



Development Permit Panel

**Council Chambers, City Hall
6911 No. 3 Road**

**Wednesday, February 27, 2019
3:30 p.m.**

MINUTES

Motion to adopt the minutes of the Development Permit Panel meeting held on February 13, 2019.

1. DEVELOPMENT PERMIT 18-818762
(REDMS No. 6027231)

APPLICANT: Christopher Bozyk Architects on behalf of Open Road Toyota

PROPERTY LOCATION: 13100 Smallwood Place

Director's Recommendations

That a Development Permit be issued which would:

1. *permit the construction of two additional floors of parking/vehicle inventory storage overtop of the existing Toyota dealership at 13100 Smallwood Place on a site zoned "Vehicle Sales (CV)"; and*
2. *vary the provisions of Richmond Zoning Bylaw 8500 to increase the maximum building height to accommodate:*
 - (a) *a parkade rooftop height of 15.1 m;*
 - (b) *a parapet height of 16.2 m;*
 - (c) *a stair tower height of 18.0 m; and*
 - (d) *an elevator over-run height of 19.9 m.*

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ITEM

2. **DEVELOPMENT PERMIT 18-825006**

(REDMS No. 6119296 v. 2)

APPLICANT: IBI Group Architects (Canada) Inc.

PROPERTY LOCATION: 9455 and 9533 Bridgeport Road

Director's Recommendations

That a Development Permit be issued which would permit the construction of two hotels at 9455 and 9533 Bridgeport Road on sites zoned “Light Industrial, Office and Hotel (ZI10) – Bridgeport Village (City Centre)”. □

3. **New Business**

4. **Date of Next Meeting:** March 13, 2019

ADJOURNMENT



City of Richmond

Minutes

Development Permit Panel Wednesday, February 13, 2019

Time: 3:30 p.m.

Place: Council Chambers
Richmond City Hall

Present: John Irving, Chair
Laurie Bachynski, Director, Corporate Business Service Solutions
Peter Russell, Senior Manager, Sustainability and District Energy

The meeting was called to order at 3:30 p.m.

Minutes

It was moved and seconded

That the minutes of the meeting of the Development Permit Panel held on January 30, 2019 be adopted.

CARRIED

1. DEVELOPMENT PERMIT 18-827622 (REDMS No. 6046065)

APPLICANT: Matthew Cheng Architect Inc.

PROPERTY LOCATION: 8280/8282 and 8300/8320 No. 3 Road

INTENT OF PERMIT:

Permit the construction of 10 townhouse units and one secondary suite at 8280/8282 and 8300/8320 No. 3 Road on a site zoned “Low Density Townhouses (RTL4)”.

Applicant’s Comments

Matthew Cheng, Matthew Cheng Architect Inc., provided background information on the proposed development and reviewed the site context and lay-out, building design, materials and colour scheme, interface with adjacent properties, building setbacks, grading plan, surveillance measures, and sustainability features.

Mr. Cheng highlighted the following significant features of the project:

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- a ground level secondary suite is proposed in one of the townhouse units and provided with a surface parking stall;
- a statutory right-of-way over the entry driveway and north-south drive aisle is provided to allow access to/from future developments to the north and south though the subject site;
- an on-site turnaround will be provided on the east side of the driveway and will be separated from the adjacent outdoor amenity space by bollards;
- the proposed contemporary design of the buildings is appropriate for the area;
- building height is stepped down from three-storey units along No. 3 Road to two-storey duplex buildings at the rear to provide an appropriate transition to the surrounding single-family homes;
- the existing grade along the east property line will be maintained; however, the existing grade of the outdoor amenity space at the southeast corner will be raised to provide a more functional children's play area;
- low retaining walls are proposed along the north and south property lines and on a portion of the east property line, along the east side of the outdoor amenity space;
- air source heat pumps are proposed for heating and cooling of townhouse units;
- all proposed units incorporate aging-in-place features; and
- one convertible unit is provided in the proposed development.

Denitsa Dimitrova, PMG Landscape Architects, briefed the Panel on the main landscaping features of the project, noting that (i) existing grades are retained in tree protection zones (TPZ) and no construction work is allowed within the TPZ, (ii) each unit is provided with a private yard with a patio, a lawn, and a shade tree, (iii) low metal fencing and a metal gate is proposed for each street-fronting unit, (iv) a six-foot wood fence with lattice and landscaping with trees are proposed along the three interior property lines to provide separation and screening from adjacent residential developments, (v) a play equipment is proposed in the children's play area to provide play opportunities for children in different age groups, (vi) pavers are proposed at the vehicle entrance, on-site turnaround area, surface parking spaces and designated walkways on the site, and (vii) a trellis is proposed at the north end of the internal drive aisle to provide visual interest.

Staff Comments

Wayne Craig, Director, Development noted that (i) there is a Servicing Agreement for frontage works associated with the project, (ii) the Servicing Agreement will be entered into as a condition for Building Permit issuance, and (iii) a statutory right-of-way will be registered over the internal drive aisle to facilitate future connection north and south of the subject site.

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Panel Discussion

In reply to queries from the Panel, Ms. Dimitrova acknowledged that (i) a six-foot high wood fence and landscaping will provide separation between the subject site and adjacent properties to the north, south and east, (ii) wood chips or an equivalent Fibar surface will be used for the children's play area surface, (iii) the outdoor amenity area will be contained within a concrete border which is wheelchair accessible, (iv) bollards will be installed adjacent to the truck turning area to provide safety to users of the children's play area, and (v) lighting will be provided in the children's play area.

In reply to queries from the Panel regarding details on the proposed air source heat pump, Mr. Cheng noted that (i) a soundproofed air source heat pump will be provided for each unit and located inside the units, (ii) a split system air source heat pump is proposed, and (iii) the applicant will ensure that the installed air source heat pumps meet the City's Noise Bylaw requirement.

The Chair advised that staff work with the applicant to review the mechanical design of the air source heat pumps to ensure that they do not present a noise issue for residents.

In reply to a query from the Panel, Mr. Cheng acknowledged that electric vehicle charging will be provided in all townhouse garages.

In reply to further queries from the Panel, Mr. Cheng reviewed the project's interface with adjacent properties to the north, south and east, noting that the adjacent properties to the north and south are designated for townhouse development and it is anticipated that their site grades would be raised to match the grade of the subject site.

In reply to a query from the Panel, Mr. Cheng confirmed that perimeter drainage will be provided on the subject site to ensure that storm water will not impact neighbouring properties.

In reply to a query from the Panel regarding details of on-site tree removal and replanting, Mr. Craig advised that (i) 23 trees on-site have been identified for removal, (ii) the City's Official Community Plan requires 46 replacement trees, (iii) 24 new trees will be planted on the site, (iv) in lieu of the additional 22 trees that cannot be accommodated on the site, the applicant will provide a cash contribution of \$11,000 to the City's Tree Compensation Fund for tree planting elsewhere in the City, (v) there was detailed analysis of the condition of on-site trees and which trees could be retained and removed through the rezoning process, (vi) the sanitary sewer right-of-way that runs along the east property line limits the planting of new trees along the sanitary sewer line, and (vii) off-site compensation for removal of on-site trees is being sought by the applicant.

At this point, the Chair advised the project's design team regarding the Panel's expectation from applicants presenting their project to provide straightforward answers to questions from the Panel.

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In reply to a query from the Panel regarding the reason for the removal of the Cypress tree at the northeast corner and the two Mountain Ash trees on the west side of the site which do not appear to conflict with the design of the project, Mr. Craig advised that (i) an arborist assessed the condition of existing trees on the site at rezoning, (ii) the arborist determined that the 23 existing trees to be removed are in poor condition, (iii) the two Mountain Ash trees located on the City's property were assessed by the City's Parks Department and were determined to be in poor condition, and (iv) the Parks Department is seeking a \$2,600 compensation for the removal of the two trees and for replanting.

In reply to a query from the Panel, Ms. Dimitrova confirmed that on-site irrigation will be provided for new trees to be planted on the site.

In reply to a query from the Panel, Mr. Craig confirmed that the amount of compensation for the removal of two Mountain Ash trees is in addition to the applicant's cash contribution to the City's Tree Compensation Fund.

Correspondence

None.

Panel Discussion

The Panel expressed support for the project, noting that (i) the design, materials and colour scheme for the project are appropriate, (ii) the proposed landscaping along the three sides of the subject site provide separation from adjacent properties, (iii) the stepping down of building heights, landscaping and fencing provide an appropriate interface with adjacent properties, and (iv) the applicant is commended for providing air source heat pumps in the project.

The Panel noted that (i) the applicant needs to address and manage storm water on the site when the retaining walls are built in order not to impact the neighbouring properties, and (ii) more information provided by the applicant would have been helpful to the Panel.

Panel Decision

It was moved and seconded

That a Development Permit be issued which would permit the construction of 10 townhouse units and one secondary suite at 8280/8282 and 8300/8320 No. 3 Road on a site zoned "Low Density Townhouses (RTL4)".

CARRIED

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2. DEVELOPMENT PERMIT 18-831623

(REDMS No. 5932925 v. 3)

APPLICANT: W.T. Leung Architects, Inc. on behalf of Grand Long Holdings Canada Ltd.

PROPERTY LOCATION: 8071 and 8091 Park Road

INTENT OF PERMIT:

Permit the construction of two residential towers and one office-residential tower on a podium with street-oriented commercial and retail uses at ground level at 8071 and 8091 Park Road on a site zoned “High Density Mixed Use (ZMU39) – Brighouse Village (City Centre).

Applicant's Comments

Wing Ting Leung, W.T. Leung Architects, Inc., provided background information on the proposed development and highlighted the following:

- the proposed development is comprised of three towers, i.e., Towers A, B, and C, and stepped mid-rise buildings over a podium with ground level commercial and retail uses and office and residential uses above;
- 353 residential housing units are proposed, including 21 affordable housing units with Basic Universal Housing (BUH) features which are provided in Towers B and C;
- four levels of indoor vehicle parking are provided, including one level of underground parking, one level of at-grade parking, and two levels of above grade parking;
- vehicle parking is accessed off the existing north-south lane and east-west lane, which will be widened;
- loading and garbage and recycling facilities are accessed from the north-south lane and the east-west lane;
- the proposed siting and form of towers, stepped midrise buildings and angled midrise building corners enhance the views into and through the site and improve privacy and separation between buildings;
- terraced forms and proposed architectural treatments provide horizontal expression to the buildings;
- a mid-block public plaza is proposed on Park Road to break up the long frontage along Park Road, provide a central focal point to the project, assist in wayfinding, and provide a potential location for public art;
- a mid-block publicly accessible north-south pedestrian connection through the building that connects Park Road, the public plaza, and the east-west lane is proposed, and will be closed and secured after stores close at night;

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- two light wells are incorporated into the design of the parking podium to provide natural lighting to the north-south pedestrian connection and enhance wayfinding and the indoor parking experience of shoppers; and
- the two on-site car share vehicles and associated parking stalls to be provided are accessed from the north-south lane and are accessible 24 hours.

Stephen Vincent, Durante Kreuk Ltd., briefed the Panel on the main landscaping features of the project, noting that (i) there are currently no existing trees on the site and 96 new trees are proposed to be planted on the ground and podium levels, (ii) building entrances are highlighted with significant landscaping, including provision of seating areas, (iii) wide boulevards are broken up with paving patterns, (iv) high quality paving is proposed on entries to building lobbies, the public plaza, and the north-south public pedestrian connection, (v) location of potential public art and food and coffee shops with tables and chairs on the public plaza will animate the plaza space, (vi) soft and hard landscaping are proposed for the north-south public pedestrian connection, (vii) the common residential outdoor space located above the parking podium provides active and passive spaces and accommodates a significant amount of planting, (viii) all soft landscaping areas will be irrigated on-site, and (ix) the large terraces on the upper levels of the buildings will be planted with sedum plants, a low-growing groundcover.

In addition, Mr. Leung noted that (i) the podium level courtyard and angled building corners improve the views through the site, and (ii) the applicant will design, construct, and transfer the ownership to the City an on-site non-carbon energy plant.

In reply to a query from the Panel, Mr. Leung confirmed that the applicant worked with City staff and a private company regarding the design of the on-site energy plant.

Staff Comments

Mr. Craig noted that (i) there is a Servicing Agreement associated with the project for frontage improvements on Park Road, Buswell Street and the north-south and east-west lanes adjacent to the site, (ii) 57 percent of the total number of residential units in the project are family-friendly, (iii) the two car share stalls provided on-site are part of the Transportation Demand Management (TDM) measures of the project and two car share vehicles will be provided, (iv) the project has been designed to achieve the City's aircraft noise standards, and (v) the applicant has submitted an acoustical evaluation report and mechanical thermal report and recommendations will be carried through to the building permit stage.

Panel Discussion

In reply to a query from the Panel regarding the design of the Park Road public plaza, Mr. Leung noted that a potential public art feature could be located on the plaza space or incorporated on the building wall depending on the public art piece to be installed.

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In reply to further queries from the Panel with regard to the design and function of the north-south pedestrian connection, Mr. Leung reviewed the proposed design for the pedestrian connection, noting that (i) the size of the two light wells on the podium courtyard are substantial and are generally open to provide natural ventilation and lighting and allow rainwater to irrigate the planters within the pedestrian connection, (ii) the light wells provide natural lighting for portions of indoor parking and enhances on-site wayfinding, and (iii) the north-south pedestrian connection could provide opportunities for potential pedestrian linkage to developments to the north of the subject site.

In reply to further queries from the Panel, Mr. Vincent acknowledged that sedum planting on the upper levels of the buildings is useful not only for storm water management but also for visual interest and provision of habitat for pollinators.

In reply to a query from the Panel, Mr. Leung advised that the concrete wall corners on the Park Road public plaza have been replaced with a glazing material in response to the comments of the City's Advisory Design Panel.

In reply to a query from the Panel regarding the small number of trees proposed to be planted along the Park Road and Buswell Street frontages, Mr. Vincent noted that (i) utilities installation including lighting along the two road frontages has impacted the number of trees to be planted, and (ii) the project's design team will investigate opportunities to install additional street trees to minimize the large gaps in tree planting along the two street frontages.

In addition, Mr. Craig clarified that all proposed off-site planting will be subject to the Servicing Agreement and the ultimate decision on tree spacing will be determined by the City's Parks Department in consultation with the Engineering Department through the Servicing Agreement process.

The Chair then advised that staff take the matter of installing additional trees along the Park Road and Buswell Street frontages under advisement.

In reply to a query from the Panel, Mr. Craig advised that (i) the proposed 21 affordable housing units complied with the City's Affordable Housing Strategy at the time that rezoning for the subject site was considered, and (ii) the affordable housing agreement has been completed and registered on Title.

In reply to a query from the Panel, Mr. Leung confirmed that occupants of affordable housing units will have access to all common residential entries and indoor and outdoor amenities in the three towers.

In reply to queries from the Panel, Mr. Leung noted that (i) a sidewalk is provided on the south side of the east-west lane for pedestrians exiting the north-south pedestrian connection, and (ii) speed bumps or markings could be installed on the east-west drive aisle on ground level indoor parking prior to approaching the raised pedestrian crossing as a traffic calming measure to enhance the safety of pedestrians using the north-south pedestrian connection.

The Chair then advised staff to take the matter of installing speed bumps or markings near the raised pedestrian crossing under advisement.

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In reply to further queries from the Panel, the design team acknowledged that (i) the common residential courtyard above the parking podium could be accessed through either of the three towers, and (ii) low-level lighting will be provided on the courtyard to avoid light pollution and disturbance to residents.

Gallery Comments

George Smith, representative of the Richmond Masonic Temple Association which owns the property at 6740 No. 3 Road located to the west of the subject site, queried how the applicant will address overflow parking on the subject site to avoid their property's parking stalls being occupied by unauthorized users.

In reply to Mr. Smith's query, Mr. Leung noted that (i) the number of proposed commercial, restaurant, and office parking spaces provided on-site are adequate and are open to residential visitors and the public, and (ii) the applicant did not take advantage of the project's proposed TDM measure for parking relaxation as the project's proposed residential and non-residential parking spaces exceed the requirements of Zoning Bylaw.

In addition, Mr. Craig advised that the 243 spaces of commercial/restaurant and office parking provided on-site comply with the City's Zoning Bylaw requirement.

In response to a further query by Mr. Smith, Mr. Leung confirmed that there will be paid parking on-site.

With regard to Mr. Smith's construction-related concerns, the Chair advised Mr. Smith to coordinate with City staff and the applicant.

Mui Fong Chiu, 6533 Buswell Street, expressed concern regarding the proximity of the subject development to the adjacent residential building to the north across the east-west lane.

In reply to Ms. Chiu's concern, Mr. Leung noted that the adjacent east-west lane to the north of the subject site will be widened from six meters to nine meters and will provide additional separation between Building B on the subject site and the existing residential building to the north.

In addition, Mr. Craig advised that the separation between Tower B on the subject site and the existing residential building to the north slightly exceeds the City Centre Area Plan Development Permit guidelines of 24 meters.

Correspondence

None.



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Panel Discussion

The Panel expressed support for the project, noting that (i) the design of the project is appropriate for the City Centre Area, (ii) the raised courtyard provides a suitable amenity for the mixed-use development, (iii) there is adequate separation between the three towers on-site, (iv) the applicant made a thorough presentation and responded well to questions from the Panel, and (v) the proposed building setbacks are appreciated.

Panel Decision

It was moved and seconded

That a Development Permit be issued which would permit the construction of two residential towers and one office-residential tower on a podium with street-oriented commercial and retail uses at ground level at 8071 and 8091 Park Road on a site zoned "High Density Mixed Use (ZMU39) – Brighouse Village (City Centre).

CARRIED

3. Date of Next Meeting: February 27, 2019

4. Adjournment

It was moved and seconded

That the meeting be adjourned at 5:05 p.m.

CARRIED

Certified a true and correct copy of the Minutes of the meeting of the Development Permit Panel of the Council of the City of Richmond held on Wednesday, February 13, 2019.

John Irving
Chair

Rustico Agawin
Committee Clerk



City of Richmond

Report to Development Permit Panel

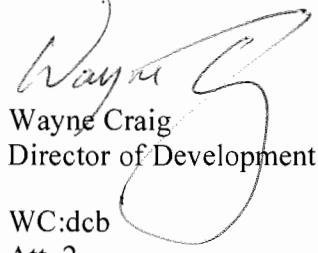
To: Development Permit Panel **Date:** February 6, 2019
From: Wayne Craig **File:** DP 18-818762
Director of Development

Re: **Application by Christopher Bozyk Architects on behalf of Open Road Toyota for a Development Permit at 13100 Smallwood Place**

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of two additional floors of parking/vehicle inventory storage overtop of the existing Toyota dealership at 13100 Smallwood Place on a site zoned “Vehicle Sales (CV)”; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to increase the maximum building height to accommodate:
 - i. A parkade rooftop height of 15.1 m;
 - ii. A parapet height of 16.2 m;
 - iii. A stair tower height of 18.0 m; and
 - iv. An elevator over-run height of 19.9 m.


Wayne Craig
Director of Development

WC:dcb
Att. 2

Staff Report

Origin

Christopher Bozyk Architects Ltd. on behalf of Open Road Toyota has applied to the City of Richmond for permission to develop an addition of two parkade levels to the existing Toyota dealership facility at 13100 Smallwood Place on a site zoned “Vehicle Sales (CV)”. The subject site is located within the Richmond Auto Mall in the East Cambie planning area.

Council previously approved the development of a two level dealership with showroom, offices, service facility and a parkade for customer / staff parking and vehicle inventory storage under ZT 16-754143 and DP 16-741123 on October 23, 2017. Construction in accordance with those applications is currently in progress.

A Zoning Text Amendment (ZT 18-818765 - Bylaw 9948) to increase the Floor Area Ratio (FAR) to 0.82 and accommodate the proposed two additional levels of parking and vehicle inventory storage overtop of the two storey dealership parkade was given third reading by Council at the Public Hearing meeting of January 21, 2019.

Development Information

The current development application will result in a four level building with rooftop parking. The current proposal will result in an additional 2,154.3 m² (23,188 ft²) of floor space to the building over the previous approved proposal (ZT 16-754143 and DP 16-741123). Several height variances, as outlined in this report, are being requested to accommodate the two additional parkade floors.

The proposal will also result in the relocation of the garbage and recycling facility to the south-east corner of the site for improved efficiency and will also allow additional vegetation screening of the car wash facility located in the south-west corner of the site.

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

- To the North, immediately across Smallwood Place, is an existing Hyundai dealership on a site zoned “Vehicle Sales (CV)” within the Richmond Auto Mall at 13171 Smallwood Place. A Zoning Text Amendment (ZT 18-810720) and a Development Permit application (DP 18-810720) have been received from Kasian Architecture Interior Design & Planning, with the objective of redeveloping that property to accommodate a new Porsche dealership building. These applications are currently under review by staff.
- To the South, across Westminster Highway and a frontage road further south, are large properties zoned “Agriculture (AG1)” in the Agricultural Land Reserve (ALR), which contain single-family dwellings and accessory buildings.

- To the East, is an existing Nissan dealership on a site zoned “Vehicle Sales (CV)” within the Richmond Auto Mall at 13220 Smallwood Place.
- To the West, across Jacombs Road, is the “Richmond Nature Park East” on a site zoned “School & Institutional Use (SI)” at 5991 Jacombs Road. The Nature Park East is designated as an Environmentally Sensitive Area (ESA).

Rezoning and Public Hearing Results

The Public Hearing for the rezoning of this site was held on January 21, 2019. At the Public Hearing, Council discussed the height variances but did not request any further changes to the plans or the report.

During the rezoning process, staff identified the following design issues to be resolved at the Development Permit stage:

- Assessing compliance with the Official Community Plan Development Permit Guidelines (*the applicant successfully completed the DP guidelines checklist which was reviewed by staff and submitted to the Advisory Design Panel*).
- A review of the proposed landscape plant/tree selections, sizes, locations and rationale. (*Vegetation in the vicinity of the building has been adjusted in response to the ornithologist recommendations to reduce the number of trees and ensure shrubs are kept low*.)
- Additional landscape securities will be calculated to address the landscaping additions (*landscape securities for the additions to the original landscape plans were provided by the project's landscape architect. The securities in the amount of \$67,320, inclusive of a 10% contingency, is to be secured prior to DP issuance. The City still holds a letter of credit in the amount of \$304,590 for landscaping security associated with the previously approved DP16-741123. Neither security will be released until all the landscaping has been satisfactorily installed*).
- A review of the proposed exterior materials and colours as they relate to the proposed parkade floor additions (*the material types and colours were reviewed and supported by the Advisory Design Panel. A key feature endorsed by the Panel is the addition of a permeable metal cladding around the upper floor of the proposed vehicle parkade*).
- A review of vehicle parking spaces to ensure compliance with the parking requirements in the Zoning Bylaw No. 8500 (*The 296 customer and staff parking spaces shown in the accompanying plans conform to the Zoning Bylaw requirements*).).
- Restriping of the six accessible parking spaces (*the current plans show the restriped layout for these parking spaces in compliance with the Zoning Bylaw*).
- A review of the height variances requested (*Pursuant to Council's November 13, 2018 referral motion, the heights of the rooftop parapet, roof deck, stair tower and the elevator overrun were all reduced from the original submission reviewed by Council*).

(The heights shown on the attached plans set is consistent with the plans shown at Public Hearing on January 21, 2019. Both staff and the Advisory Design Panel are supportive of the proposed height variances. The variances will allow better utilization of the site and reduce the need for off-site storage areas by the dealership. The overall height increases were found to be consistent with variances previously supported within the Auto Mall as these sites have been redeveloped. All the variances have also been reviewed and supported by the Richmond Automobile Association.)

- An assessment of the garbage and recycling facility to ensure it is sufficiently sized and located to address the needs of the site. A waste management overlay will be required (*The relocation of the garbage/recycling facility from the southwest corner of the site to the south east corner should allow for easier pickup. Garbage and recycling staff have reviewed the size of the facility and are satisfied it will meet the needs of the building.*)
- Confirming building elevations and variances; (*The elevations shown in the submitted DP plan set are consistent with the elevations and variances indicated through the Zoning Text Amendment plan set reviewed by Council at Public Hearing on January 21, 2019.*)
- Reviewing landscape modifications – particularly within 10 m of the building; (*The applicant has advised that the landscape plans have been revised to ensure that there are no plant over 30 cm in height within 10 m (32.8 ft.) of the building as recommended by the Ornithologist report. In addition, the number of trees has been reduced from 85 to 71 trees in the landscape plans as part of the bird mitigation strategy.*)
- Review the site lighting scheme including any use of “up-lighting”; (*The applicant advises that no up-lighting will be used in the site. Roof top lighting on the parkade will be confined to recessed downlights below the level of the parapet. Parking within the parkade itself comes with a lens which will focus the light down and prevent light spillage.*)
- Inclusion of any implementable avian mitigation measures. (*The owner confirms that they will monitor bird activity adjacent to the building for one year after completion as outlined in the Ornithologist report.*)

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan and is generally in compliance with the “Vehicle Sales (CV)” zone except for the zoning variances noted below.

Zoning Compliance/Variances (staff comments in **bold)**

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

Increase the maximum building height to accommodate:

- i. A parkade rooftop height of 15.1 m;
- ii. A parapet height of 16.2 m;
- iii. A stair tower height of 18.0 m; and
- iv. An elevator over-run height of 19.9 m.

(Staff supports the proposed height variance as this:

- *allows for improved utilization of the land base at the site;*
- *reduces the amount of land needed elsewhere for off-site vehicle storage;*
- *reduces the number of vehicle parking spaces at grade;*
- *improves site permeability and at-grade landscaping; and*
- *the applicant has made significant effort to reduce the overall height of the proposed structure in response to Council's comments.)*

Advisory Design Panel Comments

The Advisory Design Panel was supportive subject to the applicant giving consideration to the comments of the Advisory Design Panel. A copy of the relevant excerpt from the Advisory Design Panel Minutes from Wednesday, October 17, 2018, is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '***bold italics***'.

Analysis

The Development Permit application seeks to augment the previously approved building and site design. Generally, the proposed modifications include:

- The addition of the two extra parkade floors over top of the previously approved building. Staff have been advised that the initial building construction (currently underway) was designed to be able to accommodate future additional parkade floors above.
- Modifications to exterior cladding over portions of the building in part to mitigate the additional massing and enhance the appearance of the building as the two parkade levels are added.
- Relocation of twenty (20) at-grade parking spaces to the inside of the parkade.
- Restriping of the 6 accessible parking spaces to conform to recent Zoning Bylaw changes.
- Addition of trees and other landscaping to fill the spaces left by relocating the twenty (20) parking spaces.
- Relocation of the garbage and recycling facility from the southwest corner of the site to the south-east corner of the site.

Conditions of Adjacency

- In response to concerns raised at Planning Committee regarding potential sun shading of the Richmond Nature Park located approximately 42.5 m (139.4 ft) to the east, the applicant undertook a detailed shadow impact analysis (refer to the attached sun shadow analysis diagrams in the plan submission). The analysis indicates that shading of the periphery of the park will occur at several times through the year, limited to early morning hours. The diagrams indicate the duration of the shadowing to be typically short (on the order of an hour after sunrise at the extremes).
- Both the Richmond Nature Park on the west side Jacombs Road and properties on the south side of Westminster Highway are within the Agricultural Land Reserve (ALR). Consistent with the Official Community Plan (OCP) three metre wide ALR buffers have been registered on the title of the subject site for both the western and southern property boundaries through the previously approved ZT 16-754143. The ALR buffers consist of a 3 m wide setback to on-site surface parking, a 15 m setback to the south building façade plus a 1.8 m high solid fence. The buffers are also landscaped with trees and shrubs.

Urban Design and Site Planning

- Relocation of the garbage and recycling facility to the south-east corner of the site will facilitate easier pickup and allow for improved vegetation screening of the car wash facility that will remain just north of the south-west corner of the site.

Architectural Form and Character

- The addition of the perforated cadmium white cladding panels off-set from the building helps to enhance the appearance of the additional two levels of parking/storage in a manner that compliments the colour of the rest of the building. The panel's perforations and building off-set placement will allow natural air flow ventilation eliminating the need to mechanical ventilation.
- Interior lighting will have shades to direct the light downward. Sensors will also be used to limit them to times when the space is in use.
- Roof level parkade lighting will be installed below the parapet height to limit any light spill.
- A color and materials sample panel is included in the plan submission.
- An ornithologist report submitted through the Zoning Text Amendment application (ZT 18-818765) identifying bird strike potential with the building's glazing on the western side of the building. The owners have agreed with the report's recommendation to monitor bird strike events and have contracted with a Qualified Environmental Professional to do this work for a year, post construction, and will implement any necessary mitigation measures should they be required (details provided in the report to Planning Committee from the Director of Development dated Dec. 11, 2018)
- 107 roof top solar panels have been incorporated into the building design in response to a referral from Council. The panels are to be located on the building's lower rooftop level. The panel placement took the sun shading analysis (included in the plan submission) into account. Energy from the panels will be used on site to offset power requirements for the parking lot lighting. The installation and maintenance for the life of the building was secured through a legal agreement registered on title under the considerations associated with ZT 18-818765.

- An extensive review of the parapet heights was undertaken through the rezoning. The final parapet height satisfies the BC Building Code requirement for a guard of no less than 1.070 m (3.5 ft.) in height.

Landscape Design and Open Space Design

- No existing trees will be removed as a result of the current proposal, however, an additional 31 on-site trees are included in the proposed landscaping plans over the originally approved landscaping plan (DP 16-741123). These trees will help provide additional edge screening for the site.
- A large part of the tree and shrub increases are a result of the relocation of twenty parking spaces from grade into the expanded parking structure. Twelve of these spaces are located along the southern property boundary. Their relocation allows for increased tree and shrub planting along the southern boundary which will help increase the ALR buffer there and improve the screening of the higher structure from the adjacent highway (Westminster Hwy.). The other eight spaces to be relocated were along the western property boundary. These spaces will also be replaced with new vegetation strips including trees, shrubs and ground cover.
- Landscaping will cover approximately 13% ($2,105 \text{ m}^2$) of the site. The relocation of the twenty vehicles into the parkade has resulted in an additional 750 m^2 ($8,073 \text{ ft}^2$) of landscaping.
- Per the Advisory Design Panel's recommendations, 17 native conifer trees (Douglas Fir) have been added in five clusters along the southern property boundary fence line to replace the pine trees previously located there. The Fir trees are expected to grow to have a larger presence along the southern boundary of the site.
- Perimeter fencing around the southern and western property boundaries remains unchanged from the previously approved Development Permit application (DP 16-741123).
- The existing hedging along a portion of the eastern property boundary will be retained through this application.
- Six accessible parking spaces are to be restriped to conform to the recently revised sections of the Zoning Bylaw No. 8500. These are shown on the plans.
- Based on the landscaping cost estimate provided by the Landscape Architect, submission of a landscape security of \$67,320 is required prior to Council approval. This is in addition to the \$304,590 previously collected landscaping security under DP16-741123

Transportation – Vehicle and Bicycle Spaces

- With the additional two parkade floors the development will provide 296 vehicle spaces for staff and visitors.
- In addition to the staff/visitor parking the facility will provide another 279 parking spaces for vehicle inventory.
- Thirty-six Class 1 and thirty-six Class 2 bicycle spaces are provided fully addressing the Zoning Bylaw requirements for the facility.
- Six accessible vehicle parking spaces are provided close to the facility's main entrance.
- One medium loading space is provided to accommodate the facility's operational needs (approved variance from 4 loading spaces under DP 16-741123).

Conclusions

The proponent has made significant efforts to reduce the proposed buildings overall height and to address concerns raised by Council, the Advisory Design Panel and staff as they progressed through their Zoning Text Amendment application and the Development Permit application reviews. The proposed development would meet applicable policies and Development Permit Guidelines and staff recommend that the Development Permit for the construction of two additional floors of parking/vehicle inventory storage overtop of the existing Toyota dealership at 13100 Smallwood Place and the requested variances to increase the maximum building height to accommodate a parkade rooftop height of 15.1 m, a parapet height of 16.2 , a stair tower of 18.0 m and an elevator over-run of 19.9 m be endorsed, and issuance by Council be recommended.



David Brownlee
Planner 2

DCB:as

List of Attachments

Attachment 1: Development Application Data Sheet

Attachment 2: Advisory Design Panel Minutes and Applicant Responses

The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter-of-Credit for landscaping in the amount of \$67,320.00.

NOTE: staff to ensure that landscape estimates include a 10% contingency cost.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Department at 604-276-4285.*
- Incorporation of accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Department (<http://www.richmond.ca/services/ttp/special.htm>).
- If applicable, payment of latecomer agreement charges, plus applicable interest associated with eligible latecomer works.



City of Richmond

Development Application Data Sheet

Development Applications Department

DP 18-818762

Attachment 1

Address: 13100 Smallwood Place

Applicant: Christopher Bozyk Architects Owner: Open Road Toyota

Planning Area(s): East Cambie

Floor Area Net: 12,996.3 m² (139,891 ft²)

| | Existing | Proposed |
|------------------|---|---|
| Site Area: | 15,924 m ² (171,404.51 ft ²) | Same |
| Land Uses: | Auto Dealership and Service | Same |
| OCP Designation: | Commercial | Same |
| Zoning: | Vehicle Sales (CV) | Vehicle Sales (CV) with increased FAR to 0.82 per Bylaw 9948. |

| | Bylaw Requirement | Proposed | Variance |
|--|---|---|----------------|
| Floor Area Ratio: | 0.7 FAR | 0.82 FAR | none permitted |
| Lot Coverage: | Building: Max. 50% | Building: Max. 38% | None |
| Setback Front - North | Min 3.0 m | 16.0 m | None |
| Setback Side – East | Min 3.0m | 20.38m | None |
| Setback Side - West | Min 3.0m | 22.5m Main Building 3.0m Car Wash Building | None |
| Setback Rear - South | Min 3.0m | 10.63m Main Building 3.0m Garbage Enclosure | None |
| Height (m): | Max building height: 12.0 m Through DP 16-741123 building height variances to 15.5 m were granted. | Parkade rooftop 15.1 m, Parapet 16.2 m, Stair tower 18.0 m and Elevator over-run 19.9 m | Variance |
| Off-street Parking Spaces – Staff and Visitor | 281 | 296 | none |
| Off-street Parking Spaces – Vehicle Inventory: | N/A | 279 | none |

**Excerpt from the Minutes from
The Design Panel Meeting**

**Wednesday, October 17, 2018 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall**

**1. DP 18-818762 – TWO-STOREY ADDITION FOR INVENTORY PARKING TO
TOYOTA DEALERSHIP**

ARCHITECT: Christopher Bozyk Architects
LANDSCAPE ARCHITECT: Connect Landscape Architecture
PROPERTY LOCATION: 13100 Smallwood Place

Applicant's Presentation

Keiran Walsh, Christopher Bozyk Architects, and Ken Larsson, Connect Landscape Architecture, presented the project and together with Moe Saboune, Project Director, OpenRoad Auto Group, answered queries from the Panel on behalf of the applicant.

Panel Discussion

Comments from Panel members were as follows:

- appreciate the applicant considering public art in the project; ***Noted***
- consider breaking down the massing on the west face of the building as it has high visibility and exposure, e.g. through introducing a few coloured strips; ***The colour palette was agreed to be the best solution by the majority of the panel and we would prefer to keep the panels in white only***
- support the applicant's choice of colour for the cladding material for the proposed parkade addition; cadmium white would be an appropriate colour as it matches the existing colours on the building; ***Noted***
- support the cladding option for the proposed additional parkade which eliminates mechanical ventilation; however, the applicant is advised to be cognizant of light pollution and light control issues; ***The interior lighting of the building comes with a shade that will focus the lighting down and not out. The lighting will also be on a sensor and will only be on when the space is being used. The lighting at the roof level parkade is installed below the level of the parapet only.***

- support the white perforated metal panel option for the cladding of the parkade addition; the choice of perforated panel with skewed joints appears consistent with some of the geometry of the angled buttress frames on Jacombs Road elevation; ***Noted.***
- appreciate the applicant presenting the different options for cladding of the parkade addition; ***Noted & thank you!***
- support the perforated metal cladding option as it has a simple design and consistent with the applicant's design intent of the parkade addition not being imposing to the street but at the same time having a pleasant character; ***Noted.***
- in order to ensure the success of cladding option 1 (i.e., perforated metal cladding), the applicant needs to (i) ensure that the perforation does not touch the perimeter of the panel, (ii) determine the extent of visibility of the view behind the panel so that the panel will not lose its visual strength (white is a preferred colour to achieve this objective), and (iii) consider a low sheen white colour with a little bit of texture so it will not create a reflective surface; ***We are in discussion with the fabricator about details which avoid having any cut perforations at the panel perimeter. It is intended to have a dark colour behind the panels to allow the panels keep their visual strength. Samples have been requested of a lower sheen surface finish.***
- appreciate the choice of tree species to be installed on the permeable areas of the subject site; however, consider installing larger native conifer tree species at the corners; ***Noted. Douglas Fir trees (mix of sizes 2.5-3.5m ht) will be added to replace the pine trees along the fence line.***
- appreciate the applicant's efforts to explore the different options for façade treatment of the proposed parkade addition and address related issues; ***Noted & thanks.***
- support the perforated metal cladding option; ***Agreed***
- appreciate the additional height of the building as it strengthens the lower portion of the building, i.e., the angled wing that has the main showroom component; additional height helps create a more interesting massing for the building; ***Agreed***
- like the “restrained” crown of the building; choice of a powder white-coated metal cladding is on the right direction; and ***Agreed.***
- applicant could add a little bit of articulation to the metal cladding treatment, e.g. having a shadow gap in the structural supports around the perimeter to subtly break up the metal panels. ***There is a gap of 6" between the panels to create shadow and texture.***

Panel Decision

It was moved and seconded

That DP 18-818762 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Advisory Design Panel.

CARRIED



City of Richmond

Development Permit

No. DP 18-818762

To the Holder: Christopher Bozyk Architects

Property Address: 13100 Smallwood Place

Address: Suite 414 – 611 Alexander Street
Vancouver, BC, Canada, V6A 1E1

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500 is hereby varied to:
 - a) Increase the maximum building height to accommodate:
 - i. A parkade rooftop height of 15.1 m;
 - ii. A parapet height of 16.2 m;
 - iii. A stair tower height of 18.0 m; and
 - iv. An elevator over-run height of 19.9 m.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #20 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$67,320 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit
No. DP 18-818762

To the Holder: Christopher Bozyk Architects

Property Address: 13100 Smallwood Place

Address: Suite 414 – 611 Alexander Street
Vancouver, BC, Canada, V6A 1E1

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

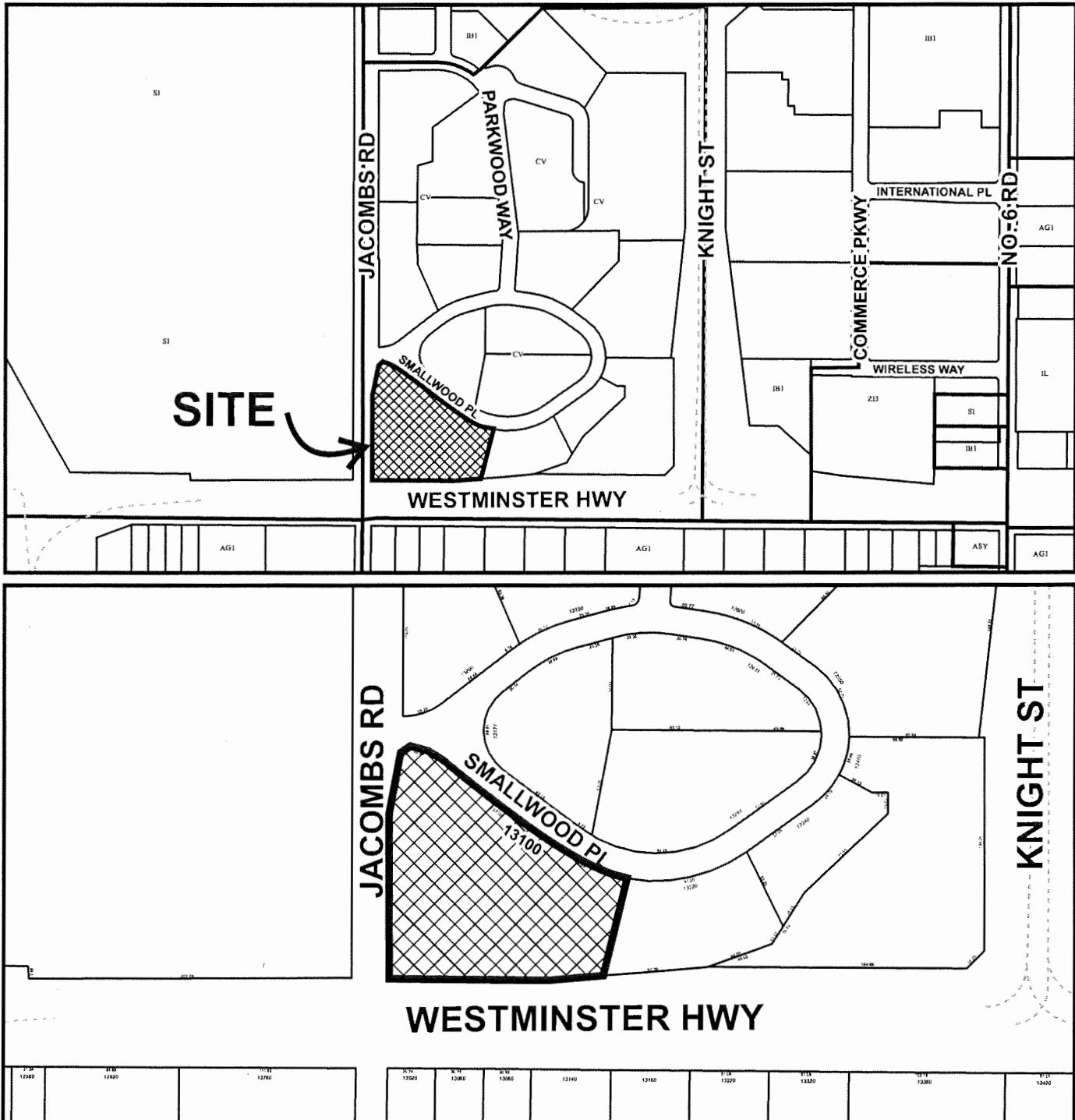
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond



**DP 18-818762
SCHEDULE "A"**

Original Date: 05/09/18

Revision Date:

Note: Dimensions are in METRES

DP 18-818762

FEB 27 2019

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| 5 | 18.01.12 | ISSUED FOR DP |
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OpenRoad Toyota Richmond

DP 18-818762

13100 SMALL WOOD PLACE, RICHMOND, BC

LEVEL 1

SCALE: 1" = 30'-0"
PROJECT NUMBER: 214007
FILE NUMBER: D:\ARCH\OPENROAD\TOYOTA\RICHMOND\18-818762\18-818762.DWG
DRAWN BY: Author

PLAN #1A

DP.005

Plan #2

① GROUND FLOOR PLAN
1" = 30'-0"

WESTMINSTER HIGHWAY



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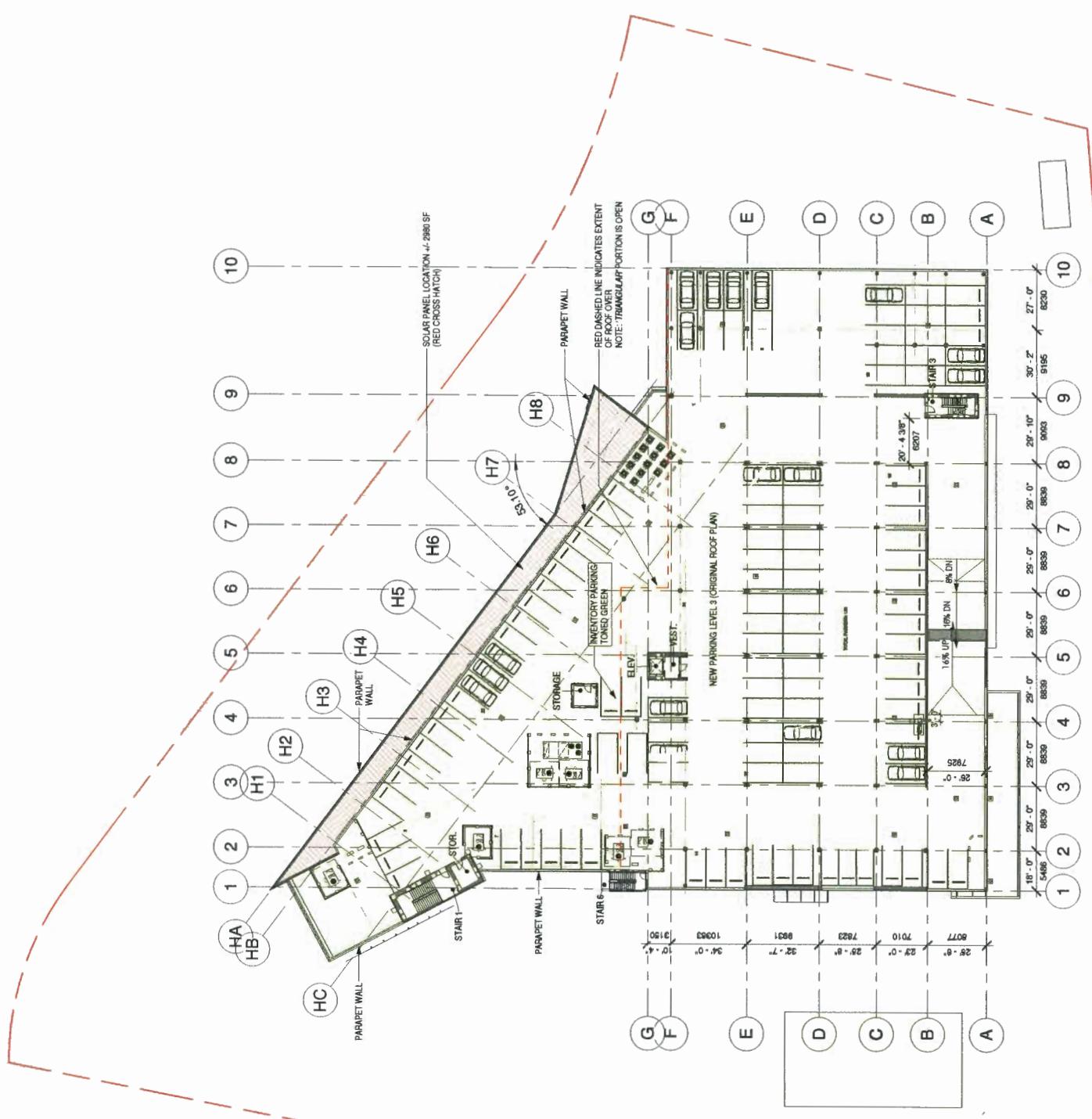
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| LEVEL 3 | DP 18-818762 | 13100 SMALL WOOD PLACE, RICHMOND, BC | DRAWING NUMBER |
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| | | | 274007 |

PLAN #1C
DP.007

Plan #4

1 OVERALL ROOF PLAN
1' = 30'-0"



148-818762

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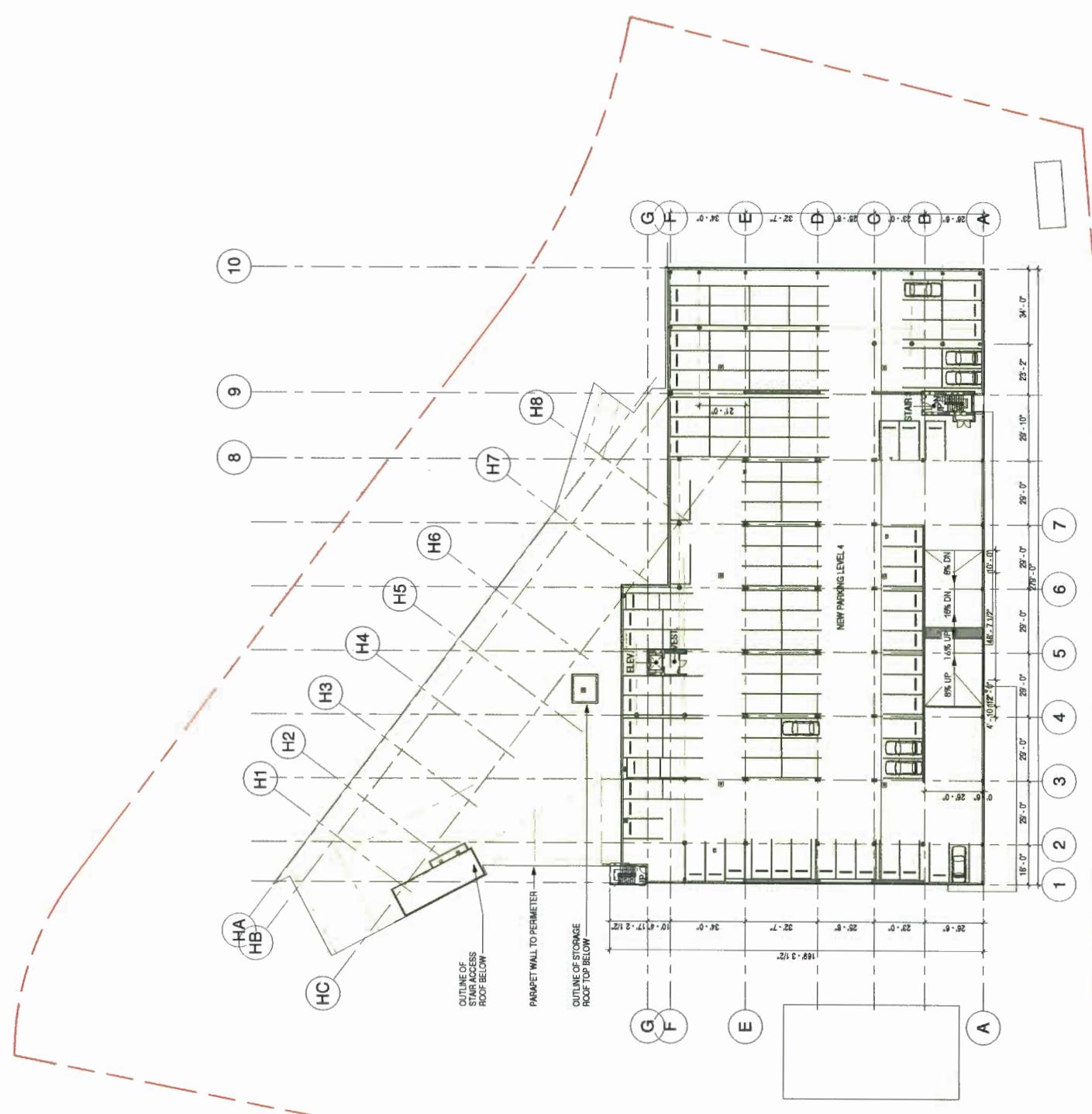
DP 18-818762

13100 SMALL WOOD PLACE, RICHMOND, BC

SCALE: 1:37-0 DATE: 2019-01-22 DRAWN/AUTHOR
PROJECT NUMBER 214007

**PLAN #1D
DP-008**

Plan #5



DP 10-818762

FEB 27 2019

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Should cancellation of errors, discrepancies, or doubtful information contained in
contract drawings and documents, failure to obtain such cancellation renders the
Contractor responsible to any resulting dispute work and the cost of re-modelling.

OpenRoad Toyota Richmond

DP 18-818762

13100 SMALLWOOD PLACE, RICHMOND, BC

LEVEL 5

SCALE: 1" = 30'-0"
DATE: 2019-01-22
PROJECT NUMBER: 21407

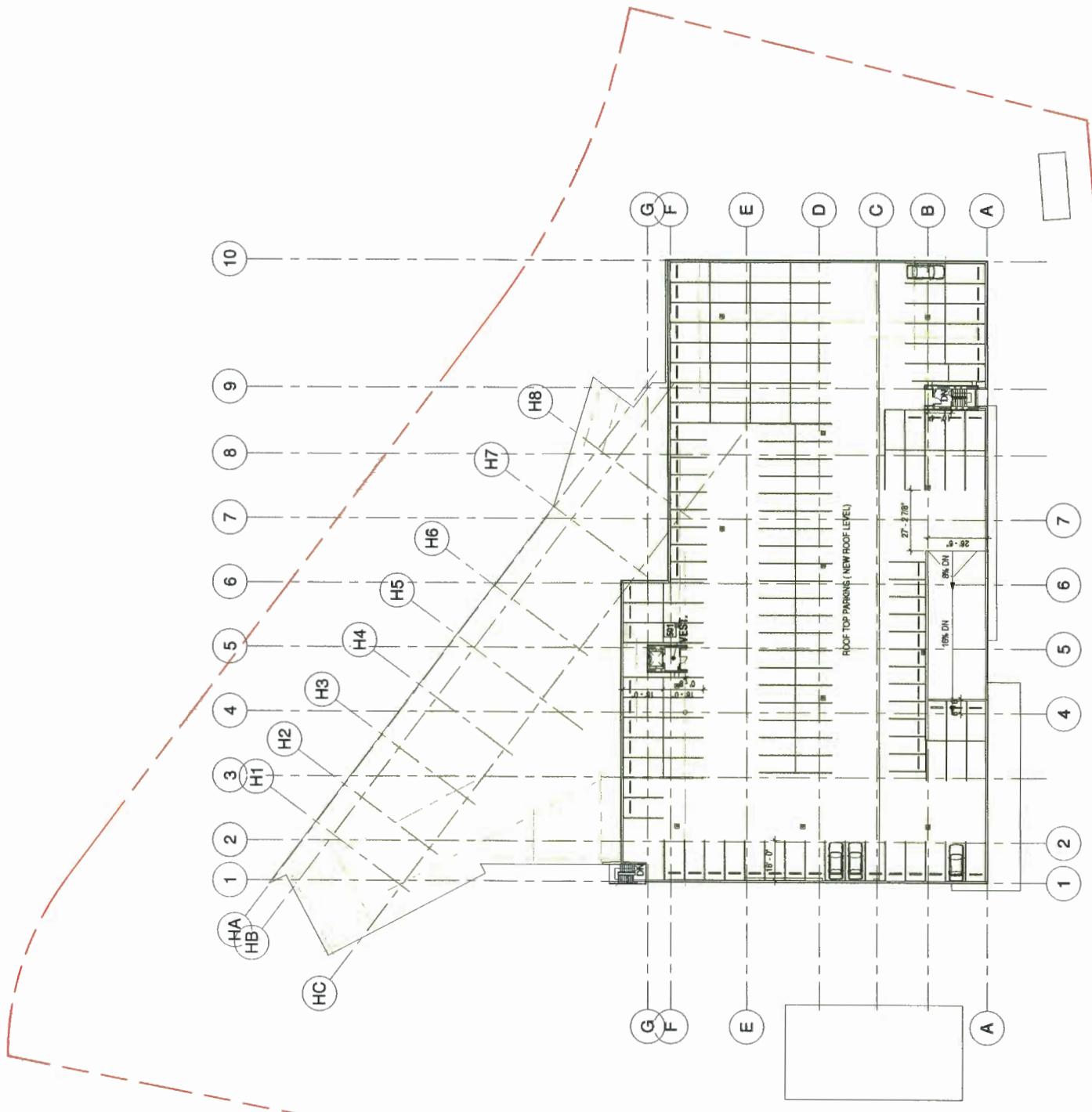
PLAN #1E

DP.009

Plan #6

1 ADDITIONAL PARKING LEVEL 2

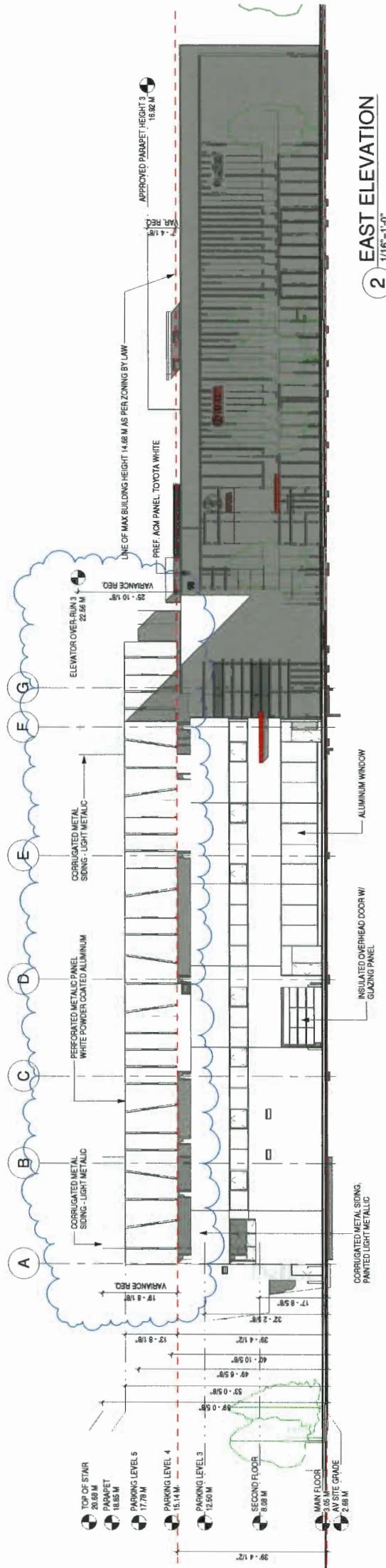
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| REF/VISION | DATE | DESCRIPTION |
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OpenBoard Toronto Richmond

DP 18-818762
13100 SMALL WOOD PLACE, RICHMOND,
ELEVATIONS AND
SECTIONS

SCALE 1/16=1'-0" DATE:2019-01-22 DRAWN/AUTHOR
PROJECT NUMBER 214007

PLAN #4
DP.010

Plan #7

SECTION A-A

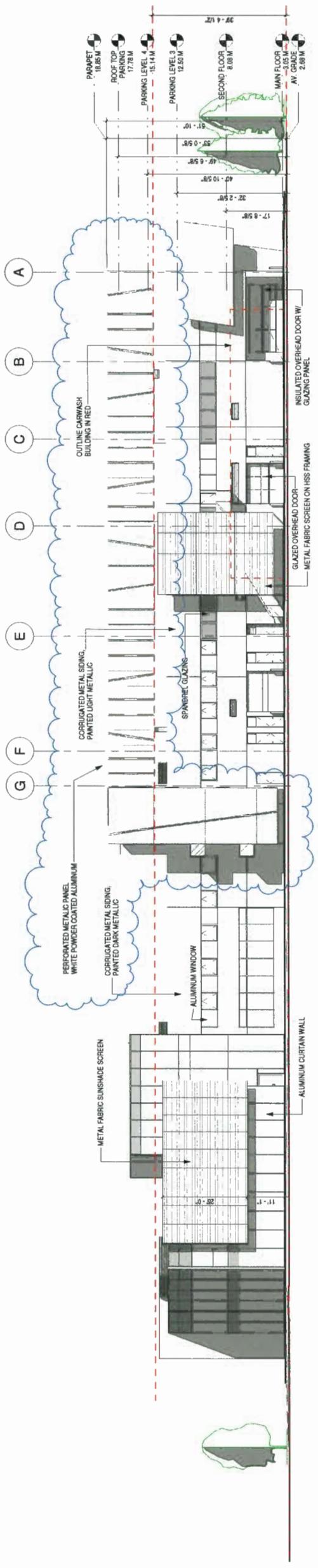
PLAN #4

DP.010

DATE: 2010-07-22
DRAWN BY: Author
PROJECT NUMBER: 1000-100-100

FEB 27 2019

CHRISTOPHER
BOZYK ARCHITECTS LTD
414-61 ALEXANDER STREET VANCOUVER BC V6A 1E1
PHONE (604) 251-3440 FAX (604) 251-3848



OpenRoad
AUTO GROUP

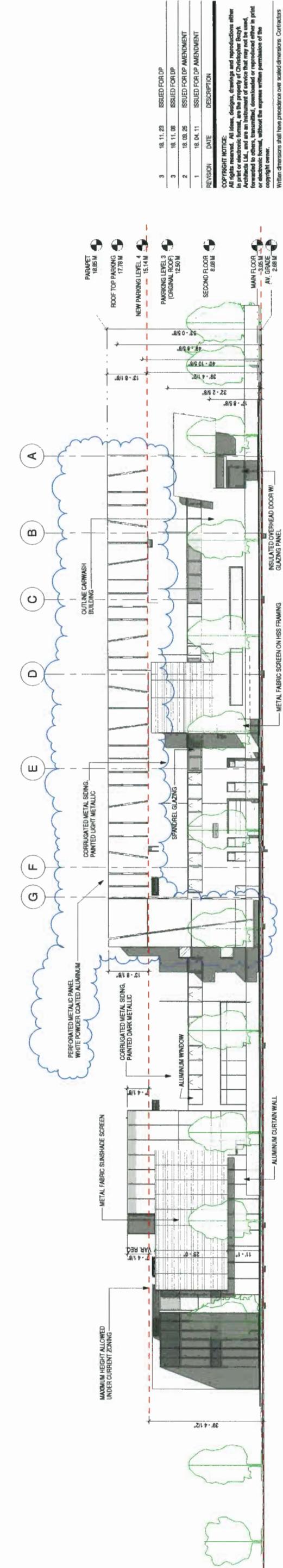
CLIENT

WEST ELEVATION
11/16' - 1'-0"

(1)

WEST ELEVATION
11/16' - 1'-0"

(1)

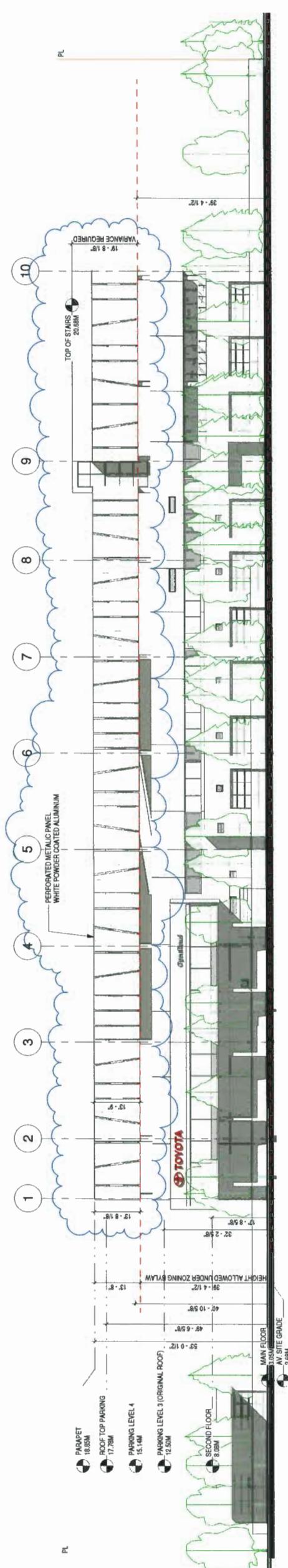


PLAN #5
DP.011
Plan #8

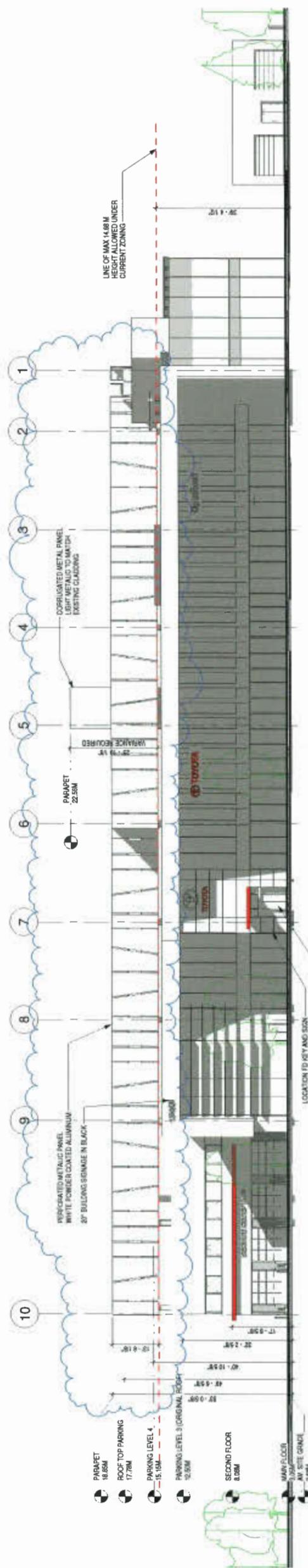
FILE PRINT: L:\Arch\OpenRoad\Auto Group\18-818762\18-818762.dwg
DRAWING NUMBER: 18-818762
SCALE: 1/16' - 1'-0"
DATE: 2019-01-22
PROJECT NUMBER: 214007
DRAFTER: Christopher Bozyk
PLOT DATE: 2019-01-22
PLOT TIME: 10:40 AM

OpenRoad
AUTO GROUP

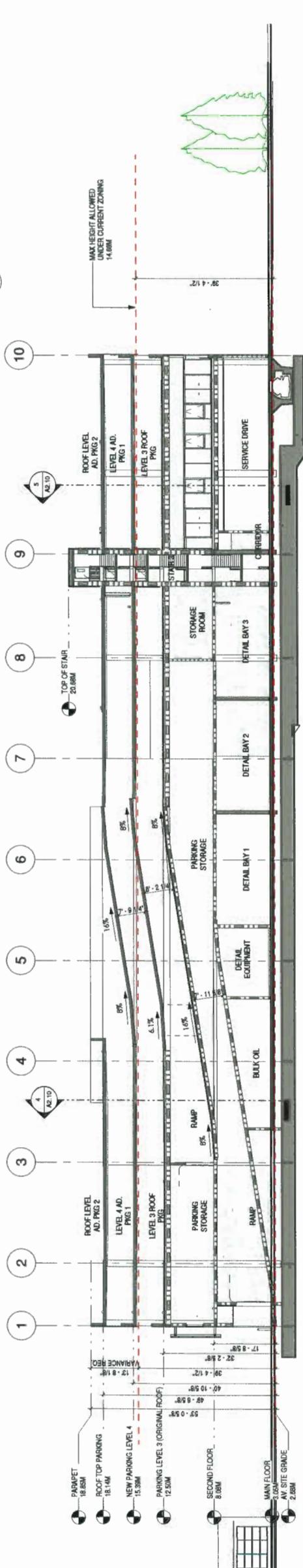
11



(3) SOUTH ELEVATION



2 NORTH ELEVATION $\frac{1}{16}'' = 1'-0''$



① SECTION THROUGH RAMP

DATE: 2019-01-22 DRAWN BY: [Redacted]
SCALE: 1/8" = 1'-0" PROJECT NUMBER: 214027
PLAN #6 DP.012

Plan #9

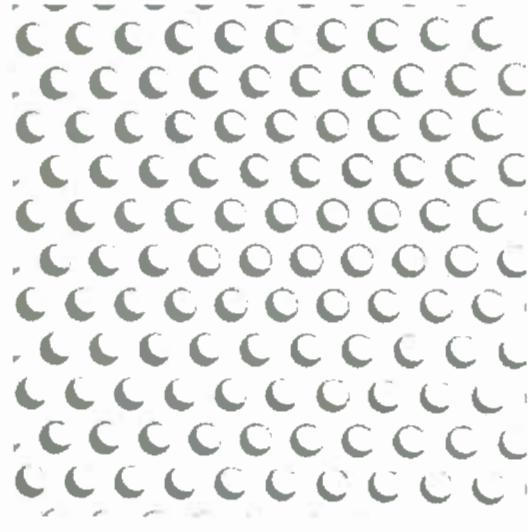
DP 18-818762

FEB 27 2019

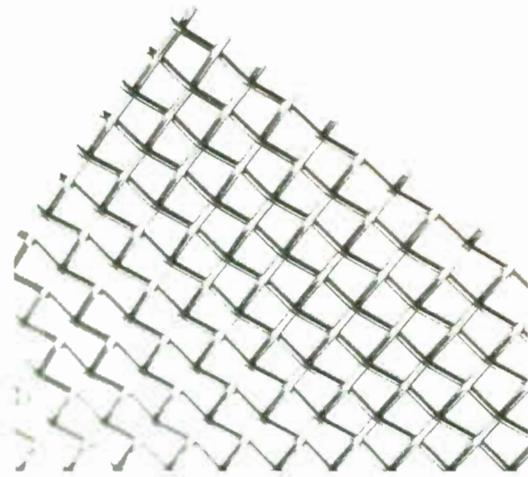
CHRISTOPHER BOZIK ARCHITECTS LTD
 414-611 ALEXANDER STREET VANCOUVER BC V6A 1E1
 PHONE (604) 251-3440 FAX (604) 251-3848

OpenRoad
 AUTO GROUP

CLIENT



WHITE PERFORATED PANEL WITH 2'x2' DIA PERFORATION



WHITE PERFORATED PANEL WITH 2'x2' DIA PERFORATION

MULLION
ANODIZED FINISHGLAZING
SPANDRELGLAZING
LOW-E

| REVISION | DATE | DESCRIPTION |
|----------|----------|---------------|
| 03 | 18.01.22 | ISSUED FOR DP |
| 02 | 18.11.23 | ISSUED FOR DP |
| 01 | 18.11.08 | ISSUED FOR DP |

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Intentional derivations shall have occurred over a period of time. Derivations
 shall not be intentional, or any derivation on the basis of a single
 or isolated set of errors, discrepancies, or statistical information contained in
 contract changes and documents. Failure to obtain such clarification renders the
 Contractor responsible for any misUSED FOR DP ARCHITECTURE rectification.

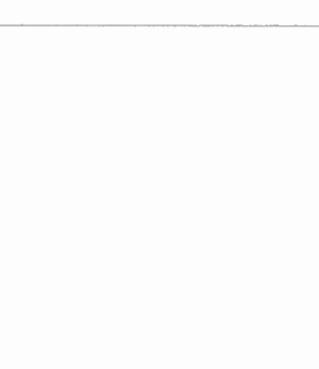
OpenRoad Toyota Richmond



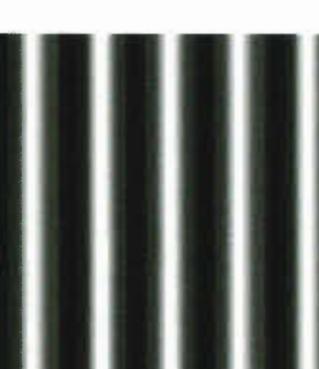
CHARCOAL ACCENT ACM PANEL



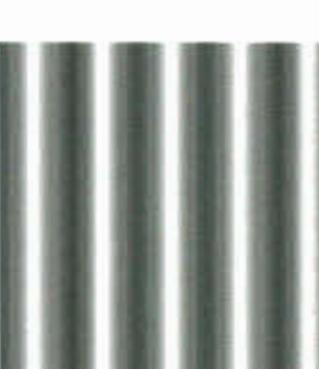
RED ACCENT ACM PANEL



WHITE ACM PANEL



DARK METALLIC CORRUGATED FINISH



LIGHT METALLIC CORRUGATED FINISH

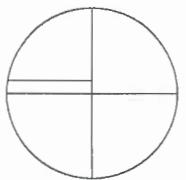
13100 SMALL WOOD PLACE, RICHMOND BC
MATERIAL SAMPLES

SCALE DATE 2018-01-22 DRAWN/AUTHOR
 PROJECT NUMBER 214007

DP.023
FBI FORM 1050 - Drawing Material Sample Sheet

Plan #10

CHRISTOPHER
BOZYK ARCHITECTS LTD
414-511 ALEXANDER STREET, VANCOUVER BC V6A 1E1
PHONE (604) 251-3440 FAX (604) 251-3948



FEB 27 2019

DP-018762

OpenRoad
AUTO GROUP

CLIENT

| | | |
|----|---------------|---------------|
| 02 | 19.01.22 | ISSUED FOR DP |
| 01 | 18.11.23 | ISSUED FOR DP |
| | REVISION DATE | DESCRIPTION |

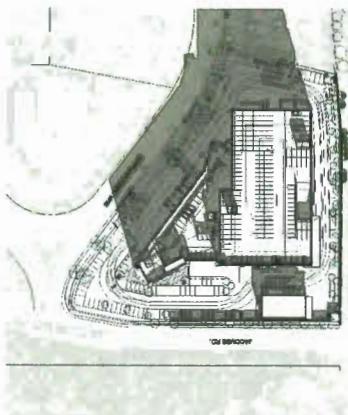
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Without dimensions shall have precedence over scaled dimensions. Contractors shall verify and be responsible for all dimensions and conditions on the plan. It is the responsibility of the contractor to obtain accurate information contained in contract drawings and documents. Failure to do so can result in unnecessary costs to the contractor, responsible for any resulting rework and the cost of refection.

