



Development Permit Panel

Council Chambers, City Hall
6911 No. 3 Road

Wednesday, February 25, 2015
3:30 p.m.

Minutes

Motion to adopt the minutes of the Development Permit Panel meeting held on Wednesday, February 11, 2015.



1. **Development Permit 14-677603**
(REDMS No. 4486783)

APPLICANT: Bil Ahira

PROPERTY LOCATION: 10760 Aintree Crescent

Director's Recommendations

That a Development Permit be issued which would:

1. *permit the construction of a Coach House at 10760 Aintree Crescent on a site zoned "Single Detached with Granny Flat or Coach House – Edgemere (RE1);"*
and
2. *vary the provisions of Richmond Zoning Bylaw 8500 to allow the coach house to be located 2.0 m from the southern interior side lot line.*



2. **Development Permit 14-676361**
(REDMS No. 4480991)

APPLICANT: Greater Vancouver Sewerage and Drainage District

PROPERTY LOCATION: 1000 Ferguson Road

ITEM

Director's Recommendations

That a Development Permit be issued which would permit the Greater Vancouver Sewerage and Drainage District to construct a 550 m² screening and degrit building, a 20 m diameter thickener, 25 m² thickener Pump station and four 36 m² digester mixing pump buildings (one for each of the existing digesters) at 1000 Ferguson Road on a site designated an Environmentally Sensitive Area (ESA).



3. Development Permit 13-645286

(REDMS No. 4294448)

APPLICANT: Andrew Cheung Architects Inc. on behalf of 684267 B.C. Ltd.

PROPERTY LOCATION: 8151 Anderson Road

Director's Recommendations

That a Development Permit be issued which would:

- 1. permit the construction of a fifteen (15) storey high-rise building and a six (6) storey mid-rise building to create approximately 111 units, and ground level commercial units at 8151 Anderson Road on a site zoned "Downtown Commercial (CDT1);" and*
- 2. vary the provisions of Richmond Zoning Bylaw 8500 to reduce the residential parking rate from the standard City wide parking rate to the City Centre Zone 1 parking rate.*



4. Development Variance Permit 14-670015

(REDMS No. 4496488)

APPLICANT: Lansdowne Congregation of Jehovah's Witnesses

PROPERTY LOCATION: 11014 Westminster Highway

ITEM

Director's Recommendations

That a Development Variance Permit be issued which would vary the provisions of Richmond Zoning Bylaw 8500 to reduce the interior side yard (west property line) from 7.5 m to 2.3 m and interior side yard (east property line) from 7.5 m to 6.9 m in order to permit the rebuild of the existing assembly hall within the same building footprint on the "Assembly (ASY)" zoned site at 11014 Westminster Highway.



5. New Business

6. Date of Next Meeting: Tuesday, March 10, 2015

7. Adjournment



**Development Permit Panel
Wednesday, February 11, 2015**

Time: 3:30 p.m.
Place: Council Chambers
Richmond City Hall
Present: Joe Erceg, Chair
Robert Gonzalez, General Manager, Engineering and Public Works
Dave Semple, General Manager, Community Services

The meeting was called to order at 3:30 p.m.

Minutes

It was moved and seconded

That the minutes of the meeting of the Development Permit Panel held on Wednesday, January 14, 2015, be adopted.

CARRIED

1. Development Permit 12-624180
(File Ref. No.: DP 12-624180) (REDMS No. 4458316)

APPLICANT: GBL Architects Group Inc.

PROPERTY LOCATION: 8451 Bridgeport Road

INTENT OF PERMIT:

Permit the construction of a high rise commercial, hotel and office development at 8451 Bridgeport Road on a site zoned "High Rise Office Commercial (ZC33) – (City Centre)."

Development Permit Panel

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Applicant's Comments

Andrew Emmerson, GBL Architects, with the aid of a visual presentation (attached to and forming part of these minutes as Schedule 1) and a model, provided an overview of the proposed development including its site context and main uses, noting that the subject site is located in a transitional area and its triangular shape presents challenges as well as design opportunities for the applicant.

Mr. Emmerson reviewed the main components of the proposed development and their distinct design features and highlighted the following:

- the 14-storey hotel tower at the southeast corner of the site serves as the fulcrum for the overall development and its angled diamond configuration and strong cuts at the lower and upper levels provide a strong corner identity;
- the nine-storey office building at the southwest corner has an elongated linear form, providing a contrast to the hotel tower form;
- the 12-storey office building at the north has a vertical and more conventional office building form;
- the small commercial spaces at the lower levels, interspersed among the tower forms, provide interest and animation at the street level; and
- the five-level podium accommodates parking and bonds the three towers together.

Also, Mr. Emmerson commented on the strong sustainability features incorporated into the three towers and the podium, noting the punched window expression on the south and west facades of the hotel tower, the horizontal louvers on the two office towers, and metal screening on the facades of the podium building.

Further, Mr. Emmerson noted that the green spaces on the podium roof cover approximately 50 percent of the overall site area and provide a shared passive outdoor amenity space for the hotel and office towers as well as a designated area including a swimming pool exclusive for hotel use.

Mr. Emmerson then spoke of the applicant's approach at the ground level, noting that the towers along Bridgeport Road were pushed back to create an opportunity for the construction of an internal drive aisle for loading and pick-up and drop-off operations and provide a strong buffer between the subject development and Bridgeport Road. Also, he noted that the large and small commercial spaces, tower lobbies and hotel lobby are located on the ground level and the site is accessible on all three sides.

Mr. Emmerson then reviewed the uses of the podium and tower levels as well as the elevations on Bridgeport Road, West Road and River Road, noting the following:

- podium parking is segregated for hotel, commercial and office uses;
- the different tower forms and heights are intended to provide variety, maximize natural daylighting, minimize overlook and meet tower spacing requirements;
- the upper tower levels have a more defined massing;

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- the materials and colour palette is relatively neutral;
- metal screening on the podium face along Bridgeport Road provides an opportunity to incorporate public art;
- double rows of trees along Bridgeport Road and the plaza treatment of the private road enhance the public realm; and
- the corner expression of the hotel tower is visible from Bridgeport Road and provides a strong identity to the proposed development.

Julian Pattison, Considered Design, Inc., gave a brief overview of the landscape design, noting the following:

- the overall concept of the landscape design is to provide an intimate pedestrian experience in the proposed development;
- the design of the service road as an “elongated civic space” has precedent in the Dutch concept of “woonerf” or shared use of a road for pedestrian, bicycle and vehicular circulation;
- the strategy for shared use of the service road includes variation in split stone paving treatment and use of light bollards for the pedestrian route; and
- the “small-scale park” design of the outdoor amenity area on the podium roof reflects the broader natural landscape and encourages interaction among users.

Panel Discussion

Discussion ensued with regard to the landscape treatment on the podium roof. In response to queries from the Panel, Mr. Pattison advised that (i) the main landscape elements include water features, timber benches and raised lawns for shared use and a swimming pool for hotel use only, and (ii) the landscape elements also provide play opportunities for children.

In response to further queries from the Panel, Mr. Pattison pointed out that (i) the main pedestrian access to the subject site along Bridgeport Road is through the hotel plaza, (ii) all frontages of the subject site are treated equally in terms of landscaping to enhance the pedestrian experience on site, and (iii) loading spaces for smaller trucks are spread out in the parkade of the three towers while larger trucks could use the loading spaces along the service road.

Staff Comments

Barry Konkin, Program Coordinator, Development, advised that the rezoning bylaw for the subject site has received third reading and that there is an associated Official Community Plan and City Centre Area Plan amendment. He added that as per staff review, the development proposal is consistent with the City Centre Area Plan guidelines in terms of density, materials use, and design.

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Also, Mr. Konkin commented that (i) garbage pick-up is located off the River Road side of the proposed development and (ii) the Brighthouse Canada Line station is approximately 400 meters from the subject site.

In response to a query from the Panel, Mr. Konkin advised that the likely pedestrian route from the Brighthouse Line Station to the subject site is through River Road. He added that the proposed development would be more accessible when the Canada Line Capstan Station will be constructed in the future.

Gallery Comments

Joseph Fung, 8571 Bridgeport Road, advised that he would like to ask the Panel to address his concerns regarding the proposed development as mentioned in his correspondence to the Panel (attached to and forming part of these Minutes as Schedule 2).

Correspondence

Mr. Konkin advised that Mr. Fung stated in his correspondence to the Panel that he owns a garment manufacturing business adjacent to the subject site and expressed concern that his business' daily operation including deliveries to and from the site would be adversely affected during the construction stage of the proposed development.

In response to Mr. Fung's concern, Mr. Konkin commented that the City's Transportation Section advised that through the Building Permit, a traffic management plan will be required from the applicant which will ensure that full access will be maintained on Mr. Fung's property. He further advised that Mr. Fung could also contact the Bylaw Division should he have further concerns regarding access to his property during the construction of the proposed development.

Panel Discussion

The Panel expressed appreciation to the design team and staff for a well-done presentation of the proposed development, noting that the project's sophisticated design will significantly improve the area.

Panel Decision

It was moved and seconded

That a Development Permit be issued which would:

Permit the construction of a high rise commercial, hotel and office development at 8451 Bridgeport Road on a site zoned "High Rise Office Commercial (ZC33) – (City Centre)."

CARRIED

Development Permit Panel
Wednesday, February 11, 2015

2. New Business

It was moved and seconded

That the Development Permit Panel meeting scheduled on March 11, 2015 be moved to March 10, 2015.

CARRIED

3. Date of Next Meeting: Wednesday, February 25, 2015

4. Adjournment

It was moved and seconded

That the meeting be adjourned at 4:09 p.m.

CARRIED

Certified a true and correct copy of the Minutes of the meeting of the Development Permit Panel of the Council of the City of Richmond held on Wednesday, February 11, 2015.

Joe Erceg
Chair

Rustico Agawin
Auxiliary Committee Clerk

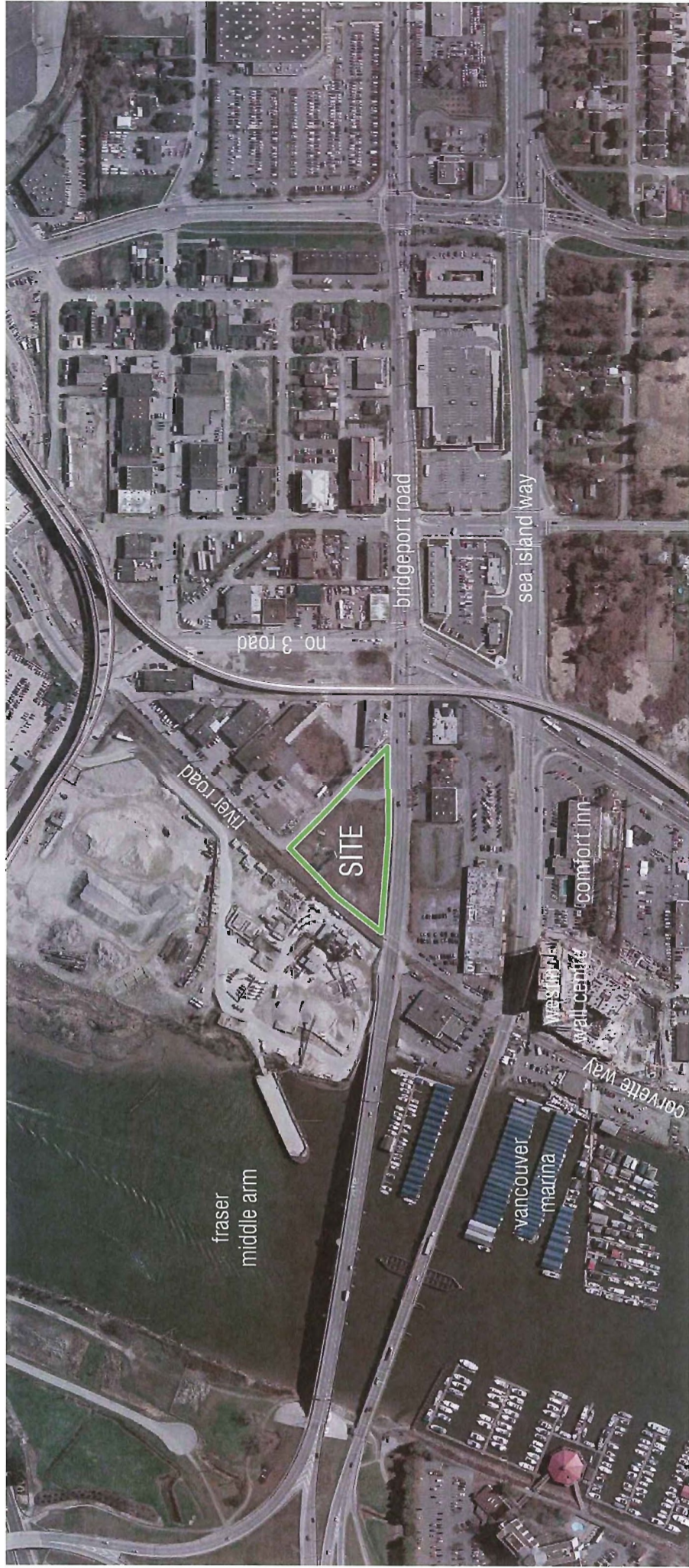
8451 bridgeport road_

ITC

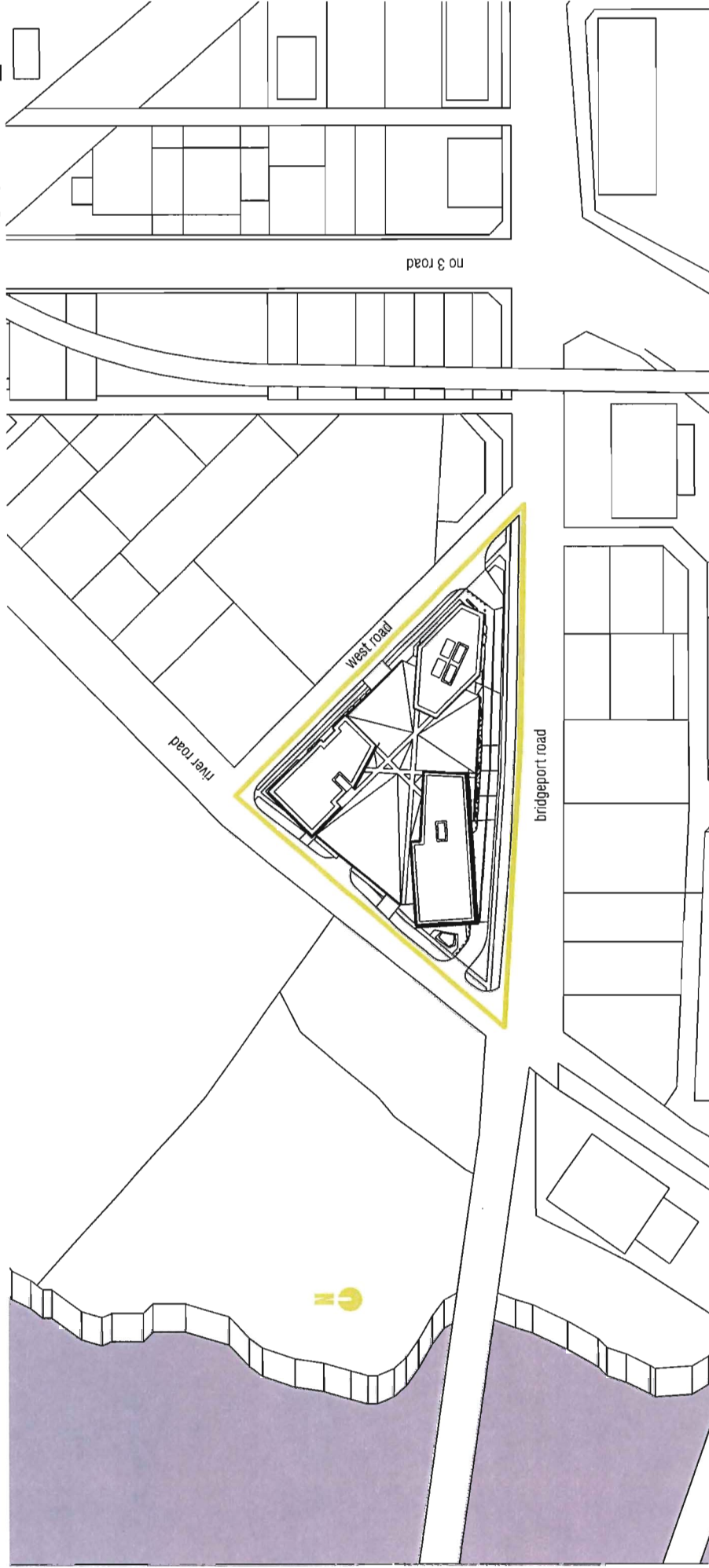
development permit panel

**Schedule 1 to the Minutes of the
Development Permit Panel
meeting held on Wednesday,
February 11, 2015.**





8451 bridgeport road_ **ITC**

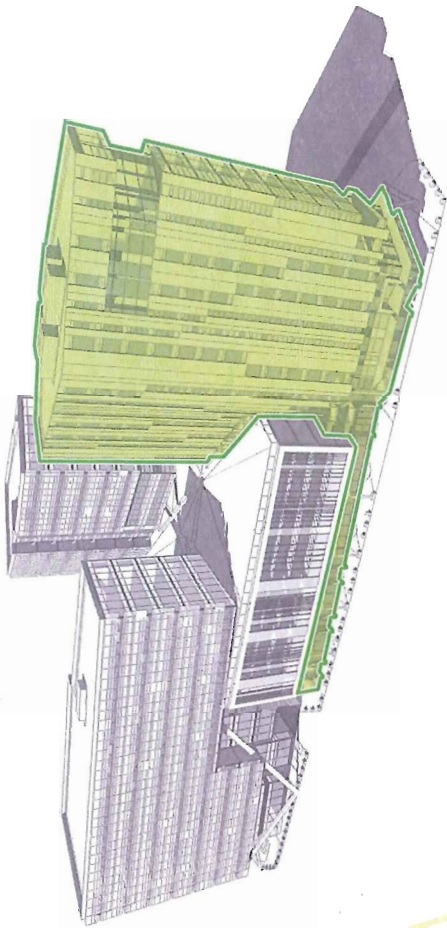
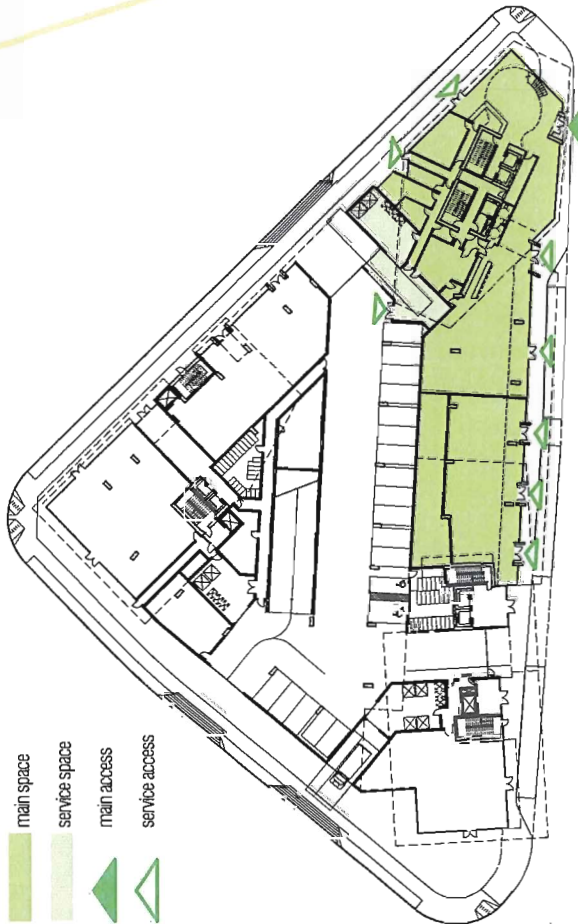


site plan |

03

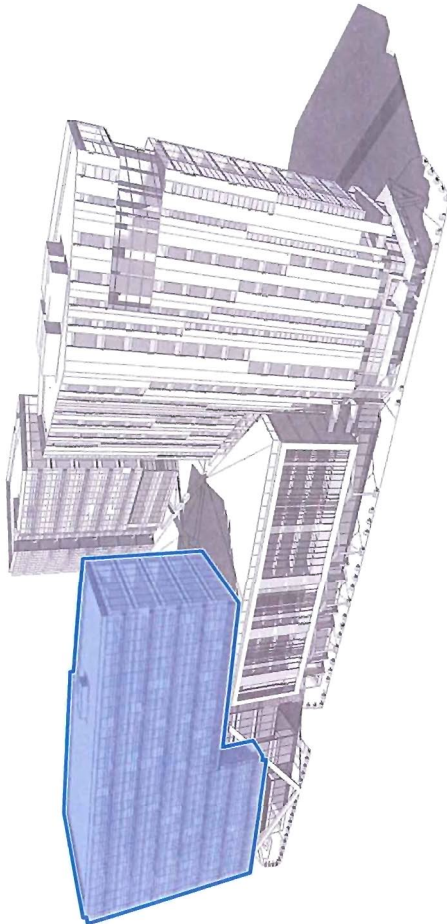
AREA
30
%

main space
service space
main access
service access



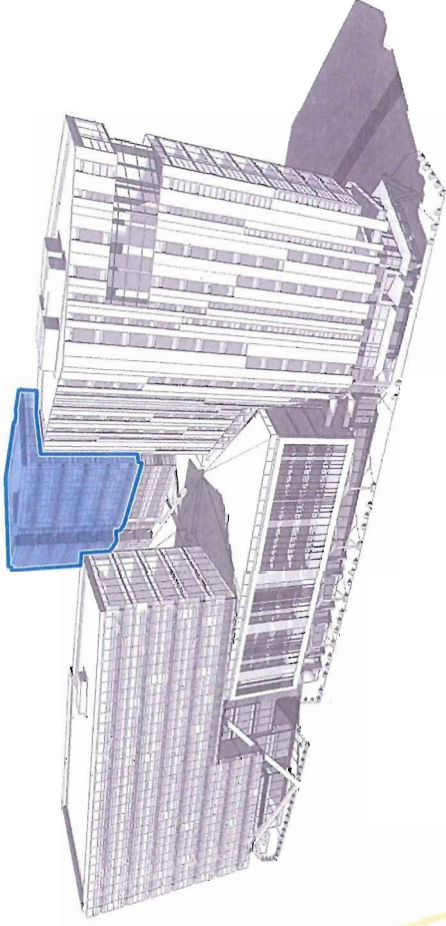
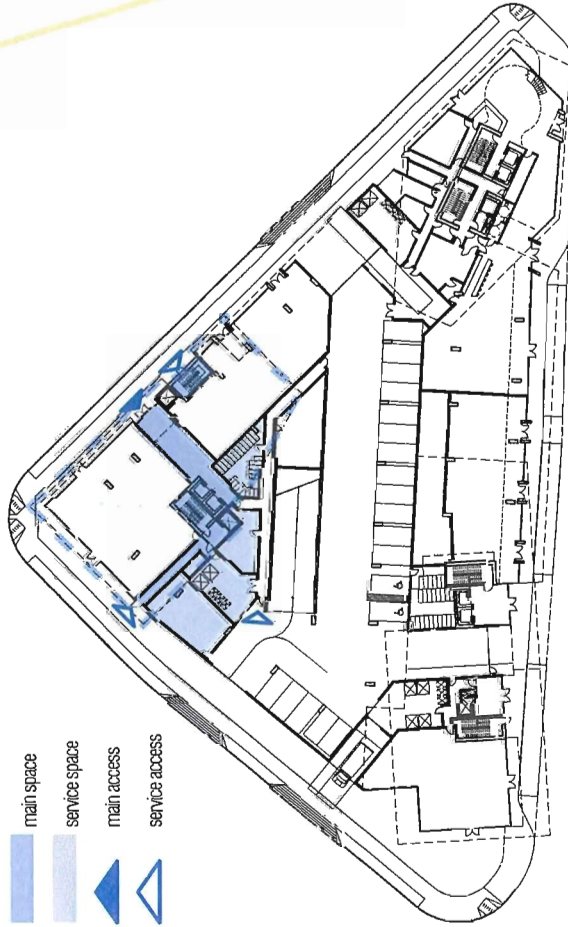
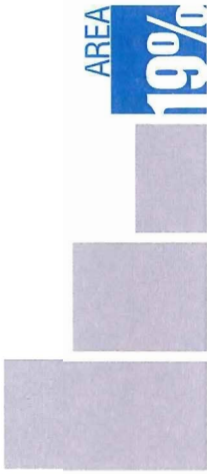
The 14 level Hotel tower is located at the south east corner of the site, acting as a fulcrum for the overall development. Its angled diamond configuration creates a strong physical identity, obliquely aligned with Bridgeport Road. The base of the tower bites back into the main podium, unifying itself with the main development without losing its own distinct character. This lower level cut is counterweighted by a recessed slot on the upper levels of the opposite side, housing the main hotel bar that overlooks the SE corner. This slot perfectly aligns itself with the main entry and lobby at the ground floor, a double height space that wraps around the entire corner providing a strong visual connection with the surrounding public realm. The lobby extends adjacent the private road interfacing with additional restaurant and bar facilities with designated outdoor space

AREA
21%



The Phase 1 office building is located at the SW corner of the site, a linear horizontal box that perches itself on the podium, flaring out from the hotel axis point. The Phase 1 office is the lowest and longest of the three building components above the podium; its height helping to minimise the overshadowing of the outdoor amenity space and its length designed to act as a linear buffer along Bridgeport Road. The massing has been deliberately cantilevered out over the private road to help visually break up the long south facing flat frontage while providing weather protection to the commercial spaces and pedestrian access below. The office is accessed via a lobby adjacent to the parkade entry off the private road.

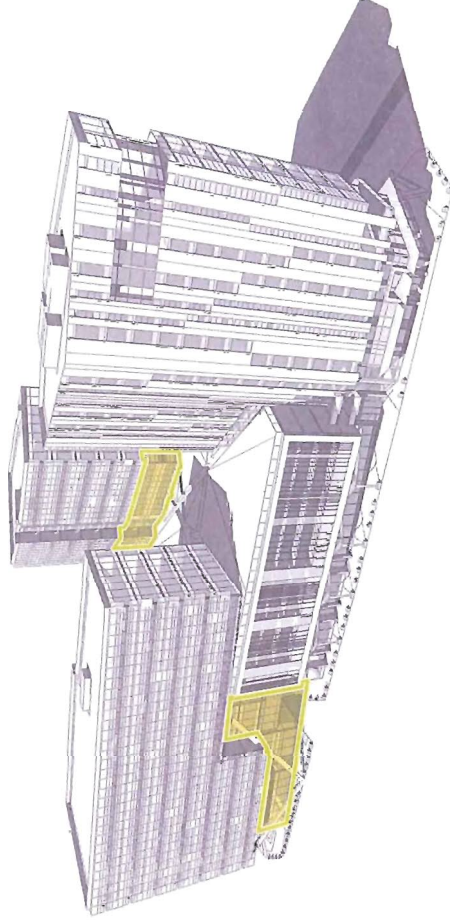
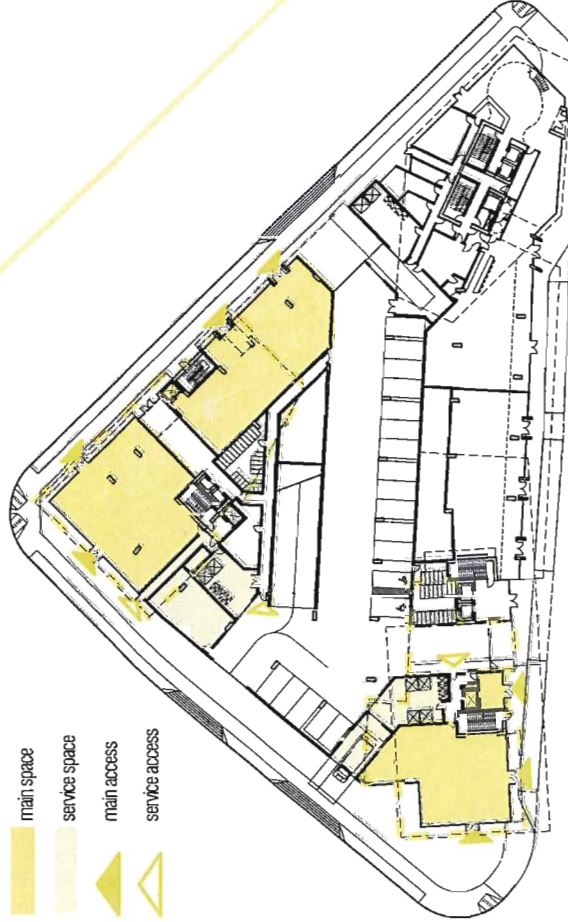




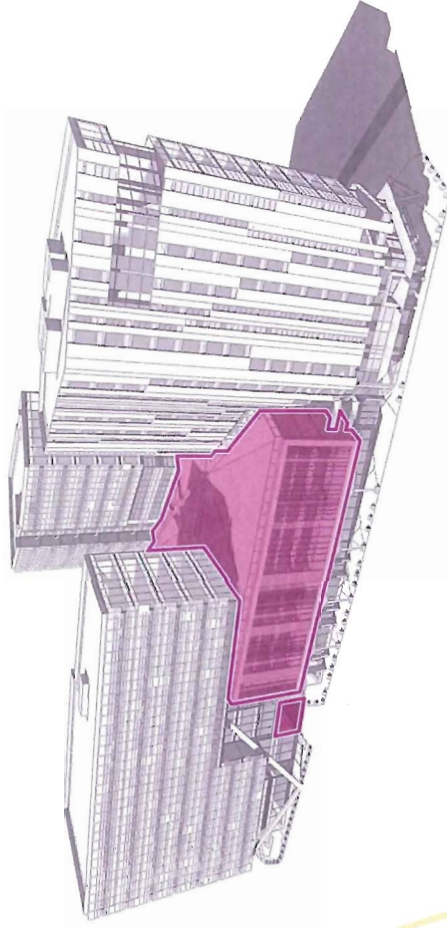
The Phase 2 office building is located at the NW corner of the site, a more vertical building form that rises up more conventionally from the podium levels. The vertical massing is accentuated by linear circulation cuts that allow for visual connections up from the street and from the podium. As with the Phase 1 office building, the building has two contrasting façade treatments with the south and west sides wrapped in horizontal aluminum louvers to provide solar control. The building is accessed via a lobby perpendicular to the north end of West Road.

AREA
14%

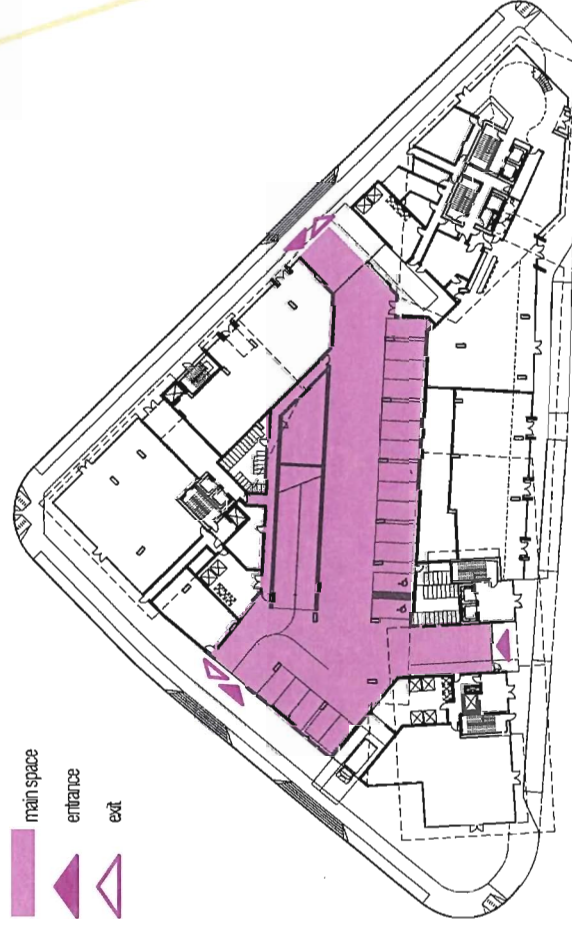
-  main space
-  service space
-  main access
-  service access

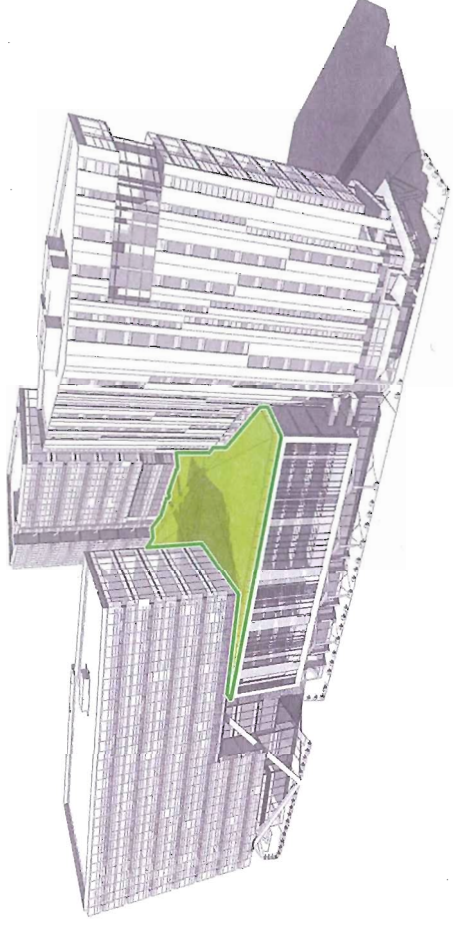


The commercial spaces interweave the lower levels of the tower components, wrapping around the podium base to provide engaging spaces that complement the surrounding public realm. The variety of scale and shape of these spaces allows for a diverse mix of tenants and services, many of which will benefit the development as a whole. Excluding the hotel, the remaining corners of the site will be anchored by sizable commercial spaces that will help animate the street and provide frontage transparency. All of the ground tenancies will be accessed directly from the street and upper level commercial spaces accessed from the parking levels or via a commercial elevator.

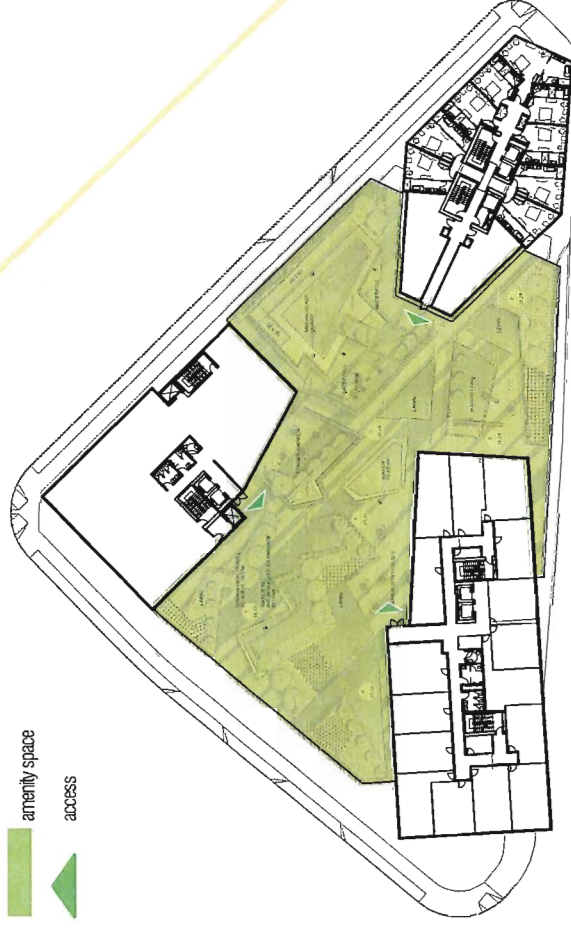


There are 5 levels of parking that form the bulk of the podium levels, into which the three tower components interlock. Rather than be treated as a hidden auxiliary space, the intention was to embrace the unique physical mass of the parking levels and use them as a supplementary design feature, exposing their faces where appropriate. Rectangular frames were stretched across the podium levels to help bond together the office and hotel building components. These linear frames were then filled with irregular spaced vertical louvers and green trellis that help screen the parking while providing a visual rhythm along the streetwall frontages.

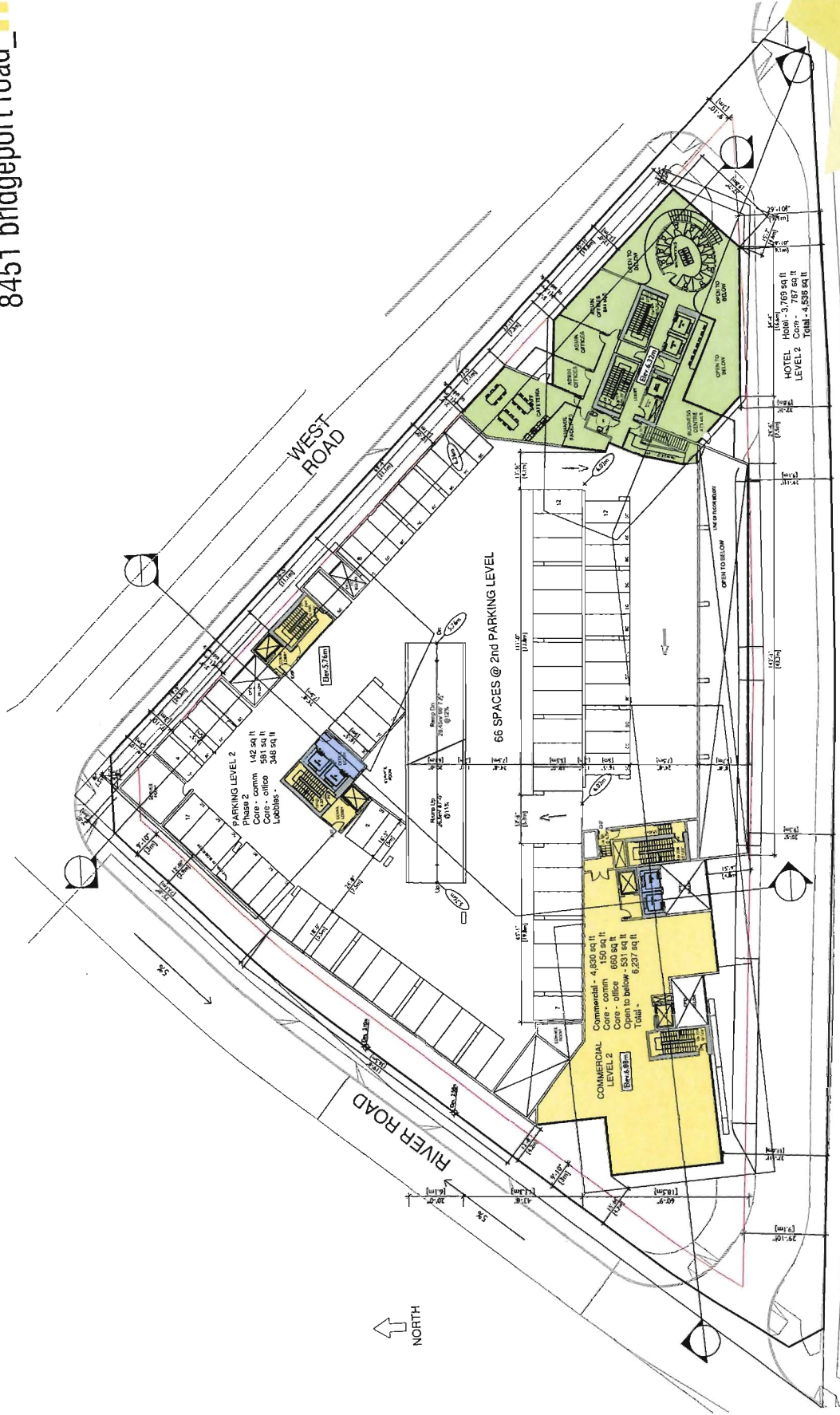




The podium roof area provides invaluable outdoor amenity that is fully accessible to the residents of the development. The space is a unifying element that comprises of a series of interconnecting landscaped features that playfully interweave the physical masses of the surrounding building forms. Soft landscaped elements and water features harmonise within hard landscaped surface treatments to create spaces for interaction, relaxation and contemplation. An outdoor pool and lounge area provide open amenity facilities for the hotel and spa while benches, water features and lawn areas provide appropriate recreational space for the office tenants. The public and private spaces are carefully delineated through the discrete use of rows of vegetation and guardrails. In addition to serving as amenity space, the landscaped podium roof provides a visually appealing overlook from the neighbouring buildings, animated throughout the day by the various tenants.

