

Development Permit Panel

Council Chambers, City Hall 6911 No. 3 Road Wednesday, March 25, 2015 3:30 p.m.

Minutes

Motion	to	adopt	the	minutes	of	the	Development	Permit	Panel	meeting	held	on
Tuesday	, M	larch 1	0, 20	15.								

1. Development Variance Permit 14-676341

(REDMS No. 4503862) (File Ref. No.: Xr: TE14-672413)

APPLICANT: Rogers Communications Inc.

PROPERTY LOCATION: 11771 Fentiman Place

Director's Recommendations

- 1. That a Development Variance Permit be issued which would vary the provisions of "Richmond Zoning Bylaw 8500" to increase the maximum accessory structure height in the "Health Care (HC)" zoning district from 12 m (39.3 ft.) to 21 m (68.9 ft.) in order to permit the installation of a temporary telecommunications antenna pole at 11771 Fentiman Place; and
- 2. That Richmond City Council grant concurrence to the proposed temporary telecommunications antenna pole installation for the site located at 11771 Fentiman Place for period of time extending up until October 1, 2015.

2. Development Permit 13-641791

(REDMS No. 4360213)

APPLICANT: Urban Design Group Architects Ltd. on behalf of 0976440

B.C. Ltd., Inc. No. 0976440

PROPERTY LOCATION: 3011 No. 5 Road

Director's Recommendations

That a Development Permit be issued which would permit the construction of a drive-through car wash and drive-through oil change service centre at 3011 No. 5 Road on a site zoned "Car Wash & Service Station (ZC35) – Bridgeport."

3. Development Permit Variance 14-658670

(REDMS No. 4375579)

APPLICANT: Habitat for Humanity Society of Greater Vancouver

PROPERTY LOCATION: 8180 Ash Street

Director's Recommendations

- 1. That a Development Variance Permit be issued which would vary the provisions of Richmond Zoning Bylaw 8500 to:
 - (a) vary the minimum lot width from 12 m to 8.3 m for proposed Lot 5; and
 - (b) vary the minimum lot frontage from 6 m to 0.38 m for proposed Lot 4, to 2.7 m for proposed Lot 5 and to 0.60 m for proposed Lot 6; and
- 2. To permit subdivision of 8180 Ash Street into six (6) lots zoned "Single Detached (RS1/B)" for the purpose of developing single-family dwellings.

4. Development Permit 14-659747

(REDMS No. 4460911)

APPLICANT: Dava Developments Ltd.

PROPERTY LOCATION: 2671, 2711, 2811, 2831, 2851, 2911, 2931, 2951, 2971 and

2991 No. 3 Road

Director's Recommendations

That a Development Permit be issued which would:

- 1. permit the construction of a development with two (2) two-storey commercial buildings totalling 2368 m² at 2671, 2711, 2811, 2831, 2851, 2911, 2931, 2951, 2971 and 2991 No. 3 Road on sites zoned "Auto-Oriented Commercial (CA);" and
- 2. vary the provisions of Richmond Zoning Bylaw 8500 to:

- (a) reduce the required manoeuvring aisle width from 7.5 m to 6.0 m (Section 7.5.5); and
- (b) reduce the required setback of parking from property lines abutting a road from 3.0 m to 2.7 m and reduce the required setback of parking from other property lines from 1.5 m to 0.0 m (Section 7.5.17).

5. Development Permit 13-644888

(REDMS No. 4448352)

APPLICANT: Balandra Development Inc.

PROPERTY LOCATION: 8600 and 8620 No. 2 Road

Director's Recommendations

That a Development Permit be issued which would:

- 1. permit the construction of nine (9) townhouse units at 8600 and 8620 No. 2 Road on a site zoned "Low Density Townhouses (RTL4);" and
- 2. vary the provisions of Richmond Zoning Bylaw 8500 to:
 - (a) reduce the minimum lot width on major arterial road from 50.0 m to 43.29 m;
 - (b) reduce the front yard setback from 6.0 m to 5.4 m; and
 - (c) permit seven (7) small car parking spaces.

6. Development Permit 14-658285

(REDMS No. 4497016)

APPLICANT: Western Gardenia Garden Holdings Ltd.

PROPERTY LOCATION: 7571 and 7591 St. Albans Road

Director's Recommendations

That a Development Permit be issued which would:

1. permit the construction of sixteen (16) three storey townhouse units on a consolidated lot including 7571 and 7591 St. Albans on a site zoned "High Density Townhouse (RTH4);" and

- 2. vary the provisions of Richmond Zoning Bylaw 8500 to:
 - (a) reduce the minimum building side yard setback from 4.5 m to 3.9 m at the southeastern corner of the building;
 - (b) increase the maximum bay window projection from 0.6 m to 0.9 m to the south property line adjacent to Jones Road; and
 - (c) allow seven (7) small car parking stalls at the site.

7. Development Permit 14-677534

(REDMS No. 4525740)

APPLICANT: Onni 7771 Alderbridge Corp. Inc.

PROPERTY LOCATION: 7008 River Parkway and 7771 Alderbridge Way

Director's Recommendations

That a Development Permit be issued which would:

- 1. permit the construction of a 324-unit apartment project in two (2) six-storey buildings over connected concrete parking structures located at 7008 Alderbridge Way and 7771 Alderbridge Way; and
- 2. vary the provisions of Richmond Zoning Bylaw 8500 to:
 - (a) reduce the required exterior side yard setbacks for portions of partially below-grade parking structures from 3.0 m to 0.0 m on the west side of the site along Cedarbridge Way;
 - (b) reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the east property line of the site;
 - (c) reduce the required rear yard setback for the attached below-grade parking structures from 1.5 m to 0.0 m on both sides of the future property line that will separate the two (2) future lots to be subdivided within the site;
 - (d) reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15 spaces/dwelling unit for the development; and
 - (e) reduce the requirement for the provision of one (1) WB-17 loading space to zero (0).

8. New Business

- 9. Date of Next Meeting: Wednesday, April 15, 2015
- 10. Adjournment





Time:

3:30 p.m.

Place:

Council Chambers

Richmond City Hall

Present:

Robert Gonzalez, Chair

Cathryn Volkering Carlile, General Manager, Community Services

Cecilia Achiam, Director, Administration and Compliance

The meeting was called to order at 3:30 p.m.

Minutes

It was moved and seconded

That the minutes of the meeting of the Development Permit Panel held on Wednesday, February 25, 2015, be adopted.

CARRIED

1. Development Permit 14-657872

(File Ref. No.: DP 14-657872) (REDMS No. 4491744)

APPLICANT:

Yamamoto Architecture Inc.

PROPERTY LOCATION:

9055 Dayton Avenue (Formerly 9051 and 9055 Dayton

Avenue)

INTENT OF PERMIT:

permit the construction of 23 two-storey townhouse units at 9055 Dayton Avenue on a site zoned "Low Density Townhouses (RTL2)."

Applicant's Comments

Taizo Yamamoto, Yamamoto Architecture Inc. and Patricia Campbell, PMG Landscape Architects, provided a brief overview of the proposed application regarding (i) urban design, (ii) architectural form and character, and (iii) landscape and open space design.

Mr. Yamamoto commented on site access and amenity areas, noting that access to the site will be via Dixon Avenue and Dayton Avenue and that there will be one amenity building, one amenity play area and one amenity gardening area.

Ms. Campbell spoke on the proposed landscape plan and noted the following:

- the children's play area will feature natural play elements;
- a small community garden will be located on the west side of the site;
- the applicant is proposing to plant 86 conifers on-site;
- the applicant has met with adjacent property owners to propose options for screening;
- screening options include a six-foot fence with a ten-foot cedar hedge, a chain-link fence with a ten-foot cedar hedge or standalone ten-foot cedar hedges;
- the proposed screening options are supplemented with hedges and trees.

Panel Discussion

Discussion ensued with regard to the screening options chosen by adjacent property owners.

In reply to queries from the Panel regarding screening, Ms. Campbell noted that consultation was done with each adjacent property owner. Mr. Yamamoto added that options can be examined to retain the existing hedge.

Jackson Lee, Jacken Homes, commented on the proposed application noting that (i) the applicant consulted with adjacent property owners with regard to privacy matters by going door-to-door and through written correspondence, (ii) the applicant is proposing that the perimeter fence and hedges be installed in four weeks' time prior to construction, (iii) temporary fencing will be used during the installation of the new screening elements, (iv) the applicant has secured a source for the new hedges, and (v) the proposed fencing will be six feet tall and the hedges will be ten feet tall.

In reply to queries from the Panel, Ms. Campbell and Mr. Lee noted that (i) drip irrigation will be installed for the new hedges, (ii) the applicant will provide a landscape security as part of the development permit application, and (iii) the future maintenance of the hedges will be conducted by the development's strata corporation.

Gallery Comments

Kathy Stephens, 8371 Heather Street, expressed concern with regard to (i) privacy matters, (ii) retaining the original perimeter hedging, (iii) the consultation and notification process related to the proposed application, (iv) perimeter drainage, and (v) potential negative effects of the proposed development to a tree on her property.

The Chair advised that the applicant made the effort to consult adjacent property owners with regard to the proposed perimeter screening. He added that the retention of the perimeter hedging was not a condition to the rezoning of the site and that a tree retention condition was placed with a 2:1 tree replacement ratio for any tree removed. Also, the City holds security for one year to ensure landscaping is maintained.

Discussion ensued with regard to flooding concerns and site preloading. The Chair noted that the applicant will be required to provide perimeter drainage on the subject site as part of the building permit process.

Irene Webster, 8291 Heather Street, expressed concern regarding (i) historical flooding in the area from insufficient perimeter drainage, (ii) the proposed perimeter hedges, and (iii) potential negative effects of the proposed development to trees on her property and the possible replacement of damaged trees.

Discussion ensued with respect to the tree retention plan and in reply to queries from the Panel, Mr. Craig advised that Ms. Webster is expressing concern that the trees on her property could be damaged from the proposed development, but are identified for protection in the proposed Tree Retention Plan. Ms. Webster added that she was of the opinion that the City should accountable if the trees cause any damage in the future.

Mike Thorne, 8371 Heather Street, expressed concern regarding possible flooding from the proposed development and retaining the existing hedges as habitat for wildlife.

Panel Discussion

Kerin Matthews, Mountain Maple Ltd., commented on the existing hedging on-site and noted that the existing hedge was not properly maintained and could not be trimmed back further, and as such, a heavy snow load could cause parts of the hedge to fail. She added that she was of the opinion that the existing hedges have reached maturity and that new plants cannot be added without damaging the existing hedges.

In reply to queries from the Panel, Ms. Campbell noted that the existing hedge was planted inside the property line.

Discussion ensued with respect to retaining sections of the existing hedge and installing new fencing. In reply to queries from the Panel, Mr. Yamamoto advised that in order to maintain continuity and visual screening; replacement of the entire perimeter hedging is the recommended option. Mr. Yamamoto added that due to its current state, the existing hedge would be difficult to maintain.

Discussion then ensued with regard to the visual screening from the proposed perimeter hedges and Ms. Matthews noted that the replacement hedges will grow over a few years and provide dense visual screening.

In reply to queries from the Panel, Mr. Craig advised that (i) the future strata corporation will not be able to remove the proposed perimeter hedges and will be responsible for its maintenance, (ii) staff are aware of the local flooding concerns in adjacent properties and, (iii) perimeter drainage is required for the proposed development.

Correspondence

Wilson Leung, 9111 Dayton Avenue, (Schedule 1)

In reply to queries from the Panel, Mr. Yamamoto advised that the proposed heat pumps used on-site comply with noise bylaw guidelines. He added that there will be a heat pump located on the roof of the amenity building.

Mr. Yamamoto noted that information regarding the heat pumps and their proposed locations on-site is available from the applicant. The applicant was then directed to provide heat pump information to staff.

Panel Discussion

Discussion ensued regarding (i) the proposed replacement perimeter hedges, (ii) privacy concerns, and (iii) potential damage to the existing perimeter hedges.

As a result of the discussion the following **referral** was introduced:

It was moved and seconded

That the staff report titled Application by Yamamoto Architecture Inc. for a Development Permit at 9055 Dayton Avenue (Formerly 9051 and 9055 Dayton Avenue), dated February 10, 2015, from the Director, Development, be referred back to staff to examine options to address:

- 1. privacy concerns of two adjacent property owners; and
- 2. a viable long-term perimeter hedge solution.

CARRIED

2. Development Permit 14-668373

(File Ref. No.: DP 14-668373) (REDMS No. 4497027)

APPLICANT:

Kirk Yuen of Cape Construction (2001) Ltd.

PROPERTY LOCATION:

13040 No. 2 Road

INTENT OF PERMIT:

- 1. Permit the construction of a four-storey mixed-use commercial/residential building containing approximately 55 residential units and 349.3 m² (3,760 ft²) of commercial space at 13040 No. 2 Road on a site zoned "Commercial Mixed Use (ZMU24) London Landing (Steveston);" and
- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to reduce the required number of off-street loading spaces from two (2) to one (1).

Applicant's Comments

Tom Bell, GBL Architects Inc., gave a brief overview of the proposed development regarding (i) urban design, (ii) conditions of adjacency, (iii) architectural form and character, (iv) vehicle parking, and (v) building setbacks.

Mr. Bell noted that the proposed development is a four storey wood frame building with townhouse units on the first two floors and other apartment units on the upper floors. He added that the building is setback from all three shared property lines.

Mr. Bell spoke of the parkade and noted that the average height of the parkade wall along the north edge of the site will be five feet above grade and will feature a landscaped planter, walkway and railings above the wall. Also, the south edge of the parkade will be the same height as the neighbouring development.

Discussion ensued with regard to universal accessibility, and in reply to queries from the Panel, Mr. Bell advised that there will be ramp access at the rear of the site and that adaptable units will be interspersed throughout the proposed development.

Ms. Campbell spoke of the proposed landscape and open space design and noted the following:

- the proposed development will have an urban streetscape along No. 2 Road and will include street trees;
- access to the site will include ramps and stairs;
- amenity features will include community garden space, a child play area, child play structures and patio areas;
- landscaping will include hedges and a variety of trees; and
- the site will connect to the City's greenway to the northeast across a right-of-way on the neighbouring site.

Panel Discussion

In reply to queries from the Panel, Ms. Campbell noted that (i) the commercial units will have access to a covered outdoor patio space, (ii) hosebibs will be installed in the common patio and in each residential patio space, (iii) irrigation will be installed for the landscaping on-site, and (iv) and the orchard will be in a grassy area, however will be accessible via a ramp.

Staff Comments

Mr. Craig advised that there is a variance associated with the proposed application to reduce the number of truck loading spaces from two to one. The loading space provided will be shared between residential and commercial units and appropriate legal agreements related to the shared use will be secured.

Mr. Craig noted that there is a servicing agreement for frontage improvements along No. 2 Road.

Correspondence

None.

Gallery Comments

None.

Panel Decision

It was moved and seconded

That a Development Permit be issued which would:

- 1. permit the construction of a four-storey mixed-use commercial/residential building containing approximately 55 residential units and 349.3 m² (3,760 ft²) of commercial space at 13040 No. 2 Road on a site zoned "Commercial Mixed Use (ZMU24) London Landing (Steveston);" and
- 2. vary the provisions of Richmond Zoning Bylaw 8500 to reduce the required number of off-street loading spaces from two (2) to one (1).

CARRIED

3. General Compliance Ruling for "Phase 2" Development Permit 13-642725

(File Ref. No.: DP 13-642725) (REDMS No. 4486028)

APPLICANT:

Amela Brudar - GBL Architects

PROPERTY LOCATION:

8888 Patterson Road and 3333 Hazelbridge Way

INTENT OF PERMIT:

That the attached plans involving changes to the design of the Phase 2 building at 8888 Patterson Road and 3333 Hazelbridge Way (addressed as 8988 Patterson Road) are considered to be in General Compliance with Development Permit (DP 13-642725).

Applicant's Comments

With the aid of a visual presentation (attached to and forming part of these minutes as Schedule 2) Amela Brudar, GBL Architects, provided an overview of the proposed application and noted the following:

proposed design changes will accommodate Vancouver Coastal Health (VCH) and structural design requirements, however the overall design of the proposed development would be sustained or improved;

- design changes are proposed for the amenity areas, the roof top mechanical areas, and the landscaping and park;
- the basketball court has been relocated from level three to level two to raise the ceiling height;
- the swimming pool will be accessed through change rooms and will have adult and children areas;
- additional mechanical equipment for boilers, water tanks and HVAC components are located on the rooftop;
- the mechanical equipment on the roof top will be enclosed or screened using metal panels;
- straight columns will be used instead of V columns along the amenity area; and
- there will be a canopy on the swimming pool area for shading.

Grant Brumpton, PWL Partnerships, spoke of the proposed changes to the landscape and open space design and noted the following:

- amenity programming was reorganized to accommodate for the proposed design changes;
- elements of the swimming pool area were redesigned to comply with VCH requirements;
- the amenity space is three feet narrower, however all original programming remain in place;
- screening was added in the pavilion area to address privacy concerns in areas facing the pool and private residential units;
- patios and walls along the Garden City Road have been lifted by 0.67 metres and plant screening has been added to compensate for the lift;
- amenities such as play and urban agriculture areas remain but have been rearranged on Level 11 to accommodate for the added mechanical structures;
- the water feature will be flowing over a weir and will be located right up to the road;
- the Garden City Road frontage will have seating elements and a tiered landscape with multi layers of vegetation, including edible plants;
- the ramp system will utilize a 5% grade;
- air intake vents will be integrated into some of the park's play structure; and
- the electrical kiosk in the lobby has been replaced with a gas meter, which will be screened by plant material.

Panel Discussion

In reply to queries from the Panel, Mr. Brumpton noted that (i) public and private areas will be defined by using the water feature and vegetation, (ii) paving material will be textured to deter any skateboarding activities on-site, (iii) the exposed wall adjacent to the Garden City Road ramp system will use a stone veneer and will feature vines, and (iv) the shrubbery along the Garden City Road frontage will not impact the adjacent sidewalk.

Staff Comments

Mr. Craig wished to acknowledge the applicant's effort in working with staff to maintain the original design intent. He added that the proposed changes to the park landscaping design were reviewed by the Parks Department.

Correspondence

None.

Gallery Comments

None.

Panel Decision

It was moved and seconded

That the attached plans involving changes to the design of the Phase 2 building at 8888 Patterson Road and 3333 Hazelbridge Way (addressed as 8988 Patterson Road) are considered to be in General Compliance with Development Permit (DP 13-642725).

CARRIED

4. Development Permit 14-660646

(File Ref. No.: DP 14-660646) (REDMS No. 4498706)

APPLICANT: Treviso Development Ltd. (0954462 BC Ltd.)

PROPERTY LOCATION: 22691 and 22711 Westminster Highway

INTENT OF PERMIT:

- 1. Permit the construction of eleven (11) three-storey townhouse units at 22691 and 22711 Westminster Highway on a site zoned "Town Housing (ZT11) Hamilton;" and
- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to allow tandem parking spaces in eleven (11) townhouse units.

Applicant's Comments

Sig Toews, Jordan Kutev Architects, provided a brief overview of the proposed application regarding (i) urban design, (ii) architectural form and character, and (iii) conditions of adjacency.

Mr. Toews noted that the proposed development will have 11 townhouse units. He added that access to the site is through Westminster Highway and the proposed development meets Hamilton Area Plan guidelines. Also, he advised that the site grade will be raised by two feet.

Mary Chan Yip, PMG Landscape Architects, commented on the proposed development's landscape and open space design and noted the following:

- units along Westminster Highway will have a semi-private yard and will feature a low fence, green buffer with shrub material and three layers of trees;
- the outdoor amenity space is located along the south edge of the site and will feature play elements, a community garden and fruit trees;
- McLean Park is within walking proximity;
- the small scale development allows opportunities for street play on site;
- the courtyard can offer play opportunities;
- the site elevation will be raised to address flood plain concerns;
- retaining walls will be used to meet grades of adjacent properties;
- the townhouses are elevated 1.55 metres, the street level is elevated 0.7 metres and entrance to the building is graded up; and
- perimeter drainage will handle run-off from the site.

Panel Discussion

In reply to queries from the Panel, Ms. Yip noted that there will be perimeter fencing onsite and will have a contemporary design to reflect the proposed development's architectural form and character.

Staff Comments

Mr. Craig advised that (i) a variance is associated with proposed development to increase the ratio of tandem parking on-site, (ii) rezoning of the site occurred prior to Council direction to limit tandem parking on new developments, (iii) the increase in tandem parking allows for lot coverage less than the maximum permitted, (iv) a covenant registered on title for all townhouse units ensures that tandem parking is not converted into habitable space, (v) there will be a servicing agreement for frontage improvements along Westminster Highway, and (vi) the proposed development will be designed to meet EnerGuide 82 standards.

In reply to queries from the Panel, Mr. Craig advised that there will be one convertible unit proposed on-site.

Panel Discussion

Discussion ensued with regard to tandem parking and bicycle storage in the garage.

Correspondence

None.

Gallery Comments

None.

Panel Discussion

Discussion ensued with regard to the proposed development's architectural form and character.

Panel Decision

It was moved and seconded

That Development Permit be issued which would:

- 1. permit the construction of eleven (11) three-storey townhouse units at 22691 and 22711 Westminster Highway on a site zoned "Town Housing (ZT11) Hamilton;" and
- 2. vary the provisions of Richmond Zoning Bylaw 8500 to allow tandem parking spaces in eleven (11) townhouse units.

CARRIED

5. Development Permit 14-671600

(File Ref. No.: DP 14-671600) (REDMS No. 4500024)

APPLICANT: Am-Pri Developments (2012) Ltd.

PROPERTY LOCATION: 9580, 9600, 9620, 9626, 9660 and 9680 Alexandra Road

INTENT OF PERMIT:

Permit the construction of 96 three storey townhomes at 9580, 9600, 9620, 9626, 9660 and 9680 Alexandra Road on a site zoned "Town Housing (ZT67) – Alexandra Neighbourhood (West Cambie)."

Applicant's Comments

Mr. Yamamoto briefed the Panel on the proposed development regarding (i) urban design, (ii) architectural form and character, (iii) conditions of adjacency, (iv) amenity spaces, and (v) public art.

Mr. Yamamoto noted that (i) the applicant is proposing 96 townhouse units over six lots along Alexandra Road and Alderbridge Way, (ii) there is a right-of-way greenway that runs along the northwest corner of the site and a wildlife corridor will be provided in the vegetation strip along the eastern edge of the site, (iii) most unit entrances will be off the street or common walkways, (iv) entrances will be off the driveway for units adjacent to the greenway, (v) there will be one amenity area on the centre of the site and a smaller amenity area on the eastern section of the site, and (vi) the developer has hosted a Public Art Studio at Emily Carr University of Art and Design as part of their Public Art response.

Darren Miller, Stantec, briefed the Panel on the landscape and open design and noted that (i) a three metre wide native species vegetation strip will run along the eastern edge of the site, (ii) the amenity area will include playground equipment and natural play elements, (iii) paving patterns are designed to give prominence to various crosswalk areas, (iv) the units will have higher shrub level planting, and (v) the two amenities spaces promote walking on-site.

Panel Discussion

In reply to queries from the Panel, Mr. Miller noted that seating benches will be installed throughout the site.

In reply to queries from the Panel, Mr. Yamamoto advised that the mailboxes will be located inside the amenity room.

Discussion ensued with regard to shared access with the adjacent property to the west and emergency access. In reply to queries from the Panel, Mr. Yamamoto noted that the proposed development will provide shared access, however the adjacent property will have a separate emergency access. He added that the site plan does not have provisions to create another driveway loop within the neighbouring site and that the ends of the drive aisles may be treated with bollards.

Discussion then ensued with regard to the design of the greenway on the northwest edge of the site, and Mr. Miller noted that landscape designs are in the preliminary stages and are under discussion with staff as the greenway will be part of the servicing agreement.

In reply to queries from the Panel, Mr. Miller noted that there will be a variety of play equipment in the amenity area that will suit all age groups. He added that the other amenity area will not have play equipment, but will have tables and seating elements.

In reply to queries from the Panel, Mr. Yamamoto, noted that there are 10 convertible units integrated throughout the proposed development.

Staff Comments

In reply to queries from the Panel, Mr. Craig noted that (i) there is a greenway along the northwest corner of the site, (ii) the servicing agreement will provide for the greenway and frontage improvements along Alexandra Road, (iii) the proposed development will have indoor amenity space, (iv) the proposed development will be designed to achieve EnerGuide 82 standards, and (v) the proposed development is not in the West Cambie Alexandra District Energy Utility area, however will achieve the City's sustainability requirements.

Correspondence

None.

Gallery Comments

None.

Panel Decision

It was moved and seconded

That a Development Permit be issued which would permit the construction of 96 three storey townhomes at 9580, 9600, 9620, 9626, 9660 and 9680 Alexandra Road on a site zoned "Town Housing (ZT67) – Alexandra Neighbourhood (West Cambie)."

CARRIED

- 6. New Business
- 7. Date of Next Meeting: Wednesday, March 25, 2015
- 8. Adjournment

It was moved and seconded *That the meeting be adjourned at 5:28 p.m.*

CARRIED

	Certified a true and correct copy of the Minutes of the meeting of the Development Permit Panel of the Council of the City of Richmond held on Tuesday, March 10, 2015.
Robert Gonzalez Chair	Evangel Biason Auxiliary Committee Clerk

CityClerk

Schedule 1 to the Minutes of the Development Permit Panel meeting held on Tuesday, March 10, 2015.

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From:

Badyal, Sara

Sent:

March 6, 2015 14:26

To:

CityClerk

Subject:

Correspondence regarding March 10, 2015 Development Permit Panel meeting

Categories:

08-4105-20-2014657872 - 9051 Dayton Ave - DP

From: W L [mailto:wleungws@gmail.com]
Sent: Friday, 06 March 2015 01:53 PM

To: Badyal, Sara

Cc: Re be

Subject: Heat pump installation at 9055 Dayton Ave (DP 14-657872)

Hi Sara,

We learn from documents related to DP 14-657872 that, heat pumps will be installed on the property. We have some concerns regarding the operating noise from these devices:

- 1. Is there any by-law regarding these devices? For example, a sound level limit in decibels.
- 2. How are these by-laws inspected and enforced?
- 3. Does the plan drawing confirm the locations of these heat pumps? If yes, I would like to take a look at the drawing in person.

Wilson



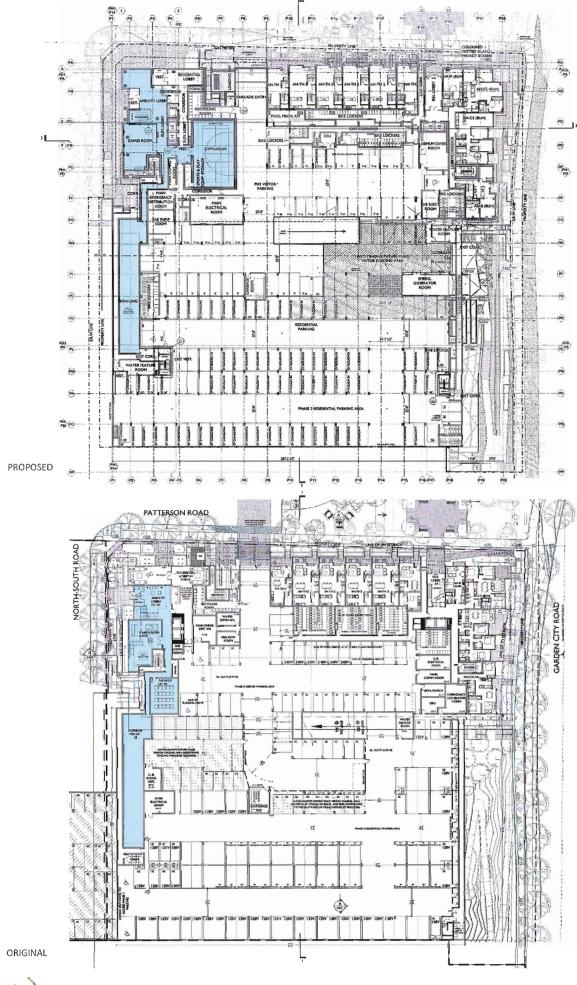
Schedule 2 to the Minutes of the

Development

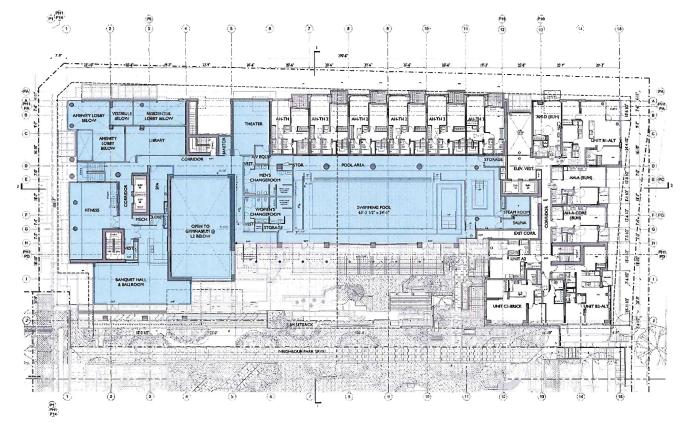
Permit

Panel

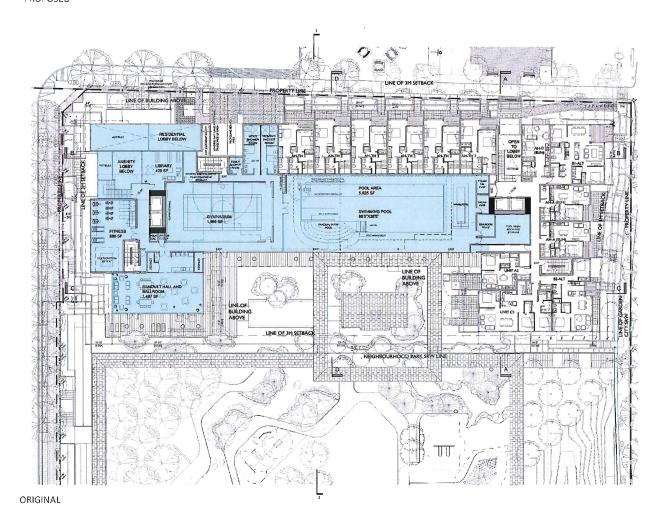




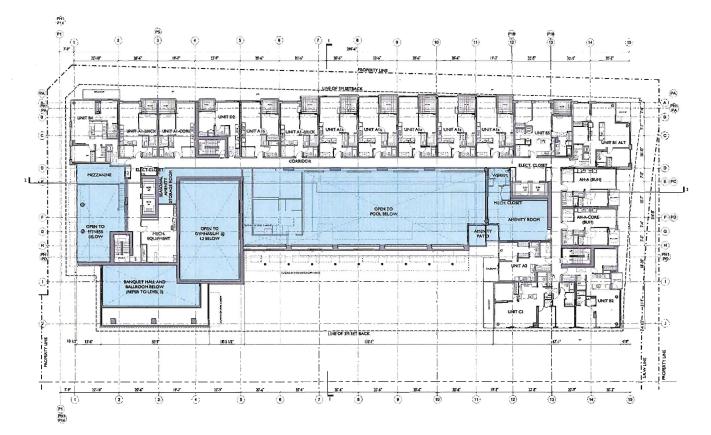




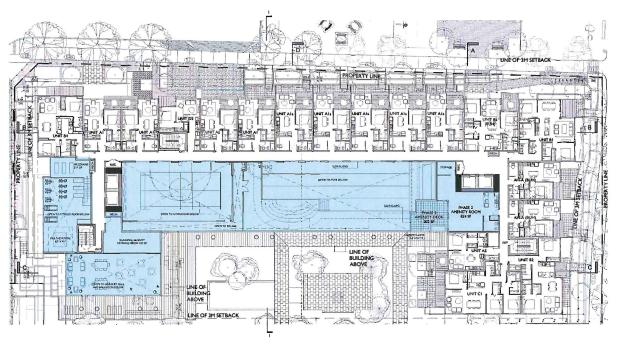
PROPOSED





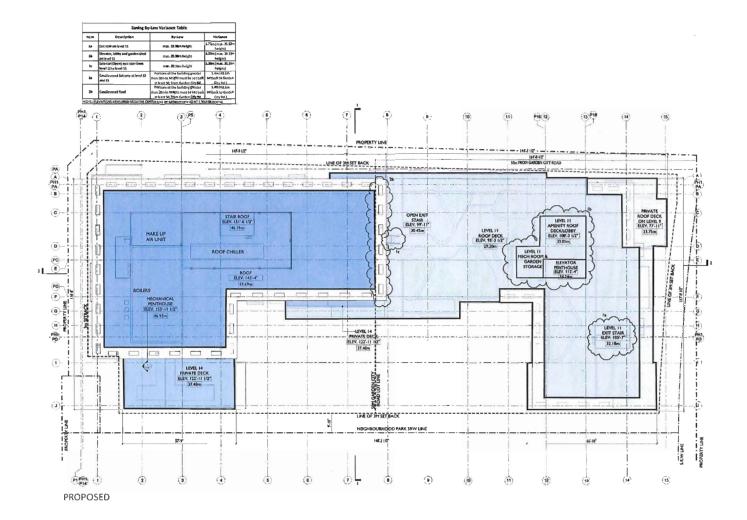


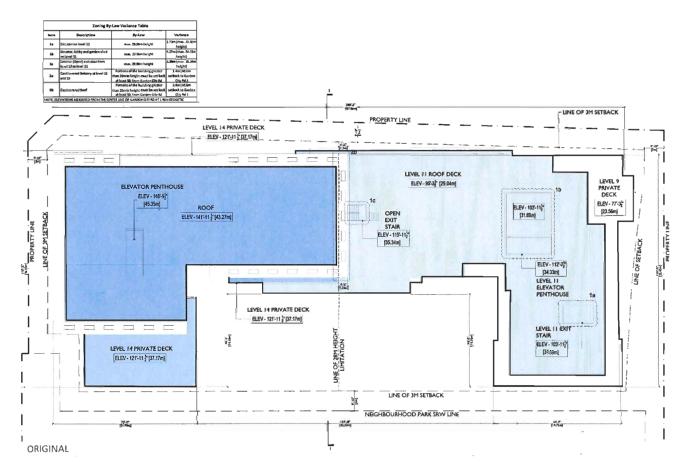
PROPOSED



ORIGINAL















PERSPECTIVE VIEW - POOL PATIO EYEBROW & COLUMNS PERSPECTIVE VIEW - LANDSCAPE PAYILION FROM BALLROOM PATIO



PROPOSED

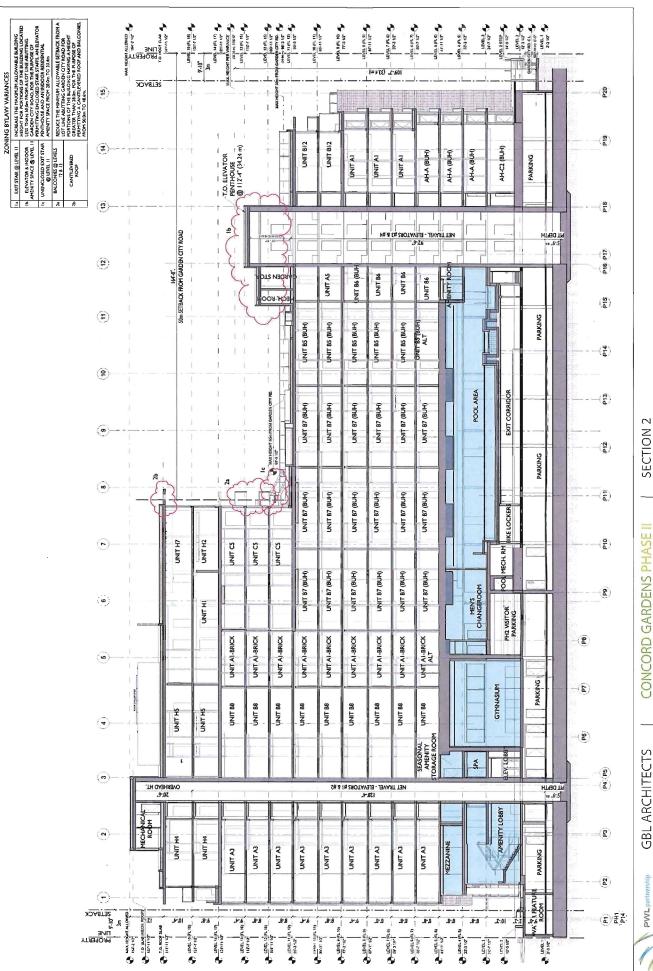


ORIGINAL









CONCORD GARDENS PHASE II

GBL ARCHITECTS

PWL partnership



PROPOSED



ORIGINAL









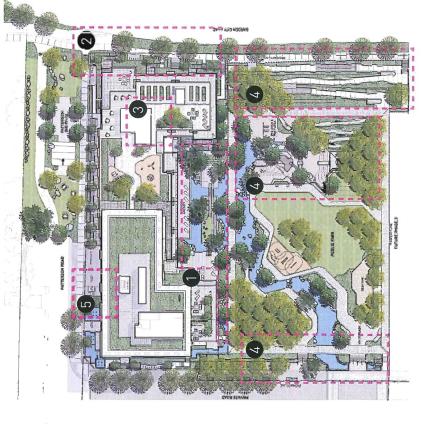






GENERAL SCOPE OF PROPOSED LANDSCAPE CHANGES TO THE APPROVED DEVELOPMENT PERMIT

- LEVEL3 PRIVATE OUTDOOR AMENITY AREA GARDEN CITY ROAD STREETSCAPE
 LEVEL11 PRIVATE OUTDOOR AMENITY AREA NEIGHBOURHOOD PARK
 ABOVE GROUND UTILITY AREA







APPROVED DP







GBL ARCHITECTS





200 200

PROPOSED AMENDMENT - VIEW FROM PARK



LEVEL 3 PRIVATE OUTDOOR AMENITY AREA APPROVED DP

PROPOSED AMENDMENT - VIEW FROM AMENITY LAWN

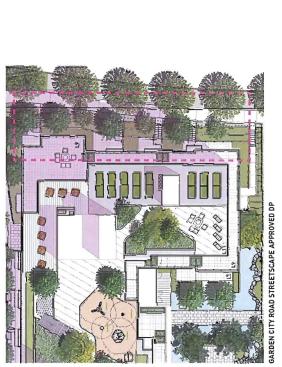


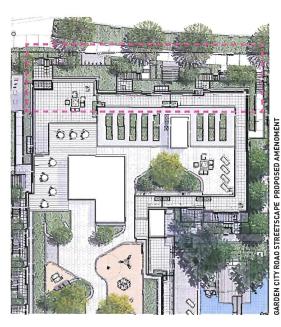
PROPOSED AMENDMENT - VIEW FROM POOL DECK





LEVEL 3 PRIVATE OUTDOOR AMENITY AREA PROPOSED AMENDMENT

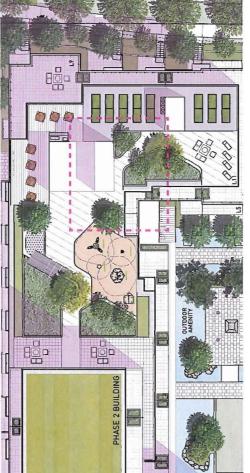




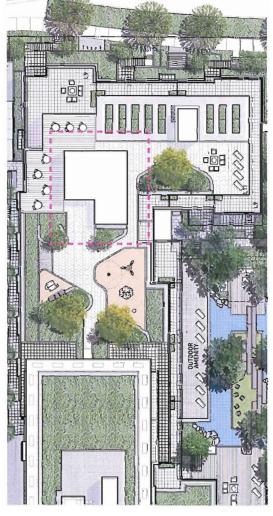




PROPOSED AMENDMENT - VIEW FROM SIDEALK



LEVEL 11 PRIVATE OUTDOOR AMENITY AREA APPROVED DP



LEVEL 11 PRIVATE OUTDOOR AMENITY AREA PROPOSED AMENDMENT

NEIGHBOURHOOD PARK PROPOSED AMENDMENT









APPROVED DP-VIEW FROM PARK ENTRY











PROPOSED AMENDMENT - CHILDREN'S PLAY AREA

NEIGHBOURHOOD PARK PRDPDSED AMENDMENT



Concord Garden Phase 2 NEIGHBOURHOOD PARK Retunned, BC





PHASE 2 BUILDING

ABOVE GRADE UTILITIES AREA APPROVED DP

PATTERSON ROAD

PATTERSON ROAD

PROPOSED AMENDMENT - VIEW FROM PATTERSON ROAD

ABOVE GRADE UTILITIES AREA PROPOSED AMENDMENT





Report to Development Permit Panel

To:

Development Permit Panel

Date:

March 2, 2015

From:

Wayne Craig

File:

DV14-676341

Director of Development

TE14-672413

Re:

Application by Rogers Communications Inc. for a Development Variance Permit

and Telecommunications Antenna Concurrence at 11771 Fentiman Place

Staff Recommendation

1. That a Development Variance Permit be issued which would vary the provisions of "Richmond Zoning Bylaw 8500" to increase the maximum accessory structure height in the "Health Care (HC)" zoning district from 12 m (39.3 ft.) to 21 m (68.9 ft.) in order to permit the installation of a temporary telecommunications antenna pole at 11771 Fentiman Place; and

2. That Richmond City Council grant concurrence to the proposed temporary telecommunications antenna pole installation for the site located at 11771 Fentiman Place for period of time extending up until October 1, 2015.

Wayne Craig

Director of Development

WC:mm Att. (5)

Staff Report

Origin

Rogers Communications Inc. has applied to the City of Richmond for permission to vary Richmond Zoning Bylaw 8500 to increase the maximum height for an accessory structure in the Health Care (HC)" zone from 12 m (39.3 ft.) to 21 m (68.9 ft.) in order to permit the construction of a temporary cellular telecommunications antenna pole for the site located at 11771 Fentiman Place.

The temporary antenna installation consists of the 21 m (68.9 ft.) antenna pole within a fenced compound with a site area of 41 m² (441 sq ft.) containing related telecom equipment to be located in the south (rear) area of the site.

The applicant had been operating antennas on the rooftop of the seven (7) storey Richmond Lions Manor on the site for approximately 12 years until Vancouver Coastal Health (VCH) vacated the building and since obtained a demolition permit from the City in February, 2015 (Attachment 2).

The initial Development Variance Permit and Telecommunications Antenna and Siting Protocol Policy (Protocol) applications were for a temporary antenna pole to accommodate cellular antenna infrastructure to provide cellular communications service in the surrounding area for up to three (3) years as the site is redeveloped by VCH.

The application was considered at the November 26, 2015 meeting of the Development Permit Panel which made the following referral motion:

That staff report titled Application by Rogers Communications Inc. for a Development Variance Permit at 11771 Fentiman Place, from the Director, Development, dated November 6, 2014, be referred back to staff to examine:

- 1. options to relocate the communications antenna on Lions Manor in the Steveston area; and
- 2. the future plans for the Lions Manor site.

Since the November 26, 2014 Development Permit Panel meeting, staff have continued to work with Rogers to develop long-term solutions to Roger's network coverage in keeping with the City's Protocol.

This report discusses how Rogers has addressed the Development Panel concerns about the future permanent relocation of the applicant's communications antenna after the removal of the subject temporary communications antenna.

Development Information

Please refer to the attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant bylaw requirements.

Background

Development surrounding the subject site is as follows:

- To the north, single-family homes zoned "Single Detached (RS1/A and RS1/E)".
- To the east, single-family homes zoned "Single Detached (RS1/A and RS1/E)".
- To the south, Steveston Park zoned "School and Institution Use (SI)".
- To the west, three (3) storey apartments zoned "Medium Density Low Rise Apartments (RAM1)".

Staff Comments

In response to the Development Permit Panel referral, Rogers has searched for an alternative permanent location which does not require a Development Variance Permit. However, after the required removal of the current antennas to allow for demolition of the Lions Manor building, there would have been a significant loss of Rogers' cellular data coverage for the Steveston area.

Emergency Special Authorization

Given the above situation, Rogers obtained a *Special Authorization* from Industry Canada to permit the planned temporary 21 m (68.9 ft.) antenna pole to be installed on the former Lions Manor site for three (3) months up until April 9, 2015. This is the maximum term that may be provided under such a *Special Authorization*. Industry Canada advised staff that that they issued this authorization due to the anticipated loss of cellular data service on which the RCMP and Richmond Fire Department depend.

The temporary pole approved under this *Special Authorization* is the same extendable, mobile 21 m (68.9 ft.) structure that was considered by Development Permit Panel on November 26, 2014 (see Attachment 2 and the plans attached to the proposed Development Permit).

New Permanent Antenna

In response to the Development Permit Panel referral, Rogers has found a permanent antenna location at the Maple Residences at 4071 Chatham Street (see Attachment 3). Rogers has received a letter of commitment from the Maple's strata council in this regard. The proposed rooftop antennas will be in conformance with Zoning Bylaw 8500 and the City's Protocol under which Rogers would obtain City staff design concurrence.

While the new permanent antenna location does not need a zoning variance or concurrence to be considered by the Development Permanent Panel, there is a need to consider a variance and concurrence for a time extension to current temporary antenna on the Lions Manor site as discussed below. This height variance is still required to be approved by Council to allow for City Telecommunications Protocol concurrence on the temporary antenna on the Lions Manor site at 11771 Fentiman Place until October 1, 2015 as discussed below.

Zoning Compliance/Variances (staff comments in bold)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

1) To increase the maximum accessory structure height of the "Health Care (HC)" zoning district from 12 m (39.3 ft.) to 21 m (68.9 ft.).

Staff do not object to the proposed height variance as discussed below.

Analysis

Temporary Antenna Pole

In response to the Development Permit Panel referral, the applicant examined the option of installing a 12 m (39.3 ft.) temporary antenna pole in conformance with the accessory structure height limit within the Healthcare (HC) zone, but found that it did not provide the necessary coverage. Thus, Rogers sought and received a *Special Authorization* from Industry Canada to permit the operation of the initially-planned temporary 21 m (68.9 ft.) antenna pole until April 9, 2015.

Also, in response to the Development Permit Panel referral, Rogers obtained a letter from VCH stating that they could operate the temporary antenna at 11771 Fentiman Place until the end of 2015, but received no further information on the future VCH plans for this property (Attachment 4). Staff have contacted VCH staff who have indicated verbally the intent is to continue to hold the site and that it is being considered within VCH's strategic facilities planning process currently underway.

Permanent Antenna Solution

Rogers estimates it will take approximately six (6) months to have the permanent site at 4071 Chatham operational. Thus, Rogers is seeking approval to operate the temporary installation on Lions Manor site to October 1, 2015 for approximately six (6) months beyond the Industry Canada's *Special Authorization* which expires on April 9, 2015 (Attachment 3).

Public Consultation

In addition to the standard 50 m (164 ft.) notification radius from sites with Development Variance Permit (DVP) applications, the City's *Telecommunication Antenna Consultation and Siting Protocol* (Protocol) requires additional consultation. Applications for telecommunications antennas over 15 m (49.2 ft.) require that the applicant mail notices to owners and occupants of properties within a radius of six (6) times the tower height from the base of the antenna pole prior to the City's DVP application notices being mailed (Attachment 5). On this basis, notices were direct-mailed to owners and occupiers within a 126 m (413.4 ft.) radius of the proposed antenna location from a mailing list provided by the City.

Under the Protocol notification process, the applicant was required to provide a 10-day reply period (ending October 31, 2014) for those receiving notices. During this period, neither the applicant nor City staff received any calls or correspondence from the public on the application. Further to applicant's consultation, the City is also required to send the standard DVP notice to all owners and occupiers within this larger 126 m (413.4 ft.) radius from the pole, in addition to the standard DVP notice of 50 m (164 ft.) from the subject property.

Notices were sent out to this expanded notification area in advance of the November 26, 2014 Development Permit Panel meeting. No correspondence was received from the public and no delegations spoke to the application at the November 26, 2014 Development Permit Panel meeting. The City has again sent notices to this expanded notification area in advance of the March 25, 2015 Development Permanent Panel meeting.

Siting and Design Considerations under the Protocol

The proposed temporary antenna pole will have relatively minor design and siting considerations under the Protocol as follows:

- The pole is proposed to be located at the rear of the relatively large 7,484 m² (1.85 acre) site at a minimum of 45 m (148 ft.) from any road frontage.
- There are a number of nearby large trees located to the south in Steveston Park and on the west of the site which provide screening of the temporary installation.
- The proposed antenna is to be painted forest green to blend-in visually with the adjacent trees.

Given the above location considerations and the shortened six (6) month maximum time period for the antenna pole to be in place, no significant design changes are recommended.

Conclusions

The proposed scheme attached to this report has satisfactorily addressed the siting and consultation requirements of the City's Protocol identified previously as part of the review of the subject Development Variance Permit application. In addition, it complies with the "Health Care (HC)" zone except for the subject zoning height variance.

Furthermore, staff support the revised application as Rogers has found an alternative location for a permanent antenna location which will be operational within six (6) months instead of the previously anticipated three (3) years for the VCH Lions Manor site. Therefore, staff recommends that the proposed Development Variance Permit be supported and forwarded to Council for consideration of issuance.

Mark McMullen

Senior Coordinator-Major Projects

MM:rg

The following are to be met prior to forwarding this application to Council for approval:

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (http://www.richmond.ca/services/ttp/special.htm).

Attachments:

- 1. Development Application Data Sheet
- 2. Letter from Rogers Communications Inc. dated October 6, 2014
- 3. Letter from Standard Land Company Inc. dated February 6, 2015
- 4. Letter from Vancouver Coastal Health dated February 27, 2015
- 5. Notification Areas for Development Variance Permit and Telecommunications Antenna Applications



Development Application Data Sheet

Development Applications Division

DV 14-676341/TE 14-672413

Attachment 1

Address: 11771 Fentiman Place

Vancouver Coastal Health

Applicant: Rogers Communications Inc.

Owner: Authority

Planning Area(s): Steveston

Floor Area Gross: Not applicable

Floor Area Net: Not applicable

	Existing	Proposed	
Site Area:	7,484 m ²	7,484 m ²	
Land Uses:	Congregate Housing	TBD	
OCP Designation:	Apartment Residential	No Change	
Zoning:	Health Care (HC)	No Change	
Number of Units:	Not applicable	Not applicable	

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	1.0	N/A	none permitted
Lot Coverage:	Max. 45%	N/A	none
Setback – Front Yard:	Min. 6 m	6 m	none
Setback – Side Yard:	Min. 6 m	19 m	none
Setback – Side Yard:	Min. 6 m	>6 m	none
Setback – Rear Yard:	Min. 6 m	14.5 m	none
Height (m):	Max. 12 m	21 m	9 m (from 12 m to 21 m)
Lot Size:	N/A	7484 m²	,



Rogers Communications Inc.

1600 – 4710 Kingsway Burnaby, British Columbia V5H 4W4 rogers.com

Attn: Wayne Craig, Mark McMullen
Director of Development
City of Richmond
6911 No. 3 Road
Richmond, BC V7C 5B2

October 6, 2014

RE: Temporary Use Permit for a Telecommunications Facility in Steveston (11771 Fentiman Place/Richmond Lions Manor)

Background

Rogers Communications Inc. (Rogers) has been operating on the rooftop of a seven story concrete building called Richmond Lions Manor at 11771 Fentiman Place for approximately 12 years (December 11, 2002). The original building was built in 1972 and renovated in 2009, serving as a residential care facility for people who require assisted living services. Vancouver Coastal Health (VCH) determined in 2012, that the 40-year old building was no longer adequate to meet the complex care and needs of residents. Approximately 100 residents of this facility was relocated to a temporary site, which was the former Executive Inn Hotel at 9020 Bridgeport road.

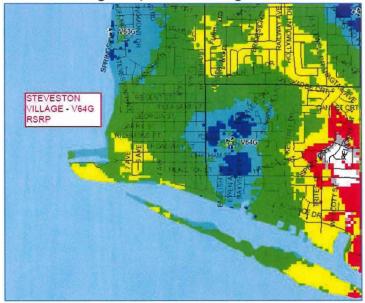
VCH has been working on a long-term solution with the Ministry of Health. At this time, VCH intends to demolish and re-develop the existing building at 11771 Fentiman Place. They have agreed to allow Rogers to relocate their antennas off the seven storey (~26m) building and onto a temporary pole structure within the same property. VCH has also agreed to allow Rogers to relocate the antennas from the temporary pole onto the rooftop of the new building.

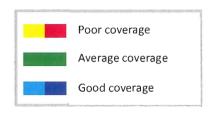
Emergency Situation – the effect of cellular site loss

The proposed temporary site services a large population of the Steveston Park neighborhood of the Steveston Area Plan. The existing cellular coverage in the Steveston Park neighborhood provides voice and data service for personal and local businesses in the area. The approximate coverage boundary for this site is as follows: east to Garry Point Park, north to Williams Road, west to No. 2 Road, and south towards the shipyards and some parts of Westham Island. The site today supports over 3,000 user interactions (i.e., calls and text messages) per day and exchange data volumes in excess of 120 gigabytes per day. Considering over 60% of 9-1-1 calls today are made from a mobile device, the loss of this site temporarily will be significant and its effect in loss of coverage will be felt in the Steveston Park neighborhood.

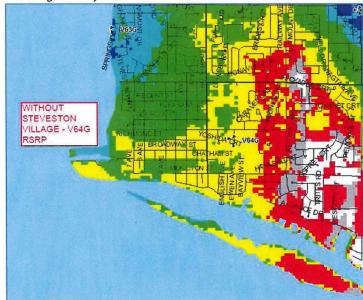


Current Coverage Plot/Loss in Coverage





Coverage today





Coverage loss if site is down

Proposed Temporary Installation

Rogers is proposing to move antennas from the rooftop of 11771 Fentiman Place onto a 21m temporary pole structure within the same property during demolishment and redevelopment of the health care facility.



Height Requirement

The existing height of the current installation is approximately 26m in height and sees onto another cellular site. A clear line of site is required between cellular sites to ensure it is connected to the network and calls/data services for users can be transferred from one site to another as they travel between two given cellular sites. In this situation, the proposed temporary pole has a minimum height requirement of 21m as that height is required to have clear line of site from the temporary pole to another cellular site. A temporary pole at 12m or 15m would not be able to communicate (transmit data) from this site to another site.

Temporary Pole Design

The proposed is a temporary pole design with no concrete pour for its foundation. The 21m pole would be similar to rooftop antenna installations where it would be secured down by multiple thick concrete blocks. The monopole design was chosen for its small footprint and thin design (at 18" in width). The antenna equipment at ground level that powered the existing antennas would also need to be relocated from its current location adjacent to building, to a location next to the temporary pole structure. Please find attached a brochure from the manufacturer (Trylon) regarding this temporary pole structure.

Duration of Temporary Pole

It is understood that Fraser Health Authority will commence demolishment of their building — Richmond Lions Manor mid-November/December of 2014. Therefore the existing rooftop on Lions Manor will need to be relocated onto the temporary pole structure the end of October. To ensure ease of transition and minimal service disruption, the temporary pole will be setup (without antenna equipment) in the last week of October. After which, the equipment from the rooftop can be moved down to the temporary pole structure for the duration of the redevelopment (approximately 3 years).

Land Use

The site is zoned for Health Care (HC) for the provision of health care and assisted living housing. Within the Official Community Plan, the site is zoned Apartment Residential (APT) for multiple family housing in the form of townhouses and apartments including congregate care, intermediated care and assisted living. It is our knowledge that this site will maintain existing zoning and land use over the next 10 years.

Next Steps

Rogers is required by VCH to remove and relocate their antennas from the rooftop onto a temporary structure by the end of October for them to commence site clearance/demolishment. Rogers is applying to the City of Richmond for temporary use permit to allow the proposed temporary pole structure to sustain existing wireless coverage service in Steveston. Rogers understands that this installation is unique and do not fall under the existing Telecommunications Protocol. Rogers is committed to working with the City in ensuring proper transition of this telecommunication infrastructure to sustain network coverage in this neighborhood.



This installation is an urgent request to ensure uninterrupted cellular coverage in this community. Please let me know if I could provide additional information.

Thank you for your assistance with this request.

Regards,

Samuel Sugita Municipal Project Manager, MCIP, RPP Rogers Communications Inc.



Notice for Temporary Telecommunications Installation

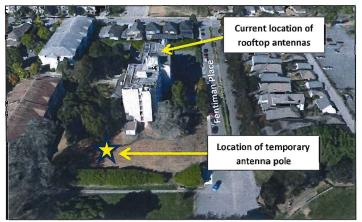
Communities across Canada rely on high quality wireless service in their day to day lives for personal, business and emergency use. To maintain service in your neighbourhood, Rogers is proposing to place a temporary 21m (69 ft) high antenna pole installation next to the Richmond Lions Manor at 11771 Fentiman Place. This installation will sustain wireless service to your neighbourhood for up to three years until new small roof-top antenna can be placed after construction work on Lion's Manor site is completed. Thus, Rogers has applied to the City of Richmond for a Telecommunications Antenna Protocol Concurrence and Development Variance Permit for the installation of this temporary antenna pole in order to maintain service in the Steveston area. As part of this permitting process, Rogers is notifying the community, ensuring residents are well informed about this project.

Included in this notice is information you may find helpful. If you have any questions, please contact Kiersten Enemark, Standard Land Company Inc., Agents to Rogers Communications Inc., before **October 31**, **2014** via e-mail at **commentsBC@standardland.com** or by telephone at (604) 687-1119.

Why does Rogers need a temporary antenna pole?

Rogers is currently servicing your neighbourhood with antennas placed on the rooftop of Richmond Lions Manor at 11771 Fentiman Place, which is a residential care facility managed by Vancouver Coastal Health (VCH). VCH has determined that the building is no longer adequate to provide the best care for its residents, and will re-develop the site over the next three years. Consequently, Rogers' existing rooftop antennas can no longer be accommodated until after major renovations are complete. Without antennas operating in the area, all wireless service dependent on the Rogers network will be lost. Rogers needs a temporary antenna pole in order to sustain existing service to the community.

Aerial View



Why is a temporary antenna installation needed at this specific location?

The current antenna location is providing high quality and dependable wireless service to residents and businesses in Steveston. Today, the rooftop antennas are servicing 3,000 user interactions (i.e., calls and text messages) per day and exchange data volumes in excess of 120 gigabytes per day. Dependable coverage from home is also critical to personal safety, with over 60% of all calls to 9-1-1 being made made from a wireless device. The loss of this antenna site will be significant to the neighbourhood without a temporary solution in place. Rogers is proposing to use the same property for a temporary antenna pole in order to maintain a high quality service.

How long will this temporary antenna pole be required?

We understand that building demolition will start in November/December 2014. To maintain service, Rogers is proposing to relocate the existing rooftop antennas to the temporary antenna pole in the first week of December 2014. Once construction on the site is completed, Rogers will relocate the antennas back onto the new building rooftop in approximatley three years time, and remove the temporary antenna pole.



What will the temporary antenna pole look like?

Similar to a hydro or a telephone pole, the temporary pole will be a slim pole design, approximately 18 inches in diametre, and 21 metres in height. Rogers proposes to place the pole on the southwest side of the property. To reduce the visibility of the structure, Rogers is painting the pole green to better blend in with the surrounding trees. The pole will be supported at the base with multiple thick concrete blocks and will occupy a ground space of approximately 4.5 metres by 9.0 metres. Below are photo-simulations that show the proposed pole design and location.



View from Fentiman Place, looking west at proposed temporary antenna pole.



View from the south end of Fentiman Place, looking northwest towards proposed antenna pole.

Is the installation safe?

The health of residents is our top concern – antenna installations are safe. All of our sites operate well below Health Canada's safety regulations, which limit the public's exposure to radiofrequency electromagnetic fields and ensure public safety for all Canadians. Rogers confirms that the temporary antenna pole will comply with Health Canada's Safety Code 6. This code is based on current, accepted scientific data. More information on health and safety can be found online at Health Canada Safety Code 6 Fact Sheet: www.hc-sc.gc.ca/ahc-asc/media/ftr-ati/_2014/2014-023fs-eng.php



Standard Land Company Inc.

Suite 610, 688 West Hastings Street
Vancouver, British Columbia
V6B 1P1

Telephone: 604.687.1119

Facsimile: 604.687.1339

standard@standardland.com

Website: www.standardland.com

February 6, 2015

MMcMullen@richmond.ca

Mark McMullen Senior Coordinator - Major Projects Planning & Development City of Richmond 6911 No. 3 Road Richmond, British Columbia V6Y 2C1

Dear Mr. McMullen,

Subject:

Temporary Antenna Pole (Development Permit 14-676341)

Address:

11771 Fentiman Place, Richmond, BC

Rogers File:

W3948 – Steveston Relocate

Rogers Communications Inc. ("Rogers") as represented by Standard Land Company ("Standard Land") respectfully requests to present to the Development Permit Panel on March 10, 2015, to seek approval for a Development Variance Permit to increase the height from 12 m (39.3 ft.) to 21 m (68.9 ft.) for the continual operation of a temporary antenna pole located at 11771 Fentiman Place, Richmond from April 1, 2015 to October 1, 2015.

Rogers, along with Standard Land, facilitated a presentation to the Development Permit Panel on November 26, 2014. The decision of the panel was not to support the temporary installation until more information is received by the City of Richmond ("City") regarding the permanent antenna solution and for Rogers to continue to work with City Planning staff. At this time, Rogers has identified a permanent antenna solution at Maple Residences at 4071 Chatham Street, Richmond. The rooftop antennas will be mounted at a height where a variance to the zoning bylaw will not be required. Rogers agrees to work with staff in obtaining design concurrence following Protocol guidelines. While a Letter of Commitment has been signed, Rogers does not expect a fully executed agreement until March 2015.

Currently, Rogers is serving the Steveston Village area with the operation of a temporary antenna pole. Rogers received special emergency authorization from Industry Canada to operate the temporary pole until April 9, 2015. By maintaining service, Rogers is able to continue to provide communication services to local RCMP and First Responders, specifically to the Richmond Fire Department who use Rogers's data lines for connectivity and Richmond RCMP who use mobile data terminals in vehicles that rely on the Rogers network to transmit data. However, Rogers estimates it will take approximately 6 months to have the permanent site operational at Maple Residences. While we have had preliminary discussions with the City regarding design, we estimate design concurrence and the building permit will take a minimum of 10 weeks. Further, it will take approximately 16 weeks to install the equipment and optimize the installation for service and a further 3 weeks to dismantle the temporary antenna pole. As a result, we are seeking approval to operate the temporary installation for a minimum of 6 months beyond the Industry Canada allowance for a temporary installation, which expired on April 9, 2015.

Rogers values its positive working relationship with the City of Richmond, and looks forward to continually serving the Steveston Village area. If you require additional information in consideration of our request, please do not hesitate to contact me at (604) 687-1119 or kierstene@standardland.com

Sincerely,

Kiersten Enemark

Director, Land and Municipal Affairs

Standard Land Company

cc: Sam Sugita, Municipal Affairs Consultant, Rogers Communications Inc.

Sharel Longhurst, Municipal Affairs Specialist, Standard Land Company









February 27, 2015

Darren Hird and Tracy Robinson Real Estate Division for Rogers Communications 1600 – 4710 Kingsway Burnaby, BC V5H 4W4

VIA: Email

Dear Darren and Tracy:

Re: Use of Land at 11771 Fentiman Place, Richmond

The purpose of this letter is to confirm, for the benefit of the City of Richmond, that Rogers Communications Inc. currently holds a legal agreement for the use of land with Vancouver Coastal Health to continuously operate a temporary antenna pole at 11771 Fentiman Place until the end of 2015, or beyond, if agreed between the parties.

Please do not hesitate to contact me should you wish for any further clarification on the above.

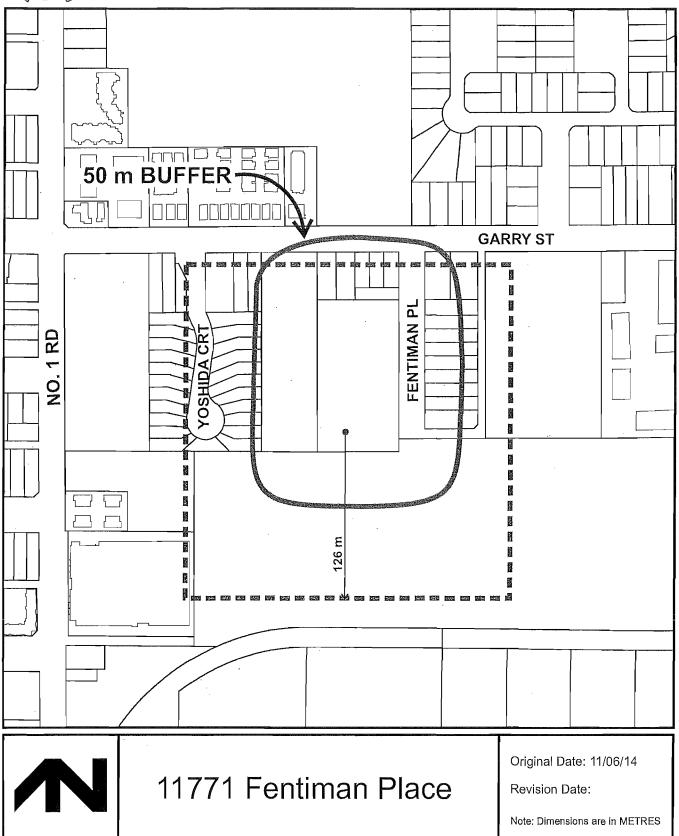
Yours sincerely,

Nicholas Bodie

Manager, Real Estate & Leasing

Cc. Mike Coney, Manager Capital Projects







Development Variance Permit

No. DV14-676341

To the Holder:

ROGERS COMMUNICATIONS INC.

Property Address:

11771 FENTIMAN PLACE

Address:

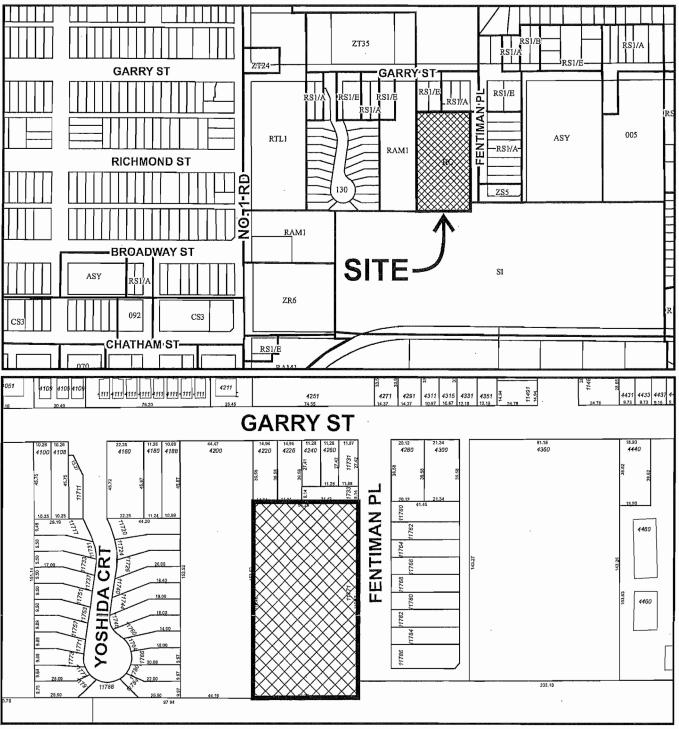
C/O SAMUEL SUGITA 1600 - 4710 KINGSWAY VANCOUVER, BC V4H 4W4

- 1. This Development Variance Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied by this Permit.
- 2. This Development Variance Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. The "Richmond Zoning Bylaw 8500" is hereby varied to increase the maximum accessory structure height of the "Health Care (HC)" zoning district from 12 m (39.3 ft.) to 21 m (68.9 ft.) in order to permit the construction of a temporary cellular antenna pole as shown on Plans #14-672413-1 to #14-672413-3 attached hereto.
- 4. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.
- 5. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse.

This Permit is not a Building Permit.

AUTHORIZING RESO DAY OF	OLUTION NO.		ISSUED BY THE COUNCIL THE	
DELIVERED THIS	DAY OF	,		
MAYOR				





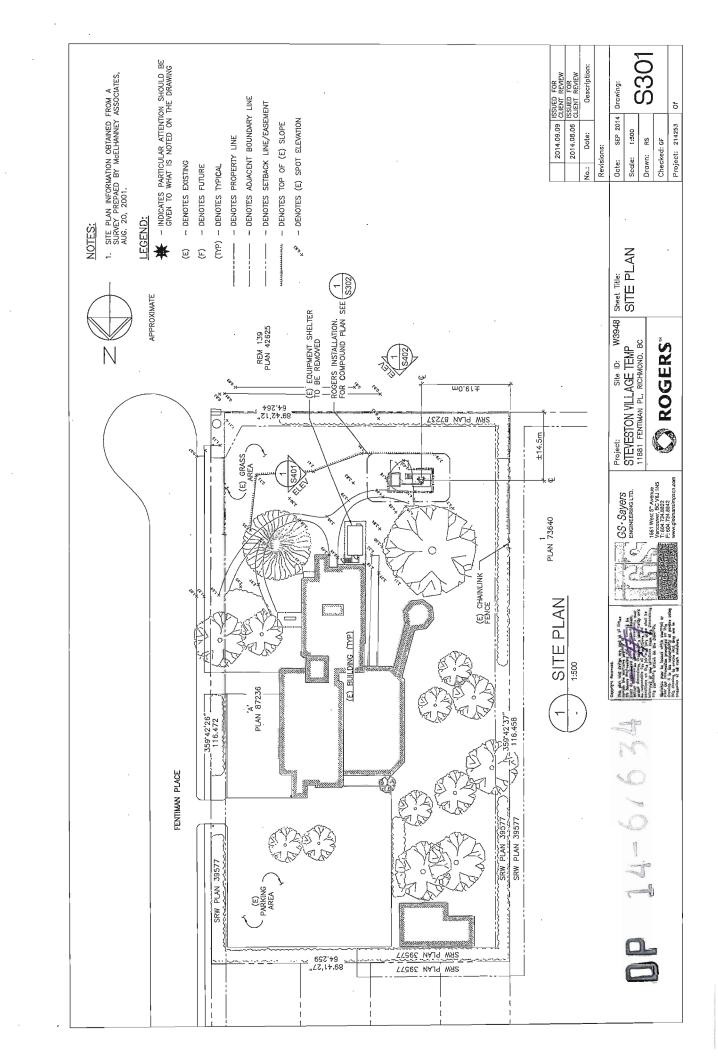


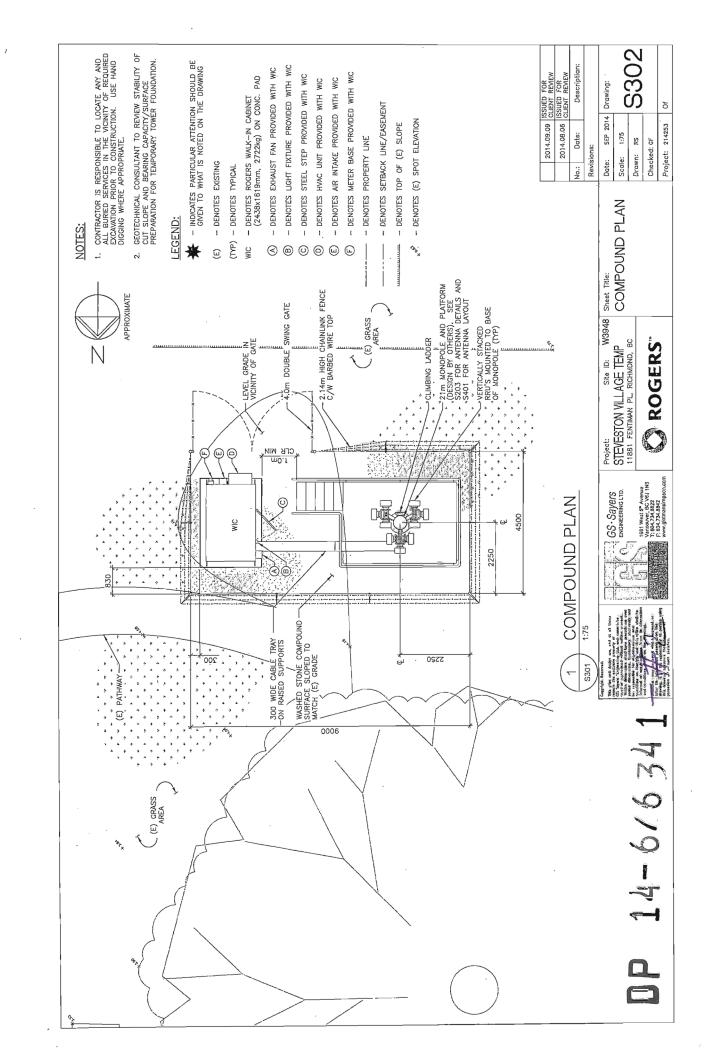
DV 14-676341 SCHEDULE "A"

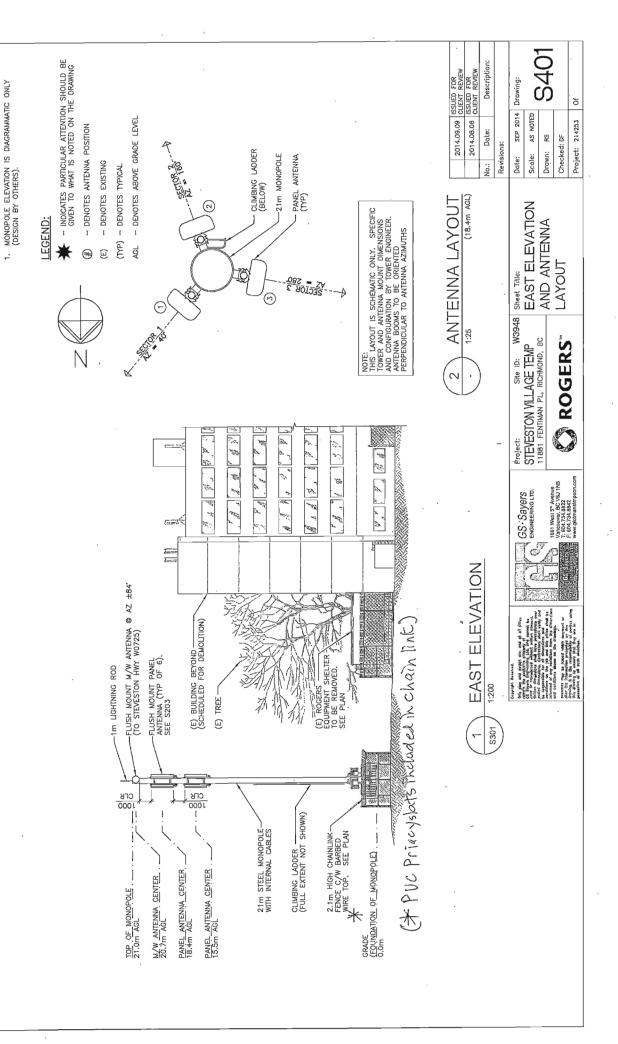
Original Date: 11/06/14

Revision Date:

Note: Dimensions are in METRES







JP 14-676341-#3



Report to Development Permit Panel

To: Development Permit Panel

Date: March 6, 2015

From: Wayne Craig

File: DP 13-641791

Director of Development

Application by Urban Design Group Architects Ltd. on behalf of 0976440 B.C.

Ltd., Inc. No. 0976440 for a Development Permit at 3011 No. 5 Road

Staff Recommendation

That a Development Permit be issued which would permit the construction of a drive-through car wash and drive-through oil change service centre at 3011 No. 5 Road on a site zoned "Car Wash & Service Station (ZC35) – Bridgeport".

Wayne Craig
Director of Dev

Director of Development

WC:dn Att.

Re:

Staff Report

Origin

Urban Design Group Architects Ltd., on behalf of 0976440 B.C. Ltd., Inc. No. 0976440, has applied to the City of Richmond for permission to develop a drive-through car wash and drive-through oil change service centre at 3011 No. 5 Road on a site that is currently vacant.

Bylaw 9174 (RZ 13-642848) to rezone 3011 No. 5 Road from "Gas Station Commercial (ZC12) –Bridgeport Road and Ironwood Area" to "Car Wash & Service Station (ZC35) – Bridgeport" has received Third Reading and the applicant is actively working to address all conditions associated with Final Adoption.

A Servicing Agreement (SA) is required in association with the rezoning application and includes but is not limited to storm sewer and sanitary service improvements that will be constructed through a separate Servicing Agreement (SA 15-692574), which must be entered into prior to final adoption of the rezoning bylaw.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

The subject site is located at the southwest corner of Bridgeport Road and No. 5 Road, is located within the Bridgeport Area generally, and is located within the Bridgeport Road Corridor specifically. The area is characterized by existing automobile-oriented commercial development. Redevelopment of the currently vacant site as a drive-through oil change and car wash service centre is compatible with nearby uses and is supported by the Bridgeport Area Plan's objective to retain the corridor as an automobile-oriented commercial area.

Development surrounding the subject site is as follows:

To the North: A range of commercial uses are located on the north side of Bridgeport Road including a Scotia Bank, restaurant and a Chevron gas station. The sites are zoned "Neighbourhood Commercial (CN)" and "Gas Station Commercial (ZC 25) – Bridgeport Area" respectively and are designated "Industrial" in the Bridgeport Area Plan.

To the East: Retail and commercial uses are located on the east side of No.5 Road. The site is zoned "Industrial Retail (IR1)" and designated "Commercial/Industrial" in the Bridgeport Area Plan.

To the South and West: Bridgeport Centre, which contains a furniture store and a tile and flooring centre that wraps around the south and west edges of the subject site with frontages on both No. 5 Road and Bridgeport Road. The site is zoned "Industrial Retail (IR1)" and designated "Commercial/Industrial" in the Bridgeport Area Plan.

Rezoning and Public Hearing Results

During the rezoning process, staff identified the following design issues to be resolved at the Development Permit stage:

- Design development of building facades including consideration of materials, signage, and illumination fixture details;
 - The building character has been redesigned from a suburban typology to a simplified building with a contemporary industrial-commercial character.
- If applicable, material and design development for screening of roof top mechanical equipment;
 - Roof top units will not be installed; instead the applicant intends to use an infrared gas tube heater and unit heaters to maximize heating efficiency.
- Opportunities to introduce additional on-site landscaping; and

The plans have been updated to include:

- o A 0.5 m wide (1.6 ft.) landscaped edge along the southern edge of the site;
- o Two (2) trees between queuing areas;
- o Landscaping around the garbage/recycling enclosure; and
- Accent pavers.
- Demonstration that the principles of Crime Prevention Through Environmental Design (CPTED) have been integrated both into the site plan and the building design.

The development design includes under soffit LED downcast light fixtures that are integrated into the building design, low perimeter landscaping, and generous use of storefront windows on all four (4) building elevations.

The Public Hearing for the rezoning of this site was held on October 20, 2014. No concerns were expressed at the Public Hearing. Staff did not receive any correspondence or phone calls related to the proposed development.

Urban Design Response

The proposed design concept is characterized as a simplified commercial building with an industrial character and addresses the significant urban design issues identified during the Development Permit application review process. The proposal addresses the urban design objectives of the Official Community Plan (OCP) and the Bridgeport Area Plan.

Advisory Design Panel Comments

The current proposal incorporates changes that reflect the proposal's consideration by the Advisory Design Panel (ADP) on November 5, 2014. Members of the ADP provided design development suggestions. In particular, the Panel advised the applicant to consider simplification of the proposed building scheme and reorganization of building elements, introduction of additional on-site landscaping, introduction of decorative pavers at the entrances to the site, and development of the proposed sustainability strategy. A copy of the relevant excerpt from the ADP Minutes from November 5, 2014 is attached for reference (Attachment 2). The design

response provided by the applicant has been included immediately following the specific Design Panel comments and is identified in "bold italics'.

Conditions of Adjacency

- The site is bordered by Bridgeport Road, No. 5 Road, and abuts a two-storey commercial building ("Bridgeport Centre") on its south and west adjacencies.
- The proposed single storey development will not cast shadows, obstruct views, or introduce overlook concerns to adjacencies.
- Noise generated by the proposed car wash is not expected to significantly affect existing and anticipated development within the area, which is designated in the Bridgeport Area Plan as an automobile-oriented commercial area. Further, the applicant has advised the overhead doors will be down when the carwash is in use and while oil change customers are serviced.
- The subject site is located more than 150 m (490 ft) from a residential area, which precludes the noises associated with operation of a car wash from affecting residential uses. Further, as a condition of rezoning bylaw adoption, the applicant is required to demonstrate that operation of the proposed car wash will comply with the City's Noise Regulation Bylaw No. 8856.

Streetscapes

- Improving the visual appearance of Bridgeport Road and enhancing the area's overall image is an objective of the Bridgeport Area Plan.
- The boulevard and street fronting conditions will be improved through the proposed development. A minimum 1.5 m (5 ft.) wide sidewalk and a treed boulevard along both Bridgeport Road and No. 5 Road will be secured through the Servicing Agreement (SA), which is a requirement of rezoning.
- The width of the boulevard ranges from the standard 1.5 m (5 ft.) requirement along Bridgeport Road to an enhanced 2.4 m (8 ft.) wide boulevard at the north east corner of the site, and 3.4 m (11 ft.) wide boulevard along No. 5 Road.

Site and Functional Planning

- Drive through carwash and oil change service is proposed on-site and the site plan accommodates the necessary turning and queuing requirements associated with the use.
- Reduction and/or consolidation of individual access points along main arterial roads is a
 priority with any redevelopment in this area. Therefore, redevelopment of the subject site
 includes traffic management provisions that control access to and from the site. The No. 5
 Road access is designed to facilitate right-in only vehicle access to the site, which will also
 be marked with signage. Through the Servicing Agreement, a raised island "pork chop" will
 be introduced to limit Bridgeport Road access to right-in, right-out movements.
- Only one-way traffic is accommodated on-site and all vehicle traffic is required to exit the site via the Bridgeport Road Access.
- The proposed low building form and modest building volume minimizes the impact of setting the building back 0.9 m (3 ft.) from Bridgeport Road. Further, the boulevard improvements that will be introduced along Bridgeport Road will mitigate the impact of the narrow building setback.

• The garbage and recycling enclosure is located at the southwest corner of the site within an enclosure to minimize its visibility.

Parking & Loading

- "Service Station" parking rates apply to the site; therefore, a total of eight (8) queuing stalls are required, in addition to five (5) parking stalls for employees and visitors.
- Two (2) parking stalls are located on the south side of the building and three (3) stalls are located on either side of the car vacuum station. Stall #8 is identified as employee parking; the remaining stalls are available for use by both employees and visitors.
- Using a turning template for a SU9 truck (9.1 m long), the applicant has demonstrated, to the satisfaction of Transportation staff, that the site plan accommodates truck movements in and out of the site.

Architectural Form and Character

- A contemporary industrial/commercial building character is proposed, which responds to the Bridgeport Area Plan's objective to enhance the overall image within the Bridgeport Road Corridor. The proposed simplified contemporary building would introduce a modern character within this automobile oriented commercial district that is expected to be replicated as the area redevelops and would refresh this corner at the busy intersection of Bridgeport Road and No. 5 Road.
- Although a single storey building is proposed, the proposed building height and roof lines are varied to distinguish the oil change and car wash uses proposed on-site.
- A simplified color scheme and materials that introduce texture are proposed and include white horizontal metal panels, sterling gray lap siding, and pewter color fiber cement panels. Corporate branding is integrated into the building character without overwhelming the contemporary character of the building. Either a horizontal blue metal band or frame is proposed for each elevation, which addresses the applicant's objective to reference a corporate identity while using color to introduce visual interest to the building façade.
- A high degree of transparency is achieved with the proposed use of store front windows and doors, spandrel glazing, and overhead doors that consist of clear glass in clear anodized frames.
- Weather protection is limited to the soffit overhang, which is supported based on the car oriented nature of the drive through uses proposed on-site.
- Signage will comply with the Sign Bylaw (No. 5560) and will be mounted to the exterior of the building.

