



**Development Permit Panel**

**Council Chambers, City Hall  
6911 No. 3 Road**

**Thursday, May 27, 2021  
3:30 p.m.**

**MINUTES**

*Motion to adopt the minutes of the Development Permit Panel meeting held on May 12, 2021.*



**1. DEVELOPMENT PERMIT 20-899883**  
(REDMS No. 6662572)

APPLICANT: Orion Construction

PROPERTY LOCATION: 3640 No. 4 Road

**Director's Recommendations**

*That a Development Permit be issued which would permit the construction of two two-storey industrial buildings at 3640 No. 4 Road on a site zoned "Light Industrial (IL)."*



**2. New Business**

**3. Date of Next Meeting: June 16, 2021**

**ADJOURNMENT**



## Development Permit Panel Wednesday, May 12, 2021

Time: 3:30 p.m.

Place: Remote (Zoom) Meeting

Present: Cecilia Achiam, Chair  
Peter Russell, Director, Sustainability and District Energy  
James Cooper, Director, Building Approvals

The meeting was called to order at 3:30 p.m.

### Minutes

It was moved and seconded

*That the minutes of the meeting of the Development Permit Panel held on April 28, 2021 be adopted.*

**CARRIED**

### 1. **DEVELOPMENT PERMIT 18-829082** (REDMS No. 6616241 v. 5)

APPLICANT: Forest International Real Estate Investment Company Ltd.

PROPERTY LOCATION: 10231, 10251, 10351, 10371, 10391, 10395 and 10397 No. 2 Road

## Development Permit Panel

### Wednesday, May 12, 2021

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#### INTENT OF PERMIT:

1. Permit the construction of 22 townhouse units at 10231, 10251, 10351, 10371, 10391, 10395 and 10397 No. 2 Road on a site zoned “Low Density Townhouses (RTL4); and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to reduce the front yard setback along No. 2 Road from 6.0 m to 4.83 m.

#### Applicant's Comments

Jiang Zhu, Imperial Architecture, with the aid of a visual presentation (copy on file, City Clerk's office), provided background information on the proposed development, including among others, its site context, design rationale, site layout, building elevations and sections, façade treatments, and building materials and colours, highlighting the following:

- three three-storey buildings are proposed at the front (along No. 2 Road) and five two-storey buildings in duplex form are proposed along the rear of the site;
- the end unit of the three-storey building adjacent to the south property line has been stepped down to two-storeys to provide an appropriate transition to the adjacent single-family homes;
- a separate utility building is proposed which includes an electrical room and garbage and recycling room;
- the proposed development includes four ground-level secondary suites and two convertible units;
- a single driveway provides vehicle access to the site from No. 2 Road and a north-south internal drive aisle is proposed;
- there is a statutory right-of-way over the internal drive aisle to provide shared access and connection to future developments to the north and south of the subject site;
- a front yard setback variance is proposed along No. 2 Road to increase the rear yard setback;
- a side-by-side double car garage is proposed for all units;
- two colour schemes are proposed including a cold tone and a warm tone;
- proposed building materials include, among others, durable materials such as natural brick and hardie panels; and
- the proposed shared outdoor amenity area is centrally located.

## Development Permit Panel

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Meredith Mitchell, M2 Landscape Architecture, provided an overview of the main landscape features of the project, noting that (i) alternating conifer and deciduous trees are proposed along the streetscape, (ii) each unit will either have a front or rear private outdoor space which includes a yard with a patio, shade tree and planting, (iii) permeable paving treatment is proposed for the driveway and along the entire internal drive aisle, with vehicle and pedestrian circulation areas separated through different paving patterns, (iv) landscaping is proposed around the utility building, and (v) a pedestrian walkway is provided at the driveway entry and along the south side of the subject site to provide pedestrian access to the interior of the site.

In addition, Ms. Mitchell noted that (i) trees and pockets of planting are proposed along the internal drive aisle, (ii) seven trees and a hedgerow in good condition located on neighbouring properties to the west will be retained and protected, (iii) a stepped yard system is proposed on the rear yards of majority of back units to provide protection to existing trees to be retained and enhance the usability of rear yards, (iv) the shared outdoor amenity includes, among others, a children's play area, a feature tree, a seating and barbeque area, a bike rack, lawn area and planting, (v) a low retaining wall and perimeter fencing is proposed along the north property line, and (vi) a low retaining wall with wood fencing on top is proposed along a portion of the south property line.

In reply to queries from the Panel, Mr. Zhu and Ms. Mitchell acknowledged that (i) a texture change in permeable paving treatment at the corner of the driveway and internal drive aisle is intended to provide traffic calming, (ii) there is adequate turning radius for garbage and recycling trucks at the corner of the driveway and internal drive aisle to access future developments to the north and south of the subject site, (iii) the applicant is committed to achieve the City requirements for the width of the entry doors for the convertible units, (iv) the patios at the rear yards of convertible units are accessible to people in wheelchairs, (v) signage will be installed to assist in wayfinding and provide identification to individual front entries of rear units, (vi) appropriate measures including installation of tree protection barriers will be installed to protect existing trees to be retained, (vii) the unit walkways and patios for the front units have been designed to be accessible, and (viii) the corners on the third floor of the front buildings have been recessed to break down their massing.

In reply to further queries from the Panel, Mr. Zhu noted that (i) the project's lighting plan includes installation of exterior downward focused wall-mounted lighting and bollard lighting which will not cause light pollution, (ii) air source heat pumps will be installed at the back of the rear units and on the roof of the front units, and (iii) landscaping will be installed to provide screening for the air source heat pumps on the ground level of the rear units.

It was noted that in addition to landscaping, the applicant will consider installing a solid acoustic barrier for the heat source heat pumps at ground level. Staff was then directed to work with the applicant to investigate opportunities to provide the proposed acoustic barrier.

## Development Permit Panel

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### Staff Comments

Wayne Craig, Director, Development, noted that (i) there is a Servicing Agreement associated with the project which include the construction of a new sidewalk and boulevard along the No. 2 Road frontage, (ii) the Servicing Agreement also includes the installation of a channelized median within the driveway letdown to ensure that vehicle entry/exit is limited to right-in/right-out, (iii) the proposed front yard setback variance would provide a larger rear yard setback and allow the retention of trees and hedges along the west property line, (iv) the distance from the back of curb to the building face would be approximately eight meters, and (v) an acoustical report was provided by the applicant confirming that the units will achieve Canada Home and Mortgage Corporation (CHMC) noise standards.

In reply to a query from the Panel, Mr. Craig confirmed that the proposed development achieves the grandfathering provision for Energy Step Code approved by Council.

### Gallery Comments

None.

### Correspondence

None.

### Panel Discussion

### Panel Decision

It was moved and seconded

*That a Development Permit be issued which would:*

1. *permit the construction of 22 townhouse units at 10231, 10251, 10351, 10371, 10391, 10395 and 10397 No. 2 Road on a site zoned "Low Density Townhouses (RTL4); and*
2. *vary the provisions of Richmond Zoning Bylaw 8500 to reduce the front yard setback along No. 2 Road from 6.0 m to 4.83 m.*

**CARRIED**

2. **Date of Next Meeting: May 27, 2021**

3. **Adjournment**

4.

**Development Permit Panel**  
**Wednesday, May 12, 2021**

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It was moved and seconded

*That the meeting be adjourned at 4:16 p.m.*

**CARRIED**

Certified a true and correct copy of the  
Minutes of the meeting of the  
Development Permit Panel of the Council  
of the City of Richmond held on  
Wednesday, May 12, 2021.

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Cecilia Achiam  
Chair

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Rustico Agawin  
Committee Clerk



City of  
Richmond

## Report to Development Permit Panel

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**To:** Development Permit Panel

**Date:** May 5, 2021

**From:** Wayne Craig  
Director, Development

**File:** DP 20-899883

**Re:** Application by Orion Construction for a Development Permit at 3640 No. 4 Road

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### Staff Recommendation

That a Development Permit be issued which would permit the construction of two two-storey industrial buildings at 3640 No. 4 Road on a site zoned "Light Industrial (IL)."

Wayne Craig  
Director, Development  
(604-247-4625)

WC:jr  
Att. 3

## **Staff Report**

### **Origin**

Orion Construction has applied on behalf of Halsbury Holdings Ltd. to the City of Richmond for permission to develop two two-storey industrial buildings at 3640 No. 4 Road on a site zoned “Light Industrial (IL)” with vehicle access from No. 4 Road. The site currently contains a single two-storey building, which would be demolished.

The proposed development consists of 13 industrial units with either a second storey or mezzanine, which would be strata-titled upon completion. As such, the application is for the exterior shell of the building only, and individual tenant improvement applications will be required for finishings in each of the units.

A Servicing Agreement is required as a condition of Building Permit issuance and includes, but is not limited to, the following improvements:

- New sidewalk and landscaped boulevard on the No. 4 Road frontage.
- New sidewalk and landscaped boulevard on the Kilby Drive frontage. The new sidewalk is to be located along the property line, except adjacent to tree protection zones the sidewalk may be located at the back of curb.
- Decommissioning of the sanitary sewer on the property and replacement with a new sanitary sewer located within the roadway.

### **Development Information**

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

### **Background**

Development surrounding the subject site is as follows:

- To the north and east, a self storage facility containing a number of one- and two-storey buildings on a property zoned “Light Industrial (IL)” with vehicle access from No. 4 Road.
- To the east, a single-family dwelling on a property zoned “Single Detached (RS1/A)” with vehicle access from Kilby Drive.
- To the south, two-storey and three-storey townhouses on properties zoned “Low Density Townhouses (RTL1)” with vehicle access from Kilby Drive.
- To the west, across No. 4 Road, single-family dwellings on properties zoned “Single Detached (RS1/B)” with vehicle access from Kilby Drive and Kilby Court.



**Staff Comments**

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan (OCP) and is generally in compliance with the “Light Industrial (IL)” zone.

**Advisory Design Panel Comments**

On March 17, 2021, the Advisory Design Panel (ADP) supported the subject Development Permit application moving forward to Development Permit Panel, subject to the applicant giving consideration to the ADP’s comments. A copy of the relevant excerpt from the ADP minutes is attached for reference (Attachment 2), together with the applicant’s responses in *‘bold italics.’*

**Analysis*****Development Site***

- The applicant has completed a Site Profile for the property, which has been reviewed by the Ministry of Environment. Prior to Development Permit issuance, the applicant must obtain a Certificate of Compliance or other acceptable instrument from MOE regarding potential site contamination issues.
- The subject site is located in Flood Construction Level Area A. Prior to Development Permit issuance, a restrictive covenant must be registered on title to address public awareness and identify the minimum flood construction level.
- The subject site is located in Aircraft Noise Area 3. Prior to Development Permit issuance, a restrictive covenant must be registered on title to address public awareness and ensure noise mitigation is incorporated into the design and construction of buildings and additions, as required.
- A minor 4.0 m by 4.0 m corner cut road dedication is required at the intersection of No 4 Road and Kilby Drive to accommodate the requested frontage improvements and maintain intersection sightlines.

***Conditions of Adjacency***

- The proposed south building abuts a residential-zoned property to the east. The building setback has been increased to 6.0 m for the main building and 3.4 m for the electrical closet, and the mature trees and hedges between the building and the single-family dwelling are proposed to be retained. No grade change is proposed along this interface as it is entirely within the tree protection zone. A 1.8 m tall wood fence is proposed along this portion of the property line to maintain a residential character.
- Windows on the upper storey of the southeastern unit are located in the double-height area, and are not accessible from the mezzanine.
- The proposed north building abuts an industrial-zoned property to the north and east. A retaining wall is proposed along the east property line as the site grade will be raised. The

retaining wall would be approximately 0.6 m (2 ft.) tall, inclusive of a 0.2 m lip above the pavement surface. A vinyl coated chain link fence is proposed on top of the retaining wall.

- The north building is proposed with a zero lot line setback on the north property line, which is permitted between abutting industrial zones. The building wall includes inward articulation at each corner and colour blocking across the entire elevation to provide visual interest and avoid an expansive blank façade.
- The applicant has provided a shadow study demonstrating the impacts of the proposal of neighbouring properties. These impacts are minimal through most of the year.
- The applicant has provided an Acoustic Report addressing the noise levels generated by activities inside the building and the rooftop mechanical units. The Report confirms that the proposed building insulation and location of the mechanical units would reduce noise to levels compliant with Richmond Noise Regulation Bylaw.

### ***Urban Design and Site Planning***

- The proposal consists of two two-storey buildings accessed via a central drive aisle. All parking is located off of this drive aisle, and a loading space is provided at the rear of the site. Vehicle maneuvering is accommodated through a small turning area opposite the loading space, allowing vehicles to perform a 3-point turn to exit the site in a forward motion.
- A total of 44 parking spaces are provided, however some of these spaces are not useable when the overhead doors are being used for loading purposes. The applicant has confirmed that 16 spaces, including the van-accessible parking space, are located outside of the overhead doors. The minimum Bylaw requirement is 11 vehicle parking spaces.
- Class 2 bicycle parking for visitors and customers is provided via bike racks in two locations flanking the driveway entrance. Class 1 bicycle parking for tenants would be provided inside each unit.
- The garbage and recycling enclosure is located at the northeast corner of the site, away from the adjacent single-family dwellings. The loading space is immediately in front of this enclosure.
- The site planning and building envelopes are a direct response to the retention of 17 trees on the property. In particular, the south building is articulated to provide an adequate setback from the critical root zone of the trees.
- Each of the units has both a door and overhead loading door, which open on to the walkway and drive aisle. Three of the units have complete second stories, while the remainder would have mezzanines. The applicant is required to register legal agreements on title restricting stratification of the second-storey floor area and notifying all purchasers of the industrial classification of the building.

***Architectural Form and Character***

- Tilt-up concrete panels are proposed, which is typical of light industrial development.
- Each building is visually broken into smaller components through use of projecting canopies and colour blocking. The canopies provide both modest weather protection for the walkway and solar screening for the upper storey windows.
- The colour scheme is primarily white, black, and grey, with occasional accents in red. This palette provides a neutral backdrop to the landscaping at the edges of the site.
- The applicant was encouraged to consider a mural treatment on the north elevation, where the building would be visible from the street until the neighbouring property redevelops. In exploring this option the applicant decided to visually break up this elevation through colour blocking treatments. The applicant is making a \$10,224.36 contribution to the City's Public Art Fund in lieu of providing art on site in accordance with Council policy.

