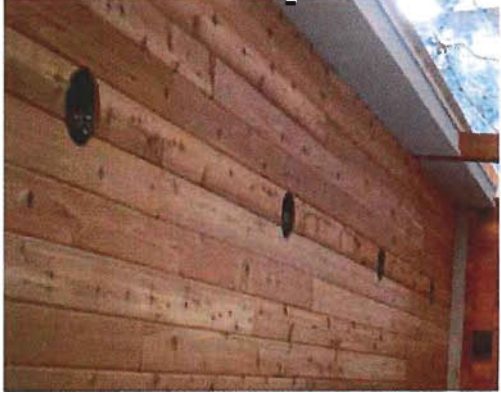
PROTOTYPE OF PERFORATION IN WHITE
FINISH METAL PANEL - DAY AND NIGHT



METAL PANEL CLADDING
-COLOUR TO MATCH MARK'S WORK
WAREHOUSE COLOUR SCHEME



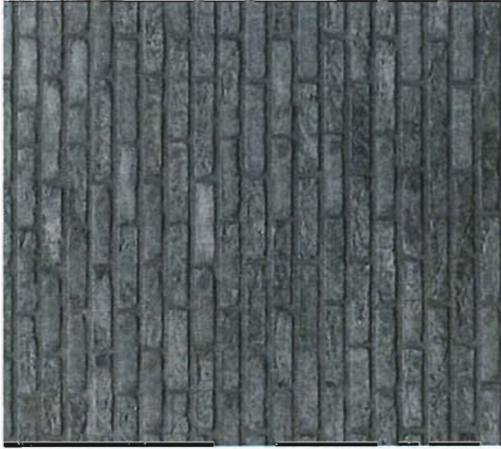
METAL PANEL CLADDING
-VERTICAL WOOD GRAIN SCREEN PRINT



WOOD SOFFIT WITH POT LIGHTS



PAINTED STEEL STRUCTURE
- CHARCOAL GREY FINISH



CONCRETE BLOCK WALL
- LIGHT CHARCOAL FINISH

westbank

SCALE: NTS

REISSUED FOR DEVELOPMENT
PERMIT

MATERIALS AND PALETTE
COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-12A
JULY 2, 2014
PROJECT 160046

kasian

DP 14-657502
Plan #4.b.

PRECEDENT I

H&M SUNSET STRIP, LA, CA
UNIVERSAL DESIGN STUDIO



DAY TIME VIEW



NIGHT TIME VIEW



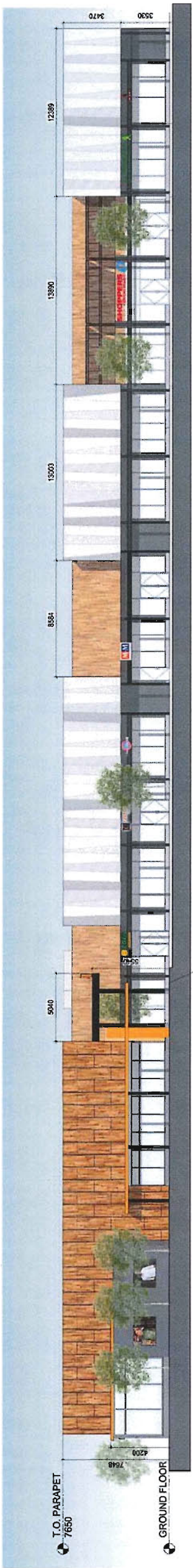
CORNER VIEW

PRECEDENT II

CLASSEN CURVE, OKLAHOMA CITY, OK
ELLIOTT ASSOCIATES ARCHITECTS



STEEL CANOPY

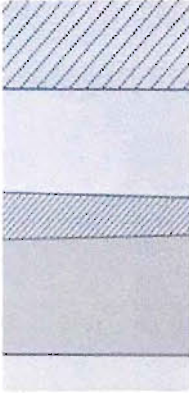


EAST ELEVATION (NTS)

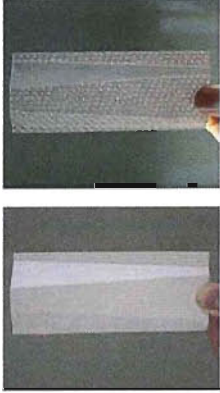
MATERIALS PALETTE



FOLDED AND PERFORATED METAL PANEL
- WHITE FINISH



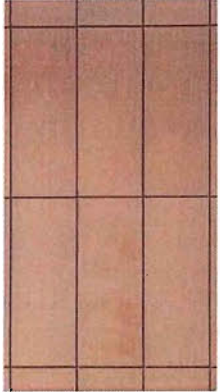
EXAMPLE OF PERFORATION PATTERN IN
WHITE FINISH METAL PANEL



PROTOTYPE OF PERFORATION IN WHITE
FINISH METAL PANEL - DAY AND NIGHT



METAL PANEL CLADDING
- COLOUR TO MATCH MARK'S WORK
WAREHOUSE COLOUR SCHEME



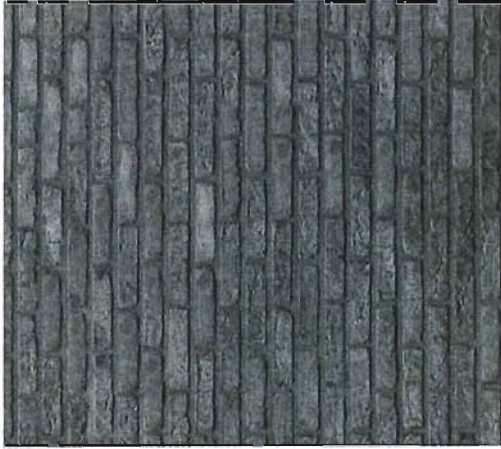
METAL PANEL CLADDING
- VERTICAL WOOD GRAIN SCREEN PRINT



WOOD SOFFIT WITH POT LIGHTS



PAINTED STEEL STRUCTURE
- CHARCOAL GREY FINISH



CONCRETE BLOCK WALL
- LIGHT CHARCOAL FINISH

westbank

SCALE: NTS

REISSUED FOR DEVELOPMENT
PERMIT

MATERIALS AND PALETTE

COPPERSMITH CORNER - PROPOSED EXPANSION
11380 STEVESTON HIGHWAY, RICHMOND, B.C.

DP-12A

JUNE 27, 2014
PROJECT 160046

kasian

DP 14-657502
Plan # 4.b.



City of Richmond

Report to Development Permit Panel Planning and Development Department

To: Development Permit Panel

Date: June 27, 2014

From: Wayne Craig
Director of Development

File: DV 13-634940

Re: **Application by Onni 7731 Alderbridge Holding Corp. for a Development Variance Permit at 5311 and 5399 Cedarbridge Way**

Staff Recommendation

That a Development Variance Permit be issued which would further vary the provisions of Richmond Zoning Bylaw 8500 to further reduce the visitor parking requirement from 0.15 spaces/unit, as per Development Permit (DP 12-615424), to 0.10 spaces/unit for a portion of the development located at 5311 and 5399 Cedarbridge Way on a site zoned "High Density Low Rise Apartments (RAH2)".