Landscape Form and Character

- There are no trees on the site and Parks supports the removal of two (2) existing City trees that are located along No. 5 Road, which will be affected by the required sidewalk and boulevard upgrades. Compensation will be secured through the associated rezoning process.
- A double row of trees will be introduced along the road frontages and includes Katsura and Sweet Gum deciduous trees on the subject site and street trees that will be selected by the City within the boulevard.
- The applicant proposes to maximize the limited on-site landscaping opportunities by undertaking the following:

- Landscaping the southern edge of the site. The 0.5 m (1.6 ft.) landscape strip will accommodate three (3) Sweet Gum trees, Hick's Yew shrubs, and Blue Fescue grass. This strip of landscaping will be protected by a barrier curb and similar to all landscaping on-site, will be irrigated by an efficient irrigation system that includes moisture sensors;
- O Planting Slender Hinoki False Cypress, which is a tall growing evergreen shrub that may reach a height of 4.5 m (15 ft.) and remains dark green through the winter, and low shrubs on either side of the garbage/recycling enclosure;
- Placing oil storage tanks within the underground pit to create an opportunity to introduce a tree that is protected by a tree grate on either side of the oil change queuing area, and using permeable pavers to interrupt the surrounding asphalt treatment; and.
- O Using aqua pave permeable pavers at both access points to the site.
- The applicant has advised that monument signs will not be introduced on-site; therefore, the perimeter landscaping will be uninterrupted by signage.

Sustainability

- The applicant has provided a summary of sustainability features that will be included within the development, which include (Attachment 3):
 - o Low maintenance building materials to increase the life cycle of the building;
 - o Radiant heating and compliance with ASHRAE 2010 for energy efficiency;
 - o LED light fixtures;
 - Tanks will be installed underground to collect rainwater from the roof and to recycle gray water from the car wash to reduce the use of potable water;
 - o Collection of used oil by a third party, which will be recycled off-site;
 - o Antifreeze will be collected for recycling.

Crime Prevention Through Environmental Design (CPTED)

- On-site wayfinding is clear and sightlines are unobstructed.
- Although the proposed drive through development is car oriented, the introduction of pavers at the site's access points and defined perimeter landscaping establish an edge that defines the pedestrian public realm and provides separation from the on-site car oriented uses.
- Glass storefront windows on each of the building's elevations create opportunity for passive surveillance.
- LED downcast light fixtures are integrated into the building design. Site lighting improves overall safety, and discourages loitering and vandalism.

Accessibility

- The building will comply with Building Code accessibility conditions and provide barrier-free access from the street.
- The washroom has been designed to accommodate the turning radii required by wheel chair users.

Conclusions

The proposed drive through oil change and car wash service centre meets Bridgeport Area Plan and OCP design objectives. While addressing on-site site planning constraints, the proposal

introduces an enhanced boulevard treatment along the site's building edges, maximizes on-site landscaping opportunities, and proposes to introduce a building with a contemporary commercial-industrial building character to refresh the site, which is located at a busy intersection. The proposed development will contribute toward the process of incrementally updating the commercial character along this section of Bridgeport Road. Based on the proposal's design response to the objectives of the area plan and OCP, and site specific context, staff support the proposed development scheme.

Diana Nikolic

Planner II, Urban Design

DN:rg

Attachment 1: Data Sheet

Attachment 2: Advisory Design Panel Minutes & Applicant Responses (inserted in bold italics)

Attachment 3: Sustainability Provisions (provided by applicant)

The following condition is to be met prior to forwarding this application to Council for approval:

• Receipt of a Letter-of-Credit for landscaping in the amount of \$29,161.55.

Prior to future Building Permit issuance, the developer is required to complete the following:

- Submission of a Construction Parking and Traffic Management Plan to the Transportation Division.
 Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
- Incorporation of accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
- Demonstrate the following to the satisfaction of the Building Department:
- Demonstration that the pit area is not water permeable;
- Provide details associated with the access hatch to the mechanical room at pit elevation. The details must demonstrate that the hatch and associated area is water tight and that the hatch cannot be closed and/or locked when someone is in the area. If the floor elevation of the hatch is higher than (or at) the flood plain elevation, then the hatch will need to demonstrate the safety aspects that allow someone from inside the mechanical room to egress at any time. If the room is accessed from within the pit, the door will be required to be watertight.
 - Compliance with the BC Plumbing Code, which requires installation of a sand trap and oil interceptor.
 - Any waste water sent to the sanitary system requires compliance with Metro Vancouver Liquid Waste Management bylaws and a Waste Water Discharge Permit from Metro Vancouver. The applicant must demonstrate compliance as a condition of Building Permit issuance.
- Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Division at 604-276-4285.



Development Application Data Sheet

Development Applications Division

DP 13-641791 Attachment 1

Address: 3011 No. 5 Road

Applicant: Urban Design Group Architects Ltd. Owner: 0976440 B.C. Ltd

Planning Area(s): Bridgeport Area Plan

Commercial

Floor Area Net: 353.18 m² (3,800 ft2) Floor Area Net: 333.7 m² (3,590 ft2)

	Existing	Proposed	
Site Area:	1,289 m2 (13,870 ft2) (0.32 acre)	1,289 m2 (13,870 ft2) (0.32 acre)	
Land Uses:	Vacant	Drive-through oil change and car was (sale of oil related to oil change service)	
OCP Designation:	Mixed Employment	Mixed Employment	
Zoning:	Gas Station Commercial (ZC12) – Bridgeport Road and Ironwood Area	Car Wash & Service Station (ZC35) - Bridgeport	
Number of Units:	vacant	1 building	

	Bylaw No. 9174 "Car Wash & Service Station (ZC35)"	Proposed	Variance
Floor Area Ratio:	Max. 0.30 FAR	0.26	none permitted
Lot Coverage:	Max. 20%	16.7%	none
Setback – Bridgeport Road (Front Yard):	Min. 0.9 m	0.9 m	none
Setback – No. 5 Road (Exterior yard):	Min. 10.0 m	10.4 m	none
Setback – Interior side yard and rear yard:	Min. 7.9 m	Min. 8.0 m (south) Min. 20.8 m (west)	none
Height (m):	Building: 9.0 m	6.4 m	none
Off-street Parking Spaces:	Service Station: 2/100 m2 of gross leasable floor area; plus: -1 space for each car wash bay; plus -3 spaces for each vehicle service bay	Customer/employee parking: 5 stalls Car wash: 2 queuing stalls Vehicle Service Bay: 3 queuing stalls for each oil change bay (total 6)	none

Total off-street Spaces:	12 (including queuing stalls) Customer/employee parking: 5 Queuing stalls: Car Wash: 1 Vehicle Service Bay: 6	13 (including queuing stalls) Customer/employee parking: 5 Queuing stalls: Car Wash: 2 Vehicle Service Bay: 6	none
Bike Parking	Class 1: 1 Class 2: 1	Class 1: 2 Class 2: 4	none
Amenity Space – Indoor:	n/a	n/a	none
Amenity Space – Outdoor:	n/a	n/a	none

Excerpt from the Minutes from The Design Panel Meeting

Wednesday, November 5, 2014 – 4:00 p.m. Rm. M.1.003 Richmond City Hall

3. DP 13-641791 – DRIVE THROUGH OIL CHANGE AND CAR WASH

APPLICANT:

Urban Design Group

PROPERTY LOCATION:

3011 No. 5 Road

Applicant's Presentation

Architect Fariba Gharaei, Urban Design Group Architects Ltd., and Denitsa Dimitrova, PMG Landscape Architects, presented the project and answered queries from the Panel on behalf of the applicant.

Panel Discussion

Comments from the Panel were as follows:

proposed planting on the street side is well-resolved; however, planting on the south side is negligible and will struggle to survive; consider carving out a narrow strip along the south edge for cedar hedge planting to provide separation from the adjacent property to the south and introduce a green vertical element on the south edge;

We made the building smaller to allow for a 2' strip of landscape and trees along the south property line.

• consider introducing decorative pavers on-site; e.g. as the sidewalks cross the drive aisles;

Decorative pavers are added at the main entrance of the site from No. 5 Road and Bridgeport and between the oil change and car wash.

pleased to know that the site is irrigated;

Irrigation has been provided.

• the exterior of the building has too many elements, e.g. awnings, considering its small size and function; simplify and organize the elements;

Building design is revised to address the Panel's comments. Awnings are removed and building is simplified.

canopies are out of place on the building; the proposed building should have an industrial character; look at the steel and glass frame Home Depot building at Terminal Avenue for precedent; simplify the building expression, the big plaster stucco parapet's purpose is to accommodate the signage;

Building design is revised to address the above comments. We simplified the building by removing the 4' overhang and the canopies, since the proposed building is an automated drive thru facility which weather protection is not required. Material selection is revised to avoid the use of stucco.

• understand the limitations of franchising with regard to lay-outs and plans; however, some degree of freedom on the part of the applicant with regard to the design of the building would be beneficial;

Building design is revised to address the Panel's comments.

appreciate the landscaping along the road frontages; however, a softer street edge could have been created along the streetfronts;

We have revised the plantings along the street edges with more layering and softer approach with more trees, ornamental grasses and perennials.

 site is small and challenging; concern on the form and character of the building; understand the brand requirements, however, the proposed development will not enhance the area;

Building design is revised to enhance the area.

• choice of materials is unfortunate; architectural elements, e.g. canopies fall short in many respects; stand-alone building has no clear interface with the public sidewalk; proposed form and character is a concern; building does not fit well with its context;

2'-6" concrete up stand reduce to 10" smooth finish concrete and extend the glazing to address the interface with the public sidewalk. Building is redesigned to create a more industrial look.

- no comments with regard to accessibility;
- concern on the car wash facility using high quality drinkable water that is dumped into the drainage; look at ways to recycle water; and

Reclaim water system will be provided to re-use the gray water and collect the rain water to minimize the use of potable water.

• applicant has done what could be done in a challenging site; however, the prescribed program for the franchise is not responsive to the site context.

Panel Decision

. . .

It was moved and seconded

That DP 13-641791 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Panel.

DEFEATED

Opposed: Matthew Thomson, Grant Brumpton, Kai Chen, Norm Chin, Paul Goodwin, and Michael Mammone

URBAN DESIGN GROUP ARCHITECTS LTD. 600 - 1140 W PENDER ST. VANCOUVER, BC V6E 4G1 (604) 687-2334 FAX (604) 688-7481

Paul Chiu, Architect AIBC, MRAIC, AAA, SAA, MAA, OAA, SBA, Principal Fariba Gharaei, Architect AIBC, MRAIC, OAA, LEED AP, SBA, Associate Steven Wagner, Architect AIBC, MRAIC, AIA, LEED AP, Associate Rudi Klauser, RID, NCIDQ, LEED AP, Senior Associate Crosbby Chiu, MRAIC, SBA, Senior Associate

Rick Jones, Principal Aaron Vornbrock, Senior Vice President Eric Ching, CSBA, Vice President Martin Grube, Associate Bojan Illc, Associate

June 23, 2014

City of Richmond 6911 No. 3 Road Richmond BC V6Y 2C1

Attention: Diana Nikolic, MCIP

Planner

Re: Sustainability

Mobile 1 - Oil Change & Car Wash, 3011 No. 5 Road, Richmond

Our Project No. 3674

Dear Mayor and Councilors,

This new development is for a Mobile 1 - Oil Change and Car Wash facility which incorporates the following:

- High quality building materials with low maintenance are proposed to increase the lifecycle of the building and minimize the need for repair/additional construction work.
- Radiant heating systems are to be used to heat the occupant rather than the space. This
 building is categorized as a semi-heated building and we will comply with the
 requirements of ASHRAE 2010 for energy efficiency.
- LED light-fixtures will be used to minimize both energy consumption and frequent replacement of the lighting.
- Low-flow plumbing fixtures and water wise landscaping is proposed in order to lower water consumption for this development.
- High efficiency plumbing fixtures to be provided.
- Rain water collection and re-use of the water system to be provided to minimize discharge of the water to the City sanitary line and reduce the water usage for the car wash.

Mobile 1 - Oil Change and Car Wash, 3011 No. 5 Road, Richmond

Page 2

- Recycled measures to be considered during the construction time.
- The used oil will be recycled and reused. Garbage recycling is provided to minimize the waste material.
- Antifreeze and used motor oil will be picked up for recycling.

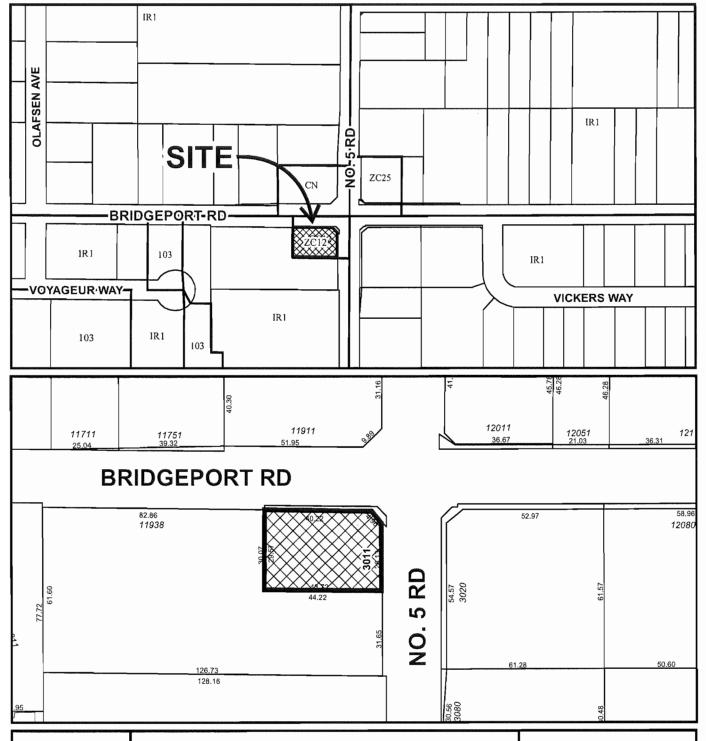
Yours Truly,

Farlba Gharael, Architect AIBC, MRAIC, OAA, LEED AP, SBA, Associate

URBAN DESIGN GROUP ARCHITECTS LTD.

FG/mp







DP 13-641791

Original Date: 03/06/15

Revision Date:

Note: Dimensions are in METRES



Development Permit

No. DP 13-641791

To the Holder:

URBAN DESIGN GROUP ARCHITECTS LTD. ON BEHALF OF

0976440 B.C. LTD., INC. NO. 0976440

Property Address:

3011 NO. 5 ROAD

Address:

C/O FARIBA GHARAEI, ARCHITECT AIBC

600 - 1140 WEST PENDER STREET

VANCOUVER, BC V63 4G1

- 1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
- 2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #7 attached hereto.
- 4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
- 5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$29,161.55 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
- 6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit

No. DP 13-641791

To the Holder:	URBAN DESIGN GROUP ARCHITECTS LTD. ON BEHALF OF 0976440 B.C. LTD., INC. NO. 0976440		
Property Address:	3011 NO. 5 ROAD		
Address:	C/O FARIBA GHARAEI, ARCHITECT AIBC 600 – 1140 WEST PENDER STREET VANCOUVER, BC V63 4G1		
	n shall be developed generally in accordance with the terms and softhis Permit and any plans and specifications attached to this a part hereof		

AUTHORIZING RESOLUTION NO. DAY OF , .

This Permit is not a Building Permit.

ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF

MAYOR



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TITING HOOT ON

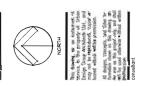
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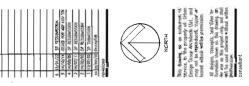
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CONC. CONC. BLOCK WALL PANTED

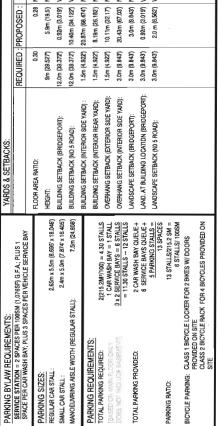








	REQUIRED :	REQUIRED : PROPOSED : NOTES :	NOTES:
	0.30	0.28 N/A	N/A
	9m (29.5277)	5.9m (19.5) N/A	N/A
BUILDING SETBACK (BRIDGEPORT):	12.0m (39.372)	0.92m (3.019)	0.92m (3.019) VARIANCE REQUIRED
BUILDING SETBACK (NO 5 ROAD):	12.0m (39.372)	10.40m (34.12Z)	12.0m (39.372) 10.40m (34.122) VARIANCE REQUIRED
BUILDING SETBACK (INTERIOR SIDE YARD) :	1.5m (4.922)	1.5m (4.922) 20.87m (68.474) N/A	N/A
BUILDING SETBACK (INTERIOR REAR YARD):	1.5m (4.922)	1.5m (4.922) 8.19m (26.182) N/A	N/A
OVERHANG SETBACK (EXTERIOR SIDE YARD):	1.5m (4.922)	10.11m (32.17) N/A	N/A
OVERHANG SETBACK (INTERIOR SIDE YARD):	3.0m (9.843)	20.43m (67.03) N/A	N/A
LANDSCAPE SETBACK (BRIDGEPORT):	3.0m (9.843)	3.0m (9.843) N/A	N/A
LAND. AT BUILDING LOCATION (BRIDGEPORT):	3.0m (9.843)	0.92m (3.019)	0.92m (3.019') VARIANCE REQUIRED
LANDSCAPE SETBACK (N0 5 ROAD):	3.0m (9.843)	2.0 m (6.562)	2.0 m (6.562) VARIANCE REQUIRED



213.4 SM / 1289.1 SM x 100 = 16.55%

327.48 SM (3,525 SF)

PROPOSED BUILDING AREA:

LOT COVERAGE:

EXISTING SITE AREA:

SITE INFORMATION:
LEGAL DESCRIPTION: 1 SEC 25 BLKGN RG6W PL 1366 EXCEPT PLAN LAMP39115, E
150 FT, REF PL 17350, & EXC BYLAW PL59971, PL LMP 21779

BC.NDARIES SHOWN HEREON ARE DERIVED FROM EXISTIVA RECORDS AND MUST BE CONTINUED BY SURVEY FROM TO THE DETERMINATION OF DIPENSIONS OR AREAS FOR DEVELOPMENT FARPOSES.

GENERAL NOTE:

ZC 35 - CAR WASH AND SERVICE STATION (BRIDGEPORT)

CIMC ADDRESS: CURRENT ZONING:

114.08 SM (1,228 SF) 213.39 SM (2,297 SF)

114.08 SM (1,228 SF)

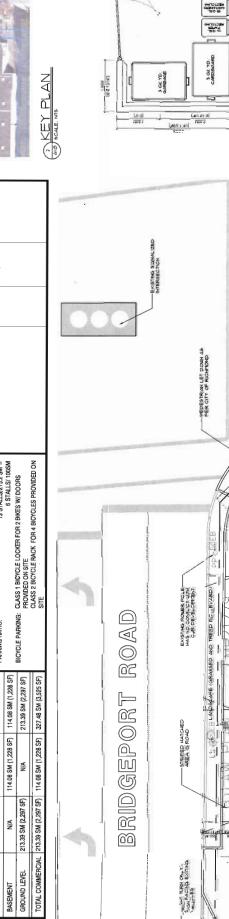
N/A 213.39 SM (2,297 SF)

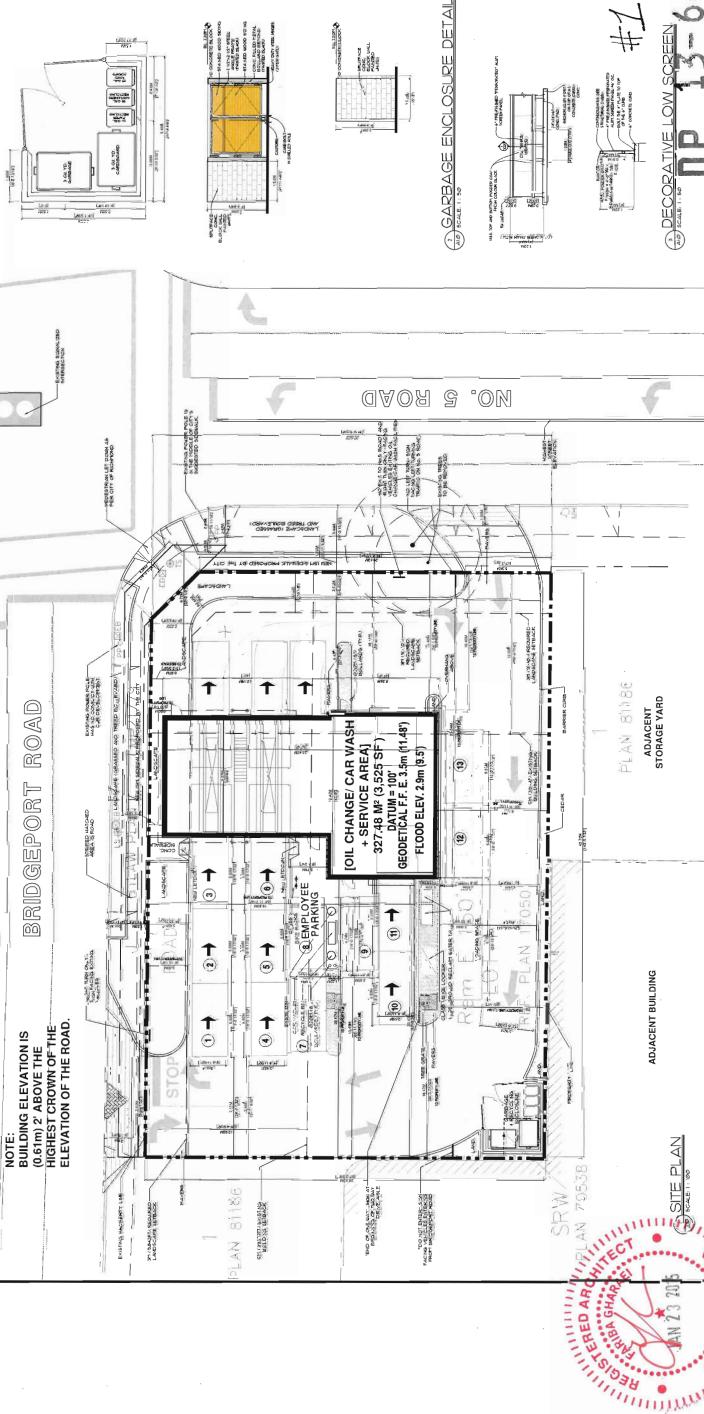
SERVICES / PIT

FLOOR AREA

SUMMARY

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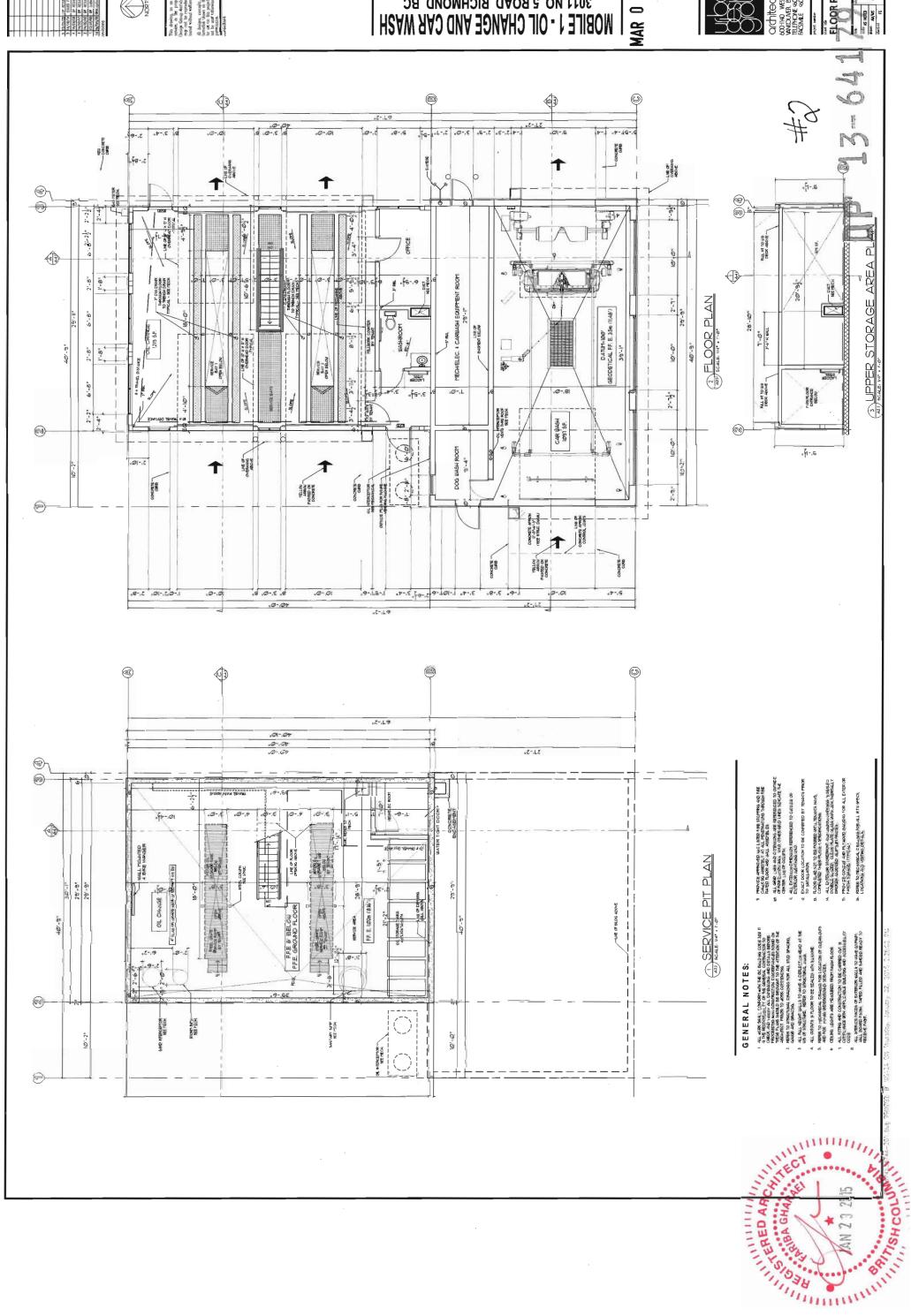
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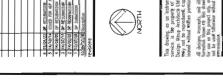


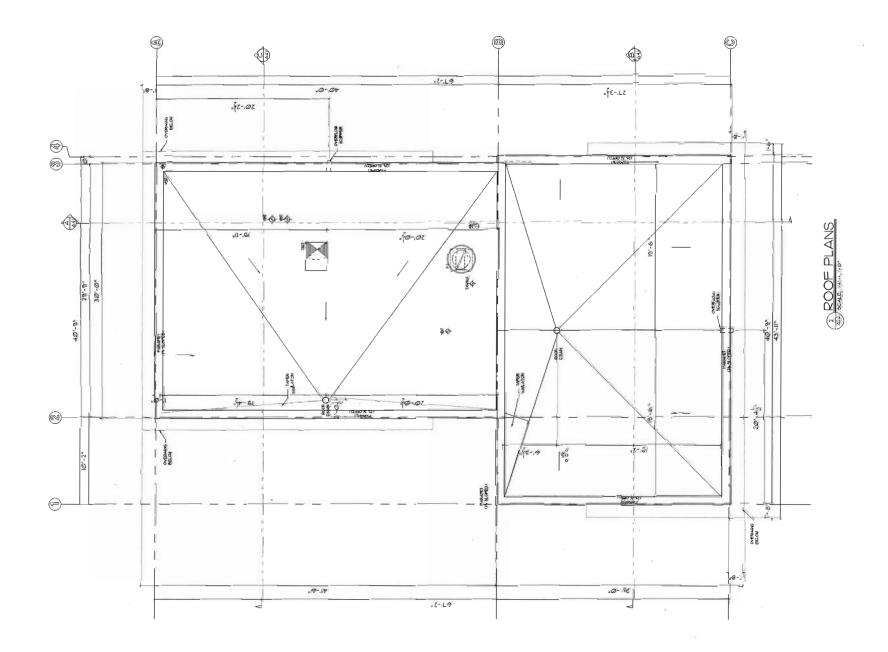
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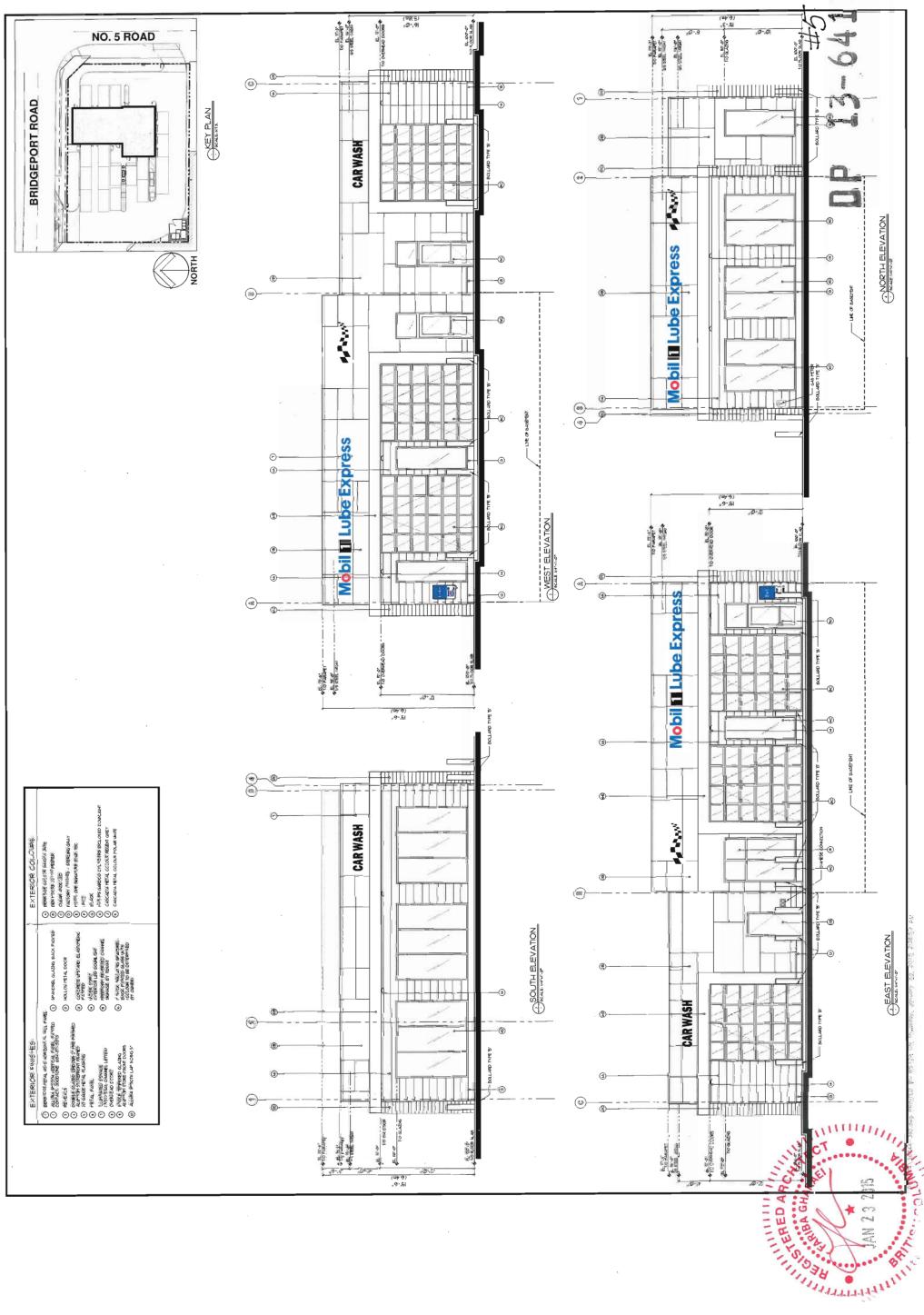
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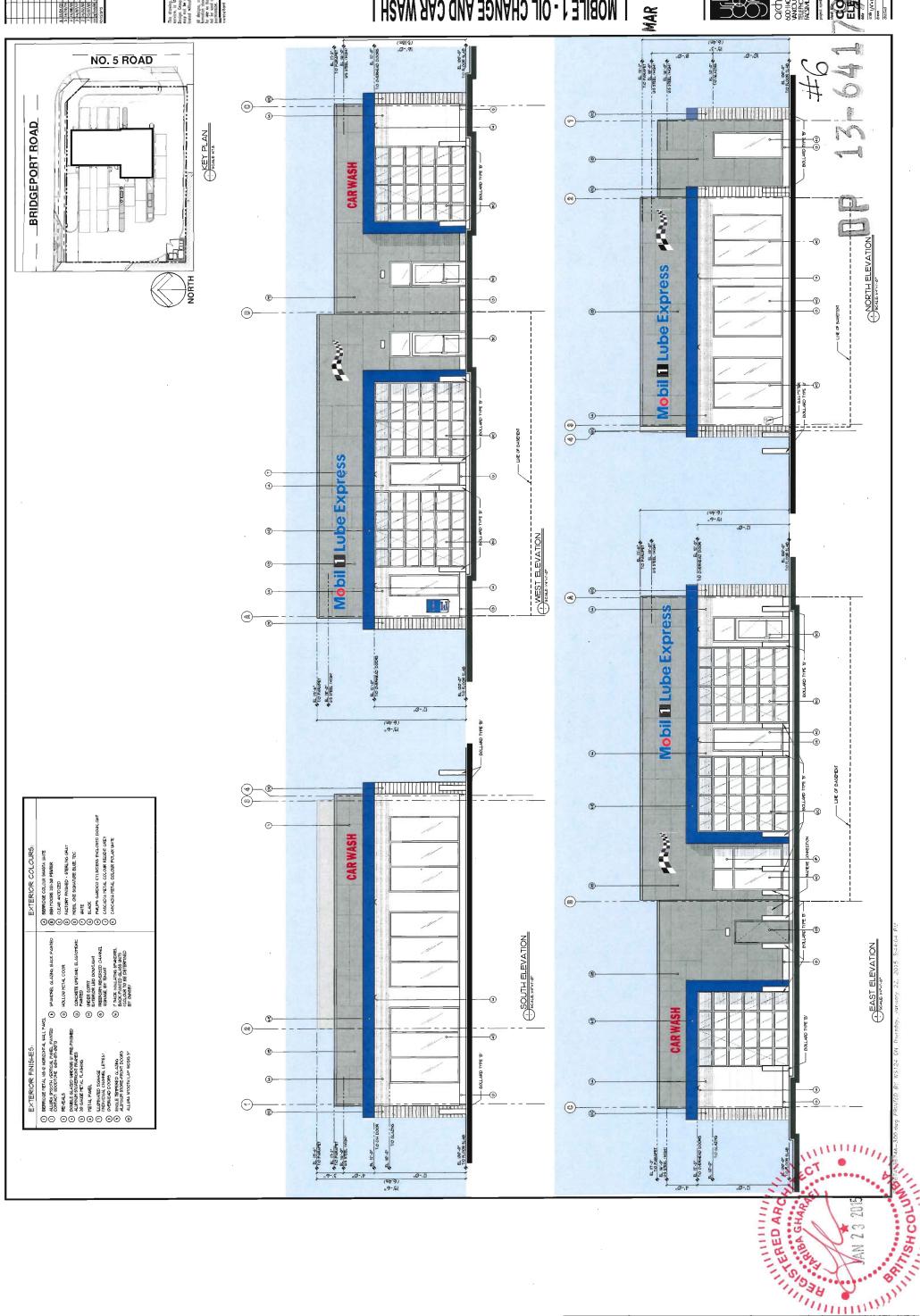


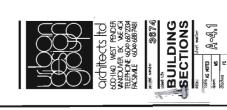
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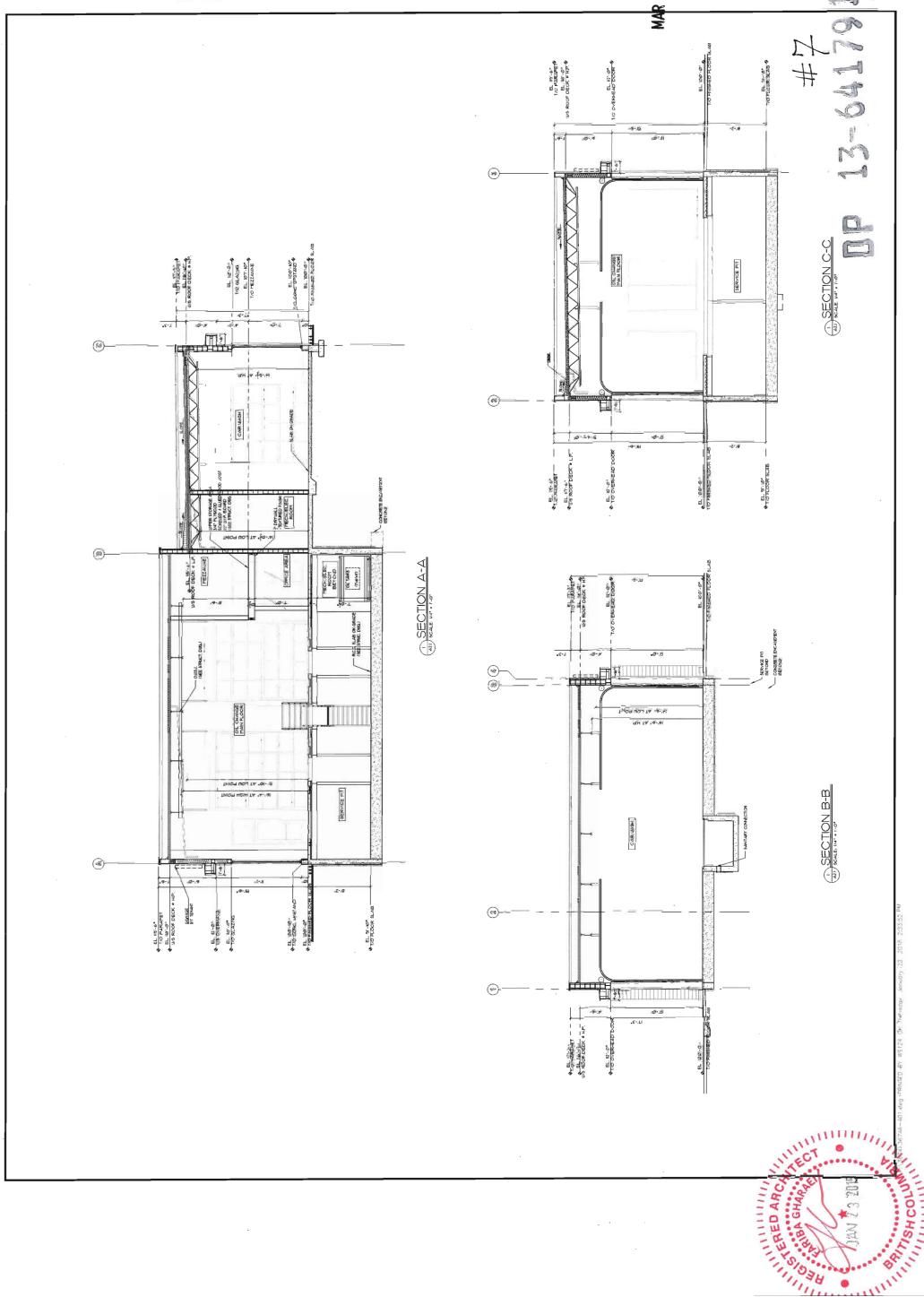
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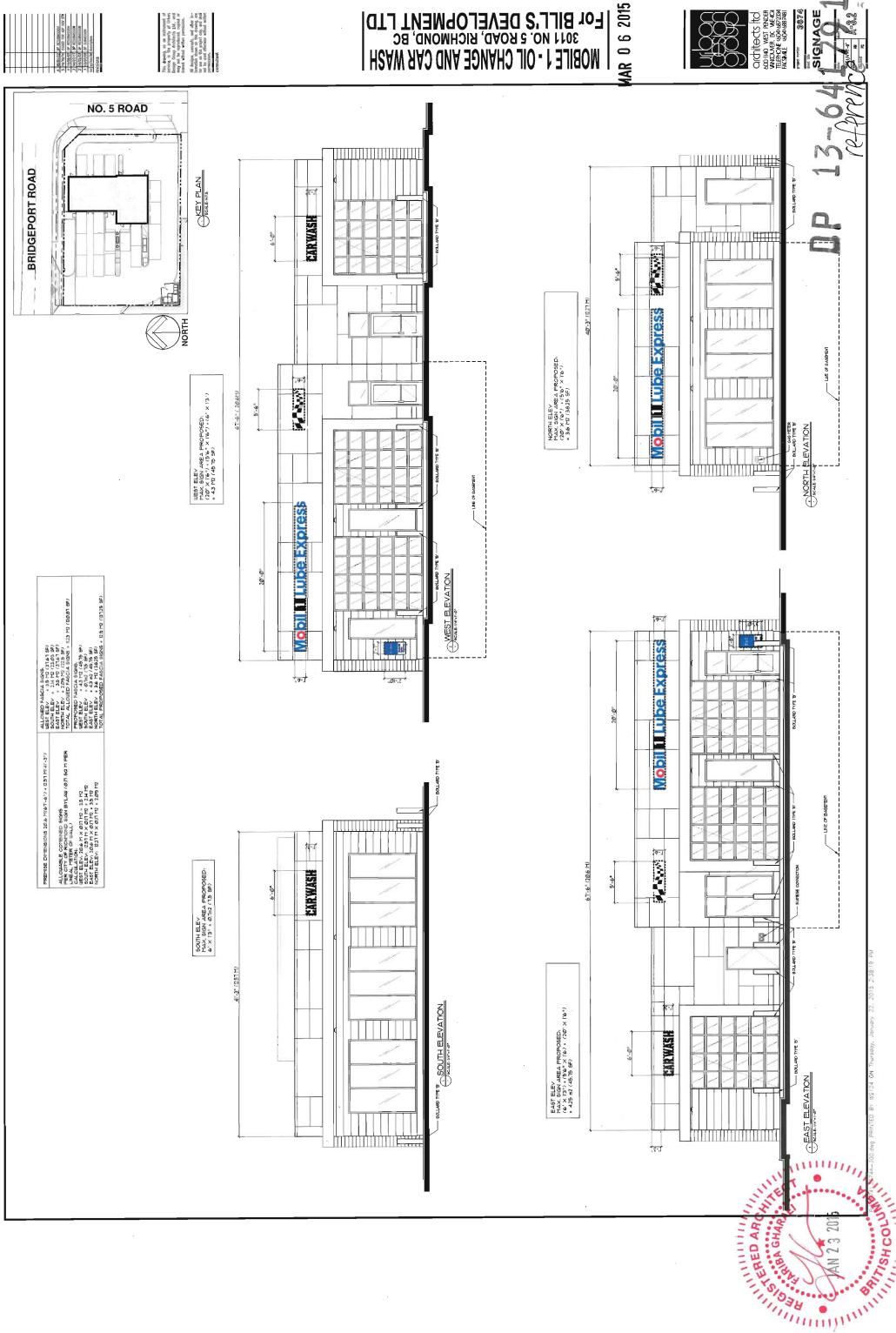




MOBILE 1 - OIL CHANGE AND CAR WASH











Report to Development Permit Panel

To:

Development Permit Panel

Date:

March 2, 2015

From:

Re:

Wayne Craig

File:

DV 14-658670

Director of Development

Application by Habitat for Humanity Society of Greater Vancouver for a

Development Variance Permit at 8180 Ash Street

Staff Recommendation

That a Development Variance Permit be issued which would vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1. Vary the minimum lot width from 12 m to 8.3 m for proposed Lot 5; and
- 2. Vary the minimum lot frontage from 6 m to 0.38 m for proposed Lot 4, to 2.7 m for proposed Lot 5 and to 0.60 m for proposed Lot 6.

To permit subdivision of 8180 Ash Street into six (6) lots zoned "Single Detached (RS1/B)" for the purpose of developing single-family dwellings.

Wayne Craig

Director of Development

DN:bl Att.

Staff Report

Origin

Habitat for Humanity Society of Greater Vancouver Inc. has applied to vary lot width and lot frontage for lots proposed to be created fronting Dayton Court. The applicant proposes to subdivide 8180 Ash Street into six (6) lots, which are zoned "Single Detached (RS1/B)". The proposed subdivision would create three (3) lots fronting Ash Street and three (3) lots fronting Dayton Court. The proposed variances apply only to the three (3) proposed lots fronting Dayton Court (Lot 4, 5, & 6) (Attachment 1).

There is no rezoning application associated with the development proposal. With the exception of the lot width and frontage variances that are proposed, the subdivision complies with the site's existing "Single Detached (RS1/B)" zoning.

The site is currently vacant and is a large lot within a developed single family neighbourhood.

A Servicing Agreement (SA) is required and is discussed in detail in Attachment 8. The scope of the SA includes improvements to the sanitary works, including provision of required rights of ways, demonstration that storm water run-off is contained within each proposed subdivided property and that run-off discharges to the City drainage system, and realignment of the eastern end of the existing emergency access.

Surrounding Development

The proposed scheme attached to this report has satisfactorily addressed the significant planning issues identified as part of the review of the subject Development Variance Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan (OCP) and, with the exception of the zoning variances requested, is in compliance with the requirements of the "Single Detached (RS1/B)" zone.

Please refer to the attached Development Application Data Sheet (Attachment 2) for a comparison of the proposed development data with the relevant Bylaw requirements.

Development surrounding the subject site is as follows:

- To the north: Single-family dwellings designated Low Density Residential in the Ash Street Sub-Area Plan and zoned "Single Detached (RS1/B)".
- To the east: Single-family dwellings designated Low Density Residential in the Ash Street Sub-Area Plan and zoned "Single Detached (RS1/A)".
- To the south: An emergency access connecting Ash Street and Dayton Court and single-family dwellings designated Low Density Residential in the Ash Street Sub-Area Plan and zoned "Single Detached (RS1/A)".
- To the west: Ash Street and single-family dwellings designated Low Density Residential in the Ash Street Sub-Area Plan and zoned "Single Detached (RS1/K) and (RS2/A)".

Background

A Development Variance Permit (DVP 10-542375), for the same lot width and frontage variances that are proposed in this report, was issued by Council on September 12, 2011 to the Provincial Rental Housing Corporation (which is BC Housing's land holding company). The property was acquired by Habitat for Humanity Society of Greater Vancouver in 2013. The issued Development Variance Permit lapsed because construction did not commence on-site within 24 months of the permit's issuance. Both the proposed subdivision plan to create six (6) lots; three (3) fronting Ash Street and three (3) fronting Dayton Court and associated variances are consistent with the plans supported in 2011.

Community Consultation

- Development signs are erected on both the Ash Street and Dayton Court frontages and the notification area has been extended beyond the standard 50 m (164 ft.) radius to include all Dayton Court properties (Attachment 3).
- In addition to the standard opportunities for residents to engage in the review process, the applicant was advised to organize and host a Public Information Meeting to provide an additional opportunity for residents to access information related to the proposal. Invitations were delivered by Canada Post to the expanded notification area, which includes not only residences within a 50 m (164 ft.) radius of the subject site but also all properties on Dayton Court.
- A Public Information Meeting was hosted by Habitat for Humanity Society of Greater Vancouver on Wednesday, October 1, 2014 from 6:00 PM to 8:00 PM at the South Arm Community Centre. Habitat for Humanity staff approached individuals who attended the Public Information Meeting to sign-in. Twenty three (23) individuals signed-in, representing 18 households (Attachment 4). Nine (9) comment forms, of which seven (7) included the individual's address, were completed by residents at the event. A letter from an area resident was submitted to the applicant and copied to staff subsequent to the meeting. Attachment 5 includes a synopsis of the event prepared by the applicant, response to issues raised by neighbourhood residents, copies of the attendance list, completed feedback forms, and a letter from an area resident, as well as a copy of the display boards presented at the Public Information Meeting.
- The following summarizes the concerns expressed in the returned comment forms, the letter submitted subsequent to the meeting, and the concerns expressed directly to staff, who received three (3) phone calls from two (2) callers.

On-site Parking (Dayton Court Fronting Lots)

- Concern that provisions for on-site turn around will not be used and that vehicles will not exit the site in a forward direction.
- Concern that parking demand resulting from the inclusion of a secondary suite cannot be appropriately accommodated on-site.
- Concern that new residents will park on the street.
- Dayton Court is a cul-de-sac where off-site street parking is in short supply and is complicated by residents using their garages as storage.

Transportation staff have reviewed the driveway access and turn-around provisions and have determined that the width of the driveway and the on-site turn around provisions adequately accommodate on-site manoeuvring needs (Attachment 6).

Two (2) parking spaces per unit are provided, which complies with the Zoning Bylaw.

Traffic Bylaw (No. 5870), which applies throughout the City, limits parking abutting a residence between 8:00 AM and 6:00 PM to no more than three (3) hours for those who do not live in the abutting dwelling.

Traffic Impact (Dayton Court)

- A sidewalk exists on the east side of Dayton Court, continues around the cul-de-sac bulb to the southern edge of the site's Dayton Court frontage and terminates at the emergency access. The sidewalk does not continue south of the emergency access to include the west side of Dayton Court.
- Children play on the Dayton Court roadway. Additional traffic generated by new homes will increase traffic within the cul-de-sac, which may affect the safety of residents and children.

Subsequent to undertaking a traffic speed study in 2011 and reviewing the results, Transportation staff have concluded that study findings do not support the introduction of traffic calming measures in the area at this time.

A sidewalk exists in front of the subject site. There are currently no plans to extend the existing sidewalk south of the emergency access.

Height and Character of Proposed Buildings/Overlook/Privacy

- The neighbourhood is characterized by garage entry two-storey homes. The two-storey contemporary homes with carports are an unwelcome departure from the established character of the neighbourhood.
- Although two-storey homes are proposed, they may be higher than the existing homes.
- Concern that the proposed common backyard/community space planned at the back of the proposed lots will affect the privacy of existing neighbouring properties.

The design of a single-family home is typically governed only by the building envelope restrictions articulated by the site's zoning. In order to provide certainty regarding the design of the proposed homes, the applicant is required to register a legal agreement on title to ensure the design is generally consistent with the plans presented at the Public Information Meeting and attached to this report.

As a result of incremental redevelopment within established residential neighbourhoods, variation in building design is common place and the diversity of building height reflects updated building practices that typically maximize the permitted building envelope potential and maximizes permitted density by increasing the slope of the roof. In this case, despite being permitted to construct homes with a maximum height of $2\frac{1}{2}$ storeys,

the applicant proposes to construct two storey homes with a modest roof pitch. The applicant has provided sections to study edge conditions if the site is significantly raised to comply with the Flood Construction Bylaw. However, despite undertaking these studies, the applicant intends to construct the homes at an elevation that is approximately 0.3 m (1 ft.) above the crown of the adjacent roads; thereby minimizing any difference in building height and construction elevation. Further, the Dayton Court fronting homes would be significantly setback from the property line, which minimizes the visibility of the homes from the street (Attachment 7).

Although six (6) individual lots are proposed, fences are not proposed between the internal lot lines. A fence would be introduced only between the subject site and adjacent properties. As shown in the attached landscape plans, an integrated landscape scheme is proposed, which would encourage residents to gather and socialize, and to draw residents into the central courtyard area, similar to the landscape strategy applied to townhouse development. To address privacy concerns between the proposed development and adjacent neighbours, the landscaping proposed on the northern portion of the site, is more generous and lush than typically associated with single family development and includes conifers. Further, the minimum required 6 m (19 ft.) rear yard building setback on proposed Lot 5 and 6 minimizes overlook and privacy impacts on the northern adjacent lots. The closest building to building relationship would be along the eastern edge of the site where a side yard relationship is proposed. Three (3) trees are proposed along this edge to address privacy and overlook concerns.

Number of Lots Proposed by the Subdivision

• Some residents expressed a preference for subdivision into fewer than the proposed six (6) lots.

No rezoning application is associated with development of the site. With the exception of the lot width and frontage variances requested, subdivision of the lot into six (6) parcels complies with the site's existing "Single Detached (RS1/B)" zoning. Reducing the number of lots would limit the impact of the innovative affordable home ownership model proposed by Habitat for Humanity.

Drainage Concerns

• The neighbourhood has a history of site drainage issues. Concern has been expressed that development of the subject site will impact drainage on adjacent sites and the area generally.

The site is within an area with soils known to be high in peat and the applicant would therefore be required to submit a geotechnical report as part of the Building Permit review. The Geotechnical report identifies any anticipated impacts to adjacent properties and appropriate measures to keep properties safe for their intended use. If required, a subsidence covenant would be registered on the title of the property being developed.

Zoning Compliance/Variances (staff comments in bold)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Vary the minimum lot width from 12 m to 8.3 m for proposed Lot 5.
- 2) Vary the minimum lot frontage from 6 m to 0.38 m for proposed Lot 4, to 2.7 m for proposed Lot 5 and to 0.60 m for proposed Lot 6.

To permit subdivision of 8180 Ash Street into six (6) lots zoned "Single Detached (RS1/B) for the purpose of developing single-family dwelling units.

(Staff supports the proposed variance.

The applicant has demonstrated that potential impacts associated with the proposed variances, which will create lots with narrow frontages fronting Dayton Court, have been reasonably addressed. Responses to the following concerns are discussed in detail in subsequent sections of this report:

Access, Parking and Safety

The proposed lots fronting Dayton Court will share a single access to the street and the applicant has demonstrated that the driveway design accommodates on-site turn-around so that motorists exit the site in a forward direction rather than backing out into the street (Attachment 6).

A double car carport is proposed for each home; thereby providing on-site parking in accordance with the bylaw.

Impact on Existing Character of the Neighbourhood

The lots proposed to access from the cul-de sac are large with narrow frontages that restrict the building envelope to an interior location. By setting the homes back more than 11 m (36 ft.) from the Dayton Court road frontage, the visual prominence of the homes from the street frontage would be minimized (Attachment 1, 7).

The applicant has provided a landscape plan, which is attached to the proposed permit and that demonstrates landscaping can be incorporated into the Dayton Court driveway design. As a condition of subdivision approval, the applicant is required to register a legal agreement on title to ensure that the design of the buildings is consistent with the plans attached to this report.

Urban Design Response

Conditions of Adjacency

- The site abuts single family residential lots on the north and east, Ash Street on the west and an existing City of Richmond dedicated 6 m (19 ft.) wide emergency lane on the south.
- The new homes would comply with required rear and side yard setbacks and although the site's existing zoning permits a 2 ½ storey building on each lot, the applicant proposes two-storey homes with a building height less than 8.5 m (28 ft.), as well as a modest roof pitch. Proposed on-site landscaping along the perimeter of the site and a new fence will further minimize privacy concerns.

- Construction is required to comply with the City's Flood Construction Bylaw (No. 8204), which requires a minimum flood construction level of 2.9 m GSC, or at least 0.3 m (1 ft.) above the highest elevation of the crown of any road that is adjacent to the parcel. The applicant has provided sections to study the most aggressive potential interface in which proposed new homes would be constructed at an elevation that is between 0.75 m to 3.04 m (2.5 ft. to 9.9 ft.) higher than the existing adjacent homes along the north and south edge of the site (Attachment 1). Despite undertaking these studies, in response to concerns previously expressed by residents within the neighbourhood, the applicant intends construction at a lower elevation. Prior to issuance of a Building Permit, the applicant will undertake a lot grading plan and a survey of adjacent road elevations, which will confirm the proposed finished grade, which is expected to be closer to 0.3 m (1 ft.) above the elevation of the crown of the road.
- The proposed development is not expected to significantly affect the existing homes located on the south side of the site, which are separated from the subject site by an existing emergency access.

Streetscape

- The applicant has provided contextual streetscape information and a rendering to demonstrate how the proposed subdivision will integrate with the existing neighbourhood (Attachment 7). The site's restricted Dayton Court frontage means that homes constructed on the proposed Dayton Court fronting lots are required to be significantly setback from the lot's frontage to facilitate on-site vehicle manoeuvering and on-site parking, as well as to allow sufficient building width for home construction. The resulting building setbacks (approximately 11 m to 14.3 m (36 ft. to 50 ft.)) minimize the visibility of the homes from the street.
- Variation in building height and design is common within residential neighbourhoods where redevelopment may introduce updated building practices that maximize the permitted building envelop and reflect changes in building design. The proposed two storey high homes (approximately 7 to 8 m (23 ft. to 26 ft.) will integrate well with the existing neighbourhood.

Site & Functional Planning

• Due to the site's narrow Dayton Court frontage, the location and design of vehicle access to the proposed Dayton Court lots has been reviewed to consider feasibility and to maximize safety and function.

Exploration of Alternative Access to Proposed Dayton Court Lots

• Early in the review process, the option of accessing the site via the existing emergency access, which is located along the south side of the subject site, was considered. The associated impacts on lot yield and overall density would be considerable and the option has not been pursued.

Consideration of Driveway Width

• The standard driveway width for single-family development is 4 m (13 ft.). A proposed shared 4.6 m (15 ft.) wide driveway to access Dayton Court, which is classified as a local

- road, is supported by Transportation staff. The proposed development is expected to generate limited traffic.
- The Richmond Fire Department supports a slight re-alignment of the existing emergency access, which consists of two (2) hard surface strips that provide access for emergency vehicles. The proposed slight realignment of the existing emergency access permits the width of the residential shared driveway to be increased. The off-site works associated with the realignment will maintain the 12 m (40 ft.) radius required for fire access. To ensure the realignment does not affect the functionality of the emergency access, the applicant will be responsible for removal of the eastern end of the two (2) drive aisle strips and resurfacing the access with a standard paved surface (Attachment 1).

Manoeuvring

- The shared driveway proposed for the lots fronting Dayton Court will provide sufficient space for vehicles to reverse out of their carports, turn around on-site, and exit the common driveway into the cul-de-sac in a forward direction (Attachment 6). This configuration is supported by Transportation.
- As a condition of subdivision, the applicant would be required to register a legal agreement on title to secure shared cross access and shared use of the driveway for the proposed lots fronting Dayton Court. To ensure on-site turn around can be accommodated, the document would include reference to a minimum building setback to ensure a carport(s) does not obstruct the area.

Vehicle/Pedestrian Relationship & Safety

- By facilitating on-site turns, vehicles may exit the site in a forward direction; thereby minimizing the potential for conflict between vehicles backing out onto Dayton Court and pedestrians/cyclists.
- Subsequent to area residents expressing safety concerns in 2011 when the original Development Variance Permit (DV 10-542375) was reviewed, Transportation staff initiated a week long traffic speed study on Dayton Court, which was conducted over a 24 hour period from 8:00 AM October 21, 2001 to 8:00 AM October 28, 2011. The study recorded an average speed of 30 km/h for both north and south bound traffic and substantiated that 85% of the vehicles observed were traveling at or below 40 km/h during the survey period. The speed limit in Richmond, unless otherwise posted, is 50 km/h. Based on the findings of the study, Transportation staff have concluded that traffic calming measures are not required at this time.

On-Site Parking

• The applicant proposes to construct a carport for two (2) cars on each proposed lot. Two (2) on-site parking spaces per lot complies with the Bylaw requirement.

Garbage and Recycling Collection

• Garbage and recycling would be collected for individual units from the street.

Architectural Form and Character

- The applicant proposes homes with simplified architectural features that are characteristic of contemporary architecture (Attachment 1).
- Contemporary, practical homes with simple roof lines constructed of simple yet durable materials, including stucco, hardi-plank and a metal roof are proposed. Carports that accommodate two (2) vehicles, rather than enclosed garages, are proposed to discourage the use of allocated parking area for storage or conversion to habitable space.
- Prior to subdivision, the applicant is required to register a legal agreement on title to ensure that the building design is generally consistent with the attached plans (Attachment 1). Future Building Permit plans must comply with all City regulations and staff will ensure that the plans are consistent with the registered legal agreement for building design.

Tree Preservation and Landscape Design

• The City Tree Preservation Coordinator has reviewed an Arborist Report and associated tree plan submitted by the applicant, which analyzes tree retention/removal on-site and is summarized below.

Table 1: Tree Preservation/Compensation Synopsis

# of trees	Tree Condition	Recommendation (retain/remove)	Compensation
33	Poor condition (dead, dying, sparse canopy foliage, previously topped or exhibit structural defects)	Remove	1:1 replacement
2 Viable (tag #691, #684)	Good condition but cannot be successfully retained because of required lot grading. #691 additionally conflicts with access to the lot.	Remove	Three (3) minimum 5 m high replacement trees. Required replacement tree security of \$3,000/tree

- A cedar hedgerow is located along the site's Dayton Court frontage and impedes access to
 the site and installation of perimeter drainage. Parks staff support removal of the hedge and
 no compensation is required.
- The landscape plan attached to this permit demonstrates that the development would provide a surplus of replacement trees, introduce a mix of conifer and deciduous trees, provide the required larger sized tree species, and encourage relationships between neighbours through the proposed comprehensive landscape strategy.
- Fences are proposed only along the north, south and east perimeter of the site; fences will not be erected between the proposed subject lots. Instead, a common courtyard area is proposed which includes hard and soft landscaping including benches, compost bin, and permeable pavers for pathways and central patio area.
- To address privacy and interface concerns expressed by neighbours, generous landscaping is proposed along the property's edges and the garbage/recycling holding area is screened by landscaping.

Affordable Housing

- The housing model proposed by Habitat for Humanity Society of Greater Vancouver, which proposes to facilitate entry level home ownership, responds to one of the primary objectives of the Affordable Housing Strategy: affordable home ownership.
- The applicant has provided the following details of their operating model:
 - o Habitat for Humanity Society of Greater Vancouver works with community support and partner families to build safe, quality homes.
 - O Habitat member families contribute 500 hours of "sweat equity" into building their home as their down payment. Families then pay an interest free mortgage inclusive of taxes and home insurance that is tailored to be no more than thirty percent (30%) of their annual gross income.
 - The money collected from mortgages is deposited into Habitat's Building fund, which
 is leveraged to build additional homes for more families while simultaneously building
 a savings fund for the partner family to be used in the traditional mortgage market.
 - The home remains within the Habitat for Humanity inventory and if the initial owner determines it is time to sell the unit, the unit returns to Habitat for Humanity to be used by another family to benefit from the mortgage equity system.
- An option to purchase agreement is registered on the site (CA3455051). The agreement
 ensures ownership of the site remains with an agency that is committed to the long term
 provision of an affordable housing option. The document articulates terms by which the
 Provincial Rental Housing Corporation would reclaim ownership of the site, as well as lists
 eligibility criteria for future home owners.
- Each home will have a secondary suite, which is permitted by the site's existing zoning. The secondary suite will provide the owners of the homes with a revenue stream. The selection criteria and rental costs threshold for the secondary suites will be the same as the home owner model and is administered by Habitat for Humanity. Prior to subdivision, the applicant is required to register a legal agreement on title to ensure a secondary suite is constructed on each lot.

Subdivision

- At the future subdivision stage, the applicant is required to register legal agreements on title
 as listed in Attachment 8, including an agreement that specifies building design, secures
 shared driveway access for the proposed Dayton Court fronting lots, and provision of
 secondary suites.
- Also at the future subdivision stage, the applicant is required to enter into a standard Servicing Agreement, which includes all requirements listed in the attached Development Variance Permit Considerations and includes but is not limited to:
 - Demonstrate all Fire Department requirements have been addressed and incorporate any required works into the Servicing Agreement drawings;
 - Realign the existing emergency access to accommodate the proposed driveway from Dayton Court;
 - Undertake improvements to the sanitary works, including provision of required rights of ways;
 - Demonstrate storm water run-off is contained within each proposed subdivided property and that run-off discharges to the City drainage system.

Conclusions

The proposed subdivision of 8180 Ash Street into six (6) single family lots to accommodate single family homes with a secondary suite, would contribute to the stock of affordable housing within the city through a program that is initiated and managed by Habitat for Humanity Society of Greater Vancouver. Analysis of the proposed subdivision includes evaluation of possible impacts on adjacencies, vehicle volumes and vehicle manoeuvring for the proposed Dayton Court fronting lots, changes to the streetscape resulting from the proposed development of the lots, and character of the proposed buildings and the associated landscaping plan. The applicant has satisfactorily demonstrated that the lots can be developed in a manner that minimizes the impact of development on the existing neighbourhood. Further, with the exception of the proposed lot width and frontage variances, subdivision and development would comply with the site's existing zoning. Based on these considerations, staff support the proposed lot width and frontage variances.

Diana Nikolic

Planner 2-Urban Design

(604-276-4040)

DN:blg

Attachment 1: Proposed Development Scheme

Attachment 2: Data Sheet

Attachment 3: Expanded Notification Area (Map)

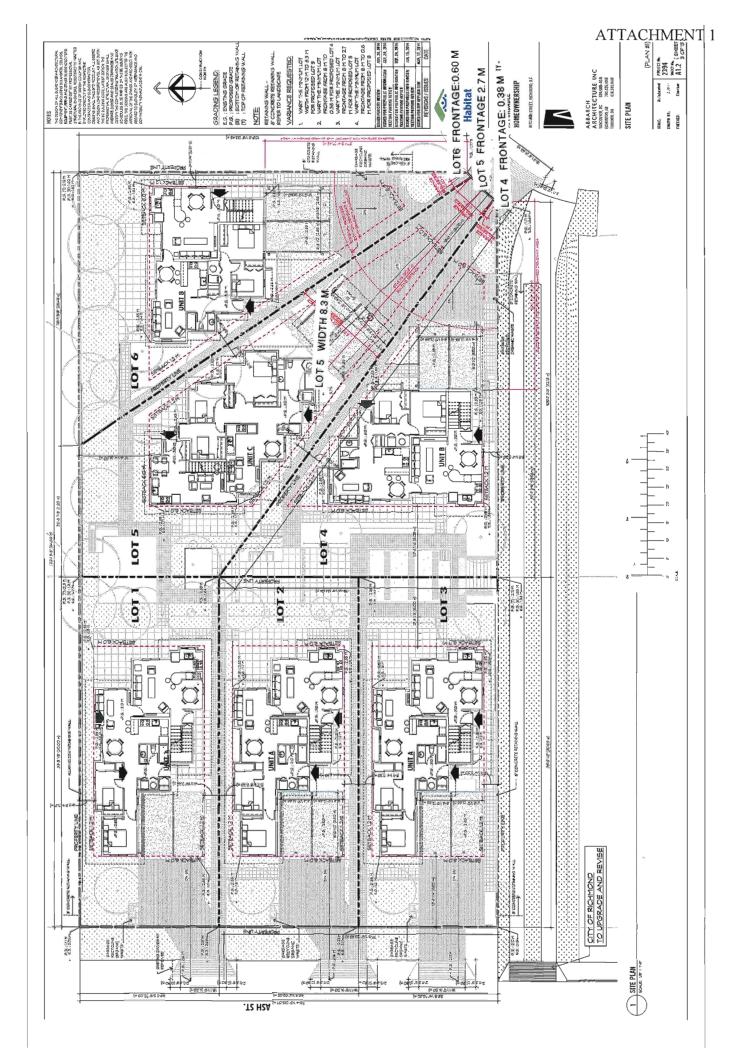
Attachment 4: Public Information Meeting Attendance Map & Meeting Notice

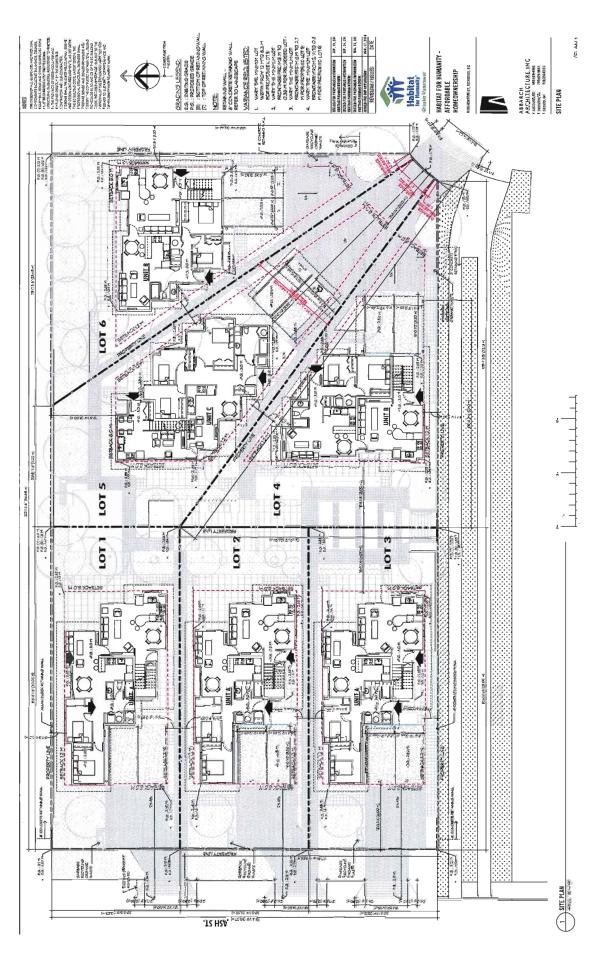
Attachment 5: Public Information Meeting Synopsis Prepared by Applicant

Attachment 6: On-Site Turning Movements

Attachment 7: Streetscape Views

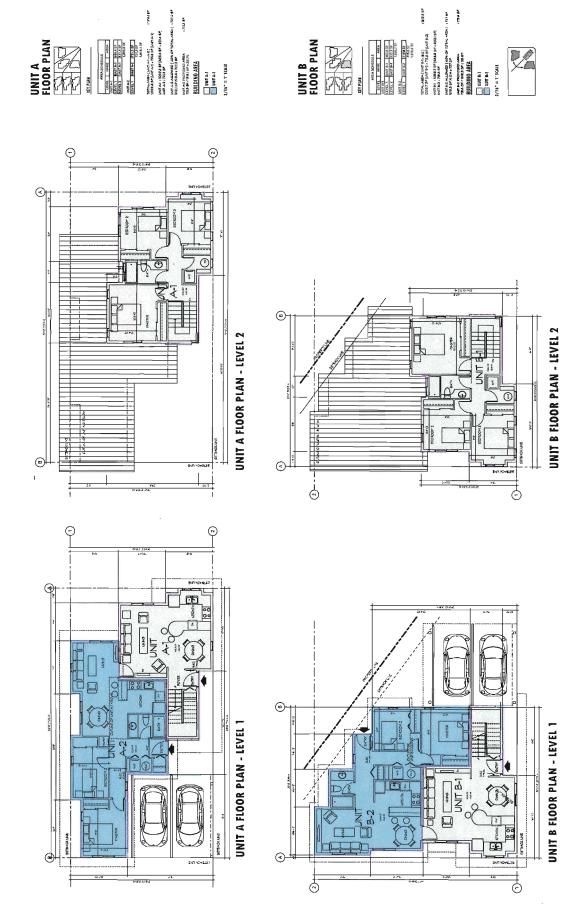
Attachment 8: Development Variance Permit Considerations







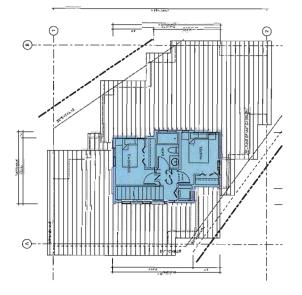
PLAN #5.a UNIT PLANS

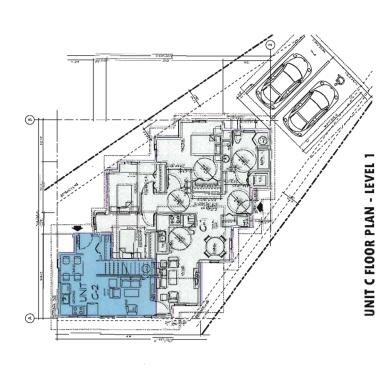






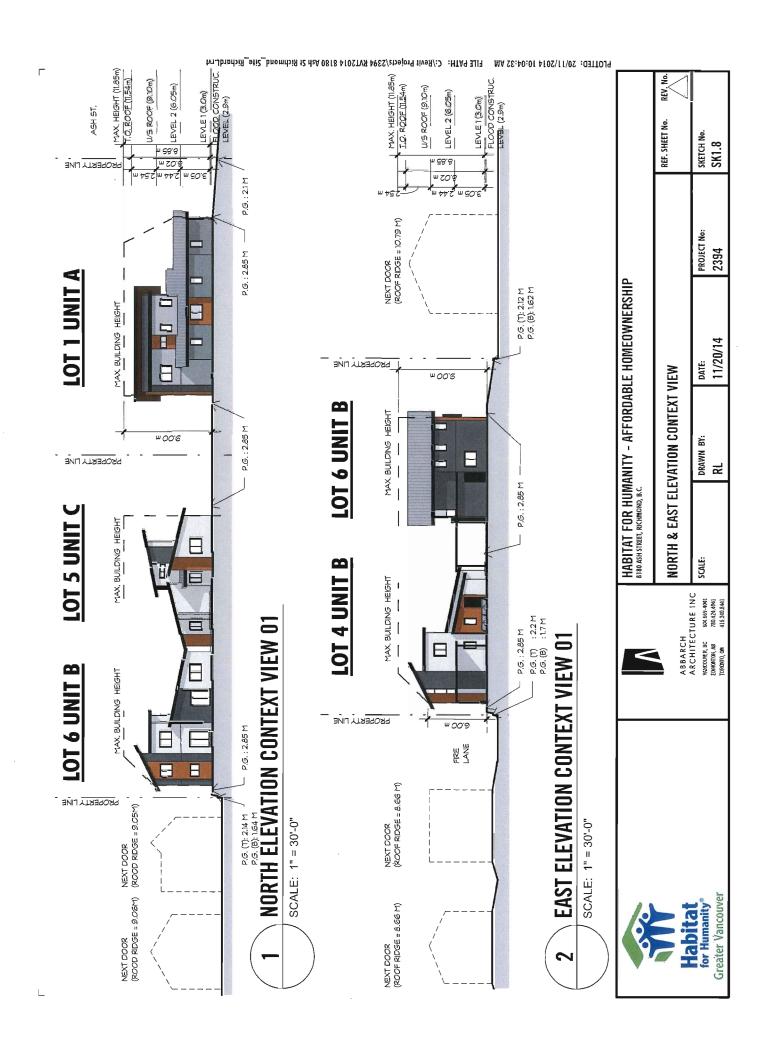


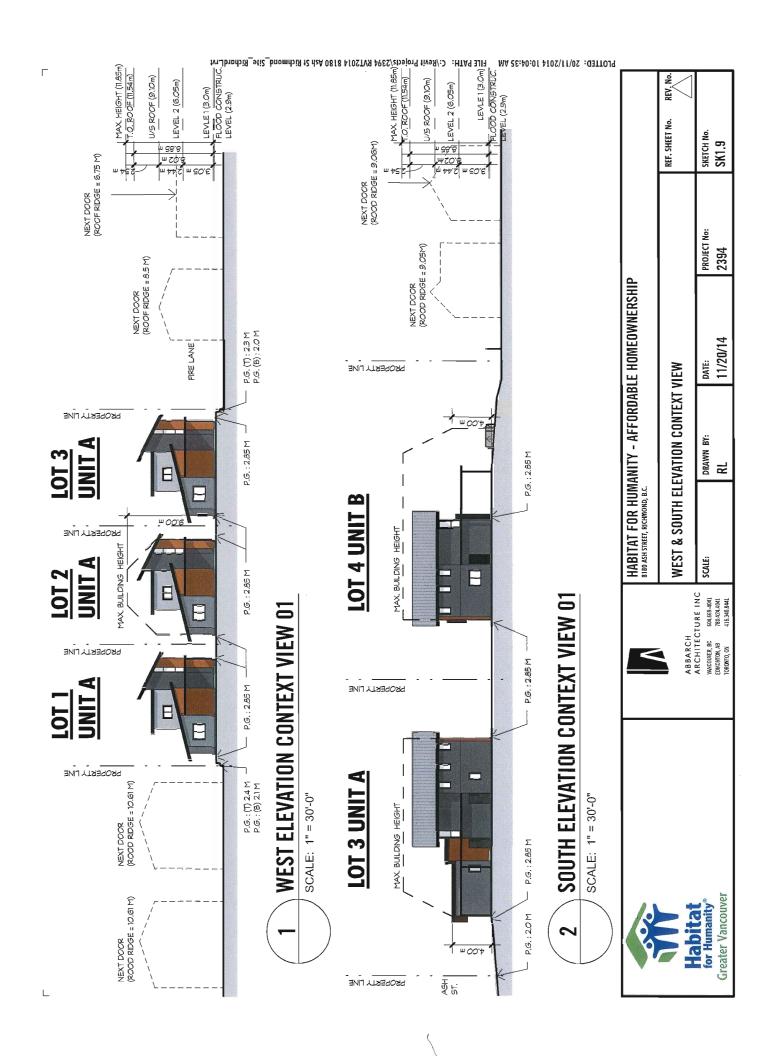




















Development Application Data Sheet

Development Applications Division

DV 14-658670 Attachment 2

Address: 8180 Ash Street

Habitat for Humanity Society of

Applicant: Habitat for Humanity Society of Greater Vancouver Owner: Greater Vancouver

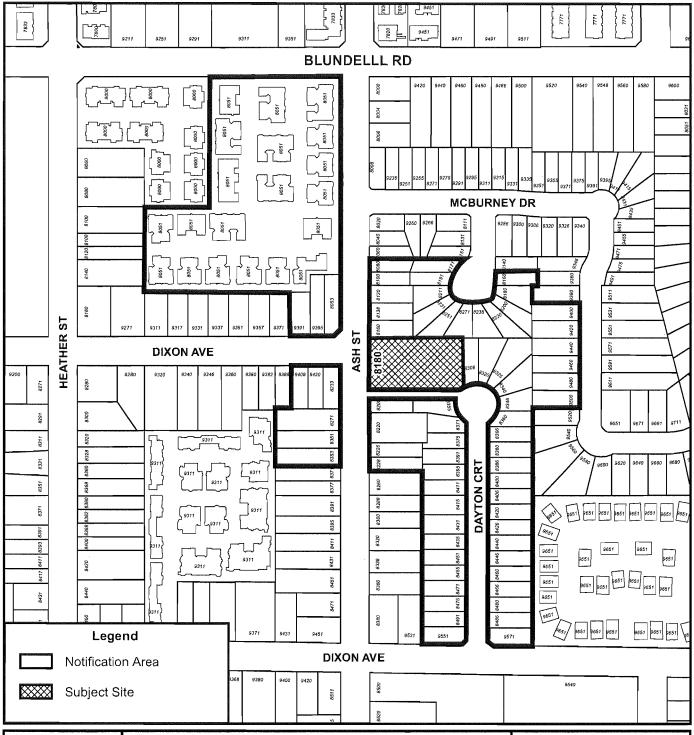
Planning Area(s): Ash Street Sub-Area Plan

	Existing	Proposed
Site Area:	2329 m ²	2329 m²
Land Uses:	Vacant	6 single-family dwelling units
OCP Designation:	Neighbourhood Residential	Neighbourhood Residential
Zoning:	Single Detached (RS1/B)	Single Detached (RS1/B)
Number of Units:	-	6

On Future Subdivided Lots	RS1/B Requirement	Proposed	Variance
Floor Area Ratio:	0.55	Lot 1, 2, 3: 0.47 Lot 4: 0.43 Lot 5: 0.35 Lot 6: 0.42	none permitted
Lot Coverage:	Max. 45%	34.2-39.5%	None
Setback – Front Yard:	Min. 6 m	Lot 1, 2, 3: 6.0 m Lot 4: 11 m Lot 5: 14.36 m Lot 6: 13.25 m	None
Setback – Interior Side Yard:	Min. 1.2 m	1.2 m	None
Setback – Rear Yard:	Min. 6 m	6.0 m	None
Height (m):	2 ½ stories (not exceeding the residential vertical lot width and depth envelope)	Lot 1-4, 6: 8.02 m Lot 5: 7.06 m	None
Lot Size:	360 m ²	Lot 1,2,3: 361 m ² Lot 4: 394 m ² Lot 5: 450 m ² Lot 6: 404 m ²	None
Frontage	6 m	Lot 1-3: 12.02 m Lot 4: 0.38 m Lot 5: 2.7 m Lot 6: 0.6 m	Variance requested for proposed Lots 4-6
Width	12 m	Lot 1-3: 12.02 m Lot 4: 12.21 m Lot 5: 8.35 m Lot 6: 12.02 m	Variance requested for proposed Lot 5

Off-street Parking Spaces – Regular/Commercial:	2 per single detached house. No additional requirement for secondary suite	12	None
Total off-street Spaces:	12	12	None







Extended Notification Area DV 14-658670

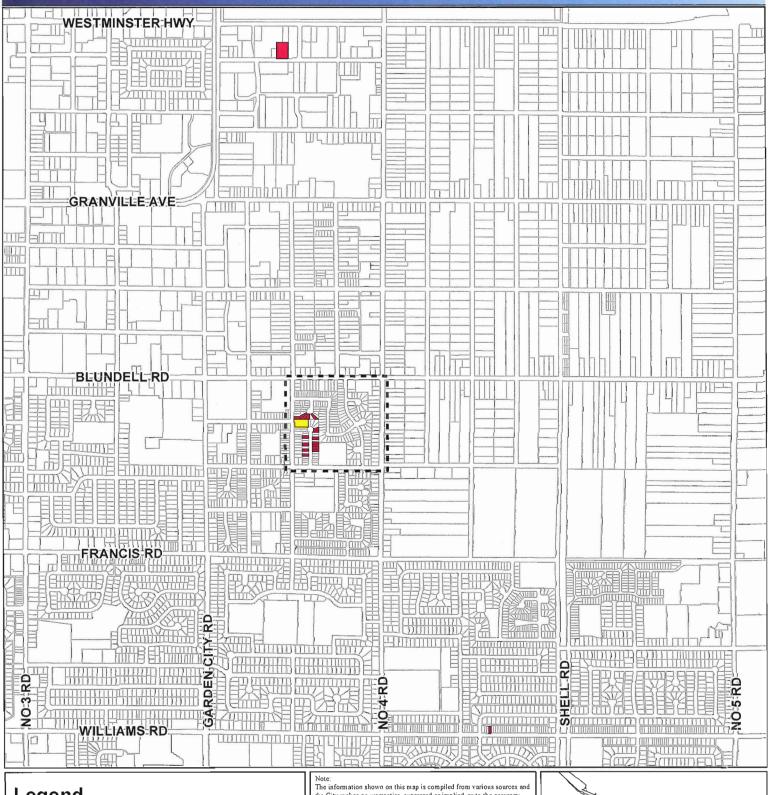
Original Date: 02/26/15

Revision Date:

Note: Dimensions are in METRES



8180 Ash Street: **ATTACHMENT 4** Public Information Meeting Attendees



Legend



Subject Site



Public Information Meeting Attendees 23 signatures from 18 households

> March 2, 2015 Prepared by Onkar Buttar

the City makes no warranties, expressed or implied, as to the accuracy or completeness of the information.

Users are reminded that lot sizes and legal description must be confirmed

at the Land Title office in New Westminster.
This IS NOT a legal document, and is published for information and

convenience purposes only.