***Landscape Design and Open Space Design***

- 17 Cedar trees on the property are proposed to be retained. Four trees are located in the rear yard between the proposed building and the adjacent single-family dwelling, and 13 trees are located in the Kilby Drive frontage. The design and construction of the new sidewalk will need to accommodate retention of these trees. An \$85,000 Tree Survival Security and proof of a contract with a Certified Arborist for supervision of works within Tree Protection Zones are required prior to issuance of the Development Permit. A post-construction impact assessment report is required and, if the report determines the trees are in good health, up to 90% of the security will be returned to the applicant following and inspection by the City, with the remainder held for up to one year to ensure the trees survive.
- One Chestnut tree on the property is proposed to be removed due to conflicts with the proposed driveway location. This tree is in fair condition and has been cut back to provide clearance to the overhead hydro lines. The ongoing need to prune back the canopy does not make this tree a good candidate for retention.
- The proposed on-site landscaping includes nine Persian Ironwood trees. A variety of shrubs and groundcovers are proposed within planting beds in the setbacks between the buildings and sidewalk. A high efficiency irrigation system will be installed in all landscaped areas.
- Prior to issuance of the Development Permit, the applicant is required to submit a \$31,471 Landscape Security. A post-construction report is required and, if the report determines the agreed upon landscaping was installed and is in good condition, up to 90% of the security will be returned to the applicant following an inspection of the landscape works, and the remainder will be held for up to one year to ensure the landscaping survives.
- Additional trees will be planted in the City Boulevard. The size, species, and location of these street trees will be determined by Parks staff through the Servicing Agreement design review process.
- A 1.5 m wide pedestrian walkway is proposed along the front of each building, and provides direct pedestrian access from the sidewalk into the site. Bollards will be installed in front of

each parking space to prevent vehicle conflicts. Bollards would be removable to allow loading access to the individual units.

### ***Crime Prevention Through Environmental Design***

- The site has a single access point for both vehicles and pedestrians. Pedestrian connections to Kilby Drive intended as building exits only.
- A perimeter fence is provided at the east side of the site. The fence has been designed to have a residential character where it abuts the neighbouring single-family dwelling. The fence ties in to the buildings to secure the site.
- Building-mounted lighting is provided in along the drive aisle and at unit entries. Lighting has been designed to reduce spillover to adjacent sites and minimize evening light pollution.

### ***Accessibility***

- One wheelchair accessible parking space is provided near the site entrance where it is most visible. The landing area outside of each door is enlarged slightly to provide adequate maneuvering space for wheelchair users.
- Each industrial unit includes a ground floor accessible washroom.

### ***Sustainability***

- There is no BC Energy Step Code requirement for industrial buildings.
- High performance building materials are proposed, including insulated tilt-up panels, double glazed windows with e-coating, and insulated overhead doors. These are intended to address both thermal and acoustic performance of the buildings.
- Building heating and cooling will be provided by individual rooftop mechanical units. Potential locations for these units are shown on the plans, however the applicant will not be providing these units in the proposed building shell construction. Selection and installation of rooftop mechanical units will be through the tenant improvement Building Permit processes for each strata unit.
- Outlets capable of providing Level 2 EV charging are proposed in each of the industrial units.
- Skylights are provided in each industrial unit, reducing the need for indoor lighting.

**Conclusions**

The proposed development is consistent with the City's objectives for the subject site as set out in the OCP and Zoning Bylaw. The list of Development Permit Considerations is provided in Attachment 3. On this basis staff recommend that the Development Permit be endorsed, and issuance by Council be recommended.



Jordan Rockerbie  
Planner 1  
(604-276-4092)

JR:js/blg

**Attachments:**

Attachment 1: Development Application Data Sheet

Attachment 2: Advisory Design Panel Meeting Minutes Excerpt

Attachment 3: Development Permit Considerations



**DP 20-899883**

**Attachment 1**

Address: 3640 No. 4 Road

Applicant: Orion Construction

Owner: Halsbury Holdings Ltd.

Planning Area(s): West Cambie

Floor Area Gross: 3,916.90 m<sup>2</sup> (42,161 ft<sup>2</sup>)

Floor Area Net: 3,862.27 m<sup>2</sup> (41,584 ft<sup>2</sup>)

	Existing	Proposed
<b>Site Area:</b>	4,618 m <sup>2</sup>	4,610 m <sup>2</sup>
<b>Land Uses:</b>	Light industrial	No change
<b>OCP Designation:</b>	Mixed Employment	No change
<b>Zoning:</b>	Light Industrial (IL)	No change

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 1.0	0.85	None permitted
Lot Coverage:	Max. 75%	52%	None
Setback – Front Yard (West):	Min. 3.0 m	3.0 m	None
Setback – Side Yard (North):	Min. 0 m	0 m	None
Setback – Exterior Side Yard (South):	Min. 3.0 m	3.0 m	None
Setback – Rear Yard (East):	Min. 0 m	3.4 m	None
Height (m):	Max. 12.0 m	11.0 m	None
Off-street Parking Spaces – Small Car:	Max. 50%	20 (45%)	None
Off-street Parking Spaces – Accessible:	Min. 1	1	None
Total off-street Spaces:	Min. 14	44	None
Bicycle Parking – Class 1:	Min. 11	Min. 11	None
Bicycle Parking – Class 2:	Min. 11	12	None
Loading:	Min. 1 medium space	1 medium space	None

Excerpt from the Minutes from  
**The Design Panel Meeting**

Wednesday, March 17, 2021 – 4:00 p.m.  
Remote (WebEx)

**1. DP 20-899883 – TWO TWO-STOREY INDUSTRIAL BUILDINGS**

ARCHITECT: KCC Architecture & Design Ltd.

LANDSCAPE ARCHITECT: KD Planning & Design Ltd.

PROPERTY LOCATION: 3640 No. 4 Road

**Applicant's Presentation**

Architect Karla Castellanos, KCC Architecture and Design Ltd., and Landscape Architect Jessica Thiessen, KD Planning and Design, presented the project and together with Jack Priestley, Orion Construction, and Brent Hanson, South Street Development, answered queries from the Panel.

**Panel Discussion**

*Comments from Panel members were as follows:*

- consider relocating the accessible parking stall to the middle parking stall of Building B to make it more accessible to all units in the proposed development;

*It is unfortunately not practical to relocate the accessible parking stall without compromising on safety as the stall is located in the only location that is not in front of an overhead door. All parking spaces in front of entranced doors are small parking spaces. However, particular care has been taken to ensure the grading in the existing location is suitable for wheelchair loading and unloading. We understand that the accessible space is also within acceptable distances to all building entrances.*

- consider providing notification to future tenants of the industrial units to install an elevator should they decide to use the mezzanine as an office space;

*If the mezzanine is used as office space, a HC accessible flex office and other facilities will need to be provided on the main floor to meet HC accessibility guidelines and code requirements. There are elevator options available to fit this application however are not regularly utilised.*



- appreciate the design rationale for the massing and colour treatment and the breaking down of the building blocks;

*Noted, thanks.*

- concerned about the design of the north building's (Building A's) north elevation, i.e., its openings, colour treatment, finishes and balconies should the future development on the neighbouring property to the north include no setback; the applicant needs to review the design of the north façade to provide an appropriate interface with the future development to the north;

*The potential of a mural on the north elevation of Building A has been explored with the City's Public Art department. A number of potential options were discussed and, in a balanced and reasonable approach to improve the interface of the north wall to the public realm, it is proposed to continue the project's paint colour theme, adding a band of ruby red, black and gray as well as an architectural form liner to the panels themselves. (Refer Elevations A4.1). Additionally, as part of the City's development cost charges, the project will contribute approximately \$10,000 to the City's public art fund.*

*It is acknowledged that there is a potential for the site to the north to redevelop to a zero lot setback however given the considerable area of that site (229,701 sq ft) there are considerable design options available. The benefits to the building design and future occupants of this project by providing the balconies outweighs the potential future impacts.*

- consider enhancing the corner treatment of the south building (Building B) as the south elevation appears like a blank wall with limited openings and is less articulated than the west elevation; the applicant needs to review the west façade treatment including the height of the parapet;

*The south elevation of Building B will be hidden from view of the street by the existing vegetation (which will be retained). In addition, the parapet height at the southwest corner has been increased to enhance and create additional articulation along this elevation. (Refer revised elevations).*

- review the site plan in relation to firefighting provisions of the BC Building Code to ensure it meets the Code requirements; the applicant is encouraged to consult with the Fire Department at this stage of the project;

*The fire code requirements have been reviewed in further detail during the preparation of Building Permit drawings and determined to be in full compliance with the BCBC.*

- appreciate the building massing, site layout, retention of existing trees, the central driveway and the delineation of vehicular and pedestrian circulation;

*Noted, thanks.*

- consider introducing more outdoor amenities along the central driveway; also consider enhancing the pedestrian area in front of the garage doors, e.g. provide usable common outdoor space;



*The site is tight and there is inadequate space to provide dedicated outdoor amenities. However, the retention of virtually all of the trees, provision of further landscaping and an attractive building design will ensure the project is a pleasant place to work.*

- appreciate the applicant retaining as many mature trees as possible on the subject site;

*Noted, thanks.*

- the location of east-facing windows on the mezzanine level of the south building (Unit 7) and not on a full second storey and the locating the stairs against the east façade without the use of glazing address the concern regarding overlook to the neighbouring residential property to the east;

*Noted, thanks. The building has been designed to be a part of the surrounding neighbourhood.*

- appreciate the applicant's approach to the interim condition of the north façade as a future development on neighbouring property could have no setback; like proposal to paint the middle portion of the north façade of the north building (i.e., gridlines B to F) to provide visual interest to the exposed concrete wall; also appreciate the balconies at the east and west ends of the north elevation;

*Noted, thanks.*

- colour-blocking appears a bit random at moments, e.g. red stripes on the east and west elevation don't go far enough; consider opportunities where there are changes in plane to fill in the colours, e.g. red infill on the west façade in locations near the balcony;

*To keep the appeal and element of surprise of the colour red, its application around the building should remain limited. However, the application of red has been increased on the west, east and north elevations and Building B interior elevation.*

- support the use of proposed colours; however, reconsider the use of vintage taupe colour as it looks like dirty cream and not visually appealing;

*The cream colour has been deleted.*

- consider redesigning the cantilevered tilt-up panel at the two balconies on the west end of the north and south buildings to reinforce the architecture and further imbed the massing of the two buildings;

*It is our strong position that the cantilevered extensions are an essential component of the building character and provide a unique feature which will differentiate the building.*

- consider using thicker material for the canopies along the interior elevation of the two buildings to avoid oil canning appearance if sheet metal cladding will be used for the canopies;

*This comment has been noted and the material's thickness will be considered and specified in the building specifications at the building permit stage.*

- consider reinforcing the colour-blocking strategy to emphasize the balcony projections on the west and northeast;

*Noted. The colour theme has been further enhanced.*

- commend the applicant for the proposed tilt up building construction as it is challenging to make it attractive;

*Noted, thanks.*

- like the play on colours; consider introducing colours on the interior elevations of the north and south buildings as they are prominent façades and face the parking lots, e.g. introduce red colour to the garage doors to provide more visual interest;

*To keep the appeal and element of surprise of the colour red, its application around the building should remain limited. However, the application of red has been increased on the west, east and north elevations and Building B interior elevation.*

- consider providing articulation to the southeast corner of the south building adjacent to the single-family dwellings as it looks very simple as opposed to the other corner treatments of the two buildings;

*The parapet height at the southwest corner has been increased to enhance and create additional articulation along this elevation. (Refer revised elevations).*

- consider further articulating and animating the south façade of the south building to visually break down the large blank wall and enhance the pedestrian experience along Kilby Drive;

*The parapet height at the southwest corner has been increased to enhance and create additional articulation along this elevation. (Refer revised elevations).*

- appreciate the applicant's response to the concern with regard to the proposed treatment of the north façade; expect to see some colour changes to the north facade;

*Noted, thanks. The potential of a mural on the north elevation of Building A has been explored with the City's Public Art department. A number of potential options were discussed and, in a balanced and reasonable approach to improve the interface of the north wall to the public realm, it is proposed to continue the project's paint colour theme, adding a band of ruby red, black and gray as well as an architectural form liner to the panels themselves. (Refer Elevations A4.1). Additionally, as part of the City's development cost charges, the project will contribute approximately \$10,000 to the City's public art fund.*

- consider incorporating planting beds and introducing a different paving treatment for the parking stalls on the parking lots of the two buildings;

*Differentiation in paving treatments is included with the use of concrete at unit entrances and asphalt in the parking area. Virtually all trees have been retained on site and additional landscaping has been provided around the perimeter of the building. As a result of this, the small-scale nature of the building, and the architectural features included, the current approach provides for a varied and appealing appearance.*

- materials for garbage enclosures and provision for necessary structures for large trucks to turn and maneuver on are not indicated on the architectural drawings; note that pavers are more resilient than asphalt for turning and manoeuvring of large trucks;

*As part of the City's Development Permit review process, a separate manoeuvring study with vehicle swept paths and garbage collection requirements have been met. We will*



*investigate the option of incorporating pavers however at this time we intend to proceed with asphalt surface.*

- consider installing full landscape beds along the south frontage as shrub planting can better protect the trees;

*Additional landscaping has been included on the south frontage as suggested. Refer Landscape Plan.*

- consider providing more details with regard to the concrete retaining wall at the east end (adjacent to the tree protection zone) to determine if additional treatment is necessary to prevent encroachment by large trucks using the maneuvering space;

*The retaining wall will include an 8" kerb, will be painted safety-yellow.*

- consider introducing a mural treatment on the north façade;

*The potential of a mural on the north elevation of Building A has been explored with the City's Public Art department. A number of potential options were discussed and, in a balanced and reasonable approach to improve the interface of the north wall to the public realm, it is proposed to continue the project's paint colour theme, adding a band of ruby red, black and gray as well as an architectural form liner to the panels themselves. (Refer Elevations A4.1). Additionally, as part of the City's consideration the project will contribute approximately \$10,000 to the City's public art fund.*

- note that there are two exit doors for the two units along Kilby Drive; consider installing an outdoor amenity, e.g. a picnic table, for these units;

*These doors are provided predominately for fire safety egress. The site is tight and there is inadequate space to provide dedicated outdoor amenities. However, the retention of virtually all of the trees, provision of further landscaping and an attractive building design will ensure the project is a pleasant place to work.*

- overall, appreciate the massing and the architectural form and character which does not contrast dramatically with the surrounding single-family residential neighbourhood; also like the proposed corner treatments;

*Noted, thanks. The building has been designed to be a part of the surrounding neighbourhood.*

- appreciate the retention of existing trees and the project's sensitivity to the neighbours;

*Noted, thanks. The building has been designed to be a part of the surrounding neighbourhood.*

- consider screening the rooftop mechanical units to hide them from the view of neighbouring developments;

*All rooftop mechanical equipment will be hidden from view from the street and neighbouring residential buildings by being setback on the roof and additionally through the retention of mature trees.*

- appreciate the project's proposed sustainability measures, e.g. insulation for tilt-up panels; however, (i) consider opportunities for installing passive cooling, e.g. installing

skylights or opening of overheads, (ii) provide more details/information on energy efficiency, e.g. how to subzone units with mezzanine and full second storey levels which are two different levels and potential sharing of energy among the various units, and (iii) clarify if the rooftop mechanical units are Heat Recovery Ventilator (HRV) units; and

*The building design at this time relates to the building shell only and it would be difficult to coordinate the requirements for each tenant to ensure the sustainable features will be useable or implemented once the building is fully occupied. The skylights have however been increased in size from 4' x 4' to 6' x 6'.*

- consider providing more details on the site plan with regard to meeting the requirements of the BC Fire Code.