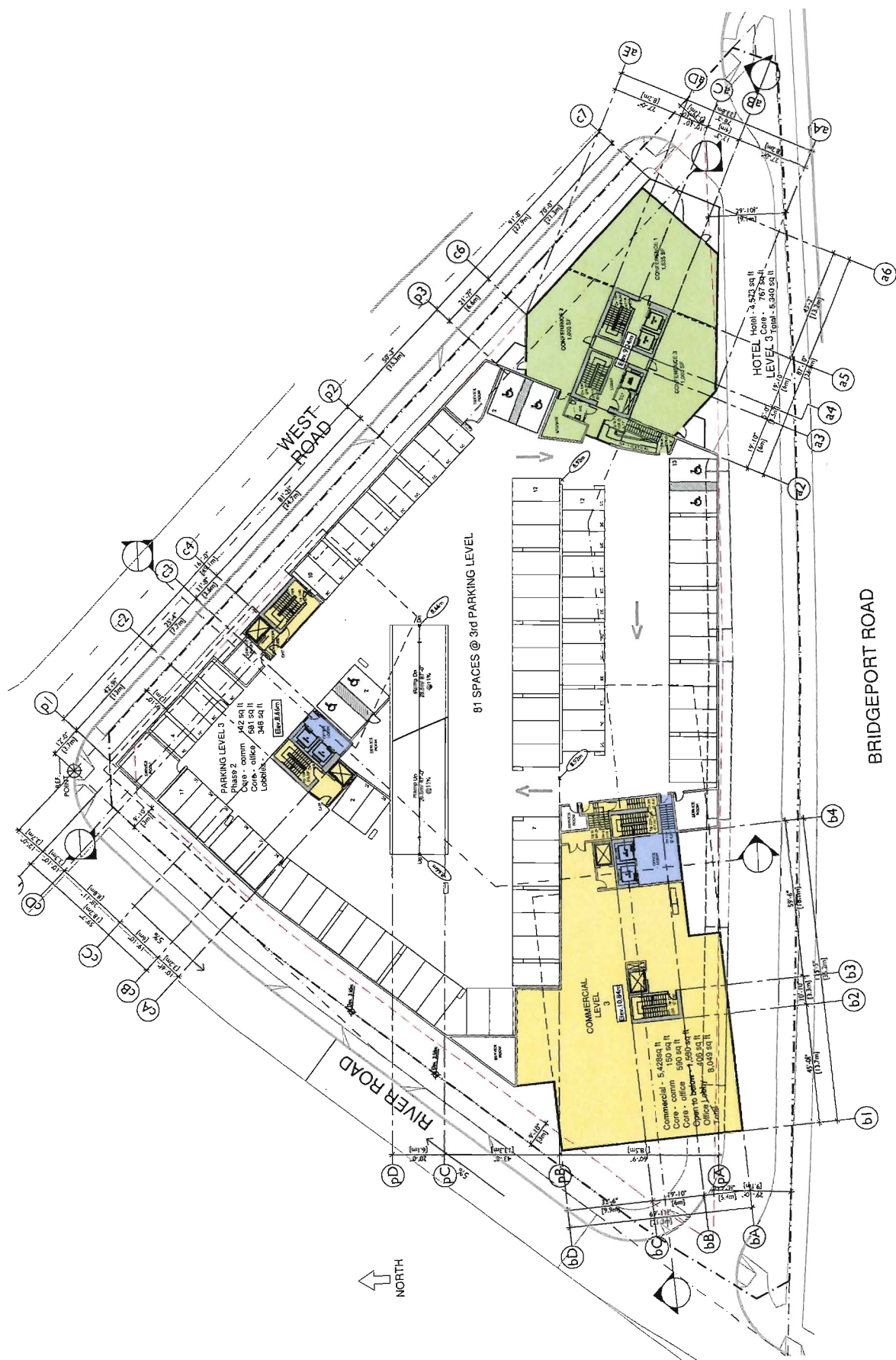


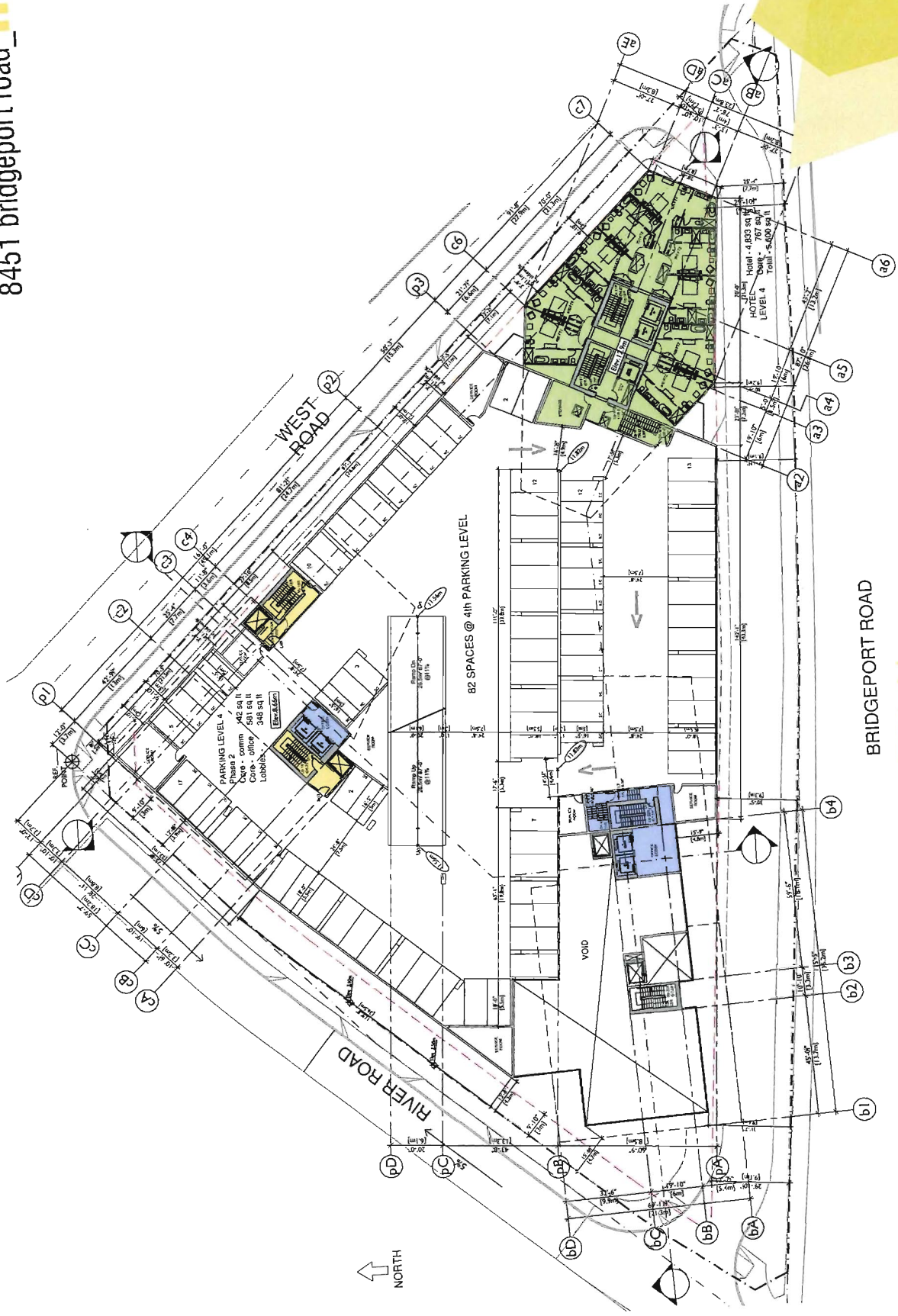
amenity space
access



BRIDGEPORT ROAD

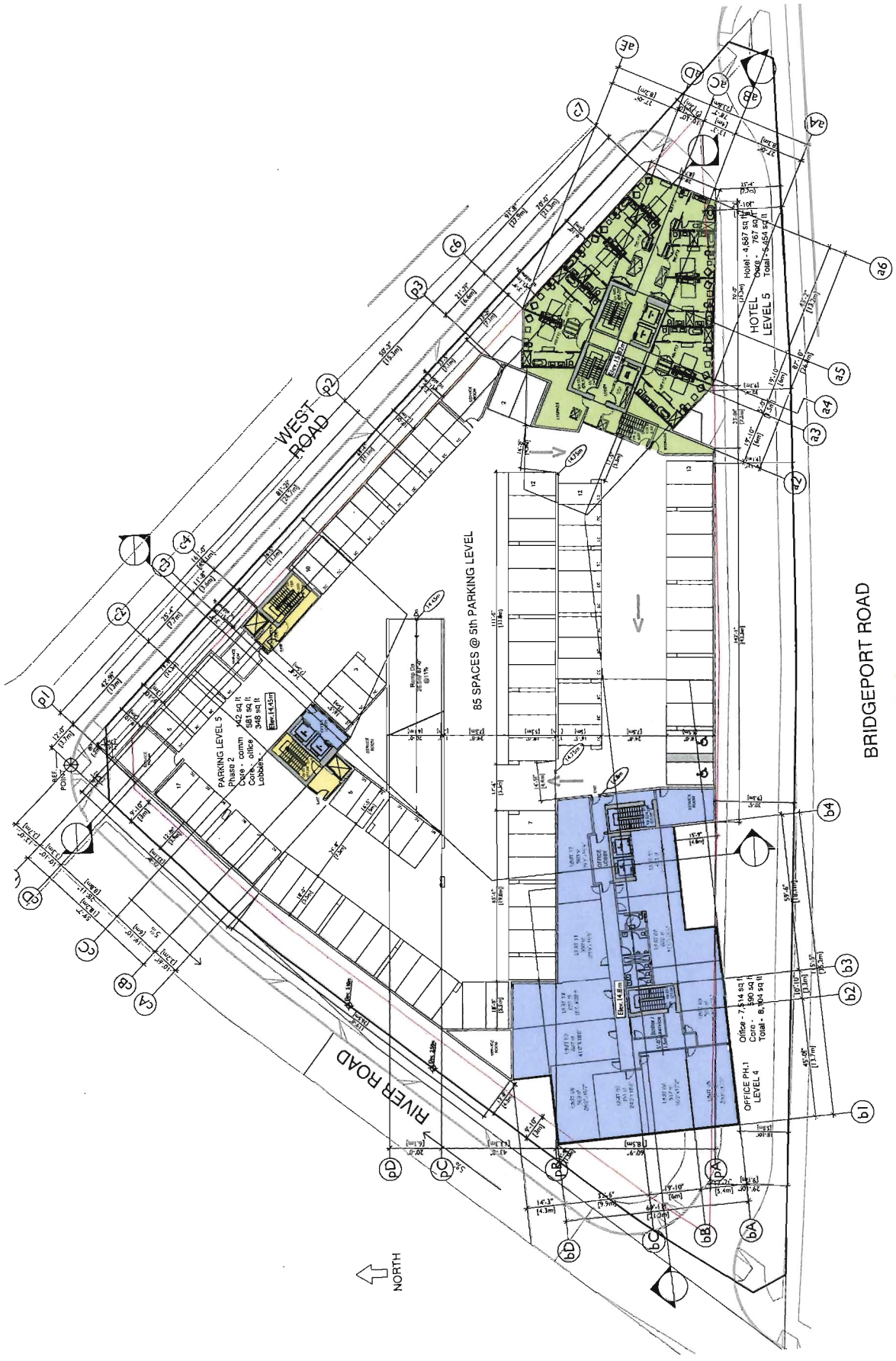
plan - **LEVEL 2 I**





BRIDGEPORT ROAD

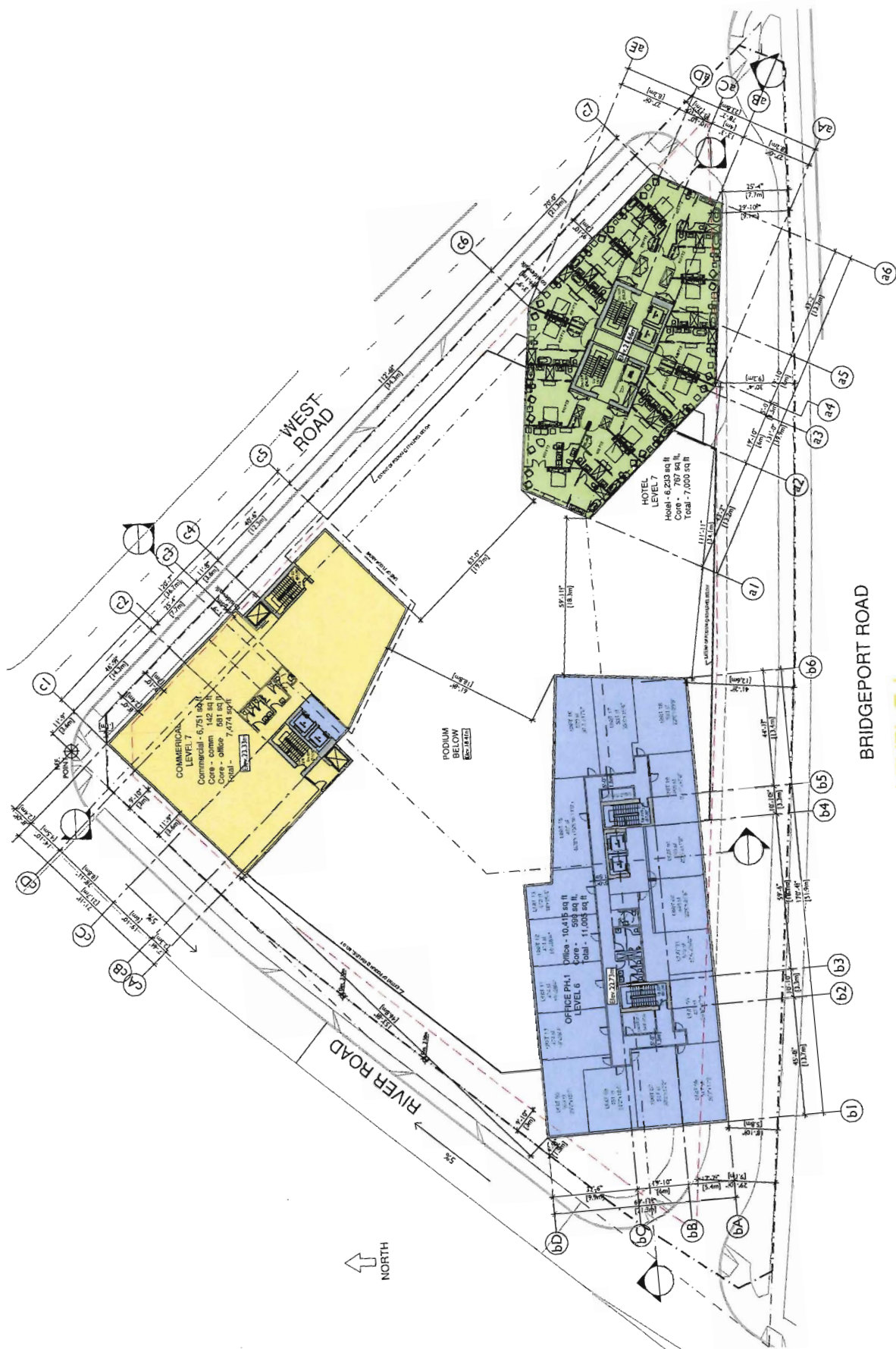
plan - **LEVEL 4 I**



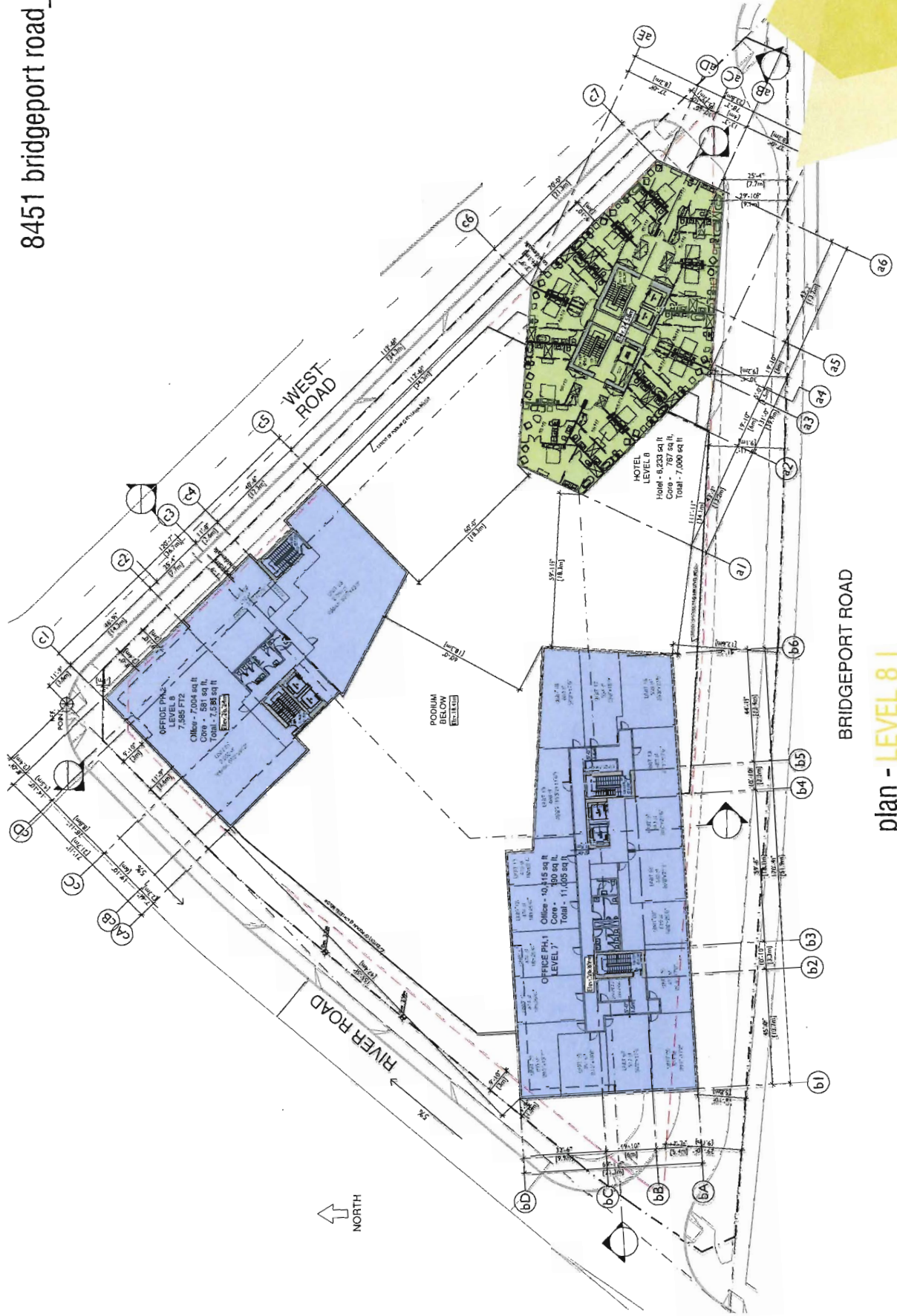
BRIDGEPORT ROAD

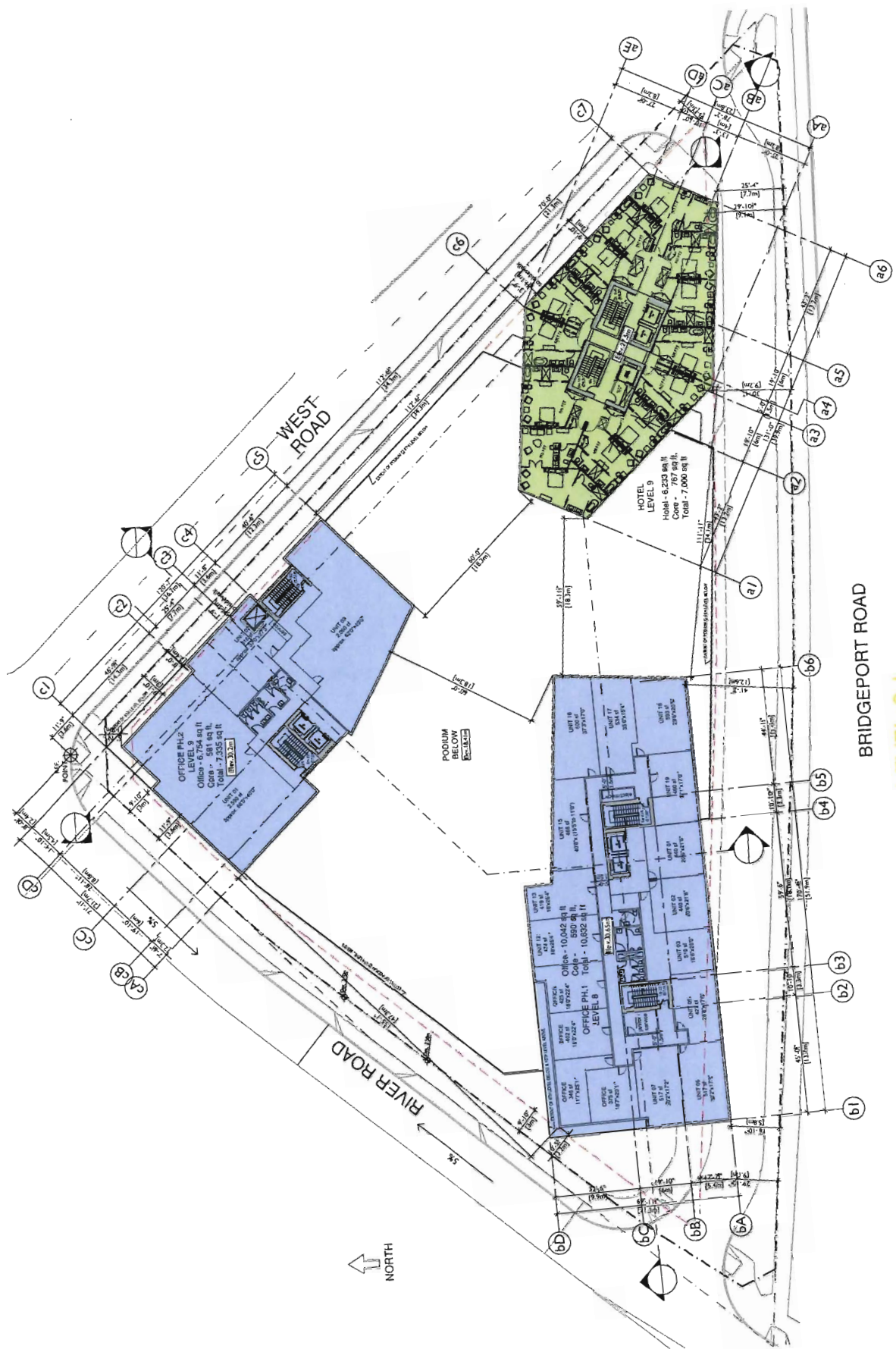
plan - LEVEL 5 I

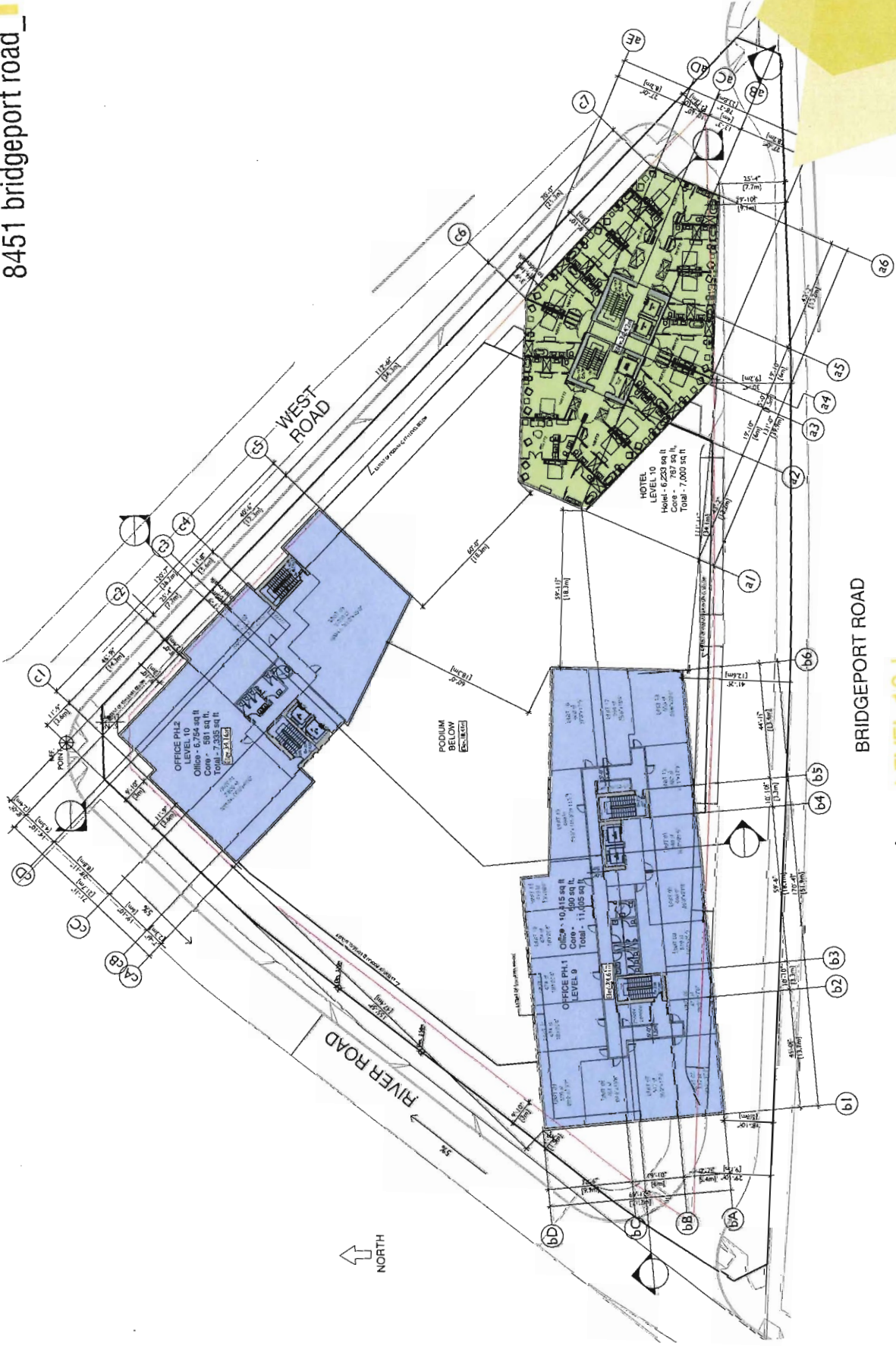


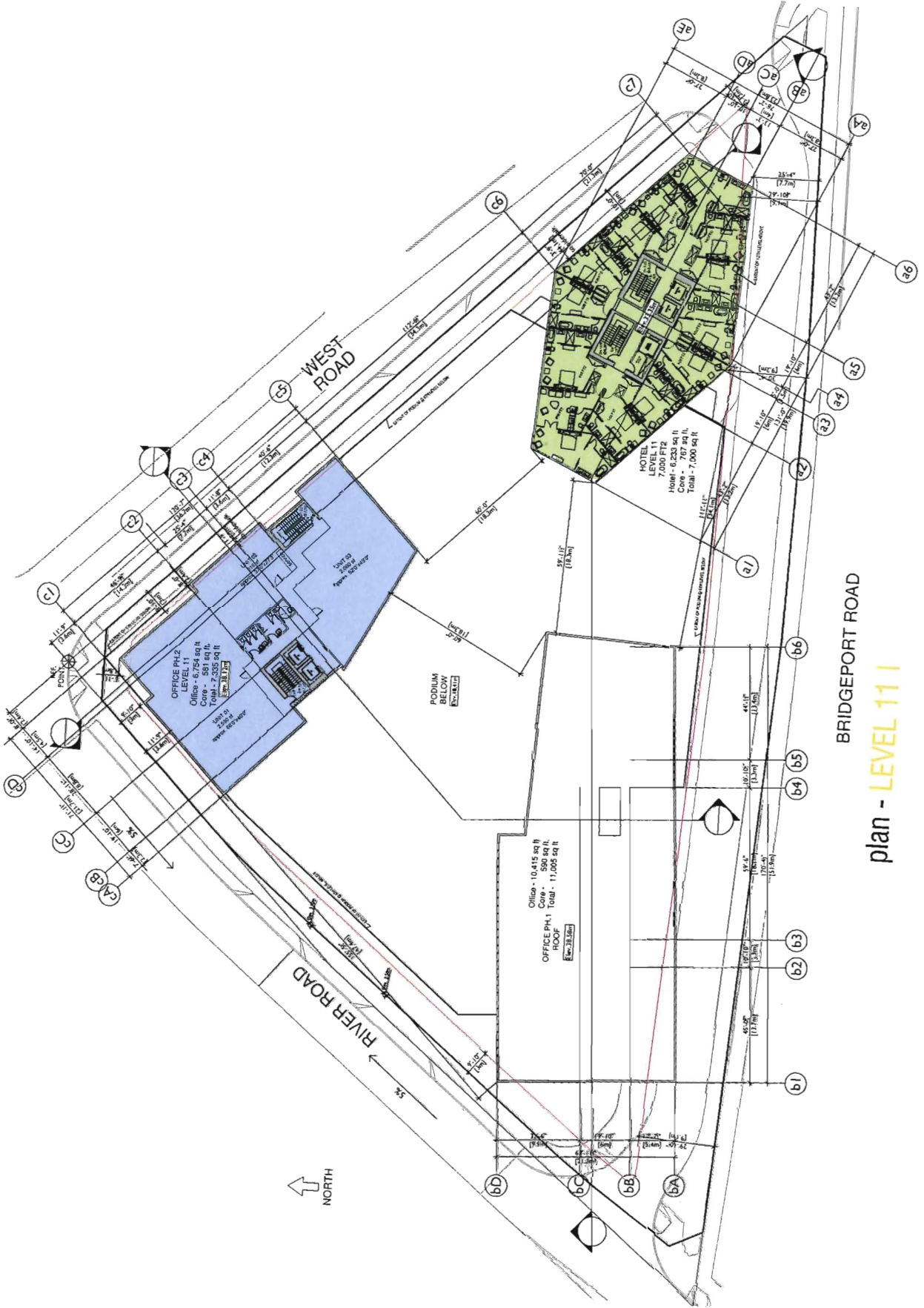


plan - LEVEL 7 I

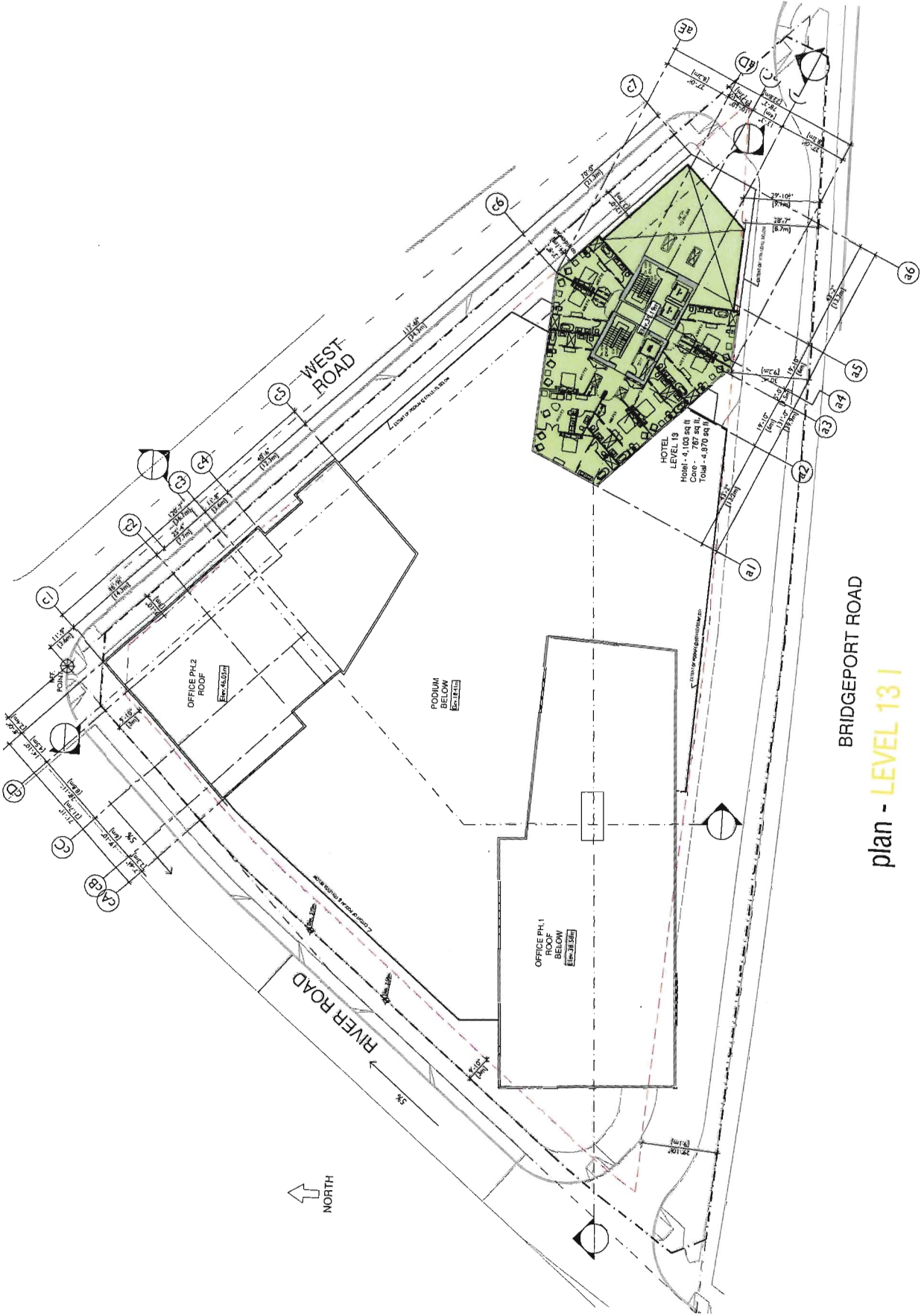




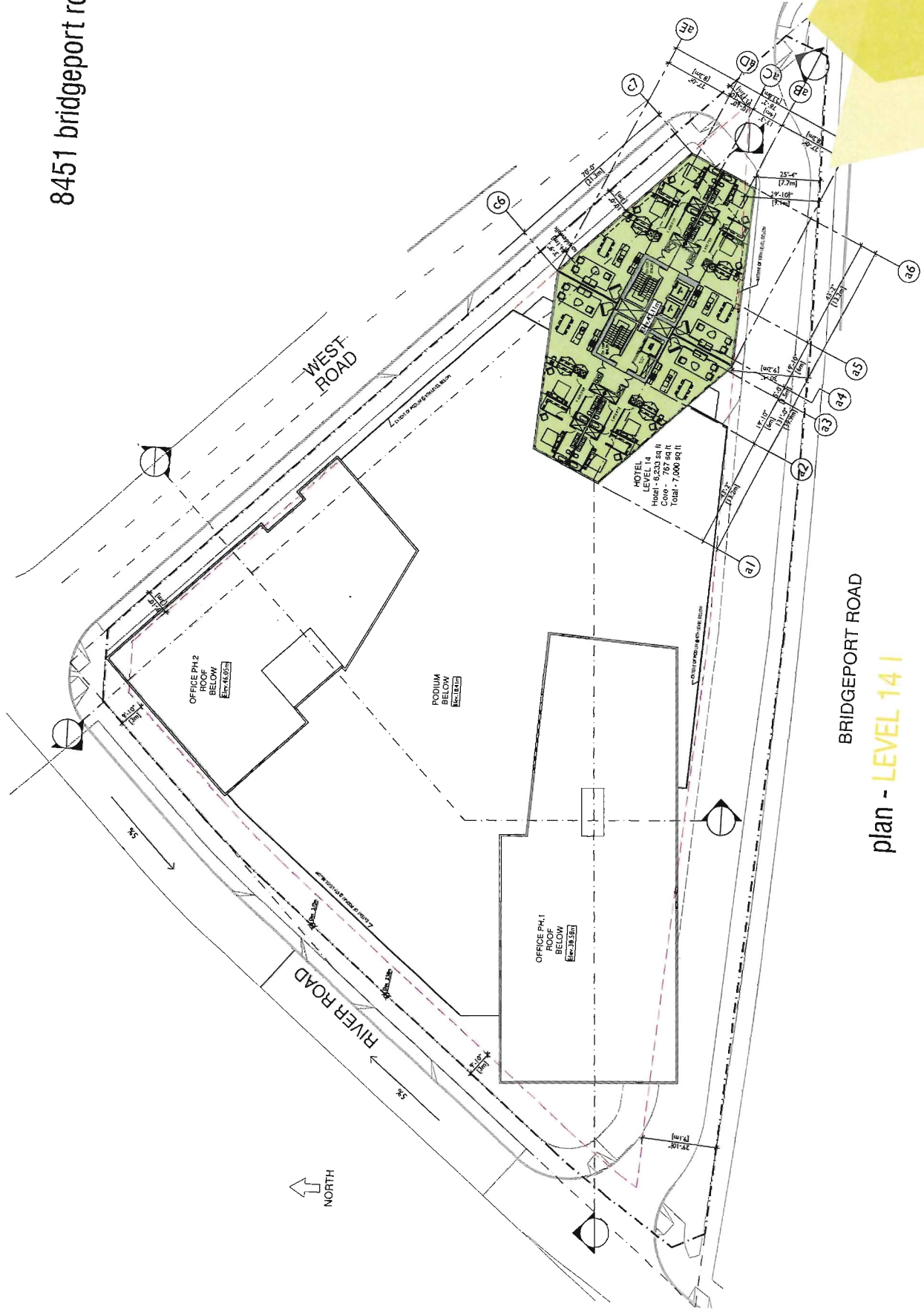








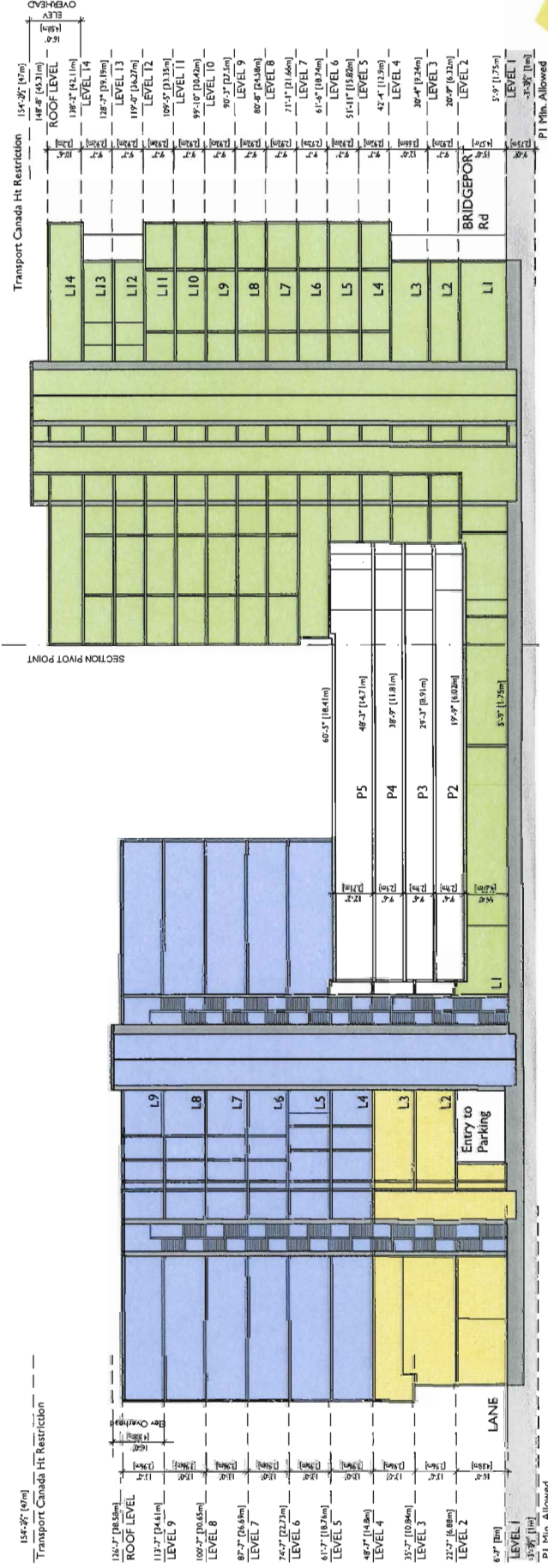
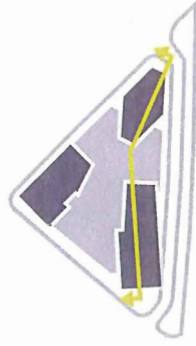
BRIDGEPORT ROAD
plan - LEVEL 13 I

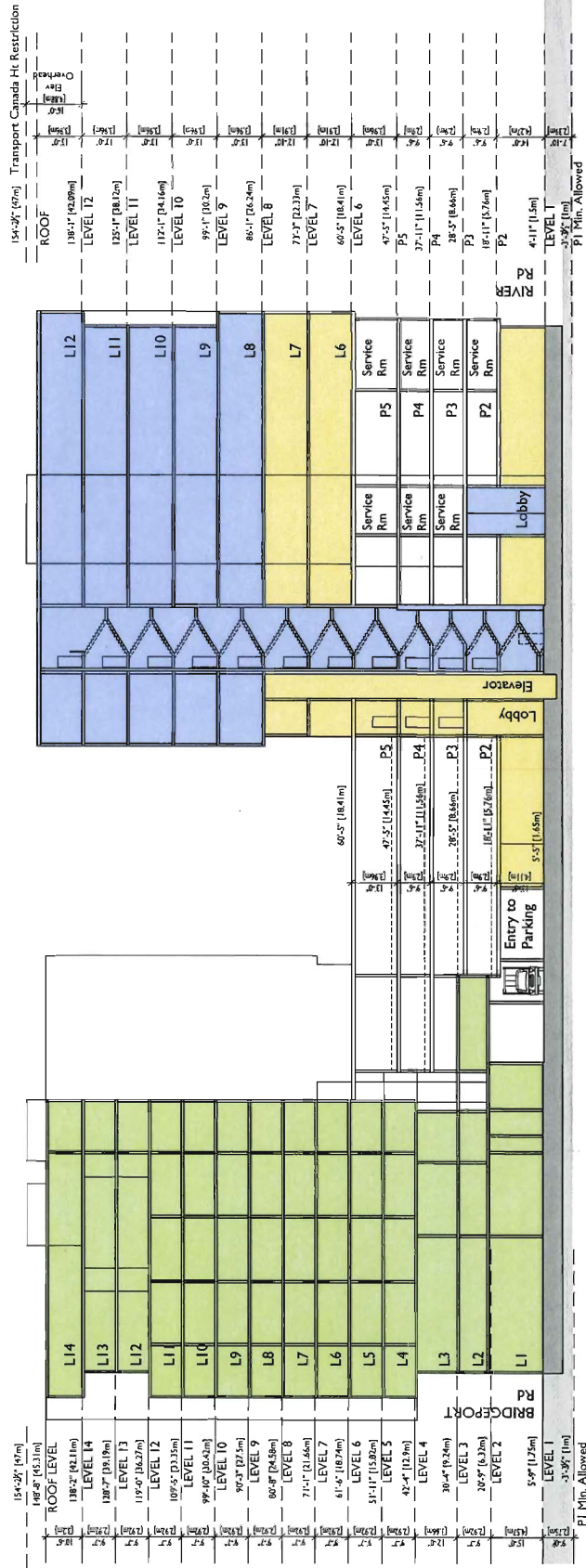
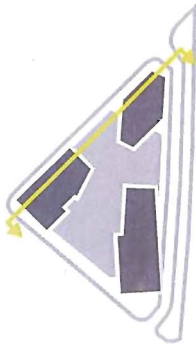


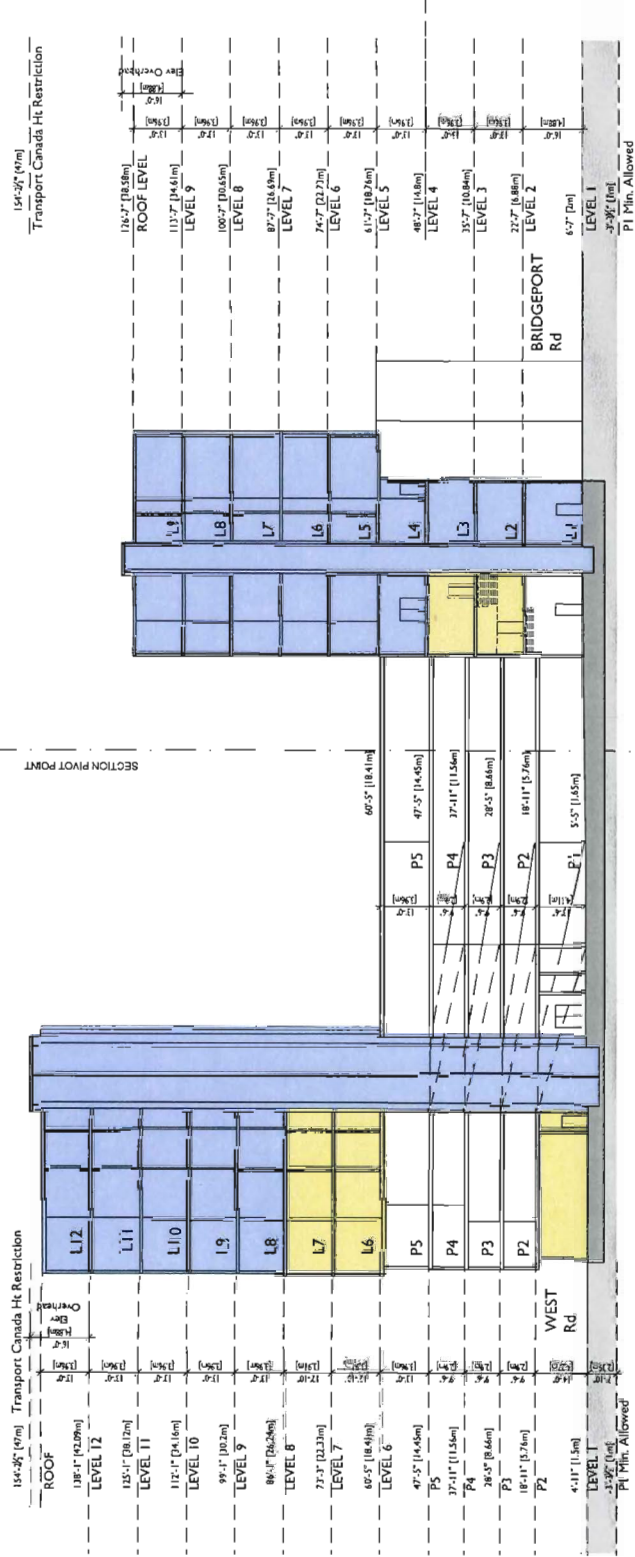
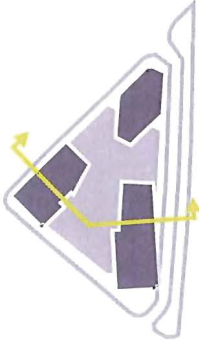
BRIDGEPORT ROAD
plan - LEVEL 14 I



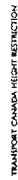
plan - LEVEL 15 I







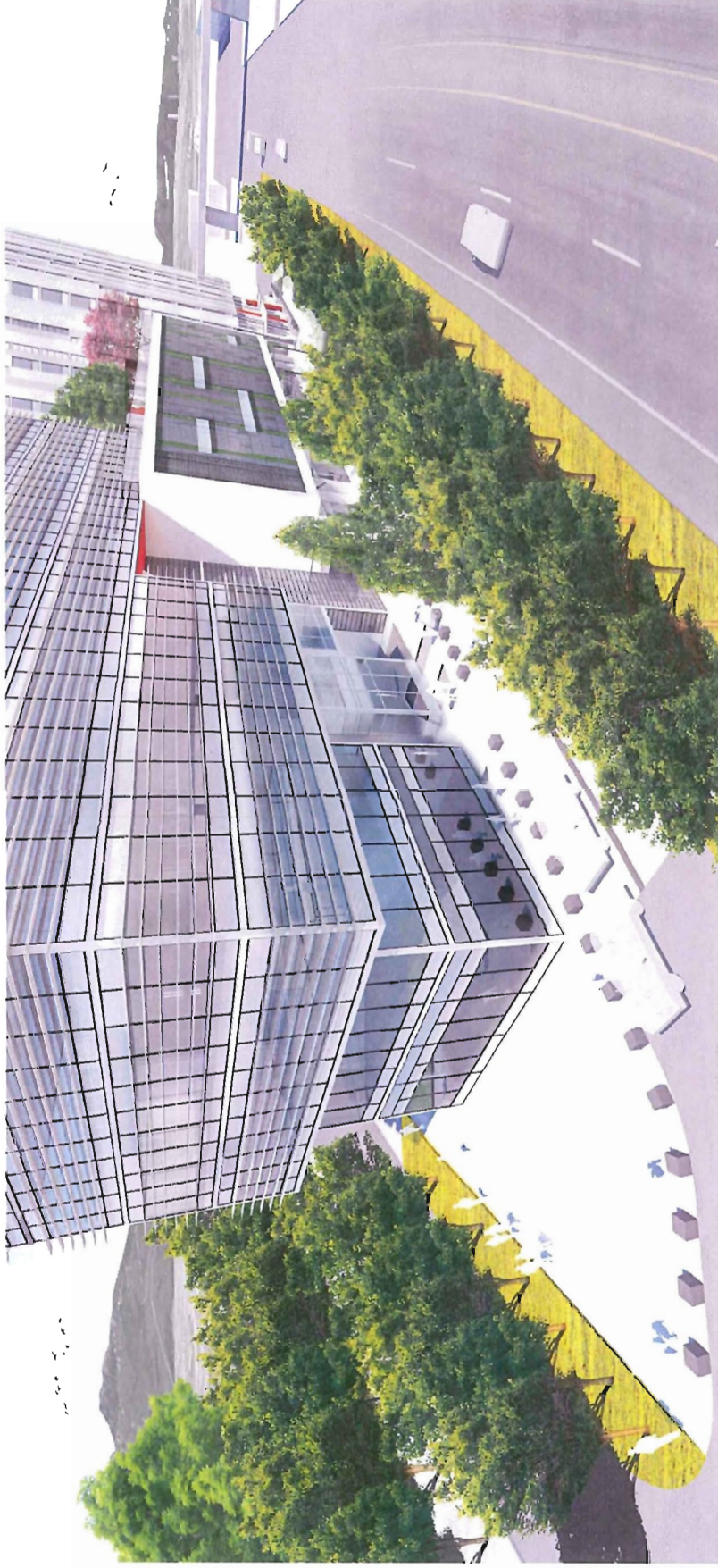




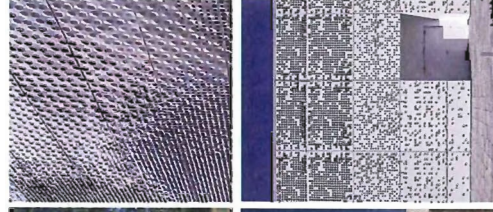
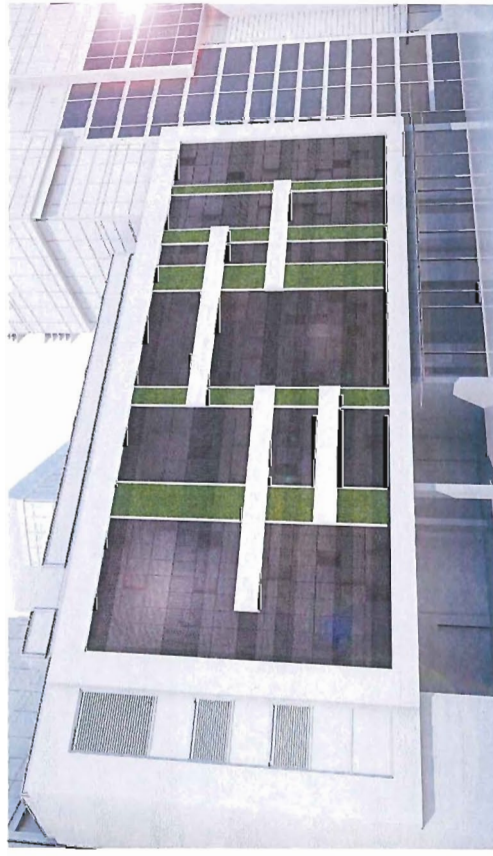