OpenRoad Toyota Richmond

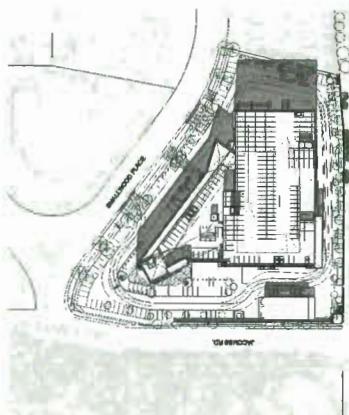
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SHADOW ANALYSIS
SCALE: 1:2000
DATE: 2019-01-22
PROJECT NUMBER: 214077

DP.024

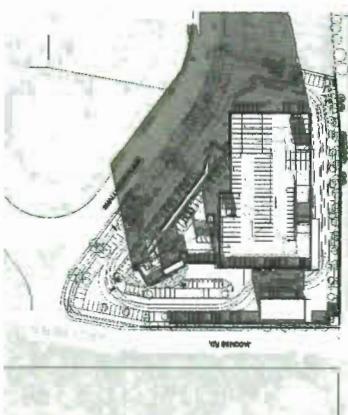
Reference



MARCH 20TH - 18:00



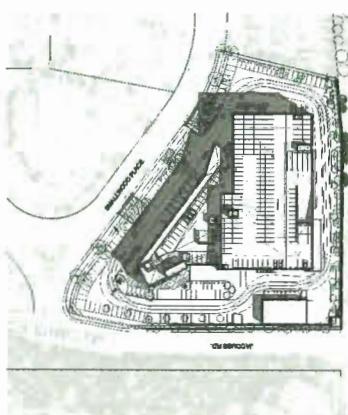
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JUNE 21ST - 18:00



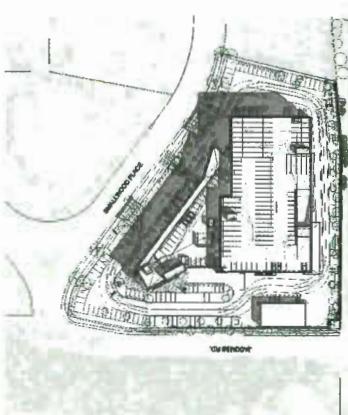
DEC 21ST - 18:00



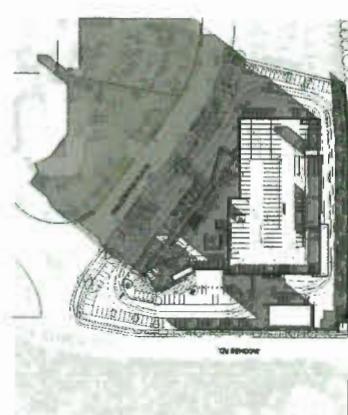
MARCH 20TH - 15:00



MARCH 20TH - 15:00



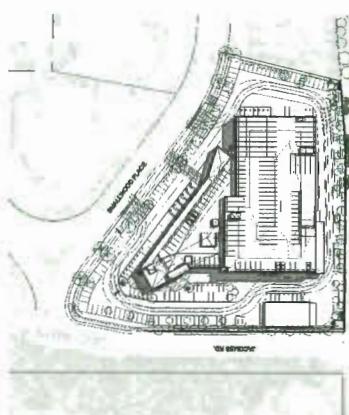
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DEC 21ST - 15:00



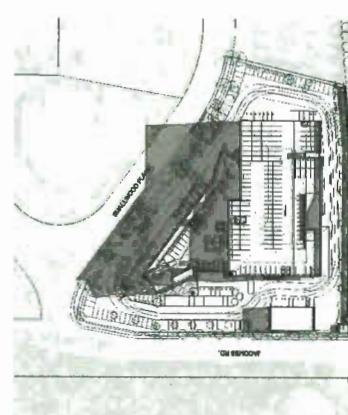
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MARCH 20TH - 12:00



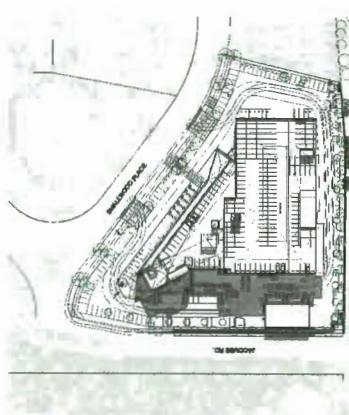
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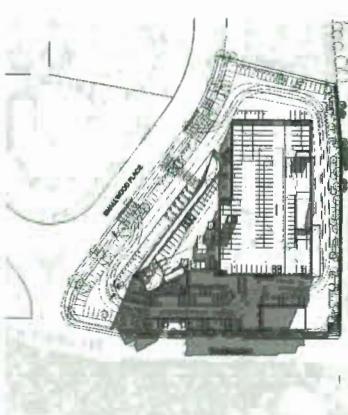
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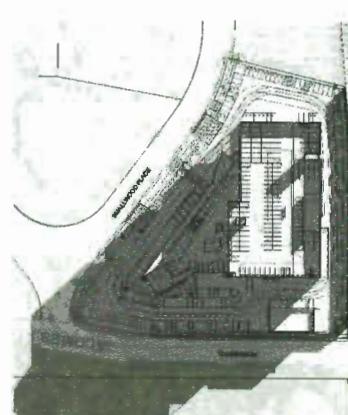
MARCH 20TH - 09:00



MARCH 20TH - 09:00



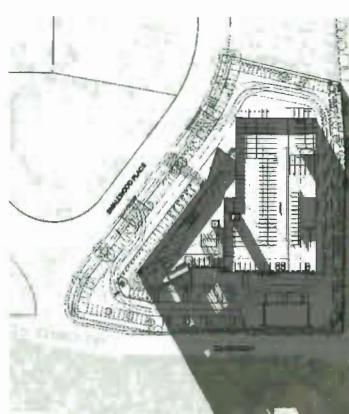
JUNE 21ST - 09:00



DEC 21ST - 09:00



MARCH 20TH - 06:00



MARCH 20TH - 06:00



JUNE 21ST - 06:00



DEC 21ST - 06:00

OpenRoad Toyota

LANDSCAPE ARCHITECTURAL SET – RE-ISSUED FOR BP

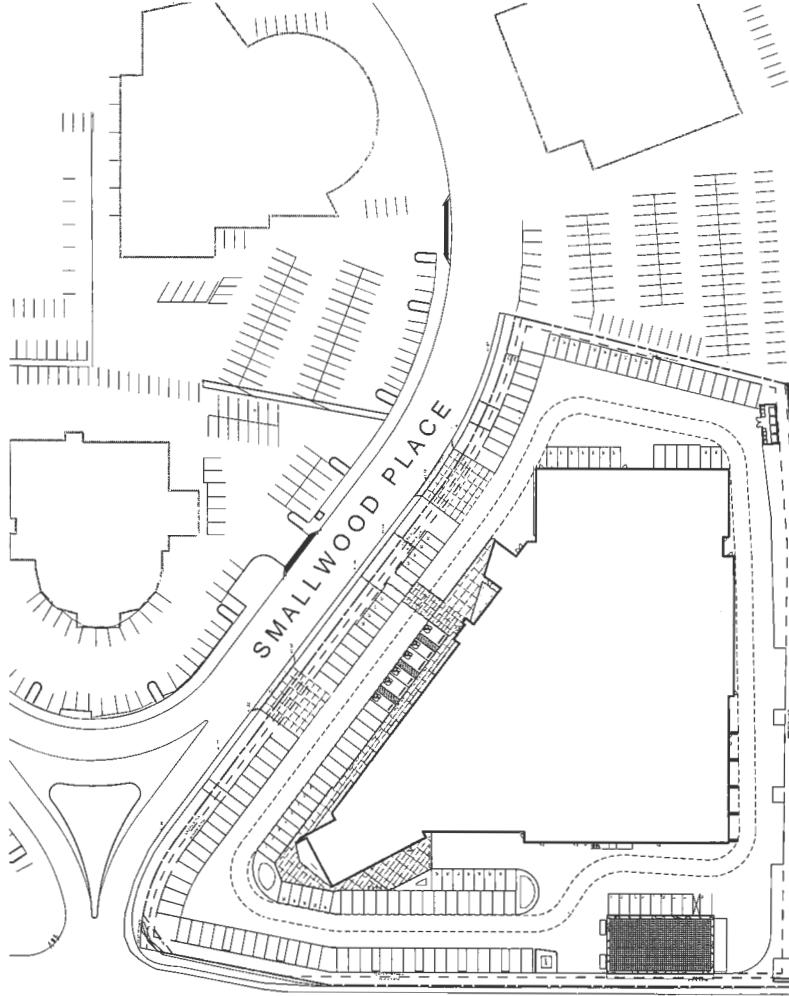
PROJECT INFORMATION

CLIENT:
OPENROAD AUTO GROUP
CONTACT NAME: MOE SABOUNE
EMAIL: MOE.SABOUNE@OPENROADAUTOGROUP.COM
PH: 604.630.0202

LANDSCAPE ARCHITECT:
CONNECT LANDSCAPE ARCHITECTURE INC.
CONTACT NAME: KEN LARSON
VANCOUVER, BRITISH COLUMBIA V6H 2V1
EMAIL: KEN@CONNECTLA.CA
PH: (604) 681-3303

LANDSCAPE DRAWING INDEX

| SHEET No. | Sheet Name |
|-----------|--|
| L.0.0 | COVER SHEET AND DRAWING LIST |
| L.0.1 | TREE MANAGEMENT PLAN |
| L.1.0 | LANDSCAPE SITE PLAN |
| L.1.1 | LANDSCAPE ENLARGEMENTS |
| L.1.2 | LANDSCAPE LAYOUT PLAN |
| L.2.0 | LANDSCAPE PLANT MATERIALS - NORTH |
| L.2.1 | LANDSCAPE PLANT MATERIALS - SOUTH |
| L.3.0 | LANDSCAPE DETAILS - SOFTSCAPE & HARDCAPE |
| L.3.1 | LANDSCAPE DETAILS - FURNISHINGS |



JACOMBS ROAD

GENERAL NOTES

1. ALL LANDSCAPE ARCHITECTURAL DRAWINGS IN THIS PACKAGE SHALL BE READ IN CONJUNCTION WITH ALL OTHER CONSULTANT DRAWINGS, DETAILS, SPECIFICATIONS, AND CORRESPONDENCE THAT MAY BE ISSUED DURING THE COURSE OF THE CONTRACT.
2. IF A DISCREPANCY OCCURS BETWEEN THE DRAWINGS AND THE SPECIFICATIONS OR ANY OTHER DOCUMENT ASSOCIATED WITH THE PROJECT, THE CONFLICT SHALL BE REPORTED IN WRITING TO THE LANDSCAPE ARCHITECT TO OBTAIN CLARIFICATION AND APPROVAL BEFORE PROCEEDING WITH WORKS.
3. THE CONTRACTOR SHALL VISIT THE SITE TO VERIFY THE TRUE EXISTING CONDITIONS ANY UNCLEAR ISSUES SHALL BE CLARIFIED WITH THE LANDSCAPE ARCHITECT. NO CLAIM SHALL BE ALLOWED FOR EXTRAS WHICH MAY ARISE THROUGH NEGLECT OF THIS ADVICE.
4. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.
5. ALL EXISTING INFORMATION IS BASED ON AVAILABLE RECORDS AND SHALL NOT BE CONSTRUED TO BE COMPLETE OR ACCURATE.
6. LAYOUT OF HARDCAPE, SITE FURNITURE, SOIL, PLANTING, AND ALL OTHER LANDSCAPE MATERIALS ARE TO BE STAKED OUT AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
7. ALL PUBLIC REALM DETAILS, AND FINAL SELECTION / APPROVAL OF ALL STREET TREES TO BE APPROVED BY THE MUNICIPALITY.

THE CONTRACTOR IS RESPONSIBLE FOR:
DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND / OR CONCEALED STRUCTURES AT THE PROJECT SITE.

FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.

ARCHITECT:

CHRISTOPHER BOZYK ARCHITECTS

PLAN #3A

| | | |
|----|--------------------------|----------|
| 17 | RE-ISSUED FOR DP | 18-01-22 |
| 16 | RE-ISSUED FOR BP | 18-01-17 |
| 15 | RE-ISSUED FOR DP | 18-11-30 |
| 14 | RE-ISSUED FOR DP | 18-11-08 |
| 13 | RE-ISSUED FOR DP | 18-09-26 |
| 12 | ISSUED FOR CONSTRUCTION | 18-02-28 |
| 11 | ISSUED FOR REVISED BP | 17-12-19 |
| 10 | ISSUED FOR TENDER | 17-11-20 |
| 9 | RE-ISSUED FOR DP | 17-06-29 |
| 8 | ISSUED FOR ADP | 17-06-07 |
| 7 | RE-ISSUED FOR DP | 17-05-30 |
| 6 | RE-ISSUED FOR DP | 17-05-19 |
| 5 | ISSUED FOR BUDGETING | 17-03-24 |
| 4 | RE-ISSUED FOR DP | 17-03-09 |
| 3 | ISSUED FOR BP | 16-11-25 |
| 2 | ISSUED FOR DP | 16-07-27 |
| 1 | ISSUED FOR CLIENT REVIEW | 16-06-20 |

REVISIONS

OPENROAD TOYOTA

13100 Smallwood Place
Richmond, British Columbia

Scale: AS SHOWN
Drawn: KD
Reviewed: KL
Project No. 06-490

LANDSCAPE
COVER PAGE

↑
0 25 50 m

CONNECT LANDSCAPE ARCHITECTURE INC. DOES
NOT GUARANTEE THE EXISTENCE, LOCATION,
AND ELEVATION OF UTILITIES AND / OR
CONCEALED STRUCTURES AT THE PROJECT SITE.

THE CONTRACTOR IS RESPONSIBLE FOR
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ELEVATION OF ALL UTILITIES AND / OR
CONCEALED STRUCTURES, AND IS RESPONSIBLE
FOR NOTIFYING THE APPROPRIATE COMPANY,
DEPARTMENT OR PERSON(S) OF ITS INTENTION
TO CARRY OUT ITS OPERATIONS.

CHRISTOPHER BOZTYK ARCHITECTS
ARCHITECT:

| TREE MANAGEMENT LEGEND | SYMBOL |
|---|--------|
| EXISTING TREE TO BE REMOVED PER ARBORIST REPORT | (X) |
| EXISTING TREE TO BE RETAINED PER ARBORIST REPORT | (+) |
| TREE PROTECTION FENCING PER CITY OF RICHMOND | (1) |

TO BE REMOVED:
JACOMBS ROAD 13
SMALLWOOD PLACE 3
TOTAL 16

TO BE INSTALLED:
JACOMBS ROAD 18 (12 @ 7.6cm CAL.)
SMALLWOOD PLACE 3 (7.6cm CAL.)
SOUTH PROPERTY LINE 44 (27 @ 7.6cm CAL.)
ON SITE 8 (5 @ 7.6cm CAL.)
TOTAL 73 (47 @ 7.6cm CAL.)
SEE PLANTING PLAN 1.0 FOR NEW AND REPLACEMENT TREES

PLAN #3B

| | |
|----------------------------|----------|
| 17 RE-ISSUED FOR DP | 19-01-22 |
| 16 RE-ISSUED FOR BP | 19-01-17 |
| 15 RE-ISSUED FOR DP | 18-11-30 |
| 14 RE-ISSUED FOR DP | 18-11-08 |
| 13 RE-ISSUED FOR BP | 18-09-26 |
| 12 ISSUED FOR CONSTRUCTION | 18-02-28 |
| 11 ISSUED FOR REVISED BP | 17-12-19 |
| 10 ISSUED FOR TENDER | 17-11-20 |
| 9 REISSUED FOR DP | 17-06-29 |
| 8 ISSUED FOR ADP | 17-06-07 |
| 7 RE-ISSUED FOR DP | 17-05-30 |
| 6 RE-ISSUED FOR DP | 17-05-19 |
| 5 ISSUED FOR BUDGETING | 17-03-24 |
| 4 RE-ISSUED FOR OP | 17-03-09 |
| 3 ISSUED FOR BP | 16-11-25 |
| 2 ISSUED FOR DP | 16-07-27 |
| 1 ISSUED FOR CLIENT REVIEW | 16-06-20 |

DP 18-01-01-0762

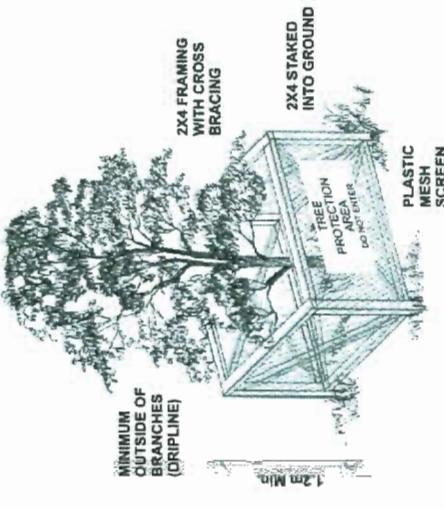
LANDSCAPE
TREE MANAGEMENT



EXISTING OFFSITE
TREES TO BE RETAINED

WESTMINSTER HIGHWAY

1 NOT TO SCALE



Tree Protection Distance Table

| Tree Trunk Diameter cm | Minimum Protection Required Around Tree | | |
|---------------------------|---|-----|------|
| | feet | m | feet |
| 20 | 0.6 | 1.2 | 3.9 |
| 25 | 1.0 | 0.8 | 4.9 |
| 30 | 1.2 | 1.0 | 5.9 |
| 35 | 1.4 | 1.2 | 6.9 |
| 40 | 1.6 | 1.3 | 7.9 |
| 45 | 1.8 | 1.5 | 8.9 |
| 50 | 2.0 | 1.7 | 9.8 |
| 55 | 2.2 | 1.8 | 10.8 |
| 60 | 2.4 | 2.0 | 11.8 |
| 75 | 3.0 | 2.5 | 14.8 |
| 90 | 3.6 | 3.0 | 16.4 |
| 100 | 4.0 | 3.3 | 19.7 |

OPENROAD TOYOTA

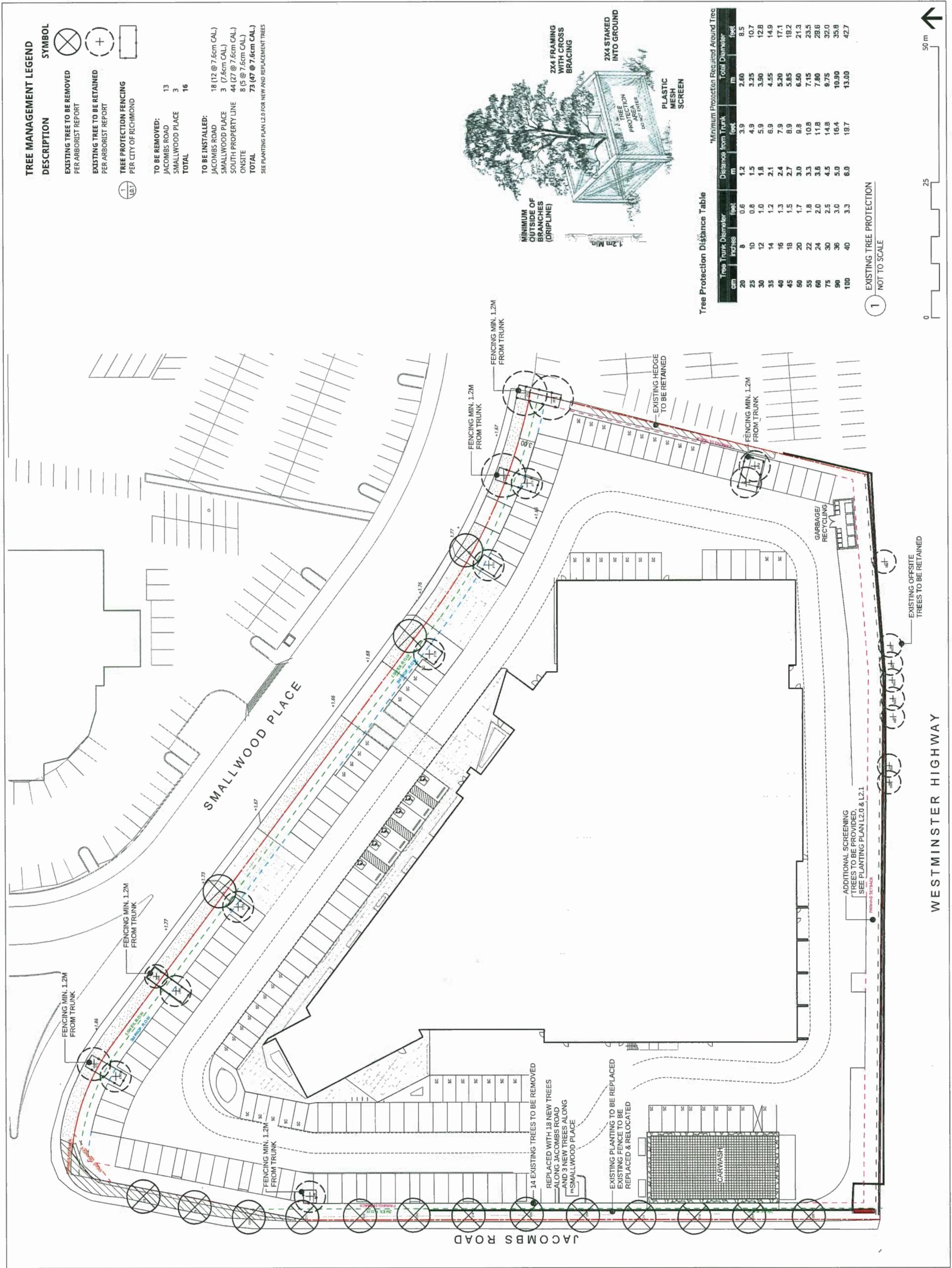
13100 Smallwood Place
Richmond, British Columbia

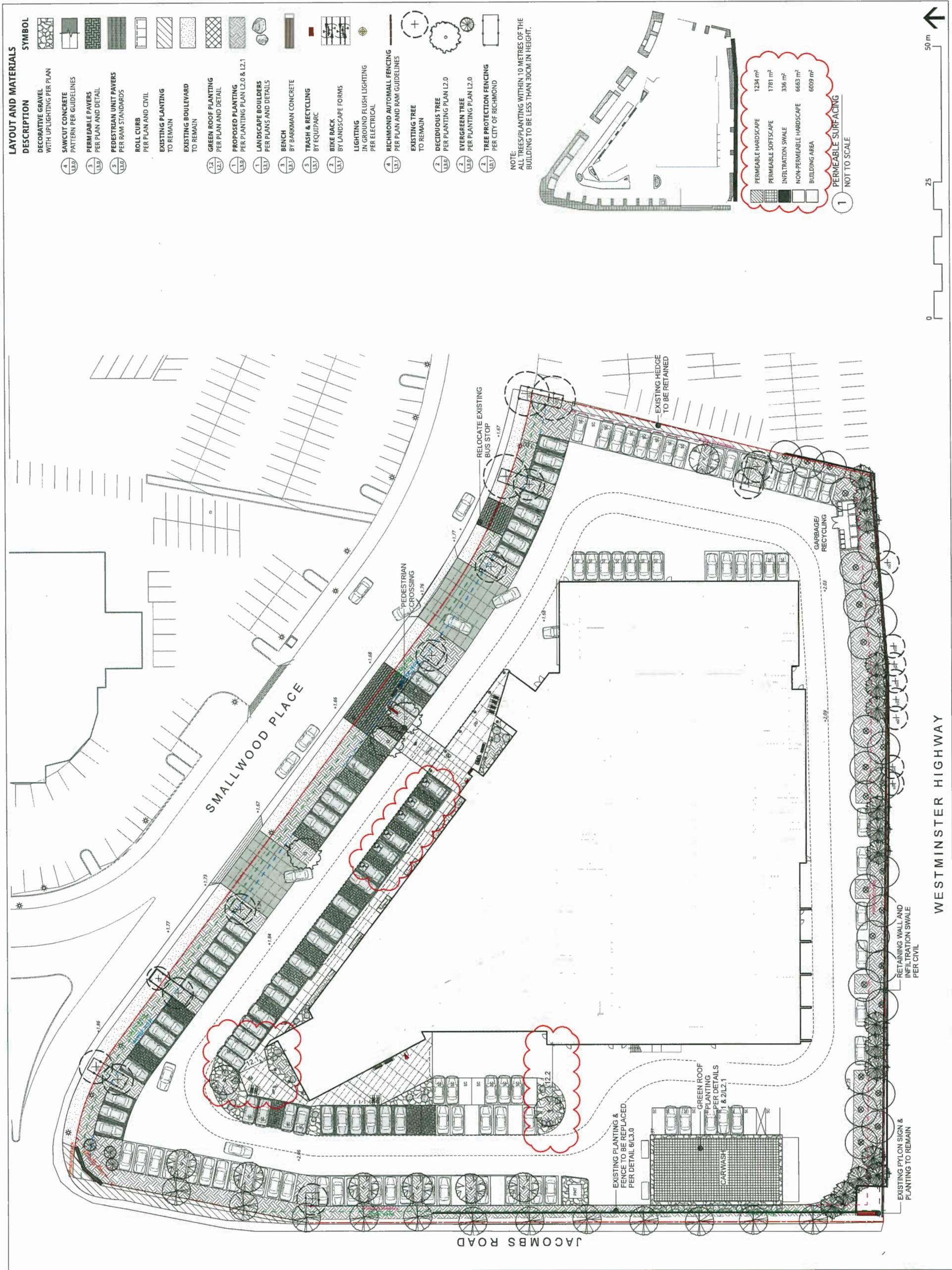
Scale: 1:300
Drawn: KD
Reviewed: KL
Project No. 06-490

LANDSCAPE
TREE MANAGEMENT

0 25 50 m ↑

LO.1
Plan #12





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ELEVATION OF ALL UTILITIES AND / OR
CONCEALED STRUCTURES, AND IS RESPONSIBLE
FOR NOTIFYING THE APPROPRIATE COMPANY,
DEPARTMENT OR PERSON(S) OF ITS INTENTION
TO CARRY OUT ITS OPERATIONS.

ARCHITECT:

CHRISTOPHER BOZYK ARCHITECTS

PLAN #3D

| | | |
|----|--------------------------|----------|
| 17 | RE-ISSUED FOR DP | 19-01-22 |
| 16 | RE-ISSUED FOR BP | 19-01-17 |
| 15 | RE-ISSUED FOR DP | 18-11-30 |
| 14 | RE-ISSUED FOR BP | 18-11-08 |
| 13 | RE-ISSUED FOR DP | 18-09-26 |
| 12 | ISSUED FOR CONSTRUCTION | 18-02-28 |
| 11 | ISSUED FOR REVISED BP | 17-12-19 |
| 10 | ISSUED FOR TENDER | 17-11-20 |
| 9 | RE-ISSUED FOR DP | 17-06-29 |
| 8 | ISSUED FOR ADP | 17-06-07 |
| 7 | RE-ISSUED FOR DP | 17-05-30 |
| 6 | RE-ISSUED FOR DP | 17-05-19 |
| 5 | ISSUED FOR BUDGETING | 17-03-24 |
| 4 | RE-ISSUED FOR DP | 17-03-09 |
| 3 | ISSUED FOR BP | 16-11-26 |
| 2 | ISSUED FOR DP | 16-07-27 |
| 1 | ISSUED FOR CLIENT REVIEW | 16-06-20 |

DP 18-818762

FEB 27 2019

OPENROAD TOYOTA

13100 Smallwood Place
Richmond, British Columbia

Scale: 1:150
Drawn: KD
Reviewed: KL
Project No. 06-490

LANDSCAPE ENLARGEMENTS



2 CORNER ENLARGEMENT PLAN

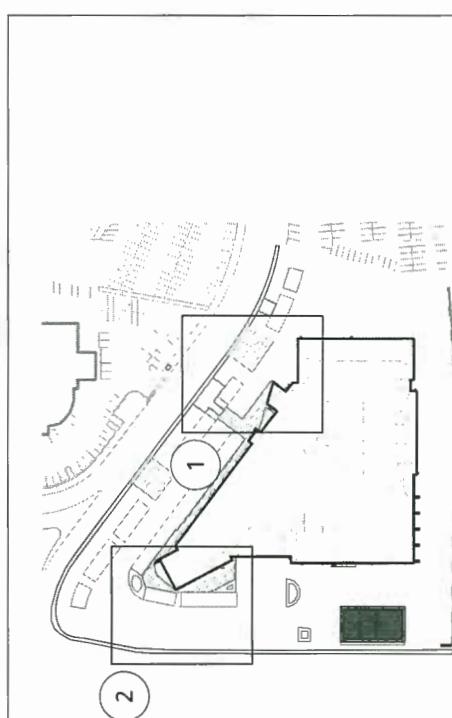
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1 MAIN ENTRY ENLARGEMENT

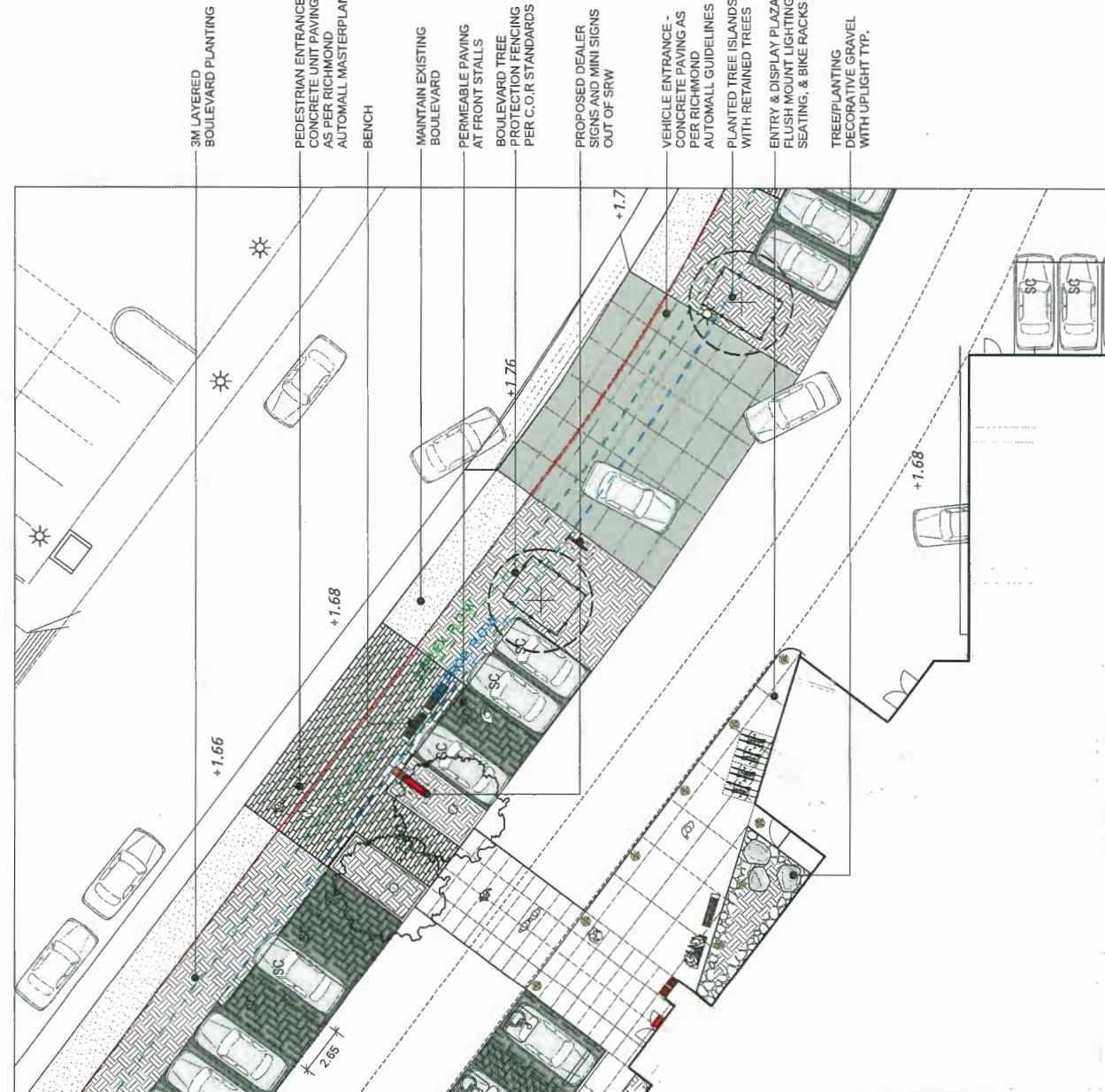
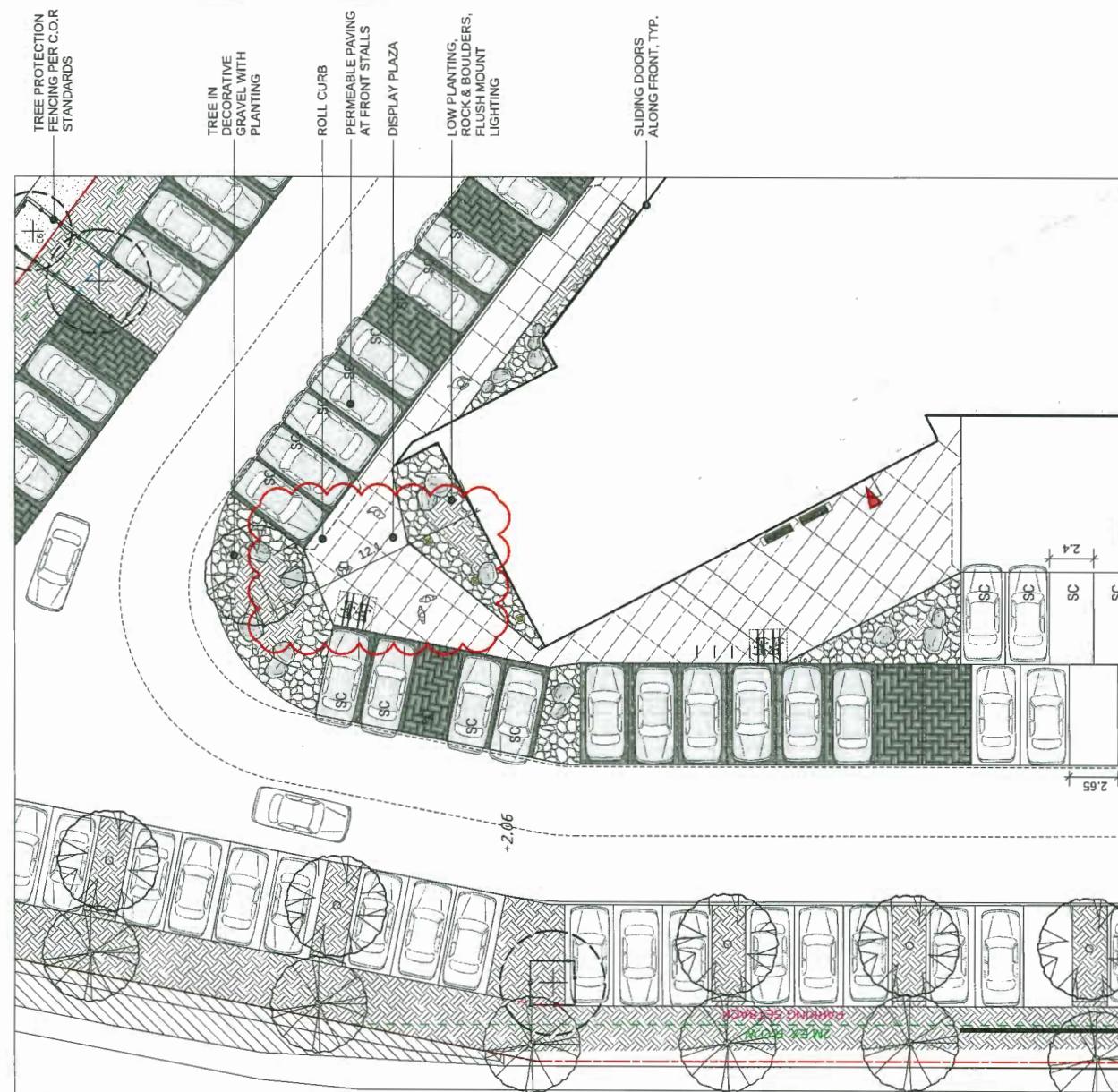
Scale: 1:160

L1.1

Plan #14



NOTE: ALL TREES/PLANTING WITHIN 10 METRES OF THE BUILDING TO BE LESS THAN 30CM IN HEIGHT.



CONNECT LANDSCAPE ARCHITECTURE INC. DOES NOT GUARANTEE THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES AND / OR CONCEALED STRUCTURES AT THE PROJECT SITE.
THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND / OR CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.

ARCHITECT:

CHRISTOPHER BOZYK ARCHITECTS

PLAN #3E

| | | |
|----|--------------------------|----------|
| 17 | RE-ISSUED FOR DP | 19-01-22 |
| 16 | RE-ISSUED FOR BP | 19-01-17 |
| 15 | RE-ISSUED FOR DP | 18-11-30 |
| 14 | RE-ISSUED FOR DP | 18-11-08 |
| 13 | RE-ISSUED FOR DP | 18-09-26 |
| 12 | ISSUED FOR CONSTRUCTION | 18-02-28 |
| 11 | ISSUED FOR REVISED BP | 17-12-19 |
| 10 | ISSUED FOR TENDER | 17-11-20 |
| 9 | REISSUED FOR DP | 17-08-29 |
| 8 | ISSUED FOR ADP | 17-08-07 |
| 7 | RE-ISSUED FOR DP | 17-05-30 |
| 6 | RE-ISSUED FOR DP | 17-05-19 |
| 5 | ISSUED FOR BUDGETING | 17-03-24 |
| 4 | RE-ISSUED FOR DP | 17-03-09 |
| 3 | ISSUED FOR BP | 16-11-25 |
| 2 | ISSUED FOR DP | 16-07-27 |
| 1 | ISSUED FOR CLIENT REVIEW | 16-08-20 |

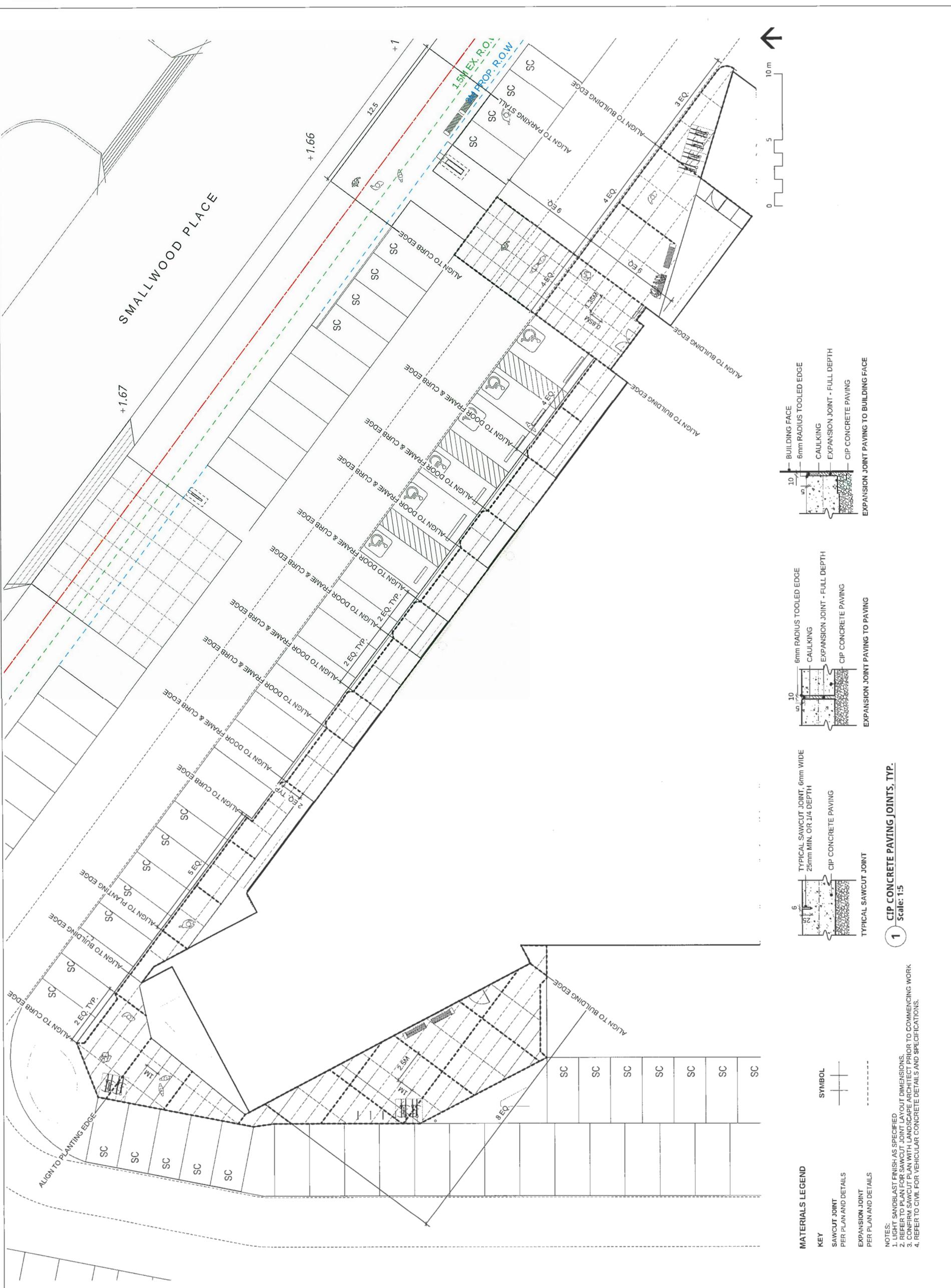
REVISIONS

OPENROAD TOYOTA

13100 Smallwood Place
Richmond, British Columbia

Scale: 1:125
Drawn: KD
Reviewed: KL
Project No. 06-490

LANDSCAPE LAYOUT PLAN



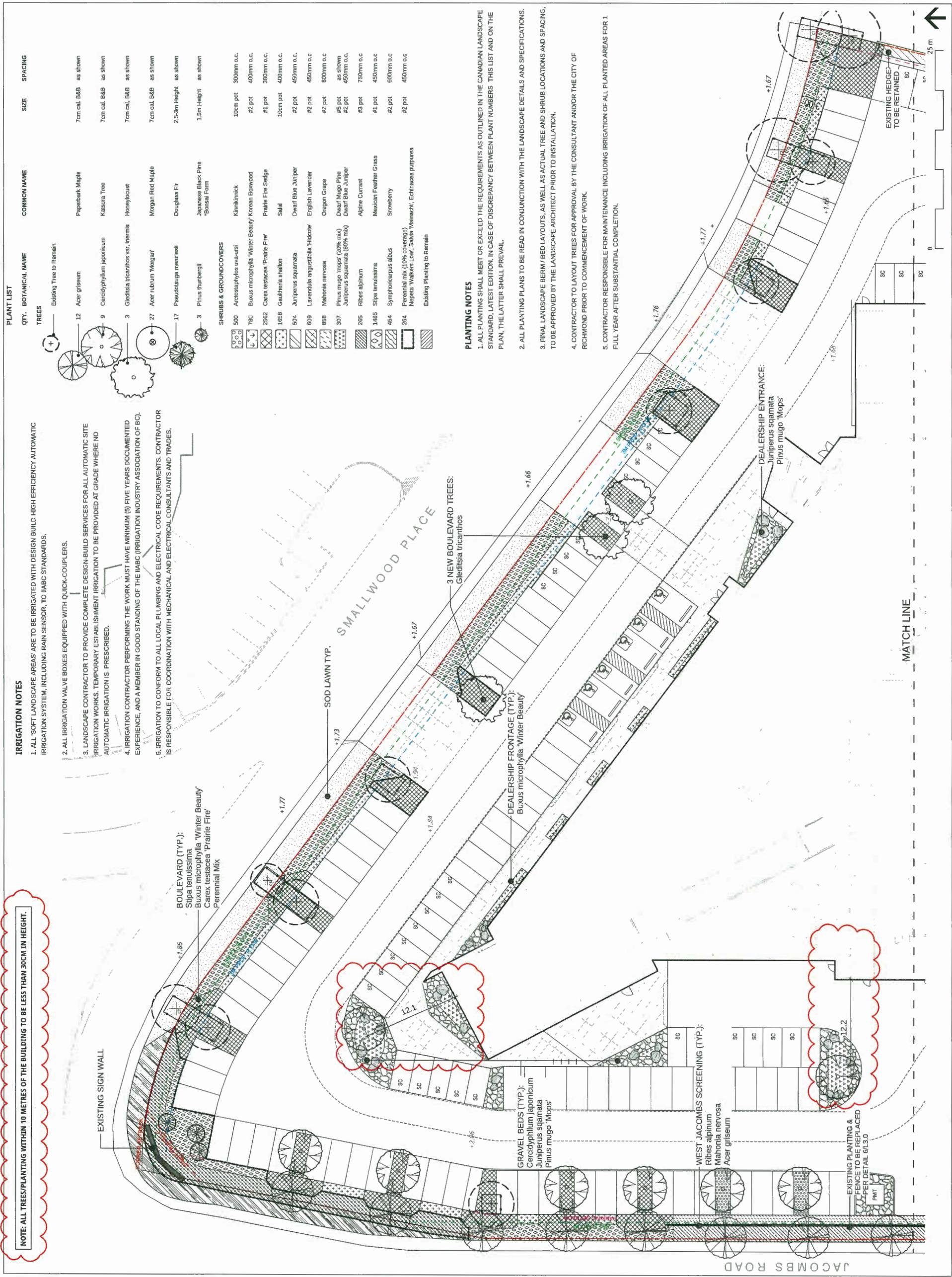
CIP CONCRETE PAVING JOINTS, TYP.

1

- NOTES:
1. LIGHT SMOOTH FINISH AS SPECIFIED
2. REFER TO PLAN FOR SAWCUT JOINT LAYOUT DIMENSIONS.
3. CONFIRM SAWCUT PLAN WITH LANDSCAPE ARCHITECT PRIOR TO COMMENCING WORK.
4. REFER TO CIVIL FOR VEHICULAR CONCRETE DETAILS AND SPECIFICATIONS.

L1.2

Plan # 15

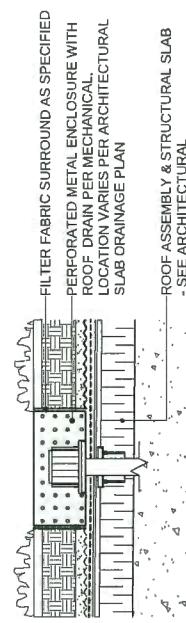


REFER TO L2.0 FOR PLANT LIST.

IRRIGATION NOTES

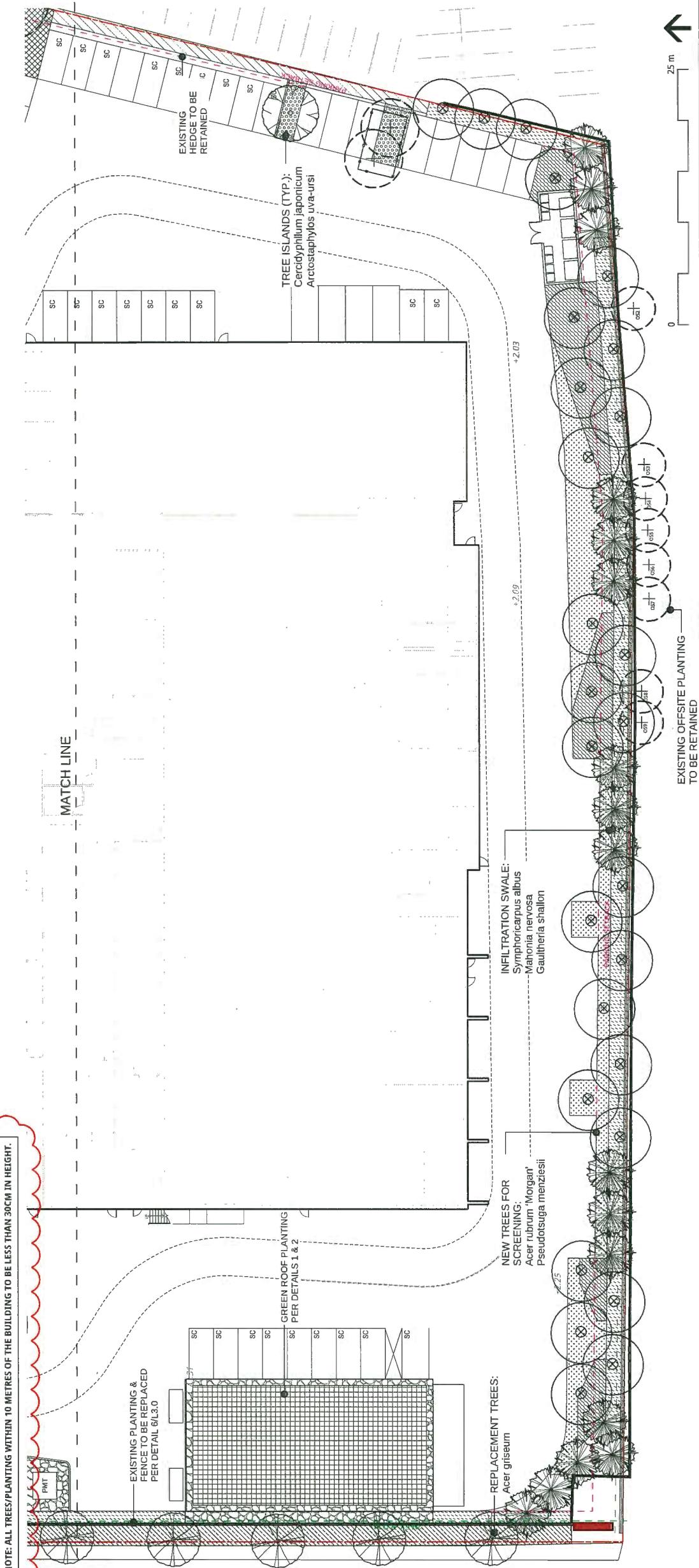
1. ALL SOFT LANDSCAPE AREAS (INCLUDING ONSITE, OFFSITE, AND GREEN ROOF AREAS) ARE TO BE IRRIGATED WITH DESIGN BUILD HIGH EFFICIENCY AUTOMATIC IRRIGATION SYSTEM, INCLUDING RAIN SENSOR, TO IABC STANDARDS.
2. ALL IRRIGATION VALVE BOXES EQUIPPED WITH QUICK-COUPPLERS.
3. LANDSCAPE CONTRACTOR TO PROVIDE COMPLETE DESIGN-BUILD SERVICES FOR ALL AUTOMATIC SITE IRRIGATION WORKS. TEMPORARY ESTABLISHMENT IRRIGATION TO BE PROVIDED AT GRADE WHERE NO AUTOMATIC IRRIGATION IS PRESCRIBED.
4. IRRIGATION CONTRACTOR PERFORMING THE WORK MUST HAVE MINIMUM (5) FIVE YEARS DOCUMENTED EXPERIENCE, AND A MEMBER IN GOOD STANDING OF THE IABC (IRRIGATION INDUSTRY ASSOCIATION OF BC).
5. IRRIGATION TO CONFORM TO ALL LOCAL PLUMBING AND ELECTRICAL CODE REQUIREMENTS. CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH MECHANICAL AND ELECTRICAL CONSULTANTS AND TRADES.

1 Scale: 1:10



2 EXTENSIVE PLANTING DRAIN
Scale: 1:10

NOTE: ALL TREES/PLANTING WITHIN 10 METRES OF THE BUILDING TO BE LESS THAN 30CM IN HEIGHT.



13100 Smallwood Place
Richmond, British Columbia

OPENROAD TOYOTA

Scale: 1:200
Drawn: KD
Reviewed: KL
Project No. 06-490

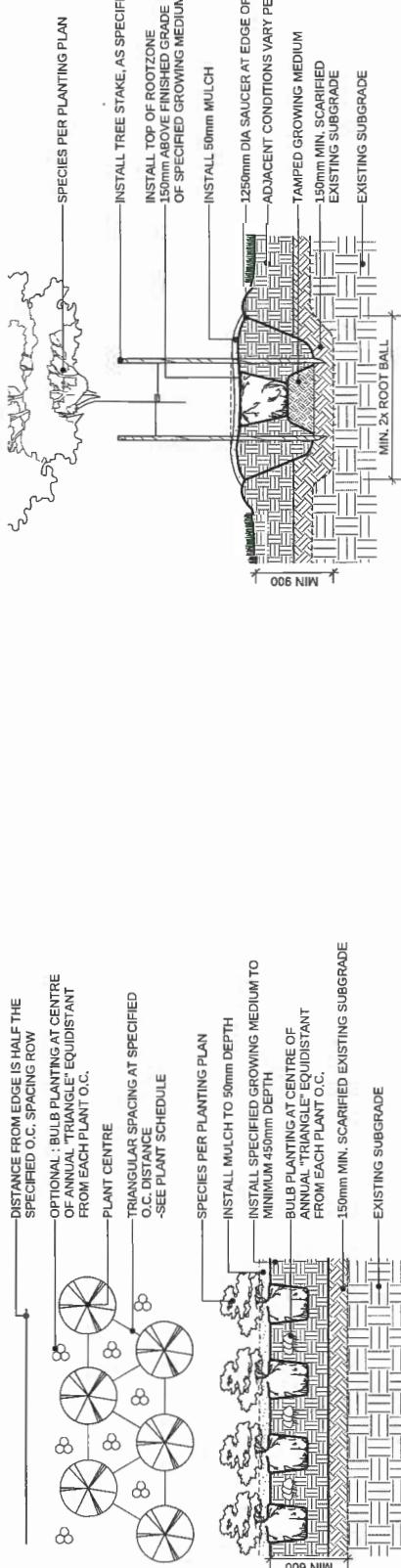
LANDSCAPE PLANT MATERIALS SOUTH

L2.1

Plan #17

CONNECT LANDSCAPE ARCHITECTURE INC. DOES NOT GUARANTEE THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES AND / OR CONCEALED STRUCTURES AT THE PROJECT SITE.
THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND / OR CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.

CHRISTOPHER BOZYK ARCHITECTS
ARCHITECT:



| | |
|----------------------------|----------|
| PLAN #3H | 19-01-22 |
| 17 RE-ISSUED FOR DP | 19-01-17 |
| 16 RE-ISSUED FOR BP | 18-11-30 |
| 15 RE-ISSUED FOR DP | 18-11-08 |
| 14 RE-ISSUED FOR DP | 18-09-26 |
| 13 RE-ISSUED FOR DP | 18-09-26 |
| 12 ISSUED FOR CONSTRUCTION | 18-02-28 |
| 11 ISSUED FOR REVISED BP | 17-12-19 |
| 10 ISSUED FOR TENDER | 17-11-20 |
| 9 REISSUED FOR DP | 17-05-29 |
| 8 ISSUED FOR ADP | 17-05-07 |
| 7 RE-ISSUED FOR BP | 17-05-30 |
| 6 RE-ISSUED FOR BP | 17-05-19 |
| 5 ISSUED FOR BUDGETING | 17-03-24 |
| 4 RE-ISSUED FOR DP | 17-03-09 |
| 3 ISSUED FOR BP | 16-11-25 |
| 2 ISSUED FOR DP | 16-07-27 |
| 1 ISSUED FOR CLIENT REVIEW | 16-06-20 |

DB-18762

OPENROAD TOYOTA

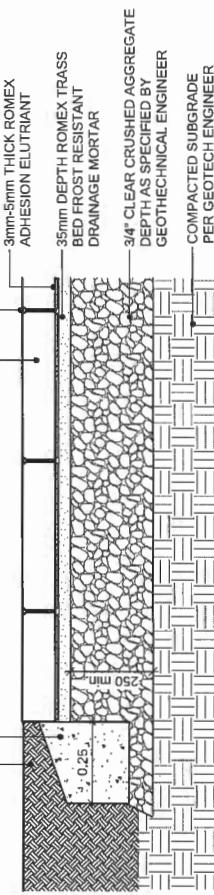
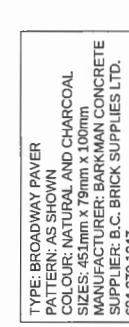
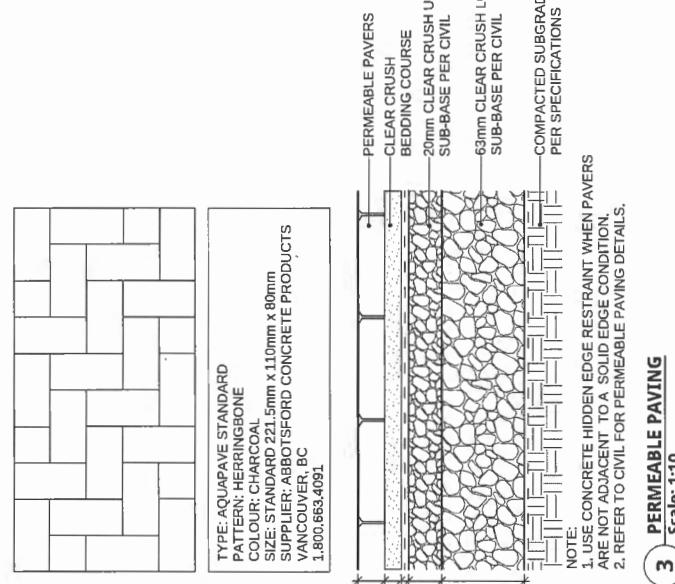
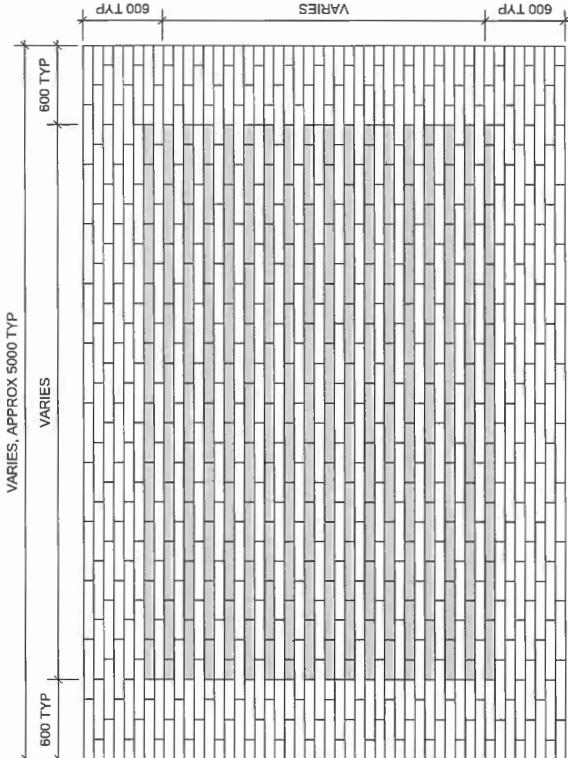
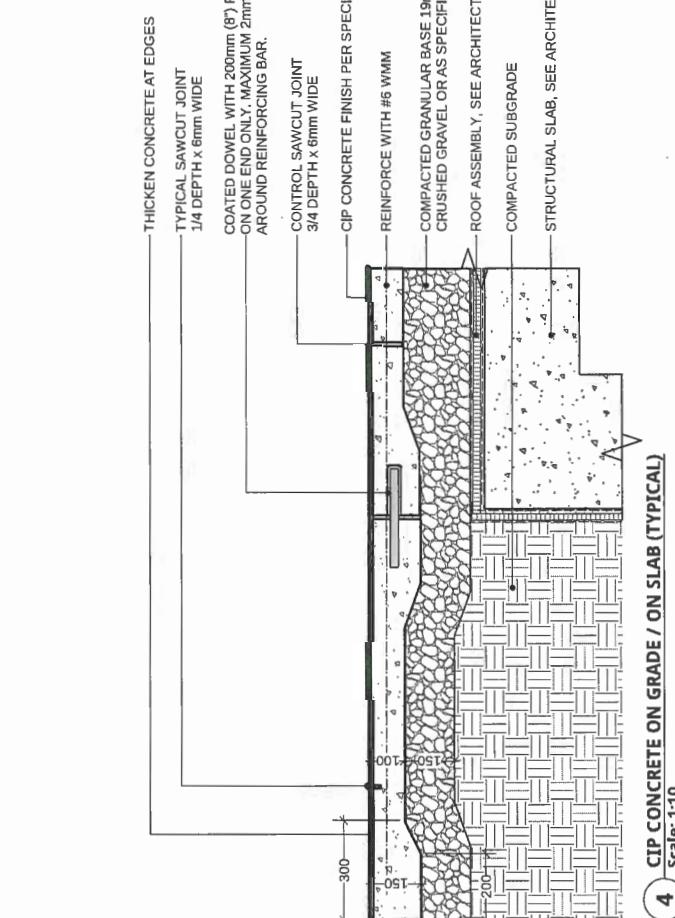
13100 Smallwood Place
Richmond, British Columbia
Scale: AS SHOWN
Drawn: KD
Reviewed: KL
Project No. 06-490

LANDSCAPE DETAILS

SOFTSCAPE & HARDCAPE

L3.0

Plan #18



CONNECT LANDSCAPE ARCHITECTURE INC. DOES NOT GUARANTEE THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES AND / OR CONCEALED STRUCTURES AT THE PROJECT SITE.

THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES AND / OR CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT, OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.

ARCHITECT:

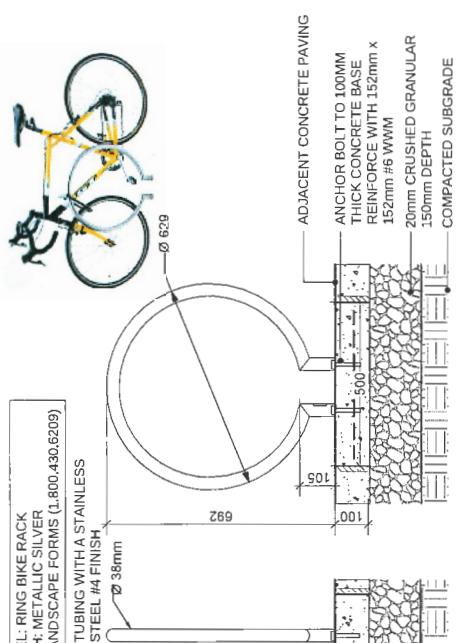
CHRISTOPHER BOZYK ARCHITECTS

PLAN #31

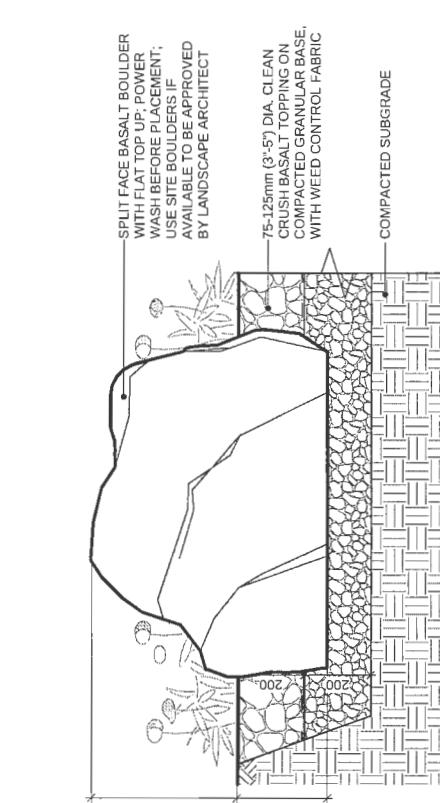
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| 16 | RE-ISSUED FOR BP | 19-01-17 |
| 15 | RE-ISSUED FOR DP | 18-11-30 |
| 14 | RE-ISSUED FOR DP | 18-11-08 |
| 13 | RE-ISSUED FOR DP | 18-09-26 |
| 12 | ISSUED FOR CONSTRUCTION | 18-02-28 |
| 11 | ISSUED FOR REVISED BP | 17-12-19 |
| 10 | ISSUED FOR TENDER | 17-11-20 |
| 9 | REISSUED FOR DP | 17-06-29 |
| 8 | ISSUED FOR ADP | 17-06-07 |
| 7 | RE-ISSUED FOR DP | 17-05-30 |
| 6 | RE-ISSUED FOR DP | 17-05-19 |
| 5 | ISSUED FOR BUDGETTING | 17-03-24 |
| 4 | RE-ISSUED FOR DP | 17-03-09 |
| 3 | ISSUED FOR BP | 16-11-25 |
| 2 | ISSUED FOR DP | 16-07-27 |
| 1 | ISSUED FOR CLIENT REVIEW | 16-06-28 |



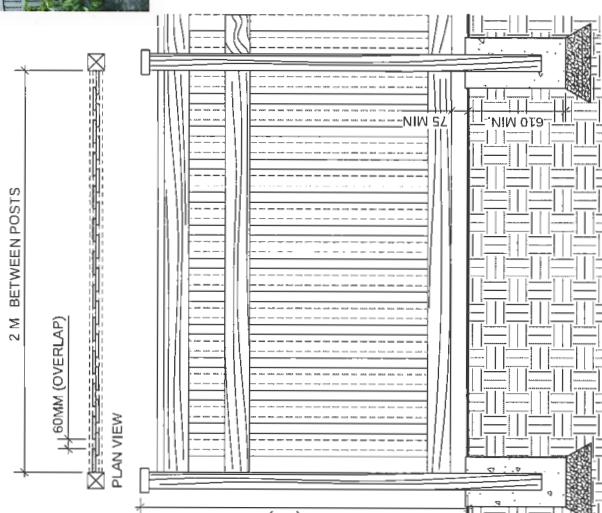
CONCRETE BENCH



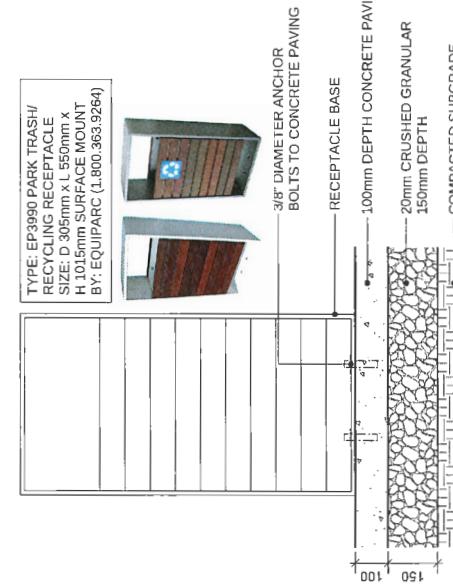
RING BIKE RACK BY LANDSCAPE FORMS



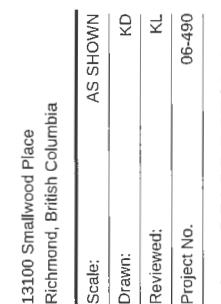
LANDSCAPE BOULDER



PERIMETER WOOD FENCE



TRASH/RECYCLING RECEPTACLE



| | |
|-------------|----------|
| Scale: | AS SHOWN |
| Drawn: | KD |
| Reviewed: | KL |
| Project No. | 06-490 |

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| Scale: | AS SHOWN |
| Drawn: | KD |
| Reviewed: | KL |
| Project No. | 06-490 |

LANDSCAPE DETAILS & FURNISHINGS

Plan #19

DP 18-818762

FEB 27 2019

DP.001

Plan # 20



EAST ELEVATION
OpenRoad Toyota Richmond
PROJECT NUMBER: 214007
CHRISTOPHER BOZYK ARCHITECTS LTD
414-611 ALEXANDER STREET VANCOUVER BC V6A 1E1
PHONE: (604) 251-3400 FAX: (604) 251-3948
FILE PATH: c:\users\christopher\desktop\openroad\toyota\214007.dwg
PLOT DATE: 10/22/2018 8:38:54 AM
SCALE: 2019-01-22



City of Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: February 8, 2019

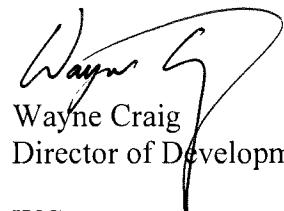
From: Wayne Craig
Director of Development

File: DP 18-825006

Re: Application by IBI Group Architects (Canada) Inc. for a Development Permit at
9455 and 9533 Bridgeport Road

Staff Recommendation

That a Development Permit be issued which would permit the construction of two hotels at 9455 and 9533 Bridgeport on sites zoned “Light Industrial, Office and Hotel (ZI10) – Bridgeport Village (City Centre)”.


Wayne Craig
Director of Development

WC: mm
Att. 5

Staff Report

Origin

IBI Architects (Canada) Inc. has applied to the City of Richmond for permission to develop two hotels at 9455 and 9533 Bridgeport Road on sites zoned “Light Industrial, Office and Hotel (ZI10) – Bridgeport Village (City Centre)”. These sites are currently vacant.

The sites were rezoned (RZ 10-539048) on September 11, 2017. The Development Permit (DP 11-594571) for the sites was also issued on September 11, 2017 for this multi-phase development that includes:

- The subject hotel sites at 9455 and 9533 Bridgeport Road.
- A Business Centre (office/light industrial building) at 9466 Beckwith Road on which a Building Permit is pending issuance.

This current Development Permit application is being sought to authorize proposed design changes to the two hotel buildings fronting Bridgeport Road (**Attachments 1, 2 and 3**).

The Servicing Agreement (SA 12-611073) required for the sites has been entered into with the City and other legal documents have been registered on title as a condition of approval of the above-noted rezoning. The Servicing Agreement includes widening of Bridgeport Road to accommodate a centre median and Beckwith Road to accommodate wider travel lanes and an on-street bike lane. The frontage works along each of these roads include new sidewalks and grassed boulevards. There will also be an upgrade of the storm sewers along both Beckwith and Bridgeport Roads and a new water line running north through the site within a Statutory Right-of-Way. A new north-south road will be constructed which straddles the Hotel 1 and 2 sites and connects Bridgeport Road to Beckwith Road. Public access over this road is secured by a Statutory Right-of-Way. All required contributions and securities from the previous applications remain with the City.

The relatively minor proposed external changes to buildings principally involve:

- For 9533 Bridgeport Road (Hotel 1): There is a proposed increase in height from 9 to 10 storeys, changes to façade articulation, increased glazing, a minimal increase to the tower floor plate size, and a decrease in the main floor area from 1,211 m² (13,035 ft²) to 1,166 m² (12,514 ft²). The number of rooms has decreased from 122 to 112 in the revised proposal.
- For 9455 Bridgeport Road (Hotel 2): While maintaining the height at 12 storeys, there are proposed changes to façade articulation, increased glazing, an increase in tower floor plate size from 791 m² (8,510 ft²) to 865 m² (9,301 ft²) and an increase in the main floor area from 1,400 m² (15,065 ft²) to 1,471 m² (15,835 ft²). The number of rooms has increased from 167 to 200 in the revised proposal.

There have also been minor changes to the parking, loading and recycling areas, tree retention and landscaping as discussed below in this staff report.

The above modifications have been required to accommodate the proposed changes to the unit layouts and design required by Marriott International who will operate the 112-room Hotel 1 as an extended-stay “Marriott Residence Inn” and the 200-room Hotel 2 as a short-stay “Marriott Courtyard Inn”.

Development Information

Please refer to attached Development Application Data Sheet (**Attachment 1**) for a comparison of the proposed development data with the relevant bylaw requirements.