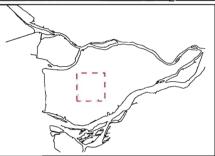
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1:17,353

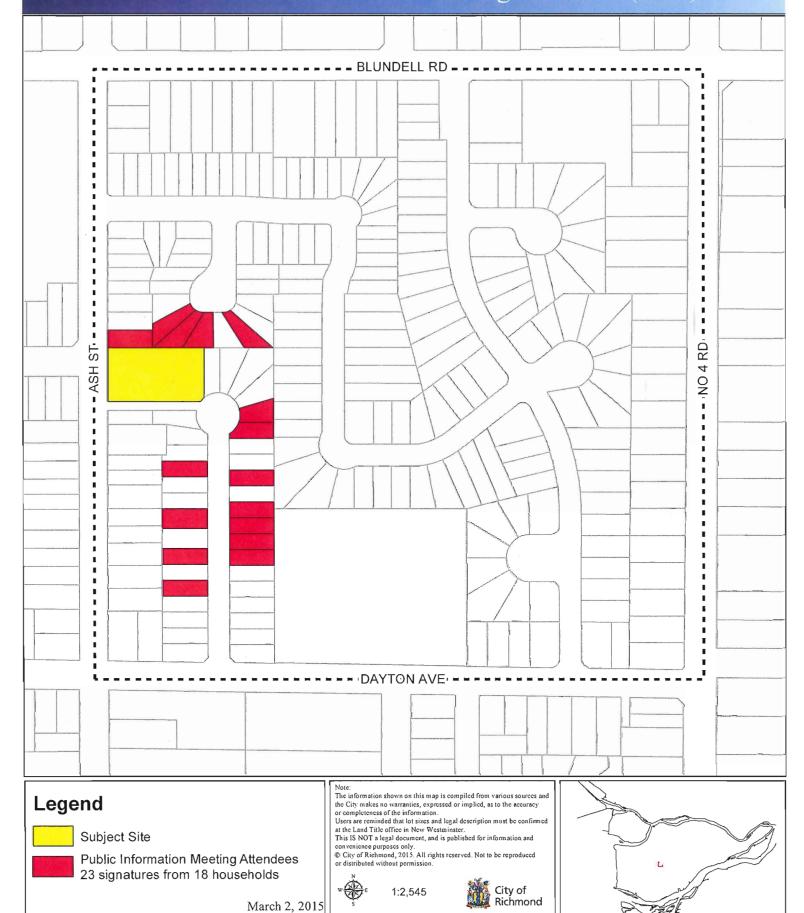


City of Richmond





8180 Ash Street: Public Information Meeting Attendees (Inset)



Prepared by Onkar Buttar





Dear Homeowner:

Habitat for Humanity Society Greater Vancouver recently purchased 8180 Ash Street from BC Housing (Provincial Rental Housing Corporation) for the intent to subdivide the lot into six (6) lots zoned "Single Detached (RS1/B)" for the purpose of developing affordable single-family dwellings, and;

- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - a) the minimum lot width from 12 m to 8.3 m for proposed Lot 5; and
 - b) the minimum lot frontage from 6 m to 0.38 m for proposed Lot 4, to 2.7 m for proposed Lot 5 and to 0.60 m for proposed Lot 6.

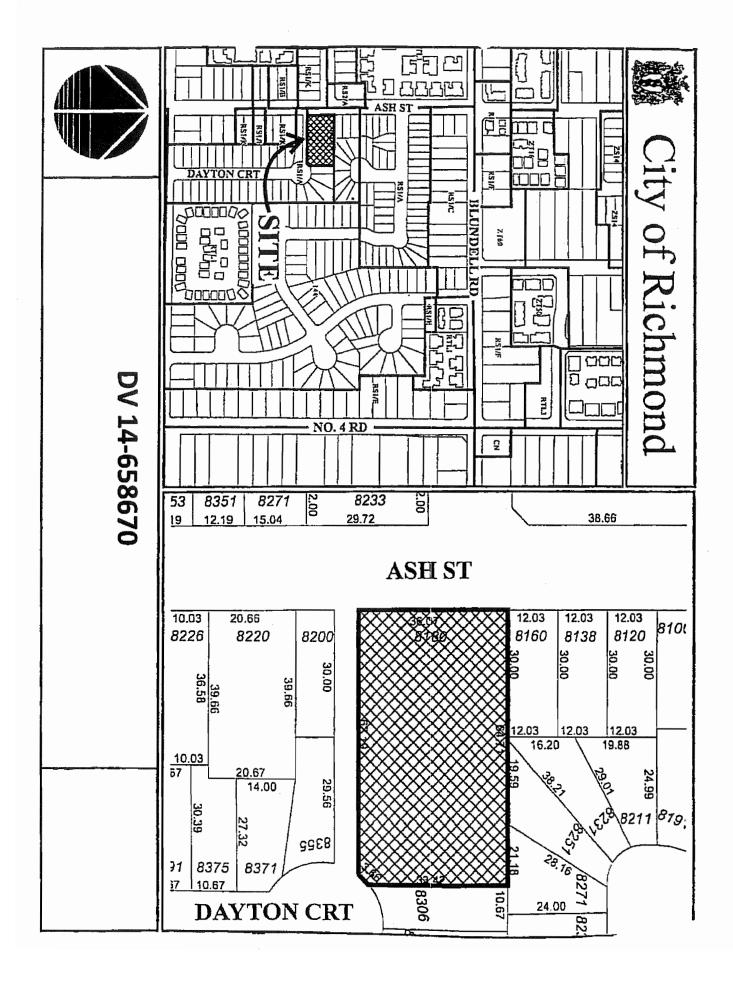
This Development Permit Application is the same application brought to the public in 2011by BC Housing Rental Society and was approved by Council. BC Housing let the Development permit lapse by not building the homes within the 2 year requirement. There have been no changes made to the original Development Permit Application.

Habitat for Humanity Greater Vancouver works with community support and partner families to build safe, decent homes that we sell to partner families with payments they can afford. These families would not qualify for a traditional mortgage due to income and down payment requirements. The Habitat for Humanity model ensures that hard-working families *own* their home. By owning their home, Habitat families move from dependency on housing subsidies to paying, on average, \$2,000 in property taxes each year – transforming a substantial tax burden into positive community impact.

Please come join us to review the proposed Development Variance. Your comments are welcome.

Sincerely,

Tim Clark CEO





October 20, 2014

Diana Nikolic, Planner II

Planning and Development Department
Development Applications
City of Richmond
6911 No. 3 Road
Richmond, BC V6Y 2C1

File: 08-4105-20-AMANDA#2014

Re: DVP Public Information Meeting, 8180 Ash Street, Richmond, BC

Dear Ms. Nikolic:

Habitat for Humanity Greater Vancouver hosted a public information session at the South Arm Community Centre on October 1, 2014. This information session was a requirement of our Development Variance Application for 8180 Ash Street, Richmond, BC.

In attendance of the meeting was ABBARCH Architecture, the architects of record, PWL Partnership, the landscape architects of record, four representatives from Habitat for Humanity Greater Vancouver, and a representative from the City of Richmond.

Habitat for Humanity Greater Vancouver provided notification to an extended geographical area providing 100 households with notification of the meeting.

Attached are the sign in sheets and written responses from attendees. There were 23 individuals in attendance, representing 17 households. One of the attendees was from outside the notification area and had just stopped in to see what was happening. Thus, 16% of households notified attended the meeting, of those 16 households, 11 had Dayton Court addresses. Of these 16 households represented 9 written comments were submitted; 2 of the written comments received came from the same household.

The general comments received were directed towards traffic on Dayton Court and the height elevations of the new flood plan requirements. I have included the responses from the architect and landscape architect for your information.

If you require any further information please contact me.

Tim Clark

Chief Executive Officer

Jim Clark

We are aware that we will not be able to suit everyone's taste as it comes to design but we would like to demonstrate that the above are well received in different communities around Vancouver. We feel our design will bring a sense of place to the surrounding neighborhood and will be something that the communities will help build and take pride in.

The discussions that I took part in related to form and character were positive. An understanding of the architecture and its need to be practical in its use of materials being low maintenance were received positively. I was able to field questions related to fenestration on the north and east side of the property. We provided information related to the house scheme and how care was taken related to size and location of windows to provide as much privacy as possible for each neighbor.

I did not receive any questions related to building height. Most of the questions were related to density. Many people were surprised that we were under the allowable density. We did have to explain how the rental helper preformed in each home. These discussions led to the overall plan for the development. The scheme is meant to be inviting and promote a sense of community allowing the public and neighbours to percolate through the site. The lack of fences with access routes through the site all leading to a central gathering space, children's park and community garden all surprised many of the public in attendance.

Our goal, similar to Habitat for Humanity is to propose a design that builds community. For the public who were interested and wanted to engage in conversation with us it was our feeling that this was well received related to form design and character of the buildings.

Aaron Urion, Associate B. Arch.

Engaging Design™

ABBARCH ARCHITECTURE INC

Suite 1830, One Bentall Centre 505 Burrard St., Box 79 Vancouver, BC V7X 1M6 WWW.ABBARCH.COM T 604 669 4041, Ext 316 F 604.683.5338 C 778.229.1077

VANCOUVER EDMONTON TORONTO

Response from Architect:

Aaron Urion, Architect of record from Abbarch Architecture, was in attendance at the public presentation. The architect's comments and responses from the public information meeting held on October 1, 2014 are summarized below.

Question 1

Most of the concerns were related to possible over parking on the site related to the single point of entry off the cul-de-sac. Concerns were raised that future residents extra cars would spill out into the already congested cul-de-sac.

Question 2

I was somewhat surprised to read some of the comments that mentioned that the homes looked cheap and did not conform. Our research related to upcoming neighborhoods in Vancouver as well as homes that are currently winning design awards in Vancouver are in the below images.







Response from Landscape Architect:

Jeff Philips, Principal of PWL Partnership Landscape Architects, was in attendance at the public presentation. The landscape architect's comments and responses from the public information meeting held on October 1, 2014 are summarized below.

Question 1

There were no relevant questions regarding the lot size variances.

Question 2

Concern about settlement of adjacent properties... As additional consultants are brought on board, an engineering design will be proposed to properly load the site with the required fill, without any negative impacts to adjacent property or adjacent grades.

Proposed concrete retaining walls will properly control the raised grades on this site, while not effecting grades on adjacent properties. Actually, the proposed raising of the subject site, to meet proposed grading guidelines, will eliminate awkward grading problems between this site and the adjacent properties.

Currently, the adjacent properties drain across this existing lower subject property. As part of the proposal to raise the site grades, this drainage will need to be addressed as part of the drainage plan for the required retaining walls.

Common area accessibility to the public might be a problem... The site is private, but does not exclude the public. Proposed walks are able to be used to move through the site. If the worry is noise or group meetings that are unwanted, it would be the option of the residents to ask the groups to be quiet or to ask them to leave this site. Privacy at ground level would be achieved by the use of a small property line fences on the proposed concrete retaining wall, massed planting and evergreen trees.

Possibility of using "lane", emergency vehicular access, as a vehicle access to site... I do not believe the municipality would be supportive of this idea. Additionally the existing lane appears too narrow for two way traffic.

Concern about drainage onto cul-de-sac, (Dayton Court)... Storm water drainage for this site will be collected and controlled on this site.

PWL Partnership Landscape Architects Inc. Jeff Philips



Public Information Meeting October 1st, 2014 Sign-in Sheet

Address Postal Code				A SAN						
Address	V6Y283					1673146	V6Y3H6			SHELDA
	#15V 0918		842 Doylar cam	45 KNING 18E8	8415 Day los (or P	8360 Ruphy Con	8360 Dayton Court	8440 Duyton Court	8391 Daylor Court	8220 Achumes Cre
Full Name	Z	Mt1 An	HAMINY SCHAMOSSAM 842 DOWNEN CA	D-N Macterall	7 HCROIL	HARI	Charuta.	Joseph Yang	G Dave egmogras	10



Public Information Meeting October 1st, 2014 Sign-In Sheet

	Full Name	Address	Postal Code	E-mail Address	Signature
4	Paul Law	8231 McBurnay Ort.	V6Y 3H5	paul lan @ metrodaucaver, org	man man
w	I'm Char	9271 MEDIRARY CT.	YEY 3H5	VCT SHS tchan3000 hotmail.con	Me
تز	Carmina her	8271 McBurney Court	V6Y 3HS	clec 000 6@ hotmail. com	The Control of the Co
0	Gary Zhuo	9333 Tendale Kol	V6/11x4		A A
وي	Brind JAGNEGUT	8435 Johnston CMT	16/346		Hans
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B	Lux of Alex Com	8386 Dougthy Cout.	2		7
7.		8 LAGDATESA CRI	s)		B



Public Information Meeting October 1st, 2014 Sign-In Sheet

	Full Name	Address	Postal Code	E-mail Address	Signature
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	Kong Lai	8420 Dayton Court.	948 L9A	V6 > 346 Fam Kang-8406-198406, now Com	Com Mess
	IN MAN MAN			, o D	(4 cm) (-1/2)
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J					The second secon





6:30pm – 8:00pm

1. Do you have any concerns with the width of the access point to the three units at Dayton Crescent Yes/No Comments: CALUES, Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character: 69 West 69th Avenue 604-681-5618 Vancouver BC V5X 2W6 habitatgv.ca





1.	Do you have any concerns with the width of the access point to the three units at Dayton Crescent

Yes/No

Comments:

THE SPACE IS SILL VERY NARROW FOR CARS TO

DRIVE THROUGH THE RESIDENTS MAY PARK IN THER

CARPORIS THE FIRST LITTLE WHILE BUT OVERTIME

THEY MAY START PARKING ON THE STREETS TO

DRIVING THROUGH WAY.

ANDID THEY DIFFICULT ATIGHT DRIVING AREA THISTER

THE

 Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character:

CIES/NO
Comments: WITH THE EVEVATED HEIGHT HOW
CAN HABITAT FOR MUMANITY ENSURE
THE FOUNDATION WILL NOT SETTLE
INTO ADVAGENT PROPERTIES AND
AFFECTING THE STRUCTURE OF NEIGHBORING
HOMES
Name: MEI Ay Address: 8160 ASH ST.





1.	Do you have any concerns with the width of the access point to the three units at Dayton Crescent
	€/No
	Comments:
	THE ENLANCE APPENES TO BE TO NATURAL FOR CARE TO GUSTER
	AND EXIT AND OVERTIME PRESIDENTS WILL PARK THERE CARES
	ON THE STEERY EATHER THAN THE CAMPORT
2.	Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character:
	Ps/No
	Comments: By RAISIAL THE EXISTING 3 MENERS OVERTIME
	SCHLENGET OF SOIL MAIN DAMME THE HEIGHBORING
	PROPERTY. PRIVACY MUSIKI BE AN ISSUE. COMMON
	DUCK BEILL BES ACCESSIBLE TO PUBLIC MOGHY BE AN ISSUE
	Taken Boiler By Moses Ble I Todac Ideal (12- Et 12)
	Till Charles All Street
	Name: Jolth SHUM Address: 8160 Ast STREET
Nes	t 69 th Avenue 604-681-5618





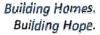
dila 1. Do you have any concerns with the width of the access point to the three units at Dayton Crescent Yes/Dio Comments: too Many 2. Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character: -6172 Dowled Address: 8455 Dayton Ct.

69 West 69th Avenue
Vancouver BC V5X 2W6

The time for mig stoole habitatgv.ca

have been time so people who coult

Walk it to night could come another time.





1.	Do you have any concerns with the width of the access point to the three units at Dayton Crescent Yes No
	Comments: ① Access limited - leading to Cars being parked in the Cul-de sac ② Increased traffic on the road 12 families - 1 car/parinty
	3) A bit of kids play in the neighbourhood & increases the hazard with increased can
2.	Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character: Yes/No Concern with Site grade elevation 4 resulting range.
	Natu/ show water drain into the cul-da-sac area.
	HARI UPATH 8366 Dayton Court Richmond # Name: Name: V64346.





1.	Do you have any concerns with the width of the access point to the three units at Dayton Crescent
	Ye/No
	Comments:
	nayton Count has 6 homes (with rentals) and
	only 6 panking spots, so punking will over Plan
	onto a street where many children play. As well
	all these trebicles must troverse the entire length
	of the street.
2.	Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character: Yes/No
	Comments:
	Name: Name Cumponas Address: 8391 Dayton Count





1.	Do you have any concerns with the width of the access point to the three units at Dayton Crestent
	(Yes/No
	Comments:
	BE A PABLEN, CHABAGE PEKLIN USIL
	BR A PABLEN, CHABBYE PICK CON CENT
	PLSO BE A PROBLEM
2,	Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character:
1	YesNo
(Comments: AN 14 TEXTURE THE PO NOT FIT IN TO
	Comments: ANCHITECTURE THE PO NOT FIT IN TO THE NEIGHBURGOD, THENE ARE NO CARROLS OR Storage ITANS will BE Storage on
	OR Storage Itans will BE Storage an
	Cor port
	Name: H. Shuphishup Address: 8428 Dayton Burnet



. Do you have any concerns with the width of the access point to the three units at Dayton Crescent
Yes No
Comments:
- There are many kids (408+) that regularly play
near or on Dayton Cot. We are very concerned about
all the additional traffic that will come through with the
3 units plus vental suites.
" Parking will also be a problem since the families will not
be able to park in their driveways.
Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character:
Ye No
Comments: Having capports actually makes the houses
look "cheaper"-
TOOK CANKA PET
Name: Address:

69 West 69th Avenue Vancouver BC V5X 2W6 604-681-5618 habitatgv.ca





habitatgv.ca



Vancouver BC V5X 2W6

Public Input October 1, 2014 South Arm Community Centre 8880 Williams Ave Richmond, BC 6:30pm – 8:00pm

 Do you have any concerns with the width of the access point to the three units at Dayton Crescent Yes/No

v.		Comments:
4	Diene	1 · CONCURN DER TRAFFIC VOLUME & HILLS AT PAGION COUNT, THERE ARE CHINDERN/STUDE
	(AT PAGION COUNT, THERE ARE CHIDARN/STUDE
		AT VAGION COUNT, THERE ARE CHILDREN STUDE AGE 26 WHICH PRIBURE DAYSON. CRY. PREFERENCE TO NOT HAVE ALESS. AT
		PRESENTED TO NOT HAVE ALLES. AT
		TO RUNGED TRAFFIC CONDITIONS ON SITE!
		TO RUMAN MARGIC CONDITIONS ON SITE!
	2.	Do you have any comments regarding the proposed homes building schemes in relation to height/building form/character:
		Yes/No
		Comments:
		Name: he Kang Lan Address: May Cal
		Name: Name: Kang Lan Address: May Cal
	69 West	69 th Avenue 604-681-5618

Tel: 604-451-6049

REGISTERED MAIL

October 15, 2014

CEO Habitat for Humanity Greater Vancouver 69 West 69th Avenue Vancouver, BC V5X 2W6

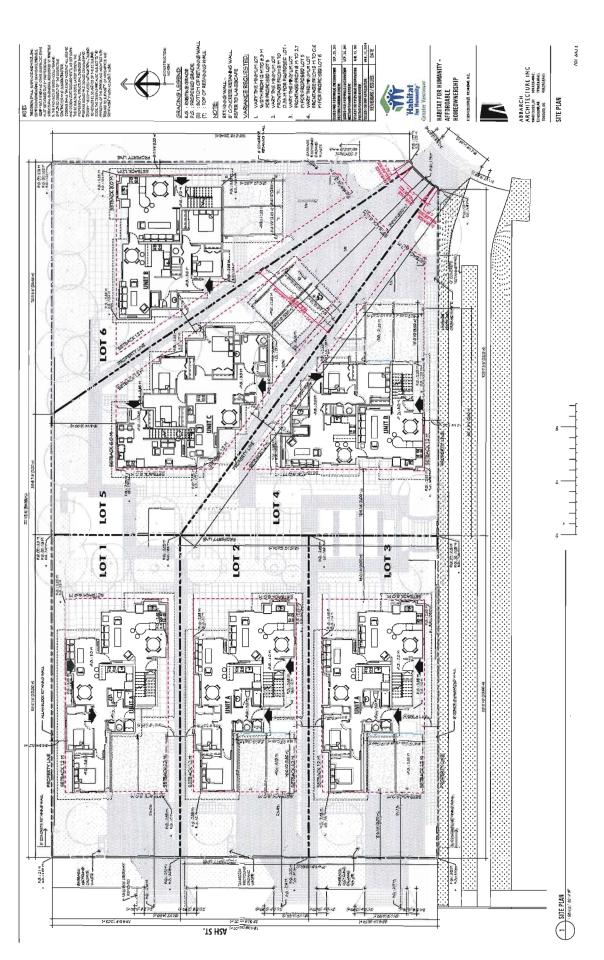
Development Variance Application # DV 14-658670 (City of Rmd ref: DP08-4105-20-AMANDA# \2014-Vol 01) 8180 Ash Street, Richmond, B.C.

Dear Ms Clark,

Thank you for organizing the Open House event two weeks ago (October 1, 2014). I have taken the opportunity to speak to a few neighbours and would like to submit our comments as follows:

- (A) General features that are of concern/interest to the neighbourhood
- A.1 The 3 housing units facing Dayton Court with a shared driveway has posed concerns to the neighbourhood during the 2011 public consultation process (when BC Housing applied for the variance). Some of our neighbours suggest that it should only be 2 instead of 3 units. Insufficient parking for this complex may result in over-flow street parking on Dayton Court and Ash Street.
- A.2 The proposed height of the new houses is apparently higher than the neighbours. This is contrary to what we were told by the Architect's surveyors when field measurements (including the elevation of our homes) were conducted some months ago.
- A.3 The general features of this proposed scheme (in relation to height/building form/character) are likely to attract more attention from the surrounding community at large. In this connexion, we would like to know how many residents in the neighborhood had been notified of the Open House event, and whether notices had been placed in the local papers before the event.
- (B) Items of specific concern to the adjoining neighbours
- B.1 It appears that several feet of fill materials will be brought in to lift the grade level much higher than that of the neighbouring houses. That could cause considerable settlement damages to adjacent structures/houses.
- B.2 Compaction of the soil by this proposed building foundation may result in very poor drainage of the surrounding areas. The effect is likely to be more serious as the new site is higher in grade level and protected by retaining walls (as shown on the architectural schmetics).

B.3	The open layout with shared backyards for these six units serves the concept of this housing scheme well. On the other hand, privacy of the neighbours hopefully can also be addressed by designing of the common activity areas away from the adjourning neighbours and shielded by plantation at the fenceline.
l loc	ok forward to your comments and responses.
Sinc	cerely,
	inal signed and mailed)
	Urban Development Division, City of Richmond (ref. DP08-4105-20-AMANDA# \2014-Vol 01) Councillor Bill McNulty, City of Richmond





PLAN #1.a SUBDIVISION/ZONING PLAN

to take and PRECEIPER. HABITAT FOR HUMANITY – Affordable Homedwwership SITE PLAW SUBDIVISION PL SEC SETEACK 6.D M UNIT **LOT 6** UNIT UNIT B BENKKYZH (1 LOT 4 LOT 5 LOT 1 LOT 3 LOT 2 UNITA UNITA UNITA Washings . PERSONAL PROPERTY OF THE PERSON PROPERTY PROPERTY OF THE PERSON PROPERTY PROPERTY PROPERTY - PROPERTY UNE 日の名が日本の SETENCY IZ M 351354613E 1 SITE PLAW SUBDIVISION PLAN





PLAN #1.b AREA PLAN



SECTION 1: DEVELOPMENT INTRODUCTION

PLAN #1.c SITE STATISTICS/ZONING Project data

LOT 689, SECTION 22, BLOCK 4, NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT, PLAN 67159 LEGAL DESCRIPTION

HABITAT FOR HUMANITY - GREATER VANCOUVER 8180 ASH STREET, RICHMOND, BC, V6Y 2S3 **MUNICIPAL ADDRESS** PROPERTY OWNER:

CITY OF RICHMOND ZONING BYLAW 8500 RS 1 /B SITE ZONING

SITE AREA:

LOT AREA		
LOT#	LOT AREA	
	Σ	r. L
LOT1 (UNITA)	361	38815
LOT2 (UNITA)	361	3881.5
LOT3 (UNITA)	361	3881.5
LOT 4 (UNIT B)	394	4242.0
LOTS (UNITC)	450	4843.0
LOTG (UNITB)	404	4352.0

PERMITTED LOT COVERAGE:

PERMITTED LOT COVERAGE	LOT COVE	RAGE					
LOT#		SITE COVERAGE	35	NON-POROUS	.0	LIVE PLANT MATERIAL	1ATERIAL
		ALLOWED	PROPOSED	ALLOWED	PROPOS≅D	ALLOWED	PROPOSED
		MAX.					
LOTI (UNITA)	[[人]]	45%	39.5%	20%	17 %	25%	27%
LOT 2 / UNI	(UNITA)	45%	39.5%	70%	17 %	25%	20%
LOTS (UNITA)	IIA)	45%	39.5.%	70%	27%	25%	23%
-	UNIT B)	45%	36.3%	7C%	20%	25%	24%
LOTS (UNI	UNIT C)	45%	34.2%	70%	20%	25%	24%
	ПВ)	45%	75 3%	404	13.8	25%	34%

PERMITTED DENSITY:

PERMITTED DENSITY				
LOT#	FSR			
	ALLOWED		PROPOSED	
	150	F5R	5F	70X
LOT: (UNITA)	2135	0.55	1918.1 (168.91 SQ M)	0.47
LOT 2 (UNIT A)	2135	0.55	1818.1 (156.91 5Q M)	0.47
LOT3 (UNITA)	2135	0.55	1918.1 (168.91 5Q M)	0.47
LOT 4 (UNIT B)	2333.10	0.55	1816.5 (168.76 SQ M)	0.43
LOTS (UNITC)	2663.65	0.55	1677.2 (155.82 SQ M)	0.35
LOTA (INITIA)	つか ためたく	250	141G F (168 G1 GO M)	272

YARDS & SETBACKS:

YARDS & SETBACKS						
LOT#	FRONT YARD		SIDE YARD		REAR YARD	
	ALLOWED	PROPOSED	ALLOWED	PROPOSED	ALLOWED	PROPOSED
LOT1 (UNITA)	6.0 M	6.0 M	1,2 M	1.2 M	6.0 M	₩ <i>0</i> :9
LOT2 (UNITA)	6.0 M	6.0 M	12 M	12 M	6.0 M	6.0 M
LOTS (UNITA)	6.0 M	6.0 M	1.2 M	1,2 M	6.0 M	6.0 M
LOT 4 (UNIT B)	Ø.0 M	9.12 M	1.2 M	12 M	6.0 M	6.0 M
LOTS (UNITC)	6.0 M	14.36 M	1.2 M	12 M	6.0 M	6.0 M
LOTG (UNITB)	6.0 M	13.25 M	1.2 M	12 M	6.0 M	6.0 M

PERMITTED HEIGHTS:

PFRM	PERMITTED HEIGHTS				
5		MAX, HEIGHT		RESIDENTIAL VERTICAL LOT	RESIDENTIAL VERTICAL LOT RESIDENTIAL VERTICAL LOT
				DEPTH ENVELOPE	WIDTH ENVELOPE
		ALLOWED	PROPOSED	ALLOWED PROPOSED	ALLOWED PROPOSED
101	LOT1 (LINITA)	9.0 M	8.02 M	REFER TO SHEET A3.1	REFER TO SHEET A3.1
LOT 2	OT2 (UNITA)	9.0 M	8.02 M	REFER TO SHEET A3.1	REFER TO SHEET A3.1
LOT 3	LOT3 (UNITA)	9,0 M	8,02 M	REFER TO SHEET A3.1	REFER TO SHEET A3,1
LOT 4	LOT 4 (UNITB)	9.0 M	8.02 M	REFER TO SHEET A3.1	REFER TO SHEET A3.1
1015	LOTS (UNITC)	9.0 M	7.06 M	REPER TO SHEET A3.1	REFER TO SHEET A3.1
101A	LOTA (LINITR)	X C a	ACOM	OREGEO TO CHEET A31	DESERT TO CHEET A31

PERMITTED SUBDIVISION PROVISIONS/MINIMUM LOT SIZE:

PERMIT	TED SUBDIME	PERMITTED SUBDIVISION PROVISIONS/MINIMUM LOT SIZE:	JI WIWINWS	27 SIZE:			
#L01		MINIMUM FRONTAGE	TAGE	MINIMUM LOT WIDTH	WIDTH	MINIMUM LOT DEPTHL	. рертнь
		ALLOWED	PROPOSED	ALLOWED	PROPOSED	ALLOWED	PROPOSED
LOT 1	(UNITA)	6.0 M	12,02 M	12,0 M	12,03 M	24.0 M	30.0 M
1012	OT2 (UNITA)	6.0 M	12.02 M	12.0 M	12.02 M	24.0 M	30.0 M
1013	OT3 (UNITA)	6.0 M	12,03 M	12.0 M	12.02 M	24.0 M	30.0 M
LOT 4	OT4 (UNITB)	6.0 M	038 M	12.0 M	12.21 M	24.0 M	36.9 M
LOT 5	OTS (UNITC)	6.0 M	2.7 M	12.0 M	9.37 M	24.0 M	M 69'05
101 0	LOTG (UNITB)	6.0 M	O.9 M	12.0 M	12.03 M	24.0 M	36.85 M

PERMITTED SUBDIM	SION PROVISION	PERMITTED SUBDIVISION PROVISIONS/MINIMUM LOT SIZE:
LOT#	MINIMUM LOT AREA	AREA
	ALLOWED	PROPOSED
LOT1 (UNITA)	360.0 SQ M	361,0 5Q M
LOT2 (UNITA)	360,0 SQ M	361,0 5Q M
LOT3 (UNITA)	360.0 SQM	3610 5Q M
LOT 4 (UNIT B)	360.0 SQ M	394.0 SQ M
LOTS (UNIT C)	360.0 SQ M	450.0 SQ M
LOT & (UNIT B)	360.0 SQ M	404.0 SQ M

PARKING: (ZONING BYLAW PART A SECTION 7)

PERMITTED PARKING		
1.0T#	PARKING	
	ALLOWED	PROP05ED
LOT1 (UNITA)	2	2
LOT2 (UNITA)	2	2
LOT3 (UNITA)	2	2
LOT 4 (UNITB)	2	2
LOTS (UNITC)	2	2 (1 HANDICAPPED SPACE & 1 SMALL SPACE)
LOTS (UNITB)	2	2

PARKING SPACE: (ZONING BYLAW PART A SECTION 7)

PERMITTED PARKING SPACES	PACES			
	LENGTH		WIDTH	
	ALLOWED	PROPOSED	ALLOWED	PROPOSED
STANDARD SPACE	H 5'5	W 55	2.65 M	2.65
SMALL SPACE	5.0 M	5.0 M	2.4 M	2,4 M
HANDICAPPED SPACE 55 M	55 M	5.5 M	3.7 M	3.7 M

VARIANCE REQUEESTED:

VARIANCE REQUEESTED:	TED:	
	ALLOWED	PROPOSED
LOT 4 FRONTAGE	6.0 M	0.38 M
LOT 5 FRONTAGE	6.0 M	2.7 M
LOT & FRONTAGE	6.0 M	0.6 M

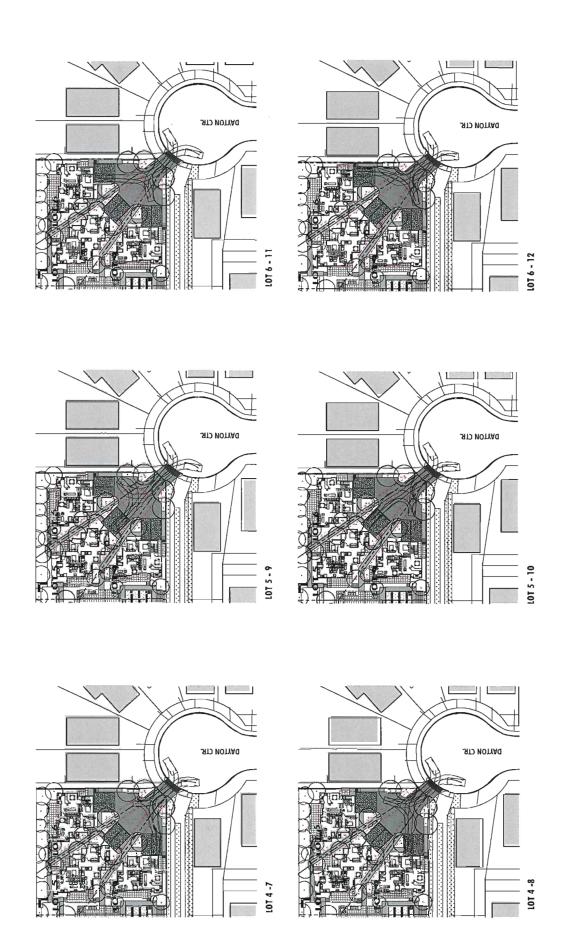


P. S. 177 26.(T): 2.12 P. 7.G.(B): 1.52 M LOT S' CONCRETE RETAINING WALL 18.1213 PG. 20H TZ HZA (ATCARC BY LANT)

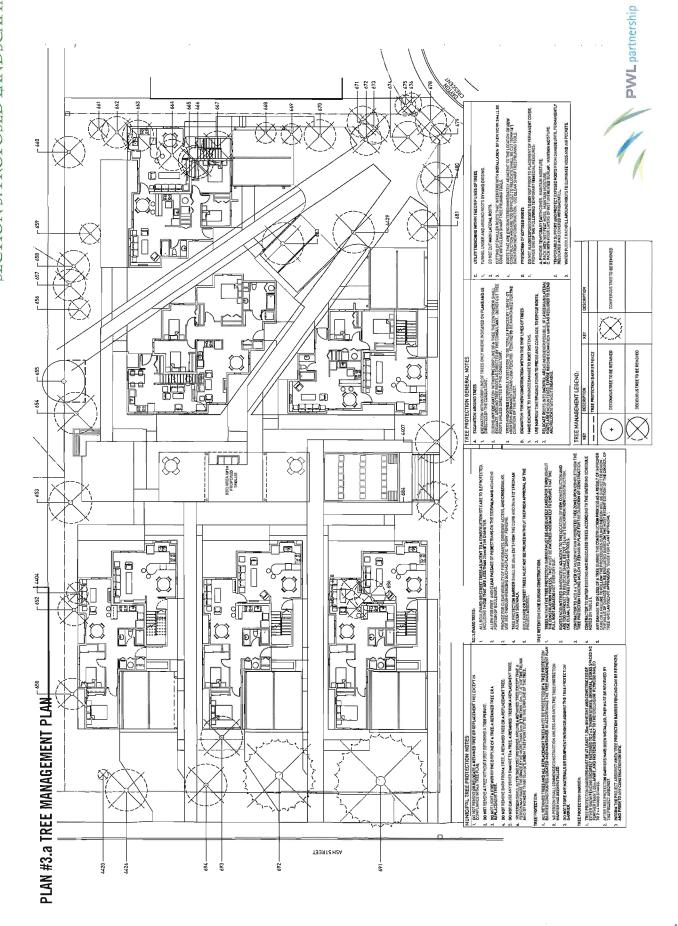
PLAN #2.a PARKING PLAN



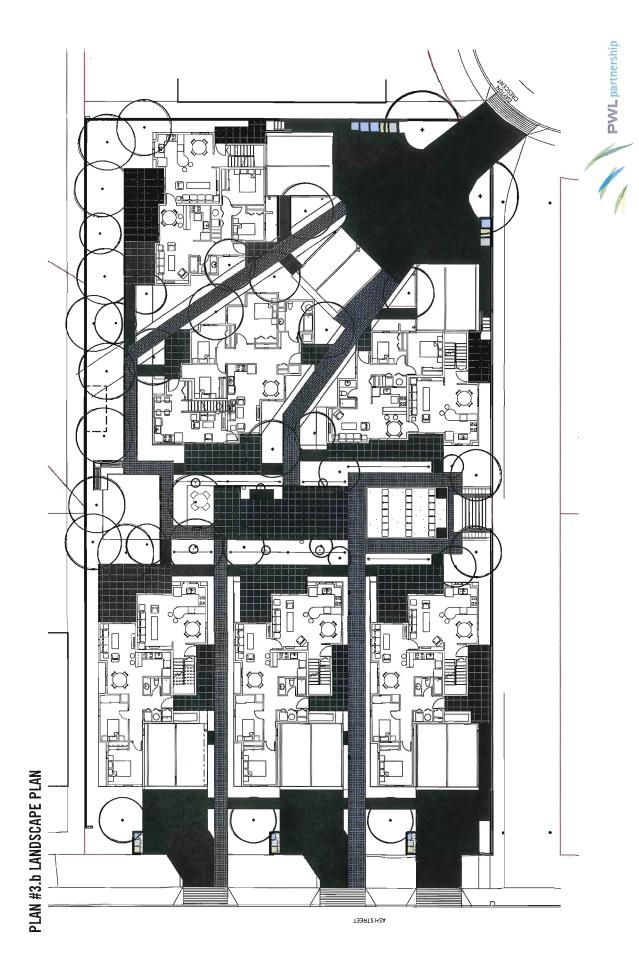
PLAN #2.b PARKING TURNING MOVEMENTS

















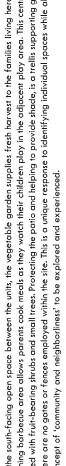
PLAN #3.d RENDERED LANDSCAPE SITE SECTION & RATIONAL

Landscape Design Rationale

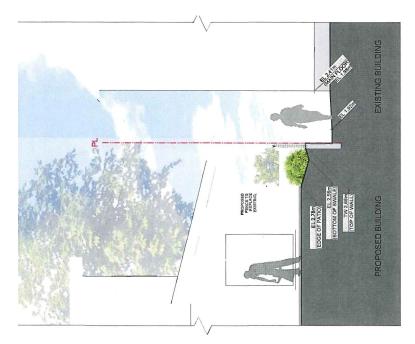
Building a strong sense of community and families are key to the landscape design at 8180 Ash Street. At the heart of the site runs a north/south axis of communal spaces where families come to relax and socialize adjacent the patio/ BBQ area, play area and communal gardens for resident families to grow their food.

joining barbecue area allows parents cook meals as they watch their children play in the adjacent play area. This central spine is lined with fruit-bearing shrubs and small trees. Protecting the patio and helping to provide shade, is a trellis supporting grapevines. There are no gates or fences employed within the site. This is a unique response to identifying individual spaces while allowing the concept of 'community and neighborliness' to be explored and experienced. In the south-facing open space between the units, the vegetable garden supplies fresh harvest to the families living here. The ad-











EXTERIOR FINISH LEGEND:

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MAXIMUM HEIGHT

36.2

T.O. ROJF

PROPERTY LIVE

(# 2078) .8/L E-\$2

LEVEL 2

LEVEL 1

8.-O. (5707 W)

(# COT6) ZN 9-82

U/S ROCF

·84 C1 - 7

U/5 ROOF

9.00

LEVEL 2

5-016

P.G.M:23 H P.G.(8):20 H EGNADT

36.0012

HAXINUM HEIGH

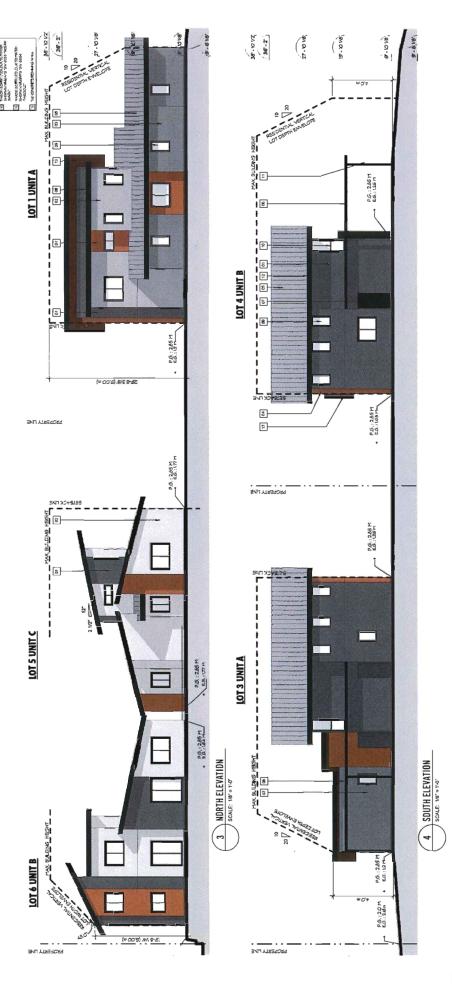
T.O. RGOF

PLAN #4.a SITE SECTIONS



2 WEST ELEVATION

EXTERIOR FINISH LEGEND.



PLAN #4.b SITE SECTIONS

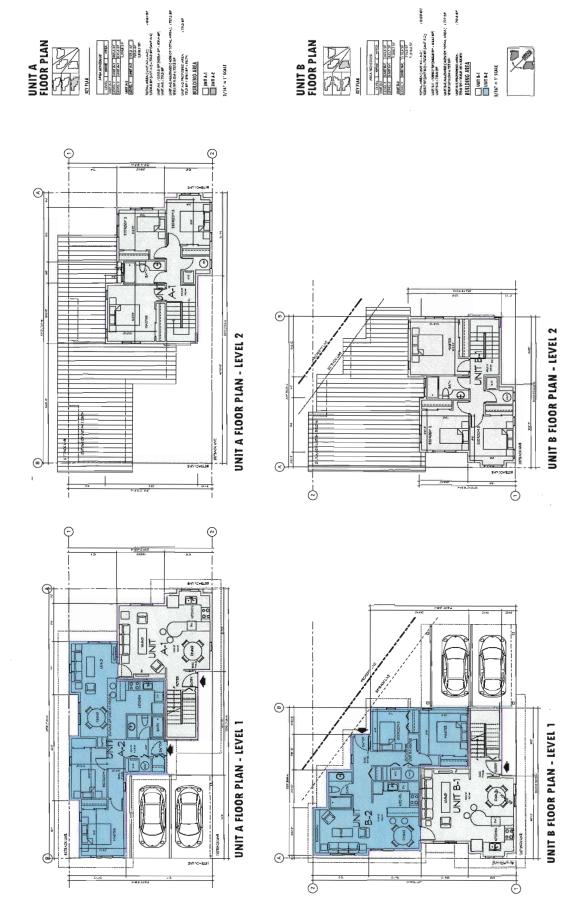


PLAN #4.c AREA SITE SECTIONS

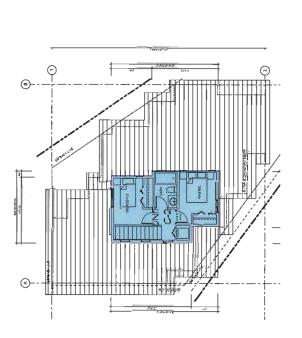




PLAN #5.a UNIT PLANS





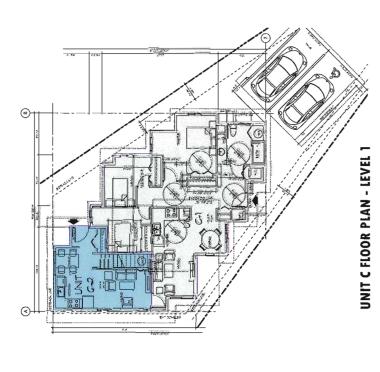


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| 1975 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 | 1976 |

FLOOR PLAN







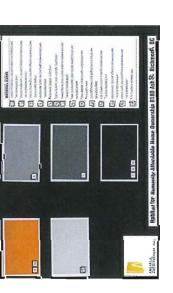




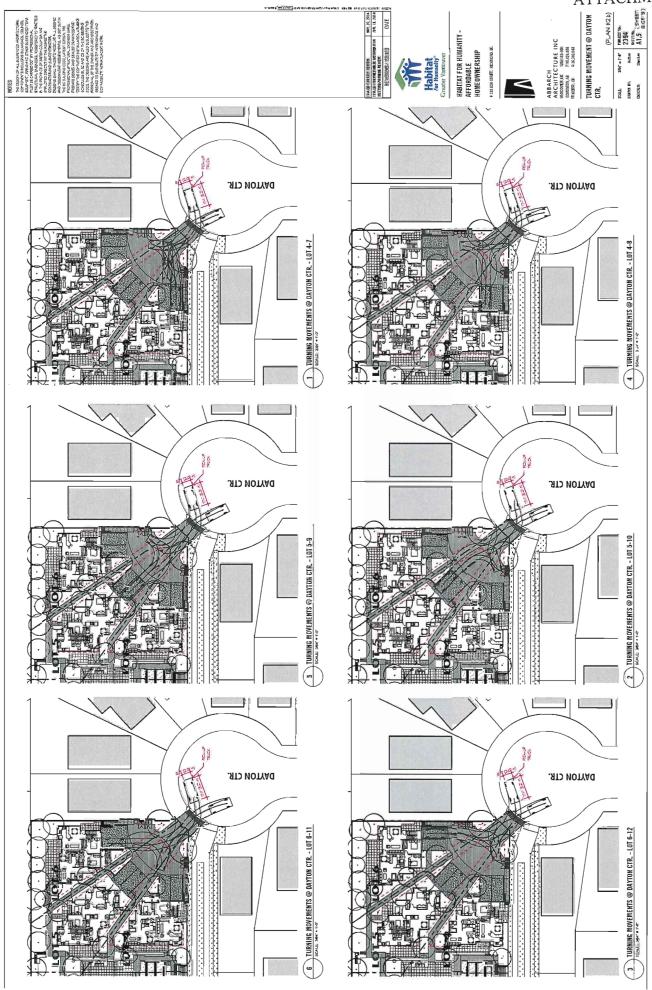






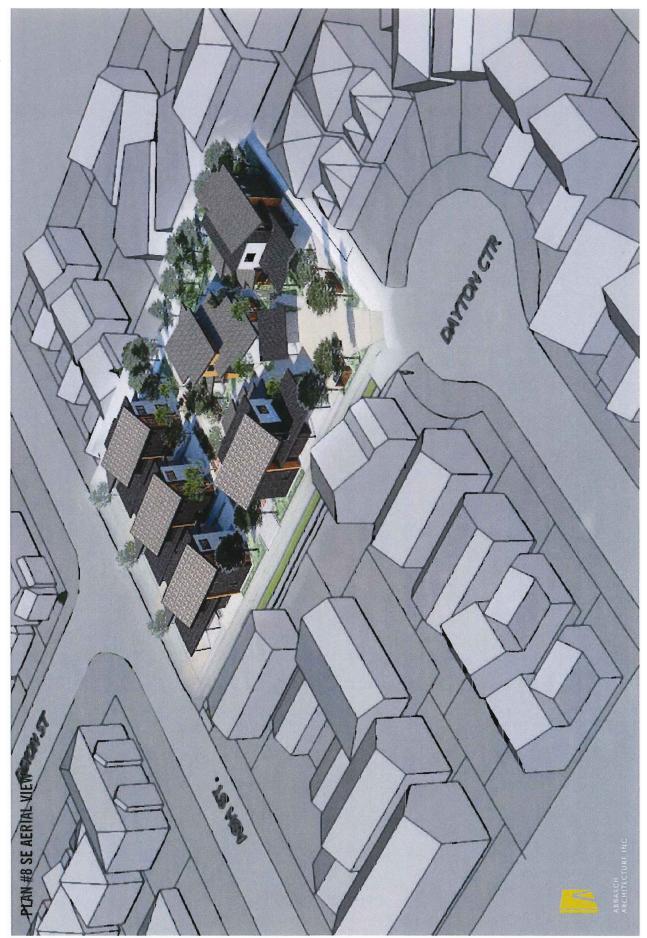


















Development Variance Permit Considerations
Development Applications Division
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 8180 Ash Street File No.: <u>DV 14-658670</u>

Prior to DV 14-658670 being forwarded to Council for approval, the developer is required to complete the following:

1. Provision of a letter of guarantee to ensure tree replacement (including tree replacement for specimen trees valued at \$3,000/specimen tree) and landscaping is in accordance with the plans attached to the Permit.

At future subdivision stage, the developer is required to complete the following:

- 1. Registration of a flood indemnity covenant on title in accordance with the Flood Management Strategy specifying the minimum flood construction level of 2.9 m GSC, or at least 0.3 m above the highest elevation of the crow of any road that is adjacent to the parcel.
- 2. Registration of a cross-access easement, statutory right-of-way, and/or other legal agreement(s) or measures, as determined to the satisfaction of the Director of Development, to secure cross access and shared use of the driveway for lots fronting Dayton Court (proposed Lot 4, 5, and 6) and to ensure a dwelling fronting Dayton Court is setback a minimum 11.0 m to guarantee vehicles have sufficient area to turn around on-site.
- 3. Registration of a legal agreement on title to ensure that the building design on the proposed future lots at future development stage is generally consistent with the architectural plans attached to this report (Attachment 1).
- 4. Registration of a legal agreement on title to ensure that final Building Permit inspection is not granted until a secondary suite is constructed on each of the lots (Lots 1-6), to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw.
- 5. Enter into a Servicing Agreement* for the design and construction of the following works, which include, but may not be limited to:
 - a) Water Works

The developer is required to:

- Coordinate with the Fire Department to address hydrant requirements;
- Incorporate any work items identified from the above process into the Servicing Agreement drawings.
- b) Sanitary Works

The developer is required to:

- From manhole SMH6236 at the northwest corner of the development extend the existing 150 mm diameter sanitary sewer approximately 30 m south and provide service connections and inspection chambers to new lots;
- Provide a 3.0 m statutory right of way for the above item;
- Install a manhole at the property's southeast corner with a 150 mm sanitary sewer connecting into the
 existing manhole (SMH1531) at Dayton Court cul-de-sac and provide service connections and
 inspection chambers to new lots;

• Provide a statutory right of way for the above listed item with a minimum 3.0 m clearance in all directions.

c) Storm Works:

- The developer is required to confirm and demonstrate that in accordance with Bylaw 7551 (Drainage, Dyke and Sanitary Sewer System bylaw), storm water run-off shall be contained within each subdivided property and discharged into a city drainage system.
- The developer is required to:
 - o Coordinate with Building Department to revise on-site storm water designs;
 - o Install up to 60 m of 200 mm storm pipe including 2 manholes and appropriate number of inspection chambers along the property's south frontage to provide lane drainage. Flow is to be directed west to Ash Street.
 - o Install drainage service connections and inspection chambers to service each new lot.

d) Private Utility Works:

The developer is required to:

- Coordinate with BC Hydro, Telus and other private communication service providers:
 - o To underground the Hydro service lines for the proposed development;
 - When relocating/modifying any of the existing power poles and/or guy wires within the property frontages (Ash Street);
 - To determine if above ground structures are required and coordinate their locations (e.g. vista, PMT, LPT, Shaw cabinets, Telus Kiosks, etc.)

e) General items:

- Realign the existing emergency access to accommodate the proposed Dayton Court shared driveway. Remove the two drive aisle strips affected by the realignment, construct a uniform hard surface for the affected area, and replace the existing emergency access barrier curb at Ash Street with a roll over curb.
- Confirm that the emergency access meets BC Building Code requirements for 3.2.5.6 Fire Department Access route design. Lane modifications may be required.
- Ensure that the emergency access lane is kept unobstructed through construction and that it suffers not negative impacts (e.g. disruption to drive aisle strips and/or lane drainage may need to be provided in the emergency access lane if significant modifications are required to address impacts resulting from proposed development of the subject site). Any such impacts will be rectified at the developer's cost.
- Removal of the existing driveway letdown on Ash Street and its replacement with barrier curb and gutter, boulevard and sidewalk per the frontage improvement standards established to the north and south of the subject site.
- Any necessary improvements to the existing sidewalk and rollover curb for the proposed Dayton Court fronting lots in accordance with City design standards.

Prior to Building Permit Issuance, the developer must complete the following requirements:

- 1. Submission of a Geotech report to identify any anticipated impacts to adjacent properties and undertake appropriate mitigation measures, including registration on title of any associated required covenants.
- 2. Submission of a Construction Parking and Traffic Management Plan to the Transportation Division. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
- 3. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and

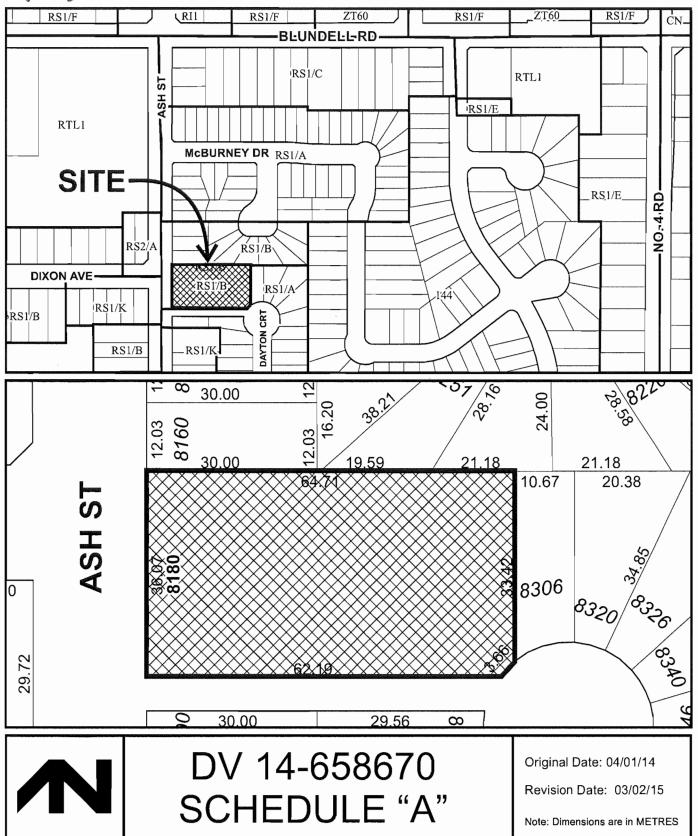
- associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Division at 604-276-4285.
- 4. The site is within a peat area, a geotechnical report is required as part of the Building Permit review. The Geotechnical report identifies any anticipated impacts to adjacent properties and appropriate measures to keep properties safe for their intended use. A subsidence covenant would be registered on the title of the properties being developed.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.
 - All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.
 - The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, Letters of Credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.
- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, preloading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial Wildlife Act and Federal Migratory Birds Convention Act, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed (original on file)		Date	







Development Variance Permit

No. DV 14-658670

To the Holder:

HABITAT FOR HUMANITY SOCIETY

OF GREATER VANCOUVER

Property Address:

8180 ASH STREET

Address:

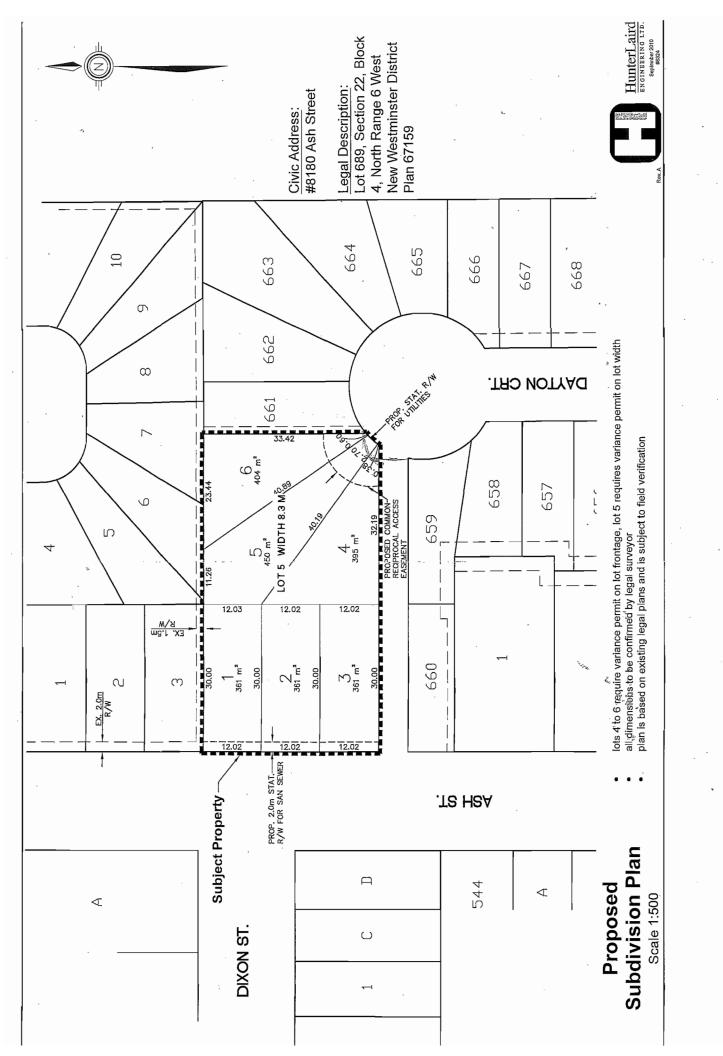
C/O TIM CLARK

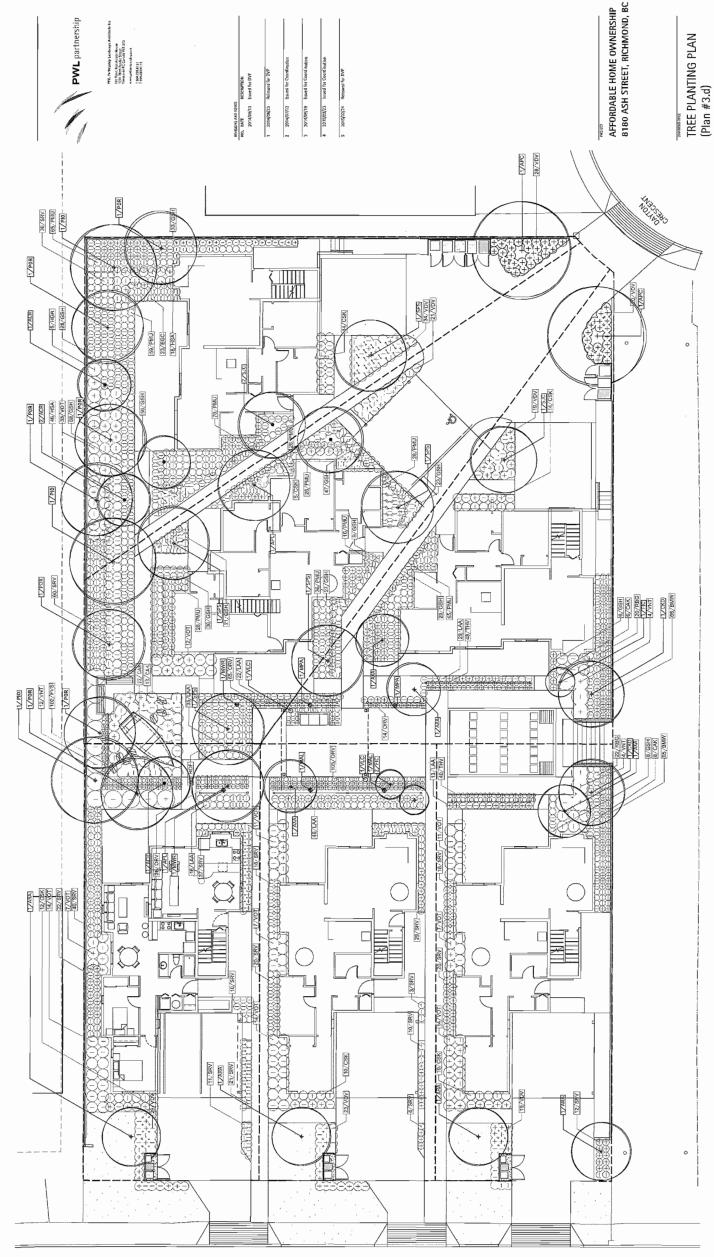
69 WEST 69TH AVENUE VANCOUVER, BC V5X 2W6

- 1. This Development Variance Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied by this Permit.
- 2. This Development Variance Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. The "Richmond Zoning Bylaw 8500" is hereby varied as follows:
 - a) Vary the minimum lot width from 12 m to 8.3 m for proposed Lot 5;
 - b) Vary the minimum lot frontage from 6 m to 0.38 m for proposed Lot 4, to 2.7 m for proposed Lot 5 and to 0.60 m for proposed Lot 6.
- 4. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.
- 5. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO. DAY OF , .	ISSUED BY THE COUNCIL THE
DELIVERED THIS DAY OF	,
MAYOR	





ASH STREET

2 2014/07/03 Issued for Coordination

1 2014/06/23 Reissued for DVP

3 2014/08/19 Issued for Coordination

REVISIONS AND ISSUES

NO. DATE

2014[06] 13 Issued for DVF

4 2015/02/23 Issued for Coordination

Plant List PWL Landma	Plant List PWL Landmark	¥				
<u></u>	aty	Botanical Name	Common Name	Scheduled Size	Spacing	Remarks
		Trees				
ACR	4	Acer circinatum	Vine Maple	2.4 m ht. [8'-0" ht.]	As Shown	B&B, Nursery grown, minimum 3 stems
APL	7	Acer palmatum 'Green'	Green Japanese Maple	2.4 m ht. x 1.8 m w. [8'ht. x 6'w.]	As Shown	B&B, Specimen and densely branched, no included bark
A C	7	Acer platanoides Cleveland	Cleveland Norway Maple	8 cm cat, (3 cat.)	As shown	B&B, Uniform branching, dense tree, 7 [2.1 m] std.
AXA AXA	t √	Acer rubrum Armstrong Amelapchier x grapdiffora "Antumo Brillance"	Armstrong Ked Mapte Antiimo Brilliance Serviceberry	8cm cat. (3 cat.) 4 cm cal. (11/2" cal.)	AS Shown As Shown	B&B, Uniform branching, derise tree, 7 (2.1 m) std. B&B: Multi-stemmed dense tree
CKO	5	Cornus kousa	Kousa Dogwood	5 cm cal. [2" cal.]	As Shown	B&B. Well branched, dense tree
FIC	က	Ficus carica 'Desert King'	Desert King Fig	5cm cal. (2" cal.)	As Shown	B&B, Uniform branching, dense tree, 4' [1.2m] std.
MWR	2	Malus 'esopus spitzenberg'	Esopus Spitzenberg Apple	1,25cm cal. (0,5" cal.)	As Shown	1/2" caliper whip for espalier
MAL	2 .	Matus 'Liberty'	Liberty Apple	1,25cm cal, [0,5" cal,)	As Shown	1/2" caliper whip for espalier
ΣL	7 8	Matus spartan	Spartan Apple	1,25cm cat, [0,5 cat.]	As shown	1/2 callper whip for espatier
YOY S	~ ~	Picea omorika Dicas emocika	Serbian Spruce	2.4 m ht. (8 -0" ht.)	As Shown	B&B, Well branched, dense tree
5 2	ים כ	Stewartia nseudocamellia	Japanese Stewartia	5 cm cal [2" cal]	As Shown	B&B Well branched dense tree
SJC	က	Styrax japonica	Japanese Snowbell	5 cm cat, [2" cat]	As Shown	B&B, Uniform branching, dense tree, 6' [1.8m] std.
		Shrubs				
BMW	131	Russis microphylla 'Winter Gem'	Winter Gem Boxwood	#2 not	38cm [15"]	Well established
CSK	65	Cornus sericea 'Kelseyii'	Kelsevii Doawood	#2 pot	60cm [24"]	Well established, nursery grown
GSH	389	Gaultheria shallon	Satal	#2 pot	45cm [18"]	Well established, nursery grown
HGR	2	Hydrangea quercifolia	Oak Leaf Hydrangea	#3 pot	90cm (36")	Well established
SRV	455	Skimmia reevesiana	Reeves Skimmia	#2 pot	45cm (18")	Well established
Z (707	Vaccinium corymbosum 'Northland'	Northland Blueberry	#3 pot	120cm [48"]	Well established
20	132	vaccinium ovatum. Inungerbirg Viburnum davidii	i nunderbird Evergreen Huckleberry David Viburnum	#3 pot #2 pot	/acm (30) 60cm (24")	welt estabulsned, nursery grown Well established
		Ground Cover				
FVS	102	Fragaria vesca	Woodland Strawberry	#1 pot	25 cm [10"]	Well established, nursery grown
C C C	22	Enimodium arradiffarum (Primesa Beauty)	Contracting Description	#100	(F cm [18"]	Wall artablished
HSA	67	Epimedidiii glandiidol diii Omiison Beadry Hosta siefoldiana "Elegans"	Cillisoli beauty ball'eliwolit Elegans Plantain I ilv	#1 pot	43 cm (18.) 60 cm (24.")	Weit established
Ą	161	Lavandula angustifolia	True Lavender	#1 pot	45 cm (18")	Well established
ORV	220	Origanum vulgare	Oregano	#1 pot	30cm [12"]	Well established
ROF	36	Rosemarinus officinalis	Rosemary	#1 pot	45 cm (18")	Well established
RBG	42	Rudbeckia fulgida 'Goldstrum'	Black-Eyed Susan	#1 pot	45 cm [18"]	Well established
À.	88	Thymus vulgare	Тһуте	#1 pot	30cm [12"]	Well established
		Ornamental Grasses				
CAK	47	Calamagrostis x acutiflora "Karl Foerster"	Karl Foerster Feather Reedgrass	#1 pot	75 cm (30")	Well established
		Vines				
VLC	2	Vitis labrusca 'Concord'	Concord Grape	#2 pot	As Shown	Staked, full development
		Ferns				
PM∪	417	Polystichum munitum	Western Sword Fern	#1 pot	45 cm [18"]	Well established, nursery grown

- 1. ALL PANTING GENERAL NOTES

 1. ALL PANTING GENERAL NOTES

 1. ALL PANTING GENERAL NOTES ANGLADS PEOPLED IN THE LATEST EDITION OF THE ARCHITECTE AND ELANDSCAPE AND WIDEER VESCORITOR. THE GESCHAPPER SPECIFIED FOR PLAN WATERALS. TO INCLUDE ALL OF WESTERN NORTH AMERICA.

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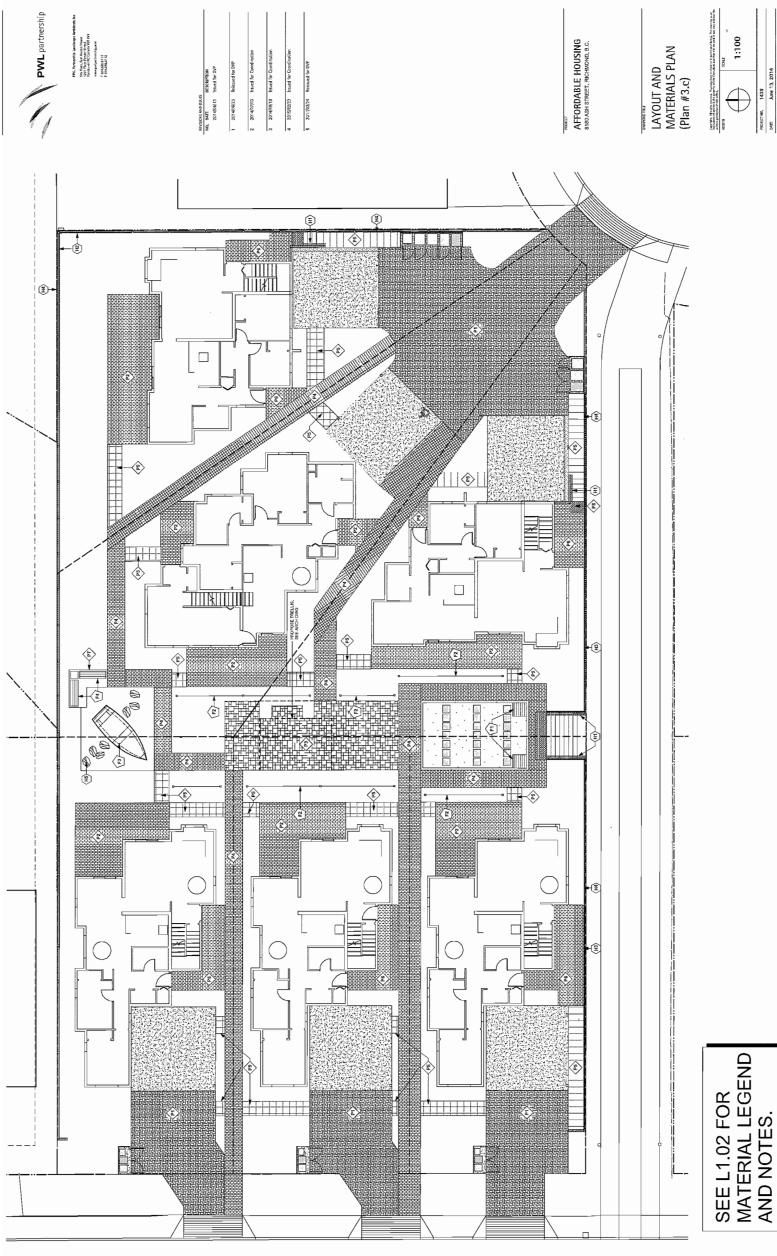
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AFFORDABLE HOME OWNERSHIP 8180 ASH STREET, RICHMOND, BC

TREE PLANTING PLAN (Plan #3.e)



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YAY	PAVING LEGEND								SITE	FURNISH	SITE FURNISHING LEGEND
KEY	DETAIL	DESCRIPTION	KEY	OETAIL	DESCRIPTION	ΚΕΥ	DETAIL	DESCRIPTION	ΚΕ	DETAIL	DESCRIPTION
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<u>\$</u>	\bigcirc	ANNIA TIPE FRANKI PARE TO THE PARE TO THE IN- SPECIAL ADDRESS AND THE TO THE IN-1992 SERVICE IN- SECRIFICAL SHAWN SHOWN SHOWN SHOWN IN-1992 SERVICE TO THE IN-1992 SERVICE TO THE THE TO THE THE THE TO THE THE TO THE THE T	(E)	\bigcirc	SURVETO TEEL MANDET TEEL MANDE	(n)		ANNIGOTORE LIGHT WARNING SIGN SERPELIZER, Absoluted Concrete Perducts (1-800-643-409)] GESCREPTION Floores SEEE - Stomm L. x. Glimm L. x.	(Z)	0	FURNISHING TYPE: Espatier Su SUPPLIER: By Contractor OESCRÍPTION: n/a SIZE: n/a FINISH: n/a
\rightarrow{\text{\text{\$\tilde{\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\text{\$\tilde{\tilde{\tilde{\tilde{\tilde{\text{\$\tilde{\tiil}}}}\tilde{\tilde{\tilde{\tilde{\tilde{\tilde{\tilde{\ti	C	PAYING TYPE: Permeable Unit Power - Type 3 SUPPELER, Abbodisord Concrete Products [1-800-453-4091] DESCRIPTION Ventian Cabble Series STEE: Verless FATE: Verless	(Sed)	C	PAVING TYPE: Hydrapressed Concrete Siab - Type 2 SUPPLIER: Abboistord Concrete Products 11-800-663-4091) DESCRIPTION: Leadin — Name W. 5gmm H [24" x 24" x 2"] FMISH: in Hysbalkset Chour						COMMENT: FURNISHING TYPE: Play Struct Row Boat) SUPPLIER: n/a

-	SITE	FURNISH	SITE FURNISHING LEGEND
_	KEY	DETAIL	DESCRIPTION
	(F)	\bigcirc	Publishind Prifer, Compact Bin SUPPLIER B. Contractor SIZER IVA SIZE, IVA COMPIENT,
	(<u>r</u>	0	FURNISHINO TYPE: Espaiker Support DESCRIPTION: n/a SIZE: n/a SIXE: n/a COMMENT: n/a COMMENT: n/a
	(3)	0	FURNISHNO TYPE, Play Structure (Patiential Refurnished Small Wooden Superuser, via Row Boah) SUPPLIER, via SESERPTURE, via SIEE, via COMMENT: COMMENT: A COMMENT:
	(I		FURNISHING TYPE. Wood and motal banch DESCRIPTION. n/a DESCRIPTION. n/a FINKSH. n/a COMMENT. n/a

PWL partnership

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3 2014(01)19 boxed for Condination
4 2015(02)24 boxed for Condination
5 2015(02)24 Research for DIP

RAYGOUS AND ESSUIS
NO. BATE BESSENTION
2014/08[13 Errock for DVP
1 2014/08[23 Refssed for DVP

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DESCRIPTION
HARDSCAPE TIPE Concrete shair with handrail
OFFICIAN Contractor
STATE STATE TO THE CONCRETE SHAIR HARDSCAPE
THIRTH-OFFIC CONTRACTOR
CONFIDENT TO THE CONTRACTOR
DESCRIPTION AND THE CONTRACTOR
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HARDSCAPE LEGEND
KEY DETAIL DESCRIP

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- LAYOUT AND MATERIALS GENERAL NOTES

 1. DO NOT SCALE CHANDSCAPE PLANS.
 REPORTANT DESCRIPTIONS TO CONSULTANT TO REPURE MAD RESPONSE.
 REPORTANT DESCRIPTIONS OF SERVICES TO CONSULTANT TO REPURE MAD RESPONSE.
 2. LAROUT AND MATERIALS ORAWINGS ARE TO BE READ IN CONJUNCTION WITH LANDSCAPE SECREMENTING.

- 3. LANSSACE DRAWINGS, REPORT ANY DECREPANCIES TO CONSULTANT FOR REVIEW
 AND RESPONSE.
 4. VEREYAL, DINNERGIOS WATER TO BE READ IN CONJUNCTION WITH ARCHITECTURAL, AND
 AND RESPONSE.
 5. PRERYAL, DINNERGIOSING WATER TO CONSULTANT OF SCREPANCIES TO
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 6. PREFERENCE ONLY, LIGHTING
 WITH THE SECRETARY ONLY AND SCAPE PLANS IS FOR REFERENCE ONLY, LIGHTING
 WITH TELEFRICATE SOMEWINGS. FOR LAYOUT OF ROAD CURBS AND GUTTERS.
 6. REFERENCE CIVIL ENGINEER'S DRAWINGS FOR LAYOUT OF ROAD CURBS AND GUTTERS.

HOUSING
AFFORDABLE HOUSING
8180 ASH STREET, RICHMOND, B.C.

LAYOUT AND MATERIALS PLAN (Plan #3.c)

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	PROJECT NO.	DATE	HLE LAME



Report to Development Permit Panel

Planning and Development Department

To:

Development Permit Panel

Date:

February 27, 2015

From:

Wayne Craig

File:

DP 14-659747

Re:

Director of Development

Application by Dava Developments Ltd. for a Development Permit at 2671, 2711, 2811, 2831, 2851, 2911, 2931, 2951, 2971 and 2991 No. 3 Road

Staff Recommendation

That a Development Permit be issued which would:

- 1. Permit the construction of a development with two (2) two-storey commercial buildings totalling 2368 m² at 2671, 2711, 2811, 2831, 2851, 2911, 2931, 2951, 2971 and 2991 No. 3 Road on sites zoned "Auto-Oriented Commercial (CA)".
- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - a) reduce the required manoeuvring aisle width from 7.5 m to 6.0 m (Section 7.5.5); and
 - b) reduce the required setback of parking from property lines abutting a road from 3.0 m to 2.7 m and reduce the required setback of parking from other property lines from 1.5 m to 0.0 m (Section 7.5.17).

Wayne Craig

Director of Development

WQ:jg Att.

Origin

Dava Developments Ltd. has applied to the City of Richmond for permission to develop 2671, 2711, 2811, 2831, 2851, 2911, 2931, 2951, 2971 and 2991 No. 3 Road with two (2), two-storey, commercial buildings totalling 2,368 m2 on a site zoned "Auto-Oriented Commercial (CA)". The site is currently vacant.

The site is being rezoned from "Light Industrial (IL)" to "Auto-Oriented Commercial (CA)" by Richmond Zoning Bylaw 8500, Amendment Bylaw 9042 (RZ 11-566630). The rezoning bylaw was given third reading following the Public Hearing on September 3, 2013. It was supported along with amendments to the Official Community Plan (OCP) Bylaws 7100 and 9000 (Amendment Bylaw 9041) to redesignate the lands from park use to commercial use, specifically "Urban Centre T5 (45m)".

As part of the rezoning, a new road - Douglas Street - will be dedicated and constructed. Improvements also are required for the new road, as well as for No. 3 Road, Bridgeport Road and the existing lane. A Servicing Agreement for the improvements is required prior to rezoning approval.

Development Information

The site is in the City Centre's "Bridgeport Village" and is located at the northwest corner of No. 3 Road and Bridgeport Road. It will be subdivided into two (2) parcels separated by the new road, which extends Douglas Street from No. 3 Road west to the lane. The Canada Line guideway runs north-south through the site.

The application proposes a two-storey commercial building on each new parcel. The building on the north parcel is proposed to be 1,423 m² in area, with retail uses on the ground floor and restaurant uses on the second floor. The building on the south parcel is proposed to be 946 m² in area, with retail uses on the ground floor and office uses on the second floor. The proposed floor area ratio (FAR) for the combined sites is 0.45 and the proposed height is 12.0 m. Parking, loading and garbage facilities are proposed at grade, to the west of the buildings, and are accessed from Douglas Street and the lane.

Please refer to the attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the north, abutting the site, is a property developed with a one-storey building housing various auto service strata units. The property is zoned "Light Industrial (IL)".

To the east, across No.3 Road, are a number of properties zoned "Light Industrial (IL)". They are generally developed with one and two-storey buildings containing a variety of auto service uses.

To the south, across Bridgeport Road, is a larger site under rezoning application (RZ 14-628557) to develop a hotel, office and commercial education complex. These properties are currently zoned LUC 126.

To the west, across the lane, are a number of properties zoned "Light Industrial (IL)". Some are vacant and others are developed with one or two-storey buildings with a variety of industrial/service uses. The properties at 8411, 8431 and 8451 West Road are part of the proposed "Duck Island" development currently under rezoning application (RZ 12-598104).

Rezoning and Public Hearing Results

During the rezoning process, no technical requirements were identified to be addressed at the Development Permit stage.

The Public Hearing for the rezoning was held on September 3, 2013. At the Public Hearing, one letter regarding a future lane dedication for 8560 River Road was discussed. Staff were asked to further explain dedication circumstances to the property owner and the application received third reading.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the design issues and other staff concerns identified as part of the review of the subject Development Permit application.

Zoning Compliance/Variances (staff comments in bold)

The application complies with the use, density and other provisions of the CA zone, as well as the general provisions of the Richmond Zoning Bylaw, except as noted below.

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- a) reduce the required manoeuvring aisle width from 7.5 m to 6.0 m (Section 7.5.5); and
- b) reduce the required setback of parking from property lines abutting a road from 3.0 m to 2.7 m and reduce the required setback of parking from other property lines from 1.5 m to 0.0 m (Section 7.5.17).

The parking layout of the proposed development is severely constrained by the site configuration, the Canada Line guideway pylons and a Canada Line substation located on the property. The proposed variances, which allow for the total number of parking spaces to be optimized whilst providing for functional manoeuvring, are supported by Transportation staff because:

- the development is of a modest size and will generate only a modest overall amount of traffic;

- the locations on the two parcels where the proposed manoeuvring variance would apply will have minimal impact on vehicle movement; and
- the locations where the proposed parking setbacks variances will apply have adequate area for landscape screening where desirable (the street frontages) and none where landscape screening is not necessary (the rear lane and internal side yard).

Advisory Design Panel Comments

The Advisory Design Panel (ADP) reviewed the project on September 17, 2014 and expressed support for the proposal. A copy of the relevant excerpt from the Advisory Design Panel Minutes is attached for reference (Attachment 2). The applicant's response to each Panel issue is included and identified in 'bold italics'. The applicant has addressed the Panel comments to the extent recommended by staff.

Analysis

Conditions of Adjacency

- Staff have not identified any significant impacts on the current or future anticipated developments on properties to the north, east, south and west. All of the surrounding properties are expected to be redeveloped at much higher densities and heights than the proposed development.
- The Canada Line guideway traverses the subject site and the roof of the building will be highly visible to travellers. The applicant has proposed to provide a decorative treatment, which includes varied gravels, artificial turf and sculptural boulders, as well as screening of rooftop mechanical equipment, to improve the view.
- To ensure public safety, Translink requires that a setback or barrier be provided between the roof of the building and the top of the guideway. The applicant, in discussions with Translink, has chosen to provide a barrier, in the form of metal fencing, which will be attached to the guideway.

Urban Design and Site Planning

- The City Centre Area Plan (CCAP) suggests that this part of Bridgeport Village be developed with airport-related, high-rise buildings that include a continuous streetwall along public frontages and parking concealed from public view. The configuration of the subject site (long and narrow) combined with multiple physical constraints (the Canada Line guideway, a skytrain substation, the addition of a new street through the middle of the site, the required preservation of a large tree at the southeast corner of the site and MOTI restrictions on access to and from Bridgeport Road) limits both the development potential and the form of development for this site. Although the proposed development is lower in scale than suggested by the CCAP, it addresses the CCAP Development Permit Guideline objectives by providing a strong, two-storey streetwall along No. 3 Road, the main north-south pedestrian linkage to the waterfront, and by providing low decorative walls with higher trellising around the surface parking.
- Both buildings in the proposed development incorporate street-animating retail uses with entries and weather protection along No. 3 Road. A 3.0 m Statutory Right-of-Way is provided as a condition of rezoning in front of the retail spaces to create a wider sidewalk and wider planted boulevard. The south building includes a lobby entrance from No. 3 Road

- for access to the upper level office space. The north building provides a colonnade from Douglas Street to a proposed restaurant lobby entrance, which is located on the west side of the building adjacent to the parking.
- The layout of the north building is constrained by the edge of the Canada Line guideway. A chain link fence attached to the guideway prevents access from the roof of the building onto the track.
- The site design will retain and highlight an existing London Plane tree located on public property at the corner of Bridgeport Rd and No. 3 Road. A small "plaza" permits circulation around the tree and away from the busy intersection.
- To satisfy MOTI requirements, the lane along the western edge of the property will no longer have a connection to Bridgeport Road. Consequently, a truck turnaround is required and is located between the sidewalk and the guideway column. Due to turning radius requirements, there is limited space for screening. To improve the appearance of this area, the applicant has provided enhanced paving materials as well as planting on the City boulevard. To ensure pedestrian and vehicular safety, bollards and lighting are provided. Where located on City property, the required landscaping, lighting and bollards will be a component of the servicing agreement.

Transportation and Traffic

- Driveway access to each site is provided from Douglas Street.
- A bank of parking on the south lot will be accessed from the lane. This arrangement was anticipated in the conceptual scheme presented at rezoning and is necessitated by the relatively shallow depth of the site.
- The site meets the Zoning Bylaw parking space requirement with a 2.5% transportation demand management (TDM) reduction, from 82 to 80 spaces. The TDM measure identified for this reduction is a contribution of \$25,000 toward a bus shelter on Bridgeport Road to the west of the site.

Architectural Form and Character

- As previously mentioned, the development potential of this site is constrained by the Canada Line guideway and other factors. Consequently, the form of development is a low scale, rectilinear form located in the remaining area of the site. To create visual interest, the building is embellished by a series of "pop-up" roofs that are used to mark the building corners and the main entries. The pop-up roofs also will screen rooftop mechanical equipment from the street view. Further screening of mechanical equipment is provided to improve the view from the Canada Line.
- Large storefront windows are provided along No. 3 Road, enhancing the inside-outside relationship with the street.
- The proposed building will utilize tilt-up construction. The exterior finishing combines smooth and stamped (brick pattern) concrete, large expanses of storefront glazing, timber knee-bracing under the soffits, and, steel and glass weather protection to provide for the "casual, edgy, urban-industrial aesthetic" recommended for the Bridgeport Village by the Development Permit Guidelines.
- The basic colour scheme is taupe with wood detailing.