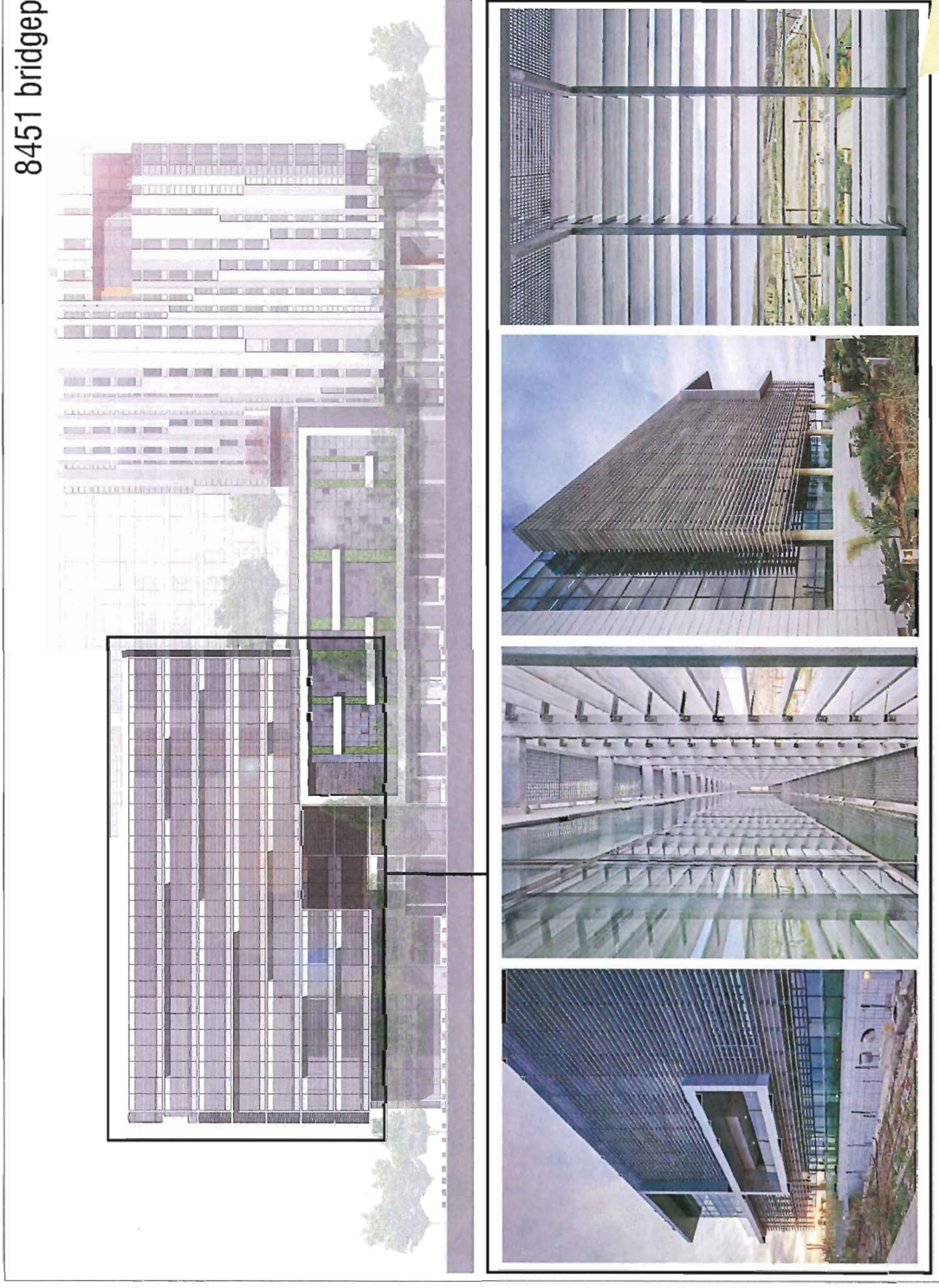
perspective view



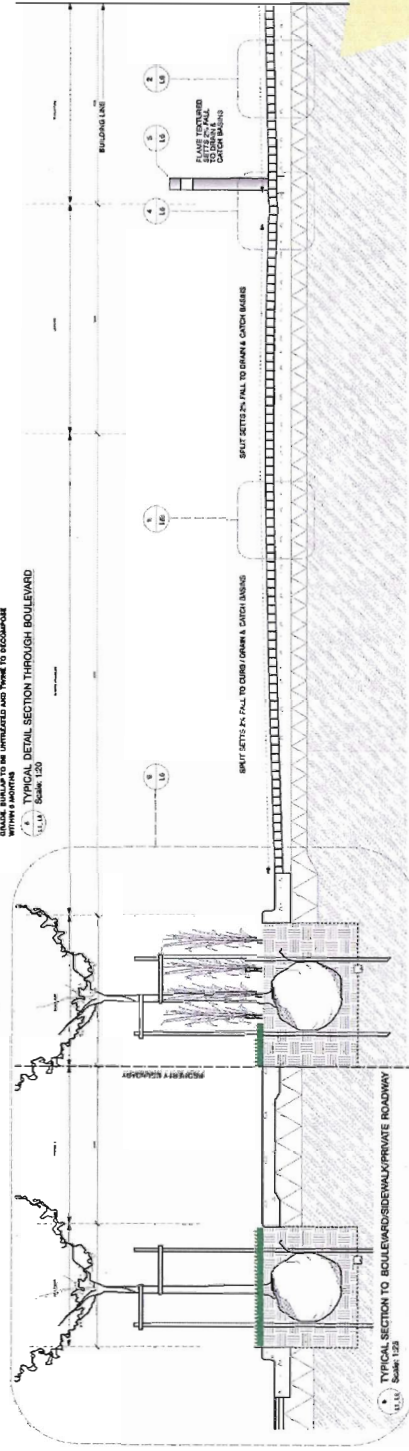
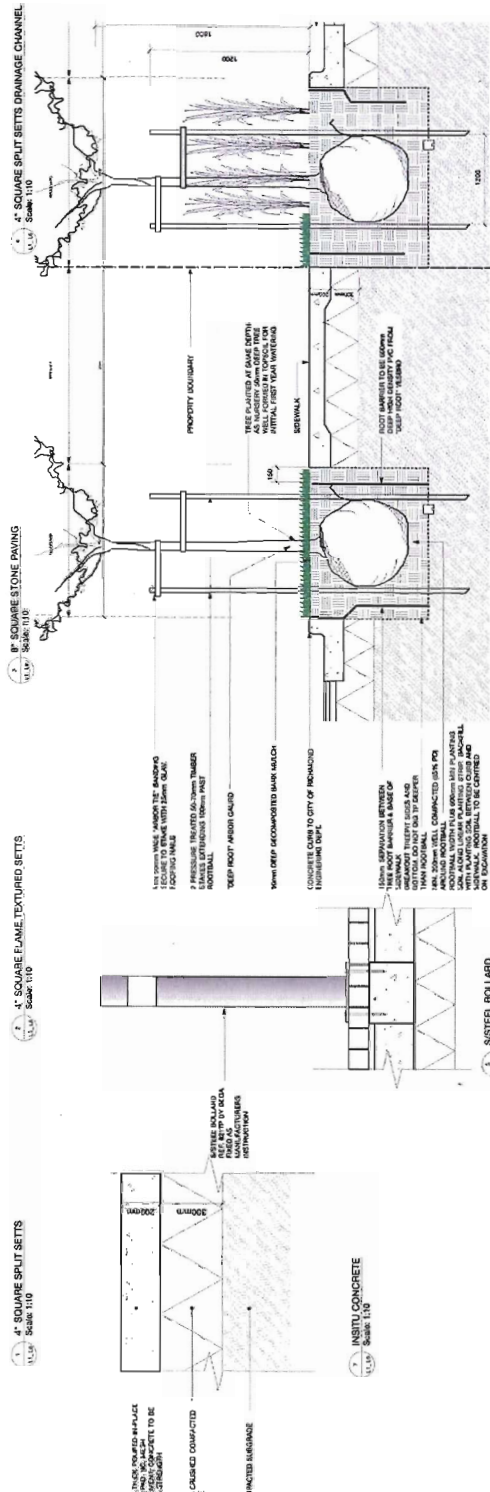


perspective view |





louver design |



1.1.1. The landscape design is based on the following principles:

- 1.1.1.1. The landscape design should be functional and aesthetically pleasing.
- 1.1.1.2. The landscape design should be sustainable and environmentally friendly.
- 1.1.1.3. The landscape design should be safe and secure.
- 1.1.1.4. The landscape design should be accessible to all users.
- 1.1.1.5. The landscape design should be flexible and adaptable to future changes.
- 1.1.1.6. The landscape design should be cost-effective.
- 1.1.1.7. The landscape design should be easy to maintain.
- 1.1.1.8. The landscape design should be in keeping with the overall design of the project.
- 1.1.1.9. The landscape design should be in keeping with the local context and culture.
- 1.1.1.10. The landscape design should be in keeping with the latest industry standards and best practices.

1.1.2. The landscape design is based on the following assumptions:

- 1.1.2.1. The site is a flat, open area with no existing structures or vegetation.
- 1.1.2.2. The site is located in a temperate climate zone.
- 1.1.2.3. The site is accessible by road and public transport.
- 1.1.2.4. The site is surrounded by residential and commercial development.
- 1.1.2.5. The site is subject to occasional flooding.
- 1.1.2.6. The site is subject to occasional high winds.
- 1.1.2.7. The site is subject to occasional drought.
- 1.1.2.8. The site is subject to occasional pollution.
- 1.1.2.9. The site is subject to occasional noise.
- 1.1.2.10. The site is subject to occasional vandalism.

1.1.3. The landscape design is based on the following constraints:

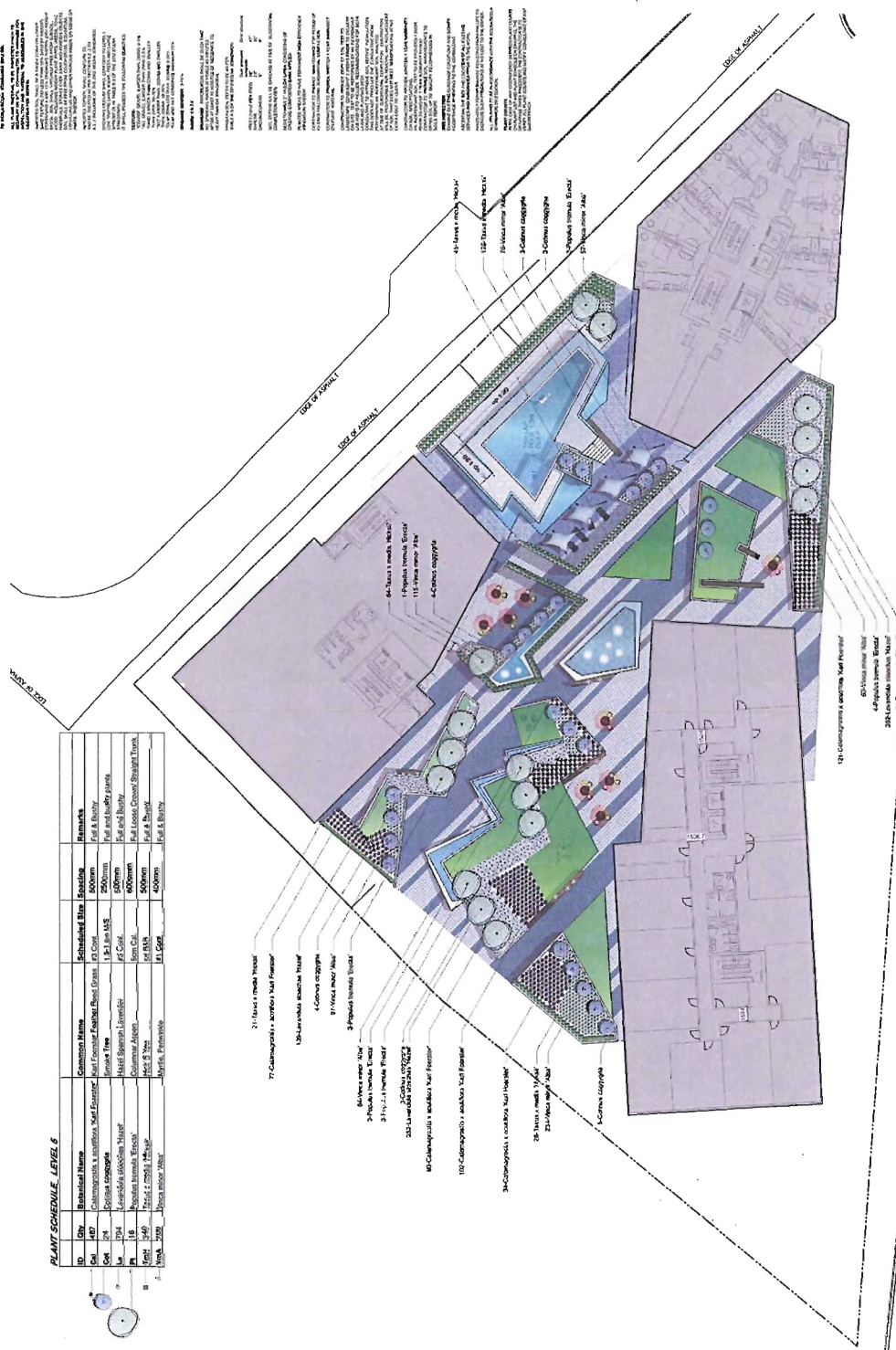
- 1.1.3.1. The site is bounded by a road to the north and a road to the south.
- 1.1.3.2. The site is bounded by a road to the east and a road to the west.
- 1.1.3.3. The site is bounded by a road to the north-east and a road to the south-west.
- 1.1.3.4. The site is bounded by a road to the north-west and a road to the south-east.
- 1.1.3.5. The site is bounded by a road to the north-east and a road to the south-east.
- 1.1.3.6. The site is bounded by a road to the north-west and a road to the south-west.
- 1.1.3.7. The site is bounded by a road to the north-east and a road to the north-west.
- 1.1.3.8. The site is bounded by a road to the south-east and a road to the south-west.
- 1.1.3.9. The site is bounded by a road to the north-east and a road to the south-east.
- 1.1.3.10. The site is bounded by a road to the north-west and a road to the south-west.

1.1.4. The landscape design is based on the following objectives:

- 1.1.4.1. To create a functional and aesthetically pleasing landscape.
- 1.1.4.2. To create a sustainable and environmentally friendly landscape.
- 1.1.4.3. To create a safe and secure landscape.
- 1.1.4.4. To create an accessible landscape.
- 1.1.4.5. To create a flexible and adaptable landscape.
- 1.1.4.6. To create a cost-effective landscape.
- 1.1.4.7. To create an easy to maintain landscape.
- 1.1.4.8. To create a landscape in keeping with the overall design of the project.
- 1.1.4.9. To create a landscape in keeping with the local context and culture.
- 1.1.4.10. To create a landscape in keeping with the latest industry standards and best practices.

PLANT SCHEDULE - LEVEL 5

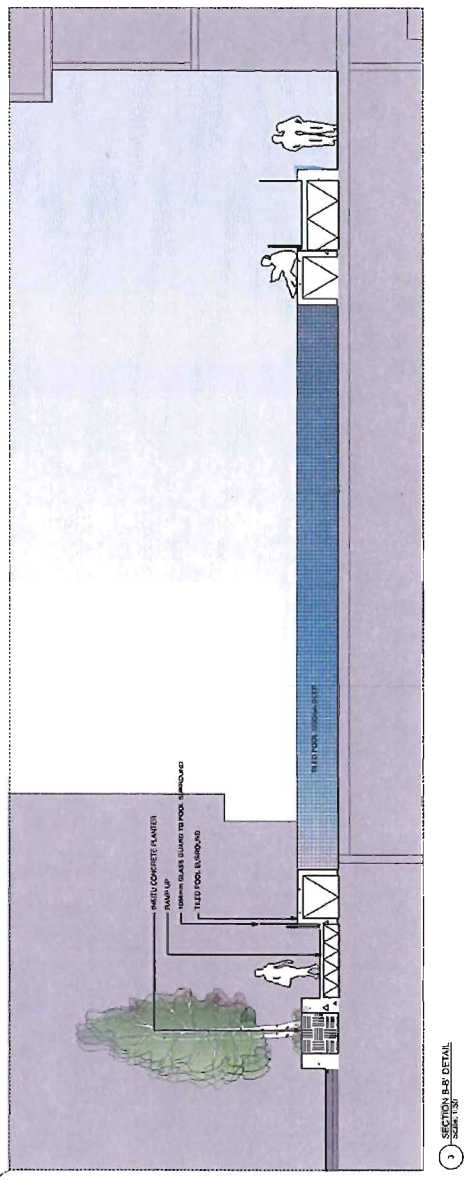
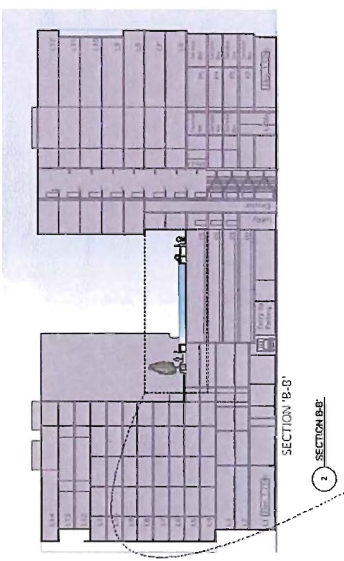
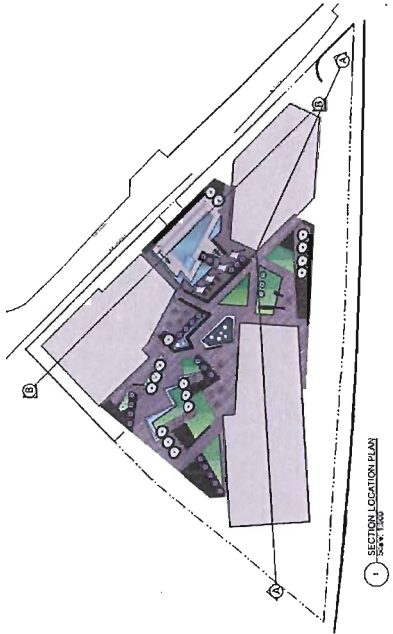
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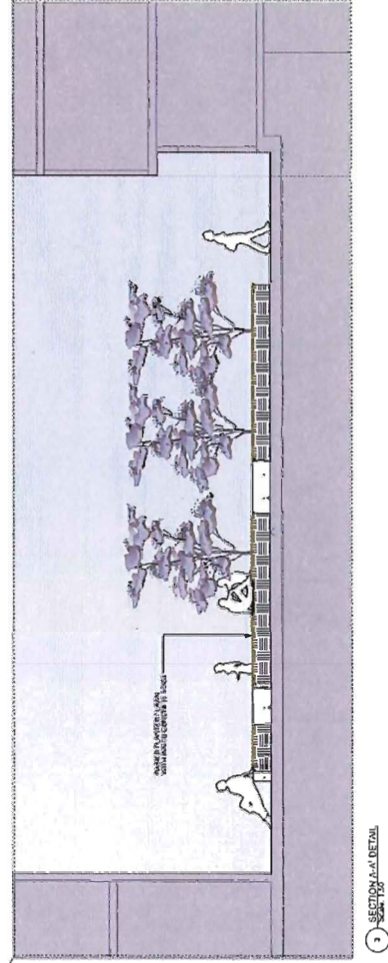
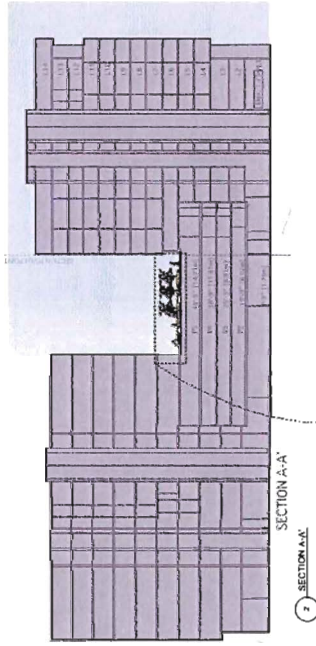
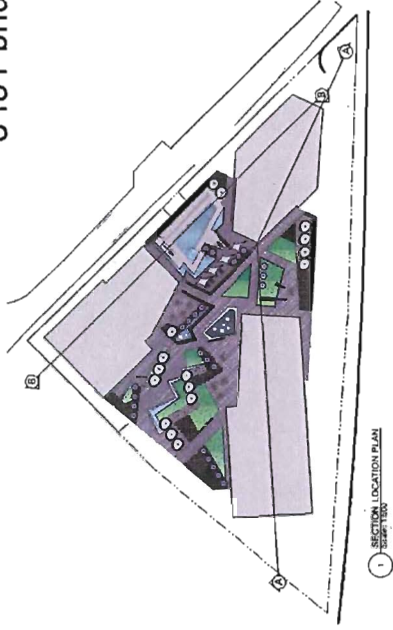
LANDSCAPE PLAN
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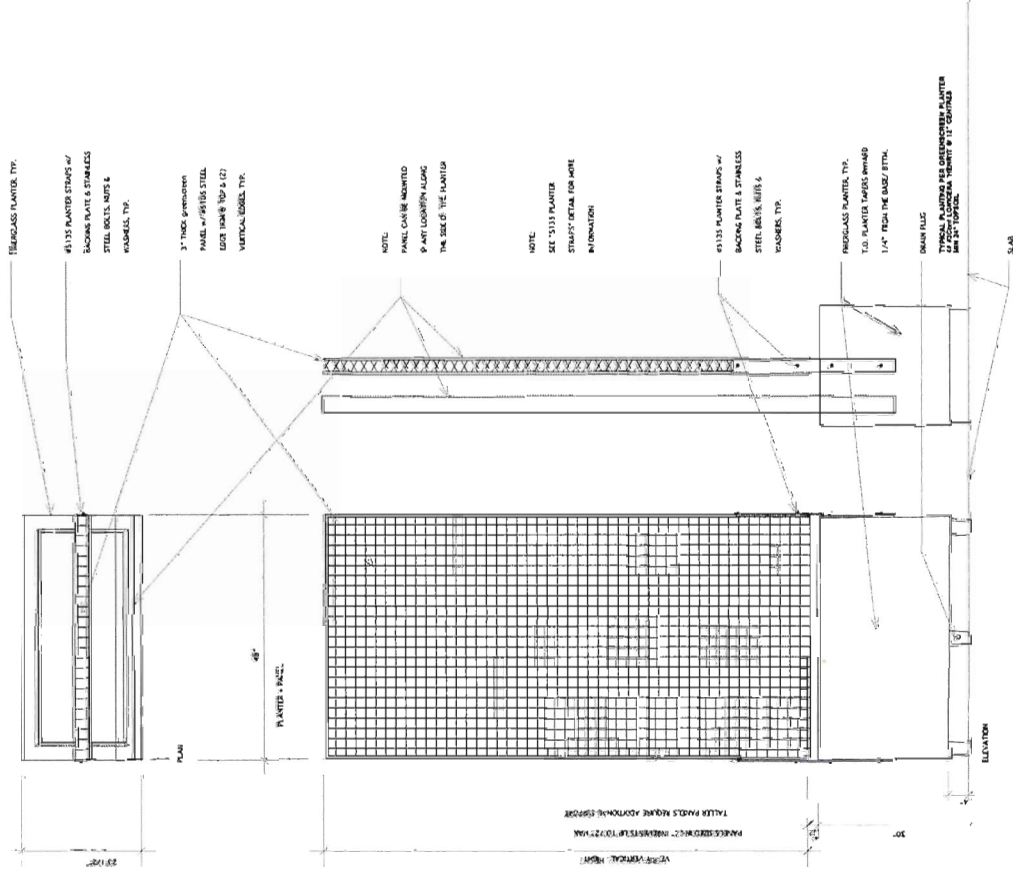


landscape - LEVEL 5_PLANTING PLAN I



landscape - LEVEL 5_SECTION I





M J FASHIONS LTD.

8571 BRIDGEPORT ROAD, RICHMOND, BC, V6X 1R7



2/11/2015

Joseph Fung
M J Fashions Ltd.
8571 Bridgeport Road, Richmond, BC, V6X 1R7

To Development Permit Panel
Date: <u>February 11, 2015</u>
Item # <u>1</u>
Re: <u>Development Permit</u>
<u>12-624180</u>
<u>8451 Bridgeport Road</u>

David Weber, Director, City Clerk's Office
6911 No. 3 Road, Richmond, BC, V6Y 2C1d

Re: Written Submission on the application by GBL Architects Group Inc. for a Development Permit at 8451 Bridgeport Road

Dear Sir,

M J Fashions Ltd., established over 20 years, has been operated in Richmond BC as a cycling apparel garment manufacturer since year 2008. The Company is currently employing around 50 employees, mostly are Richmond residents. Some use public transportation and walk to work while some drive and park on site. Our parking lot practices one-way traffic and has 2 entrances: from the back lane parallel to No. 3 Road, and West Road, and one exit: on Bridgeport Road. Our only loading dock is located on West Road. All our incoming raw materials and outgoing merchandise are received and shipped from here. We use 56 feet containers. Our operating hours are Monday to Friday 8:00 a.m. to 6:30 p.m. Saturday 8:00 a.m. to 4:30 p.m. Employee normally arrives 15 minutes early and leaves 15 minutes later.

Being the only immediate neighbor of 8451 Bridgeport Road, we request unobstructed traffic arrangement and use of road for the complete duration of 8451 Bridgeport Road development period.

Sincerely,

A handwritten signature in black ink that reads 'Joseph Fung'.

Joseph Fung
General Manager
M J Fashions Ltd.





City of Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: February 3, 2015

From: Wayne Craig
Director of Development

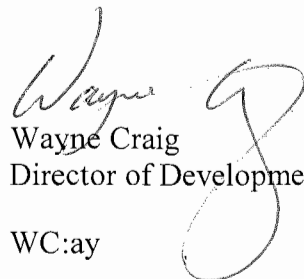
File: DP 14-677603

Re: Application by Bil Ahira for a Development Permit at 10760 Aintree Crescent

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of a Coach House at 10760 Aintree Crescent on a site zoned "Single Detached with Granny Flat or Coach House – Edgemere (RE1)"; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to allow the coach house to be located 2.0 m from the southern interior side lot line.


Wayne Craig
Director of Development
WC:ay

Staff Report

Origin

Bil Ahira has applied to the City of Richmond for permission to construct a coach house at 10760 Aintree Crescent on a site zoned “Single Detached with Granny Flat or Coach House – Edgemere (RE1)”. The property currently contains a single-detached dwelling which will be demolished to accommodate the coach house and a new single-detached dwelling.

Development Information

Please refer to attached Development Application Data Sheet (**Attachment 1**) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

The subject site is located in the Edgemere neighbourhood and is oriented east-west with an existing rear lane along the west property line and Aintree Crescent along the east property line. The property is surrounded by other single-family residential lots zoned “Single Detached with Granny Flat or Coach House – Edgemere (RE1)”.

The subject proposal is the first Development Permit Application to allow construction of a coach house in accordance with the provisions of the “Single Detached with Granny Flat or Coach House – Edgemere (RE1)” zone, and with the OCP Development Permit Area guidelines for coach houses in the Edgemere neighbourhood.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable Development Permit Guidelines and other sections of the Official Community Plan and is in general compliance with the “Single Detached with Granny Flat or Coach House – Edgemere (RE1)” zone, except for the variance noted below.

Zoning Variance

The applicant has requested to vary the provisions of Richmond Zoning Bylaw 8500, Section 8.14.6(3), which requires the coach house to be located 2.0 m from the northern interior side lot line to reduce shadowing on the adjacent lot to the south.

Staff have closely reviewed this section of the bylaw, and have determined that the restrictions on the coach house location in relation to the property lines is in error. The restriction should be to limit the location of the coach house to 2.0 m from the southern property line to limit shadowing on the adjacent lot to the north.

Staff support the proposed variance on the following basis:

- Requiring a granny flat or coach house to be located on a lot with an east-west orientation to be located 2.0 m from the northern interior side lot line would result in shadowing impact on the adjacent north lot, contrary to the intent of the provision.

- Locating the coach house 2.0 m from the southern property line ensures that any shadowing occurs on the subject lot only and complies with the minimum interior side yard setback for the coach house.
- Staff will bring forward an amendment to Richmond Zoning Bylaw 8500, Section 8.14.6(3) to clarify this issue as part of an upcoming house-keeping amendment process to the Zoning Bylaw later in 2015.

Advisory Design Panel Comments

Due to the small scale of the proposed development, the application was not presented to the Advisory Design Panel.

Analysis

Conditions of Adjacency

- The proposed coach house design complements the existing single-family form, character and scale of the surrounding neighbourhood. The proposed setbacks of the coach house conform with the requirements of the “Single Detached with Granny Flat or Coach House – Edgemere (RE1)” zone to protect the privacy of adjacent single-family residential lots and minimize impacts of shadowing on neighbouring properties.

Site Planning

- Consistent with OCP guidelines, the proposed coach house fronts onto the rear lane with the front access door and garage entrance facing the west elevation. The tallest element of the proposed coach house faces the rear lane.
- The orientation of the coach house acts to enhance the visibility and appearance of the lane as a public road.
- All primary windows on the 2nd storey of the proposed coach house are oriented towards the lane.
- The proposed location of the coach house minimizes the impacts of shadowing on the principal dwelling and on the private open space of the coach house.
- In accordance with Section 8.14.9(7) of Richmond Zoning Bylaw 8500, a minimum 0.9 m wide permeable pedestrian walkway is proposed on-site leading from Aintree Crescent to the coach house.
- A private outdoor space (30 m² in area) exclusively for the benefit of the coach house is proposed directly east of the coach house.
- Proposed garbage and recycling bin locations are sufficiently set-back from the rear property line and screened by 1.8 m tall cedar fencing.
- The proposal provides a total of four (4) parking spaces on-site, satisfying the minimum parking space requirements of Richmond Zoning Bylaw No. 8500. Proposed vehicle access to the property is from the lane only.
- Visibility and a pedestrian-oriented experience are enhanced through the proposed use of outdoor lighting on the coach house façade and on landscaped areas adjacent to the lane.

Architectural Form and Character

- The coach house design complements the proposed principal dwelling design through the use of similar building materials (a combination of cultured stone and cap, wood trim and acrylic stucco) as well as detailing and architectural features including dormers and secondary roofs.
- Proposed articulation along the west façade of the coach house through the use of dormers and secondary roof features enhance architectural depth and detail along the lane.
- Proposed roof articulation contributes to the unique architectural form and design of the coach house.

Landscaping and Trees

- The applicant proposes to remove ten (10) trees located on-site. The applicant has agreed to plant and maintain five (5) replacement trees on the subject property to compensate for the ten (10) trees proposed to be removed. Replacement trees and other landscaping should be strategically located to provide sufficient screening from adjacent lots and to enhance the lane as a public road.
- To ensure that the five (5) replacement trees are planted and maintained on-site, and that the lot is enhanced with additional landscaping including individual shrubs and flowering plants, the applicant is required to submit a Landscaping Security in the amount of \$5,500. Upon project completion, the City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.

Conclusion

The proposal is for a new coach house fronting an existing rear lane on the property at 10760 Aintree Crescent in the Edgemere neighbourhood.

The development proposal aims to fit into the existing form, character and scale of the surrounding neighbourhood, which consists primarily of single-family dwellings. The proposal conforms with the design guidelines of applicable sections in the Official Community Plan, as well as the “Single Detached with Granny Flat or Coach House – Edgemere (RE1)” zone.

On this basis, Staff recommends support for this Development Permit application.



Andrew Yu
Planning Technician (Temp)
AY:rg

The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter-of-Credit for landscaping in the amount of \$5,500.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*

- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).
- If applicable, payment of latecomer agreement charges associated with eligible latecomer works.



DP 14-677603

Attachment 1

Address: 10760 Aintree Crescent

Applicant: Bil Ahira

Owner: Bilvinder S. Ahira

Planning Area(s): Shellmont

	Existing	Proposed
Site Area:	659.7 m ²	659.7 m ²
Land Uses:	Single-family residential	Single-family residential with detached Coach House
OCP Designation:	Neighbourhood Residential	No change
Zoning:	Single Detached with Granny Flat or Coach House – Edgemere (RE1)	No change
Number of Lots:	One (1)	No change

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.60	Max. 0.60	none
Lot Coverage - buildings:	Max. 45%	39%	none
Lot Coverage – buildings, structures and non-porous surfaces:	Max. 70%	Max. 70%	none
Lot Coverage – landscaping:	Min. 30%	Min. 30%	none
Setback – Front Yard:	Min. 6.0 m	Min. 6.0 m	none
Setback – Northern Interior Side Yard (for coach house):	2.0 m from northern property line	6.0 m	Variance requested
Setback – Southern Interior Side Yard (for coach house):	Min. 2.0 m	Min. 2.0 m	none
Setback – Rear Yard (for coach house):	Min. 1.2 m for no more than 65% of rear façade. Min. 3.0 m for at least 35% of rear façade. Min. 1.5 m for building entry.	Min. 1.2 m for no more than 65% of rear façade. Min. 3.0 m for at least 35% of rear façade. Min. 1.5 m for building entry.	none
Height (m):	Max. 6.0 m or 1½ storeys	Max. 6.0 m or 1½ storeys	none
Lot Size (m ²):	550.0 m ²	659.7 m ²	none
Minimum Frontage (m):	Min. 7.5 m	20.1 m	none
Minimum Lot Width (m):	Min. 18.0 m	20.1 m	none
Minimum Lot Depth (m):	Min. 24.0 m	32.8 m	none



No. DP 14-677603

To the Holder: BIL AHIRA

Property Address: 10760 AINTREE CRESCENT

Address: 3441 EAST 48th AVENUE, VANCOUVER, BC V5S 1H6

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. That "Richmond Zoning Bylaw 8500" is hereby varied to allow the coach house to be located 2.0 m from the southern interior side lot line.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #5 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$5,500 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit

No. DP 14-677603

To the Holder: BIL AHIRA

Property Address: 10760 AINTREE CRESCENT

Address: 3441 EAST 48th AVENUE, VANCOUVER, BC V5S 1H6

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

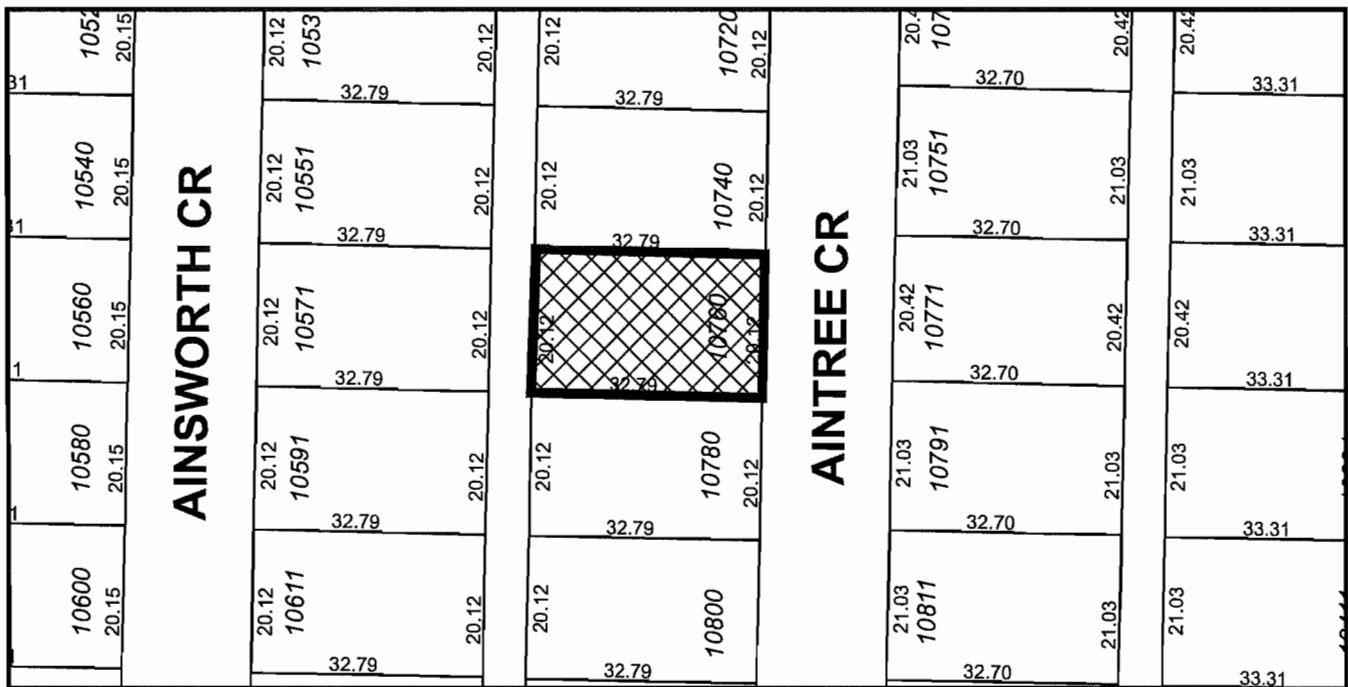
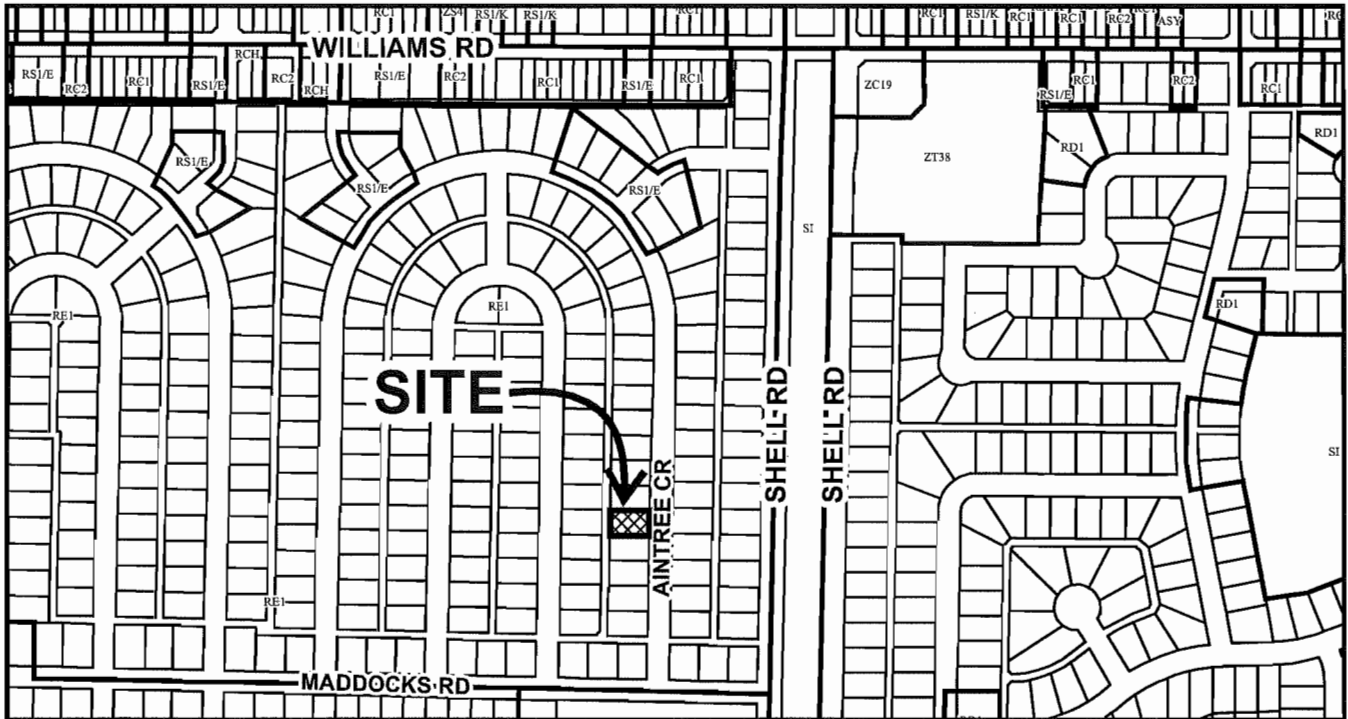
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond



DP 14-677603

Original Date: 12/10/14

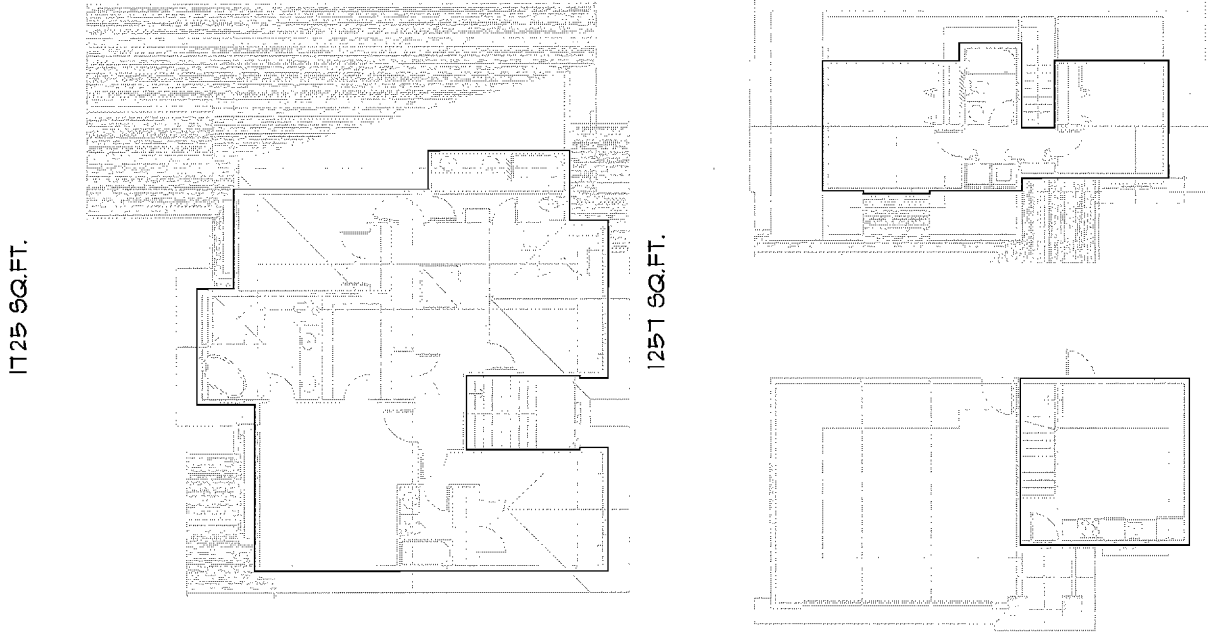
Revision Date:

Note: Dimensions are in METRES

LEGAL DESCRIPTION: 35 SEC 35 BLK 4N RG 6W PL 20612 9B 15

SETBACKS:	REG'D.	PROPOSED.
FRONT:	6.00m	
REAR:	6.00m	
SIDE (interior)	2.00m	
COACH HOUSE SIDE	2.00m	
COACH HOUSE REAR	1.00m	
LOT COVERAGE:	45.00% MAX.	39.60%
BUILDING AREA:	257.82 sq. m.	
LOT AREA:	660 sq. ft.	
BUILDING HEIGHT:	9.0m	9.95m
COACH HOUSE HEIGHT:	6.0m	5.93m
GROSS FLOOR AREA:	3629 PROPOSED 3639 sq. ft. MAX	
(inclusion of all floor areas and the garage)		

CALCULATIONS:
E.A.R. CALCULATION
LOT AREA: 7107 sq. ft.
2.00% REQUIRED: 142.15 sq. ft.
1735 sq. ft. MAIN FLOOR
1259 sq. ft. UPPER FLOOR
358 sq. ft. MAIN FLOOR COACH HOUSE
381 sq. ft. UPPER FLOOR COACH HOUSE
COACH HOUSE E.A.R. CALCULATION
646 SQ.FT. MAX 48% M.F. = 258 + 60% U.F. = 381
M.F. 258 + U.F. 381 PROPOSED
COVERED AREAS
3627.50 x 10% = 362.8 SQ. FT.
331 SQ.FT. TOTAL
COACH HOUSE 3.382 PROPOSED (6m2 MAX)
GARAGE AREA
PROPOSED 928 SQ.FT. (53.8 SQ.FT MAX)
MAX INTERFERENCE
PROSED 322.32 (49.0 SQ.FT. MAX)



7101 sq. ft.

SCALE: $1/8" = 1'-0"$

PLAN #1

DP 14-677603

258 SQ. FT.

387 SQ. FT.

OVERVIEW

SCALE: 1/8" = 1'-0"

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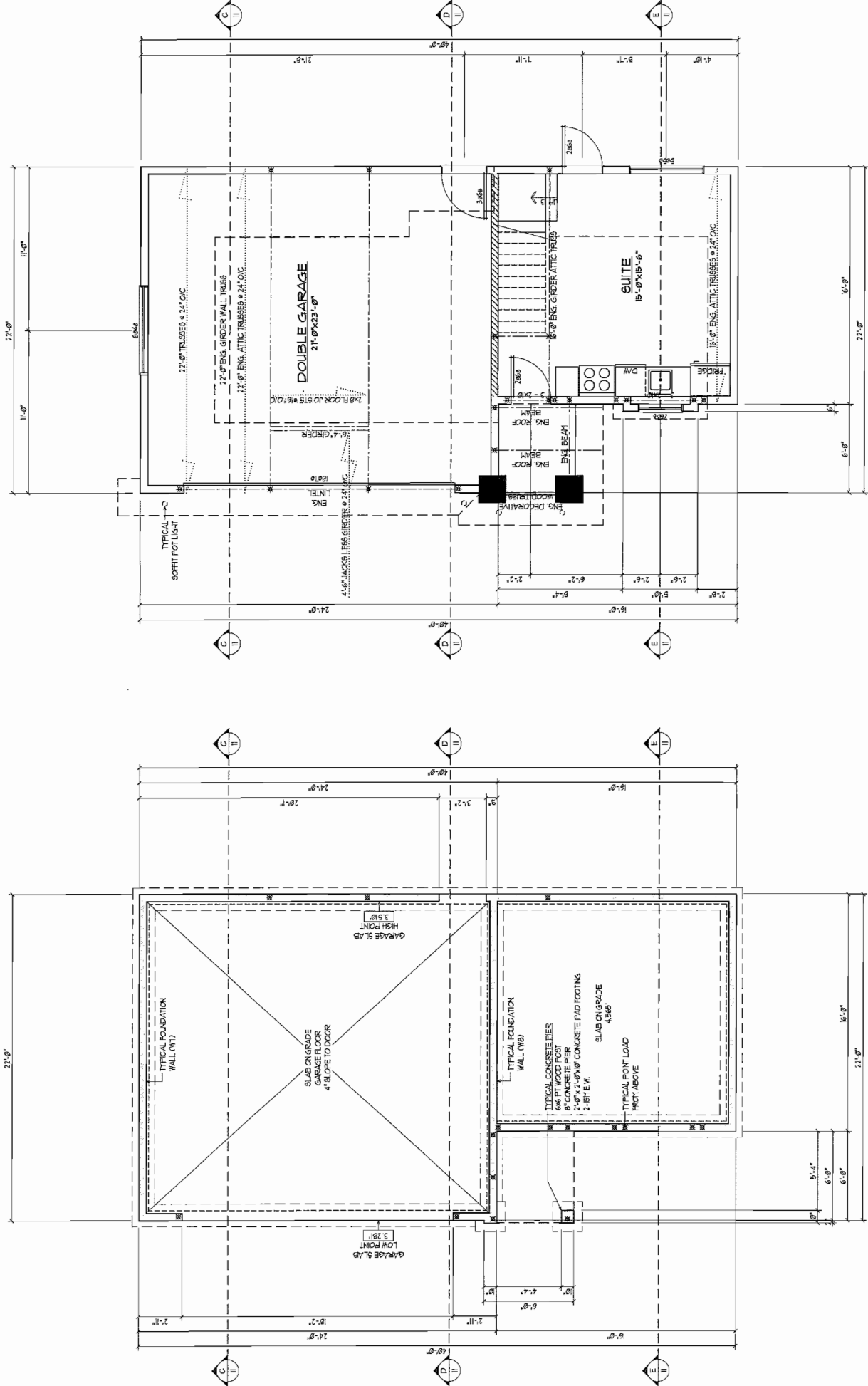
Design & Drafting Services

CLIENT:
JAG GIL AND BIL AHIRA
3441 E 48th AVE.
VANCOUVER, B.C.
604-765-5192

PROJECT TITLE:
PROPOSED RESIDENCE
10760 AINTREE CRESENT
RICHMOND, B.C.

SHEET TITLE:
SITE PLAN
ZONING ANALYSIS

PROJECT: RN-14-35	DATE: 11/3/2014
DESIGNED: J.M.	DRAWN: R.M.
DRAWING NO.	



COACH HOUSE
FOUNDATION PLAN

SCALE: 1/4" = 1'-0"

COACH HOUSE
MAIN FLOOR PLAN

258 sq. ft.