Background

Development surrounding the subject site is as follows:

- To the North: A large lot at 9466 Beckwith Road zoned “Light Industrial, Office and Hotel (ZI10) – Bridgeport Village (City Centre)” and subject to the above-noted approved Development Permit (DP 11-594571) which authorizes the construction of an 11-storey Business Centre building on this lot and applies to the subject hotel sites. There is also a vacant lot at 9520 Beckwith Road currently zoned “Single Detached (R1/F)” that subject to an in-process Rezoning Application (RZ 18-821103) to permit construction of a seven storey office and light industrial building. This rezoning application is currently under staff review. A staff report on this rezoning application will be presented to Council at the time of completion of this staff review. This lot is designated “General Urban – Transect 4 (25m) Area B – Industrial Reserve: Limited Commercial” within the City Centre Area Plan (CCAP);
- To the East: There are four large retail/commercial buildings on a site zoned “Auto-Oriented Commercial (CA)” and designated “General Urban – Transect 4 (25m) Area B – Industrial Reserve: Limited Commercial” within the CCAP;
- To the South: Across Bridgeport Road is the access ramp to Highway 99 which is zoned “School and Institutional Use (SI)”; and
- To the West: There is the Highway 99 / Oak Street Bridge corridor which is zoned “School and Institutional Use (SI)”.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan, City Centre Area Plan and is generally in compliance with the “Light Industrial, Office and Hotel (ZI10) – Bridgeport Village (City Centre)” zone.

Advisory Design Panel Comments

Given the subject Development Permit application involves relatively minor changes to the approved DP 11-594571, the application was not referred again to the Advisory Design Panel. This existing Development Permit was previously reviewed and supported by the Advisory Design Panel. A copy of the relevant excerpt from the Advisory Design Panel Minutes from December 8, 2011 is attached for reference (**Attachment 4**). The design response provided by

the applicant in 2011 is included immediately following the specific Design Panel comments and is identified in '***bold italics***'.

In addition to the changes included the applicant's ADP response in 2011, the current proposed Development Permit addresses the following elements previously noted by the ADP:

- More details on the location of the exterior building lighting have been included with the proposed new Development Permit plans. Also, there is a Building Permit issuance condition requiring a report from the applicant's lighting engineer confirming that that the exterior lighting system equipment, design and brightness will not cause excessive light spill onto the Oak Street bridge, the surrounding area and into the atmosphere. There will be a covenant registered on title ensuring that the exterior lighting remains as originally designed and that provides the City with the ability request the owner dim the lighting if needed.
- The canopies over both porte cochere entrances have been reduced in depth and have been revised to be constructed of a combination of glass and more refined structural members.
- The main floor podium roofs, visible from the hotel towers above podiums, include extensive green roof plantings (e.g. grasses and mosses) and several clusters of small shrubs.
- The parking area adjacent to the tree retention area at the northeast corner of the Hotel 1 site has been reduced in size and re-configured to enhance and enlarge the tree retention area.
- The revised landscape plans include additional trees within the building setbacks to Bridgeport Road and adjacent to the Oak Street Bridge.

Analysis

Conditions of Adjacency

- The Hotel 1 and Hotel 2 building podiums are set relatively close respectively at 2.5 m (8.2 ft.) and 3.0 (9.8 ft.) to Bridgeport Road to create an animated, urban environment. The hotel towers are further setback on the podiums to reduce shadowing and provide a more comfortable pedestrian environment. The hotel setbacks also include deciduous trees to provide an attractive soft streetscape along Bridgeport Road.
- The exterior sides of the sites facing the Airport Gateway Plaza to the east and Oak Street Bridge to the west include well-planted landscape strips with deciduous trees to provide a visual buffer and a sense of natural enclosure of the hotel surface parking lots. These well landscaped buffers also wrap around the southeast and southwest corners of the site to visually screen the parking lots facing Bridgeport Road.
- The north side of the Hotel 1 site has a tree retention area that includes preservation of 10 large coniferous and deciduous trees that provides an attractive buffer to the property at 9520 Beckwith Road on which further tree retention area is proposed as part of the rezoning application under staff review for that site (as noted above).
- The north side of the Hotel 2 site includes a driveway to the hotel's rear surface parking lot and also provides access to the parkade on the adjacent Business Centre at 9466 Beckwith Road. This driveway includes a sidewalk on its north side leading to the parkade entrance on the Business Centre building.

Urban Design and Site Planning

Site Planning:

- In the existing Development Permit, the two hotel towers have been sited to create a logical circulation pattern along the north-south road that extends northward adjacent to the Business Centre building and Beckwith Road.
- The hotel podiums provide continuity of the streetwall at-grade while the slender hotel towers permit slot views across the site along the north-south road the Business Centre building and further to the north.
- The at-grade common area and dining areas in the hotel podiums will help animate and enhance the pedestrian environment along Bridgeport Road and the north-south road.
- Vehicle access continues to be provided by the proposed north-south road that straddles the Hotel 1 and Hotel 2 sites and that connects Bridgeport Road to Beckwith Road. This private road was secured for public access via registration of a Statutory Right of Way on title prior to the approval of the rezoning for the subject hotel sites and the adjacent 11-storey Business Centre site (see **Attachment 3**).
- The two hotel towers continue to have their main front pedestrian and vehicle drop-offs located within the porte cochères directly adjacent to the north-south road.
- The majority of parking and loading areas for each hotel continue to be located in surface parking lots to the functional rear sides of the hotels, adjacent to the Airport Gateway Plaza (east of Hotel 1) and the Oak Street Bridge (west of Hotel 2). The parking spaces at the north-east corner of the Hotel 1 site have been re-configured to enhance the tree protection area previously established with the approved Development Permit.
- The remainder of both hotels' parking continues to be located within the large parkade structure within the Business Centre at 9466 Beckwith Road. As noted above, this building was included in the same Rezoning and Development Permit applications approved for the hotel sites. The Hotel 2 site driveway to its rear surface parking lot also provides access to the Business Centre parkade on the adjacent site. Further discussion of parking is provided below in this report.
- In summary, the proposed revisions to the hotel buildings do not alter the overall site design which has satisfactorily considered conditions of adjacency.

Urban Design:

- Both proposed hotel podiums extend toward the street and contain a restaurant and a guest dining facility fronting Bridgeport Road with streetfront glazing. There are minor changes to the fenestration of both hotel podiums that are the result of changes to the interior layout and programming of the hotels.
- The podiums continue to provide continuity of the streetwall at-grade facing Bridgeport Road and the north-south road through the site.
- The overall designs of the two towers remain essentially the same. The towers are located on top of relatively small podiums and provide a comfortable transition to Bridgeport Road and

the north-south road. The improved tree retention area north of Hotel 1 will also provide a natural, green buffer to proposed office/light industrial building at 9520 Beckwith Road.

Parking

- The total number of parking spaces has increased to 187 spaces from 174 spaces in the existing Development Permit. Specifically, the parking is proposed to be changed as follows:
 - The revised Hotel 1 is provided with 67 parking spaces, a decrease from the 75 spaces in the original proposal. The number of surface parking spaces on the Hotel 1 site has been increased from 49 to 56 spaces in the revised proposal.
 - The revised Hotel 2 is provided with 120 parking spaces, an increase from the 99 in the original proposal. The number of surface parking spaces on the Hotel 2 site has been decreased from 62 to 61 spaces in the revised proposal.
 - The remaining 70 parking spaces for Hotel 1 and Hotel 2 are provided within the parkade within the adjacent Business Centre at 9466 Beckwith Road; this results in the number of hotel parking spaces in the parkade being increased from 63 to 70 spaces.
- To accommodate these additional seven parking spaces in the Business Centre, the existing registered restrictive covenant and easement (LTSA No. CA6265843) will need to be amended as condition of issuance of the Development Permit. With these changes, the two hotels and Business Centre will continue to be in accordance with the requirements of the Zoning Bylaw.
- There are two accessible parking spaces for Hotel 1 and three accessible parking spaces for Hotel 1 as required under the Zoning Bylaw.
- The hotels are provided with a total of 10 Class 1 (employee/occupant) bicycle parking spaces and the required 10 Class 2 (visitor) bicycle parking spaces.
- Each hotel is provided with two medium (SU9) loading spaces as required under the Zoning Bylaw.

Architectural Form and Character

There continues to be overall building design differences that visually distinguish the two hotels from each other, including variation in height, façade composition and roof treatment as discussed below.

Design of Hotel 1 (9533 Bridgeport Road):

- There has been an increase in height of Hotel 1 from nine to 10 storeys. There also has been a minimal increase to the tower floor plate size. The main floor area has been decreased from 1,211 m² (13,035 ft²) to 1,166 m² (12,514ft²).
- While the overall design remains similar, the revised design includes more façade articulation, increased glazing and a reduction in the use of metal panels.
- While the colour and material palette remains generally the same, there has been a subtle change in the mix of materials and colours within facades for Hotel 1 (see comparison of elevations in Plans A1013 and A1014 in the attached Reference Plans).

Design of Hotel 2 (9455 Bridgeport Road):

- The height of Hotel 2 has been maintained at 12 storeys.
- There has been an increase in tower floor plate size from 791 m² (8,510 ft²) to 865 m² (9,301 ft²) and increase in the main floor area from 1,400 m² (15,065 ft²) to 1,471 m² (15,835 ft²).
- While the overall design remains similar, the revised design includes increased glazing and a reduction in the use of metal panels.
- There have also been changes to the façade articulation related to the larger floor plates and changes in interior layout (see comparison of elevations in Plans A1015 and A1016 in the attached Reference Plans).

Design Elements Common to Both Hotels:

- Each hotel podium continues to include large sections of window wall facing onto Bridgeport Road and facing the north-south private road. Substantial stone panels surround each of the building's main entrances. The remainder of the building podiums facing the private road and Bridgeport Road continue to be composed of metal panels.
- The cladding on the functional rear elevations of the hotel podiums on east side of Hotel 1 and west side of Hotel 2 has been changed from painted concrete to include fluted metal panels. A larger architectural metal canopy has also been added to the east elevation of the Hotel 1 podium.
- The main building roofs and the porte cochere roofs for both hotel buildings are comprised of the similar aerodynamic shapes and are clad in metal panels as in the original Development Permit.
- The depth of the porte cochere canopies has been reduced and their design refined to include finer structural members with glass.
- While Hotel 1 continues to include sections of light brown and orange glass, there are also common light blue and green glass colours and white metal panels in Hotels 1 and 2 that unify the design of both buildings.
- There have been refinements to the design of the lighted glass panels and metal louvers added to the concrete building cores on the centre of the east façade of Hotel 1 (facing the adjacent Airport Gateway Plaza) and the centre of the west façade of Hotel 2 (facing the Oak Street Bridge).
- The smaller backlit glass panels and strips at south ends of each hotel facing Bridgeport Road have also been refined to provide subtle differentiation between the buildings.
- The applicant's lighting consultant has advised that the above-noted building lighting includes translucent covers and dimming capabilities to reduce glare and any unnecessary brightness that may affect the neighboring area and the Oak Street Bridge. This lighting will automatically shut off at pre-set times between midnight or business closing, whichever is later, and 6am or business opening.

- There is a Building Permit issuance condition requiring that detailed exterior lighting specifications and dimmers be provided within the building permit plans for each of the hotels based on the recommendations of a lighting engineer to reduce light spill onto the Oak Street Bridge, the adjacent properties and the broader area.
- In this regard, there will be registration of a restrictive covenant that will require the continued operation of both hotel building's exterior lighting and light dimmers in accordance with lighting engineering report and plans required by the City prior to Building Permit issuance. The covenant would also allow the City to request the owners and operators of the hotels to adjust the lighting timing and lighting levels to address concerns regarding light spill onto Highway 99, the surrounding area and the broader atmosphere.

Tree Retention

- Existing Trees: There are 10 trees adjacent to the south property line of the adjacent site at 9520 Beckwith Road that will be protected in comparison to the nine trees to be retained in the current approved Development Permit.
- The 12 adjacent trees at 9520 Beckwith Road are proposed to be protected as part of the proposed development for this site now under staff review (RZ 18-821103).
- Together these on-site and off-site trees form a viable grove of 22 trees with 11 Douglas Fir, four Western Red Cedar, two Western Hemlock, two Hazel, two Lawson Cypress and one Mountain Ash.
- The proposed landscape plan accommodates 80 replacement trees, the same number as included within the portion of the existing approved Development Permit for the Hotel 1 and Hotel 2 sites.

Landscape Design and Open Space Design

Public Realm Design:

- Behind the proposed 2.0 m (6.6 ft.) off-site sidewalk within the Bridgeport Road allowance, the setbacks of the Hotels 1 and 2 respectively include 2.5 m (8.0 ft.) to 3.0 m (9.8 ft.) wide intensively planted landscape beds. The revised Development Permit also includes the addition of 11 Red Maples adjacent within the hotel setbacks to the Bridgeport Road frontage.
- There also continue to be planting areas framing both hotel driveway entrances to the north-south private road with ornamental Cherry and Black Locust trees.

Site Landscape Design:

- There continue to be sweeping semi-circular driveways leading from the north-south private street to each of the hotel's covered porte cochere entrances.
- The open-air dining areas, located between each of the hotel porte cochères and Bridgeport Road, have been expanded in size to provide more seating and a more animated streetscape.
- There continues to be a 2.0 m (6.6 ft.) wide shrub and Aspen tree buffer strip along the east property line of the Hotel 1 next to the Airport Gateway Plaza. The previously proposed

small overhead trellises with vine plantings over the parking spaces will be removed to reduce maintenance to improve visibility of the surface parking stalls for CPTED reasons. The existing wood fence on the adjacent property will remain in place.

- There continues to be a 1.5 m (5.0 ft.) wide shrub and Aspen tree buffer strip along west property line of the Hotel 2 site facing the Oak Street Bridge. A 1.8 m (6.0 ft.) solid wood fence has been added to provide an additional physical and visual buffer to the area under the Oak Street Bridge.
- The use of permeable pavers has been expanded to cover all of the parking spaces and bus laybys within both hotel surface parking lots.
- The north-south road continues to be paved with sandblasted concrete, but with a finer grain saw-cut pattern than in the approved Development Permit. There continues to be 1.5 m (5.0 ft.) wide sidewalks along both sides of this road.
- Full cut-off luminaires with efficient light sources and direct light downward where needed, will be used in the pedestrian pathways, sidewalks and landscaped areas.
- Comprehensive irrigation plans (L-06A to L-06F) have been added to the landscape plans.
- The applicant will be required to provide an updated landscaping security in the amount of \$284,990 for Hotel 1 and \$289,323 for Hotel 2 prior to Development Permit issuance.

Sustainability

The proposed development includes the following sustainable features:

- The proposed buildings will continue to meet minimum LEED Silver (version 4) equivalent standards including the use of effective water and waste management systems, passive solar shading strategies and low flow fixtures. See **Attachment 5** for the LEED checklist.
- There will be a legal agreement registered on title as a condition of the Building Permit issuance that ensures that the buildings are designed to be connected to a District Energy Utility (DEU) when available and provide the necessary access to the buildings by Lulu Island Energy Company (LIEC). The developer will also be required to provide mechanical drawings and energy modeling, which shall be reviewed by the City and LIEC for compliance with DEU requirements (i.e. capable of connecting to a future DEU system) prior to Building Permit issuance.

Accessibility

The proposed development also includes the accessible design elements as listed below:

- There are three wheelchair accessible suites in Hotel 1 and five such suites in Hotel 2.
- Both hotels include porte-cochere areas at the front lobbies to provide weather protection for the drop-off and pick-up of mobility-impaired clients.

Crime Prevention Through Environmental Design

- The substantial area of windows at the hotel lobbies and ground-level amenity spaces provide clear views onto the exterior pedestrian areas associated with the building and Bridgeport Road. Pedestrian-scale bollard lighting is provided on the sites.

- Hotel visitor and staff activity will provide more ‘eyes on the street’ contributing to site safety and security.

Aircraft Noise Policy & City Noise Bylaw

- As a condition of the previous rezoning of the site, an aircraft noise covenant was registered on title as provided by the OCP Aircraft Noise Policy. This covenant indemnifies the City with regards to aircraft noise and does not require completion of an acoustical report for hotel uses.
- The applicant’s acoustic engineers have provided a letter advising that the proposed hotel designs can satisfy the requirements of the City’s Noise Bylaw 8856. In this regard, there is a Building Permit issuance condition that requires that the developer provide an acoustic engineer’s report addressing the City’s Noise Bylaw 8856, and that any recommended detailed design elements to be included within the Building Permit plans to the satisfaction of the City.

Duration of Stay

The applicant is proposing that Hotel 1 include cooking facilities in each of the rooms to facilitate longer stays. Under the original rezoning application, a covenant that prohibits stratification and sale of rooms to individuals was secured. To ensure that the hotel operates as a hotel, the applicant has agreed to register a covenant on the Hotel 1 site to limit guest stays within a calendar year as follows:

- 85% of the guest stays would not exceed 30 nights,
- 12% of the guest stays would not exceed 100 nights;
- 2% of the guest stays would not exceed 180 nights; and
- 1% of guest stays would not exceed 270 nights.

Conclusions

As the proposed development would meet applicable policies and Development Permit Guidelines, staff recommends that the Development Permit be endorsed, and issuance by Council be recommended.



Mark McMullen

Senior Coordinator - Major Projects

MM:rg

Attachment 1: Development Application Data Sheet

Attachment 2: Location Map

Attachment 3: Overall Development Site Plan

Attachment 4: Excerpt from the Advisory Design Panel Minutes

Attachment 5: LEED Scorecard

The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter-of-Credit for landscaping in the amount of \$284,990 for 9533 Bridgeport Road / Lot 1 (Hotel 1) and \$289,323 for 9455 Bridgeport Road (Hotel 2).
- Amendment the existing restrictive covenant and easement (LTSA No. CA6265843) to change the total number of hotel parking spaces secured on Business Centre (Lot 3) at 9466 Beckwith Road from 63 to 70 spaces and as follows:
 1. The minimum number of parking spaces secured for Lot 1 (Hotel 1) be decreased from 26 to 11 spaces; and
 2. The minimum number of parking spaces secured for Lot 3 (Hotel 2) be increased from 37 to 59 spaces.
- Registration of a restrictive covenant or other legal agreements or measures, as determined to the satisfaction of the Director of Development, to limit the length of guest stay in Hotel 1 at 9533 Bridgeport Road to provide that:
 1. 85% of the guest stays at the hotel would be for not more than 30 continuous or total nights in a given calendar year;
 2. No greater than 12% of the guest stays at the hotel may be up to a maximum of 100 continuous nights or total nights in a given calendar year;
 3. No greater than 2% of total guest stays may be for up to 180 continuous nights or total nights in a given calendar year;
 4. No greater than 1% of total guest stays may be for up to 270 continuous nights or total nights in a given calendar year; and
 5. The operations of the hotel will not fall under the *British Columbia Residential Tenancy Act* (travel accommodations are specifically excluded from the Act under Section 4) but rather the *British Columbia Hotel Keepers Act*.
- Registration of a restrictive covenant or other legal agreements or measures, as determined to the satisfaction of the Director of Development, that will:
 1. Require the continued operation of both hotel building's exterior lighting and light dimmers in accordance with lighting engineering report and detailed plans required by the City prior to Building Permit issuance; and
 2. Permit the City to request the owners and operators of the hotels to adjust the lighting timing and lighting levels to address concerns regarding light spill onto Highway 99 and into the atmosphere effecting aircraft navigation.

Prior to future Building Permit issuance, the developer is required to complete the following:

- Registration of a restrictive covenant and/or alternative legal agreement(s), to the satisfaction of the City and the Lulu Island Energy Company (LIEC), securing the owner's commitment to connect to District Energy Utility (DEU), which covenant and/or legal agreement(s) will include, at minimum, the following terms and conditions:
 1. No building permit will be issued for a building on the subject site unless the building is designed with the capability to connect to and be serviced by a DEU and the owner has provided an energy modeling report satisfactory to the Director of Engineering;
 2. If a DEU is available for connection, no final building inspection permitting occupancy of a building will be granted until:
 - a. the Owner has applied for and connected to the DEU for the purposes of providing internal space heating and domestic hot water heating to the Building, in accordance with the Service Provider Agreement;
 - b. the owner enters into a Service Provider Agreement with the City and/or the City's DEU service provider, Lulu Island Energy Company, on terms and conditions satisfactory to the City; and
 - c. the owner grants or acquires the Statutory Right-of-Way(s) and/or easements necessary for the installation, operation and maintenance on each intervening property of all necessary facilities for supplying the Services to the Lands or subdivided portion thereof, by the City and/or the City's DEU service provider, Lulu Island Energy Company.
 3. If a DEU is not available for connection, no final building inspection permitting occupancy of a building will be granted until:

- a. the City receives a professional engineer's certificate stating that the building has the capability to connect to and be serviced by a DEU;
 - b. the owner enters into a covenant and/or other legal agreement to require that the building connect to a DEU when a DEU is in operation;
 - c. the owner grants or acquires the Statutory Right-of-Way(s) and/or easements necessary for supplying DEU services to the building; and
 - d. the owner provides to the City a letter of credit, in an amount satisfactory to the City, for costs associated with acquiring any further Statutory Right of Way(s) and/or easement(s) and preparing and registering legal agreements and other documents required to facilitate the building connecting to a DEU when it is in operation.
4. The Owner shall, on or before the Connection Deadline, apply for, connect to and utilize the DEU for the purposes of providing internal space heating and cooling and domestic hot water heating to all Buildings on the Lands and enter into a Service Provide Agreement to the satisfaction of the City;
 5. No Building(s) shall be constructed, used or maintained on the Lands except in full compliance with the Agreement, the City's bylaws, regulations and policies, including the Bylaw, the applicable Service Provider Agreement(s) and the Approved Heating and Cooling System Plans for the Building(s).
- The developer shall also provide mechanical drawings and energy modeling, which shall be reviewed to satisfaction of LIEC and the City for compliance with District Energy System requirements (i.e. capable of connecting to future DEU system).
 - The developer shall provide a report from an acoustic engineer addressing the City's Noise Bylaw 8856 and include any necessary design elements within the Building Permit plans to the satisfaction of the City.
 - The developer shall provide detailed exterior lighting specifications and light dimmers within the building permit plans for each of the hotels based on a report from the applicant's lighting engineer that confirms that that the exterior lighting system equipment, design and brightness will not cause excessive light spill onto Highway 99, the surrounding area and the broader atmosphere.
 - The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Department at 604-276-4285.*
 - Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Department (<http://www.richmond.ca/services/ttp/special.htm>).



City of Richmond

Development Application Data Sheet

Development Applications Department

DP 18-825006

Attachment 1

Address: 9455 and 9533 Bridgeport Road

Beckwith One BT Holdings Ltd.,
Bridgeport East BT Holdings Ltd. &
Bridgeport West BT Holdings Ltd.

Applicant: IBI Group Architects (Canada) Inc.

Owner: Bridgeport West BT Holdings Ltd.

Planning Area(s): City Centre

8,545 m² for Hotel 1 &

Floor Area Gross: 11,376 m² for Hotel 2

7,696 m² for Hotel 1 &

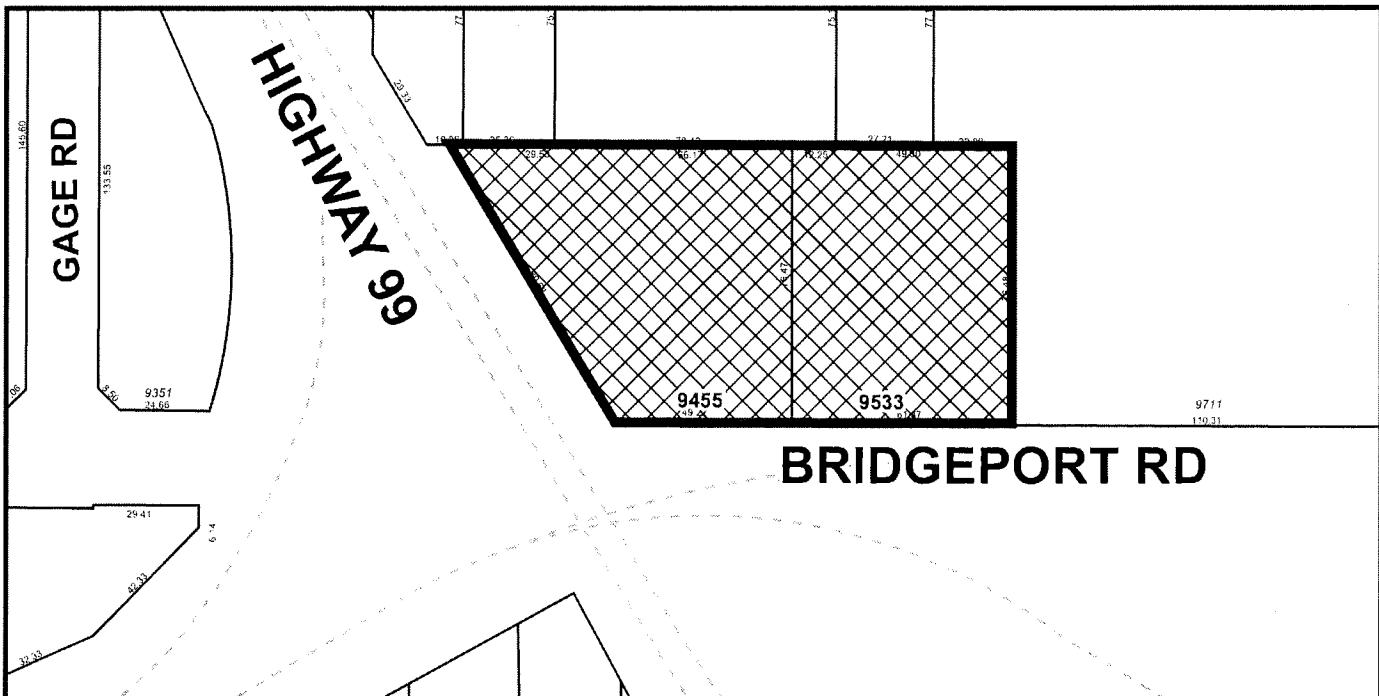
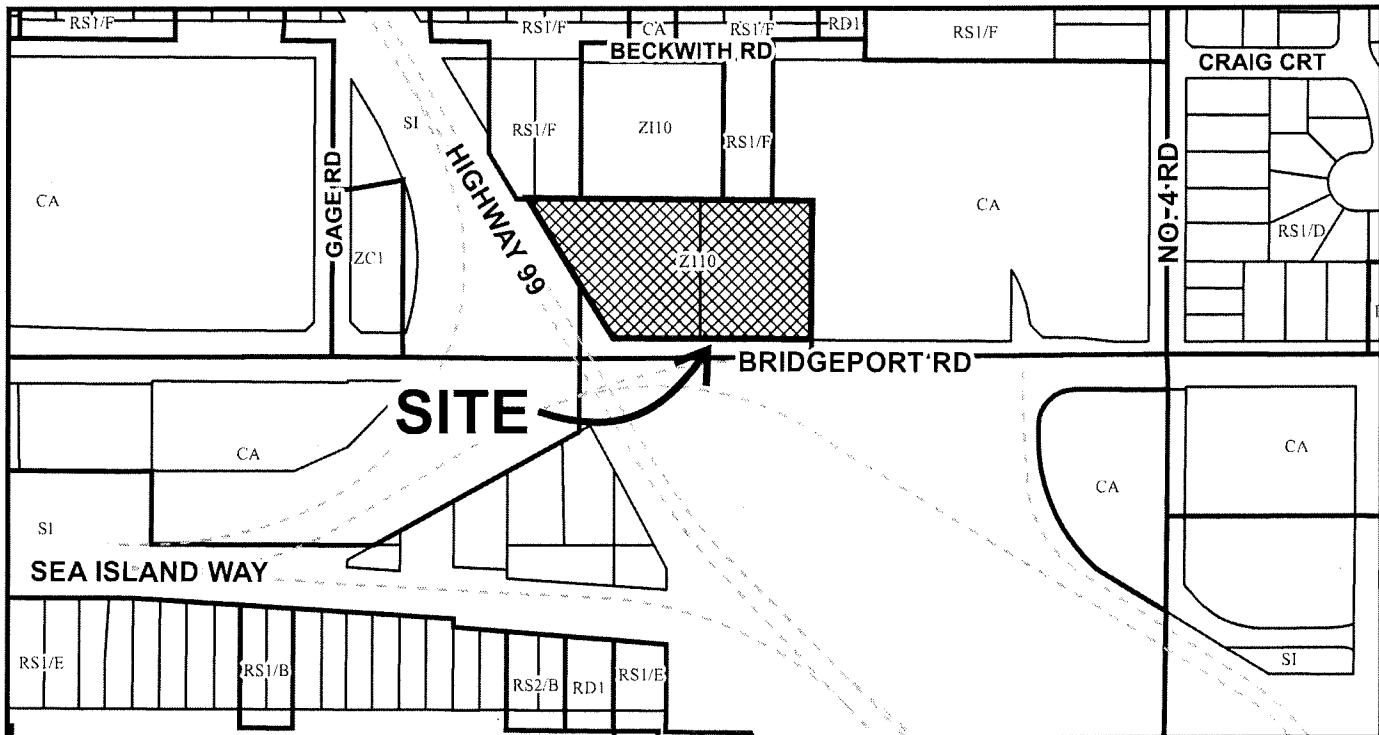
Floor Area Net: 10,193 m² for Hotel 2

| | Existing | Proposed | |
|----------------------------------|--|--|-----------------|
| Site Area: | 4,700 m ² for Hotel 1 site & 5,544 m ² for Hotel 2 site | 4,700 m ² for Hotel 1 site & 5,544 m ² for Hotel 2 site | |
| Land Uses: | Vacant | Two Hotels | |
| OCP Designation: | MEMP – Mixed Employment | MEMP – Mixed Employment | |
| Area Plan Designation: | General Urban T4 (25m) – Area B | General Urban T4 (25m) – Area B | |
| Zoning: | “Light Industrial, Office and Hotel (ZI10) – Bridgeport Village (City Centre)” | “Light Industrial, Office and Hotel (ZI10) – Bridgeport Village (City Centre)” | |
| Number of Units: | None | 112 rooms in Hotel 1 & 200 rooms in Hotel 2 | |
| | Bylaw Requirement | Proposed | Variance |
| Floor Area Ratio: | 1.85 FAR (Shared Between 3 Lots Covered by Zone) | 1.85 FAR (Shared Between 3 Lots Covered by Zone) | none permitted |
| Lot Coverage: | Max. 50% | 25% for Hotel 1 28% for Hotel 2 | none |
| Setback – Front Yard | Min. 2.5 m | 2.50 m 3.06 m | none |
| Setback – West Side (Hotel 1): | Min. 10.0 m for building Min. 8.0 m for canopy | > 10.0 m for building 8.0 m for canopy | none |
| Setback – East Side (Hotel 1): | Min. 10.0 m | 14.5 m | |
| Setback – Hwy 99/West (Hotel 2): | Min. 10.0 m | 10.0 m | none |
| Setback – East Side (Hotel 2): | Min. 10.0 m for building Min. 8.0 m for canopy | > 10.0 m for building 8.0 m for canopy | |
| Setback – Rear Yard: | Min. 9.0 m | 17.17 m for Hotel 1 14.08 m for Hotel 2 | none |
| Height (m): | Max. 47.0 m | 43.60 m for Hotel 1 46.41 m for Hotel 2 | none |

| | | | |
|---|--|---|------|
| Lot Size: | None | 4,700 m ² for Hotel 1 site & 5,544 m ² for Hotel 2 site | none |
| Off-street Parking Spaces – Regular/Commercial: | 67 spaces for Hotel 1 and 120 spaces for Hotel 2 | 67 for Hotel 1 and 120 for Hotel 2 | none |
| Off-street Parking Spaces – Accessible: | 2 spaces for Hotel 1 and 3 spaces for Hotel 2 | 2 spaces for Hotel 1 and 3 spaces for Hotel 2 | none |
| Total off-street Spaces: | N/A | N/A | none |
| Tandem Parking Spaces | not permitted | none | none |
| Amenity Space – Indoor: | Min. 70 m ² | N/A | none |
| Amenity Space – Outdoor: | N/A | N/A | none |



City of Richmond

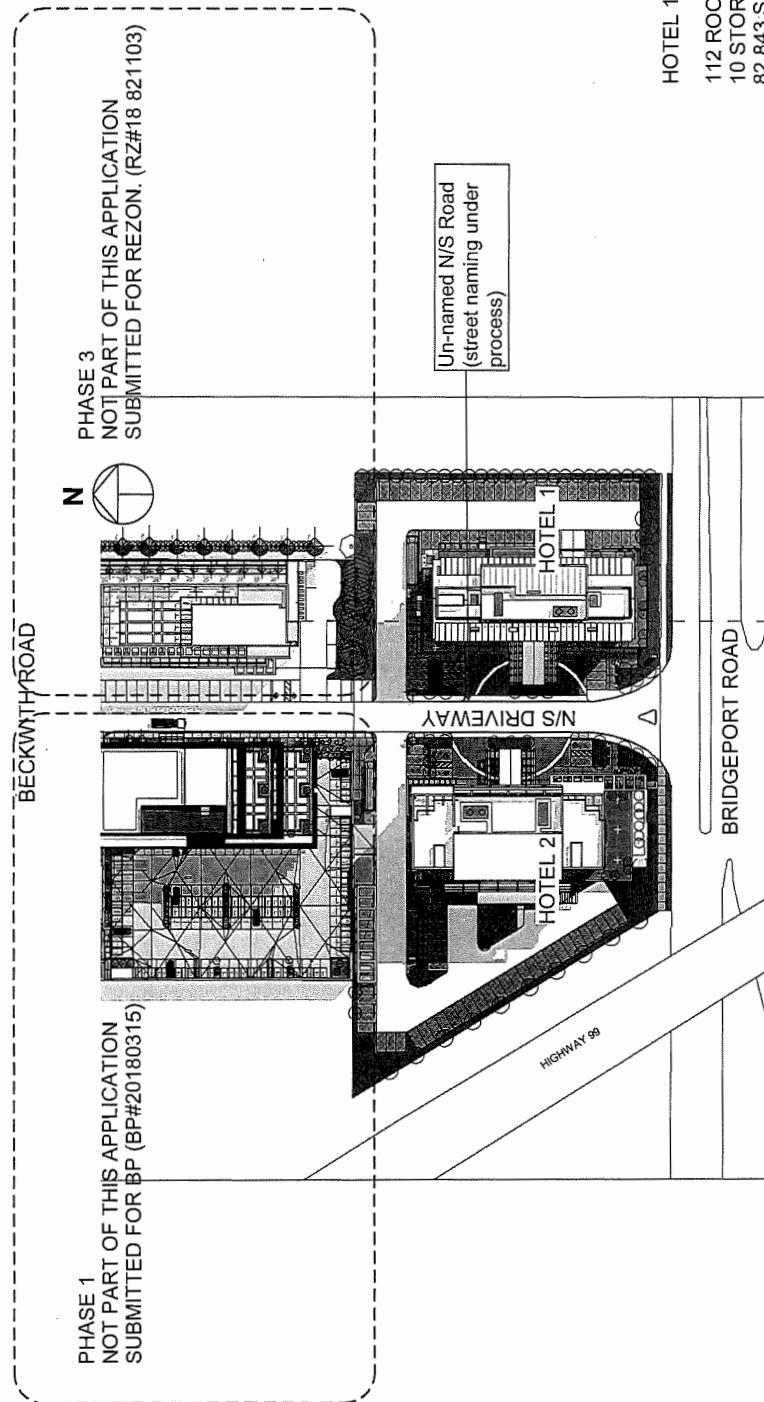


| | | |
|--|--------------|--|
| | DP 18-825006 | Original Date: 06/14/18 Revision Date: 06/19/18 Note: Dimensions are in METRES |
|--|--------------|--|

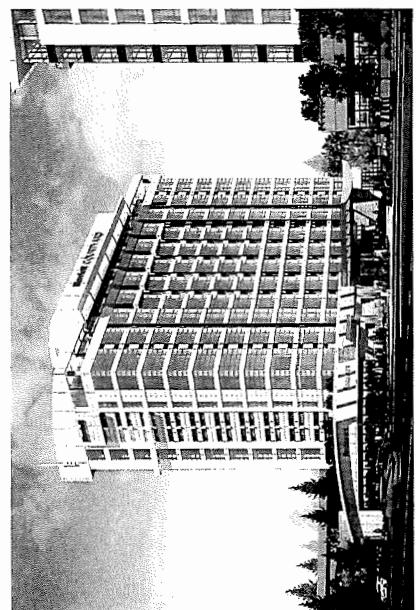
IBI

ATTACHMENT 3

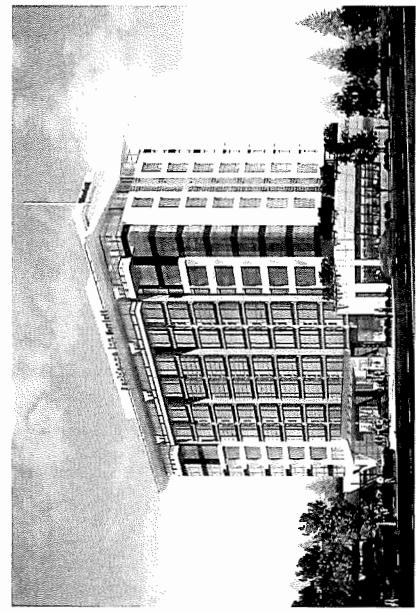
9455 & 9533 BRIDGEPORT ROAD



HOTEL 2 (PHASE 2)
200 ROOMS
12 STOREYS
109,721 SF



SITE PLAN
SITE: 176,128 SF
FAR: 321,285 SF



HOTEL 1 (PHASE 2)
112 ROOMS
10 STOREYS
82,843 SF

PHASE IMAGE

A 1001

NTS

**Excerpt from the Minutes from
The Advisory Design Panel Meeting**

**December 8, 2011 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall**

DP 11-594571 – HIGH-RISE COMMERCIAL DEVELOPMENT WITH TWO HOTEL TOWERS AND ONE BUSINESS CENTRE TOWER

ARCHITECT: IBI Group

PROPERTY LOCATION: 9451, 9491, 9511, 9531 and 9551 Bridgeport Road and 9440, 9460 and 9480 Beckwith Road

Panel Discussion

Comments from the Panel were as follows:

- like the project; scale is appropriate for the area; section between the hotel towers is proportionally correct;
- landscape design is simple, clean and effective;
- Hotel 1 is very different from the office building and Hotel 2; Hotel 1 is nicely articulated; roof form works well;
- Hotel 2 and office building are very different; provide architectural expression to Hotel 2 to achieve visual connectivity to the business building;
- *Similar lighting strategies to be utilized by both buildings, including the addition of a light feature on the middle vertical portion of the west face of the office building between the sloping glass walls. This additional light feature as well as the others will be dimmable and low intensity to conserve energy and prevent excessive light pollution. As seen from the bridge deck of Highway 99, this additional feature on the Business Centre will provide connectivity between the two buildings, as well as contribute to the Public Art component for all three buildings.*
- appreciate the efforts put by the applicant into the project since its initial presentation to the Panel;
- simplicity of the landscape treatment unifies the project nicely;
- treatment along the edges are appropriate; like the idea of the retention of the trees which provide a nice remnant of the site;
- consider reducing parking spaces in the area near the existing trees; expand the vegetative area to give more room for trees to survive;
- *We have already initiated an appropriate response to this item through preliminary discussion with Ken Bell, arborist consultant for the project, in which parking in this location may be reoriented to provide more clearance around the existing trees,*

and ensure their protection during the preloading and construction phases of the project.

- consider reducing the scale of canopies above the hotels' entrance to provide a sense of intimacy;
- *Scale of the canopies to be considered again during BP stage. Structure has been oversized and will most likely be constructed of smaller structural members, which will result in an overall lighter appearance once constructed.*
- consider increasing the quality and quantity of trees in the parking and patio areas and moving those row of trees to the outside edge of the stalls to act as bollards;
- *Landscape architect to continue to revise overall landscape design as per comments, including the provision of Swedish poplar and Amanogawa cherry trees (see following comment).*
- Swedish poplar trees in front of the office building (west side of the north-south internal road) may not have enough room to grow; consider smaller columnar trees, e.g. Amanogawa cherry to increase the quantity of and spacing between trees;
- appreciate the efforts of the applicant in addressing the concerns of a challenging project;
- like the roof deck landscape treatment on level 3 of the office building; ensure that this will be carried through in the project; provide irrigation on the roof parking;
- *Client is committed to providing roof deck landscape treatment as depicted on all levels. Top deck of parkade to have appropriate irrigation system to ensure vegetation at this level is not compromised.*
- consider repeating the Bridgeport Road entrance way treatment at the Beckwith Road entrance; Beckwith Road entrance is expected to be used frequently, particularly by motorists/pedestrians going to the office building;
- *Landscape architect to reconsider appropriate treatment of landscape at Beckwith Road entry to site, to be enhanced to similar condition as entry off of Bridgeport Road.*
- piece of north blank wall not a concern as it is a parkade wall;
- consider providing treatments, e.g. green roof treatments or patterned aggregates for canopies that don't have skylights to enhance their visual appeal when viewed from above;
- *Further consideration to be given to those lower levels of each building visible from higher floors, including roof deck treatments as well as the hotel entry canopies.*
- project is nicely done; has higher quality compared to the other buildings in the neighbourhood; will improve the neighbourhood; has opportunity to become a landmark in Richmond; appreciate the quality of the applicant's presentation to the Panel;

- the three buildings are distinctly different; consider providing a unifying feature to present them as part of one development, e.g. repeating lighting element in Hotel 1 into Hotel 2; will lighten up the neighbourhood;
- *As noted above, the addition of a vertical light feature on the Business Centre will provide connectivity between the three buildings.*
- consider improving the relationship between the two hotels which are vertically facing each other; consider angled mass for Hotel 2 to soften the space between the two hotels;
- *Connectivity between the two hotels has been intended through the use of similar light features/Public Art, as well as providing similar expression in their porte cochères.*
- appreciate the changes to the project made by the applicant in response to the previous comments of the Panel;
- excellent response to the west side of the office building; appreciate the angled curtain wall of the office building; a win-win situation; increases the quality of massing and architectural design of the building;
- appreciate the increase in the number of universal design suites in the two hotels; look forward to future increases if warranted to accommodate more seniors;
- appreciate the applicant for the quality of presentation materials and model;
- proposed development has the potential to become a landmark project in a key location in Richmond;
- look forward to hearing applicant's response to Panel's comments regarding public art;
- carry the lighting in Hotel 1 into Hotel 2; will have a strong impact on the west side of Hotel 2 which is facing Oak Street Bridge; and
- *See comments above.*
- multi-phase projects take time to evolve; applicant is requested to provide visual details on how the rest of the site will look like in the interim.
- *This has been provided as part of the current Landscape package, with further revision to be provided as per comments at the BP stage.*

Panel Decision

It was moved and seconded

That DP 11-594571 move forward to the Development Permit Panel subject to the applicant addressing the items discussed by the Panel, including the items highlighted below:

1. consider visual connectivity between Hotel 1 and Hotel 2; and

As noted above, further consideration is to be given to the light features of all three buildings as work commences with the Public Art consultant, in order to provide visual

connectivity as well as reinforce the landmark/gateway intent of the project as the site is built out.

2. consider looking at the quality and quantity of trees both in terms of species, placement and spacing.

As noted above, quality and quantity of trees and other surface vegetation to be revisited by both arborist and landscape architect for appropriateness and overall desired effect.

CARRIED

9455 & 9533 BRIDGEPORT ROAD

IBI

LEED v4 for BD+C: New Construction and Major Renovation
Project Checklist



Y ? N
1 Credit
Integrative Process

| 13 1 0 Location and Transportation | | | 16 |
|--|----------|----------|----------|
| LEED for Neighborhood Development Location | 0 Credit | Presq | Required |
| Sensitive Land Protection | 1 Credit | Y | Required |
| High Priority Site | 0 Credit | Y | 5 |
| Surrounding Density and Diverse Uses | 5 Credit | 2 Credit | |
| Access to Quality Transit | 5 Credit | 0 Credit | |
| Bicycle Facilities | 1 Credit | 2 Credit | 2 |
| Reduced Parking Footprint | 1 Credit | 1 Credit | 2 |
| Green Vehicles | 1 Credit | 1 Credit | 2 - |

| 13 1 0 Materials and Resources | | | 13 |
|---|----------|----------|----------|
| Storage and Collection of Recyclables | 0 Credit | Presq | Required |
| Construction and Demolition Waste Management Planning | 1 Credit | Y | Required |
| Building Life-Cycle Impact Reduction | 2 Credit | 0 Credit | |
| Building Product Disclosure and Optimization - Environmental Product Declarations | 5 Credit | 2 Credit | |
| Building Product Disclosure and Optimization - Sourcing of Raw Materials | 5 Credit | 1 Credit | 2 |
| Building Product Disclosure and Optimization - Material Ingredients | 5 Credit | 1 Credit | 2 |
| Construction and Demolition Waste Management | 1 Credit | 1 Credit | 2 - |

| 16 1 0 Indoor Environmental Quality | | | 16 |
|---|----------|----------|----------|
| Minimum Indoor Air Quality Performance | 0 Credit | Presq | Required |
| Environmental Tobacco Smoke Control | Y Credit | 2 Credit | Required |
| Enhanced Indoor Air Quality Strategies | 1 Credit | 1 Credit | 2 |
| Low-Emitting Materials | 1 Credit | 1 Credit | 3 |
| Construction Indoor Air Quality Management Plan | 1 Credit | 1 Credit | 1 |
| Indoor Air Quality Assessment | 3 Credit | 1 Credit | 2 |
| Indoor Air Quality Assessment | 2 Credit | 1 Credit | 2 |
| Thermal Comfort | 1 Credit | 1 Credit | 1 |
| Interior Lighting | 1 Credit | 1 Credit | 2 |
| Daylight | 1 Credit | 1 Credit | 2 |
| Quality Views | 1 Credit | 1 Credit | 3 |
| Acoustic Performance | 1 Credit | 1 Credit | 1 |

| 16 1 0 Innovation | | | 6 |
|------------------------------|----------|----------|----------|
| Innovation | 0 Credit | 1 Credit | Required |
| Innovation | 5 Credit | 5 Credit | Required |
| LEED Accredited Professional | 1 Credit | 1 Credit | 1 |

| 16 1 0 Regional Priority | | | 4 |
|------------------------------------|----------|----------|---|
| Regional Priority: Specific Credit | 0 Credit | 0 Credit | 1 |
| Regional Priority: Specific Credit | 0 Credit | 0 Credit | 1 |
| Regional Priority: Specific Credit | 0 Credit | 0 Credit | 1 |
| Regional Priority: Specific Credit | 0 Credit | 0 Credit | 1 |

| 16 0 0 Energy and Atmosphere | | | 33 |
|--|----------|-------|----------|
| Fundamental Commissioning and Verification | 0 Credit | Presq | Required |
| Minimum Energy Performance | 6 Credit | Presq | Required |
| Building-Level Energy Metering | 8 Credit | Presq | Required |
| Enhanced Commissioning | 1 Credit | Presq | Required |
| Optimize Energy Performance | 0 Credit | Presq | Required |
| Advanced Energy Metering | 1 Credit | Presq | Required |
| Demand Response | 2 Credit | Presq | Required |
| Renewable Energy Production | 3 Credit | Presq | Required |
| Enhanced Refrigerant Management | 1 Credit | Presq | Required |
| Green Power and Carbon Offsets | 0 Credit | Presq | Required |

57 | 18 | 0 TOTALS
Certified: 40 to 49 points, Silver: 50 to 59 points, Gold: 60 to 79 points, Platinum: 80 to 110
Possible Points: 110



City of Richmond

Development Permit

No. DP 18-825006

To the Holder: IBI Group Architects (Canada) Inc.

Property Address: 9455 and 9533 Bridgeport Road

Address: 700 - 1285 West Pender Street
Vancouver, BC V6E 4B1

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #DP18-825006-1 to #DP18-825006-63 attached hereto.
4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$284,990 for 9533 Bridgeport Road (Hotel 1) and \$289,323 for 9455 Bridgeport Road (Hotel 2) to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit
No. DP DP18-825006

To the Holder: IBI Group Architects (Canada) Inc.

Property Address: 9455 and 9533 Bridgeport Road

Address: 700 - 1285 West Pender Street
Vancouver, BC V6E 4B1

7. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

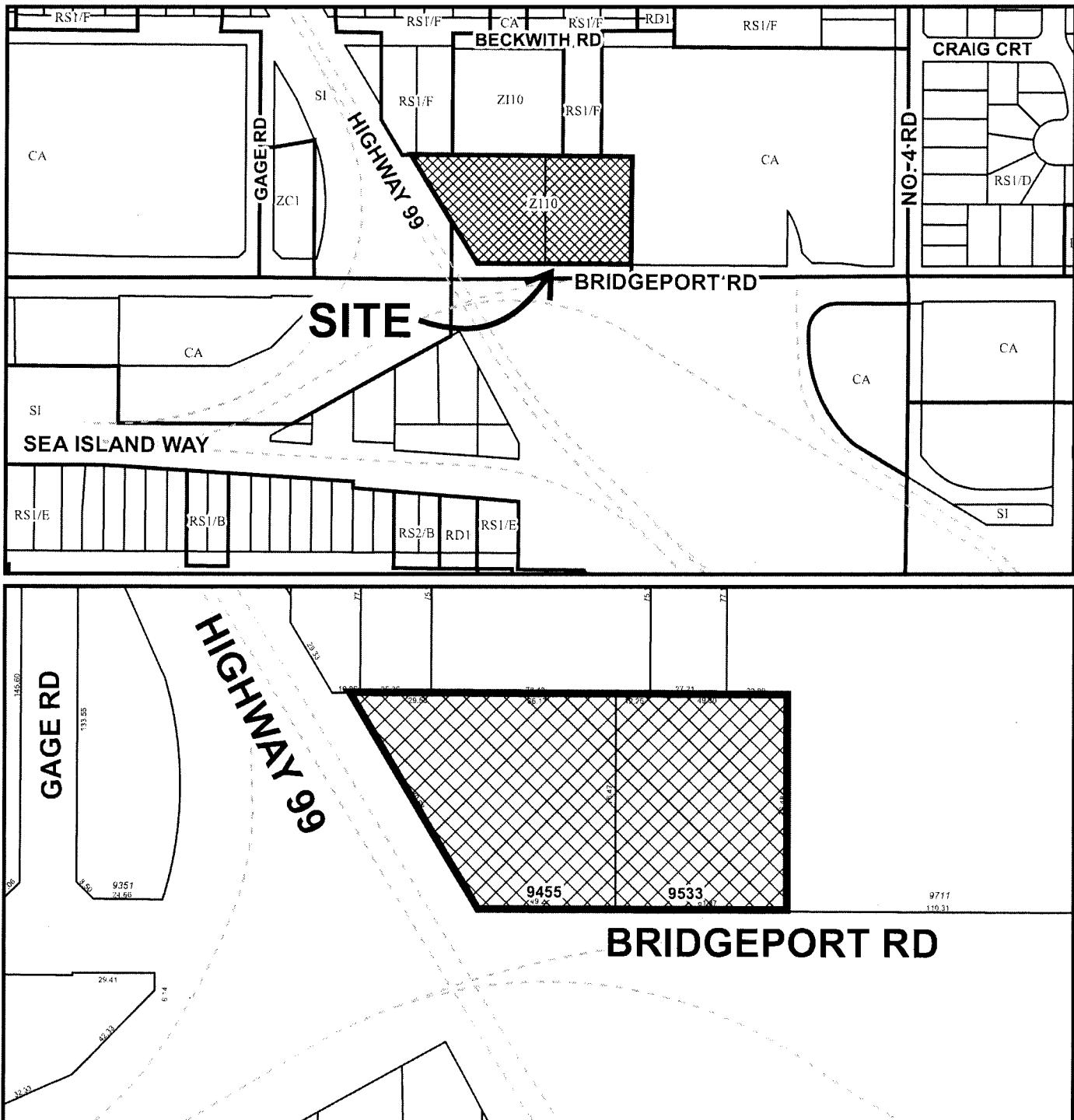
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond



DP 18-825006
SCHEDULE "A"

Original Date: 06/14/18

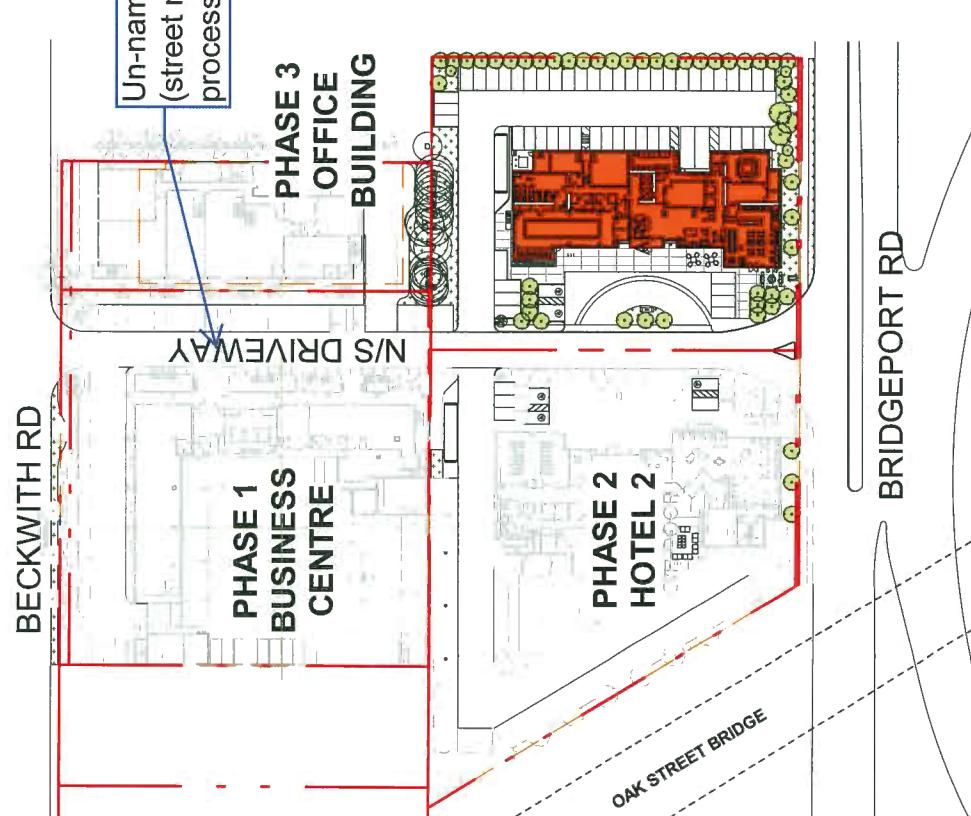
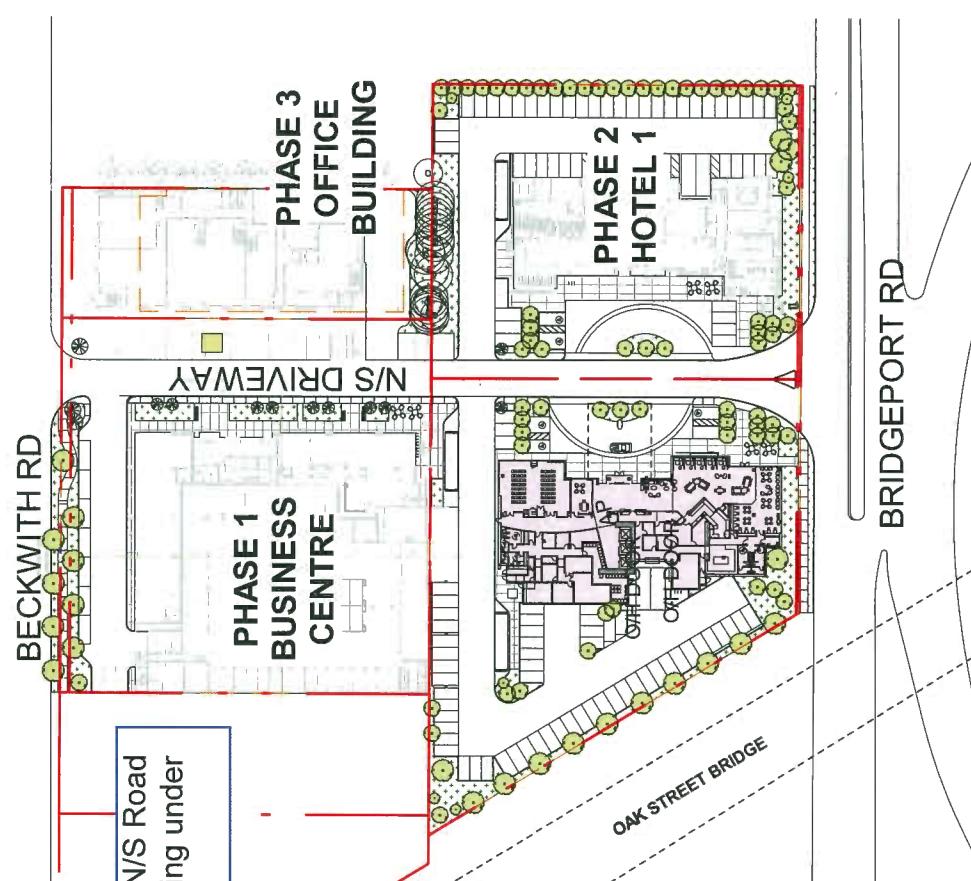
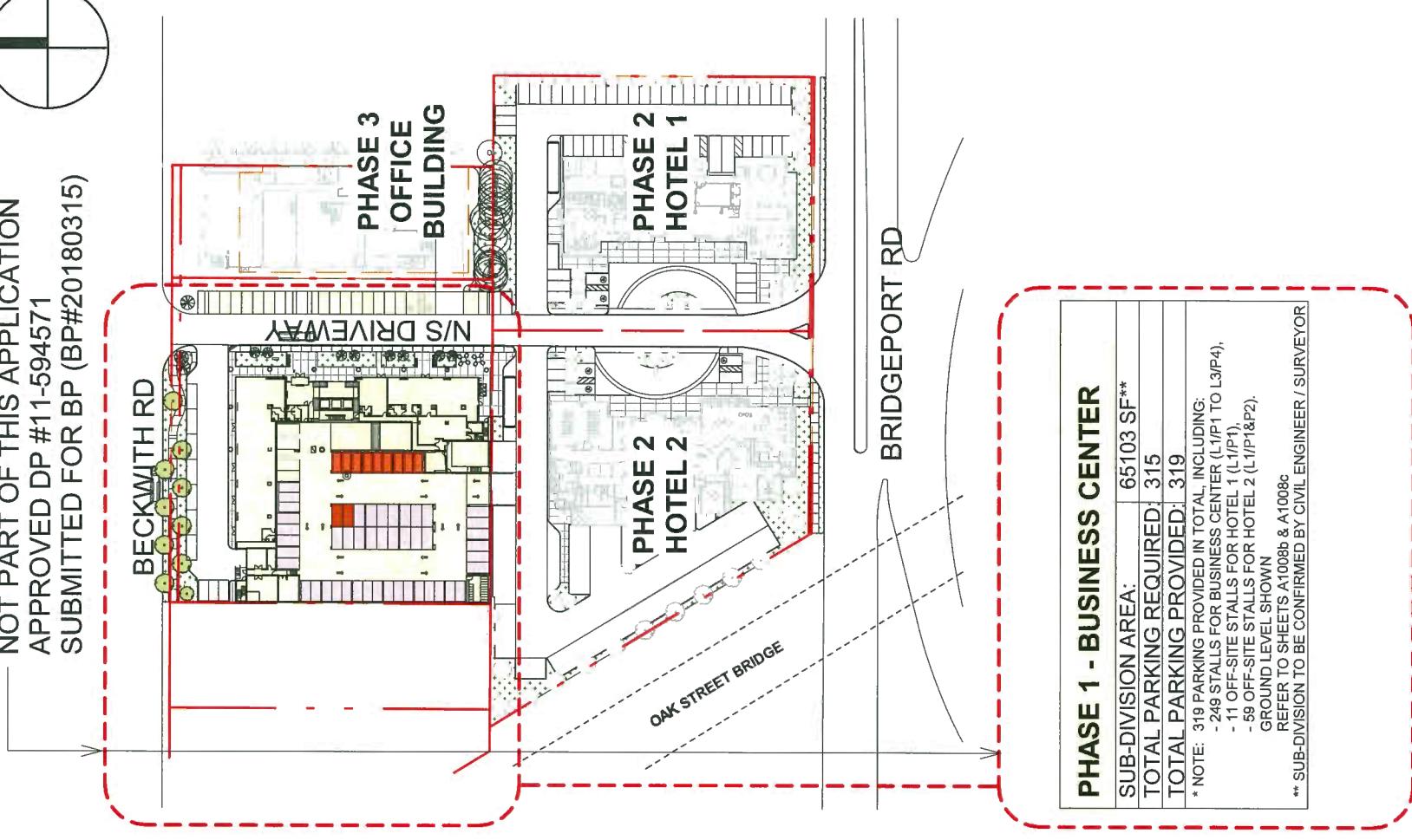
Revision Date: 06/19/18

Note: Dimensions are in METRES

IBI

NOT PART OF THIS APPLICATION
APPROVED DP #11-594571
SUBMITTED FOR BP (BP#20180315)

9455 & 9533 BRIDGEPORT ROAD



PHASING PLAN - GROUND LEVEL SHOWN
DP 18-825006 $\frac{1}{164''} = 1'-0''$

A 1008

-

9455 & 9533 BRIDGEPORT ROAD

| HOTEL 01 UNIT BREAKDOWN | | | | | | |
|-------------------------|------------------|---------------------|------------------------------|---------------------------------|------------------------------------|-----|
| FLOOR | UNIT TYPE | | | SUB-TOTAL | | |
| | TYPE A STUDIO | TYPE B 1 BEDROOM | TYPE C ACCESSIBLE UNIT | TYPE D PENTHOUSE (PH) STD | TYPE E PENTHOUSE (PH) CORNER | |
| L10 | 1 | 4 | | 4 | 4 | 9 |
| L9 | 9 | 4 | | | | 13 |
| L8 | 9 | 4 | | | | 13 |
| L7 | 9 | 4 | | | | 13 |
| L6 | 9 | 4 | | | | 13 |
| L5 | 9 | 4 | | | | 13 |
| L4 | 8 | 4 | 1 | | | 13 |
| L3 | 8 | 4 | 1 | | | 13 |
| L2 | 7 | 4 | 1 | | | 12 |
| L1 | | | | | | |
| Sub-Total | 69 | 32 | 3 | 4 | 4 | 112 |

| FLOOR | UNIT TYPE | | | | | SUB-TOTAL |
|----------------------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------|
| | TYPE A | TYPE B | TYPE C | TYPE D | TYPE E | |
| DOUBLE QUEEN SUITE (NO. 8) | KING SUITE (KS) | KING SUITE (KS) | PENTHOUSE UNITS | PENTHOUSE UNITS | PENTHOUSE UNITS | |
| L12 | 2 | 2 | PH-STD | PH-STD | PH-CORNER | |
| L11 | 7 | 12 | | | | 19 |
| L10 | 7 | 12 | | | | 19 |
| L9 | 7 | 12 | | | | 19 |
| L8 | 7 | 12 | | | | 19 |
| L7 | 7 | 12 | | | | 19 |
| L6 | 7 | 11 | 1 | | | 19 |
| L5 | 7 | 11 | 1 | | | 19 |
| L4 | 7 | 11 | 1 | | | 19 |
| L3 | 7 | 11 | 1 | | | 19 |
| L2 | 7 | 9 | 1 | | | 17 |
| L1 | | | | | | |
| SUB-TOTAL | 72 | 115 | 5 | 4 | 2 | 200 |

As per City of Richmond Parking Bylaw Section 7, Table 7.7.2.3 General Parking Requirements & Table 7.9.4.1 Blended City Centre Parking

- 10 spaces per 100.0 m² of gross leasable floor area of building used for accessory restaurant, meeting/convention facilities, lounges and banquet halls purposes; plus
- 3 spaces per 100.0 m² of gross leasable floor area of building used for general or convenience retail (Also Note: On the first floor of the building, a rate of 4.4 spaces per 100.0 m² of gross leasable floor area of building would be applicable for retail, restaurant and office and on the second floor or above, a rate of 3 spaces per 100.0 m² of gross leasable floor area of building would be applicable).

| PARKING BREAKDOWN PROVIDED (PER TYPE/Building) | | | | |
|--|------------|------------|------------|--------------|
| | REGULAR | SMALL | ACCESSIBLE | TOTAL STALLS |
| HOTEL 01 | 37 | 28 | 2 | 67 |
| HOTEL 02 | 93 | 24 | 3 | 120 |
| TOTAL | 129 | 117 | 3 | 249 |
| TOTAL | 259 | 169 | 8 | 436 |

| PARKING SUMMARY COMPARISON BETWEEN APPROVED OF AND NEW DP APPLICATION | | | | |
|---|---|---|------------------------------|---|
| | Approved DP #11-594571 (After TDM reduction) | New DP Application (After TDM reduction) | Notes for New DP Application | |
| HOTEL 01 | REQUIRED PROVIDED | 75 PROVIDED | 67 | 56 on-street surface; 11 in BC Building |
| HOTEL 02 | REQUIRED PROVIDED | 99 PROVIDED | 120 | 61 on-street surface; 59 in BC Building |
| TOTAL | REQUIRED PROVIDED | 242 PROVIDED | 241 | Dedicated to BC after TDM |
| TOTAL | REQUIRED PROVIDED | 256 PROVIDED | 249 | Shared with H142 after TDM |
| TOTAL | REQUIRED PROVIDED | 416 PROVIDED | 428 | Surplus to BC after TDM to share |

| LOADING SUMMARY | | REFERENCE: Section 7.13 Of the Bylaw | | |
|-----------------|-------------------|--------------------------------------|------------------------------|--------|
| | | MEDIUM SIZE | LARGE SIZE | TOTAL |
| HOTEL 01 | REQUIRED PROVIDED | 2 REQUIRED 2 PROVIDED | N/A REQUIRED N/A PROVIDED | 2 2 |
| HOTEL 02 | REQUIRED PROVIDED | 2 REQUIRED 2 PROVIDED | N/A REQUIRED N/A PROVIDED | 2 2 |
| TOTAL | REQUIRED REQUIRED | 4 REQUIRED 4 PROVIDED | N/A REQUIRED N/A PROVIDED | 4 4 |

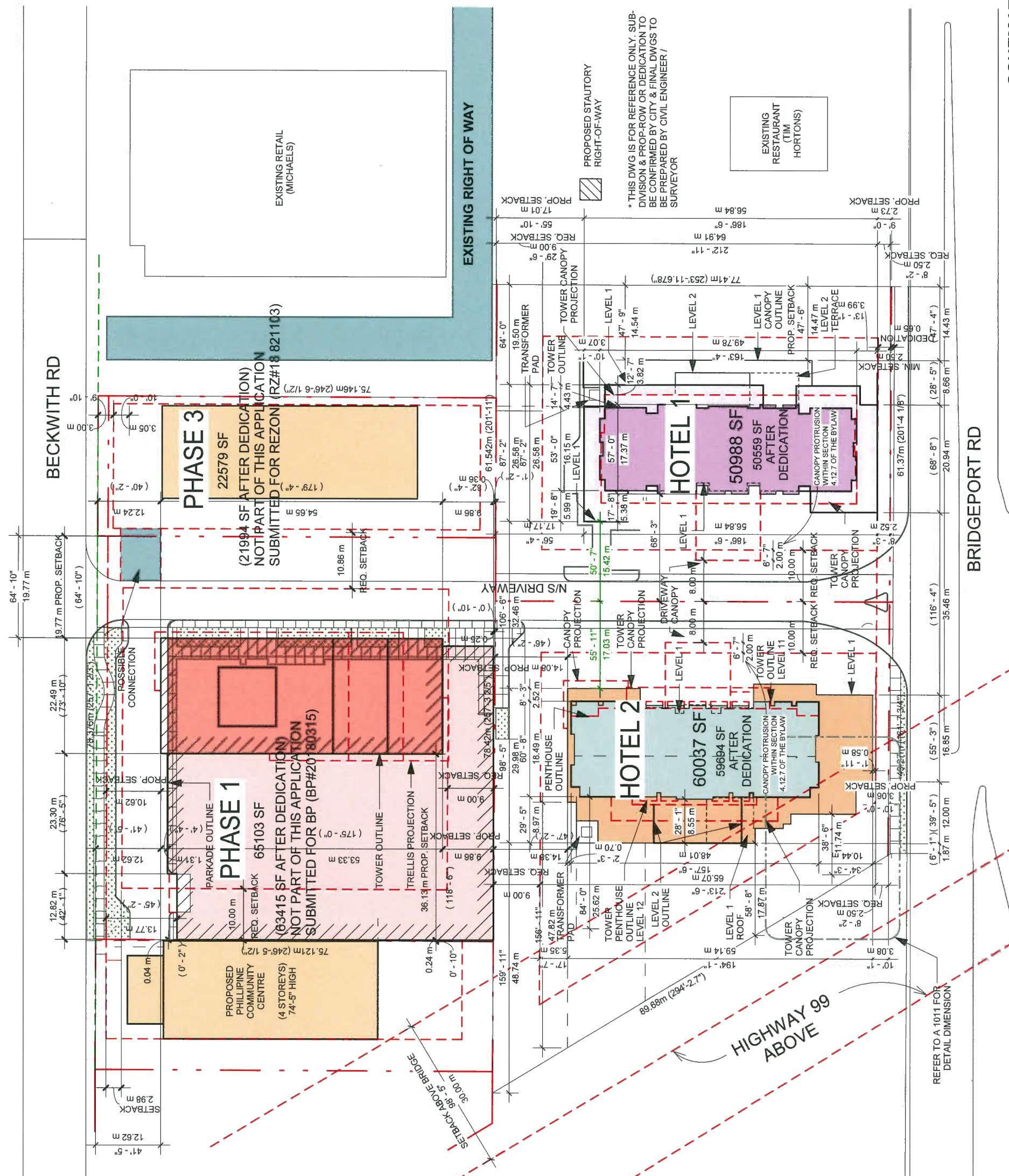
| | | FAR | | | NON FAR (MEP/SERV) | | | GROSS FLOOR AREA | |
|------|-------|---------------------|------------------|------------|--------------------|---------------------|---------|------------------|---------------|
| | | HOTEL / OTHER AREAS | LOUNGE/BREAKFAST | CONFERENCE | BREAKFAST PREP | LOBBY & COMMON AREA | AMENITY | ADMIN | FAR SUB-TOTAL |
| TOP | | | | | | | | | |
| MECH | | | | | | | | | |
| 10 | | 6,154 | | | | 1,105 | | | 7,719 |
| 9 | | 6,128 | | | | 1,105 | | | 8,033 |
| B | | 6,128 | | | | 1,105 | | | 8,033 |
| 7 | | 6,128 | | | | 1,105 | | | 8,033 |
| 6 | | 6,128 | | | | 1,105 | | | 8,033 |
| 5 | | 6,128 | | | | 1,105 | | | 8,033 |
| 4 | | 6,128 | | | | 1,105 | | | 8,033 |
| 3 | | 6,128 | | | | 1,105 | | | 8,033 |
| 2 | | 6,128 | | | | 1,086 | | | 8,028 |
| 1 | | 2,777 | | 1,213 | 649 | 423 | 3,378 | 417 | 10,725 |
| | TOTAL | 64,428 | 1,213 | 649 | 423 | 11,734 | 8,378 | 958 | 82,443 |

| FAR | | | | | | | NON FAR (MEPS/RSR.) | | GROSS FLOOR AREA | |
|--------------------|----|--------------|--------------|-------------------|--------------|---------------------|---------------------|--------------|------------------|----------------|
| HOTEL/ OTHER AREAS | | RESTAURANT | KITCHEN | LOUNGE/ BREAKFAST | CONFERENCE | LOBBY & COMMON AREA | AMENITY | HOTEL ADMIN | FAR SUB-TOTAL | |
| TOP | | | | | | | | | | 3,269 |
| MECH. | 12 | 6,619 | | | | 1,369 | | | | 630 |
| | 11 | 7,477 | | | | 1,369 | | | | 720 |
| | 10 | 7,296 | | | | 1,366 | | | | 609 |
| | 9 | 7,296 | | | | 1,366 | | | | 609 |
| | 8 | 7,296 | | | | 1,366 | | | | 609 |
| | 7 | 7,296 | | | | 1,366 | | | | 609 |
| | 6 | 7,296 | | | | 1,366 | | | | 609 |
| | 5 | 7,296 | | | | 1,366 | | | | 609 |
| | 4 | 7,296 | | | | 1,366 | | | | 609 |
| | 3 | 7,296 | | | | 1,366 | | | | 609 |
| | 2 | 6,530 | | | | 1,569 | | | | 609 |
| | 1 | 4,977 | 1,263 | 659 | 1,093 | 1,933 | 2,677 | 1,143 | 216 | 16,114 |
| TOTAL | | 8,981 | 1,263 | 650 | 1,493 | 1,813 | 18,082 | 1,332 | 937 | 122,831 |