*The fire code requirements have been reviewed in further detail during the preparation of Building Permit drawings and determined to be in full compliance with the BCBC.*

### **Panel Decision**

It was moved and seconded

*That DP 20-899883 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Advisory Design Panel.*

**CARRIED**



**Address:** 3640 No. 4 Road

**File No.:** DP 20-899883

**Prior to a Development Permit being forwarded to the Council for issuance, the developer is required to:**

1. Receipt of Ministry of Environment (MOE) Certificate of Compliance or alternative approval to proceed granted from MOE regarding potential site contamination issues.
2. Dedication of a 4.0 by 4.0 m corner cut road dedication at the intersection of No 4 Road and Kilby Drive.
3. Submission of a Landscape Security to the City in the amount of \$31,471, inclusive of a 10% contingency (as per the landscape cost estimate provided by KD Planning and Design Ltd. on April 26, 2021). Up to 90% of the security will be returned following an inspection of the works, with the remainder held for up to one year to ensure the agreed upon plantings survive.
4. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
5. Submission of a Tree Survival Security to the City in the amount of \$85,000 for the 17 trees to be retained. Up to 90% of the security will be returned following an inspection of the works, with the remainder held for up to one year to ensure that the trees survive.
6. City acceptance of the developer's offer to voluntarily contribute \$0.26 per buildable square foot (i.e. \$10,224.36) to the City's Public Art Fund.
7. Registration of an aircraft noise indemnity covenant on Title (Area 3).
8. Registration of a flood indemnity covenant on Title (2.9 m GSC - Area A).
9. Registration of a legal agreement on title identifying that the property is located in an industrial area and that use of the property must be consistent with the Zoning Bylaw, and requiring that the owner provide an acknowledgement of the same in all purchase and sale agreements.
10. Registration of a legal agreement on title restricting the horizontal stratification of units.

**Prior to Building Permit\* Issuance, the developer must complete the following requirements:**

1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
2. Obtain a Building Permit\* (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.
3. Enter into a Servicing Agreement\* for the design and construction of engineering infrastructure improvements. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to, the following:

Water Works:

- a) Using the OCP Model, there is 236 L/s and 190 L/s of water available at the No. 4 Road frontage and Kilby Drive frontage respectively, at a 20 psi residual, under the build out scenario. Based on your proposed development, your site requires a minimum fire flow of 250 L/s. To attain adequate fire flows, tie-in of the watermain junction

located south of the cul-de-sac further east at Kilby Drive to the main at the south side of Cambie Road through the City lane between 10311 and 10331 Cambie Road is required.

- b) At Developer's cost, the Developer is required to:
- i. Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage building designs.
  - ii. Provide approximately 50 meters of 200mm watermain to loop the existing watermain junction located south of the cul-de-sac at Kilby Drive to the existing watermain at the south side of Cambie Road. Alignment of the new watermain shall be through the City lane between 10311 and 10331 Cambie Road.
  - iii. Review hydrant spacing on all road frontages and install new fire hydrants as required to meet City spacing requirements for the proposed land use.
  - iv. Provide a right-of-way for the water meter. Minimum right-of-way dimensions to be the size of the meter box (from the City of Richmond supplementary specifications) + any appurtenances (for example, the bypass on W2o-SD) + 0.5 m on all sides. Exact right-of-way dimensions to be finalized via the servicing agreement process.
- c) At Developer's cost, the City will:
- i. Disconnect all existing water service connections prior to demolition of existing onsite structures. It is the developer's responsibility to coordinate with the City the required disconnections via the Demolition Permit process.
  - ii. Install a service connection and tie-in to the existing 200mm diameter watermain at Kilby Drive frontage. The size and location of the service connection will be determined via the SA design process.
  - iii. Complete all proposed water tie-ins to existing City infrastructures.

#### Storm Sewer Works:

- a) At Developer's cost, the Developer is required to:
- i. Provide an erosion and sediment control plan for all on-site and off-site works, to be reviewed as part of the servicing agreement design.
  - ii. Confirm via capacity analyses whether the existing 525mm diameter storm sewer that crosses No. 4 Road at Kilby Drive has adequate capacity to service the proposed site.
  - iii. If existing 525mm diameter has capacity:
    - Tie-in required storm service connection to existing manhole STMH6939 located at Kilby Drive just east of No. 4 Road.
    - Provide an over build manhole at the tie-in of the existing 525mm diameter storm sewer to the existing box culvert at the west side of No. 4 Road.
    - Fill and abandon as per MMCD the existing storm service connection(s).
  - iv. If existing 525mm diameter storm sewer is insufficient:
    - Provide a new service connection with tie-in to the existing box culvert at the west side of No. 4 Road. Alignment of the new storm service connection shall be determined via the servicing agreement.
- b) At Developer's cost, the City will:
- i. Disconnect all existing drainage service connections prior to demolition of existing onsite structures. It is the developer's responsibility to coordinate with the City the required disconnections via the Demolition Permit process.
  - ii. Install a new service connection and tie-in to existing manhole STMH6939 located at Kilby Drive just east of No. 4 Road. This is subject to the existing 525mm storm sewer which crosses No. 4 Road having adequate capacity.
  - iii. Complete all proposed storm sewer tie-ins to existing City infrastructure.

#### Sanitary Sewer Works:

The western edge of the proposed buildings conflicts with the existing sanitary line which is contained in an existing City utility right of way along the west property line. To avoid the conflict and maintain maintenance access to the existing sanitary line, the western edge of the proposed buildings needs to be 3m from the east edge of the existing City utility right of way.

Alternatively, the existing sanitary line may be removed and the existing City utility right of way along the west property line may be discharged if the existing sanitary line contained in the right of way is replaced at the developer's costs with new sanitary lines in No. 4 Road. The details and alignment of the new sanitary main in No. 4 Road, approximately 120 meters long, shall be finalized via the servicing agreement process. The replacement sanitary lines in No. 4 Road need to be completed and operational before discharge of the right of way, prior to BP issuance and before start of any onsite works (e.g., preload, soil densification, and etcetera).

The existing utility right of way will only be discharged after the relocated sanitary line in the roadway is completed, operational and accepted by the City and the existing sanitary line and manholes contained in the right of way are removed. The SA letter of credit (i.e., bond to secure the SA works) will be a condition of Development Permit issuance. And, the completion and City acceptance of the SA works (e.g., sanitary relocation, etcetera) will be a condition of Building Permit issuance because the sanitary line relocation needs to be completed first prior to start of site preparation or building construction.

The processing of the right of way discharge will commence only after the City has accepted the new sanitary line. This will be before BP issuance or before start of any site preparation works (e.g., pre-load, etcetera).

- a) At Developer's cost, the Developer is required to:
  - i. Provide approximately 120 m of new sanitary lines in the No 4 Road roadway. The location and dimensions of the sanitary line will be determined through the SA design process.
- b) At Developer's cost, the City will:
  - i. Disconnect all existing sanitary service connections prior to demolition of existing onsite structures. It is the developer's responsibility to coordinate with the City the required disconnections via the Demolition Permit process.
  - ii. Install a new service connection and tie-in to the sanitary main that crosses No. 4 Road. The size of the service connection will be determined via the SA design process.
  - iii. Complete all proposed sanitary tie-ins to existing City infrastructure.

#### Frontage Improvements:

- a) At Developer's cost, the Developer is required to:
  - i. Coordinate with BC Hydro, Telus and other private communication service providers:
    - To pre-duct for future hydro, telephone and cable utilities along all road frontages.
    - Before relocating/modifying any of the existing power poles and/or guy wires along the frontages and within the proposed site. Relocation of existing BC Hydro overhead lines and Shaw underground lines are required prior to preload or site preparation.
    - To underground overhead service lines.
  - ii. Locate/relocate all above ground utility cabinets and kiosks required to service the proposed development and proposed undergrounding works, and all above ground utility cabinets and kiosks located along the development's frontages, within the development's site (see list below for examples). A functional plan showing conceptual locations for such infrastructure shall be included in the development design review process. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements (e.g., statutory right-of-way dimensions) and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of statutory right-of-ways that shall be shown on the architectural plans/functional plan, the servicing agreement drawings, and registered prior to SA design approval:
    - BC Hydro Vista – confirm dimensions with BC Hydro.
    - BC Hydro PMT – 4.0 x 5.0 m
    - BC Hydro LPT – 3.5 x 3.5 m

- Street light kiosk – 1.5 x 1.5 m
  - Traffic signal kiosk – 2.0 x 1.5 m
  - Traffic signal UPS – 1.0 x 1.0 m
  - Shaw cable kiosk – 1.0 x 1.0 m
  - Telus FDH cabinet – 1.1 x 1.0 m
- iii. Review street lighting levels along all road and lane frontages, and upgrade as required.
- iv. Complete frontage improvements on No 4 Road as follows:
- New 1.5 m wide concrete sidewalk at the property line;
  - New grass and tree boulevard between the sidewalk and curb;
  - New driveway crossing in the centre of the site frontage; and
  - Removal of the existing sidewalk and driveway crossings and replacement with the frontage works as described above.
- v. Complete frontage improvements on Kilby Drive as follows:
- New 1.5 m wide concrete sidewalk at the property line. The new sidewalk may have to be aligned to go around the existing trees identified for retention across the eastern half of the frontage; and
  - New grass and tree boulevard between the sidewalk and curb.

General Items:

a) At Developer's cost, the Developer is required to:

- i. Provide, prior to start of site preparation works or within the first servicing agreement submission, whichever comes first, a preload plan and geotechnical assessment of preload, dewatering, and soil preparation impacts on the existing utilities (especially fronting the development site) and provide mitigation recommendations. The preload shall not encroach into the required 3 meter wide City utility right of way mentioned under the Sanitary portion. Registration of the 3 meter wide City utility right of way is required prior to preload installation.
- ii. Provide a video inspection report of the existing sanitary line along the west property line prior to start of site preparation works or within the first servicing agreement submission, whichever comes first. A follow-up video inspection, complete with a civil engineer's signed and sealed recommendation letter, is required after site preparation works are complete (i.e. pre-load removal, completion of dewatering, etc.) to assess the condition of the existing utilities and provide recommendations to retain, replace, or repair. Any utilities damaged by the pre-load, de-watering, or other ground preparation shall be replaced or repaired at the Developer's cost.
- iii. Conduct pre- and post-preload elevation surveys of all surrounding roads, utilities, and structures. Any damage, nuisance, or other impact to be repaired at the developer's cost. The post-preload elevation survey shall be incorporated within the servicing agreement design.
- iv. Monitor the settlement at the adjacent utilities and structures during pre-loading, dewatering, and soil preparation works per a geotechnical engineer's recommendations, and report the settlement amounts to the City for approval.
- v. Submit a proposed strategy at the building permit stage for managing excavation de-watering. Note that the City's preference is to manage groundwater onsite or by removing and disposing at an appropriate facility. If this is not feasible, the Developer will be required to apply to Metro Vancouver for a permit to discharge into the sanitary sewer system. If the sanitary sewer does not have adequate capacity to receive the volume of groundwater, the Developer will be required to enter into a de-watering agreement with the City wherein the developer will be required to treat the groundwater before discharging it to the City's storm sewer system.
- vi. Not encroach into City rights-of-ways with any proposed trees, retaining walls, or other non-removable structures.
- vii. Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director



of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

**Note:**

- \* This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

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Signed

---

Date



# City of Richmond

## Development Permit

**No. DP 20-899883**

To the Holder:                      ORION CONSTRUCTION

Property Address:                3640 NO. 4 ROAD

Address:                              UNIT 104, 3950 191 STREET  
   SURREY, BC V3Z 0Y6

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1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans # 1 to # 15 attached hereto.
4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$31,471 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

# Development Permit

No. DP 20-899883

To the Holder: ORION CONSTRUCTION  
Property Address: 3640 NO. 4 ROAD  
Address: UNIT 104, 3950 191 STREET  
SURREY, BC V3Z 0Y6

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7. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