Wayne Craig
Director of Development

MM:blg

Staff Report

Origin

Onni 7731 Alderbridge Holding Corp. has applied to the City of Richmond to further vary the visitor parking requirement to 0.10 spaces/unit from the reduced 0.15 spaces/unit approved under DP 12-615424 for a 660-unit project in four (4) buildings located at 5311 and 5399 Cedarbridge Way (formerly 7731 Alderbridge Way) and 7771 Alderbridge Way.

The Development Permit Panel considered this application at its July 10, 2013 meeting; at which the Panel made the following recommendation on this variance application:

“That the application be referred back to staff for more consideration and additional research.”

In response to the above Development Permit Panel referral, the applicant's consultants conducted a more detailed parking study to the satisfaction of the Transportation Division as outlined below and that was included in the Staff Report considered at the April 16, 2014 DP Panel. At this meeting, the Panel made the following recommendation:

That Development Variance 13-634940, be referred back to staff to examine methods that would secure additional parking capacity for future demand in the proposed development and report back to the April 30, 2014 Development Permit Panel.

This report discusses the applicant's revised proposal to address the following referral from the April 16, 2014 Development Permit Panel meeting. It is noted that the applicant required more time to examine options to address the above referral, and the Staff Report has been brought to this later meeting of the Panel.

In response to the April 16, 2014 referral and staff comments, the applicant now proposes to seek a variance to the visitor parking requirements to 0.10 spaces/unit only for Buildings 1 and 2 on Lot 1 forming the western side of the development located at 5311 and 5399 Cedarbridge Way, containing 341 of the development's 660 units (Attachment 2), as discussed below in the Staff Report.

Development Information

Please refer to the attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant bylaw requirements.

Background

Development surrounding the subject site is as follows:

- To the north, lies a section of New River Road (the former CPR line) which is under construction. Further to the north, one (1) large light industrial building is located on a site zoned “Industrial Business (IB1)”.

- To the south, lies Alderbridge Way; with the former Grimm's meat factory site on the south side of the street. This site was rezoned in July, 2013 (RZ11-593705) from "Industrial Retail (IR1)" to "Residential Limited Commercial (RCL)" to allow for a higher density, mixed-use development.
- To the east, two (2) light industrial/retail buildings are located on a site to the east of an adjacent lane and on a site zoned "Industrial Retail (IR1)".
- To the west, the Gilbert Road approach to the Dinsmore Bridge forms the north-west boundary of the subject site. The remainder of the site is bounded by the former "V-Tech" building site which is zoned "Industrial Retail (IR1)".

Staff Comments

Current Approved Visitor Parking Rate for Entire Development

Under DP 12-615424 and the associated rezoning (RZ 11-585209), the combined visitor and resident parking was reduced by 7.5% below the parking requirements set out in Zoning Bylaw 8500. As part of this reduction, the visitor parking rate was varied by 25% from 0.20 to 0.15 spaces/unit on each of Lot 1 (Buildings 1 / 2) at 5311 and 5399 Cedarbridge Way and Lot 2 (Buildings 3/4) at 7771 Alderbridge Way. The following Transportation Demand Management (TDM) measures were required for this 7.5% overall reduction:

- A developer contribution of \$100,000 to the City for the construction of a 3.0 m (9.8 ft.) wide bike/pedestrian pathway along the east side of Gilbert Road from the southern end of the applicant's required frontage improvements to Lansdowne Road.
- The developer entering into an agreement to secure the provision of 20% of the total resident parking spaces with electric service for vehicle plug-ins with conduits, circuit breakers and wiring, and provision of one (1) standard 120 volt electric plug-in for every 40 resident bicycle parking spaces.

Proposed Additional Visitor Parking Variance for Lot 1 at 5311 and 5399 Cedarbridge Way

The applicant is proposing a further variance for the required visitor parking from 0.15 spaces/unit approved under DP 12-615424 to 0.10 spaces/unit on Lot 1 (Buildings 1 and 2 only) at 5311 and 5399 Cedarbridge Way. The proposed variance results in a further reduction of 16 spaces from the 51 visitor parking spaces to be provided under DP 12-615424; leaving a total of 35 visitor spaces being provided. At the same time, the resident parking is increased by 22 spaces from 378 spaces under DP 12-615424 to 400 spaces. The addition of 22 resident parking spaces is more than the 16 visitor parking space reduction due to structural and building efficiencies that have been achieved during the Building Permit process.

Based on the concerns from the previous Development Permit Panel consideration of this variance application, the applicant has expanded the parking study to:

- Cover a total of five (5) sites in Richmond, three (3) of which have a similar proximity to the Canada Line as the subject Riva development (i.e. within 750 m).

- Include three (3) additional developments, studied for four (4) weeks in February and March on Tuesdays, Wednesdays, Fridays and Saturdays for the times of day as per original study. The observations were taken every half-hour; instead of hourly as done in the original study which studied two (2) developments on a Saturday.
- Outline in greater detail the 2012 Metro Vancouver Parking Study (i.e. time of day, day of week, unit occupancy, duration of survey, frequency of survey, adjacent surroundings and parking opportunities, etc.).

Addressing the April 16, 2014 DP Panel Concerns on Future Visitor Parking Demand

To address the Development Permit Panel's concern regarding the potential for visitor parking demand exceeding the proposed 0.10 spaces/unit, the applicant has reduced the scope of the variance which is now being sought to now reduce visitor parking within Buildings 1 and 2 only on Lot 1. If the variance is approved, this would bring the overall visitor parking rate for the whole development to 0.125 spaces/unit since the parking rate for Buildings 3 and 4 on Lot 2 would remain at the current 0.15 spaces/unit instead of the parking rate for whole development being reduced to 0.10 spaces/unit through this variance as previously proposed. It should be noted that, as a condition of the rezoning for the development, an easement was registered allowing all four (4) permanent visitor parking areas in Buildings 1 and 2 (being constructed on Lot 1 subject to the proposed variance) and Buildings 3 and 4 (to be constructed on Lot 2) to be shared.

The following staged approach is proposed to accommodate the provision of additional visitor parking for Lot 1 if needed and provide for future consideration of a similar visitor parking variance for Lot 2 if warranted.

1. The requested visitor parking variance for Lot 1 only at this time, if approved, would provide a total of 35 visitor spaces in the parkade for Buildings 1 and 2 on Lot 1 as shown on Attachment 3.
2. The applicant has agreed to register an easement prior to issuance of the Development Variance Permit on Lot 2 in favor of Lot 1 which would require for a total of 43 visitor spaces being available with 35 visitor spaces within the parkade on Lot 1 and an additional interim eight (8) visitor surface spaces on Lot 2 should they be required as discussed below.

A \$25,000 security is required to ensure the applicant undertakes the visitor parking study, to the satisfaction of the Director of Transportation, within six (6) months of both Buildings 1 and 2 on Lot 1 being fully occupied. If the study determines the 35 visitor spaces on Lot 1 are insufficient, the construction the eight (8) interim parking spaces on Lot 2 will be required until the ultimate parking garages on Lot 2 are built. If the study determines the 35 visitor spaces on Lot 1 are sufficient, the eight (8) interim parking spaces will not be required, the \$25,000 security released and the easement released.

The \$25,000 security would also cover the cost of construction of the eight (8) interim surface visitor spaces on Lot 2 as may be required.

This approach would also maintain the proposed visitor parking rate at 0.10 spaces/unit on Lot 1 and at 0.15 spaces/unit for Buildings 3 and 4 to be constructed on Lot 2 (approved under DP12-615424) for an overall development visitor parking rate of 0.125 spaces/unit.