SCALE: 1/4" = 1'-0"

PLAN # 2

DP 14-677603

REV.	DATE	DESCRIPTION

Design & Drafting Services
o Residential & Commercial Design Service
o Office: (604) 557-9465 Fax: 1(866) 614-8581
Email: info@4ddrafting.com

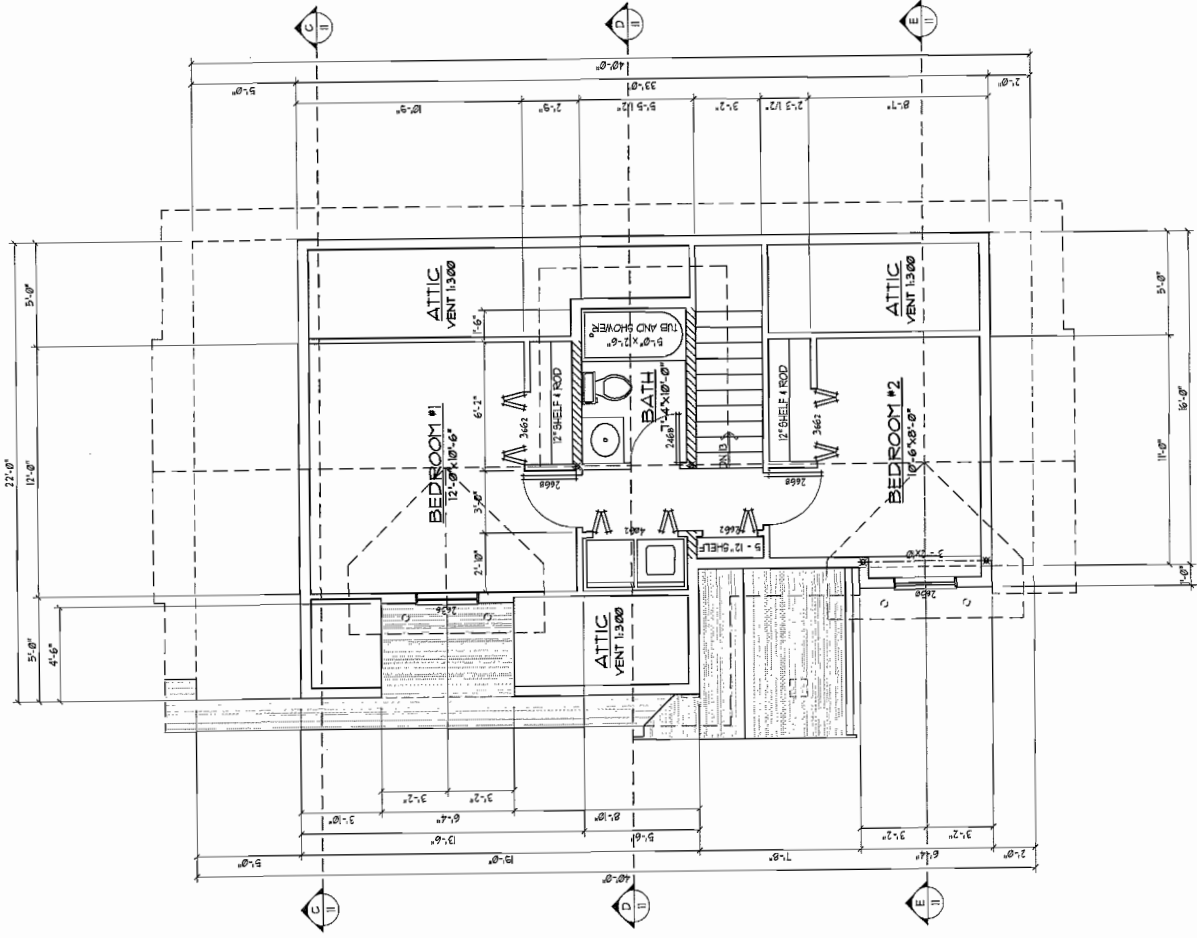
CLIENT:
JAG GIL AND BL AHRA
3441 E 48th AVE.
VANCOUVER, B.C.
604-765-5192

PROJECT TITLE:
PROPOSED RESIDENCE
10760 ANTREE CRESENT
RICHMOND, B.C.

SHEET TITLE:
COACH HOUSE
FOUNDATION PLAN
MAIN FLOOR PLAN

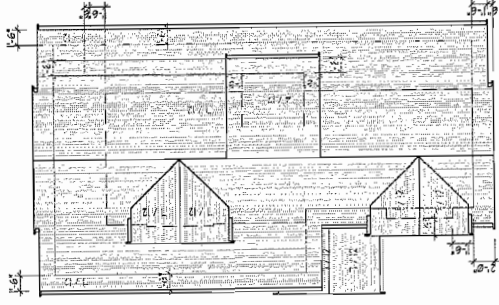
PROJECT: RM14-35 | DATE: 1/10/2014
DESIGNED: J.M. | DRAWN: P.M.
DRAWING NO.

8
OF 12



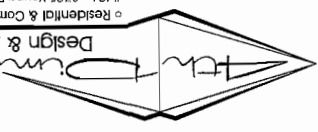
COACH HOUSE
UPPER FLOOR PLAN

387 sq. ft.
SCALE: 1/4" = 1'-0"



COACH HOUSE
ROOF PLAN

SCALE: 1/8" = 1'-0"

 <p>Design & Drafting Services Residential & Commercial Design Services Office: (604) 557-9455 Fax: 1(866) 614-8581 Email: info@ddrafting.com #406 - 8705 Young Rd., Chilliwack, BC V2P 4P3</p>	<p>CLIENT: JAG GIL AND BIL AHIRA 3441 E 48th AVE. VANCOUVER, B.C. 604-765-5192</p>	<p>PROJECT TITLE: PROPOSED RESIDENCE 10760 ANTREE CRESENT RICHMOND, B.C.</p>	<p>SHEET TITLE: COACH HOUSE UPPER FLOOR PLAN ROOF PLAN</p>	<p>PROJECT: RM14-35 DATE: 1/19/2014 DESIGNED: J.M. DRAWN: B.M.</p>	<p>DRAWING NO. 9</p>	<p>OF 12</p>
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PLAN # 3
DP 14-677603

WALL ASSEMBLIES

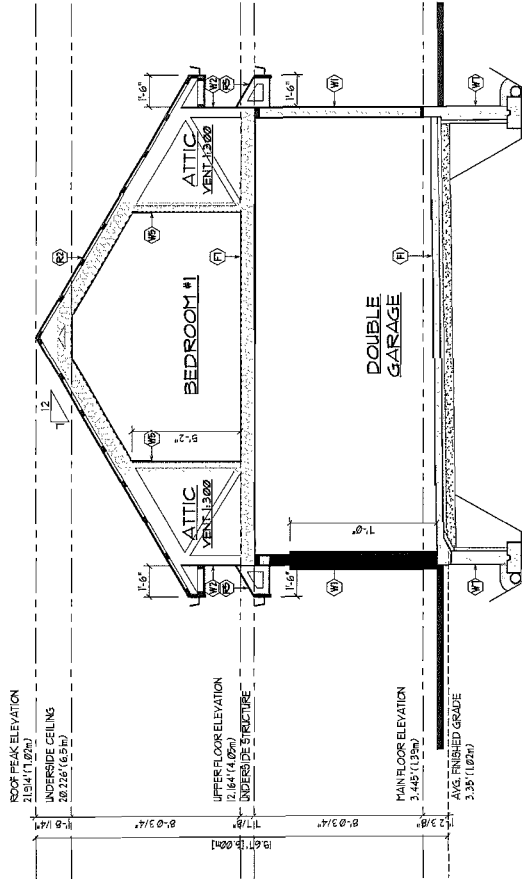
- (A) EXTERIOR WALL
EXTERIOR FINISH (SEE ELEVATIONS)
R-15 BUILDING PAPER
1/2" FL Y WOOD SHEATHING • 24" O.C.
2x6 STUDS • 8" O.C.
R-18 BATT INSULATION
6" ML POLY V.B.
1/2" PAINTED GYPSUM BOARD
- (B) EXTERIOR WALL - ATTIC TRUSSES
EXTERIOR FINISH (SEE ELEVATIONS)
R-15 BUILDING PAPER
1/2" FL Y WOOD SHEATHING • 24" O.C.
2x6 STUDS • 8" O.C.
R-18 BATT INSULATION
6" ML POLY V.B.
1/2" PAINTED GYPSUM BOARD
- (C) BEARING WALL
1/2" GYPSUM BOARD - PAINTED
2x4 OR 2x6 WOOD • 8" O.C.
2x6 STUDS • 8" O.C.
1/2" FL Y WOOD SHEATHING • 24" O.C.
ENGINEERED ATTIC TRUSSES • 24" O.C.
- (D) PARTITION WALL
1/2" GYPSUM BOARD - PAINTED
1/2" FL Y WOOD SHEATHING • 24" O.C.
2x4 CROSS STRIPPING • 24" O.C.
1/2" GYPSUM BOARD - PAINTED
- (E) PARTITION WALL - ATTIC TRUSSES
1/2" GYPSUM BOARD - PAINTED
ENGINEERED ATTIC TRUSSES • 24" O.C.
R-18 BATT INSULATION
- (F) PARTITION WALL - ATTIC
1/2" GYPSUM BOARD - PAINTED
1/2" FL Y WOOD SHEATHING • 24" O.C.
2x6 STUDS • 8" O.C.
R-18 BATT INSULATION
- (G) FOUNDATION WALL
CONCRETE WALL ON FINISHED GRADE
ASPHALT WATERPROOFING
R-18 CONCRETE STRIP FOOTING
ASPHALT WATERPROOFING
R-18 RIGID INSULATION
- (H) FOUNDATION WALL - INTERIOR
R-18 CONCRETE WALL ON FINISHED GRADE • 24" O.C. EW
R-18 CONCRETE STRIP FOOTING

FLOOR ASSEMBLIES

- (A) GARAGE FLOOR SLAB
4" CONCRETE SLAB
1" ML POLY V.B.
R-18 RIGID INSULATION
1" ML POLY V.B.
1" ML POLY V.B.
R-18 RIGID INSULATION
R-18 RIGID INSULATION
- (B) CONCRETE SLAB - HEATED
4" CONCRETE SLAB ON WOOD HEATING
1" ML POLY V.B.
R-18 RIGID INSULATION
R-18 RIGID INSULATION
- (C) CONCRETE DECK
4" CONCRETE SLAB - MIN 1% SLOPE TO DRAIN
1" ML POLY V.B.
R-18 RIGID INSULATION
- (D) WOOD FLOOR - HEATED
FINISHED FLOOR ON UNDERLAYMENT
1/2" CONCRETE TOPPING ON SLAB HEATING
5/8" 1x6 FL Y WOOD SUBFLOOR (GLUED + SCREWED)
ENGINEERED 11/8" FLOOR JOISTS
5/8" GYPSUM BOARD - PAINTED
- (E) WOOD FLOOR - HEATED EXTERIOR
FINISHED FLOOR ON UNDERLAYMENT
1/2" CONCRETE TOPPING ON SLAB HEATING
5/8" 1x6 FL Y WOOD SUBFLOOR (GLUED + SCREWED)
ENGINEERED 11/8" FLOOR JOISTS
R-40 BATT INSULATION
3/4" 1x6 GYPSUM BOARD
- (F) WOOD FLOOR - ATTIC TRUSSES
FINISHED FLOOR ON UNDERLAYMENT
5/8" 1x6 FL Y WOOD SUBFLOOR (GLUED + SCREWED)
ENGINEERED ATTIC TRUSSES • 24" O.C.
R-40 BATT INSULATION

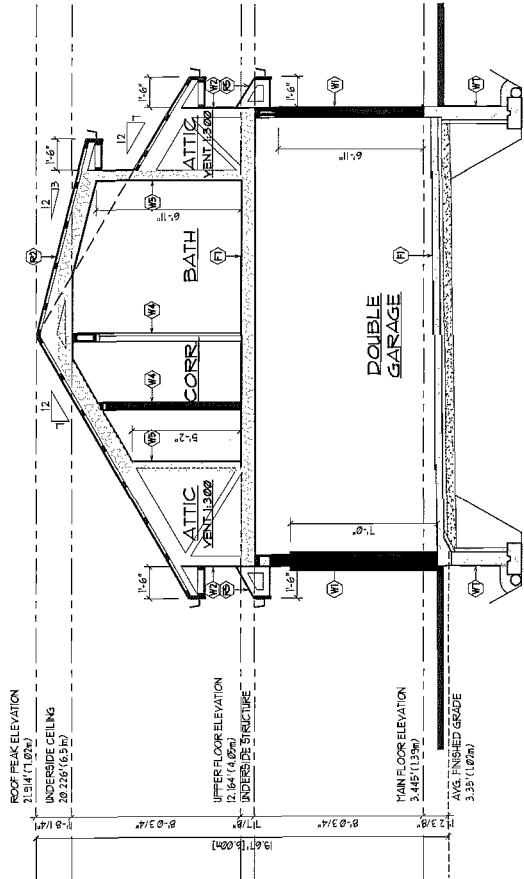
ROOF ASSEMBLIES

- (A) TRUSS ROOF
REINFORCED FIBERGLASS ASPHALT ROOF SHINGLES
5" LBS UNDERLAYMENT
1/2" FL Y WOOD SHEATHING ON H CLIPS
ENGINEERED WOOD TRUSSES • 24" O.C.
ATTIC SPACE VENTED 1300
R-40 BATT INSULATION
6" ML POLY VAPOUR BARRIER
5/8" GYPSUM BOARD - PAINTED
- (B) TRUSS ROOF - ATTIC TRUSSES
REINFORCED FIBERGLASS ASPHALT ROOF SHINGLES
5" LBS UNDERLAYMENT
1/2" FL Y WOOD SHEATHING ON H CLIPS
ENGINEERED WOOD TRUSSES • 24" O.C.
ATTIC SPACE VENTED 1300
R-40 BATT INSULATION
6" ML POLY VAPOUR BARRIER
5/8" GYPSUM BOARD - PAINTED
- (C) TJI ROOF
REINFORCED FIBERGLASS ASPHALT ROOF SHINGLES
5" LBS UNDERLAYMENT
1/2" FL Y WOOD SHEATHING ON H CLIPS
2x4 CROSS STRIPPING • 24" O.C.
ENGINEERED 11/8" FLOOR JOISTS
R-40 BATT INSULATION
2x6 WOOD JOISTS • 8" O.C. (AS REQUIRED)
6" ML POLY VAPOUR BARRIER
5/8" GYPSUM BOARD - PAINTED
- (D) WOOD DECK ROOF
REINFORCED FIBERGLASS ASPHALT ROOF SHINGLES
5" LBS UNDERLAYMENT
1/2" FL Y WOOD SHEATHING ON H CLIPS
2x6 ROOF DECKING
ENGINEERED WOOD BEAMS AND TRUSSES
- (E) SKIRT ROOF
REINFORCED FIBERGLASS ASPHALT ROOF SHINGLES
5" LBS UNDERLAYMENT
1/2" FL Y WOOD SHEATHING ON H CLIPS
ENGINEERED WOOD TRUSSES
3/4" 1x6 GYPSUM BOARD



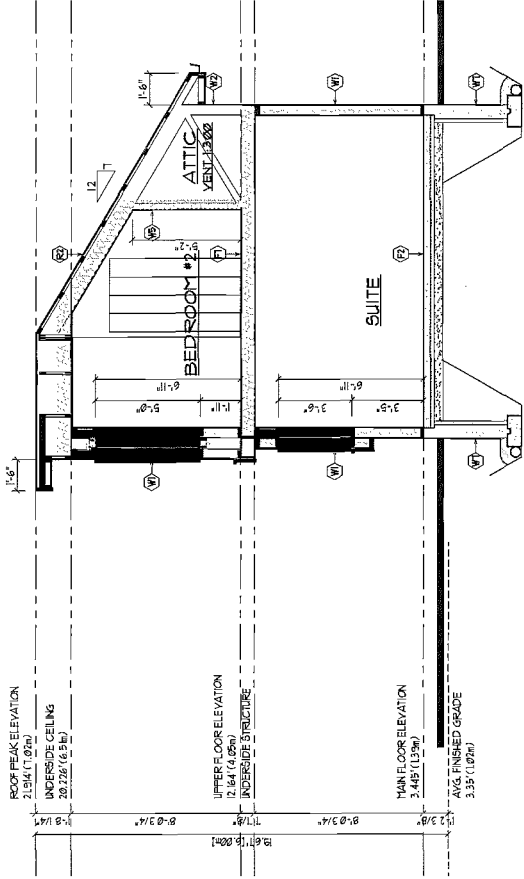
SECTION C

SCALE: 1/4" = 1'-0"



SECTION D

SCALE: 1/4" = 1'-0"



SECTION E

SCALE: 1/4" = 1'-0"

CLIENT:
JAG GIL AND BIL AHIRA
3441 E 48th AVE.
VANCOUVER, B.C.
604-765-5192

PROJECT TITLE:
PROPOSED RESIDENCE
10760 ANIRÉE CRESENT
RICHMOND, B.C.

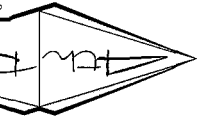
SHEET TITLE:
COACH HOUSE
BUILDING SECTIONS

PROJECT R44-1-35
DESIGNED J.M.
DRAWN B.M.
DRAWING NO.

11
OF 12

REV. DATE DESCRIPTION

Atkinson
Design & Drafting Services
Residential & Commercial Design Services
#406 - 8705 Young Rd., Chilliwack, BC V2P 4P3
Office: (604) 557-9465 Fax: (604) 614-6581
Email: info@4dcdrafting.com



PLAN #5
DP 14-677603



City of
Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: February 5, 2015


From: Wayne Craig
Director of Development

File: DP 14-676361

Re: Application by the Greater Vancouver Sewerage & Drainage District for a
Development Permit at 1000 Ferguson Road

Staff Recommendation

That a Development Permit be issued which would permit the Greater Vancouver Sewerage & Drainage District to construct a 550 m² screening & degrit building, a 20 m diameter thickener, 25 m² thickener Pump station & four 36 m² digester mixing pump buildings (one for each of the existing digesters) at 1000 Ferguson Road on a site designated an Environmentally Sensitive Area (ESA).


Wayne Craig
Director of Development

WC:dcb

Staff Report

Origin

Greater Vancouver Sewerage & Drainage District (aka Metro Vancouver) has applied to the City of Richmond for permission to construct a 550 m² screening & degrit building, a 20 m diameter thickener, 25 m² thickener Pump station & four 36 m² digester mixing pump buildings (one for each of the existing digesters). The site is designated an Environmentally Sensitive Area (ESA). The Iona Island Wastewater Treatment Plant (IIWWTP) is located at 1000 Ferguson Road on a site zoned “School and Institutional Use (SI)”.

The 119 acre site currently contains the main portion of the Iona Island Wastewater Treatment facility that provides primary sewage treatment facilities for approximately 600,000 residents in Vancouver, the UBC Endowment Lands and parts of Burnaby and Richmond.

Under current Provincial legislation, the Greater Vancouver Sewerage & Drainage District (GVS&DD) is not required to submit a Development Permit application to the City for the proposed works however the GVS&DD has voluntarily agreed to bring its proposal forward for formal review through this Development Permit application.

A rezoning is not required for the proposed works.

Development Information

Please refer to the attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Surrounding Development:

- To the north, the North Arm of the Fraser River;
- To the east, is Canfor Point within the Iona Island Regional Park – zoned “School and Institutional Use (SI)”;
- To the south, the Iona Island causeway and lands under Port Metro Vancouver jurisdiction – both are zoned “School and Institutional Use (SI)”;
- To the west, Iona Beach, Iona Jetty and the North Arm Jetty. Iona Beach is zoned “School and Institutional Use (SI)”. The North Arm Jetty is not zoned.

With the exception of the Iona and North Arm Jetties, Iona Island is entirely designated as “Conservation” in Richmond’s Official Community Plan and is zoned “School and Institutional Use (SI)” under Zoning Bylaw 8500.

The GVS&DD advises that the intent of the upgrade project is to “better enable the existing plant to meet the effluent quality requirements specified in its Operating Certificate” (Tera Environmental submission dated October, 2014). Specifically the upgrade will provide grit removal from the primary sludge, minimize screening debris entering the digesters, increase sludge thickening capacity and improve digester mixing efficiency.

The upgrade will result in seven (7) new structures erected on-site through this project:

- one – 550 m² Screening and Degrit Building;
- one – 20 m diameter Thickener;
- one – 25 m² Thickener Pump Station; and
- four – 25 m² Digester Mixing Pump Buildings.

All the structures except the digester mixing pump buildings will be located off the east edge of the existing treatment facility. The four digester mixing pump buildings will be located adjacent to each of the four existing digesters currently on the site and will not impact any environmental features on the site.

The screening and degrit building, the thickener facility and the thickener pump station need to be located on a raised and stabilized base in close proximity to the existing treatment plant. The location selected will result environmental impacts which are further detailed in this report along with a proposed mitigation/compensation strategy.

Site preparation activities anticipated with this project include:

- Clearing of trees and vegetation;
- Grubbing and stripping to remove organics and overburden;
- Ground improvements, including placement of fill and ground densification;
- Removing biosolids stockpiles from some of the project footprint area;
- Paving and fencing installations.

The GVS&DD is hoping to undertake the upgrade construction commencing in June, 2015 and completing the work by December, 2016. Project management is being undertaken by CH2M Hill Energy Canada. Environmental studies (environmental impact assessment and mitigation / compensation plan) were undertaken by Tera Environmental Consultants. EDI Environmental Dynamics Inc. were retained to delineate both the extent of the natural boundary/high water mark of McDonald Slough and an upland wetland feature present in vicinity of the project site.

Background

The development site is located on Iona Island and is surrounded by portions of Metro Vancouver's Iona Beach Regional Park. The existing wastewater treatment plant opened in 1963 and has undergone 6 expansions since that time to accommodate growth and treatment upgrades. Metro Vancouver's web site notes that the facility is regulated through an Operational Certificate issued by the BC Ministry of Environment.

Related Policies and Bylaws

The site is designated "Conservation" in Richmond's Official Community Plan. The OCP also designates the property as a Development Permit area as an Environmentally Sensitive Area. The specific designation at the subject site acknowledges the presence of Intertidal, Shoreline and Freshwater Wetlands. The focus of the ESA DP guidelines within the OCP are intended to protect and enhance the environmental resources and ecosystem services.

Zoning Compliance

Under the “School and Institutional Use (SI)” zoning there are no maximums in terms of floor area ratio, site coverage or building/structure height (however Transport Canada maximum height regulations apply). The proposed development complies with the existing “School and Institutional Use (SI)” zone.

Advisory Design Panel Comments

This application was not referred to the Advisory Design Panel as the upgrade project does not involve design components and is primarily focused on the mitigation and compensation efforts related to the foreseeable environmental impacts.

Analysis

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan and is generally in compliance with the “School and Institutional Use (SI)”. The environmental impacts and proposed Compensation Plan were reviewed by the City’s Environmental Review Panel (ERP) on December 10, 2014. The Compensation Plan elements outlined in this report reflect the changes recommended by the ERP.

An environmental assessment was undertaken by Tera Environmental (Compensation Plan final report dated January 2015). The assessment focuses on the anticipated impacts from the screening & dewatering facilities and the associated sludge thickener facilities that will be located to the east of the existing treatment plant since all the other building additions occur within the existing plant itself.

Mitigation Measures

Tera’s report states that the initial planning and design of the project incorporated measures to mitigate environmental impacts. Specifically;

- The majority of the Project footprint is situated on previously disturbed land, an old parking area and the existing biosolids storage site.
- The Project footprint has been selected to minimize potential disturbance to the adjacent wetland and the forested buffer on the north edge of the wetland.
- The existing row of trees along the western edge of the Project site, south of Area C, will not be removed.
- The existing berm surround the wetland (outlined in blue on the attached reference plan) will not be removed.

Environmental Surveys

Based on the addition requirements and site surveys, vegetation surveys were undertaken in four areas that would be impacted by the new addition. The survey areas are labelled A through D on Plan # 1.

Area A was found to be significantly vegetated with 68 trees (red alder, paper birch and cottonwood species) and a variety of understorey vegetation species (red elderberry, salmonberry, ivy, oceanspray, holly, etc.) The understorey was noted to include extensive areas were noted to include invasive species (Himalayan blackberry) and non-native species.

Area B is located adjacent to a cattail marsh associated with a freshwater wetland to the southeast of the proposed addition. Treed overstorey in this area includes semi-mature red alders (14 trees) and 3 mature black cottonwoods. Understorey within the treed area was dominated by Himalayan blackberry, pockets of Oregon grape and Sitka willow. EDI Environmental Dynamics categorized the wetland to the east of Area B as having medium ecological value with high bird use (EDI 2014a). The wetland was noted as having no apparent direct connection to McDonald Slough.

Area C is located along the west side of the proposed addition. Vegetation in this area will be eliminated by the project. The report notes that a stand of 188 black cottonwood trees had been previously removed from this area. The remaining understorey vegetation includes Himalayan blackberry, red elderberry, grasses and Canada thistle.

Area D is located on biosolids stockpiles previously generated by the treatment plant operations. No trees are present in this area and ground vegetation includes herbaceous species such as grasses, nightshade and cattails. Approximately half of Area D will be impacted by the project.

Bird Habitat

Field observations undertaken in August 2014 noted sixteen species of birds including spotted towhee, marsh wren, American robin, black-capped chickadee, cedar waxwing, American goldfinch, northwestern crow, song sparrow, common yellowthroat, Bewick's wren, Anna's hummingbird, willow flycatcher, barn swallow, glaucous-winged gull, Canada goose and a great horned owl. The breeding and non-breeding habitat of the observed bird species was used in determining which trees and shrub species would enhance the habitat in the area as part of the overall compensation strategy.

Environmental Assessment and Impacts

Tera's Compensation Plan notes that a total of 0.66 ha of native vegetation would be removed as a result of the project. Their assessment notes that the understory vegetation in the impacted area is poor and weedy, highly invasive species dominate the area. The existing site disturbance and invasive species pose limitations to native plant growth and the development of native ecosystems needed by bird and animal species with more specific habitat requirements.

Additionally, the existing treed areas are noted as being patchy and dominated by red alder and cottonwood which limit secure cover or buffer vegetation. Based on the relatively poor

condition of the habitat present, Tera proposes that a compensation ratio of 50% (i.e. 0.33 ha) is appropriate for compensation and enhancement.

Compensation Measures and Overall Approach to Compensation

Compensation measures proposed for this project are generally described in Tera's report as follows:

- Removal of invasive species from areas identified for planting.
- Riparian restoration – planting of native trees and shrubs on the wetland berm on the east side of the existing parking lot and the project site.
- Planting with native, fruit-producing shrubs adjacent to the wetland berm and along the west side of the existing parking area.
- Native grass seeding adjacent to the new access road at the southeast end of the project.
- Tree replacement with native trees valued by bird species in selected areas (e.g. along the wetland berm, around the sewage lagoons and west of the existing parking area).
- Bird habitat enhancement through planting native trees and shrubs adjacent to the sewage lagoons west of the existing treatment plant.

The Compensation Plan proposes a maximum of 3,300 trees and shrubs to be planted inclusive of a minimum 564 trees (3:1 compensation for the trees removed in Area C). Compensation trees will be a mix of sizes with two thirds being greater than 1.2 m in height dbh and one third being small stock (30-45 cm) – sizes found to have a better survival rate than larger trees in natural areas and are also more likely to be available from local providers. The tree sizes are generally consistent with the Riparian Restoration Guidelines provided by the Ministry of Environment's planting guidelines.

Plan # 2 shows the primary areas proposed to be cleared and the general areas for compensation measures outlined above.

Plan # 3a provides a summary of the compensation plan showing the tree and shrub species and quantities to be planted in each of the proposed compensation locations identified in Plan #2. An overall balance sheet summarizing the anticipated impacts and the objectives of compensation measures proposed is provided in Plan #3b.

Plan # 4 provides plan details of the proposed building additions and site modifications needed to accommodate the treatment plant upgrades.

The Compensation Plan strategy assesses the different species of birds using this area and outlines their specific habitat needs in relation to what the existing habitat currently provides. From this information tree, shrub and ground cover selections are proposed which will fill in gaps and enhance the usefulness of the area for these birds. The enhancement planting is proposed to be installed in areas where invasive vegetation or lower quality vegetation will be removed. This approach should result in both a higher quality and more productive habitat for the bird species using this area.

Landscape Securities and Commitments

As Metro Vancouver is a superior level of government they have indicated that they are not able to bond for the landscaping installations as normally required through a Development Permit. Metro Vancouver has agreed, however, to provide a letter of commitment to undertake the vegetation installation and the other works outlined in the Compensation Plan. Metro Vancouver will also commit to preparing and reviewing the final detailed planting plan with the City of Richmond prior to the landscaping installations which will not occur until the majority of the site construction works have been completed (i.e. in approximately 2 years time). Consideration for notification signage of the compensation areas as protected natural areas can also be reviewed with the City at that time.

The Compensation Plan includes provision for monitoring the vegetation installations for a period of 3 years with a target of at least 80% survival. Replanting for any shortfall less than this target will be undertaken by the installer.

Other Issues Associated With the Project

a.) Anticipated Traffic Impacts

CH2M Hill representatives have confirmed that the plant increase is expected to result in an increase of one truck every two days to the site. The City's Transportation staff have reviewed this from a traffic management perspective and advise that there are no transportation concerns with the expansion.

b.) Environmental Management Act Release

The BC Ministry of Environment has provided a release pursuant to the Local Government Act (section 946.2(2)(b)) allowing the City of Richmond to approve the Development Permit for this project.

c.) External approvals

CH2M Hill representatives have indicated that the project had been reviewed by Port Metro Vancouver in 2014 and the advice provided was that the project would not require notification to the BC Ministry of Forests, Lands and Natural Resource Operations as the shoreline will not be impacted by the expansion and the overall use is not changing.

Conclusions

The Greater Vancouver Sewerage & Drainage District has voluntarily agreed to submit an application for Development Permit to address their expansion project and respond to the environmental impacts arising from the proposal. They have worked with City staff to address concerns related to these impacts and achieve a solution that will result in a net benefit to wildlife and habitat enhancement in the area.

Staff recommend support for the Development Permit to permit construction of a 550 m² screening & degrit building, a 20 m diameter thickener, 25 m² thickener Pump station & four 36 m² digester mixing pump buildings (one for each of the existing digesters) and the implementation of the Compensation Plan as outlined in this report.



David Brownlee
Planner 2

DCB:cas

The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter of Commitment from Greater Vancouver Sewerage & Drainage District to:
 - Undertake and maintain the landscaping installations (outlined in the Compensation Plan from Tera Environmental Consultants (dated January 2015);
 - Implement the wetland mitigation measures outlined in the Memo from Tera Environmental Consultants (dated October 24, 2014); and,
 - Consult with the City of Richmond on the preparation of the detailed planting plan and consideration of the installation of Protected Natural Area signage prior to the landscaping installations as outlined in the Compensation Plan (Tera January 2015).

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).
- If applicable, payment of latecomer agreement charges associated with eligible latecomer works.



DP 14-676361

Attachment 1

Address: 1000 Ferguson Road

Applicant: Greater Vancouver Sewerage & Drainage District Owner: Greater Vancouver Sewerage & Drainage District

Planning Area(s): Sea Island

	Existing	Proposed
Site Area:	48.35 ha (Approx. 119.5 acres)	Unchanged
Land Uses:	Park and Waste Water Treatment Facility	No Change
OCP Designation:	Conservation	No Change
Zoning:	School and Institutional Use (SI)	No Change

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	No Maximum	Undetermined	none permitted
Lot Coverage:	No Maximum	Undetermined – aerial photo analysis conservatively estimates 6.5% of the site.	None
Setback – Front Yard:	Min. 6.0 m	Greater than 6.0 m	None
Setback – Interior Side and Rear Yards:	Min. 3.0 m	Greater than 3.0 m	None
Height (m):	Max. 12 m within 10 m of a residential zone. Elsewhere No Maximum	18.68 m including the support base.	None
Lot Size:	No Minimum	48.35 ha (Approx. 119.5 acres)	None



City of Richmond

Development Permit

No. DP 14-676361

To the Holder: Greater Vancouver Sewerage & Drainage District
Property Address: 1000 Ferguson Road
Address: 4330 Kingsway,
Burnaby BC V5H 4G8

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #4k attached hereto.
4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
6. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

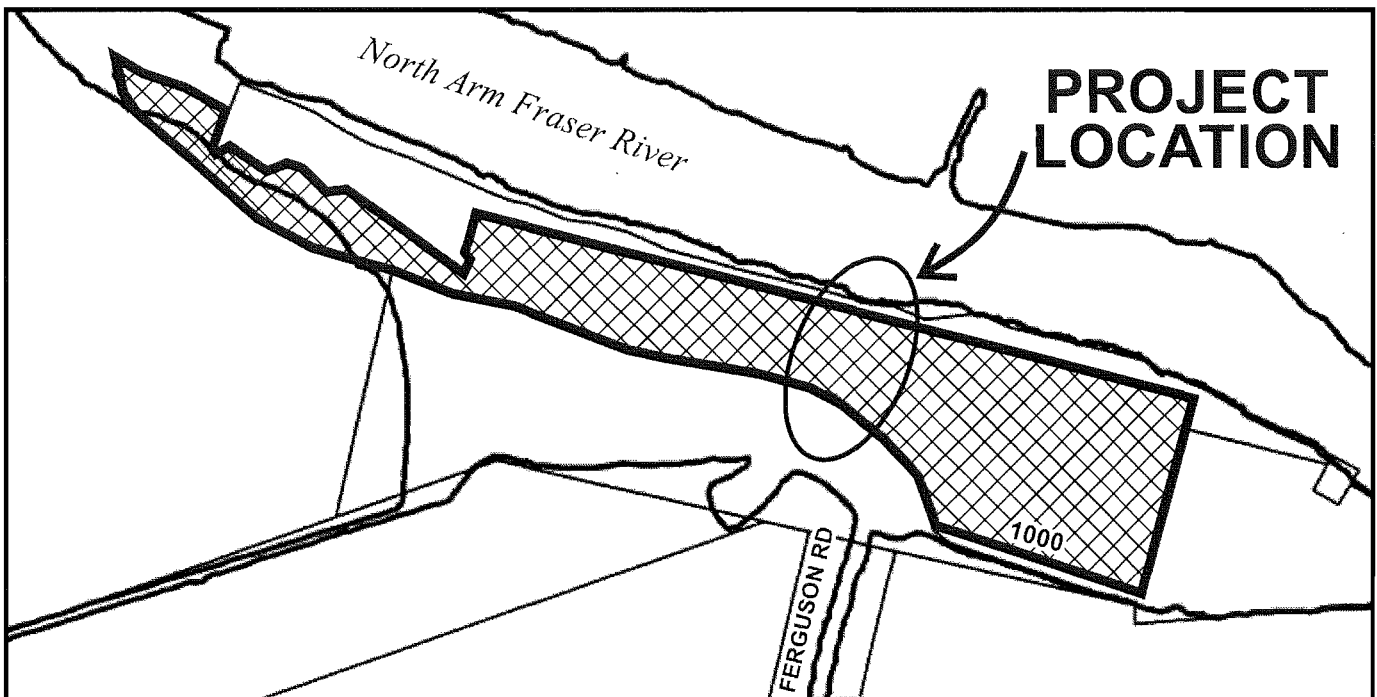
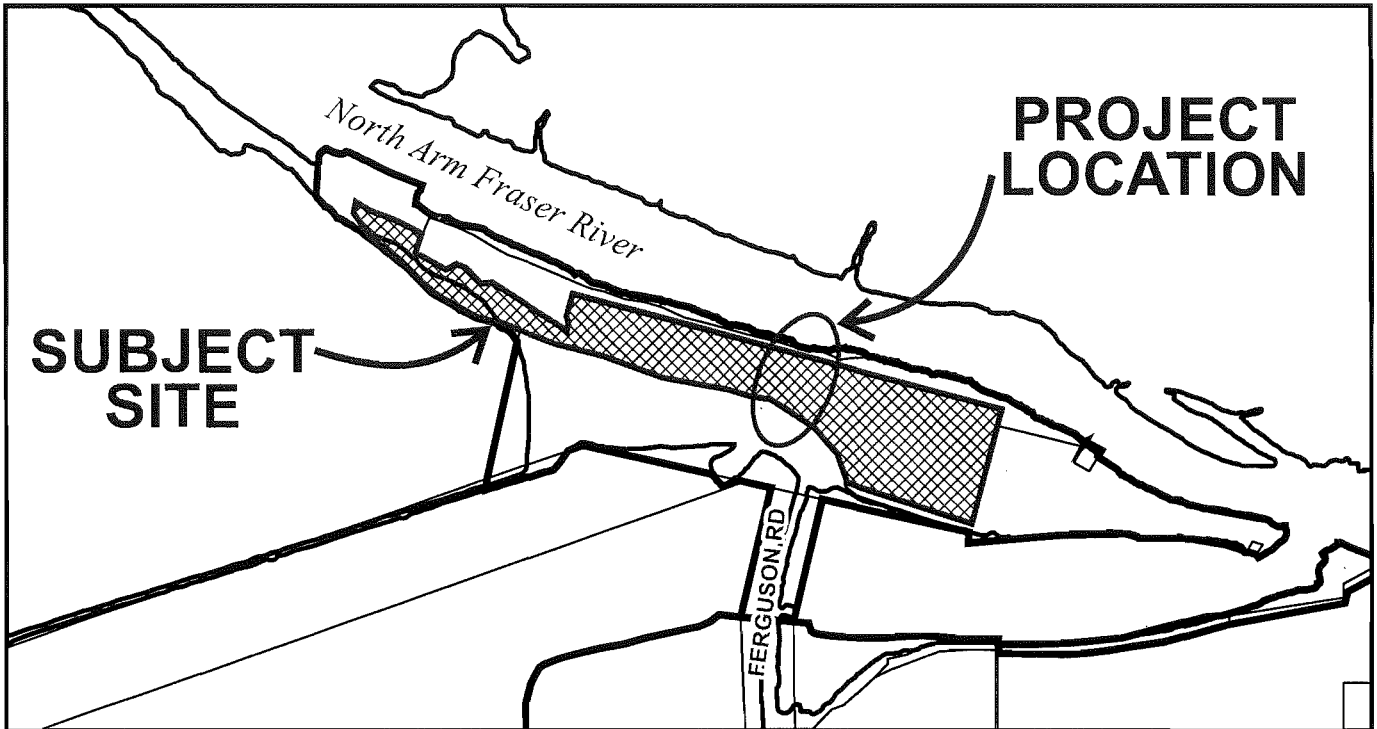
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of
Richmond



DP 14-676361
SCHEDULE "A"

Original Date: 11/18/14

Revision Date: 02/05/15

Note: Dimensions are in METRES



City of
Richmond



DP 14-676361

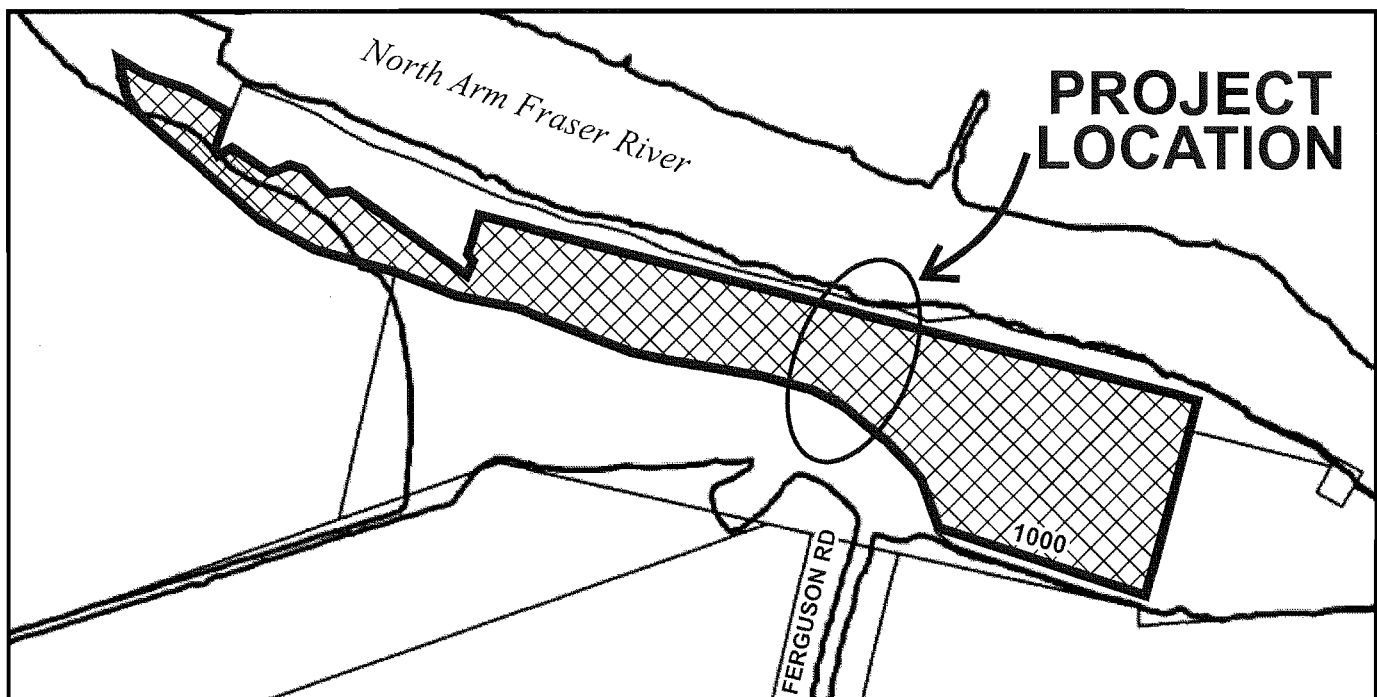
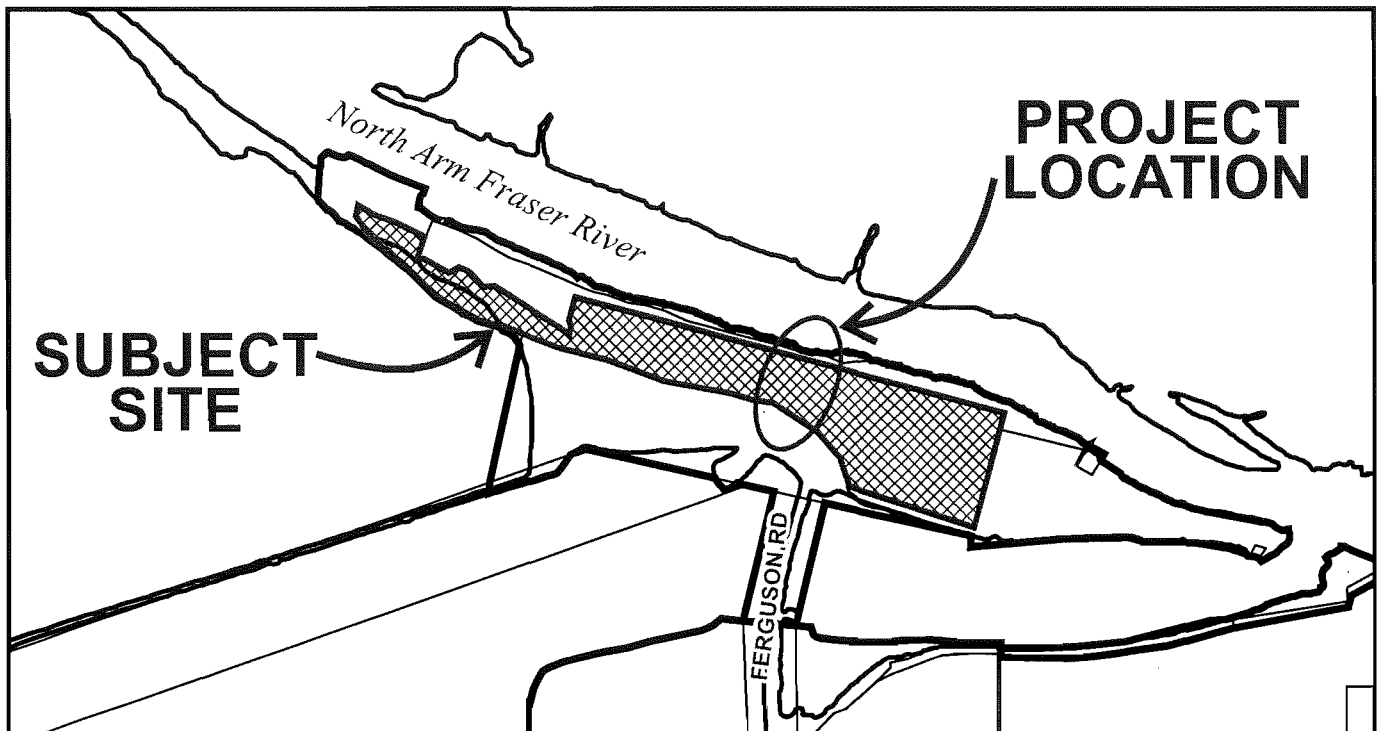
Original Date: 11/19/14

Revision Date

Note: Dimensions are in METRES



City of
Richmond



DP 14-676361

Original Date: 11/18/14

Revision Date: 02/05/15

Note: Dimensions are in METRES



Iona Island Watsewater Treatment Plant			
Map 1 : Solids Handling System Upgrade Wetland and Project Jurisdiction			
Drawn: P. Jost	Checked: J. Prive		
Date: 20/08/2014	Data Sources: See References Section		
EDI Project Number: 14V0537			

Legend

Project Footprint- Approximate

Port Metro Vancouver

Metro Parks Boundary

Wetland

0

25

50

100

150

200


Meters

N

Map scale 1:5,000 (printed at 8.5x11)

Map Projection: NAD 1983 UTM Zone 10N





January 2015

FIGURE 2

VEGETATION SURVEY
AREAS (BAAK AND
LOVERIDGE 2014)

IWWTP SOLIDS
HANDLING SYSTEM
UPGRADE PROJECT

462523

Survey Area A

Survey Area B

Survey Area C

Survey Area D

SCALE: 1:2,500

0

25

50 m

(All Locations Approximate)

terra


A
CH2M HILL
Company

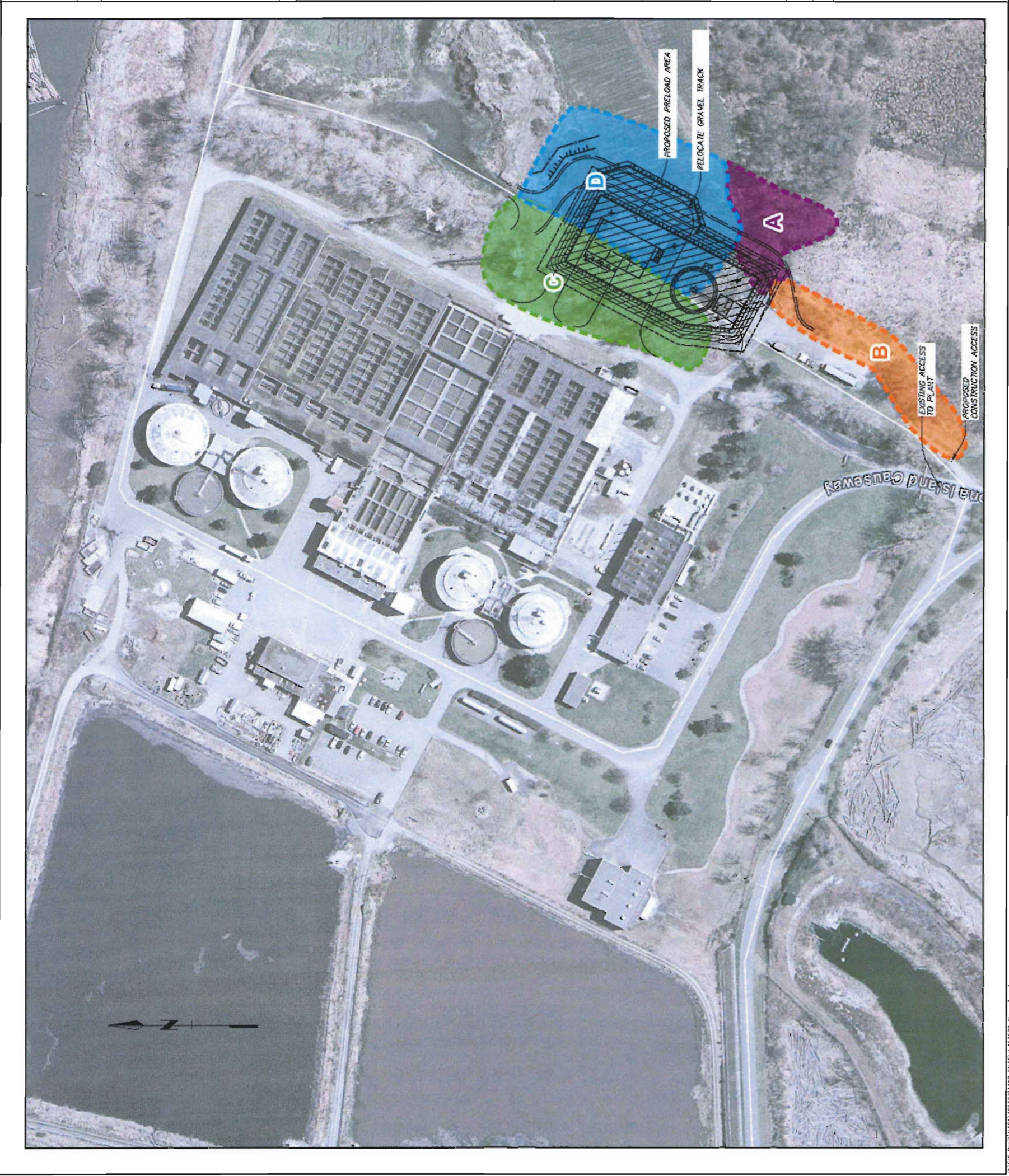
UTM Zone 10N


CAD Drawing and Images: CH2M HILL, Burnaby, Imagery
based on Google Earth 2014, ©2014 Province of British
Columbia.