Landscape Design and Open Space Design

- Ground level soft landscaping is focused in the No. 3 and Bridgeport Roads intersection corner plaza, around the base of the retained London Plane tree; in the building setback areas along Douglas Road; along the boulevard and in the setback along Bridgeport Road; and, in the setback along the northern edge of the north parcel. These areas serve to provide visual interest and layering in front of the buildings and parking screens.
- Street trees and a wider pavement band will be provided along No. 3 Road to encourage pedestrian travel into Bridgeport Village and toward the river.
- The London Plan tree will be pruned, in consultation with the arborist, to improve its appearance and accommodate vehicular and pedestrian movement around it.
- Bench seating is provided in various locations along No. 3 Road and in the corner plaza.

Sustainability

- The application is required to demonstrate compliance with LEED Silver (equivalency) or better, paying particular attention to features significant to Richmond.
- The developer has retained a sustainability consultant and a LEED checklist has been provided (Attachment 3).

Crime Prevention through Environmental Design (CPTED)

- The application proposes to address CPTED objectives and criteria by providing:
 - o clearly defined, direct and illuminated pedestrian routes;
 - o lobbies that are placed in prominent locations and have clear sightlines to fronting streets and publicly-accessible open spaces; and
- o visibility into the surface parking areas from the streets through the low wall and high trellis screening.

Kinder Morgan/Trans Mountain

The rezoning conditions note that the developer is responsible for getting all necessary approvals for work within the vicinity of the jet fuel pipeline. An e-mail from Kinder Morgan, copied to staff, notes that the project is not within the 7.5 m right of way and that works (including off-site improvements) within 30m of the pipeline will require the completion of a 30m Ground Disturbance Safety Zone field permit through Kinder Morgan before commencement of any works.

Conclusion

The mixed retail, restaurant and office development proposed by Dava Developments Ltd. at 2671 and 2911 No. 3 Road complies with the provisions of the "Auto-Oriented Commercial (CA)" zoning, which received Third Reading on September 3, 2013. The proposed design also satisfactorily addresses the CCAP Development Permit Guidelines and other Council policy.

Variances related to the parking layouts on both the north and south parcels support provision of the maximum number of spaces within the physical constraints of the site. In consideration of a total parking reduction from 82 spaces to 80 spaces (per Section 7.4.4 of the Richmond Zoning Bylaw Parking and Loading), the applicant has offered to provide a contribution of \$25,000 to a new bus shelter on Bridgeport Road just to the west of the site.

Staff support this Development Permit application and the proposed variances and recommendits approval subject to the Development Permit Considerations (Attachment 4).

Janet Digby

Senior Planner 3-Urban Design

Attachments: Development Application Data Sheet

Advisory Design Panel Minutes

Sustainability Checklist

Development Permit Considerations



Development Application Data Sheet

Development Applications Division

DP 14-659747

Address:

2671, 2711, 2811, 2831, 2851, 291, 2931, 2951, 2971 and 2991 No 3 Road

Applicant:

Dava Development

Owner:

B.C. 675249 B.C. Ltd

Planning Area(s):

City Centre - Bridgeport Village

Floor Area Gross:

2,487.9 m²

Floor Area Net:

2,360.6.2m²

	Existing	Proposed
Site Area:	6,246.6 m ²	5230.0 m ² (net of road dedication)
Land Uses:	Vacant	Commercial
OCP Designation:	Park	Commercial
Zoning:	Light Industrial (IL)	Auto-Oriented Commercial (CA)
Number of Units:	Nil	Two-storey, multi-unit commercial building

	Required	Proposed	Variance
Floor Area Ratio:	0.50 (based on combined sites)	0.45 (based on combined sites)	none permitted
Lot Coverage:	Max. 50%	North Parcel - 27% South Parcel - 20%	none
Setback – Front Yard (No 3 Rd):	Min. 3.0 m	3.0 m	none
Setback – Ext. Side Yard (Bridgeport Rd):	Min. 3.0 m	10.0 m	none
Setback – Ext. Side Yard (Douglas Street):	Min. 3.0 m	3.0 m	none
Setback – Int. Side Yard:	Min. 3.0 m	23.0 m	none
Setback – Rear Yard (Lane):	Min. 3.0 m	18.0 m	none
Height (m):	Max. 12.0 m	12.0 m	none
Off Street Parking Spaces – Commercial:	74 (with TS/ TDM measures) 82 (without TS/ TDM measures)	80 (with TDM measures)	none
Disabled Spaces Provided:	2	2	none
Parking Aisle Width:	7.5 m	North Parcel – 6.0 m South Parcel – 6.7 m	variance required
Parking Setback (Street)	Min. 3.0 m	North Parcel – 2.8 m South Parcel – 2.7/2.9 m	variance required
Parking Setback (Other):	Min. 1.5 m	North Parcel – 0.0 m South Parcel – 1.4 m	variance required

Excerpt from the Minutes from

The Design Panel Meeting

Wednesday, October 22, 2014 – 4:00 p.m. Rm. M.1.003 Richmond City Hall

2. DP 14-659747 – 2-STOREY COMMERCIAL DEVELOPMENT

APPLICANT:

Lo Studio Architecture

PROPERTY

2671-2991 No. 3 Road (at Bridgeport)

LOCATION:

Applicant's Presentation

Architect Marco Ciriello, Lo Studio Architecture, and Landscape Architect Meredith Mitchell, M2 Landscape Architecture, presented the project and answered queries from the Panel on behalf of the applicant.

Panel Discussion

Comments from the Panel were as follows:

- 1) great presentation; like the detailing of the canopies and edges;
 - Noted.
- 2) corner pop-up elements have a stage set feel; should be treated as threedimensional pop-up boxes as the proposed low-rise buildings are viewed from the street level as well from above; pop-up element on the east side set in on the north; suggest that the actual box be proud all the way around;
 - The Pop-up Elements have been integrated with the decorative treatment of the roof to provide aesthetical pleasing architectural components.
- 3) consider green roofs on the subject buildings as future buildings in the area are expected to be higher;
 - A combination of decorative light weight aggregate, artificial turf and glass material has been designed for the limited areas of the roofs, to provide an attractive feature for general public view from skytrain, while still allowing low maintenance, maintenance access to rooftop mechanical and light weight material considerations.
- 4) parking lane at the rear of the south building could be narrowed to 12 feet to provide opportunities for tree planting and enhance pedestrian circulation in the area;

Landscaping has been introduced wherever possible; feature paving has been introduced to provide wayfinding and light fixtures proposed in numerous locations provide comfortable pedestrian feel.

5) landscaping is well resolved; supportive of (i) the treatment around the tree at the corner, (ii) the proposed planting scheme, and (iii) keeping the narrower sidewalk on the street edge;

Noted.

6) consider a stronger and safer pedestrian connection from the outdoor parking area to the back of the north building;

A pedestrian path has been identified by a change in the paving material; change in material type will be visible for both pedestrians and vehicles; while still allowing the required clearance to skytrain structure and door clearances.

7) consider pavers in lieu of less durable stamped concrete; also consider incorporating decorative paving materials at entry points to provide hierarchy to the paving materials;

Currently we are proposing stamped concrete in truck turnaround areas only, with clean broom finish concrete with cutmarks to match modern building for the sidewalk areas of the site on all four sides. There are no paves noted due to maintenance issues for pavers at truck turnaround.

8) look at long-term maintenance issues for small lawn areas;

Small lawn areas have been removed from the landscape plans.

9) consider green roofs, e.g. strips or accents, in view of the overlook from Canada Line guideway and future higher developments in the area;

See Item 3.

10) challenging site; applicant should give attention to pedestrian access to entries to the back of the buildings;

See Item 6.

scale is suitable and appropriate for the street front; however, look at hierarchy to better differentiate the parts of the buildings;

This item has been reviewed and considered adequate.

12) difficult site; north building is well resolved; form and character is good; *Noted.*

concern on (i) how the proposed development relates to the future developments to the west, and (ii) how the condition underneath the Canada Line structure relates to the south building as viewed from the public realm street level and Canada Line;

No indication at this time on how the future developments will be proposed.

14) front face of the buildings are treated well with a lot of transparencies; however, the back façade of the south building requires more detail as people will be arriving from the back, e.g. emphasize the rear entry and break down the building's linear form to provide more visual interest from Canada Line

and future developments to the west;

A canopy has been introduced, which is in line with the treatment of the back façade of the North Building.

site is significant; corner location of the proposed development is prominent and highly visible; consider further treatment to the southeast corner of the south building to provide a gateway feel;

The building is richly articulated, considering its massing and scale. The southeast corner has a higher canopy that highlights this prominent location.

agree with previous comment regarding concern on pedestrian access to the buildings from parking area;

See Item 7.

17) consider further treatment to the west elevation of the proposed buildings to provide more visual interest to incoming customers;

See Item 14.

18) access to elevator lobbies should be visible from the parking area at the rear of the proposed buildings; look at safe pedestrian passage from parking in view of vehicular traffic in the area;

The access to the elevator has been highlighted with the introduction of a canopy.

19) parking lot underneath the Canada Line structure is currently a vast hard surface; consider further landscaping, e.g. introduce planters to provide interest to future neighbouring developments;

The parking requirements do not allow for landscaping. Furthermore Trans Link has commented on minimizing the landscaping under the line.

20) consider further treatment to the roofs, e.g. add patterns;

See Item 3.

21) agree with comment to consider three-dimensional box pop-ups;

See Item 2.

22) consider public art in the proposed project;

Contribution will be provided for the art program

23) agree with comment to consider green roofs;

See Item 3.

concern on the proximity of the office spaces on the second floor to the Canada Line guideway; hope that an acoustic study has been done by the applicant;

The spaces closer to the Canada line are service spaces.

25) look at energy conservation, e.g. energy recovery as CRUs are expected to consume large amounts of energy; would like to see the project having a different approach to energy conservation than a conventional commercial building; and

Attention will be given to this aspect as the project will move forward.

26) tricky site; appreciate the approach to provide a public edge to the streetscape; agree with the comment to consider augmenting the small plaza space at the southeast corner to provide an indoor-outdoor connection, e.g. a café or retail unit.

The green space at the corner has been treated as an amenity.

Panel Decision

It was moved and seconded

That DP 14-659747 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Panel.

CARRIED



2

Credit 6

Green Power

LEED Project Checklist Prepared By:

C. Lorenzen & Associates Ltd.



LEED & Energy Consultants

Preliminary Project Checklist Commercial Project - 2671 #3 Road, Richmond

> CaGBC Project # July 23, 2014

> > 2

No Yes 26 30 110 Possible Points 52 **Project Totals** (pre-certification estimates) Certified 40-49 points Silver 50-59 points Gold 60-79 points Platinum 80 points and above Yes No 13 2 Sustainable Sites 11 28 Points Prereq 1 **Construction Activity Pollution Prevention** Required Credit 1 1 Site Selection 3 2 Credit 2 **Development Density and Community Connectivity** 3, 5 1 Credit 3 **Brownfield Redevelopment** 1 3 3 Credit 4.1 Alternative Transportation: Public Transportation Access 3, 6 2 Credit 4.2 Alternative Transportation: Bicycle Storage & Changing Rooms 2 1 Credit 4.3 Alternative Transportation: Low-Emitting & Fuel-Efficient Vehicles 3 1 Credit 4.4 Alternative Transportation: Parking Capacity 1 2 1 Credit 5.1 Site Development: Protect and Restore habitat 1 Credit 5.2 Site Development: Maximize Open Space 1 **Stormwater Design: Quantity Control** 1 Credit 6.2 Stormwater Design: Quality Control 1 Credit 7.1 Heat Island Effect: Non-Roof 1 Credit 7.2 Heat Island Effect: Roof Credit 8 **Light Pollution Reduction** 1 1 Credit 9 **Tenant Design and Construction Guidelines** Yes 2 Νo 2 Water Efficiency 4 4 10 Points \checkmark Prereq 1 Water Use Reduction Required 2 1 1 Credit 1 Water Efficient Landscaping 2, 4 2 Credit 2 **Innovative Wastewater Technologies** 2 1 Credit 3 2 1 Water Use Reduction 2 - 4 Yes ? No 14 5 Energy & Atmosphere 18 37 Points **Fundamental Commissioning of Building Energy Systems** Prereq 1 Required **Minimum Energy Performance** Prereq 2 Required Prereq 3 **Fundamental Refrigerant Management** Required 4 **Optimize Energy Performance** 8 9 Credit 1 3 - 214 Credit 2 On-Site Renewable Energy 2, 4 2 Credit 3 **Enhanced Commissioning** 2 2 Credit 4 **Enhanced Refrigerant Management** 2 3 Credit 5.1 Measurement and Verification: Base Building 3 2 1 Credit 5.2 Measurement and Verification: Tenant Submetering 3

Oredit o.	2 Daylight and Views. Views	'
Yes ? No		
6 0 0 Innov	ation in Design	6 Points
Credit 1.	Innovation in Design - EAc6 Green Power	1
1 Credit 1.	Innovation in Design - SSc4.1 Public Transportation Access	1
1 Credit 1.	Innovation in Design - MRc5 Regional Materials	1
1 Credit 1.	Innovation in Design- Providing an Online Ownwer's Manual	1
1 Credit 1.	Innovation in Design - Very low mercury flourescent fixtures	1
1 Credit 2	LEED® Accredited Professional	· 1
Yes ? No		
4 0 0 Regio	onal Priority	4 Points
1 Credit 1	Durable Building	1
1 Credit 2.	Regional Priority Credit - SSc2 Development Density & Community Connectivity	1
1 Credit 2.	Regional Priority Credit - Water Use Reduction (greater than 35%)	1
1 Credit 2.	Regional Priority Credit - Construction Waste Management (greater than 75%)	1



Development Permit Considerations

Address: 2671-2991 No. 3 Road

File No.: DP 14-659747

Prior to approval of the Development Permit, the developer is required to complete the following:

- 1. <u>Rezoning (RZ 11-566630)</u>: Final adoption of Zoning Bylawylaw 9042, including the developer's compliance with the terms of the Rezoning Considerations, as approved by Council.
- 2. <u>Landscape Security:</u> Receipt of a Letter-of-Credit for landscaping in the amount of \$243,688.89 (\$135,970.53 for the Building A parcel and \$107,718.36 for the Building B parcel).
- 3. <u>Servicing Agreement</u>: In addition to the Servicing Agreement requirements established as considerations of rezoning, the following improvements will also be required:
 - a) provision of landscaping, lighting and traffic bollards in the Bridgeport Road boulevard, adjacent to the truck turnaround, as required to enhance visual screening and pedestrian and vehicular safety in this area.

Prior to approval of the Building Permit*, the developer must complete the following requirements:

- 1. <u>Sustainability:</u> Incorporation of sustainability measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
- 2. <u>Fire Flow:</u> Submission of fire flow calculations signed and sealed by a professional engineer based on the Fire Underwriter Survey to confirm that there is adequate available water flow.
- 3. <u>Fire Safety Plan Construction</u>: Submission of a Fire Safety Plan applicable for the duration of construction to the satisfaction of the Richmond Building & Fire Departments.
- 4. <u>Construction Parking and Traffic Management:</u> Submission of a Construction Parking and Traffic Management Plan to the Transportation Division, including: parking for services, deliveries, workers, loading, application for any lane closures, and construction traffic controls as per Traffic Control Manual for works on Roadways (by MOTI) and MMCD Traffic Regulation Section 01570.
- 5. <u>Construction Hoarding:</u> Obtain a Building Permit (BP) for any construction hoarding. If required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required.

Note:

- * This requires a separate application.
- Where the Director of Development deems it appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.
 - All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial Wildlife Act and Federal Migratory Birds Convention Act, which contains prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed	Date	



Development Permit

No. DP 14-659747

To the Holder:

DAVA DEVELOPMENTS LTD.

Property Address:

2671/2711/2811/2831/2851/2911/2931/2951/2971/2991

No. 3 ROAD

Holder Address:

C/O MR. DAVID CHUNG 228-2680 SHELL ROAD RICHMOND, BC V6X 4C9

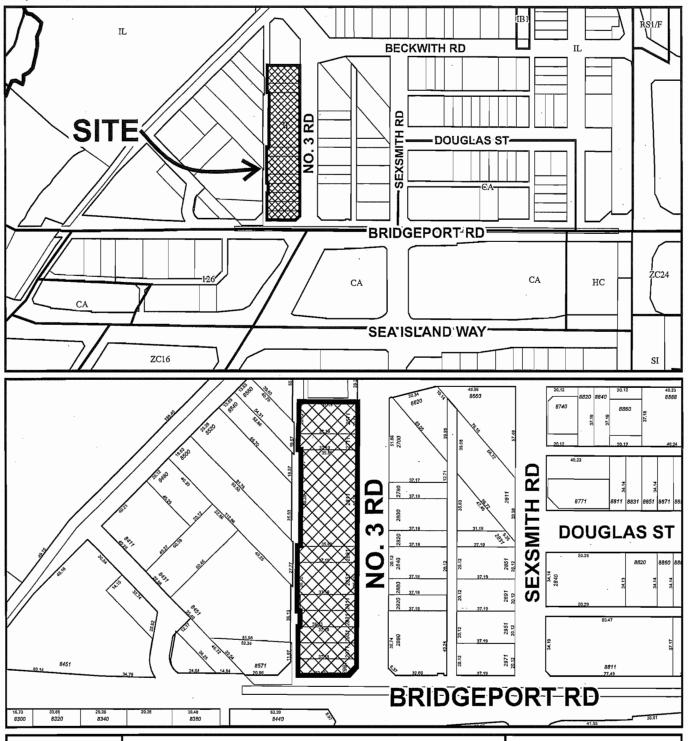
- 1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
- 2. This Development Permit applies to and only to those lands shown crosshatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - a) reduce the required manoeuvring aisle width from 7.5 m to 6.0 m (Section 7.5.5); and
 - b) reduce the required setback of parking from property lines abutting a road from 3.0 m to 2.7 m and reduce the required setback of parking from other property lines from 1.5 m to 0.0 m (Section 7.5.17).
- 4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plan # 1 to Plan # 14 attached hereto.
- 5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
- 6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$243,688.89 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. A condition of the posting of the security is that, should the Holder fail to carry out the development hereby authorized according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this Permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.

Development Permit

No. DP 14-659747

			NO. DI	14-03374
To the Holder:	DAVA DEVELOPME	NTS LTD		
Property Address:	2671/2711/2811/283 No. 3 ROAD	1/2851/2911/2931/2951	/2971/299)1
Holder Address:	C/O MR. DAVID CHI 228-2680 SHELL RO RICHMOND, BC V6	DAD		
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8. The land described herei conditions and provision Permit which shall form	s of this Permit and an			
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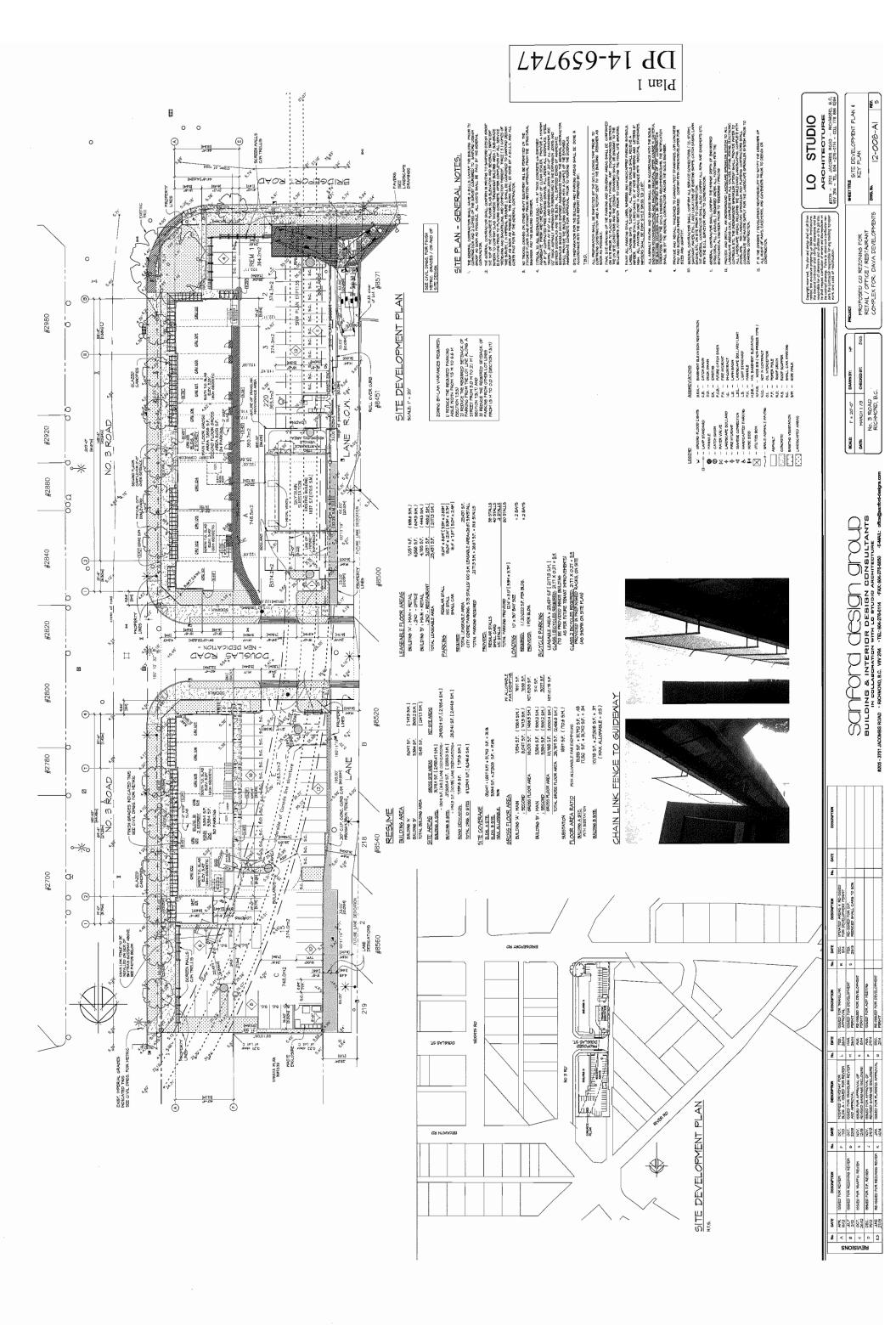


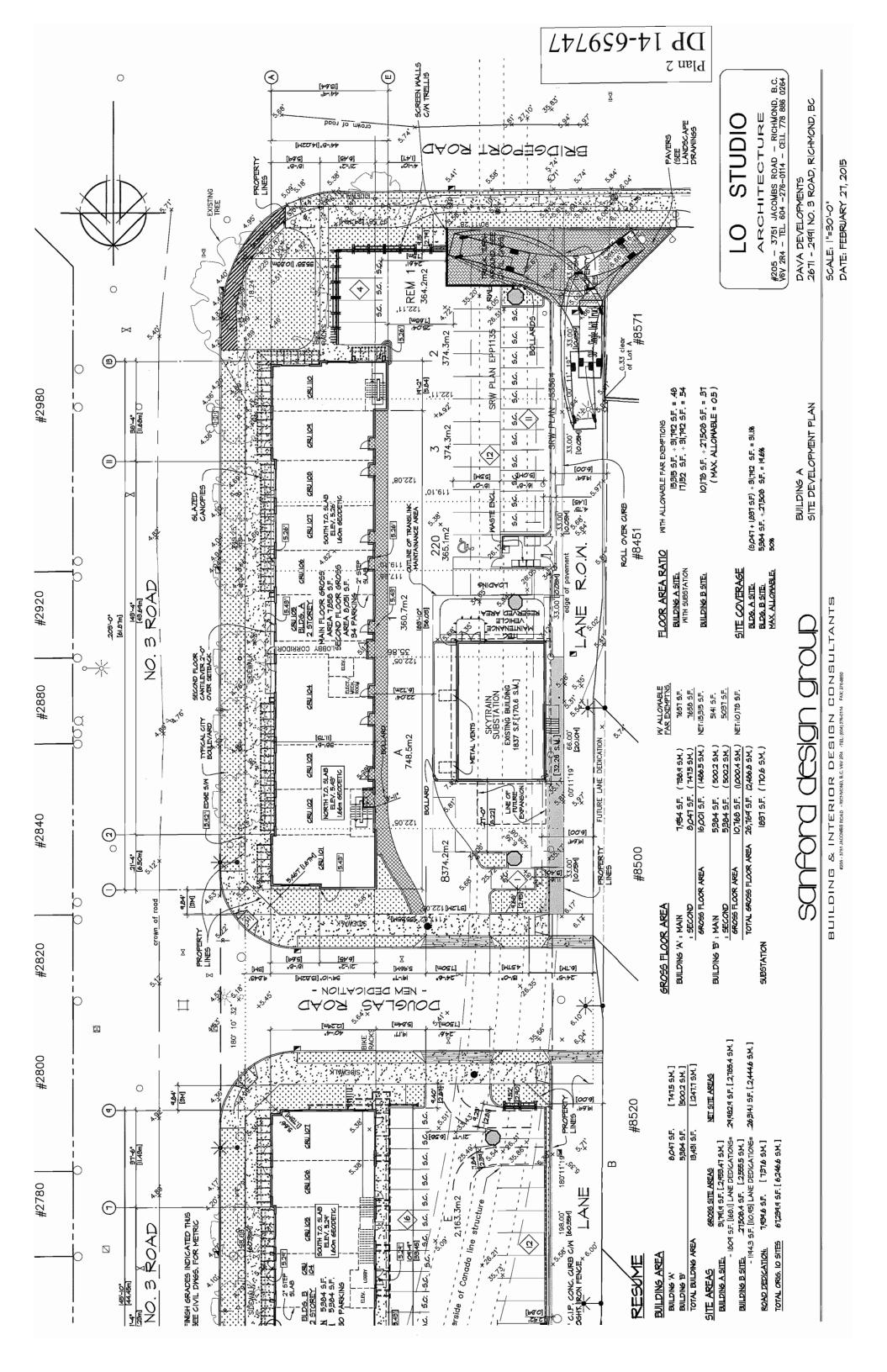
DP 14-659747 SCHEDULE "A"

Original Date: 04/02/14

Revision Date:

Note: Dimensions are in METRES





Db 14-659747 Plan 3 ARCHITECTURE \$205 - 3761 JACOMBS RAD - RICHÁNN, B.C. (87 JR4 - TL. 604 -276-0114 - CEL 778 886 0264 LO STUDIO PROPOSED GD REZONING FOR: RETAIL / OFFICE COMPLEX FOR: DAVA DEVELOPMENTS Uges - OLAZED CANOPT ABOVE FOR 2nd PLOOR ACCESS 26TI - 249I No. 3 ROAD
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107 s. 1-0"

108 s. 1-0"

109 s. 1 SCALE; 1/6' = 1'-O" DRAWNBY; CRU 110 <u>(2</u>) <u>a</u> (Z) ELDG. B NORTH FAR NET AREA: 1695 SF. FAR NET AREA: 1659 SF. TOTAL NET AREA: 18,340 SF. CRU 104 **-** \bigcirc ✐ 5UTE 204 635 S.F. TYPICAL GLAZED CANOPIES ABOVE CRU 108 656 S.F. **@** 0 <u>@</u> 0 CRU 107 652 5.F. 5UITE 208 (D) **(P)** <u>-(b)</u> 6 A- SECTION March March A melce. SUITE 201 (a) (a) (a) (a))((1) CARTAN MALL SLAZINS CRU ICE 664 5.F. NORTH TOP OF SLAB FL. SAB (1.66 H) GEODETIC EL, 1001-2" CONSTR STEP BLAB AT CON 9UTE 206 641 S.F. 203'-0' 6:81 (-) \odot 149-4 <u>@</u> @ ROOF CANOPY AND KYNEE BRANCES ABOVE ELEX. Š SUITE 205 814 S.F. ELECTRICAL ROOM (n) (r) (n) (D) フ<u>©</u> 01 CRU 104 1048 SF. 5UITE 204 361 SE. 4 4 4 4 9UTE 209 672 57. 2 (m) (b) (e) (b) 423 a.r. SSM 65. Det. RE-1998D FOR DEVE. THE CALLED 9 (J) 9 (D) g b SUITE 201 888 S.F. CRU IOI \bigcirc \bigcirc Na DATE

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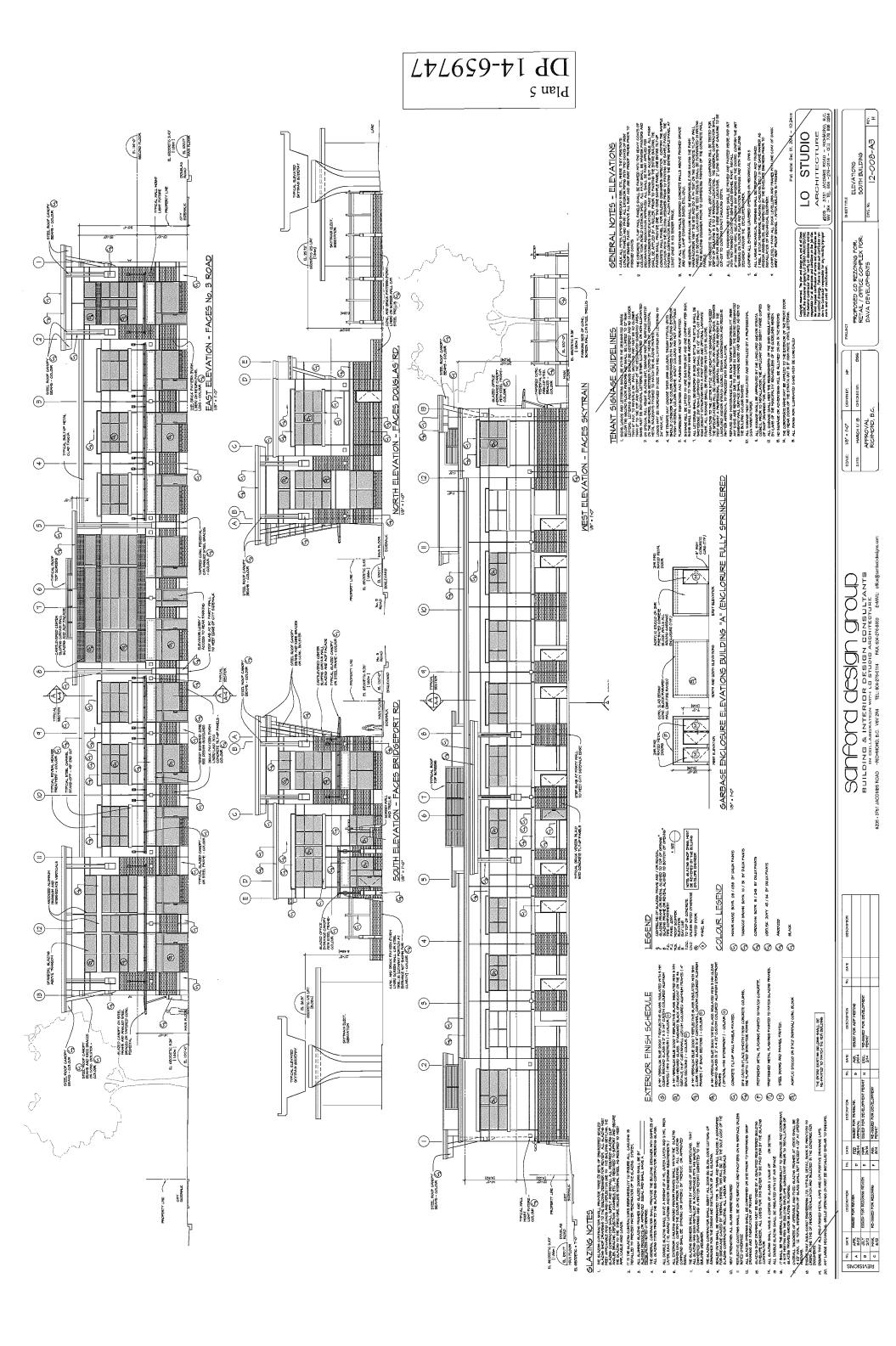
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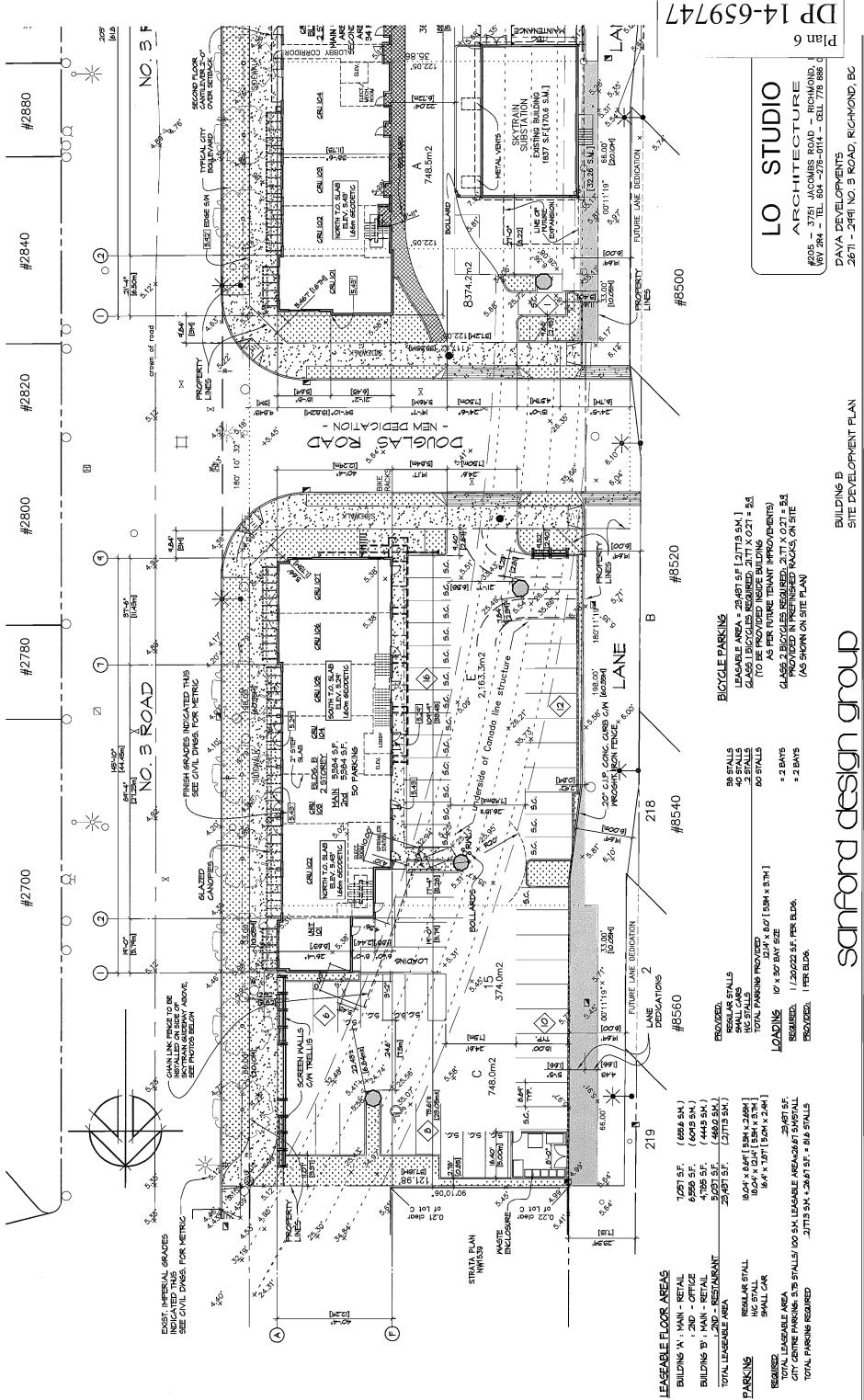
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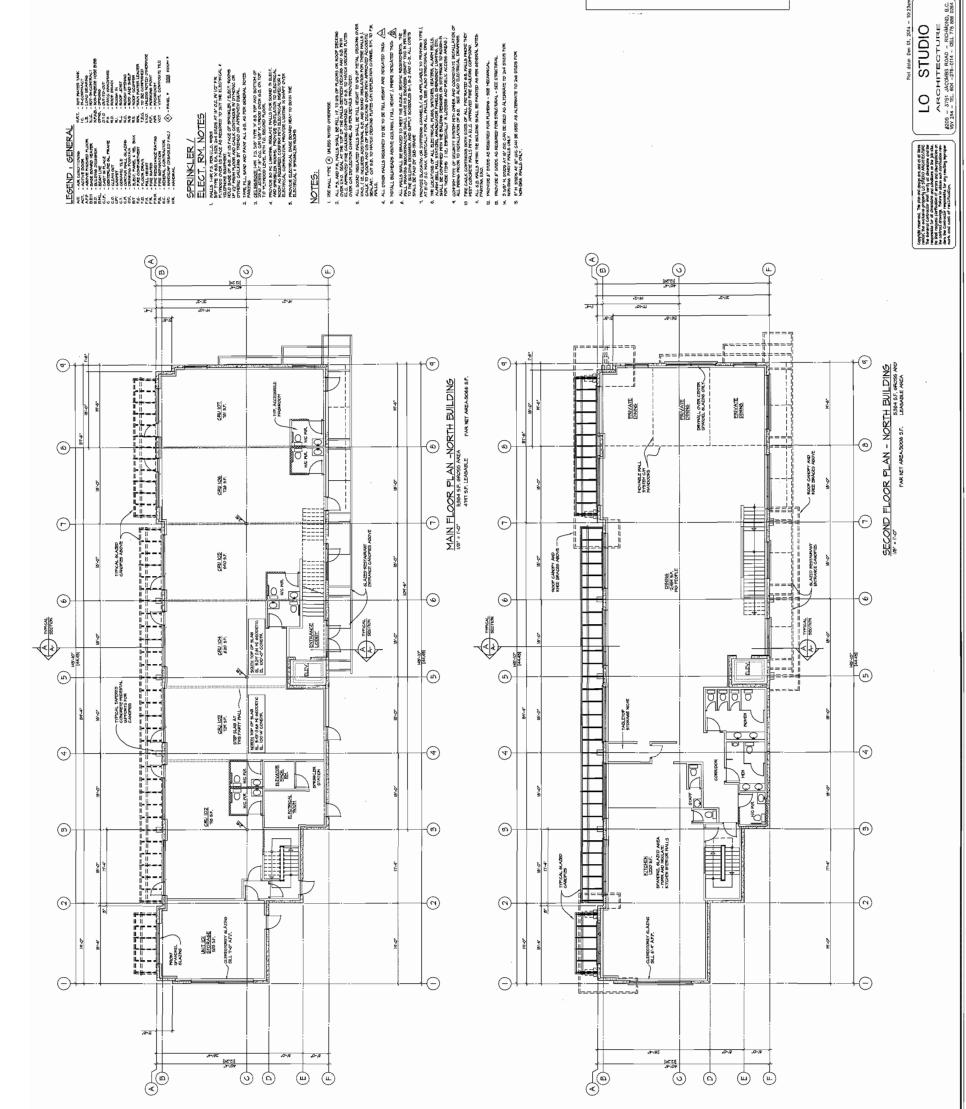




BUILDING & INTERIOR DESIGN CONSULTANTS

SCALE: I"=30'-0" DATE: FEBRUARY 21, 2015

Lt-629-t1 da Plan 7



BUILDING & INTERIOR DESIGN CONSCIUR ** COLABORATION WITH TO STUDIO ARCHITECTURE *** PART TO STUDIO ACCUPANCE FOR THE PROPERS FAMIL FINGER sanford design group

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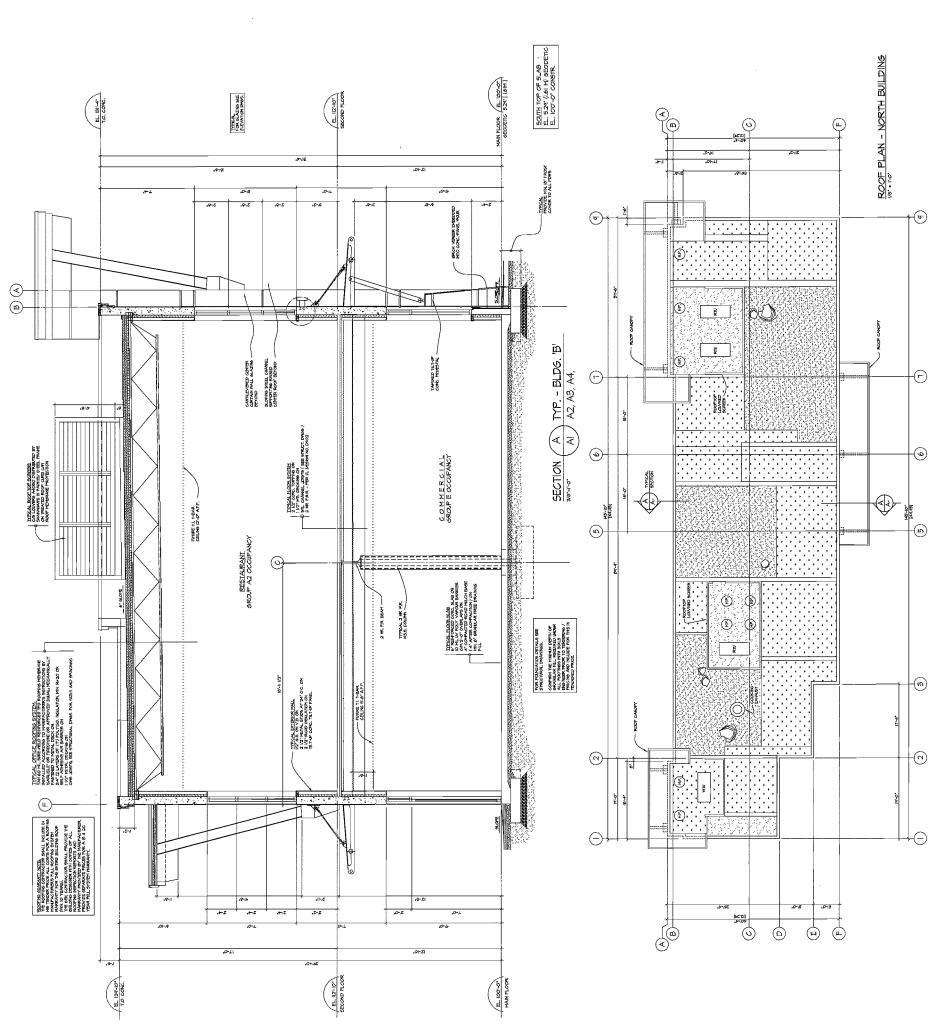
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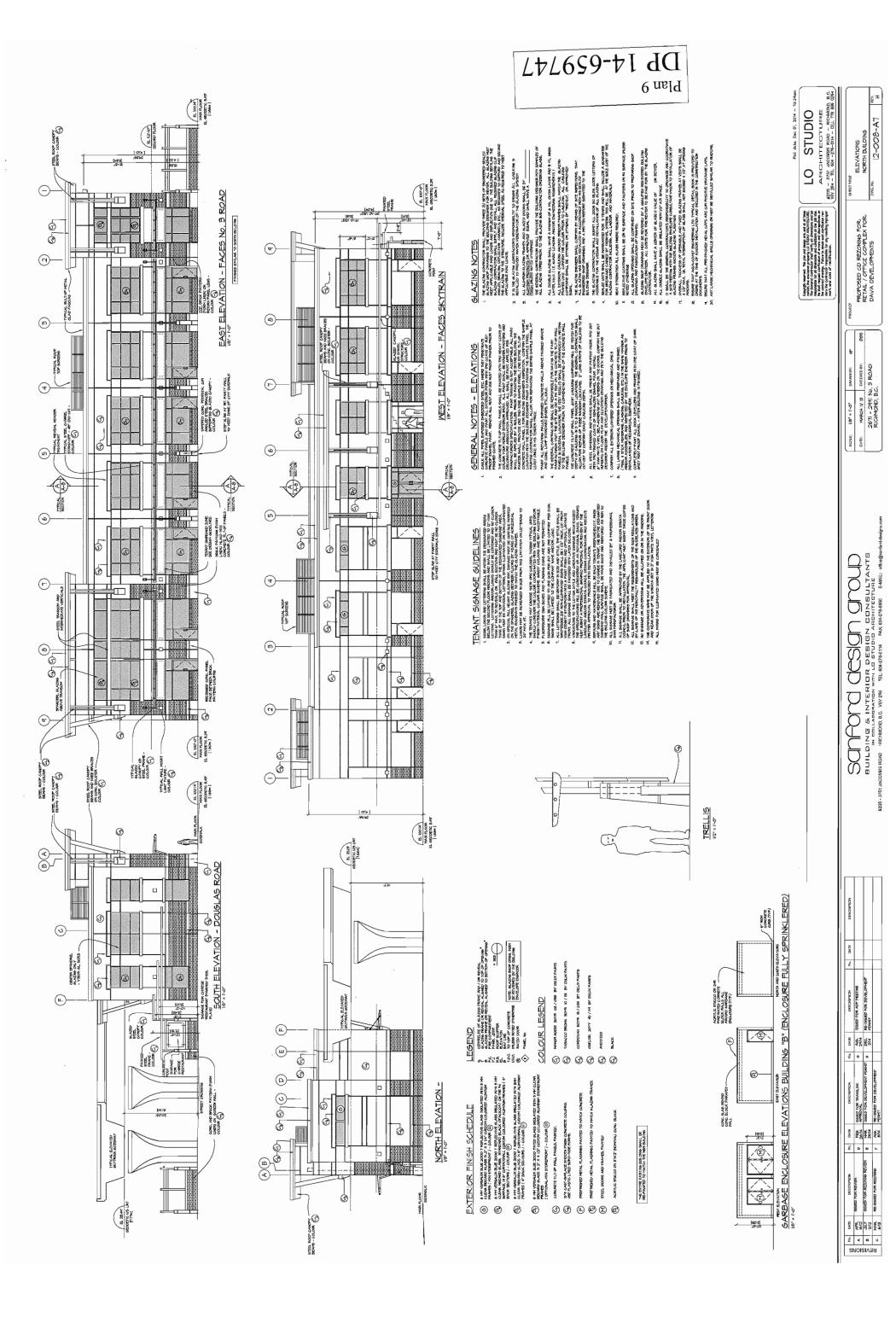
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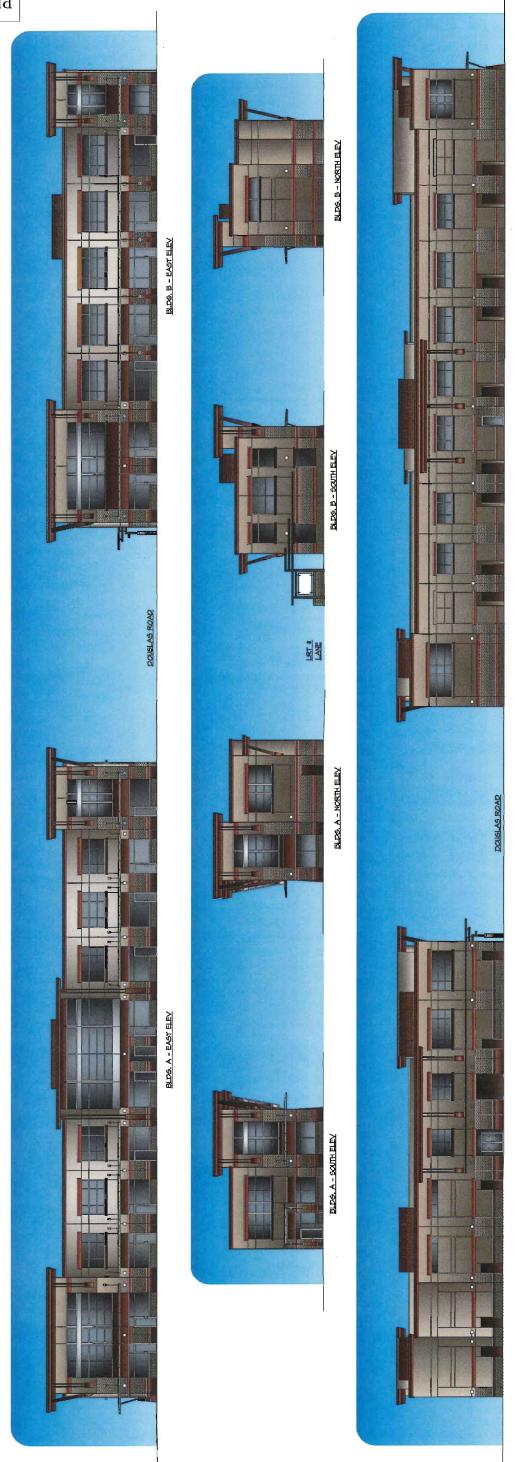
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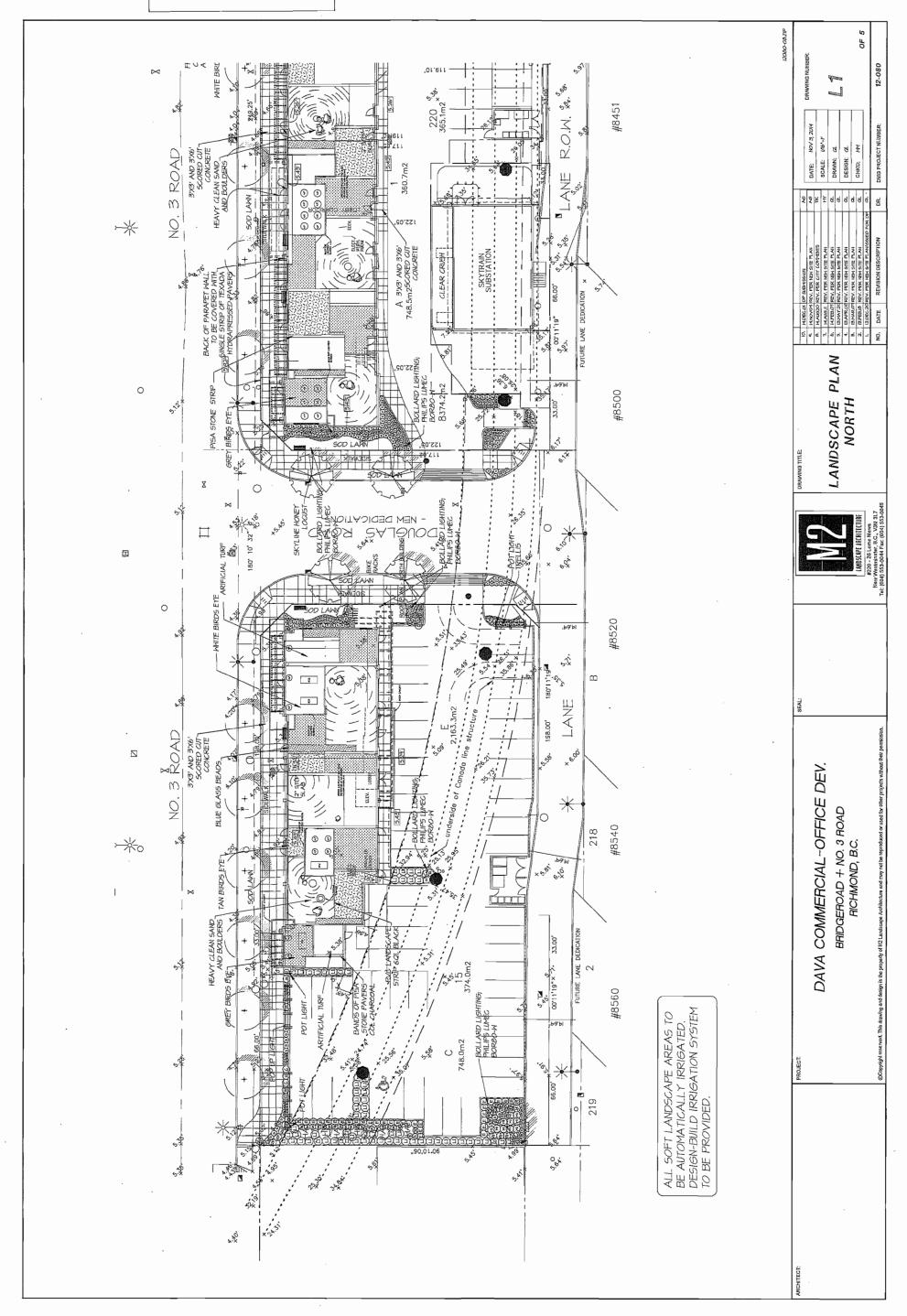
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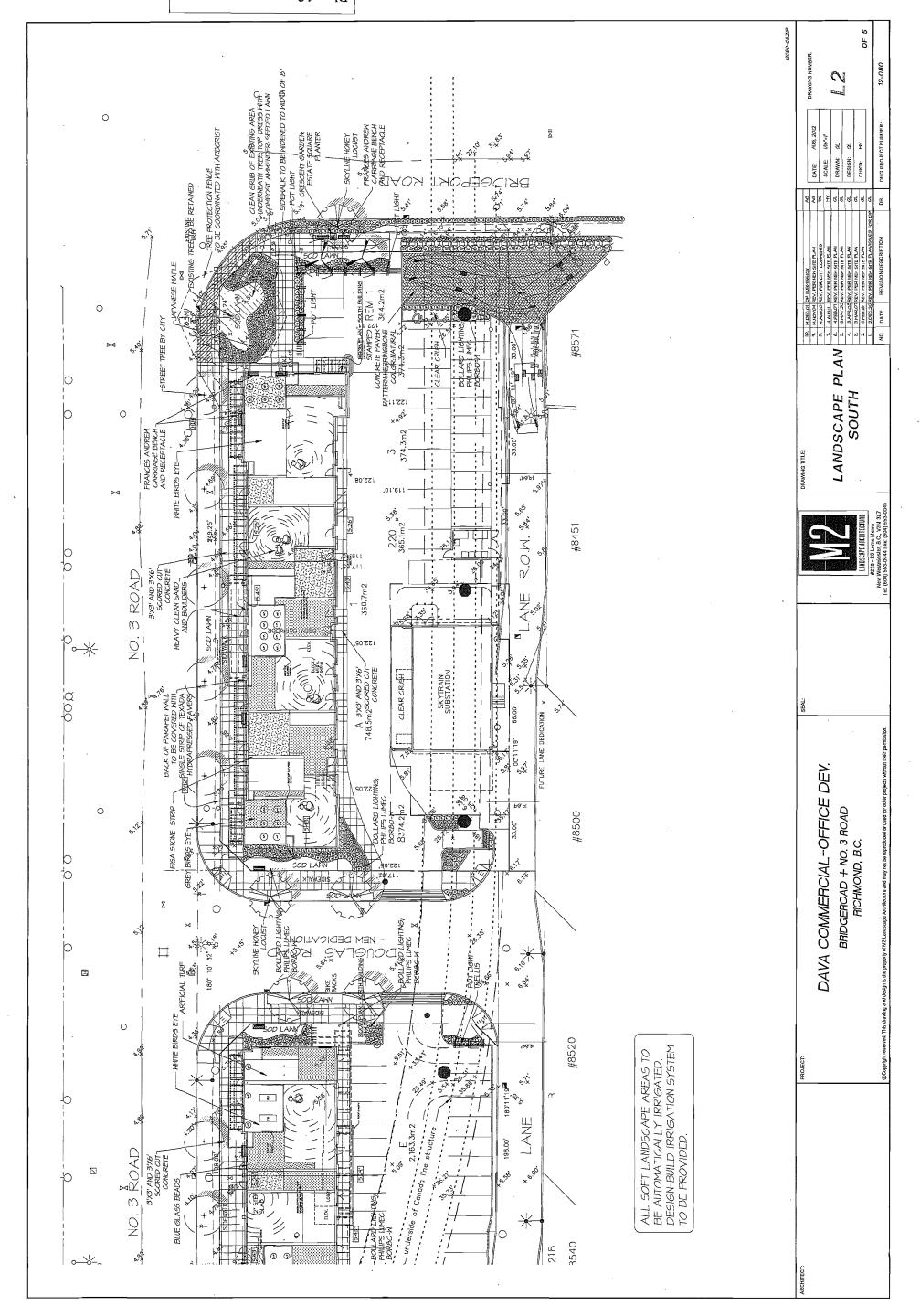
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LANDSCAPE DETAILS

DAVA COMMERCIAL-OFFICE DEV.

BRIDGEROAD + NO. 3 ROAD RICHMOND, B.C.

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(4) ENTRY SIGNAGE

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LANDSCAPE SPECIFICATIONS

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ARCHITECT:

DAVA COMMERCIAL-OFFICE DEV.

BRIDGEROAD + NO. 3 ROAD RICHMOND, B.C.



Report to Development Permit Panel

To:

Development Permit Panel

Date:

February 17, 2015

From:

Wayne Craig

File:

DP 13-644888

Re:

Director of Development

Application by Balandra Development Inc. for a Development Permit at 8600 and

8620 No. 2 Road

Staff Recommendation

That a Development Permit be issued which would:

- 1. Permit the construction of nine (9) townhouse units at 8600 and 8620 No. 2 Road on a site zoned "Low Density Townhouses (RTL4)"; and
- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - a) reduce the minimum lot width on major arterial road from 50.0 m to 43.29 m;
 - b) reduce the front yard setback from 6.0 m to 5.4 m; and
 - c) permit seven (7) small car parking spaces.

Wayne Craig

Director of Development

WC:el

Att.

Staff Report

Origin

Balandra Development Inc. has applied to the City of Richmond for permission to develop nine (9) townhouse units at 8600 and 8620 No. 2 Road. The site is being rezoned from "Single Detached (RS1/E)" to "Low Density Townhouses (RTL4)" for this project under Bylaw 9146 (RZ 13-644887), which received Third Reading following the Public Hearing on September 2, 2014. The site is currently vacant.

A Servicing Agreement for frontage improvements along No. 2 Road is required prior to issuance of a Building Permit for the site. Works include, but are not limited to, removing the existing sidewalk behind the curb and gutter, and installing a new 1.5 m concrete sidewalk at the property line with a 1.5 m grass and treed boulevard up to the curb.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the North: Single-family homes on large lots zoned "Single Detached (RS1/E)" and under Land Use Contract (LUC141); and then two (2) newer homes on compact lots with lane access, zoned "Compact Single Detached (RC1)", at the corner of

Danube Road and No. 2 Road.

To the South: Single-family homes on large lots zoned "Single Detached (RS1/E)"; and then a

10 unit two-storey townhouse development on a lot zoned "Low Density Townhouses (RTL1)" at the corner of Francis Road and No. 2 Road.

To the East: Single-family homes on smaller lots under Land Use Contract (LUC141), fronting

Delaware Road.

To the West: Across No. 2 Road, a mix of single-family homes on larger and smaller lots zoned

"Single Detached (RS1/E)" and a duplex on a lot zoned "Two-Unit Dwellings (RD2)" with accesses to No. 2 Road as well as newly developed dwellings with

lane accesses on lots zoned "Coach Houses (RCH)".

Rezoning and Public Hearing Results

The Public Hearing for the rezoning of this site was held on September 2, 2014. The following concerns were expressed during the Public Hearing. The response to the concern is provided in *italics*.

1. Increased Traffic

Transportation staff advised that the proposed nine (9) unit townhouse development will result in a manageable increase in traffic volume over the existing two single family houses. It is anticipated this increase will result in five (5) additional vehicles per hour during the morning and afternoon peak periods, or about one (1) vehicle every 12 minutes. This marginal increase is expected to have minimal impact on the surrounding road system and can be accommodated within the capacity and geometry of No. 2 Road.

2. Parking

The proposed redevelopment meets the off-street parking requirements in the Zoning bylaw with two (2) parking spaces for each unit and two (2) on-site visitor parking spaces.

3. Building Height

Building height will be kept to two (2) storeys along all property lines. The height of the duplex units along the rear (east) property line will be approximately 8.45 m (27.72 ft.) measured to the average finished site grade, which is below the maximum building height allowed in single-family zones (i.e. 9.0 m or 29.53 ft.).

4. Loss of Sunlight and Shadowing

Shadow study provided by the applicant shows no shadowing upon neighbouring properties.

5. Loss of Privacy

The second floor of the rear units will be set back 6.0 m from the rear (east) property line; which is similar to the setback required from single-family homes.

6. Non Compliance with Zoning Requirements

a. Minimum Lot Width

The frontage of the development site is 43.29 m, which does not comply with the minimum frontage requirement of 50.0 m on a major arterial road. In support to the application, the applicant has developed a concept plan showing how the rest of the block could be redeveloped (on file). The subject proposal will not restrict future development of lands to the north or south of the subject site. A Public Rights of Passage (PROP) Statutory Right-of-Way (SRW) on the subject site is secured as part of the Rezoning to provide vehicle access to future developments to the north and south along No. 2 Road.

b. Minimum Front Yard Setback

A front yard setback variance from 6.0 m to 5.4 m is being requested. The Arterial Road Guidelines for Townhouses in the OCP support reduced front yard setback where a larger rear yard setback is deemed necessary, provided that there is an appropriate interface with neighbouring properties. The proposed rear yard setbacks at 4.5 m to ground floor and 6.0 m to the second floor exceed the zoning requirement of 3.0 m under the "Low Density Townhouses (RTL4)" zone.

The buildings on the adjacent properties to the north and south are significantly set back from No. 2 Road. A number of new trees are proposed within the side yard setbacks to screen the view of the proposed buildings from the neighbours' view, and mitigate potential impacts of this variance.

7. High Density

The proposed density at 0.60 FAR is within the density range for townhouse developments under the Arterial Road Policy in the OCP. The streetscape and rhythm created by the proposal (i.e., duplex and triplex along No. 2 Road) will respect existing developments along both sides of No. 2 Road.

8. Potential Flooding on Neighbouring Properties

Perimeter drainage will be installed as part of the Building Permit to ensure the proposed grade change does not adversely impact the surrounding sites.

9. Dark Building Colour on the East Elevation

To address neighbours' concerns related to the proposed dark grey colour on the east elevation, a lighter shade of grey has been selected. The new colour will maintain a visual contrast with the white trim detailing.

10. Natural Screening along the East Property Line

The developer has agreed to the neighbours' request to retain the existing hedge along the southern half of the east property line and provide a minimum 2.75 m (9 ft.) tall hedge along the north half of the east property line to address overlook concerns and provide natural screening between the townhouse development and the existing single-family homes to the east. This arrangement has been incorporated into the landscaping plan; 3.0 m tall Eddie Yew is proposed along the north half of the east property line.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan and is generally in compliance with the "Low Density Townhouses (RTL4)" zone except for the zoning variances noted below.

Zoning Compliance/Variances (staff comments in bold)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Reduce the minimum lot width on major arterial road from 50.0 m to 43.29 m.
 - (The proposed variance will not restrict future development of lands to the north or south of the subject site. A Public Rights of Passage (PROP) Statutory Right-of-Way (SRW) on the subject site has been secured at Rezoning to provide vehicle access to future developments to the north and south along No. 2 Road.)
- 2) Reduce the front yard setback from 6.0 m to 5.4 m.
 - (The Arterial Road Guidelines for Townhouses in the OCP support reduced front yard setback where a larger rear yard setback is deemed necessary, provided that there is an appropriate interface with neighbouring properties. The proposed rear yard setbacks at 4.5 m to ground floor and 6.0 m to the second floor exceed the zoning requirement of 3.0 m under the "Low Density Townhouses (RTL4)" zone. The proposed rear setbacks in tandem with proposed landscaping should address any interface concerns.)
- 3) Permit seven (7) small car parking spaces.

(The Zoning Bylaw permits small car parking stalls only when more than 31 parking stalls are proposed on site. The proposed 9-unit project will provide 18 residential and two (2) visitor parking spaces on site. Transportation staff support the proposed variances to allow one (1) small car stall in each of the side-by-side double car garages.)

Advisory Design Panel Comments

The Advisory Design Panel supported the design of the project and the Design Panel's suggested design changes have been incorporated into the proposal. A copy of the relevant excerpt from the Advisory Design Panel Minutes from Wednesday, November 19, 2014 is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in 'bold italics'.

Analysis

Conditions of Adjacency

- The proposed height, siting and orientation of the buildings generally address the massing of the surrounding residential developments.
- Two (2) 2½ storey buildings are proposed along No. 2 Road. The end units adjacent to the north and south side yards have no habitable space in the third level (attic) and are considered 2 storeys, which meet the development guidelines under the Arterial Road Policy that building height is limited to 2 storeys within 7.5 m of the side yard interface with single-family housing and other townhouse development along arterial road.
- Two-storey duplex units are proposed along the rear property line to minimize privacy and overlook concerns. The proposed roof line has been minimized to diminish the impact of shadowing and scale to the neighbours. There is no half storey associated with these units.

- The proposed rear yard setbacks of 4.5 m on the ground floor and 6.0 m on the second floor meet the minimum rear yard setback specified in the "Low Density Townhouses (RTL4)" zone and in the Arterial Road Guidelines for Townhouses in the OCP.
- The existing site grade along the rear property line will be maintained in order to retain and protect two (2) rows of existing cedar hedge at the southeast corner of the site. In addition to the existing cedar hedge, a combination of Trompenburg Japanese Maple tree, Serbian Spruce trees, Golden Beech trees, and Eddie Yew hedge will be planted in the side and rear yards to increase privacy in the yard spaces of the adjacent single-family homes.
- Perimeter drainage will be required as part of the Building Permit to ensure storm water is managed and addressed through the development and will not impact the neighbouring properties.
- A 1.8 m tall wood fence is also proposed along the rear property line to protect the privacy of the neighbouring single-family homes.

Urban Design and Site Planning

- The site layout includes one (3) triplex and three (6) duplex units. A triplex and a duplex are proposed along No. 2 Road to minimize repetitive building form.
- The internal road layout is a T-shape with future connections to the neighbouring properties to the north and south, secured by SRW.
- All units have two (2) vehicle parking spaces; seven (7) units have a side-by-side double car garage; two (2) units have a single car garage and a surface parking space in the side yard.
- A total of two (2) visitor parking spaces are proposed, which meet the minimum bylaw requirement. No accessible visitor parking space is required. Both residential and visitor bicycle parking are provided and are also in compliance with the zoning bylaw requirements.
- Outdoor amenity space is proposed opposite the site entry for maximum casual surveillance opportunity and sun exposure. The size and location of the outdoor amenity space is appropriate in providing open landscape and amenity space convenient to all of the units.
- A covered mailbox kiosk and short-term bicycle parking spaces are provided at the southwest corner of the internal drive aisle intersection.
- The required garbage, recycling and organic waste storage enclosures have been incorporated into the design of Building No. 1 (at the north side of the entry driveway) to minimize their visual impact.

Architectural Form and Character

- The exterior form and materials are reminiscent of the architecture of heritage row houses. The building design includes several elements common to historic styles such as Georgian, Adam and Greek Revival including pedimented dormers, quoins, dentils and mouldings, entablatures over the entries, and windows with lintel.
- The proposed buildings have consistent setbacks, height, and roofline along No. 2 Road. The proposed urban character is cohesive with intricate detail that reinforces the row house form.
- The impact of blank garage doors has been mitigated with panel patterned doors, transom windows, unit entrances, and planting islands along the drive aisle.

• The proposed building materials (asphalt roof shingles, horizontal lap siding, hardi panel, wood fascia/trim/dentil/bracket, and brick) are generally consistent with the Official Community Plan (OCP) Guidelines and compatible with the existing single-family character of the neighbourhood.

Landscape Design and Open Space Design

- Tree preservation was reviewed at rezoning stage and all 16 bylaw-sized trees noted on-site were identified for removal due to general poor condition, proposed grade changes, and conflict with the building envelope; 32 replacement trees are required.
- The applicant is proposing to plant 23 replacement trees on-site, including 15 conifers and 8 deciduous trees. A voluntary contribution of \$4,500 to the City's Tree Compensation Fund in lieu of planting the remaining nine (9) replacement trees has been secured at Rezoning.
- Hedges, shrubs and ground covers, as well as perennials and grasses have been selected to ensure the landscape treatment remains interesting throughout the year.
- Two (2) cedar hedge rows located along the east and south property line are identified for retention. Tree protection fencing is required to be installed as per the Arborist Report recommendations prior to any construction activities (including demolition) occurring onsite. A Tree Survival Security will be required as part of the Landscape Letter of Credit at Development Permit stage to ensure that these hedges will be protected. No Landscape Letter of Credit will be returned until the post-construction assessment report, prepared by the Arborist, confirming the protected hedge rows survived the construction, is reviewed by staff.
- A pedestrian-oriented streetscape along No. 2 Road is proposed with a landscaped edge treatment, low metal fencing with brick columns, and gates to individual townhouse unit front doors.
- Each unit will have a private yard with landscaping and lawn area.
- The amenity space consists of a low climbing wall, a sandbox, and a small lawn area.
- The entry driveway and internal drive aisle will be treated with permeable pavers for better water infiltration and variety in paving surfaces. Surface parking stalls will be treated with grass pavers (GrassPave2) for additional store water mitigation and added green space. Feature paving will be used to identify and enhance the pedestrian route from the site entrance to the internal units.
- In order to ensure that the proposed landscaping works are completed, the applicant is required to provide a landscape security of \$149,090.00 in association with the Development Permit.
- Indoor amenity space is not proposed on-site. A \$9,000 cash-in-lieu contribution has been secured as a condition of rezoning approval, consistent with the OCP.

Crime Prevention Through Environmental Design

- The architect advised that the following CPTED design/features are incorporated into the proposal:
 - Site lighting and clear site lines provide unobstructed views of surrounding area.

- Plantings near residential entries are low to maximize views and casual surveillance opportunities.
- "Good neighbour" fencing between rear gardens are 3 ft. high to encourage interaction between neighbours and foster a sense of community.
- All entrances are visible and overlooked by pedestrians or by neighbour's windows.
- The landscape design avoids hiding places that would conceal criminals.
- Security lights including wall mounted lighting for drive aisles and lighting at entries to provide adequate outdoor security illumination.

Sustainability

- A Restrictive Covenant specifying that all units are to be built and maintained to the EnerGuide rating of 82 or higher, and that all units are to be solar-hot-water-ready, has been secured at Rezoning. A Certified Energy Advisor has confirmed that the proposed townhouse units will be designed to achieve an EnerGuide rating of 82. The report prepared by the Energy Advisor is on file and will be utilized through the Building Permit review process to ensure these measures are incorporated in the permit drawings.
- The developer also advises that the following sustainability features will be incorporated into the development:
 - use of drought tolerant or native dominant plantings;
 - use of locally available materials where possible;
 - use of permeable surfaces wherever possible;
 - use of exterior building materials require low maintenance and provide highly durable surfaces (i.e., hardie siding and brick);
 - use of low-emitting paint for suite interiors;
 - use of climate-based automatic irrigation controls to minimize the use of water on site;
 - use of Energy Star appliances in all units; and
 - use of at least 6 CEL light bulbs in each unit.

Accessible Housing

- The proposed development includes one (1) convertible unit that is designed with the potential to be easily renovated to accommodate a future resident in a wheelchair. The potential conversion of this unit will require installation of a chair lift (where the staircase has been dimensioned to accommodate this in unit 9D3) in the future, if desired.
- All of the proposed units incorporate aging in place features to accommodate mobility constraints associated with aging. These features include:
 - stairwell hand rails:
 - lever-type handles for plumbing fixtures and door handles; and
 - solid blocking in washroom walls to facilitate future grab bar installation beside toilets, bathtubs and showers.

Conclusions

The applicant has satisfactorily addressed staff's comments regarding conditions of adjacency, site planning and urban design, architectural form and character, and landscape design. The applicant has presented a development that fits into the existing context. On this basis, staff recommend support of this Development Permit application.

Edwin Lee Planner 1

EL:cas

Attachment 1: Development Application Data Sheet

Attachment 2: Excerpt from the Minutes of the Advisory Design Panel Meeting – November 19, 2014

The following are to be met prior to forwarding this application to Council for approval:

- Final adoption of the Zoning Amendment Bylaw 9146.
- Receipt of a Letter-of-Credit for landscaping and hedge survival in the amount of \$149,090.00; Letter-of-Credit
 will not be released until the Letter of Assurance that landscaping installed from Landscape Architect as well as
 the Post Construction Impact Assessment Report confirming the protected hedgerows survived the construction
 from Arborist are reviewed by staff.

Prior to future Building Permit issuance, the developer is required to complete the following:

- Enter into a Servicing Agreement* for the design and construction of frontage improvements and service connections as per Rezoning Considerations.
- Submission of DCC's (City & GVS&DD), School site acquisition charges, Address Assignment Fee, and servicing charges, etc.
- Incorporation of accessibility, CPTED, and sustainability features/measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For further information on the Building Permit, please contact *Building Approvals Division at 604-276-4285*.
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (http://www.richmond.ca/services/ttp/special.htm).
- If applicable, payment of latecomer agreement charges associated with eligible latecomer works.



Development Application Data Sheet

Development Applications Division

DP 13-644888 Attachment 1

Address: 8600 and 8620 No. 2 Road

Applicant: Balandra Development Inc. Owner: Balandra Development Inc.

Planning Area(s): Blundell

Floor Area Gross: 1624.3 m² Floor Area Net: 1185.7 m²

	Existing	Proposed
Site Area:	1981.8 m ²	1981.8 m ²
Land Uses:	Single-Family Residential	Multiple-Family Residential
OCP Designation:	Neighbourhood Residential	No Change
Zoning:	Single Detached (RS1/E)	Low Density Townhouses (RTL4)
Number of Units:	2	9

4 2 mg 12 mg 2 mg 2 mg 2 mg 2 mg 2 mg 2 m	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.60	0.60	none permitted
Lot Coverage – Building:	Max. 40%	40%	none
Lot Coverage – Non-porous Surfaces:	Max. 65%	49.25%	none
Lot Coverage – Landscaping:	Min. 25%	25.44%	none
Setback – Front Yard (m):	Min. 6.0 m	5.4 m Min.	Variance Requested
Setback – North Side Yard (m):	Min. 3.0 m	3.0 m Min.	none
Setback – South Side Yard (m):	Min. 3.0 m	3.0 m Min.	none
Setback – Rear Yard (m):	Min. 3.0 m	4.5 m Min.	none
Height (m):	Max. 12.0 m (3 storeys)	10.45 m Max.	none
Lot Width:	Min. 50.0 m	43.29 m	Variance Requested
Off-street Parking Spaces – Regular (R) / Visitor (V):	2 (R) and 0.2 (V) per unit	2 (R) and 0.2 (V) per unit	none
Off-street Parking Spaces – Total:	20	20	none
Tandem Parking Spaces:	Max. 50% of proposed residential spaces in enclosed garages (16 x Max. 50% = 8)	0	none

Small Car Parking Spaces	None when fewer than 31 spaces are provided on site	7	Variance Requested
Handicap Parking Spaces:	None when fewer than 3 visitor parking spaces are required	0	none
Amenity Space – Indoor:	Min. 70 m² or Cash-in-lieu	Cash-in-lieu	none
Amenity Space – Outdoor:	Min. 6 m² x 9 units = 54 m²	66 m²	none

Excerpt from the Minutes from The Design Panel Meeting

Wednesday, November 19, 2014 – 4:00 p.m. Rm. M.1.003 Richmond City Hall

Panel Discussion

Comments from the Panel were as follows:

 a unique project; proposed project design minimizes the project's impact to a primarily single family neighbourhood;

No Comment.

• look at the project's domestic water heating efficiency to increase the project's Energuide 82 rating;

Project design meets Energuide 82 requirements and goes above and beyond code with R22 insulation instead of R20, HRV, and DHW heat recovery, thus already maximizing the domestic hot water system.

appreciate the provision of a convertible unit and use of sliding doors in the powder room on the ground floor and in the bathroom on the upper floor; consider pocket doors for the ensuite bathroom and laundry room on the upper floor in lieu of swinging doors to free up floor space and improve accessibility; also consider the same approach to the other units in the proposed development;

Please see attached plan#07.

- introduction of a different architectural style in Richmond is encouraging; look at how to effectively blend the materials at the front and back of the buildings;
 Materials have been blended via the trim details which present consistency across all elevations.
- side elevations are not symmetrical; look at the rooflines;
 No Comment.
- consider increasing the area of the outdoor amenity to make it more visually significant; We have reviewed on increasing in the bulge of the road intersection. There was no significant increasing in the landscaping and the curb pattern was less pleasing in central open space. Final solution is shown on plan# L3b.
- concern on the interface of the north and south property lines with neighbouring properties in view of the potential 10-12 feet surface height of the fence on top of a retaining wall on the subject property; applicant should continue looking for ways to address the interface to mitigate the visual impact to the neighbourhood; concern on shadowing upon the adjacent property to the north;

We lowered the drive aisle grade at the north and south end to drop down the retaining wall height. The maximum height of the retaining wall is about 40". Furthermore the height of retaining wall has been dropped again about 11" from back of parking stall to the front yard. Also in an effort to reduce the apparent height of the retaining wall and fencing, the fence at the top of the retaining wall has been reduced to 42" guardrail height. Arbour at each end of internal drive aisle will provide more privacy for the residents.

site planning is well done;

No Comment.

 the applicant is proposing a unique architectural style; the applicant should maintain the detailing through the construction stage to enhance the character of the proposed development;

No Comment.

• concern on the extreme differences in the roof pitch; understand the rationale for the proposed 4/12 roof pitch; however, it takes away the proposed architectural style for the buildings; look at ways to integrate a steeper roof pitch closer to the standard 8/12 while also addressing the concerns of the neighbouring properties;

The proposed roof pitch changed to 4/12 to address shadowing the neighbour's properties.

• site planning is well-handled; appreciate the massing of the building and the applicant's efforts to keep it down; shadow study provided by the applicant shows no shadowing upon neighbouring properties; an 8/12 roof pitch for the back buildings will not create more shadows:

No Comment.

- appreciate the proposed architectural style; however, some of the detailing appears fuzzy, e.g. use of cultured stone and too much horizontal banding (brick could be carried upwards);
 Cultured stone removed from the drawings and replaced with precast concrete.
- window treatment is rigorous on the street edge; look at precedent in the MOSAIC development where the window treatment is more relaxed, i.e., living room windows are bigger than the upper floor windows;

The window treatment is picked carefully to address the proposed style. The living room windows need to be bigger to provide more light for bigger area.

- walls on the street edge appear high in one of the renderings presented by the applicant;
 No Comment.
- applicant should have provided a street view to enable the Panel to visualize how the proposed architectural style fits into the neighbourhood context;
 Please see attached streetscape.
- consider toning down the fuzziness of the detailing of the buildings;
 The detailing will be provided on BP stage.

 appreciate the applicant's efforts and due diligence to address the concerns of the neighbouring properties at the back;

No Comment.

There is verticality and formality in the proposed development; however, the neighbourhood is expected to redevelop in the future with increased density and more rigorous pattern of vertical elements;

No Comment.

 high standard of proposed materials for the project (e.g. metal, wood and brick as opposed to vinyl) should be maintained as the project develops;

No Comment.

landscaping is well-resolved;

No Comment.

 proposed play equipment is a good choice; appreciate the proposed materials for the outdoor amenity area;

No Comment.

• concern on the height of the walls (6-foot fence on top of the retaining wall) on the north, south and east property lines; applicant should look at opportunities to work with neighbouring properties to ameliorate the impact of the walls, e.g. making the walls more permeable; and

We lowered the drive aisle grade at the north and south end to drop down the retaining wall height. The maximum height of the retaining wall is about 40". Furthermore the height of retaining wall has been dropped again about 11" from back of parking stall to the front yard. Also in an effort to reduce the apparent height of the retaining wall and fencing, the fence at the top of the retaining wall has been reduced to 42" guardrail height. Arbour at each end of internal drive aisle will provide more privacy for the residents.

Appreciate the applicant's efforts to increase the permeability of the site; however, the applicant is encouraged to get an engineering input in view of Richmond's high water table. In general, we prefer to avoid large areas of permeable pavers as they can be subject to significant differential settlement. Additionally, as the water table in Richmond is high, there is little point to increasing permeability as the infiltrated water won't necessarily go anywhere.

Panel Decision

It was moved and seconded

That DP 13-644888 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Panel.

CARRIED



Development Permit

No. DP 13-644888

To the Holder:

Balandra Development Inc.