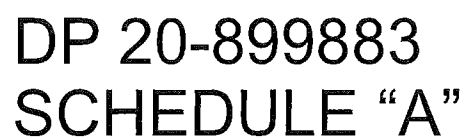
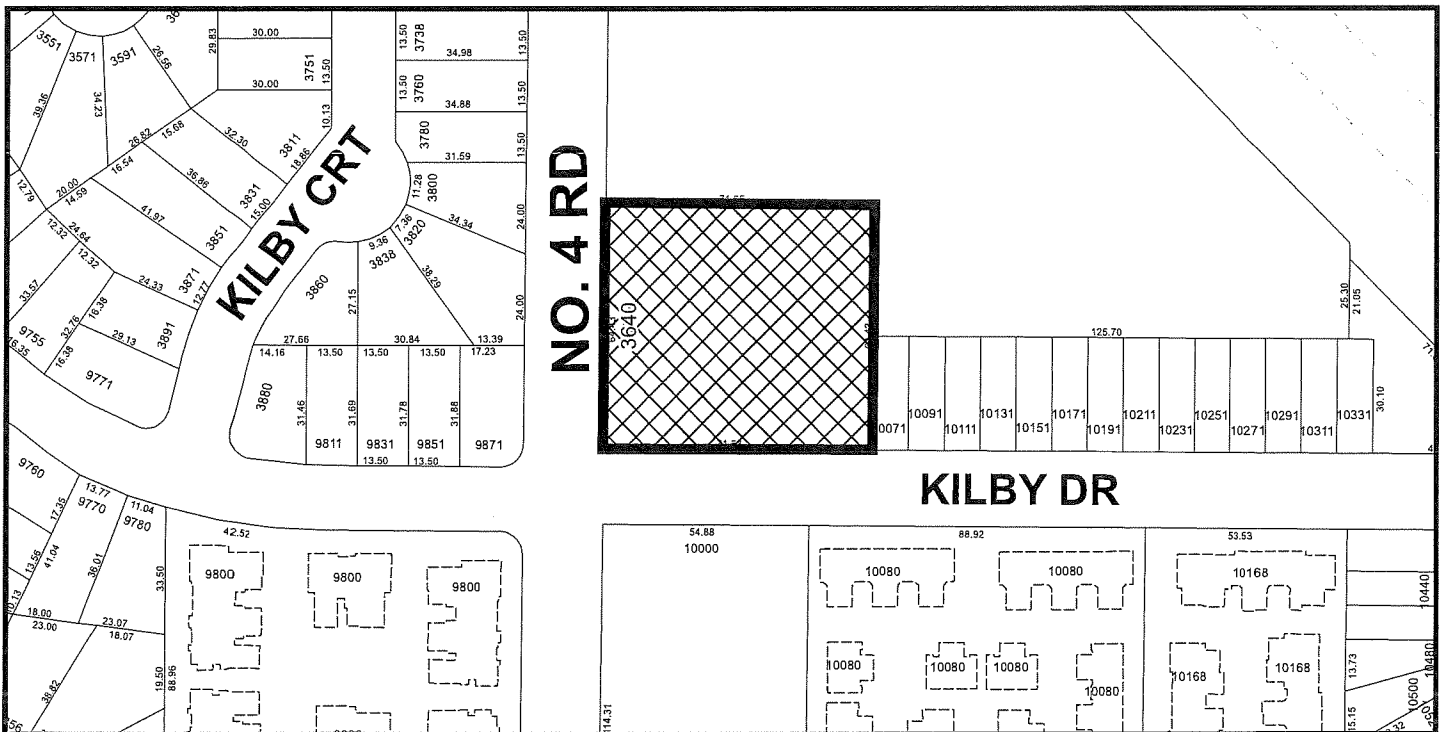
AUTHORIZING RESOLUTION NO.  
DAY OF , .

ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

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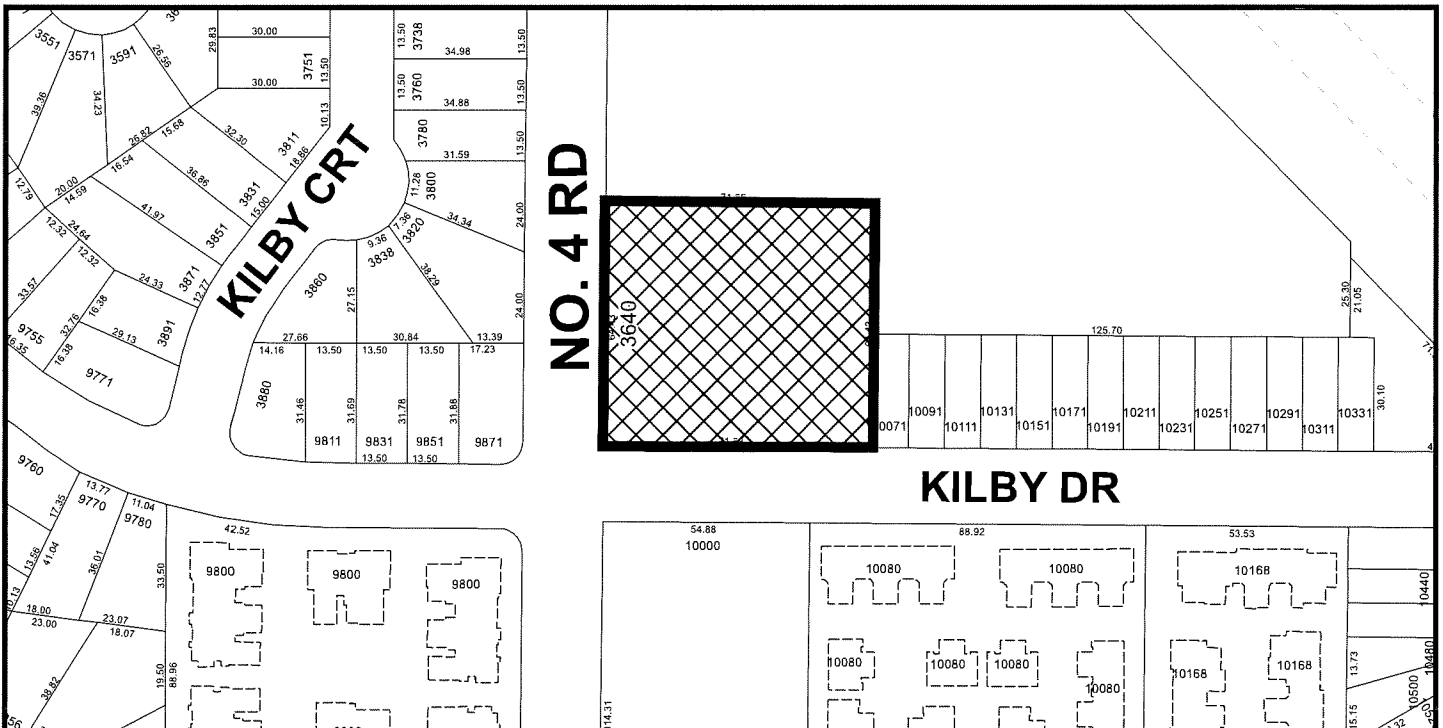
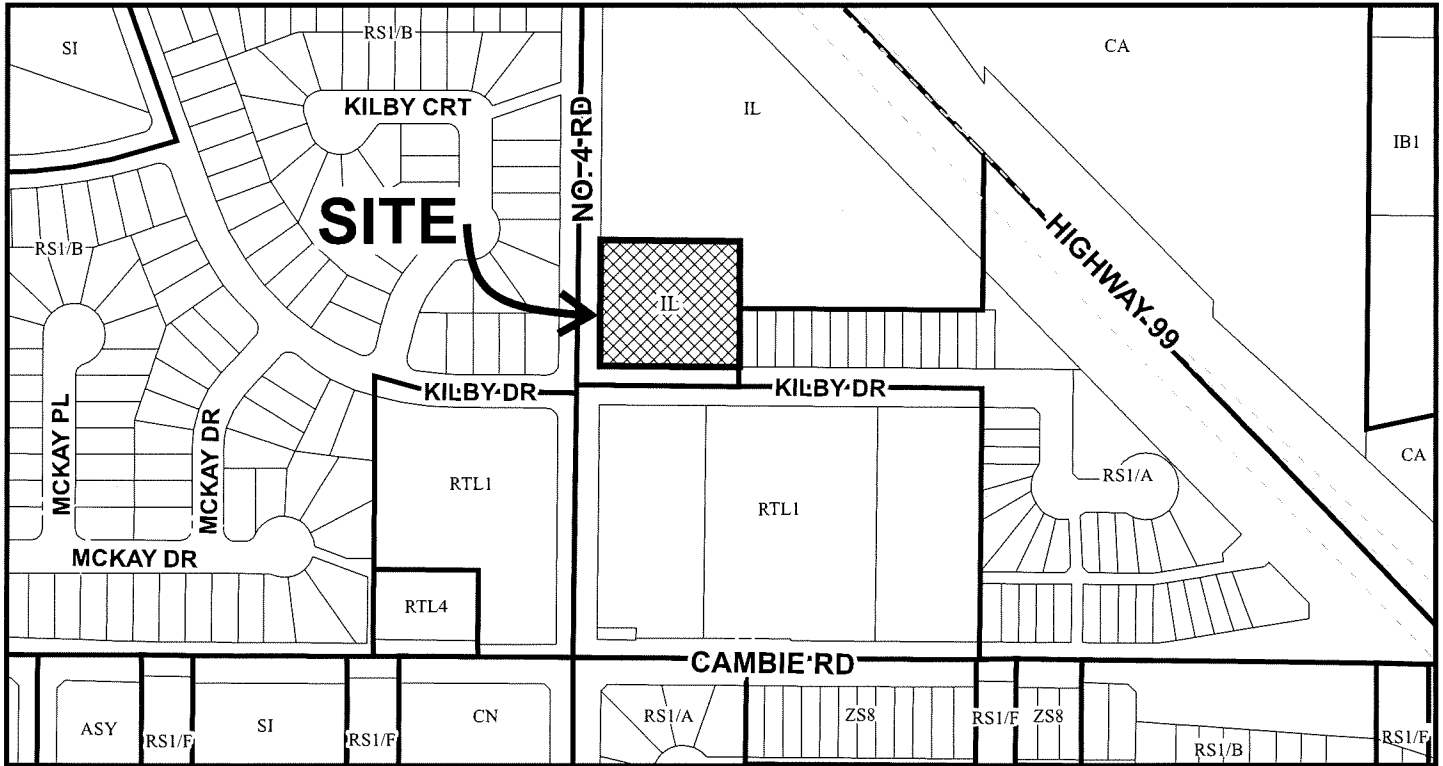
MAYOR



Note: Dimensions are in METRES



# City of Richmond











**GENERAL NOTES**  
- FOR TOPOGRAPHIC SURVEY REFER TO TOPOGRAPHIC PLAN 19432\_03TP DATED 2020 01 30 PREPARED BY METRO VANCOUVER LAND SURVEYORS.

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NO.	DATE	ISSUANCE
4	2020 04 17	ISSUED FOR COORDINATION
5	2020 04 23	ISSUED FOR DP
6	2020 09 21	RE-ISSUED FOR DP
7	2021 01 26	RE-ISSUED FOR DP
8	2021 02 02	RE-ISSUED COORDINATION
9	2021 02 22	RE-ISSUED FOR DP
10	2021 03 05	ISSUED FOR ADP
11	2021 04 14	RE-ISSUED FOR DP



PROPOSED NEW DEVELOPMENT  
3640 No. 4 Road  
Richmond BC

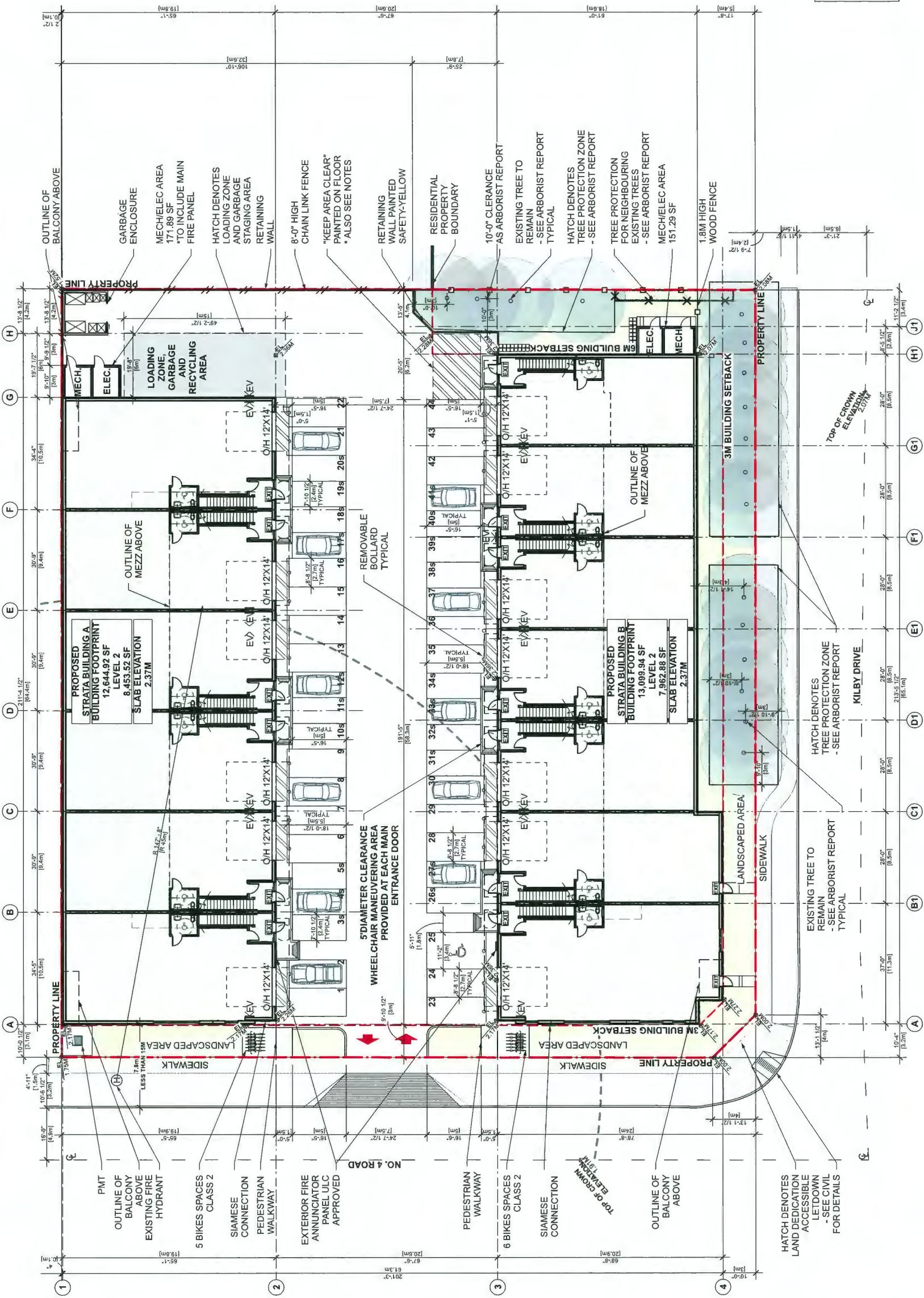
**SITE PLAN**

**A 2.0**



DRAWN: BR  
CHECKED: KC  
SCALE: 1:400  
FILE: 2003

KCC Architecture & Design Ltd.  
Unit 300 15300 Croydon Drive Surrey BC V3Z 0Z5  
Tel 604 283 0912  
kccarchitecture.com