Although there is no reason to believe that there is any error associated with
the data used to generate this product or in the product itself, users of these
data are advised to exercise caution in the use of the data.

Mapped By: DN Checked By: LA







January 2015

FIGURE 3
PROJECT AREAS THAT
WILL BE CLEARED
AND AREAS TO RECEIVE
COMPENSATION
PLANTINGS

IWWTP SOLIDS
HANDLING SYSTEM
UPGRADE PROJECT

462523

To be Cleared

Compensation Areas

SCALE: 1:5,000

050100 m

(All Locations Approximate)

terra

A
CH2M HILL
Company

UTM Zone 10N

CAD Drawing: CH2M HILL Burnaby 2014; Imagery: Sources:
Esri, DigitalGlobe, GeoEye, i-cubed, USDA, USGS, AEX,
Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS
User Community

Although there is no reason to believe that there are any errors associated with
the data used to generate this product or in the product itself, users of these
data are advised that errors in the data may be present.

Mapped By: DN

Checked By: LA

British
Columbia

Vancouver

Strait of Georgia

USA

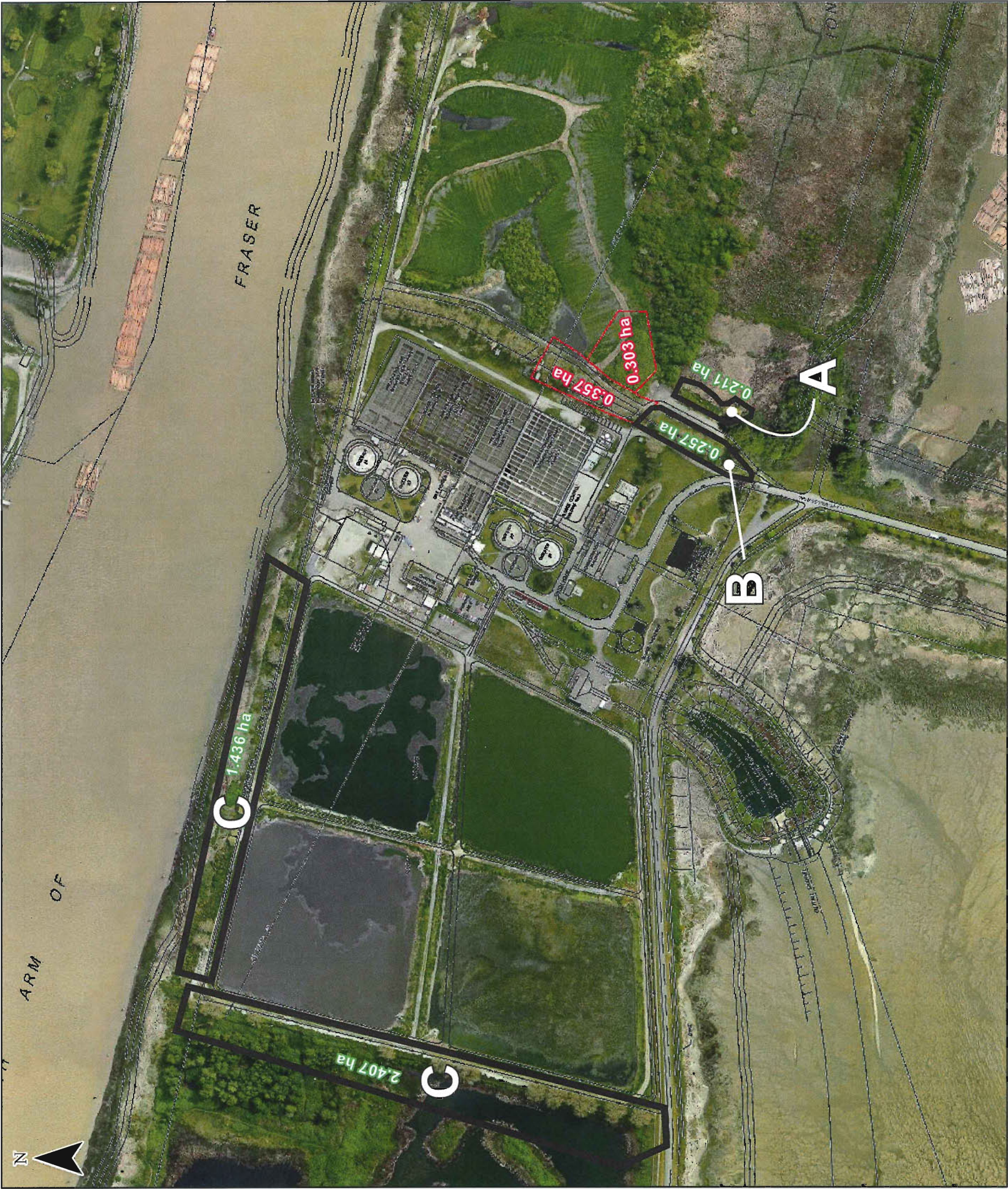


TABLE 2

COMPENSATION PLAN SUMMARY

Compensation Location	Total Planting Area (ha)*	Total Number of Plants for the Location	Tree Species ^	No. of Tree Species ≥ 1.2 m	No. of Tree Species ≥ 30 cm	Shrub Species ^	No. of Shrub Species
A Wetland Berm	0.211	2110	• black cottonwood	30	10	• salmonberry	350
			• red alder	30	10	• thimbleberry	350
			• Pacific crabapple	20	5	• red elderberry	350
			• western redcedar	26	10	• willow	272
			• Sitka spruce	20	5	• hardhack	272
Plant totals				126	40	• Nootka Rose	350
						--	1944
B Western Fenceline	0.04	400	• Sitka spruce	25	20	• salmonberry	70
			• western redcedar	50	25	• thimbleberry	70
						• Red elderberry	70
Plant totals				75	45	• Nootka Rose	70
						--	280
C Sewage Lagoons	0.08	800	• Sitka spruce	25	13	• salmonberry	200
			• Douglas fir	25	15	• thimbleberry	122
			• black hawthorn	125	75	• Nootka rose	200
Plant totals				175	103	--	522
Grand Total				376	188	--	2746

Note: * planting area may differ from the total Location area (Map 2) because of planting patterns.

"A, B, C" correspond to the locations shown on Plan # 2.

FEB 14 2015

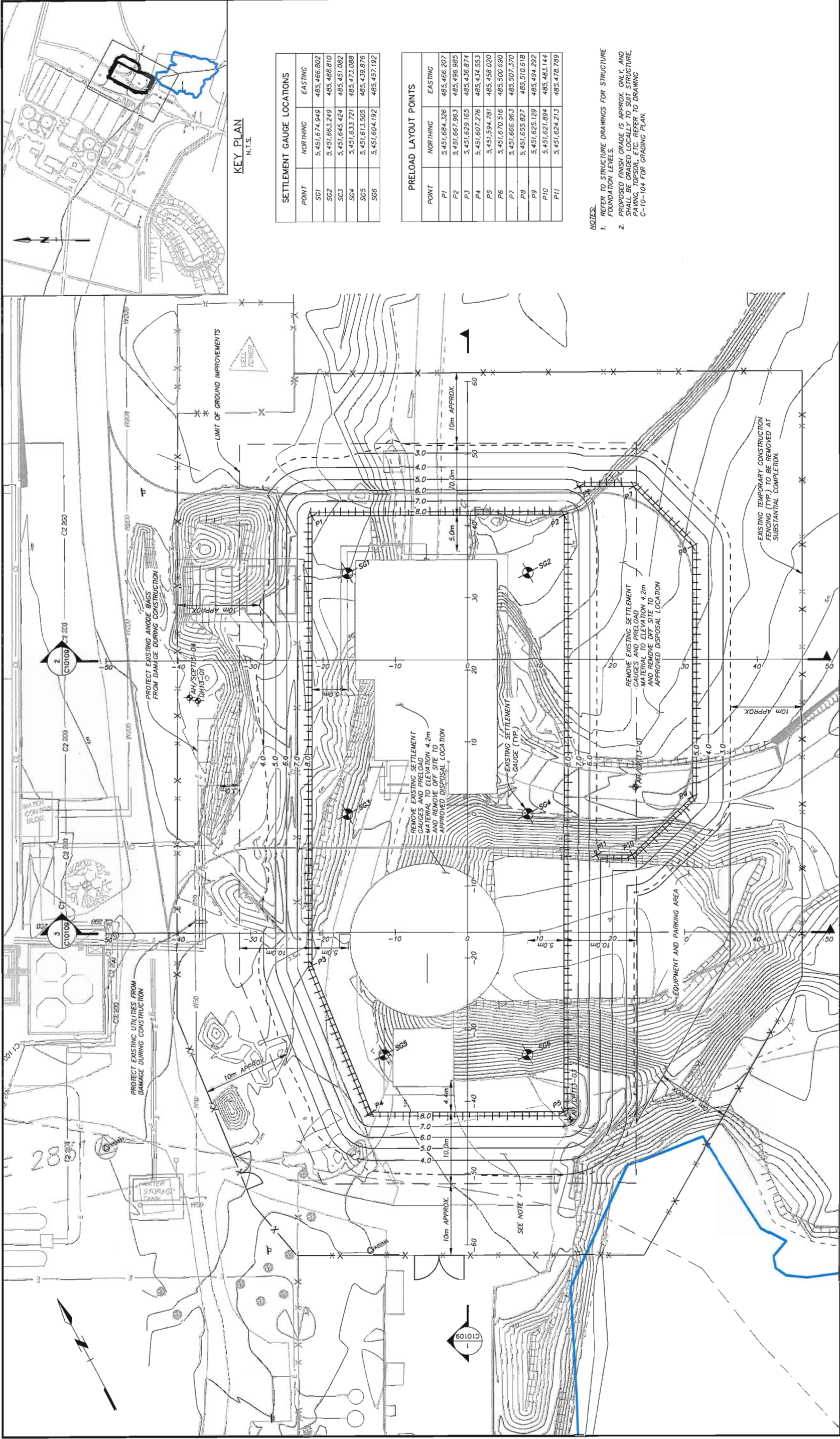
DP 14-676361

Plan #3A

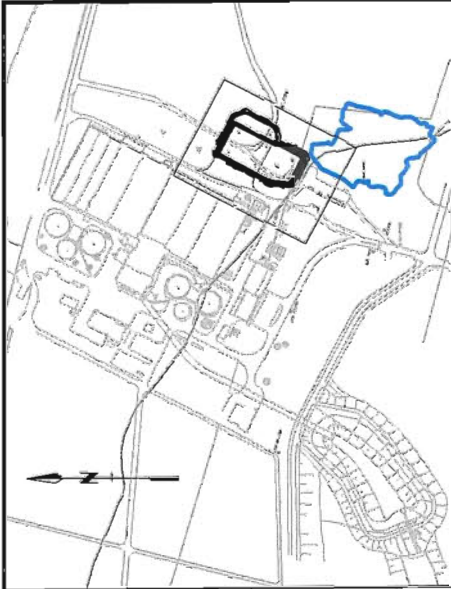
TABLE 3
BALANCE SHEET

Impacts of Expansion Project	Compensation Measures	Plant Species (+ Identifies Berry Producing Species)	Plant Ratios (% of Planted Stock)
Permanent loss of highly disturbed and weedy native vegetation.	<p>Improve the quality of wildlife habitat in three other areas of the site, the wetland berm, western fenceline and sewage lagoons. Methods include:</p> <ul style="list-style-type: none"> Removal of invasive plants on the wetland berm and along the western fenceline. Replanting native shrub and tree species with wildlife value including berry producing shrubs and trees and conifer and broadleaf trees that provide thermal cover and nesting and perching sites. Increase habitat structure. 	<p>Areas will be replanted with native shrubs and trees following the removal of invasive plants. Plant species are listed below.</p> <ul style="list-style-type: none"> thimbleberry+ 15% salmonberry+ 21% red elderberry+ 12% Nootka rose+ 21% western redcedar 1% red alder; 1% black cottonwood; 1% Pacific crabapple 1% Sitka spruce 3% Douglas-fir 1% red alder 1% black cottonwood 1% black hawthorn+ 6% willow 8% hardhack 8% 	
Loss of large black cottonwoods and red alders in the construction site. These trees were cut down earlier and the downed trees remain on site.	Use the large stems to stabilize the wetland berm after invasive plant removal. The stems will provide microsites for shrub and tree planting, habitat for amphibians and small mammals and perch sites for birds.	Black cottonwood and red alder.	The number of stems used in the berm will depend on tree length and diameter. Trees that are ≥ 30 cm breast height diameter (dbh) will be placed in two rows on either side of the berm.
Establishment of invasive plants in disturbed soils	Seed disturbed soils with coastal sod-forming grasses and legumes. Mulch planted areas with wood chips.	NA	NA

Note: Berry producing species account for 77% of the planting stock.



KEY PLAN
N.T.S.



SETTLEMENT GAUGE LOCATIONS		
POINT	NORTHING	EASTING
SG1	5,451,674.949	485,466.802
SG2	5,451,663.249	485,488.810
SG3	5,451,645.424	485,451.082
SG4	5,451,633.721	485,473.088
SG5	5,451,613.505	485,439.876
SG6	5,451,604.192	485,457.192

PRELOAD LAYOUT POINTS		
POINT	NORTHING	EASTING
P1	5,451,684.326	485,466.207
P2	5,451,667.963	485,496.985
P3	5,451,629.165	485,436.874
P4	5,451,607.276	485,434.553
P5	5,451,594.781	485,458.020
P6	5,451,670.516	485,500.690
P7	5,451,666.963	485,507.370
P8	5,451,655.827	485,510.618
P9	5,451,625.129	485,494.292
P10	5,451,621.894	485,483.144
P11	5,451,624.213	485,478.789

- NOTES:
- REFER TO STRUCTURE DRAWINGS FOR STRUCTURE FOUNDATION LEVELS.
 - PROPOSED FINISH GRADE IS APPROX. ONLY, AND SHALL BE GRADED LOCALLY TO SUIT STRUCTURE, PAVING, TOPSOIL, ETC. REFER TO DRAWING C-10-104 FOR GRADING PLAN.

NOTES:
1. EXISTING UTILITY LOCATIONS WERE TAKEN FROM RECORD DRAWINGS AND MAY NOT BE COMPLETELY ACCURATE. NOT ALL STRUCTURES AND UTILITIES ARE NECESSARILY SHOWN.

**CHECK
PRINT**
THIS DRAWING HAS NOT BEEN APPROVED, AND MAY CONTAIN ERRORS AND OMISSIONS.



**Associated
Engineering**
AE PROJECT No. 20122282

CH2MHILL®

GREATER VANCOUVER SEWERAGE AND DRAINAGE DISTRICT			
Design:	PY	Scale:	1:250
Drawn:	LY	District File:	XX-XXXX
Checked:	SFM	Sheet:	XX-XXXX
Approved:		Area:	AREA 10 - GENERAL SITES
Manager:		Preload Removal / Site Preparation Plan:	PRELOAD REMOVAL / SITE PREPARATION PLAN

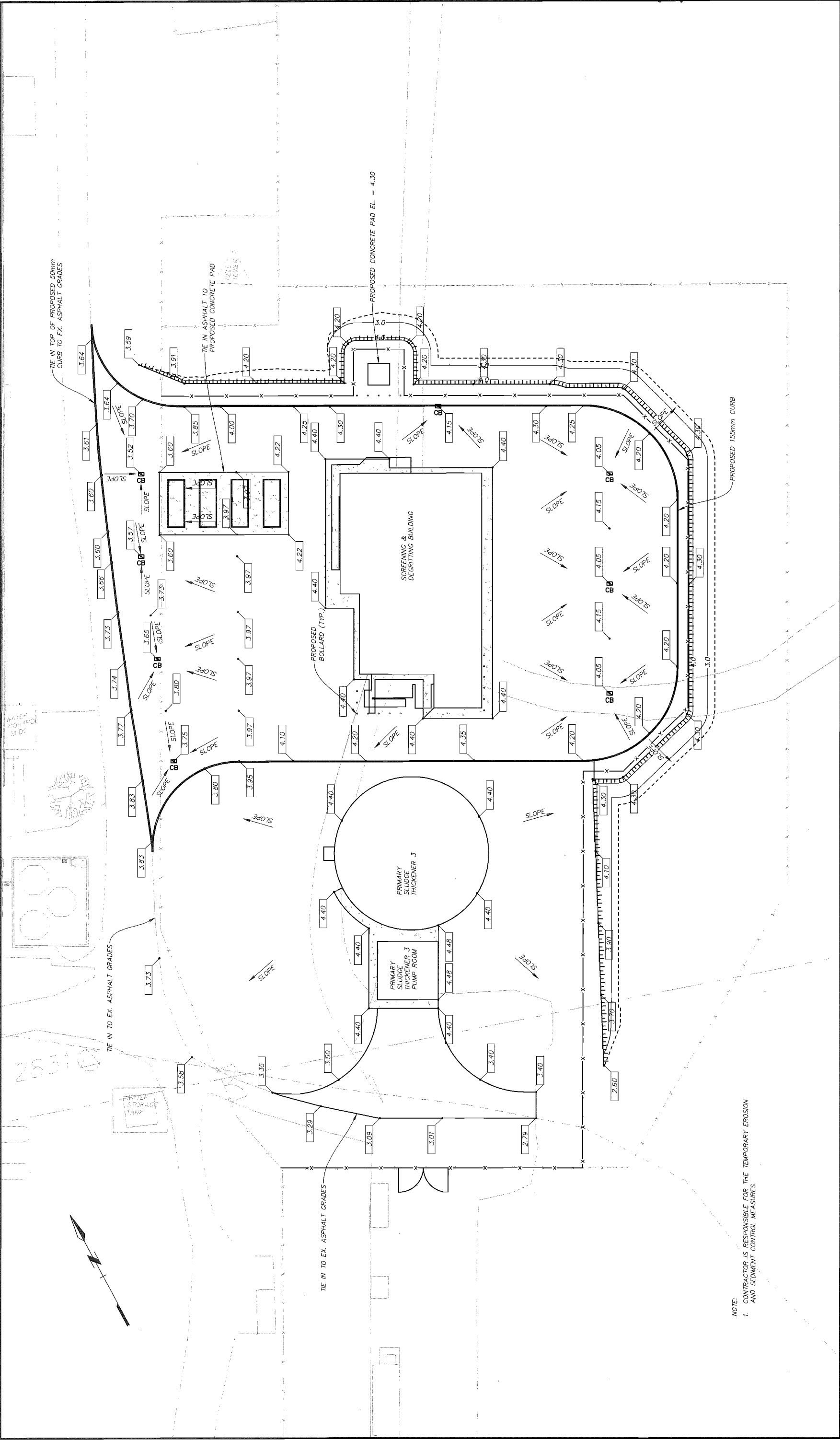
SUPERSEDES PRINTS OF THIS DRAWING NUMBER WITH LETTERS PREVIOUS TO P1

Plan #46

FEB 4 2015

14-676361

DP



NOTE:
1. CONTRACTOR IS RESPONSIBLE FOR THE TEMPORARY EROSION
AND SEDIMENT CONTROL MEASURES.

**CHECK
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BEEN APPROVED, AND MAY
CONTAIN ERRORS AND
OMISSIONS.

Professional Seal

GREATER VANCOUVER SEWERAGE AND DRAINAGE DISTRICT									
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SUPERSEDES PRINTS OF THIS DRAWING NUMBER WITH LETTERS PREVIOUS TO P1									

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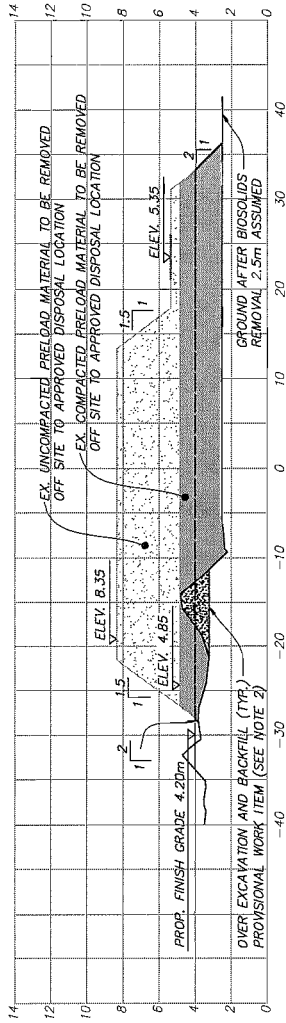


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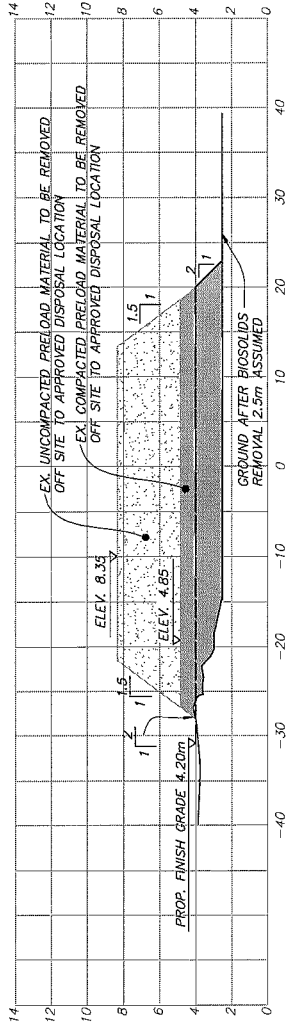
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2014-05-08 C10104

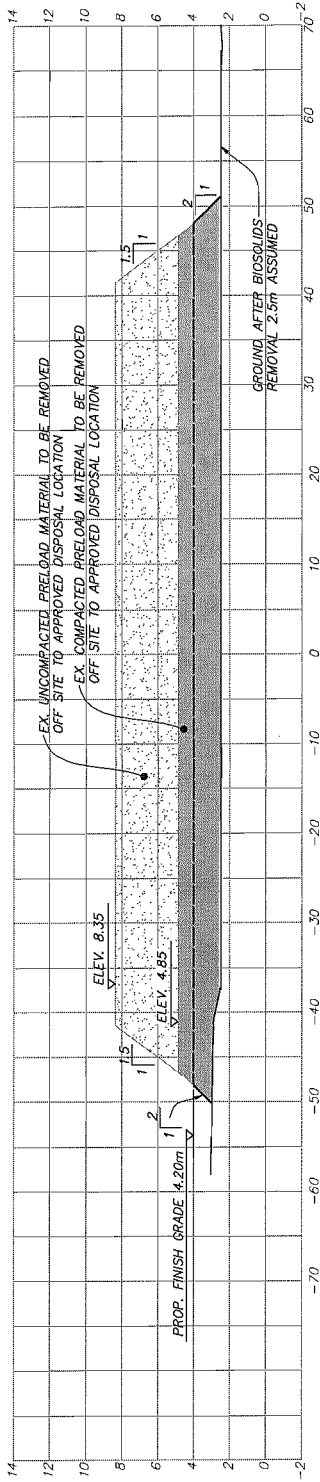
Plan #45



SECTION 2
SCALE: HORIZ. 1:400 VERT. 1:200
C10109

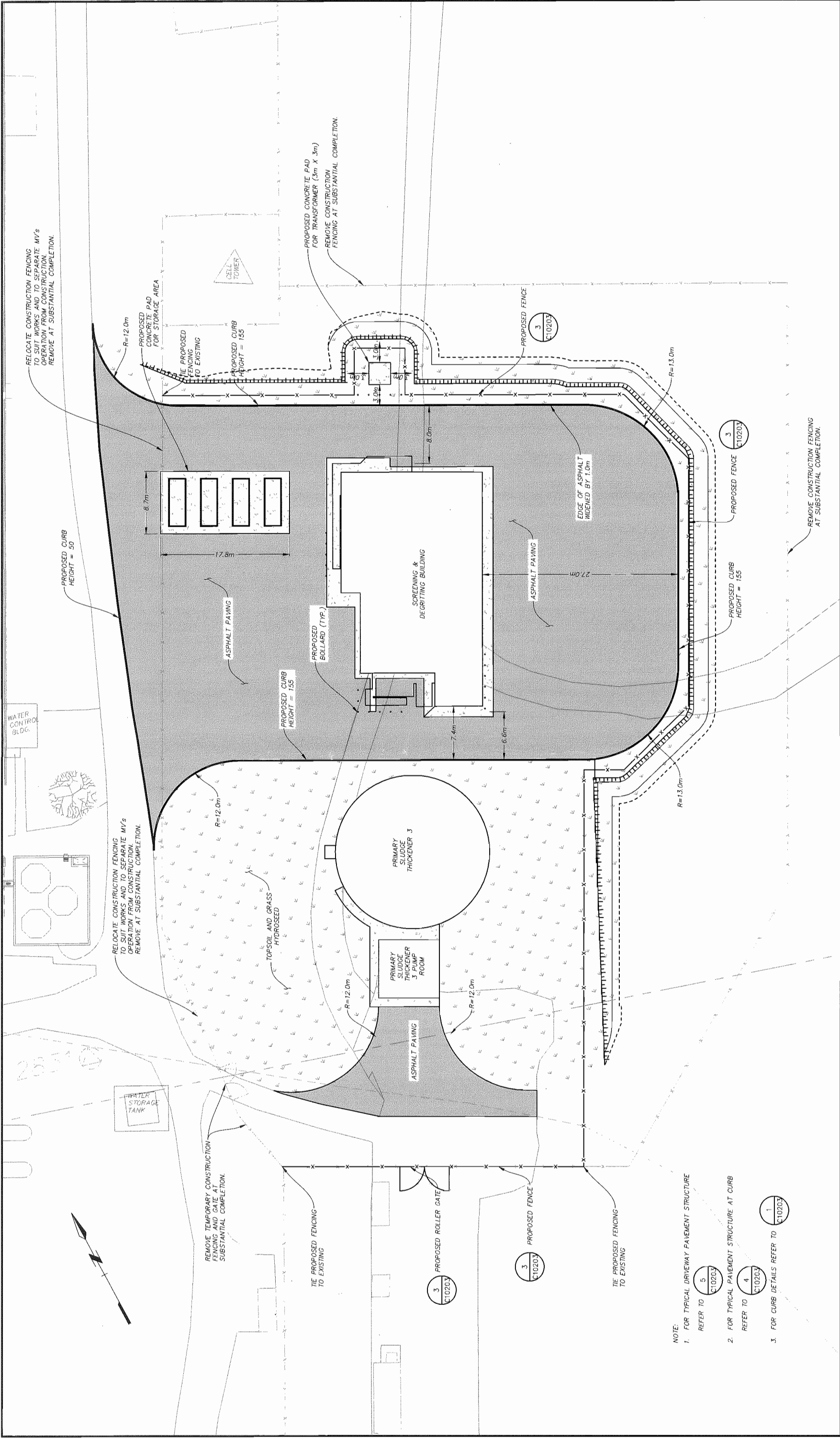


SECTION 3
SCALE: HORIZ. 1:400 VERT. 1:200
C10109



SECTION 1
SCALE: HORIZ. 1:400 VERT. 1:200
C10109

- NOTE:
1. PROPOSED FINISH GRADE IS APPROX. ONLY AND SHALL BE GRADE TO SUIT PROPOSED FINISH TOIL SOIL, ETC. REFER TO DRAWING C-10109 FOR DETAILED DESIGN.
 2. OVER EXCAVATION AND BACKFILL AS DIRECTED BY ENGINEER. INCLUDES:
 - OVER EXCAVATION AND DISPOSAL (APPROVED OFFSITE FACILITY)
 - IMPORTED FILL MATERIAL
 - COMPACTION



- NOTE:
1. FOR TYPICAL DRIVEWAY PAVEMENT STRUCTURE REFER TO 5 C10203
 2. FOR TYPICAL PAVEMENT STRUCTURE AT CURB REFER TO 4 C10203
 3. FOR CURB DETAILS REFER TO 1 C10203

CHECK PRINT

THIS DRAWING HAS NOT BEEN APPROVED, AND MAY CONTAIN ERRORS AND OMISSIONS.

Professional Seal		DATE		DESCRIPTION		APPROVED		MANAGER	
P2		AUG 2014		PY		LY		SFM	
P1		MAY 2013		PY		LY		SFM	
Issue		Date		Desn		Dr'n		Chkd	
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DESIGN: SFM		DRAWN: LY		CHECKED:		APPROVED		MANAGER	
SCALE: 1:250		DISTRICT FILE XX-XXXX		SHT. \SK. #		DRAWING NUMBER			
IONA ISLAND WTP		SOLIDS HANDLING UPGRADE		THICKENER 3		AREA 10 - GENERAL SITES - PART SITE		PLAN - SCREENING AND DEGRIT BUILDING	
GREATER VANCOUVER SEWERAGE AND DRAINAGE DISTRICT		PACKAGE 2 - SCREENING, DEGRIT AND THICKENER 3		AND SLUDGE THICKENER NO. 3					

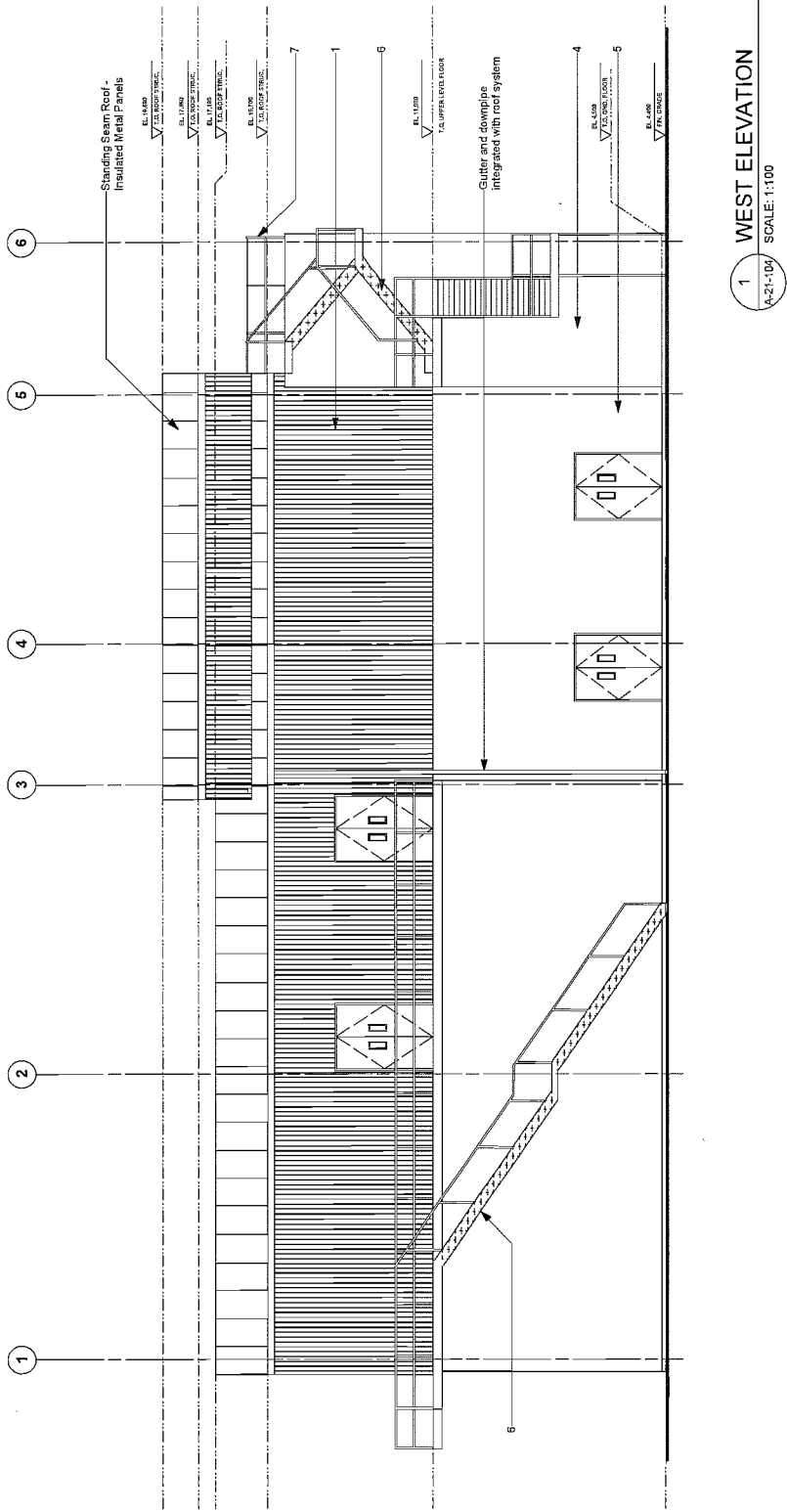
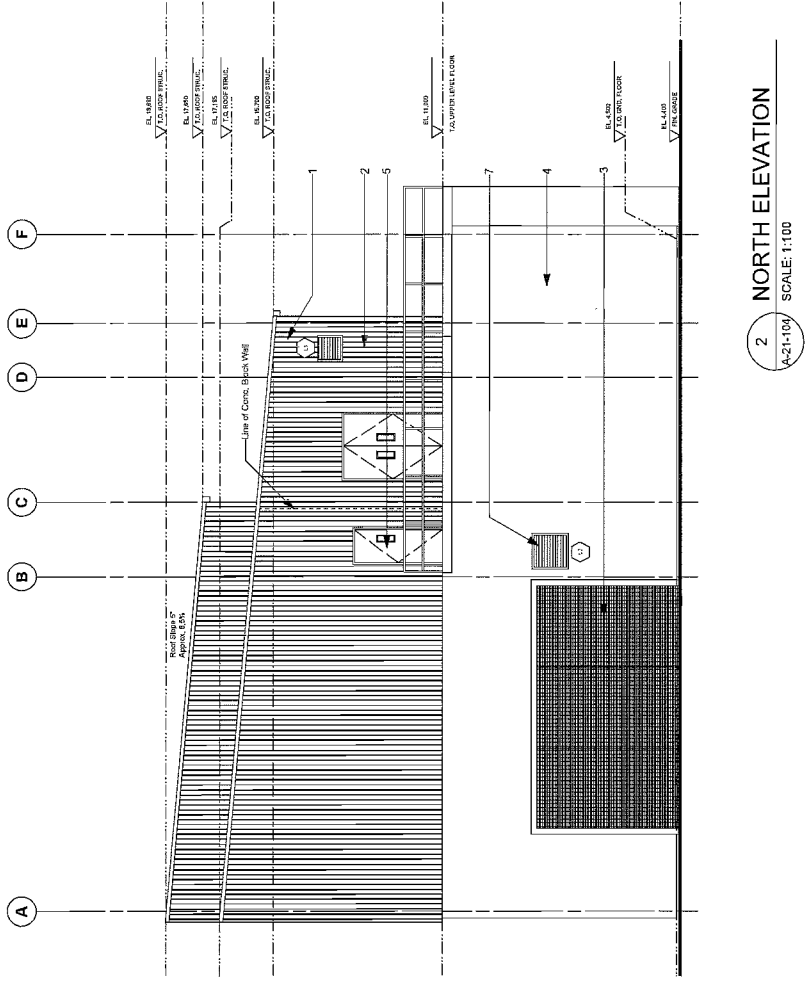


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AE PROJECT No. 2012262

Plan # 44

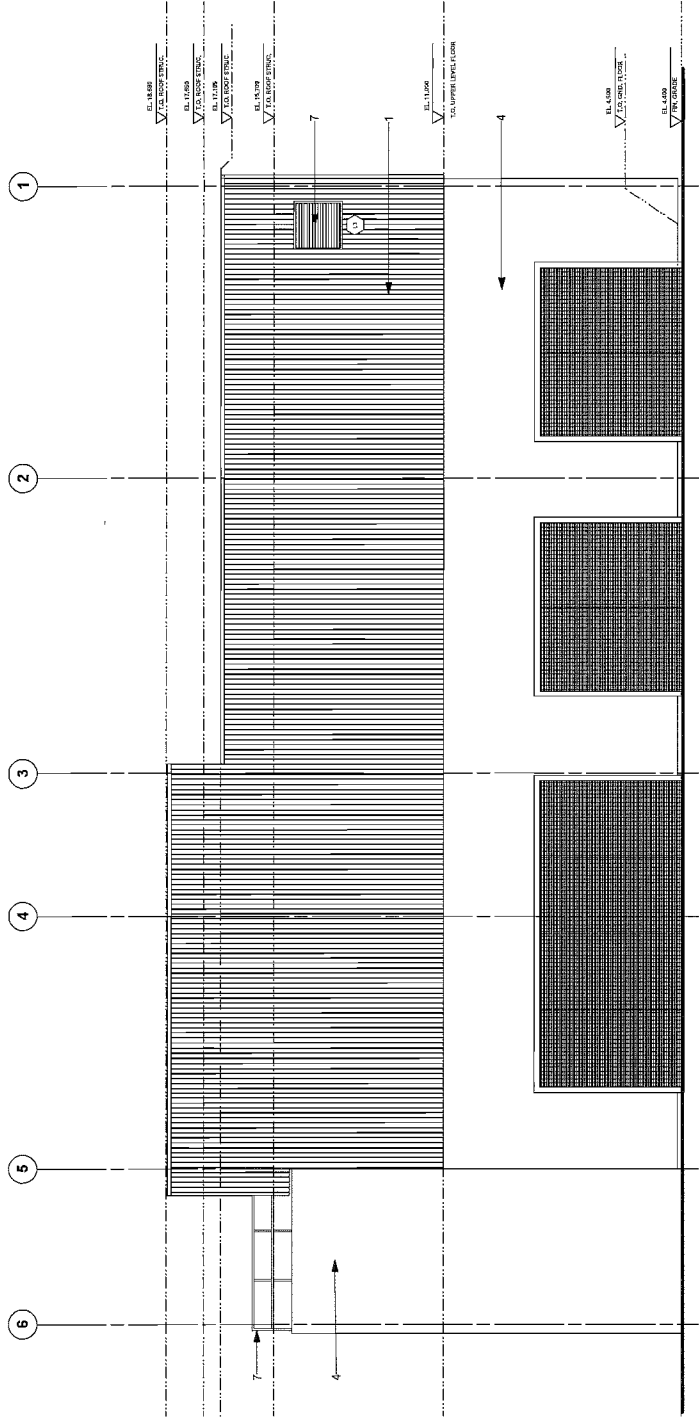
DP 14-676361 FEB 14 2015



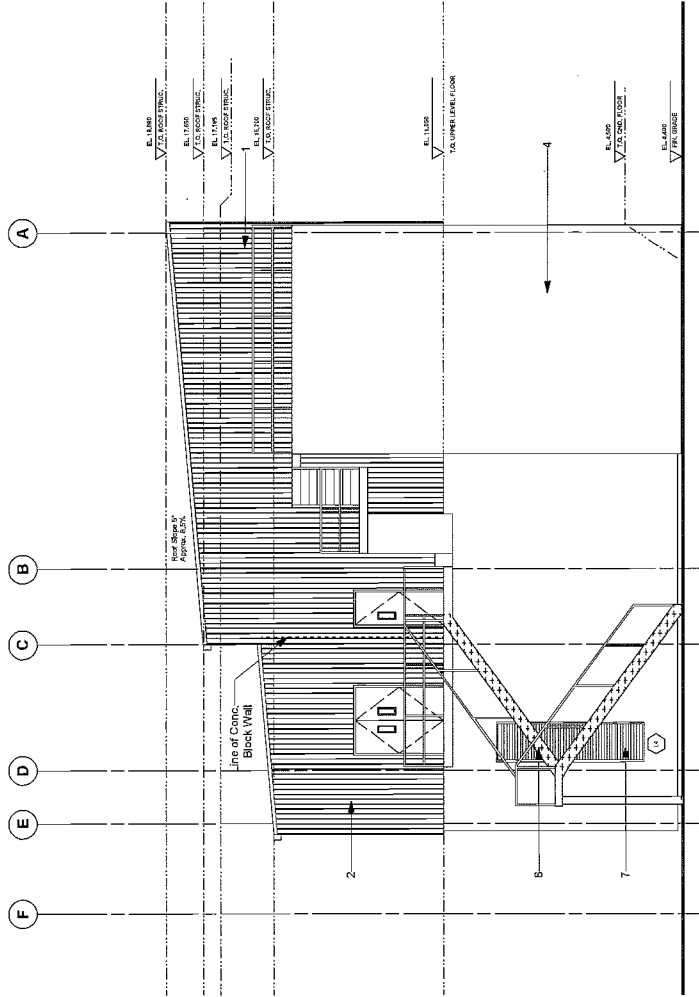
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LEGEND

1. Metal Clad Rainscreen Insulated Metal Panel
2. Metal Clad Rainscreen Conc. Block Wall
3. Galvanized Steel Flat-bar or Expanded Metal Mesh Gate (mesh size / bar spacing to provide bird mitigation)
4. Architectural concrete, medium sand blast c/w sealer/anti-graffiti coating
5. Painted, galvanized steel door
6. Galvanized Steel Flat Bar Stair
7. Prefinished Louvers (see Mech)

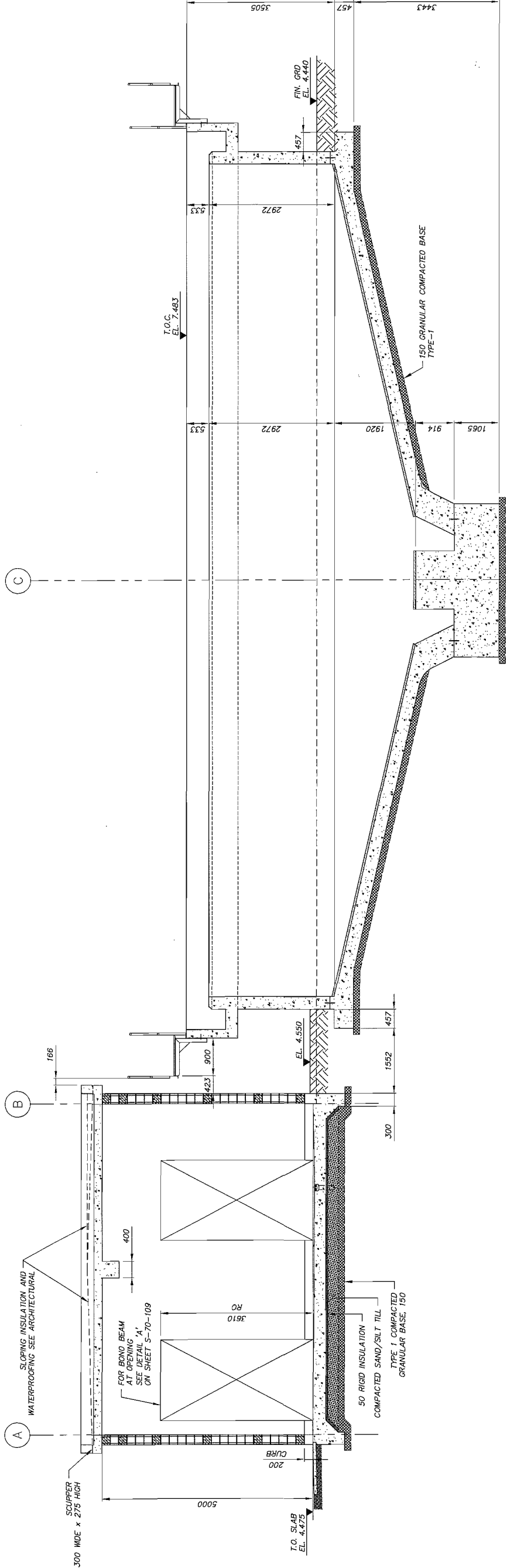


2
A-21-105
WEST ELEVATION
SCALE: 1:100



1 SOUTH ELEVATION
A-21-105 SCALE: 1:100

[illegible]





SECTION 2 2
1:50 S-70-100 S-70-102

NOTES:

1. SPLICES CLASS-B EXCEPT WHERE SPECIFIED DIFFERENT.
2. REFER GENERAL NOTES ON SHEET S-21-100, AND CONTRACT SPECIFICATIONS.

CHECK PRINT
THIS DRAWING HAS NOT BEEN APPROVED, AND MAY CONTAIN ERRORS AND OMISSIONS.