3. Thirdly, once constructed, the future east parkade for Buildings 3 and 4 on Lot 2 would provide the required 0.15 visitor spaces/unit or 48 parking spaces (approved under DP12-615424) as shown on Attachment 5. All four (4) of the permanent parking garages can then be shared under existing parking easement secured at rezoning, providing an overall visitor parking rate of 0.125 spaces/unit or 83 spaces over the entire development. If Buildings 3 and 4 on Lot 2 are commenced before the parking study is completed for Lot 1, this current parking easement would allow sharing of the visitor parking between Lots 1 and 2 if needed.
4. If the parking study concludes there is no visitor parking shortage with the 35 spaces provided under the proposed variance (0.10 visitor spaces/unit) for Buildings 1 and 2, the applicant may then apply for a subsequent Development Variance Permit to reduce the visitor parking from the 0.15 spaces/unit to 0.10 spaces/unit for Buildings 3 and 4 on Lot 2. This application would be considered by the Development Permit Panel and Council as a separate application.

Summary of Expanded Parking Study

To provide rationale for the proposed 0.10 spaces/unit parking variance, the expanded parking study included three (3) further developments in addition to the initial two (2) development sites studied.

The total five (5) comparable to the subject Onni development are as follows:

- The Perla Towers (6331 Buswell Street and 8100 Saba Road) located are at approximately 250 m from the Canada Line (Brighthouse Station).
- The Seasons Towers (5028, 5068, 5088 Kwantlen Street) and Ocean Walk Towers (7535, 7555, 7575 Alderbridge Way) have a similar distance of 750 m to the Canada Line (Lansdowne Station) as the applicant's subject Riva site.
- The Golden Leaf Towers (7680, 7700, 7760 Granville Ave.) and Fullerton Towers (9171 Ferndale Rd) are farther away from the Canada Line; by 900 m and 1100 m respectively. These two (2) sites are also served by local buses (stops less than 200 m away) that connect to the Canada Line.
- All five (5) developments are a comparable form of residential market housing,
- All five (5) developments have on-street parking available on adjacent streets.

The expanded survey, including three (3) additional comparable developments, was completed on dates between February 12, 2014 and March 1, 2014, with the counts taken every half (1/2) hour at the same times for each building as summarized in the following Figures 1 and 2.

Figure 1: Comparable Residential Developments Surveyed by Applicant & Metro Vancouver

| | Metro Van Study | Initial Survey Series #1 | | Expanded Survey Series #2 | | |
|--|------------------------------|--------------------------------------|----------------------------------|----------------------------------|------------------|----------------------------------|
| Name of Development | Paloma 1 & 2 Towers | Perla | Seasons | Golden Leaf Towers | Fullerton Towers | Ocean Walk Towers |
| Development location in Richmond | 6068 No 3 Rd, & 8033 Saba Rd | 6331 Buswell Street and 8100 Saba Rd | 5028, 5068, 5088 Kwantlen Street | 7680, 7700, 7760 Granville Ave | 9171 Ferndale Rd | 7535, 7555, 7575 Alderbridge Way |
| # of low-rise buildings | 0 | 0 | 0 | 1 | 0 | 2 |
| # of mid-rise buildings | 0 | 0 | 0 | 0 | 5 | 0 |
| # of high-rise buildings | 2 | 2 | 3 | 2 | 0 | 4 |
| Type of dwelling units | Market | Market | Market | Market | Market | Market |
| # of dwelling units ¹ | 288 | 219 | 288 | 128 | 148 | 265 |
| # of occupied dwelling units | All | All | All | All | All | All |
| # of visitor parking spaces | n/a | 30 | 59 | 26 | 28 | 42 |
| # of visitor spaces reserved for other use | | 0 | 0 | 15 | 6 | 9 |
| Visitor Parking Supply Rate (net) | | 0.14 | 0.20 | 0.09 | 0.15 | 0.12 |
| Location of nearby on-street parking | Saba Rd | Buswell, Saba | None | Minoru Blvd | Ferndale Rd | Alderbridge Way |
| Distance to the Canada Line station | ~150m | ~250m | ~750m | ~900m | ~1,100m | ~750m |
| Maximum Observed Usage | | 9 | 26 | 11 | 10 | 26 |
| Maximum Usage Rate (per unit) | | 0.04 | 0.09 | 0.09 | 0.07 | 0.10 |

Figure 2: Survey Methodology

| Day of Week | Time of Day | Frequency of Survey | Duration of Survey | Survey conducted on | Total Observations |
|----------------------------------|-----------------|----------------------------|--------------------|----------------------------|--------------------|
| Initial Survey Series #1 | | | | | |
| Friday | 6:00pm - 9:00pm | every 1 hour x 2 locations | 1 day | 8-Feb-13 | 10 |
| Saturday | 2:00pm - 6:00pm | every 1 hour x 2 locations | 1 day | 16-Feb-13 | 8 |
| Expanded Survey Series #2 | | | | | |
| Tuesday | 6:00pm - 9:00pm | every 1/2 hour | 1 day | 12-Feb-14 | 21 |
| Wednesday | 6:00pm - 9:00pm | every 1/2 hour | 1 day | 18-Feb-14 | 21 |
| Friday | 6:00pm - 9:00pm | every 1/2 hour | 3 days | 14, 21, 28-Feb-14 | 63 |
| Saturday | 2:00pm - 8:00pm | every 1/2 hour | 3 days | 15, 22-Feb-14 and 1-Mar-14 | 117 |

The initial study parking observations on a half-hourly basis on a Friday and Saturday for the two (2) developments with a total of 18 observations. The peak usage demands for each of these developments are:

- Perla Towers; 0.04 visitor parking spaces/unit based on the observed peak parking demand of nine (9) parked vehicles.
- Seasons Towers; 0.09 visitor parking spaces/unit based on the observed peak parking demand of 26 parked vehicles.

The expanded study included parking observations on a half-hourly basis for the three (3) additional developments. According to the revised study, the maximum visitor parking demand observed for the surveyed residential developments is outlined below. It should be noted that most of the 222 observations showed less visitor parking demand than the peak usage demands indicated below:

- Golden Leaf Towers; 0.09 visitor parking spaces/unit based on the observed peak parking demand of 11 parked vehicles.
- Fullerton Towers; 0.07 visitor parking spaces/unit based on the observed peak parking demand of 10 parked vehicles.
- Ocean Walk Towers; 0.10 visitor parking spaces/unit based on the observed peak parking demand of 26 parked vehicles.

Regarding the above observations, it should be noted that the peak visitor parking demand of 0.07 to 0.10 parking spaces/unit was reached very few times out of the total 222 observations made (e.g. six (6) observed times with a usage rate of 0.09 visitor spaces/unit at the Golden Leaf Towers and one (1) observed time with a usage rate of 0.10 visitor spaces/unit at the Ocean Walk Towers). Thus, the proposed 0.10 visitor spaces/unit for Buildings 1 and 2 on Lot 1 under the subject Development Variance Permit application is expected to meet the anticipated visitor parking demand.

The peak visitor demand varied between each of the days and developments observed. It should be noted that the overall average visitor parking demand was 0.05 spaces/unit over the three (3) studied developments with only one (1) of 222 observations reaching the highest demand rate of 0.1 spaces/unit.