FOR REFERENCE ONLY: EXTRACT FROM APPROVED DP #11-594571

STATISTICS A 1009

7-900928-81 do NTS

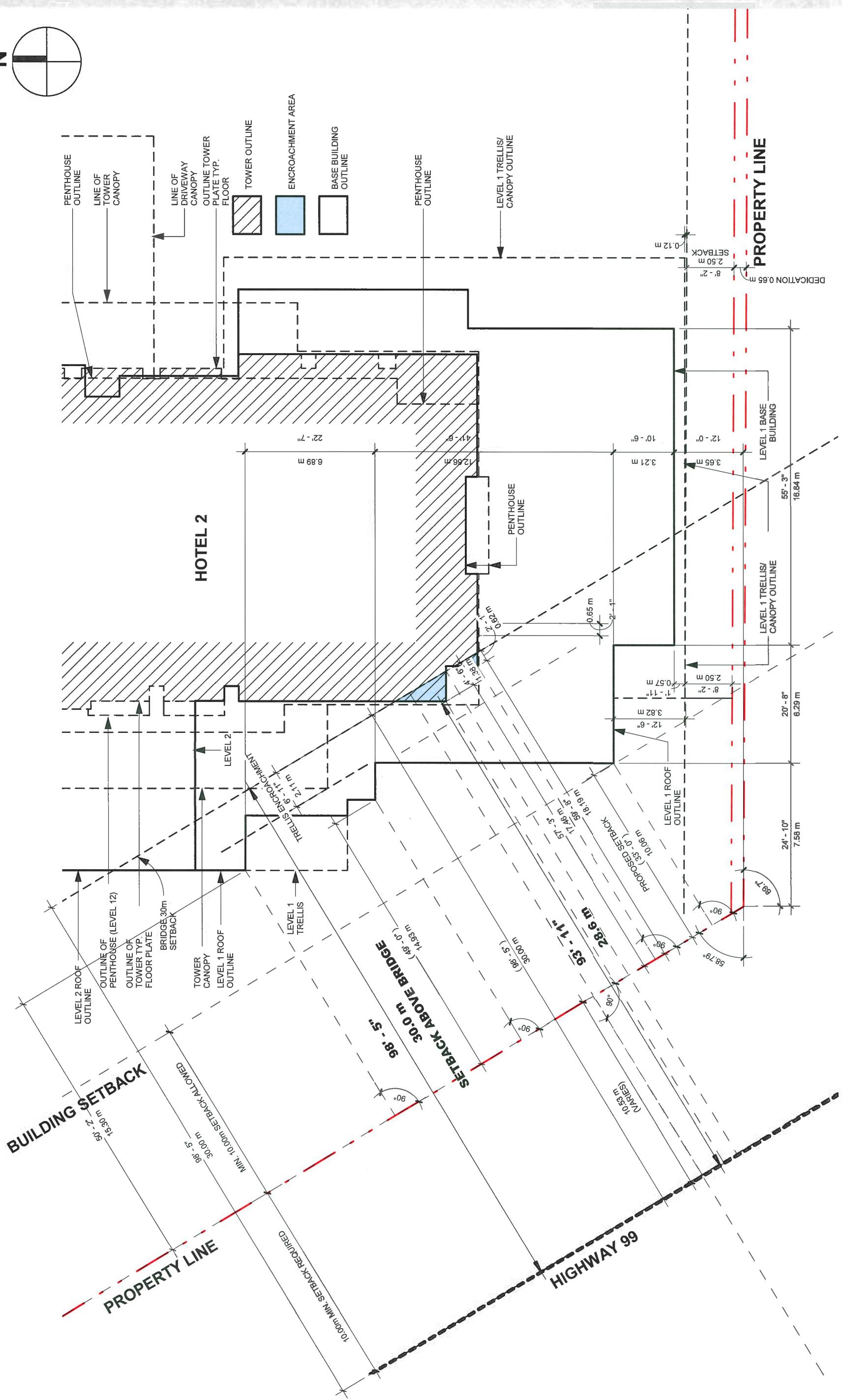
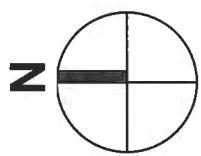


B1

9455 & 9533 BRIDGEPORT ROAD

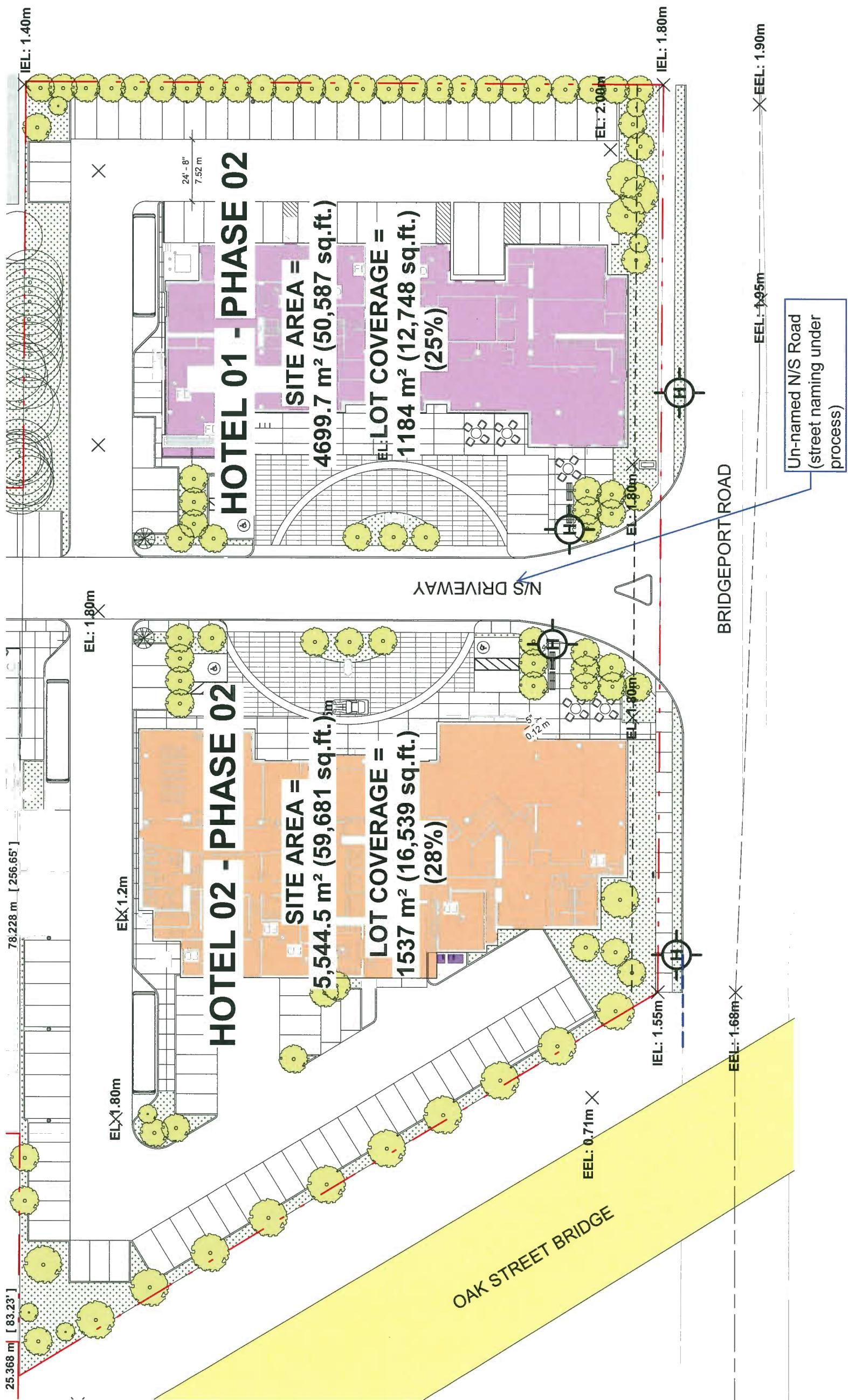
A 1011

$$8'' = 1'-0"$$



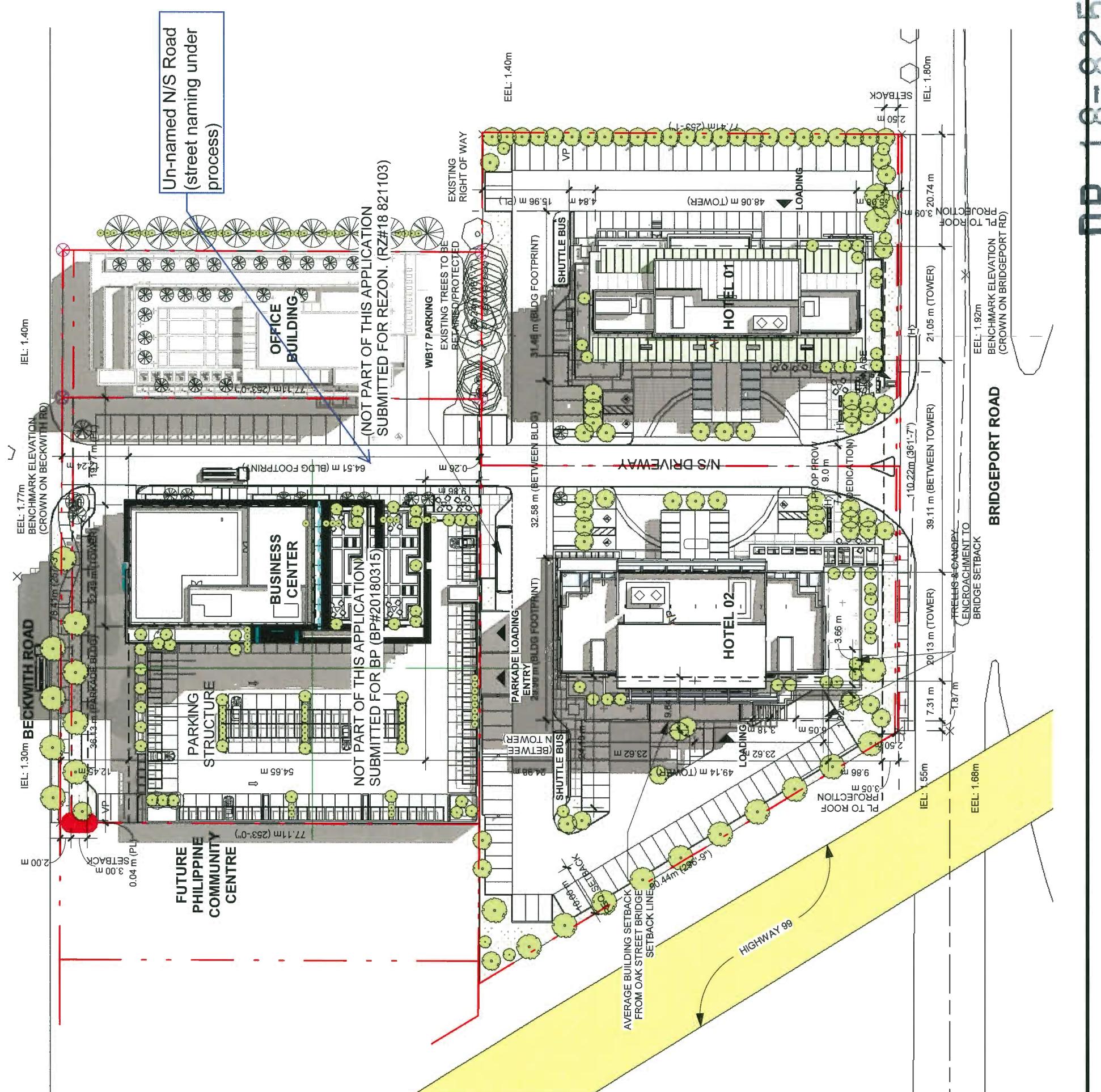
-
B
-

9455 & 9533 BRIDGEPORT ROAD



B

9455 & 9533 BRIDGEPORT ROAD



B

9455 & 9533 BRIDGEPORT ROAD

N



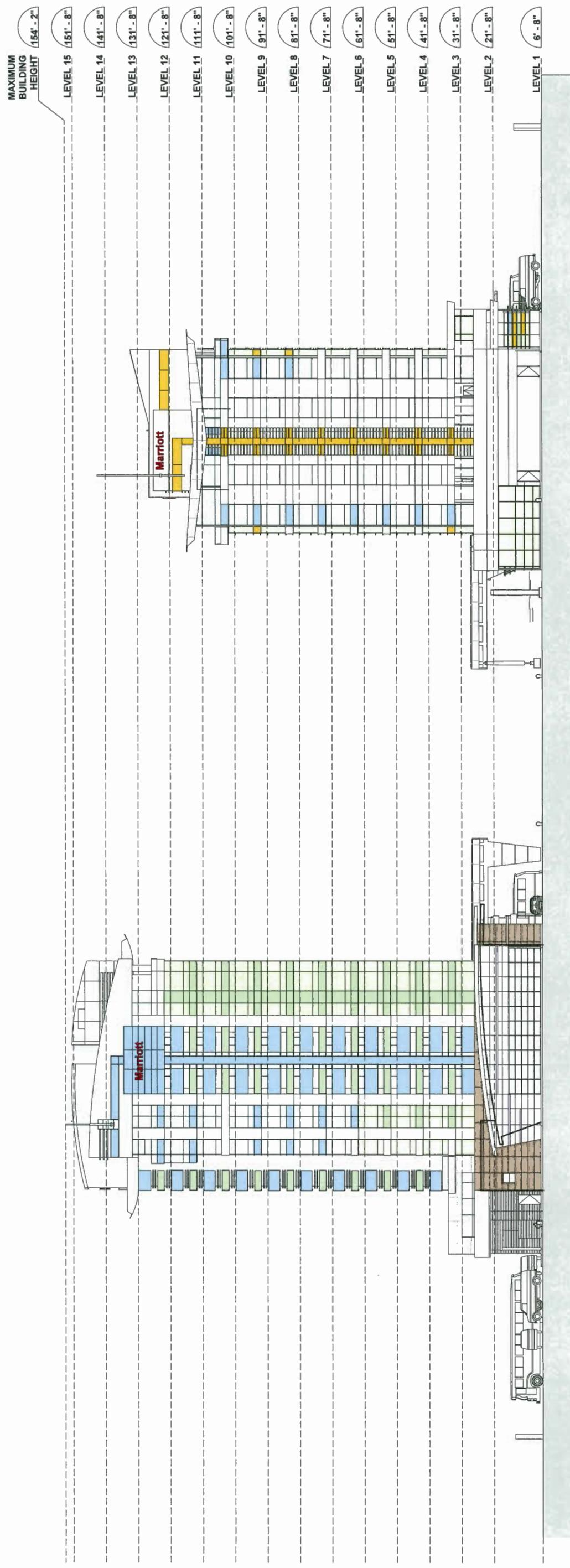
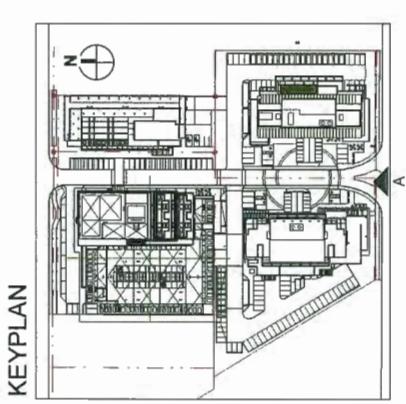
A 1102

SITE PLAN-GROUND FLOOR LAYOUT

UP 18-82500 6^{1/32"} = 47"

IBI

9455 & 9533 BRIDGEPORT ROAD



PROJECT SOUTH ELEVATION

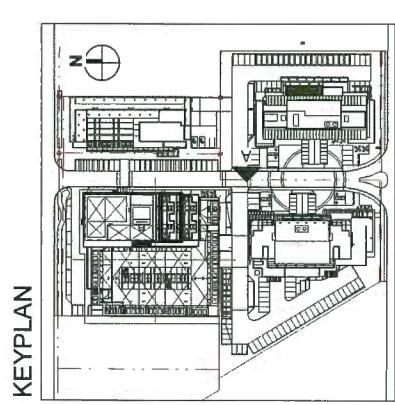
A 1201

DP 18-825006-8

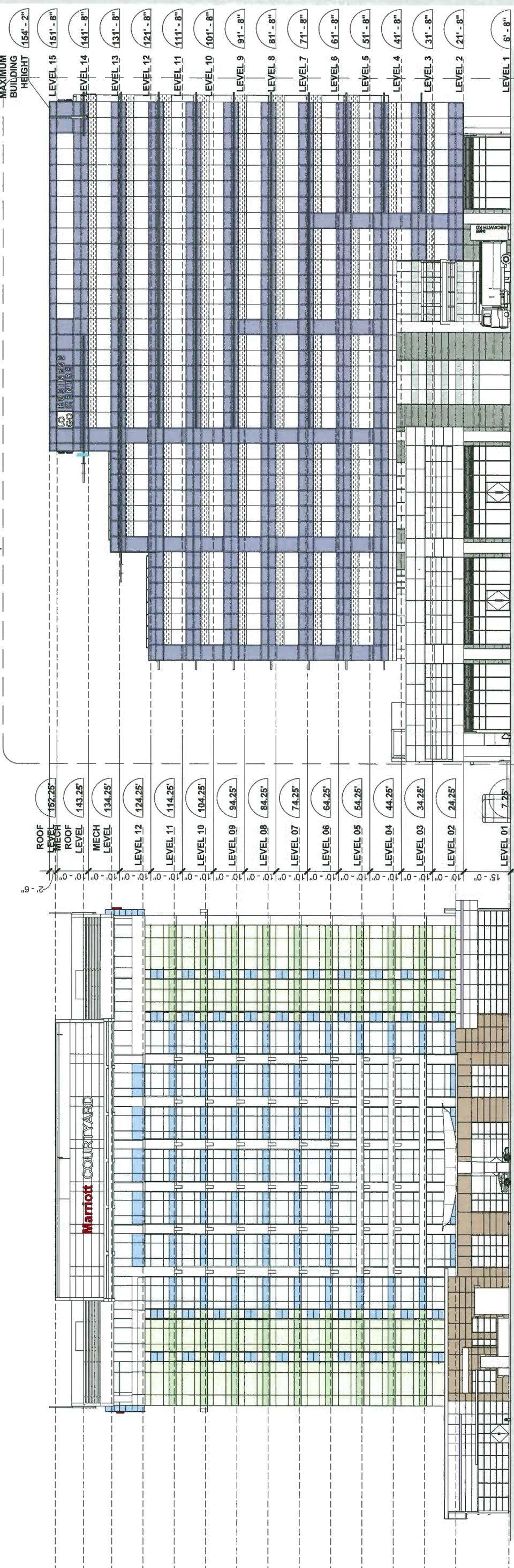
116" = 1' 0"

IBI

9455 & 9533 BRIDGEPORT ROAD



NOT PART OF THIS APPLICATION
SUBMITTED FOR BP (BP#20180315)



PROJECT EAST ELEVATION

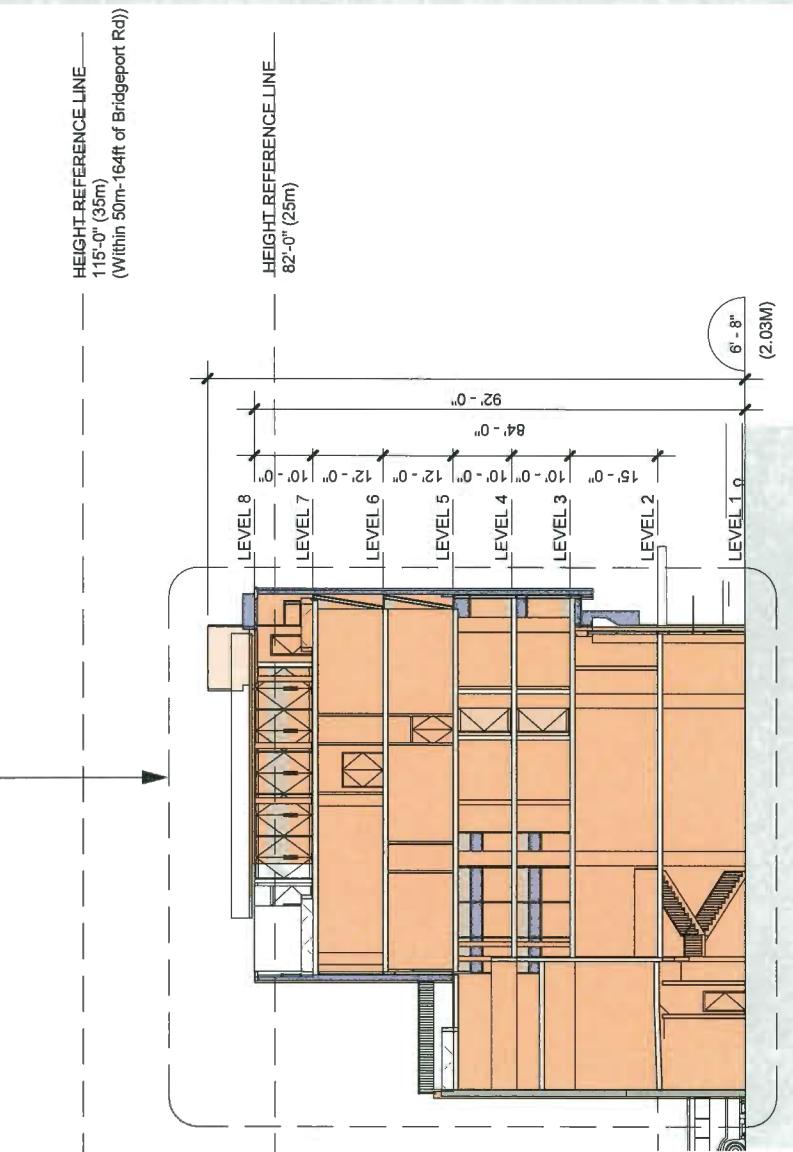
A 1202

DP 18-825006 1/16" - 0"

B-

9455 & 9533 BRIDGEPORT ROAD

NOT PART OF THIS APPLICATION
SUBMITTED FOR REZON.
RZ#18 821103)



O - OFFICE

H1 -HOTEL 1

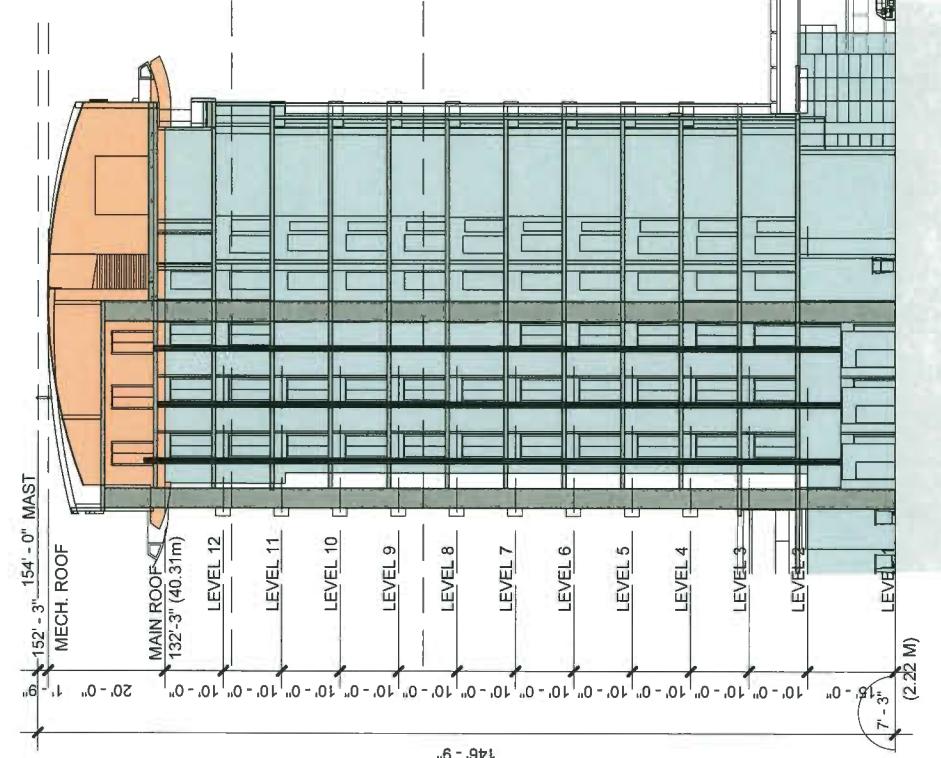
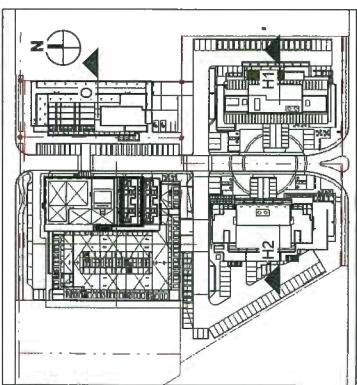
H2-HOTEL 2

SCHEMATIC SECTION

A 1401

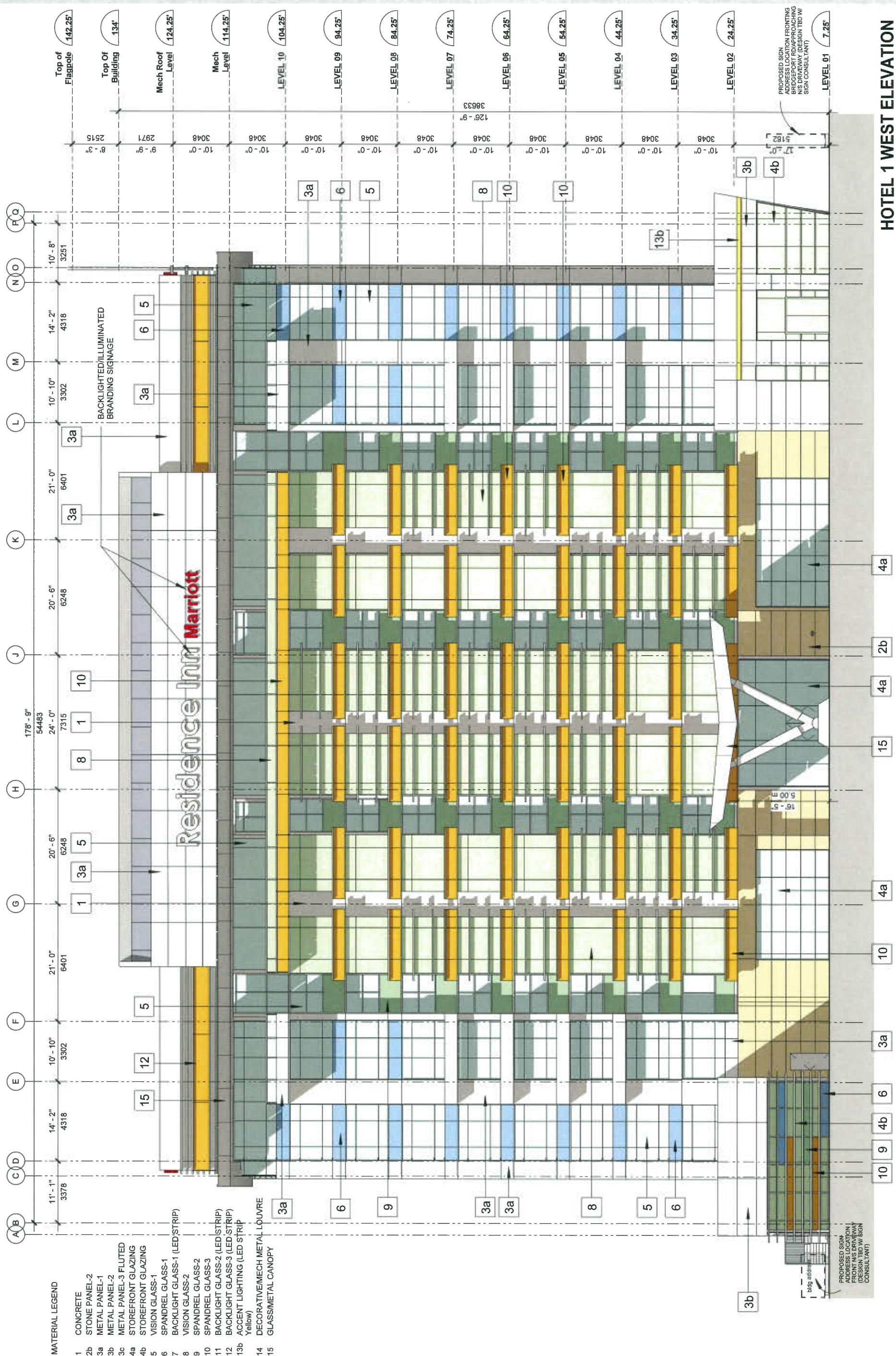
DP 18-825006 - 1116" Ø 1'-0"

KEYPLAN



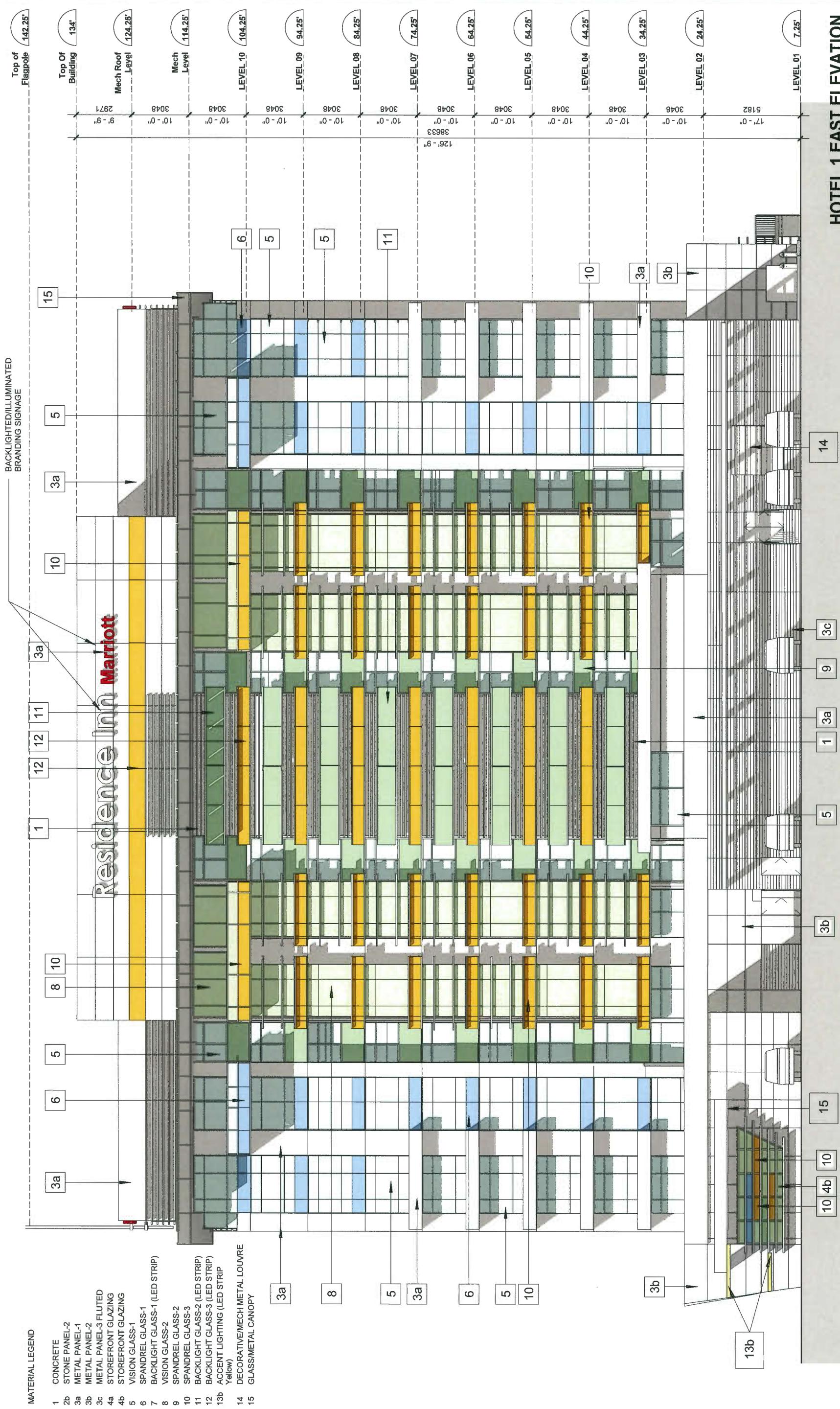
B

HOTEL 1



IBI

HOTEL 1



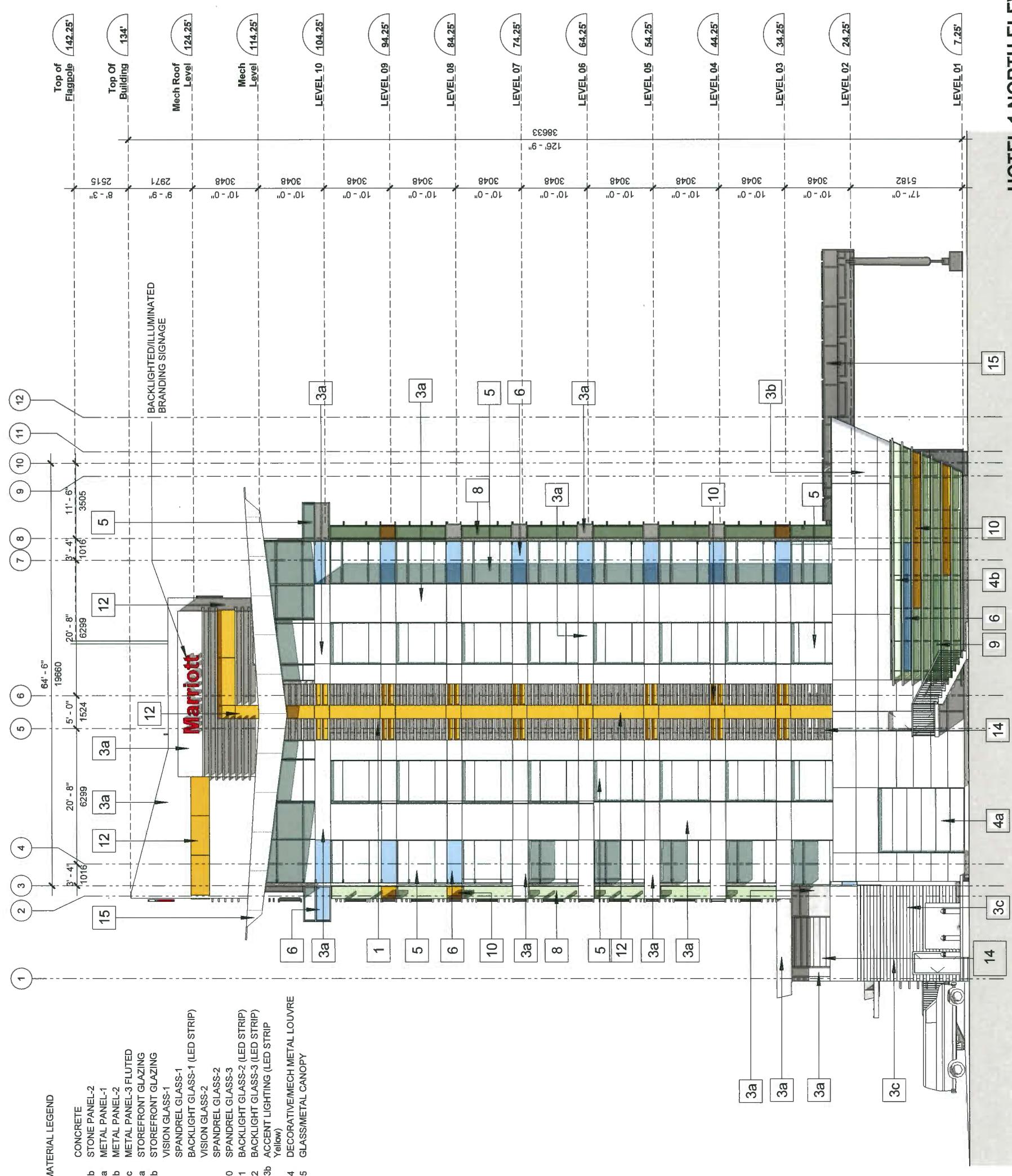
HOTEL 1 EAST ELEVATION

UP 18-825006-12

A 2102

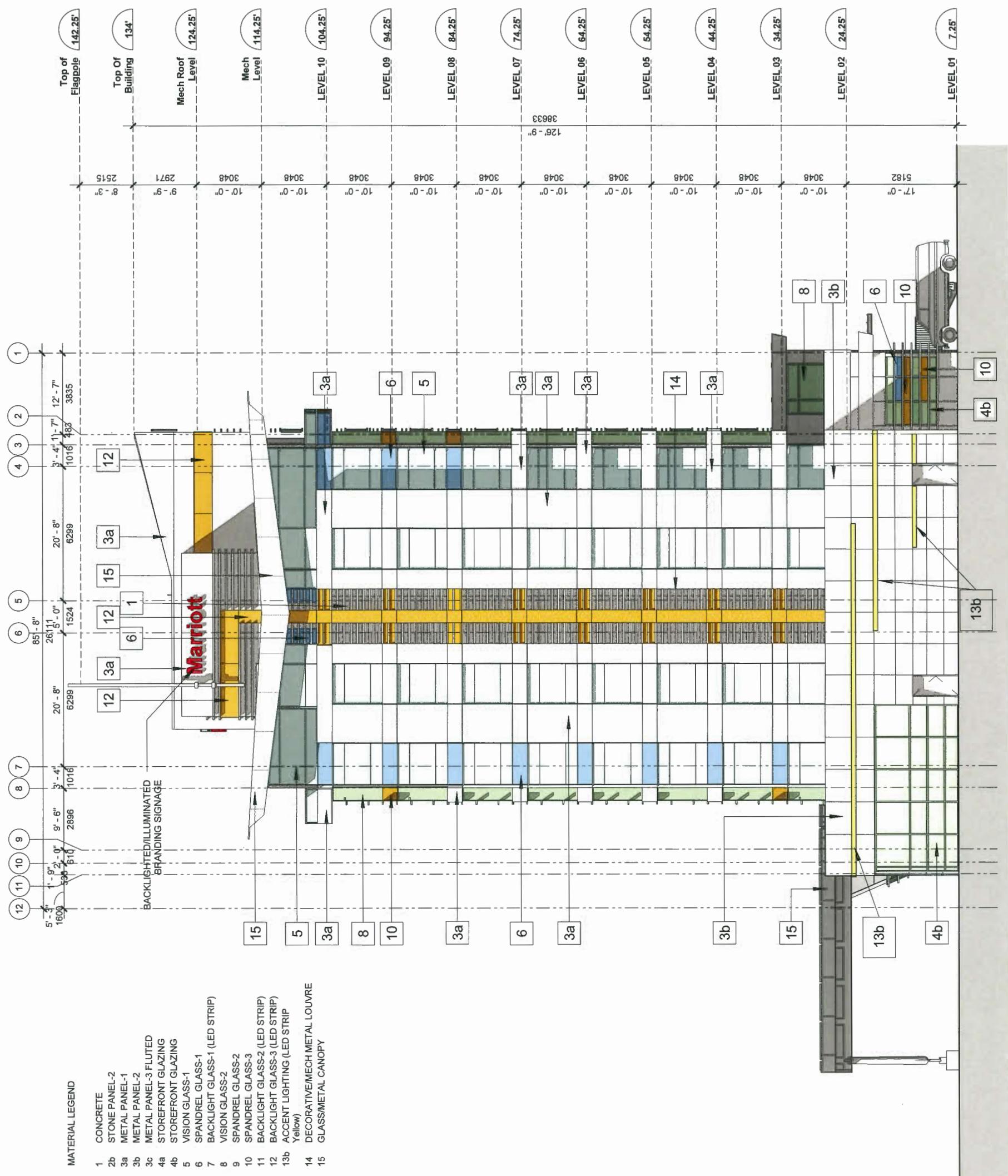
-B-

HOTEL 1



B

HOTEL 1



HOTEL 1 SOUTH ELEVATION A 2104

UP 18-825006-14

-
B
-

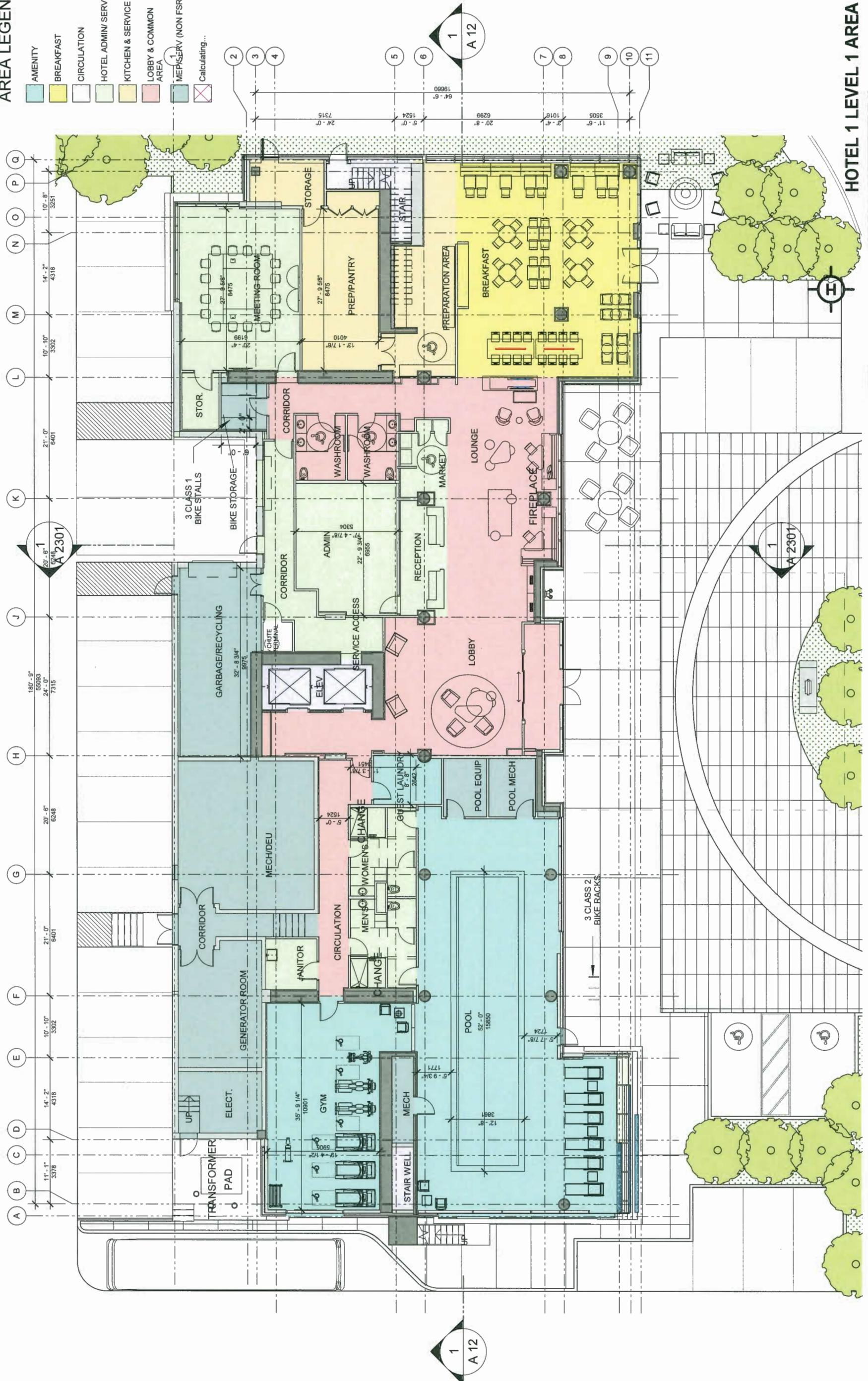
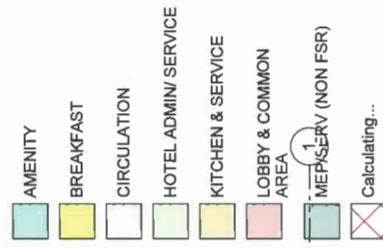
HOTEL 1

A 2201

HOTEL 1 LEVEL 1 AREA PLAN

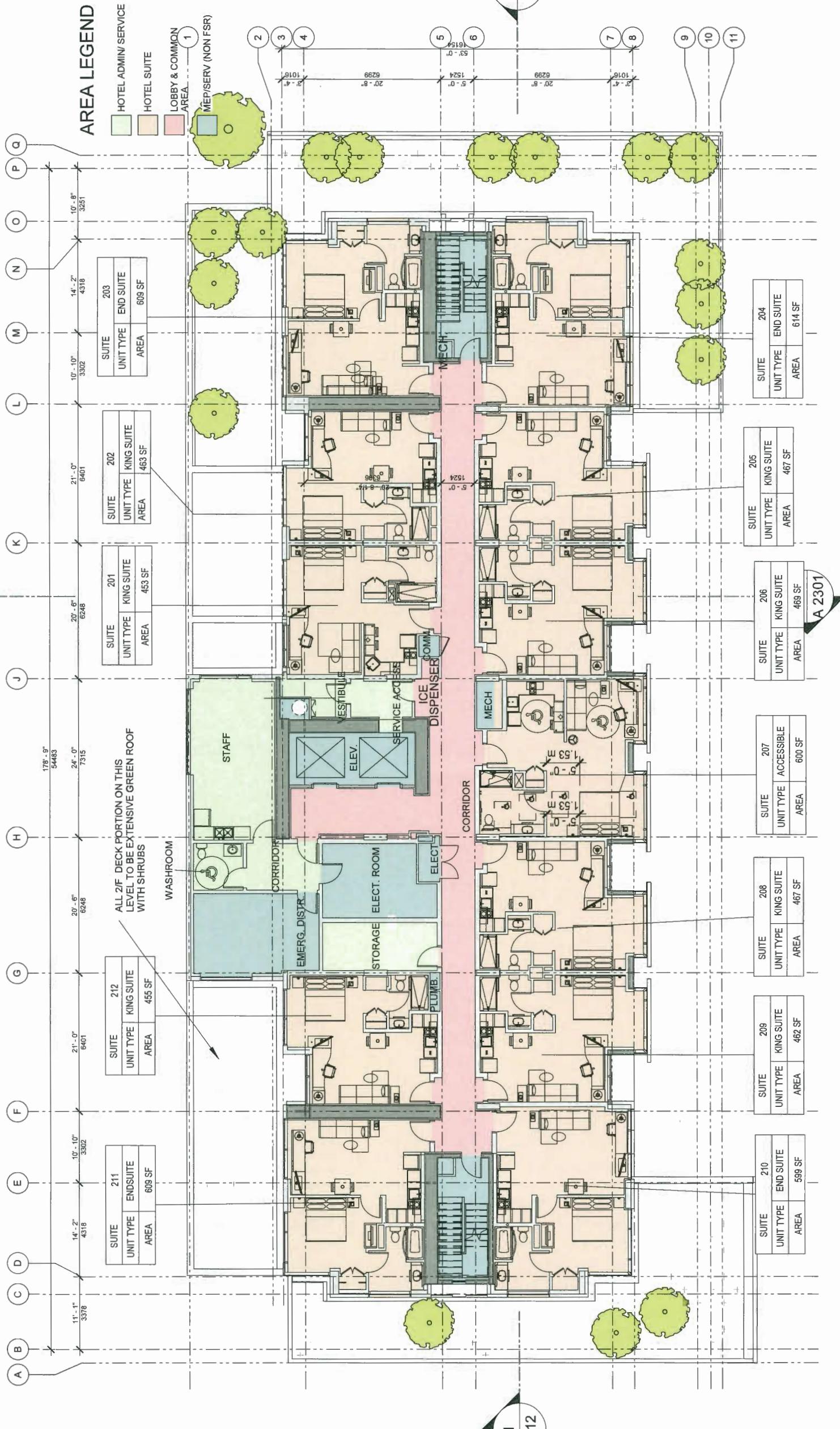
DP 18-825006-15

AREA LEGEND



IBI

HOTEL 1



HOTEL 1 LEVEL 2 AREA PLAN
DP 18-825006 A 2202
 $1/16'' = 1'-0''$

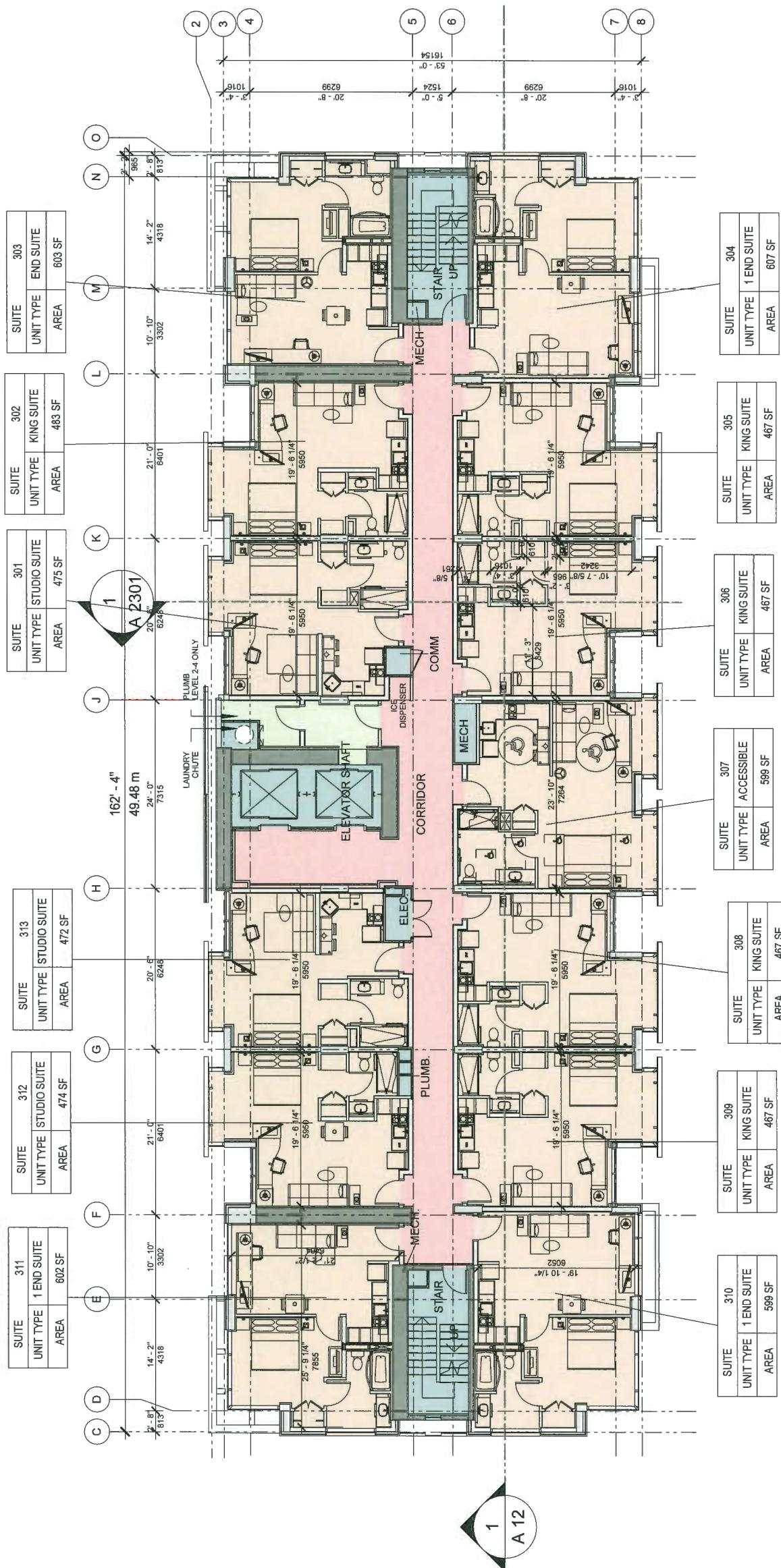
IBI

HOTEL 1

A 2203

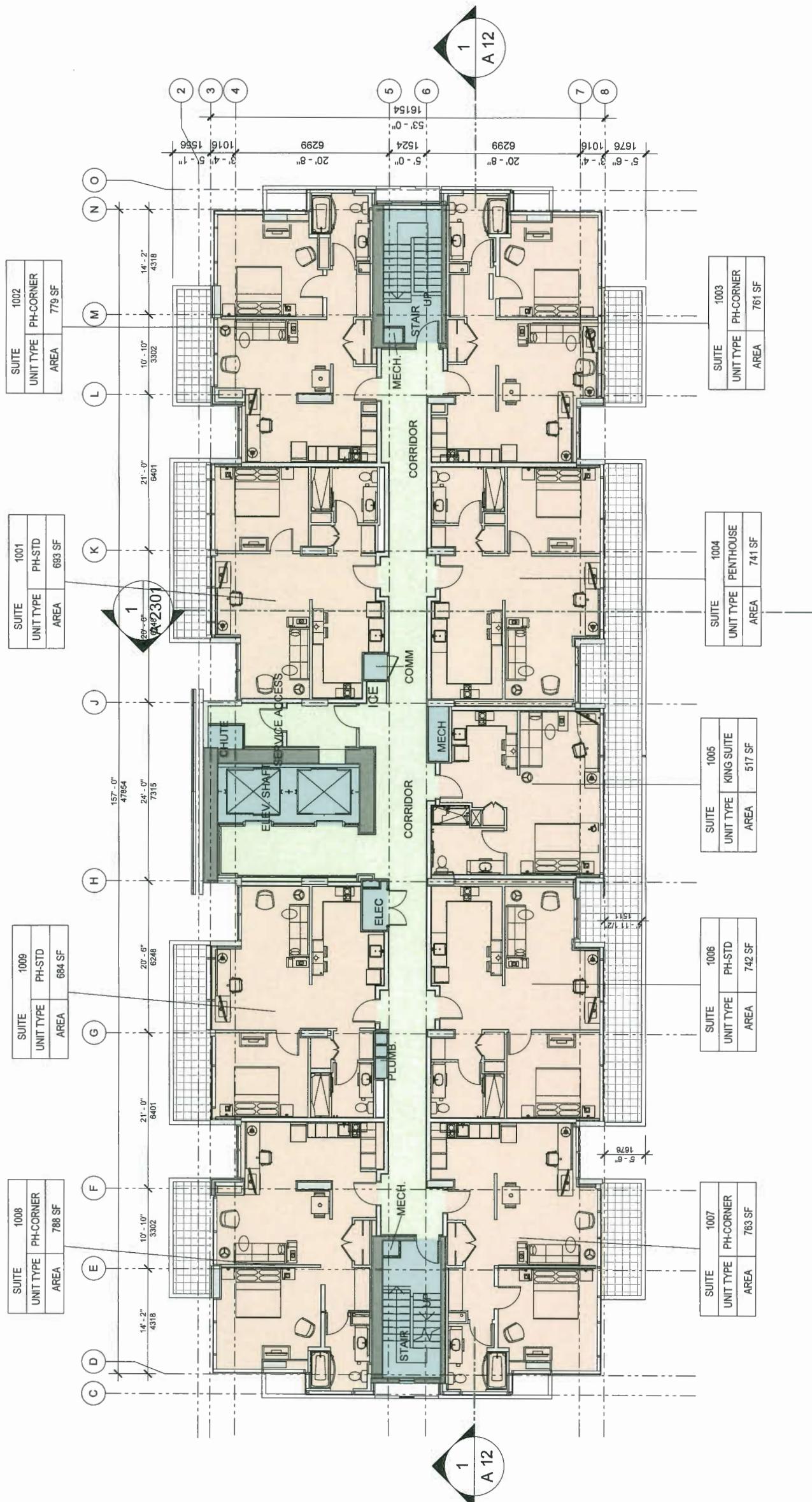
HOTEL 1 LEVEL 3-9 TYP AREA PLAN

UP 18-825006 1/16" - 1'-0"



IBI

HOTEL 1



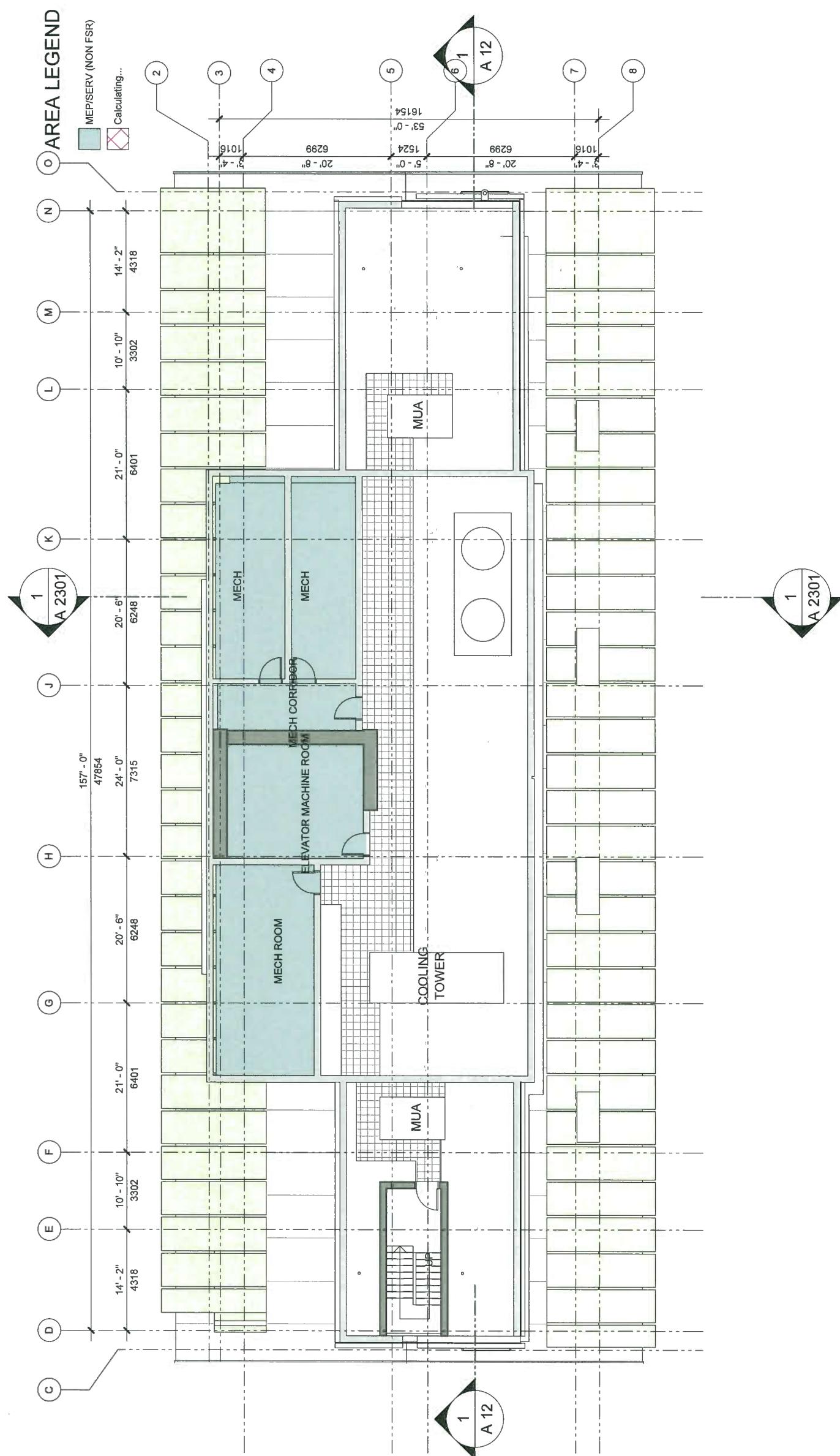
HOTEL 1 LEVEL 10 AREA PLAN

A 2204

DP 18-825006-18
1/16" = 1'-0"

[B]

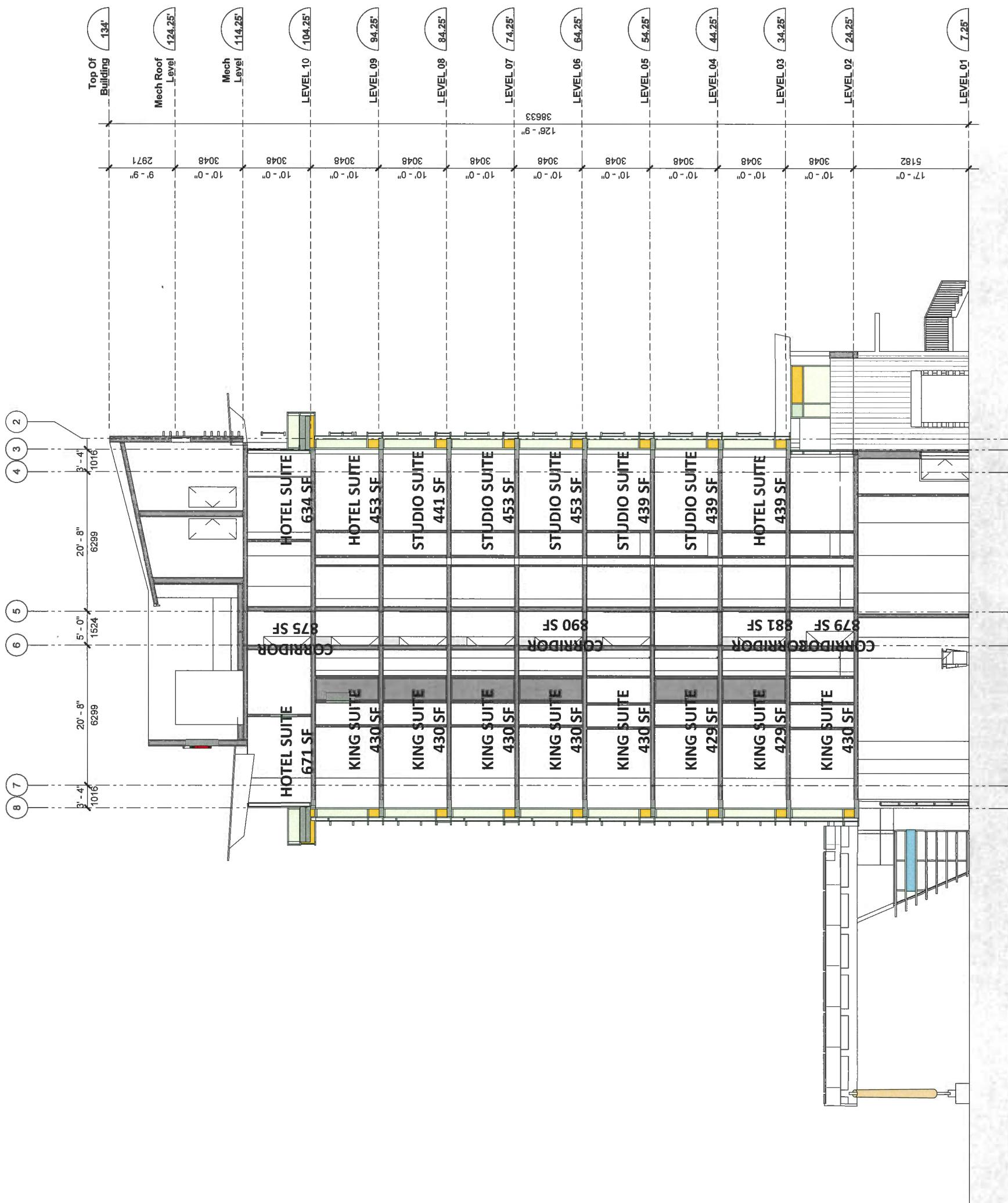
HOTEL 1



HOTEL 1 MECHANICAL ROOF AREA PLAN A 2205
D.P. 18-825006-19

-
B
-

HOTEL 1



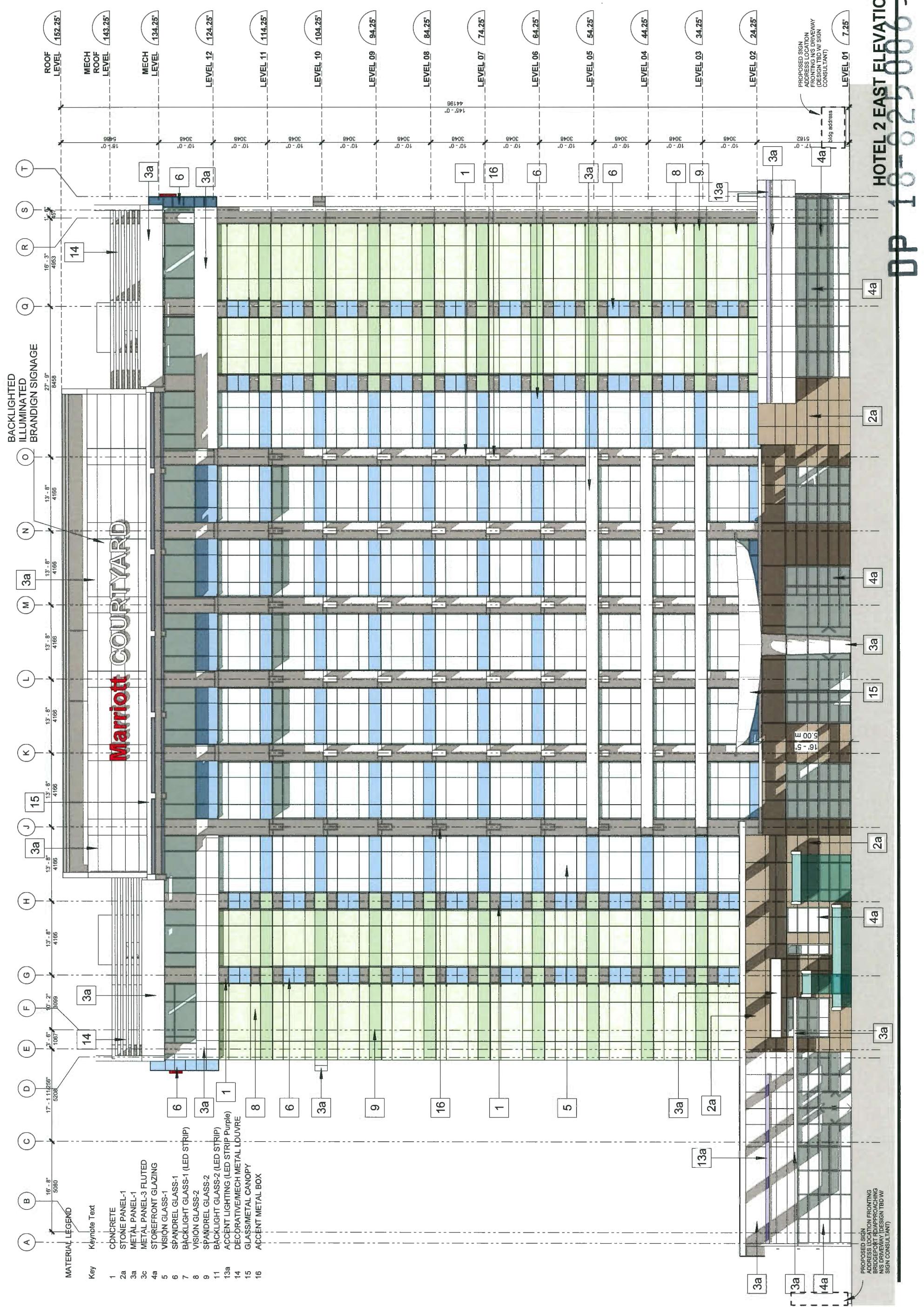
A 2301

NOTE 1 - Section B-B

呂

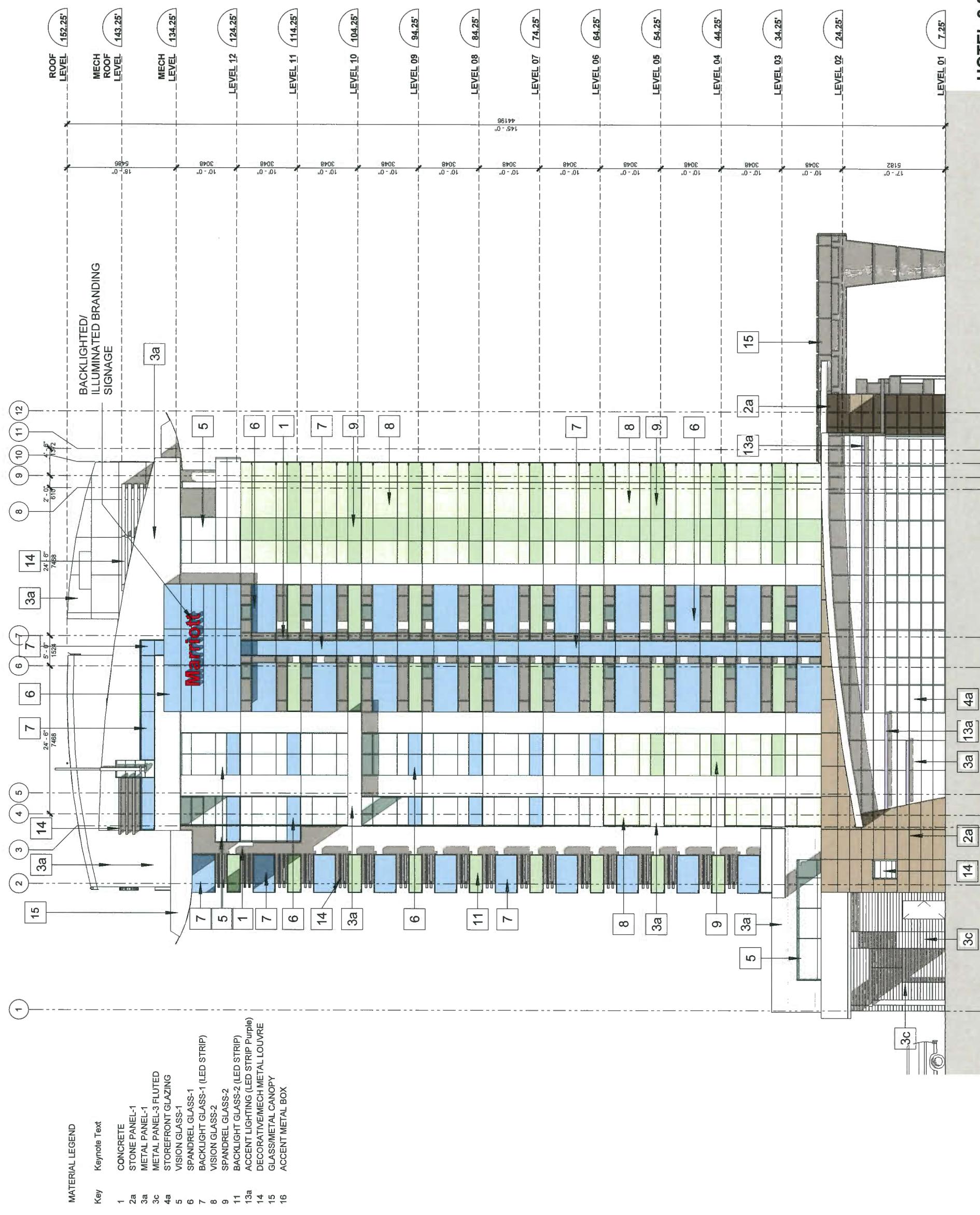
-B-

HOTEL 2



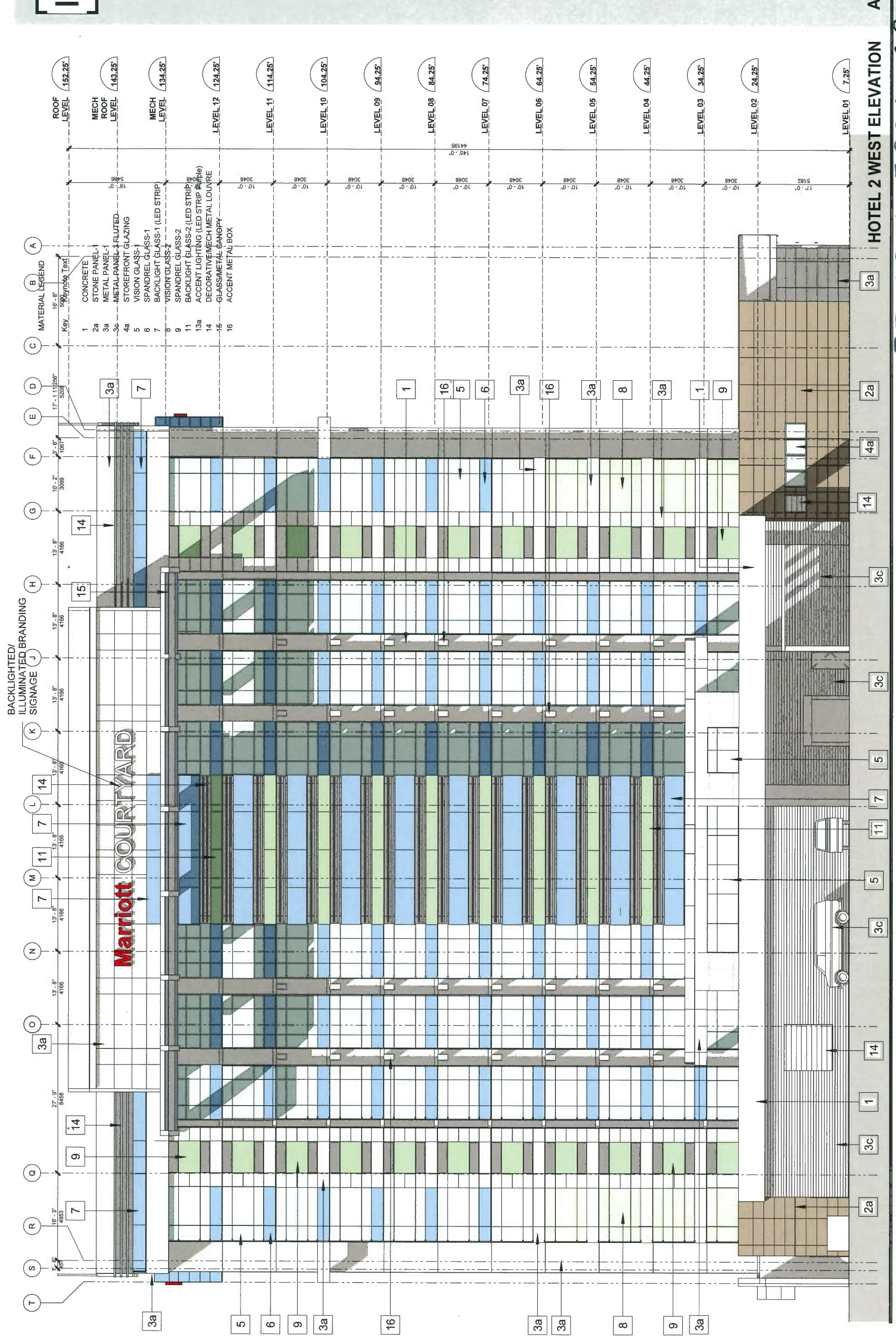
IBI

HOTEL 2



-B-

HOTEL 2



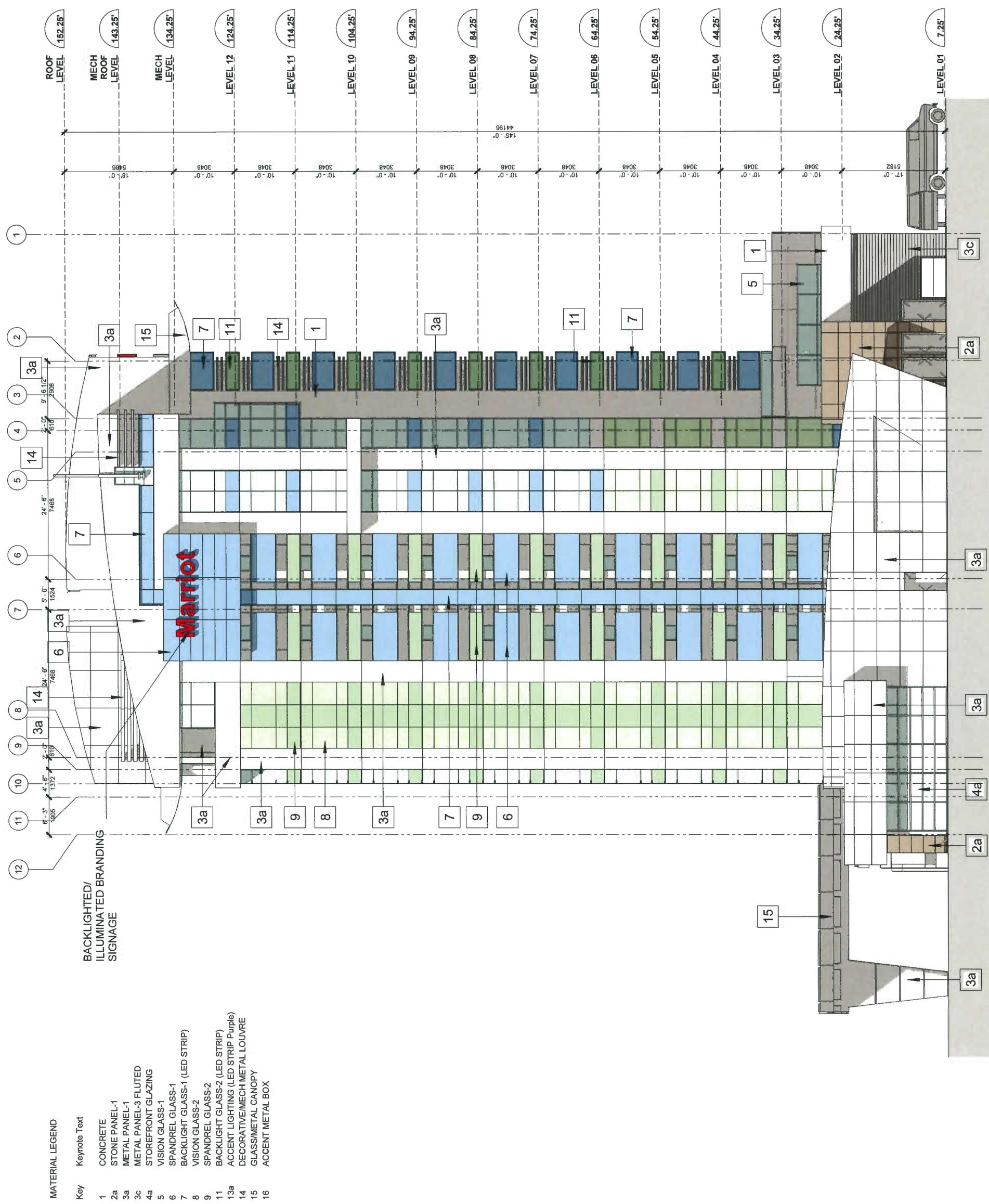
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HOTEL 2