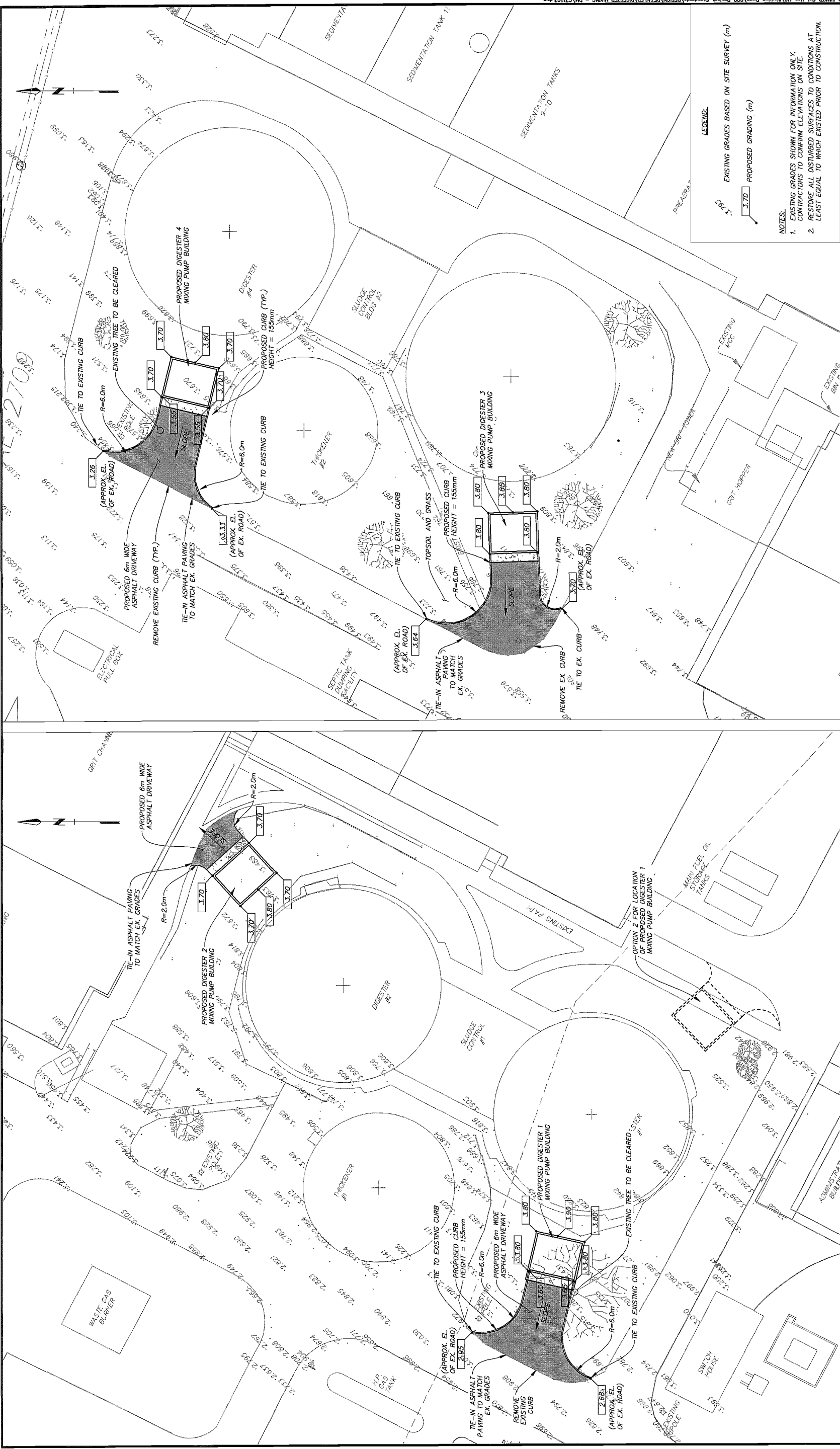
Associated Engineering
AE PROJECT No. 20122262

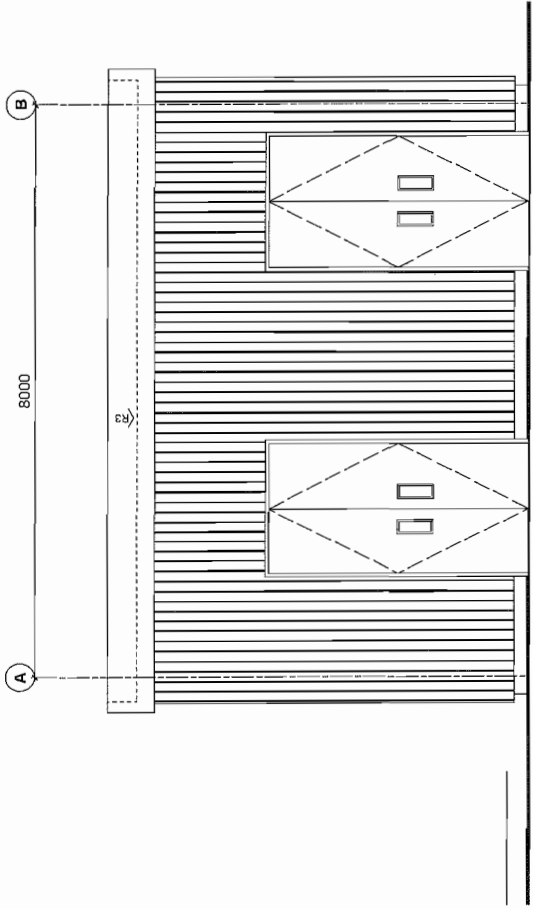
GREATER VANCOUVER SEWERAGE AND DRAINAGE DISTRICT									
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Drawn: HD		PROJECT: IONA ISLAND WWTP SOLIDS HANDLING UPGRADE PACKAGE 2		SCREENING, DEGRITTING AND THICKENER 3		AREA 70		GRAVITY THICKENER 3 AND PUMP ROOM SECTION - SHEET 2	
Checked: MP		LM		ISSUED FOR 90% DETAILED DESIGN		Approved KJM		Manager	
P2		AUG. 2014		GDR		HD		MP	
P1		MAY 2014		GDR		HD		MP	
Issue		Date		Desn		Dr'n		Chkd /App'd	
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Plan #44

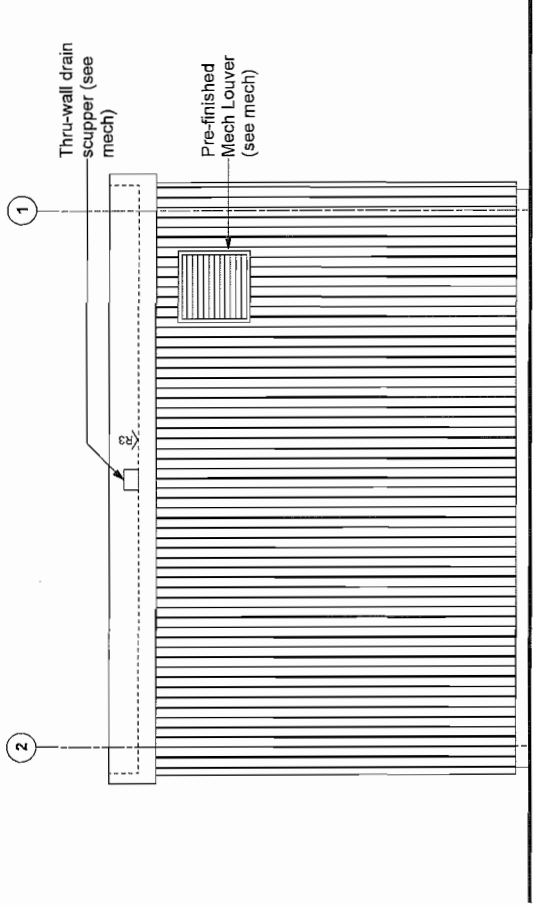
FEB 14 2015

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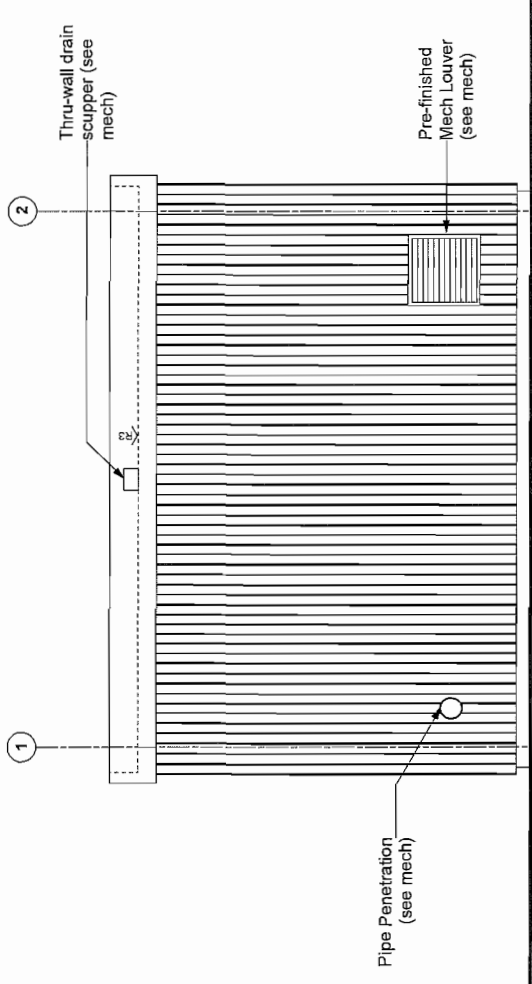




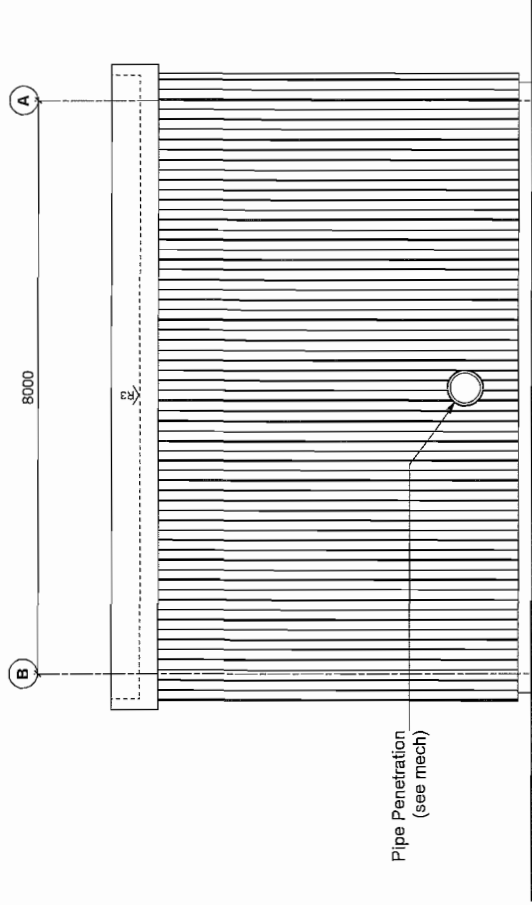
1 FRONT ELEVATION
SCALE: 1:50



2 SIDE ELEVATION
SCALE: 1:50



3 SIDE ELEVATION
SCALE: 1:50



4 BACK ELEVATION
SCALE: 1:50

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PRINT
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BEEN APPROVED AND MAY
CONTAIN ERRORS AND
OMISSIONS.

Professional Seal

GREATER VANCOUVER SEWERAGE AND DRAINAGE DISTRICT									
IONA ISLAND WWTTP									
SOLIDS HANDLING UPGRADE									
PACKAGE 3 - DIGESTER MIXING									
AREA 71 - MIXING PUMP BUILDING									
EXTERIOR ELEVATIONS									
SCALE: AS NOTED									
DISTRICT FILE ###									
SHT./SK. #									
DRAWING NUMBER A-71-101									
SUPERSEDES PRINTS OF THIS DRAWING NUMBER WITH LETTERS PREVIOUS TO P1									

Plan #4K

FEB 14 2015

DP-14-676361



To: Development Permit Panel

Date: January 28, 2014

From: Wayne Craig
Director of Development

File: DP 13-645286

Re: Application by Andrew Cheung Architects Inc. on behalf of 684267 B.C. Ltd for a
Development Permit at 8151 Anderson Road

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of a fifteen (15) storey high-rise building and a six (6) storey mid-rise building to create approximately 111 units, and ground level commercial units at 8151 Anderson Road on a site zoned "Downtown Commercial (CDT1)";
2. Vary the provisions of Richmond Zoning Bylaw 8500 to reduce the residential parking rate from the standard City wide parking rate to the City Centre Zone 1 parking rate.


Wayne Craig
Director of Development

WC:dn

Staff Report

Origin

Andrew Cheung Architects Inc., on behalf of 684267 B.C. Ltd., has applied to the City of Richmond for permission to develop approximately 111 units in a fifteen (15) storey high-rise building and a six (6) storey mid-rise building, as well as ground level commercial units at 8151 Anderson Road on a site zoned "Downtown Commercial (CDT1)". The proposal will also provide eight (8) units that will be secured as affordable housing with a Housing Agreement. The site is currently developed as a low rise commercial building with surface parking and vehicle access to the site via both Anderson Road and Buswell Street.

There is no rezoning associated with the development proposal. The applicant proposes to develop the site in accordance with the site's CDT1 zoning and Urban Core T6 (45m) designation in the City Centre Area Plan (CCAP).

A Servicing Agreement (SA) is required and is discussed in detail in Attachment 6. The scope of the SA includes upgrading the watermain along Anderson Road, extending, upgrading and reconnecting the storm sewer, and either proportionately contributing to a City Capital Project to construct a new gravity sanitary system for the catchment area or undertaking associated works.

Surrounding Development

The subject site is in the Brighthouse Village in an area designated for high-density, high-rise commercial and mixed-use development under the City Centre Area Plan (CCAP) (Attachment 2). Development surrounding the subject site is as follows:

To the north, a mixed multiple-family residential and commercial high-rise ("Nova") that includes retail along Park Road, and four (4) two-level townhouse units fronting Buswell Street with a layout that supports live/work uses. The site is zoned "Downtown Commercial (CDT1)" and designated Urban Core T6 in the CCAP (Brighthouse Village);

To the east, Buswell Street and an existing low-rise multi-family rental complex that consists of four (4) buildings ("Dolphin Square"). The site is zoned "Land Use Contract (LUC) 73" and designated Urban Centre T5 and Park in the CCAP (Brighthouse Village);

To the south, Anderson Road and an existing Petro Canada gas station. The site is zoned "Gas & Service Station Commercial (ZC18) Brighthouse Village (City Centre)" and designated Urban Core T6 in the CCAP (Brighthouse Village); and

To the west, a high-rise residential tower with commercial use along the Anderson Road frontage ("Anderson Building"). The site is zoned "Downtown Commercial (CDT1)" and designated Urban Core T6 in the CCAP (Brighthouse Village).

Development Information

The proposed development site is located in the Brighthouse Village area of the City Centre (Attachment 2). It abuts Anderson Road to the south, Buswell Street to the east, a city lane to the north and private property to the west. The site is currently developed with a low rise

commercial plaza and associated surface parking with access from both Anderson Road and Buswell Street.

The proposed building integrates a three-storey podium with ground-level commercial space and two levels of screened parking; a three-storey residential volume that intersects the podium and runs parallel to Buswell Street; and a two to twelve-storey stepped tower volume that also intersects the podium but runs perpendicular to Buswell Street.

Pedestrian access is provided to the commercial spaces from both street frontages and to the residential lobby from Buswell Street. Access to the parking, loading and garbage/recycling facilities is provided from the lane. Common outdoor amenity space for the residents is provided at the fourth level, on the southwest portion of the podium adjacent to Anderson Road. Common indoor amenity space is also provided on the fourth level, with direct access to the outdoor amenity space.

Please refer to the attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Related Policies and Bylaws

The applicant is required to comply with City bylaws and policies, including the following. With the exception of the variance noted below, the application complies with the Richmond Zoning Bylaw. In applicable cases, the terms are articulated in the Development Permit Considerations (Attachment 6):

- OCP
- CCAP
- OCP Sustainability Policy
- OCP Crime Prevention Through Environmental Design (CPTED)
- OCP Accessibility Policy
- Flood Plain Designation and Protection (Bylaw 8204)
- OCP Aircraft Noise Sensitive Development (ANSO) Policy
- Affordable Housing Strategy
- Public Art Program (Policy 8703)

OCP/CCAP

The site is designated “Downtown Mixed Use” in the OCP, which supports the proposed mixed residential and commercial use. The site is located within the Brighthouse Village Plan in an area designated for high-density, high-rise commercial and mixed-use development, which supports the use and density proposed by the subject application.

OCP Sustainability

- Through the process of design review, the applicant has proposed a site specific geo-exchange system that is supported by Engineering staff. Although other recent development in the City Centre have committed to connect to an existing or a future City Centre District Energy Utility (DEU), the proposed site specific system is supported based on the expected long time line associated with introduction of a City DEU to service this location within the City Centre.

- A ground source heat pump system is proposed, which would provide both heating and cooling that may be operated simultaneously for both the commercial and residential components of the building. Details of the proposed geo-exchange system are attached (Attachment 3).
- In accordance with the CCAP, the proposed development will be constructed to achieve LEED Silver equivalency. The applicant has provided a list of sustainability features that may be incorporated to ensure the development proposal achieves LEED Silver equivalency (Attachment 4). The list includes, but is not limited to the following:
 - On-site geothermal heat exchange facility;
 - Green roofs;
 - Minimum 20% locally sourced materials;
 - Minimum 10% recycled materials;
 - Low VOC emitting materials and low flow toilets, showers and lavatories;
 - Low-E coating of all exterior glazing;
 - Extension of slabs beyond the plane of glazing by 0.5 m (20") on all sides of the proposed residential tower to minimize solar heat gain on the west and south elevations;
 - Metal panels and 51% perforated metal screens on the south and east façade of the parking podium facilitate natural light and ventilation into the parking garage;
 - LED luminaires with occupancy sensors within the parkade, and photocells to harvest daylight penetrating the perforated metal screen.; and
 - Electric charging plug-ins within enclosed bike rooms and for 20% of required parking stalls in accordance with the OCP.

OCP Accessible Housing

- The proposed development includes forty seven (47) single storey basic universal housing units that are designed to be easily renovated to accommodate a future resident in a wheelchair. Provided the units incorporate all of the accessibility provisions listed in the Basic Universal Housing Features section of the City's Zoning Bylaw, a density exclusion of 1.86 m² (20 ft²) per unit is permitted and has been utilized in the project.
- All of the proposed units incorporate aging in place features to accommodate mobility constraints associated with aging. These features include:
 - stairwell hand rails;
 - lever-type handles for plumbing fixtures and door handles; and
 - solid blocking in washroom walls to facilitate future grab bar installation beside toilets, bathtubs and showers.
- The proposed development would provide barrier-free access to the commercial units, the residential lobby, and to the required indoor and outdoor amenity space.

OCP Crime Prevention Through Environmental Design (CPTED)

The development proposal incorporates a range of CPTED principles that include but are not limited to the following (Attachment 4):

- The parking structure and the residential lobby are designed to minimize alcoves and hidden corners, and are well illuminated. The interior of the parking structure would be painted white and elevator lobbies and vestibules include glazing in accordance with Building Code requirements;

- Visitor and commercial parking is separated from residential parking and secured with overhead gates;
- Commercial units are accessed both directly from the sidewalk and from a rear access service corridor to provide direct access to the parking and loading area; and
- The proposed site plan and the use of transparent building material at the ground level and between the indoor amenity and the outdoor amenity areas introduce opportunities for passive surveillance.

Flood Management Implementation Strategy

The development proposal is required to comply with the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. In accordance with the Flood Management Strategy, a Flood Indemnity Restrictive Covenant is required prior to issuance of the Development Permit.

OCP Aircraft Noise Sensitive Development (ANSND) Policy

The subject site is located within an area that permits consideration of all aircraft noise sensitive land use types. However, as the site is affected by OCP Airport Noise Contours, the development is required to register a covenant prior to Development Permit issuance.

Affordable Housing

- The applicant has applied to take advantage of a provision in the CDT1 zone to increase permitted density from 3.0 FAR to 3.15 FAR by providing a minimum of 5% of the proposed residential floor area as affordable housing units. To secure affordable housing units within the proposed development, the applicant is required to enter into a Housing Agreement prior to issuance of the Development Permit. In order to enter into a Housing Agreement, the Local Government Act, Section 905, requires enactment of a bylaw by the City. A report will be drafted by the Affordable Housing Coordinator, with a bylaw and associated Housing Agreement attached.
- Out of the proposed 111 units, eight (8) units will be secured as affordable housing. The units will be located on the north side of the east-west oriented mid-rise building. The following units have been identified as affordable housing units and details are provided in Attachment 6:
 - Two (2) one bedroom units and one (1) one bedroom and den unit on both the fourth and fifth storey of the building; and
 - One (1) one bedroom unit and one (1) one bedroom and den unit on the sixth storey of the building.
- Typically, a mix of one and two bedroom units are secured for affordable housing within a development. However, in this case, Community Services staff support the typology, size and location of units proposed to be secured as affordable housing units based on consideration of:
 - The size of two bedroom units within the development, many of which are less than the minimum 80 m² (860 ft²) required by the Affordable Housing Strategy;
 - The size of the one bedroom units proposed to be secured as affordable housing units, which all exceed the minimum 50 m² (535 ft²) requirement; and
 - The provision of one bedroom and den units that will be rented at the rate applied to a one bedroom unit.

Public Art

- As a condition of development permit issuance, the applicant would voluntarily contribute approximately \$87,462.43 (\$0.77/ft² per residential buildable square foot, excluding affordable housing, and \$0.44/ft² per commercial buildable square foot) towards the City's public art reserve fund. The contribution would be allocated to administration and on-site installation of public art.
- The applicant has submitted a public art plan, which was considered and supported by the Public Art Committee on November 18, 2014. The parking podium façade, which is proposed to be covered in solid and perforated metal panels arranged in a barcode arrangement, has been identified as an opportunity for the integration of a variety of possible art mediums. The parking façade treatment stands on its own as a design element but is expected to be further enhanced with the addition of public art. The process of incorporating art into the parking podium façade will be undertaken in accordance with the Public Art Program (Policy 8703).

Zoning Compliance/VariANCES (staff comments in **bold**)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Reduce the residential parking rate from the standard City wide parking rate to the City Centre Zone 1 parking rate.

(Staff support the proposed variance based on consideration of the subject site's location within the City Centre Zone 1 parking area and proximity to the Canada Line. Consistent with similar past approvals, Transportation staff support the variance subject to the applicant agreeing to:

1. *Comply with the Affordable Housing Strategy and secure a minimum of five percent (5%) of the total residential floor area as affordable housing units; and*
2. *Provision of transportation improvements to the satisfaction of Transportation staff that contribute toward a transit-oriented environment. Provisions include:*
 - *\$50,000 voluntary contribution to the future upgrading of the Park Road and Buswell Street intersection to include full traffic signalization;*
 - *Upgrading the crosswalks at the Anderson Road and Buswell Street intersection with decorative stamped asphalt treatment to improve pedestrian visibility and safety;*
 - *Continuous weather protection along the street wall; and*
 - *Installation of a minimum of four (4) benches along the site's frontages as shown in the attached landscape plans.*

Urban Design Response

The proposed design concept is characterized as "intersecting building volumes" by the applicant and satisfactorily addresses the significant urban design issues identified during the Development Permit application review process. The proposal addresses the urban design objectives of the OCP and CCAP Development Permit Guidelines and the CCAP High Rise Commercial & Mixed Use Sub Area Guidelines.

Advisory Design Panel Comments

The current proposal incorporates changes that reflect the proposal's consideration by the Advisory Design Panel (ADP) on October 8, 2014. Members of the ADP appreciated many elements of the proposed design and provided suggestions for further design development, in particular, simplification of the overall design, resolution of the intersecting building volumes, and design development of the parking façade treatment. A copy of the relevant excerpt from the ADP Minutes from October 8, 2014 is attached for reference (Attachment 5). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '***bold italics***'.

Conditions of Adjacency

Anderson Road and Buswell Street (south and east)

- The site is bordered by Anderson Road and Buswell Street. Future development of the remainder of the block is anticipated to introduce similar high-rise building forms, which is supported by the CCAP. Across Buswell Street, on the north portion of the block, mid-rise (25 m) high development is anticipated in accordance with the area's designation in the Brighthouse Village plan. The southern portion of the site is designated as park (Attachment 2).
- The proposed development is not expected to significantly affect existing and anticipated development across Anderson Road. The subject site is located on the north side of the street and there would be negligible shadow impacts on properties to the south. The separation provided by the street and building placement minimizes impacts on outlook and privacy.
- The proposed development would not significantly impact the existing development across Buswell Street. Shadowing, although increased, would only occur on parts of the site during the later portions of the day.
- The separation provided by the street would limit resulting outlook and privacy impacts to existing residents on the east side of Buswell Street.
- Significant impacts on the future park across Buswell Street are not anticipated. The tower massing is proposed to be located toward the north side of the subject site, which would preserve daytime sun penetration to the park.
- The proposed landscaped open space on the fourth level of the development and the proposed green roofs on the mid-rise volumes would introduce an attractive outlook for surrounding existing and anticipated future development.

Lane (north)

- The site is bordered by an existing City lane to the north, which will be widened in accordance with City Centre lane standards and provide access to all properties on the block. As part of this application, the portion of the lane adjacent to the subject site would be widened and would introduce a sidewalk and overhead lighting.
- The proposed development would have limited impacts on the existing development across the lane ("Nova"). Like the proposed development, the Nova development has a low-rise parking podium with south facing common open space above. Some shadowing of this open space will occur during mid-day at the equinoxes but sun would penetrate into this area until the mid-morning and after the mid-afternoon. The common open space area would not be affected by shadows in the summer.

- The tower component of the existing northern development (“Nova”) is situated toward the northeast corner of the site and is separated from the proposed tower and most of the midrise volumes on the subject site by 38.0 m (125.0 ft.). This exceeds the tower separation suggested by the CCAP.

Western Adjacency (west)

- To the west, the site is bordered by an existing “stepped podium and tower” development (“Anderson Building”).
- The tower component of the proposed development is separated from the existing Anderson Building tower by 24.0 m (78.75 ft.). This is consistent with the expectations of the CCAP. Further, the western elevation of the proposed parking podium will not be visible to residents of the Anderson Building (see attached Reference plan).
- The proposed development also addresses adjacency impacts by:
 - Proposing a parking podium that is lower than that of the western adjacent Anderson Building;
 - Increasing the building setback at the fifth level of the building to reduce overlook impacts;
 - Orienting most units toward the north and south to reduce overlook issues; and
 - Enhancing the outlook for residents of the Anderson Building by proposing to introduce a landscaped open space over the parking podium and green roof treatments of the exposed mid-rise rooftops.

Streetscapes and Lane

Anderson Road and Buswell Street

- The CCAP designates both Anderson Road and Buswell Street pedestrian-oriented retail streets.
- Improvements to the public realm will be undertaken through a series of right of ways, which are located along Buswell Street, the lane, Anderson Road and corners abutting road/lane as discussed in detail in Attachment 6.
- The proposed development contributes to these streets by:
 - Defining the edges with a three to six-storey streetwall;
 - Providing continuous, highly-transparent, commercial space at the ground level to both physically and visually animate the public realm;
 - Incorporating design elements that encourage pedestrian activity including continuous weather protection, street furniture, special paving patterns and landscaping; and
 - Incorporating public art into the parking podium façade. The parking façade is proposed to be covered in a combination of solid and perforated metal panels arranged in a bar code pattern that will create different effects during the day and night.

Lane

- Required lane improvements would include discharging the existing right of way and replacing it with an updated utilities and public passage right of way (284728C, 285749C, 265760C, SRW Plan 79051). The right of way area will be dedicated to the City after Provisional Occupancy and/or Final Occupancy and prior to registration of Strata Plans. Transfer of land to the City will be secured by way of an option to dedicate/purchase for a nominal amount (Attachment 6).

- To support connectivity between the street fronting public realm and the lane, as well as to add spatial depth to the streetscape experience, the podium façade, including the glass storefront windows, the parking level metal panels, and the overhead canopy are proposed to wrap the corner from Buswell Street into the lane.
- An existing covenant (AD233647), which permits an awning to encroach above the sidewalk in front of a building would be discharged as a condition of DP issuance.
- Parking, loading, and garbage/recycling is enclosed within the parking podium and screened. Although the western portion of the parking podium façade is relatively unarticulated, the treatment is similar to the Nova development on the north side of the lane and the introduction of a sidewalk and overhead lighting will improve safety and pedestrian amenity.

Site and Functional Planning

Building Features

- The site and functional planning is consistent with many developments within the City Centre characterized by:
 - Mixed-use development including commercial spaces along the street frontages;
 - Parking, loading and waste management that is accessed from the lane;
 - Multi-level parking enclosed within a building podium; and
 - Residential uses above the podium level.
- The development proposal includes commercial use and the residential lobby at ground level.
- The residential levels propose a double-loaded corridor layout. This maximizes both outlook and light access for the units. The interior corner unit on level five and level six has somewhat reduced outlook but achieves good southwest light access.
- Common outdoor amenity space of 819 m² (8,815 ft²), which exceeds the OCP guideline recommendation, is provided on the south side of the development at the podium level to maximize sun access. An outdoor kitchen with barbeque and sink, seating benches, a fire place, and a designated children's play area are features of the outdoor amenity space.
- The adjacent common indoor amenity space is south facing and sited to provide a good visual and physical relationship with the outdoor amenity space. It is also centrally located within the building and within immediate proximity of the elevator core.
- The indoor amenity space, consisting of 180 m² (1,937 ft²), which exceeds the OCP guideline recommendation, includes a gym and a multi-purpose room that includes a kitchen.
- All units are provided with private outdoor space. On the podium level, the private outdoor space is screened from the common area with planters and soft landscape material.
- Private outdoor spaces range in size from 5.7 m² (61 ft²) to greater than 9 m² (96 ft²), which maximize opportunity for active use of balconies. Some units located on the sixth, seventh and ninth storey propose larger private outdoor spaces that are between 20 m² (215 ft²) and 85 m² (915 ft²) and include private hose bibs to encourage personal landscaping/gardening activities.
- The proposed functional planning and massing result in building volumes that create significant variation in building heights and setbacks. In addition to minimizing the impacts on development on nearby properties and maximizing the livability, views and sun penetration for the subject development, this arrangement contributes to a more dynamic skyline than a simple podium and tower development.

Parking & Loading

- The site is located within the City Centre and benefits from being near transportation options that are available to future residents, including access to the Canada Line. As noted earlier, Transportation staff support the proposed variance to allow application of City Centre Zone 1 parking rates at this site, provided that affordable housing units are secured on-site in accordance with the Affordable Housing Strategy and other transportation improvements that support a transit-oriented environment are secured. These include:
 - \$50,000 voluntary contribution to the future upgrading of the Park Road and Buswell Street intersection to include full traffic signalization;
 - Upgrading the crosswalks at the Anderson Road and Buswell Street intersection with decorative stamped asphalt treatment to improve pedestrian visibility and safety;
 - Continuous weather protection along the street wall; and
 - Installation of a minimum of four (4) benches along the site's frontages as shown in the attached landscape plans.
- Resident and commercial/visitor parking is enclosed within a parkade that is accessed via the existing lane. A total of 114 residential stalls and 47 shared visitor/commercial off-street parking stalls are proposed on-site, which exceeds the City Centre Zone 1 parking rate requirement.
- Transportation staff support shared visitor and commercial parking conditional to the terms outlined in Attachment 6.
- The applicant has demonstrated to the satisfaction of Transportation staff that loading vehicles and waste collection vehicles can be accommodated on-site via the existing lane.
- Garbage and recycling is enclosed within the parkade and would be accessed from the loading spaces.
- Both long term and short term bicycle parking is provided on-site and either meets or exceeds the Zoning Bylaw requirement.
- The proposed development provides electric vehicle charging infrastructure in accordance with the OCP (Attachment 1).

Architectural Form and Character

Building Articulation

The proposed development employs a variety of architectural strategies to minimize the building mass, add variety to the building form and character, and achieve a pedestrian scale along the street frontages.

- The proposed “intersecting” building volumes break the overall mass into smaller volumes, creating a less monolithic expression. The various building and façade setbacks also create upper level spatial interest. The applicant has chosen to express the individual building volumes by using different materials, detailing and colors to further differentiate the volumes.
- The proposed ground level consists of glass storefront windows with minimal gray spandrel panels along the street frontages. This treatment extends to the second and third levels around the two-storey residential lobby entry on Buswell Street. The parking podium is clad in a combination of solid metal anodized finish panels and perforated charcoal painted metal screens. The panels are arranged in a barcode pattern and this textured treatment extends along both road frontages and wraps around the Buswell Street/lane corner.

- The proposed north-south oriented mid-rise volume is characterized by painted charcoal concrete walls with reveals, punched windows and balconies, and gray spandrel panels.
- The proposed east-west oriented mid-rise and tower volumes are lighter in colour than the north-south mid-rise volume and are characterized by the generous use of windows with accent blue spandrel panels.
- The tower portion of the proposed development incorporates the strategic use of architectural frames to establish a façade hierarchy and highlight the verticality of the tower on the four elevations. This is particularly effective at breaking up the apparent building mass on the longer north and south elevations.
- The terminus of the tower responds to the CCAP's objective to diversify the skyline. On the Buswell elevation, the window wall treatment at the centre of the tower extends to the maximum permitted building height of 47 m (147 ft.) geodetic and is expressed with a two-storey penthouse element that stands proud of the remainder of the façade. This element also extends west to fully screen rooftop mechanical equipment. On the north, south and west elevations, the aforementioned frames also stand proud of the main building and roofs. Between these, various roof and balcony elements are stepped in plan and elevation.

Landscape Form and Character

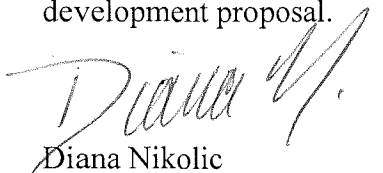
- Within the property lines, the ground level public realm is proposed to be primarily hard surface in response to the commercial uses located at grade, the potential to extend these uses outdoors, and the future urban character of the site. Tan colored concrete pavers are interrupted by complementary 1.5 m (5 ft.) wide tan-coloured concrete bands with planting islands that extend between the sidewalk and the building face. The lobby entrance is punctuated with basalt pavers. Street furniture includes benches and bike racks.
- The proposed podium-level outdoor amenity space includes active and passive areas and is sited to maximize southern exposure. The common space is separated from semi-private patios by a landscaped 45 cm (18") high planter. Proposed hard landscaping elements include an outdoor kitchen with barbeque and sink, seating benches, and a fire place located within proximity of the indoor amenity area, as well as benches throughout the area. A designated children's play area includes a Kompan active climbing structure that is designed to encourage active, imaginative play while retaining a naturalistic appearance that complements the overall landscape design. In addition, rounded granite boulders, in various sizes, are proposed to further encourage active play. A combination of lighting fixtures (downcast wall lights, bollard lighting) are proposed to sensitively illuminate the outdoor amenity area without affecting adjacent residential units. A row of deciduous, columnar Dawyck Beech trees is proposed at the west end of the podium to soften and introduce visual interest to the interface with the existing podium wall of the adjacent Anderson Building.
- The proposed building design maximizes on-site landscaping opportunities and proposes to treat mid-rise rooftops as green roofs. These areas, which total 650 m² (6,995 ft²), are proposed to be planted with grasses in contrasting colors and punctuated with small flowering trees in Corten steel planters on level six and seven.
- The total landscaped rooftop area, which includes both the outdoor amenity space and mid-rise rooftops, is 1,469 m² (15,812 ft²).
- The applicant has provided confirmation that there are no bylaw sized trees on the site and that there are no trees on adjacent sites that would be affected by the proposed development.

Servicing Agreements/Engineering

- Engineering issues will be addressed through the required Servicing Agreement (SA), which is discussed in detail in Attachment 6. The scope of the SA includes upgrading the watermain along Anderson Road, extending, upgrading and reconnecting the storm sewer, and either proportionately contributing to a City Capital Project to construct a new gravity sanitary system for the catchment area or undertaking associated works.
- Portions of Buswell Street consist of individual fee simple lots that are currently incorrectly titled to the Province. Real Estate Service staff are in the process of securing City ownership of the lots, which are required to facilitate the 2014 Buswell Street Sanitary Sewer Construction Capital Project. The capital project is required to accommodate demand generated by high density re-development underway in the area. The applicant is responsible for the costs associated with transfer of ownership including survey and plan registration costs for the two (2) Buswell Street parcels located between Park Road and Anderson Road as referenced in Attachment 6.

Conclusions

The proposed development is responsive to the City of Richmond's urban design objectives within the Brighthouse Village of the City Centre. While responding to CCAP urban design objectives, the proposal sets itself apart from existing development in the area by proposing a building design that is characterized by the intersection of building volumes and an unique treatment of the parking podium façade. In addition, the proposal demonstrates a strong commitment to sustainability by committing to introduce an on-site geo-exchange system that would heat and cool the development, and green roof treatment of all exposed mid-rise roof tops that are not used as private patio space. The proposed building design, ground level uses, and public realm design would contribute to the incremental process of strengthening the evolving urban, pedestrian-oriented character within this neighbourhood. Based on the proposal's design response to the objectives of the CCAP and the site context, staff support the proposed development proposal.



Diana Nikolic
Planner II – Urban Design

DN:rg

Attachment 1: Data Sheet

Attachment 2: Brighthouse Village Specific Land Use Map

Attachment 3: Geo-Exchange System Details (provided by applicant)

Attachment 4: Sustainability and CPTED Provisions List (provided by applicant)

Attachment 5: Advisory Design Panel Minutes & Applicant Responses (inserted in bold italics)

Attachment 6: Development Permit Considerations



DP 13-645286

Attachment 1

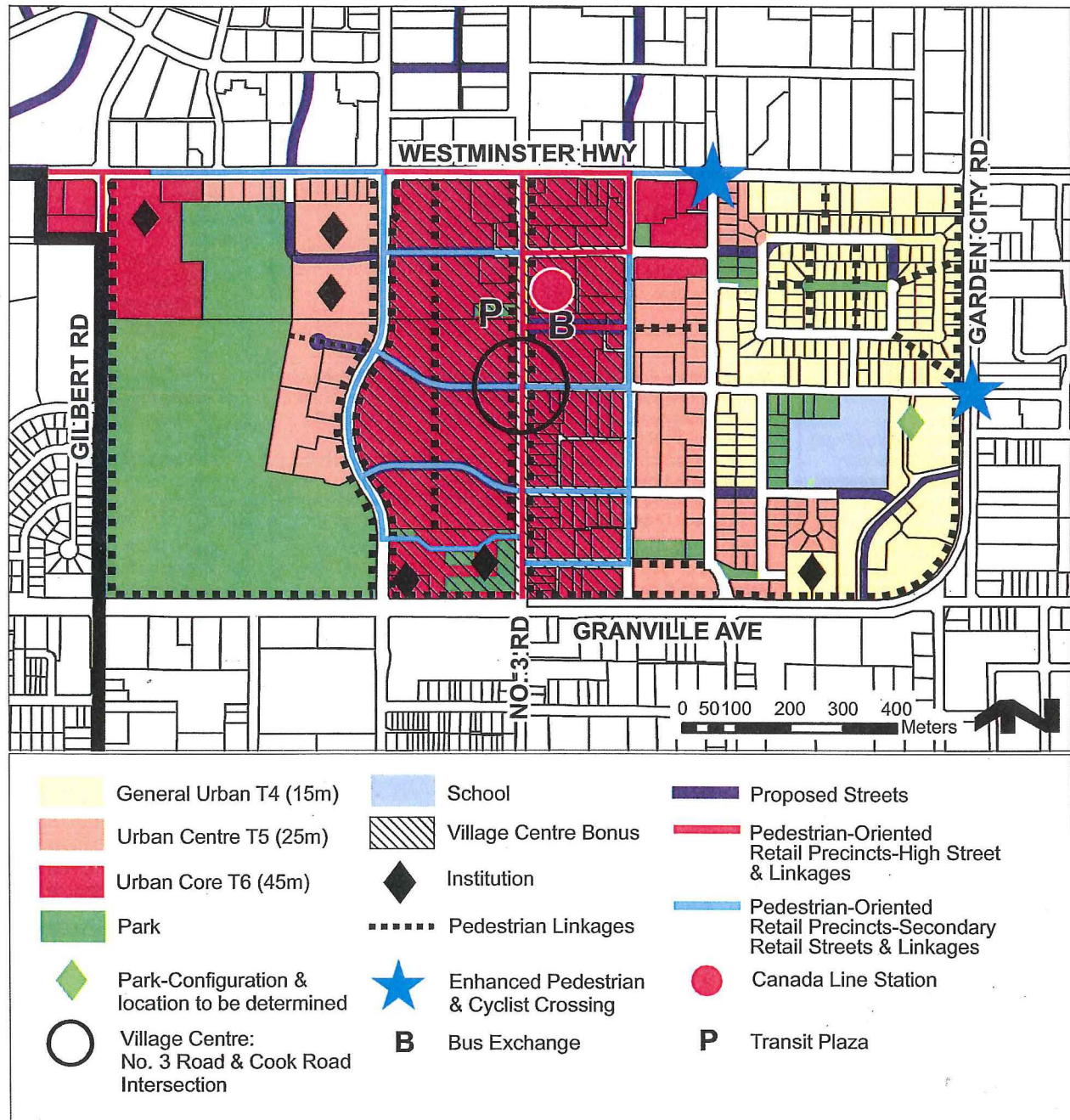
Address: 8151 Anderson Road
Applicant: Andrew Cheung Architects Inc. on behalf of 684267 B.C. Ltd Owner: 684267 B.C. Ltd., Inc. No. 684267
Planning Area(s): City Centre Area Plan (Brighthouse Village)
Floor Area Gross: 11041.65 m² (118,850 ft²) Floor Area Net: 10,192.81 m² (109,715 ft.2)

	Existing	Proposed
Site Area:	3,484.4 m ²	3,484.4 m ²
Land Uses:	Restaurant, office	Retail commercial, residential
OCP Designation:	Downtown Mixed Use	Downtown Mixed Use
Zoning:	CDT1	CDT1
Number of Units:	site is currently developed as commercial units in a one and two storey building with surface parking	111

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	3.15 provided Affordable Housing is provided in accordance with Council policy	3.142	none permitted
Lot Coverage:	Max. 90%	81.3%	None
Setback – Front Yard and exterior side yard	Anderson Road: 6.0 m, with provisions to reduce to 3.0 m Buswell Street: 6.0 m, with provisions to reduce to 3.0 m	Anderson Road: 3.0 m Buswell Street: 3.0 m	None
Setback – Interior Yard	Min. 0 m	0.1 m	None
Setback – Rear Yard	Min. 0 m if there is a lane	0 m	None
Building Height (m):	Max. 47 m geodetic	46.87 m	None
Lot Size:	No minimum lot width, depth, area	61.44 m x 56.69 m	None

Off-street Parking Spaces – Regular/Commercial:	City Centre Zone 1 Rate: Commercial: 3.75/100 m ² : 45 stalls Visitor: 0.2/unit: 23 stalls (note: commercial and visitor stalls are shared therefore a total of 45 stalls are required) Residential (1.0/unit, AH 0.9/unit): 111(103 regular, 8 AH) Total required: 156	Commercial/visitor shared: 47 (Commercial: 45, Visitor: 23) Residential: 115 Total: 162	City Centre Zone 1 Rate in lieu of compliance with Affordable Housing Strategy and other transportation improvements supported by Transportation staff
Off-street Parking Spaces – Accessible:	2% of total Residential: 3 Visitor/Commercial: 1	Resident: 3 Visitor/commercial: 2	None
Total off-street Spaces:	156	162	None
Electric vehicle charging	-20% provided with 120 volt receptacle -Additional 25% predicted for future wiring -Minimum 1 120 volt receptacle for every 10 Class 1 bicycle parking spaces	# 120 volt receptacles: 38 # stalls pre-ducted: 46 # 120 volt receptacles: 16	None
Bike Parking	Class 1: 143 Class 2: 28	Class 1: 151 Class 2: 28	None
Amenity Space – Indoor:	Min. 100 m ²	180 m ²	None
Amenity Space – Outdoor:	Min. 666 m ² (based on 111 units)	819.4 m ² plus an additional 650 m ² of green roof area	None

City of Richmond

Specific Land Use Map: Brighthouse Village (2031)



Stantec Consulting Ltd.
1100 - 111 Dunsmuir Street, Vancouver BC V6B 6A3

October 27, 2014

File:

Attention: Kassra Tavakoli, MAIBC. AIA
ANDREW CHEUNG ARCHITECTS INC.
Suite 410, 1639 W 2nd Avenue
Vancouver BC
V6J 1H3
Canada

Dear Kassra,

Reference: Mechanical System for the Tower at 8151 Andreson Rd., Richmond BC

The following will provide a brief description of the geoexchange system planned for the Tower at 8151 Anderson Road in Richmond, BC, to assist in the DP approval process, and relaxation of the DEU Ready Requirement.

The mechanical system planned for at 8151 Anderson Road will include:

- o Air handling units with hydronic coils and four pipe fan coil units (in residences) to provide heating and cooling.
- o Central heat pump system which will allow simultaneous heating and cooling.
- o Geoexchange field (ground loops) for a ground source heat pump system.
- o Peaking and backup hot water boiler system.
- o Place to connect a cooling system backup cooling tower.

Both the commercial and the residential components of the building will require, and are planned to include, mechanical cooling. Once the required refrigeration equipment (chillers or heat pumps) is available in a building, it makes technical sense to use the equipment efficiently, and include the heat recovery concepts. The system naturally fitting with these principles is the geoexchange system, also referred to as geothermal system, or a ground source heat pump system.

The geoexchange systems use ground as a heat source when net heating is required, and also as a heat sink when net cooling is required. This constitutes the renewable energy component for the use by the building. The mechanical system uses a heat pump to facilitate the transfer of heat from a low temperature source to a higher temperature sink (load).

For the component of simultaneous heating and cooling demand in the building (between different zones), the heat pump system allows heat recovery, i.e. the use of heat recovered from the zone requiring cooling, which is rejecting heat and which would otherwise need to be rejected to the outside, for heating in zones that require heat. The building only requires the heat



October 27, 2014
Kassra Tavakoli, MAIBC. AIA
Page 2 of 2

Reference: Mechanical System for the Tower at 8151 Andreson Rd., Richmond BC

from a supplemental source for the balance of heat required which is not covered by the heat pump system (heating season), and for component of heat requiring higher temperatures than what the heat pump system provides (such as domestic water heating). When excess heat is available, it is rejected (deposited) into the ground (cooling season). Through the mechanism of the daily or seasonal heat storage, the heat from the building is recovered and reused.

Heat pumps in geexchange systems achieve a coefficient of performance (COP) of 2.5 to 4.5, i.e. for every unit of electric energy input for the heat pump compressors, the machine provides 2.5 to 4.5 units of heat. Combined with simultaneous cooling, the combined COP-s of over 6 can be achieved. This efficiency is the basis of the economical operation of the building, and the economy for the building occupants.

The supplemental heat is provided, typically, from a heating system with boilers. This is a peaking system which provides for the high peak heating demand load during short duration load peaks, as well as the component of heat requiring higher temperatures than what the heat pump system provides (such as domestic water heating); It also serves as a backup system for the building heat, in case the heat pump system is down or unavailable. While this system needs to be sized for the peak load, the use, and the energy consumption from this system is small.

The system will be sized to meet cooling demand, and up to 80% of the peak heating load (in compliance with the recommendations of the CSA 448 Standard (Design and installation of earth energy systems). The boiler backup system will provide higher temperature domestic water heating, peaking heat and backup service for the heating heat pump system.

The system and the sizing is still in the preliminary evaluation stage; the preliminary sizing will follow conceptual design stage.

Please call with any questions or suggestions.

Regards,

STANTEC CONSULTING LTD.

A handwritten signature in black ink, appearing to read 'Dejan Radolcic'.