Property Address:

8600 and 8620 No. 2 Road

Address:

c/o Wayne Fougere

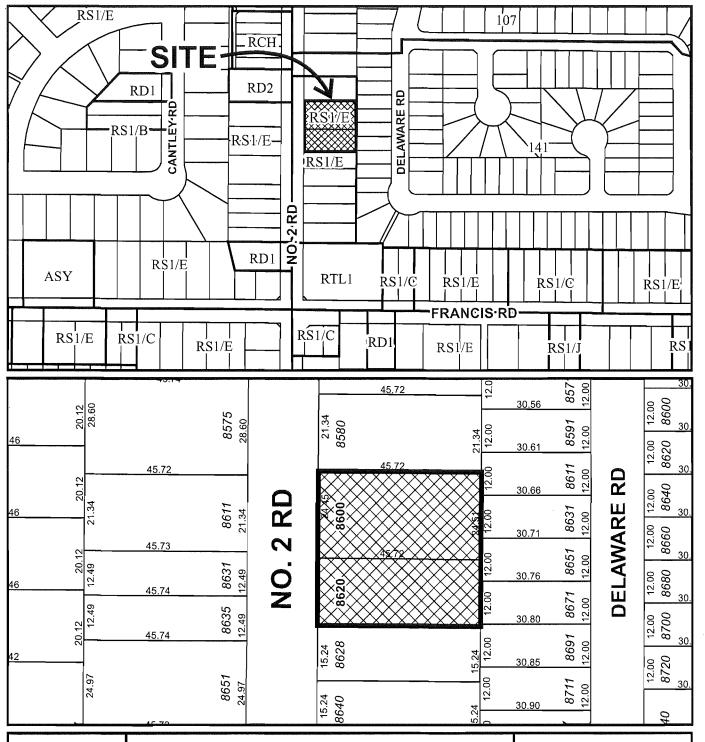
#202 – 2425 Quebec Street Vancouver, BC V5T 4L6

- 1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
- 2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - a) reduce the minimum lot width on major arterial road from 50.0 m to 43.29 m;
 - b) reduce the front yard setback from 6.0 m to 5.4 m; and
 - c) permit seven (7) small car parking spaces.
- 4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #4 attached hereto.
- 5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
- 6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$149,090.00 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
- 7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit No. DP 13-644888

To the Holder:	Balandra Development Inc.			
Property Address:	8600 and 8620 No. 2 Road			
Address:	c/o Wayne Fougere #202 – 2425 Quebec Street Vancouver, BC V5T 4L6			
	n shall be developed generally in accordance with the terms and softhis Permit and any plans and specifications attached to this a part hereof.			
This Permit is not a Buil	ding Permit.			
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DELIVERED THIS	DAY OF , .			
MAYOR				







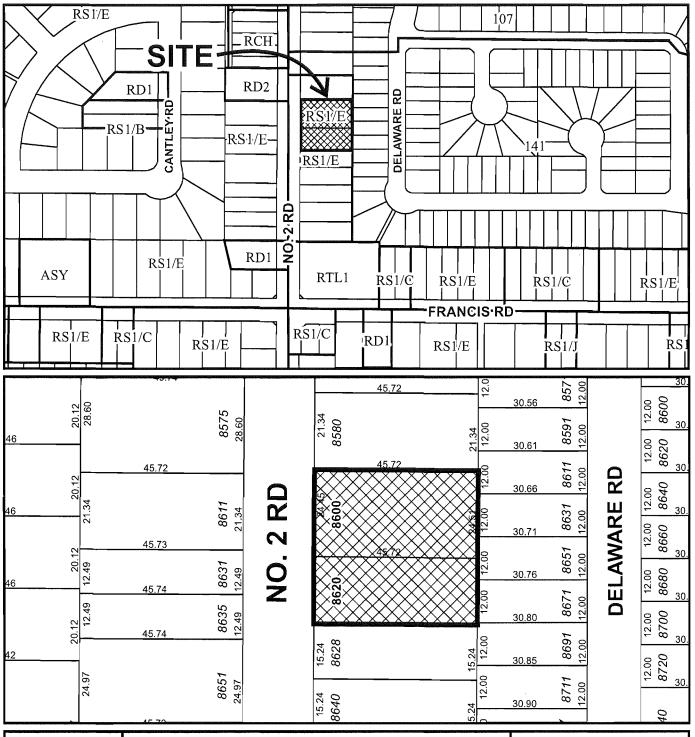
DP 13-644888 SCHEDULE "A"

Original Date: 02/18/15

Revision Date:

Note: Dimensions are in METRES





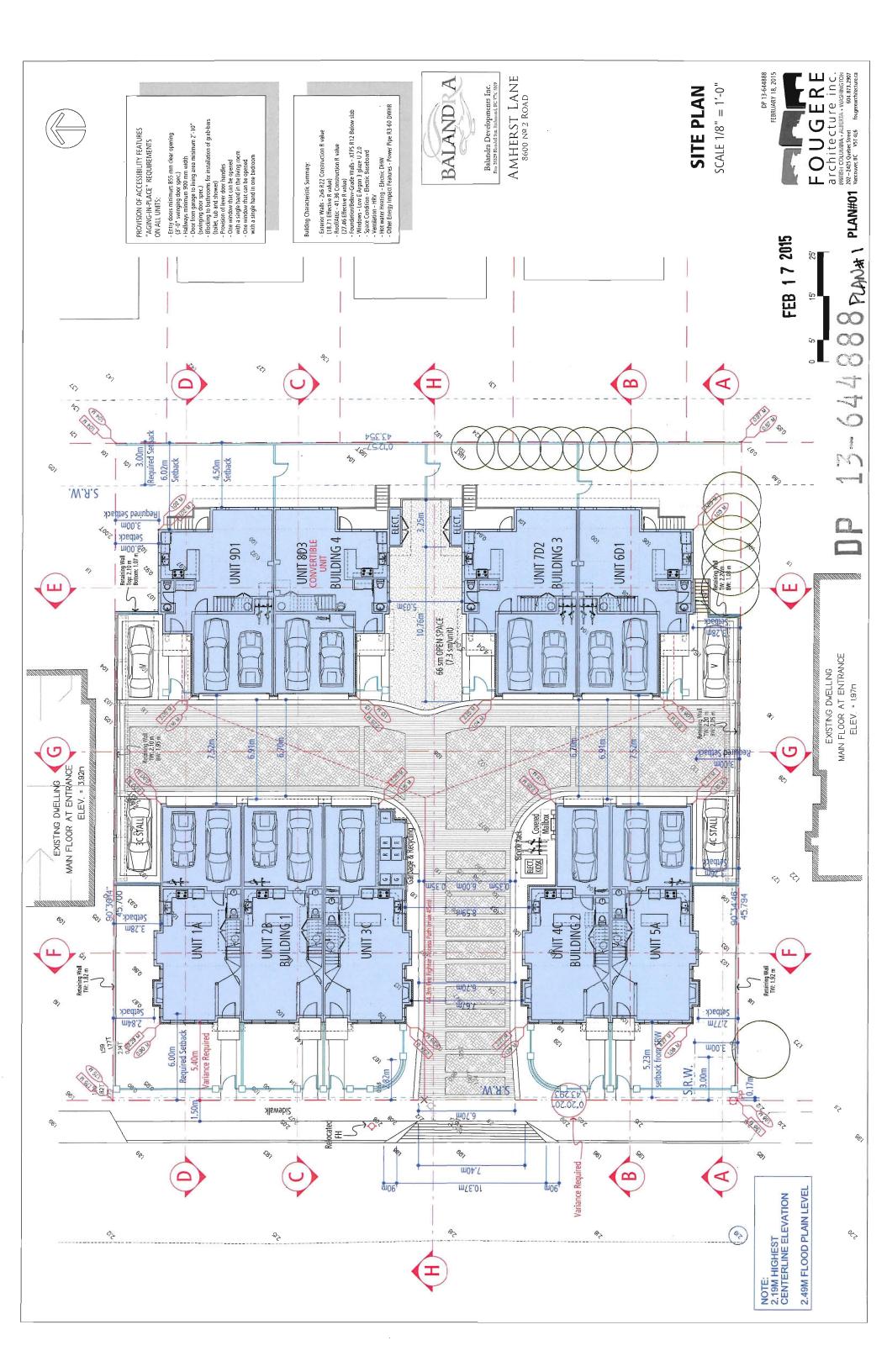


DP 13-644888

Original Date: 02/18/15

Revision Date:

Note: Dimensions are in METRES





FOUGERER EREPRESSION OF THE PROPERTIES OF THE PR

DP 13-644888 FEBRUARY 11, 2015

2 2 2 3 3 Covered Area 353 353 358 358 343 Garage Area Attic Floor Staircaise Exemption 22 Upper Floor Staircaise Exemption 23 48 33 49 3 8 3 3 Exemption Main Floor Stair Site Area 21,333 sf (1981.9 sm) Floor Area Total Gross 2,183 3,940 3,758 1,918 17,471 1,918 3,754 Floor Area Unit Gross 1,877 2,183 FLOOR AREA CALCULATION Proposed Zoning RTL4 Floor Area Attic Gross Upper Gross Floor Area 925 824 907 907 937 907 Floor Area 920 820 972 1,011 1,011 946 Main Gross Unit Jype 2 2 2 2

Total GFA

SFA GFA

Total Net Area

Unit Net Floor Area

3,002 1,503 1,518

1,662 1,501 1,503

2,714 1,357

1,374 1,507 1,357 1,359 1,357

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ACCESSORY CALC.	Total Accessory Area	90/	353	490	358	716	343	95	107	3,129
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AMHERST LANE 8600 Nº 2 ROAD

Balandra Developments Inc.

Site Area 21,333 sf (0.49 acres)	Maximum Lot Coverage	40%	Maximum Accessory Area	358 sf	Maximum Covered Area	10.0%
Proposed Density 18.4 upa	Proposed Lot Coverage	40%	Proposed Average Accessory Area	261 sf	Proposed Covered Area	2.92%

Maximum FAR Proposed FAR

variance required: lot width 43.3 m complies

variance required: 5.4m complies complies

complies

OCP requirement:
14.4.13 Arterial Road Guidelines for Townhouses Along the rear yard interface with single-family housing:
may have a 6 m setback where deemed necessary;
may have 1 storey projections less than 1.5 m

complies

12.0 m (3 storeys)

Our Design

- ZONING ANALYSIS

Zoning Requirement

RTL4 Zoning District 8.11.1 Purpose

complies

complies complies

Low Density Housing Single Detached Housing Housing, Town 0.60 FAR

complies complies complies

40% maximum 65% maximum 25% minimum

8.11.2 Permitted uses 8.11.4.2. a) Permitted Density 8.11.5. Lot Coverage 1. buildings

2. non-porous
3. live plants
8.11.6. Yards & Setbacks
1. front yard
2. interior side yard
4. rear yard

complies complies variance required: 7 small car stalls

complies complies

complies

4 vertical stalls (33% of Class 1)

18 stalls (2 cars/unit) 2 stalls (0.2 cars/unit) no small car stalls

8.11.10 On-Site Parking and Loading 7.7.2.1 residential

visitors 7.5.13 small car

8.11.7.1 Permitted Height 8.11.8. Minimum Lot Size 1. minimum lot width 2. minimum lot depth

12 stalls (1.25 spaces/unit) 2 stalls (0.2 spaces/unit)

7.14.5 On-site Bicycle 7.14.10 Requirements Class 1 Class 2

8.7.11 Other Regulations

complies complies

complies complies complies complies comples

no closer than 3.0 m no closer than 1.5 m

side or rear yard 4.9.5. entry stairs projection front lot line

1.5 m maximum 1.2 m maximum

in the form of the fire side or rear lot line 4.9.6. balconies & porches projection front, exterior side & rear yard 4.1.2.3. cattlevered roofs, eaves & gutters into the yard into the yard 4.1.6. basic universal housing features

1.0 m maximum 0.6 m maximum

4.1.1 density calulation exclusion 10% of floor area for covered area maximum of 50 sm for garage exclusion of 10 sm of stairs & entry 4.9.4. bay window projection front yard

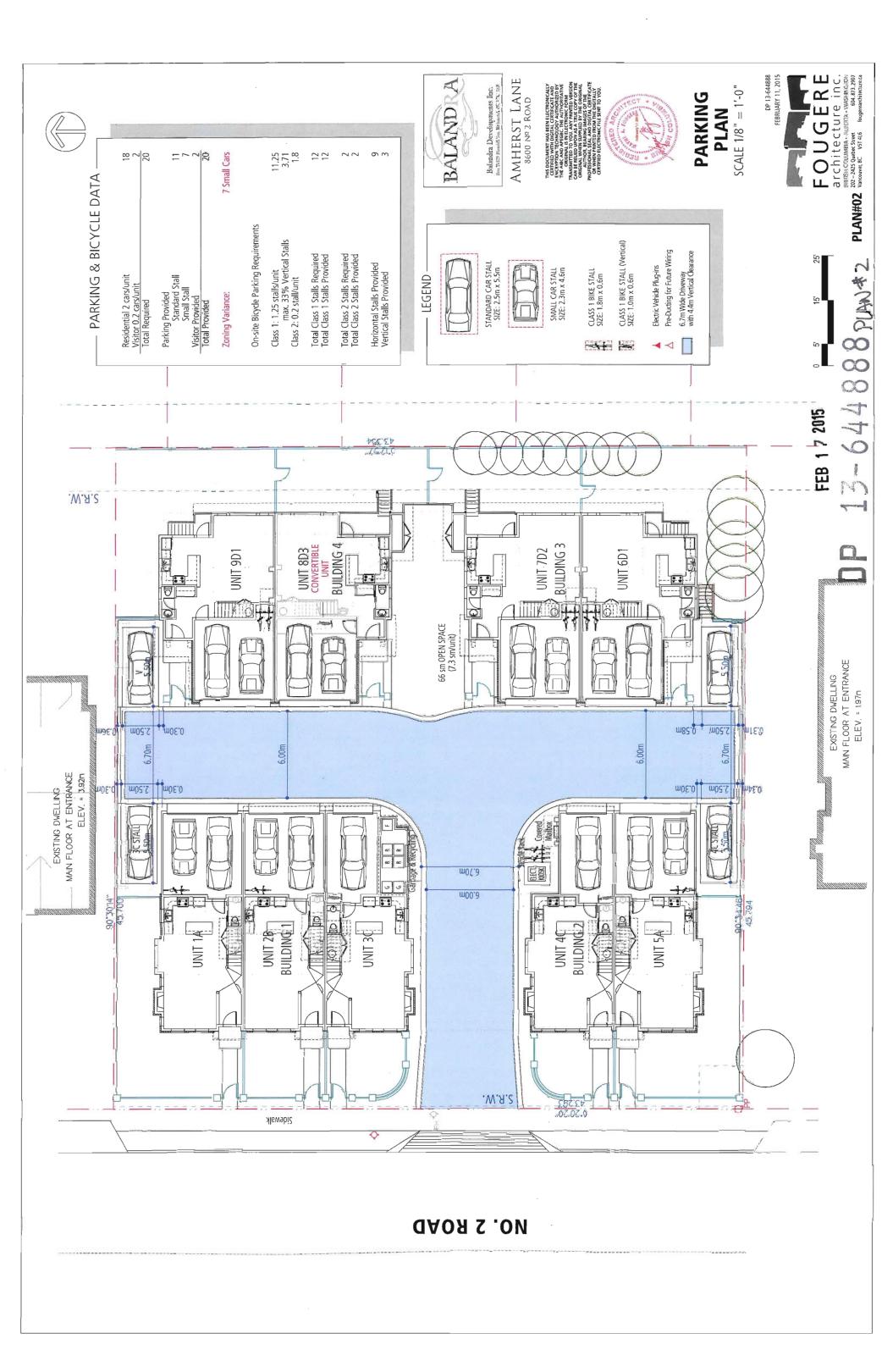
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	1 7 2015	# 1K

SITE DATA



AMHERST LANE

for Balandra Development

8600 No 2 Road. Richmond, BC Civic Address:

landscape architecture

Owner: Architect: Landscape:

1690 West 2nd Avenue, Vancouver . BC . V6J 1H4 t | 604.683.1456 f | 604.683.1459 w | www.etala.ca

Balandra Development Fougere architecture inc. eta landscape architecture

re-ISSUED FOR DP February 18, 2015

LANDSCAPE:

Amenity Area Landscape Plan and Schedules Landscape Planting Plan Tree Management Plan Landscape Plan Sheet Name Sheet No. L3b L3c L3d

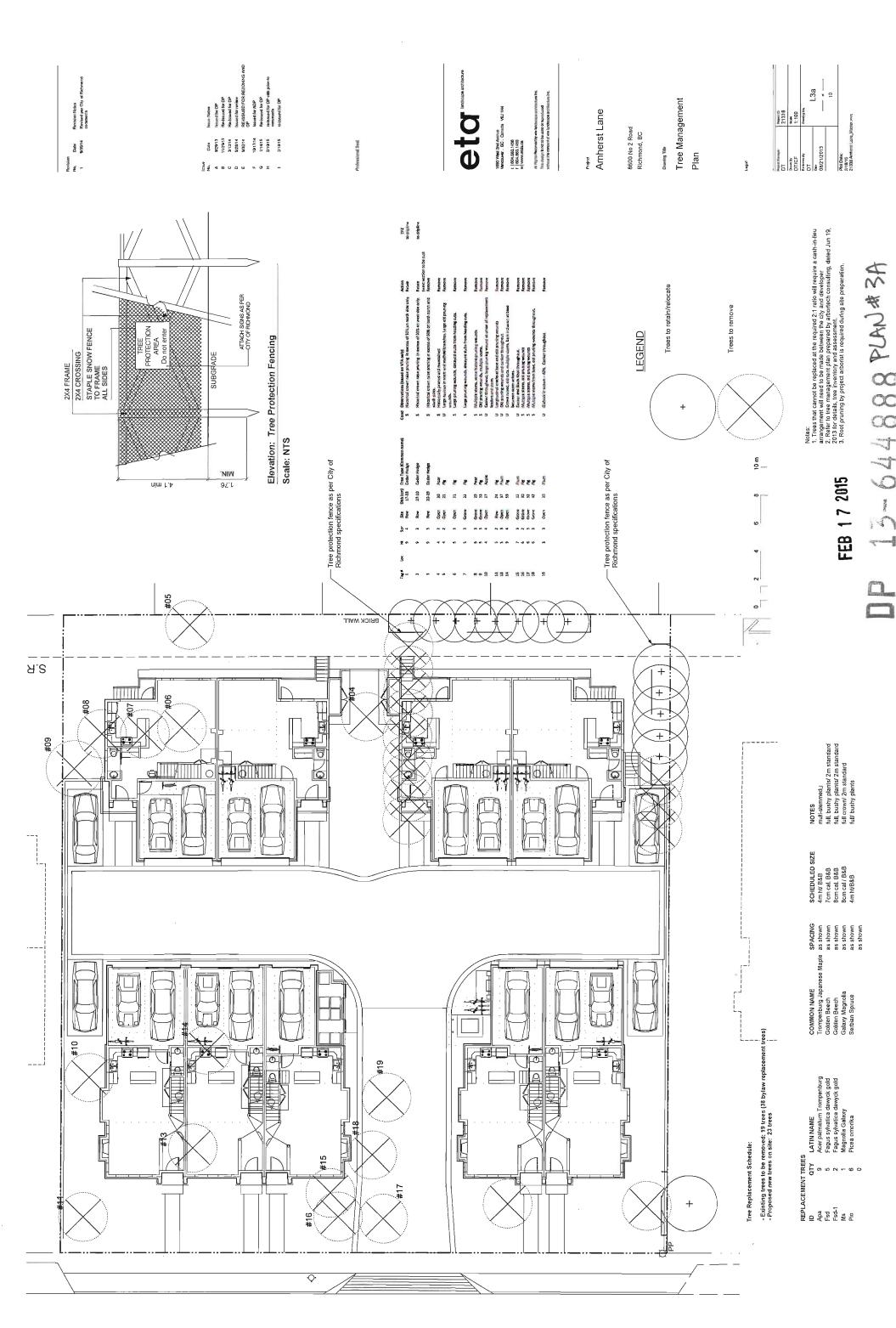
Elevations- Crib Retaining/ detail Landscape Details - Paving L3e L3f

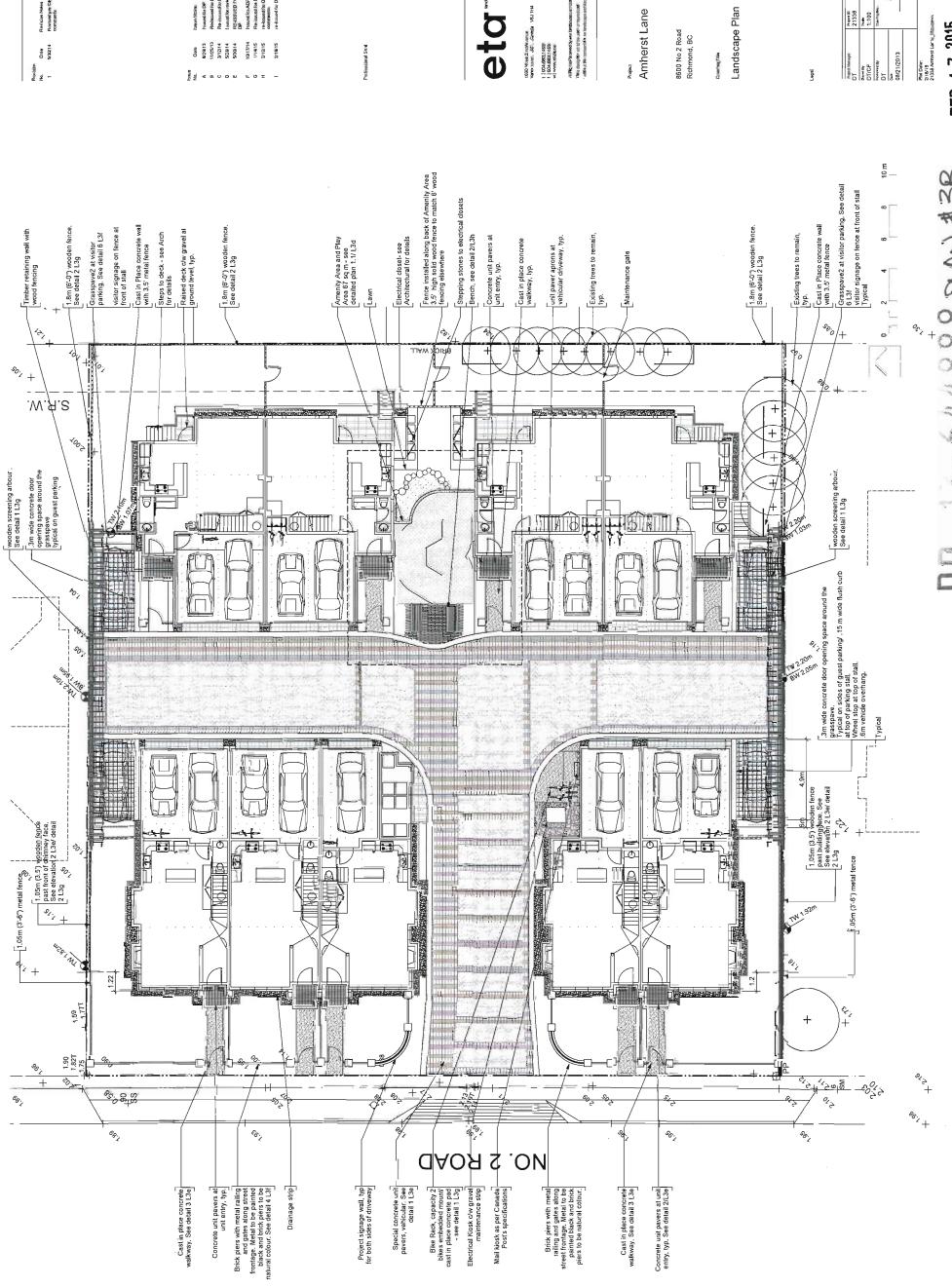
Landscape Details - Brick Piers, Sign Wall and Fences L3g

Landscape Details - Site Furnishings Permeability Plan L3i

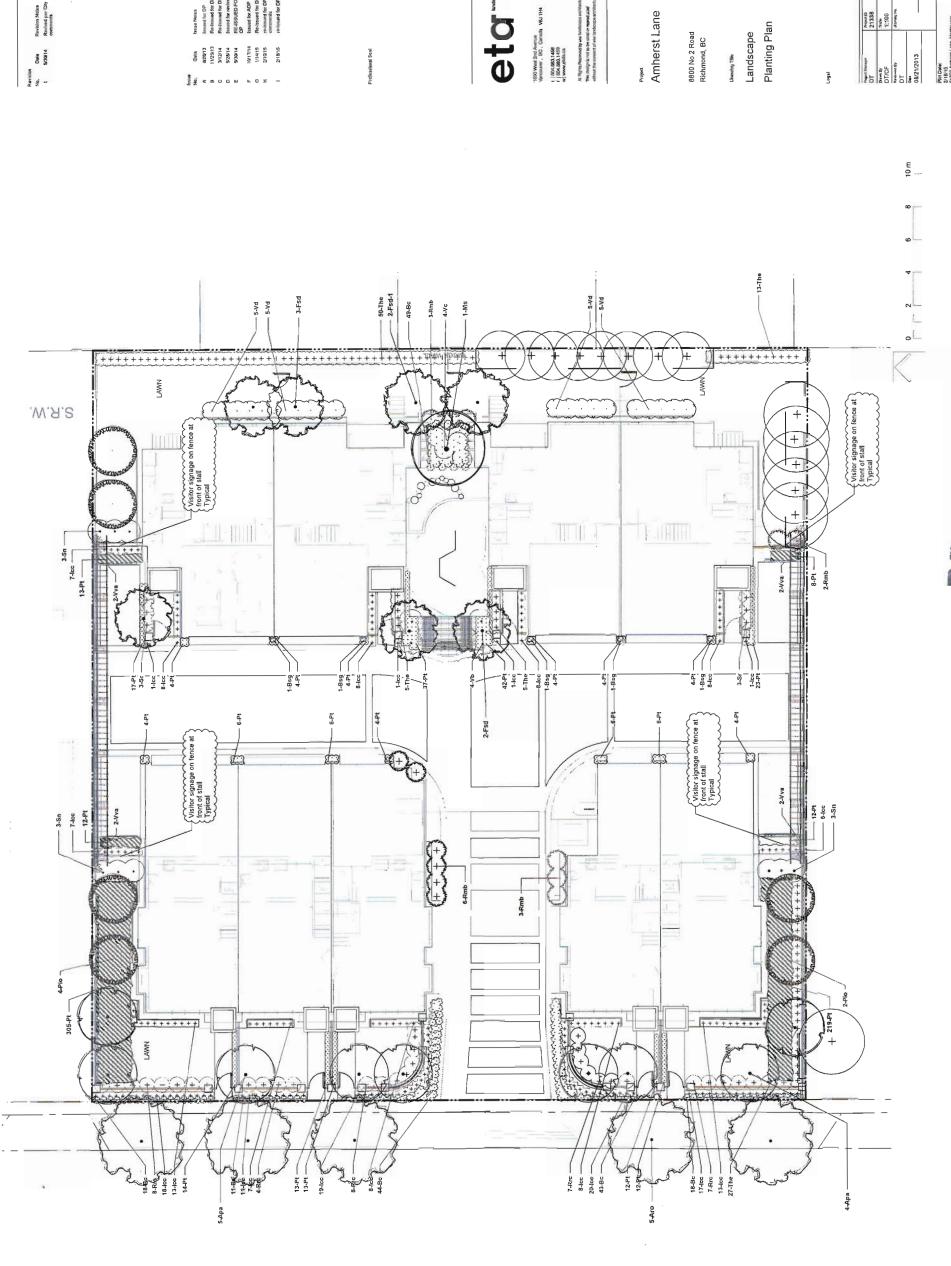
L3h

Lighting Plan L3j

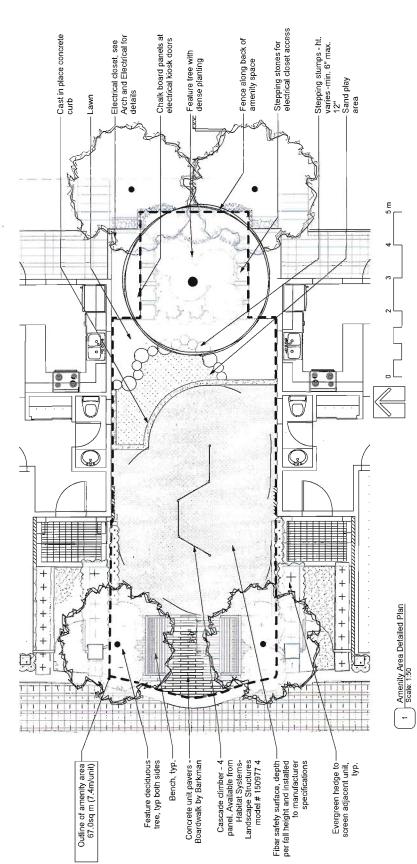




15-644888 PLAN #3B



DP 15-644088 PURN#3C



Revision No. Dale 1 \$/30/14

PLANT LIST					
TD QIY	LATIN MAME	COMMON NAME	SPACING	SCHEDULED SIZE	NOTES
TREES- OFFSITE					
Aro 5	Acer rubrum 'October Glory' TO BE TO THE SATISFACTION OF RICHMOND PARKS	October Glory Red Maple	as shown	7cm Cal	full, bushy plants/ 2m standard
TREES- ONSITE					
Ара 9	Acer palmatum Trompenburg	Trompenburg Japanese Maple as shown	as shown	4m ht/ B&B	multi-stemmed ()
Fsd 5	Fagus sylvatica dawyck gold	Golden Beech	as shown	7cm cal. B&B	full, bushy plants/ 2m standard
Fsd-1 2	Fagus sylvatica dawyck gold	Golden Beech	as shown	8cm cal. B&B	full, bushy plants/ 2m standard
Ms 1	Magnolia Galaxy	Galaxy Magnolia	as shown	8cm cal / B&B	full crown/ 2m standard
Pio 8	Picea omorika	Serbian Spruce	as shown	4m hVB&B	full' bushy plants
0			as shown		
SHRUBS					
Bc 183	Bergenia cordifolia Bressingham White	Heartleaf Bergenia		0.41 #1 cont.	full bushy plants/ heavy
Bsg 5	Buxus sempervirens 'Graham Blandy'	Graham Blandy Boxwood		0.25 #5 cont	full, bushy plants
lcc 190	llex crenata compacta	Japanese Holly		0.41 #5 cont.	full, bushy plants/ previously clip
Pt 812	Pachysandra terminalis	Japanese spurge		0.25 SP#4	full bushy plants
Rmb 14	Rhododendron 'mission bells'	Rhododendron (White)		1.07 1mx1m/ field grown/ B&B	full/ bushy plants
Rrc 34	Rosa Royal City	Royal Cily Rose		0.91 #2 cont.	full, bushy plants
Sn 9	Sambucus nigra	European Elder		0.76 #5 cont.	full bushy plants
Sr 6	Sarcococca ruscifolia	Fragrant Sarcococca		0.76 #3 conL	7
The 100	Taxus x media 'H.M. Eddie'	Eddie yew		0.46 3m/B&B	full bushy plants
Vb 4	Vibumum burkwoodii	Korean Spice Bush		1.07 1m high B&B	full bushy plants
Vc 4	Vaccinium corymbosum	Blueberry		0.41 #5 cont	full bushy plants
Vd 20	Viburnum davídii	Vibumum		0.76 #3 cont.	full, bushy plants
Vva 8	Vitis vinifera atropurpurea	Purple Leaf Grape		0.91 #2 cont/ staked	full bushy plants

NOTE: ALL LANDSCAPE TO CONFORM TO BC LANDSCAPE STANDARDS CURRENT ED. FOR LEVEL 2 'GROOMED' LANDSCAPE TREATMENT IN THE EVENT OF A DISCREPENCY BETWEEN THE PLANT LIST AND THE PLANTING PLAN, THE PLANTING PLAN TAKES PRECEDENCE.

CAWN

CONTRACTOR TO PROVIDE WRITTEN 1 YEAR WARFANTY
ON SOLL SEFFICIANTONS.
AN INDEPENDANT SOLL TEST TO BE PROVIDED 1 WEEK
PRIGHT TO ENDOUGE SOLL AMENDAMENTS TO BRINGS
SOLL UP TO CALALITY RECOMMENDED IN
SOLL UP TO CALALITY RECOMMENDED IN
SOLL PREPORT.

SITE INSPECTION
ACCEPTANCE IN WRITING TO THE CONSULTANT.
ACCEPTANCE IN WRITING TO THE CONSULTANT.
ASCERTAIN THE SIZE AND LOCATION OF ALL EKSTRING
SERVICES AND SUBGRADES PRIOR TO THE WORK.

TEXTURE—
COASSE GRAVEL (LARGER THAN 25MM): 0-1%
ALL GRAVEL (LARGER THAN 25MM): 0-5%
THAN 20 MAN; 260-779;
THAN 20 MAN; 260-779;
THAN 20 MAN; 10-25%
THAN 20 MAN; 10-25%
CLAY (SWALLER THAN 3022MM; D-20%
CLAY (SWALLER THAN 3024MM; D-20%
CLAY MOS ILL COARBINED, NAXIMAM 25%

GOOWING MEDIUM SHALL CONFORM TO LEVEL 1 LOW TRANSFOL CONFORM THEIS AND LANGE SHAUBS (ILL IN TABLE EAS OF THE CURRENT EDITION BICLIA STANDARD TO TS SHAUL POSSESS THE FOLLOWING OUALITIES.

IMPORTED TOPSOIL SHALL CONFORM TO AND BE TREATED AS PER SECTION 6.2..3 TO 62..7 NGCLISIVE OF THE CURRENT EDITION BCLINA STANDARDS.

IMMEDIATELY REPAIR DAMAGE RESULTING FROM FAILURE TO EMERCISE SUCH PRECAUTIONS AT NO COST TO THE OWNER.

ALL PRIMINING TO BE IN ACCORDANCE WITH THE BCLINARGSLA STANDARDS CURRENT EDITION.

PLANT COUNTS

THE CASE OF WHY DISCREPENCY BETWEEN PLANT
COUNTS ON PLANT LIST AND PLANT SYMBOLS ON
DEARWING THE PRAWNINGS TAKES PRECEDENT. THE
CONTRACTION IS TO VEHEY ALL PLANT COUNTS AND
NOTIFY COARSULFANT OF ANY DISCREPENCY.

MINIMUM SOIL DEPTH TO BE AS PER TABLE 6-5 OF THE CURRENT EDITION BCLNA STANDARDS:

DRAINAGE: PERCOLATION SHALL BE SUCH THAT NO STRANDING WATER IS VISIBLE 60 MINUTES AFTER AT LEAST 10 MINUTES OF MODERATE TO HEAVY AAIN OR IRRIGATION.

ORGANIC CONTENT: 3-10% AcIdity: 6.0-7.0 Over structure

36. 5.4. 6.

Over prapared
SHRUBS
SHRUBS
GROUNDOVERS
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C

SOIL DEPTHS WILL BE CHECKED AT TIME OF SUBSTANTIAL COMPLETION REVIEW

CONTRACTOR TO PROVIDE CORY OF SCIL TEST TO LANDSCAPE CONSULTANT WERES PRINT TO DEFENDENT TO BELLYERY ON STILL TEST TO BE FERFORMED BY ANI NUCREPULANT LAWN AND PLANTING BEDS.

CONSULTANT TO APPROVE SCIL BETOR BE INSTILLATION. THIS DOES NOT PRECLUDE THE CONSULTANT FROM PREFORMING ANI NUFFORMANT SOLL ARALT YSIS AT TIME OF SUBSTANTIAL COMPLETION. CONTRACTOR WITH BE FOR SMISH TO STILL SON PREPACEMENT OF SOLL THAT DOES NOT THAT DOES NOT THAT DOES NOT THAT TO STILL THAT THON SONT THAT TO STILL THAT THE STILL THAT THE SECFICIATIONS AT NO EXTRA COST TO CLIENT.

CONTRACTOR TO PROVIDE MAINTENANCE FOR PERIOD OF 1 YEAR FOLLOWING SUBSTANTIAL COMPLETION. PLANTED AREAS TO HAVE PERMANENT HIGH EFFICIENCY IRRIGATION SYSTEM

ALL PLANT MATERIAL TO BE INSPECTED PRIOR TO BELIVERY ON SITE, CONTRACTOR TO ARRANGE FOR INSPECTION AND MATERIAL TO ASSEMBLED IN ONE LOCATION FOR REVIEW.

ALL PLANTS TO BE NURSERY GROWN ALL PLANT MATERIALS AND LABOUR TO CONFORM TO BCSLAJBCLNA STANDARDS (CURRENT ED).

BEDS TO HAVE 2" MULCH LAYER CONSISTING OF ORGANIC COMPOSTED BARK APPLIED.

CONTRACTOR TO PROVIDE WRITTEN 1 YEAR WARRANTY ON PLANT MATERIAL

IMPORTED SOIL SHALL BE A SANDY LOAM OF LOAMY
SAND TRYTIER (INO LESS THAN 90% SAND BY WIEBHT)
CONTAINING 4 AND 15% ORGANIC MATTER (ORY WIEBHT)
BASIS, SOIL SHALL WIED THAN THE FROM WIEBHT
WOOD, INCLUDING NAL WIED THAN THE FREE TONG
WAS TREAD. STOKES OVER THAN THE MEDIS OUR SOIL SHALL BE FREE FROM COUCH/GRAGS. EQUISETING.
SOIL SHALL BE FREE FROM COUCH/GRAGS. EQUISETING.
SOUN WILL WILL SAND OTHER NOXIOUS WEEDS OR SEEDS OF
PARTS THERDOR.

SITE FURNISHINGS DESCRIPTION	SIZE	MODEL	MANUFACTURER	нсиснт	COLOUR	QTY
BENCH BIKE RACK PLAY STRUCTURE	40	Parc Vue RING CASCADE CLIMBER	are vue LANDSCAPE FORMS RING LANDSCAPE FORMS CASCADE CLIMBER LANDSCAPE STRUCTURES		Black Black	2 -
MATERIALS DESCRIPTION	SIZE	MODEL	MANUFACTURER	нсниснт	COLOUR	QTY
PERMEABLE PAVERS- panels PERMEABLE PAVERS- bands PEDESTAINA PAVERS of stops PEDESTAINA PAVERS AT AMENITY AREA WOODEN DECK - RESIDENTIAL	80mm depth 80mm depth 60mm depth	Standard Standard BOARDWALK BOARDWALK	ABBOTSFORD CONCRETE ABBOTSFORD CONCRETE ABSOTSFORD CONCRETE ABBOTSFORD CONCRETE PER ATCHIVECTS	Herringbone pattern Brown Stack Bond pattern Charcoal Stack Bond pattern Brown pattern as shown Brown	Brown Charcoal Brown Brown	
ENSINEERED WOOD PROLECTIVE SURFACE VEHICLE PARKING STALL SURFACING		Grasspave2	SOF FALL OR APPROVED EQUAL Invisible Structures Inc. 1.800.233.1510		ыk	

3.57 6ft ht WODDEN FENCE

1690 West 2nd Avenue Vancouver , BC , Canada V&J 1H4		lights Reserved by 410 landscape auchitochuo fro. design is not to be used or reproduced
1690 West 2nd Av. Vancouver . BC .	t 604,663,1455 f 604,683,1459 w www.etaba.ca	A Rights Reserved b This design is not to

	erst Lane
Project	Amhe

300 No 2 Road	lichmond, BC	
8600	Rich	

Drawdag Tillo	Amenity Area Detailed Pla	Plant List and	Materials Schedule

Project 10 21338	scale as noted	Chemica Na.	
Аqed Мизун DT	DT/CF	Reviewed By DT	OB/21/2013

2015
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FFR

04-088 PLAN # 28

Section: Through parking to adjacent property Scale: 1:24

grasspave parking- refer to details 6&7/ L3f — planting strip —

Elevation: View from North Scale: 1:100

deadmen tied back to secondary wall' timber retaining for paving —

finish grade on neighbouring property — 6x6 timber crib wall — 4" perf PVC pipe- tie to storm

erst Lane ation Views

Revision Date 1 RADIO	Paraw Na. Dale Na. A 4 4252	Professional Se	W Seek Dead of the Park	P CORROSO E FICHER PARK P CORROSO E P CORR	Amhe Amhe 8600 No.	Dearling Tille Elevati
PROPERTY LINE	Screen arbour Outline of neighbor property	ROPERTY LINE	Dropped sideyard refaming wall height 11" (2 wall timbers) from base of damny base of damny street.	STING GRADE	arbour situature property and pa	2.5 S S S S S S S S S S S S S S S S S S S

UNIT 6D1

Revision No. Date 1 5/30/14

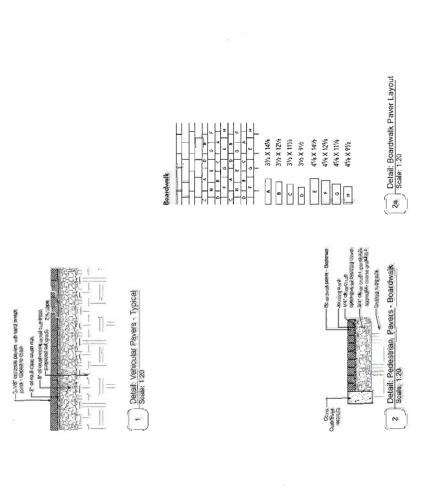
PROPERTY LINE

NO 2 ROAD

Elevation: View from the South Scale: 1:100

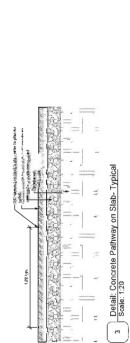
PROPERTY LINE

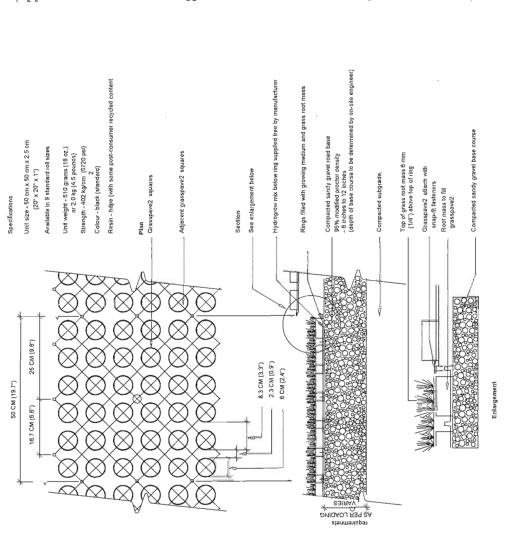
Z1338	State as noted	Chaming No.	L3e	



4 Detail: Drip Strip- Typical

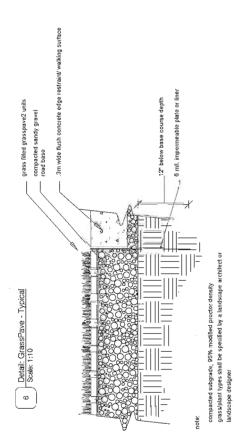
S | Detail; Threshold - Typical





Revised Notes Revised per Cily of Rithmond compression





Project Amherst Lane

8600 No 2 Road Richmond, BC

7 Detail: GrassPave2 with concrete edger Scale: 1:10

Landscape Details

Typical

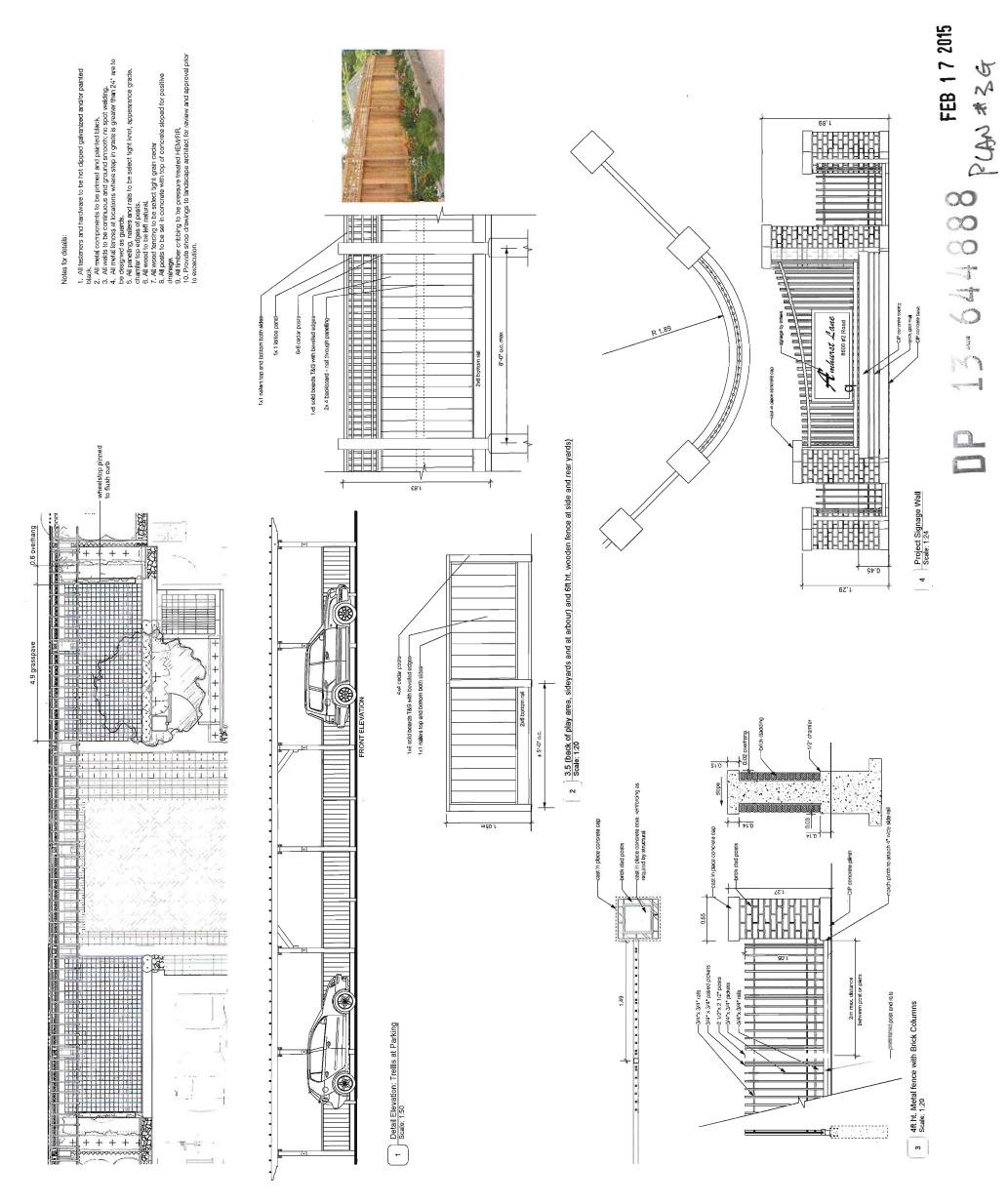






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13-644888 PURN#3F



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Revision Notes Revised per City of Richmond comments

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1 (904.882.1459
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A Roys Reserved by ele transcript architectur be

Project Amherst Lane

8600 No 2 Road Richmond, BC Orawing Tale
Details

Detalls Brick Piers, Sign Wall and Fences

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Personner

Director

Dire

During Strang Oracle Strang Oracle Strang Oracle Strang Oracle Or

Notes for details: 3 Children's Climbing Structure Colours to be decided landscapeforms (MSK) (M1 R) Page 1 of 1 **Product Drawing** Parc Vue Bench - Landscape Forms Bike Rack - Landscape Forms Bola Parc Vue®

Roribian No. Oalo Revidon Koles. 1 \$20x14 Revised por City of Katmand overments

landscapeforms•

Installation Guide

Bola® Bike Back

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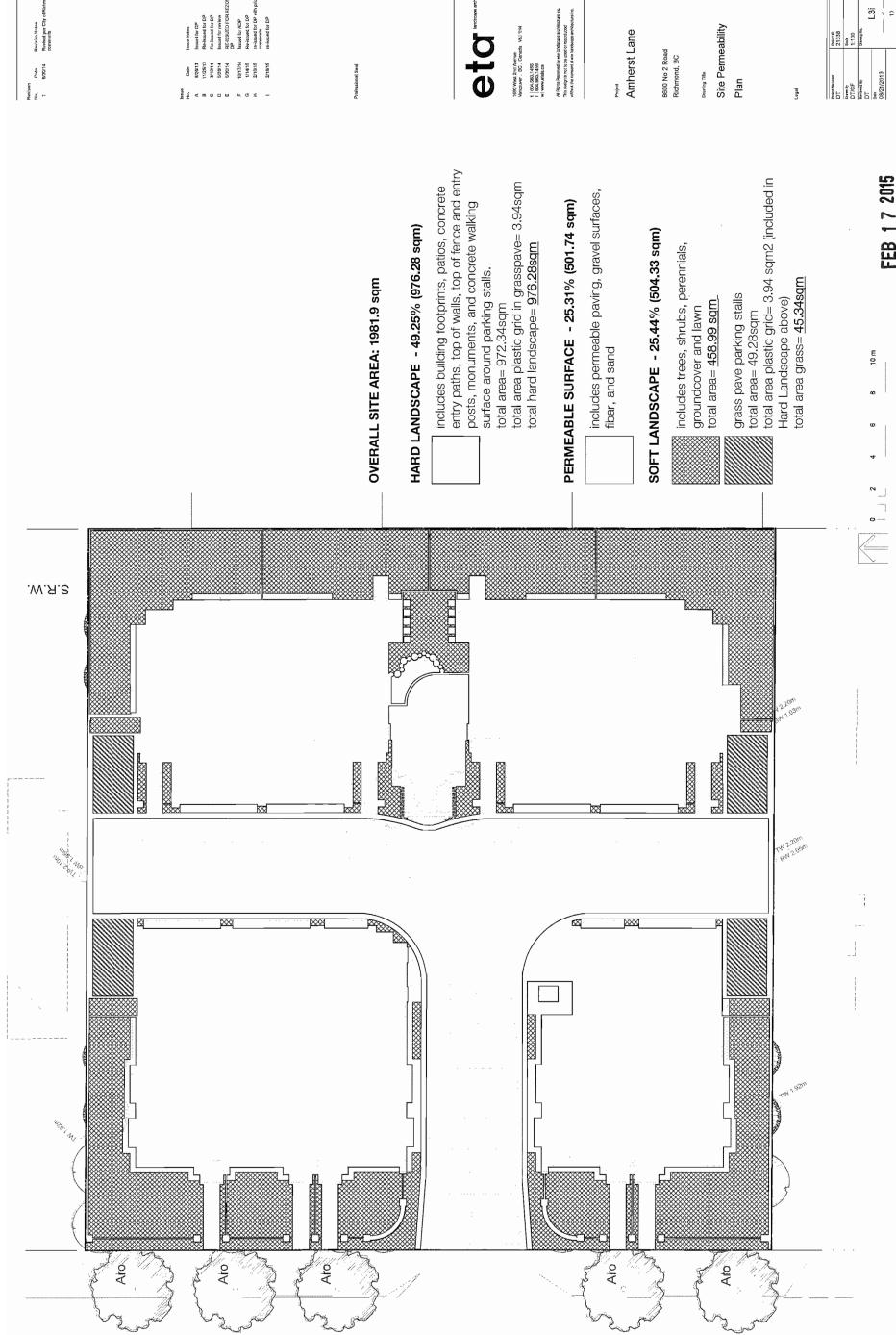
Registary Pane Amherst Lane

8600 No 2 Road Richmond, BC

Site Furnishings Details

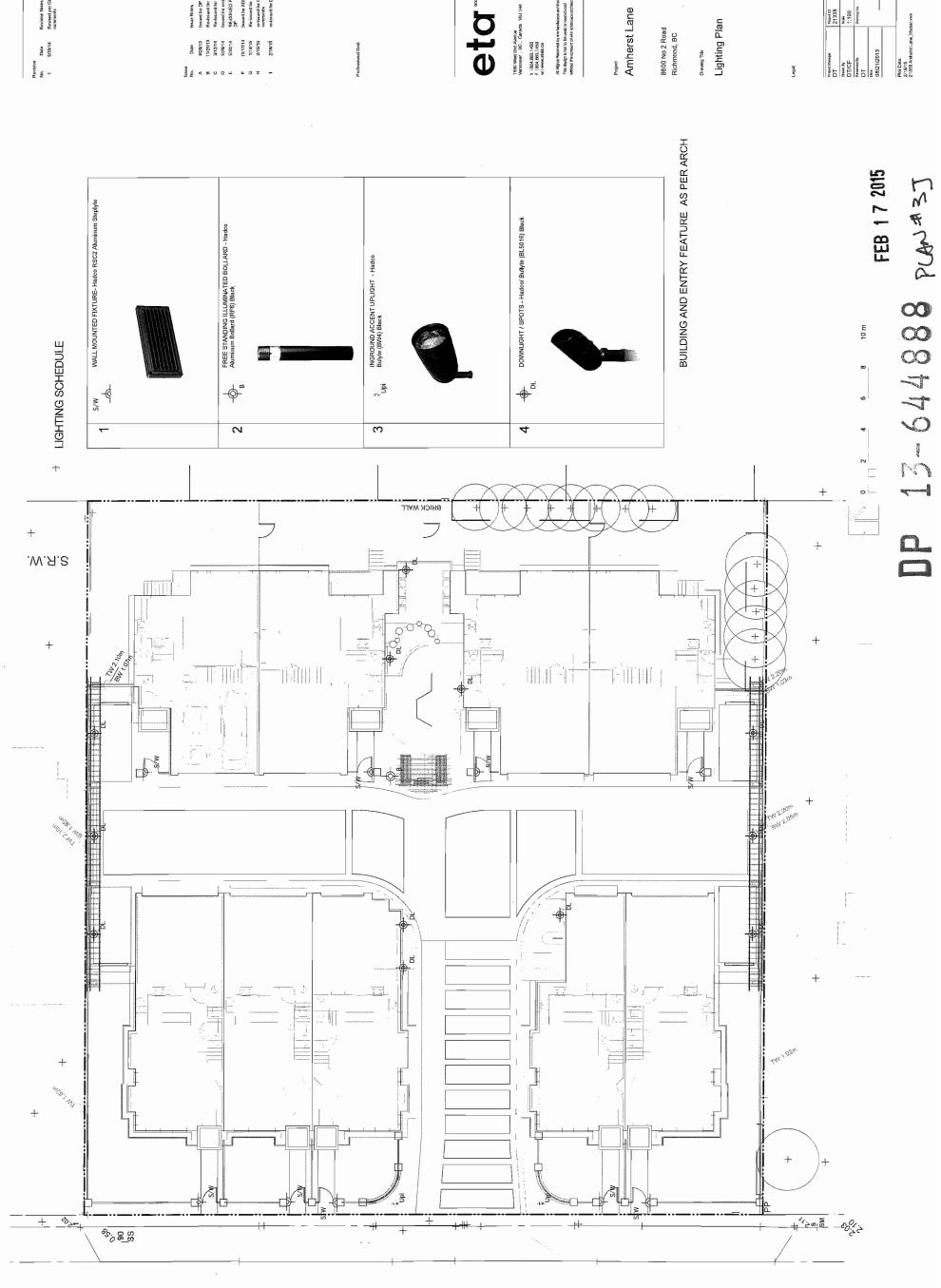
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Project Amherst Lane

8600 No 2 Road Richmond, BC

Cherring Tile
| Mages





























Brick pier and metal fencing





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FOUGERE architecture inc. ENTERTOWNENA-ALPEKTA-WASHINGTON PLAN#04 VANCOUVER, ENT. POLY 125 GUARDEN STATE OF THE STATE O

15-644888 PLAN#4

DP 13-644888 FEBRUARY 11, 2015

BUILDING 1 SCALE 1/8" = 1'-0"

Balandra Developments Inc.

BALANDRA

AMHERST LANE

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WEST ELEVATION



SOUTH ELEVATION

GROUND FLOOR 2.49M 5.81M UNS TRUSS (10.97M) ATTIC FLOOR 8.52M .t/E 0-.8 885 12.25M RIDGE

NORTH ELEVATION

EAST ELEVATION

ELEVATIONS

ELEVATIONS

BUILDING 2 SCALE 1/8" = 1'-0" DP 13-644888





WEST ELEVATION

EAST ELEVATION

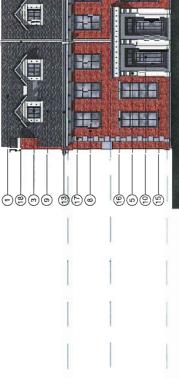


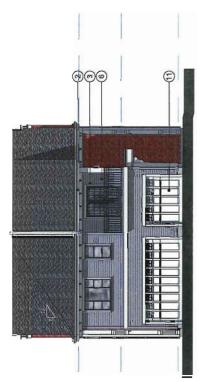


AMHERST LANE











SOUTH ELEVATION

5.81M WS TRUSS (10.97M ATTIC FLOOR 8.52M .8/9 01-,01 10.43m (34"-2 1/2") 1.82M AVERAGE
STIE CHAP (*2-'9E) m10.S1 THDI3H MUMDXAM 12.25M RIDGE

SOUTH WEIGHBOUR BUIDING OUTLINE

相關語

FOUGERE architecture inc. entitier counsets, Aleerth, Washington PLAN#06 202-2425 Quebec Street 604.873.2907

press 4B

15-644888

ELEVATIONS

DP 13-644888 FEBRUARY 11, 2015

BUILDING 3 SCALE 1/8" = 1'-0"

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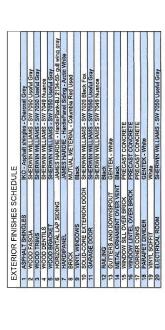
AMHERST LANE 8600 Nº 2 ROAD

Balandra Developments Inc.

BALAND A

SOUTH ELEVATION

EAST ELEVATION



ELEVATION

U/S TRUSS 7.96M UPPER FLOOR 5.50M 1 .b/E 0-,8 .9/S 01-,6 NORTH ELEVATION 8.56m (28*-1") 10.39M

WEST

FOUGERE architecture inc. purits roundara, alberth. WASHWOTON plan#07 202-203 Quebes Street 64 0373 209. Plan#07 Vancouve, BC VST446 fougeranchitecture.a

13-644888 PUN-34C

ELEVATIONS

BUILDING 4 SCALE 1/8" = 1'-0"

AMHERST LANE 8600 Nº 2 ROAD

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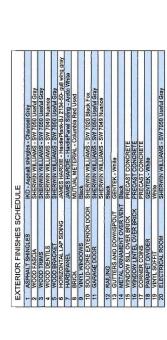
SOUTH ELEVATION

Balandra Developments Inc. 8tm 26829 Hendell Sty. Netword, RC V7C SN9

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EAST ELEVATION



WEST ELEVATION

U/S TRUSS 7.96M UPPER FLOOR 5.50M 8,-0 3/v. .8/5 01-.6 NORTH ELEVATION 8.56m (28'-1") BUILDING HEIGHT 1.82M AVERAGE 10,39M



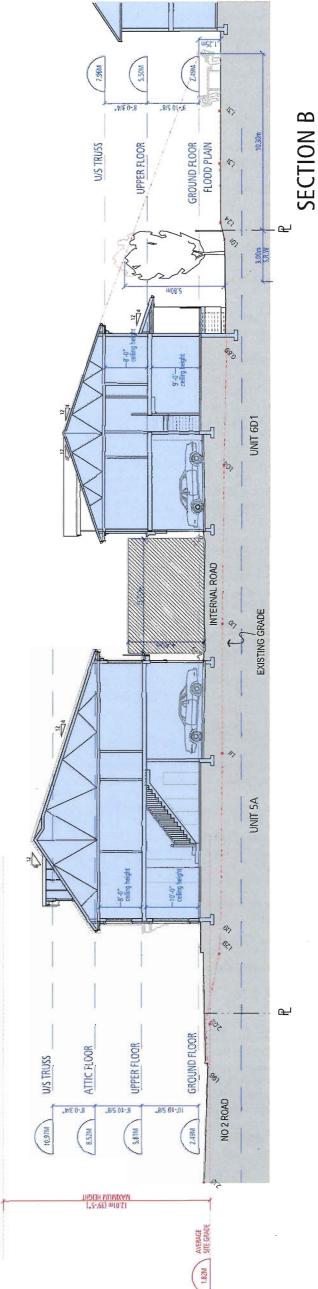
13-644888 PLAN#4D

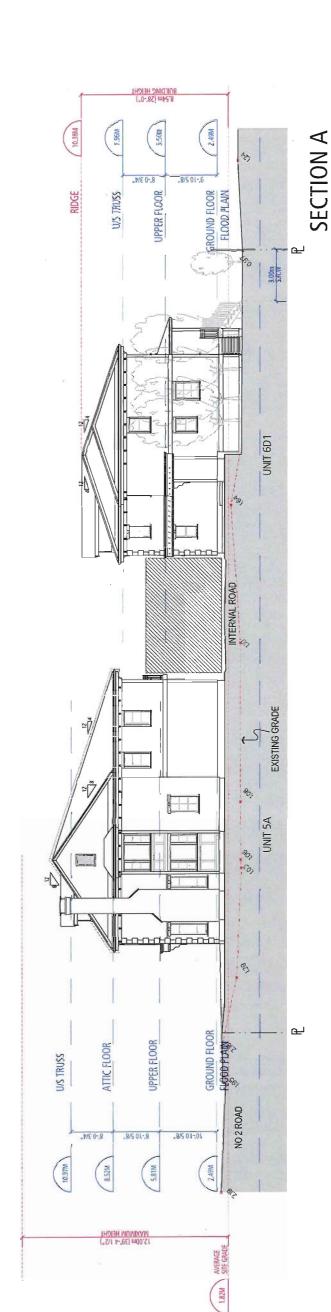
SITE SECTIONS SCALE 1/8" = 1'-0"

AMHERST LANE

BALANDRA

Balandra Developments Inc.







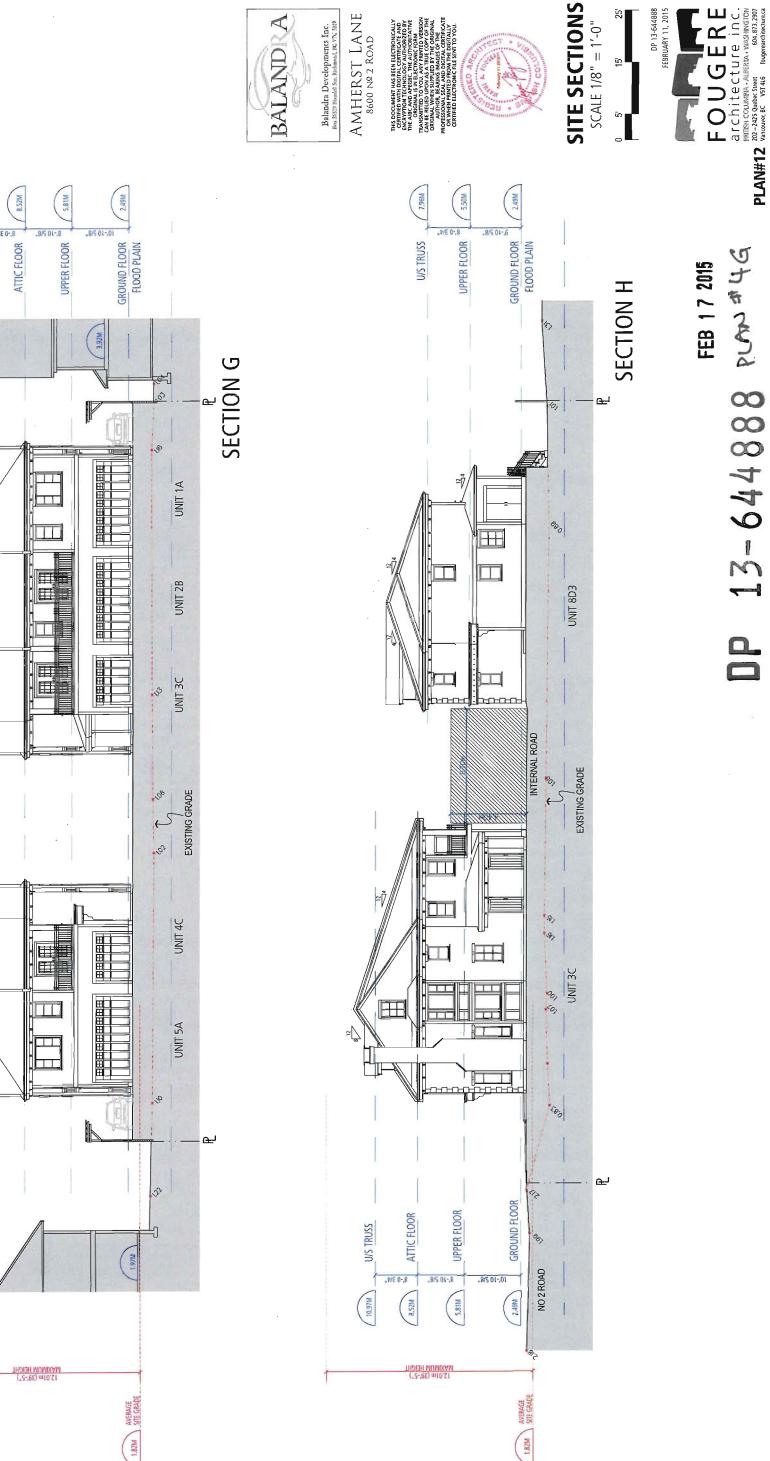
FOUGERE architecture inc.

PLAN#10 Vancouver, 8C V57 44.6 fougerearchirecture.ca

PUNDALE

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U/S TRUSS

13-644888



NO. 2 ROAD STREETSCAPE



REAR PROPERTY LINE YARDSCAPE

13-644888 pun 44 FEB 17 2015



AMHERST LANE 8600 Nº 2 ROAD

STREETSCAPES

DP 13-644888 SCALE 1/16" = 1'-0"

FOUGERE architecture inc.

PLAN#13 Varcouve; BC VST 416 fougerearchitecture.cs



972 sf 907 sf

FLOOR AREA CALCULATION

- Interior Doors to main living areas, 1 bedroom and 1 bedroom min 800 mm clear opening with flush thresholds max. 13 mm height.
- Wall blocking for future installation of grab-bars (toilet, tub and shower)

- Toilet clear floor space min. 1020 mm at side and in front

Summary of the Convertible Unit Features Checklist as Submited

- Entry doors minimum 863 mm clear opening, but ideally 914 mm.

(CONVERTIBLE UNIT) SCALE 1/4" = 1'-0"

DP 13-644888 FEBRUARY 11, 2015

FLOOR PLANS

UNIT 9D3

,516 sf 108 sf 56 sf 0 sf 352 sf Ground Floor Upper Floor Attic Floor Gross SubToTAL
less covered area
SUBTOTAL
less entry / stair
less upper stair
less attic stair
TOTAL NET less garage

future work space (stove, sink & min. 810 mm wide counter)
- 1500 mm turning diameter or turning path diagram
- One window that can be opened with a single hand in the living room
- One window that can be opened with a single hand in one bedroom

pipes (in-wall and in-floor) located clear of under counter area of - Clear area needed under future work space. Plumbing and gas

ır opening. s max.13mm height.

- Interior doors to entry & main living areas, min. 800 mm clear

plus 600 mm on latch side.

(2'8' sliding or 2'10''swinging door spec.) with flush thresholds max. 1-Stair lift (as per manufacturer spec)
- Hallways minimum 900 mm width
- Door from garage to living area minimum 800 mm clear opening.
- Min. clear opening 860 mm clear opening to Patios and Balconies.

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644888

REFERENCE PLAN

FOUGERE architecture inc. Rentsher, evastherious PLAN#08 Vancouver, 65 V57445 (suggesenthickurers)

DP 13-644888

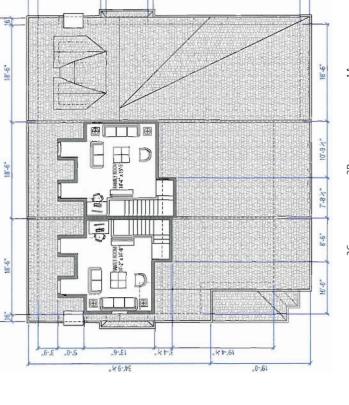
BUILDING 1 SCALE 1/8" = 1'-0"

-0-.04 .6-,5 .0-,5 .9-.EL 44.61 " 16. VE 8-8' x 9-4' 18 to 18.6 Deck 10,3 sm 9.55 18-6

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AMHERST LANE 8600 Nº 2 ROAD

Balandra Developments Inc.

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ROOF PLAN

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2B 1A GROUND FLOOR

30

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Deck 7 sm

13, 10 × °

CD VIEW

UPPER FLOOR

PROVISION OF ACCESSIBILITY FEATURES "AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

- Emry doots minimum 855 mm clear opening (3-0" swinging door spec.)
- Halways minimum 900 mm width
- Halways minimum 900 mm width
- Booking door spec.)
- Booking door spec.)
- Booking to bathrooms for installation of grab-bars (Joliet, tub and shower)
- Provision of lever door handles
- One withdow that can be opened
with a single hand in the living room
- One whitdow that can be opened
with a single hand in one bedroom

- Exterior Walls - 2x6 R22 Construction R value (18.7) Effective R value)
- Rool/Airt - 41.36 Construction R value (27.46 Effective R value)
- Rool/Airt - 41.36 Construction R value (27.46 Effective R value)
- Value Sidon Walls - XIPS R12 Below siab - Windows - Low E Agon 3 glaze U 2.0
- Space Condition - Efectic Beseboard
- Wendiation - HRV
- HOt water Heating - Electric DHW
- Other Energy Impact Features - Power Pipe R3-60 DWHR Building Characteristic Summary:

BLOCK PLANS

DP 13-644888

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AMHERST LANE 8600 Nº 2 ROAD

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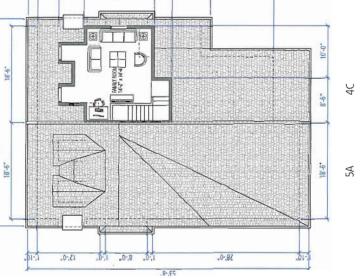
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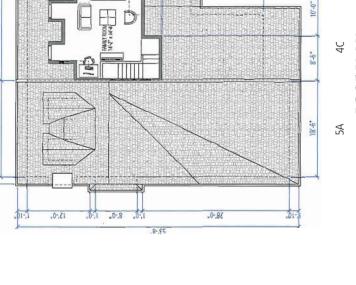


BUILDING 2

SCALE 1/8" = 1'-0"

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BOSON BRANCH BRA

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ROOF PLAN

UPPER FLOOR

9-21

18,-5 **5A**

-0-9

GROUND FLOOR

5A

Deck 7 sm

PROVISION OF ACCESSIBILITY FEATURES "AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

- Enry doors minimum 855 mm clear opening (3-0" swinging door spec.)
- Hallways minimum 900 mm width
- Boor from gaage to living area minimum 2"-10" (swinging door spec.)
- Provision of lever door thandles
- One window that can be opened with a single hand in the living room
- One window that can be opened with a single hand in one bedroom

- Exterior Walls - 2x6 R22 Construction R value (IR.) Tiffercive R value)
- RoofAutric - 41.36 Construction R value (27.36 Effective R value)
- Foundatorigidelow-Grade Walls - XTPS R12 Below slab
- Windows-Low E Argon 3 glaze U. 2.0
- Space Condition - Efectric Baseboard
- Verifiablicon - FIRM
- Hor water Hearing - Bectric OHW
- Other Energy Impact Features - Power Pipe R3-60 DWHR Building Characteristic Summary;

13-644888

BLOCK PLANS

DP 13-644888

BUILDING 3 SCALE 1/8" = 1'-0"

REFERENCE PLAN

ATTIC PLAN 7D2

11,11

1511

6.6

GROUND FLOOR

6D1

7D2

AMHERST LANE 8600 NR 2 ROAD

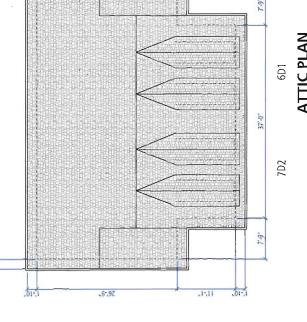
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BALANDRA



UPPER FLOOR

7D2

Building Characteristic Summary: PROVISION OF ACCESSIBILITY FEATURES "AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

- Enny doors minimum 855 mm clear opening (3-0" swinging door spec.)
- However, while and spec and swinging door spec.)
- Brocking to bathorons for installation of gab-bass (toiler, up and shower)
- Provision of lever door handles
- One window that can be opened
with a single hand in the living room
- One window that can be opened
with a single hand in one bedroom

- Exterior Walk - 2x6 R22 Construction R value (18.71 Effective R value)
- RooffAttic - 41.36 Construction R value (27.46 Effective R value)
- Foundianon/Below-Grade Walk - XTPS R12 Below slab
- Foundianon/Below-Grade Walk - XTPS R12 Below slab
- Windows - Low E Argon 3 glaze U 2.0
- Space Condition - Efectric Baseboard
- Ventiation - HRV
- Hot water Heating - Efectric DHW
- Other Energy Impact Features - Power Pipe R3-60 DWHR

15-644888 **BLOCK PLANS**

DP 13-644888

REFERENCE PLAN

13-644888

BUILDING 4 SCALE 1/8" = 1'-0"

AMHERST LANE 8600 Nº 2 ROAD

Balandra Developments Inc.

901

ATTIC PLAN

UPPER FLOOR

901

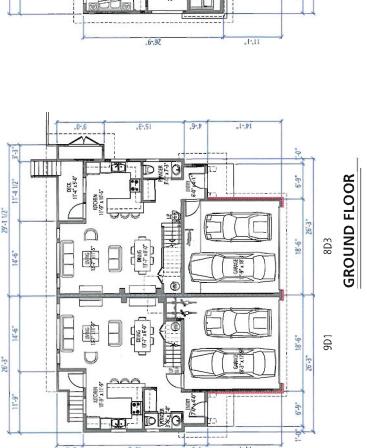
PROVISION OF ACCESSIBILITY FEATURES "AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

Building Characteristic Summary:

- Enry doors minimum 855 mm clear opening (3-0" swinging door spec.)
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BLOCK PLANS



56.3

.6-97

10-6' x9-0



Report to Development Permit Panel

To:

Development Permit Panel

Date:

March 4, 2015

From:

Wayne Craig

File:

DP 14-658285

Director of Development

Re:

Application by Western Gardenia Garden Holdings Ltd. for a Development Permit

at 7571 and 7591 St. Albans Road

Staff Recommendation

That a Development Permit be issued which would:

- 1. Permit the construction of sixteen (16) three storey townhouse units on a consolidated lot including 7571 and 7591 St. Albans on a site zoned "High Density Townhouse (RTH4)"; and
- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - a) Reduce the minimum building side yard setback from 4.5 m to 3.9 m at the southeastern corner of the building;
 - b) Increase the maximum bay window projection from 0.6 m to 0.9 m to the south property line adjacent to Jones Road; and
 - c) Allow seven (7) small car parking stalls at the site.

Wayne Craig

Director of Development

WC:dcb

Staff Report

Origin

Western Gardenia Garden Holdings Ltd. has applied to the City of Richmond for permission to develop 16 three storey townhouse units at 7571 and 7591 St. Albans Road on a site zoned "High Density Townhouse (RTH4)". The site currently contains two single family residential homes.

The site is being consolidated and rezoned from "Single Detached RS1/E)" to "High Density Townhouse (RTH4)" for this project under Bylaw 9189 (RZ 14-658284). The Bylaw received third reading at the Public Hearing on December 22, 2014.

A Servicing Agreement for offsite improvements (e.g. sidewalks, boulevards, lighting, utility installations, etc) along both Jones Road and St. Albans Road frontages is required prior to Building Permit issuance.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Surrounding Development

Development surrounding the subject site is as follows:

To the North: A two and one half storey townhouse development on a lot zoned "Town Housing

(ZT45) - Gilbert Road, Acheson - Bennett Sub-Area, St. Albans Sub-Area, South

McLennan (City Centre)".

To the East: A 23 unit three storey townhouse development on a lot zoned "High Density

Townhouses (RTH4)".

To the South: A four storey apartment building on a lot zoned "Medium Density Low Rise

Apartments (RAM1)" located on the south-western corner of Jones Road and St. Albans Road. On the south-eastern corner of Jones Road and St. Albans Road is

a 3 storey, 10 unit townhouse complex.

To the West: A four storey apartment building on a lot zoned "Medium Density Low Rise

Apartments (RAM1)". General Currie School and park space are located just to the west at 8191 Jones Road/8220 General Currie Road (approximately 400 m

away).

Background

The proposed development site is located at the north-west corner of St. Albans Road and Jones Road in the City Centre area. The consolidated site will be approximately 0.475 acres in size.

The applicant's proposal is to build a 1,626.36 m² (17,506 ft² net) medium density, ground-oriented, 3 storey townhouse project on the consolidated lot. The development will contain

16 dwellings ranging in size from 97.5 m² (1,054 ft²) to 140.5 m² (1,512 ft²). The units are proposed to be a mix of two, three and four bedroom dwellings. All of the units will have two areas of private outdoor space: at grade and on the proposed elevated podium overtop the parking structure. A 114 m² (1,227.09 ft²) shared outdoor amenity space will be located on the elevated podium. The parking area will have a common gated vehicle entrance, and each unit having its own garage space with individual garage doors inside the parkade.

Ten of the homes will have direct pedestrian access from either St. Albans Road or Jones Road. Six homes will have pedestrian access from an asphalt walkway that will run along the site's western property boundary leading to Jones Road. A single vehicle access will be provided from Jones Road on the south property line.

The grade of the site will be kept close to the existing grade (i.e. approximately two feet below the adjacent public sidewalk) to facilitate the retention of a row of large trees within the St. Albans and Jones Road frontages partially on the lot and partially on City lands.

Related Policies & Studies

Official Community Plan and the St. Albans Sub-Area Plan

The Official Community Plan designates the subject properties as "Neighbourhood Residential" (i.e. areas where the principal uses are single family, two-family and multiple family housing [specifically townhouses]).

The St. Albans Sub-Area of the City Centre, designates the properties for "Multi-Family Low Rise (3 Storey apartments, Townhouses, Two-Family or Single-Family Dwellings)".

The proposed two storey over parking (effectively 3 storey) townhouse development will comply with both the OCP and the St. Albans Sub-Area Plan land use designations.

Rezoning and Public Hearing Results

During the rezoning process, staff identified the following design issues to be resolved at the Development Permit stage:

- Confirmation that the proposed design generally meets the Development Permit guidelines (the applicant has submitted a complete checklist showing how the guidelines have been addressed this was submitted to the Advisory Design Panel for review);
- Ensuring that the landscaping plan addresses all the tree replacement requirements as per the Official Community Plan (an Arborist's report indicates 21 trees being removed, the landscape plan attached to this application shows 56 new trees to be planted exceeding the OCP's 2:1 replacement criteria);
- Submission of an acceptable report by a Certified Energy Advisor prior to the formal review by the Development Permit Panel (a report prepared by DW Energy Advisors has been submitted to the City);
- Confirmation of the site grading to allow preservation of the retained trees (*The submitted site plan shows grade elevations will be retained in the vicinity of the trees. An Arborist has also been retained and has been working with Parks staff to ensure impacts to the trees to be retained will be minimized*);

- Confirmation of the aging in place and convertible unit elements are incorporated into the Development Permit plans (aging in place features are shown on the plan sets); and
- Detailing of all sustainability measures incorporated into the design (a list of sustainability features proposed is provided later in this report).

The Public Hearing for the rezoning of this site was held on December 22, 2014. At the Public Hearing, the following concerns about rezoning the property were expressed:

A resident from the south side of St. Albans Road expressed concerns for the anticipated impact upon their views of the mountains, perceived devaluation of their property and effects of construction noise on their personal health.

A resident from an adjacent property to the west raised concerns regarding increases to the existing traffic congestion in the area, obstruction of the skyline and mountain views and infringement on the privacy of adjacent property owners.

Zoning Compliance/Variances (staff comments in bold)

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan and is generally in compliance with the "High Density Townhouse (RTH4)" zone except for the zoning variances noted below.

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1. Reduce the minimum building side yard setback from 4.5 m to 3.9 m at the southeastern corner of the building.
 - At the southeastern corner of the site the building will be closer to the property line after the required road dedication of a 4 by 4 corner cut adjacent to the St. Albans Road and Jones Road intersection. Staff support the variance as it is a function of the road dedication and will not result in an obstruction to visibility for motorists.
- 2. Increase the maximum bay window projection from 0.6 m to 0.9 m at the southern side of the building adjacent to Jones Road.
 - Staff support the proposed variance as it will apply only to the upper floor bay windows for 3 units (14D, 15B6 and 16E). The proposed building will be located parallel to St. Albans Road but Jones Road tapers slightly northward as it moves away from the St. Albans Road intersection resulting in the variance. The variance does not impact any pedestrian circulation and provides for additional building articulation along the Jones Road frontage.
- 3. Vary the Zoning Bylaw, to allow 7 small car parking stalls at the site.
 - Staff support the proposed variance as two of these stalls are considered surplus stalls as the proponent is providing more stalls than required by the Bylaw. Overall, this project will provide 29 parking stalls. The variance is required because the 16 unit development will require less than 31 total vehicle stalls. The Zoning Bylaw allows up to 50% small car stalls if 31 or more stalls are provided. The seven small car stalls proposed represent less than 25% of the overall total number of stalls.

Urban Design Response

Advisory Design Panel Comments

The Advisory Design Panel was supportive of the proposed project. A copy of the relevant excerpt from the Advisory Design Panel Minutes from December 18, 2014 is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in 'bold italics'. The applicant has addressed these suggestions to the extent recommended by staff.

Conditions of Adjacency

- Both the form and massing of the proposed development are consistent with the existing surrounding development which is comprised of three and four storey townhouses and apartments.
- Vegetation planting strips are proposed to incorporate a variety of shrubs species along most of the site's perimeter creating a soft edge to the project.
- Six foot high cedar fencing is proposed along both the western and northern property boundaries providing a degree of privacy for adjacent properties and the future owners of this development.
- A total of 7 significant trees are proposed to be protected and retained on this site. Four of these are very large trees located prominently along the south eastern edge of the site. These trees are a significant component of the neighbourhood and their retention is considered an important amenity contribution to the area.
- Dwelling units fronting onto Jones Road or St. Albans have sidewalks leading directly to the street contributing to a more pedestrian friendly streetscapes.
- As the project is located within the City Centre, the standard requirement is typically for a 2.0 m wide public sidewalk and a 1.5 m wide grass and treed boulevard along all road frontages. The public frontage sidewalk will be allowed to meander to minimize impacts upon the trees being preserved. The exact details will be determined through the Servicing Agreement submission at Building Permit stage. Similar meandering is evident across the street along the east side of St. Albans along the frontage of 8399 Jones Road. The grass and tree boulevard in front of the subject site will be adjusted as needed to accommodate the changes in the sidewalk alignment.

Site and Functional Planning

- The vehicle access to the site has been placed as far west as practical to ensure proper clearance from the Jones Road / St. Albans intersection.
- Visitor parking is located adjacent to the entrance to the site and easily located. This parking area also includes one handicapped parking space.
- Individual unit parking garages are all secured with individual garage doors.
- Garbage and recycling areas have been designed to be fully accessible with double doors for easy entry.
- A 1.2 m wide common walkway is proposed along the west side of the complex to provide access to all the units along the western side of the building and also leads to one of the two stair accesses to the upper podium level.

• The podium level provides "at grade" access to the common open space and play areas from the second level all the units. The Advisory Design Panel felt that the use of the upper podium space will serve the residents very well and makes particularly good use of the limited space on the site as a whole. Being raised above the ground level there will be less shading and more natural light for the residents using this area.

Architectural Form and Character

- Viewed from the eastern side, the proposed building mass will give the appearance of three buildings with space separations apparent for the second and third floors above the podium level.
- The building massing is articulated by variations in the roof design, the use of gable ends and shed roofing over projected areas. Window and box projections are also employed along the building sides breaking up what would otherwise be large vertical planes.
- At grade, stone veneer finishes, vertical board and batten and solid core doors with glazing elements are proposed. The upper two storey elevations are proposed to include hardie plank siding, board and batten, "Nichiha" architectural block, wood trims and wood facia. Paint colors include Boothbay Blue, Serape, Smoky Blue for main elements and Natural Tan colors for trims and facia boards and "Gale Force" gray colors for the vinyl windows. Asphalt shingles are proposed to be a pewter gray color. Extensive glazing is proposed for both the second and third floors. The net result is clear horizontal definition between floors and a distinctive frontage appearance that should fit well with the neighbouring developments.

Landscaping Form and Character

- This project's design is very much oriented to retention of the significant trees along the Jones Road / St. Albans frontage. These trees will continue to lend a presence for both this project and the community as a whole. An arborist has been retained to ensure that both the on-site and off-site works in the vicinity of the trees will not be detrimental to the survival of these trees.
- The proposed landscape design achieves the required City Centre Area Plan's requirement for an additional 10% landscape area providing a total of 196 m² (2,110 ft²) of outdoor landscape area in addition to the standard of 6.0 m² (64.59 ft²) per unit landscape area requirements. The total proposed outdoor space area with this design is 310 m² (3336.8 ft²).
- Per the Arborist's report (Michael Mills Consulting, dated October 8, 2014) fourteen of the site's 21 trees will be removed. The seven retained trees are large trees in good condition. A total of 56 new trees will be added to the site more than meeting the two for one replacement requirement of the Official Community Plan. Replacement trees will consist of a five varieties of maple trees and a Japanese Stewartia species.
- Significant shrub and ground cover plantings are proposed for the combined at grade and upper podium planting areas. The majority of the 24 species proposed for inclusion are bushy plants and many are flowering varieties.
- Permeable pavers, gravel, Turfpavers and raised wooden boardwalks are used in various locations allowing natural infiltration in areas which might otherwise contain impervious materials. The boardwalks are proposed in areas which might otherwise negatively impact the tree roots of the retained existing trees rather than concrete or asphalt surfaces.

- A 1.8 m (6 ft) foot high cedar fencing is proposed along both the western and northern property boundaries providing a degree of privacy for adjacent properties and the future owners of this development. Fencing materials are proposed to consist of 4x4 posts, 2 x 4 and 1 x 4 horizontal elements.
- An existing swale that runs between the proposed units and the frontage sidewalk from unit 11 to unit 15 is proposed to be enhanced with native and naturalized planting.
- On the upper amenity level the green space includes a bermed planting area for trees, open lawn areas and a children's play area.
- The children's play area is proposed to contain a .9 m (3 ft) wide concrete circular path, resilient play surfaces, a concrete climbing wall with a green-screen arbour and a concrete seating wall for care givers.
- Screens are proposed between unit patios to allow privacy while the bermed planting will be strategically located to give privacy across the courtyard.
- Cedar arbors are located at each unit's entry pathway and at the entrance to the common pathway at the western side of the project.
- In response to the Advisory Design Panel's comments, two feature trees are proposed to be located in flanking positions adjacent to the vehicle entry and the main pedestrian entry off St. Albans Road.
- A landscape security in the amount of \$198,490.26 has been included in the Development Permit Considerations required to be submitted prior to Council review of the application.