A 3104

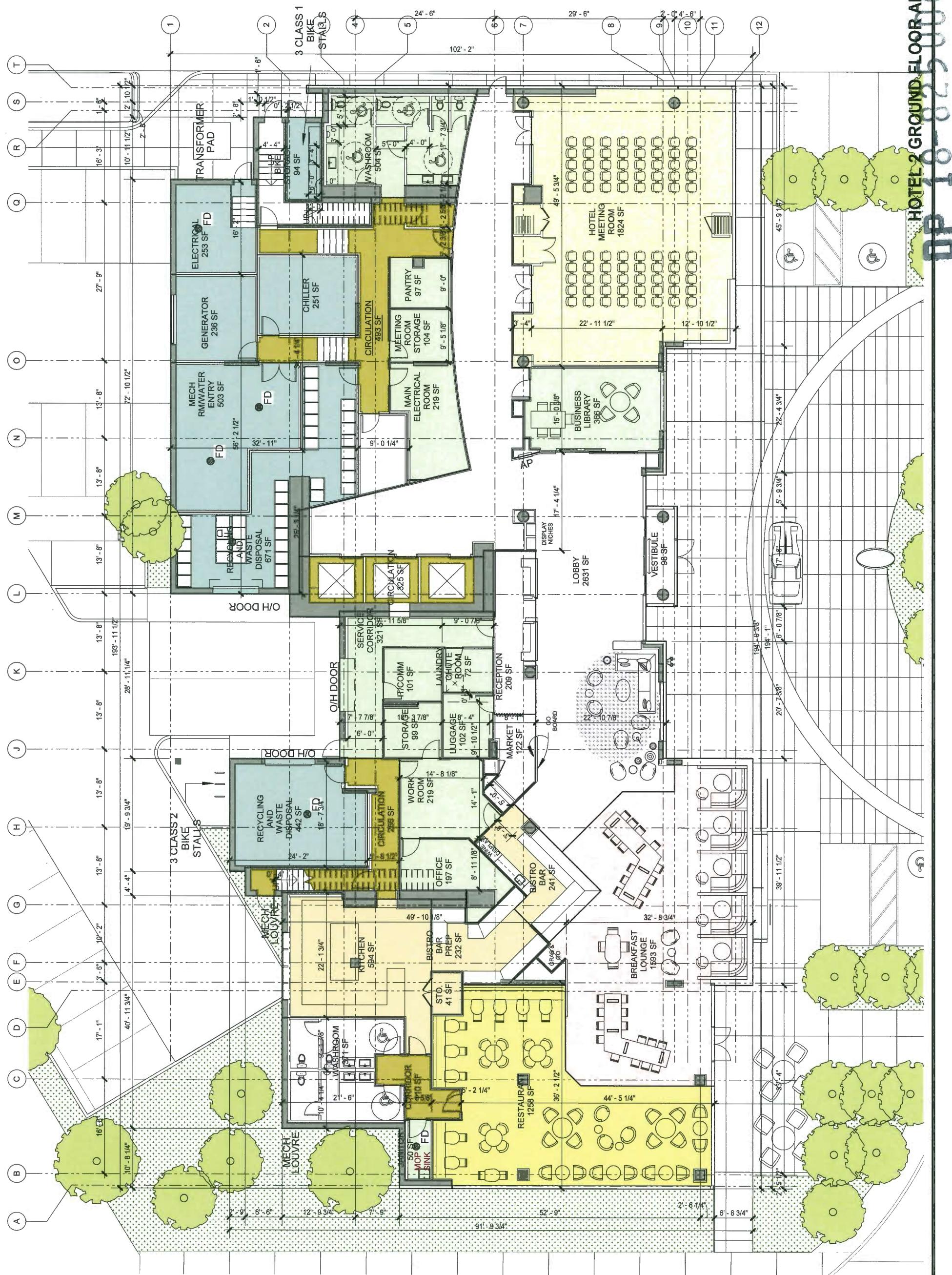
HOTEL 2 NORTH ELEVATION

DP 18-825006 -74



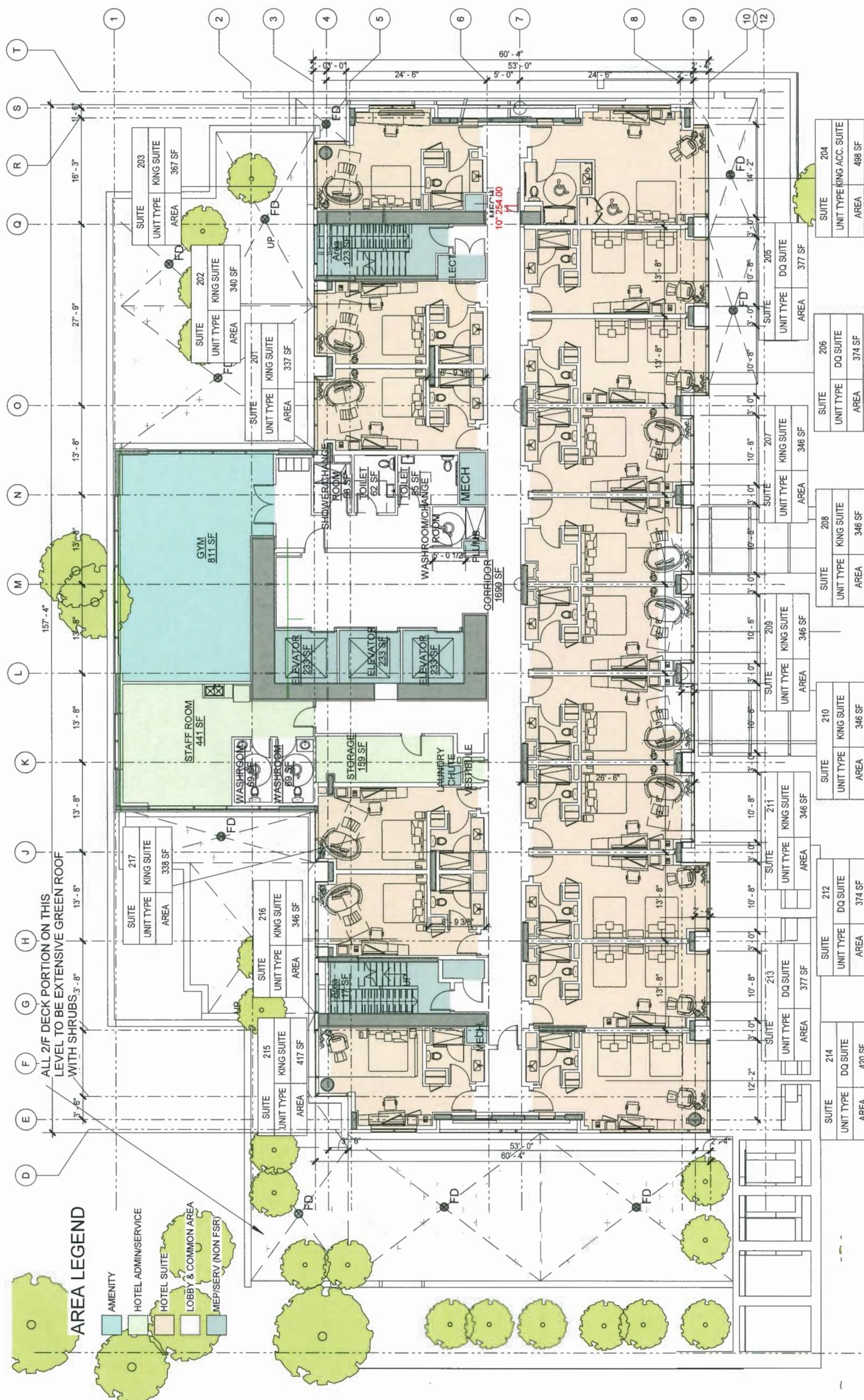
B

HOTEL 2



-
B
-

HOTEL 2

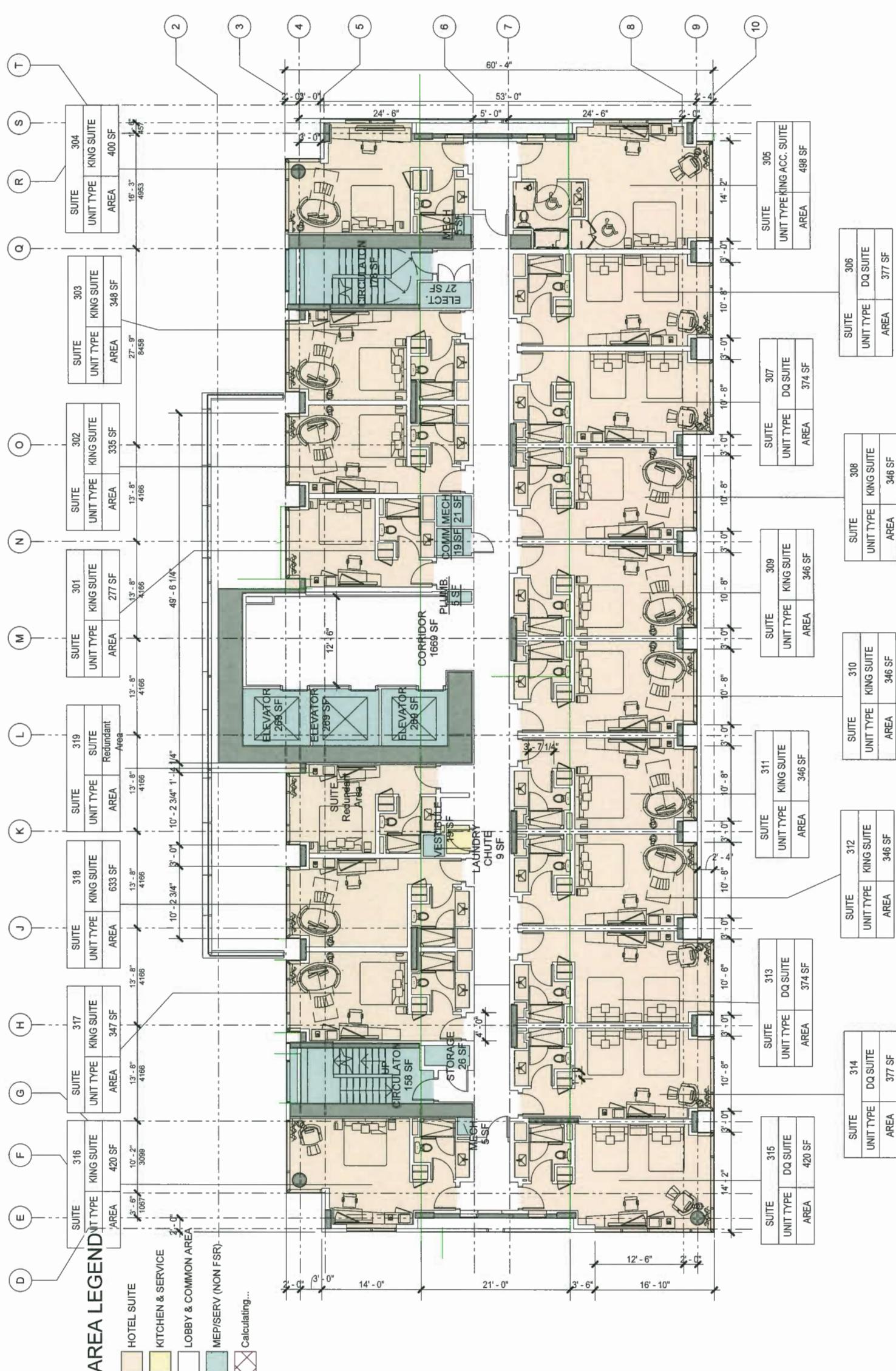


HOTEL 2 LEVEL 2 AREA PLAN A 33202

DP 18-825006 - 26

IBI

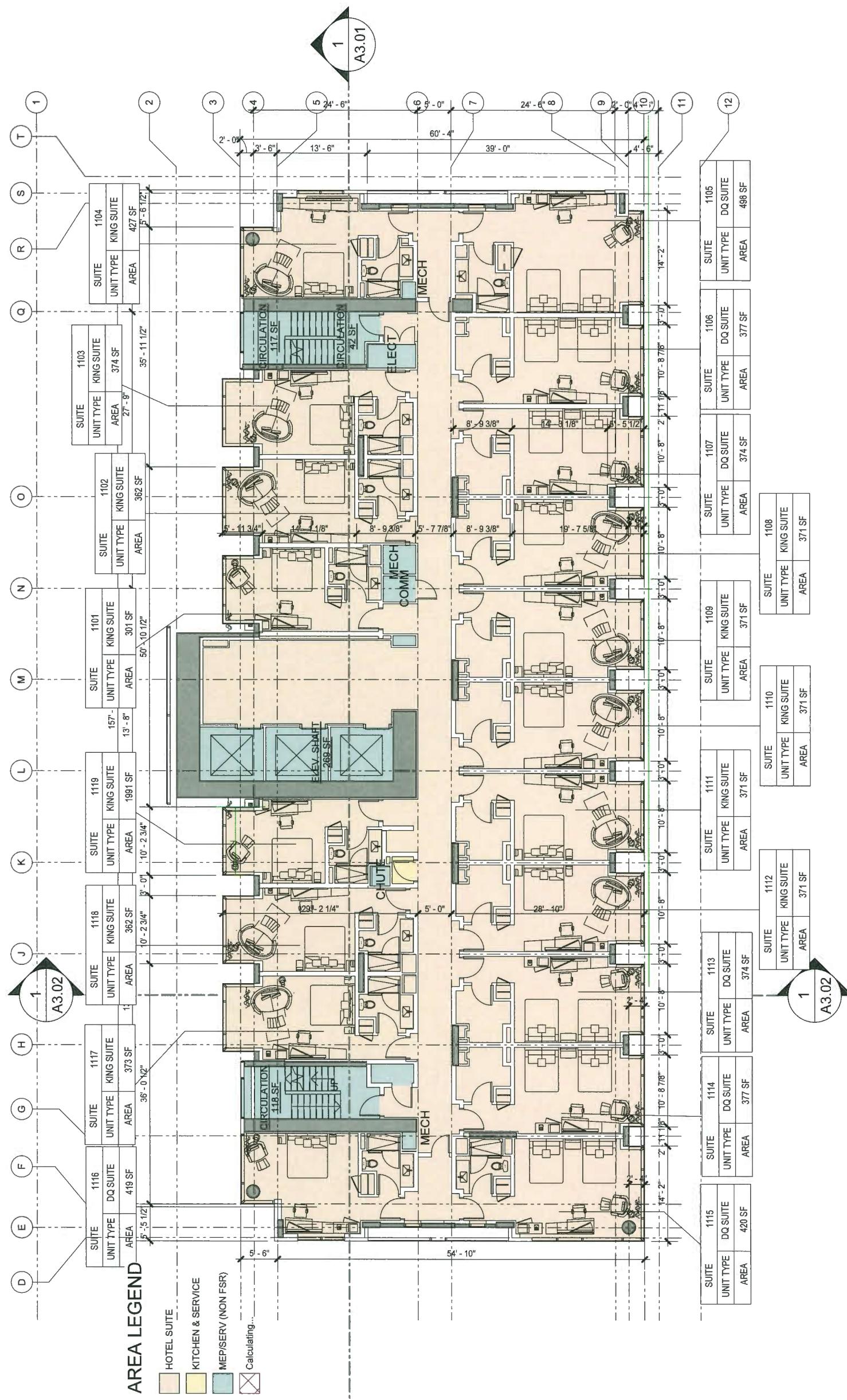
HOTEL 2



HOTEL 2 LEVEL 3-10 (TYP) AREA PLAN
A 3203
DP 18-825006-27

IBI

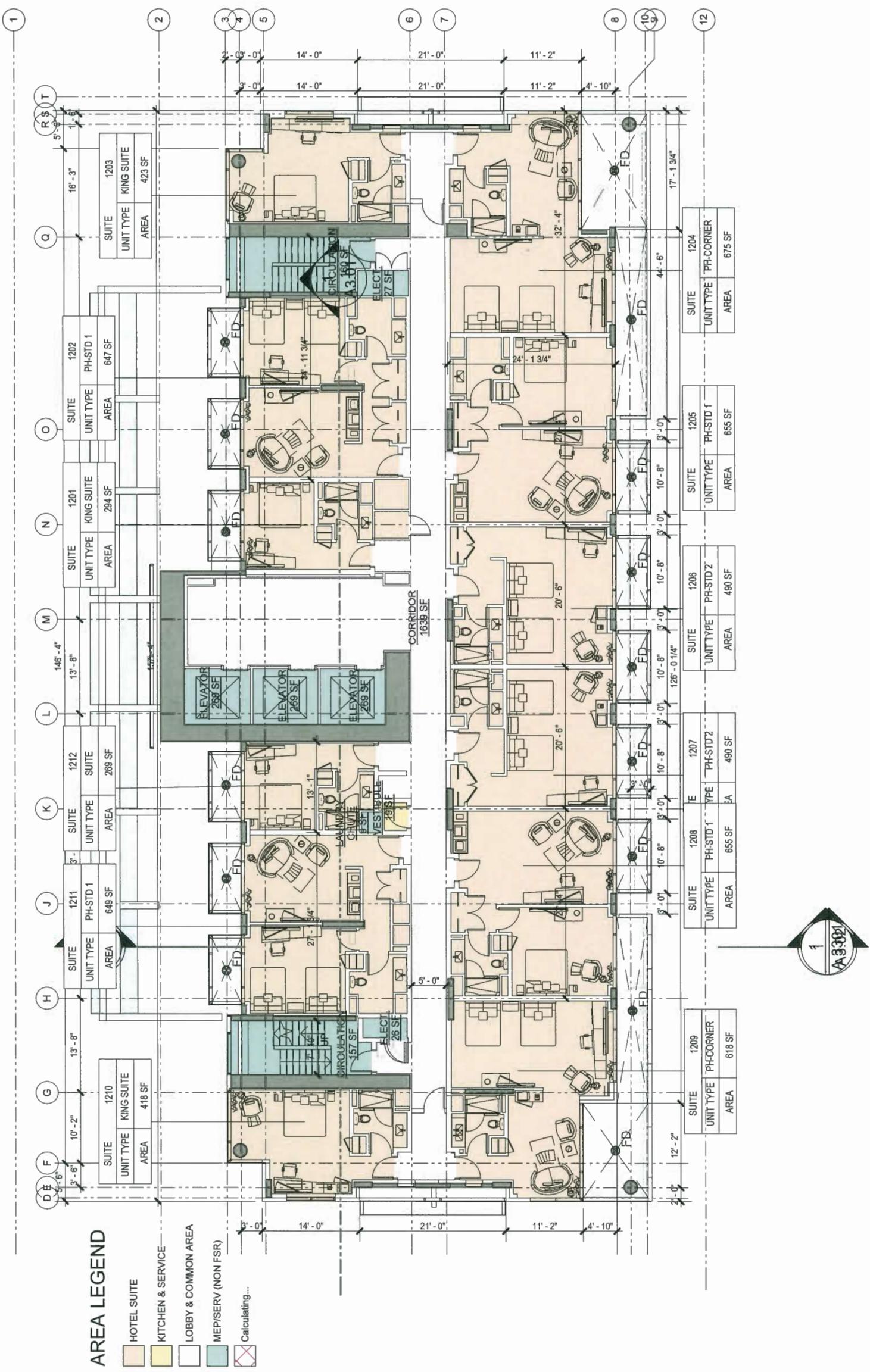
HOTEL 2



DP 18-825006-28
HOTEL 2 LEVEL 11 AREA PLAN A 3204

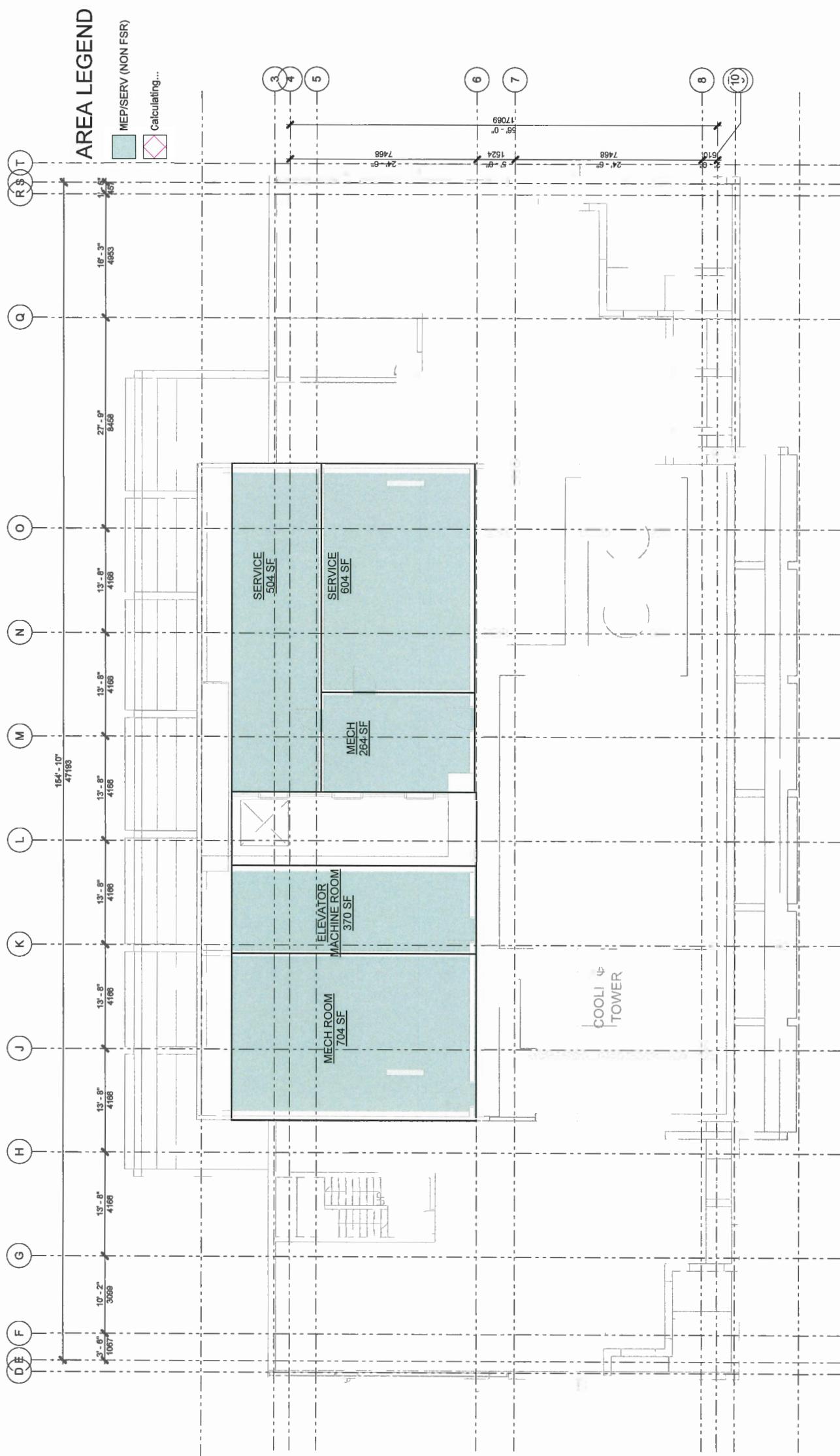
-B-

HOTEL 2



-
B
-

HOTEL 2

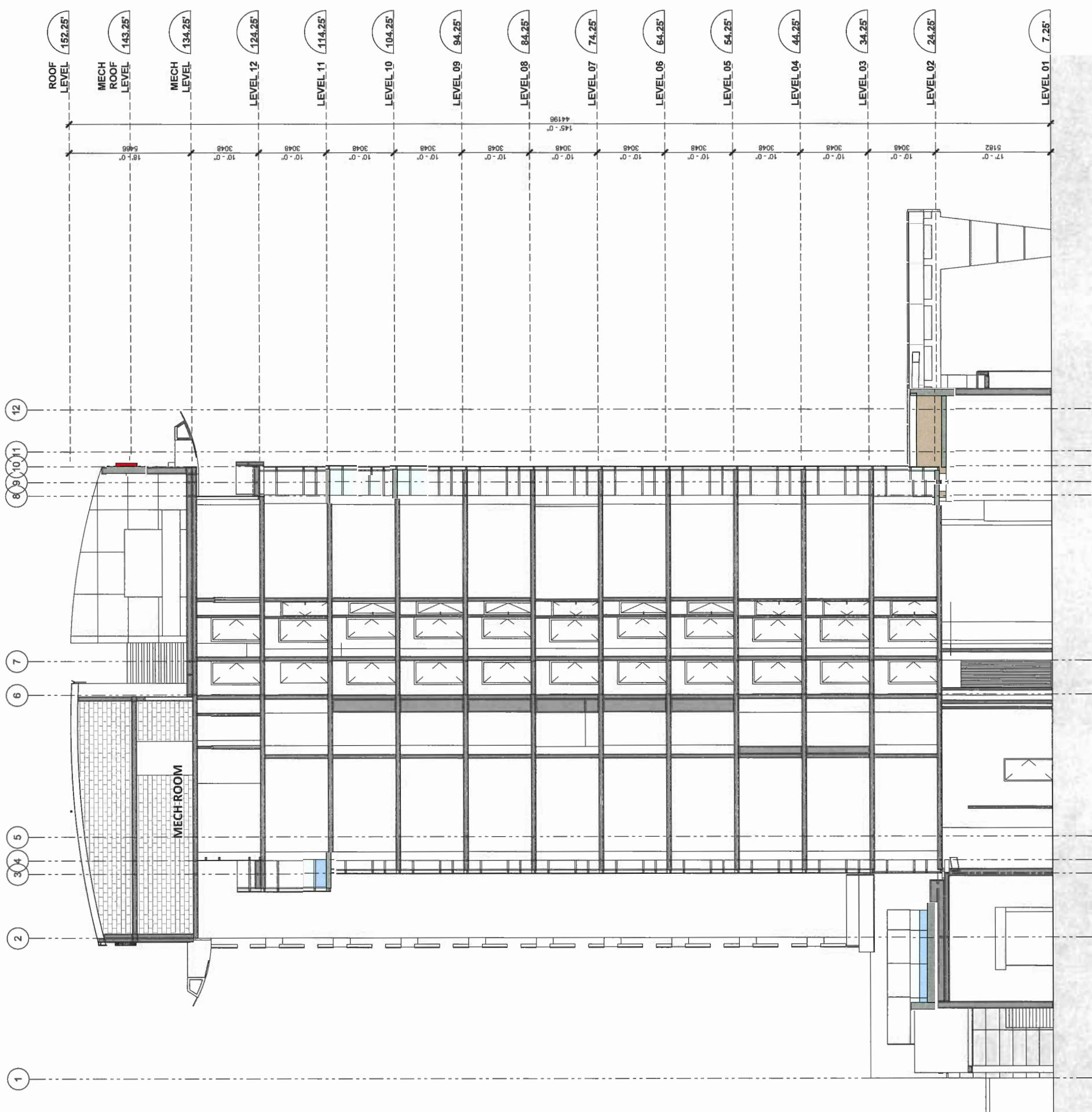


HOTEL 2 MECHANICAL LEVEL AREA PLAN

A 3206

IB 1

HOTEL 2



HOTEL 2 - Section B-B A 3301

Up 18-825006-31

IBI

9455 & 9533 BRIDGEPORT ROAD

A 5011



BECKWITH ROAD

PHASE 1

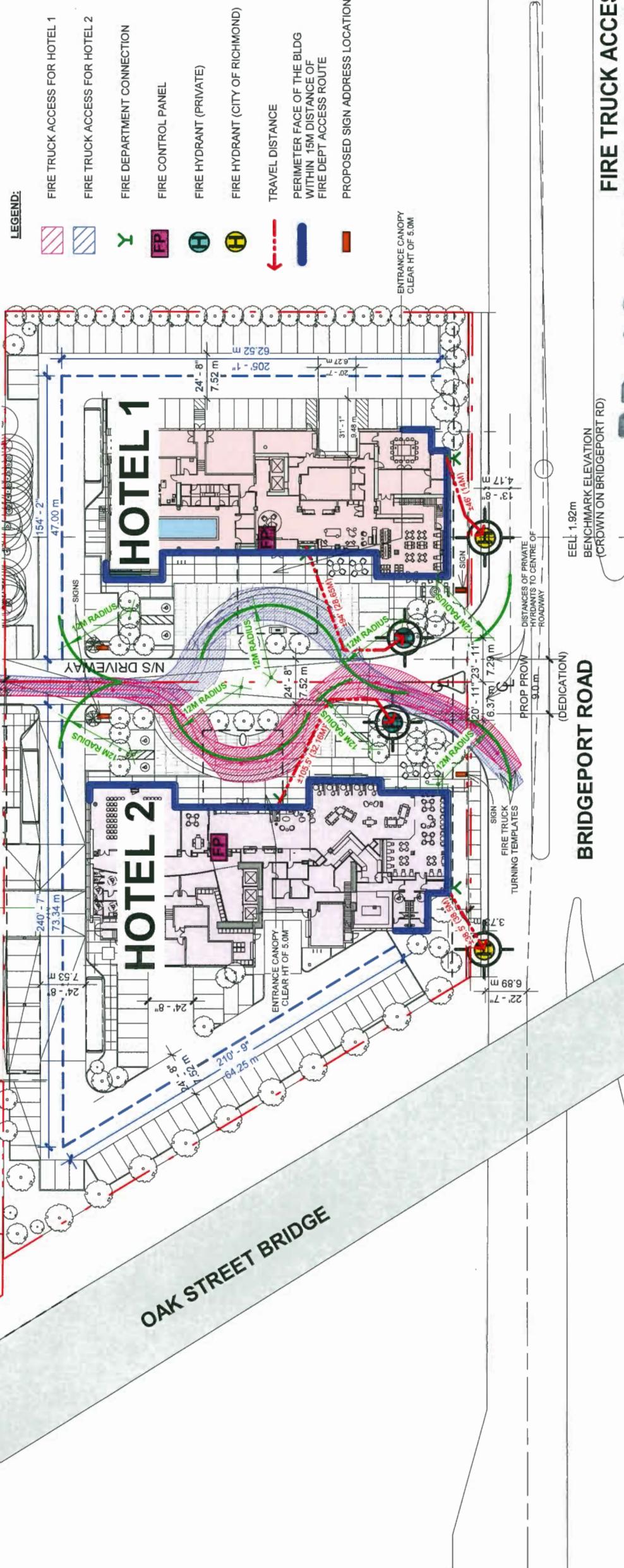
NOT PART OF THIS APPLICATION
SUBMITTED FOR BP (BP#20180315)

FUTURE
PHILLIPINE
COMMUNITY
CENTRE

PHASE 3

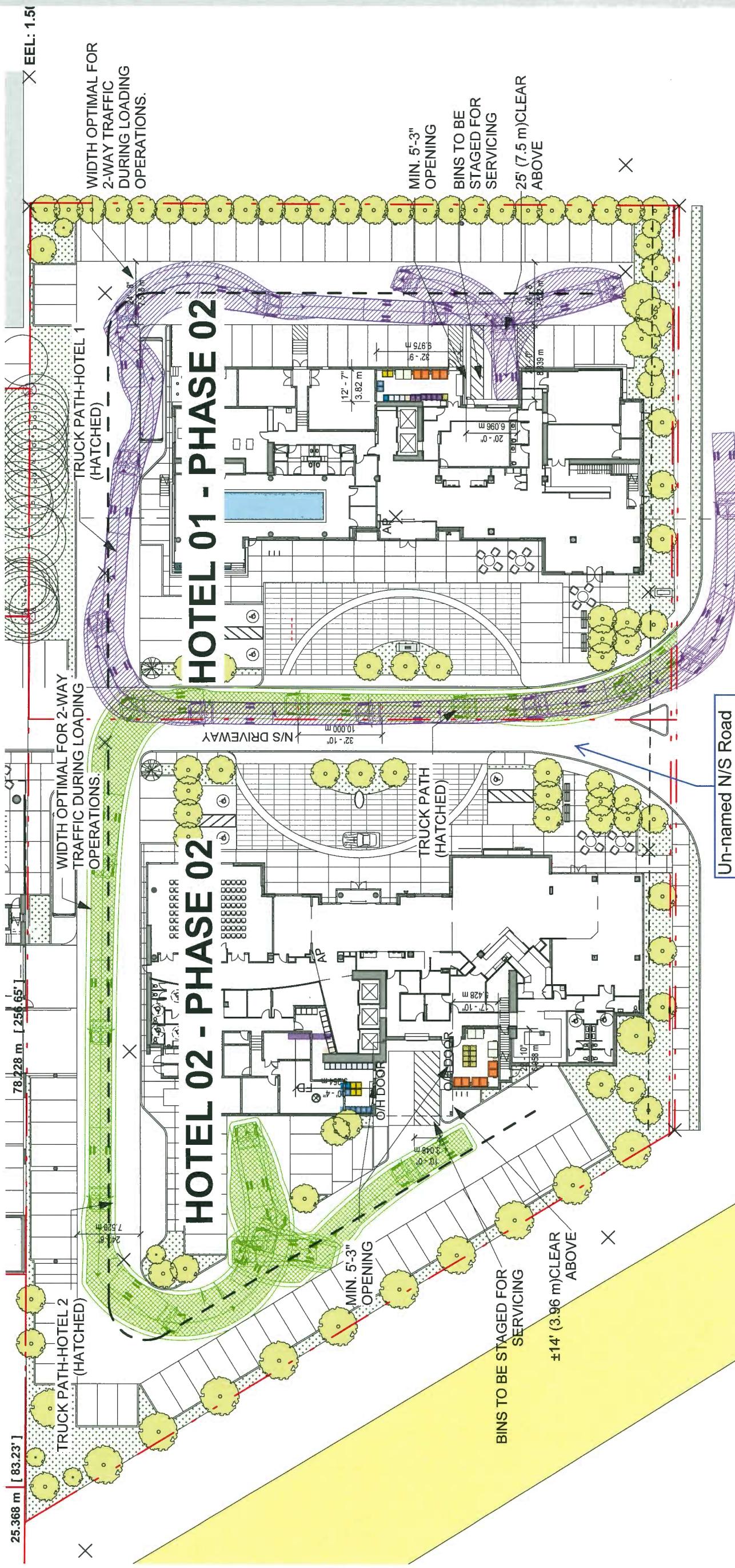
NOT PART OF THIS APPLICATION
SUBMITTED FOR REZON. (RZ#18 821103)

Un-named N/S Road
(street naming under
process)



IBI

9455 & 9533 BRIDGEPORT ROAD



| | Storage Area Required for One | Number of Bins | Total Space Required |
|--|-------------------------------|----------------------|------------------------|
| Commercial Mixed Containers | 1.26 m ² | 7 | 8.82 m ² |
| Commercial Refundable Beverage Containers | 0.97 m ² | 2 | 1.94 m ² |
| Commercial Mixed Paper (including newspaper) | 1.26 m ² | 9 | 11.34 m ² |
| Commercial Glass | 0.97 m ² | 2 | 1.94 m ² |
| Commercial Food Scraps, and Yard Trimmings | 0.97 m ² | 17 | 16.49 m ² |
| Commercial Cardboard | 4.88 m ² | 2 | 9.76 m ² |
| Commercial Garbage | 4.88 m ² | 3 | 14.64 m ² |
| Commercial Grease/Tallow | 0.12 m ² | 4 | 0.48 m ² |
| Total | | 65.41 m ² | 704.07 ft ² |

| | Storage Area Required for One | Number of Bins | Total Space Required |
|--|-------------------------------|----------------------|------------------------|
| Commercial Mixed Containers | 1.26 m ² | 2 | 2.52 m ² |
| Commercial Refundable Beverage Containers | 0.97 m ² | 1 | 0.97 m ² |
| Commercial Mixed Paper (including newspaper) | 1.26 m ² | 3 | 3.78 m ² |
| Commercial Glass | 0.97 m ² | 2 | 1.94 m ² |
| Commercial Food Scraps, and Yard Trimmings | 0.97 m ² | 6 | 5.82 m ² |
| Commercial Cardboard | 4.88 m ² | 1 | 4.88 m ² |
| Commercial Garbage | 4.88 m ² | 2 | 9.76 m ² |
| Commercial Grease/Tallow | 0.12 m ² | 2 | 0.24 m ² |
| Total | | 29.91 m ² | 321.95 ft ² |

WASTE MANAGEMENT PLAN

A 6000

DP 18-8250006-33

Bridgeport Hotel 1 and 2

Issued for Development Permit

Sheet List Table

| Contact Information | Other Key Contacts: | Sheet Number | Sheet Title |
|---|---|--------------|---|
| van der Zalm + associates Inc. Project Landscape Architecture Suite 1 - 20177 97th Avenue Langley, British Columbia, V1M 4B9 t. 604 546 0024 f. 604 882 0042 Primary project contact: Andrew Danielson andrew@vdz.ca o. 604 546 0931 Alternate contacts (incase away): Mark van der Zalm Principal Landscape Architect mark@vdz.ca o. 604 546 0920 | Chungwa Investment Co. Ltd. Project Owner Unit 1800 - 570 Granville Street Vancouver, BC V6C 3P1 p. 604 683 8797 Legal Address and Description: LOT 1 SEC 22 BLK SN RG 6W PL EPP72726 LOT 3 SEC 22 BLK SN RG 6W PL EPP72726 | L-01 | COVER SHEET |
| | | L-02 | TREE PRESERVATION PLAN |
| | | L-03 | OVERALL SITE AND TREE PLAN |
| | | L-04A | NORTH WEST LANDSCAPE PLAN |
| | | L-04B | SOUTH WEST LANDSCAPE PLAN |
| | | L-04C | NORTH CENTRAL LANDSCAPE PLAN |
| | | L-04D | SOUTH CENTRAL LANDSCAPE PLAN |
| | | L-04E | NORTH EAST LANDSCAPE PLAN |
| | | L-04F | SOUTH EAST LANDSCAPE PLAN |
| | | L-05A | NORTH WEST PLANTING PLAN |
| | | L-05B | SOUTH WEST PLANTING PLAN |
| | | L-05C | NORTH CENTRAL PLANTING PLAN |
| | | L-05D | SOUTH CENTRAL PLANTING PLAN |
| | | L-05E | NORTH EAST PLANTING PLAN |
| | | LN-01 | GENERAL NOTES |
| | | 6 | AD Issued for Development Permit Feb 1, 2019 |
| | | 5 | AD Issued for Development Permit Jan 29, 2019 |
| | | 4 | AD Issued for 50% BP Review Dec 21, 2018 |
| | | 3 | AD Issued for Development Permit Nov 26, 2018 |
| | | 2 | AD Issued for Development Permit June 4, 2018 |
| | | 1 | EW Issued for Review May 22, 2018 |

COVER SHEET

Drawing DWZ Project # DP2018-33



NORTH

DP2018-33

L-01

1 SITE PLAN OVERVIEW

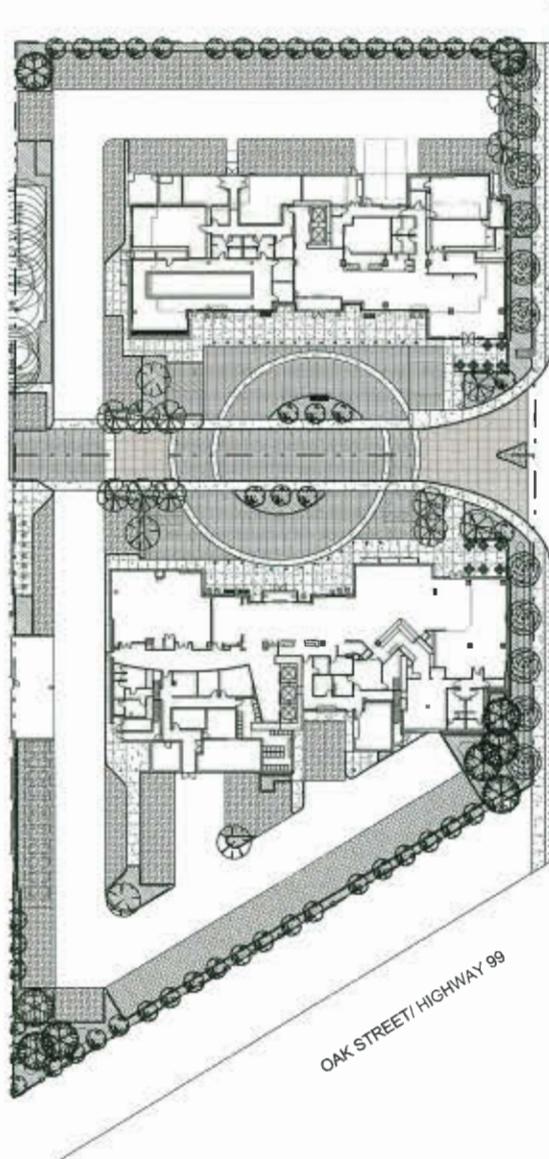
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2 LOCATION MAP

Drawing DWZ Project # DP2018-33

L-01

34



BRIDGEPORT ROAD

OAK STREET/ HIGHWAY 99

OAK STREET/ HIGHWAY 99

BRIDGEPORT ROAD

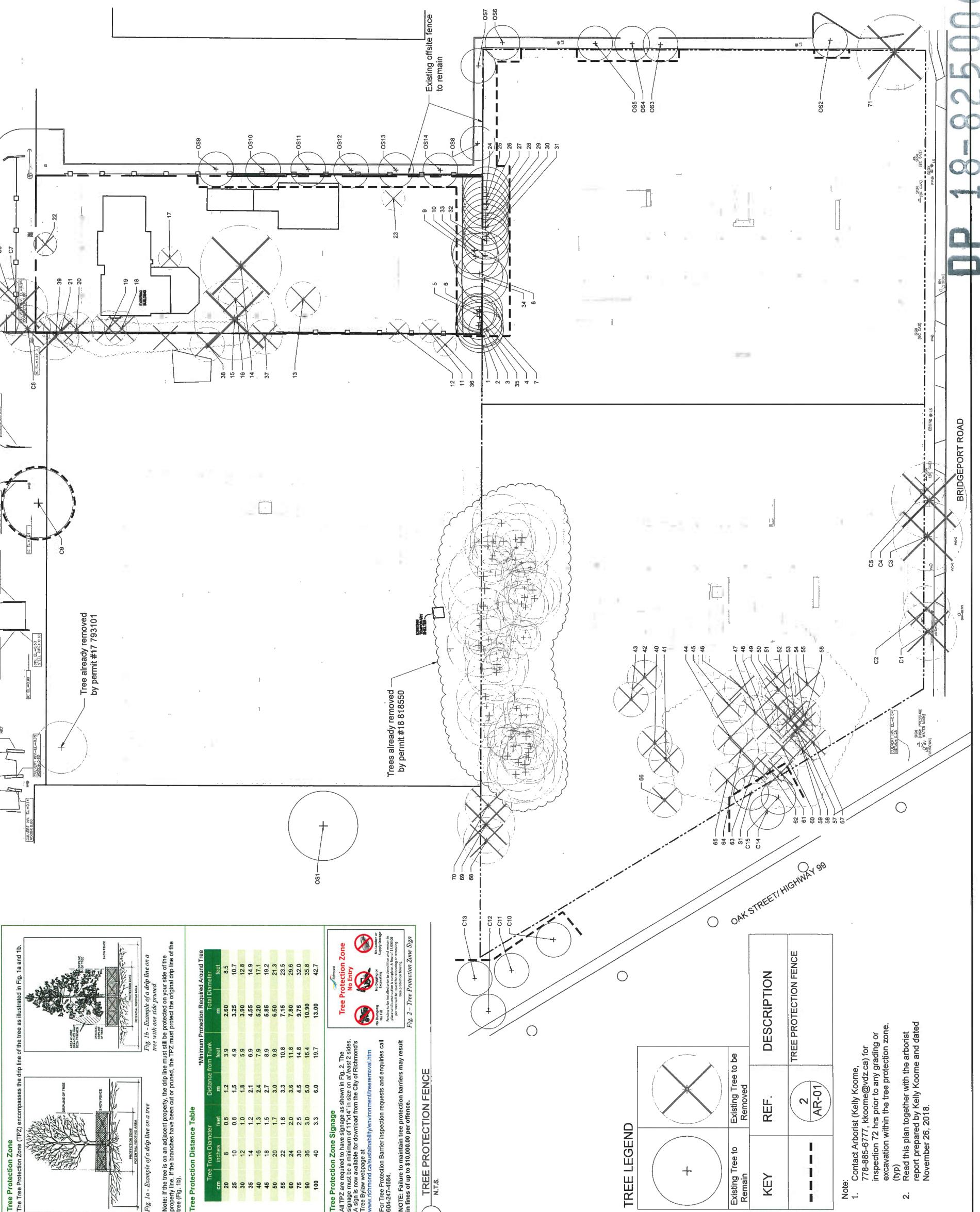


van der Zalm + associates Inc.
Parts & Berenson Civil Engineering
Urban Design + Landscape Architecture
Suite 1, 20177 77th Avenue
P. Box 10204
Delta, British Columbia
V4G 4S9
Info@vdz.ca

TREE PRESERVATION PLAN

DP2018-33 VDZ Project Title: Drawing Title:

L-02 Drawing #:





OVERALL SITE AND TREE PLAN

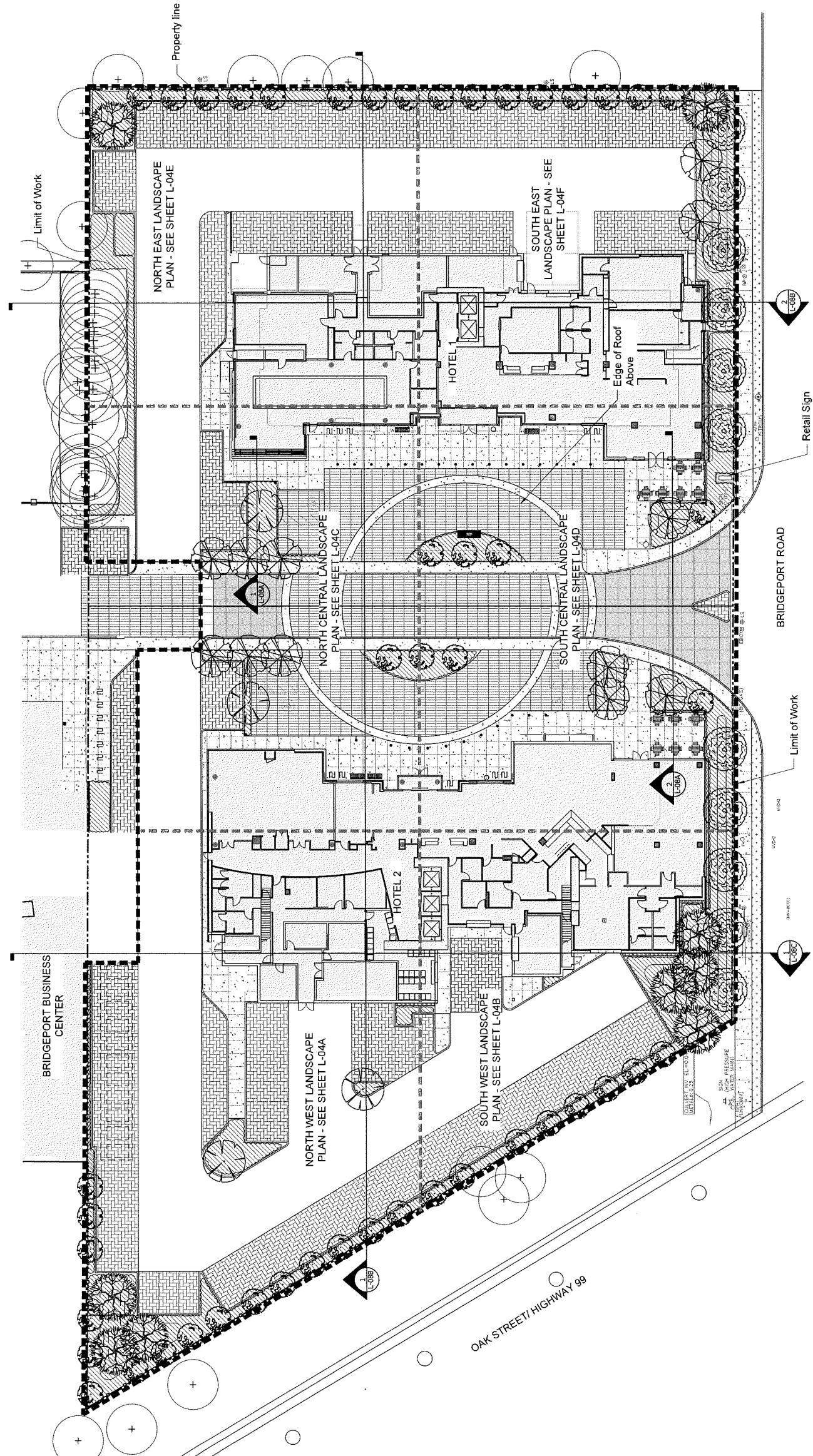
DP2018-33

L-03

VZD Project #:

Drawing Title:

van der Zalm + associates inc.
Plans & Renderings Civil Engineering
Urban Design • Landscape Architecture
Suite 1, 20177 78th Avenue | 604.682.0004
Langley, British Columbia | 604.682.0042
V1M 4B9 info@vdz.ca



TREE LEGEND

| | | | | |
|--|---------------------------------|-------------------------------------|-----------------------------|----------------------|
| | <i>Populus tremula 'Erecta'</i> | <i>Prunus x yedoensis 'Akibono'</i> | <i>Robinia pseudoacacia</i> | <i>Thuja plicata</i> |
| | <i>Acer rubrum</i> | <i>Red Maple</i> | <i>Red Maple</i> | |

| SYMBOL | BOTANICAL NAME | COMMON NAME | SIZE | SPACING |
|------------|-------------------------------------|--------------------|------------------------------|-----------|
| See Legend | <i>Acer rubrum</i> | Red Maple | B&B; 6cm cal.; 1.8m std. | Per Plant |
| See Legend | <i>Populus tremula 'Erecta'</i> | Silver Aspen | B&B; 6cm cal.; 3.0m ht. | Per Plant |
| See Legend | <i>Prunus x yedoensis 'Akibono'</i> | Daybreak Cherry | B&B; 6cm cal.; 1.2m std. | Per Plant |
| See Legend | <i>Robinia pseudoacacia</i> | Golden Leaf Locust | B&B; 6cm cal.; 1.2m std. | Per Plant |
| See Legend | <i>Thuja plicata</i> | Western Red Cedar | B&B; 5.0m ht.; Single leader | Per Plant |

| No. | By: | Description | Date |
|------------------------------|-----|-------------|------|
| REVISIONS TABLE FOR DRAWINGS | | | |

Original Sheet Size: 24" x 36"

Drawn: AD

Stamp:

Scale: 1:250

Checked: MVDZ

Approved: MVDZ

Comments: CONTRACTOR SHALL OBTAIN DIMENSIONS ON THE WORK AND REFER TO APPENDIX C ON THE CONSULTANT'S DRAWING FOR THE EXACT LOCATION OF THE PROPERTY LINE. THE OWNER AND EXCLUSIVE PROPERTY OF THE PROPERTY MUST BE NOTIFIED AT THE COMPLETION OF DRAWINGS AND APPROVAL. DRAWINGS ARE NOT TO BE REUSED OR COPIED. THIS SHEET IS DATED JUNE 2018 AND IS NOT TO BE USED AFTER THAT DATE. THIS SHEET IS DATED JUNE 2018 AND IS NOT TO BE USED AFTER THAT DATE.

Project: Bridgeport Hotel 1 and 2

Location:
9533 Bridgeport Rd & 9455
Bridgeport Rd

Drawing Title:

DP2018-33

VZD Project #:

Overall Site and Tree Plan

Drawing Title:

DP2018-33

VZD Project #:

Overall Site and Tree Plan

Drawing Title:

DP2018-33

VZD Project #:

Overall Site and Tree Plan

Drawing Title:

DP2018-33

VZD Project #:

Overall Site and Tree Plan

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VZD Project #:

Overall Site and Tree Plan

Drawing Title:

DP2018-33

VZD Project #:

Overall Site and Tree Plan

Drawing Title:

DP2018-33

VZD Project #:

Overall Site and Tree Plan

Drawing Title:

DP2018-33

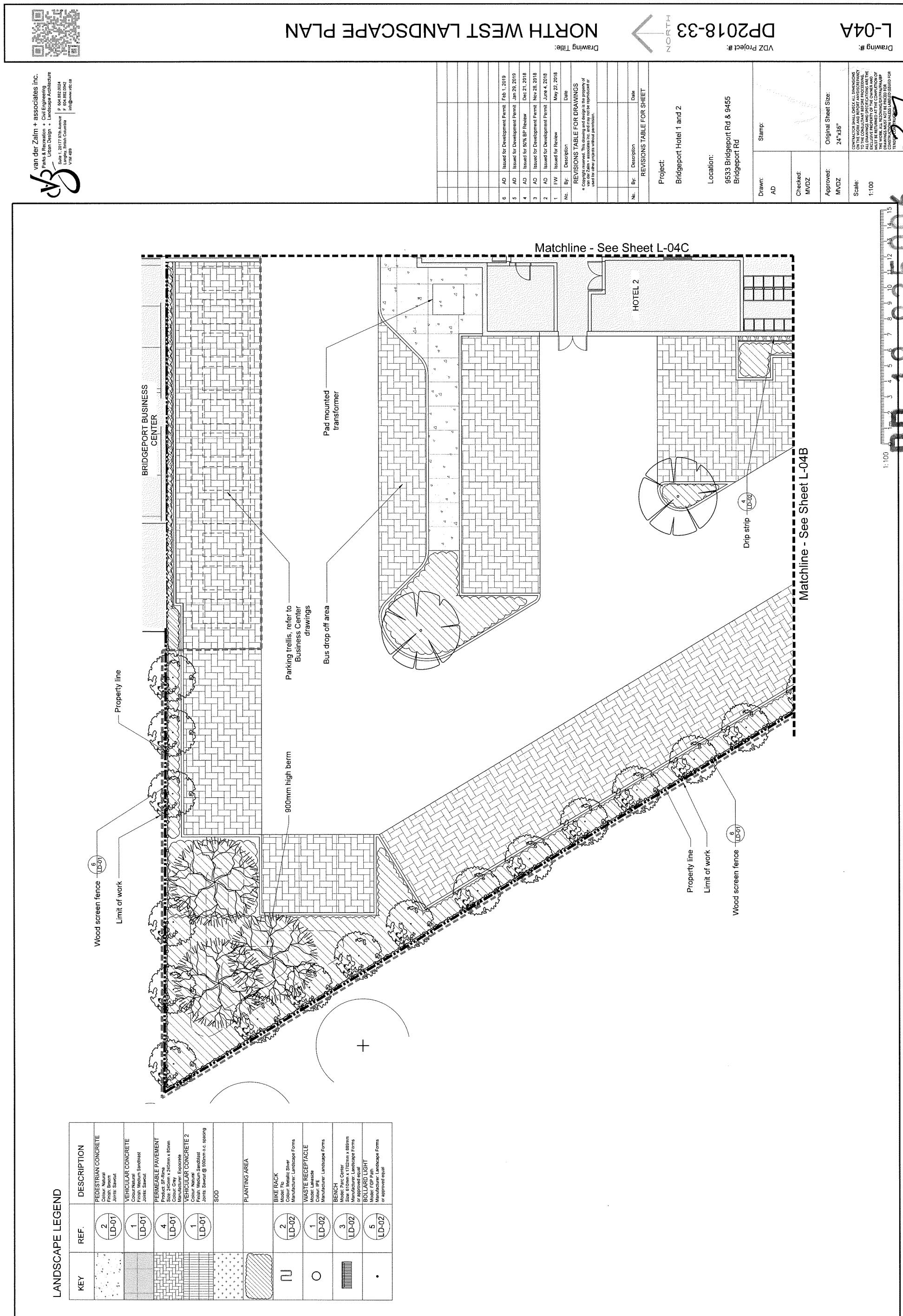
VZD Project #:

Overall Site and Tree Plan

Drawing Title:

DP2018-33

VZD Project #:



ANDSCAPE | EGEND



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SOUTH WEST LANDSCAPE PLAN

L-04B

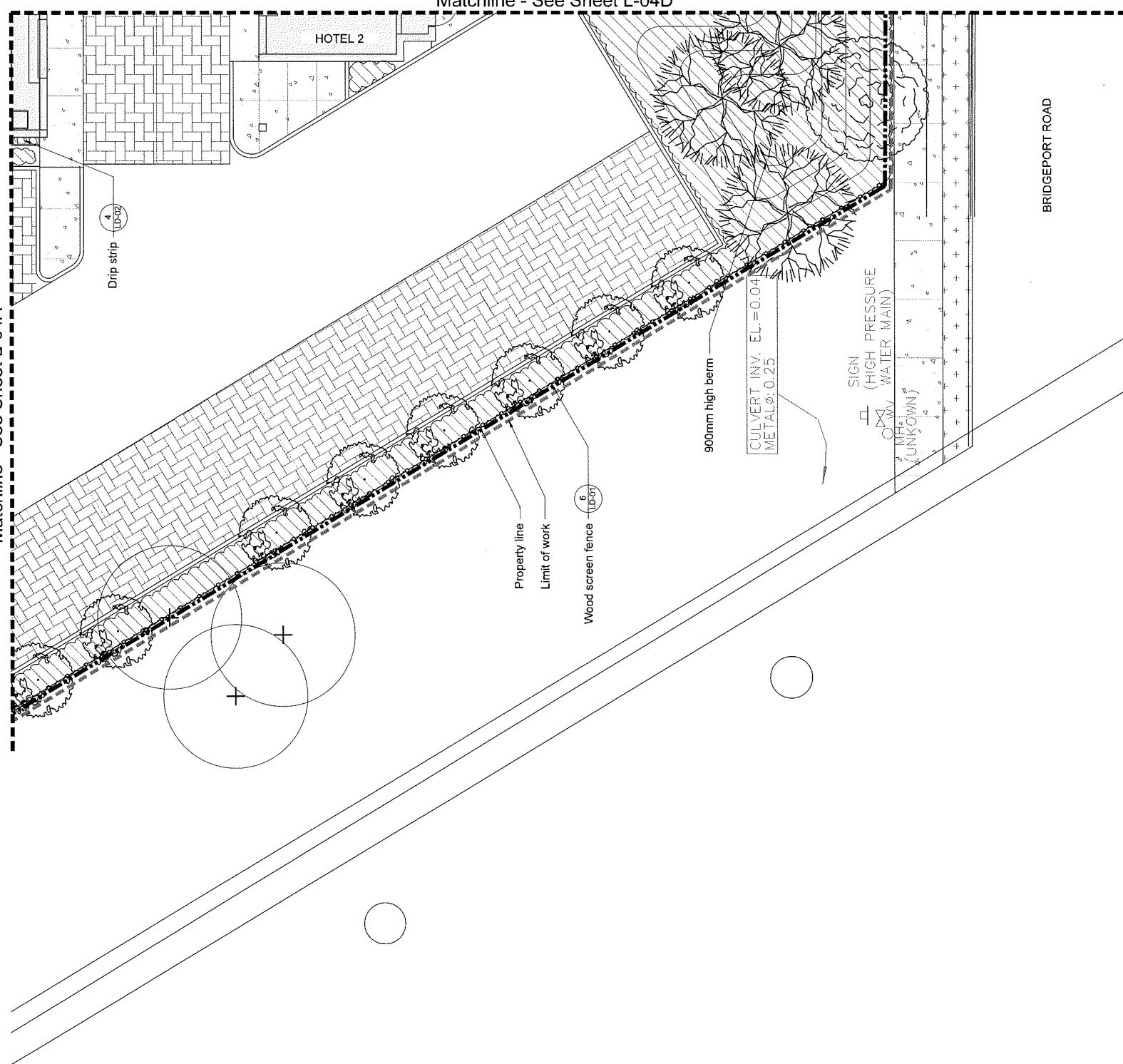
VDW Project #:

DP2018-33



Drawing Title:

Matchline - See Sheet L-04A



LANDSCAPE LEGEND

| KEY | REF. | DESCRIPTION |
|-----|-------|--|
| 1 | LD-01 | VEHICULAR CONCRETE Colour: Natural Finish: Medium Sandblast Joints: Sawcut |
| 2 | LD-01 | PEDESTRIAN CONCRETE Colour: Natural Finish: Medium Sandblast Joints: Sawcut |
| 3 | LD-01 | PERMEABLE PAVEMENT Product: ST Forms Size: 240mm x 240mm x 30mm Colour: Gray Manufacturer: Exopcrete |
| 4 | LD-01 | VEHICULAR CONCRETE 2 Colour: Natural Finish: Medium Sandblast Joints: Sawcut @ 50mm o.c. Spacing |
| 5 | LD-01 | SOD |
| 6 | LD-02 | PLANTING AREA |
| 7 | LD-02 | BIKE RACK |
| 8 | LD-02 | WASTE RECEPTACLE |
| 9 | LD-02 | BENCH |
| 10 | LD-02 | BOLLARD LIGHT |

Contractor shall check all dimensions
and locations of all structures and
all drawings and specifications and the
same shall be returned at the completion of
the work, all recognized parts and
material used in the construction of
the work, all labor and expense of
construction, unless otherwise provided
in the contract.

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6

AD Issued for Development Permit Feb. 1, 2019

5 AD Issued for Development Permit Jan. 19, 2019

4 AD Issued for 50% SP Review Dec. 21, 2018

3 AD Issued for Development Permit Nov. 28, 2018

2 AD Issued for Development Permit June 4, 2018

1 FW Issued for Review May 22, 2018

No. By Description Date

REVISIONS TABLE FOR SHEET

Project:

Bridgeport Hotel 1 and 2
Location:
9533 Bridgeport Rd & 9455
Bridgeport Rd

Drawn: AD Stamp: MVDZ

Checked: MVDZ

Approved: MVDZ

Original Sheet Size: 24" x 36"

CONTROLLER SHALL CHECK ALL DIMENSIONS
AND LOCATIONS OF ALL STRUCTURES AND
ALL DRAWINGS AND SPECIFICATIONS AND THE
SAME SHALL BE RETURNED AT THE COMPLETION OF
THE WORK, ALL RECOGNIZED PARTS AND
MATERIAL USED IN THE CONSTRUCTION OF
THE WORK, ALL LABOR AND EXPENSE OF
CONSTRUCTION, UNLESS OTHERWISE PROVIDED
IN THE CONTRACT.