Dejan Radolcic, P.Eng., LEED AP
Associate
Phone: (604) 696-8075
Fax: (604) 696-8100
dejan.radolcic@stantec.com

c. File, Stantec: M Dhont

Sep. 24th 2014

Sustainable Features

8151 Anderson Rd. : Residential tower with grade oriented retail.

The following is a list of some of the sustainable related aspects and features included in this development design:

- The site is centrally located and close to transit, schools, shopping and parks.
- The building uses concrete construction for longer life span.
- In response to the CCAP requirement this project will comply with LEED Silver (equivalency) or better, paying particular attention to features significant to Richmond (e.g., green roofs, Geothermal energy, storm water management and quality). Applicant will submit a LEED scoreboard confirming that the subject development will achieve between 50 and 60 points (Silver equivalency) prior to issuance of a Development Permit.
- The building will have it's own Geothermal Heat Exchange facility onsite, and will be heated and cooled using an efficient hydronic system.
- At least 20% of the materials will be locally sourced and at least 10% of the materials used in the building will be recycled materials.
- Low VOC emitting materials and low flow toilets, showers and lavatories will be used in the building.
- The building aims to achieve at least 23% savings in energy consumption and a 10% in energy cost savings. All exterior glazing will have low 'e' coatings.
- Extended slabs beyond the plane of glazing by 0.5M on all sides of tower residential levels provide additional shading against solar heat gains on the West and South facing elevations.
- "In-set" balconies ensure a better weather protection for outdoor experience of residents, and help provide better resistance against solar heat gains for units facing East, South and West directions.
- Building design incorporates continuous overhangs along both sidewalks to improve usage and durability of the materials for a better lifespan of building elements.
- Large areas of non-roof (green /sod roofs) at levels 4, 6, and 7 with some tree shading on landscaped roof areas will help the heat island effect and contribute to additional insulation value and help reduce energy loss.
- All landscaping on roof are based on low maintenance planting/landscape materials, and are irrigated with a highly efficient irrigation system (smart irrigation controllers, moisture sensors and other devices that will help avoid over-watering). Climate-tolerant plants and diversity of species will contribute to reduce water consumption.
- Accessible roof decks have pre-cast pavers or similar sustainable materials.
- Semi open facade with precast panels and perforated metal screen provided on South and East sides of parking podium will provide natural light and ventilation to the parking garage and reduce dependency on artificial lighting and mechanical ventilation.
- Electric charging plug provisions are provided in enclosed bike storage rooms and for 20% of the required parking stalls.
- All units are provided with open balconies or landscape treated roof decks.
- Balconies have glazed guards to allow visibility.

- All bedrooms and living rooms have large glazed windows to allow maximum natural light and provide a lively atmosphere. All habitable rooms are provided with windows that open to allow natural/ventilation.
- Glazed windows and other exterior wall assemblies will have adequate acoustic performance as required by building code and regulations.
- Consideration will be given to sustainable fit and finish of the building including energy star appliances, lighting fixtures and other electrical appurtenances used throughout the building, as well as reduced allergen carpeting/ finishing materials.

Crime Prevention Through Environmental Design (CPTED)

The proposed development includes the following CPTED features:

- Further setbacks creating wider sidewalk with residential units cantilevering over parts of the sidewalk, providing “watchful” residents’ eyes on the public realm and the sidewalk.
- Double height (23’ high) entry alcove with fully glazed wall facing Buswell street provides ample lighting & visibility at entry point.
- Security features in the building lobby to limit access.
- Full street length of retail at grade fronting both streets, with secured rear access to parking and building services.
- Single vehicular access point to the parkade via a recessed entry gate with full security system, with a secondary residential parking gate on 2nd level to limit visitor access to private parking.
- Sliding gate at the lane to close off the loading / garbage removal bay when not in use.
- Parkade lighting with available day time daylight through parkade facade opening, with interior parkade walls and columns painted white to increase visibility.
- Glazing in all vestibule doors and enclosed corridors.
- Additional safety windows on side walls of vestibules in parkade for more view angles.
- Secured garbage rooms with access that is located close to the elevator lobby.
- Secured bike storage rooms located on parking floor perimeter with daylight and ventilation.
- Secure rooftop common outdoor amenity area with children’s play area located in the mid section. Direct access and views provided to the outdoor amenity room from both indoor amenity rooms . Overlooking views of the outdoor amenity room from residential units adjacent and above.

Excerpt from the Minutes from
The Advisory Design Panel Meeting

Wednesday, October 8, 2014 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall

6. **DP 13-645286 – 116 APARTMENT UNITS AND GROUND LEVEL RETAIL**

APPLICANT: Andrew Chung Architects Inc.

PROPERTY LOCATION: 8151 Anderson Road

Applicant's Presentation

Architect Kassra Tavakoli, Andrew Cheung Architects Inc., and Landscape Architect Alain Lamontagne, Durante Kreuk Ltd., presented the project and answered queries from the Panel on behalf of the applicant.

Panel Discussion

Comments from the Panel were as follows:

- project is nicely done; applicant has done the right moves; size of the floor plate appears appropriate; appreciate the elongated form of the tower as opposed to a square;

Acknowledged

- three main volumes of the building appear to be a strong part of the concept of the project; however, it is diluted by the mid-rise “knuckle” at the east elevation (Buswell); consider eliminating this piece to simplify the forms; lost density could possibly be accommodated by the adjacent low-rise form;

We have chosen to maintain this form, as it is an integral part of the stepping tower. This portion extends the tower's shape and language of materials immediately over the main residential entry, making an important connection.

- simplify the building residential lobby entry at the east elevation as it currently appears messy;

We have simplified the main entry and eliminated the asymmetrical canopy which extended over it.

- screening mechanism for the parking garage appears solid; consider other more appropriate and contemporary methods;

We have changed the extent of the parking garage façade and reduced the solid sections in favour of more open mix of solid vs. translucent. The parking façade is currently being considered as an area of interest for the public art implementation and is therefore subject to being further improved by application of some form of art to be determined through the appropriate process.

- the project has many good elements; appreciate the clarity of the four-storey volume; however, agree with the previous comment regarding the mid-rise “knuckle” at the east elevation and consideration of its elimination;

Acknowledged, see comments above.

- architectural renderings of the parking garage screening presented by the applicant do not appear to reflect the project’s concept;

Acknowledged

- the proposed tower is well done and nicely articulated;

Acknowledged, we have maintained the tower unchanged.

- appreciate the simple treatment of the parking garage which effectively covers the parking; lighting at night will animate the façade;

Acknowledged

- agree with the comment on the mid-rise form that projects out at the east elevation and consideration of its elimination;

See comments above.

- consider removing the bike room interruption in the parking façade on the south elevation (Anderson) to make the pattern of interlocking volumes more successful;

The bike room projection has been eliminated as suggested. The building overhang is now only on level 4 where the residential block overhangs the parking structure.

- appreciate the lay-out of the tower and mid-rise forms; will maximize the views of the green spaces;
- locating public art on the building façade above eye level may not be advisable as it cannot be experienced by pedestrians at street level;

We have since had the public art plan presented to the Richmond Public Art Advisory Committee who is in support of the proposal, giving it merit on its visibility from a distance among other things.

- appreciate how the massing has been broken down; parkade wall appears blank; consider utilizing it as canvass for public art; look forward to seeing the project’s public art plan;

See comments above

- consider introducing natural lighting to the parkade wall along Anderson Road;
Acknowledged. Design has changed and majority of parking façade along Buswell now receives similar treatment as on Anderson Road.

- site planning is good; stepping of volumes is an interesting approach to a corner site;

Stepping approach has been largely maintained in a revised configuration.

- agree with previous comment regarding the tower form getting lost in the lower portion which projects out along Buswell Street; needs to be “cleaned up” and refined to maintain the purity of the tower form;

We have revised the projections, eliminating the smaller single projection of the parking volume at Buswell Street. The “projection” has been extended to both sides of the entry hence marking the entrance as a recess.

- does not agree with the orientation of the tower; investigate shading of north-facing units;

All towers suffer from the same issue as their north facing units receive less light. We have tried to improve this by having a mid-rise section with more units with east, south and west exposures than north.

- retail frontages are handled well; transparency of a large corner retail element works well; however, this character is not carried through the Buswell Street elevation; canopy above the left-over retail spaces appears heavy; consider transitioning back to the typical glazed canopy in order not to conflict with the entry to the residential lobby;

As noted in previous comments, the canopy over the main entry has been revised, and the canopy over the north retail unit along Buswell Street has been changed to a glazed canopy.

- proposed parkade screening element along levels two and three fronting Anderson Road appears monolithic; concern on (i) how it will be executed, (ii) how it will interface with the public realm and (iii) how it will evolve into a public art piece;

The monolithic appearance of the parkade screen along those levels is largely an artefact of the renderings. Regardless, we have revised the proportion between the solid precast concrete panels and the translucent metal screens which make the gap between them to be at least half open. The public art integration with this façade will depend to some degree on the artist selected and the artwork that is ultimately accepted by the selection committee. However, we are confident and convinced that this dynamic pattern of precast vs. metal screen panels is a bold and interesting design for screening above grade parking on its own, without the application of any art work.

- levels two and three along Buswell Street lacks transparency and visual interest; need further attention and treatment;

As noted above, we have extended the parking façade treatment facing Anderson Road further into the Buswell Street frontage. The remaining portions of parkade along Buswell Street are now faced with a curtainwall system with colored spandrel panels near the main entry at Buswell Street.

- monolithic parkade wall along Anderson Road needs to be broken up;

Acknowledged, The proportions of solid to open has been changed in favor of more open (translucent/perforated metal screen) façade.

- appreciate the provision of a number of wheelchair accessible residential units;

Acknowledged

- consider reducing the curb to enhance the accessibility to public areas;

Acknowledged. Details will reflect.

- incorporate pocket doors in lieu of hinged doors in bedrooms and dens; will create extra floor space and provide more access to residents;

Acknowledged. Level 4 plan reflects this change.

- applicant is encouraged to consider going for a LEED Silver certification, rather than equivalency, for the project as it will add value to the project;

Acknowledged. Currently seeking LEED consultants to join the design team

- appreciate the proposed on-site geo-exchange system; look at the technical aspects, e.g. reading/metering the residential and retail units;

Acknowledged

- ensure that the proposed louvers for ventilation of retail units will achieve maximum efficiency during the base building construction stage;

Acknowledged

- a radius corner as opposed to a square corner is more inviting in a prominent corner such as the Anderson Road and Buswell Street corner;

We have chosen to maintain the square corner in a slightly revised plan with different (reduced) setbacks. The weather protection is now continuous and turns the corner with retail façade.

- need to have universal access in the public areas of the project;

Acknowledged. See landscape plans

- privacy walls appear heavy; consider incorporating planting;

Acknowledged

- consider introduction of trellis structure(s) on the landscaped podium to address overlook into semi-private and private areas of the courtyard level;

Acknowledged

- consolidate tree planting along the edge of the green roof and patios on levels 6 and 7 to provide shading; soil volumes are a concern for trees on the roof decks in 4x4 planters; planting shown on roofs that can only be accessed by hatch and ladder is a concern; maintenance of plant materials using this type of access can be challenging; growing medium volumes should comply with current arboricultural and urban forestry guidelines for urban trees;

Landscape architects have considered the above comments and have revised the tree planters on the green roof, providing fewer trees with larger planters allowing for more substantial root ball.

- planting list is only “suggested”; a complete plant list is required as part of the submission;

Acknowledged

- articulation of retail spaces is not clear on the drawings; signage is lacking;

Retail signage is designed to be located only in a linear “band” above the retail doors, and below the glass weather protection canopy. Notes have been added to the elevation drawings demarcating this signage band.

- building massing responds to the contextual massing; appreciate the south-facing outdoor amenity space and its interface with the indoor amenity space;

Acknowledged

- information regarding the opportunity and potential to incorporate public art on the exterior parkade wall is lacking; and

We are currently in the process of further defining the direction in which the public art is to follow in order to integrate with the parkade wall.

- consider use of clear glass versus opaque glass for enclosed bike locker areas.

The window wall/curtain wall system has been eliminated from the bike rooms. The bike storage rooms are now designed to fill the space between the parked cars and the parkade’s exterior walls.

Panel Decision

It was moved and seconded

That DP 13-645286 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Panel.

CARRIED

Opposed: Paul Goodwin



Address: 8151 Anderson Road

File No.: DP 13-645286

Prior to approval of the Development Permit, the developer is required to complete the following:

1. Registration of a 1.1 m wide public rights of passage right of way along the length of the site's Buswell Street frontage to accommodate frontage upgrades. Upgrades include a 2.0 m wide sidewalk at the back of the new statutory right of way, the remaining area to the existing west curb of Buswell Street, which is to be landscaped boulevard consistent with the frontage improvements in the SA (SA 09-486006) for 8160 Park Road. The developer is responsible for construction; the City is responsible for maintenance of hard surfaces and infrastructure, and accepts liability for areas that it maintains.
2. Discharge of the existing Statutory Right of Way (284728C, 285749C and 285760C (SRW Plan 79051)) along the site's northern frontage.
3. Registration of 3.0 m wide public rights of passage right of way along the length of the site's northern frontage that abuts the existing lane and includes an Option to Purchase/Dedicate the right of way area in favor of the City for nominal consideration. The owner agrees to, at the owner's expense, prepare and register the necessary dedication plans. The right of way is to accommodate utilities and lane upgrades in accordance with City Centre standards. The developer is responsible for construction; the City is responsible for maintenance of hard surfaces and infrastructure, and accepts liability for areas that it maintains.
4. Registration of a Section 219 Covenant(s) providing for no Stratification (Subdivision by way of Strata Plan) and no occupancy of lands until the right of way area described in #3 has been transferred to or dedicated to the City.
5. 4.0 m x 4.0 m corner cut secured with a statutory right of way for public rights of passage at the intersection of Anderson Road and Buswell Street. The developer is responsible for construction; the City is responsible for maintenance of hard surfaces and accepts liability for areas that it maintains.
6. 3.0 m x 3.0 m corner cut secured with a statutory right of way for public rights of passage at the intersection of the lane and Buswell Street. The developer is responsible for construction; the City is responsible for maintenance of hard surfaces and accepts liability for areas that it maintains. Any non-permanent encroachment (e.g. removable canopy) into the statutory right of way is accommodated by an unregistered letter agreement. Any non-permanent encroachment into City lane is accommodated by a license agreement, which is not registered but would include obligation to remove the encroachment on notice by the City, and a letter of credit to cover potential future costs (repair, removal, etc.) may be required.
7. 1.5 m x 9.0 m statutory right of way on the Buswell Street frontage for the purposes of a concrete bus pad. The developer is responsible for construction; the City is responsible for maintenance of the hard surface and accepts liability for areas that it maintains.
8. Receipt of a Letter of Credit for landscaping in the amount of \$486,131.25.
9. Registration of an aircraft noise sensitive use covenant on title.
10. Registration of a covenant on title that identifies the building as a mixed use building.

12. Registration of a flood indemnity covenant on title identifying a minimum habitable elevation of 2.9 m GSC, or at least 0.3 m above the highest elevation of the crown of any adjacent parcel, or as exempted by Section 4.3(a) of Flood Plain Designation and Protection Bylaw No. 8204.
13. Registration of a legal agreement on title stipulating that the development is subject to potential impacts due to other development that may be approved within the City Centre including without limitation, loss of views in any direction, increased shading, increased overlook and reduced privacy, increased ambient noise and increased levels of night-time ambient light, and require that the owner provide written notification of this through the disclosure statement to all initial purchasers, and erect signage in the initial sales centre advising purchasers of the potential for these impacts.
14. Registration of a legal agreement on title stipulating that 47 shared commercial and visitor parking stalls, will be located on parkade levels 1 and 2. The minimum number of parking stalls provided must be the greater of the two individual uses. The agreement must specify the parking spaces remain unassigned, visitor parking is accessible 24 hours a day and commercial parking is available during standard business operating hours. This legal agreement is subject to the approval of the Director of Transportation.
15. Registration of a legal agreement on title stipulating provisions that secure:
 - a) A minimum of 20% of parking stalls are provided with a 120 volt receptacle to accommodate electric vehicle charging equipment;
 - b) A minimum of 25% of parking stalls are constructed to accommodate the future installation of electric vehicle charging equipment (e.g. pre-ducted for future wiring);
 - c) A minimum of one 120 volt receptacle is provided to accommodate electric charging equipment for every 10 Class 1 bike parking spaces.
16. Discharge of covenant AD233647, which allows an awning to extend above the sidewalk in front of the building.
17. City acceptance of the developer's offer to voluntarily contribute \$50,000 to the future upgrading of the Park Road and Buswell Street intersection to include full traffic signalization.
18. City acceptance of the developer's offer to voluntarily contribute \$87,462.43 (\$0.77/ft² per residential buildable square foot, excluding affordable housing, and \$0.44/ft² per commercial buildable square foot) to the City's public art fund.
19. Registration of the City's standard Housing Agreement to secure eight (8) affordable housing units, the combined habitable floor area of which shall comprise at least 5% of the subject development's total residential building area. Occupants of the affordable housing units subject to the Housing Agreement shall enjoy full and unlimited access to and use of all on-site indoor and outdoor amenity spaces. The terms of the Housing Agreements shall indicate that they apply in perpetuity and provide

Unit Type	Number of Units	Minimum Unit Area	Maximum Monthly Unit Rent**	Total Maximum Household Income**	Unit Location	Unit Numbers
1 bedroom	5	50 m ² (535 ft ²)	\$950	\$38,000 or less	Building level 4,5,6	507, 510, 608, 611, 705
1 bedroom & den	3	50 m ² (535 ft ²)	\$950	\$38,000 or less	Building level 4,5,6	511, 612, 708

** May be adjusted periodically as provided for under adopted City policy.

20. Complete an acoustical and mechanical report and provide recommendations prepared by an appropriate registered professional, which demonstrates that the interior noise levels and noise mitigation standards comply with the City's Official Community Plan and Noise Bylaw requirements. The standard required for air conditioning systems and their alternatives (e.g. ground source heat pumps, heat exchangers and acoustic ducting) is the ASHRAE 55-2004 "Thermal Environmental

Conditions for Human Occupancy” standard and subsequent updates as they may occur. Maximum interior noise levels (decibels) within the dwelling units must achieve CMHC standards follows:

Portions of Dwelling Units	Noise Levels (decibels)
Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

21. Owner’s written commitment to use a geo-exchange system to supply space heating, cooling and domestic hot water to the building, including the following terms and conditions:
- No building permit will be issued for this development unless the building is designed to use a geo-exchange system for space heating, cooling and domestic hot water and the owner has provided an energy modelling report satisfactory to the Director of Engineering;
 - The following is required prior to issuance of a Building Permit:
 - The City receives an energy modeling report showing:
 - Space heating, cooling and domestic hot water heating peak loads and hour by hour consumption, and
 - Percentage of annual space heating, cooling and domestic hot water requirements supplied by the geo-exchange system.
 - The City receives for review mechanical, plumbing and architectural drawings (in PDF format) showing that a building mechanical system is designed to utilize the geo-exchange system for not less than 70% of all the annual space heating, cooling and domestic hot water heating for a building as determined in the energy modeling report.

Prior to Building Permit Issuance, the developer must complete the following requirements:

- Enter into a Servicing Agreement* for the design and construction of the following works, which include but may not be limited to:

Water:

Anderson Road

- Using the OCP Model, there is 137 L/s available at 20 psi residual. The proposed development requires a minimum fire flow of 220 L/s. The developer is required to submit fire flow calculations that are signed and sealed by a professional engineer based on F.U.S. or ISO to confirm that there is adequate available flow.
- The developer is required to upgrade the existing 150 mm asbestos cement watermain along the Anderson Road frontage to the greater of a 200 mm watermain or as required by the OCP, in accordance with city requirements. If adequate flow is not available, the upgrade will need to extend beyond the frontage to achieve minimum requirements.
- The City may opt to extend the works beyond the frontage thru a cost share, subject to available funding.
- Additional fire hydrants are required to achieve a minimum 74 m spacing.

Buswell Street

Since there is no watermain along this frontage, the developer’s consultant is responsible to ensure adequate fire hydrants are available to service the development site. Approval may be required from the Richmond Fire Department. Written confirmation (signed and sealed) is required from the developer’s consultant.

Storm:

Storm analysis and upgrades are not required; however, the developer is required to complete the following frontage works:

Anderson Road

- Extend the 600 mm diameter storm sewer, to be constructed via SA 12-626904, along the centre of the roadway from the east property line of 8111 Granville Avenue to Buswell Street and connect to the existing storm sewer system on Buswell Street with a manhole.
- Remove/abandon existing storm sewer system and re-connect existing services that are in line with the proposed storm sewer construction.

Buswell Street

- Upgrade the existing storm sewer along the frontage to a minimum 600 mm from existing manhole STMH6253 (Anderson Road) to the north property line of the development site.
- Reconnect existing services that are in line with the proposed storm sewer construction.

Sanitary:

Under the OCP scenario the Richmond Centre Pump Station does not have adequate capacity to service the existing catchment area. The City has proposed to modify the catchment area and redirect flows from several properties to the Buswell Pump Station. This will require construction of a new gravity sanitary system. Each development site will be responsible, at a minimum, for their frontage.

The City is proposing to construct the required gravity sewer system as a Capital Project. If the City Capital Project proceeds prior to the development site requiring servicing, then the developer is required to provide a contribution in the amount of \$130,000 (to be deposited into the account 2253-10-000-14913) and connect the service to Buswell Street.

If the development site requires servicing prior to the City Capital Project, then the developer is required to design and construct the gravity sanitary sewer along both frontages (Anderson Road and Buswell Street) and connect service to existing manhole SMH55048, which is located in the lane, 21 west of the east property line of the development site. The City may opt to extend the works on Anderson Road west of the development site through a cost share arrangement, subject to available funding.

Frontage Upgrades:

- Anderson Road: a 2.0 m wide sidewalk at the back of the new statutory right of way, remaining area to existing south curb of Anderson Road to be hardscaped boulevard. Works are to be consistent with Park Road frontage details for SA 09-486006 for 8160 Park Road.
- Buswell Street: 2.0 m wide sidewalk at the back of the new statutory right of way, remaining area to existing west curb of Buswell Street is to be landscaped boulevard. Works are to be consistent with frontage details for SA 09-486006 for 8160 Park Road.
- Lane upgrades to City Centre standards.
- Upgrade the crosswalks (three (3) in total) at the Anderson Road and Buswell Street intersection with decorative stamped asphalt treatment to improve the safety and visibility of pedestrian use.

Additional Requirements:

- The developer is responsible for the installation of pre-ducting for private utilities along Anderson Road and Buswell Street frontage and coordination required with the Private Utility Companies. The developer must contact Private Utility companies to determine what equipment will be required (vistas, kiosks, transformers, etc.) and where they can be located. Such equipment is not permitted to be located in the City street. The developer is encouraged to investigate whether it can be located within the building, so that it is not visible from the fronting streets.

- Additional legal agreements, as determined via the subject development's Servicing Agreement (s) and/or Development Permit(s), and or building Permit(s) to the satisfaction of the Director of Engineering may be required, including , but no limited to site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-lading, ground densification or other activities that may result in settlement, displacement, subsidence, damage, or nuisance to City and private utility infrastructure.
 - Existing City infrastructure (fire hydrant, parking meter, street lighting, etc.) may need to be relocated at the developer's cost to accommodate frontage improvements.
 - Engineering recommends that the ultimate lane works be within a dedicated lane along the development site frontage.
 - Portions of Buswell Street consist of individual fee simple lots that are currently incorrectly titled to the Province. Real Estate Service staff are in the process of securing City ownership of the lots, which are required to facilitate the 2014 Buswell Street Sanitary Sewer Construction Capital Project. The capital project is required to accommodate demand generated by high density re-development in the area. The applicant is responsible for the costs associated with transfer of ownership including survey preparation (\$3,008.25) and plan registration costs for the following lots:
 - Northern lot:
PID: 003-717-534
SEC 9 BLK4N RG6W PL 7312 Parcel B, Subsidy Lot 18, BYLAW 55878
 - Southern lot:
PID: n/a
1 SEC 9 BLK4N RG6W PL 7557 Suburban Block 10,6.
2. Submission of a Construction Parking and Traffic Management Plan to the Transportation Division. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
 3. Incorporation of accessibility measures in Building Permit (BP) plans as determined via the Development Permit processes.
 4. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Division at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act. All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.
The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.
- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contains prohibitions on the removal or disturbance of both birds and their

nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed (original on file)

Date



City of Richmond

Development Permit

No. DP 13-645286

To the Holder: Andrew Cheung Architects Inc.
Property Address: 8151 Anderson Road
Address: Suite 410 1639 West 2nd Avenue
Vancouver BC
V6J 1H3

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - a) Reduce the residential parking rate from the standard City wide parking rate to the City Centre Zone 1 parking rate.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #18 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$486,131.25 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit
No. DP 13-645286

To the Holder: Andrew Cheung Architects Inc.
Property Address: 8151 Anderson Road
Address: Suite 410 1639 West 2nd Avenue
Vancouver BC
V6J 1H3

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

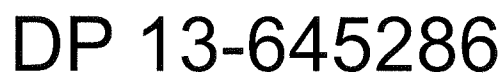
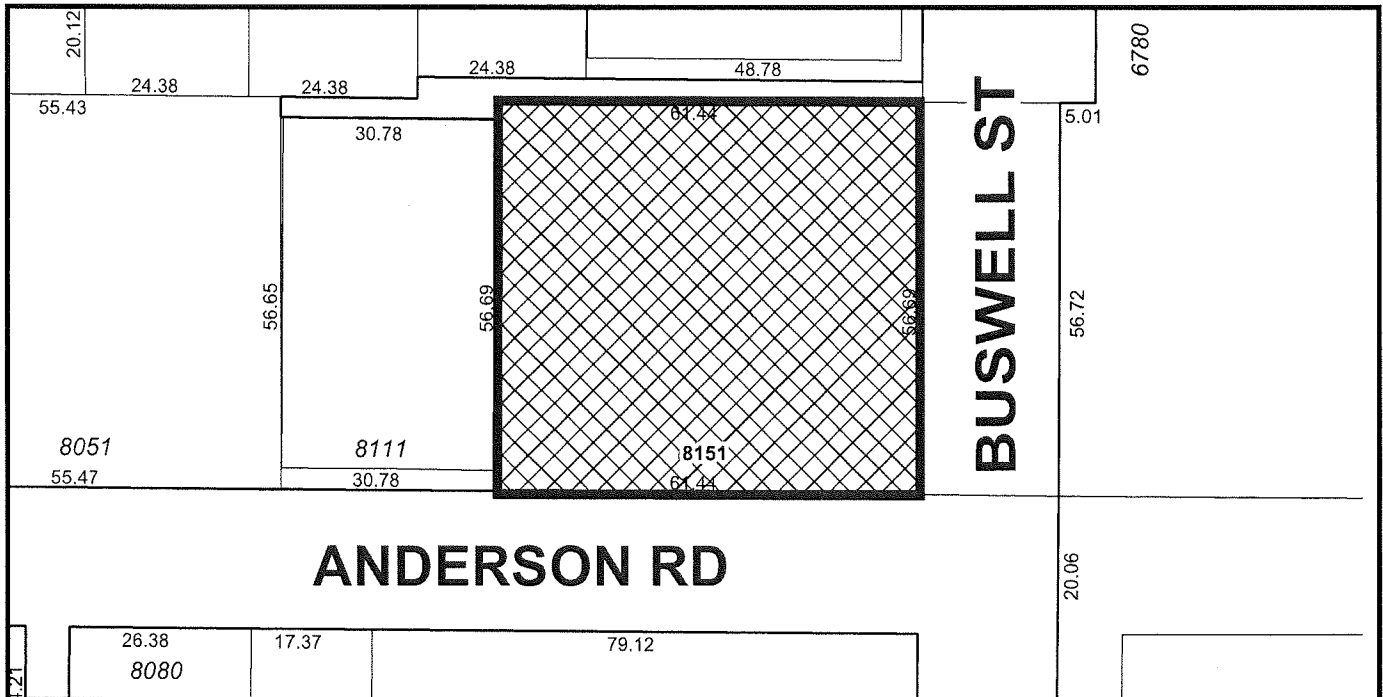
This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



Note: Dimensions are in METRES

notes

date

revisions

SEP. 4, 2013 ISSUED FOR DP
JUL. 16, 2014 ISSUED FOR DP
SEP. 24, 2014 ISSUED FOR DP
DEC. 16, 2014 ISSUED FOR DP
DEC. 16, 2014 ISSUED FOR DP
JAN. 29, 2015 ISSUED FOR DP

consultants

ANDREW CHEUNG
ARCHITECTS INC.

13-64529
suite 410
1639 West 2nd Ave.
Vancouver, B.C.
V6J 1H3
tel (604) 685-2088
fax (604) 685-1889

project
8151 ANDERSON ROAD
RICHMOND

level
LEVEL 1 PLAN

project number
21212

file name
FEB 10 2015

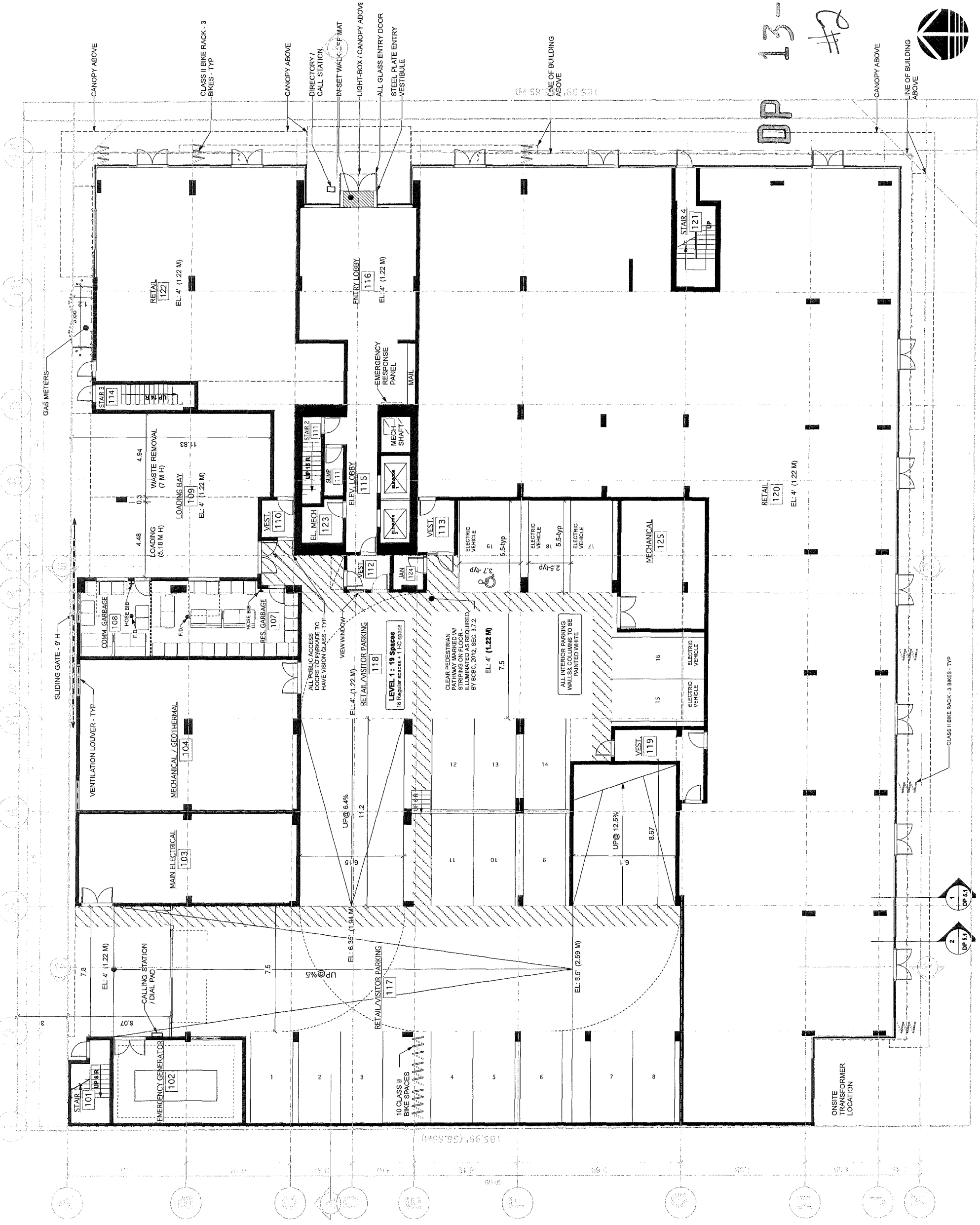
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DP 2.1

This drawing was created using an Apple Macintosh computer



CLASS II BIKE RACK - 3 BIKES - TYP

1 DP 5.1
2 DP 5.1

ONSITE
TRANSFORMER
LOCATION

notes

date	revisions
SEP. 4, 2013	ISSUED FOR DP
JUL. 18, 2014	ISSUED FOR DP
SEP. 24, 2014	ISSUED FOR DP
DEC. 8, 2014	ISSUED FOR DP
DEC. 16, 2014	ISSUED FOR DP
JAN. 23, 2015	ISSUED FOR DP

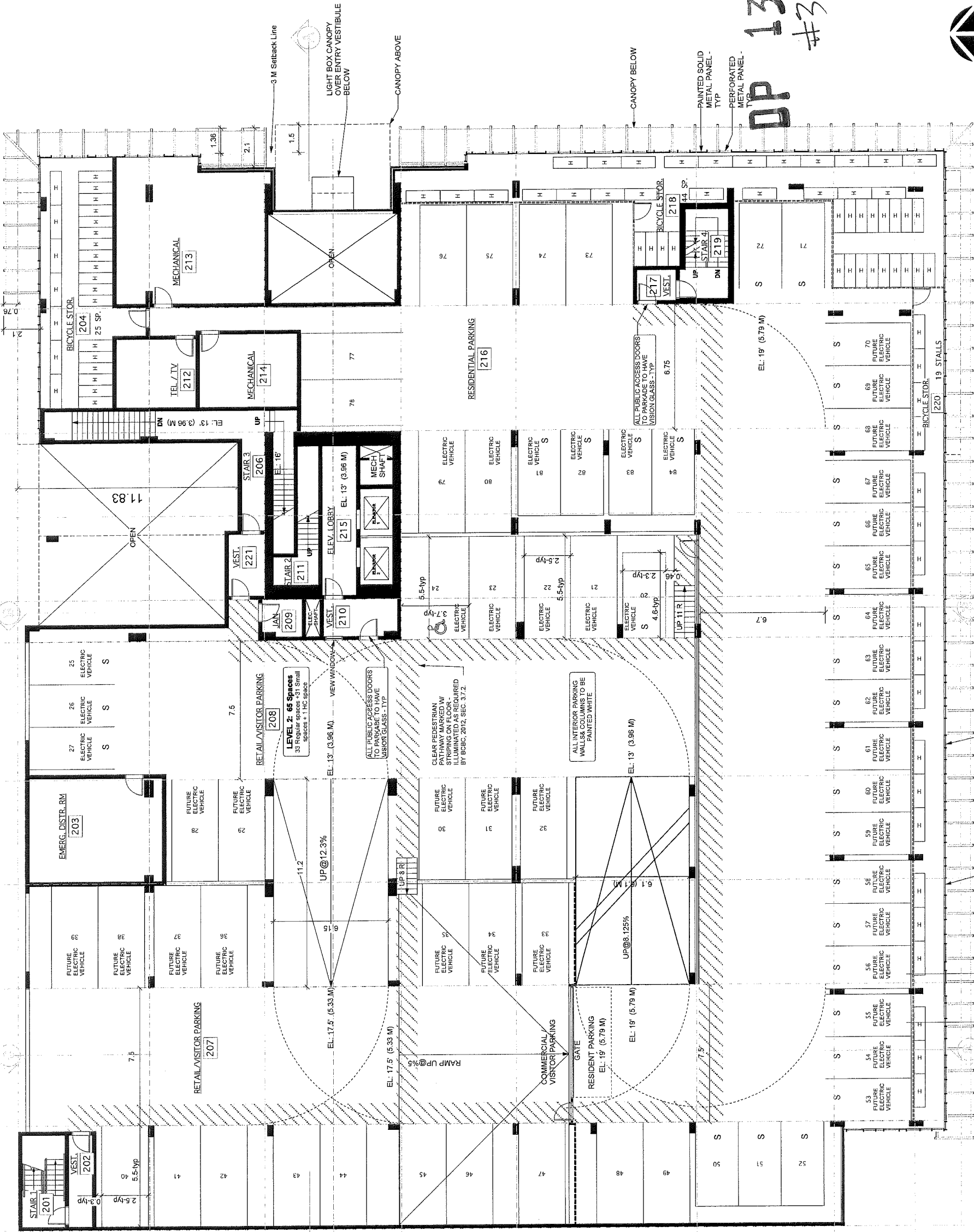
consultants

13-645ARCHITECTURE INC.
1639 west 2nd ave
vancouver, b.c. v6j 1h3
tel (604) 685-2088
fax (604) 685-1889

FEB 10 2015

project	8151 ANDERSON ROAD RICHMOND
title	LEVEL 2 PLAN
project number	21212
drawn by	al
checked by	ds
scale	1:100
drawing number	DP 2.2

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PERFORATED METAL PANEL - TYP

SOLID METAL PANEL - TYP

1 DP 5.1

2 DP 5.1

NOTES

revisions

SEP. 4, 2013 ISSUED FOR DP
OCT. 16, 2014 ISSUED FOR DP
DEC. 14, 2014 ISSUED FOR DP
DEC. 8, 2014 ISSUED FOR DP
DEC. 16, 2014 ISSUED FOR DP
JAN. 29, 2015 ISSUED FOR DP

consultants

JAN 29 2015

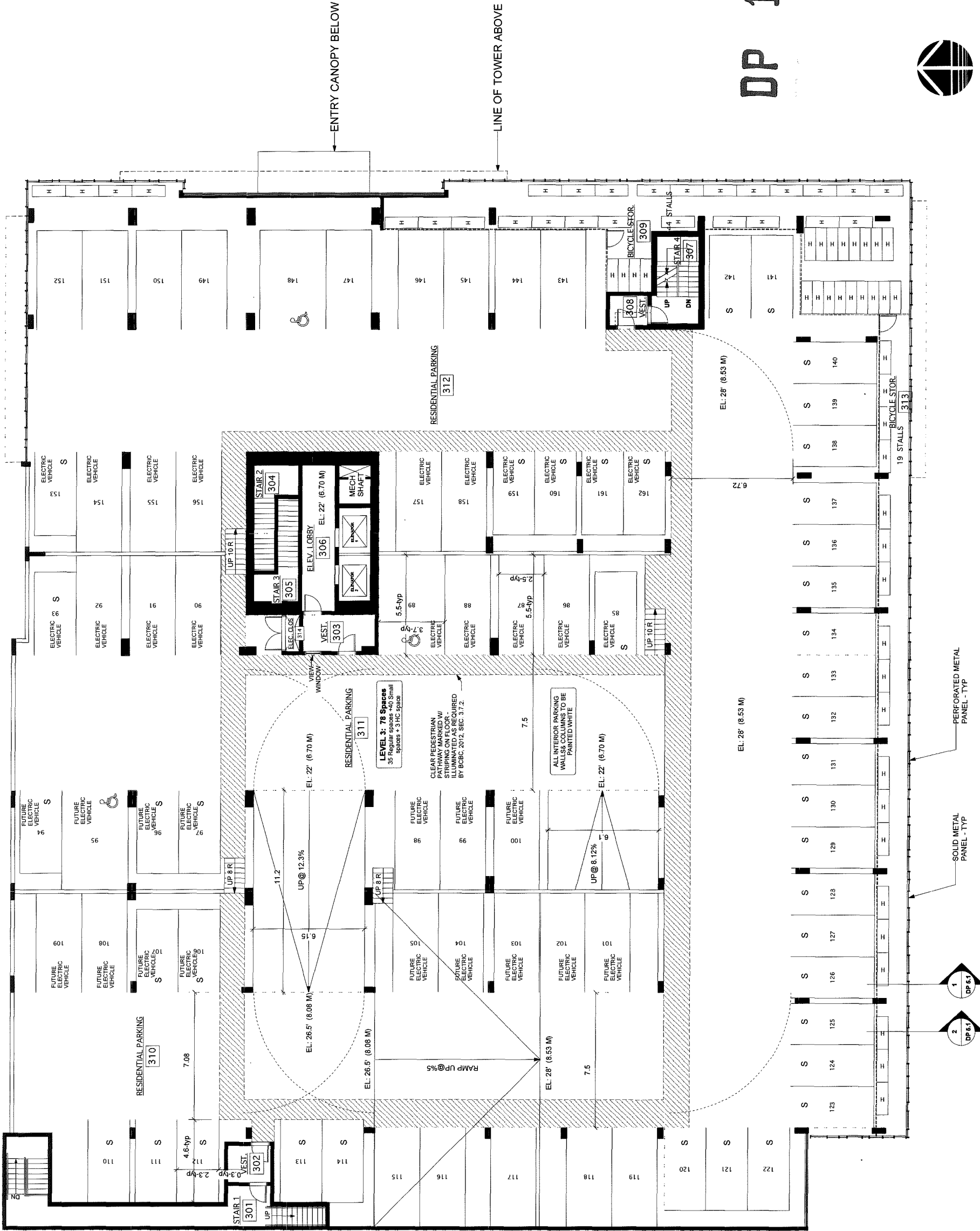
ANDREW CHEUNG
ARCHITECTS INC.

DP 13-645286

tel (604) 685-2088
fax (604) 685-1889

project 8151 ANDERSON ROAD RICHMOND	
site	LEVEL 3 PLAN
project number 21212	file name
drawn sl	checked ac
scale 1:100	drawing number DP 2.3

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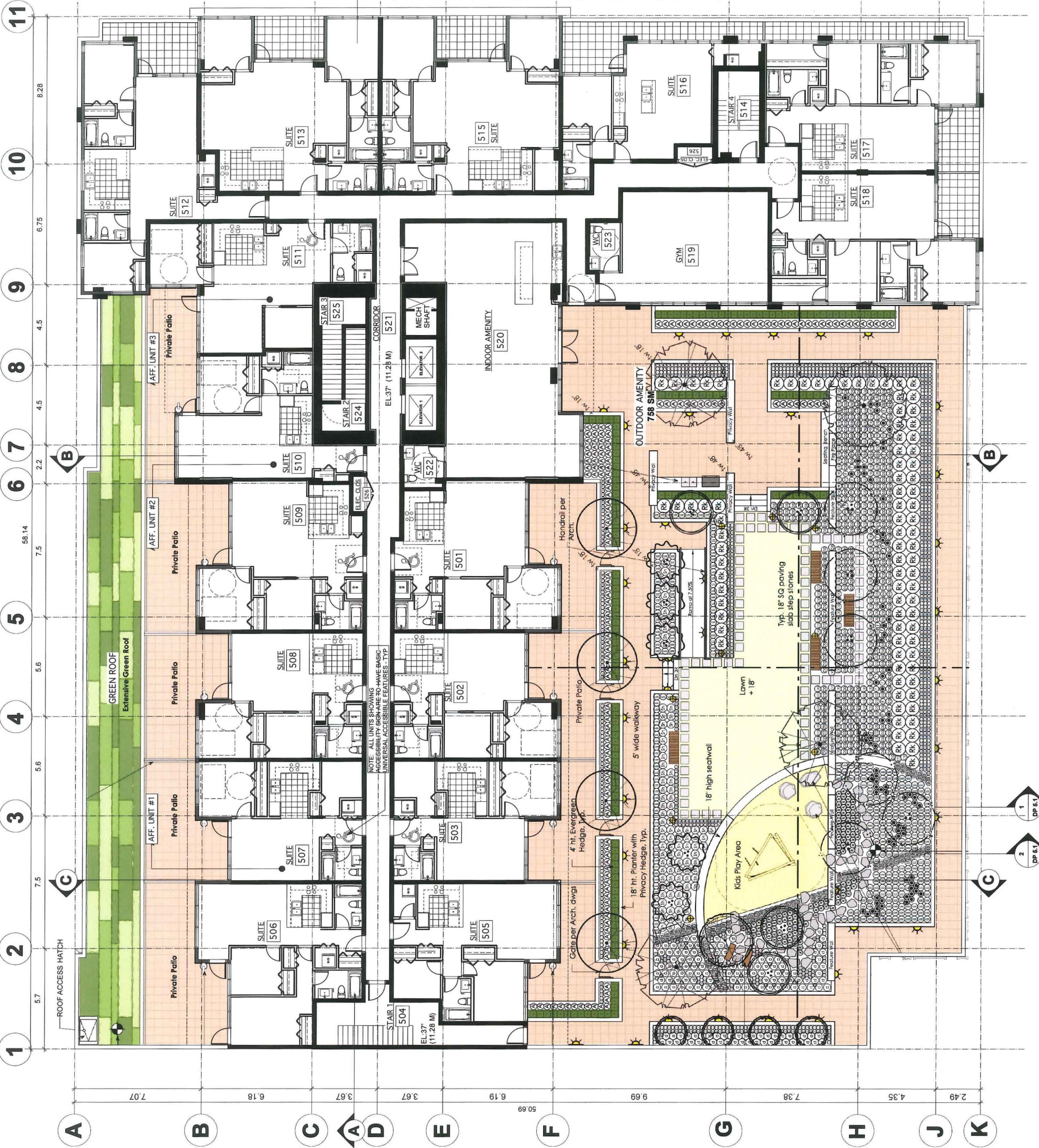


PERFORATED METAL
PANEL - TYP

SOLID METAL
PANEL - TYP

1
DP 6.1

2
DP 6.1



Accessibility and Aging-in-Place Features :
(Typical, all floors)

- All universally accessible suites (marked with accessibility symbol @ their entries on the plans) to comply with the requirements of City of Richmond Zoning Bylaw, section 4.16. -
- All units in the project are to have "Aging in place features as indicated in City of Richmond CCAP.
- All main doors to the entry of the tower and the common use facilities provided with automatic door opening devices or designed to address accessibility requirements. Bathrooms in the common amenity spaces are designed to be accessible.
- Common corridors are minimum 1220 mm wide and a minimum 1500 mm turning radius is provided at dead ends and other required areas.
- Common use areas and affordable (accessible) housing units designed to facilitate ready access from the road and the on-site parking area, and for use/occupancy by a person with disability. The elevators are located on an accessible path.
- Switch controls, electrical panels and intercom buttons shall be at a maximum height of 1220 mm and the electrical / telephone / cable outlets will at least be 450 mm from the floor surface.
- Thermostats and other controls located between 900 — 1350 mm from the floor surface and provided with a clear space of 750 mm for access.
- The accessible bedrooms are designed such that they can accommodate a twin size bed and have a 1500 min turning diameter clearance on one side of the bed.
- All bathrooms and powder rooms are provided with reinforced backing for future grab bars at water closets, tub/shower locations. (Aging in place requirement)
- All stairs to have hand rails as required by City of Richmond OCP (Aging in place requirement)
- All door handles and plumbing fixtures are to have 'lever' type handles (Aging in place requirement)

notes	revisions	
	date	SEP. 4, 2013 ISSUED FOR DP JUL. 18, 2014 ISSUED FOR DP DEC. 1, 2014 ISSUED FOR DP DEC. 8, 2014 ISSUED FOR DP DEC. 16, 2014 ISSUED FOR DP JAN. 29, 2015 ISSUED FOR DP
consultants		
JAN 29 2015		

ANDREW CHEUNG
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V6J 1H3
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fax (604) 685-1889

#5

DP 13-645286

project		8151 ANDERSON ROAD RICHMOND	
sheet		LEVEL 4 PLAN	
project number	21212	file name	
	drawn		
checked	si	scale	1:100
	ac		
drawing number		DP 2.4	

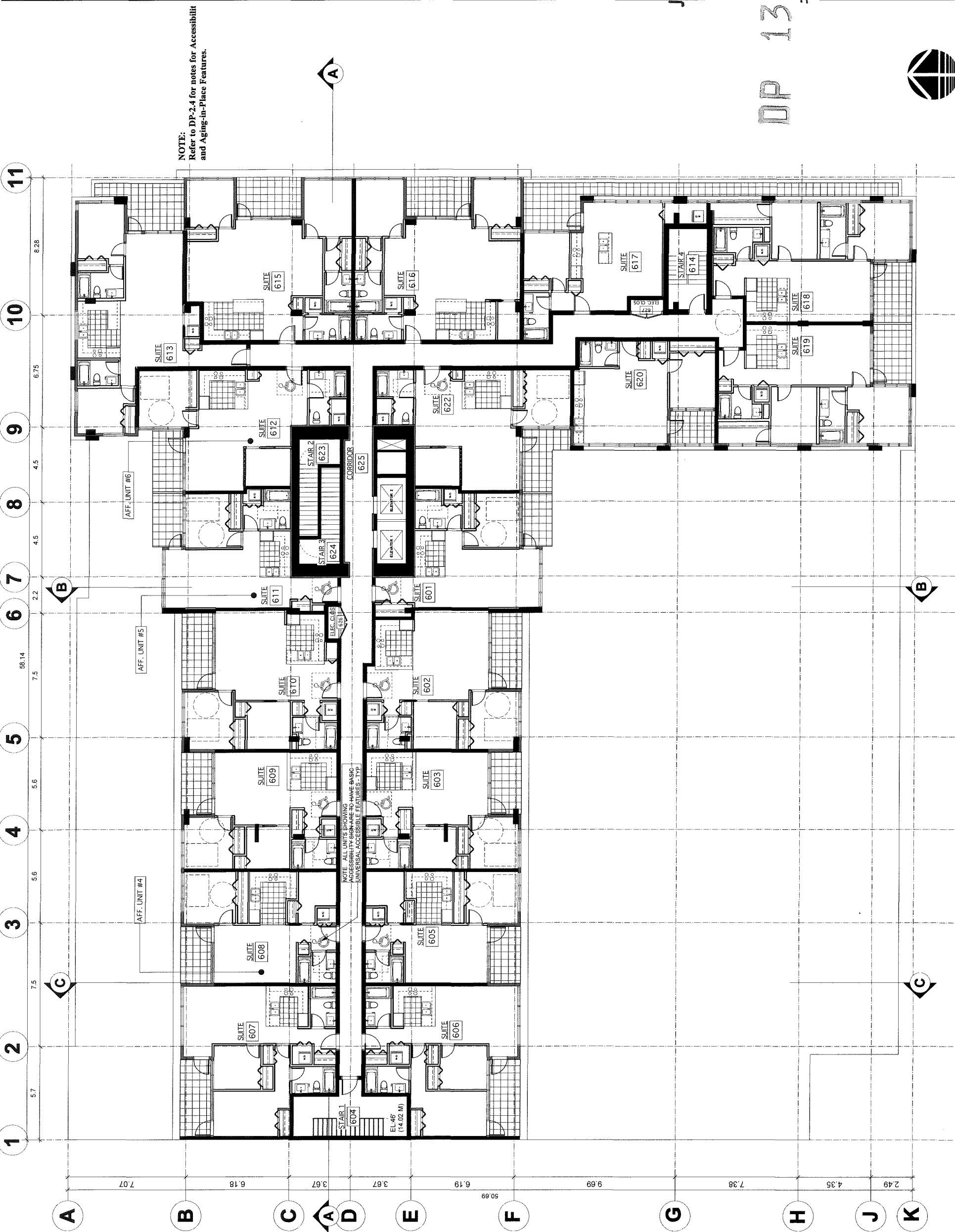
notes	revisions	consultants
JAN 29 2015		

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suite 410
639 West 21st Ave.
Vancouver, BC
V6C 5R6
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fax (604) 685-1889

project	8151 ANDERSON ROAD RICHMOND		
title	LEVEL 5 PLAN		
project number	21212	file name	
drawn	sl	checked	ac
scale	1:100	drawing number	DP 2.5

This drawing was created using an Apple Macintosh computer.