**NOTES**  
\* ADD "NO PARKING ANYTIME" SIGN WITH A TAB BELOW ADVISING "THIS AREA FOR TRUCK TURN-AROUND ONLY"  
**DP 20-899883**  
**PLAN # 2**





**PARKING**

REQUIRED  
Commercial storage + Level 2/100 x.5 SM  
2,000/100\*0.5 + 1,863.26/100\*0.2 =13.72  
SUBTOTAL 14

FULL TIME 37  
TEMPORARY 7  
STANDARD 24  
SMALL 20

TOTAL  
REQUIRED 14  
PROVIDED 44

**ACCESSIBLE PARKING**

PROVIDED 1

**LOADING**

REQUIRED

PROVIDED

**BIKE STORAGE**

CLASS 1

1 every 1,861 sqm GFA, +1 every 5,000 sqm above 1,861 sqm

REQUIRED

PROVIDED

**BIKE STORAGE**

CLASS 2

3,863.26/100\*0.27 = 10.43

11 WILL BE PROVIDED @ TI

3,863.26/100\*0.27 = 10.43

PROVIDED 11

\* ALL PARKING SPACES MARKED WITH AN "X" ARE COMMON PARKING TEMPORARY SPACES.

DP 20-899883  
PLAN # 3

A 2.2

PARKING PLAN

3640 No. 4 Road  
Richmond BC

ORION  
CONSULTING



**ISSUANCE**

ISSUED FOR COORDINATION  
RE-ISSUED FOR DP  
RE-ISSUED FOR DP  
RE-ISSUED FOR DP  
RE-ISSUED FOR DP

**NO.**

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DRAWN: BR  
CHECKED: KC  
SCALE: 1:400  
FILE: 2003

KCC ARCHITECTURE  
KCC Architecture & Design Ltd.  
Unit 300 15300 Croydon Drive Surrey BC V3Z 0Z5  
Tel 604 283 0912  
kccarchitecture.com





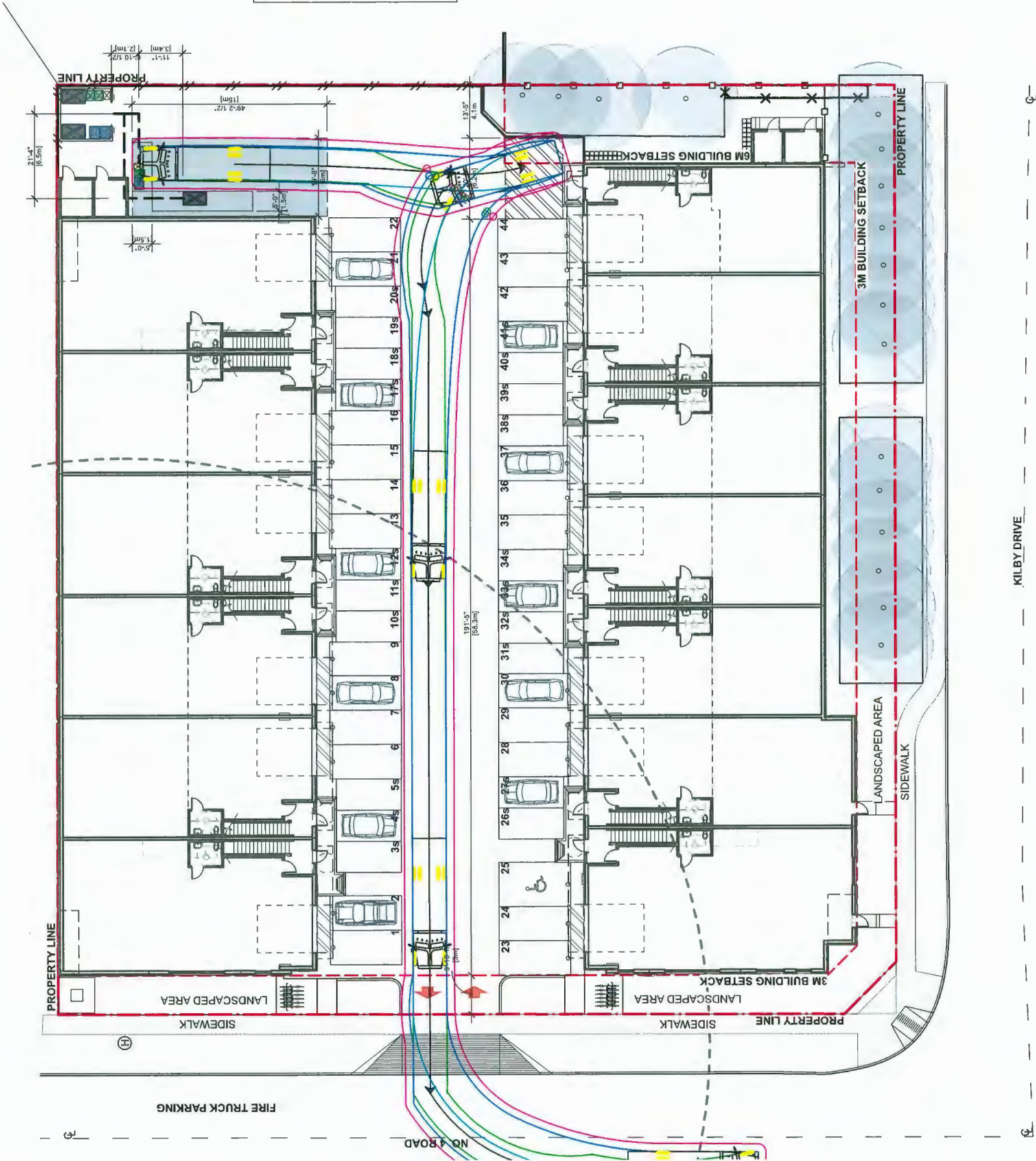
INDUSTRIAL WASTE ROOM INFORMATION, INCLUDE OFFICE

ROOM: 179 SF (16.63 SM)  
GROUND LEVEL - AT GRADE  
OPEN 7am - 7pm  
INDUSTRIAL/OFFICE

SIZE		LOCATION		ACCESS INFORMATION		BUILDING USE	
BIN TYPE	NUMBER OF BINS	FOOTPRINT OF 1 BIN(SM)	COLLECTION SERVICE	FREQUENCY			
YARD	2	1.96	SIERRA WASTE	WEEKLY		GARBAGE	
360L	2	0.66	SIERRA WASTE	WEEKLY		MIXED CONTAINERS	
240L	2	0.43	SIERRA WASTE	TWICE WEEKLY		FOOD SCRAP	
120L	1	0.22	SIERRA WASTE	WEEKLY		PAPER RECYCLING	
		FREQUENCY OF COLLECTION				ONCE A	

BIN TYPES

- LOADING ZONE AND GARBAGE STAGING AREA
- TRUCK REAR / SIDE LOADER BUFFER
- TRUCK REAR / SIDE LOADER BUFFER
- FOOT PATH TO LOADING ZONE
- 3 YRD GARBAGE
- 360L MIXED CONTAINERS
- 240L FOOD SCRAP
- 120L PAPER RECYCLING



6

KILBY DRIVE

6

6

6

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DATE	ISSUANCE
2021 01 26	RE-ISSUED FOR DP
2021 02 22	RE-ISSUED FOR DP
2021 03 05	ISSUED FOR ADP
2021 04 14	RE-ISSUED FOR DP