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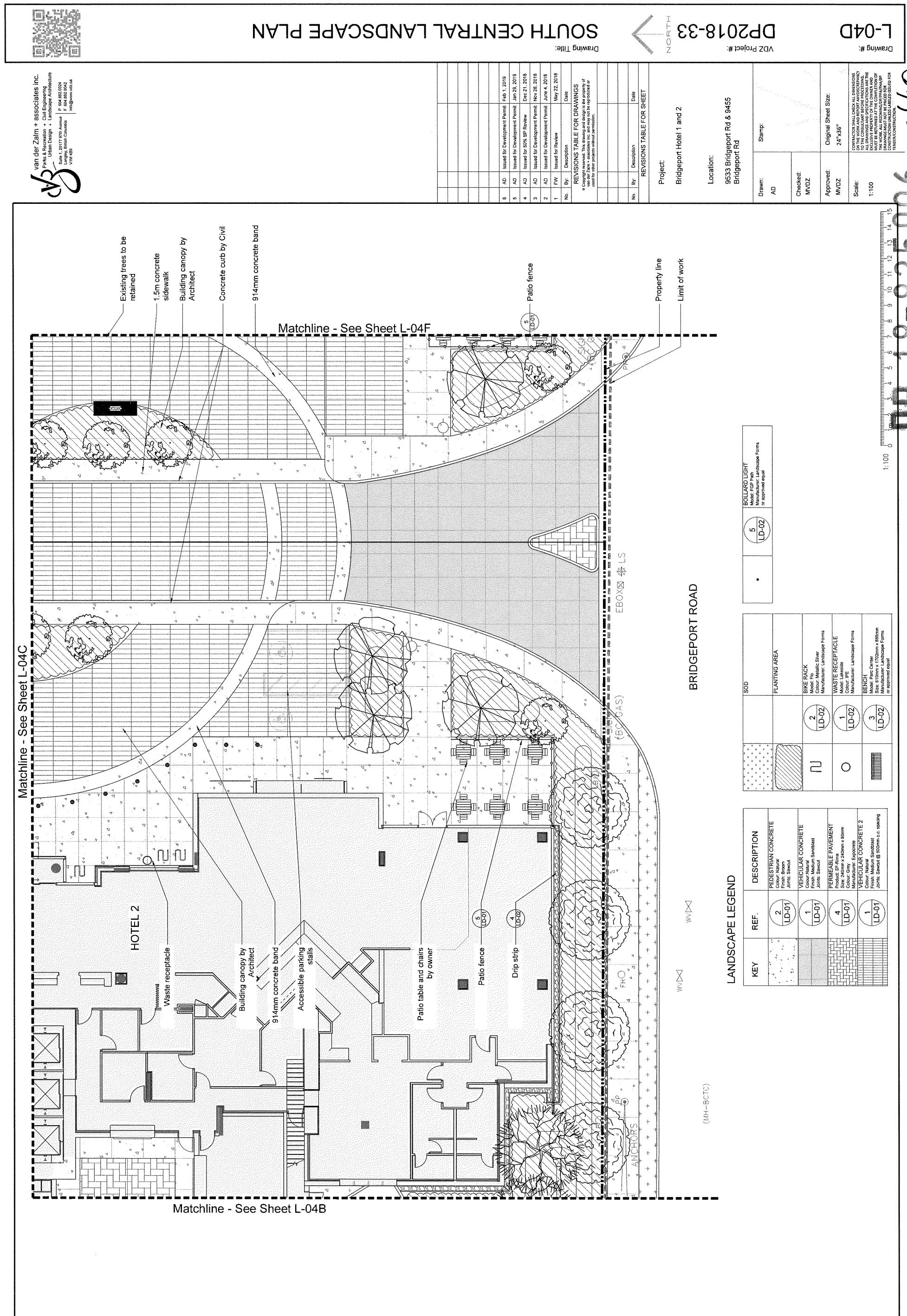
Approved: MVDZ

Original Sheet Size: 24" x 36"

Scale: 1:100

DP 18-92500

mm



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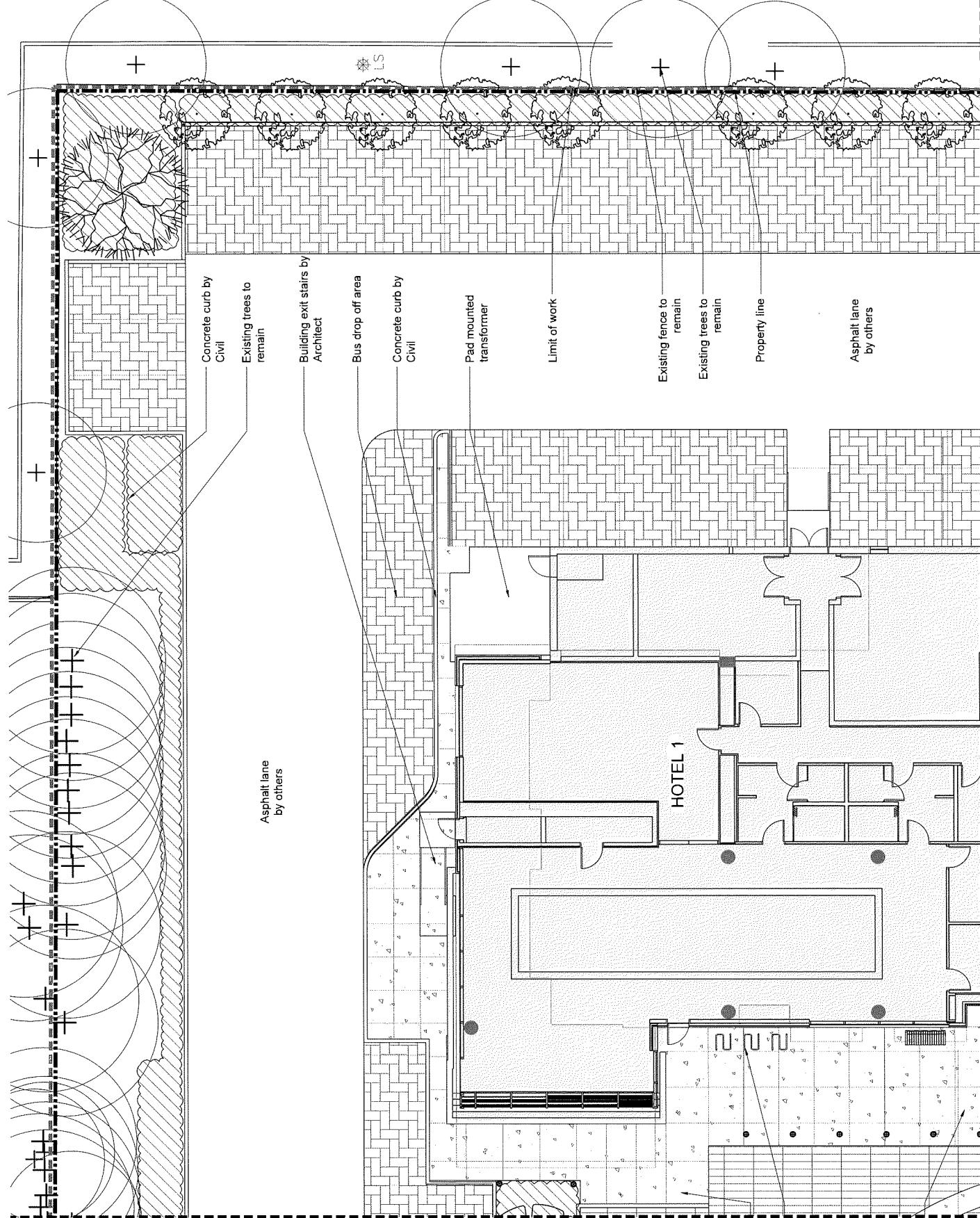
NORTH EAST LANDSCAPE PLAN

DP2018-33 Drawing Title: L-04E



LANDSCAPE LEGEND

| KEY | REF. | DESCRIPTION |
|-----|------------|---|
| | 2 LD-01 | PEDESTRIAN CONCRETE Colour: Natural Finish: Brown Joints: Smooth |
| | 1 LD-01 | VEHICULAR CONCRETE Colour: Natural Finish: Medium Sandblast Joints: Smooth |
| | 4 LD-01 | PERMEABLE PAVEMENT Product: S-Hardscape Size: 40mm x 240mm x 80mm Manufacturer: Exocrete Color: Grey Material: Recycled Concrete Joints: @ 50mm c.c. spacing |
| | 1 LD-01 | VEHICULAR CONCRETE 2 Colour: Natural Finish: Medium Sandblast Joints: Smooth @ 50mm c.c. spacing |
| | SOD | SOD |
| | | PLANTING AREA |
| | 2 LD-02 | BIKE RACK Model: Ho Description: River Manufacturer: Landscape Forms |
| | 1 LD-02 | WASTE RECEPTACLE Model: Lakeside Colour: IPE |
| | 3 LD-02 | BENCH Model: Park Bench Size: 2000 x 1700mm x 85mm or approved equal |
| | 5 LD-02 | BOLLARD LIGHT Model: FGP Path or approved equal |
| | | (5) bike rack |
| | | Accessible parking stall |
| | | Building canopy by Architect |



Matchline - See Sheet L-04C

Matchline - See Sheet L-04F

1:150 15 16.5 18 19.5 21 22.5 24 25.5 27 28.5 30 31.5 33 34.5 36 37.5 39 41 42.5 44 45.5 47 48.5 50 51.5 53 54.5 56 57.5 59 61 62.5 64 65.5 67 68.5 70 71.5 73 74.5 76 77.5 79 81 82.5 84 85.5 87 88.5 90 91.5 93 94.5 96 97.5 99 101 103 105 107 109 111 113 115 117 119 121 123 125 127 129 131 133 135 137 139 141 143 145 147 149 151 153 155 157 159 161 163 165 167 169 171 173 175 177 179 181 183 185 187 189 191 193 195 197 199 201 203 205 207 209 211 213 215 217 219 221 223 225 227 229 231 233 235 237 239 241 243 245 247 249 251 253 255 257 259 261 263 265 267 269 271 273 275 277 279 281 283 285 287 289 291 293 295 297 299 301 303 305 307 309 311 313 315 317 319 321 323 325 327 329 331 333 335 337 339 341 343 345 347 349 351 353 355 357 359 361 363 365 367 369 371 373 375 377 379 381 383 385 387 389 391 393 395 397 399 401 403 405 407 409 411 413 415 417 419 421 423 425 427 429 431 433 435 437 439 441 443 445 447 449 451 453 455 457 459 461 463 465 467 469 471 473 475 477 479 481 483 485 487 489 491 493 495 497 499 501 503 505 507 509 511 513 515 517 519 521 523 525 527 529 531 533 535 537 539 541 543 545 547 549 551 553 555 557 559 561 563 565 567 569 571 573 575 577 579 581 583 585 587 589 591 593 595 597 599 601 603 605 607 609 611 613 615 617 619 621 623 625 627 629 631 633 635 637 639 641 643 645 647 649 651 653 655 657 659 661 663 665 667 669 671 673 675 677 679 681 683 685 687 689 691 693 695 697 699 701 703 705 707 709 711 713 715 717 719 721 723 725 727 729 731 733 735 737 739 741 743 745 747 749 751 753 755 757 759 761 763 765 767 769 771 773 775 777 779 781 783 785 787 789 791 793 795 797 799 801 803 805 807 809 811 813 815 817 819 821 823 825 827 829 831 833 835 837 839 841 843 845 847 849 851 853 855 857 859 861 863 865 867 869 871 873 875 877 879 881 883 885 887 889 891 893 895 897 899 901 903 905 907 909 911 913 915 917 919 921 923 925 927 929 931 933 935 937 939 941 943 945 947 949 951 953 955 957 959 961 963 965 967 969 971 973 975 977 979 981 983 985 987 989 991 993 995 997 999 1001 1003 1005 1007 1009 1011 1013 1015 1017 1019 1021 1023 1025 1027 1029 1031 1033 1035 1037 1039 1041 1043 1045 1047 1049 1051 1053 1055 1057 1059 1061 1063 1065 1067 1069 1071 1073 1075 1077 1079 1081 1083 1085 1087 1089 1091 1093 1095 1097 1099 1101 1103 1105 1107 1109 1111 1113 1115 1117 1119 1121 1123 1125 1127 1129 1131 1133 1135 1137 1139 1141 1143 1145 1147 1149 1151 1153 1155 1157 1159 1161 1163 1165 1167 1169 1171 1173 1175 1177 1179 1181 1183 1185 1187 1189 1191 1193 1195 1197 1199 1201 1203 1205 1207 1209 1211 1213 1215 1217 1219 1221 1223 1225 1227 1229 1231 1233 1235 1237 1239 1241 1243 1245 1247 1249 1251 1253 1255 1257 1259 1261 1263 1265 1267 1269 1271 1273 1275 1277 1279 1281 1283 1285 1287 1289 1291 1293 1295 1297 1299 1301 1303 1305 1307 1309 1311 1313 1315 1317 1319 1321 1323 1325 1327 1329 1331 1333 1335 1337 1339 1341 1343 1345 1347 1349 1351 1353 1355 1357 1359 1361 1363 1365 1367 1369 1371 1373 1375 1377 1379 1381 1383 1385 1387 1389 1391 1393 1395 1397 1399 1401 1403 1405 1407 1409 1411 1413 1415 1417 1419 1421 1423 1425 1427 1429 1431 1433 1435 1437 1439 1441 1443 1445 1447 1449 1451 1453 1455 1457 1459 1461 1463 1465 1467 1469 1471 1473 1475 1477 1479 1481 1483 1485 1487 1489 1491 1493 1495 1497 1499 1501 1503 1505 1507 1509 1511 1513 1515 1517 1519 1521 1523 1525 1527 1529 1531 1533 1535 1537 1539 1541 1543 1545 1547 1549 1551 1553 1555 1557 1559 1561 1563 1565 1567 1569 1571 1573 1575 1577 1579 1581 1583 1585 1587 1589 1591 1593 1595 1597 1599 1601 1603 1605 1607 1609 1611 1613 1615 1617 1619 1621 1623 1625 1627 1629 1631 1633 1635 1637 1639 1641 1643 1645 1647 1649 1651 1653 1655 1657 1659 1661 1663 1665 1667 1669 1671 1673 1675 1677 1679 1681 1683 1685 1687 1689 1691 1693 1695 1697 1699 1701 1703 1705 1707 1709 1711 1713 1715 1717 1719 1721 1723 1725 1727 1729 1731 1733 1735 1737 1739 1741 1743 1745 1747 1749 1751 1753 1755 1757 1759 1761 1763 1765 1767 1769 1771 1773 1775 1777 1779 1781 1783 1785 1787 1789 1791 1793 1795 1797 1799 1801 1803 1805 1807 1809 1811 1813 1815 1817 1819 1821 1823 1825 1827 1829 1831 1833 1835 1837 1839 1841 1843 1845 1847 1849 1851 1853 1855 1857 1859 1861 1863 1865 1867 1869 1871 1873 1875 1877 1879 1881 1883 1885 1887 1889 1891 1893 1895 1897 1899 1901 1903 1905 1907 1909 1911 1913 1915 1917 1919 1921 1923 1925 1927 1929 1931 1933 1935 1937 1939 1941 1943 1945 1947 1949 1951 1953 1955 1957 1959 1961 1963 1965 1967 1969 1971 1973 1975 1977 1979 1981 1983 1985 1987 1989 1991 1993 1995 1997 1999 2001 2003 2005 2007 2009 2011 2013 2015 2017 2019 2021 2023 2025 2027 2029 2031 2033 2035 2037 2039 2041 2043 2045 2047 2049 2051 2053 2055 2057 2059 2061 2063 2065 2067 2069 2071 2073 2075 2077 2079 2081 2083 2085 2087 2089 2091 2093 2095 2097 2099 2101 2103 2105 2107 2109 2111 2113 2115 2117 2119 2121 2123 2125 2127 2129 2131 2133 2135 2137 2139 2141 2143 2145 2147 2149 2151 2153 2155 2157 2159 2161 2163 2165 2167 2169 2171 2173 2175 2177 2179 2181 2183 2185 2187 2189 2191 2193 2195 2197 2199 2201 2203 2205 2207 2209 2211 2213 2215 2217 2219 2221 2223 2225 2227 2229 2231 2233 2235 2237 2239 2241 2243 2245 2247 2249 2251 2253 2255 2257 2259 2261 2263 2265 2267 2269 2271 2273 2275 2277 2279 2281 2283 2285 2287 2289 2291 2293 2295 2297 2299 2301 2303 2305 2307 2309 2311 2313 2315 2317 2319 2321 2323 2325 2327 2329 2331 2333 2335 2337 2339 2341 2343 2345 2347 2349 2351 2353 2355 2357 2359 2361 2363 2365 2367 2369 2371 2373 2375 2377 2379 2381 2383 2385 2387 2389 2391 2393 2395 2397 2399 2401 2403 2405 2407 2409 2411 2413 2415 2417 2419 2421 2423 2425 2427 2429 2431 2433 2435 2437 2439 2441 2443 2445 2447 2449 2451 2453 2455 2457 2459 2461 2463 2465 2467 2469 2471 2473 2475 2477 2479 2481 2483 2485 2487 248



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SOUTH WEST PLANTING PLAN

Drawing Title:

DP2018-33

L-05B

Drawing #:

Matchline - See Sheet L-05A

| PLANT SCHEDULE | | | |
|----------------|--|-------|-------|
| | BOTANICAL NAME / COMMON NAME | CONT | CONT |
| ITEM # | QTY | HRBBS | HRBBS |
| b | <i>Abies kawakamiae</i> / Nana / Dwarf Balsam Fir | #2 | Pot |
| i | 119 <i>Azalea japonica</i> 'Gumpo Pink' / Dwarf Pink Azalea | #2 | Pot |
| s | 10 <i>Corus sinensis</i> / Red Twig Dogwood | #2 | Pot |
| a | 117 <i>Euonymus stans</i> / Rudy Haag / Rudy Haag Burning Bush | #2 | Pot |
| s | 363 <i>Festuca glauca</i> 'Elijah Blue' / Blue Fescue | #1 | |
| a | 329 <i>Gaultheria shallon</i> / Salal | #1 | |
| In | 184 <i>Lavandula angustifolia</i> / Hidcote Blue / Lavender | #1 | |
| s | 75 <i>Mahonia nervosa</i> / Oregon Grape | #2 | |
| d | 75 <i>Micanthus sinensis</i> 'Morning Light' / Eulalia Grass | #2 | Pot |
| c | 217 <i>Nandina domestica</i> / Fire Power / Firepower Nandina | #2 | Pot |
| | 45 <i>Spirea x bumalda</i> / Crispa / Crisp Leaf Spirea | #2 | Pot |
| | 1,087 <i>Spiraea tenueissima</i> / Firetempest Needlefingers | #1 | |
| a | 55 <i>Symplocarpus albus</i> / Common White Strawberry | #3 | |
| o | 111 <i>Tilia cordata</i> / Branden / Branden Aboretum | #2 | Pot |
| o | 10 <i>Vaccinium ovalifolium</i> / Evergreen Huckleberry | #2 | Pot |

1. All plant material shall meet or exceed standards required by Canadian Landscape Standards guidelines.
2. Topsoil supplied shall be from a reputable source. A full analysis of the growing medium will be required at the contractor's expense, submit to landscape consultant for approval.
3. Amend growing medium per soil analysis recommendations prior to spreading on site.
4. Rejected growing medium shall be removed off site immediately at the landscape contractors expense.
5. Growing medium depths for planting as follows:
 - a. Shrubs: 450mm
 - b. Sod: 150mm
 - c. Tree pits: 1000mm with 300mm (below root ball) 25mm minus compost mulch to be installed in all shrub planting areas.

Matchline - See Sheet L-05D

HOTEL 2

Property line

Limit of work

**CULVERT INV. EL. = 0.04
METAL Ø 0.25**

**SIGN HIGH PRESSURE
(HIGH WATER MAIN)
MH
(UNKNOWN)**

BRIDGEPORT ROAD

adian Landscape

**advice of the growing medium
consultant for approval.
to spreading on site.
at the landscape
areas.**

Location: 2533 5th St. S.E.

17

Original Sheet

24 X 30

CONTRACTOR SHALL CHECK
ON THE WORK AND REPO-
RT TO THE CONSULTANT BEF-
ORE COMMENCING CON-
STRUCTION. ALL DRAWINGS AND
SPECIFICATIONS ARE THE
PROPERTY OF THE CONSUL-
TANT AND MUST NOT BE
MADE PUBLIC OR COPIED
BY CONTRACTOR. ALL DRAWINGS
MUST BE RETURNED AT THE
END OF THE CONTRACT. THE
CONSULTANT IS NOT RESPONSIBLE
FOR THE COST OF CON-
STRUCTION UNLESS AS
STATED IN THE CONTRACT.

sale:

1100

1100 mm 2 3 4 5 6 7 8 9 10 11 12 13 14 15

1:106



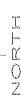
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Edmonton, Alberta, Canada
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NORTH CENTRAL PLANTING PLAN

DP2018-33

L-05C

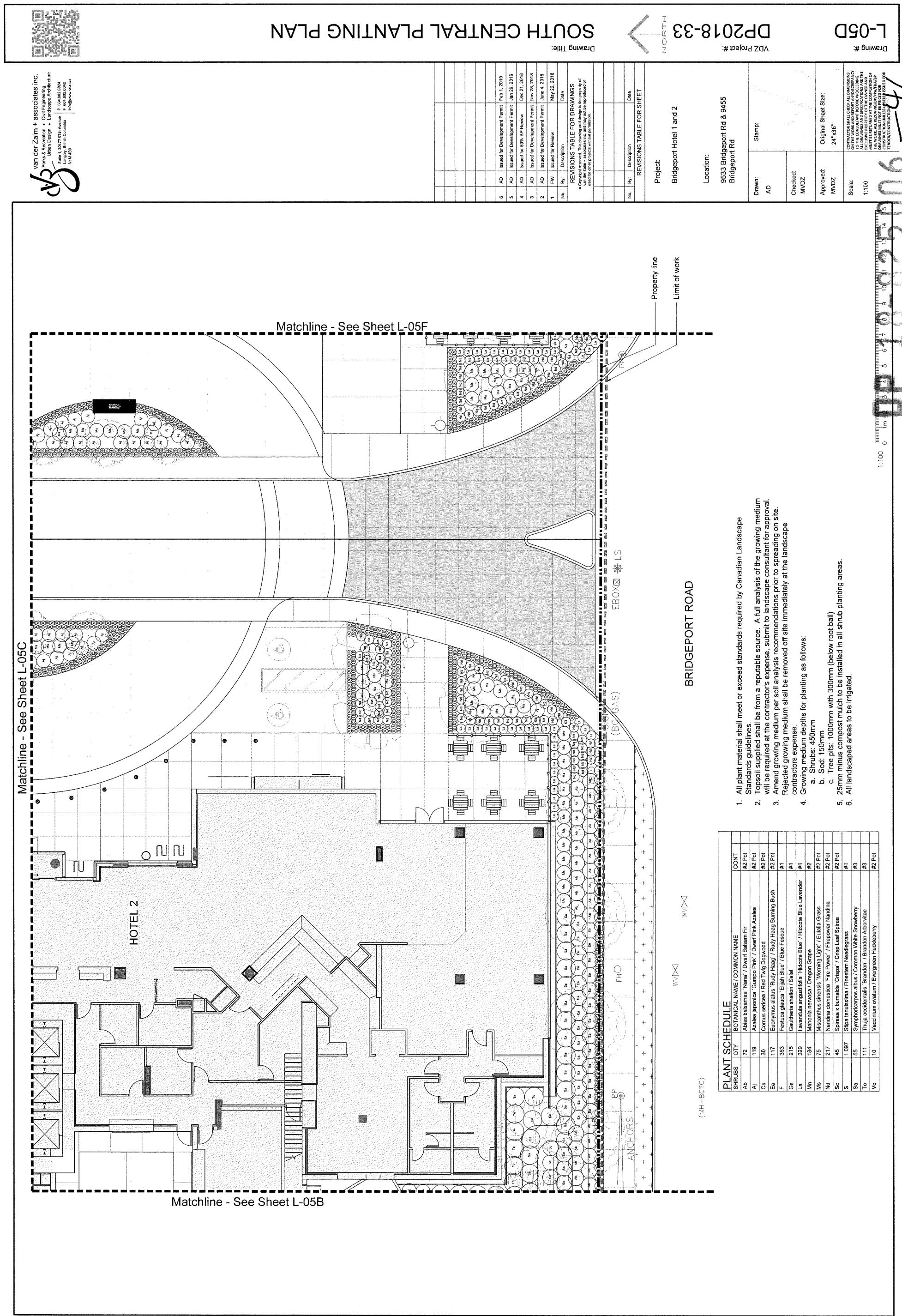
Drawing Title:



DP2018-33

Drawing Title:

DP2018-



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Matchline - See Sheet L-05C

Matchline - See Sheet I-05B



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SOUTH EAST PLANTING PLAN

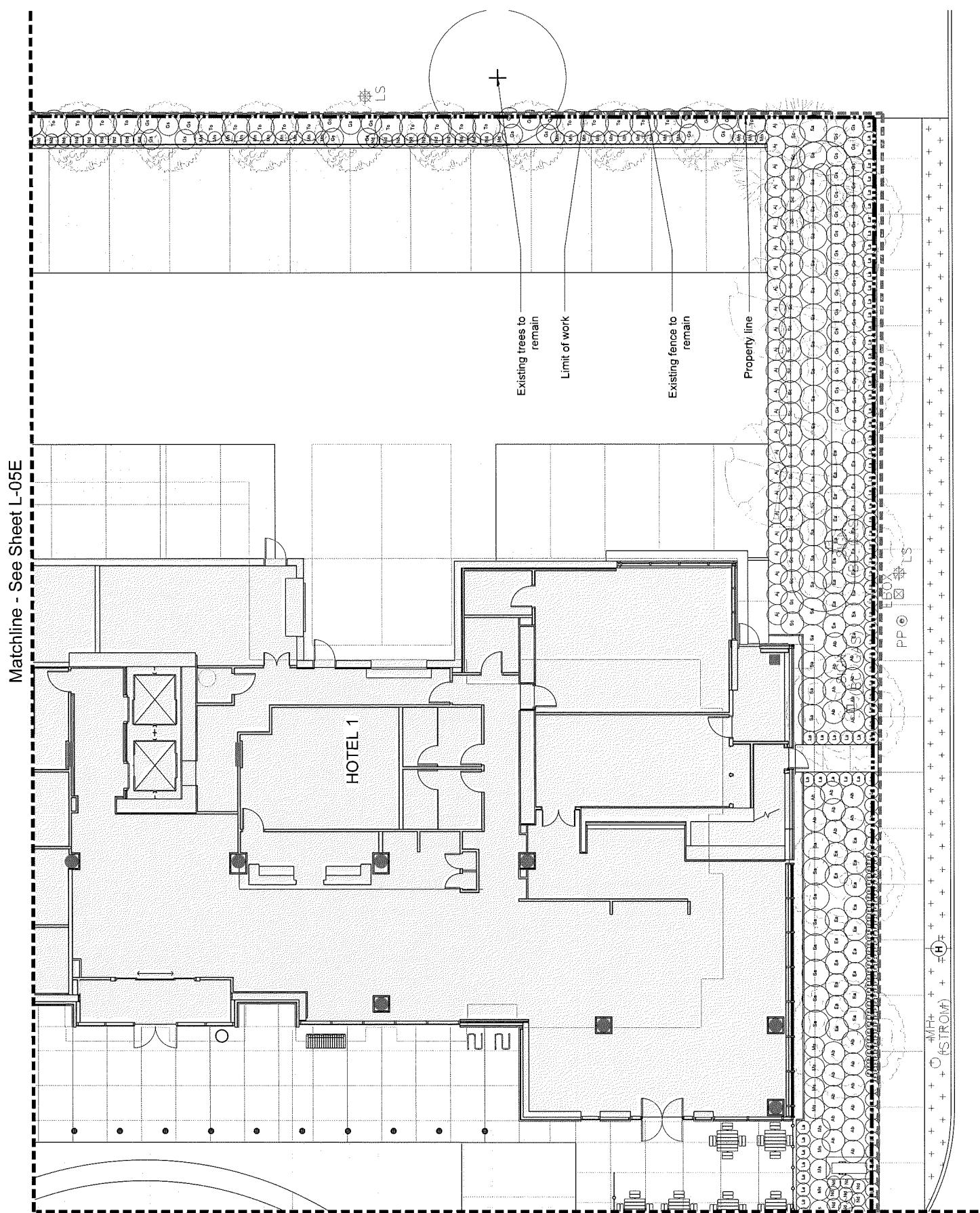
Drawing Title:

DP2018-33

L-05F

VZD Project #:

Drawing #:



Matchline - See Sheet L-05D

BRIDGEPORT ROAD

Location:

Bridgeport Hotel 1 and 2

Project:

Bridgeport Hotel 1 and 2

Date:

May 22, 2018

No. By:

Description

Date

REVISIONS TABLE FOR SHEET

REVISIONS TABLE FOR DRAWINGS

Comments:

Contractor shall check all dimensions on the work and report any discrepancy to the owner and submit a written report to the owner and engineer. All drawings and specifications set forth in the contract documents shall be followed. The work will be performed in accordance with the drawings. All revisions to the drawings must be made by the contractor and submitted to the owner and engineer for review and approval. The contractor shall not make any changes to the drawings without the written consent of the owner and engineer.

Approved:

MVDZ

Drawn:

AD

Stamp:

Checked:

MVDZ

Original Sheet Size:

24" x 36"

Scale:

1:150

DP 18-825006

1:150 13.5m 3.4.5 6 7.5 9 10.5 12 13.5 15 16.5 18 19.5 21 22.5



NORTH WEST IRRIGATION PLAN

DP2018-33

L-06A

VDZ Project #:

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Machine See Sheet E-000

Property line

Limit of work

BRIDGEPORT BUSINESS CENTER

HOTEL 2

Limit of work

Property line

8.62 1 50mm

18.42 2 50mm

15.47 3 50mm

| QTY | GPM |
|-----|-----|
| 6 | 0.5 |
| 5 | 0.5 |
| 75 | 1.1 |
| 15 | 0.5 |
| 11 | 0.2 |
| 3 | 1.5 |
| 18 | 0.8 |
| 6 | 0.4 |
| 2 | 0.8 |
| 6 | 1.7 |
| 13 | 0.6 |
| 2 | 0.8 |
| 6 | 1.8 |
| 1 | 3.3 |
| 17 | 0.8 |
| 14 | 0.8 |
| 1 | 2.5 |

IRRIGATION NOTES:

- System is based on 50 PSI (min), and 25 GPM (max), at the connection point.
 - Contractor to determine pipe layout to ensure system operates as per specifications.
 - Velocity in all irrigation lines not to exceed 5' per second. Contractor to adjust pipe sizing accordingly.
 - If a discrepancy occurs between drawings and specifications, specifications are to be followed.
 - All length quantities shown are approximate. Contractor to verify all quantities.
 - Back Flow Prevention Valve and Water Meter as per City of Richmond standards. New service boxes specified to contain all required valves and backflow prevention devices. To much City of Richmond standards.
 - Each irrigation line is to have a SCH 40 PVC clearing under all elevations. Steel sleeve to be used under all vehicle roads and driveways.
 - All service connections to be coordinated with civil and mechanical engineers. Allow for future water connection through ball valve installation.
 - Coordinate all irrigation trench and pipe locations to avoid conflicts with proposed utility lines.
 - Low-point irrigation heads require anti-freezing check valves if not low-point provided by head specification.
 - All products to be installed as per manufacturer's specifications unless otherwise directed. Contractor to provide all permits required for installation of irrigation system.
 - 1. All irrigation heads in non-turbine to be 12'- High rope heads.
2. Contractor will verify the existence, location and alleviation of all underground utilities and services prior to commencement of the work.
3. Main line to be installed at a minimum of 1/4" below finished grade.
4. All valves to be installed in 10' or larger valve boxes.
 - 1. All irrigation heads in turbine to be 12'- High rope heads.
2. Cable from the controller to valves shall be a minimum 14-gauge bare type, CSA approved type PE direct burial wire.
3. Contractor to confirm piping size at connection point and install pressure reduce as required.
4. Provide all field drawings for any changes made to the irrigation plan.
 - The irrigation system will be inspected by the consultant upon completion.

REGULATIONS SCHEDULE II E

| INVENTORY CONTROL CODE | | MANUFACTURER/MODEL | QTY | GPM |
|------------------------|---|--------------------|-----|-----|
| SYMBOL | DESCRIPTION | | | |
| ① | Rain Bird 1812-SAM-PRS-NP-U 15 Strip Series | | 6 | 0.5 |
| ② | Rain Bird 1812-SAM-PRS-NP-U 15 Strip Series | | 5 | 0.5 |
| ③ | Rain Bird 1812-SAM-PRS-NP-U 15 Strip Series | | 75 | 1.1 |
| ④ | Rain Bird 1812-SAM-PRS-NP-U 8 Series | | 15 | 0.5 |
| ⑤ | Rain Bird 1812-SAM-PRS-NP-U 8 Series | | 11 | 0.2 |
| ⑥ | Rain Bird 1812-SAM-PRS-NP-U 10 Series | | 3 | 1.5 |
| ⑦ | Rain Bird 1812-SAM-PRS-NP-U 10 Series | | 18 | 0.8 |
| ⑧ | Rain Bird 1812-SAM-PRS-NP-U 10 Series | | 6 | 0.4 |
| ⑨ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | | 3 | 2.4 |
| ⑩ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | | 45 | 1.2 |
| ⑪ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | | 13 | 0.6 |
| ⑫ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | | 2 | 0.8 |
| ⑬ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | | 6 | 1.8 |
| ⑭ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | | 1 | 3.3 |
| ⑮ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | | 17 | 1.7 |
| ⑯ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | | 14 | 0.8 |
| ⑰ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | | 1 | 2.5 |

| | |
|-------------------|--|
| AD | |
| Checked: MVDZ | |
| Approved: MVDZ | Original Sheet 24" x 36" |
| Scale: | CONTINUATION SHEET ON THE WORK AND IN ALL DRAWINGS AND PLANS WHICH MAY BE FURNISHED HEREIN. |
| 1:100 | |

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DP 18-82500



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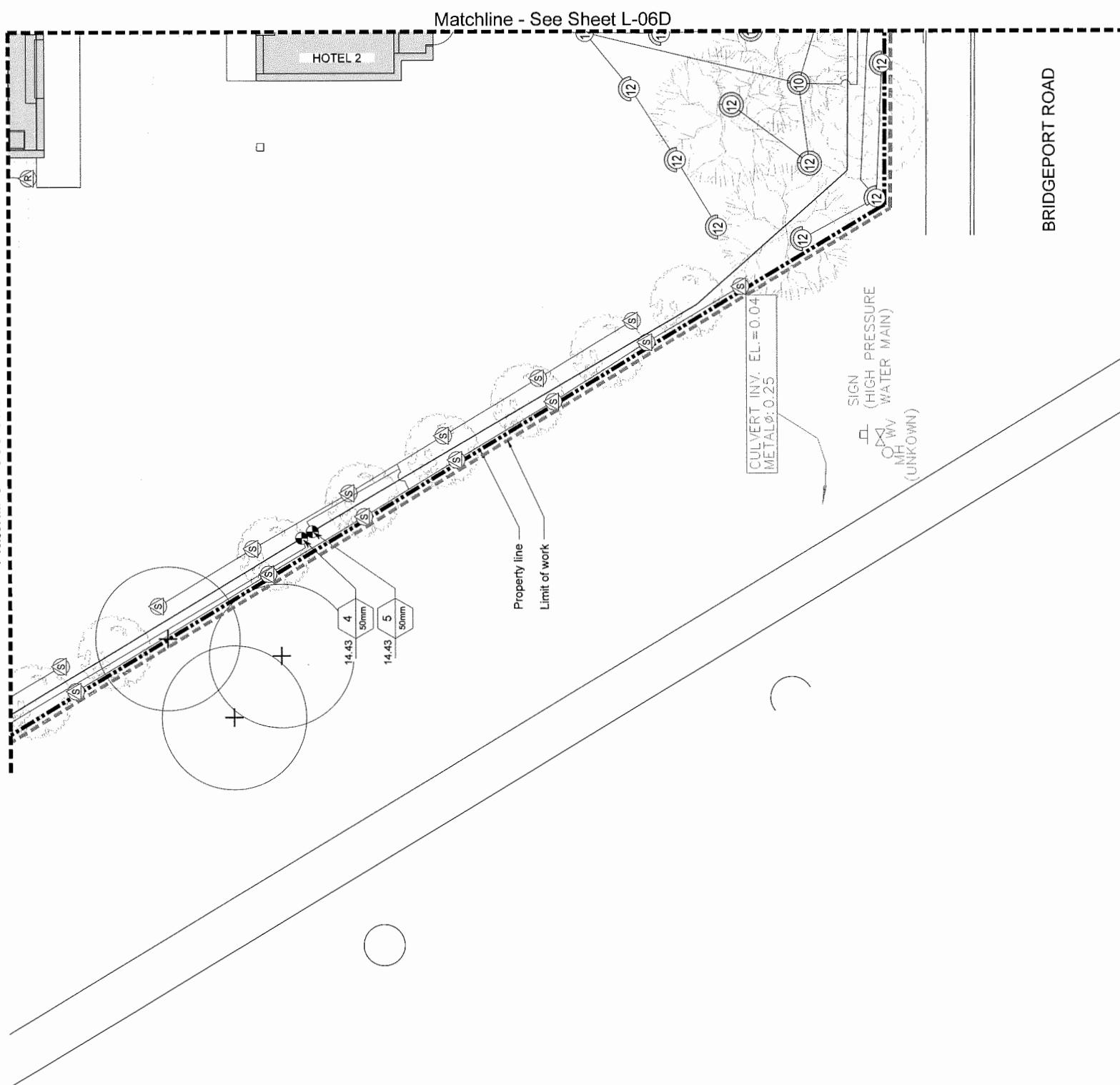
SOUTH WEST IRRIGATION PLAN

Drawing Title:
NORTH

DP2018-33

L-06B
Drawing #:

Matchline - See Sheet L-06A



IRRIGATION NOTES:

1. System is based on 50 PSI (min) and 25 GPM (max) at the connection point.
2. Contractor to determine pipe layout to ensure system operates as per specifications.
3. Velocity in all irrigation lines not to exceed 5 feet second. Contractor to adjust pipe sizing accordingly.
4. If a discrepancy occurs between drawings and specifications, specifications are to be followed.
5. All length quantities shown are approximate. Contractor to verify all quantities.
6. Back Flow Prevention Valve and Water Meter as per City of Richmond standards. New service pipes need to contain all required valves and blow-out tees. To match City of Richmond standards.
7. Each irrigation line to have 4" SCH 40 PVC serving under all sidewalks. Steel stanching to be used under all vehicular roads and driveways.
8. All service connections to be coordinated with civil and mechanical engineers and irrigation contractors through liaison in installation of underground utilities and services prior to commencement of the work.
9. Coordinate all irrigation lines with vehicles and pipe locations to avoid conflicts with vehicles and utility lines.
10. Low point irrigation heads require anti-draining check valves if not already provided in irrigation head specification.
11. All products to be installed as per manufacturer's specification unless noted otherwise. Contractor to provide all permits required for installation of irrigation system.
12. Contractor to verify the existence, location and elevation of all underground utilities and services prior to commencement of the work.
13. All pipe to be installed at a minimum of 18" below finish grade.
14. Main line to be installed at a minimum of 24" below finish grade.
15. All valves to be installed in 1" or larger valve box.
16. All irrigation heads in non-turf areas to be 12" High Pop Heads.
17. Control wire from the controller to valves shall be a minimum 14-gauge burial type. CSA approved type PE direct burial wire.
18. Contractor to confirm pressure and pipe sizing at connection point and initial pressure reduce as required.
19. Provide as built drawings for any changes made to the irrigation plan.
20. The irrigation system will be inspected by the consultant upon completion.

IRRIGATION SCHEDULE

| SYMBOL | MANUFACTURER/MODEL | QTY | SPM |
|--------|---|-----|-----|
| ① | Rain Bird 1612-SAM-PRS-NP-U 15 Strip Series | 6 | 0.5 |
| ② | Rain Bird 1612-SAM-PRS-NP-U 15 Strip Series | 5 | 0.5 |
| ③ | Rain Bird 1612-SAM-PRS-NP-U 15 Strip Series | 75 | 1.1 |
| ④ | Rain Bird 1612-SAM-PRS-NP-U 18 Series | 15 | 0.5 |
| ⑤ | Rain Bird 1612-SAM-PRS-NP-U 18 Series | 11 | 0.2 |
| ⑥ | Rain Bird 1612-SAM-PRS-NP-U 10 Series | 3 | 1.5 |
| ⑦ | Rain Bird 1612-SAM-PRS-NP-U 10 Series | 18 | 0.8 |
| ⑧ | Rain Bird 1612-SAM-PRS-NP-U 10 Series | 6 | 0.4 |
| ⑨ | Rain Bird 1612-SAM-PRS-NP-U 12 Series | 3 | 2.4 |
| ⑩ | Rain Bird 1612-SAM-PRS-NP-U 12 Series | 45 | 1.2 |
| ⑪ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 13 | 0.6 |
| ⑫ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 2 | 0.8 |
| ⑬ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 6 | 1.8 |
| ⑭ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 1 | 3.3 |
| ⑮ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | 17 | 1.7 |
| ⑯ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | 14 | 0.8 |
| ⑰ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | 1 | 2.5 |

| SYMBOL | MANUFACTURER/MODEL/DESCRIPTION | QTY | QTY |
|-----------|--------------------------------------|---------|---------|
| — — — — — | Irrigation Mainline: PVC Schedule 40 | 15 | 15 |
| — — — — — | Pipe Slave: CPVC Schedule 40 | 116.2 m | 91.9 m |
| — — — — — | Valve Catalog | 1.306 m | 1.306 m |
| — — — — — | Value Number | 1 | 1 |
| — — — — — | Value Flow | 1 | 1 |
| — — — — — | Value Size | 1 | 1 |



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V1M 4B9

SOUTH CENTRAL IRRIGATION PLAN

L-06D

DP2018-33

VZD Project #:



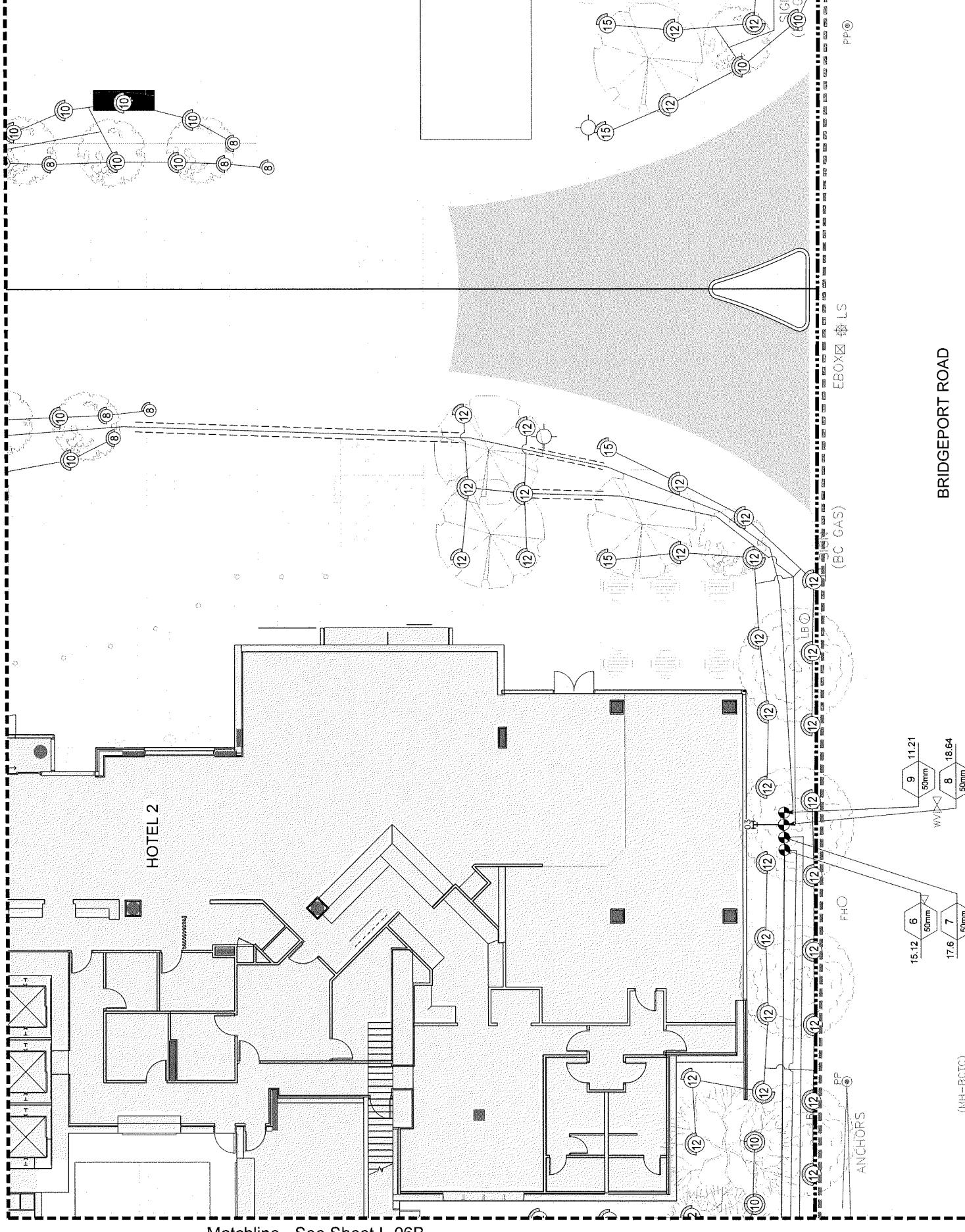
NORTH



Drawing Title:

Matchline - See Sheet L-06C

Matchline - See Sheet L-06F



IRRIGATION NOTES:

- System is based on 50 PSI (min.) and 25 GPM (max) at the connection point.
- Contractor to determine pipe layout to ensure system operates as per specifications.
- Velocity in all irrigation lines not to exceed 5 ft per second. Contractor to adjust pipe sizing accordingly.
- Inlet pipe sizing accords between drawings and specifications.
- Special accommodations are to be facilitated by the Contractor to verify all ground conditions.
- Back-Flow Prevention Valve and Water Meter as per City of Richmond standards. New service boxes stated to contain all required valves and blow-out tees. To match City of Richmond standards.
- Each irrigation line to have 4" SCH 40 PVC steaming under all sidewalks. Steel sleeving to be used under all vehicular roads and driveways.
- All service connections to be coordinated with civil and mechanical engineer. Allow for proper winterization through ball valve installation.
- Coordinate all irrigation tranches and pipe locations to avoid conflicts with proposed utilities.
- Low-point irrigation heads require anti-draining check valves if not already provided in irrigation head specification.
- All products to be installed as per manufacturer's specifications unless noted otherwise. Contractor to provide all permits required for installation of irrigation system.
- Contractor to coordinate with civil and mechanical engineer. Allow for proper winterization through ball valve installation.
- All pipe to be installed at a minimum of 18" below finish grade.
- Main line to be installed at a minimum of 24" below finish grade.
- All valves to be installed in a 1" or larger valve box.
- All irrigation heads to be 1" High Pop heads.
- Control wire from the controller to valves shall be a minimum 14-gauge burial type, CSA approved type PE direct burial wire.
- Contractor to confirm dimensions and pipe sizing at connection point and install pressure reducers as required.
- Irrigation system will be inspected by the consultant upon completion.

IRRIGATION SCHEDULE

| SYMBOL | MANUFACTURER/MODEL | DESCRIPTION | | |
|--------|---------------------------------------|-------------|-----|---------|
| | | QTY | GPM | COMMENT |
| ① | Rain Bird 1812-SAM-PRS-NP-U U1 Series | 6 | 0.6 | |
| ② | Rain Bird 1812-SAM-PRS-NP-U15 Series | 5 | 0.5 | |
| ③ | Rain Bird 1812-SAM-PRS-NP-U15 Series | 75 | 1.1 | |
| ④ | Rain Bird 1812-SAM-PRS-NP-U15 Series | 15 | 0.5 | |
| ⑤ | Rain Bird 1812-SAM-PRS-NP-U15 Series | 11 | 0.2 | |
| ⑥ | Rain Bird 1812-SAM-PRS-NP-U10 Series | 3 | 1.5 | |
| ⑦ | Rain Bird 1812-SAM-PRS-NP-U10 Series | 18 | 0.8 | |
| ⑧ | Rain Bird 1812-SAM-PRS-NP-U10 Series | 6 | 0.4 | |
| ⑨ | Rain Bird 1812-SAM-PRS-NP-U12 Series | 3 | 2.4 | |
| ⑩ | Rain Bird 1812-SAM-PRS-NP-U12 Series | 45 | 1.2 | |
| ⑪ | Rain Bird 1812-SAM-PRS-NP-U12 Series | 13 | 0.6 | |

| SYMBOL | MANUFACTURER/MODEL | QTY |
|--------|--|-----|
| ● | Rain Bird PEB-PRSD | 15 |
| ○ | 25 mm 40 mm 30 mm Plastic Industrial Valves, Low Flow Operating Ability, Globe Configuration, With Pressure Regulating Module. | |
| △ | Irrigation lateral line: CPVC Schedule 40 | |

116.2 m

g 1.9 m

116.2 m



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SOUTH EAST IRRIGATION PLAN

Drawing Title:

DP2018-33

L-06F

Drawing #:

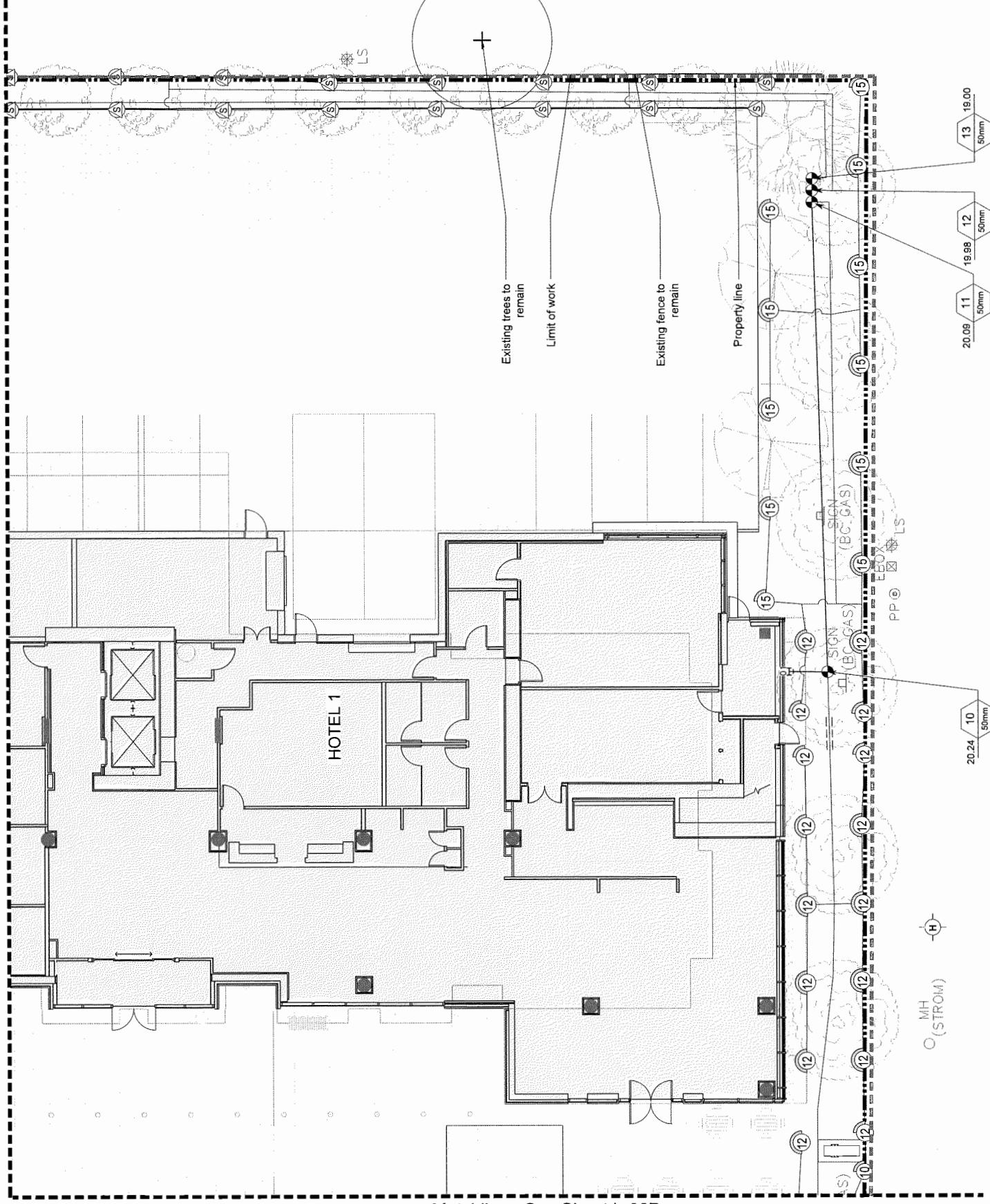


VZD Project #:

DP2018-826006

UP 18-826006 - 54

Matchline - See Sheet L-06E



Matchline - See Sheet L-06D

IRRIGATION NOTES:

- System is based on 80 PSI (min.) and 25 GPM (max.) at the connection point.
- Contractor to determine pipe layout to ensure system operates as per specifications.
- Vehicle in all irrigation lines not to exceed 5' per second. Contractor to adjust pipe sizing accordingly.
- If a discrepancy occurs between drawings and specifications, specifications are to be followed.
- All irrigations quantities shown are approximate. Contractor to verify all quantities.
- Back Flow Prevention Valve and Water Meter as per City of Richmond standards. New service boxes sized to contain all required valves and back-flow fees. To match City of Richmond standards.
- Each irrigation line to have 4" SCH 40 PVC sleeves under all sidewalks. Steel teeing to be used under all vehicular roads and driveways.
- All service connections to be coordinated with civil and mechanical engineer. Allow for proper winterization through ball valve installation.
- Coordinate all irrigation trenches and pipe locations to avoid conflicts with proposed utilities.
- Low-point irrigation heads require anti-draining check valves if not already provided in irrigation head specification.
- All products to be installed as per manufacturer's specifications unless noted otherwise. Contractor to provide all permits required for installation of irrigation system.
- Contractor to verify the existence, location and elevation of all underground utilities and services prior to commencement of the work.
- All irrigation heads must be installed in 10' ceiling height areas.
- All irrigation heads in non turf areas to be 12" high crop heads.
- Control wire from the controller to valves shall be a minimum 14-gauge burial type CSA approved type PE direct buried wire.
- Contractor to confirm pressure and pipe sizing at connection point and install pressure reducer as required.
- Provide as built drawings for any changes made to the irrigation plan.
- The irrigation system will be inspected by the consultant upon completion.

IRRIGATION SCHEDULE

| SYMBOL | MANUFACTURER/MODEL | QTY | GPM |
|--------|---|-----|-----|
| ① | Rain Bird 1812-SAM-PRS-NP-U 15 Strip Series | 6 | 0.5 |
| ② | Rain Bird 1812-SAM-PRS-NP-U 15 Strip Series | 5 | 0.5 |
| ③ | Rain Bird 1812-SAM-PRS-NP-U 15 Strip Series | 75 | 1.1 |
| ④ | Rain Bird 1812-SAM-PRS-NP-U 15 Strip Series | 15 | 0.5 |
| ⑤ | Rain Bird 1812-SAM-PRS-NP-U 18 Series | 11 | 0.2 |
| ⑥ | Rain Bird 1812-SAM-PRS-NP-U 18 Series | 3 | 1.5 |
| ⑦ | Rain Bird 1812-SAM-PRS-NP-U 10 Series | 45 | 1.2 |
| ⑧ | Rain Bird 1812-SAM-PRS-NP-U 10 Series | 18 | 0.8 |
| ⑨ | Rain Bird 1812-SAM-PRS-NP-U 10 Series | 6 | 0.4 |
| ⑩ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 3 | 2.4 |
| ⑪ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 45 | 1.2 |
| ⑫ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 13 | 0.6 |
| ⑬ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 2 | 0.8 |
| ⑭ | Rain Bird 1812-SAM-PRS-NP-U 12 Series | 6 | 1.8 |
| ⑮ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | 1 | 3.3 |
| ⑯ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | 17 | 1.7 |
| ⑰ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | 14 | 0.8 |
| ⑱ | Rain Bird 1812-SAM-PRS-NP-U 15 Series | 1 | 2.5 |

| SYMBOL | MANUFACTURER/MODEL/DESCRIPTION | QTY |
|--------|--|---------|
| ⑲ | Rain Bird PEB-PRS-D 25 mm, 40 mm, 50 mm Plastic Industrial Valves, Low Flow Regulator Module. | 15 |
| ⑳ | Irrigation Lateral Line: CPVC Schedule 40 | 1.306 m |
| ㉑ | Irrigation Mainline: PVC Schedule 40 | 116.2 m |
| ㉒ | Pipe Sleeve: CPVC Schedule 40 | 91.9 m |

| | |
|--------------|------------|
| Value Cutoff | Value Flow |
| # | # |



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ROOF LANDSCAPE PLANS

DP2018-33 VDZ Project #

L-07 Drawing #

LANDSCAPE LEGEND

| KEY | REF. | DESCRIPTION |
|-----|------|----------------------|
| | | EXTENSIVE GREEN ROOF |
| | | DRIp STRIP |

REVISIONS TABLE FOR SHEET

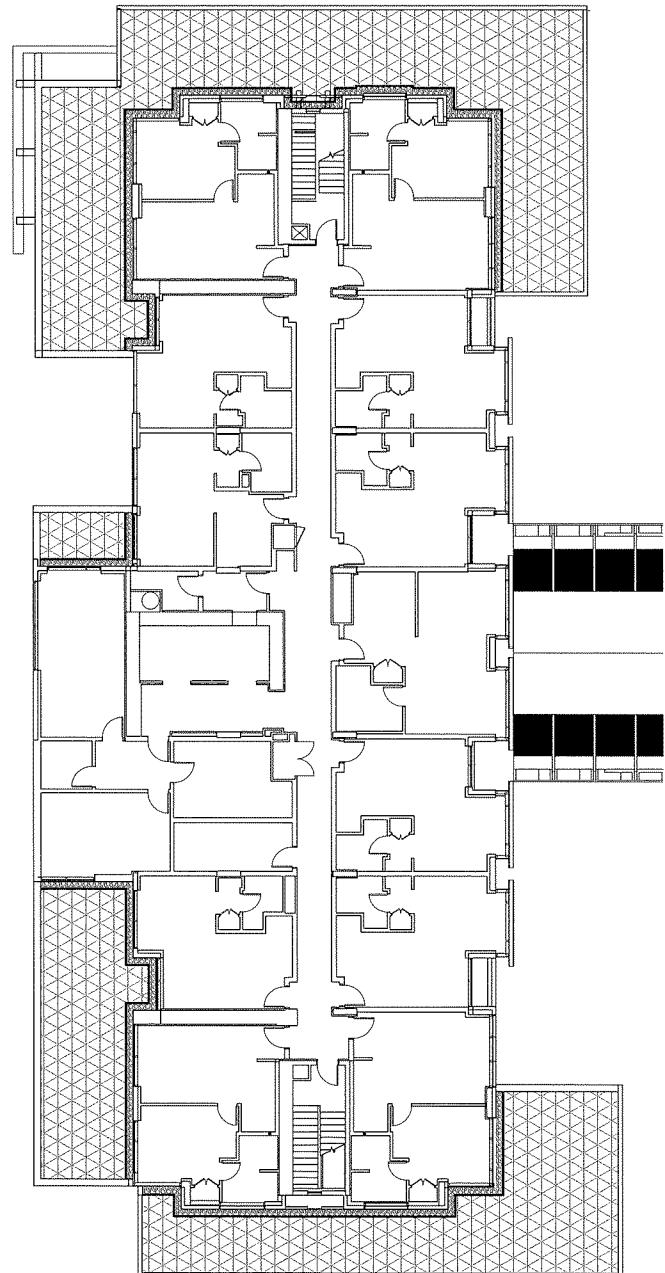
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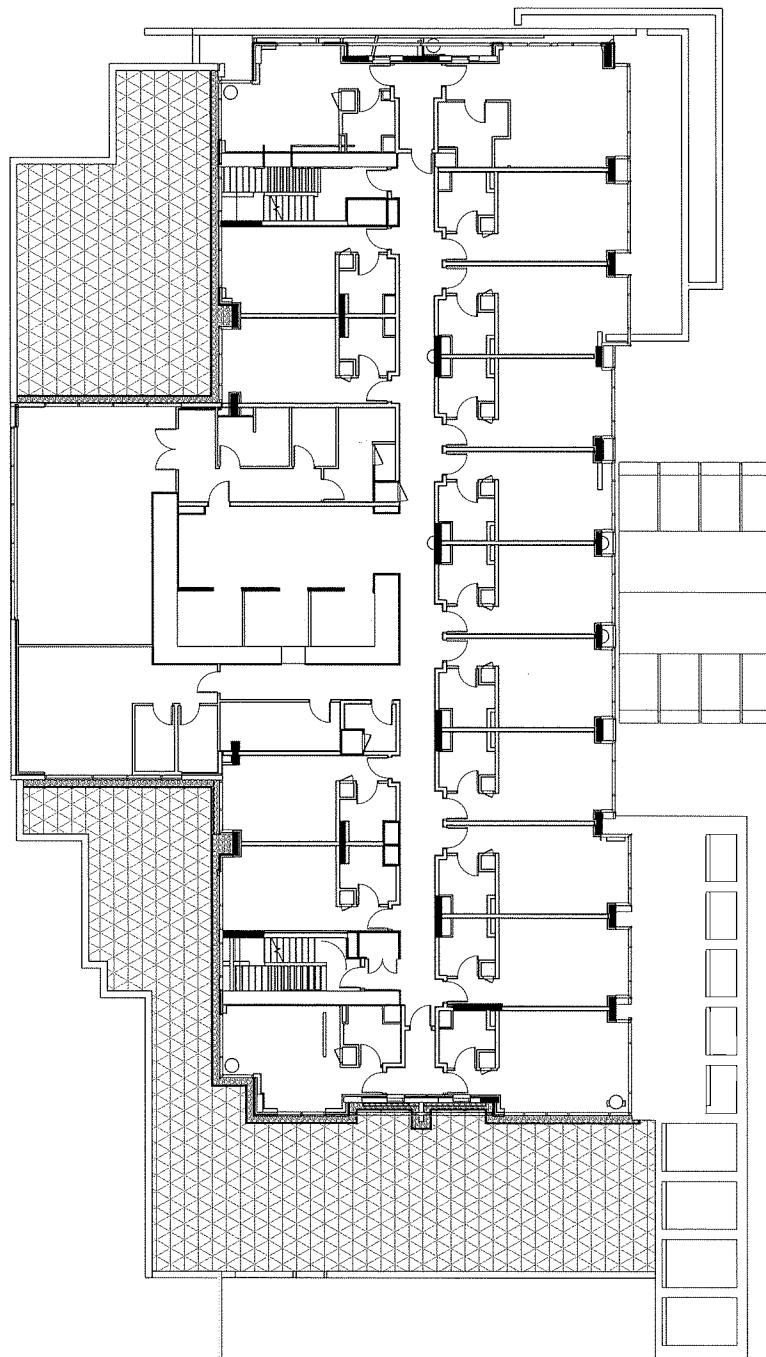
DP2018-33

Location:
9533 Bridgeport Rd & 9455
Bridgeport Rd

Approved: MV/DZ
Original Sheet Size: 24" x 36"
Scale: 1:150
Comments: CONTRACTOR SHALL OBTAIN ALL DIMENSIONS ON THE WORK AND DRAFTS FOR AND CHEREVACY TO THE CONSULTANT BEFORE PROSECUTING ANY ACTION AGAINST THE CONSULTANT. THE CONSULTANT IS NOT RESPONSIBLE FOR ANY DAMAGES WHICH MAY BE SUFFERED BY THE OWNER AND EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE TURNED OVER AT THE COMPLETION OF CONSTRUCTION UNLESS AGREED FOR IN WRITING. DRAWINGS MUST NOT BE USED FOR CONSTRUCTION UNLESS LIADED ISSUED FOR FURTHER CONTRACTOR.



1 HOTEL 1 - LEVEL 2 ROOF PLAN
Scale 1:150



1:150 0 1.5m 3 4.5 6 7.5 9 10.5 12 13.5 15 16.5 18 19.5 21 22.5

DP 18-025006

- 55



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SECTIONS

DP2018-33
NORTH

L-08A

Drawing #: DP 10-825006-56

Drawing Title:

VZD Project #:

DP2018-33



6 AD Issued for Development Permit Feb 1, 2019

5 AD Issued for Development Permit Jan 29, 2019

4 AD Issued for 5% RP Review Dec 21, 2018

3 AD Issued for Development Permit Nov 28, 2018

2 AD Issued for Development Permit June 4, 2018

1 FW Issued for Review May 22, 2018

No. By Description Date

REVISIONS TABLE FOR DRAWINGS

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No. By Description Date

REVISIONS TABLE FOR SHEET

Project:
Bridgeport Hotel 1 and 2

Location:
9533 Bridgeport Rd & 9455 Bridgeport Rd

Drawn:
AD

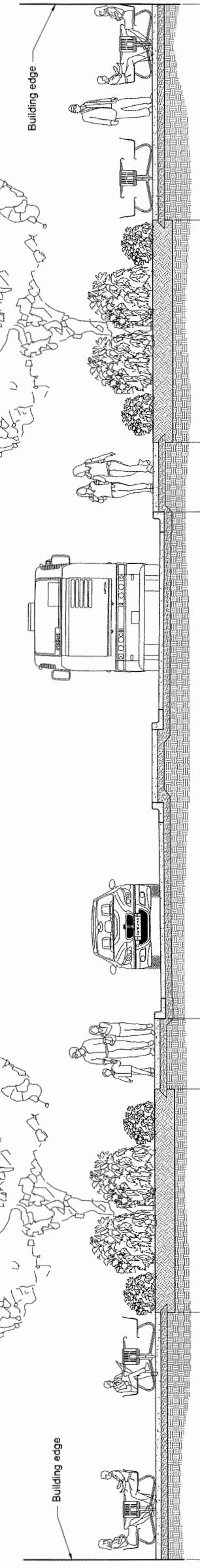
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MVDZ

Approved:
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SECTION A
1
Scale 1:50

SECTION B
2
Scale 1:50



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SECTIONS

DP2018-33
NORTH

L-08B
Drawing #:

DP2018-006-57

Drawing Title:

Vdz Project #:

9533 Bridgeport Rd & 9455
Bridgeport Rd

Project:
Bridgeport Hotel 1 and 2

Location:
9533 Bridgeport Rd & 9455
Bridgeport Rd

Drawn:
AD

Stamp:

Checked:
MVZ

Approved:
MVDZ

Original Sheet Size:
24" x 36"

Scale:
AS SHOWN

Contractor shall copy all dimensions
and descriptions from these drawings
to the consultant before proceeding.
All drawings and specifications are the
property of the consultant. They must
not be retained after the completion of
the work. All records of plans and
construction unless saved should be
turned over to the architect.

6 AD Issued for Development Permit Feb 1, 2018

5 AD Issued for Development Permit Jan 29, 2019

4 AD Issued for 50% BP Review Dec 21, 2018

3 AD Issued for Development Permit Nov 26, 2018

2 AD Issued for Development Permit June 4, 2018

1 FW Issued for Review May 22, 2018

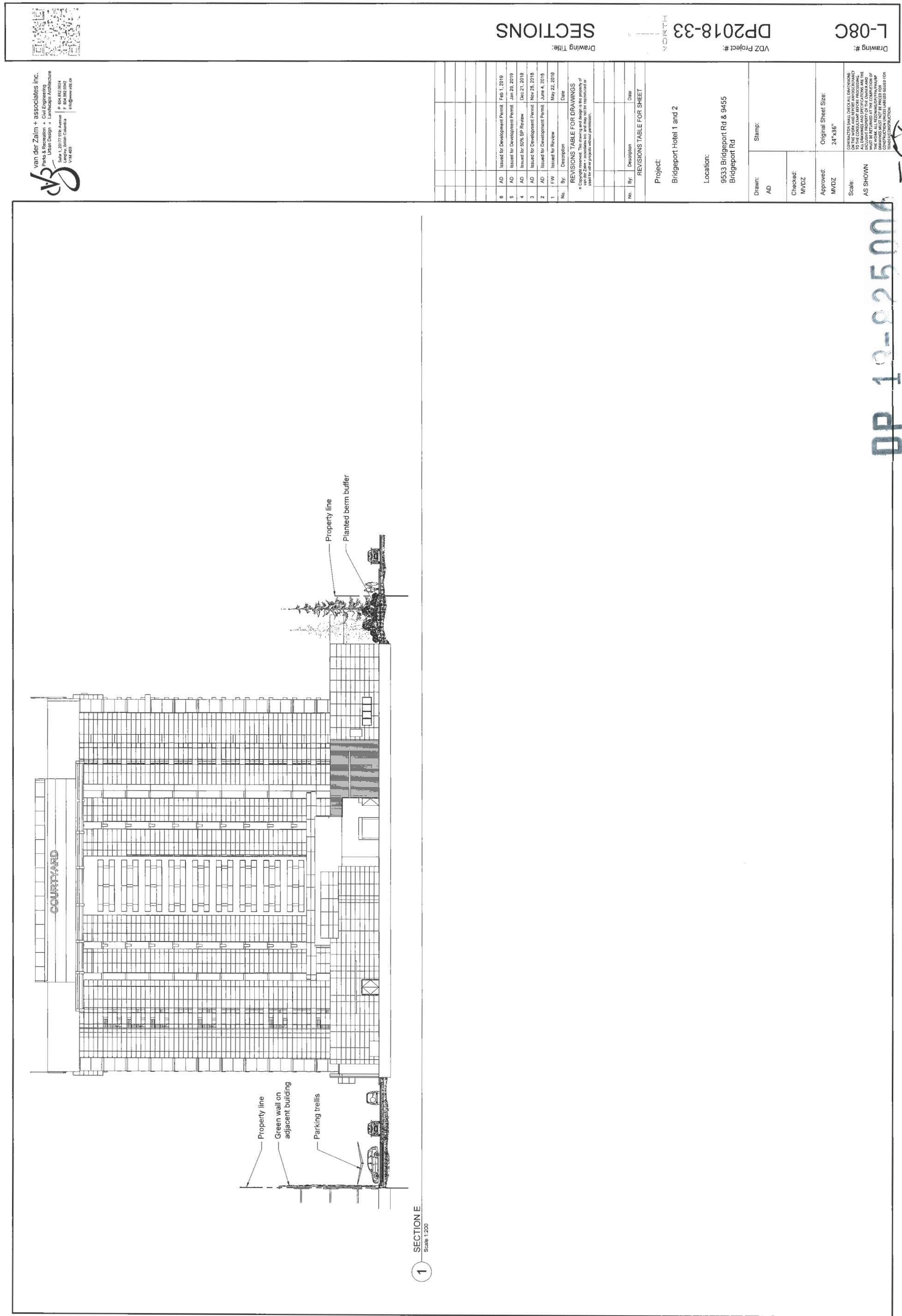
Date

No.

By:

Description

Date



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OFFSITE PLAN

Drawing Title:

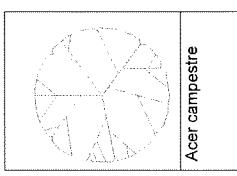
DP2018-33

L-09

Vdz Project #:



TREE LEGEND



Property line

Existing trees to remain

TREE SCHEDULE

| SYMBOL | QTY | BOTANICAL NAME / COMMON NAME | COMMON NAME | SIZE | SPACING |
|----------------|-----|------------------------------|-------------|------------------------|----------|
| Acer campestre | 30 | Acer campestre | Hedge Maple | B&B, 6cm cal; 3.0m ht. | Per Plan |

| SHRUBS | QTY | BOTANICAL NAME / COMMON NAME | COMMON NAME | CONT |
|--------|-----|--|-------------|------|
| Ab | 48 | Abies balsamea 'Nana' / Dwarf Balsam Fir | #2 Pot | |
| Aj | 35 | Aralia japonica 'Gumpo Pink' / Dwarf Pink Azalea | #2 Pot | |
| Td | 59 | Thuja occidentalis 'Drogo's Spire' / Drogo's Spire Cedar | 2.5m ht. | |

Existing retaining wall and fence to remain
Extend hedge to south edge of proposed building. Remove existing shrub material to accommodate



6

AD

Issued for Development Permit

Feb 1, 2019

AD

Issued for Development Permit

Jan 29, 2019

AD

Issued in 50% BPR Review

Dec 21, 2018

AD

Issued for Development Permit

Nov 28, 2018

AD

Issued for Development Permit

June 4, 2018

FW

Issued for Review

May 22, 2018

No.