Parking, Loading and Waste Collection

- Twenty nine (29) parking stalls are proposed including 18 standard residential parking stalls, 7 small residential stalls and 4 visitor stalls. One of the four outside visitor parking stalls also serves as a handicapped stall.
- Only four stalls (in two units) are in a tandem configuration with all the remaining stalls being either side by side or single car stalls.
- All the interior garages have individual garage doors for security and privacy.
- The project will also provide the required number of Class 1 (20 inside) and Class 2 (4 outside) bicycle stalls. An outdoor bicycle rack is located near the driveway entrance to the project.
- Garbage and food scraps containers are located in an open roof enclosure just to the west of the vehicle entrance. Landscaping and a feature tree will provide screening of the enclosure. A recycling centre is built into the south-west corner of the building. Double doors will allow full access to the facility.
- Registration of a covenant to ensure that the building's main vehicle gate is kept open during the day time to allow the service vehicle to partially enter the parkade to turn around is a requirement of the Development Permit issuance.

Crime Prevention through Environmental Design (CPTED)

- A lighting design will be provided for the project (as indicated in the plan submission) that will provide lighting to common area such as the western pedestrian pathway, each of the entry gates, the open space on top of the podium, the mail box facility and to the two stairways leading to the podium.
- All entrances are visible and overlooked by pedestrians or by neighbour's windows.

- The mail room is located just inside the eastern stairway leading to the upper podium.
- A security gate will prevent access to the parkade area at night time.
- Gates are provided for each unit's entry pathway and to the common pathway along the western side of the site to differentiate the private/semi private space from the public walkways along the street frontages.
- Stairs to the upper podium are gated to the public.
- Each unit's garage is equipped with its own garage door providing security and privacy. The Rezoning Considerations included a covenant to prohibit garages from being converted into habitable space.

Accessible Housing

- The proposed development includes one convertible unit designed with the potential to be easily renovated to accommodate a future resident in a wheelchair. The potential conversion of these units will require installation of a stair mounted wheelchair lift mechanism on each floor. Backing for these mechanisms will be installed during construction.
- All of the proposed units incorporate aging in place features to accommodate mobility constraints associated with aging. These features include:
 - stairwell hand rails;
 - entry doors minimum 855 clear opening
 - hallways minimum 900 mm width
 - one window that can be opened with a single hand in the living room and in one bedroom
 - lever-type handles for plumbing fixtures and door handles; and
 - solid blocking in washroom walls to facilitate future grab bar installation beside toilets, bathtubs and showers.

Servicing Agreements/Engineering

A Servicing Agreement application has not yet been submitted for this project but will be required prior to Building Permit issuance in order for the proposed project to be adequately serviced. The off-site improvements identified through the Rezoning application include a new water service connection, upgrading of the existing storm sewer, replacement of existing inspection chambers, service connections and lawn basins, sidewalk and treed boulevard installations along both St. Albans Road and Jones Road. The exact specifications and alignment well be determined through the forth coming Servicing Agreement.

Sustainability

The applicant has submitted a full Hot 2000 report prepared by DW Energy Advisors (report dated September 24, 2014) which indicates that the project design will achieve the EnerGuide 82 rating and the Architect has advised that all the units will be pre-ducted for solar hot water heating.

In addition to the EnerGuide 82 response, the proponent has identified a number of elements they proposed to incorporate into the development to improve the overall sustainability of the project including:

• construction of a landscaped roof and open space over portions of the parkade;

- preservation of on-site mature trees (the building design and Flood Construction Level [FCL] have been adjusted to preserve these trees);
- low V.O.C. paints on interior spaces;
- use of durable, low maintenance surfaces on building exteriors;
- locating buildings and windows to maximize natural light and ventilation;
- use of permeable surfaces wherever possible (e.g., interlocking grass pave at the visitor parking area);
- the buildings are located and windows placed to maximize natural light and ventilation;
- incorporating "energy star" appliances and light bulbs in each unit; and
- use of drought tolerant and native plants plus high efficiency climate-based irrigation systems are proposed within the site's landscaping.

Conclusions

The proposed 16 unit townhouse project for the north-western corner of St. Albans and Jones Road has been well thought out, well designed and should be an attractive complement to the surround developments. The project has place considerable emphasis on the preservation of existing mature trees and commitments have been made to ensure the highest possibility of their survival. Staff recommend approval of the application by Western Gardenia Garden Holdings Ltd. for the proposed Gardenia Gardens project at 7571 / 7591 St. Albans Road.

David Brownlee

Planner 2

DCB:cas

The following are to be met prior to forwarding this application to Council for approval:

- Final adoption of the Zoning Amendment Bylaw No. 9189.
- Registration of a legal agreement on title requiring the main parkade gate to remain open or unlocked during daytime (7:00 am to 7:00 pm) hours to allow service vehicle to turn around in the complex; and
- Receipt of a Letter-of-Credit for landscaping in the amount of \$198,490.26.
- Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
- Registration of a legal agreement on title ensuring that car stalls in a tandem arrangement are assigned to the same dwelling unit.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.
- Incorporation of accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (http://www.richmond.ca/services/ttp/special.htm).
- If applicable, payment of latecomer agreement charges associated with eligible latecomer works.



Development Application Data Sheet

Development Applications Division

Attachment 1 DP 14-658285

Address: 7571 and 7591 St. Albans Road

Western Gardenia Garden

Applicant: Western Gardenia Garden Holdings Ltd. Owner: Holdings Ltd.

Planning Area(s): City Centre - St. Albans Sub Area Plan 2.10 A

Floor Area Gross: 1,818.67 m² (19,576 ft²) gross Floor Area Net: 1,609.08 m2 (17,320 ft²) net

	Existing	Proposed
Site Area:	1, 930 m ² (20,774 ft ²)	1,922 m ² (20,687 ft ²) after corner cut dedication
Land Uses:	Single-Family Residential	Multi-Family Residential
OCP Designation:	Neighbourhood Residential	No Change
Area Plan Designation:	Multi-Family Low Rise (3 Storey apts., Townhouses, Two-Family or Single- Family Dwellings)	No Change
Zoning:	Single Detached (RS1/E)	High Density Townhouses (RTH4)
Number of Units:	2	16
Other Designations: FCL	2.9m GSC – or 0.3 m above crown of adjacent road per Area A	Same. Proposed 1.59 m GSC.

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.90 with contribution to affordable housing reserve	0.84 with contribution to affordable housing reserve	none permitted
Lot Coverage: Buildings	Max. 45%	44.2%	None
Lot Coverage – Non-Porous Surface	Max. 70%	68%	None
Lot Coverage – Landscaping live plant material	Min. 20%	22%	None
Setback – Front Yard (m): St. Albans Road	Min. 4.5 m Max. 1.0 m projection of bay window	3.9 m Min. to SE building corner. Rest of the building will be 4.5 m; 3.6 to the bay window	Variance for SE building corner to 3.9 m due to corner cut (RTH4)
Setback – Exterior Side, Road setback : Jones Road (m):	Min. 4.5 m Max. 0.6 m bay window projection	Min. 4.5 m to building 3.66 m to bay window. Max. 0.9 m to bay window	Variance for bay window projection (0.28 m)

	Bylaw Requirement	Proposed	Variance
Setback – Interior Side Yard (m):	Min. 2.0 m Max. 0.6 m bay window projection	2.0 m to building 1.45 m to bay window	None
Setback – Rear Yard (m):	Min. 2.0 m Max. 0.6 m bay window projection	Min. 5.5 m to building Min. 4.59 m to bay window	None
Height (m):	12.0 m (3 storeys)	11.03 m (3 storeys)	None
Lot Size (min. dimensions):	Min 20 m wide x 30m deep Min. Area 600 m²	40.12 m wide by 48.46m deep Area 1,922 m² (Net)	None
Off-street Parking Spaces – Regular (R) / Visitor (V):	1.4 (23 R) and 0.2 (4 V) per unit	1.625 (25 R) and 0.2 (4 V) per unit	None
Off-street Parking Spaces - Total:	27	29	None
Off-street Parking Spaces - Accessible	1	1	None
Tandem Parking Spaces:	Permitted	4 (2 units)	None
Small Car Parking Spaces	0	7	Variance to Sec 7.5.13
Onsite Bicycle Parking Facilities – Class 1	Class 1 (indoor) 1.25 per unit = 20 stalls	Class 1 (indoor) 1.25 per unit = 20 stalls (6 vertical & 14 horizontal stalls)	None
Onsite Bicycle Parking Facilities – Class 2	Class 1 (outdoor) 0.2 per unit = 4 stalls	Class 1 (outdoor) 0.2 per unit = 4 stalls	None
Amenity Space – Indoor:	50 m ²	\$16,000 Cash-in-lieu	None
Amenity Space – Outdoor:	6.0 m ² per unit	7.875 m² per unit = 126 m²	None
CCAP 10% Additional Landscaping Requirement	192 m²	196 m²	None

Other: Tree replacement compensation required for loss of significant trees.

Excerpt from the Minutes from The Design Panel Meeting

Wednesday, December 18, 2014 – 4:00 p.m. Rm. M.1.003 Richmond City Hall

CARRIED

1. DP 14-658285 - 16 THREE-STOREY TOWNHOUSES

APPLICANT:

Western Gardenia Garden Holdings

Ltd. PROPERTY LOCATION:

7571 and 7591 St. Albans

Road Applicant's Presentation

Architect Wayne Fougere, Fougere Architecture Inc., and Landscape Architect Daryl Tyacke, ETA Landscape Architecture Inc., presented the project and answered queries from the Panel on behalf of the applicant.

Panel Discussion

Comments from the Panel were as follows:

- like the project; applicant used a different approach for the type of site where the subject development is located; lifting the plane one level up is beneficial to residents, e.g. more daylight exposure and privacy of outdoor amenity area;
 - No Comment
- appreciate the proposed materials and contemporary approach to the architecture; however, the three garbage and recycling elements on the ground floor are spread out; consider integrating these elements to enhance the appearance of the entry to the proposed development; Please refer to attached letter from property Management Company that confirms separating garbage and recycling causes no problem for the residents and operators who are handling the pick-up.
- Good project; applicant gave attention to details to ensure that the proposed development fits well into the neighbourhood;
 No Comment
- appreciate the quality of glazing; however, consider introducing opaque elements into the bay window to minimize the energy impact to the proposed development;

Based on the proposed window design, we will achieve Energuide 82.

- like the design of the convertible unit; appreciate the pocket door in the ensuite bathroom on the upper floor; also consider a pocket door for the powder room on the main floor in lieu of a swinging door;
 There are not enough side walls for switching to a pocket door in the powder.
- consider enlarging the transfer areas for the chair lift; The circulation of the stair lift has been revised.
- consider a future location for a vertical lift in the convertible unit; look at the parking space on the ground floor adjacent to the living area as a potential location;
 - There is not enough space in the garage to provide a vertical lift.
- consider locating the laundry on the same floor as one of the bedrooms; also consider introducing a PVC pipe to facilitate the transport of things from the upper floor to other floors/areas in the convertible unit:
 - We explored various options but in the end prefer laundry on the main floor. Please see attached plan# 15.
- a successful project from a landscape perspective;
 No Comment
- garbage and recycling elements at the entry point on grade need further design development; could be shifted and clustered together; also consider bigger and taller plant materials to soften the frontage and provide more screening;
 - Please refer to attached letter from property Management Company that confirms separating garbage and recycling causes no problem for the residents and operators who are handling the pick-up. We explored various options and the proposed approved by Recycling and Garbage staff. Feature trees added at the entry. Please see attached plan#3b.
- introduction of TurfPave is an innovative approach for the grassed area on the podium; however, minimize its use and introduce pavers and/or stepping stones so that residents can avoid walking on wet ground during rainy days; also consider a regular lawn in lieu of TurfPave in some areas;

Because the area is so small, the potential for excessive wear and tear on the turf seemed great. As a means of minimizing damage to it, the decision was made to use TurfPave throughout. Maintenance will be easier with reduced compaction requiring aerating.

Paving stones have been added across the amenity garden connecting the two staircases

 appreciate the planting islands on the courtyard which provide screening for townhouse units from other units directly across; consider introducing green screen for exposed units;

No Comment

appreciate the form and landscaping of the proposed development; like the materiality and finishes; appreciate the different approach for the outdoor amenity space; applicant was able to fit in a lot in a tight site; however, ensure adequate lighting at the narrow stairs to address potential CPTED concerns;

The distance between two buildings is 6' and the stairs end up to open area at each end with adequate light and they will are gated to the public and used by the residence only.

- the project is well-designed and efficient;
 No Comment
- consider further design development to garbage and recycling;
 We explored various options and the proposed approved by Recycling and Garbage staff.
- ensure adequate lighting at the internal drive aisle; explore innovative ways to introduce natural lighting;

In our opinion the addition of daylighting into the drive aisle, other than that already afforded by the open ends, will be ineffectual. The level of lighting required in these spaces even during the day will far exceed that provided by light tubes, for example. The small amount of natural light entering the parkade will not be noticed above the ambient artificial light. The only way to make an impact would be to open large areas of the parkade roof-minimizing an already tight amenity space above.

 applicant is encouraged to introduce public art in the proposed development; public art pieces could be integrated at the outdoor amenity space on podium level;

The client is going to cash-in-Lieu.

 appreciate the raised podium; provides parking and adequate outdoor amenity space in a tight site; a good precedent for future projects on a similar site;

No Comment

appreciate the forms, roofs and cross sections; however, the architecture appears plain and confused as it is a mix of heritage and modern architecture; consider a more unique and modern architecture to make it more exciting;

Exterior design blends traditional and modern elements so as to provide interest while maintaining local flavor.

- applicant should pay more attention to the entry sequence on grade; look at opportunities to reorganize; agree with comment to explore the possibility of planting bigger and taller trees at the frontage and utilizing structural soil cells; should consider a specimen tree to pay homage to the fir tree to be removed;
 - Feature trees added at the site entry.
- look at synergy and efficiency of garbage and recycling lay-outs; and Please refer to attached letter from property Management Company that confirms separating garbage and recycling causes no problem for the residents and operators who are handling the pick-up.
- good approach for a tricky site; applicant needs to address potential CPTED issues, e.g. lighting at the internal drive aisle and security for vehicle entrance gate.
 - At the BP stage, electrical engineer will design lighting that provides a well illuminate drive aisle.

Panel Decision

It was moved and seconded

That DP 14-658285 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Panel.

CARRIED



Development Permit

No. DP 14-658285

To the Holder:

Western Gardenia Garden Holdings Ltd.

Property Address:

7571 and 7591 St. Albans Road

Address:

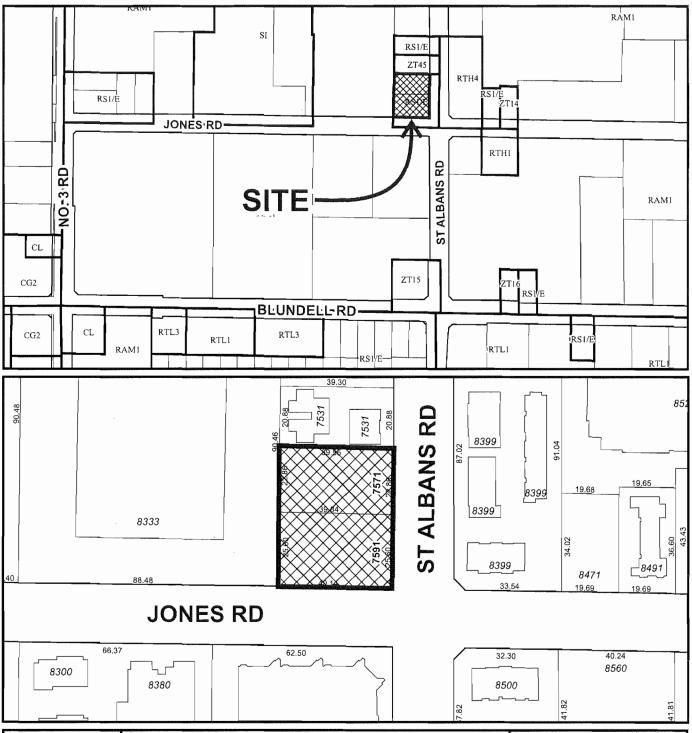
202 – 2425 Quebec Street Vancouver, BC V5T 4L6

- 1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
- 2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - a. Reduce the minimum building side yard setback from 4.5 m to 3.9 m at the southeastern corner of the building;
 - b. Increase the maximum bay window projection from 0.6 m to 0.9 m at the southern side of the building adjacent to Jones Road; and
 - c. Vary the Zoning Bylaw, to allow seven (7) small car parking stalls at the site.
- 4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #28 attached hereto.
- 5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
- 6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$198,490.26 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
- 7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

No. DP 14-658285

To the Holder:	Western Garder	nia Garden Holdings Ltd.
Property Address:	7571 and 7591	St. Albans Road
Address:	202 – 2425 Que Vancouver, BC	
	ons of this Permit ar	ped generally in accordance with the terms and and any plans and specifications attached to this
This Permit is not a Bu	ilding Permit.	
AUTHORIZING RESOLU DAY OF ,	JTION NO.	ISSUED BY THE COUNCIL THE
DELIVERED THIS	DAY OF	,
MAYOR		







DP 14-658285 SCHEDULE "A"

Original Date: 03/21/14

Revision Date: 02/06/15

Note: Dimensions are in METRES

GARDENIA GARDENS 7571-7591 St Albans Rd

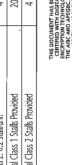


DP 14-658285 JANUARY 23, 2015	E	OUGERE	chitecture inc.
		PO	arch

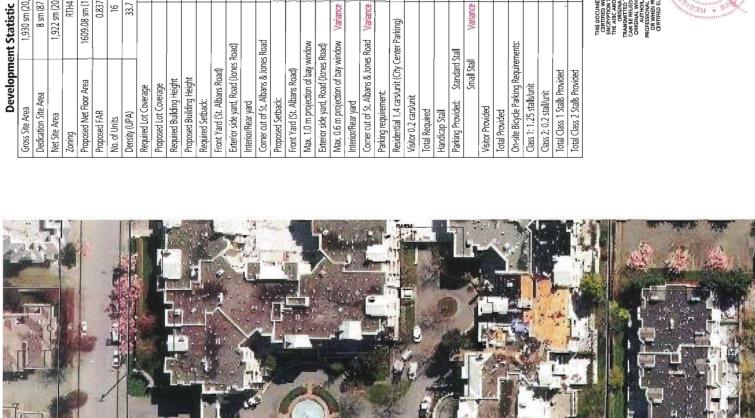
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s 2: 0.2 stall/unit	4
il Class 1 Stalls Provided	20
Il Class 2 Stalls Provided	4
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07
lass 2 Stalls Provided



Total Class 1 Stalls Provided ,
al Class 1 Stalls Provided



STALBANS RD

11.03 m 12.00 m

1609.08 sm (17,320 sf)

CURRIERD

GENERAL

0.837 16 33.7

1,930 sm (20,774 sf)

1,922 sm (20,687 sf) 8 sm (87 sf)

RTH4

4.50m 4.50m

4.50m 3.58m

3.624m



K. exerciaca

CONTEXT PLAN SCALE 1" = 40'-0" Maximum FAR Proposed FAR



GARDENIA GARDENS

FOUGERRE architecture inc. BRITCH COLUMBACA, SALBERTA, AMASHUSTON 00 202-243 Quebec Street 604-243 Quebec Street 604-243 Quebec Street 604-243 Quebec Street 7571-7591 St Albans Rd DP 14-658285 JANUARY 23, 2015

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Plan#

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ũ	INCITA ILLO AND A DOMESTICAL	NO.	Current Address	7571 - 7591	7571 - 7591 St. Albans Road		Current Zoning	Zoning	RS1/E	Gross Site Area:	20,774 sf (1,930 sm)	(ms 0£6'		
22	IN AREA CALCULAI	ON	Proposed Address	8383 Jones Road	oad		Proposed Zoning	d Zoning	RTH4	Net Site Area:	20,687 sf (1,922 sm)	,922 sm)		
-		Ground	Main	Upper	Unit		Total		Ground Floor	Main Floor	Upper Floor	Unit Net	ب	Tota
	Unit Type	Gross	Gross	Gross	Gross		Gross		Stair	Staircaise	Staircaise	Floor Area	, re	Net A
		Floor Area	Floor Area	Floor Area	Floor Area		Floor Area	(III	Exemption	Exemption	Exemption			
-	A1 (3 bedroom)	105	554	603	1,262	λ	1,262	₹ 5	99	42	27	1,127	sf	1,127
7	A2 (4 bedroom)	273	554	603	1,430	₹	2,860	₹	78	30	77	1,295	ş	2,590
2	B1 (2 bedroom)	91	464	206	1,061	₹⋝	2,122	₹	65	43	35	918	sę	1,836
2	B2 (2 bedroom)	182	469	512	1,163	λ	2,326	₺	99	43	35	1,020	sf	2,040
2	83 (3 bedroom)	334	469	512	1,315	ş	2,630	ŞĮ	99	43	35	1,172	şĮ	2,344
2	B4 (2 bedroom)	137	464	909	1,107	Ş	2,214	sf	65	43	35	964	şĮ	1,928
-	B5 (3 bedroom)	298	469	512	1,279	Σę	1,279	ş	65	43	35	1,136	ş	1,136
-	B6 (2 bedroom)	79	464	909	1,049	Sf	1,049	ş	63	43	35	806	ş	806
-	C (3 bedroom)	332	595	615	1,512	Sŧ	1,512	ş	72	36	31	1,373	Σę	1,373
-	D (2 bedroom)	87	467	200	1,054	ş	1,054	ş	55	53	33	913	sf	913
-	E (3 bedroom)	104	595	599	1,268	ŞĮ	1,268	β	99	42	35	1,125	ΣĘ	1,125
16							19,576	ŞĮ						17,320

	sory	Sf	Şŧ	ş	ş	ş	st	st	Sf	sŧ	sf	Sf	Sf	sf	Sf
ACCESSORY CALC.	Total Accessory Area	383	700	834	398	726	492	333	212	321	203	217	142	80	5,041
ACCES	Accessory Area	383	350	417	199	363	246	333	212	321	203	217	142	80	
UNIT DENSITY	Unit Type	A1	A2	81	82	83	84	B5	B6	J	D	ш	Elec / Mech	Garbage / Recycling	
_	Unit	-	2	2	2	2	2	-	1	-	-		1	-	

e Area	20,687	20,687 sf (0.475 Acres)	Maximum Accessory Area	417 sf
Desedo	Density	33.7 upa	Proposed Average Accessory Area	314.5

	Sony	τţς	ş	ξ	ξ	St.	ş	st	Sf	Şŧ	sŧ	st	şţ	sł	Ş
ACCESSORY CALC.	Total Accessory Area	383	700	834	398	726	492	333	212	321	203	712	142	08	5,041
ACCES	Accessory	383	350	417	199	363	246	333	212	321	203	217	142	80	
UNIT DENSITY	Unit Type	A1	A2	81	82	83	84	85	86	C	D	ш	Elec / Mech	Garbage / Recycling	
_	Unit	-	2	7	2	2	2	-	1	-	-	-	1	-	

complies variance required

1.0 m maximum 0.6 m maximum complies complies

1.5 m maximum

4.9.6. balconies & porches projection front, exterior side & rear yard

4.9.5. entry stairs projection side or rear lot line

front lot line

side or rear yard

front yard

no closer than 3.0 m no closer than 1.5 m

2.0 m maximum to a lot line

4.9.7. gateways, pergola and similar landscape structure 4.12.3. catilevered roofs, eaves & gutters

1.2 m maximum

into the yard 4.16. accessable unit

complies complies complies

a. 10% of floor area for covered area b. maximum of 50 sm for garage c. exdusion of 10 sm of stairs & entry 4.9.4. bay window projection

4.4.1. density calulation exclusion

8.7.11 Other Regulations

complies complies variance required (7 small cars)

23 stalls (1.4 cars/unit) 4 stalls (0.2 cars/unit) no small car 1 stall (2% of stalls)

7.5.13 small car 7.5.14 handicap 7.14.10 On-site Bicycle

complies complies complies

1.25 spaces/unit 33% of required 0.2 spaces/unit

vertical

Class 1 Class 2

variance required (3.9 m) complies

4.5 m 2.0 m

corner of St Albans & Jones Road

2. interior side yard

and rear yard

complies complies

1.2 m 12.0 m(3 storeys)

complies complies complies

20.0 m 30.0 m 600 sm

3.a. minimum lot size 8.8.10 On-Site Parking and Loading

7.7.2.1 residential

visitors

1.a. minimum lot width

3. lane setback 8.8.7. Permitted Height 8.8.8. Minimum Lot Size

2. minimum lot depth

complies

4.5 m

8.8.6. Yards & Setbacks 1. front yards, exterior side yard

or road setback

complies complies complies

45% maximum 70% maximum 20% minimum

High Density town housing (City Center) Housing, Town 0.90 FAR

8.8.2 Permitted uses 8.8.4.2. d) Permitted Density 8.8.5. Lot Coverage 1. buildings

2. non-porous

3. live plants

Zoning Requirement

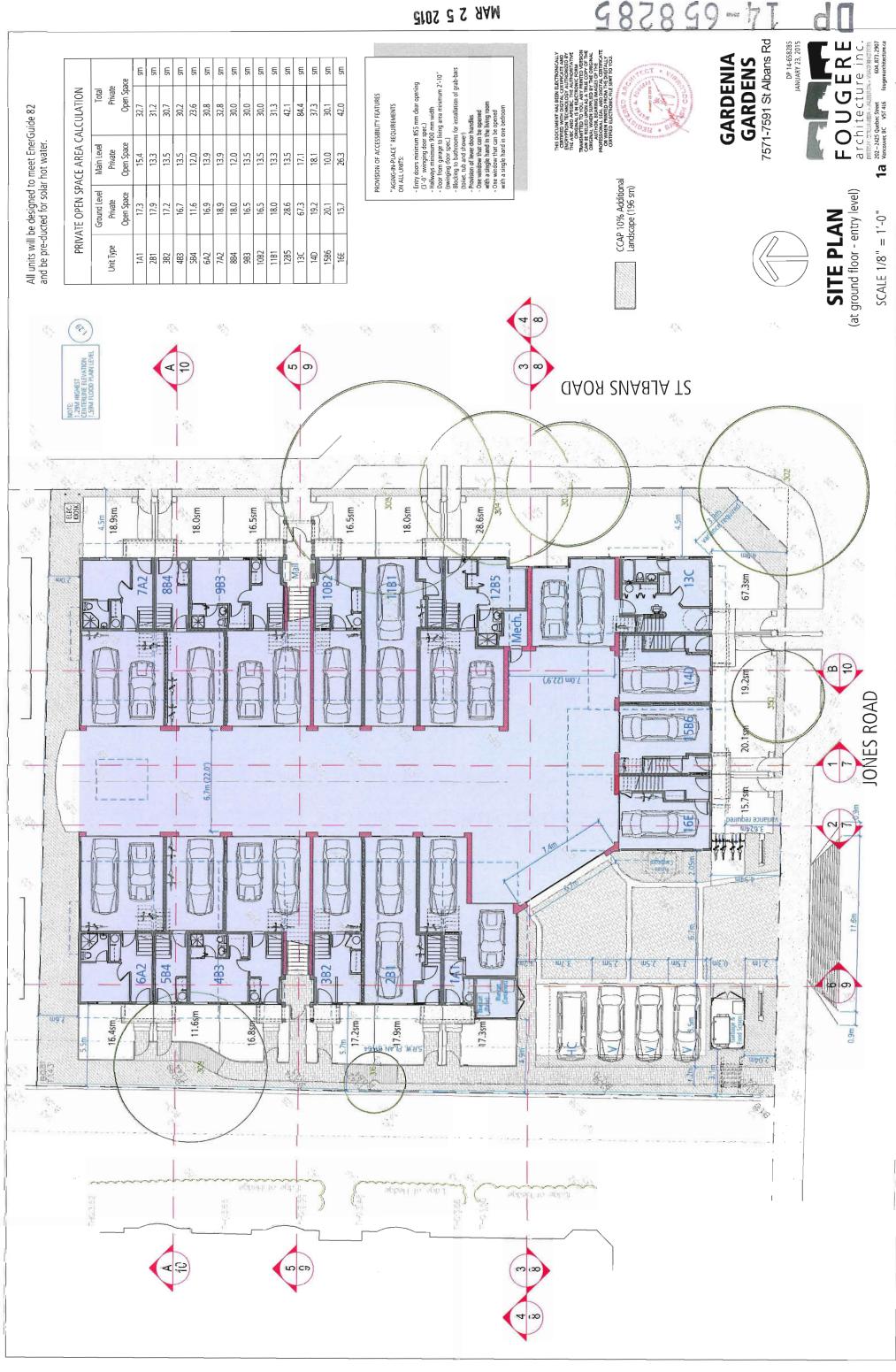
RTH4 Zoning District

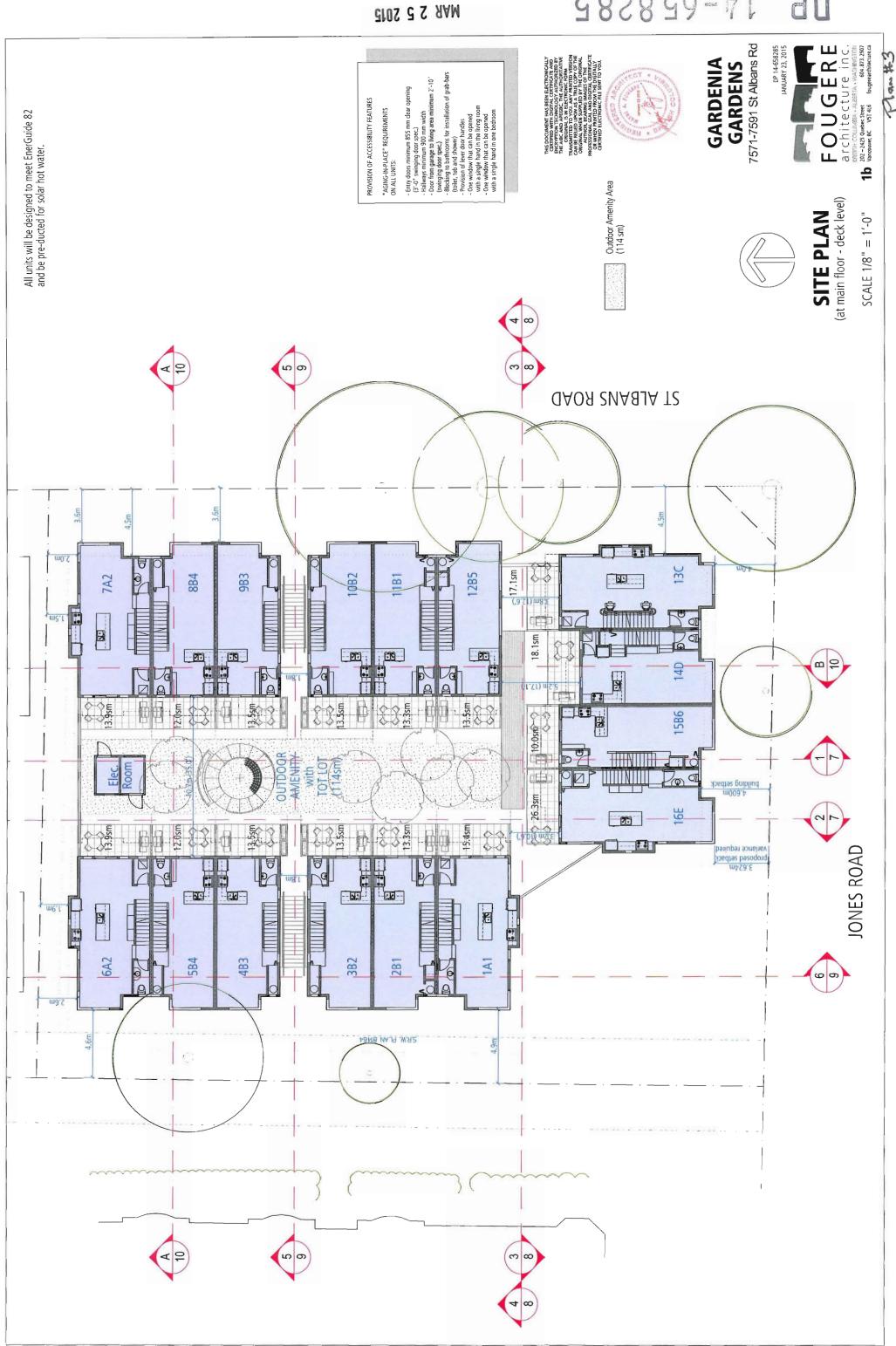
8.8.1 Purpose

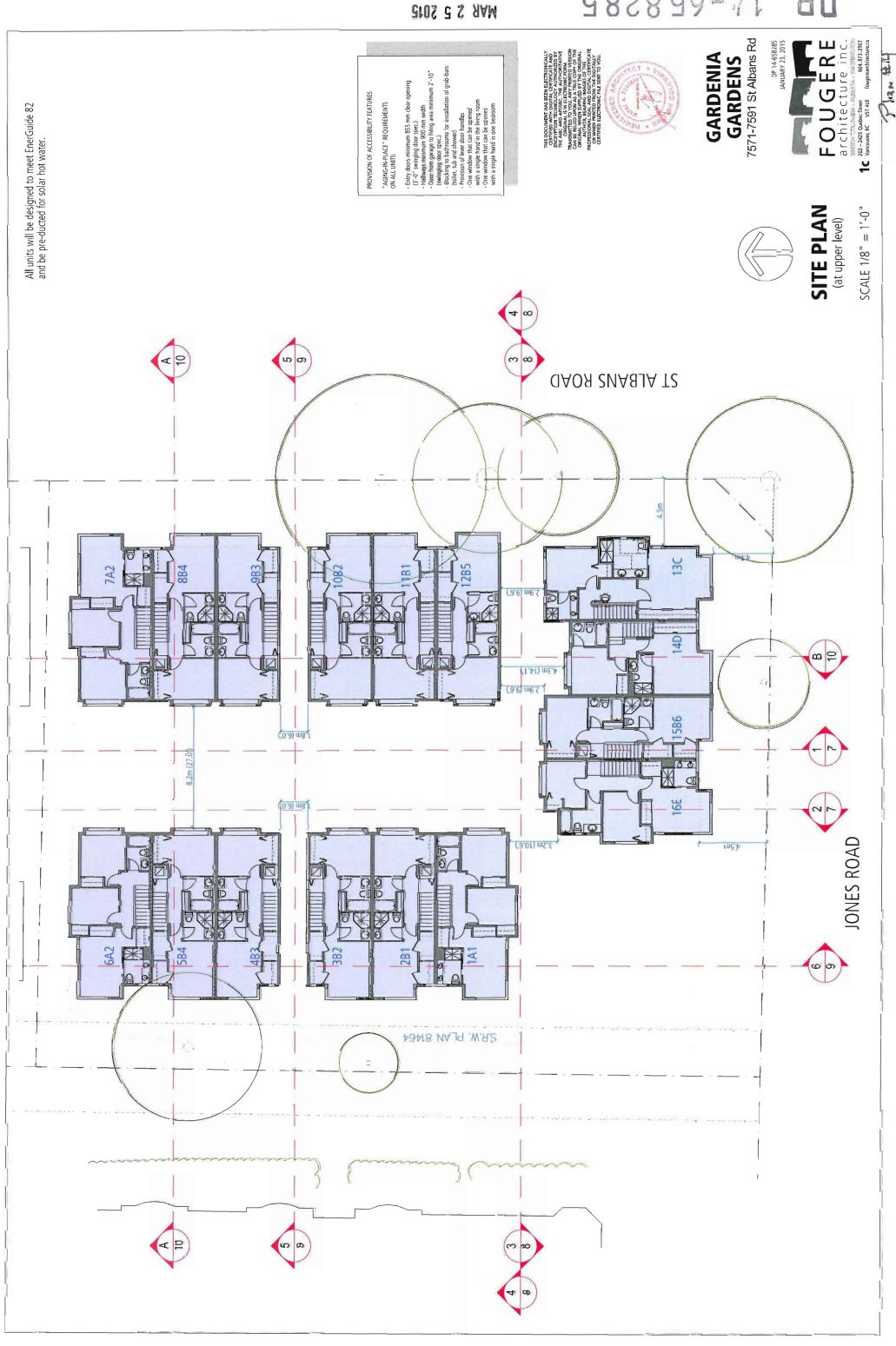
-ZONING ANALYSIS

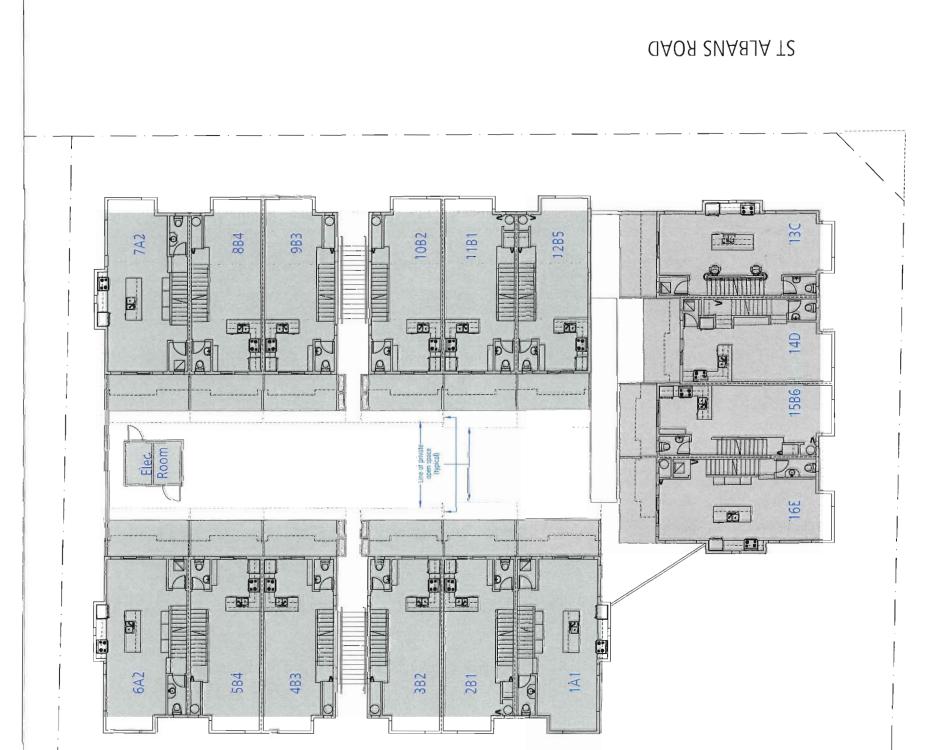
Required Outdoot Amenity Space (6 sm/unit) 96 sm Required Additional Lanscaped Outdoor 192 sm (10% of net site area) 96+192-288 sm Required Open Space 96+192-288 sm Proposed Open Space 114+196=3 t0 sm

																96	4.4.4
													Required Outdoot Amenity Space (6 sm/unit)	Required Additional Lanscaped Outdoor	(10% of net site area)	Required Open Space	
	St.	ş	ş	ş	st	sę	sę	sf	sf	sf	sf	sf	عل	Sf		417 sf	200
Area	383	700	834	398	726	492	333	212	321	203	217	142	80	5,041		Area	



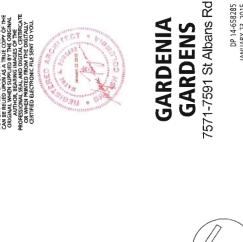






48418 MA.P. W.R.2





GARDENIA GARDENS



LOT COVERAGE

SCALE 1/8" = 1'-0"

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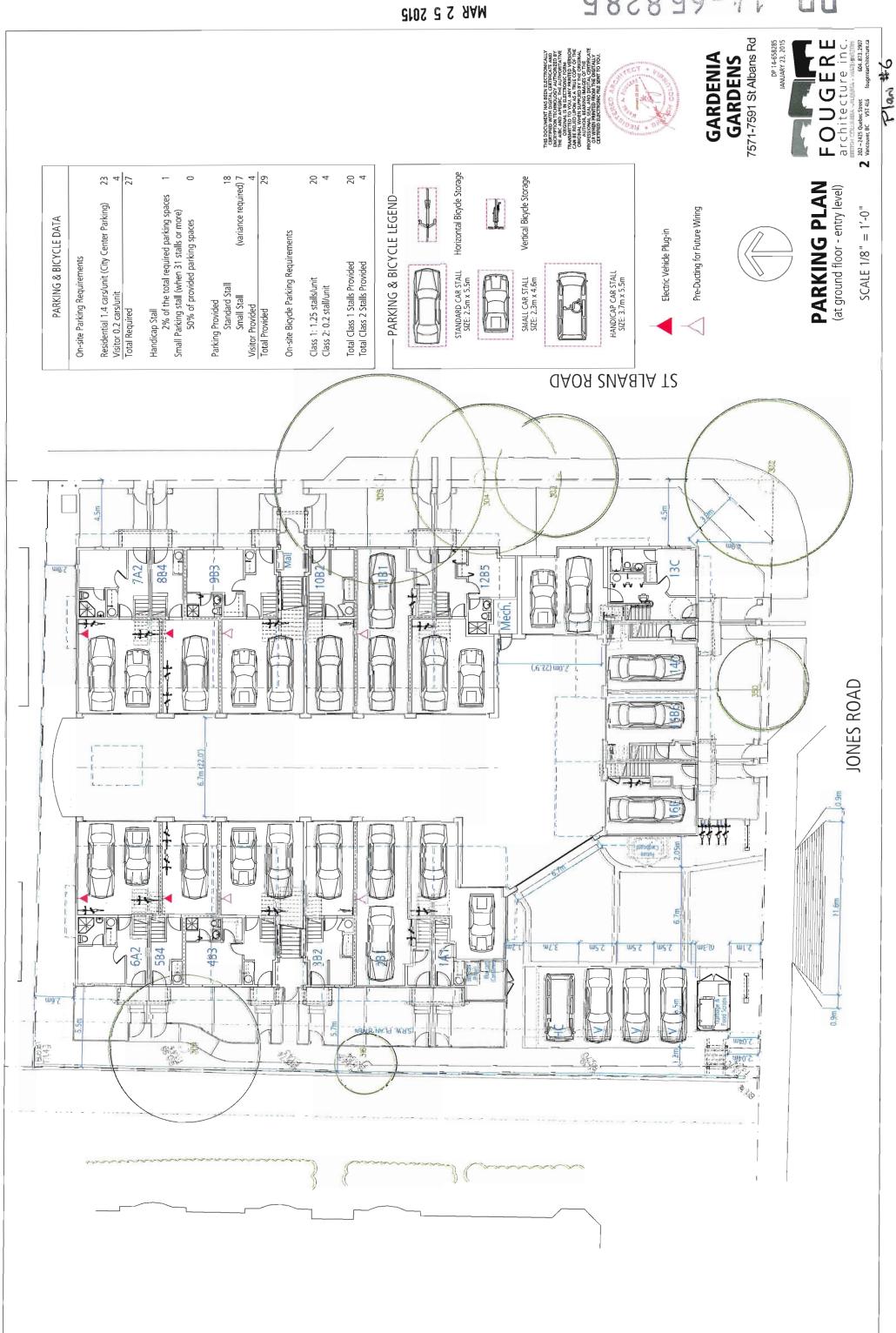
LOT COVERAGE CALCULATION

Maximum Lot Coverage Proposed Lot Coverage

All units will be designed to meet EnerGuide 82 and be pre-ducted for solar hot water.

17-658285

Man #5



Reference

Gardenia Gardens

for Thomas Leung

Legal Description: Civic Address: 7571-7591 St. Albans Road, Richmond



landscape architecture inc. 1990 West 2nd Avenue, Vancouver, BC, VKJ 1H4 t | 604,683,1458 t | 604,683,1459 w | www.etala.ca

Owner: Architect:

Consultants: Electrical: Civil: Structural: Landscape:

eta landscape architecture

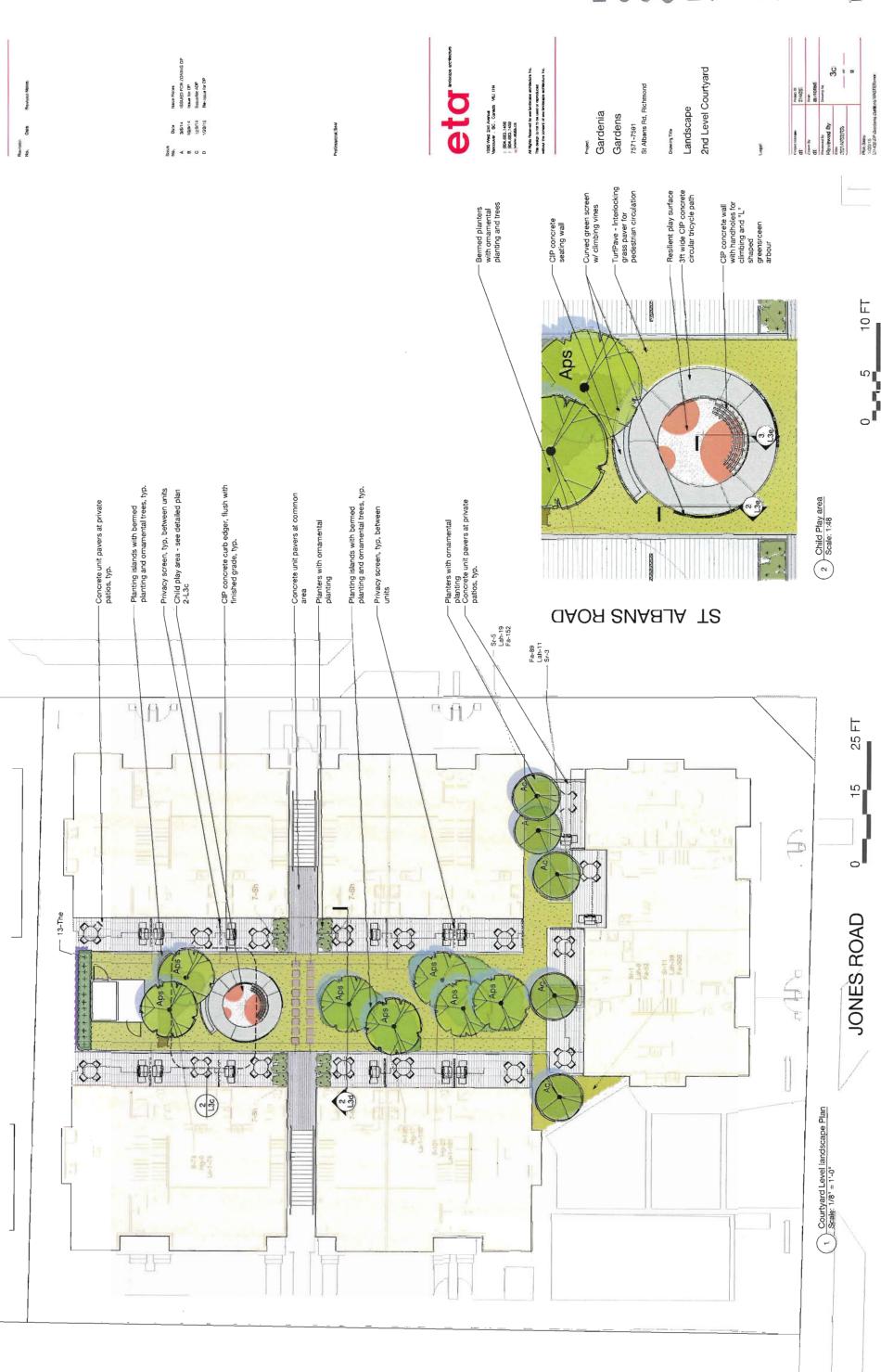
RE-ISSUED FOR: DP January 20, 2015

LANDSCAPE:

Sheet Name	Tree Management Plan	Landscape at Grade	Landscape second level Courtyard	Groundfloor landscape lighting	Courtyard landscape lighting	Landscape Sections and Schedules	Landscape Details and precedent Images	Landscape Permeability Plan
Sheet No.	L3a	L3b	L3c	P87	L3e	L3f	L3g	L3h

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Plan #9

OP 14-658285



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1904 480,1469

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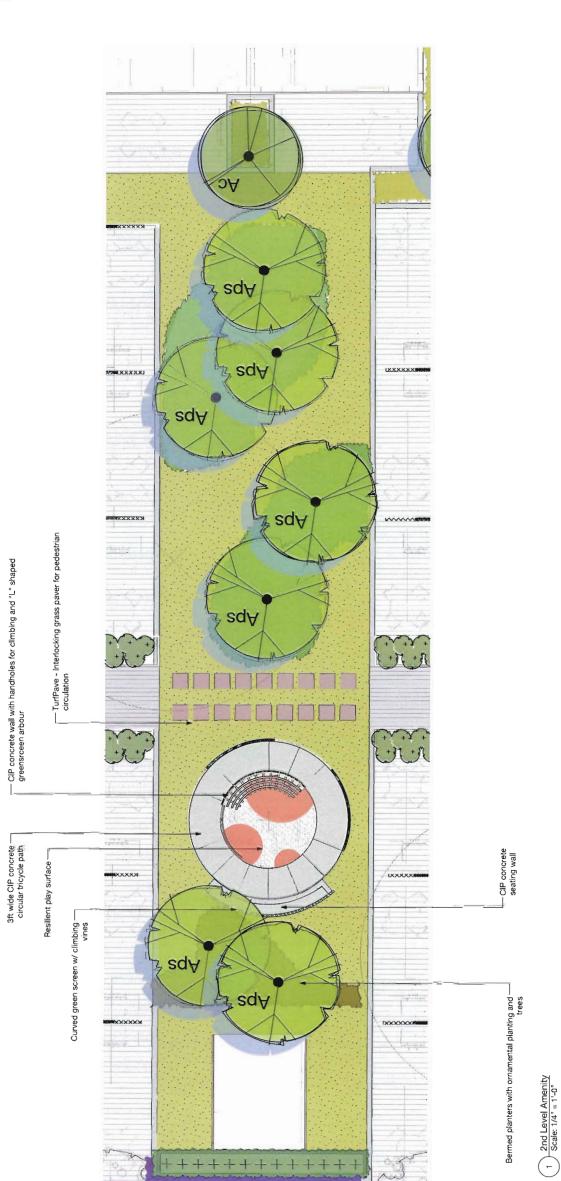
Gardenia Gardens 7571-7591 St Albans Rd. Richmond

Demonstrate
Landscape
2nd Level Courtyard

2nd Level Courtyard



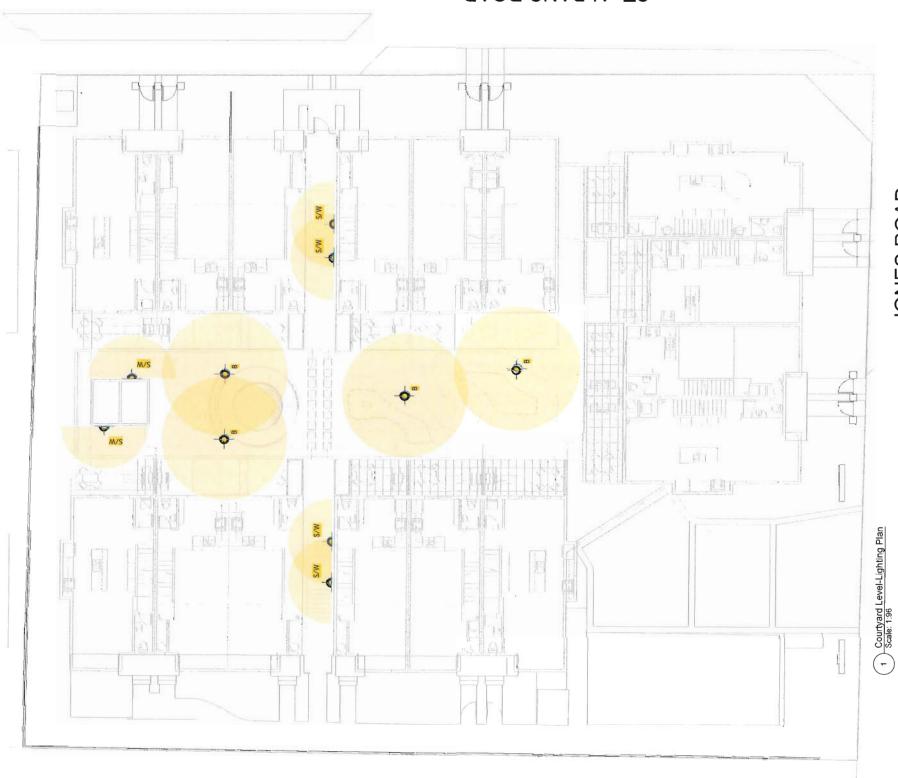
15 25 FT



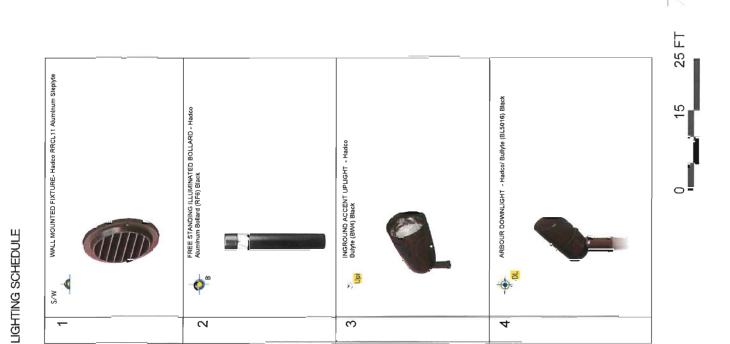
ST ALBANS ROAD



Plan #11



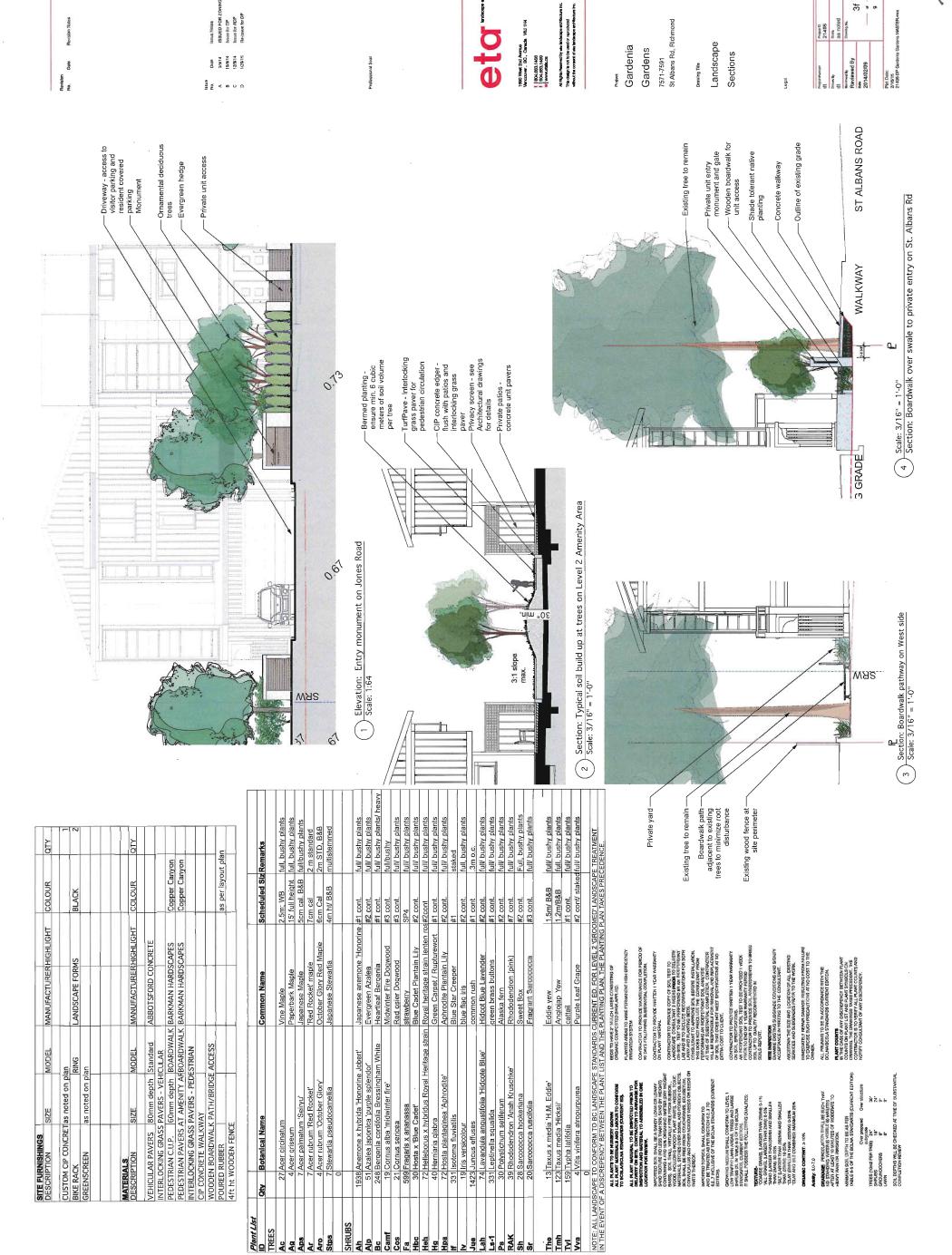
ST ALBANS ROAD



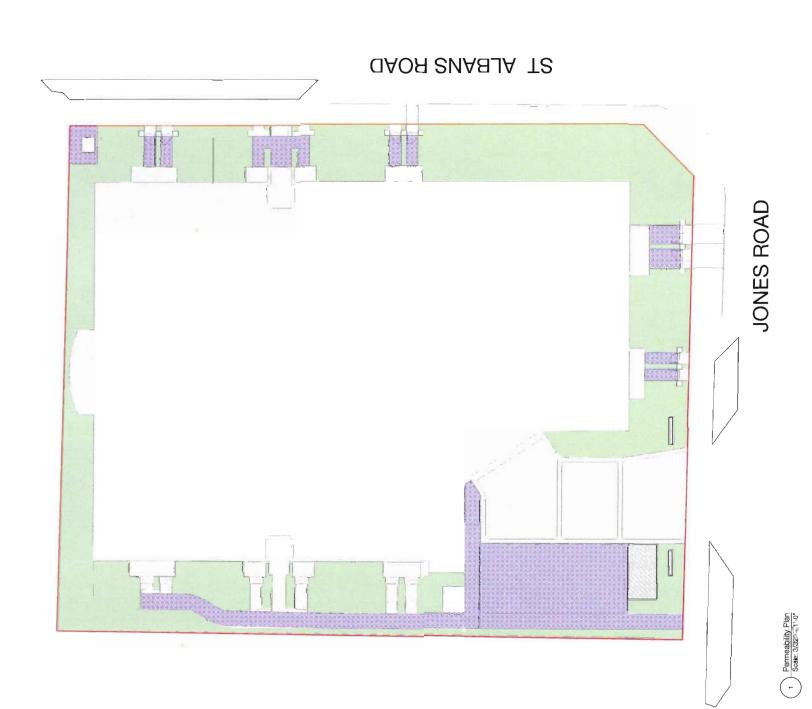
Gardenia Gardens 7571-7591 SI Albans Rd, Richmond

OP 14-658285

Landscape 2nd Level Lighting



DIA WILL



Plan # 15

17-658285

MAR 2 5 2015

SCALE 1/8" = 1'-0"

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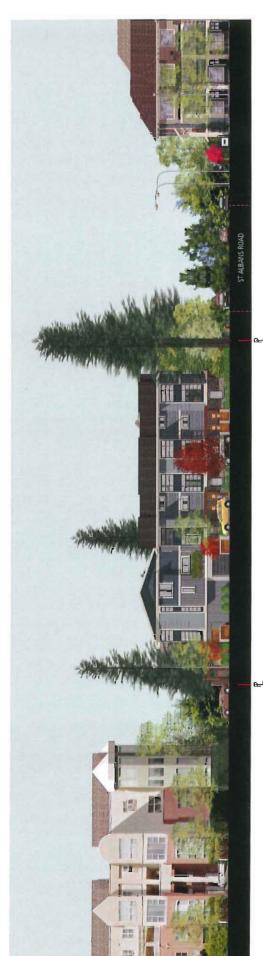
FOUGERE architecture inc. 1877 Country Algebra, Washington 1872-243 Quebec Street God 832 2897 4 Vancoure, 8C VST416 (ougelearchitecture.co.

Dias # 16









STREETSCAPE Jones Road

SCALE 1/16" = 1'-0"

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Ĕ	EXTERIOR FINISHES SCHEDULE	
-	ASPHALT SHINGLES	PABCO - Asphalt shingles - Pewter Gray
7	WOOD TRIMS	SHERWIN WILLIAMS - SW 7567 Natural Tan
m	WOOD FASCIA	SHERWIN WILLIAMS - SW 7567 Natural Tan
4	HARDIE SIDING	JAMES HARDIE - HardiePlank - Boothbay Blue
20	NICHIHA - Architectural Block (18"x6" with score)	SHERWIN WILLIAMS - SW 7603 Poolhouse
9	BOARD & BATTEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
1	VINYL WINDOWS	SHERWIN WILLIAMS - SW 7605 Gale Force
88	WOOD POST & ARBOUR & BRACKET	SHERWIN WILLIAMS - SW 7710 Brandywine
88	WOOD POST & ARBOUR	SHERWIN WILLIAMS - SW 6656 Serape
6	SOLID-CORE EXTERIOR DOOR	SHERWIN WILLIAMS - SW 6656 Serape
10	GARAGE DOOR	OVERHEAD DOOR Thermacore-Color Desert Tan
-	METAL RAILING	SHERWIN WILLIAMS - SW 7605 Gale Force
12	SERVICE DOOR (electrical room)	SHERWIN WILLIAMS - SW 7604 Smoky Blue
m	SERVICE DOOR (recycle enclosure)	SHERWIN WILLIAMS - 5W 7604 Smoky Blue
149	PRIVACY SCREEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
14b	PRIVACY SCREEN	SHERWIN WILLIAMS - SW 7567 Natural Tan
15	MAIN GARAGE DOOR	OVERHEAD DOOR Aluminum Obscure Glass Door - Black
91	CONCRETE	
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ELEVATION

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GARDENIA GARDENS 7571-7591 St Albans Rd







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EAST ELEVATION SCALE 1/8" = 1'-0"



STREETSCAPE St. Albans Road SCALE 1/16" = 1'-0" EXTERIOR FINISHES SCHEDULE

	ASPHALT SHINGLES	PABCO - Asphalt shingles - Pewter Gray
2	WOOD TRIMS	SHERWIN WILLIAMS - SW 7567 Natural Tan
m	WOOD FASCIA	SHERWIN WILLIAMS - SW 7567 Natural Tan
4	HARDIE SIDING	JAMES HARDIE - HardiePlank - Boothbay Blue
20	NICHIHA - Architectural Block (18"x6' with score)	SHERWIN WILLIAMS - SW 7603 Poolhouse
	BOARD & BATTEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
1	VINYL WINDOWS	SHERWIN WILLIAMS - SW 7605 Gale Force
8a	WOOD POST & ARBOUR & BRACKET	SHERWIN WILLIAMS - SW 7710 Brandywine
88	WOOD POST & ARBOUR	SHERWIN WILLIAMS - SW 6656 Serape
6	SOLID-CORE EXTERIOR DOOR	SHERWIN WILLIAMS - SW 6656 Serape
0	GARAGE DOOR	OVERHEAD DOOR Thermacore-Color Desert Tan
-	METAL RAILING	SHERWIN WILLIAMS - SW 7605 Gale Force
12	SERVICE DOOR (electrical room)	SHERWIN WILLIAMS - SW 7604 Smoky Blue
13	SERVICE DOOR (recycle enclosure)	SHERWIN WILLIAMS - SW 7604 Smoky Blue
49	PRIVACY SCREEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
46	PRIVACY SCREEN	SHERWIN WILLIAMS - SW 7567 Natural Tan
15	MAIN GARAGE DOOR	OVERHEAD DOOR Aluminum Obscure Glass Door - Black
9	CONCRETE	
1.1	CTONIC VENICED	CHITHDEN STONE Dra 6+ Ladonstona Grau

ELEVATION

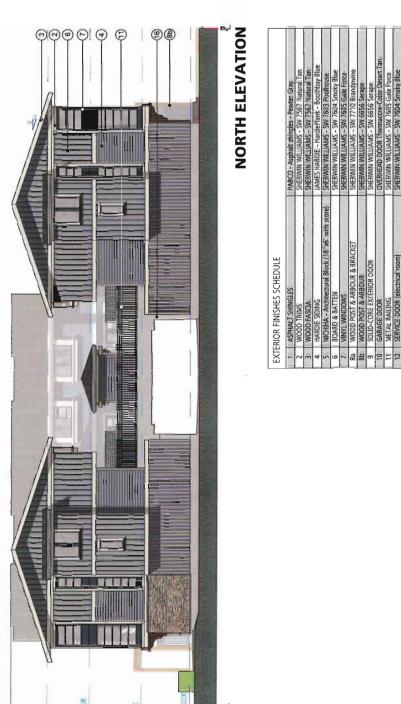






Don #18









ELEVATION SCALE 1/8" = 1'-0"

000335





GARDENIA

GARDENS 7571-7591 St Albans Rd



Plan # 19

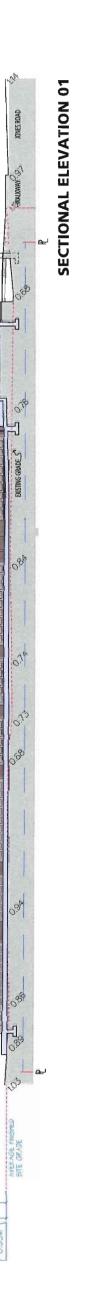
CULTURED STONE - Pro-fit Ledgestone Grey



(2)

9.87M

(2)



GROUND FLOOR

M961

0.95M

9/0

10,01 14giaH mumbseM 20,11 14giaH gnibliu8

SECTIONAL ELEVATION 03

(P)

(3)



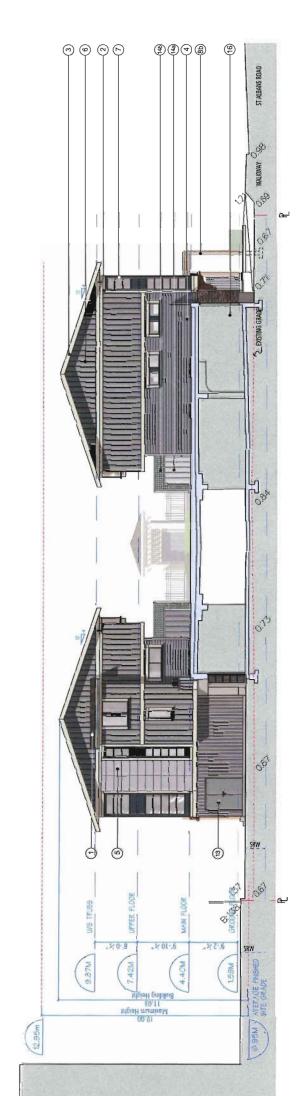






DIAN #20





SECTIONAL ELEVATION 04

Z	EXTERIOR FINISHES SCHEDULE	
	ASPHALT SHINGLES	PABCO - Asphalt shingles - Pewter Gray
7	WOOD TRIMS	SHERWIN WILLIAMS - SW 7567 Natural Tan
m	WOOD FASCIA	SHERWIN WILLIAMS - SW 7567 Natural Tan
4	HARDIE SIDING	JAMES HARDIE - HardiePlank - Boothbay Blue
2	NICHIHA - Architectural Block (18"x6" with score)	SHERWIN WILLIAMS - SW 7603 Poolhouse.
9	BOARD & BATTEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
1	NINAT MINDOMS	SHERWIN WILLIAMS - SW 7605 Gale Force
83	WOOD POST & ARBOUR & BRACKET	SHERWIN WILLIAMS - SW 7710 Brandywine
80	WOOD POST & ARBOUR	SHERWIN WILLIAMS - SW 6656 Serape
6	SOLID-CORE EXTERIOR DOOR	SHERWIN WILLIAMS - SW 6656 Serape
10	GARAGE DOOR	OVERHEAD DOOR Thermacore-Color Desert Tan
=	METAL RAILING	SHERWIN WILLIAMS - SW 7605. Gale Force
12	SERVICE DOOR (electrical room)	SHERWIN WILLIAMS - SW 7604 Smoky Blue
13	SERVICE DOOR (recycle enclosure)	SHERWIN WILLIAMS - SW 7604 Smoky Blue.
143	PRIVACY SCREEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
14b	PRIVACY SCREEN	SHERWIN WILLIAMS - SW 7567 Natural Tan
15	MAIN GARAGE DOOR.	OVERHEAD DOOR Aluminum Obscure Glass Door - Black
16	CONCRETE	
17	CTOME VENIETD	CHITHRED STONE - Pra-fit Ladractona Gran

Dlan #21

GARDENIA GARDENS 7571-7591 St Albans Rd

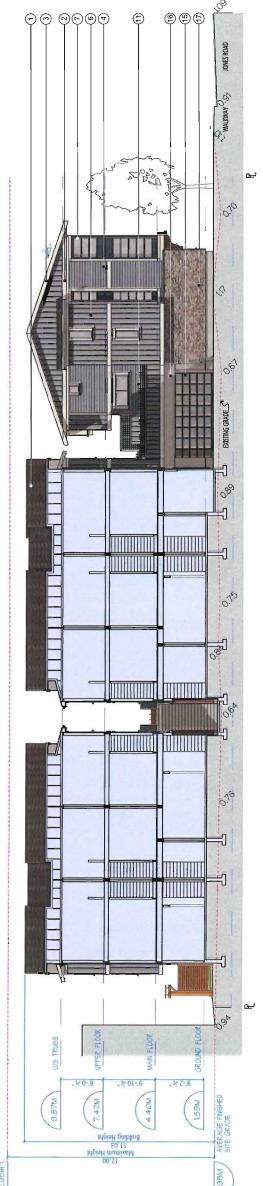




SECTIONAL ELEVATION 06

K	EXTERIOR FINISHES SCHEDULE	
-	ASPHALT SHINGLES	PABCO - Asphalt shingles - Pewter Gray
2	WOOD TRIMS	SHERWIN WILLIAMS - SW 7567 Natural Tan
m	WOOD FASCIA	SHERWIN WILLIAMS - SW 7567 Natural Tan
4	HARDIE SIDING	JAMES HARDIE - HardiePlank - Boothbay Blue
'n	NICHIHA - Architectural Block (18"x6" with score)	SHERWIN WILLIAMS - SW 7603 Poolhouse
9	BOARD & BATTEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
7	VINYL WINDOWS	SHERWIN WILLIAMS - SW 7605. Gale Force
88	WOOD POST & ARBOUR & BRACKET	SHERWIN WILLIAMS - SW 7710 Brandywine
8b	WOOD POST & ARBOUR	SHERWIN WILLIAMS - SW 6656 Serape
6	SOLID-CORE EXTERIOR DOOR	SHERWIN WILLIAMS - SW 6656 Serape
10	GARAGE DOOR	OVERHEAD DOOR Thermacore-Color Desert Tan
=	METAL RAILING	SHERWIN WILLIAMS - SW 7605 Gale Force
12	SERVICE DOOR (electrical room)	SHERWIN WILLIAMS - SW 7604 Smoky Blue
13	SERVICE DOOR (recycle enclosure)	SHERWIN WILLIAMS - SW 7604 Smoky Blue
143	PRIVACY, SCREEN	SHERWIN WILLIAMS - SW 7604 Smoky Blue
14b	PRIVACY SCREEN	SHERWIN WILLIAMS - SW 7567 Natural Tan
15	MAIN GARAGE DOOR	OVERHEAD DOOR Aluminum Obscure Glass Door - Black
16	CONCRETE	
17	STONE VENEER	CULTURED STONE - Pro-fit Ledgestone Grey

7 (-)		RECTIONAL ELEVATION 05
		DKSTINK GRADE \$ 50
	. 48.8	COVERED INTERNAL ROAD.
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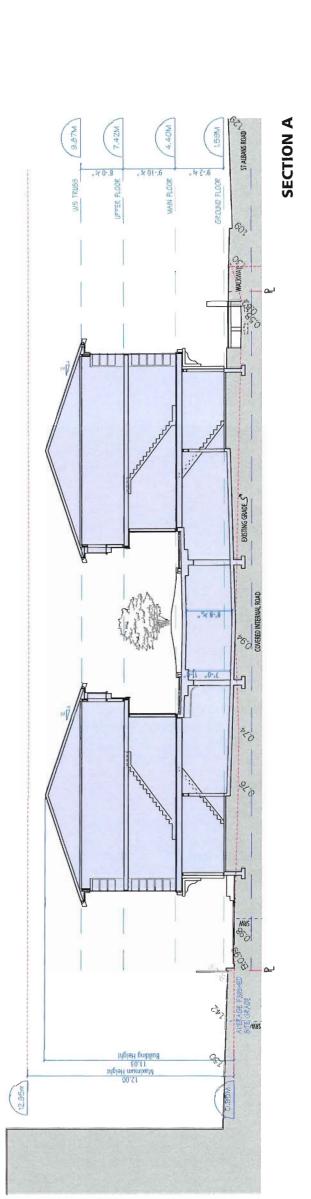


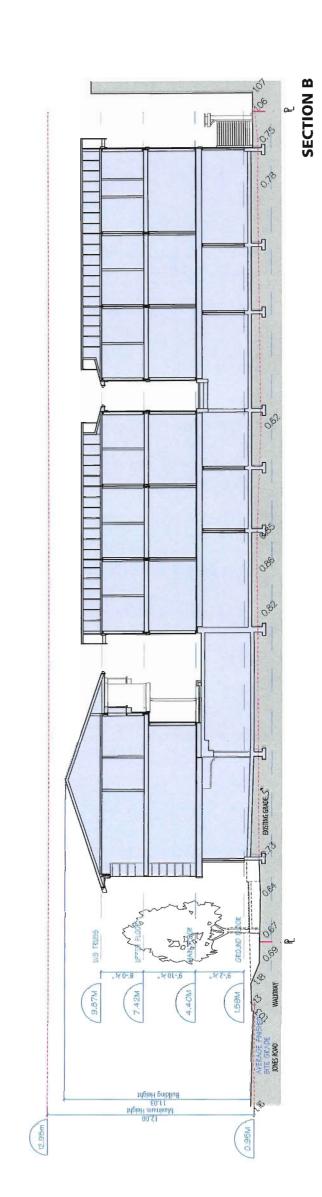


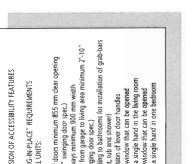




Plan # 22







Exemption Area Gross Area

Parking Area

GARDENIA GARDENS



"AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

PROVISION OF ACCESSIBILITY FEATURES

UPPER FLOOR

UPPER FLOOR

down 14.R

Pantry

16.5

6

Kitchen 10'-5" x 13'-10"

5,-0.

All units will be designed to meet EnerGuide 82 and be pre-ducted for solar hot water.

UNIT A2 FLOOR PLAN

Bedroom 2 9'-2" x 9'-10"

Bedroom 3 8'-0"x10'-6"

Master Bedroom 13'-4" x 9'-4"

.0-.01

UNIT A1 FLOOR PLAN

Ground Floor Main Floor Upper Floor Gross

Bedroom 2 9'-2" x 9'-10"

Bedroom 3 / 8'-0"x10'-6"/

15.0.

.7-,91

38.-0.

34'-0"

entry stair exemption main stair exemption upper stairs exemption TOTAL NET

_7-,9

Ground Floor Main Roor Upper Floor Gross

Kitchen 10'-5" x 13'-10"

10,-04

.7-,91

Pantry

.7-.9

- Entry doors minimum 855 mm clear opening (3-0° swinging door spec.)

- (3-0° swinging door spec.)
- (3-0° swinging door spec.)
- (3-0° swinging door spec.)
- (3-0° swinging door spec.)
- (3-0° swinging door spec.)
- (3-0° swinging door spec.)
- (4) swinging door spec.)
- (5) swinging door spec.)
- (6) swinging door spec.)
- (6) swinging door spec.)
- (6) swinging thand in the living room
- (6) swinging hand in one bedroom

Patiol 7'-C" x 16'-2"

MAIN FLOOR

MAIN FLOOR

22'-0.4"

. X 1-15

. 29-,12

Bedroom4 8'-4"x10'-4"

GROUND FLOOR .7-,91 .7-,91

.411.6

.1-,11

GROUND FLOOR

14,10

FLOOR PLANS

Now # 23

Ξ

Plan # 24

- Enry doors minimum 855 mm clear opening (3-0" swinging door spec.)

- Halways minimum 900 mm width
- Door from gaage to living area minimum 2'-10" (swinging door spec.)
- Brodwignto bahrooms for installation of glab-bars (foilet, tub and shower)
- Provision of lever door handles
- One window that can be opened with a single hand in the living room
- One window that can be opened with a single hand in one bedroom

Exemption Area Gross Area

Parking Area

GARDENIA



3 202 – 2425 Quebec Street 604, 873.2907 Vancouver, 8C V5T 4L6 fougerearchitecture.ca FOUGERE architecture inc.

GROUND FLOOR

Plan # 25

UNIT B4 FLOOR PLAN

All units will be designed to meet EnerGuide 82 and be pre-ducted for solar hot water.

34.0.

GROUND FLOOR MAIN FLOOR UPPER FLOOR GROSS

UPPER FLOOR

PROVISION OF ACCESSIBILITY FEATURES "AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

UPPER FLOOR

see unit B1 for alternate bathroom Master Bedroom 12'-4" x 9'-10"

,0-,01

Ground Floor Main Floor Upper Floor Gross

14,-5.

4,-0.,

see unit B1 for alternate bathroom

UNIT B3 FLOOR PLAN

4,-0,

14,-0.

14.-5.

MAIN FLOOR

" 1 T-1

"KE'SI

MAIN FLOOR

GROUND FLOOR

FLOOR PLANS

MAR 2 5 2015

Exemption Area Parking Area **Gross Area**

GARDENIA

GARDENS 7571-7591 St Albans Rd DP 14-658285 JANUARY 23, 2015

FOUGERE architecture inc. 202 – 2425 Quebec Street 604, 873, 2907

A vancouver, BC V5T 416 Sougerearchitecture.ca Man # 26

464 st 506 st 1,049 st 63 st 43 st 35 st	90 8 sf	
MAIN FLOOR UPPER FLOOR GROSS entry stair exemption main stair exemption upper stairs exemption	TOTAL NET	

UNIT B6 FLOOR PLAN

Bedroom 2 9'-0"x10'-10"

All units will be designed to meet EnerGuide 82 and be pre-ducted for solar hot water.

UPPER FLOOR

see unit B1 for alternate bathroom



- Enry doors minimum 855 mm clear opening (3-0" swinging door spec.)
- Flakeys minimum 900 mm width
- Flakeys minimum 900 mm width
- Flakeys minimum 2-10"
(swinging door spec.)
- Brodwig to behrown for installation of grab-bars (tailer, tub and shower)
- Provision of lever door handles
- One window that can be opened
with a single hand in the living room
- One window that can be opened
with a single hand in nee bedroom "AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

Dining 10'-6"x9'-10"

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.7-,01

.7-,11

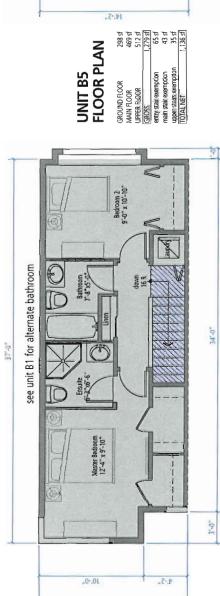
MAIN FLOOR

34'-0"

GROUND FLOOR

FLOOR PLANS

GROUND FLOOR



14-5.

4.0.

UPPER FLOOR

14,-5.

MAIN FLOOR

18'-4" 3,-11. MAR 2 5 2015

Exemption Area Gross Area

Parking Area

Summary of the Convertible Unit Features Checklist as Submited

All units will be designed to meet EnerGuide 82 and be pre-ducted for solar hot water.

UNIT C FLOOR PLAN

9

Q

0

,9-,7

34'-0"

GROUND FLOOR
MAIN FLOOR
UPPER FLOOR
GROSS
entry Stair exemption
main stair exemption
upper stairs exemption
TOTAL NET

Bedroom 2 11'-4"x9'-2"

0

9

UPPER FLOOR

31,-0,

J,-0, J,-0.

1219mm

.0-.Z .0-.Z

Kitchen 10'-5"x13'-10"

.9-,91

- plus 600 mm on latch side.
- Interior doors to entry & main living areas, min. 800 mm clear opening.
 (2'8" sliding or 2'10" swinging door spec.) with flush thresholds max.13mm height.
 Stair lift (as per manufacturer spec)

 - and framing to accommodate shaft construction.

MAIN FLOOR

- Hallways minimum 900 mm width
- Door from garage to living area minimum 800 mm clear opening.
 Min. clear opening 860 mm clear opening to Patios and Balconies.
 Toilet clear floor space min. 1020 mm at side and in front Interior Doors to main living areas, 1 bedroom and 1 bedroom min 800 mm clear opening with flush thresholds max. 13 mm height.
 Wall blocking for future installation of grab-bars (toilet, tub and shower)
 - Clear area needed under future work space. Plumbing and gas
- pipes (in-wall and in-floor) located clear of under counter area of
 - future work space (stove, sink & min. 810 mm wide counter) - 1500 mm turning diameter or turning path diagram
- One window that can be opened with a single hand in the living room One window that can be opened with a single hand in one bedroom

7571-7591 St Albans Rd **GARDENIA GARDENS**



Dlan # 81

FLOOR PLANS SCALE 1/4" = 1'-0"

8-8

31.-0"

Bathroom 2 7'-6"xS'-0"

,9-,91

GROUND FLOOR

Exemption Area **Gross Area**

Parking Area

GARDENS 7571-7591 St Albans Rd **GARDENIA**

DP 14-658285 JANUARY 23, 2015

FOUGERE architecture inc. 16 Vancouver, 8C VST 416 fougerearthingtown volumes.

GROUND FLOOR

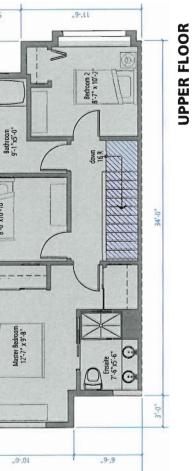
Plan # 28

PROVISION OF ACCESSIBILITY FEATURES "AGING-IN-PLACE" REQUIREMENTS ON ALL UNITS:

- Entry doors minimum 855 mm clear opening (3-0' swinging door spec.)
- Hallways minimum 900 mm width
- Hallways minimum 900 mm width
- Door from garage to thiving area minimum 2'-10" (swinging dear spec.)
- Brodking to bathrooms for installation of grab-bars (tailer, tub and shower)
- Provision of lever door handles
- One window that can be opened with a single hand in the living room
- One window that can be opened with a single hand in the living room

UPPER FLOOR

UPPER FLOOR



All units will be designed to meet EnerGuide 82 and be pre-ducted for solar hot water.