Metro Vancouver Parking Study

Metro Vancouver undertook a study of apartment parking regulations across Canada and actual parking usage within Metro Vancouver in 2011-12. The regional review included 80 locations near rapid transit lines and primary bus routes within the Frequent Transit Network (FTN), as well as other locations away from the FTN.

Visitor parking was observed at a subset of the 80 regional sites; with one (1) mixed-use site in Richmond being reviewed (Paloma at 6068 No. 3 Road and Paloma 2 at 8033 Saba Road with a total of 282 dwelling units) as shown in Figure 1 above. This site is located approximately 150 m from a Canada Line station. Observed peak parking demand rate was 0.04 occupied visitor parking spaces/unit. As Paloma is very close to the Canada Line station, and its demand would likely be higher if it were further from the station.

Rationale for Support for Proposed Variance

With the above-noted inclusion of the Metro Vancouver Parking Study and information from the applicant's expanded parking study required by City staff, staff support the proposed visitor parking variance for the following reasons:

- 1) *Parking Study Results:* With the expanded parking study submitted by the applicant included, there is now a review of five (5) large developments of a similar character. The findings of the applicant's expanded study indicate the highest demand for visitor parking was 0.10 spaces/unit or less in similar developments. The study found that the visitor parking demand for comparable developments studied was generally lower than 0.10 spaces/unit.
- 2) *Metro Vancouver Parking Study:* To supplement these findings, the Richmond-specific results of the 2012 Regional Residential Parking Study prepared by Metro Vancouver were also reviewed. It concluded that residential visitor parking supply may be over supplied in most apartment developments.

- 3) *Street Parking*: There will be approximately 33 street parking spaces permitted on the section of Cedarbridge Way between Alderbridge Way and New River Road (the former CPR line) which is to be constructed by the proposed development.
- 4) *Transit Proximity*: The development is in relatively close proximity (10 minute walk) to the Canada Line Lansdowne Station and a number of Translink Frequent Transit Network services; such as the No. 403 and No. 410 bus routes on No. 3 Road. The C94 bus route, connecting the Canada Line and the Oval, also supports visitors using transit. This transit availability may also reduce the need for residential visitor parking spaces.
- 5) *Requirement for Future Parking Study and Additional Visitor Parking*: The requirement for a follow-up visitor parking study after occupancy of Buildings 1 and 2 on Lot 1 along with the proposed easement provide access to Lot 2 and a \$25,000 security provide the ability for the City to have the applicant provide eight (8) additional spaces on Lot 2 if needed in the interim before Buildings 3 and 4 are constructed on that lot. In the future, there will be an overall visitor parking rate of 0.125 spaces/unit over the entire development once all four (4) buildings are constructed on Lots 1 and 2 if the parking study does not support a further variance from 0.15 to 0.10 visitor spaces/unit for Buildings 3 and 4 on Lot 2.
- 6) *Multiple Parking Garages*: The proposed development includes four (4) separate visitor parking areas for the four (4) buildings. This configuration provides flexibility for visitors to chose alternate visitor parking areas if the visitor parking area within the building being visited is full. To facilitate the use of the multiple garages, a reciprocal easement was registered at the time of rezoning and issuance of the original DP 12-615424 that provides that the visitor parkade entrance intercoms for each building will be interconnected. This shared approach ensures that all four (4) parking garages are available to serve the all four (4) buildings within the entire development. This would equate to a total of 83 visitor parking spaces if the visitor parking rate is reduced from 0.10 spaces/unit for Buildings 1 and 2 on Lot 1, but is maintained at 0.15 spaces/unit for Buildings 3 and 4 on Lot 2.
- 7) *Parking Stays Within the Approved Transportation Demand Management (TDM) Reduction*: It is important to note that with the proposed variance, the total combined overall resident and visitor parking is reduced by less than the 7.5% TDM reduction allowed under the initial rezoning which included 429 spaces within Buildings 1 and 2 on the West Lot 1 and required a total of 845 resident spaces and visitor spaces overall on Lots 1 and 2.

Based on the above factors, City staff support the proposed visitor parking variance. It should be noted that if other such visitor parking variance applications are made in the future for other developments, the City will assess the proposed visitor parking reductions along with the package of TDM measures for such developments on a case-by-case basis.

Conclusions

Staff support the proposed visitor parking variance from the currently approved 0.15 spaces/unit to 0.10 spaces/unit for the Lot 1 at 5311 and 5399 Cedarbridge Way given: the findings of the expanded, more detailed parking study for the development and the findings of the 2012 Metro Vancouver Residential Parking Study; the interconnected parkade intercoms that provide flexibility in the overall use of the four (4) visitor parking areas; proximity to transit; and the TDM measures which include the provision of the electric vehicle plug-ins and contribution of the Gilbert Road pedestrian/cycling facilities.

Furthermore, if the visitor parking study required after occupancy of Buildings 1 and 2 on Lot 1 determines there is a visitor parking shortage, an interim additional eight (8) spaces will be provided on Lot 2 under the proposed interim parking easement. Over long-term, the 0.15 visitor spaces/unit parking requirement would be maintained for Buildings 3 and 4 on Lot 2. All four (4) of the permanent parking lots can then be shared under current permanent parking easement, providing an overall visitor parking rate of 0.125 spaces/unit or 83 spaces over the entire development.



Mark McMullen
Senior Coordinator-Major Projects
(604-276-4173)

MM:blg

The following are to be met prior to forwarding this application to Council for approval:

- Registration of an easement in favor of Lot 1 that requires a visitor parking study to be completed within six (6) months of full occupancy of Buildings 1 and 2 on Lot 1 to the satisfaction of the Director of Transportation; that requires eight (8) additional visitor parking spaces to be on Lot 2 if the results of the parking study confirm there is insufficient visitor parking on Lot 1, and allows for the City draw on a \$25,000 Letter of Credit for the construction of the eight (8) interim parking spaces on Lot 2 if needed. The easement would state that if the study is completed and concludes there is sufficient visitor parking on Lot 1, the easement would be discharged and the security returned to the applicant.
- Receipt of a Letter-of-Credit for \$25,000 to ensure the preparation of a visitor parking study and the provision of eight (8) additional visitor parking spaces, as needed, as provided for in the above easement.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).


DV 13-634940
Attachment 1

Address: 5311 & 5399 Cedarbridge Way (Lot 1 - Subject Lot within Revised Variance Application)
7771 Alderbridge Way (Lot 2 - Related Lot Within Same Development)

Applicant: Onni 7731 Alderbridge Holding Corp. Owner: Onni 7731 Alderbridge Holding Corp. & Onni 7771 Alderbridge Holding Corp.