ORION

PROPOSED NEW DEVELOPMENT  
3640 No. 4 Road  
Richmond BC

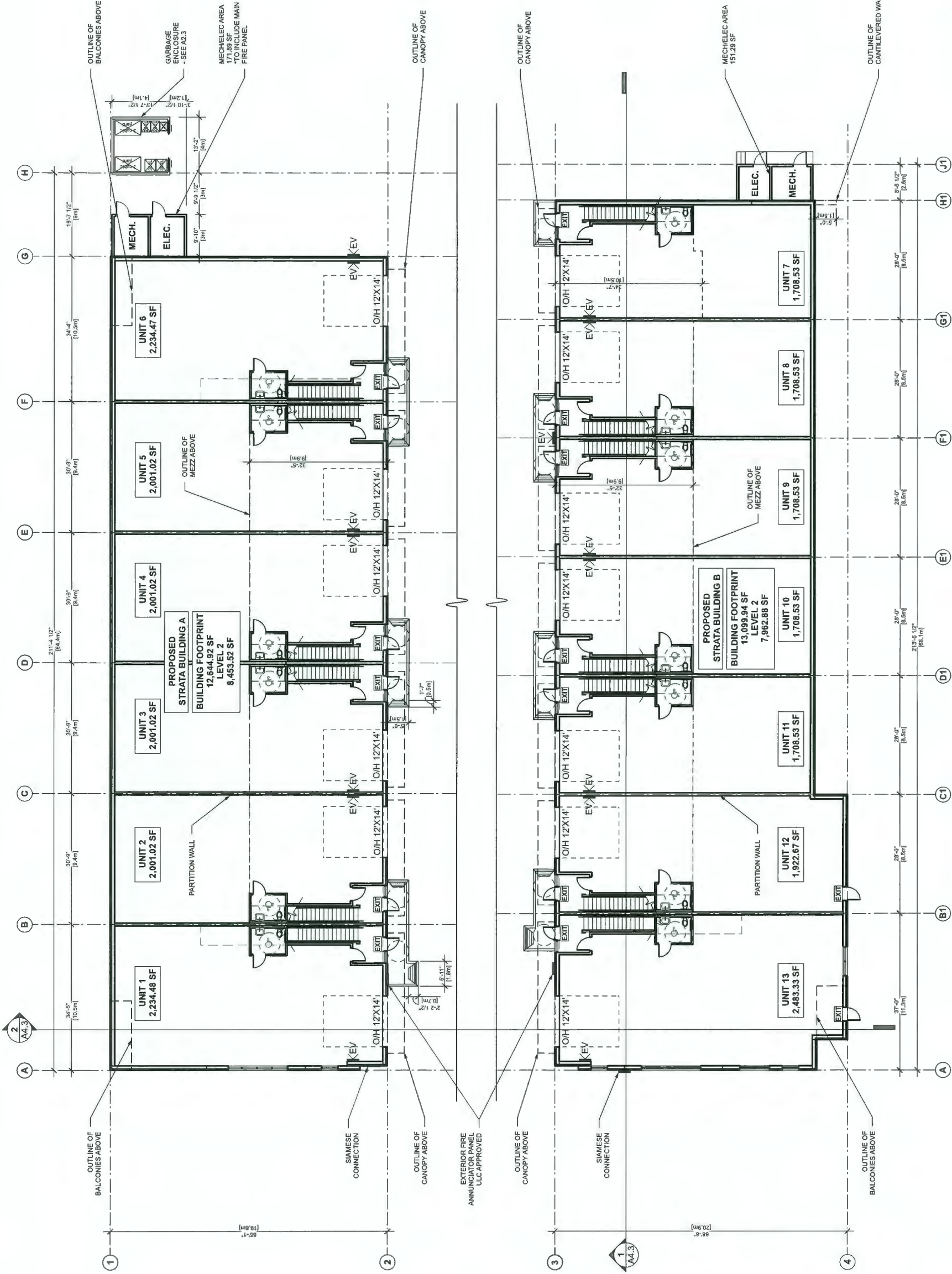


WASTE MANAGEMENT PLAN  
A 2.3



DP 20-899883  
PLAN # 4





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## LEVEL 1 FLOOR PLAN

A 3.0

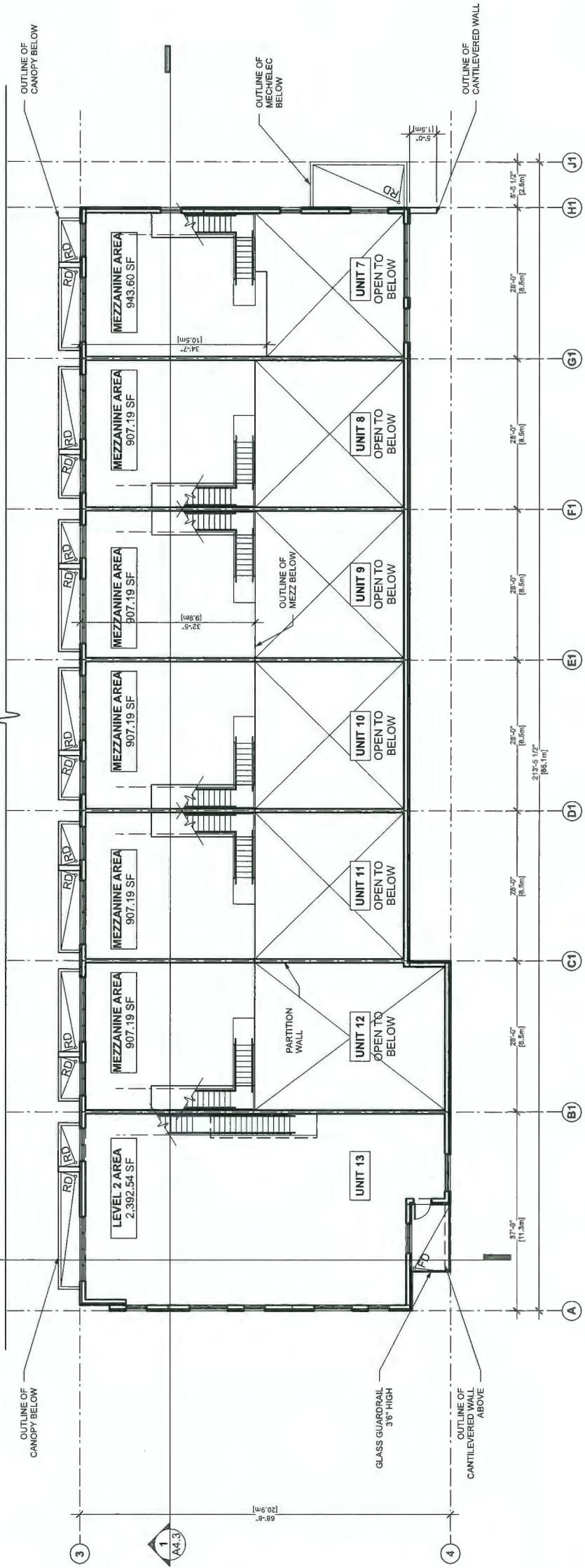
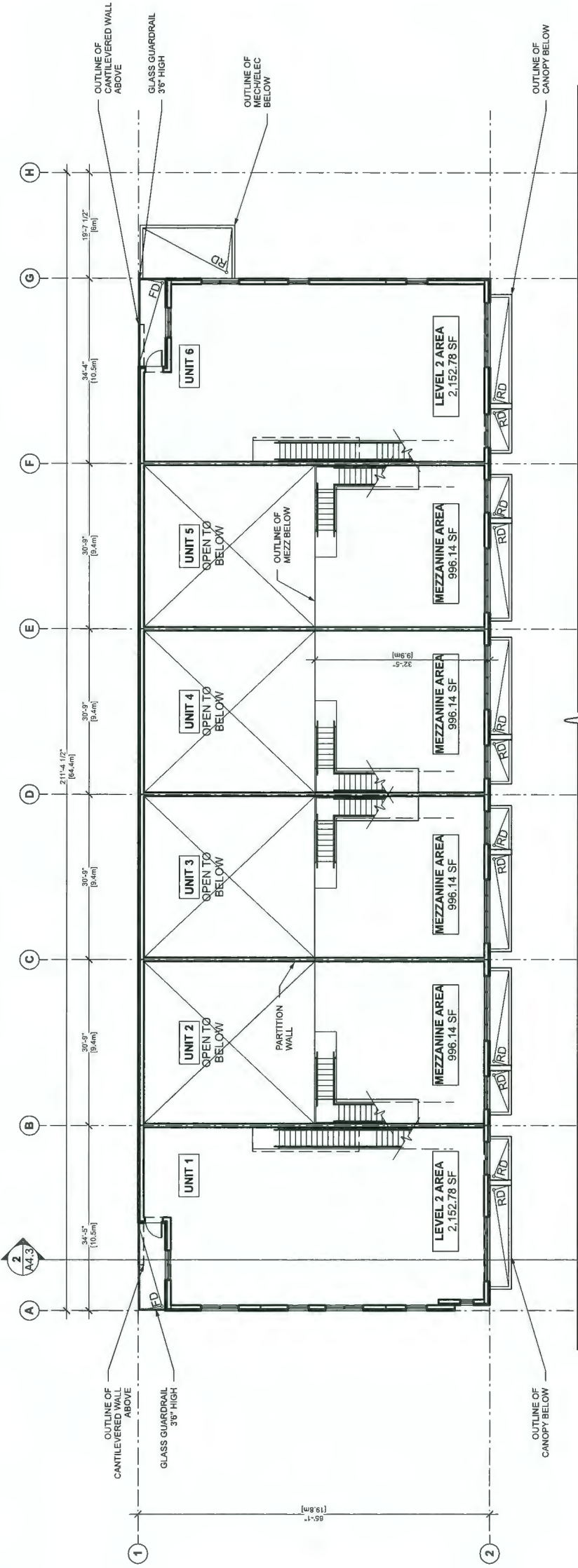


DP 20-899883  
PLAN # 5

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CHECKED: KC  
SCALE: 1:300  
FILE: 2003

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6	2021 02 22
7	2021 03 05
8	2021 04 14

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**PROPOSED NEW DEVELOPMENT**  
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## LEVEL 2 FLOOR PLAN

A 3.1

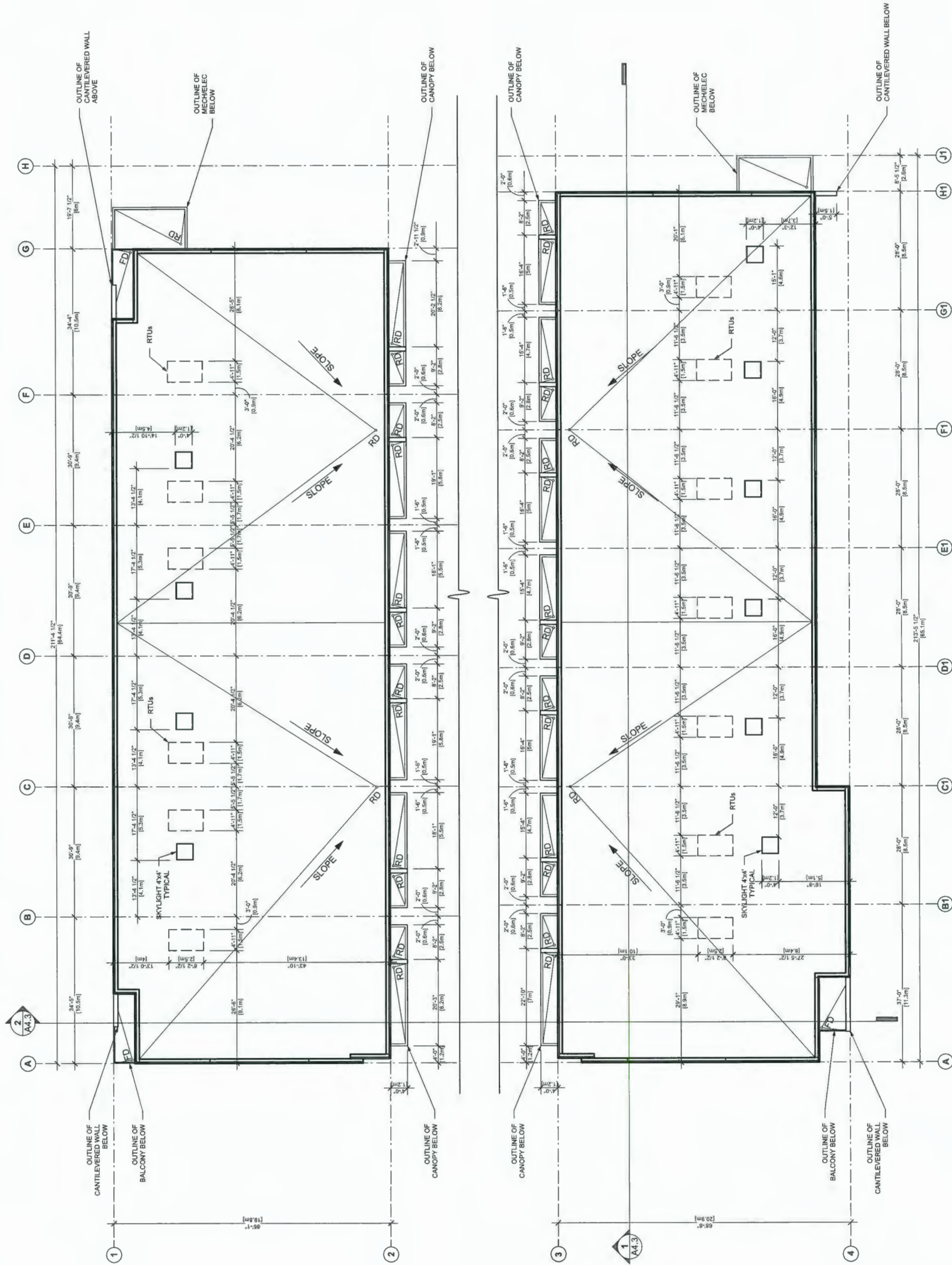


**DP 20-899883  
PLAN # 6**

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FILE: 2003

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2021 03 05	RE-ISSUED FOR DP
2021 04 14	RE-ISSUED FOR DP



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## PROPOSED NEW DEVELOPMENT

3640 No. 4 Road  
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## ROOF PLAN

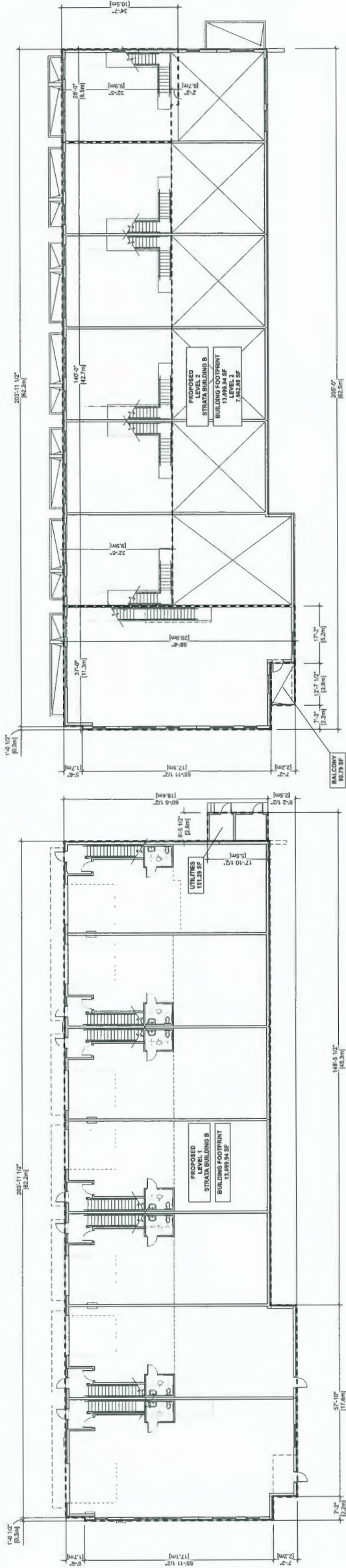
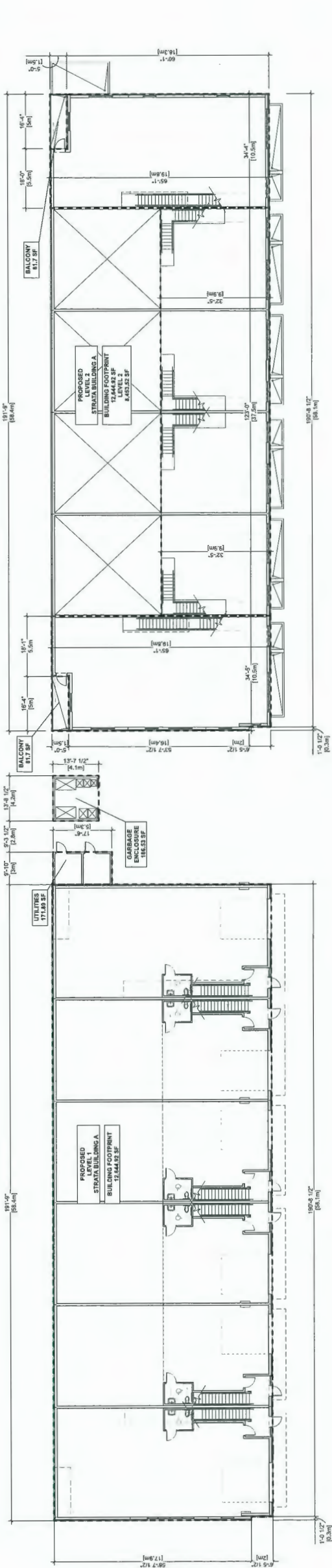
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SCALE: 1:300  
FILE: 2003

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**DP 20-899883**  
**PLAN # 7**





① GROUND FLOOR  
1/16"=1'-0"

② LEVEL 2  
1/16"=1'-0"

BUILDING AREAS	BUILDING A	BUILDING B	TOTAL
LEVEL 1	12,473.03	12,948.85	25,421.88
UTILITIES	171.89	151.29	323.18
SUBTOTAL	12,644.92	13,099.94	25,744.86
LEVEL 2	4,305.58	2,392.54	6,698.10
MEZANINES	3,984.56	5,479.55	9,464.11
BALCONIES	163.40	90.79	254.19
SUBTOTAL	8,453.52	7,962.88	16,416.40
TOTAL USE	20,798.15	20,820.74	41,558.89

**NOTES**  
- UTILITIES AND BALCONIES STAIRS ARE NOT INCLUDED IN THE TOTAL

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PROPOSED NEW DEVELOPMENT  
3640 No. 4 Road  
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FSR OVERLAY

A 3.3



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Revision	Description	Date
3	ISSUED FOR BUILDING PERMIT REVIEW	20/APR/2021
2	ISSUED FOR BUILDING PERMIT REVIEW	13/APR/2021
1	ISSUED FOR COORDINATION	09/APR/2021



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Port Coquitlam, BC, Canada V3B 1C3  
Email: admin@opaleng.com Tel: 604-475-6725  
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DP 20-899883  
PLAN # 9

PROPOSED NEW  
DEVELOPMENT  
3840 NO.4 ROAD  
RICHMOND, BC

SITE PLAN  
SITE LIGHTING

1/16" = 1'-0"  
Drawing No. **E1.0A**



### ELECTRICAL SYMBOL LEGEND

#### LIGHTING

- LUMINAIRE - SURFACE/SUSPENDED MOUNTED
- LUMINAIRE - RECESSED MOUNTED
- LUMINAIRE - WALL MOUNTED
- STRAIGHT LUMINAIRE
- LUMINAIRE - EMERGENCY/NIGHT LIGHT (N/L)
- LUMINAIRE - SURFACE MOUNTED
- POT LUMINAIRE - RECESSED MOUNTED
- DIRECTIONAL POT LUMINAIRE - RECESSED MOUNTED
- LUMINAIRE - WALL MOUNTED
- LUMINAIRE - LED TAPE/PIPE
- LUMINAIRE - RECESSED WALL MOUNTED
- LUMINAIRE - POLE MOUNTED
- DOUBLE HEAD LUMINAIRE - POLE MOUNTED
- FLOOR MOUNTED UPLIGHT
- TRACK LIGHTING
- SINGLE POLE SWITCH - TYPE AS NOTED
- LUMINAIRE CALL OUT