UNIT E FLOOR PLAN

UNIT D FLOOR PLAN

Bedroom 2 9:11"x10"-4"

Master Bedroom 12'-4" x 13'-4"

GROUND FLOOR
MAIN FLOOR
UPPER FLOOR
GROSS
enty Stair exemption
main stair exemption
upper stairs exemption
TOTAL NET

_9-9

,,0-,5

Ground Floor Main Floor Upper Floor Gross

Kitchen 10'-5"x14'-2" Pantry 16.R 3.-0. 9.9 .0-,01

Patte 7'-0"x16'-2"

MAIN FLOOR

MAIN FLOOR

12,-3,

GROUND FLOOR

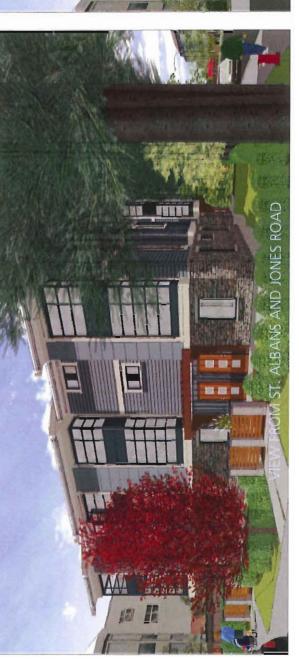
FLOOR PLANS

GARDENIA GARDENS 7571-7591 St Albans Rd

















VIEW FROM ST. ALBANS AND JONES ROAD



Report to Development Permit Panel

To:

Development Permit Panel

Date:

March 4, 2015

From:

Wayne Craig

File:

DP 14-677534

Director of Development

Re:

Application by Onni 7771 Alderbridge Corp. Inc. for a Development Permit at

7008 River Parkway and 7771 Alderbridge Way

Staff Recommendation

That a Development Permit be issued which would:

- 1. Permit the construction of a 324-unit apartment project in two (2) six-storey buildings over connected concrete parking structures located at 7008 Alderbridge Way and 7771 Alderbridge Way; and
- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - (a) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures from 3.0 m to 0.0 m on the west side of the site along Cedarbridge Way;
 - (b) Reduce the required interior side yard setback for limited portions of partiallybelow grade parking structures from 1.5 m to 0.0 m along the east property line of the site;
 - (c) Reduce the required rear yard setback for the attached below-grade parking structures from 1.5 m to 0.0 m on both sides of the future property line that will separate the two (2) future lots to be subdivided within the site;
 - (d) Reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15 spaces/dwelling unit for the development; and
 - (e) Reduce the requirement for the provision of one (1) WB-17 loading space to zero (0).

Wayne Craig

Director of Development

WC:mm Att. (5)

Staff Report

Origin

Onni 7771 Alderbridge Corp. Inc. has applied for a development permit to revise the parking and building design for two (2) apartment buildings within a 659-unit, four (4) building project previously approved under DP 12-615424 in November, 2012.

The two (2) subject buildings (Buildings 3 and 4) were planned to be located on a single site above one (1) large single-storey shared parkade on the east side of Cedarbridge Way (Attachments 2 and 3).

The proposed Development Permit makes changes to Building 3 to provide additional parking within a second above-grade parkade level. It also includes changes to the internal courtyard and the sixth floor on the east side of the building. The proposed Development Permit also includes an additional setback variance to facilitate construction of the adjacent parkades under Buildings 3 and 4 on separate lots. The proposed Development Permit does not change the exterior form and character of Building 4.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

- To the north, lies a section of River Parkway currently under construction. Further to the north, one (1) large light industrial building is located on a site zoned "Industrial Business (IB1)".
- To the south lies Alderbridge Way; with the former Grimm's meat factory site on the south side of the street. This site was rezoned in July, 2013 (RZ11-593705) from "Industrial Retail (IR1)" to "Residential Limited Commercial (RCL)" to allow for a higher density, mixed-use development now under construction.
- To the east, two (2) light industrial/retail buildings are located on a site to the east of an adjacent lane and on a site zoned "Industrial Retail (IR1)".
- To the west, lies a new section of Cedarbridge Way and Buildings 1 and 2 further to the west within the overall development as approved under DP12-615424.

Staff Comments

The proposed development scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, the proposal complies with the intent of

the applicable sections of the Official Community Plan (OCP) and the City Centre Area Plan (CCAP) which designates the site as "Urban Centre T5 (25 m)".

Zoning Compliance/Variances (staff comments in bold)

The proposal complies with the "High Density Low Rise Apartments (RAH2)" zone of the Richmond Zoning Bylaw 8500 except for the variances noted below. With the exception of variance No. 3, the variances are the same as those previously approved under the current development permit (DP 12-615424). These previous variances are included within the new proposed Development Permit to ensure that they are applied to the applicant's revised development plans within the new Development Permit.

The single new variance (no. 3) is required to facilitate the proposed subdivision of the subject site into two (2) lots which better accommodates the applicant's desired construction phasing.

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures from 3.0 m to 0.0 m on the west side of the site along Cedarbridge Way;
 - (Staff supports continuation of the existing variance for the parkade setback to Cedarbridge Way as the sections of parkade walls located at 0.0 m. are largely located below the Cedarbridge Way street grade where the east-west greenway crosses the street. To conceal the parkade, lower parkade ceilings over bike parking and the front vehicle parking have been used to assist in accommodating these planters and hiding the parkade structures. This treatment leaves only short sections of these parkade walls rising up to approximately 0.75 m (2.5 ft.) to meet adjacent terraced planters.)
- 2) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the east property line of the site;
 - (Staff supports continuation of the existing variance which is for the parkade wall under Buildings 3 and 4 on the east side of the site adjacent to a 6.0 m (20.0 ft.) lane. This is an interim condition as a future north-south pedestrian path will be constructed within the east lane at a higher grade rising up to the level of the intersecting east-west greenway. An ultimate grading plan is included within the Development Permit plans that address this future raised pathway with planter walls not exceeding 1.2 m (4.0 ft.) against the final pedestrian pathway elevation. Staff have also discussed this proposed change of grade with the owners of the future development site to the east who will be required to construct the pathway over the current lane alignment as discussed above.)

- 3) Reduce the required rear yard setback for the two (2) attached below-grade parking structures from 1.5 m to 0.0 m on both sides of the future property line that will separate the two (2) future lots to be subdivided within the site;
 - (Staff supports the proposed variance as it applies to the interim south elevation of the parkade to be constructed with Building 3 and is located adjacent to the remaining existing light industrial building to be demolished for the future Building 4. When Building 4 is built, its parkade will abut and be connected to the Building 3 parkade.)
- 4) Reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15 spaces/dwelling unit for the site.
 - (Staff supports continuation of the existing visitor parking reduction to 0.15 spaces/unit as accommodated within the 7.5% TDM reduction in total parking requirements (resident and visitor parking combined). As the visitor parking component was reduced by 25% under the existing DP12-615424 (more than the 10% TDM maximum), a formal variance was required. The applicant registered an easement that requires the interconnection of the visitor parkade intercoms and includes parkade entrance signage allowing for visitor parking sharing for Buildings 1/2 and Buildings 3/4. The interconnection provides flexibility for use and access to all of the 49 visitor spaces within the parkade for Buildings 3 and 4.)
- 5) To reduce the requirement for the provision of one (1) WB-17 loading space to zero (0).

(Staff supports continuation of this proposed variance as there will be four (4) medium SU9 loading spaces within the development site with one (1) space being located next to each building's lobby entrance off of Cedarbridge Way as included within the current Development Permit.)

Analysis

Urban Design and Site Planning

The current approved development includes four (4) similar buildings of six (6) storeys each. There are two (2) buildings on each of the two (2) lots on the east and west sides of Cedarbridge Way. The four (4) previously-approved buildings have a similar U-shaped form of development with each building having three (3) sides surrounding a garden courtyard.

The proposed revisions to Building 3 accommodate a second level of parking under the courtyard and within the inner part of the first storey of the building (see Attachment 5 and Development Permit plans), creating an additional "step" in the courtyard. Staff support the stepped concept as it provides visual interest when viewed in perspective from the street, creates an alternative to the other courtyard designs and breaks the courtyard into smaller, programmable sub-areas, including a passive recreation area on the lower level and more active children's play and urban agriculture areas on the upper level. No other substantive changes are proposed to be included on the first five (5) storeys of Building 3.

To off-set the loss of residential space on the first storey, units have been added to the sixth storey on Building 3. These units are located in the central (eastern) section of the "U" and match the approved six-storey height of the adjacent north and south wings. When combined with the raising of the interior courtyard, there is no appreciable increase in the height of the building's east wing when viewed from Cedarbridge Way. The increased height will be more apparent when experienced from the current east lane (future north-south greenway) and when the building is viewed from the east. Measures to mitigate the impact of this added mass are discussed in the following section.

Architectural Form and Character

Buildings 3 and 4 remain consistent with the approved Development Permit DP 12-626615 with the exception of the following changes:

- The additional units included on the sixth storey of the east wing of the building will provide for a more strongly defined sixth storey than the current approved design that includes a covered, open breezeway connecting the north and south wings (see Attachment 4).
- The additional building mass included on the sixth storey is purposely set back from the north and south wings to provide breaks in the building mass in the building's east wing. These breaks in building mass, which provide roof top decks for the adjacent units, are intended to reduce the apparent bulk of the upper floor level on both sides of the building, as well as allow for more sun penetration to public and private grade-level spaces.
- The glass curtain wall lobby entrance on River Parkway has been doubled in width to provide a more prominent break within the building facade.

Landscape Design Changes

Revisions to the landscaping plan for Building 3 are as follows:

- The one (1) storey increase in grade through the centre of the courtyard is addressed with the inclusion of three (3) landscape walls with terraces between two (2) main levels of the courtyard.
- The outdoor play area has been relocated to the centre of the courtyard away from the adjacent private unit patio space. The play area has also been slightly enlarged to provide for four (4) pieces of play equipment instead of the former one (1) piece.
- There is an increased number of community garden plots but the total garden space is somewhat reduced to provide for more pedestrian circulation area around the greater number of plots.

East-West Greenway:

The east-west greenway runs between Buildings 3 and 4 as required under the CCAP. The separation between the buildings averages approximately 22 m (72.5 ft.) along the greenway, leaving sufficient area for ground floor patios and common strata property on each side. The greenway features a 3.5 m (11.5 ft.) wide hard-surfaced public path with landscaping on both sides and is included within a 10 m (33 ft.) wide Statutory Right of Way (SRW) registered at the time of rezoning.

The proposed Development Permit application and subdivision do not change the ultimate location and design of the greenway and ensures that the full pathway surface is provided when Building 3 is constructed. In the interim, the portion of the greenway on Lot B4 will be located on fill and will be reconstructed when the parkade and Building 4 are built on Lot B4. The greenway will straddle the proposed lot line between Lots B3 and B4 and be included within the current greenway SRW and a proposed joint-maintenance easement for the interim and ultimate greenway as discussed below (see Attachment 3).

On-Site Vehicle Parking and Loading

The proposed parkade level 1 layout is similar to the combined parkade structure under Buildings 3 and 4 under the approved Development Permit, but with the inclusion of a partial second parking level within Building 3 and under its courtyard as described above. The proposed additional parkade level allows for 394 resident spaces (an increase of 39 spaces). The same number of visitor spaces are provided for Buildings 3 and 4 as included within the approved Development Permit which includes a variance to 0.15 visitor spaces/unit.

The design does not include a large WB17 loading space as it was relaxed under the current Development Permit DP12-615424 and is proposed to be relaxed under this new development permit.

Future Subdivision and Legal Documents

The phasing of construction of Buildings 3 and 4 and the associated street frontage and east-west greenway construction are consistent with that provided at rezoning and within the current approved DP12-615424.

The interim subdivision will allow for the construction of Building 3 on Lot B3 first and Building 4 on Lot B4 later, after the remaining light industrial existing building is demolished. The ultimate connected parkade structure will require the registration of a reciprocal parking and access easement on the proposed Lots B3 and B4. The subdivision and parking easement are requirements of Development Permit issuance.

The greenway located between Buildings 3 and 4 will be located on both future Lots B3 and B4 on the existing SRW. A reciprocal easement over these lots and the existing greenway SRW is required to ensure that the owners of both lots share the maintenance responsibilities and costs for the greenway and is a condition of Development Permit issuance.

The applicant has included an interim 1.5 m (5.0 ft.) asphalt pathway along the eastern side of the Cedarbridge Way along the Lot B4 frontage to the new crosswalk and traffic light being now installed at the Cedarbridge Way / Alderbridge Way intersection. When Building 4 is completed, the ultimate street frontage improvements, including sidewalks and boulevards, will be constructed on the sections of Cedarbridge Way and Alderbridge Way fronting Lot B4.

Lastly, the internal changes to provide the second parking level within Building 3 has resulted in shifting the current eight (8) affordable housing units from floors 2 and 3 to floors 3 and 4. The total floor area of the affordable housing and unit types would remain as provided in the current affordable housing covenant and housing agreement, but would require amendment to change the above-noted floor plans.

Conclusions

The proposed development permit to accommodate the additional parkade within Building 3 provides 39 additional resident parking spaces and maintains the same visitor parking at 0.15 spaces/unit as provided in the original Development Permit DP12-615424. Moreover, stepped terracing of the interior courtyard, on-site outdoor amenity space and revisions to the fifth and sixth floors of the east wing of Building 3 have improved the design of the development.

The revised Building 3 design is also consistent with the OCP and CCAP Development Permit Area guidelines and land-use policies as well as the RCL2 zoning.

On this basis, staff support issuance of the Development Permit for the revisions to Building 3 and provide for the parkade phasing.

Mark McMullen

Senior Coordinator-Major Projects

MM:rg

The following are to be met prior to forwarding this application to Council for approval:

- Holding of the current Letter of Credit for \$833,599 (provided under DP13-615424) to secure the Buildings 3 and 4 landscaping and that may be reduced after the completion of the Building 3 landscaping/interim greenway to \$327,044 for the remaining Building 4 landscaping and ultimate greenway.
- Prepare a subdivision plan for Lots B3 and B4 as generally shown in Attachment 3 to satisfaction of the City and the Approving Officer.
- Prepare and register a reciprocal easement on the title of the proposed Lots B3 and B4 that provides for sharing of the maintenance responsibilities for the greenway within the existing registered SRW as generally shown on Attachment 3 to the satisfaction of the City.
- Prepare and register a reciprocal easement on the title of Lots B3 and B4 that provides for shared use of and access to the parkades on each lot as generally shown on Attachment 5 to the satisfaction of the City.
- Prepare and register an amendment to the existing registered affordable housing covenant (#CA2848659-CA2848664) that includes the revised affordable housing units associated with changes in the proposed DP14-677534 while ensuring the affordable units and sizes are to the satisfaction of the City and that their total floor area is consistent with the existing affordable housing covenant to the satisfaction of the City.

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (http://www.richmond.ca/services/ttp/special.htm).

Attachments:

- 1. Development Application Data Sheet
- 2. Location Map
- 3. Site Plan for Current Development Permit for Buildings 1 to 4 and Proposed Development Permit for Buildings 3 and 4
- 4. Current Development Permit Elevation Plans for Building 3
- 5. Plans for Proposed Parkade Under Buildings 3 and 4



Development Application Data Sheet

Development Applications Division

DP 14-677534 Attachment 1

Address: 7771 Alderbridge Way and 7008 River Parkway

Applicant: Onni 7771 Alderbridge Corp. Inc. Owner: Onni 7771 Alderbridge Corp. Inc.

Planning Area(s): City Centre Area Plan (Lansdowne Village)

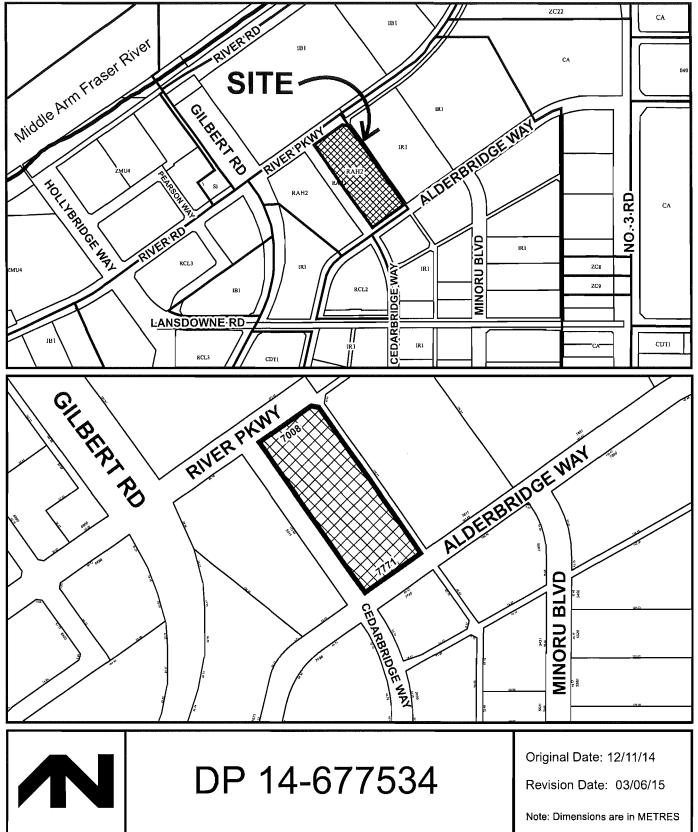
Floor Area Gross: 323,602m² Floor Area Net: 300,271m²

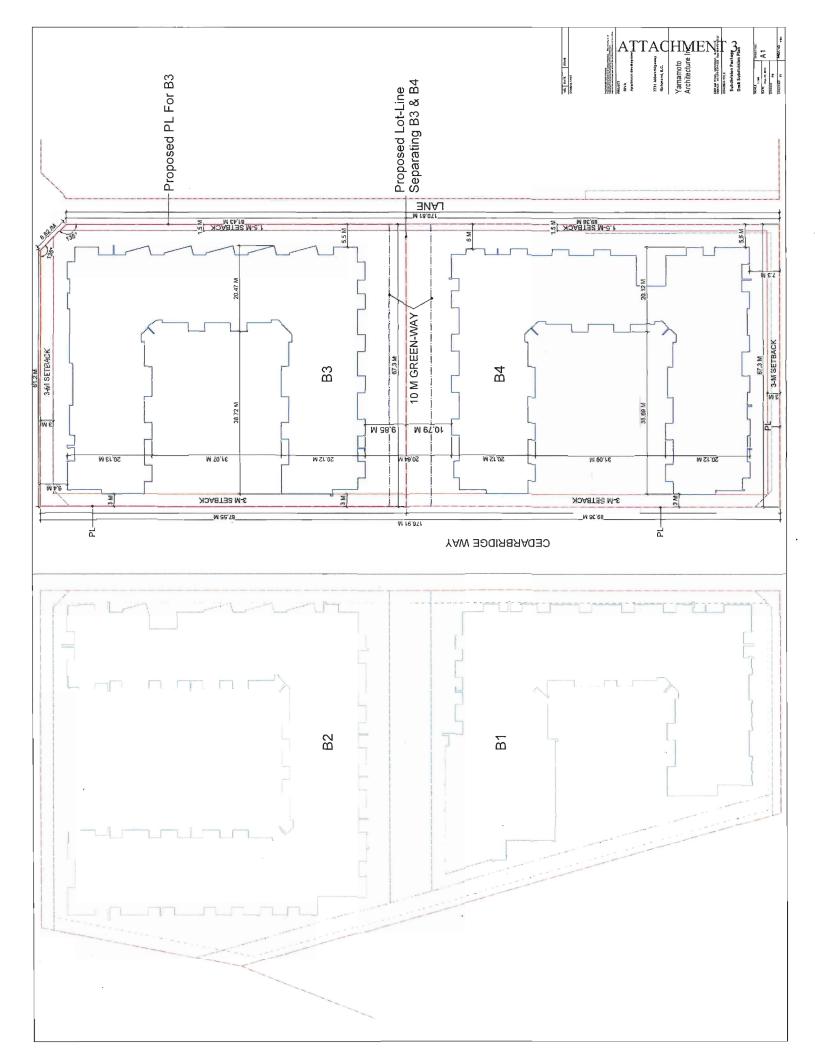
	Existing	Proposed
Site Area:	11,886 m ²	11,886 m²
Land Uses:	Multi-Family Residential Light Industrial / Retail	Multi-Family Residential
OCP Designation:	Mixed-Use	Mixed-Use
Zoning:	High Density Low Rise Apartments (RAH2)	High Density Low Rise Apartments (RAH2)
Number of Units:	319	324

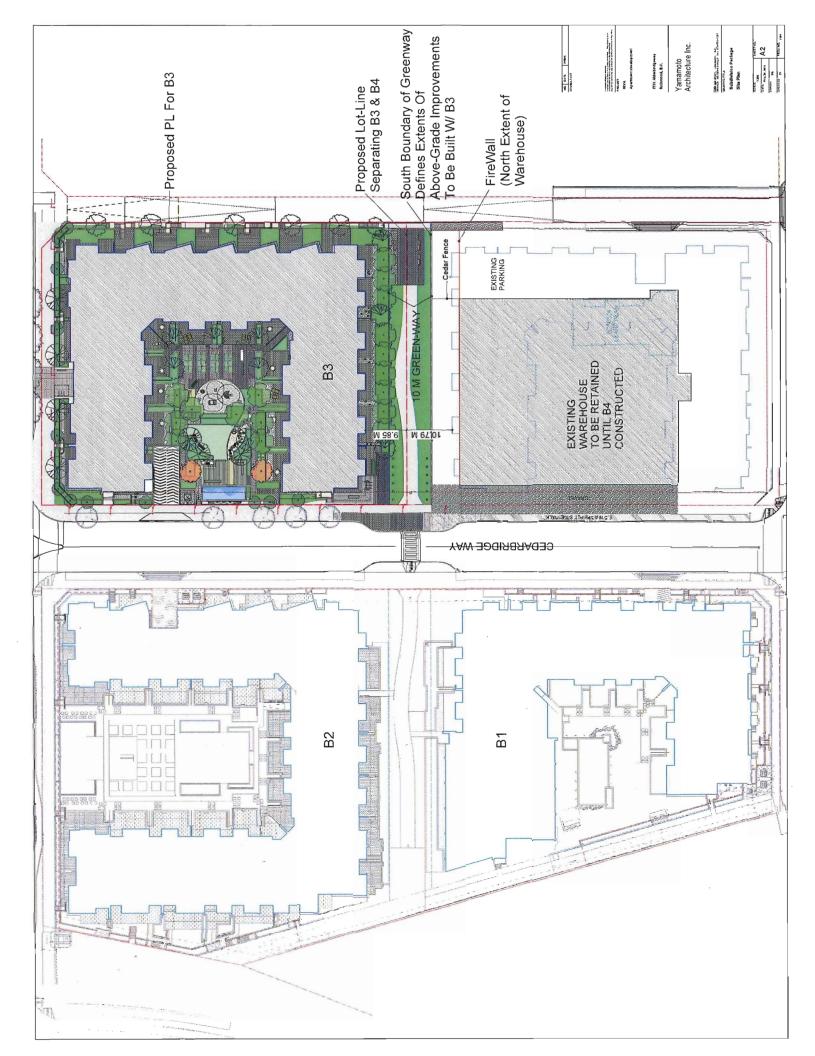
	Bylaw Requirement With DP 12-615424 Variance	Proposed Development	Variance
Lot Size (Min.)	• 2,400 m ² (25,833 ft ²)	• 11,886 m² (127,947 ft²)	• None
Lot Coverage (Max.)	60% for buildings80% for building and non porous surfaces	Buildings: 45%Buildings and non-porus surface: 70.3%	None
FAR	2.0 FAR of the total site with provision of 5% of total floor area for affordable housing units.	2.0 FAR of the total site with provision of 5% of total floor area for affordable housing units.	• None
Habitable Floor Elevation (Min.)	Residential: 2.9 m geodetic	 Residential: 4.064 m geodetic Local exception permitted for one (1) lobby per building. 	• None
Height (Max.)	 25 m, but with specific areas allowing up to 35 m as outlined in CCAP. 	Varies, but less than 25 m above finished grade in all cases.	• None

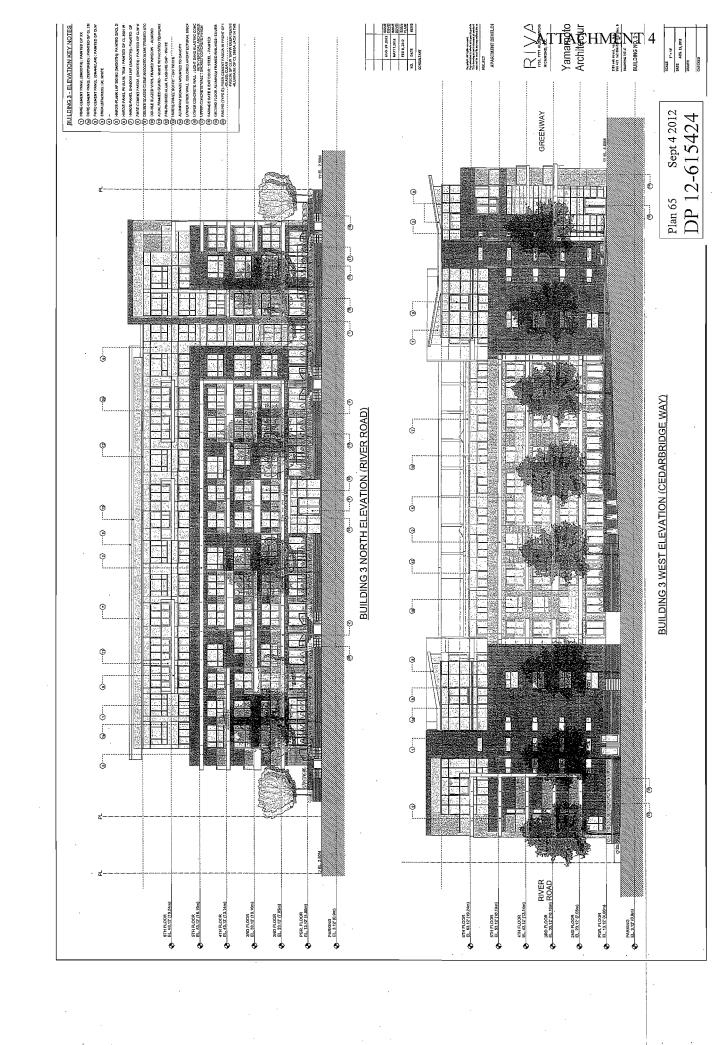
Setbacks (Min.)	a) 3.0m @ Alderbridge P/L b) 1.5m @ East Lane P/L c) 3.0m @ River Road P/L d) 3.0m @ Cedarbridge P/L e) 1.5m @ Interior Lot Line Separating Parcels B3 & B4	a) 5.3m @ Building 4 to Alderbridge Way SRW¹ b) 0.0m @ East Lane P/L for parkade c) 3.0m @ River Road P/L d) 0.0m @ Cedarbridge Way P/L for parkade e) 1.5m @ Interior Lot Line Separating Parcels B3 & B4 ¹Based on setback to the sidewalk SRW on Alderbridge Way; setbacks from the actual property lines are greater.	Same Variances for a) to d) as provided under DP 12- 615424. New Variance for e) from 1.5m to 0.0m
Off-Street Parking ¹ ¹ With maximum 7.5% TDM overall reduction approved at Rezoning	Building 3 Resident: 183 (small car 50%) Visitors: 25 Building 4 Resident: 172 (small car 50%) Visitors: 24 Combined Total (Buildings 3 & 4) Resident: 355 (small car 50%) Visitors: 49	Building 3 Resident: 220 (small car 47%) Visitors: 25 Building 4 Resident: 174 (small car 50%) Visitors: 24 Combined Total (Buildings 3 & 4) Resident: 394 (small car 48%) Visitors: 49	Same Variance to 0.15 visitor spaces/un it as provided under DP 12- 615424
Bicycle Parking	Resident (1.25/unit): 405 Visitor (0.2/unit): 65	Resident (1.25/unit): 419 Visitor (0.2/unit): 65	• None
Loading	1 Large WB17 On-site that can be shared with 2 Medium SU9 loading spaces	2 Medium SU9 loading spaces	Same as per DP 12-615424 to relax the Large WB17 Loading Space.

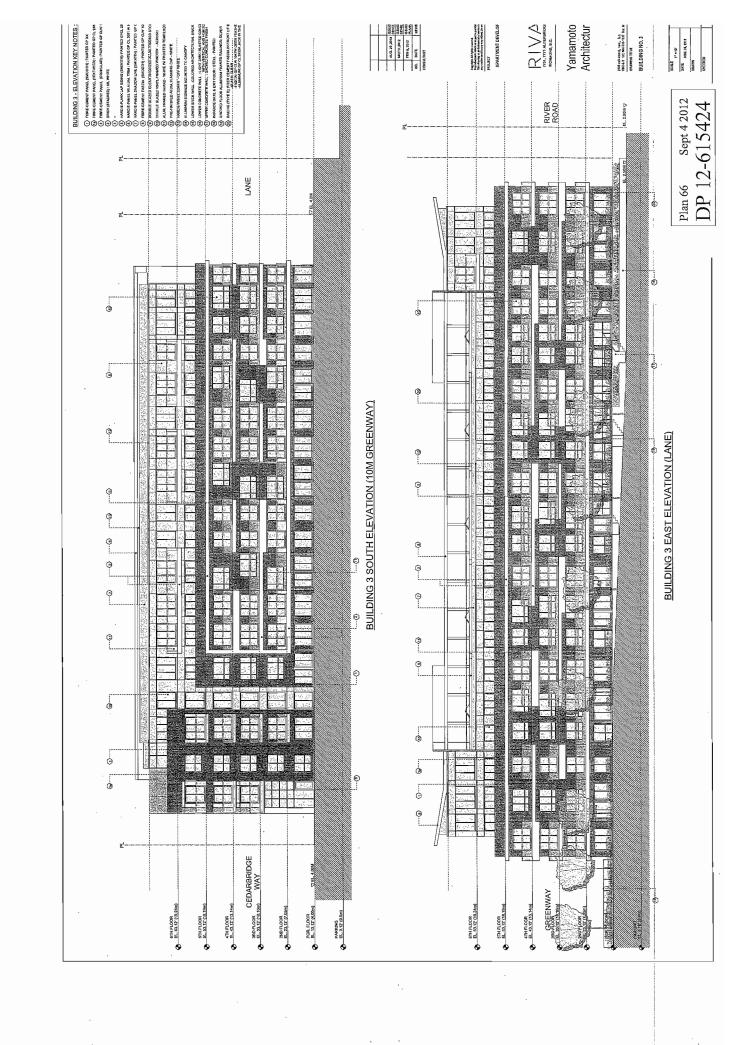


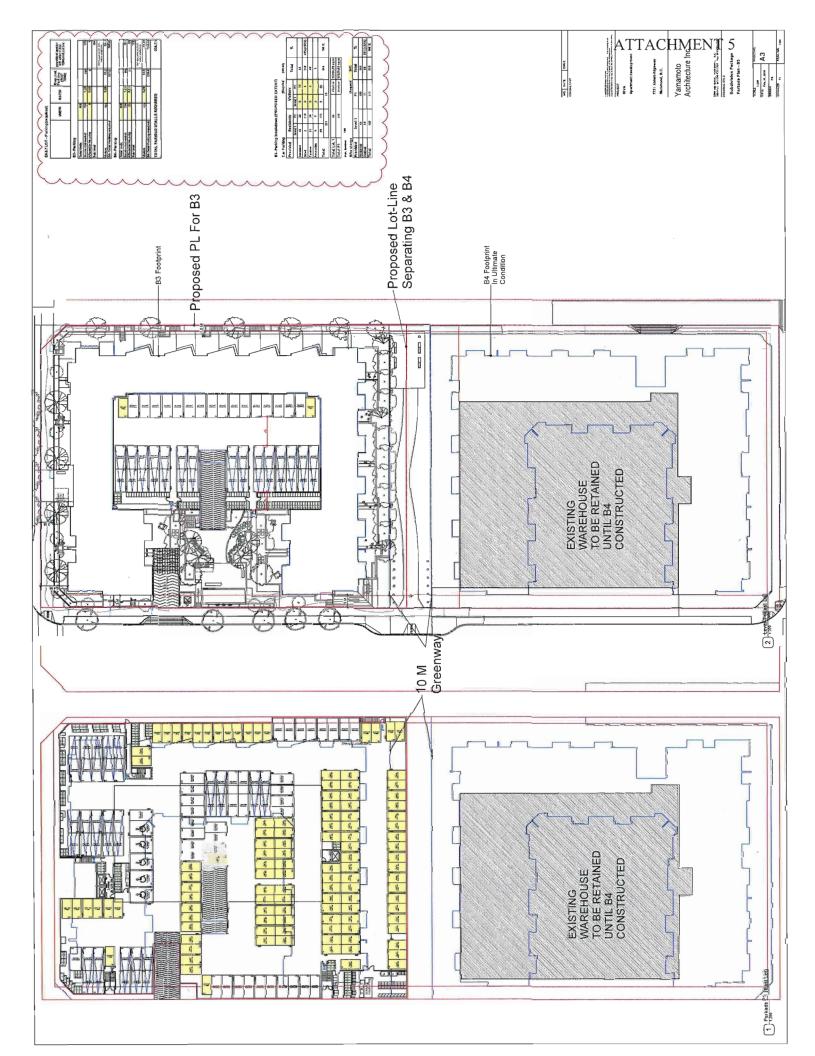


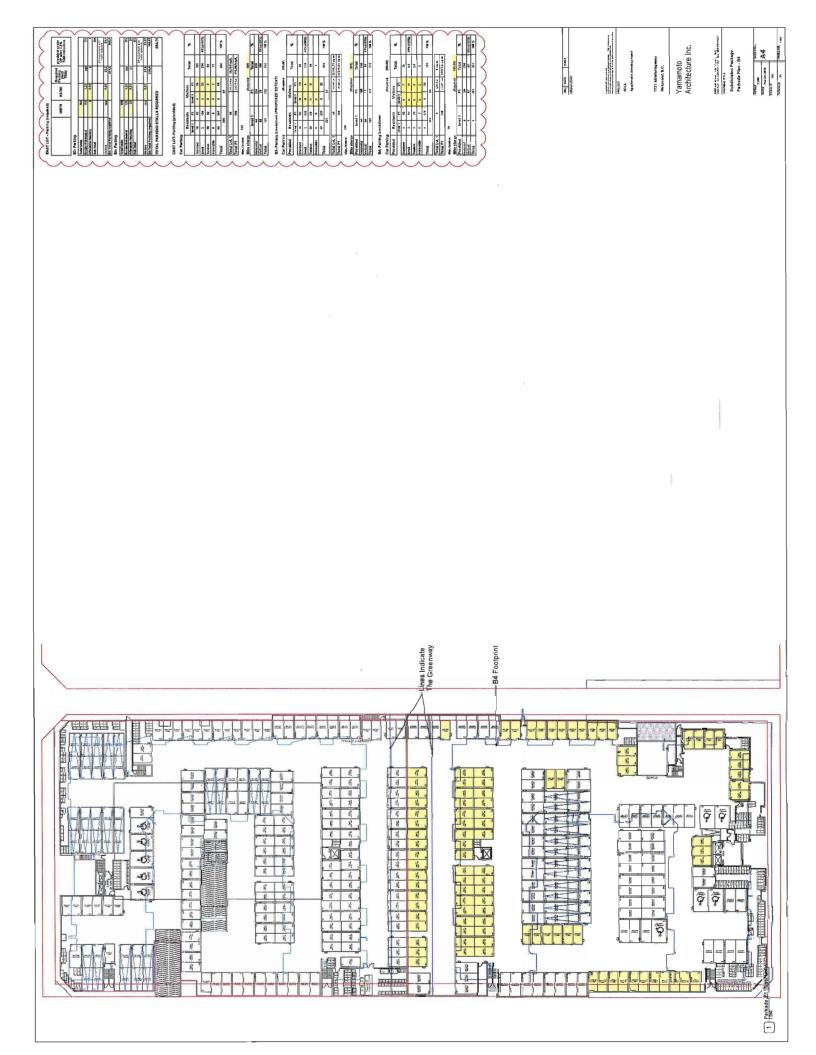














Development Permit

No. DP 14-677534

To the Holder:

ERIC HUGHES C/O

ONNI 7771 ALDERBRIDGE WAY CORP. INC.

Property Address:

7771 ALDERBRIDGE WAY AND 7008 RIVER PARKWAY

Address:

300-550 ROBSON STREET VANCOUVER, BC V6B 2B7

- 1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
- 2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - (a) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures from 3.0 m to 0.0 m on the west side of the site along Cedarbridge Way;
 - (b) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the east property line of the site;
 - (c) Reduce the required rear yard setback for the two (2) attached below-grade parking structures from 1.5 m to 0.0 m on both sides of the future property line that will separate the two (2) future lots to be subdivided within the site;
 - (d) Reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15 spaces/dwelling unit for the development; and
 - (e) Reduce the requirement for the provision of one (1) WB-17 loading space to zero (0).
- 4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #DP 14-677534-1 to #DP 14-677534-43 attached hereto.
- 5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
- 6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$833,599 (provided under DP13-615424 to secure the Buildings 3 and 4 landscaping and that

Development Permit

No. DP 14-677534

To the Holder:

ERIC HUGHES C/O

ONNI 7771 ALDERBRIDGE WAY CORP. INC.

Property Address:

7771 ALDERBRIDGE WAY AND 7008 RIVER PARKWAY

Address:

MAYOR

300-550 ROBSON STREET VANCOUVER, BC V6B 2B7

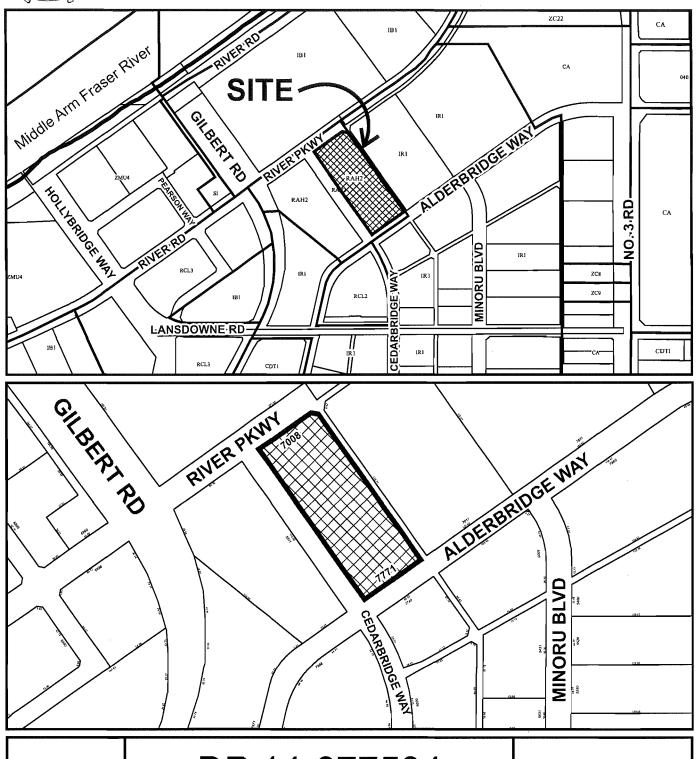
may be reduced after the completion of the Building 3 landscaping/interim greenway to \$327,044 for the remaining Building 4 landscaping and ultimate greenway) to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.

- 7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
- 8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESO DAY OF	OLUTION NO.		ISSUED 1	BY THE COUNCIL THE
DELIVERED THIS	DAY OF	,		







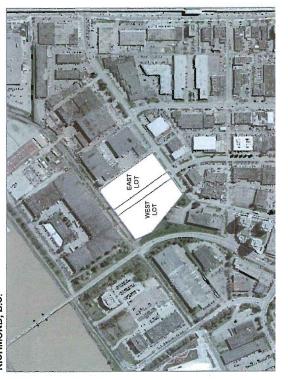
DP 14-677534 SCHEDULE "A"

Original Date: 12/11/14

Revision Date: 03/06/15

Note: Dimensions are in METRES





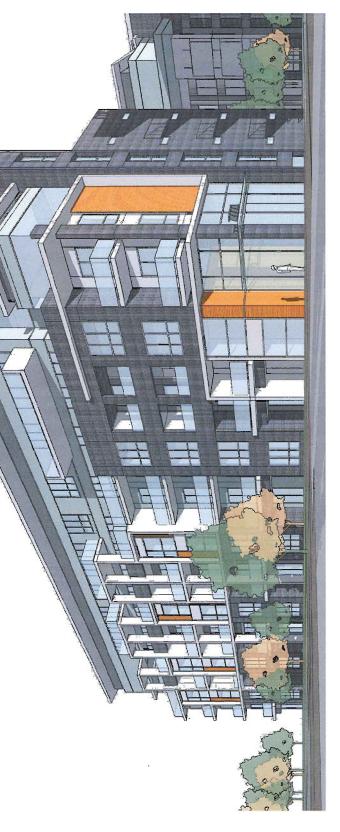
SITE					
	ž.				PERMITTED CENSITY PER LOT EXCLUDING BONUS FOR UNIVERSAL HOUSING
SITE AREA		Z8,713 SQMT	309,050 SQFT.		
PROPOSED FLOOR AREA	Ī				
	260	TACK SCATE	618 120 SOFT		
NET SITE AREA AFTER CEDARBRIDGEWAY DEDICATION	BRIDGEWAY	DEDICATION			
MESTICITI		13 289 SOMT.	143.040 SOFT.	52.79 %	326 285,57 SOFT,
EAST LOT (2)		TALBE SOMT	127.937 SQFT.	47.21 %	
TOTAL	2,281	25,175 SQMT.	270,977 SQFT,		618,120 SGFT.
West lot	2 668	E	328.385.57 enm		
Universal towns @ Total Permitted FAR	. 86 sq.mt.	251 Units	5,025,31 sq.ft.		
Bull Density PAR			325,643,40 sq.ft.		
Surplus FAR for East lot			4,467.45 sq.ft.		
East lot					
Permitted Genally	2 FAR		291, 834,45 ng R		
Universal brand (2) Surplice FAR from West lot	1.86 sq.mt.	264 Units	5,285,35 sq.ft. 4,467,45 sq.ft.		
Yetal Avsilable FAR for East Let Less B4 FAR (DP)			102,587,44 nq.ft. 102,505.00 nq.ft.		
Total Amiliaha CAS for B.1			192 381.44 sq.ft.		

	CONTRACT	UNIT MIX (ACCESSILBLE UNITS)	UNITS)			FLOOR AREA	;	AFF	AFFORDABLE UNITS	IINO:
	108	20R	188	TOTAL	GROSS AREA	DEDUCTION	BLDG, FL AREA	188	28R	TOTAL
BUILDING	151 67	183 88	(2)	143 [32]	155,942.4 SO,FT	21,327,0 SO,FT.	134,515,4 50,FT.	7	ş	5
BUILDING # 2	151 20	149 [138]	13 [12]	194 [146]	199,988,8 SO,FT.	4,718,6 SO,FT.	192,228 SQ.FT.	0	٥	٥
TOTAL	(01) 62	245 [226]	17 (16)	341 [251]	352,909 SQ.FT.	75,065,6 SQ.FT.	332,309 50.FT 26,063,6 50.FT, 326,M3.4 50.FT.	4		•
EASTLOT										
	UNITRAK	UNIT RAX (ACCESSILBLE UNITS)	UNITS			PLOOR AREA		AFFC	AFFORDABLE UNITS	UNIT
	10R	20R	38R	TOTAL	GROSS AREA	DEDUCTION	BLDG, FL, AREA	188	28R	TOTAL
BUILDING # 3	41 [34]	111 [50]	101 20	166 [134]	170,663,4 SQ.FT.	19.283.2 SOLFT.	151,380.Z SQ.FT.			•
BUILDING	26 5	121 [115]	10 [10]	189 [130]	153,254,1 SQ.FT.	4,048.1 SQ.FT.	M9,295.0 SQ.FT.	œ	9	2
TOTAL	68 [39]	232 [265]	24 [20]	325 [264]	323,517,5 SQ.FT. 23,331,3 EQ.FT.		300,586,2 SQ.FT,		23	=
OVERALL										
	UNITENE	UNITIMALACCESSINGLEUNITS)	UNITS			FLOOR AREA		AFFC	AFFORDABLE UNITS	
	188	288	388	TOTAL	CROSS AREA	DEDUCTION	BLDG, FL AREA	188	ZBR	TOTAL
BURLDING # 1	13 63	(18) 98	4(3)	143 [35]	165,942,4 SQ.FT.	21,327,0 9O,FT.	134,615,4 SQ,FT.	,	9	r
BUILDING \$2	(6) 50	149 (133)	13 [12]	198 (168)	196,968,6 SQ,FT.	4,738.6 SQ.FT.	192,228 SQ.FT.		۵	٥
BUILDING # 3	(56) 1.5	111 (90)	14 110	164 (132)	170,653,4 SQ.FT.	19,283.2 SQ.FT.	151,380,2 SQ.FT.	0		•
BURDINGER	28 [5]	121 [115]	(01) 01	129 [130]	153,254,1 SO,FT.	4,048,1 SQ.FT.	149,205,0 SO,FT.	•	5	23
TOTAL	102 [43]	477 [431]	41 (35)	665 [515]	STERRES SOFT, 49,398,9 SO,FT.		627,429,6 SQ.FT.	13	38	ş

										Mildergraph Olym
					Gross Area	Declaritions	Party assid panodole	1 Bre 2	2014	Total
6th Ploor	7 [7]	16 (10)	2 2	(61) 52	22.50%.2 vq.R.	10.00 to 10.	21,308,4 sq.ft.	0	0	٥
5th Place	1/18	17 (13)	lol »	(96) 62	75,096.0 sq.ft.	Tupe 8.098	25, 102.Z sqft.	0	0	٩
4th Floor	7 161	22 [16]	2 [2]	31 (26)	28,795,5 sq.ft.	1996.8 sq.ft.	28,125,6 sq.ft.	0	,,	-
3rd Floor	7 [6]	181122	2 2	31 (26)	28,795.4 sq.R.	695.8 sq.ft.	28,199,5 sq.ft.	0	-7	
2nd Floor	1918	(21) 02	2 2	30 (24)	28,795,5 sq.R.	950,5 sq.ft.	27,845,0 sqft.	0	0	۰
Ground Spor	4 [2]	14 [14]	2 [2]	20 (18)	36,074,8 99,70	15,949,3 sq.ft.	20,125.4 1911.	0	0	°
A) Total	41 [34]	111 (90)	14 [10]	165 [136]	170,663.9 sq.ft.	19,283,2 1q.ft.	151,389,2 sq.ft.	0	0	***
Unit mix	25%	67%	13.5	100%	169%	113	3,60			Ш
B) Arnitable floor area for B3	rea for B3				THE REAL PROPERTY.		102,381.4 sq.ft.	THE REAL PROPERTY.	13	100
Describes on zon fore feeland Statemen study. Elevator Status + Americy of sealing from Describes on ground files induce misland positing. Bryste strasgo serv. + 100mpml Exec BUILDING 4 OVERALL-	her indude motor VERALL-	shafts, Elevator Shi and pathing, Stryde	dis • Amerily of Me donage area • 100	Describtos es zos fore federal Subvent desta, Elevata Sentals - Acercity es senting vone Describtos en grounflass indoce without parkey, fingels étemps enn + 100 epuil Enemplien les inconnatual peu, BUILDING 4 - OVERALL-	Danital arek					
UNIT MIX [ACCESSIBLE UNITS]	SSIBLE UNITS	Section 1		The same of	FLOOR AREA				AFFORDABLE UNIT	Š
	188	208	306	TOTAL	GROSS AREA	DEDUCTION	BLDG, PLDORANEA	AB1	20R	-
CTH FLOOR	lolo	16)6	3 (3)	12 [12]	14,337,8 BOAT	928,3 9097	13,409,4 SOFT	•	0	ŀ
\$TK FLOOR	3 10	(6) (3)	2/2	32 ZD	29,617,9 SOFT	653.1 SOFT	24,964.6 SQFT	0	0	Н
4TH PLOOR	0,0	26 25	0.0	31 25	28,395,4 502-1	642.8 SGFT	27,752,7 SQFT	٥	0	H
JRD FLOOR	lois	Ze 1251	0.0	31 12	24,395,4 SOFT	642.8 SC#1	17527 SOFT	2	9	Н
ZNO FLOOR	E 11	25 24	olo	31 25	28.487.5 SOFT	643.0 SOFT	ZT, BAA, S SOFT	•	6	Н
GROUND FLOOR	9(4)	50 13	010	20 62	28,021,1 SOFT	538.2 SGFT	27,482.\$ SQFT	2	۰	Н
•	•	•	-					[
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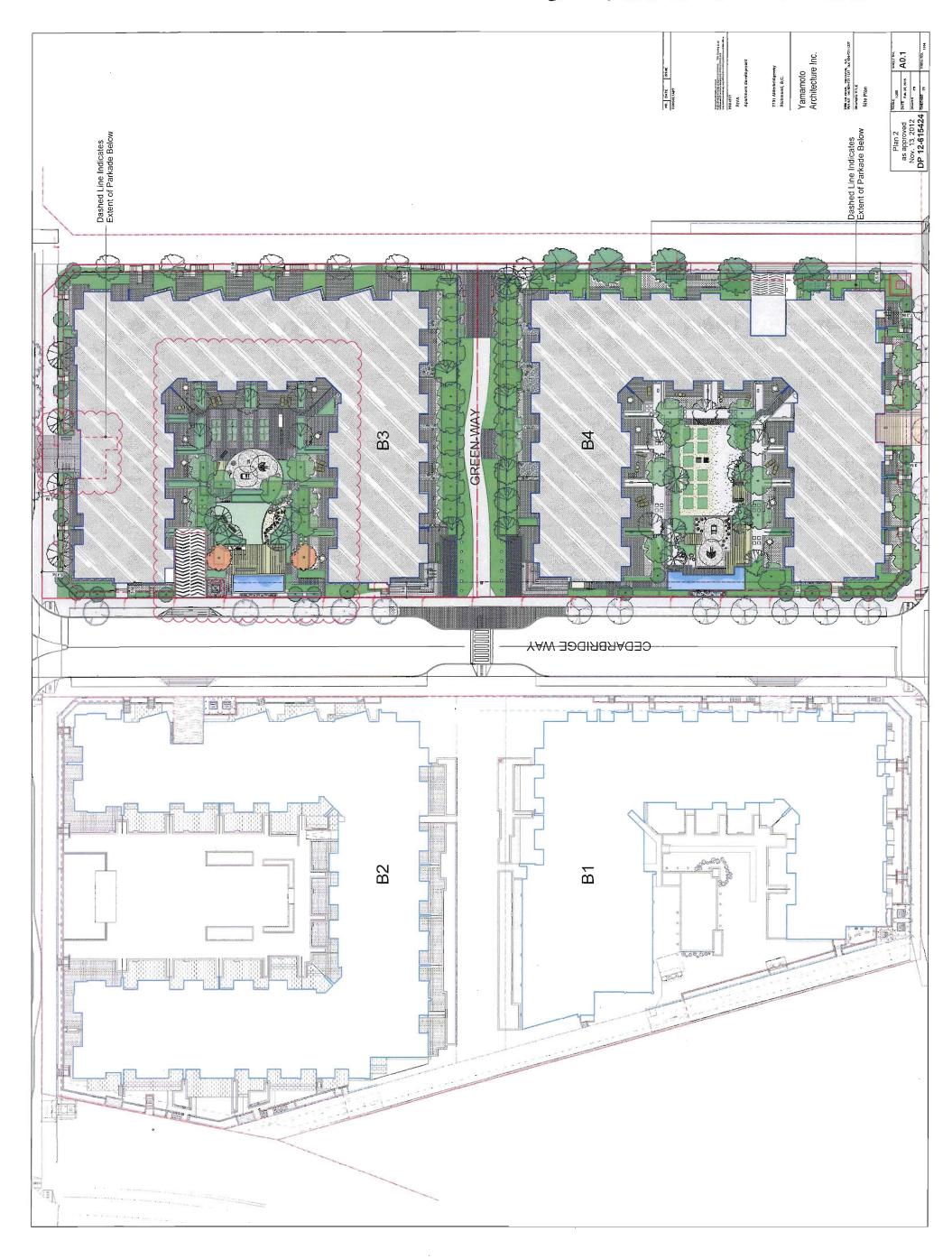




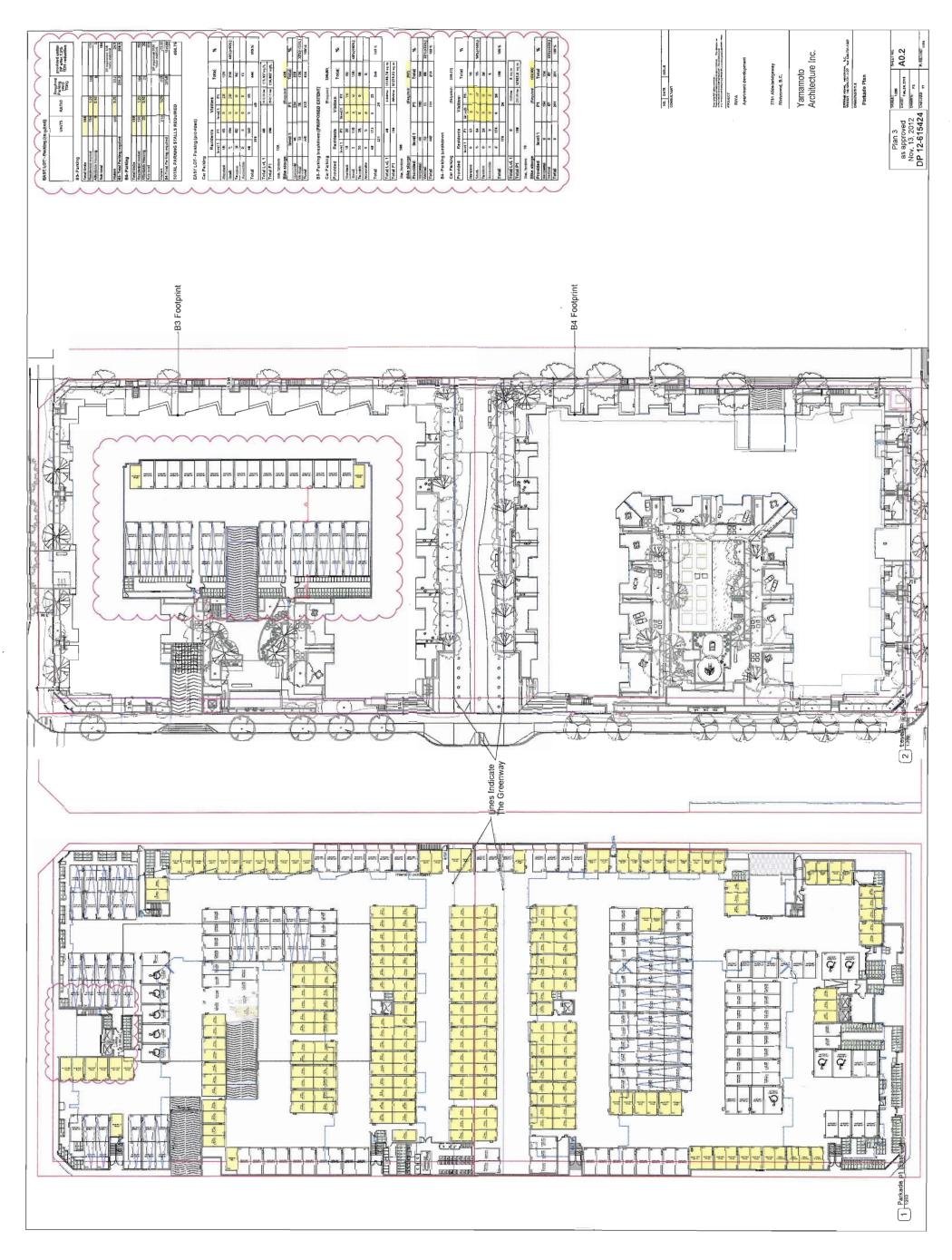
Yamamoto Architecture Inc. Perel Permit Panel Panel Panel (East Lot)



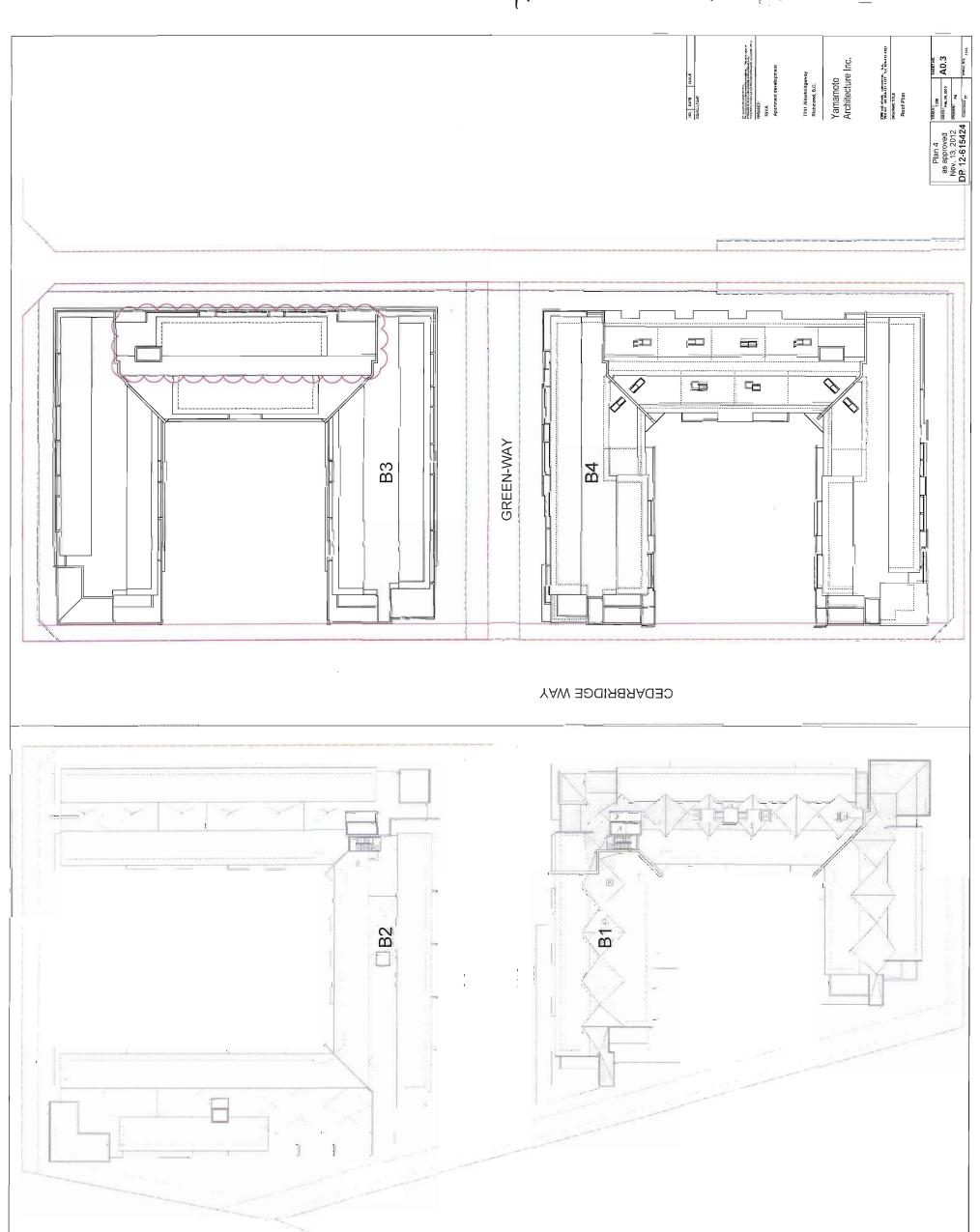
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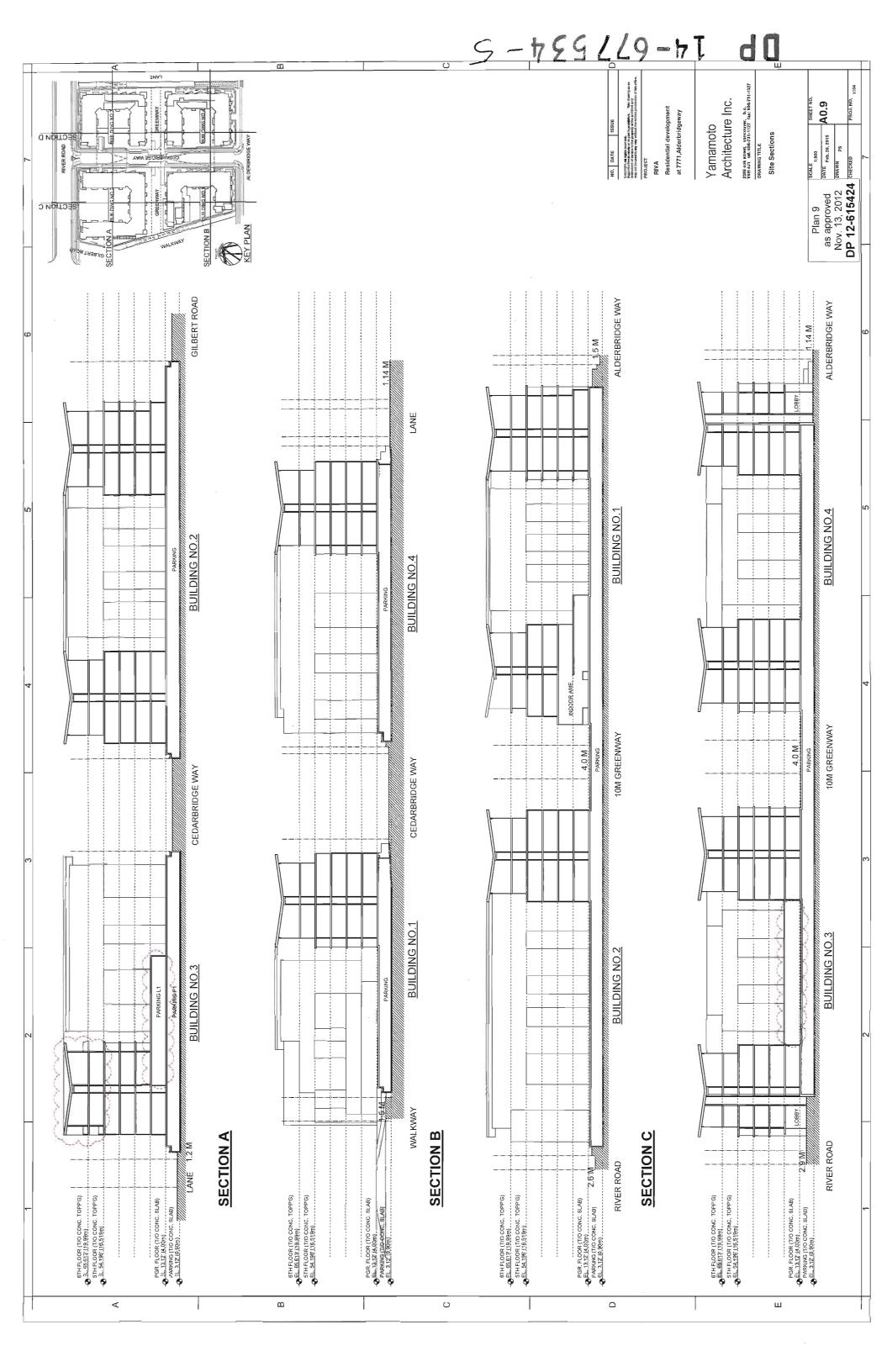


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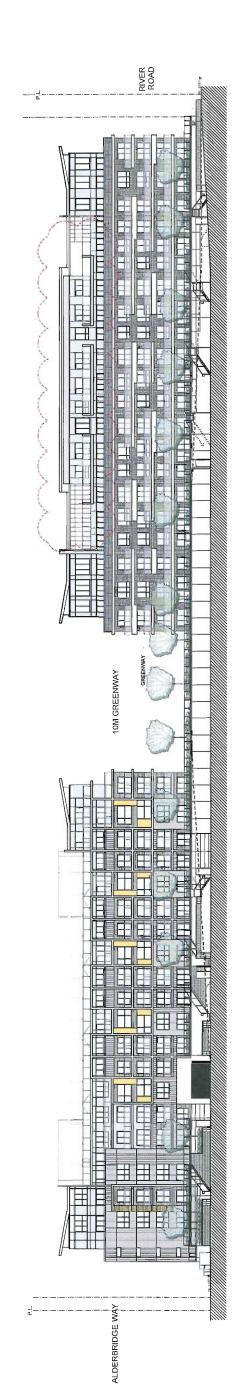
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Plan 73
as approved
Nov. 13, 2012

DP 12-615424

Yamamoto Architecture Inc.

7731 Alderbridgeway Richmond, B.C.

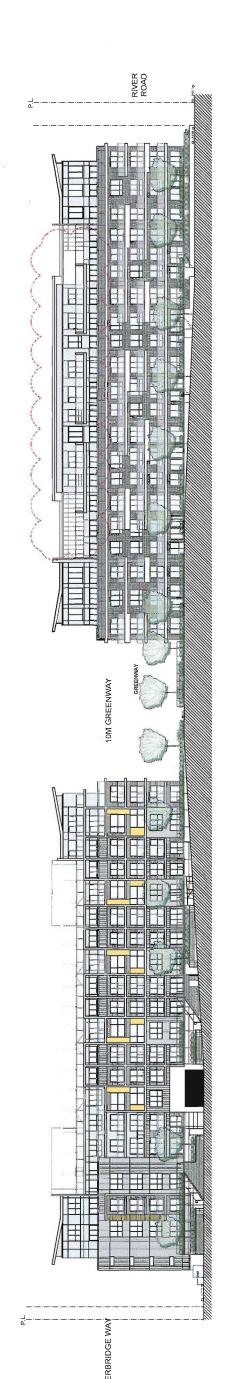


LANE INTERIM CONDITION (BUILDING 4 & 3 - EAST)

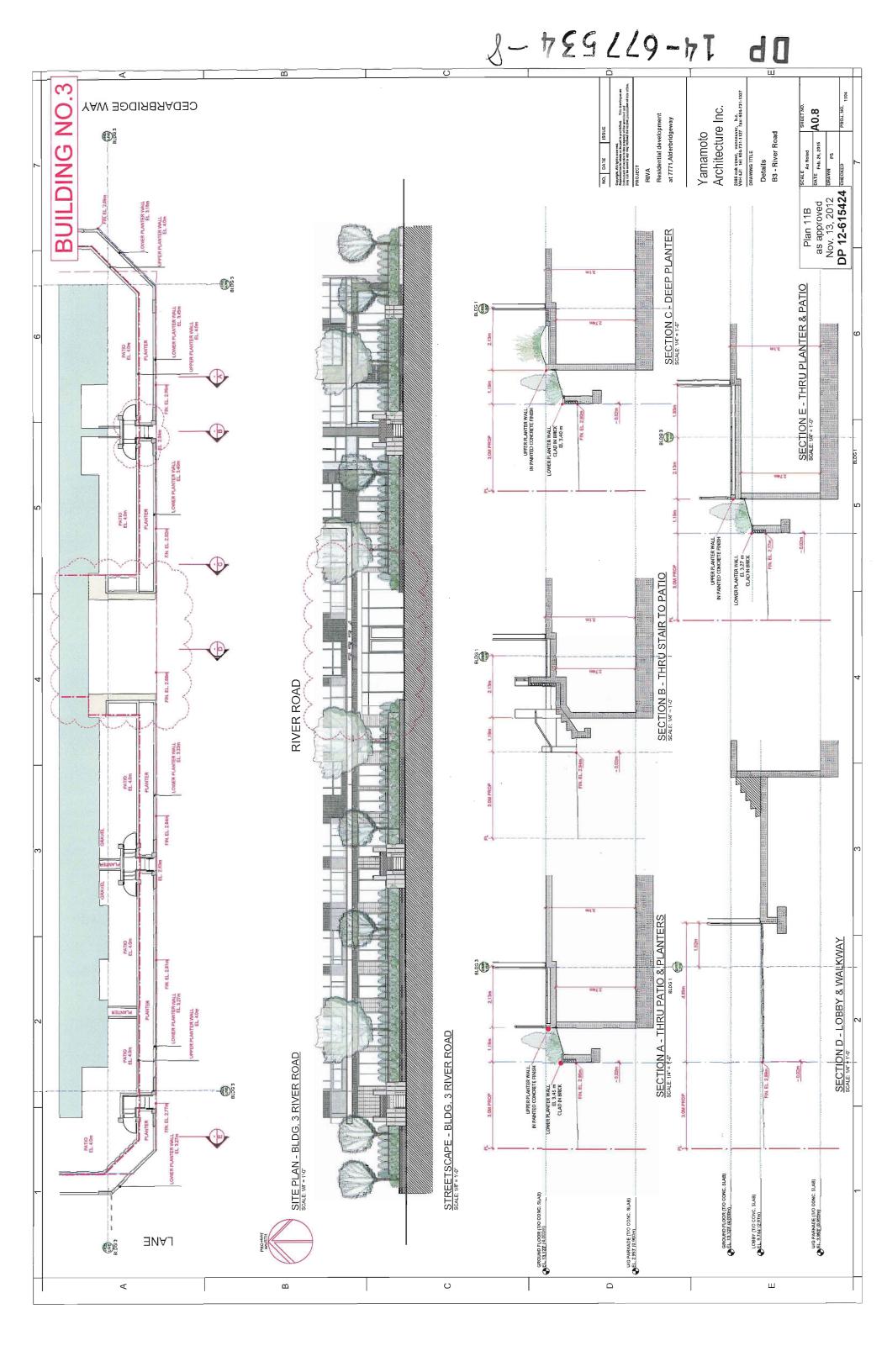
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Yamamoto Architecture Inc. 200 od anet, varoune, bc, vet al. in the station in

Feb. 28, 2015 AO. 5

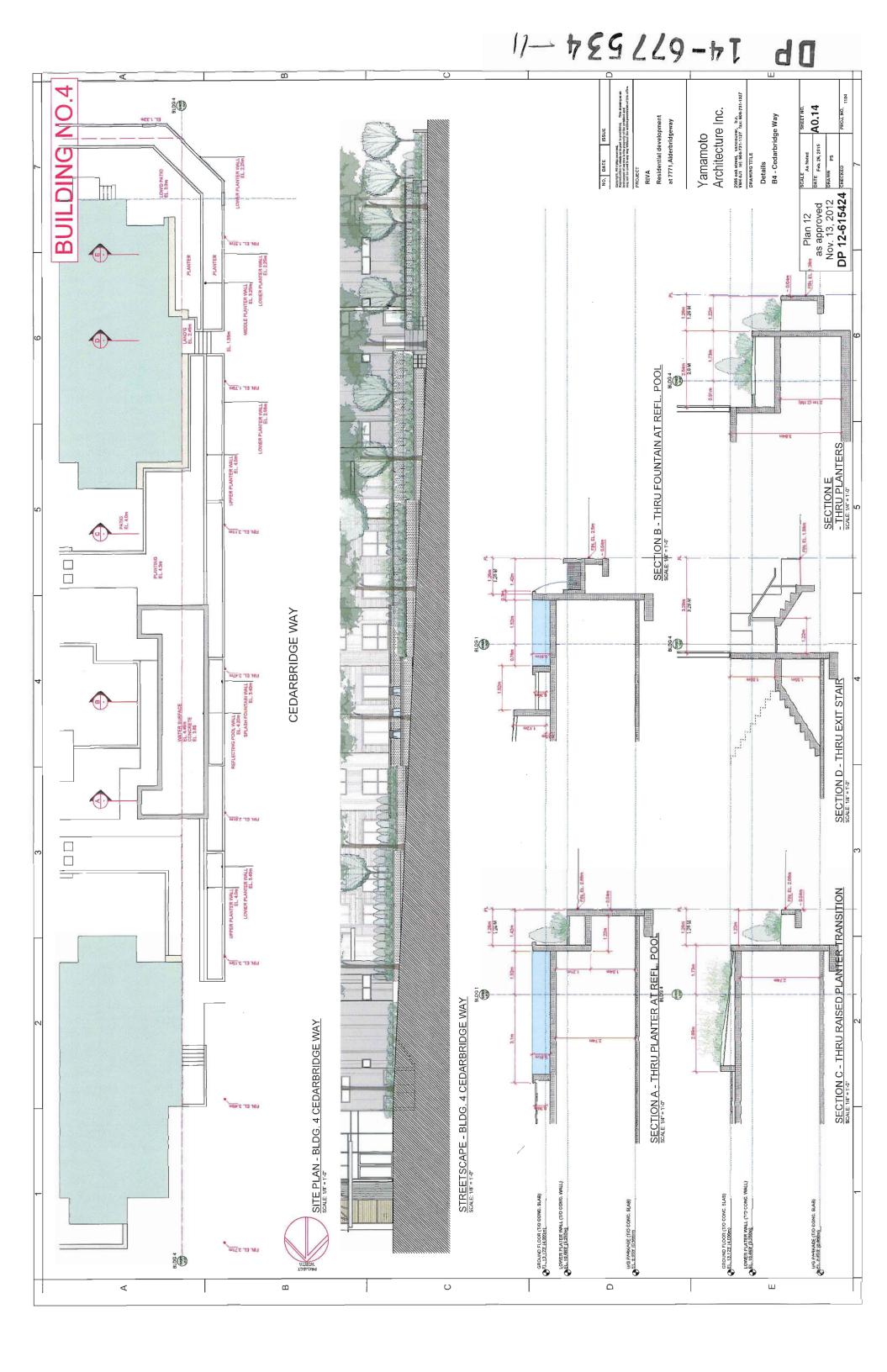


LANE ULTIMATE CONDITION (BUILDING 4 & 3 - EAST)

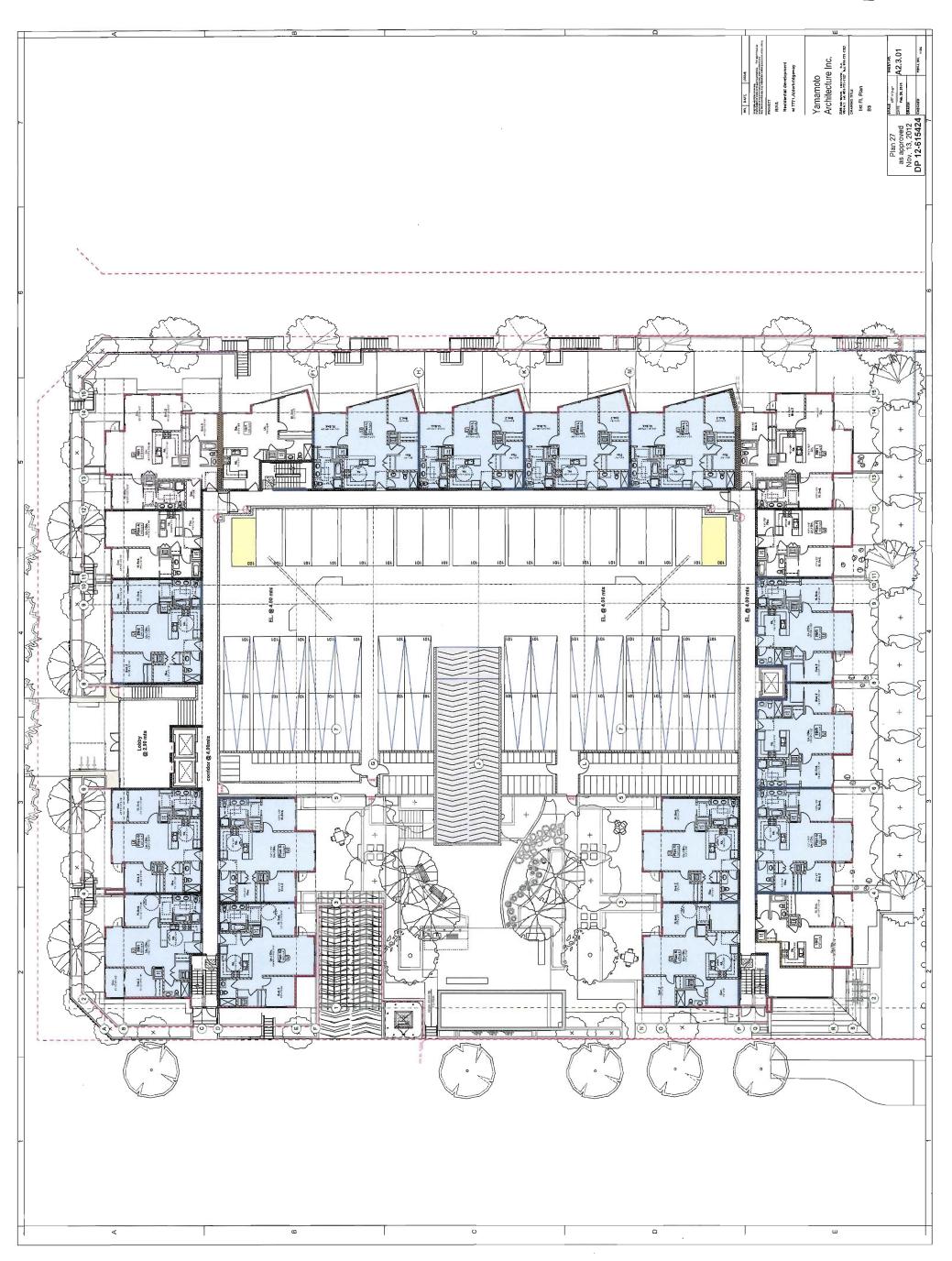


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V6H 4JI 10|: 604-731-1127 fax: 604-731-132
DRAWING TITLE Yamamoto Architecture Inc. Details B3 - Cedarbridge Way Blog 3 at 7771, Alderbridgeway SCALE As Noted DATE Feb. 26, 2015 Plan 11E SOATE FOR 28 BATE FOR 28 NOV. 13, 2012 DRAWN P. DP 12-615424 GHEDRED (E) FIN. EL. 3.79m FIN. EL. 3.69m Trellis above B3 Entry Ramp SECTION B - THRU EXIT STAIRS SECTION D - THRU B3 ENTRY RAMP CEDARBRIDGE WAY 3.02m SECTION C - THRU FOUNTAIN AT REFL. POOL SOALE:10" - 110" SECTION A - THRU PLANTERS STREETSCAPE - BLDG. 3 CEDARBRIDGE WAY, CONTROL OF STOLE 1/8" = 1-0" Stoke work SISTEMPLAN - BLDG, 3 CEDARBRIDGE WAY SOLE: 16F = 1-0F aloo2 - 0.02m BLOG 3 UPPER TER WALL EL. 4.0m LOWER VTER WALL EL 3.25m ⋖ ပ

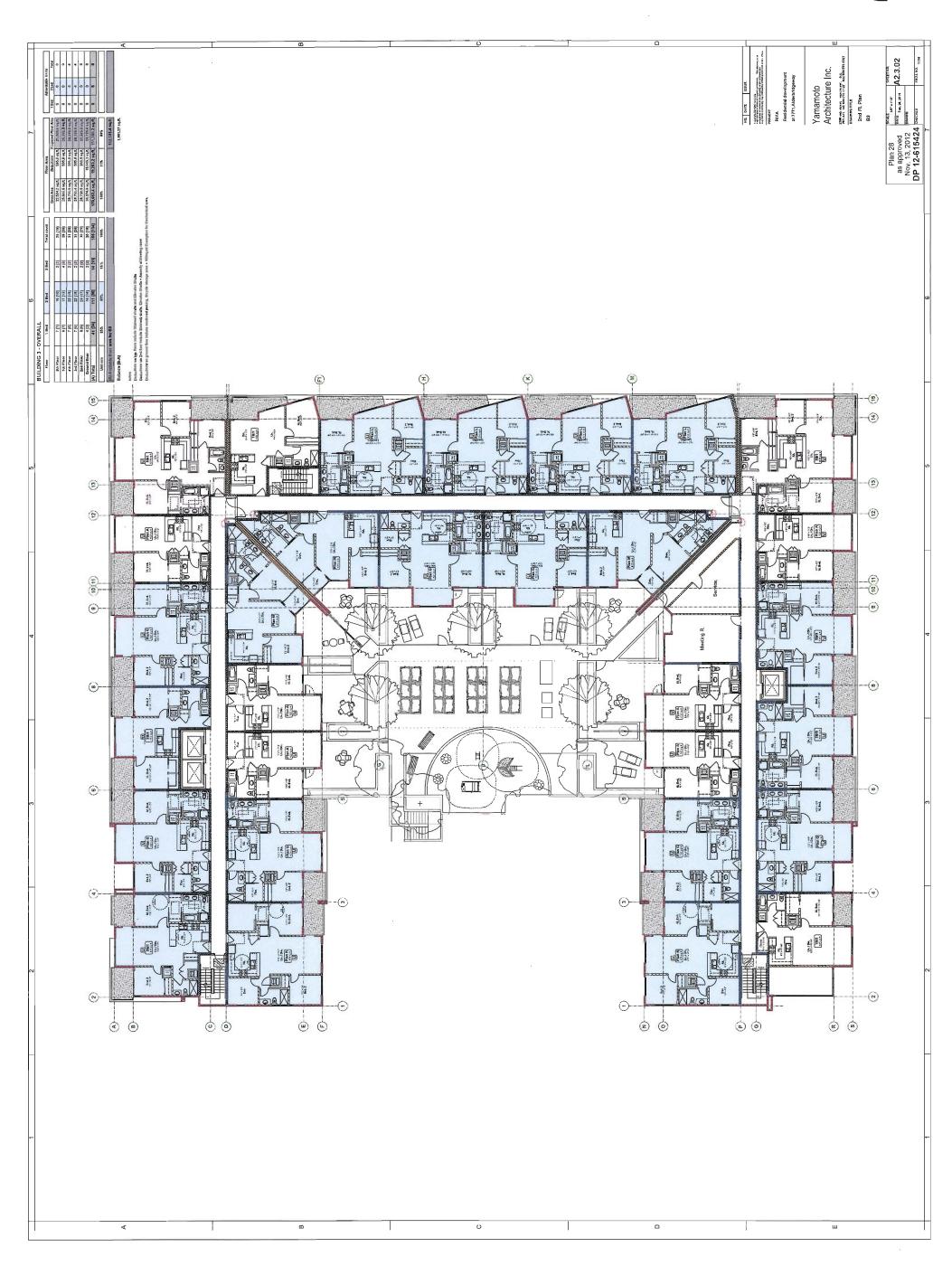
01- 789 229-71 **BUILDING NO.4** 2386 ank ervelt, vancouver, b.c.
Veh 4.11 let; 604-731-1127 faz: 604-731-1137
Datawins TITLE
Details
B4 - Alderbridge Way Yamamoto Architecture Inc. A0.6 Residential development at 7771, Alderbridgeway $\exists NA \exists$ SCALE As Noted DATE Fob, 26, 2015 BLOG 4 Plan 10B as approved Nov. 13, 2012 DP 12-615424 SECTION D - THRU EAST PLANTERS SCALE 140" - 1-0" SECTION B - THRU PLANTER/TANKED PORTION LOWER PLANTER WALL
EL. 1.57m (AVE. HT. 0.45m)
UPPER PLANTER WALL
EL. 2.62m ALDERBRIDGE WAY 8Eps 4 SECTION A - THRU LOWER STAIRS/TANKED PARKADE PORTION LOWER PLANTER WALL
EL 1.68m (AVE. HT. 0.45m)
UPPER PLANTER WALL
EL. 2.7m 2.63 M STREETSCAPE - BLDG, 4 ALDERBRIDGE WAY SITE PLAN - BLDG. 4 ALDERBRIDGE WAY SCALE 1/8" = 1.0" BBY SECTION C - THRU LO 2.64m 2.64M 800 8 CEDARBRIDGE WAY ⋖ α \circ