Planning Area(s): City Centre Area Plan (Lansdowne Village)

Floor Area Gross: 58,406 m² Floor Area Net: 57,425 m²

| | Existing | Proposed |
|-------------------------|---|---|
| Site Area: | 25,175 m ² for Lots 1 & 2 28,713 m ² incl. Cedarbridge Way | 25,175 m ² for Lots 1 & 2 28,713 m ² incl. Cedarbridge Way |
| Land Uses: | Multi-Family Residential – Lot 1 Light Industrial – Lot 2 | Multi-Family Residential – Lots 1 & 2 |
| OCP Designation: | Mixed-Use | Mixed-Use |
| Zoning: | High Density Low Rise Apartments (RAH2) | High Density Low Rise Apartments (RAH2) |
| Number of Units: | Lot 1 – 341 Lot 2 – 319 Total – 660 | Lot 1 – 341 Lot 2 – 319 Total – 660 |

| | Bylaw Requirement With DP 12-615424 Variance | Proposed Development | Variance |
|----------------------------------|--|---|--|
| Lot Size (Min.) | <ul style="list-style-type: none"> 2,400 m² (25,833 ft²) | <ul style="list-style-type: none"> Lot 1: 13,288 m² (143,040 ft²) Lot 2: 11,885 m² (127,937 ft²) | <ul style="list-style-type: none"> None |
| Lot Coverage (Max.) | <ul style="list-style-type: none"> 60% for buildings 80% for building and non porous surfaces | <ul style="list-style-type: none"> Lot 1: building footprint: 45% non-porous surfaces: 69.5% Lot 2: building footprint: 45% non-porous surface: 70.3% | <ul style="list-style-type: none"> None |
| FAR | <ul style="list-style-type: none"> 2.0 FAR of the total site with provision of 5% of total floor area for affordable housing units. | <ul style="list-style-type: none"> 2.0 FAR of the total site with provision of 5% of total floor area for affordable housing units. | <ul style="list-style-type: none"> None |
| Habitable Floor Elevation (Min.) | <ul style="list-style-type: none"> Residential: 2.9 m geodetic | <ul style="list-style-type: none"> Residential: 4.064 m geodetic Local exception permitted for one (1) lobby per building. | <ul style="list-style-type: none"> None |
| Height (Max.) | <ul style="list-style-type: none"> 25 m, but with specific areas allowing up to 35 m as outlined in CCAP. | <ul style="list-style-type: none"> Varies, but less than 25 m above finished grade in all cases. | <ul style="list-style-type: none"> None |

| | Bylaw Requirement With DP 12-615424 Variance | Proposed Development | | Variance |
|-----------------------|---|---|---|--|
| Setbacks (Min.) | a) 3.0m @ Alderbridge P/L b) 1.5m @ East Lane P/L c) 3.0m @ River Road P/L d) 3.0m @ Cedarbridge P/L e) 3.0m @ Gilbert P/L f) 1.5m @ West Side P/L | a) 4.8 m for Building 1 and 5.3 m Building 4 @ Alderbridge from PROP b) 0.0m @ East Lane P/L for parkade c) 3.0m @ River Road P/L d) 0.0m @ Cedarbridge Way P/L for parkade e) 0.0m @ Gilbert Road P/L f) 0.0m @ West Side P/L for parkade Based on setback to PROP/SROW on Alderbridge Way; setbacks from the actual property lines are greater. | | None under this application. Previous variance under DP 12-615424 |
| Off-Street Parking | Overall Lots 1 & 2 (Bldgs1-4) Resident: 746 (small car: 50%) Visitors: 99 (w/ 0.15/unit rate) <u>Total: 845 (w/ 7.5% TDM)</u> | Previously Proposed <u>DP 12-615424</u> Lot 1 Only (Bldgs1-2) Resident: 378 (small car: 45.5%) Visitors: 51 <u>Total: 429</u> | Currently Proposed <u>DV 13-634940</u> Lot 1 Only (Bldgs1-2) Resident: 400 (small car: 47%) Visitors: 35 <u>Total: 435</u> | To further vary the visitor parking required from 0.15 (as per DP 12-615424) to 0.10 space/unit or from 51 to 35 visitor spaces for Lot 1 only. |
| Bicycle Parking | <u>Lot 1: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 425 Visitor (0.2/unit): 68 <u>Lot 2: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 425 Visitor (0.2/unit): 68 | <u>Lot 1: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 399 Visitor (0.2/unit): 68 <u>Lot 2: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 436 Visitor (0.2/unit): 64 | | <ul style="list-style-type: none"> None |
| Loading | <ul style="list-style-type: none"> 2 medium; 2 large with one being provided for each building with sizes as per Section 7.10.2. To be on-site. | <ul style="list-style-type: none"> Required 4 SU9 loading spaces with 1 provided for each of the four buildings. | | None under this application. Previous variance under DP 12-615424 |



City of Richmond

Development Variance Permit

No. DV 13-634940

To the Holder: ONNI 7731 ALDERBRIDGE HOLDING CORP.

Property Address: 5311 AND 5399 CEDARBRIDGE WAY

Address: C/O ERIC HUGHES
300 - 550 ROBSON STREET
VANCOUVER, BC V6B 2B7

1. This Development Variance Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied by this Permit.
2. This Development Variance Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied to relax the visitor parking requirement from 0.15 spaces/unit, as per DP 12-615424, to 0.10 spaces/unit for Buildings 1 and 2 only within the that part of the development at 5311 and 5399 Cedarbridge Way.
4. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.
5. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

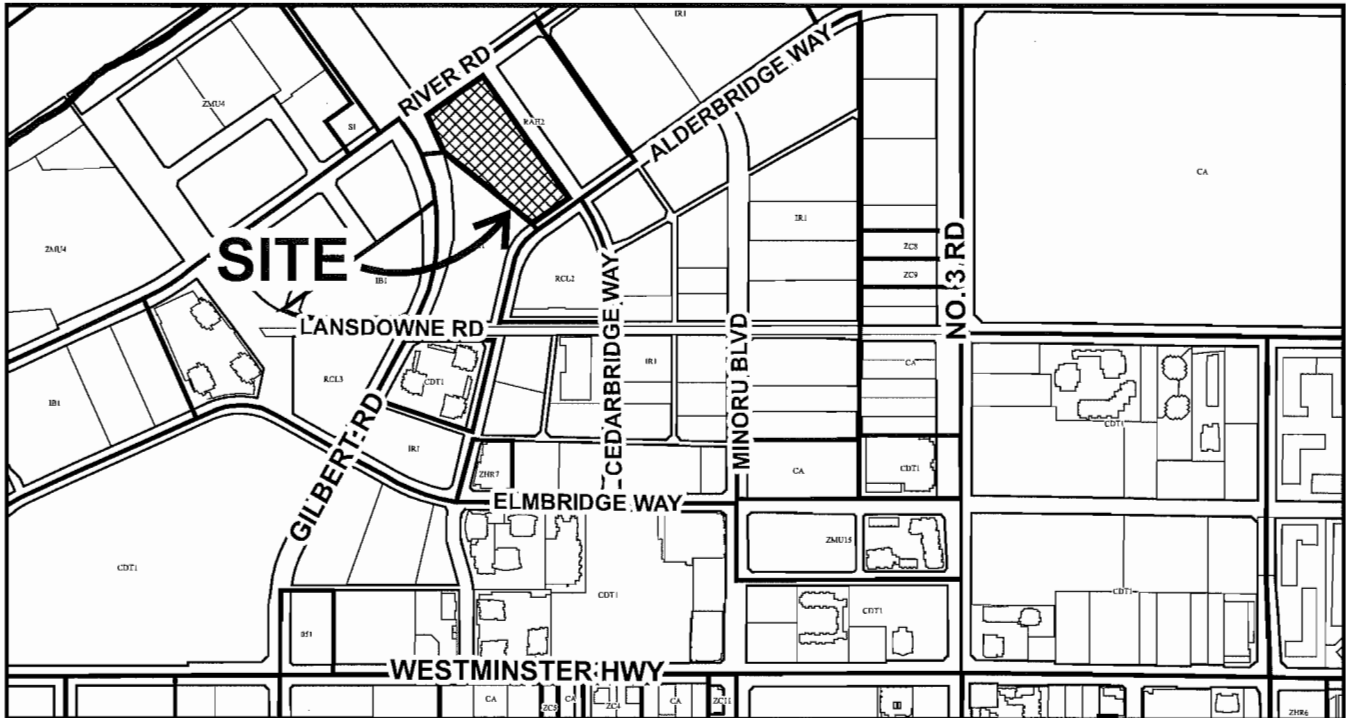
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond

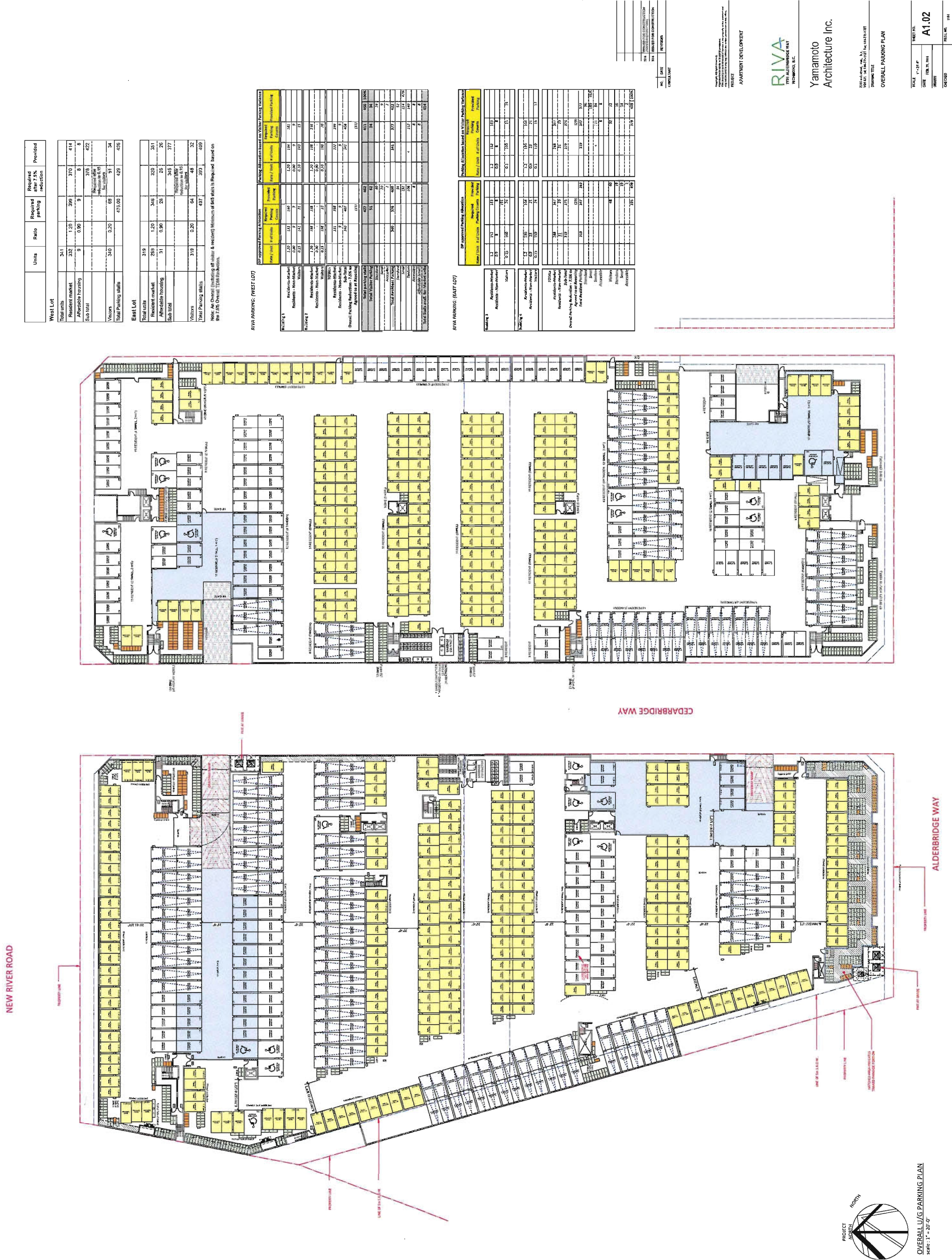


DV 13-634940
SCHEDULE "A"

Original Date: 03/26/14

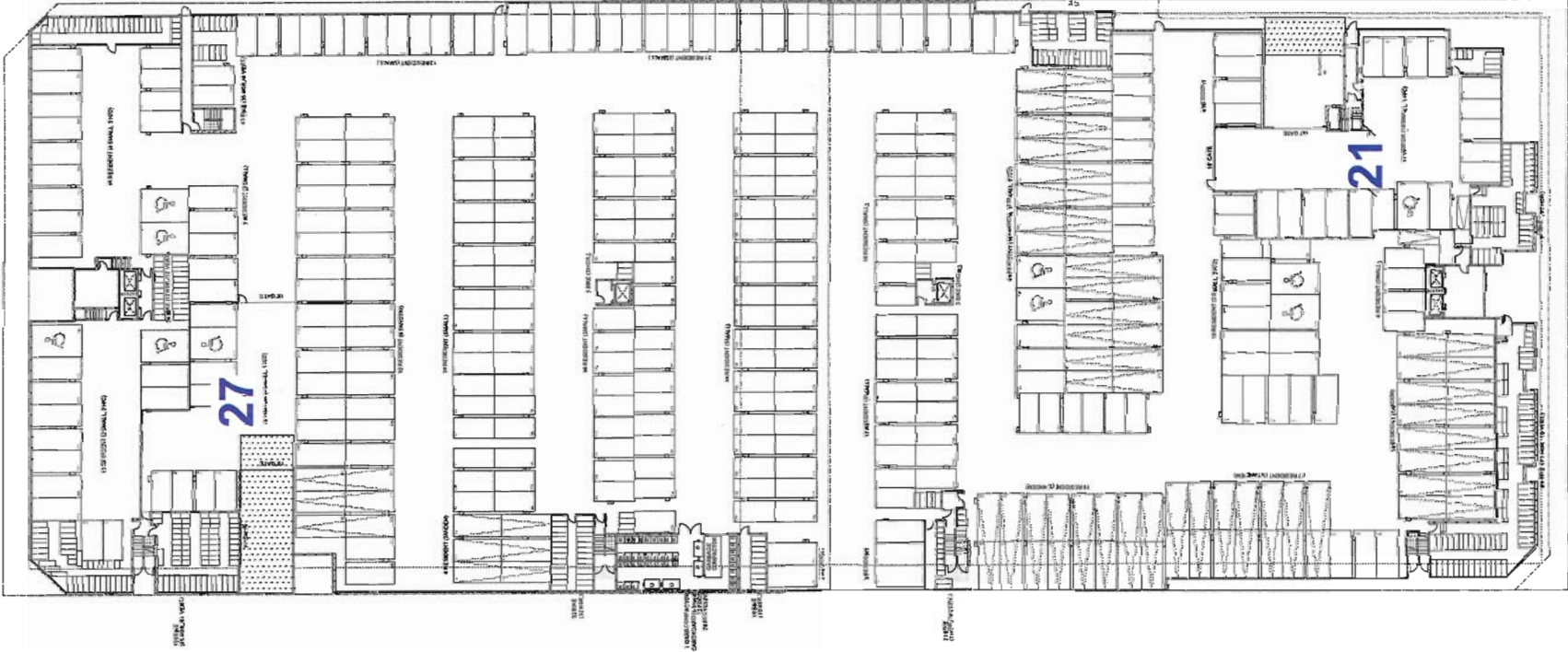
Revision Date: 06/25/14

Note: Dimensions are in METRES



PLAN -01:

RELAXATION TO 0.1 APPROVED ON THE WEST LOT.
POST OCCUPANCY STUDY CONFIRMS 0.1 SUFFICIENT.