#### POWER

- DUPLEX RECEPTACLE
- SINGLE RECEPTACLE
- FOUR-POLE RECEPTACLE
- SPLIT-FEED RECEPTACLE
- SPECIAL 208V 1PH RECEPTACLE
- SPECIAL 208V 3PH RECEPTACLE
- DUPLEX RECEPTACLE - MTD ABOVE COUNTER
- SPLIT-FEED RECEPTACLE - MTD ABOVE COUNTER
- DUPLEX T RECEPTACLE 20A - MTD ABOVE COUNTER
- DUPLEX T RECEPTACLE 20A
- FOUR-POLE T RECEPTACLE 20A
- GFI DUPLEX RECEPTACLE
- GFI DUPLEX RECEPTACLE - MTD ABOVE COUNTER
- GFI DUPLEX T RECEPTACLE 20A
- GFI DUPLEX T RECEPTACLE 20A - MTD ABOVE COUNTER
- FLOOR MOUNTED DUPLEX RECEPTACLE
- RADIANT HEATER - BI-BUILT IN THERMOSTAT
- FORCED FLOW HEATER - BI-BUILT IN THERMOSTAT
- EQUIPMENT CONNECTION
- EQUIPMENT TAG
- MOTOR
- NON FIXED DISCONNECT SWITCH
- MANUAL MOTOR STARTER C/M PILOT LIGHT
- VARIABLE SPEED SWITCH
- DISBURBITION
- HORN / BUZZER
- PANELBOARD
- THERMOSTAT
- WALL / CEILING MOUNTED JUNCTION BOX

#### FIRE ALARM

- MANUAL BREAKGLASS STATION
- BELL / HORN
- BELL/HORN C/M STROBE LIGHT
- STROBE LIGHT
- SMOKE DETECTOR
- HEAT DETECTOR - RATE OF RISE
- DUCT MOUNTED SMOKE DETECTOR

#### AUXILIARY

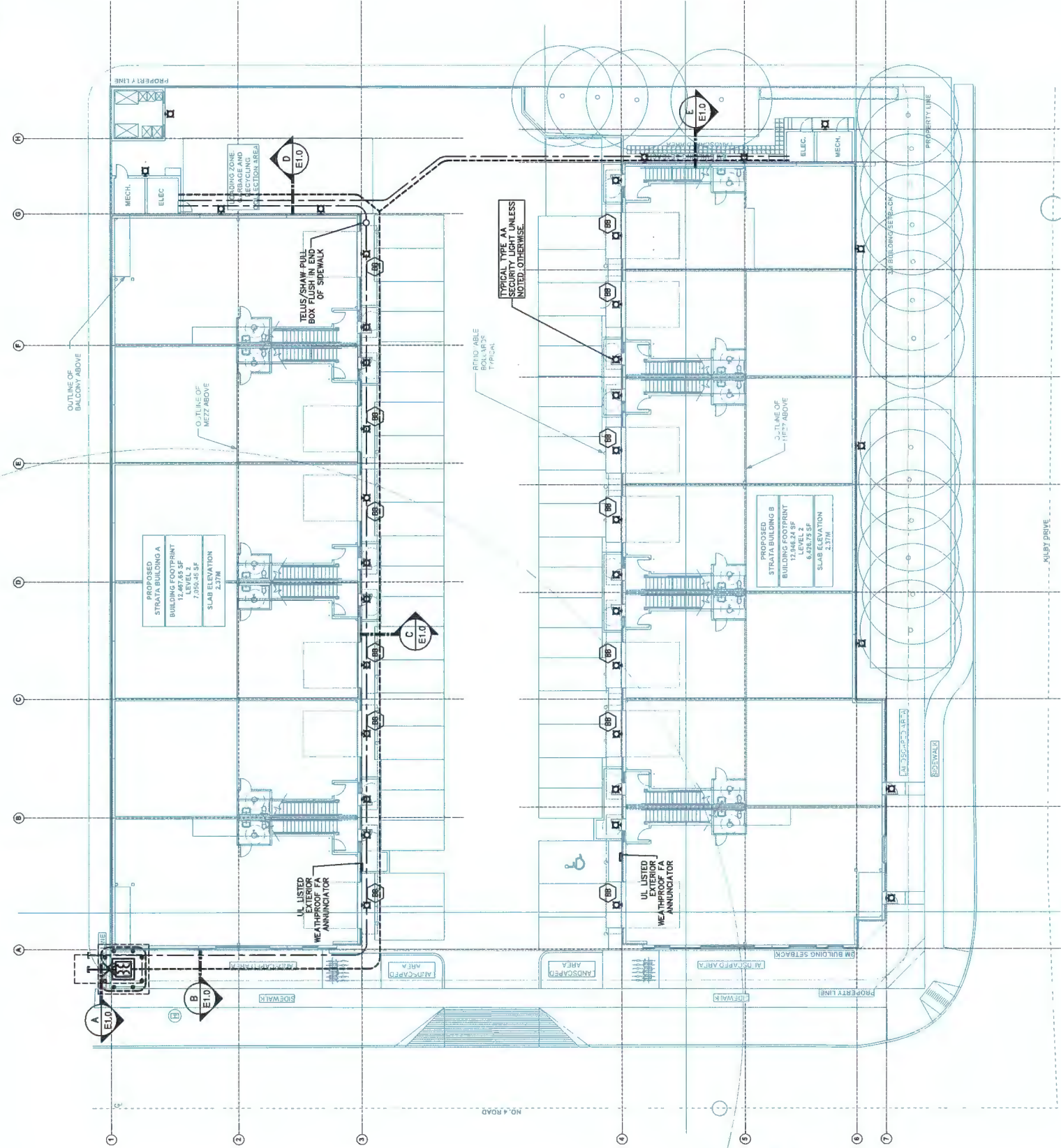
- TELEPHONE OUTLET
- DATA OUTLET
- TELEVISION OUTLET

#### EXIT & EMERGENCY LIGHTING

- CEILING EXIT LIGHT - (ARROWS AS INDICATED)
- WALL EXIT LIGHT - (ARROWS AS INDICATED)
- WALL EXIT LIGHT EMERGENCY REMOTE HEAD COMBO
- BATTERY PACK
- BATTERY PACK (C/M HEADS AS INDICATED)
- WALL / CEILING MOUNTED EMERGENCY REMOTE HEADS

#### NOTES

WHERE INDICATED REFER TO THE FOLLOWING ABBREVIATIONS:  
PL - PILOT LIGHT, NL - NIGHT LIGHT, MC-WIRE GUARD, BI-BUILT IN THERMOSTAT, WP-WEATHERPROOF, TL-TWIST LOCK, IS-ISOLATED GROUND, TP-TAPER PROOF, US-2 PORT USB COMBO UNIT



DWG #	DRAWING NAME	SCALE
E1.0	SITE PLAN, SYMBOL LEGEND & SCHEDULE D ABBREV	1/16" = 1'-0"
E1.1	BUILDING A HYDRO PROFILE & SITE DETAILS	AS SHOWN
E1.2	BUILDING B HYDRO PROFILE & SITE DETAILS	AS SHOWN
E1.3	BUILDING A LIGHTING LAYOUT	1/8" = 1'-0"
E1.4	BUILDING A POWER LAYOUT	1/8" = 1'-0"
E1.5	BUILDING B LIGHTING LAYOUT	1/8" = 1'-0"
E1.6	BUILDING B POWER LAYOUT	1/8" = 1'-0"
E1.7	SCHEMATIC	NTS
E1.8	SPECIFIC CALL OUTS	NTS



LUMINAIRE AA - LOW ON WALL



LUMINAIRE BB - HIGH ON WALL





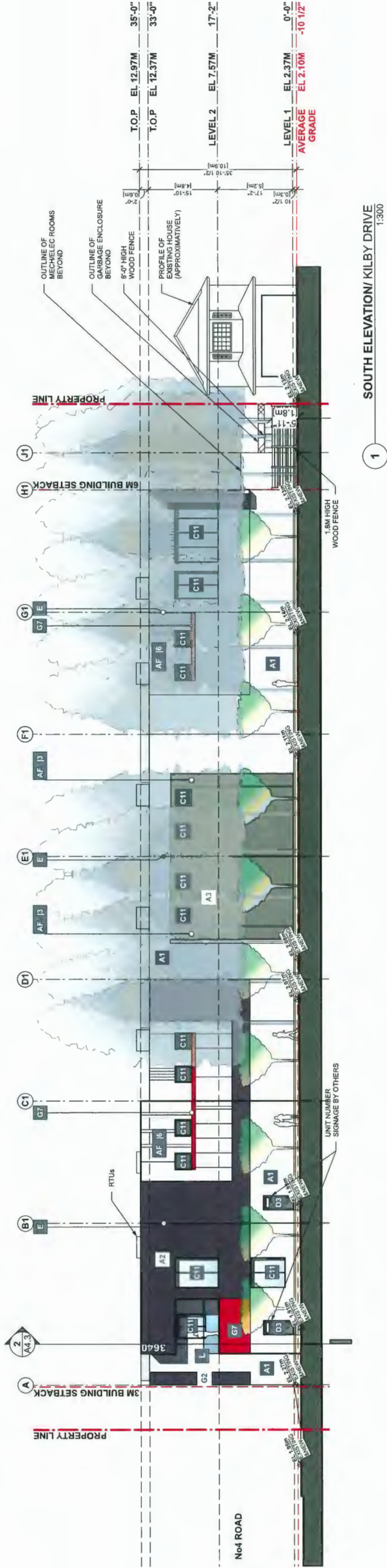




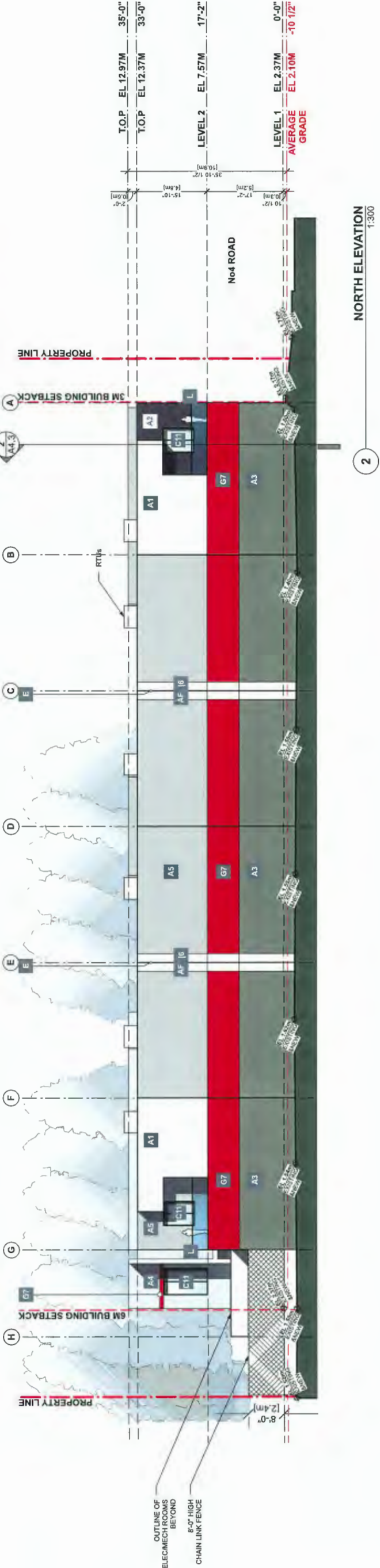








SOUTH ELEVATION/ KILBY DRIVE  
1:300



NORTH ELEVATION  
1:300

MATERIAL LEGEND

- A INSULATED TILT-UP CONCRETE PANEL
- B PAINTED
- C CANTILEVERED WALL
- D CURTAIN WALL
- E STEEL FRAME HOLLOW METAL DOOR
- F PANEL JOINT

- F CONCRETE FORM 1/46 RIPPE
- G PAINTED TRIM
- H CANOPY
- J STEEL FRAME HOLLOW METAL DOOR
- K INSULATED OVERHEAD DOOR
- L PAINTED (R15)

- L BALCONY WITH GLASS RAILING

COLOUR LEGEND

- 1 BENJAMIN MOORE VINTAGE TAUPE 2110-70
- 2 BENJAMIN MOORE BLACK JACK 2133-20
- 3 BENJAMIN MOORE BABY SEAL BLACK 2119-30
- 4 BENJAMIN MOORE DIOR GRAY 2133-40
- 5 BENJAMIN MOORE GRAY SHOWER 2125-30

- 6 BENJAMIN MOORE STORMY MONDAY 2112-50
- 7 BENJAMIN MOORE RUBY RED 2001-10
- 8 VICWEST DEEP METAL CLADDING EWH WHITE 3-EWH-70
- 9 VICWEST DEEP METAL CLADDING CNC CHARCOAL 3-CNC-30
- 10 VICWEST DEEP METAL CLADDING VICWEST CRY OYSTER 3-CRY-50

- 11 BLACK ALUMINUM FRAME & TRANSPARENT DOUBLE GLASS UNIT

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8	2021 04 14	RE-ISSUED FOR DP



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ELEVATIONS

A 4.1



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PROPOSED NEW DEVELOPMENT  
3640 No. 4 Road  
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## SECTIONS