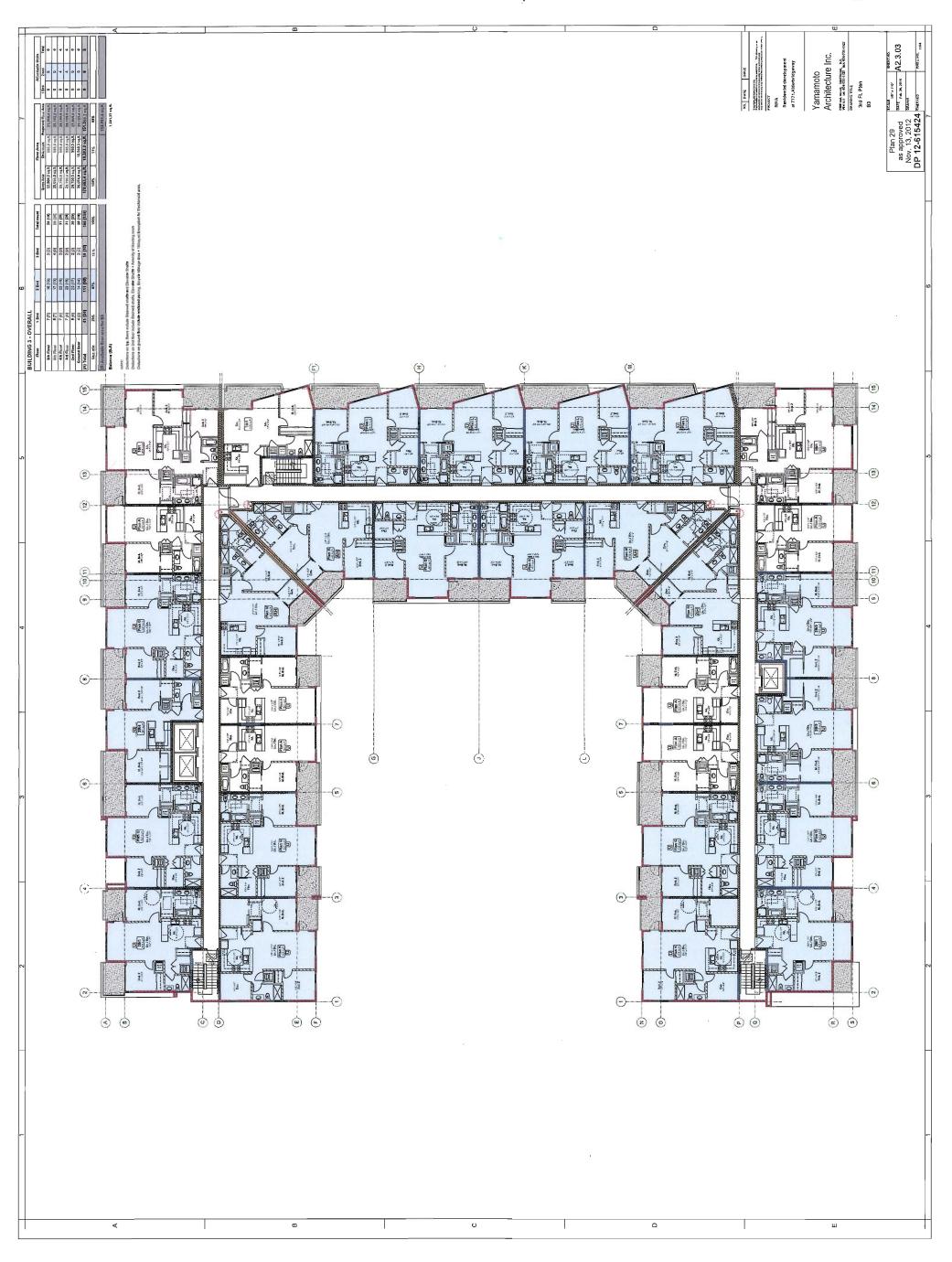
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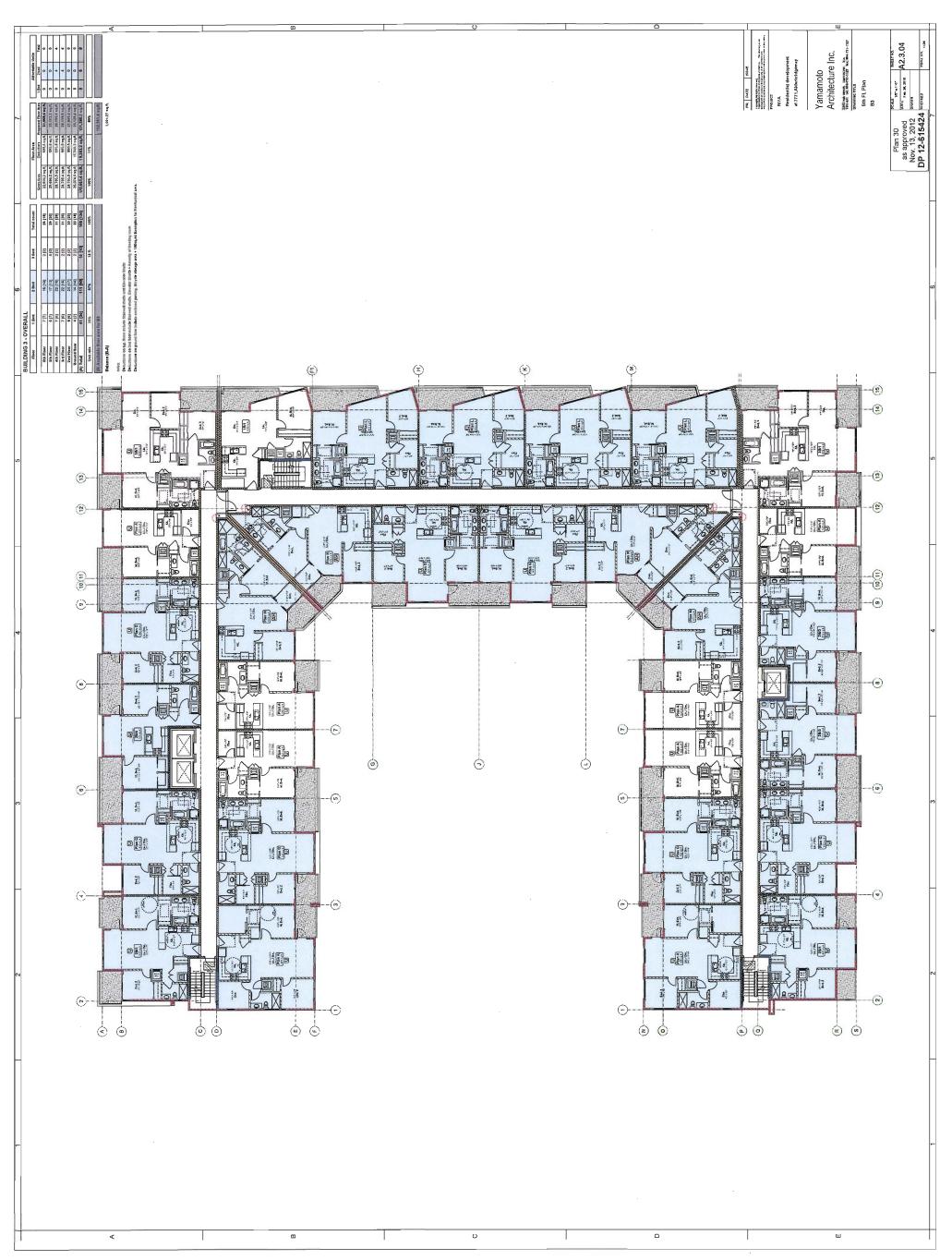
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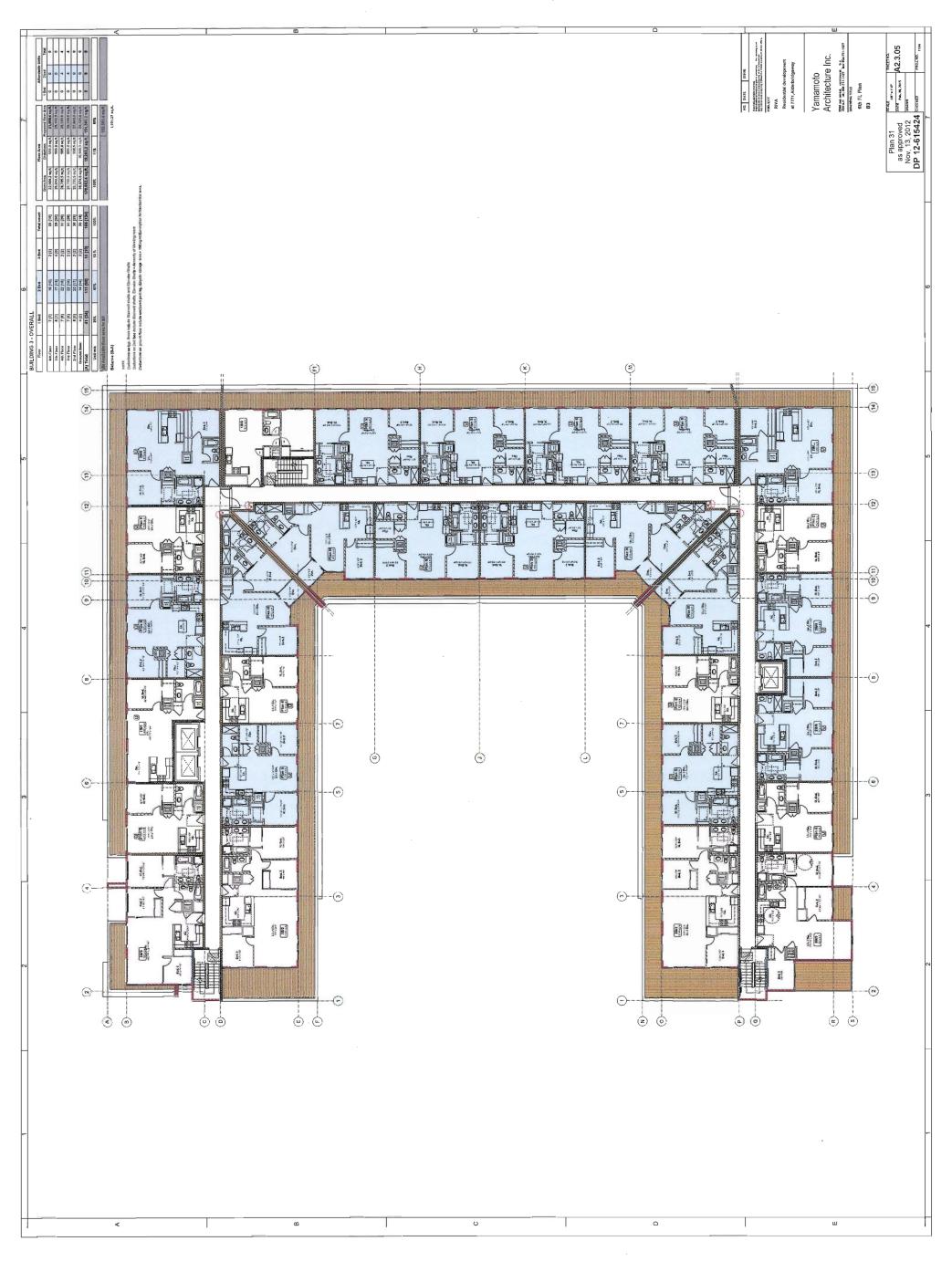
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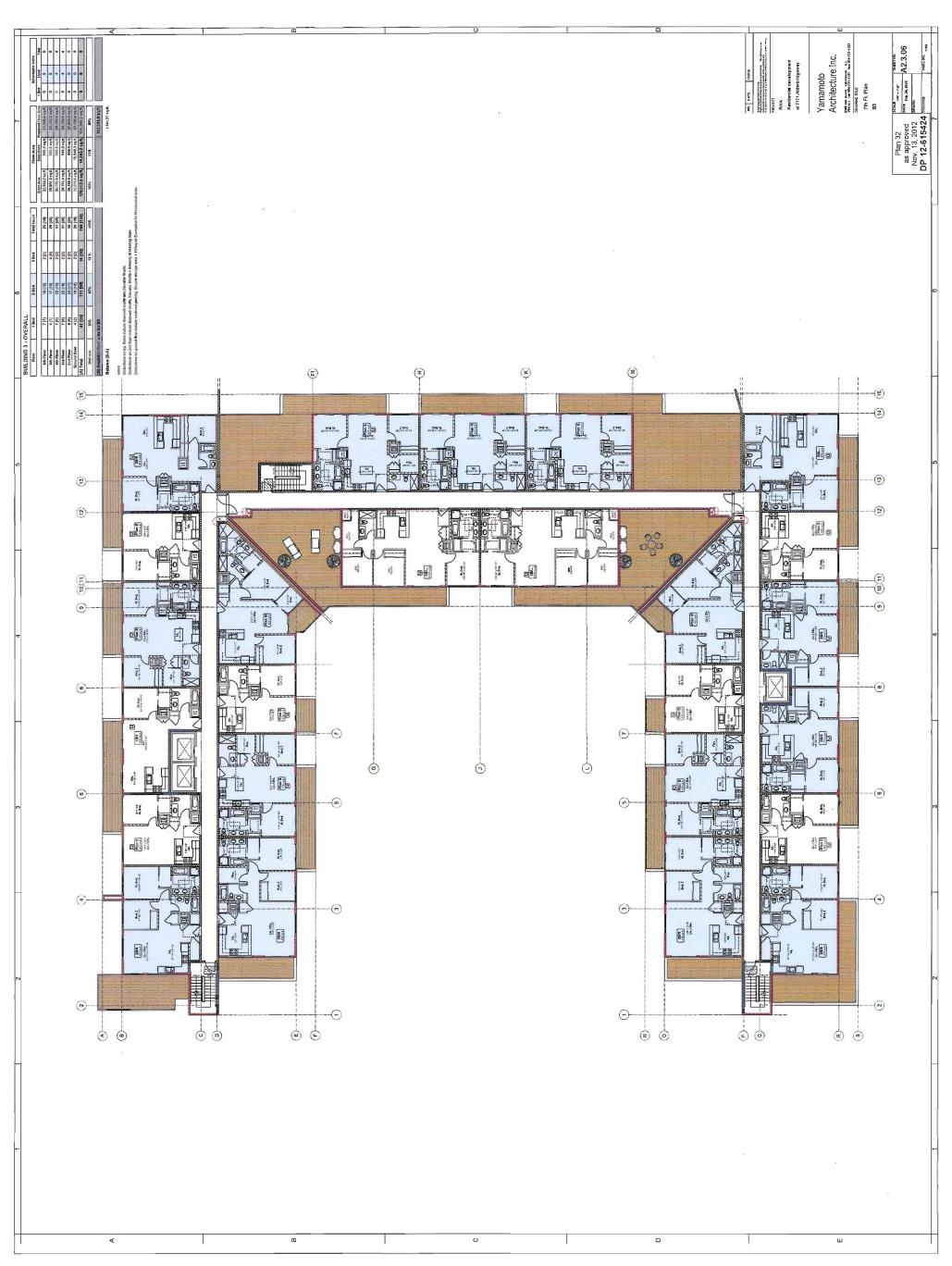
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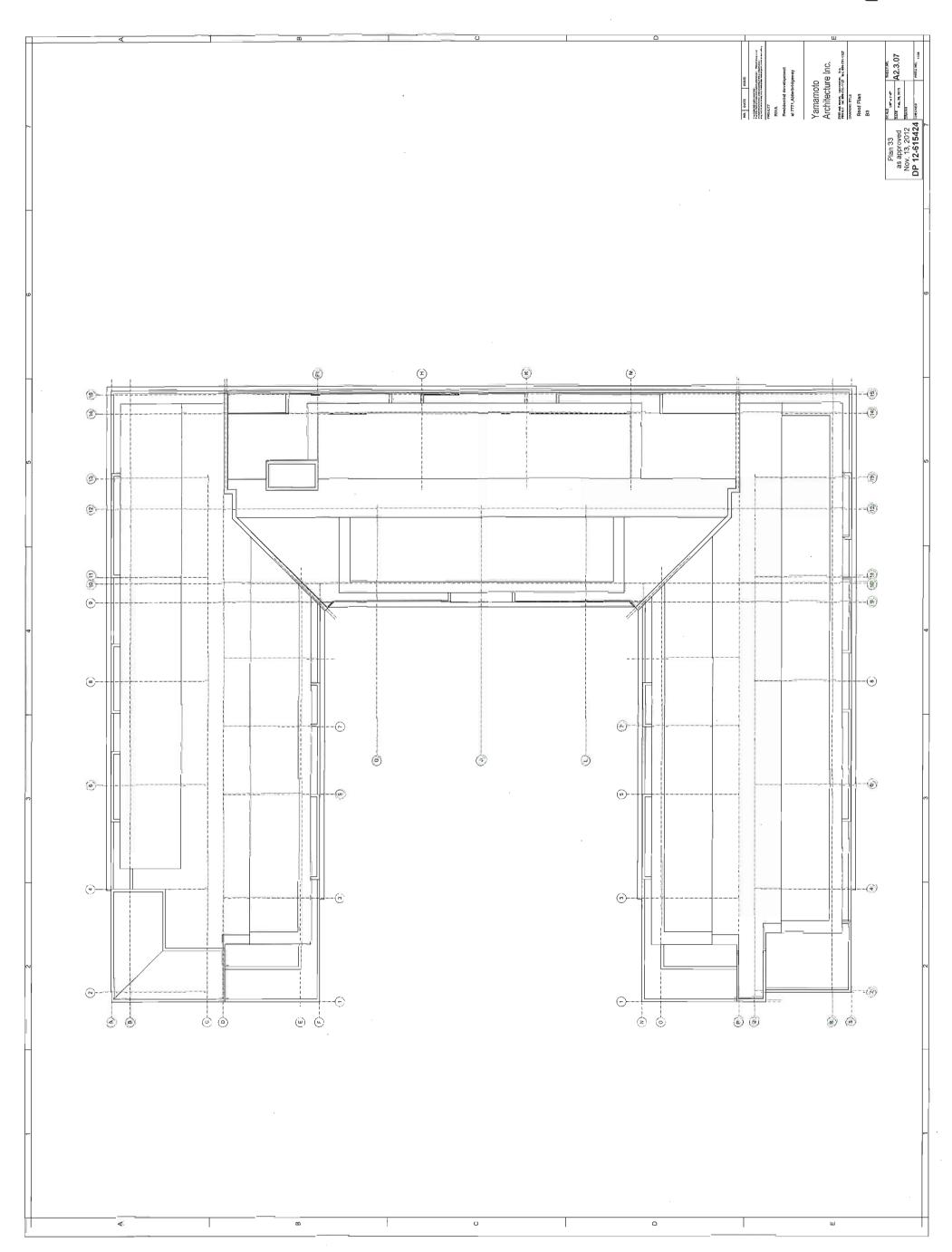


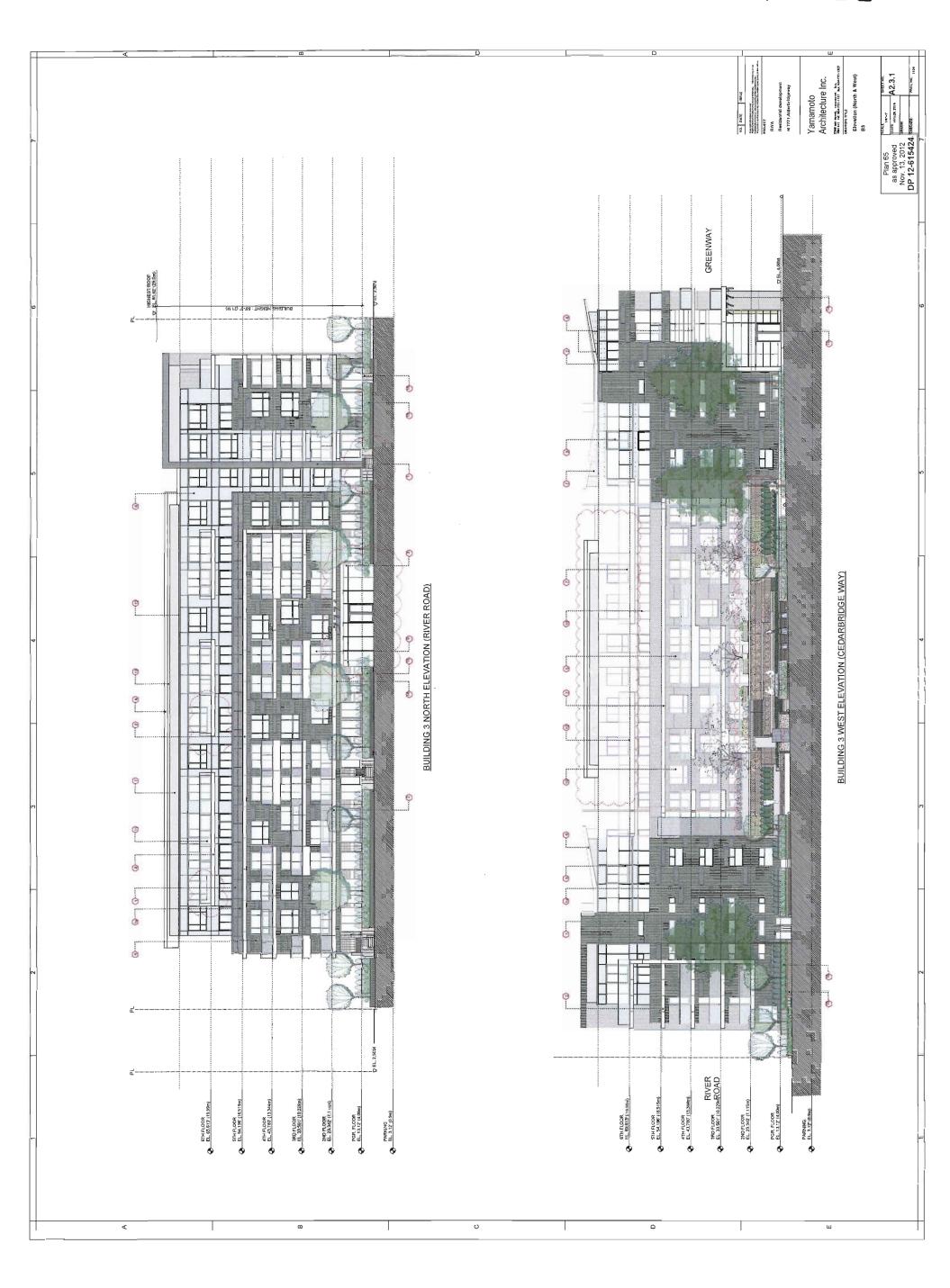
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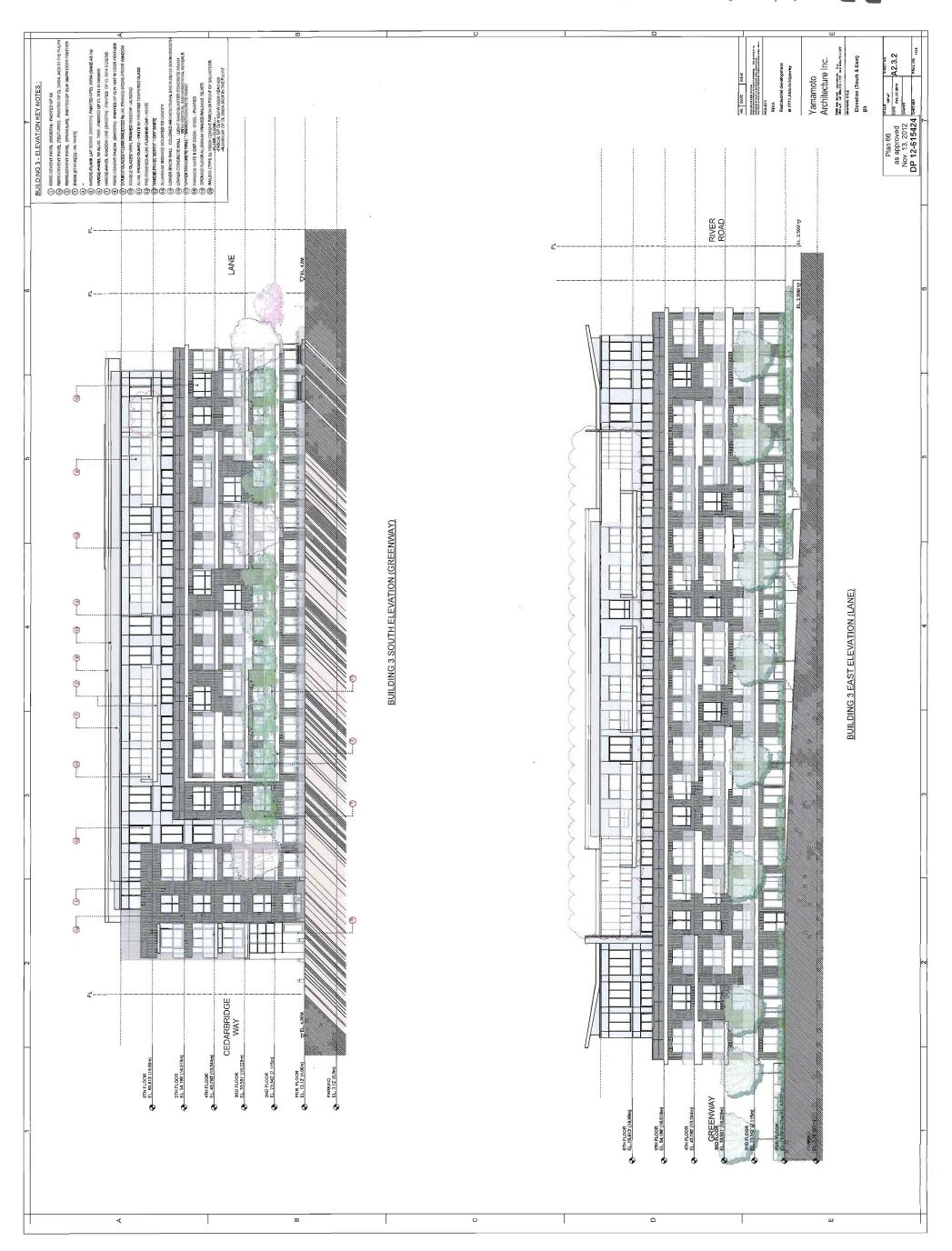
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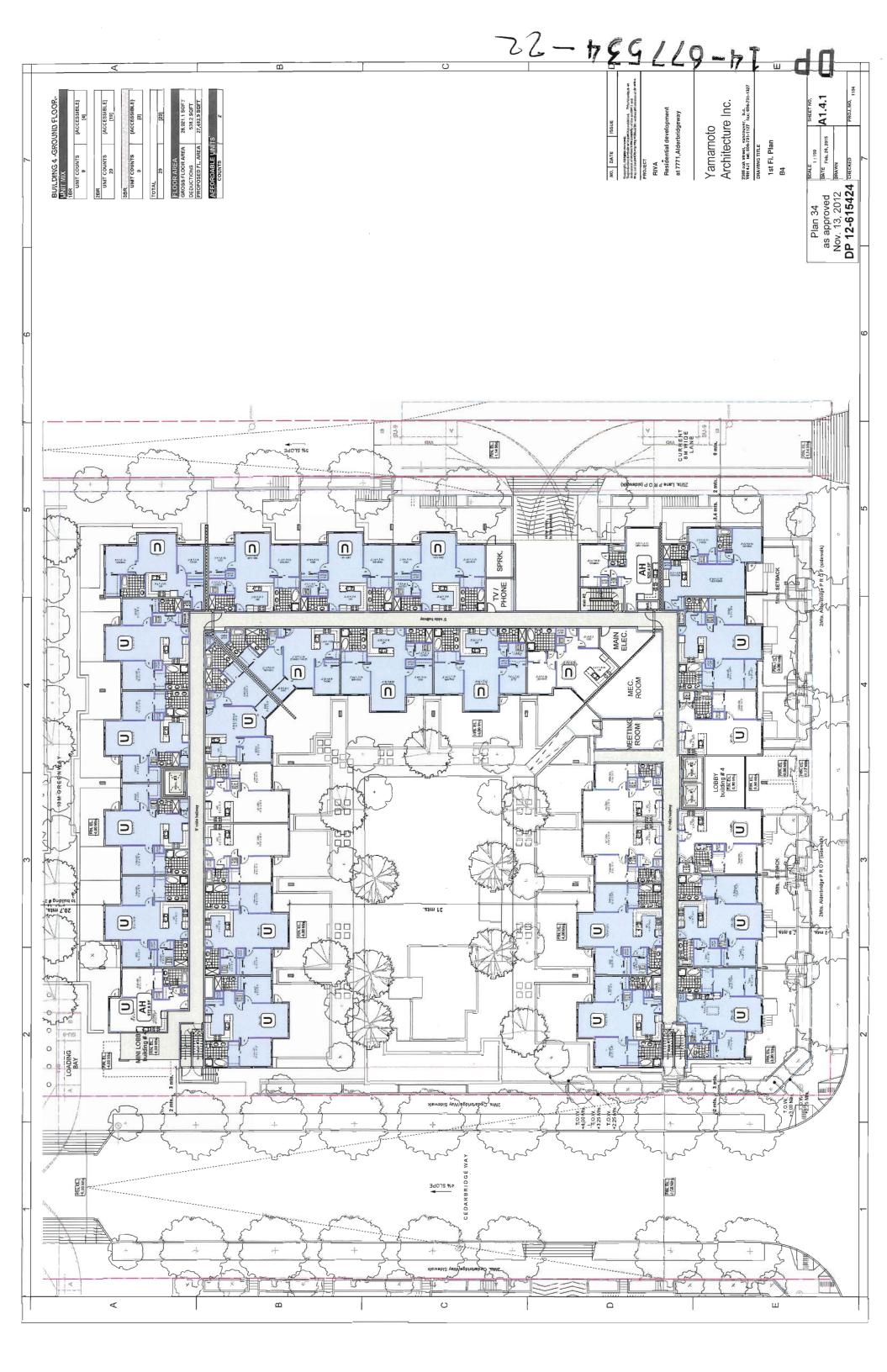


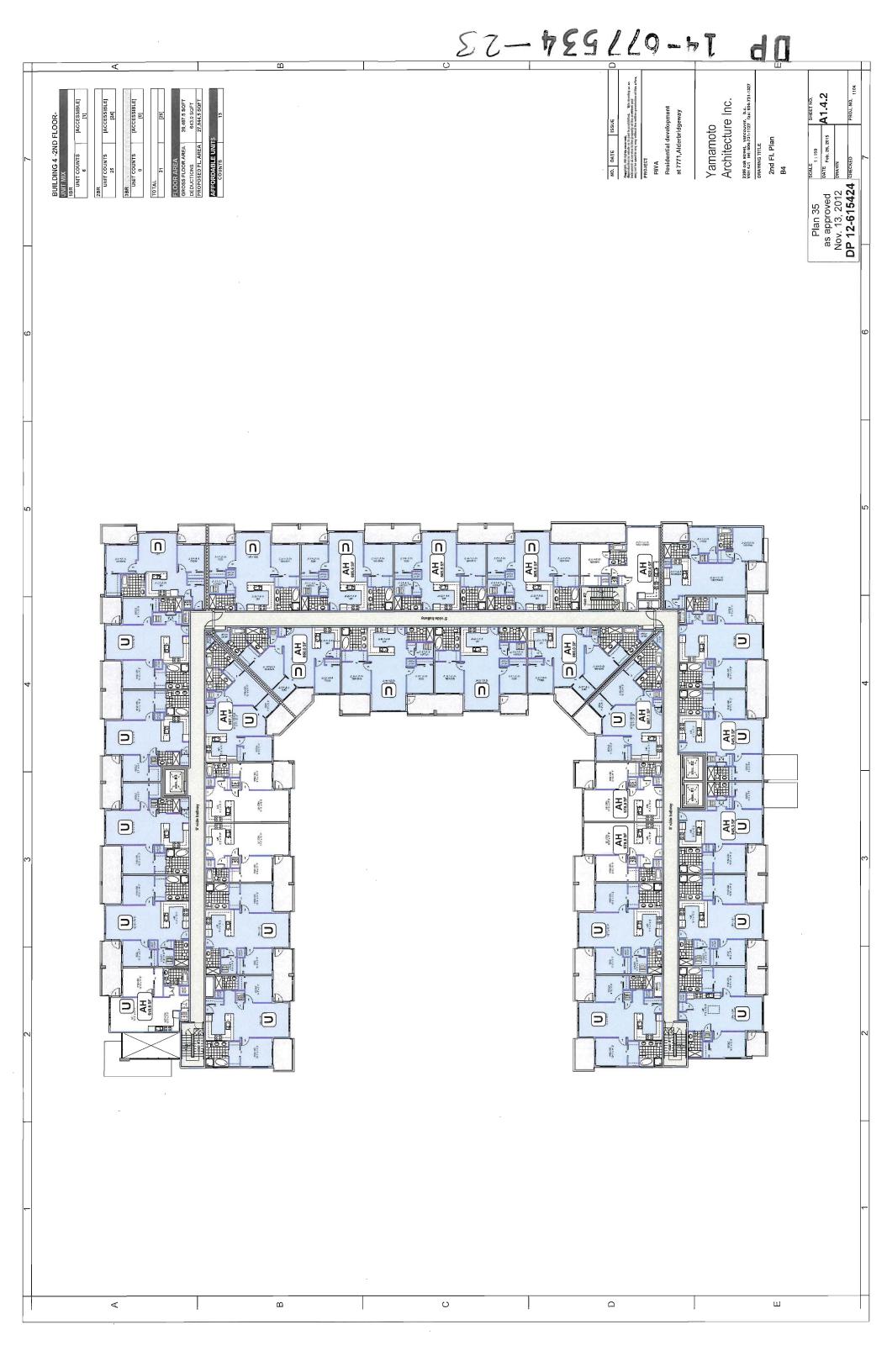


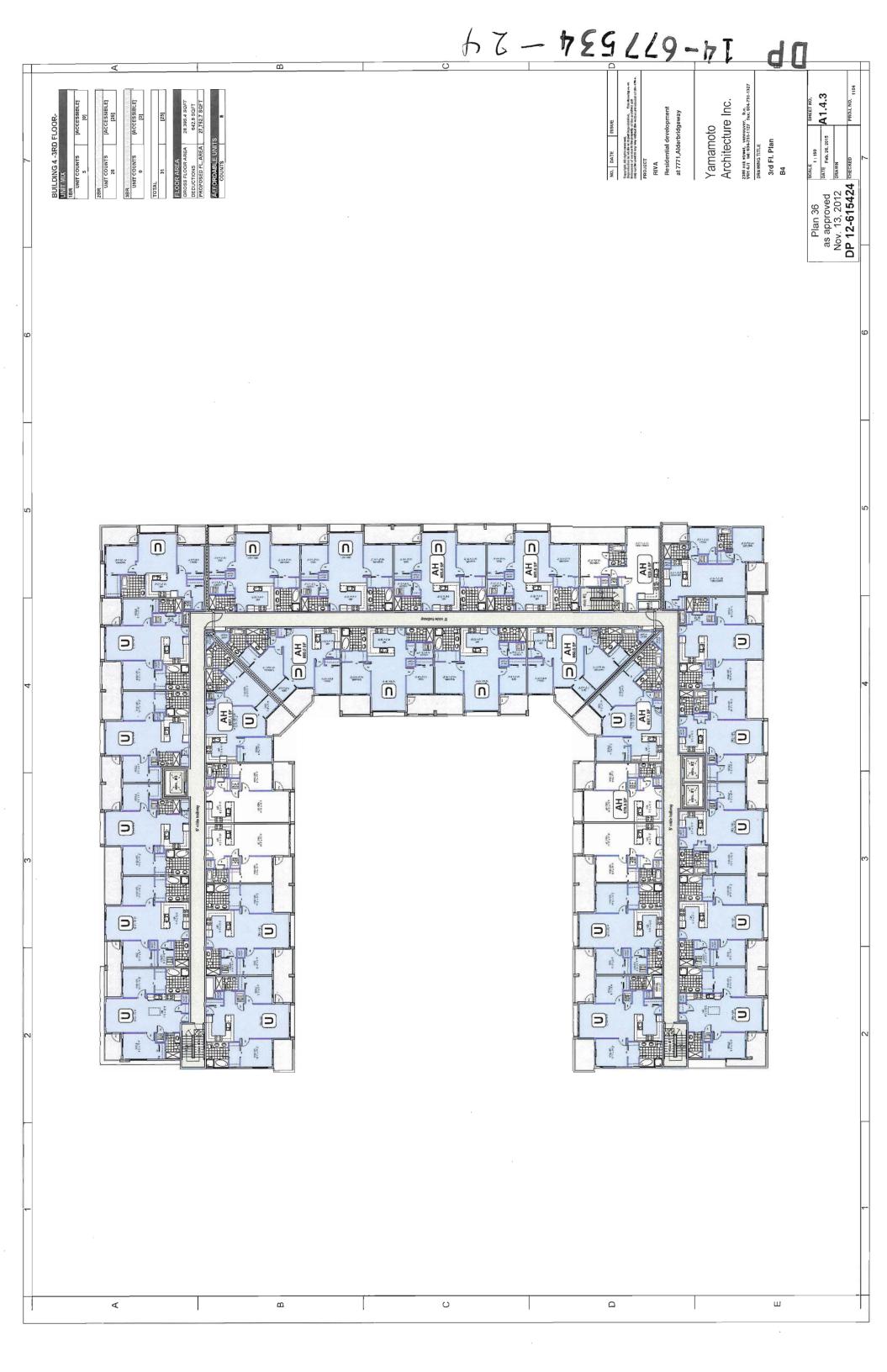
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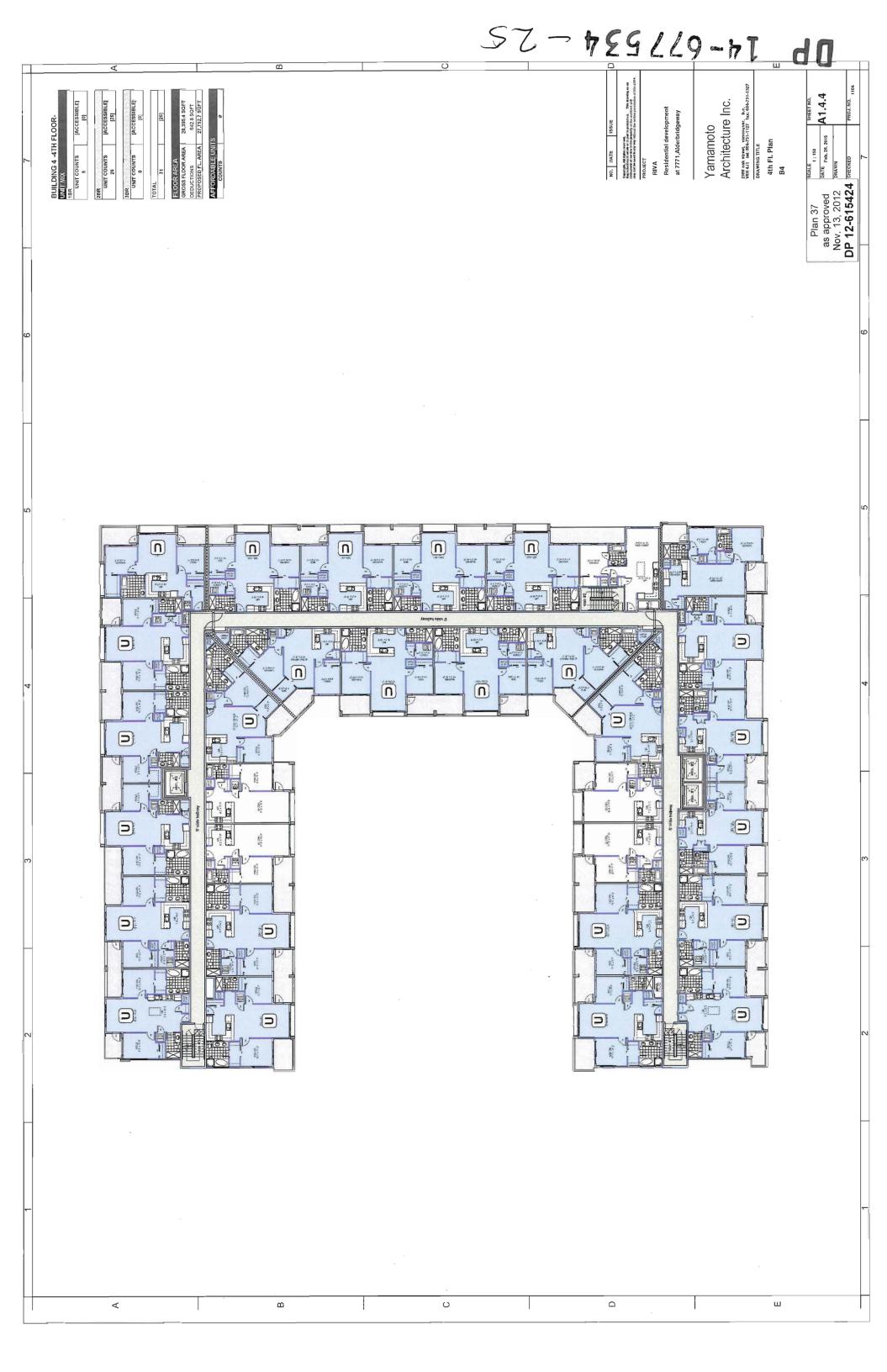




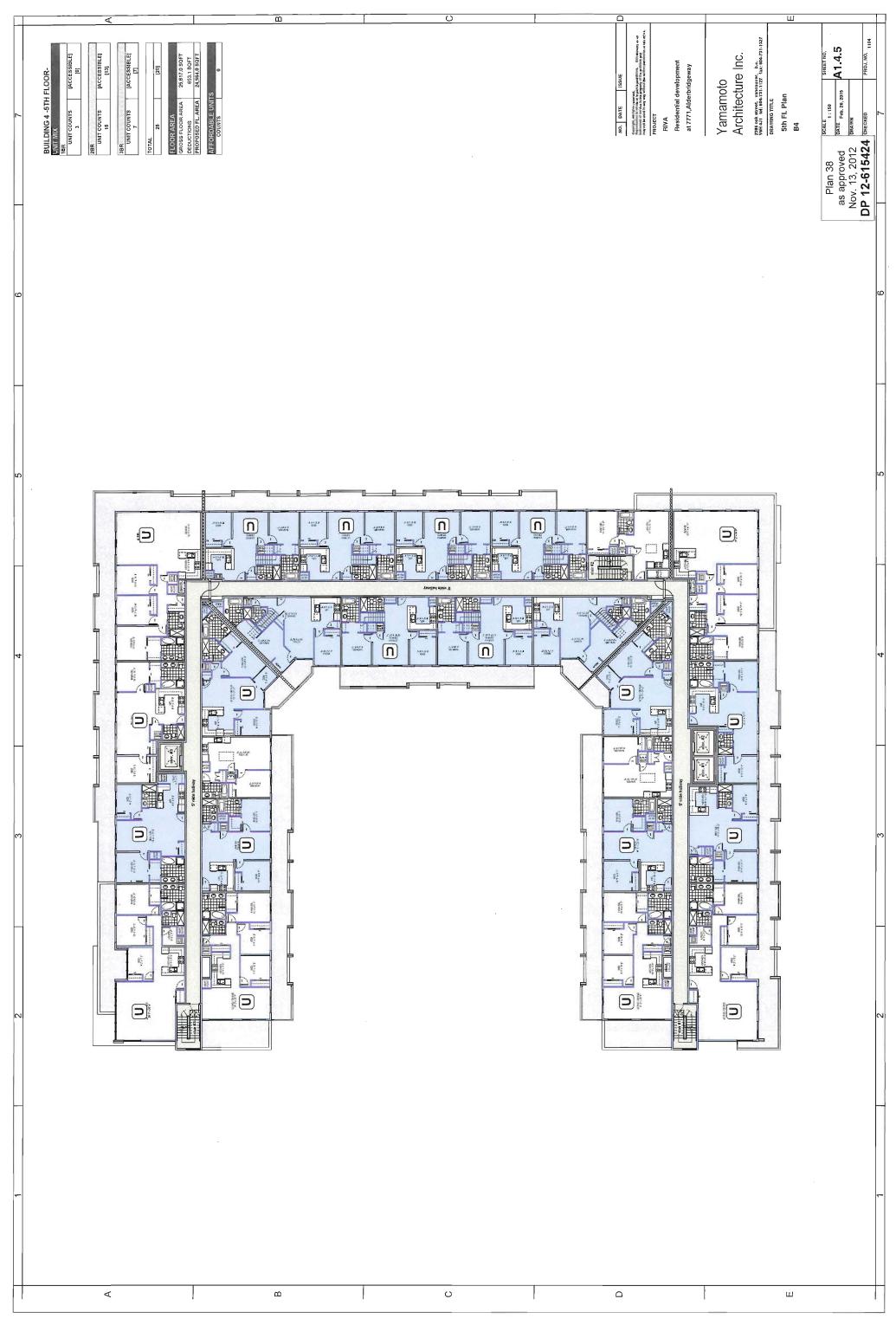


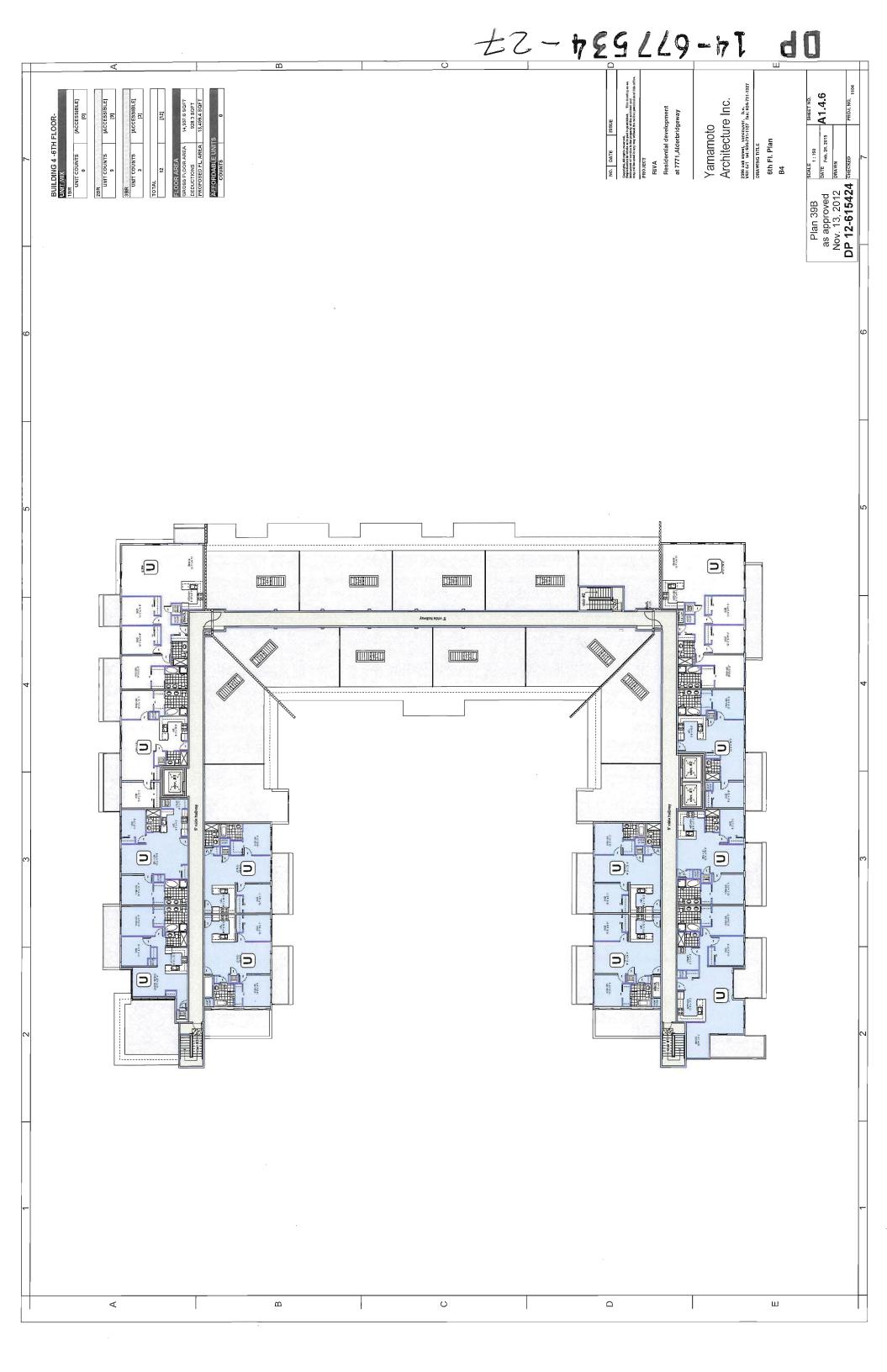


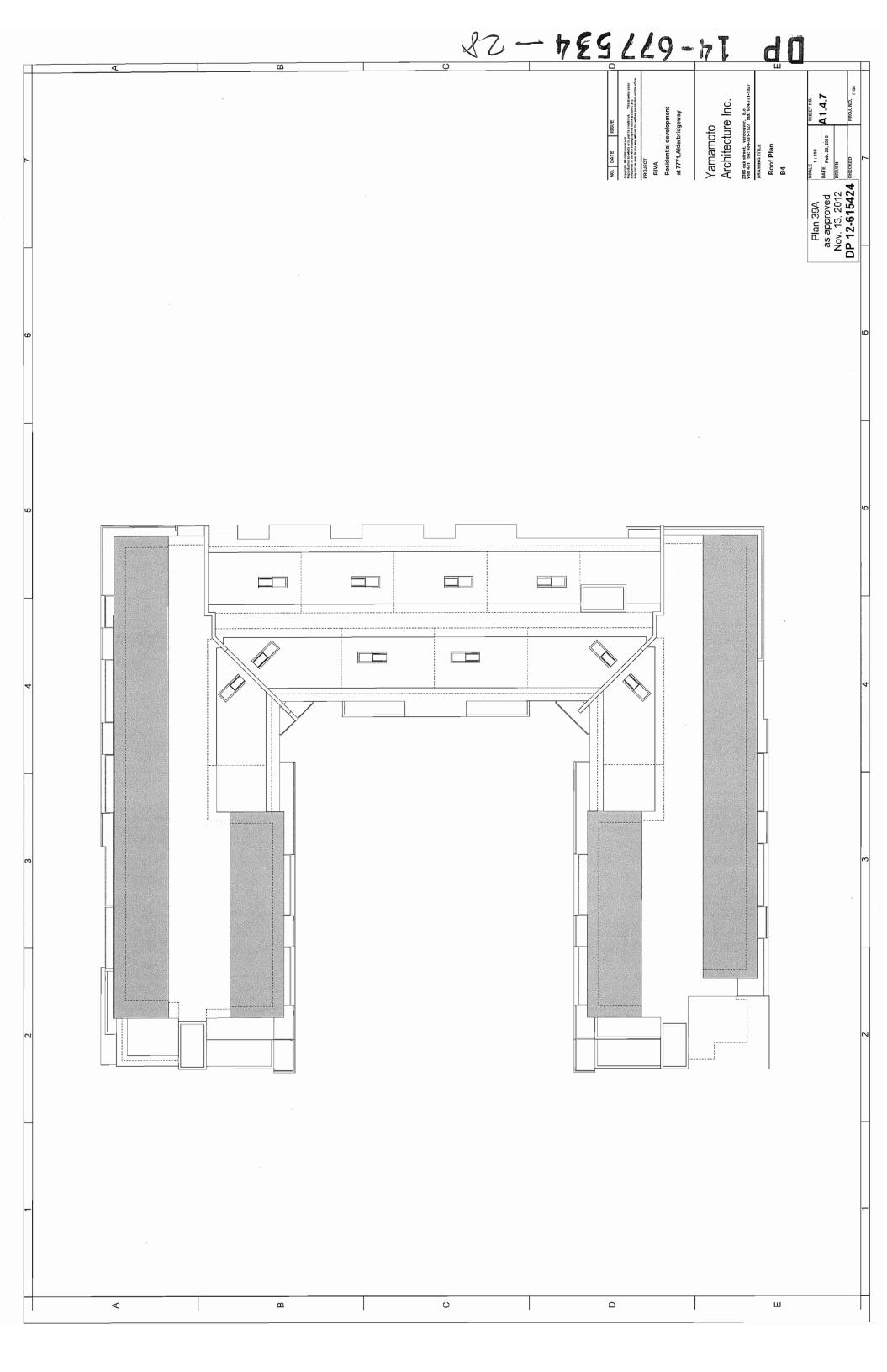




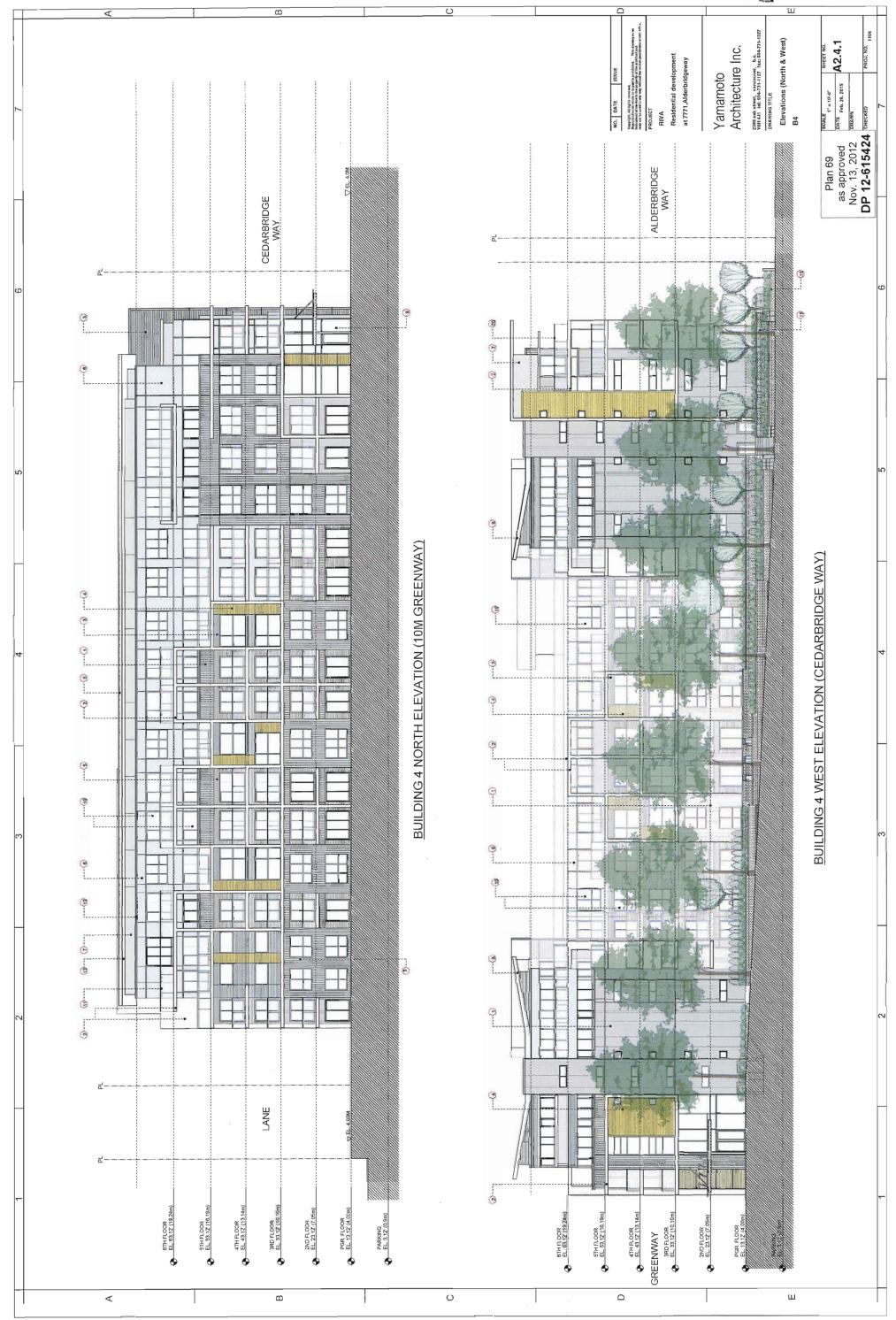
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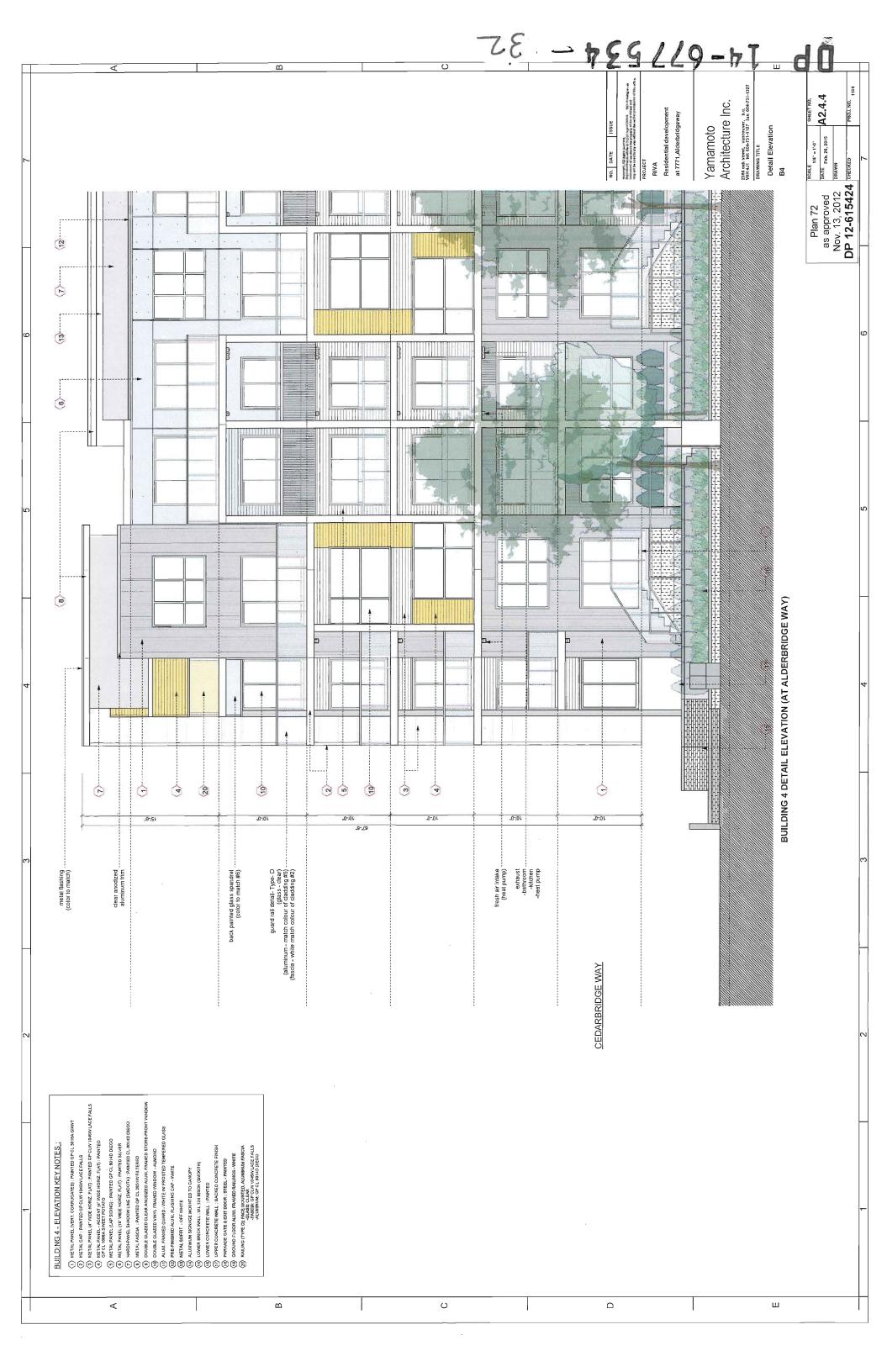


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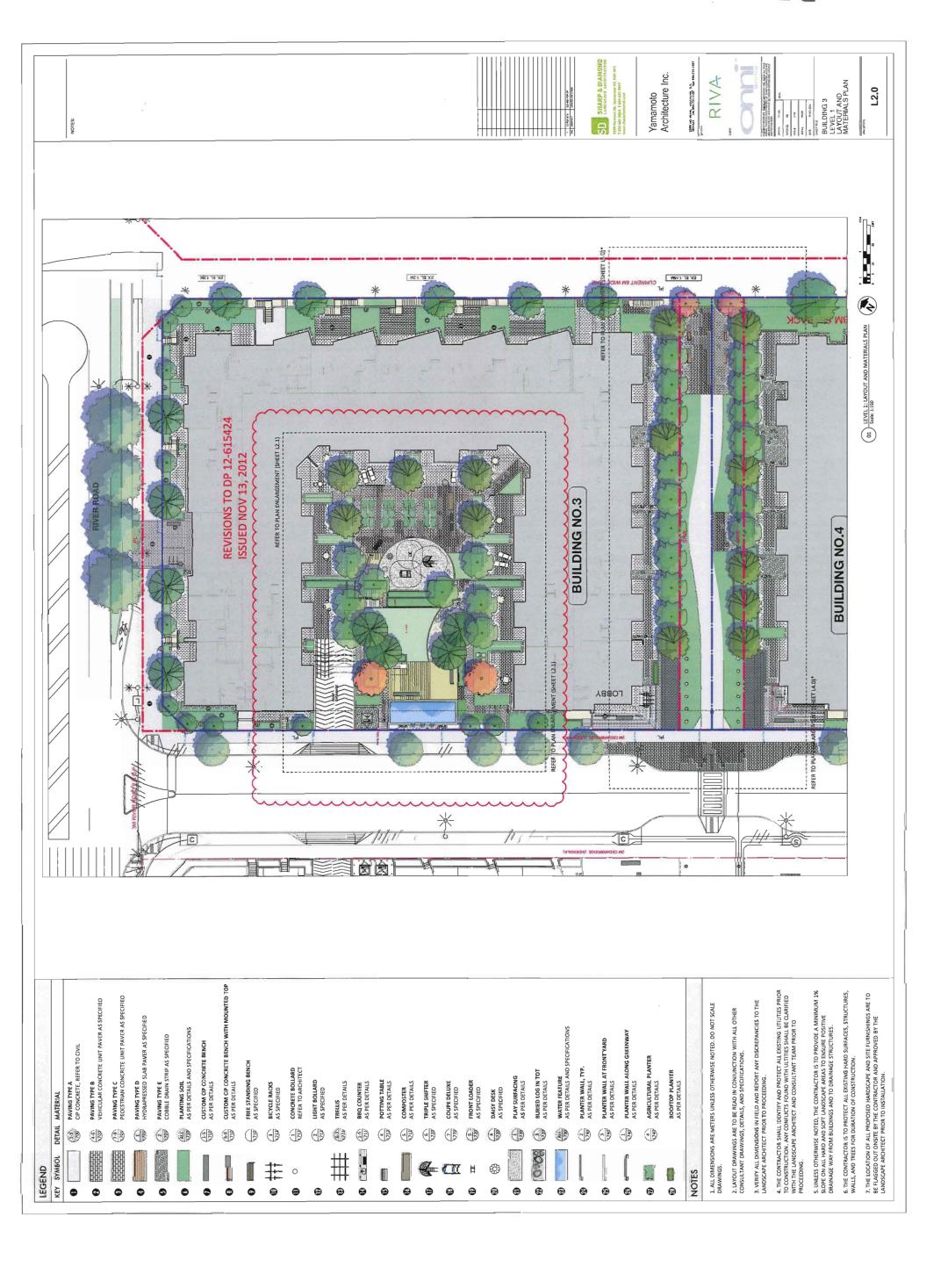
08 - 789 LL9-71 2386 pak street, vancouver, b.c. V6H 4J1 tel: 604-731-1127 1ax: 604-731-1327 DRAWING TITLE A2.4.2 Elevations (South & East) B4 Architecture Inc. at 7771, Alderbridgeway Yamamoto SCALE 1° = 10°0" DATE Feb. 26, 2015 DRAWN Plan 70 as approved Nov. 13, 2012 **DP 12-615424** LANE G **BUILDING 4 SOUTH ELEVATION (ALDERBRIDGE WAY) BUILDING 4 EAST ELEVATION (LANE)** CEDARBRIDGE WAY O



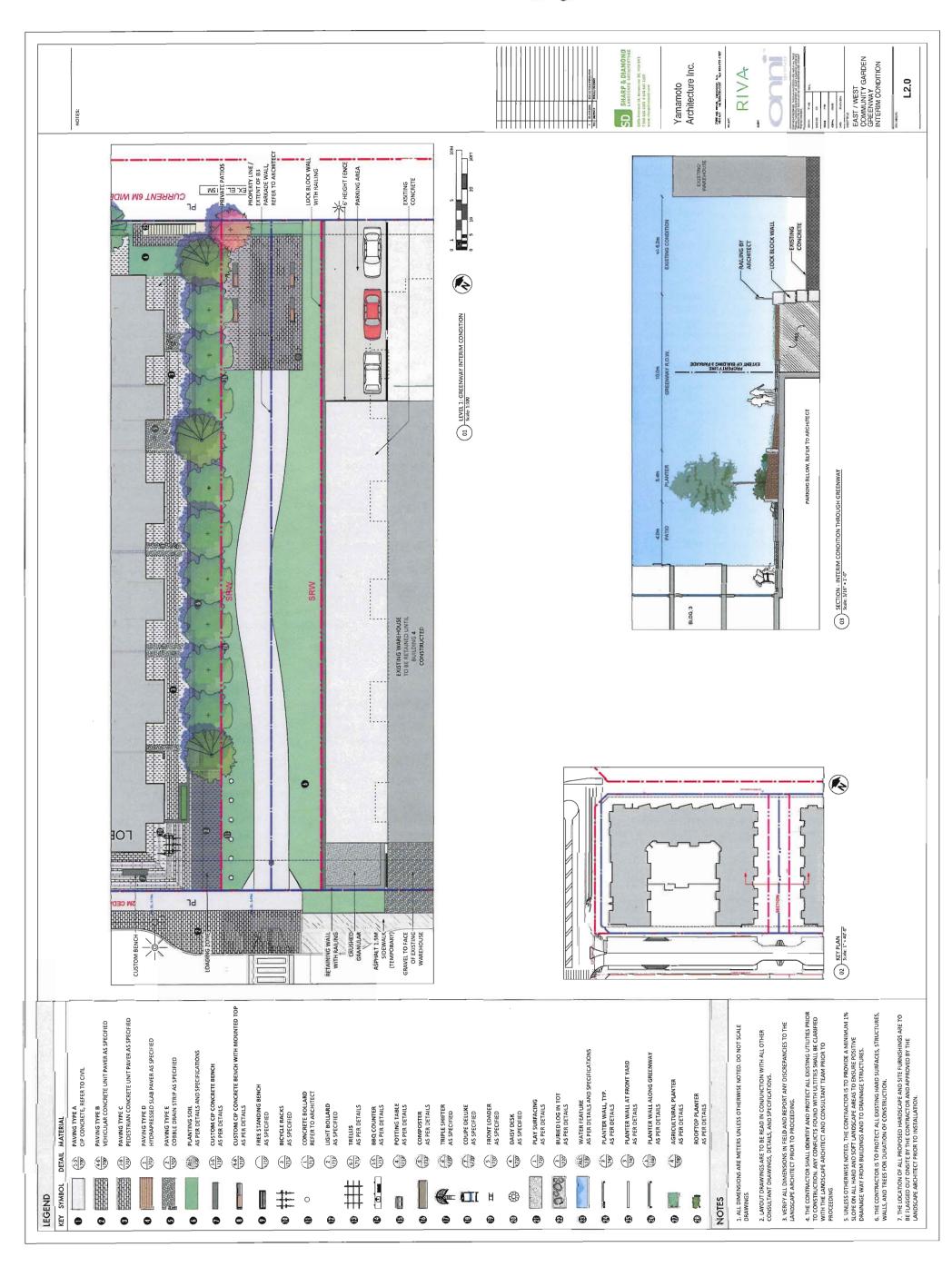




48-789119-71 do



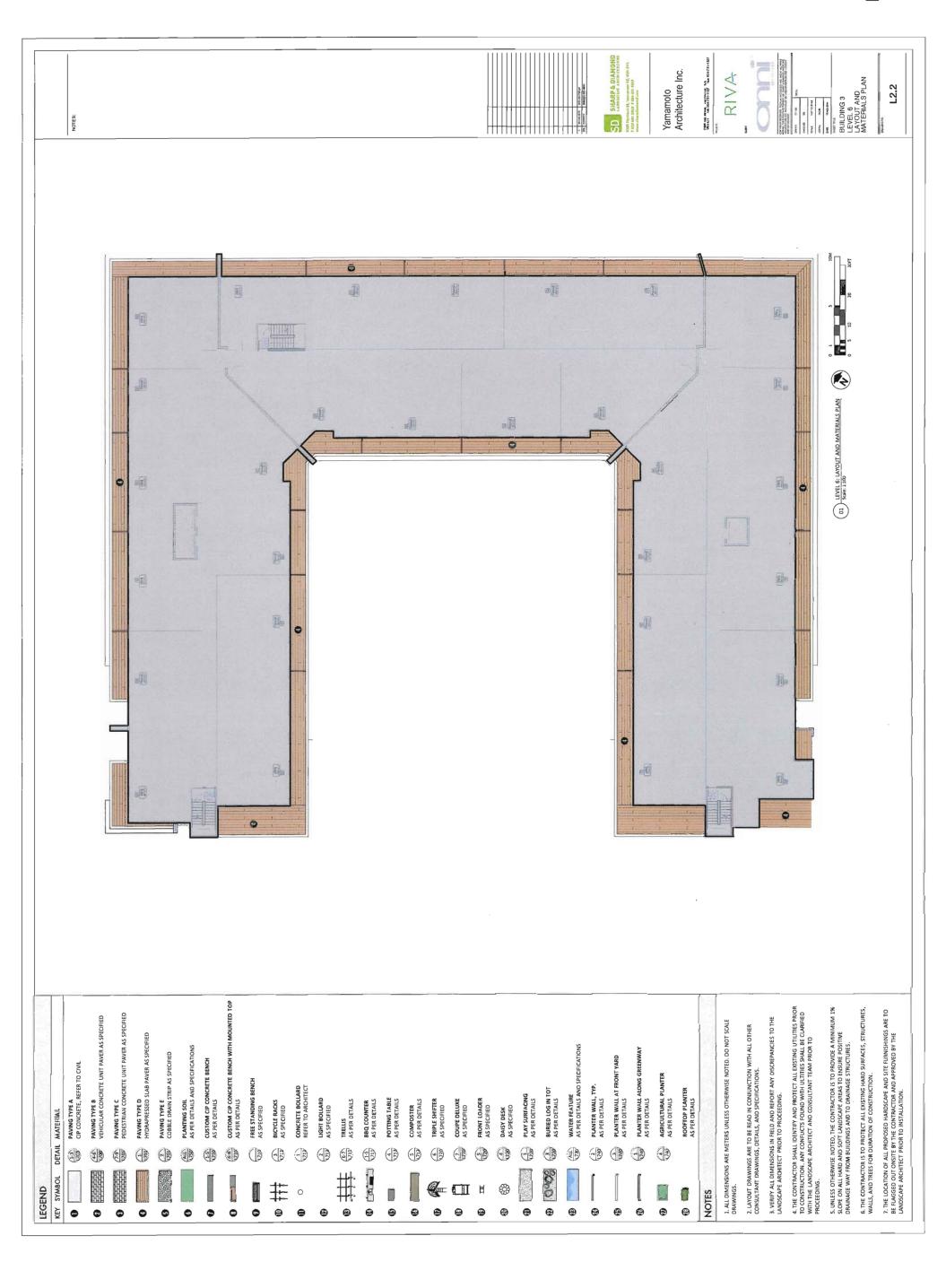
58- 790110-77 do



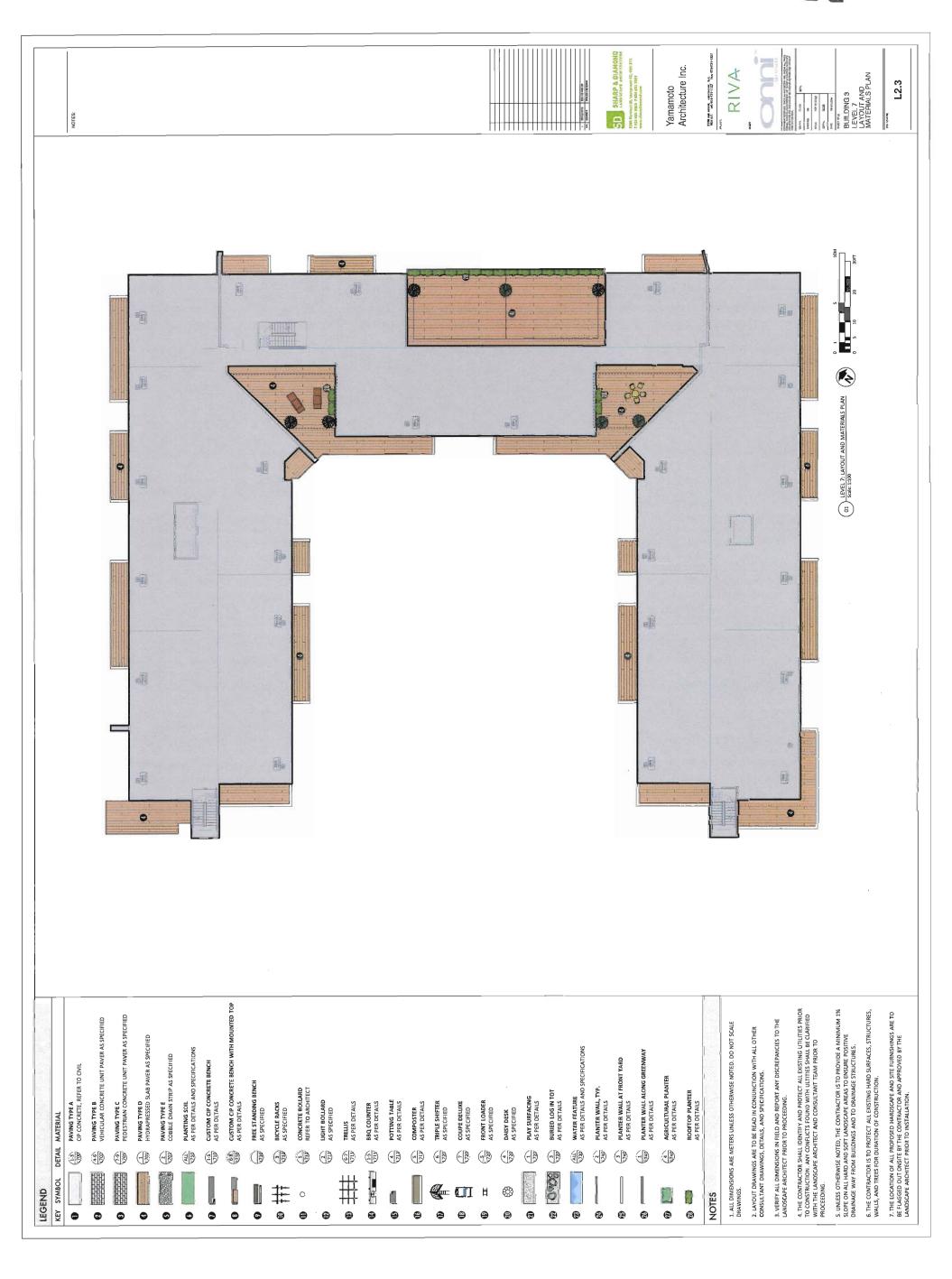
9E- 789229-71 do



+8-789119-71 do



35- 759 LL9-71 do



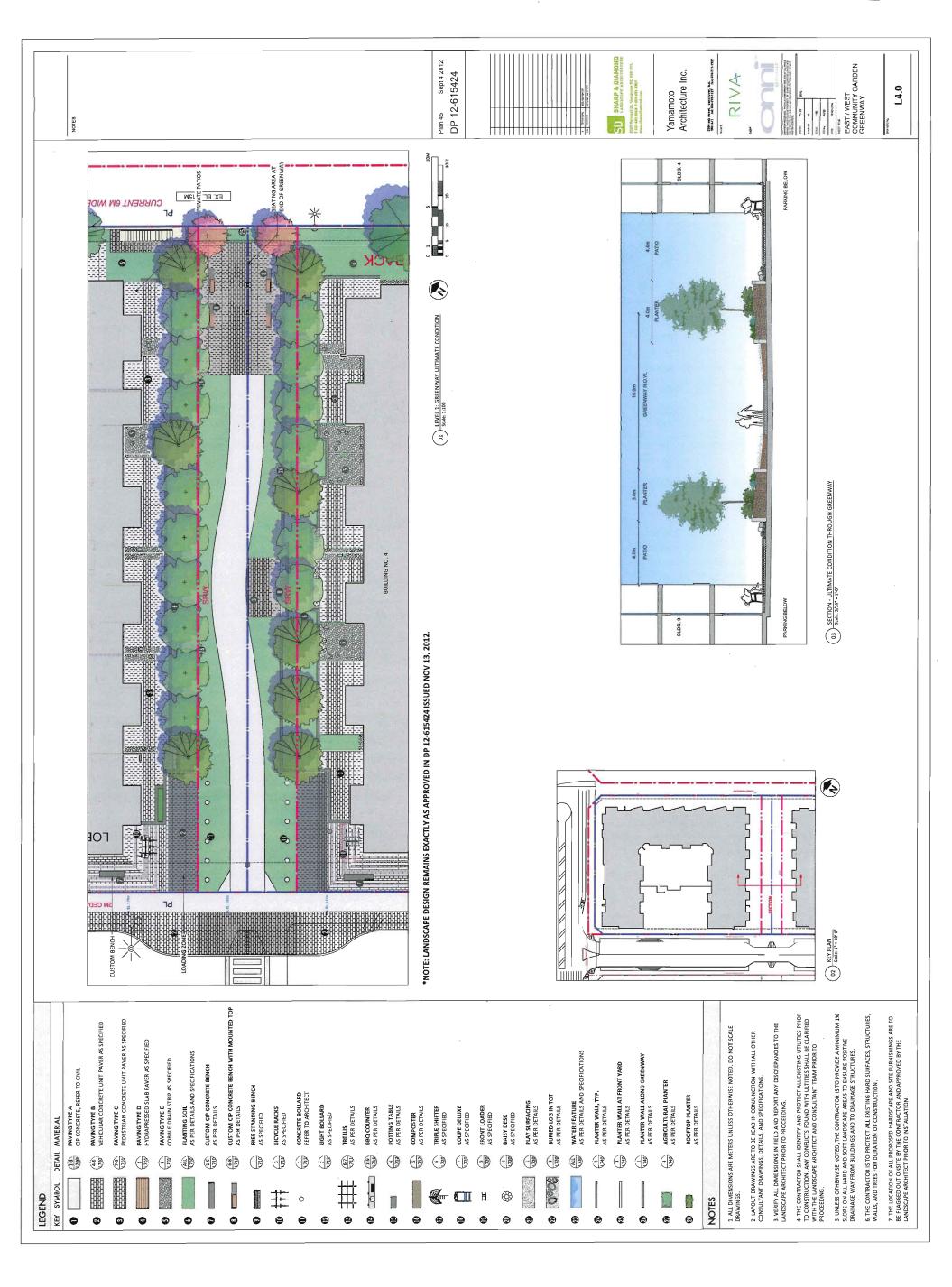
65- 486110-HI 90



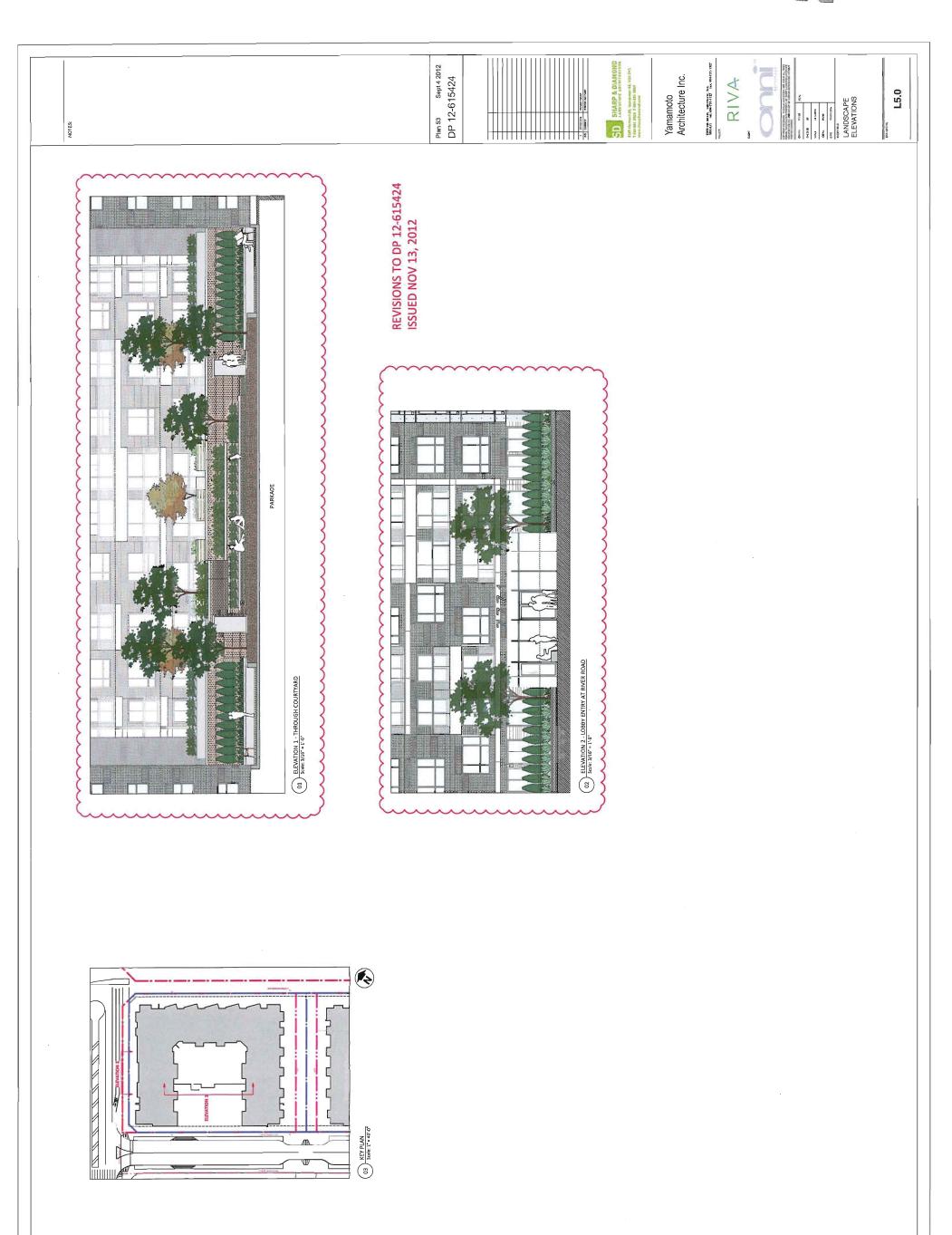
ah- 759/10-47 do



1h- 759/10-51 do



2h-789/29-51 do



Eh- 756/10-41 do



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