By:

Description:

Date:

REVISIONS TABLE FOR DRAWINGS

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| No. | By: | Description | Date |
|-----|-----|-------------------------------|--------------|
| 2 | AD | Issued for Neighbour Review | Jan 26, 2019 |
| 1 | AD | Issued for Development Permit | Jan 18, 2019 |
| No. | By: | Description | Date |
| | | | |

REVISIONS TABLE FOR SHEET

Project:

Bridgeport Hotel 1 and 2

Location:

9533 Bridgeport Rd & 9455 Bridgeport Rd

Drawn:

AD

Stamp:

Approved:

MVDZ

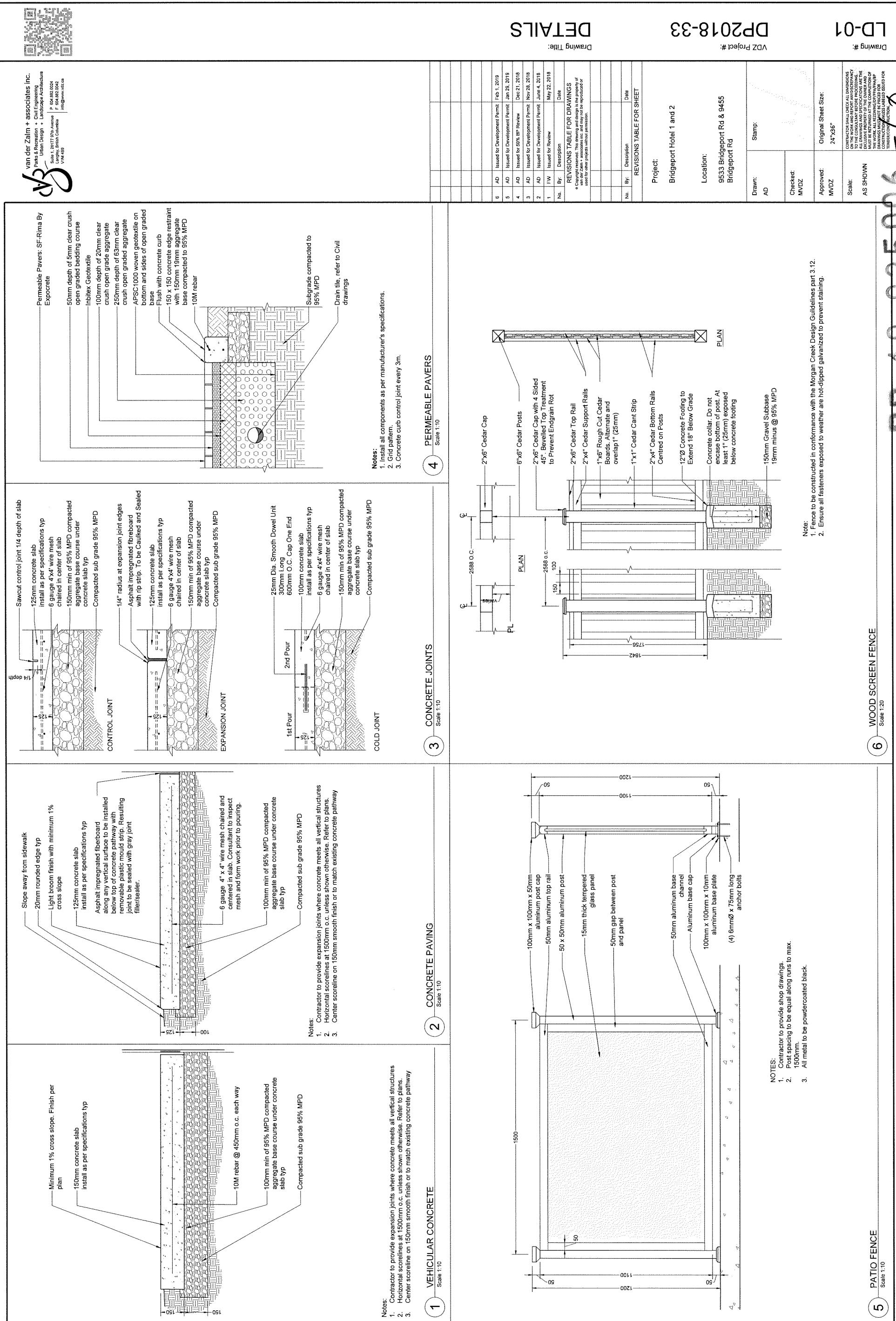
Original Sheet Size:

24" x 36"

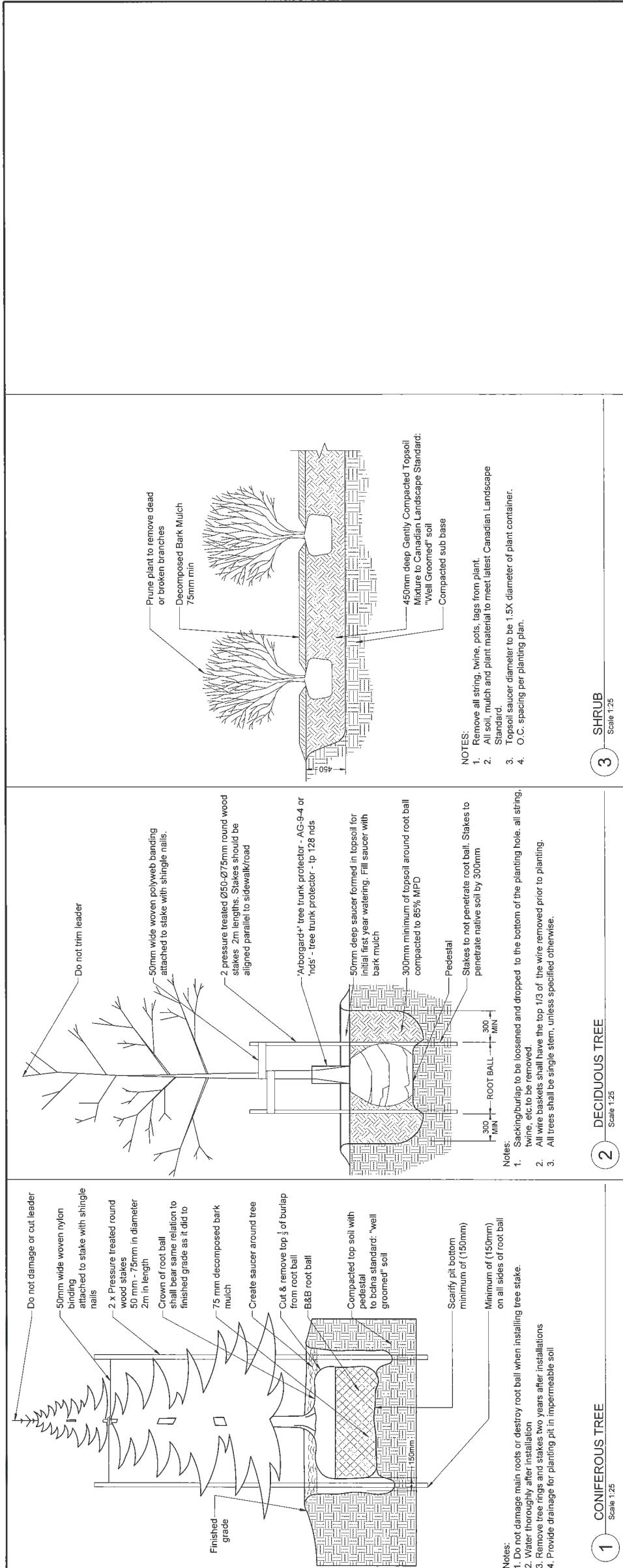
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DP2018-33 BRIDGEPORT HOTEL 1 AND 2 DRAWING SHEET L-09 OFFSITE PLANS
1:100 Scale
Drawing #:
59

DETAILS



DETAILS



BRIDGEPORT HOTELS

PROJECT OVERVIEW:



The proposed phase of the project consisting of two hotels is located at the lot north of Bridgeport Road, east of the Highway 99/Oak Street Bridge and south of Beckwith Road. The site is surrounded by mixed-use retail and hotel uses to the south, west and east along the arterial roads. The north is primarily old residential buildings, with some light-industrial uses. The site is also approximately 1km walking distance from Bridgeport Canada Line station.

The primary design approach for this phase is to maximize the full potential of the strategic location of the site that has considerable street frontage on the south and north sides, with exposure to the major traffic along the arterial roads, proximity to the airport, Canada Line and surrounding mixed use commercial and hotel uses. This phase consists of two Hotel structures with 10 and 12 storeys each placed along Bridgeport Road and will present a vibrant urban planning and architectural solution that will revitalize the character of the neighbourhood and stimulate its future potential. A generously central landscape located along north-south internal road connecting Bridgeport to Beckwith Road, serves as the main access to both buildings, while a perpendicular east-west internal road serves as the main distributor to parking and services for each building. Each of the hotels are to have their own surface parking to their rear and north sides with a landscaped buffer between them and Bridgeport Road.

Every effort has been taken in the development of these buildings to respect the OCP design guidelines, as well as to implement an environmentally friendly and sustainable design approach, while introducing strong design elements that will enhance the City's vision for future growth in this area. It is the intent of the project, with its landscaped site layout and proportionately articulated building masses, that combined with efficient land use that it in turn may become a landmark for the north Richmond area, to stand as a gateway to Richmond as one approaches from the north on Highway 99.



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TEL: 604-683-8797

van der zalm + associates inc.
97TH AVENUE
LANGLEY BC V1M 4B9
TEL: 604-882-0024

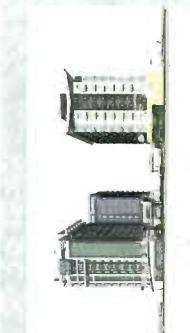
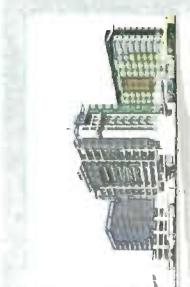
IBI GROUP ARCHITECTS (CANADA) INC.
700-1285 W PENDER ST
VANCOUVER BC V6E 4B1
TEL: 604-683-8797

CORE CONCEPT CONSULTING LTD
220-2639 VIKING WAY
RICHMOND BC V6V 3B7
TEL: 604-249-5040

MATSON PECK & TOPLISS
320-11120 HORSESHOE WAY
RICHMOND BC V7A 5H7
TEL: 604-270-9331

GEO PACIFIC
1779 W. 75TH AVENUE
VANCOUVER BC V6P 6P2
TEL: 604-439-0922

2019-02-07 RE-ISSUED FOR DEVELOPMENT PERMIT



IBI

DP 18-825006 - Reference

IBI

9455 & 9533 BRIDGEPORT ROAD

| DRAWING | ARCHITECTURAL | SCALE |
|--------------|---|------------|
| COVERPAGE | BRIDGEPORT HOTELS CONSULTANT LIST/PROJECT OVERVIEW | - |
| 01 | BRIDGEPORT HOTEL BUSINESS CENTER (BIBC) RICHMOND B.C. TABLE OF CONTENTS INTRICATE CONTEXT/ZONING (EXTRACT FROM D#11-594571) | - |
| 02 | BIBC SITING/BUILDING ORIENTATION/MASSING & BUILDING HEIGHT (EXTRACT FROM D#11-594571) | - |
| 03 | BIBC ARCHITECTURAL CHARACTER/LIVABILITY & SECURITY/LANDSCAPE & SUSTAINABILITY (EXTRACT FROM D#11-594571) | - |
| 04 | BIBC PUBLIC ART/PARKING & LOADING (EXTRACT FROM D#11-594571) | - |
| A0002 | DRAWING LIST | - |
| A1001 | PHASES/PHASE IMAGE | NTS |
| A1002 | CONTEXT PLAN 1 | NTS |
| A1003 | CONTEXT PLAN 2 | NTS |
| A1004 | GIS MAP | NTS |
| A1005 | SITE IMAGE PRIOR TO SITE CLEARANCE | NTS |
| A1006 | SURVEY DRAWING | NTS |
| A1007 | SUBDIVISION LAYOUT FOR PHASED DEVELOPMENT | 1/32=1'-0" |
| A1008 | PHASING PLANGROUND LEVEL SHOWN | 1/64=1'-0" |
| A1008a | PHASE 1 PARKING COUNT - HOTEL 1 ON-SITE PARKING | NTS |
| A1008b | PHASE 1 PARKING COUNT - BUSINESS CENTRE GROUND FLOOR | NTS |
| A1008c | PHASE 1 PARKING COUNT - HOTEL 2 ON-SITE PARKING | NTS |
| A1009 | STATISTICS | - |
| A1010 | SCHEMATIC BUILDING SETBACK DETAIL | 1/32=1'-0" |
| A1011 | SCHEMATIC BUILDING SETBACK DETAIL | 1/8=1'-0" |
| A1012 | HOTEL 1 - WEST & SOUTH ELEVATIONS COMPARISON | NTS |
| A1013 | HOTEL 2 - NORTH & EAST ELEVATIONS COMPARISON | NTS |
| A1014 | HOTEL 2 - NORTH & EAST ELEVATIONS COMPARISON | NTS |
| A1015 | HOTEL 2 - NORTH EAST ELEVATIONS COMPARISON | NTS |
| A1016 | HOTEL 1 RENDERING - DAY VIEW | NTS |
| A1021 | HOTEL 2 RENDERING - DAY VIEW | NTS |
| A1022 | HOTEL 2 RENDERING - DAY VIEW | NTS |
| A1023 | RENDERINGS, NIGHT VIEW | NTS |
| A1026 | STREETSCAPE RENDERING | NTS |
| A1027 | HOTELS 1 & 2 - STREETSCAPE RENDERINGS | NTS |
| A1030 | VISUAL CONNECTION BETWEEN BUILDINGS | NTS |
| A1031 | VISUAL CONNECTION BETWEEN BUILDINGS | NTS |
| A1100 | LOT COVERAGE | 1/40=1'-0" |
| A1101 | SITE PLAN - ROOF | 1/32=1'-0" |
| A1102 | SITE PLAN - GROUND FLOOR LAYOUT | 1/32=1'-0" |
| A1103 | SHADOW STUDY | 1=80'-0" |
| A1104 | SHADOW STUDY | 1=80'-0" |
| A1201 | PROJECT SOUTH ELEVATION | 1/16=1'-0" |
| A1202 | PROJECT EAST ELEVATION | 1/16=1'-0" |
| A401 | SCHEMATIC SECTION | 1/16=1'-0" |
| A1601 | AERIAL VIEW 1 | NTS |
| A1602 | AERIAL VIEW 2 | NTS |
| A1603 | AERIAL VIEW 3 | NTS |
| A1611 | PERSPECTIVE BRIDGEPORT RD 1 | NTS |
| A1612 | PERSPECTIVE BRIDGEPORT RD 2 | NTS |
| A1613 | PERSPECTIVE FROM OAK ST. BRIDGE | NTS |
| A2100a | PERSPECTIVE 01 | NTS |
| A2100b | PERSPECTIVE 02 | NTS |
| A2101 | HOTEL 1 WEST ELEVATION | - |
| A2102 | HOTEL 1 EAST ELEVATION | - |
| A2103 | HOTEL 1 NORTH ELEVATION | - |
| A2104 | HOTEL 1 SOUTH ELEVATION | - |
| A2201 | HOTEL 1 LEVEL 1 AREA PLAN | 1/16=1'-0" |
| A2202 | HOTEL 1 LEVEL 2 AREA PLAN | 1/16=1'-0" |
| A2203 | HOTEL 1 TYPICAL AREA PLAN | 1/16=1'-0" |
| A2204 | HOTEL 1 LEVEL 10 AREA PLAN | 1/16=1'-0" |
| A2205 | HOTEL 1 MECHANICAL ROOF AREA PLAN | 1/16=1'-0" |
| A2301 | HOTEL 1 - SECTION B-B | - |
| A3001 | HOTEL 2 PERSPECTIVE FROM BRIDGEPORT ROAD | NTS |
| A3002 | HOTEL 2 PERSPECTIVE FROM NORTHEAST | NTS |
| A3003 | HOTEL 2 PERSPECTIVE FROM OAK ST. BRIDGE 1 | NTS |
| A3004 | HOTEL 2 PERSPECTIVE FROM OAK ST. BRIDGE 2 | NTS |
| A3101 | HOTEL 2 EAST ELEVATION | - |
| A3102 | HOTEL 2 SOUTH ELEVATION | - |
| A3103 | HOTEL 2 WEST ELEVATION | - |
| A3104 | HOTEL 2 NORTH ELEVATION | - |
| A3201 | HOTEL 2 GROUND FLOOR AREA PLAN | 1/16=1'-0" |
| A3202 | HOTEL 2 LEVEL 1 AREA PLAN | 1/16=1'-0" |
| A3203 | HOTEL 2 LEVEL 2 AREA PLAN | 1/16=1'-0" |
| A3204 | HOTEL 2 LEVEL 12 (PENTHOUSE) AREA PLAN | 1/16=1'-0" |
| A3205 | HOTEL 2 MECHANICAL LEVEL AREA PLAN | 1/16=1'-0" |
| A3301 | HOTEL 2 SECTION B-B | - |
| A4000 | FAIR OVERLAY STATISTICS | 1/16=1'-0" |
| A4101 | HOTEL 1 LEVEL 0F FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4102 | HOTEL 1 LEVEL 02 FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4103 | HOTEL 1 LEVEL 03-09 FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4104 | HOTEL 1 LEVEL 10 FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4105 | HOTEL 1 LEVEL MECH/FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4106 | HOTEL 1 LEVEL OF FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4107 | HOTEL 2 LEVEL 02-10 FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4108 | HOTEL 2 LEVEL 03-10 FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4109 | HOTEL 2 LEVEL 11 FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4110 | HOTEL 2 LEVEL 12 FLOOR AREA OVERLAY | 1/16=1'-0" |
| A4111 | HOTEL 2 LEVEL MECH/FLOOR AREA OVERLAY | 1/16=1'-0" |
| A5000 | WB17 TRUCK TURNING WHEEL PATH - PART A SOUTH ENTRANCE (EXTRACT FROM D#11-594571) | 1/15=1'-0" |
| A5001a | WB17 TRUCK TURNING WHEEL PATH - PART A NORTH ENTRANCE (EXTRACT FROM D#11-594571) | AS SHOWN |
| A5004 | FIRE TRUCK ACCESS | AS SHOWN |
| A5011 | LOADING TRUCK ACCESS | AS SHOWN |
| A5012 | WASTE MANAGEMENT PLAN | AS SHOWN |
| A6000 | WASTE MANAGEMENT PLAN | AS SHOWN |
| A7000 | LEAD SCORE CARD | 0 |
| L-01 | LANDSCAPE COVER SHEET | 1/300 |
| L-02 | TREE PRESERVATION PLAN | 1/250 |
| L-03 | OVERALL SITE AND TREE PLAN | 1/150 |
| L-04A | WEST LANDSCAPE PLAN | 1/150 |
| L-04B | CENTRAL LANDSCAPE PLAN | 1/150 |
| L-04C | EAST LANDSCAPE PLAN | 1/150 |
| L-04D | ROOF LANDSCAPE PLANS | AS SHOWN |
| L-05 | SECTIONS | AS SHOWN |
| LD-01 | DETAILS | AS SHOWN |
| LD-02 | DETAILS | AS SHOWN |
| DRAWING LIST | A 0002 | REFERENCE |

DRAWING LIST

DP 18-825006TS Reference

IB

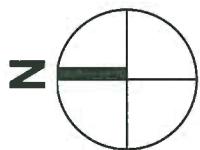
9455 & 9533 BRIDGEPORT ROAD



NP 18-82 CONTEXT PLAN 100%
NTP 100%
NTS



IBI

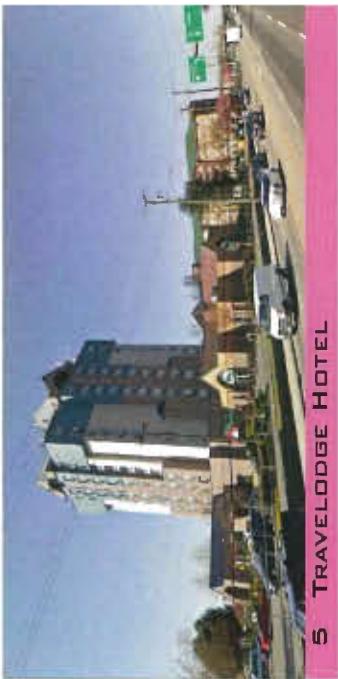


9455 & 9533 BRIDGEPORT ROAD

CONTEXT PLAN 2 A 1003
DP 18-825006 Reference



3 OFFICE DEPOT



5 TRAVELODGE HOTEL



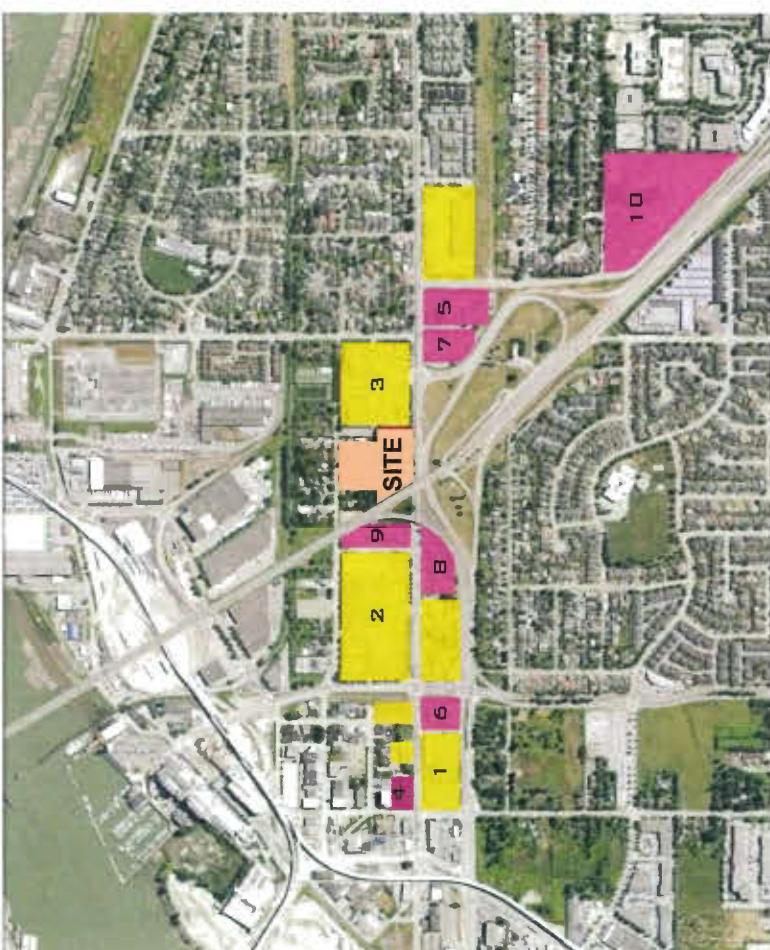
7 SANDMAN HOTEL



10 SANDMAN SIGNATURE



2 COSTCO



9 HOLIDAY INN



1 RETAIL SPACE



4 HAMPTON INN



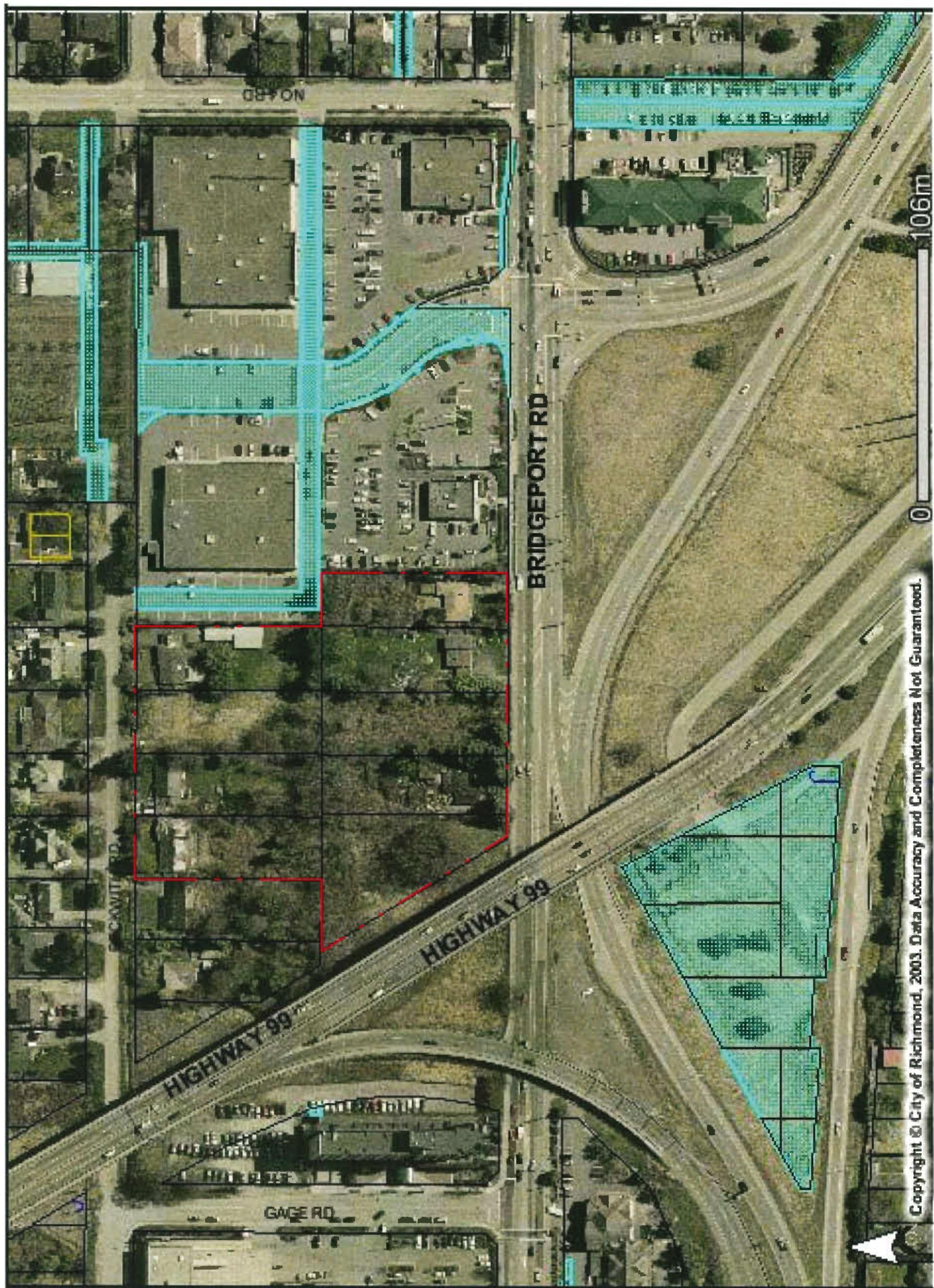
6 EXECUTIVE INN



8 BEST WESTERN

IBI

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DP 18-825 GIS SITE MAP
A1004 NTS
Revised

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SOUTH OF SITE



NORTH OF SITE



NORTH OF SITE

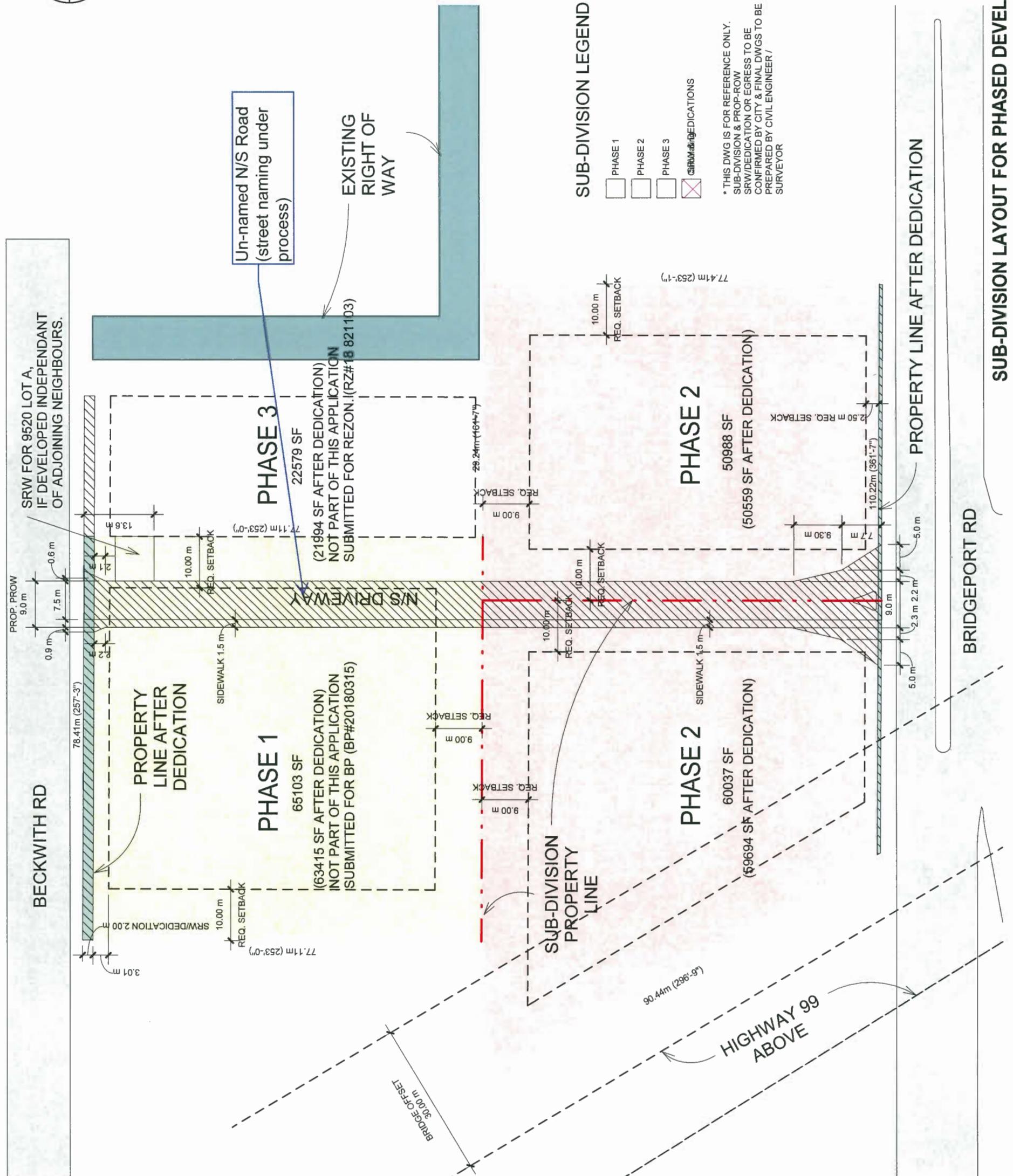
SITE IMAGE PRIOR TO SITE CLEARANCE

A 1005

DP 18-825006 ~~before~~

B

9455 & 9533 BRIDGEPORT ROAD

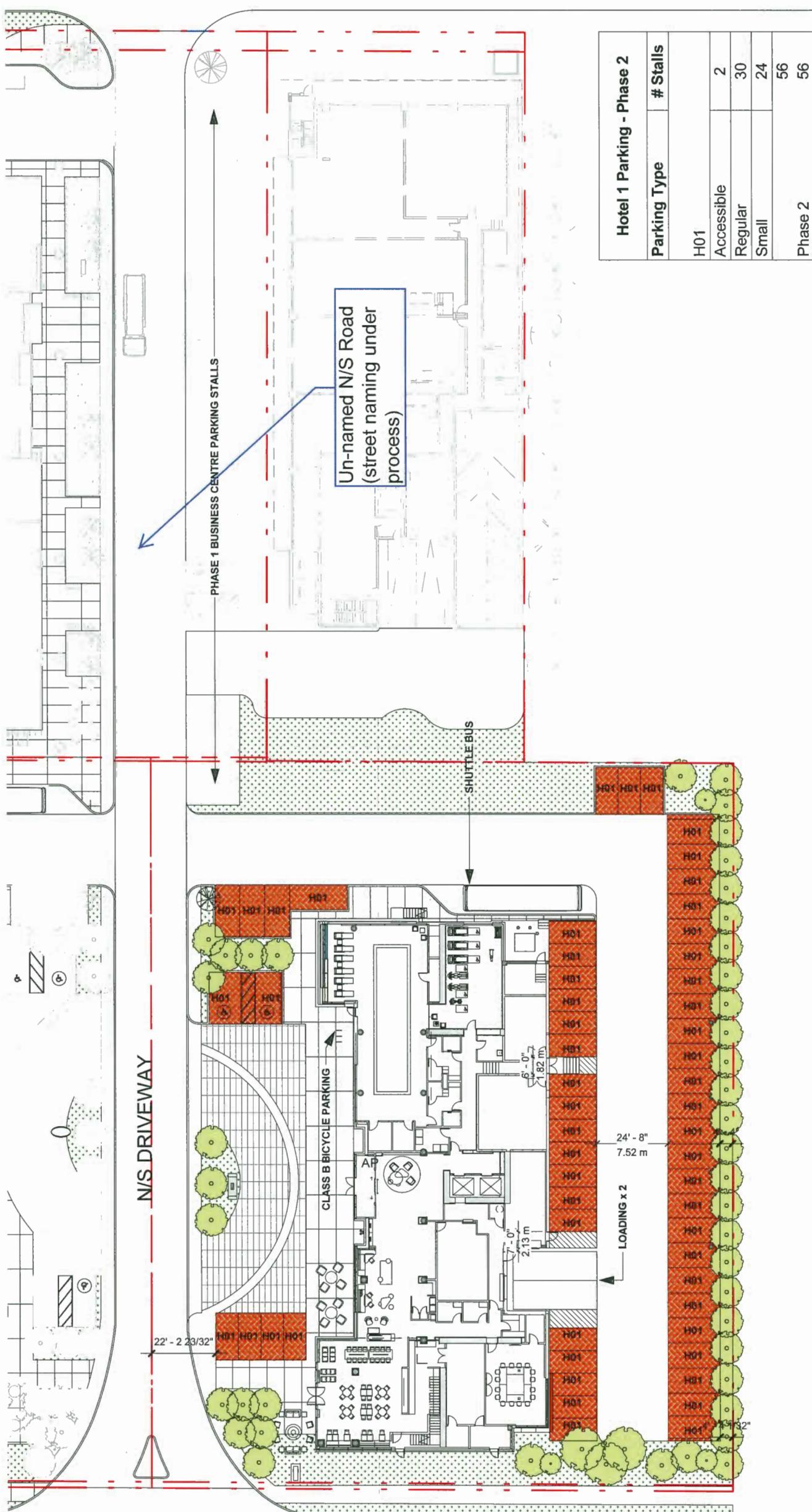


A 1007

DP 18-825006-Ref^a
^b32" = 11'0"

IBI**9455 & 9533 BRIDGEPORT ROAD**

BECKWITH RD



BRIDGEPORT RD

Hotel 1 Parking - Phase 2

| Parking Type | # Stalls |
|--------------|----------|
| H01 | 2 |
| Accessible | 30 |
| Regular | 24 |
| Small | 56 |
| Phase 2 | 56 |

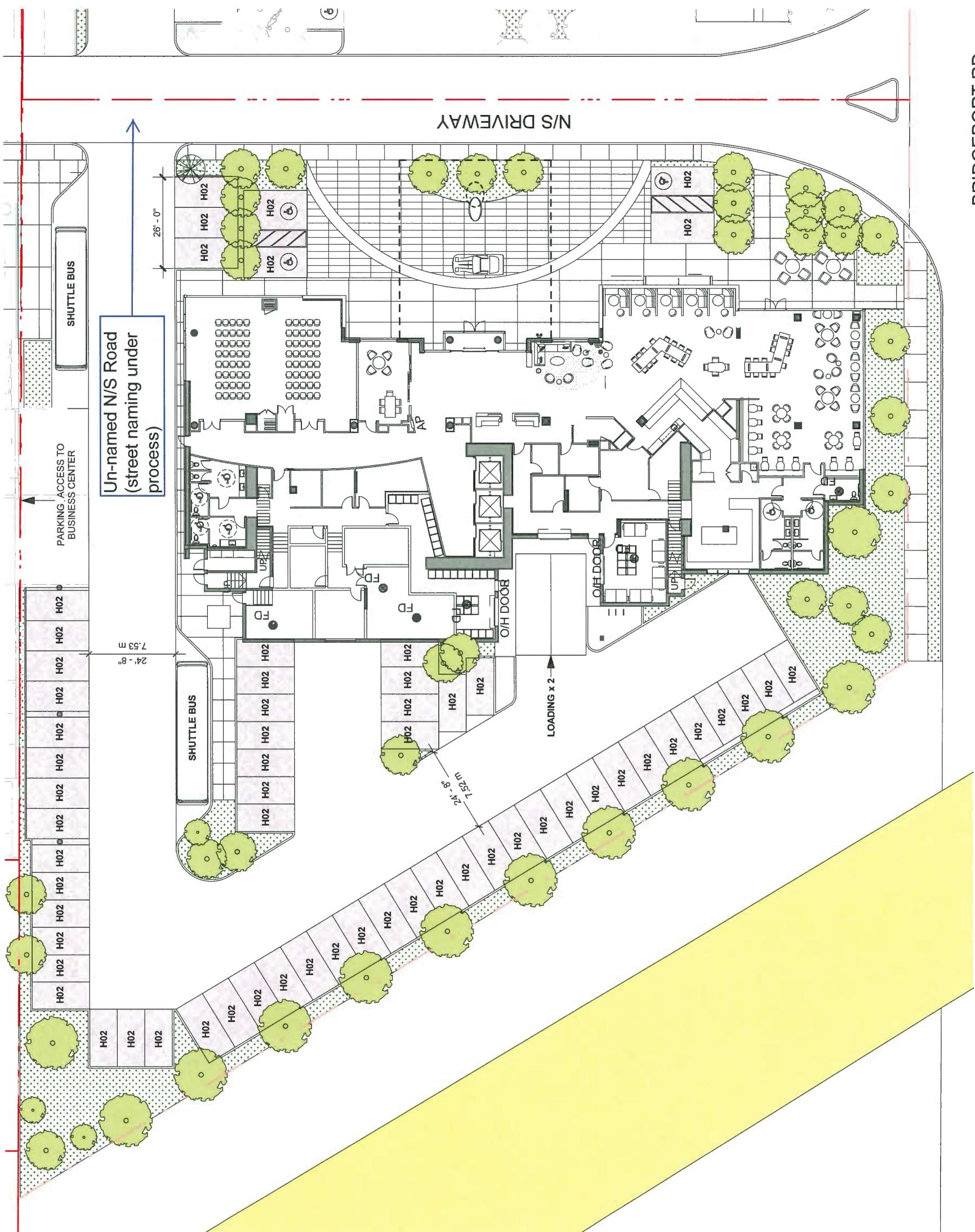
PHASE 2 PARKING COUNT - HOTEL 1 ON-SITE PARKING

A 1008a

DP 18-825006TS Reference

IBI

9455 & 9533 BRIDGEPORT ROAD

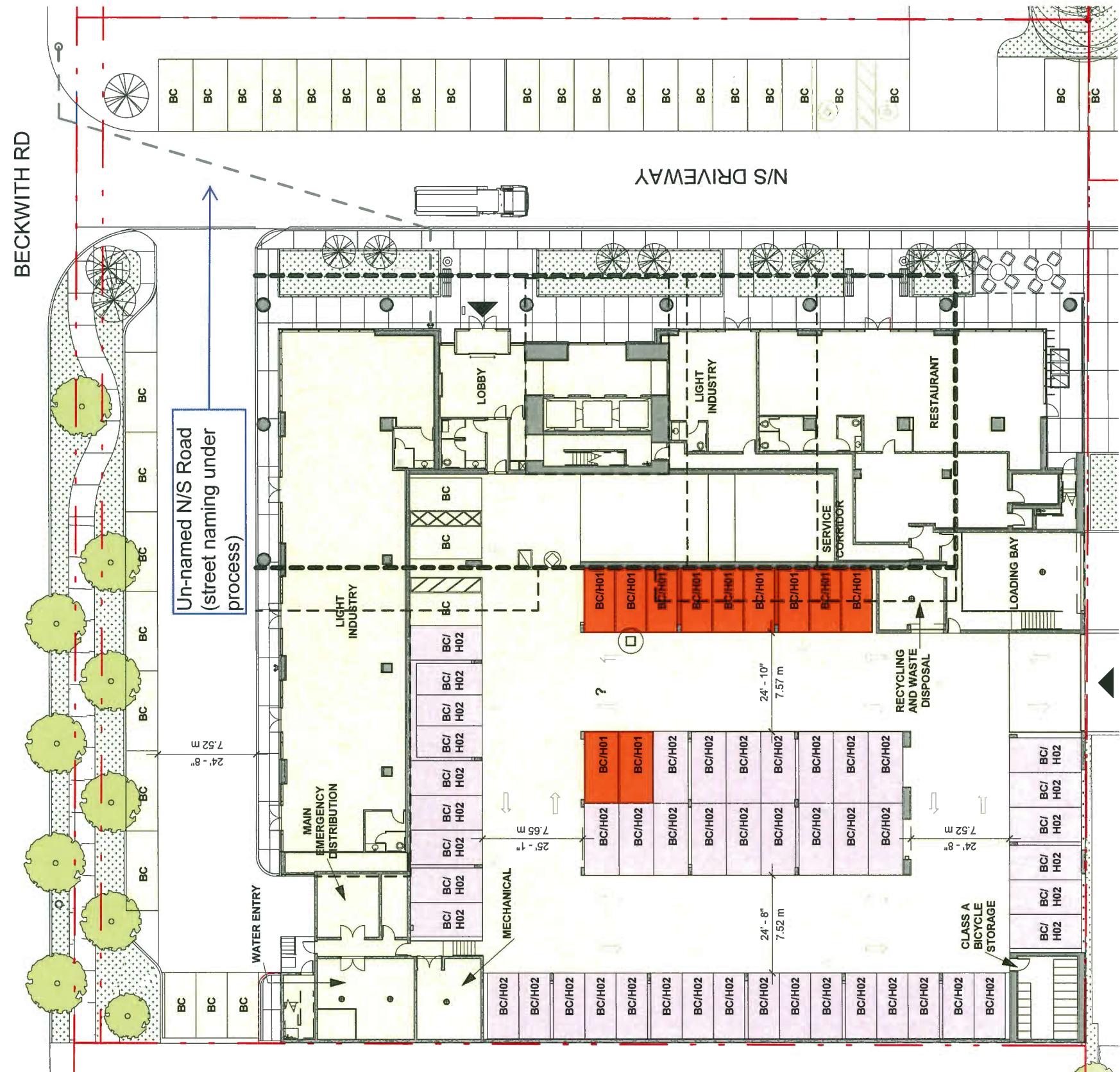


| Hotel 2 Parking - Phase 2 | |
|---------------------------|----------|
| Parking Type | # Stalls |
| H02 | |
| Accessible | 3 |
| Regular | 31 |
| Small | 27 |
| Phase 2 | 61 |

DP 18-825006-Permit
PHASE 2 PARKING COUNT - HOTEL 2 ON-SITE PARKING A 1008b
BRIDGEPORT RD

IBI

9455 & 9533 BRIDGEPORT ROAD



PHASE 1 PARKING COUNT - BUSINESS CENTRE GROUND FLOOR

A 1008c

DP 18-825006 - Reference

-
B
-

9455 & 9533 BRIDGEPORT ROAD

BECKWITH RD

A technical line drawing of a vertical pipe assembly. The pipe is shown in cross-section, revealing internal components like a ribbed liner and a central support. The exterior of the pipe is rough and textured. A horizontal flange or valve component is attached to the top of the pipe. The drawing uses perspective lines to show the three-dimensional nature of the object.

Section 7.5.14;

(d) provided with a ramp located at the end of the shared aisle as shown in Section 7.5.14.A accompanying and forming part of Section 7.5.14, and

(e) provided in the arrangement shown in Section 7.5.14.B accompanying and forming part of Section 7.5.14.

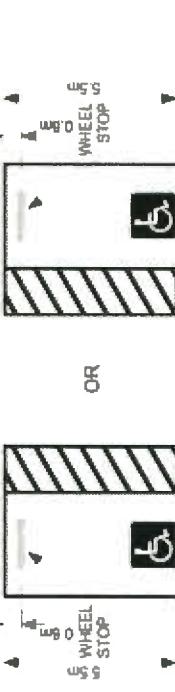
7.5.15 For residential uses that require a minimum of three visitor parking spaces, a minimum of 2% of the total required parking spaces, rounded upward to the nearest

whole number, shall be:

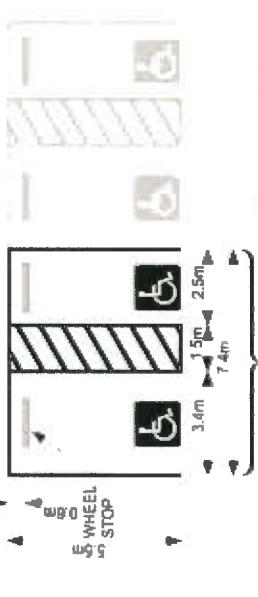
- (a) marked with a clearly visible sign identifying the spaces for use by disabled persons only as shown in Section 7.5.14.A accompanying and forming part of Section 7.5.14;
- (b) marked on the parking surface with the international symbol for wheelchair accessibility as shown in Section 7.5.14.A accompanying and forming part of Section 7.5.14; and

7.5.14.B

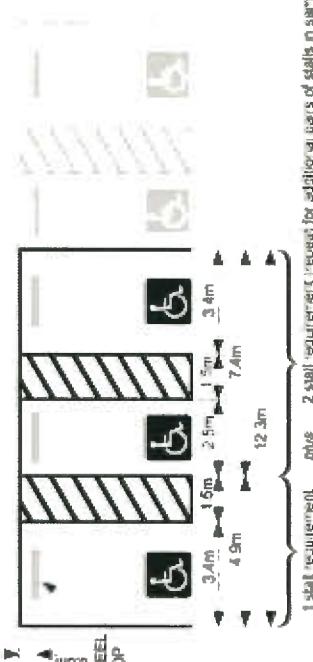
1. For one accessible stall required, such a stall shall be van accessible and have the following dimensions:



2. For two or more even number of accessible stalls required, the stalls shall be marked as-



3. For three or more odd number of accessible stalls required, a combination of requirements 1 and 2 shall be used and marked as:



NOTE: BUSINESS CENTRE IS NOT PART OF
THIS APPLICATION
APPROVED DP #11-594571
SUBMITTED FOR BP (BP#20180315)

LEGEND

BC = BUSINESS CENTRE
H01 = HOTEL 1
H02 = HOTEL 2
BC/H01, BC/H02 = SHARED

PARKING LEVEL P1&P2

PARKING SPACE TYPE (SMALL):
7'-10" x 16-5"

PARKING SPACE TYPE (REGULAR):
8'-8" x 18'

N/S DRIVEWAY

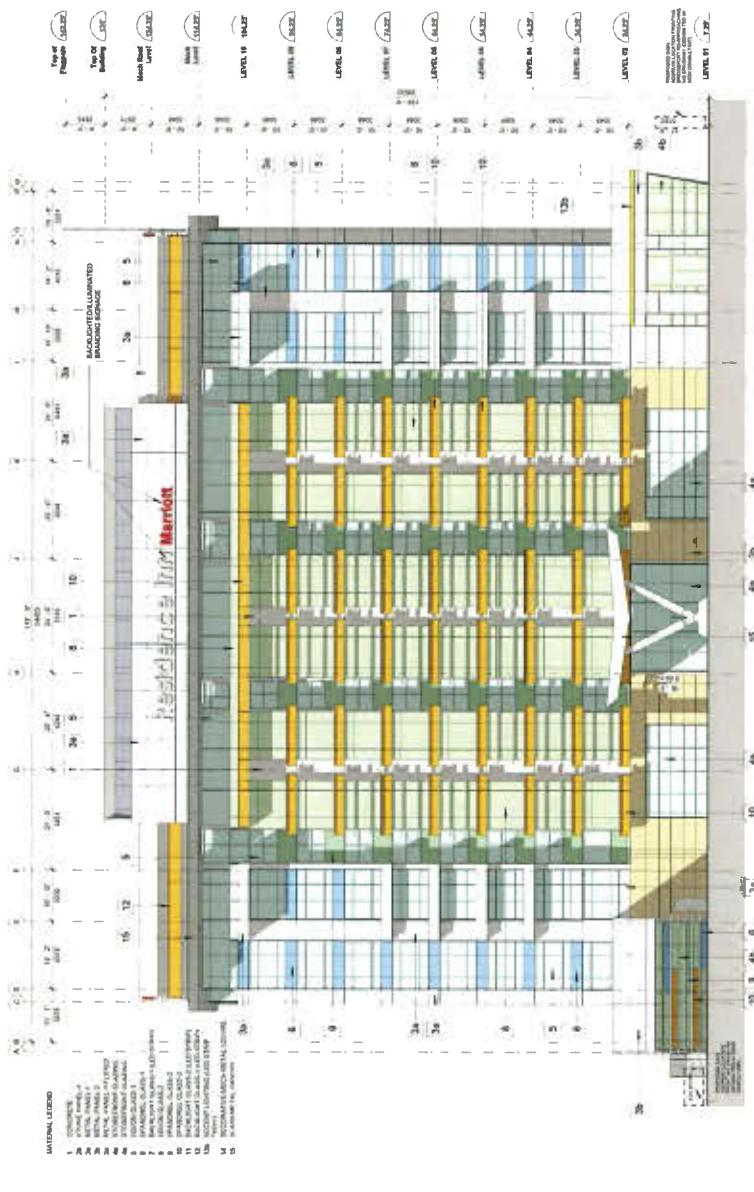
PHASE 1 PARKING COUNT - BUSINESS CENTRE PARKING LEVEL P2 A 1008d



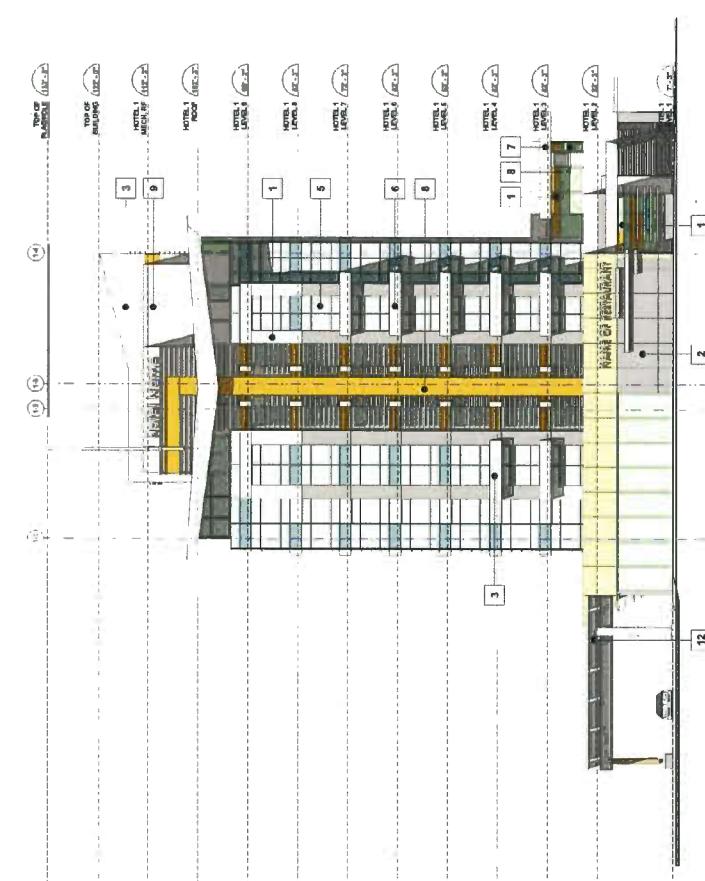
9455 & 9533 BRIDGEPORT ROAD

PROPOSED (NEW DP)

DP # 11-594571 (OLD)



HOTEL 1 WEST ELEVATION (OLD)



HOTEL 1 WEST ELEVATION (NEW)



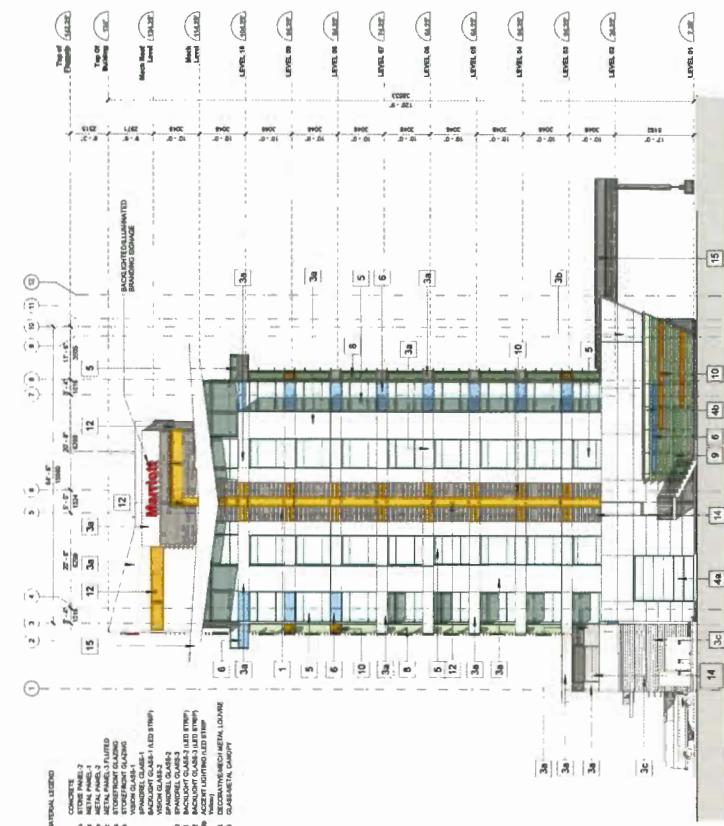
HOTEL 1 SOUTH ELEVATION (OLD)

HOTEL 1 SOUTH ELEVATION (NEW)

HOTEL 1 ELEVATION COMPARISON A 1013

IBI

PROPOSED (NEW DP)

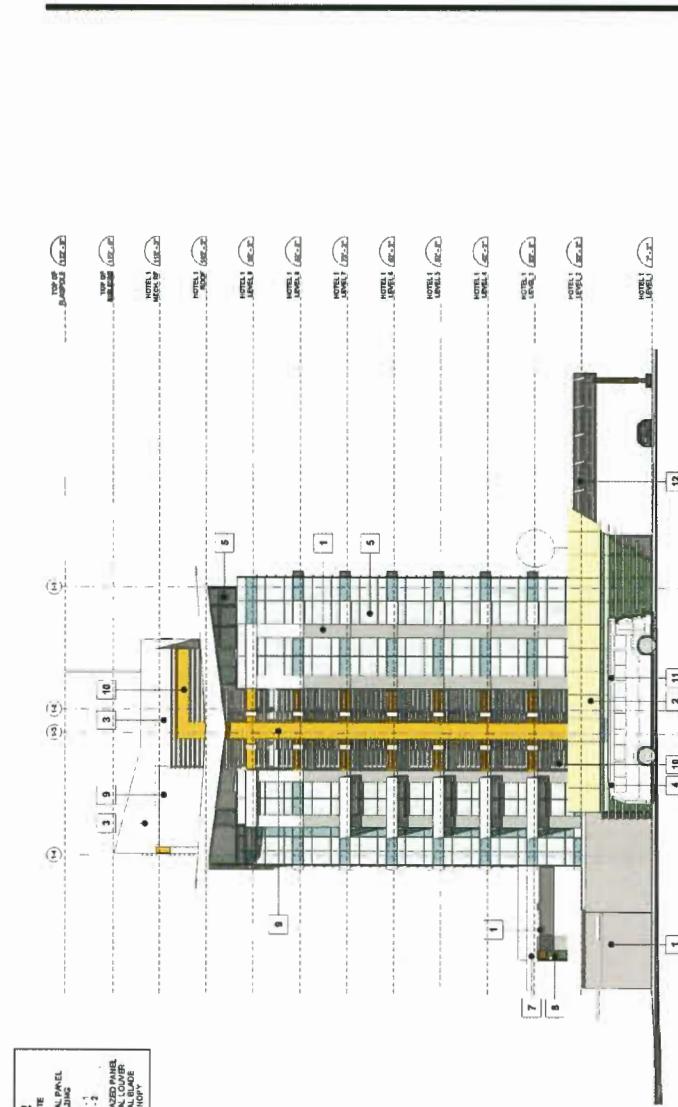


9455 & 9533 BRIDGEPORT ROAD

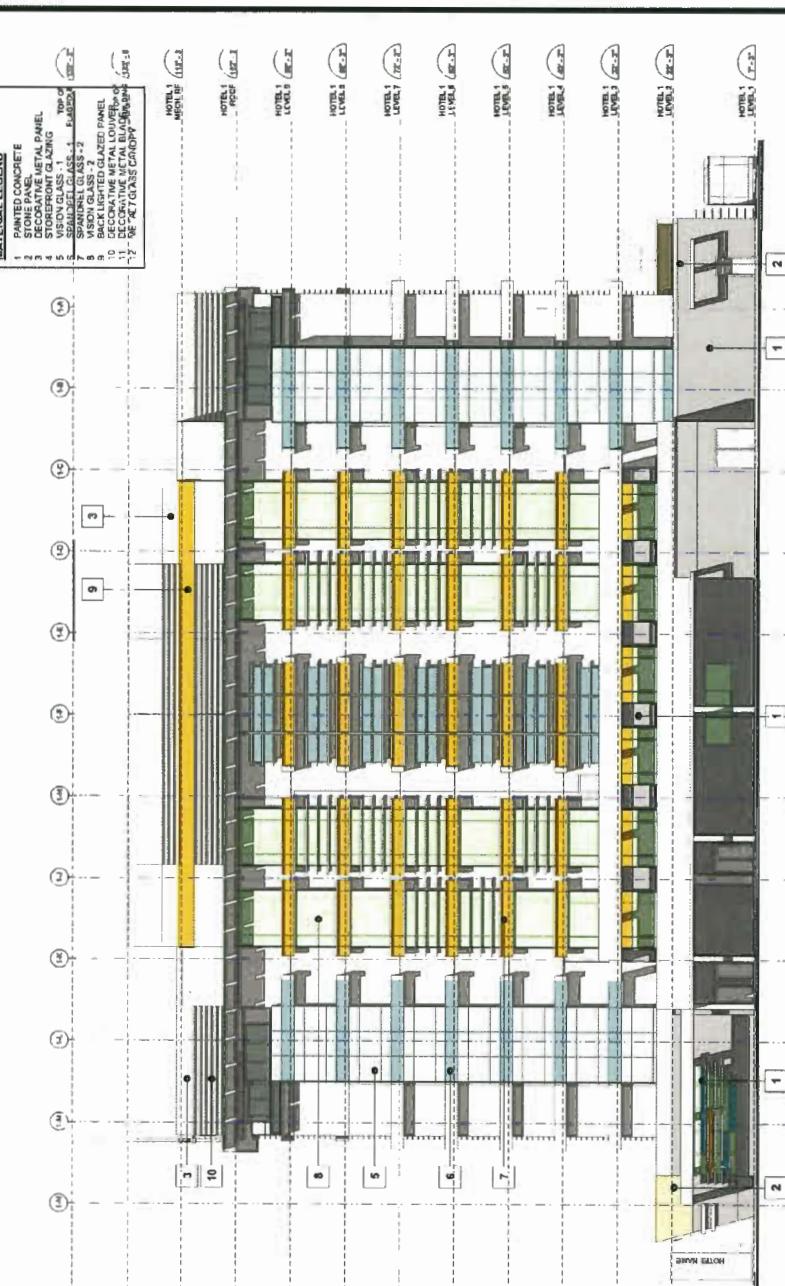
HOTEL 1 NORTH ELEVATION (NEW)



DP # 11-594571 (OLD)



HOTEL 1 NORTH ELEVATION (OLD)



HOTEL 1 EAST ELEVATION (OLD)

HOTEL 1 EAST ELEVATION COMPARISON

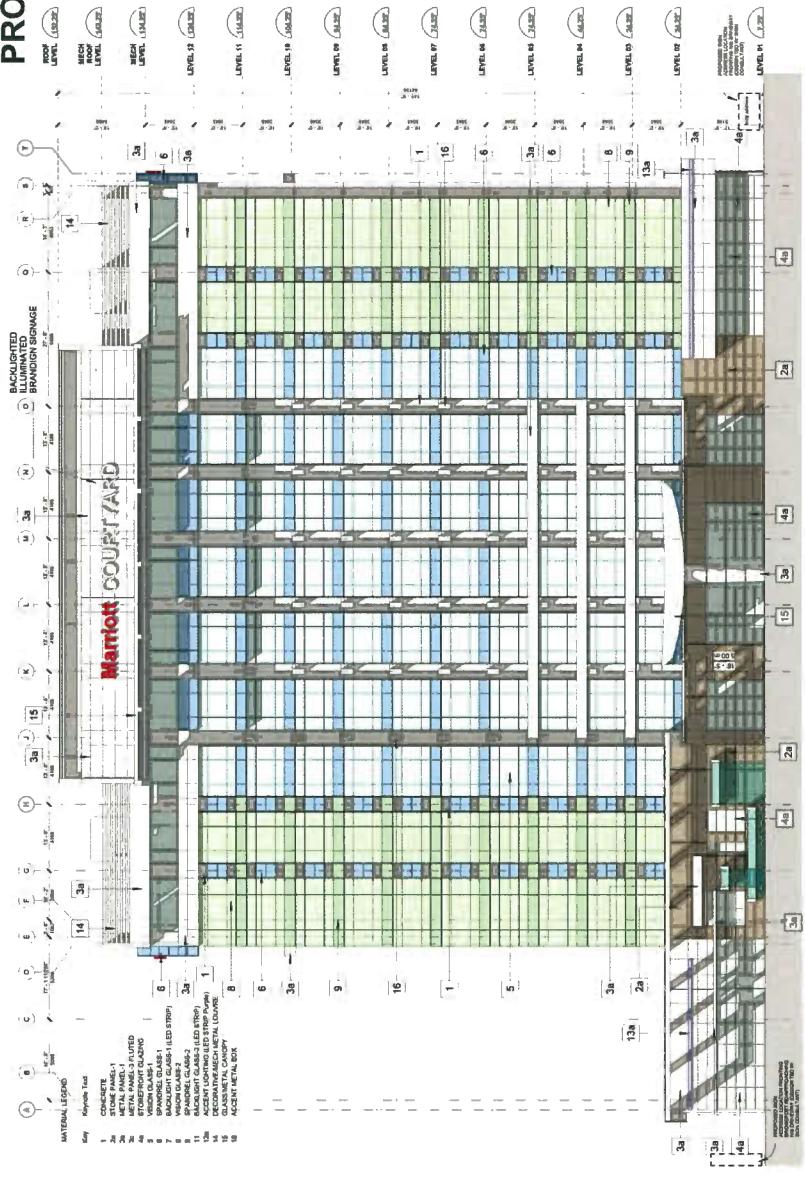
DP 18-8251116 - Reference

A 1014

IBI

9455 & 9533 BRIDGEPORT ROAD

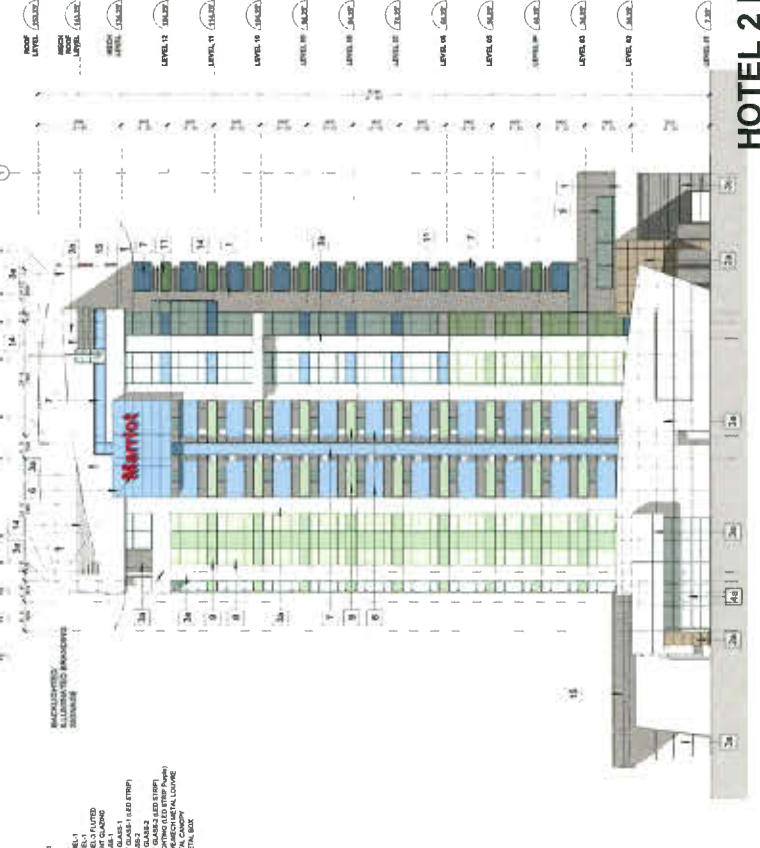
PROPOSED (NEW DP)



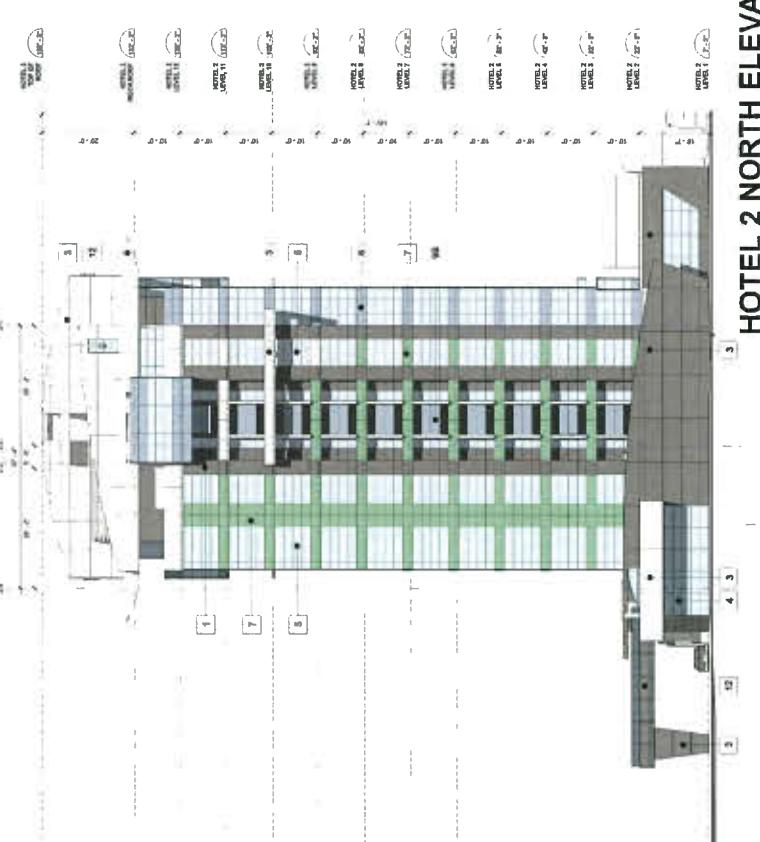
DP # 11-594571 (OLD)



HOTEL 2 EAST ELEVATION (NEW)



HOTEL 2 EAST ELEVATION (OLD)



HOTEL 2 NORTH ELEVATION (NEW)

HOTEL 2 NORTH ELEVATION (OLD)

Up 18-825006-Preference

A 1016

IBI

9455 & 9533 BRIDGEPORT ROAD

A 1021

HOTEL 1 RENDERING - DAY VIEW

DP 18-825006^{nts} Peterlee



IBI

9455 & 9533 BRIDGEPORT ROAD



HOTEL 2 RENDERING - DAY VIEW A 1022

UP 18-825006 - Reference

IBI

9455 & 9533 BRIDGEPORT ROAD



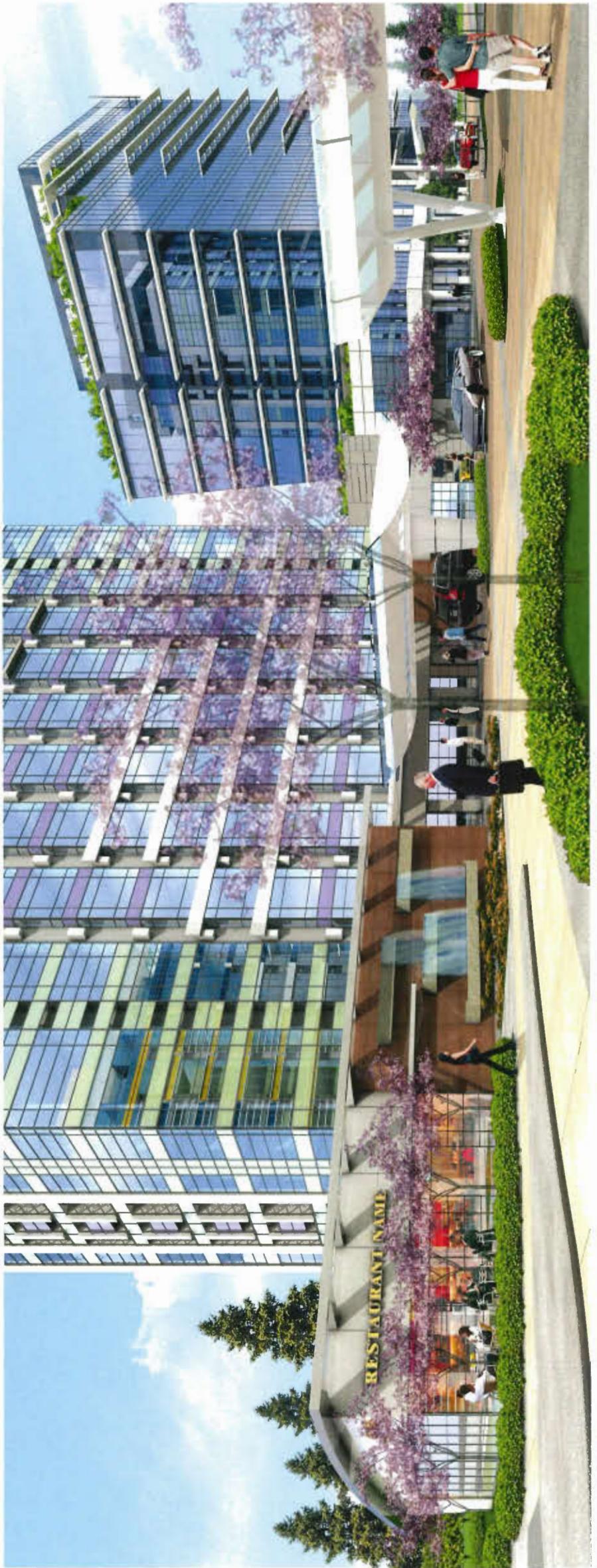
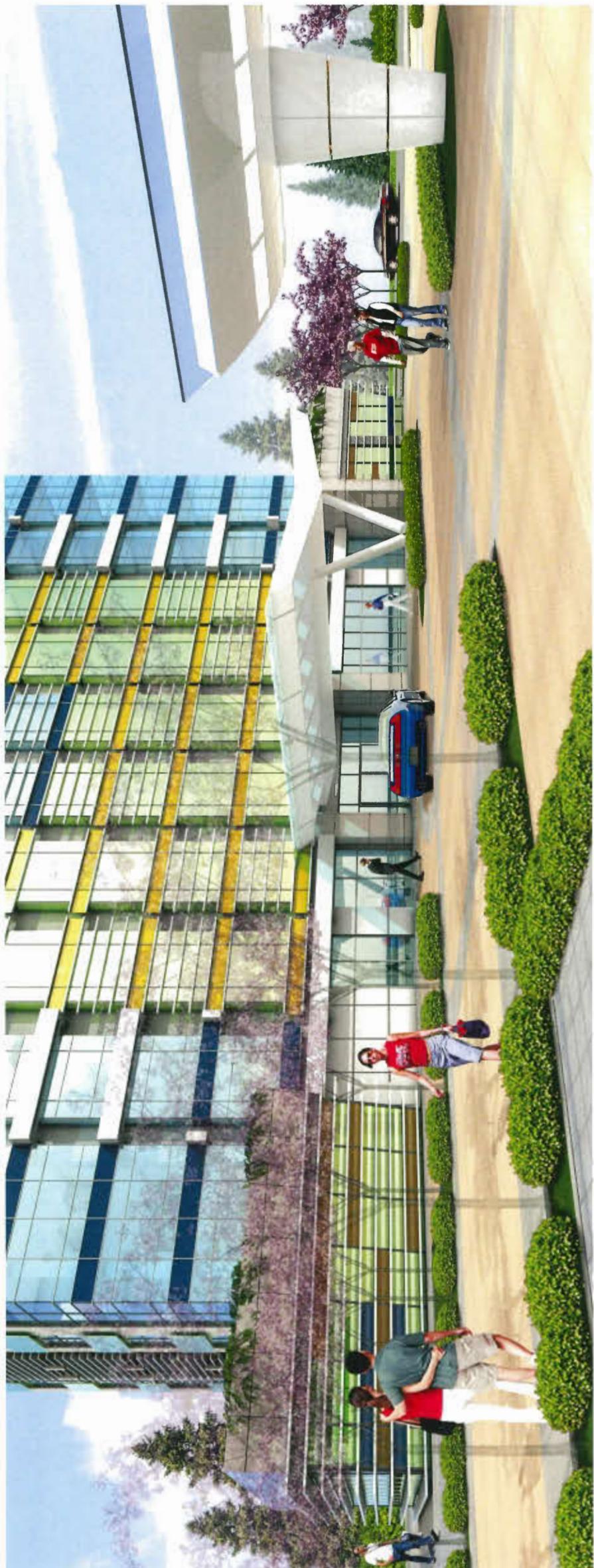
STREETSCAPE RENDERING

A 1026

DP 18-825006 - Reference

IBI

9455 & 9533 BRIDGEPORT ROAD



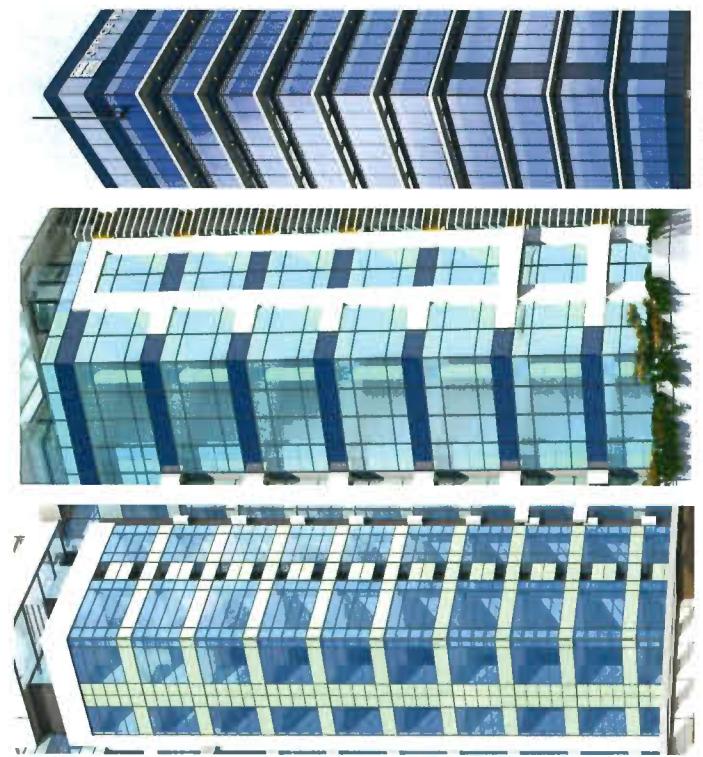
HOTELS 1 & 2 -STREETSCAPE RENDERING

A 1027

DP 18-825006-^{NTS}Preference

IBI

9455 & 9533 BRIDGEPORT ROAD



HOTEL 2.