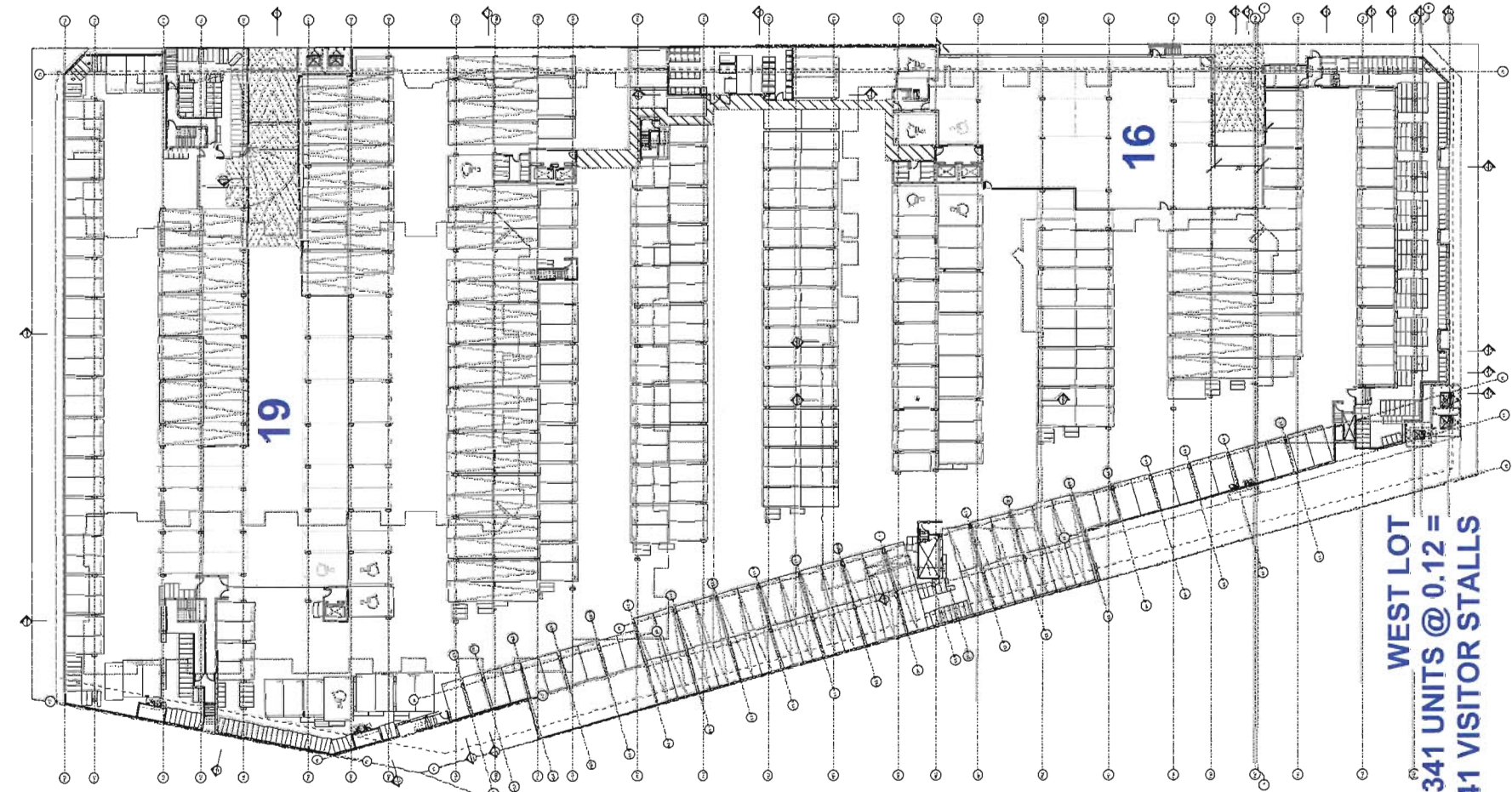


WEST LOT
341 UNITS @ 0.1 = 35
VISITOR STALLS

EAST LOT
319 UNITS @ 0.15 =
48 VISITOR STALLS

BASED ON
APPROVED DP

| NO. | DATE | ISSUE |
|---|------|-------|
| CONSULTANT | | |
| PROJECT | | |
| RIVA | | |
| Apartment development | | |
| 7731 Alderbridgeway | | |
| Richmond, B.C. | | |
| Yamamoto | | |
| Architecture Inc. | | |
| 2005 Oak Street, Vancouver, B.C. | | |
| VAN-011 TEL: 604-731-1127 FAX: 604-731-1327 | | |
| DRAWING TITLE | | |
| PARKING PLAN-1 | | |
| SCALE 1/4"=1' | | |
| SHEET NO. | | |
| A1.02-1 | | |
| DATE 12/05/2014 | | |
| DRAWN PS | | |
| CHECKED TY | | |
| PROJ. NO. 1104 | | |



PLAN -02:

RELAXATION TO 0.1 APPROVED ON WEST LOT
POST OCCUPANCY STUDY CONFIRMS 0.1 IS
INSUFFICIENT

TEMPORARY SOLUTION
SURFACE PARKING LOT TO MEET BYLAW
REQUIREMENTS WITH 8 SPACES



**8 VISITOR
PARKING STALLS**

East warehouse

89

PLAN 38045

16

19

WEST LOT
341 UNITS @ 0.12 =
41 VISITOR STALLS

35 IN THE PARKADE AND 6
TEMPORARY- EAST LOT

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PROJECT
RIVA
Apartment development