A 4.3



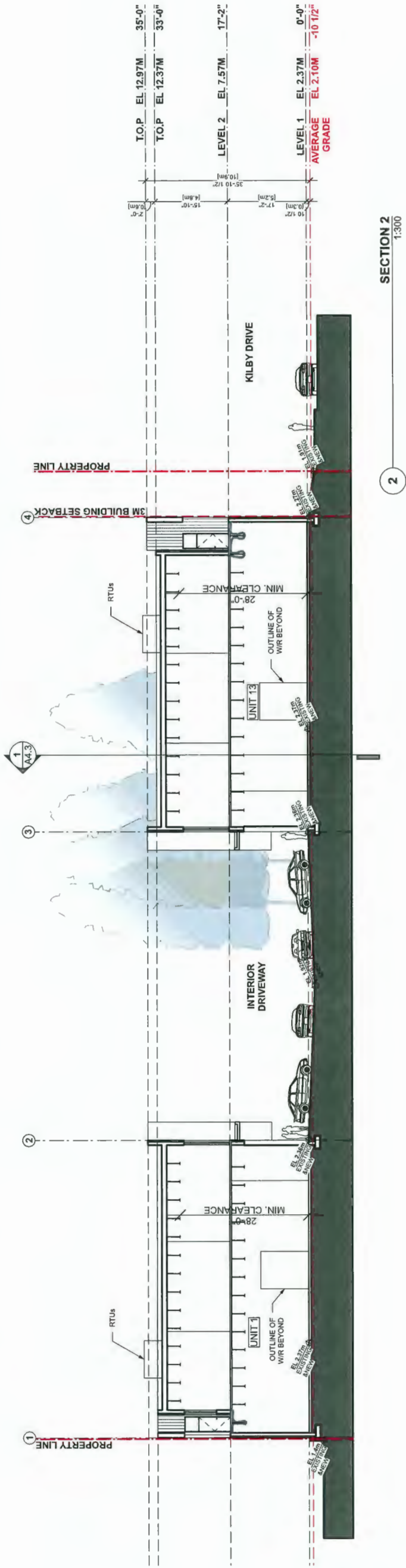
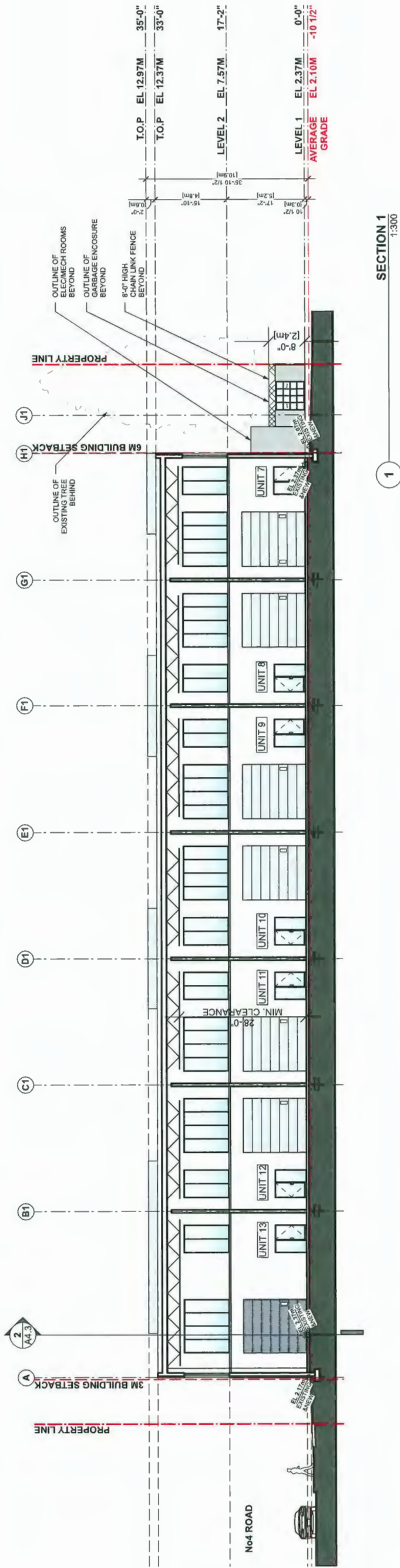
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PLAN # 15

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SCALE: AS NOTED

FILE: 2003

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ISO VIEWS

A 5.1



DP 20-899883  
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DP 20-899883  
REF # 3



A 5.2

ISO VIEWS

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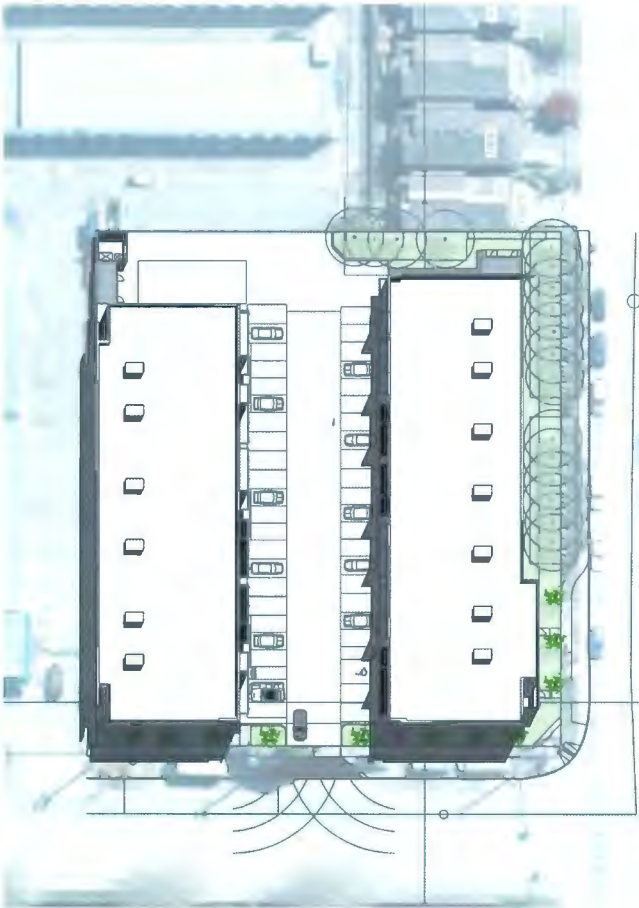
ISO VIEW

A 5.3

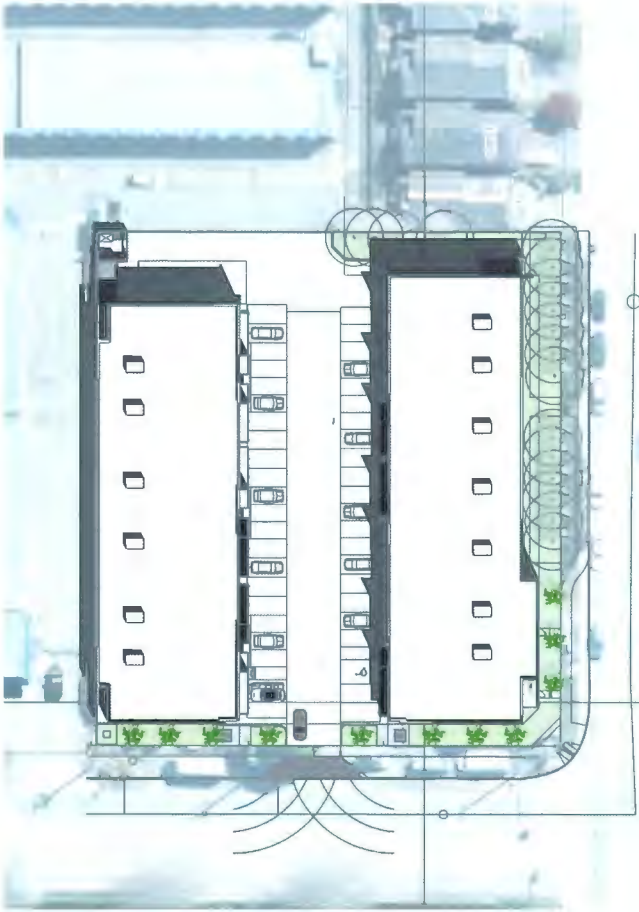


DP 20-899883  
REF # 4





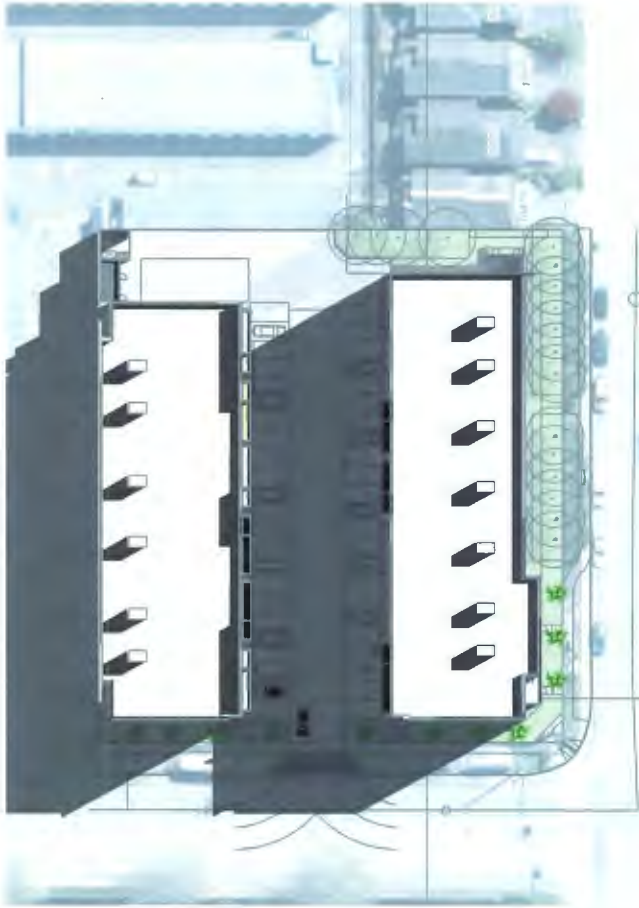
SUMMER SOLSTICE JUNE 20 2020 10:00 AM



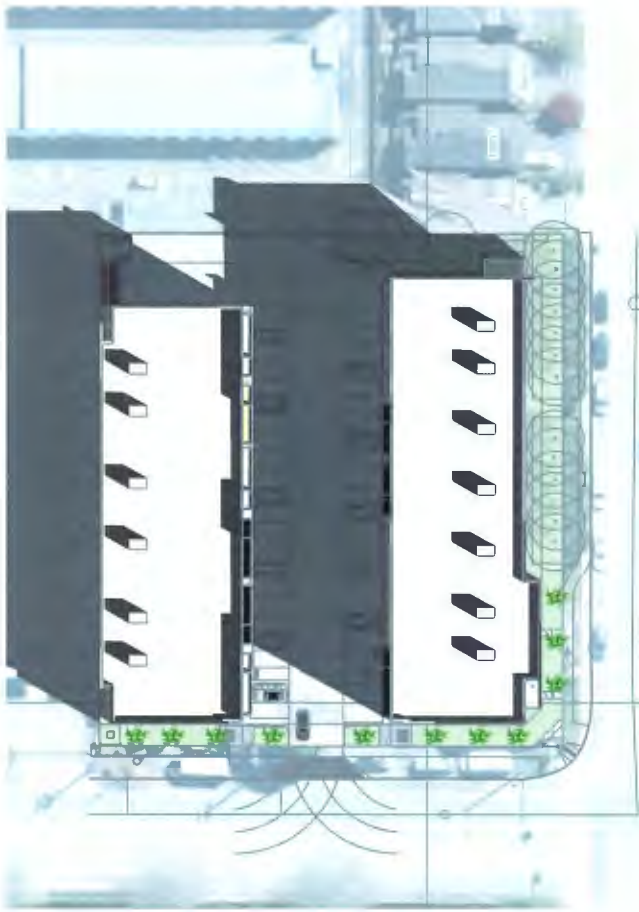
SUMMER SOLSTICE JUNE 20 2020 2:00 PM



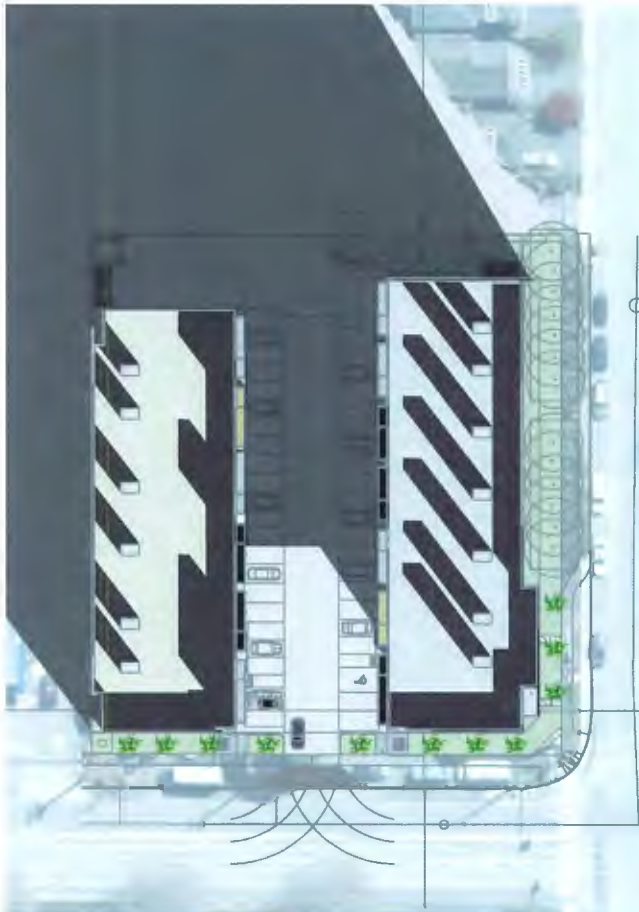
SUMMER SOLSTICE JUNE 20 2020 4:00PM



WINTER SOLSTICE DEC 21 2020 10:00 AM



WINTER SOLSTICE DEC 21 2020 2:00 PM



WINTER SOLSTICE DEC 21 2020 4:00 PM

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NO.	DATE	ISSUANCE
1	2020 06 18	ISSUED FOR REVIEW
2	2020 06 26	RE-ISSUED FOR REVIEW
3	2020 09 21	RE-ISSUED FOR DP
4	2021 01 26	RE-ISSUED FOR DP
5	2021 02 22	RE-ISSUED FOR DP
6	2021 03 05	ISSUED FOR ADP
7	2021 04 14	RE-ISSUED FOR DP

ORION  
STRUCTURAL

PROPOSED NEW DEVELOPMENT

3640 No. 4 Road  
Richmond BC

A 6.0

SHADOW ANALYSIS

DP 20-899883

REF # 5

CHECKED: KC

SCALE: NTS

FILE: 2003



ARCHITECTURE

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