A1 - All buildings show curtain glazing facades creating a check pattern.
A2 - All three buildings have strong horizontal elements thus creating harmonious connection between the buildings while maintaining individual character.
A3 - Shading components have been incorporated into all three building designs providing a connected functionality visually, while the practical functionality of the building differs.



Height and mass reinforce their similarities and visual connection.



F1 - Set backs created on different levels between the Business Center and the Hotels 1 & 2 still emits a correlation between all three buildings allowing a prominent horizontal feature, relating the three buildings visually.

* BUSINESS CENTER IS NOT PART OF THIS APPLICATION
SUBMITTED FOR BP (BP#20180315)

C1 - Curtain glazed corners are all visually showing the same wrap-around concept using different colors and material to achieve the same visual aesthetic.
C2 - Verticality aspects are also visually present within all three buildings strengthening the relationship between each build but diversity in the materials used allows for individuality.



D1 - The canopy overhangs of the three buildings create a strong identity of good visual and physical connection between the street, their lobbies and both hotels. Their identical joint connections replicated with a similar frame and glass finish connect all three canopies of different proportions.
D2 - The columns used for support of the large Hotels canopy speak the same language as the columns around the Business Center, replicating importance and grandeur.



E1 - The predominant base is found for all three buildings with an ideal ratio proportion to their building counterparts.
E2 - They are completed with interesting features such as, a green roof or water featured wall combining the relationship of natural elements amongst the three buildings.
E3 - Active ground floor use shown in all three buildings base have similar function which will create interest, activity and safety along the street front but also a relationship commercially and visually.

VISUAL CONNECTION BETWEEN BUILDINGS

DP 18-825006 - Le Florence

A 1030

IBI**9455 & 9533 BRIDGEPORT ROAD**

Hotel 2 - Entrance view

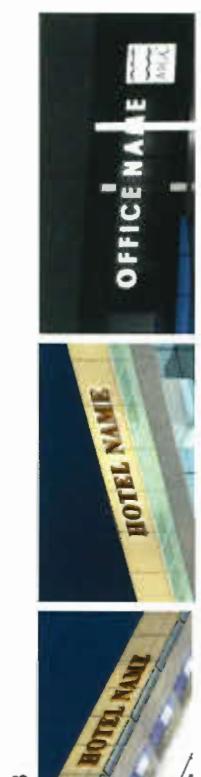
Hotel 2 - Rear view

Business Center

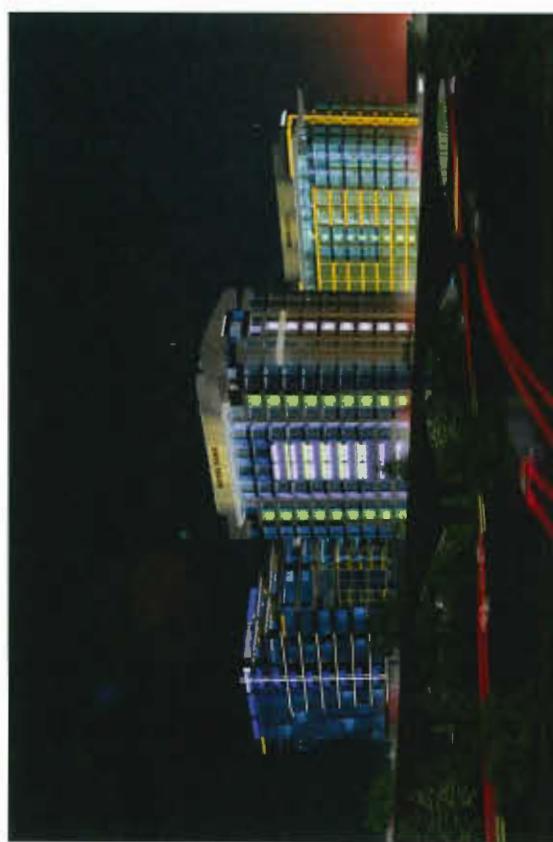
A



B

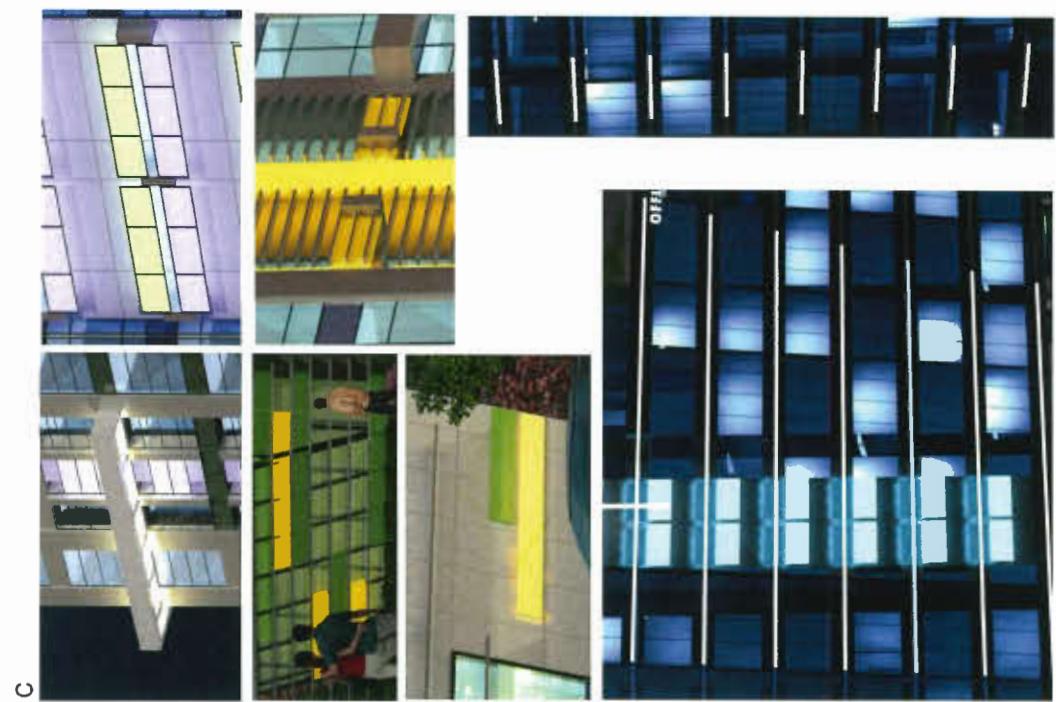


Signage is illuminated by lighting for all buildings. Visibility is clear from street view and allows a uniformity between all buildings.



Accent Lighting features on each of the three buildings providing distinct character, creating a public art, emphasizing the relationship and connectivity between each of the buildings as shown in images A, and elaborating the unique functional use and nature of each structure.

Accent Lighting is visible on each building facade highlighting particular elements as shown in images C in an artistic fashion of public display. This gives a connectivity between each building while the different illuminated aspects give each building an individuality.



* BUSINESS CENTER IS NOT PART OF THIS APPLICATION
SUBMITTED FOR BP (BP#20180315)

VISUAL CONNECTION BETWEEN BUILDINGS

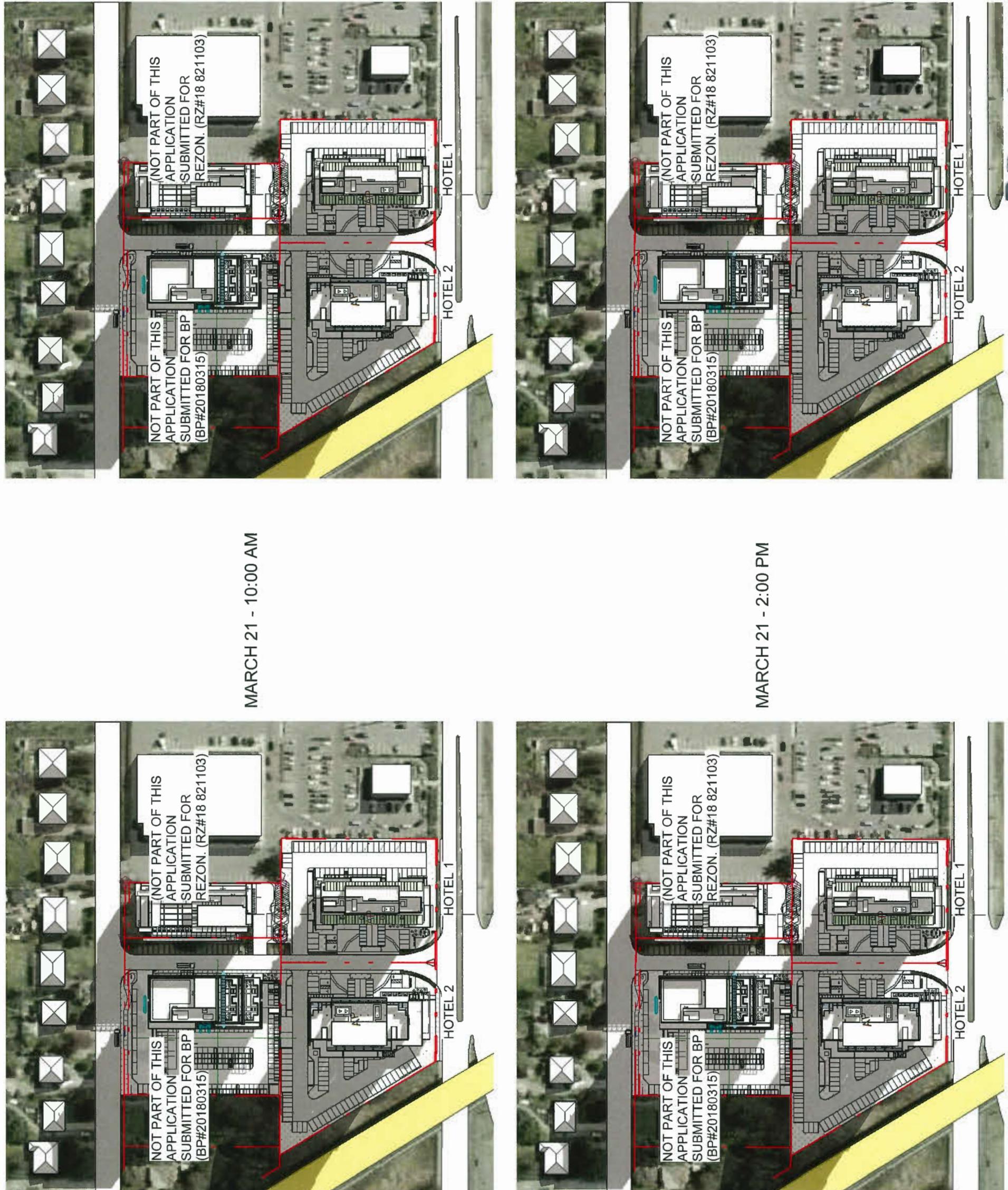
A 1031

DP 18-825006 Reference

IBI



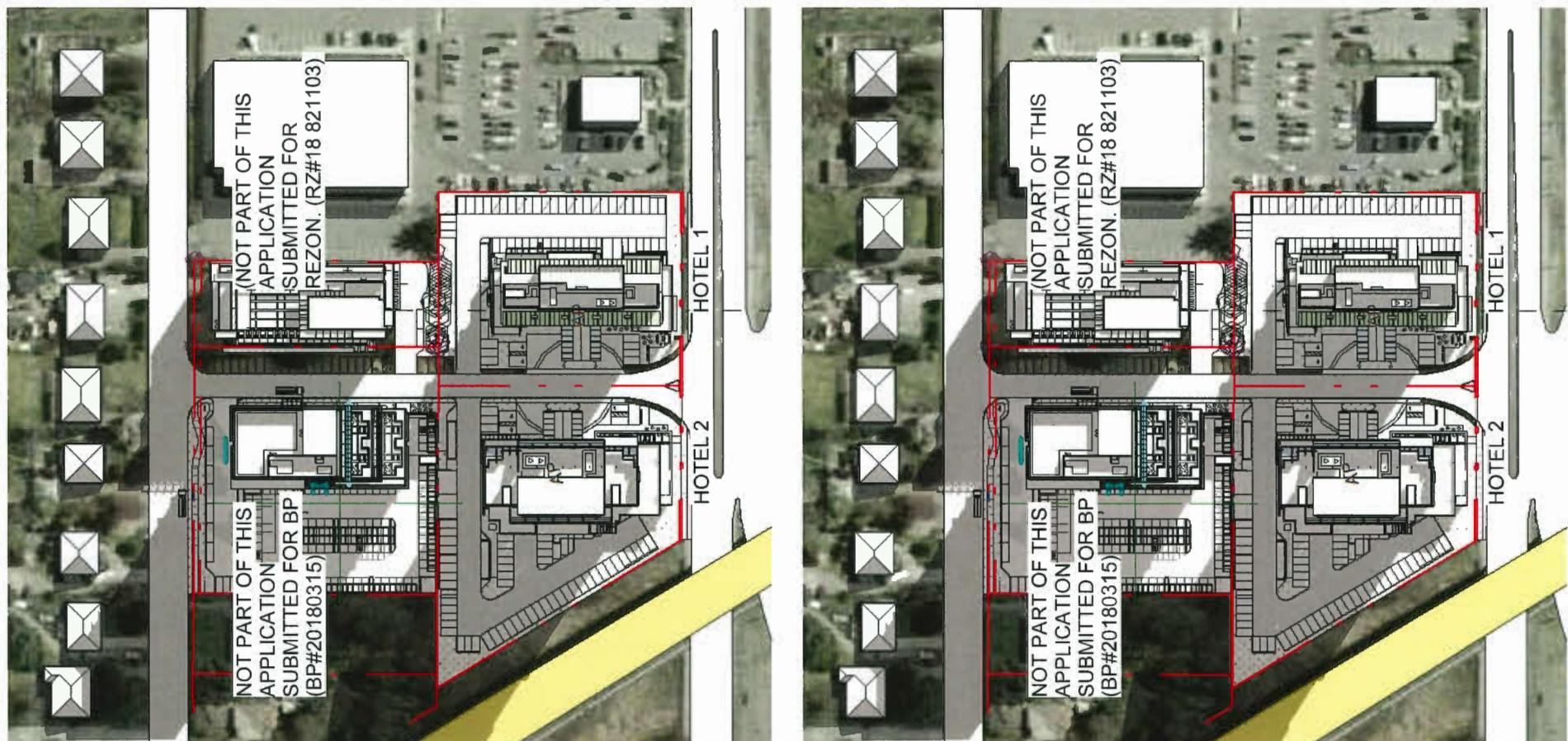
9455 & 9533 BRIDGEPORT ROAD



DP 18-825006-Permit
SHADOW STUDY A 1103

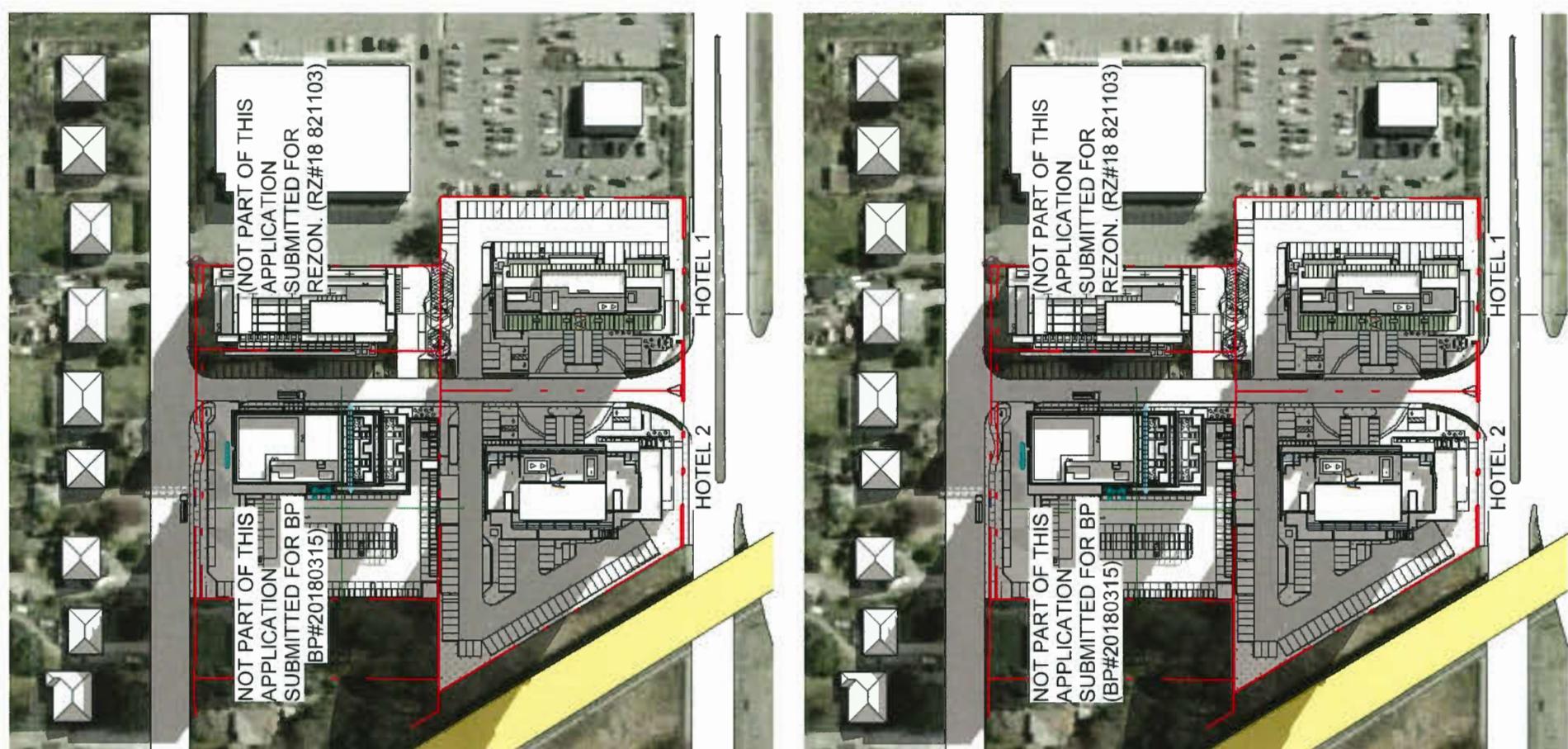
IBI

9455 & 9533 BRIDGEPORT ROAD



SEPT. 22 - 10:00 AM

SEPT. 22 - 2:00 PM



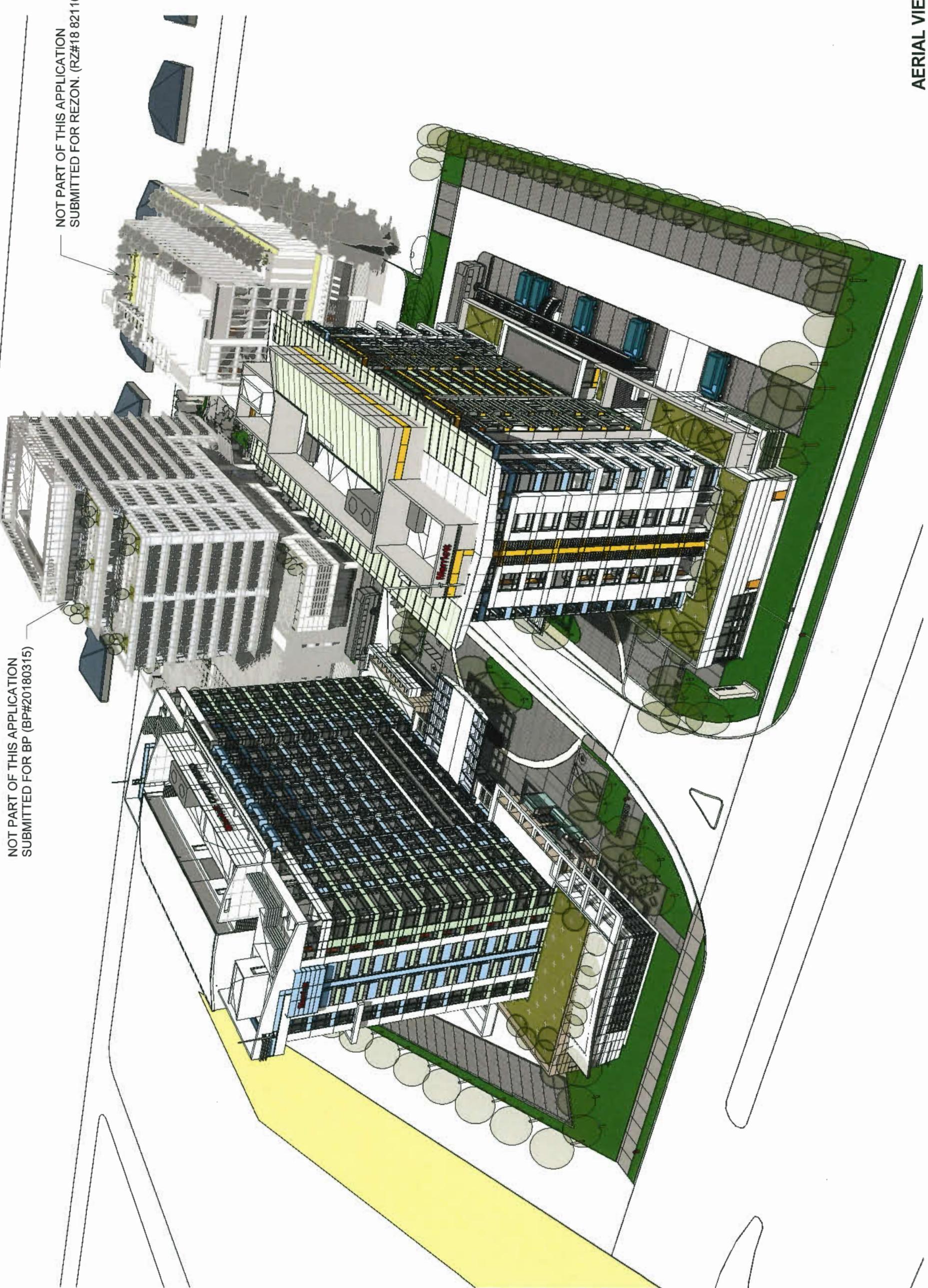
SHADOW STUDY

A 1104

DP 18-824006 Reference
1st = 80'-0"

IBI

9455 & 9533 BRIDGEPORT ROAD



AERIAL VIEW 1

A 1601

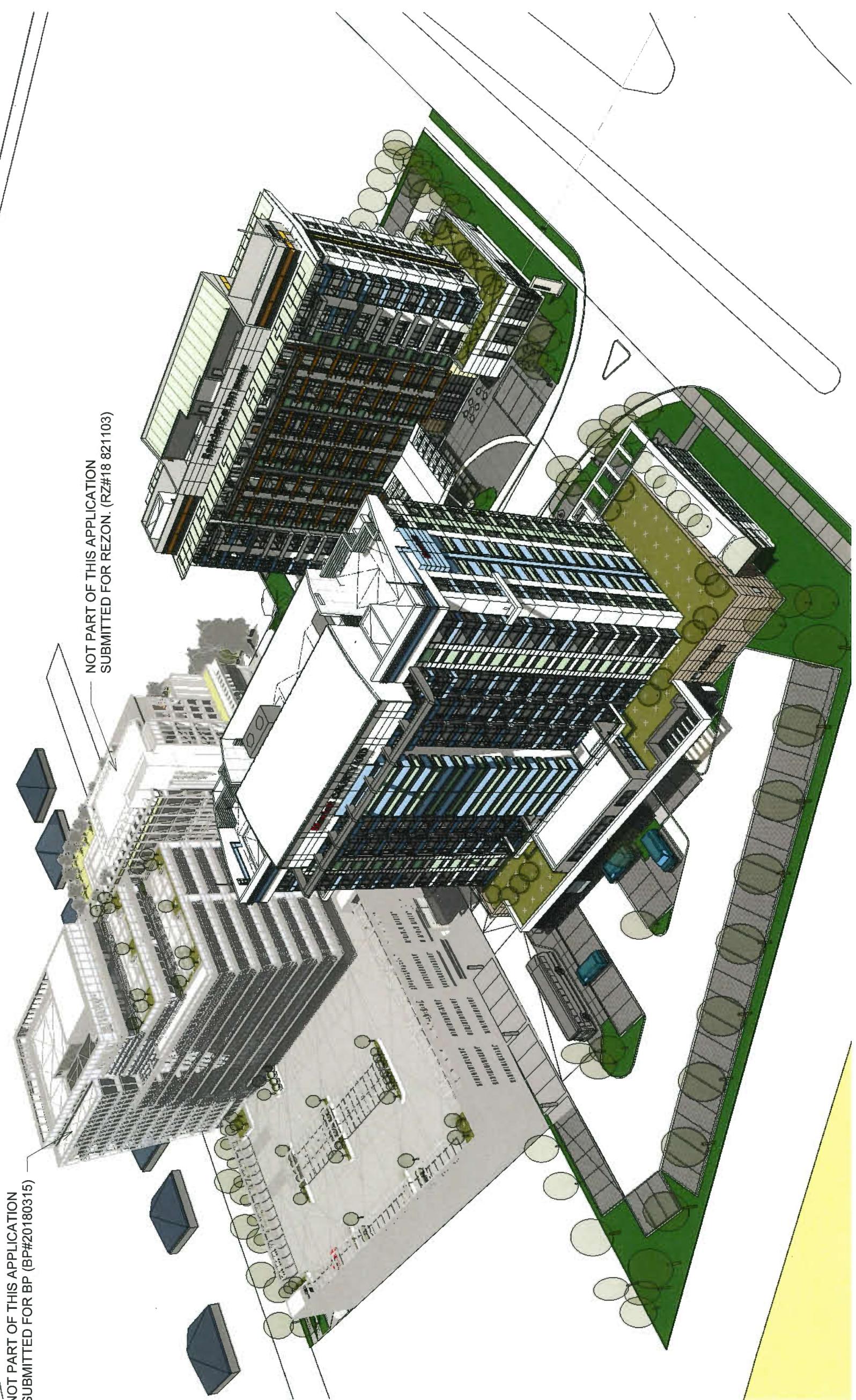
DP 18-825006 -Retenue
NTS

IBI

9455 & 9533 BRIDGEPORT ROAD

NOT PART OF THIS APPLICATION
SUBMITTED FOR BP (BP#20180315)

NOT PART OF THIS APPLICATION
SUBMITTED FOR REZON. (RZ#18 821103)



AERIAL VIEW 2 A 1602

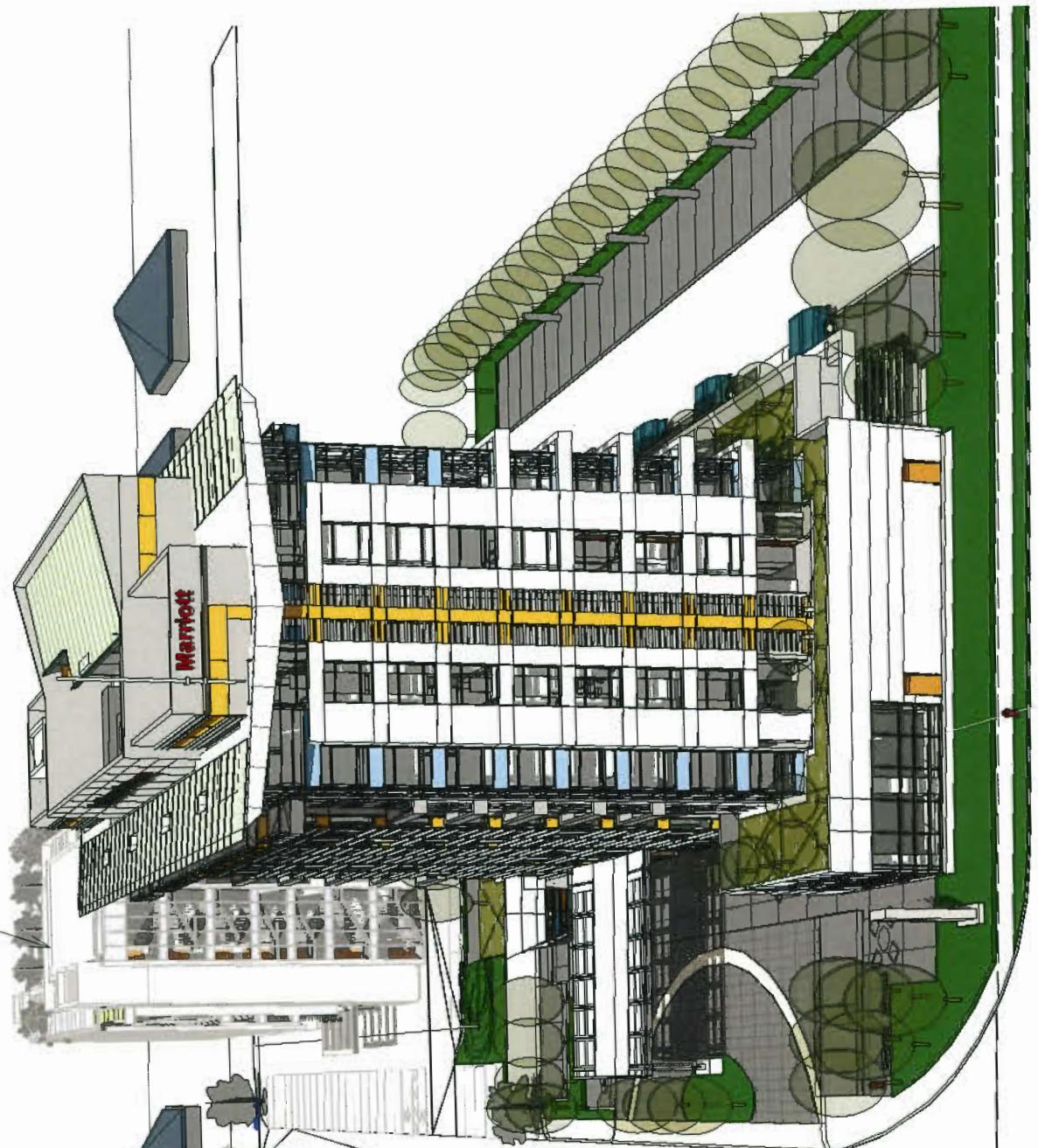
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NTS

IBI

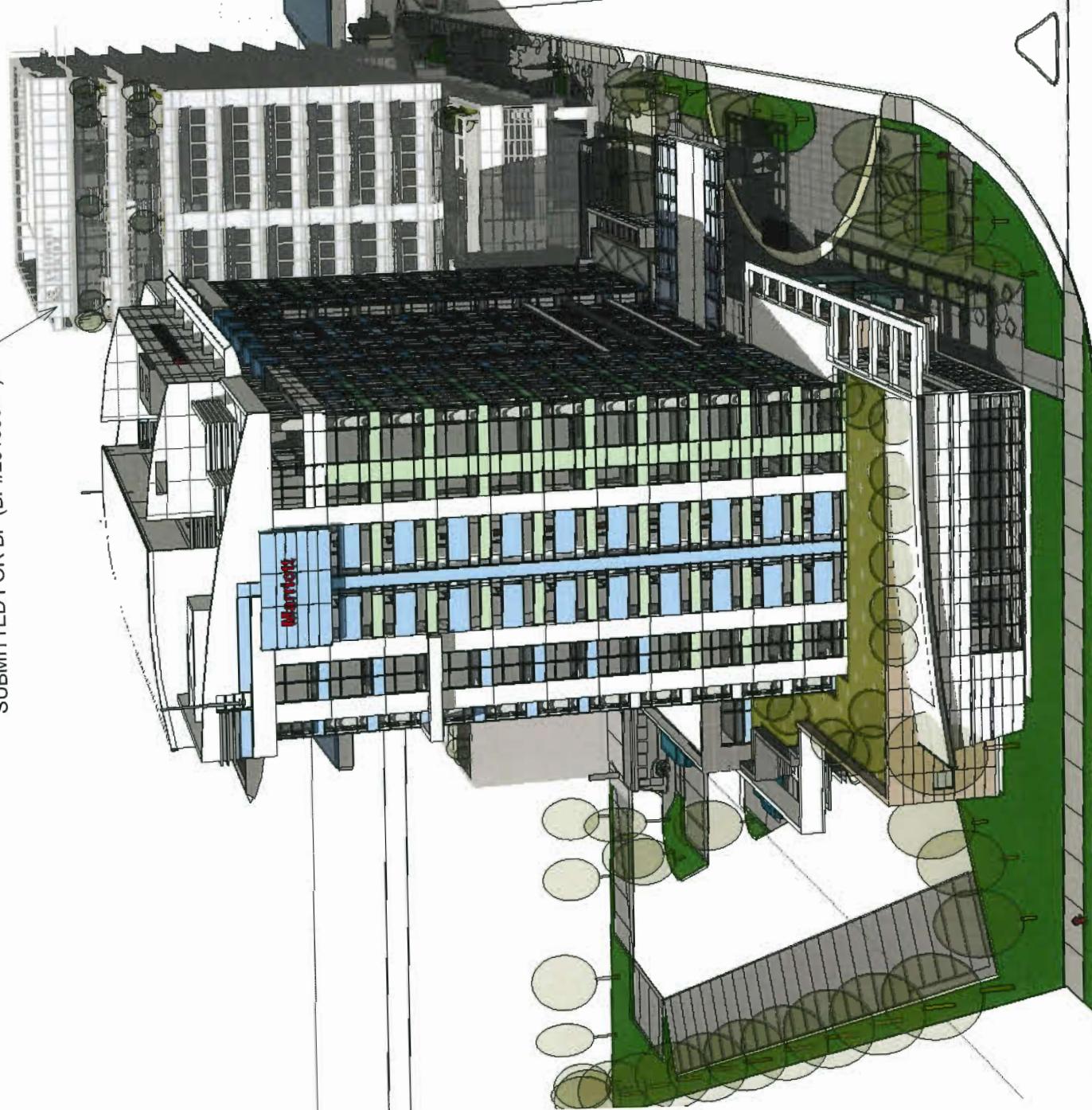
9455 & 9533 BRIDGEPORT ROAD

UP 18-825006 - Reference
AERIAL VIEW 3 A 1603

NOT PART OF THIS APPLICATION
SUBMITTED FOR REZON. (RZ#18 821103)

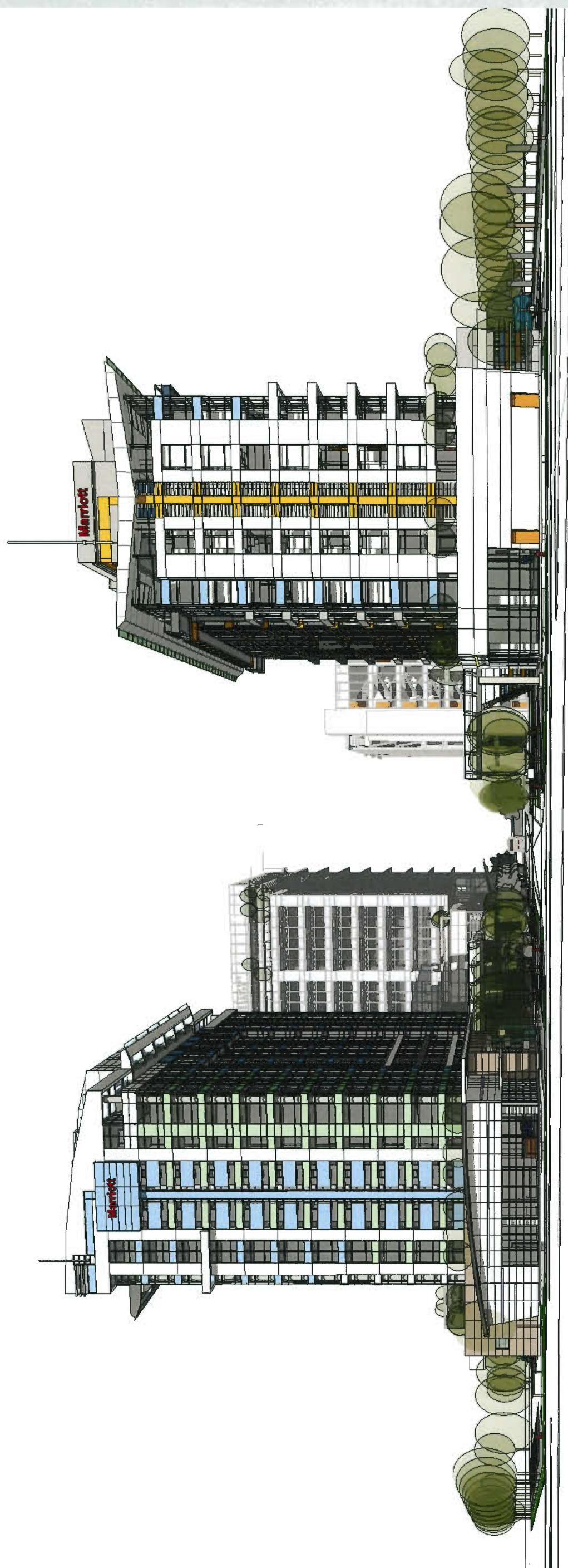


NOT PART OF THIS APPLICATION
SUBMITTED FOR BP (BP#20180315)



IBI

9455 & 9533 BRIDGEPORT ROAD



PERSPECTIVE BRIDGEPORT RD 1
A 1611

DP 18-825006 - Reference

IBI

9455 & 9533 BRIDGEPORT ROAD

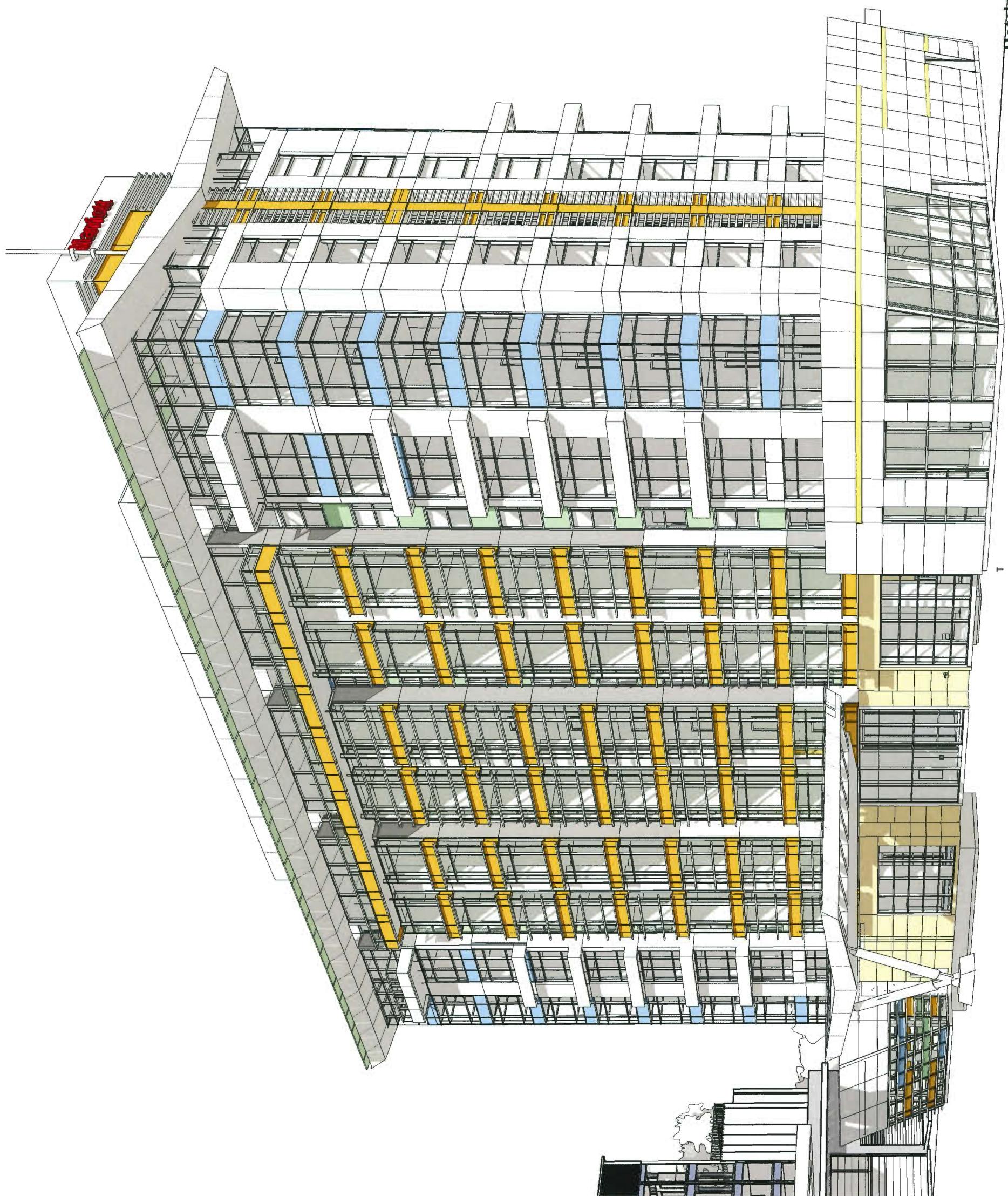


PERSPECTIVE BRIDGEPORT RD 2
A 1612
DP 18-825006 - Reference

IBI

HOTEL 1

Hotel 01 - Perspective 01 A 2100a



DP 18-825006 - Reference

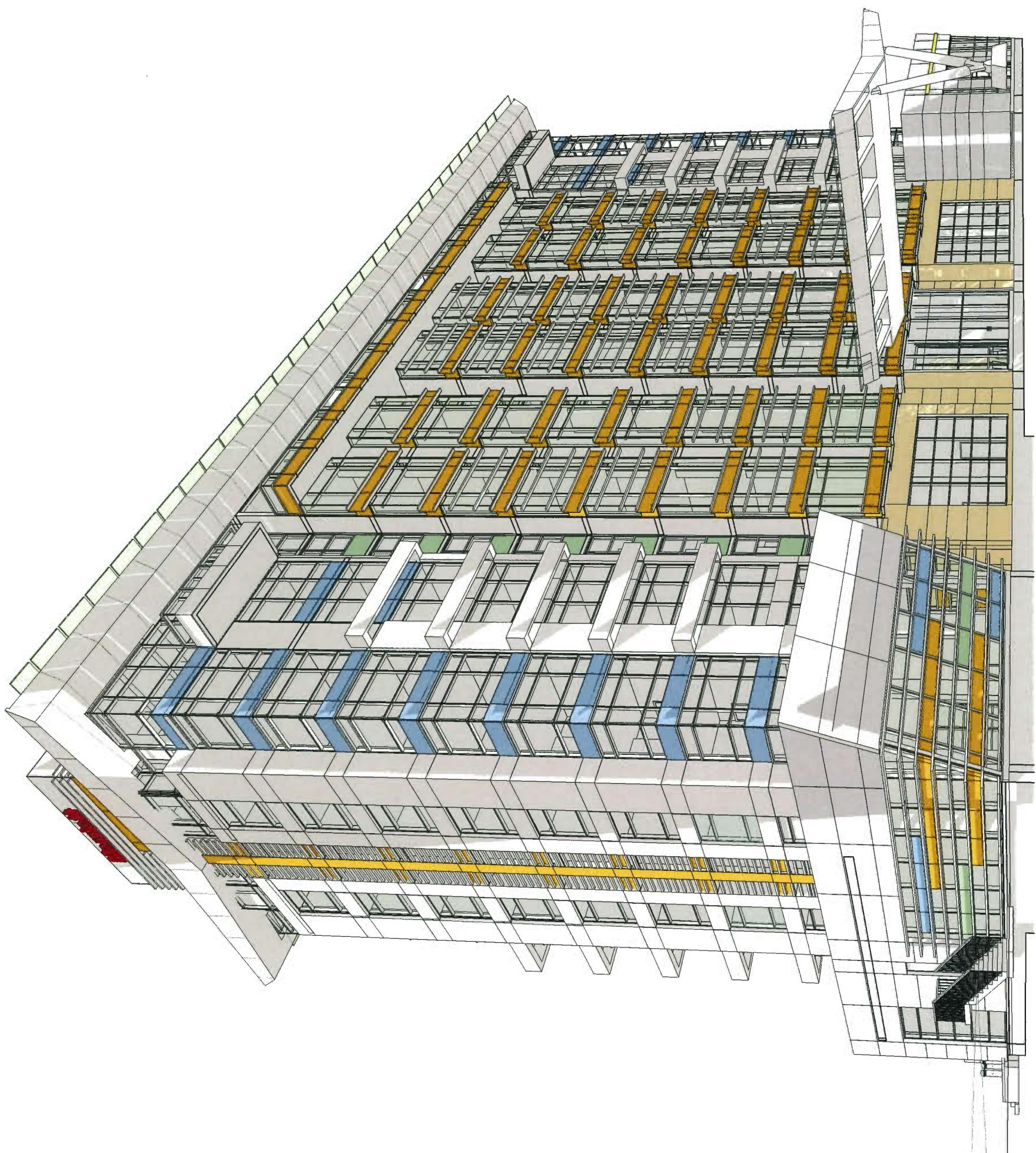
IBI

HOTEL 1

Hotel 01 - Perspective 02

DP 18-825006 - Reference

A 2100b

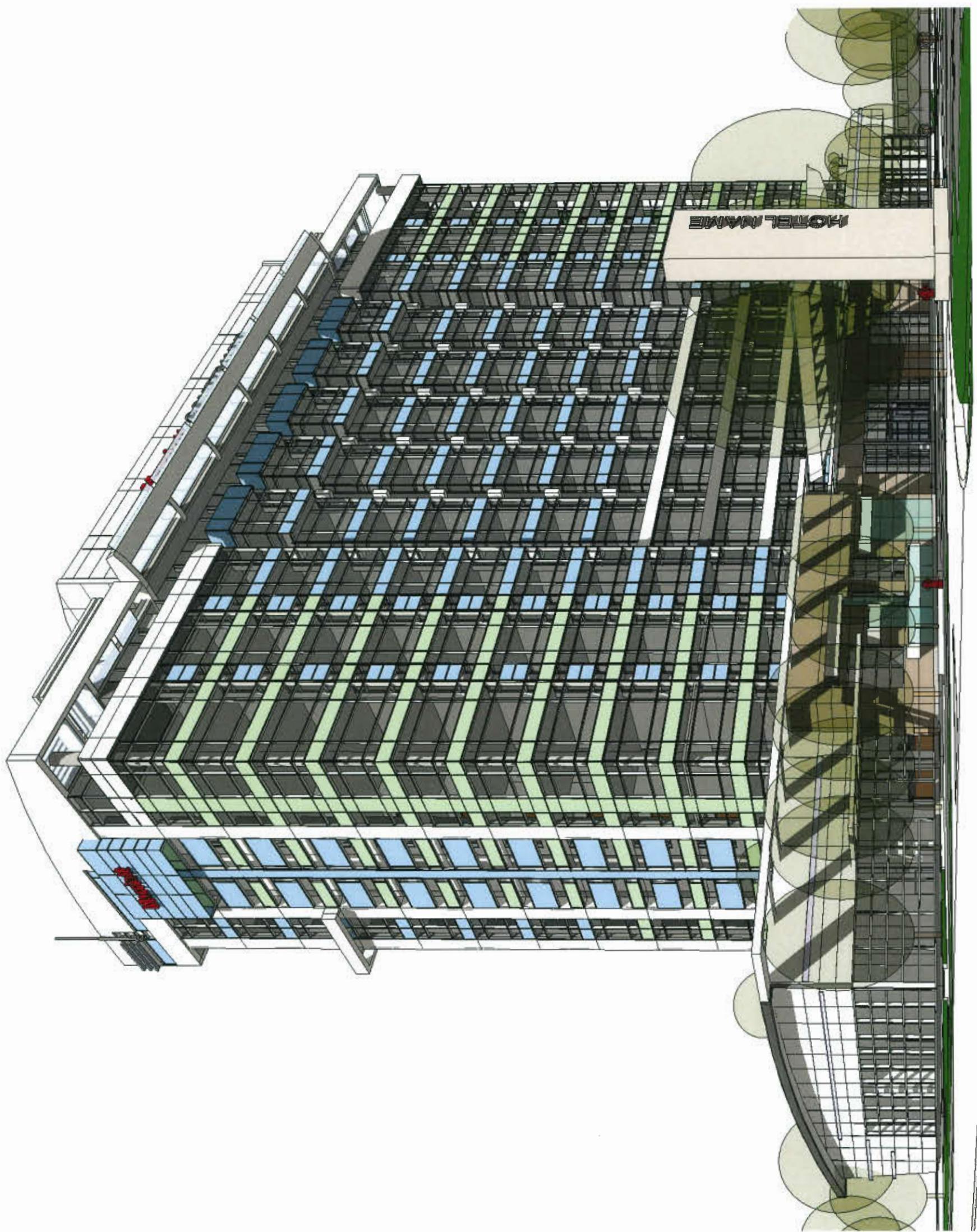


IBI

9455 & 9533 BRIDGEPORT ROAD

HOTEL 2 PERSPECTIVE FROM BRIDGEPORT ST 1 A 3001

DP 10-095006 - Peterence



IBI

9455 & 9533 BRIDGEPORT ROAD

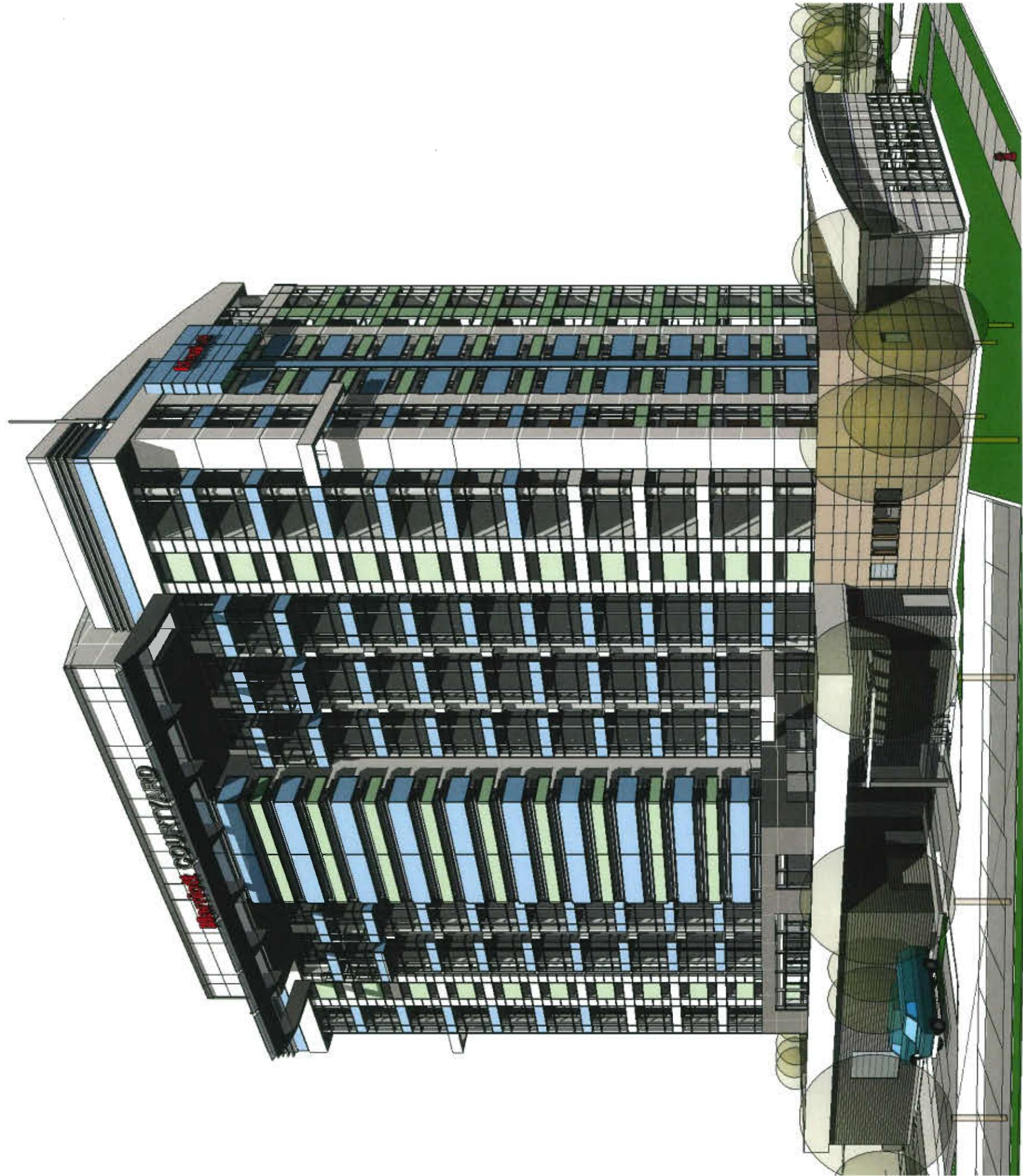
DP 18-825006 - Reference
HOTEL 2 PERSPECTIVE FROM NORTH EAST A 3002

IBI

9455 & 9533 BRIDGEPORT ROAD

HOTEL 2 PERSPECTIVE FROM OAK ST BRIDGE 1 A 3003

DP 18-825006 NTP Reference

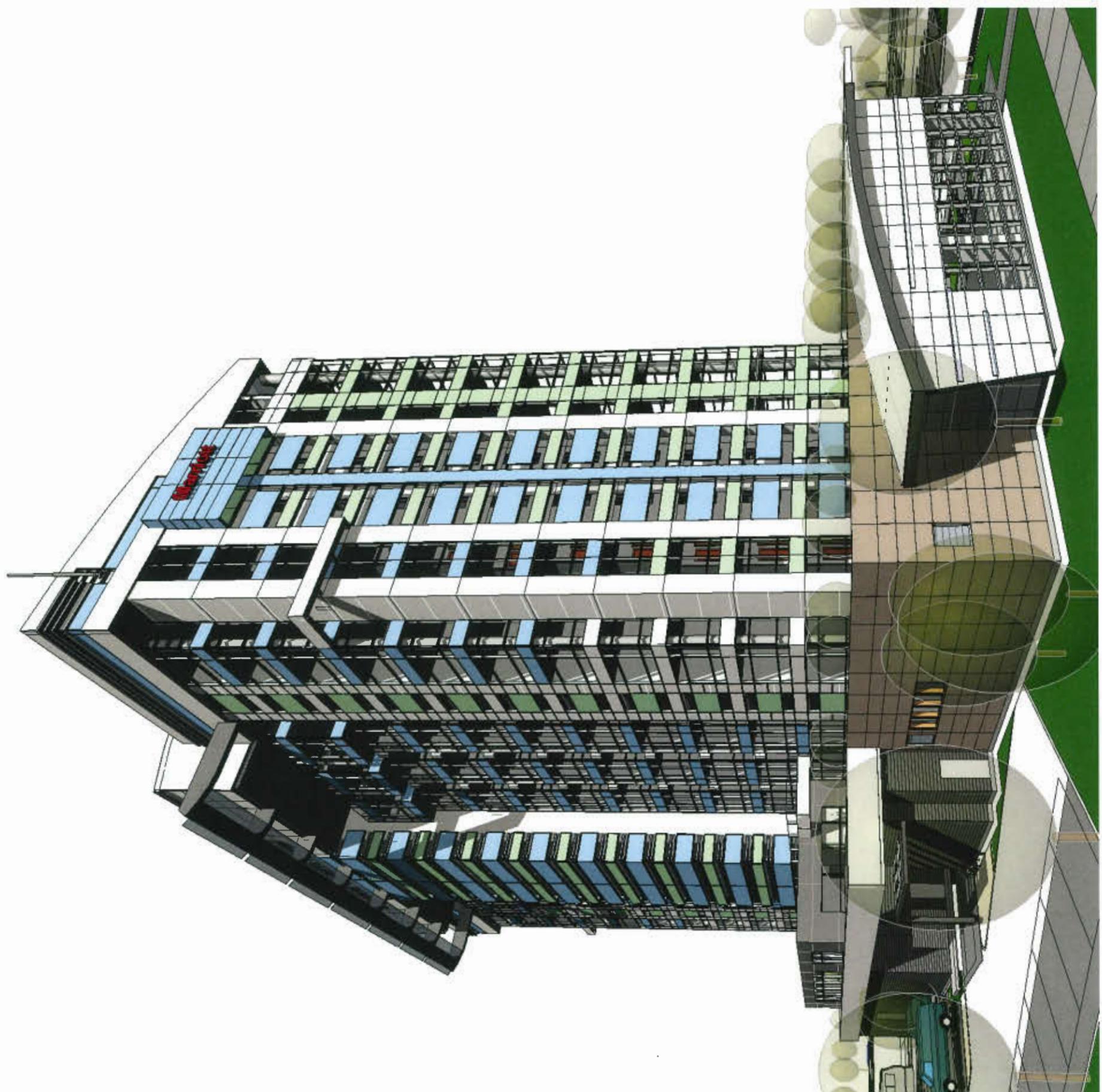


IBI

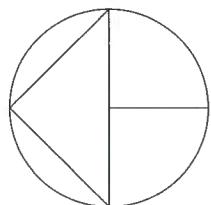
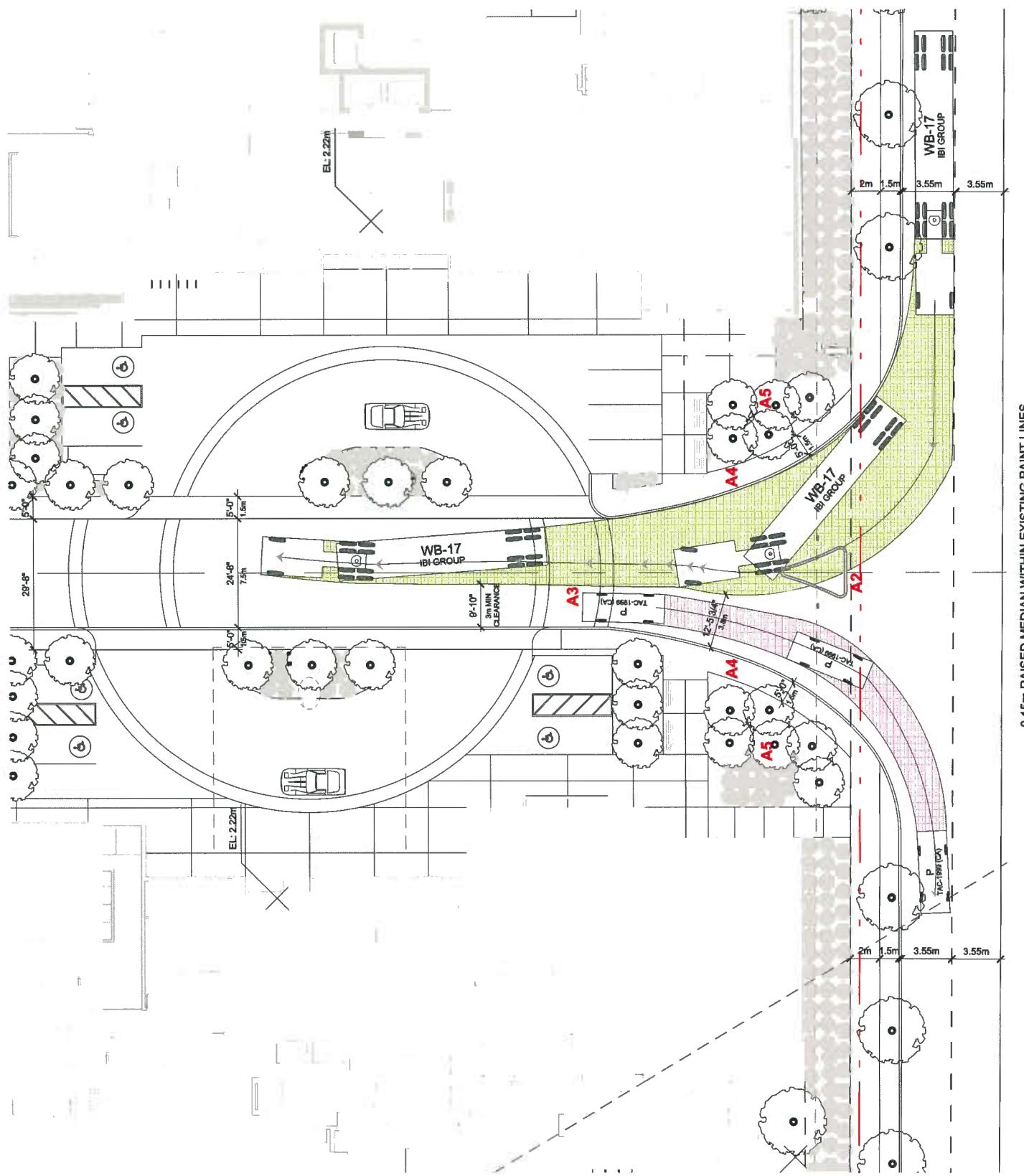
9455 & 9533 BRIDGEPORT ROAD

HOTEL 2 PERSPECTIVE FROM OAK ST BRIDGE 2 A 3004

DP 18-825006 ^{NT} Reference



26986 BRIDGEPORT HOTEL
BRIDGEPORT ROAD
 RICHMOND, BC



IBI

A 5000

1/15"=1'-0"

EXTRACT FROM DP #11-594571
WB17 TRUCK TURNING WHEEL PATH

PART A - SOUTH ENTRANCE

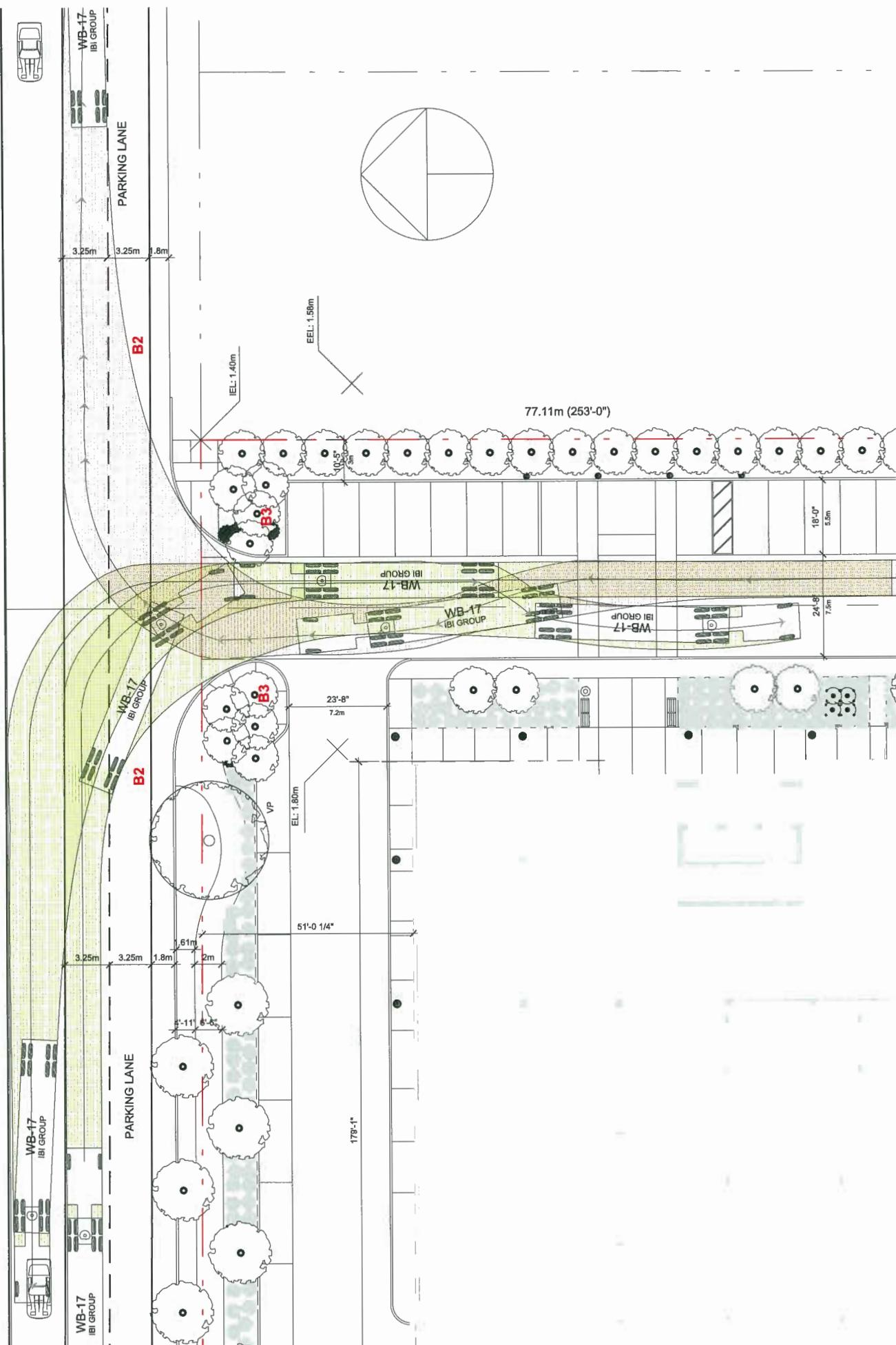
BRIDGEPORT RD

0.15m RAISED MEDIAN WITHIN EXISTING PAINT LINES.

DP 18-825006 Reference

26986 BRIDGEPORT HOTEL
BRIDGEPORT ROAD
 RICHMOND, BC

BECKWITH RD



IBI

A50001a

EXTRACT FROM DP #11-594571
WB17 TRUCK TURNING WHEEL PATH

PART A - NORTH ENTRANCE

1/15"=1'-0"

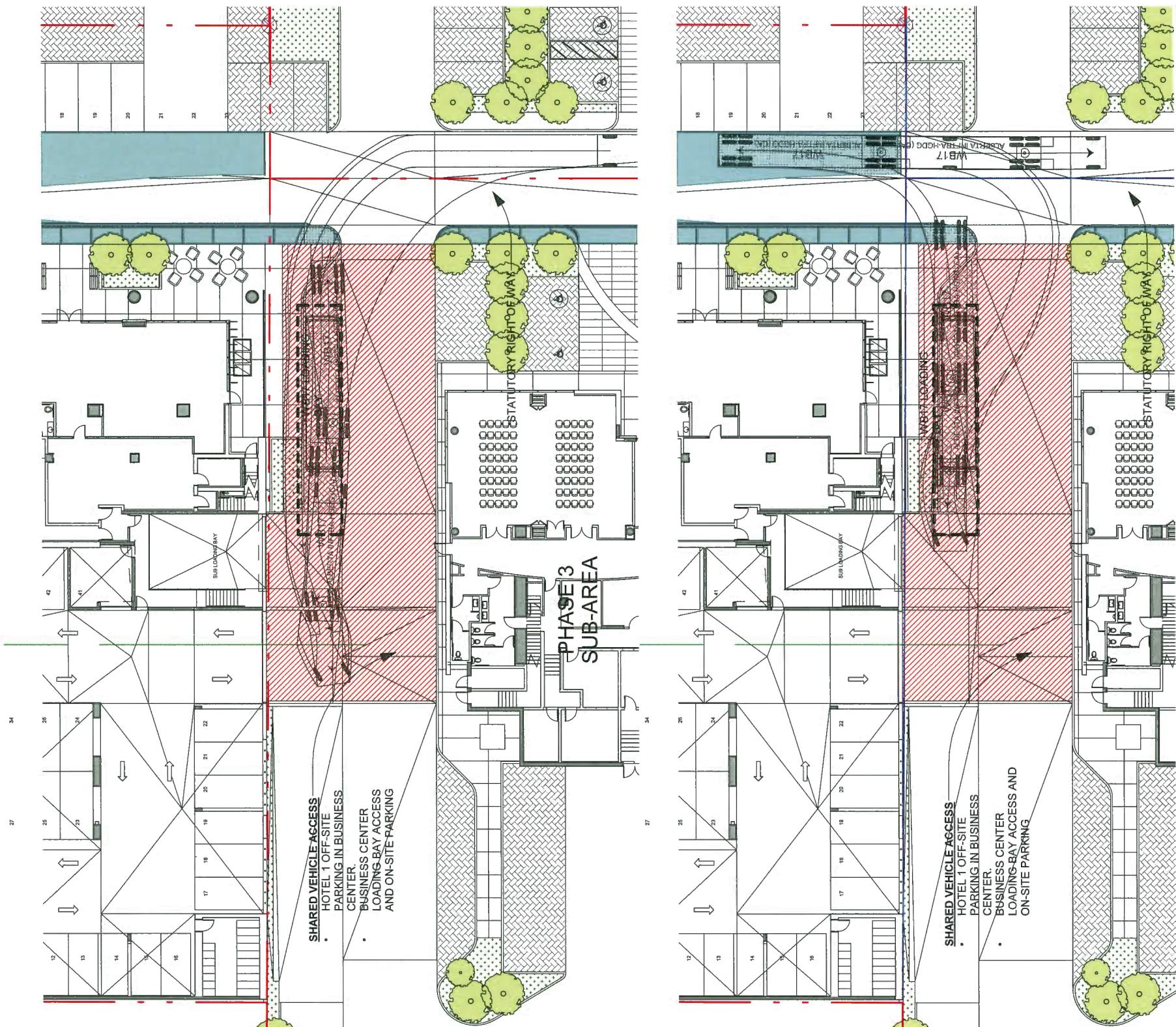
2012-06-11

DP 18-825006 Reference

IBI

9455 & 9533 BRIDGEPORT ROAD

WB17 TRUCK TURN
DP 13-025000 Reference
A 5004



B

9455 & 9533 BRIDGEPORT ROAD



PHASE 3

NOT PART OF THIS APPLICATION
SUBMITTED FOR BP (BP#20180315)

Un-named N/S Road
(street naming under
process)