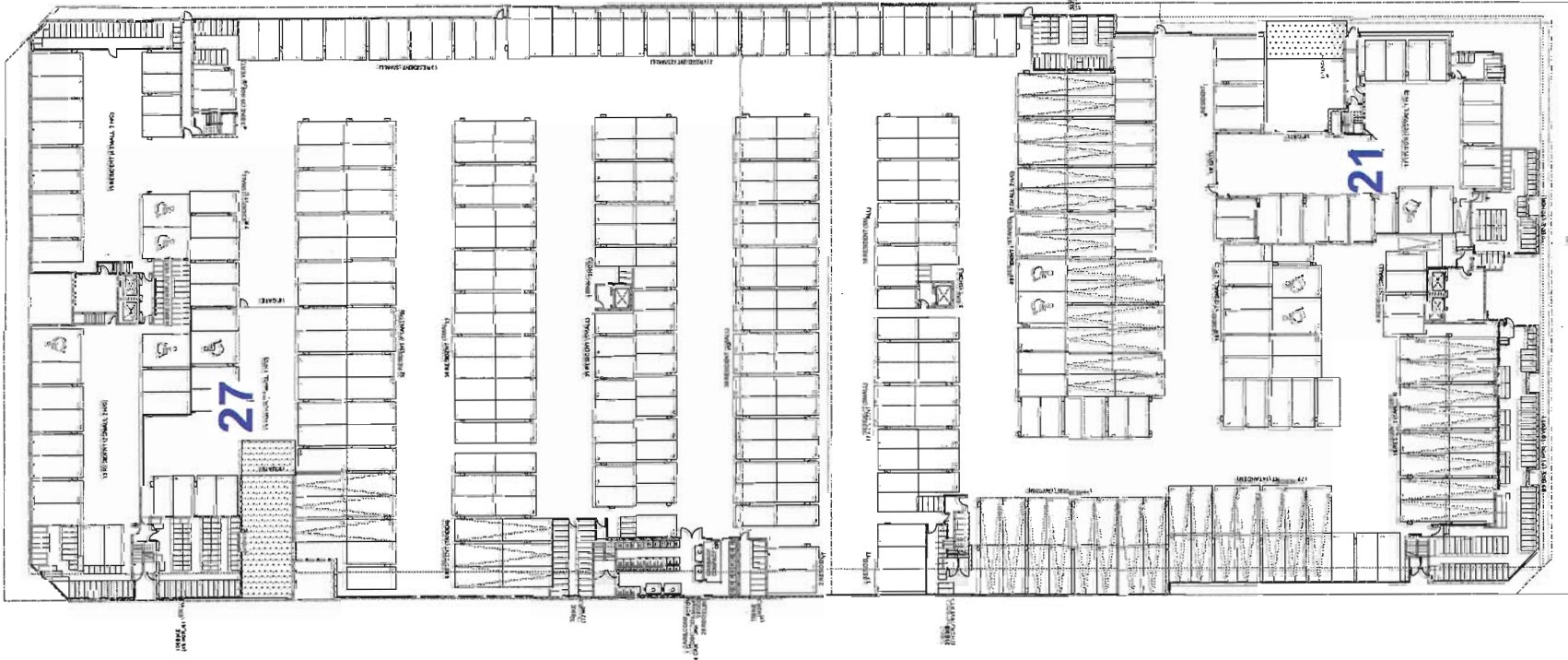
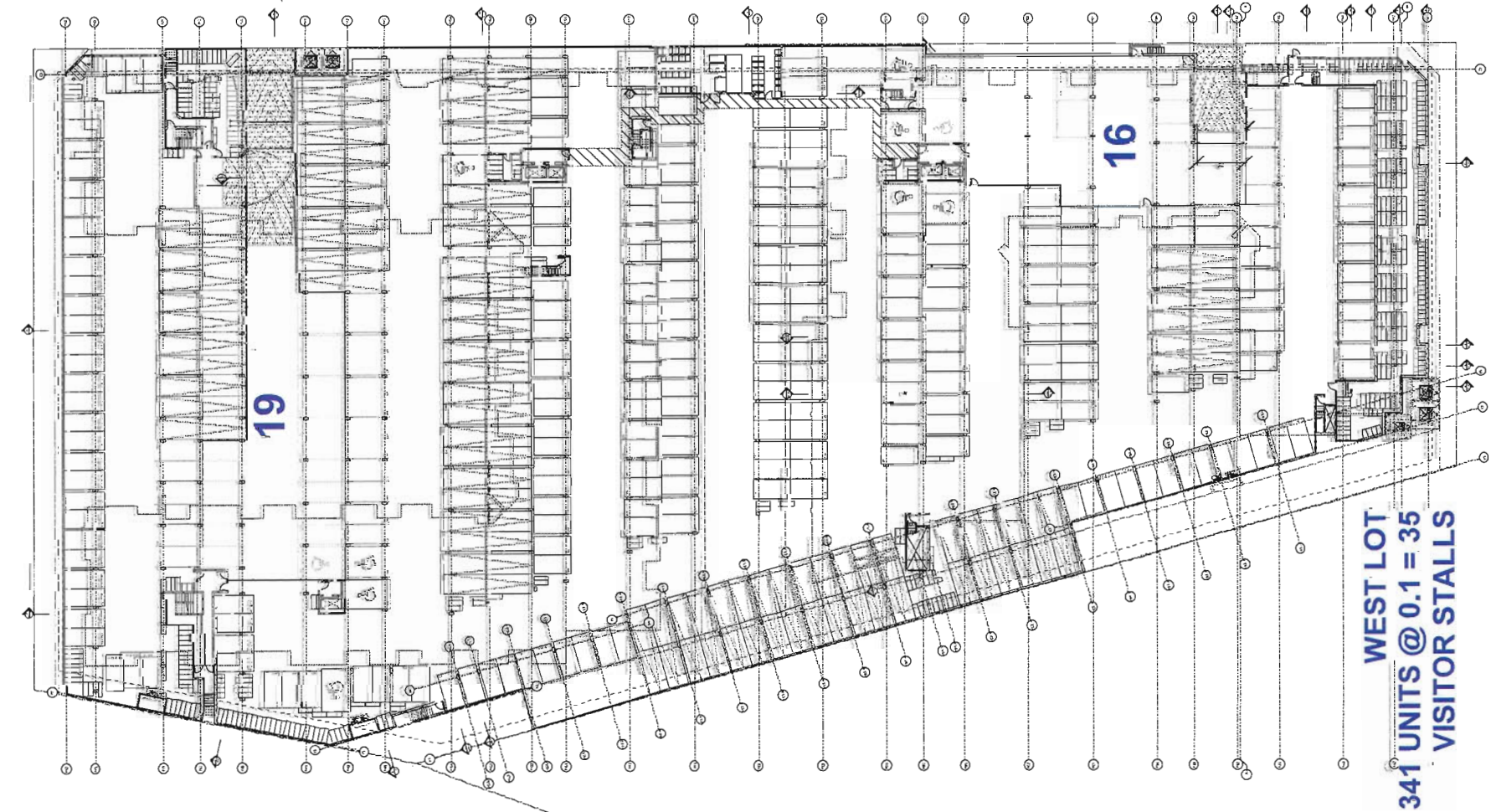
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DRAWING TITLE
PARKING PLAN-2

| | |
|-----------------|-----------------|
| SCALE 1/4"=1' | SHEET NO. |
| DATE 15/05/2014 | A1.02-II |
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PLAN -03:

RELAXATION TO 0.1 APPROVED ON WEST LOT
POST OCCUPANCY STUDY CONFIRMS 0.1 IS
INSUFFICIENT.

FINAL SOLUTION
EXISTING PARKING EASEMENT ALLOWS WEST
LOT ACCESS TO ALL VISITOR PARKING STALLS
ON THE EAST LOT

TOTAL VISITOR STALLS
WEST LOT - 35
EAST LOT - 48
TOTAL - 83

VISITOR PARKING RATE OVERALL
660 UNITS @ 0.125 =
83 VISITOR STALLS

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PROJECT
RIVA
Apartment development

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DRAWING TITLE
PARKING PLAN-3

| | |
|--------------------|------------------------|
| SCALE 1/4"=1' | SHEET NO. A1.02-III |
| DATE 15/02/2014 | DRAWN PS |
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