DP 13-645036
#6

NOTES

revisions

SEP. 4, 2013 ISSUED FOR DP

SEP. 12, 2014 ISSUED FOR DP

SEP. 15, 2014 ISSUED FOR DP

SEP. 16, 2014 ISSUED FOR DP

DEC. 8, 2014 ISSUED FOR DP

DEC. 16, 2014 ISSUED FOR DP

JAN. 25, 2015 ISSUED FOR DP

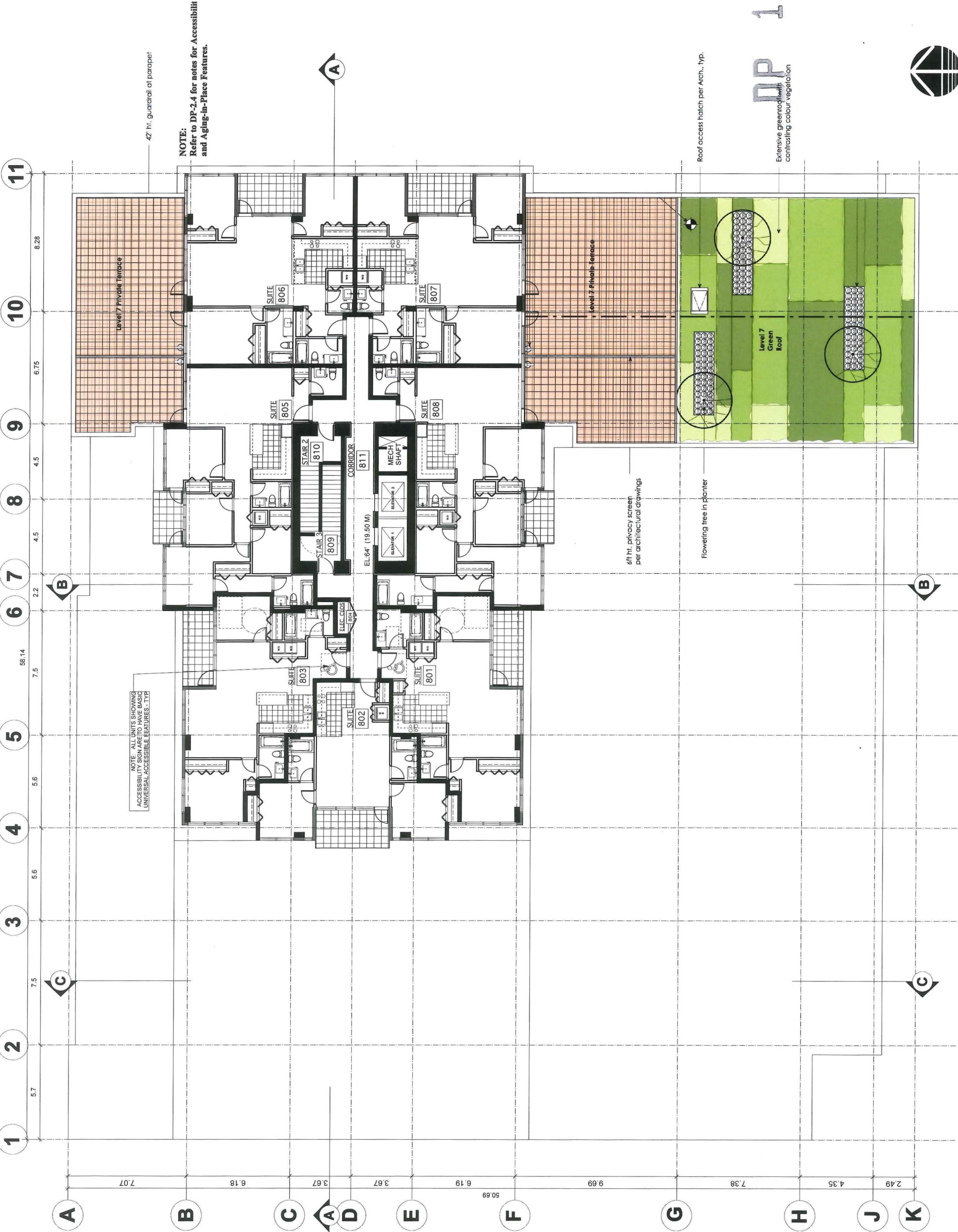
CONSULTANTS

JAN 29 2015

ANDREW CHEUNG
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13-645286
#8

project	8151 ANDERSON ROAD RICHMOND
file name	LEVEL 7 PLAN
project number	21212
drawn	si
checked	ac
scale	1:100
drawing number	DP 2.7



NOTES	revisions	DATE
	SEP. 4, 2013	ISSUED FOR DP
	SEP. 4, 2014	ISSUED FOR DP
	SEP. 24, 2014	ISSUED FOR DP
	DEC. 8, 2014	ISSUED FOR DP
	DEC. 16, 2014	ISSUED FOR DP
	JAN. 29, 2015	ISSUED FOR DP
CONSULTANTS		
JAN 29 2015		

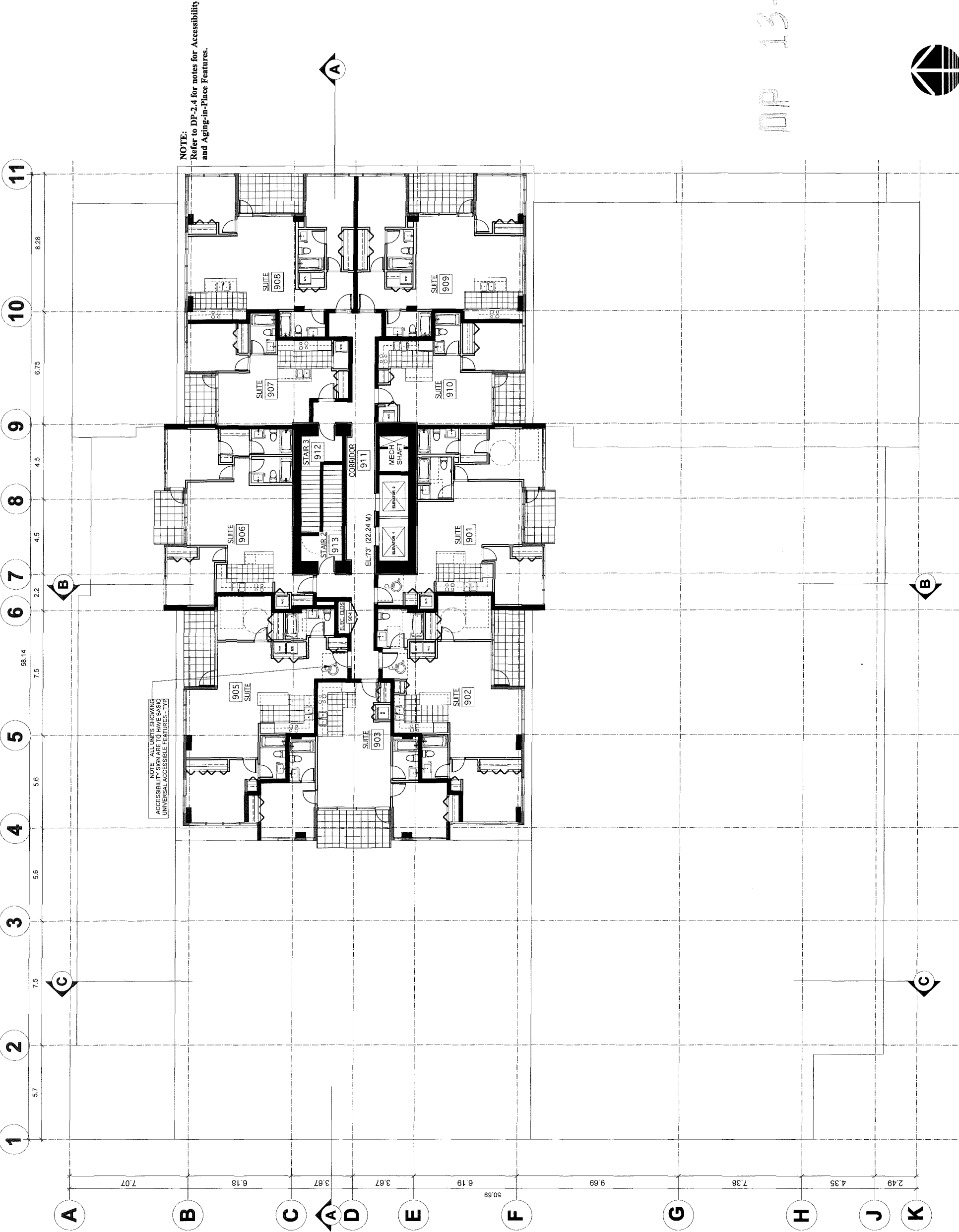
ANDREW CHEUNG
ARCHITECTS INC.

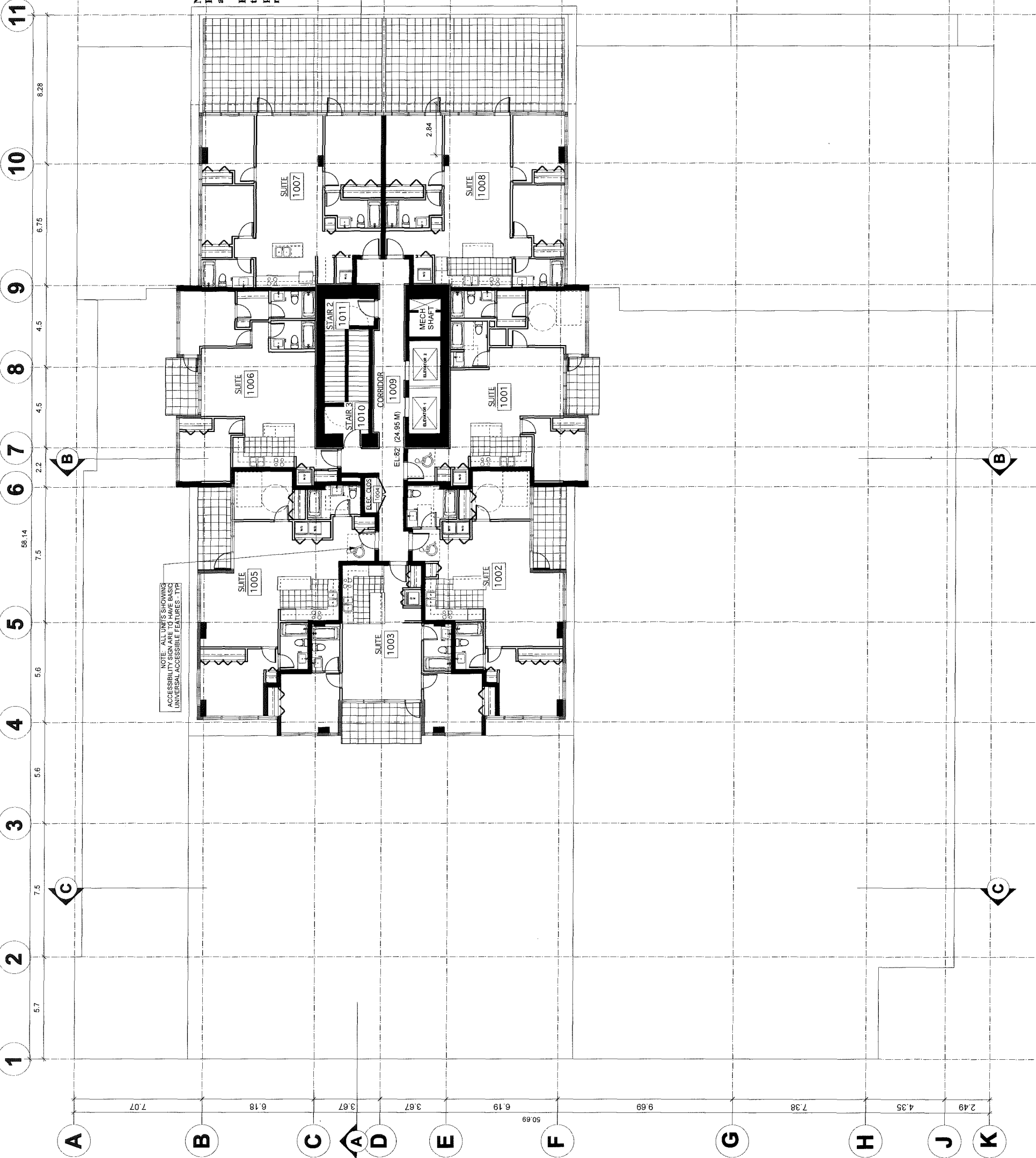
DP 13-645200
#9
style #10
1635 west 2nd ave
vancouver b.c
v6j 1t5
tel (604) 685-2088
fax (604) 685-1889

PROJECT		
8151 ANDERSON ROAD RICHMOND		
TYPE		
LEVEL 8 PLAN		
PROJECT NUMBER	FILE NAME	
21212		
DRAWN	CHECKED	SCALE
sl	ac	1:100
DRAWING NUMBER		
DP 2.8		



THIS DRAWING WAS CREATED USING A/CAD/RENDER/COMPASS





NOTE:
Refer to DP-2.4 for notes for Accessibility
and Aging-in-Place Features.
Floor plan on this level has an area of less
than 650 SM (Above 25 M, Geodetic), as
per City of Richmond OCP/CCAP
requirements.

notes	
date	revisions
SEP. 4, 2013	ISSUED FOR DP
SEP. 16, 2014	ISSUED FOR DP
SEP. 24, 2014	ISSUED FOR DP
DEC. 8, 2014	ISSUED FOR DP
DEC. 16, 2014	ISSUED FOR DP
JAN. 25, 2015	ISSUED FOR DP
consultants	
JAN 29 2015	

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#10

DP 13-64286

project	
8151 ANDERSON ROAD RICHMOND	
date	
LEVEL 9 PLAN	
project number	file name
21212	
drawn	checked
sl	ac
scale	1:100
drawing number	
DP 2.9	



notes

date

revisions

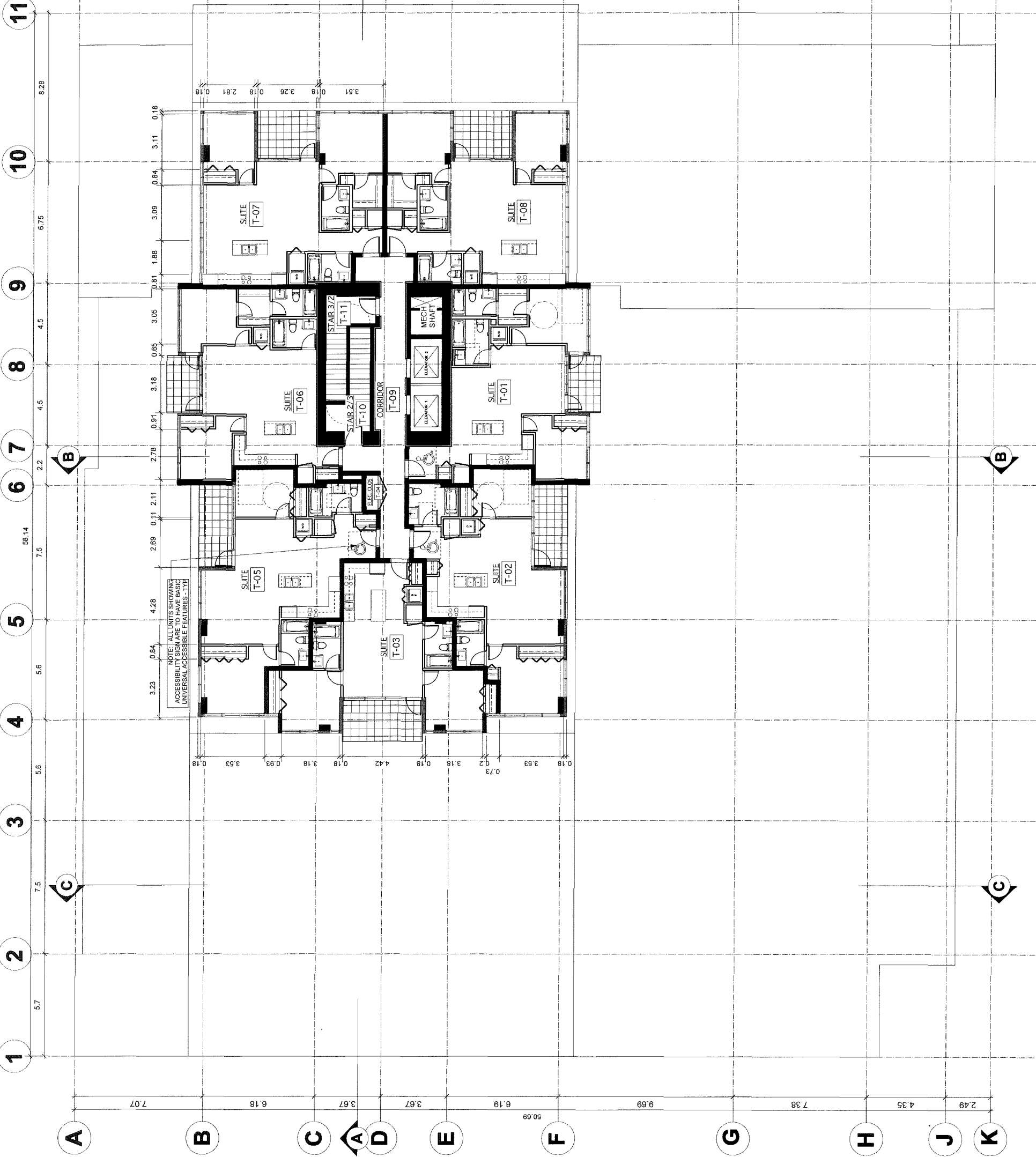
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JUL. 18, 2014 ISSUED FOR DP
SEP. 24, 2014 ISSUED FOR DP
DEC. 16, 2014 ISSUED FOR DP
DEC. 16, 2014 ISSUED FOR DP
JAN. 29, 2015 ISSUED FOR DP

consultants

JAN 29 2015

NOTE:
Refer to DP-2.4 for notes for Accessibility
and Aging-in-Place Features.

Floor plan on these levels have an area of
less than 650 SM (Above 25 M, Geodetic),
as per City of Richmond OCP/CCAP
requirements.



ANDREW CHEUNG
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DP 13-645286
#11

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Fax: (604) 885-1889

Project 8151 ANDERSON ROAD RICHMOND	
Title LEVEL 10-14 PLANS	
Project number 21212	File name
Drawn SI	Checked AC
Scale 1:100	
Drawing number DP 2.10	



The drawings are issued under the name of Andrew Cheung Architects Inc.

NOTES

date

revisions

SEP. 4, 2013

ISSUED FOR DP

SEP. 4, 2014

REVISION FOR DP

SEP. 24, 2014

ISSUED FOR DP

DEC. 8, 2014

ISSUED FOR DP

DEC. 16, 2014

ISSUED FOR DP

JAN. 29, 2015

ISSUED FOR DP

consultants

JAN 29 2015

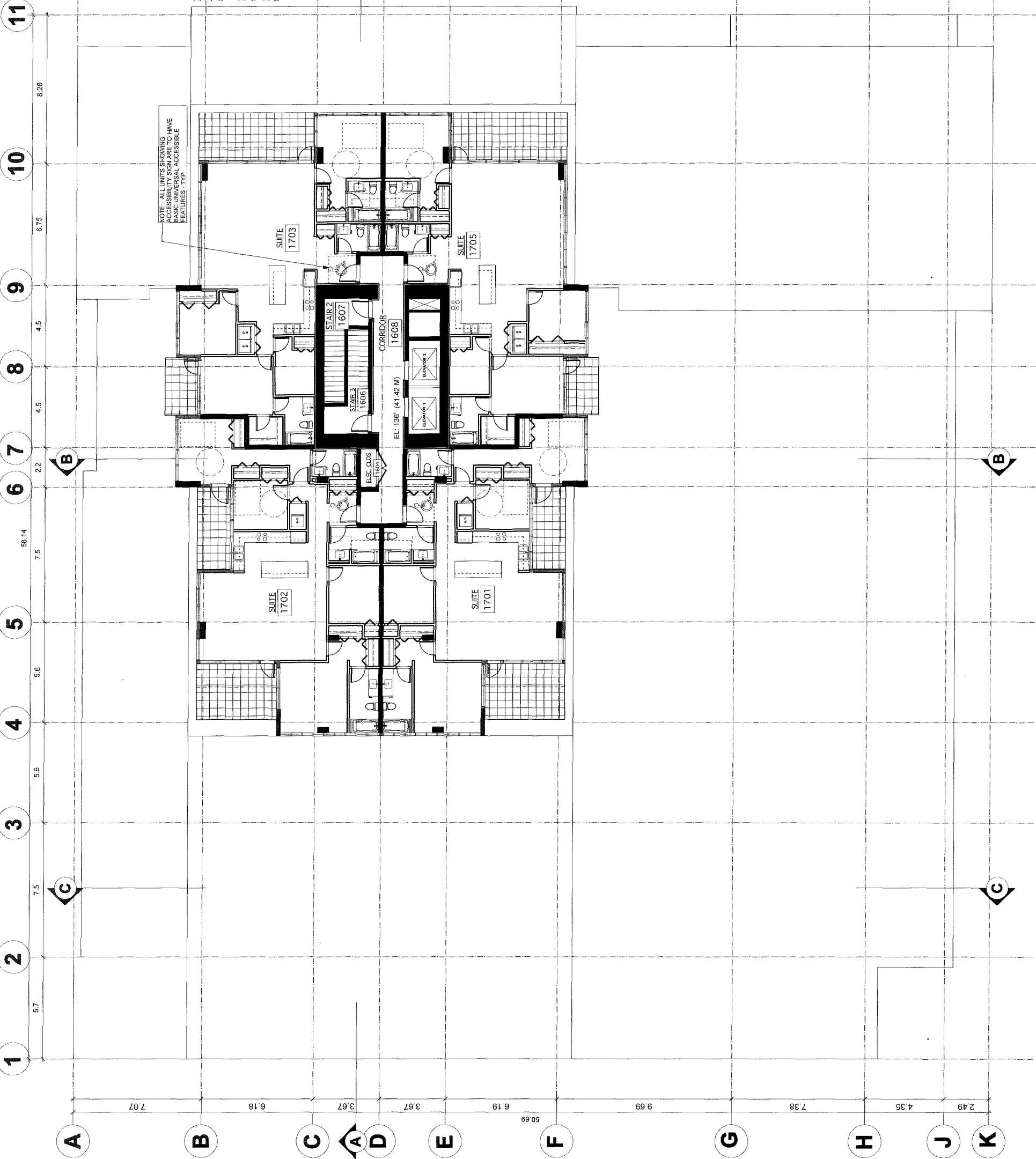
ANDREW CHEUNG
ARCHITECTS INC.

DP 13-645206

#12

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Vancouver, B.C.
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tel (604) 685-2088
fax (604) 685-1889

project 8151 ANDERSON ROAD RICHMOND			
title LEVEL 15 PLAN			
project number 21212	file name		
drawn sl	checked ac	scale 1:100	drawing number DP 2.11
Notes: All drawings are the property of Andrew Cheung Architects Inc. and shall remain the property of Andrew Cheung Architects Inc. and shall not be used for any other project without the written consent of Andrew Cheung Architects Inc.			



notes

revisions

SEP. 4, 2013 ISSUED FOR DP
JUL. 18, 2014 ISSUED FOR DP
SEP. 24, 2014 ISSUED FOR DP
DEC. 15, 2014 ISSUED FOR DP
DEC. 15, 2014 ISSUED FOR DP
JAN. 29, 2015 ISSUED FOR DP

consultants

JAN 29 2015

ANDREW CHEUNG
ARCHITECTS INC.

13-645286

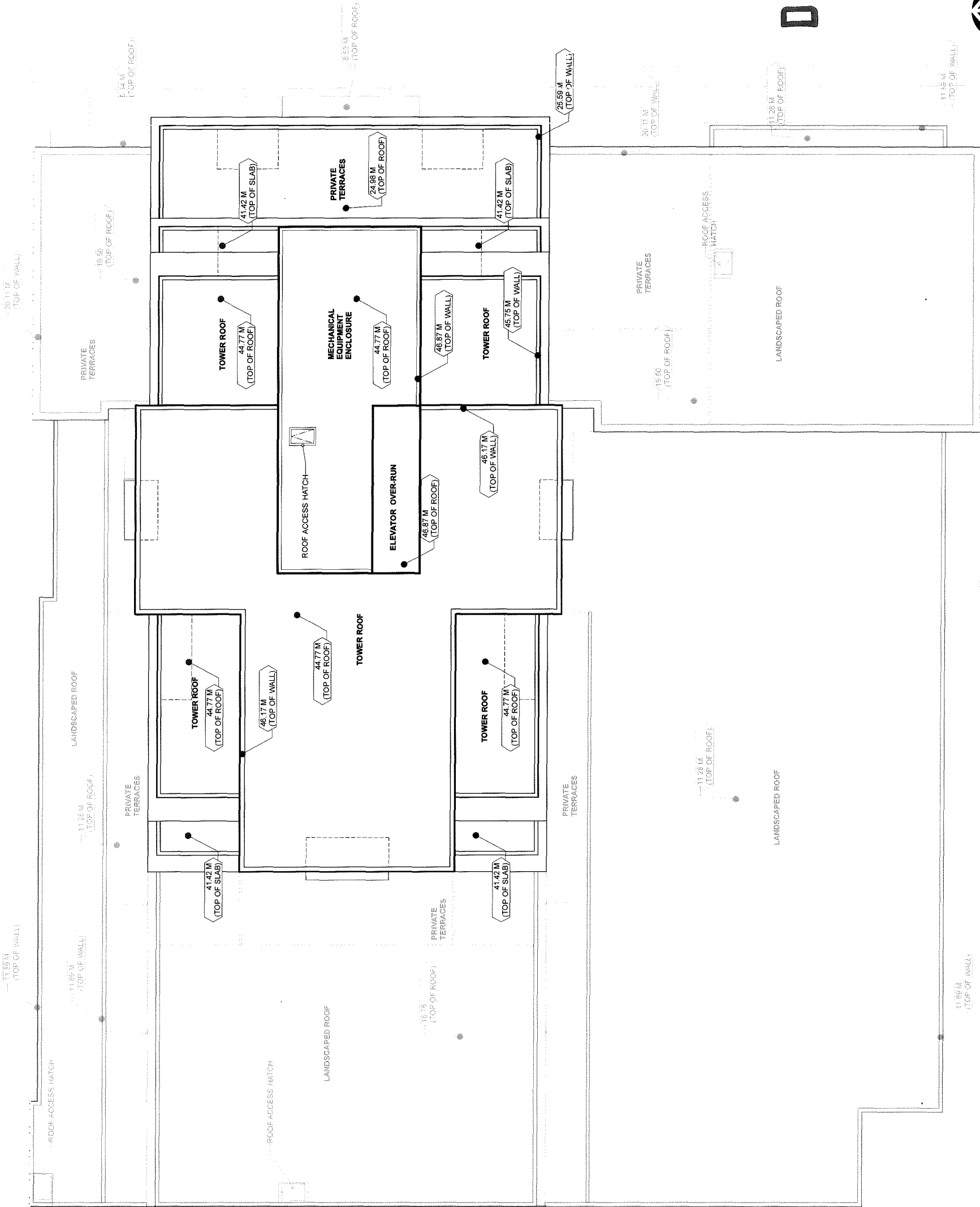
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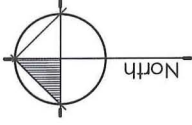
168 West 2nd Ave.
Vancouver, B.C.
V6J 1H3
Tel: (604) 685-0988
Fax: (604) 685-1889

Project 8151 ANDERSON ROAD RICHMOND		Title ROOF PLAN	
Project number 21212	File name	Scale 1:100	Drawing number DP 2.12
Drawn SI	Checked SC	The drawings are prepared using AutoCAD software and are for informational purposes only. They are not to be used for construction without the written consent of the architect.	



DP





durantekreuk

kp

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Vancouver BC V6J 1N5
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JAN 29 2015

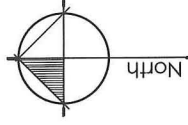
Drawing Title:

941#

L-2 of 7

Landscape Lighting Legend

 Wall Light Bollard Lighting



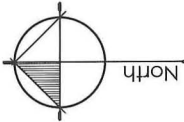
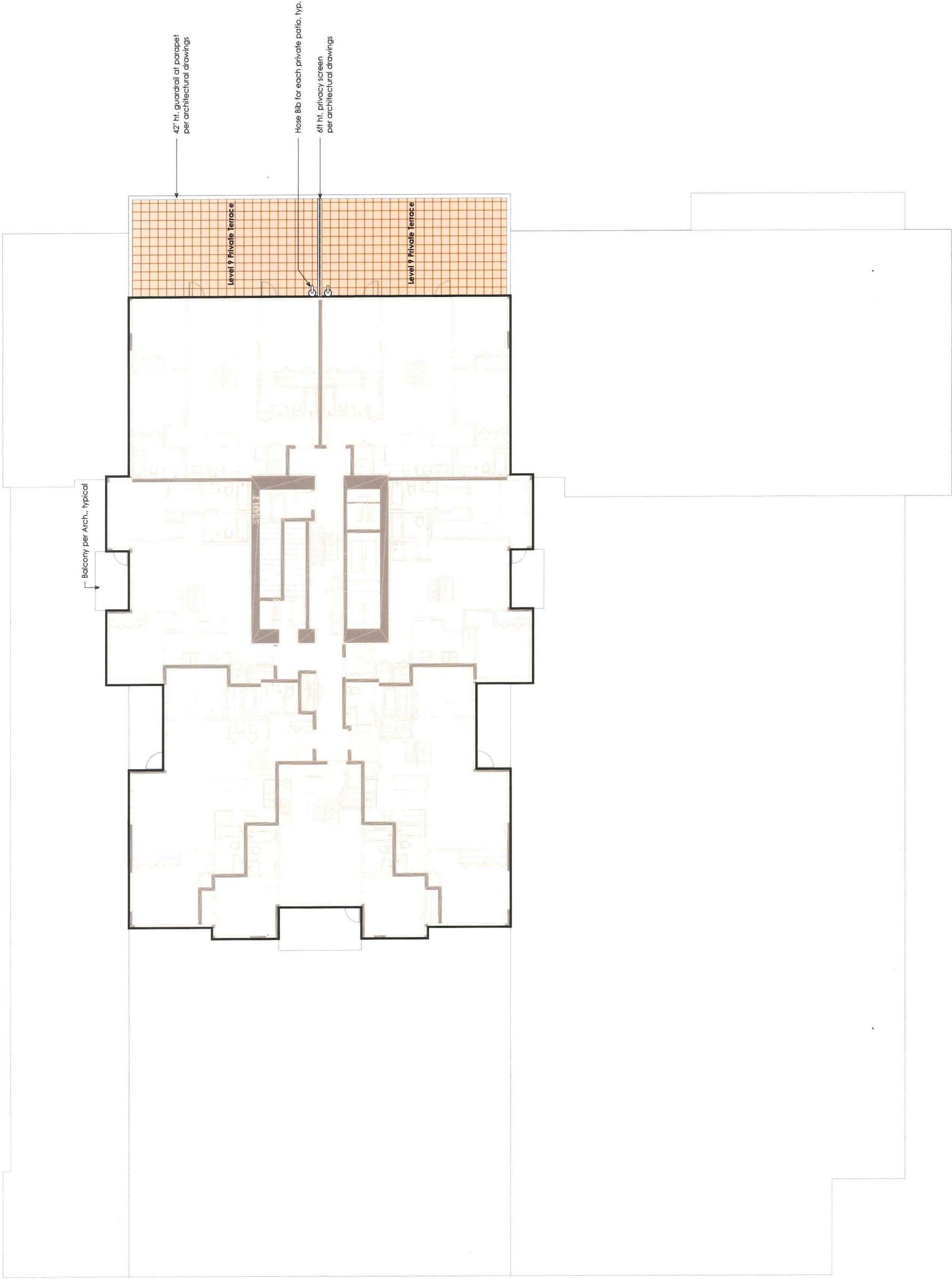
durantekreuk
dkp

JAN 29 2015

Drawing Title:

五十一

Sheet No.:



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07	Dec 16-14	Re-issued for DP
06	Dec 04-14	Re-issued for DP
05	Sept 24-14	Re-issued for DP
04	July 18-14	Re-issued for DP
03	May 06-14	Re-issued for DP
02	Sept 04-13	Issued for DP
01	Aug 07-13	Issued for Review
no.: date:		Item:

Revisions:

pk
durantekreuk
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Vancouver BC V6J 1N5
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Project:

8151 Anderson Road

JAN 29 2015

Drawn by:	EN
Checked by:	AL
Date:	August 07, 2013
Scale:	1/8" = 1'-0"
Drawing Title:	

Level 9
Landscape Plan
DP 13-645286

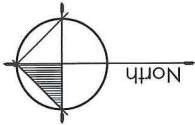
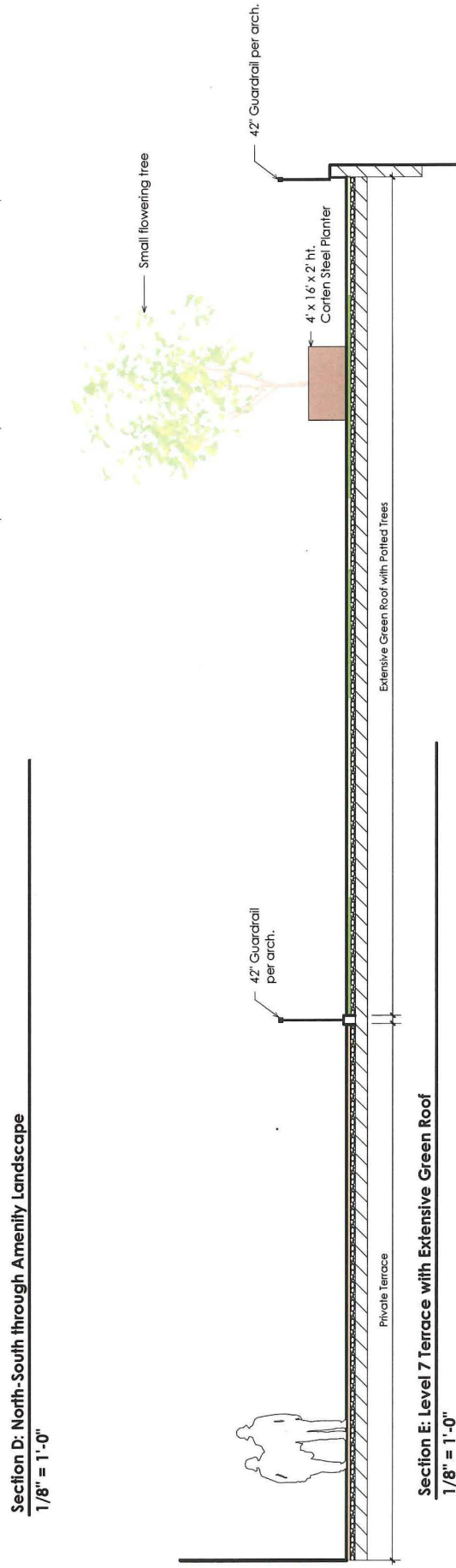
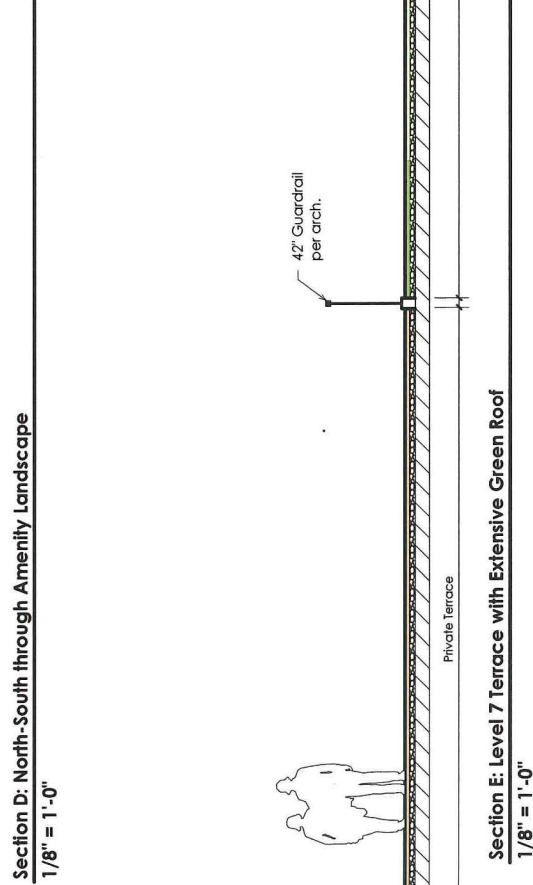
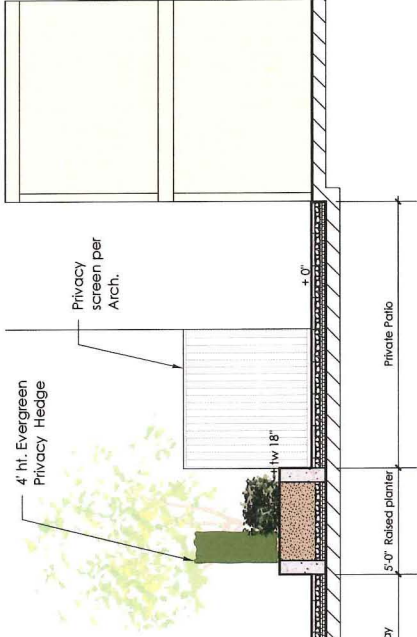
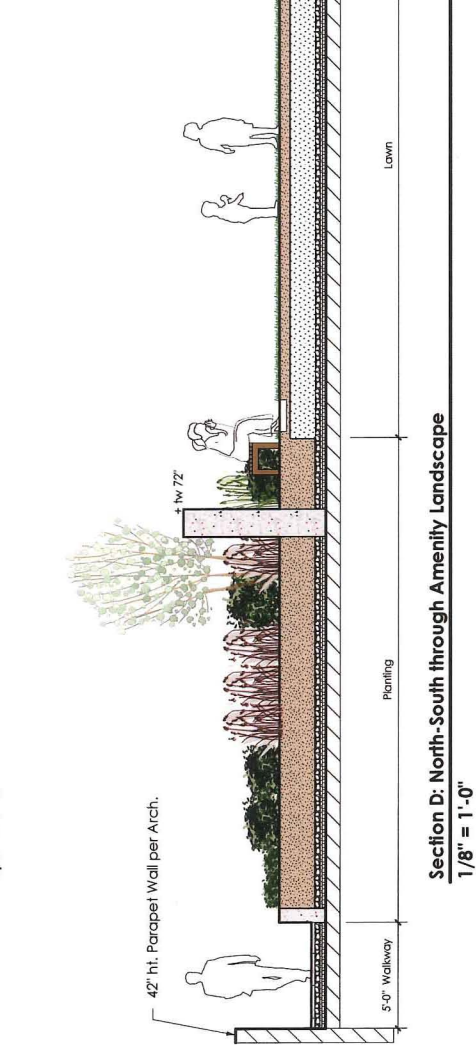
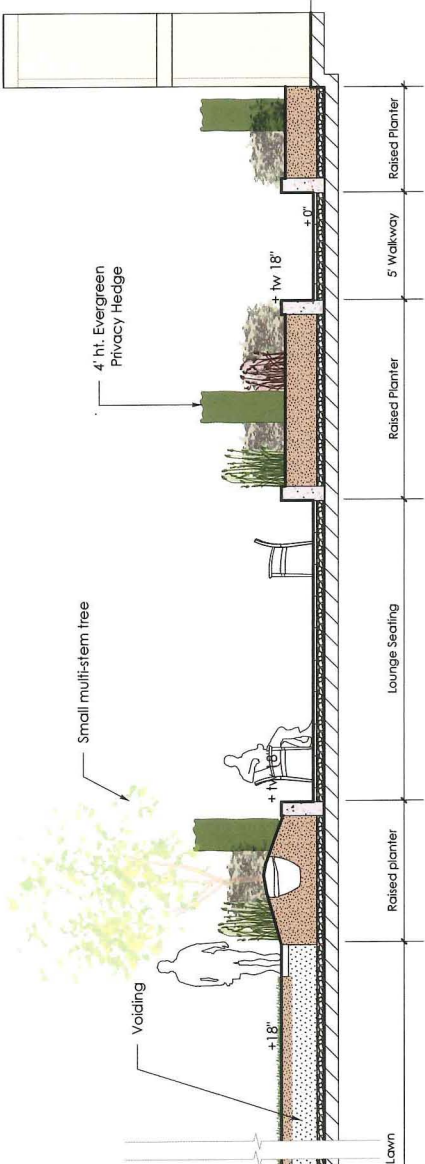
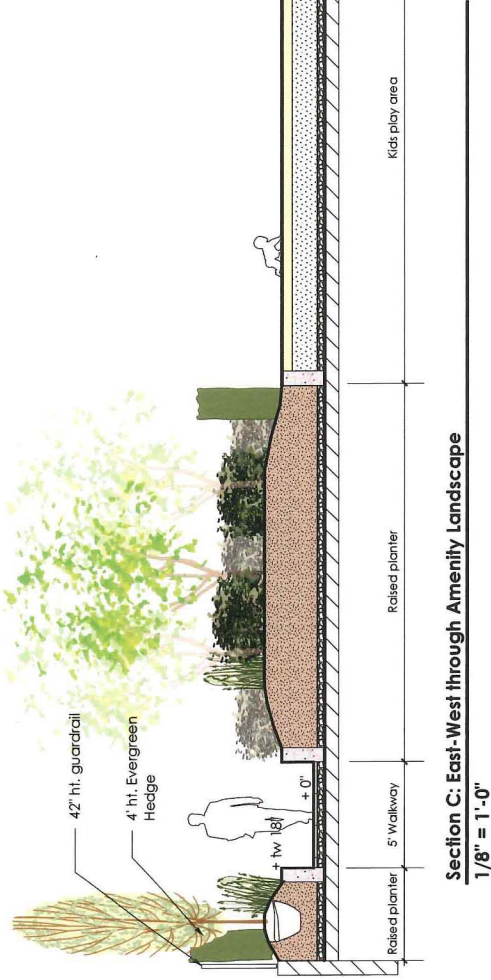
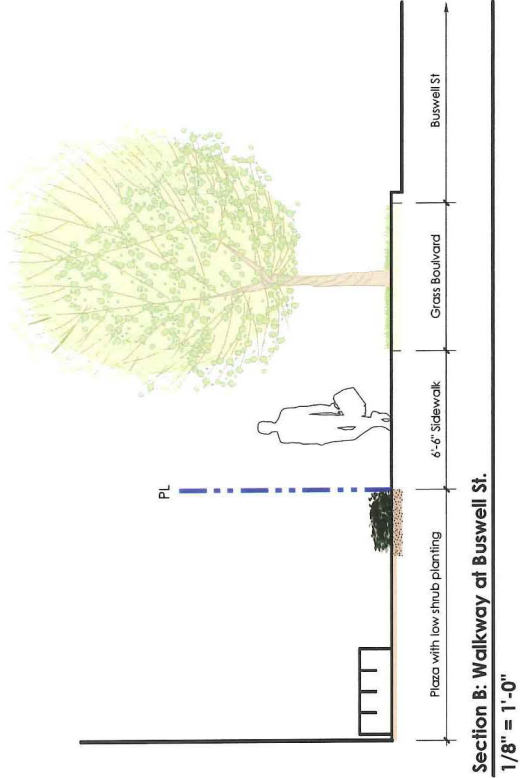
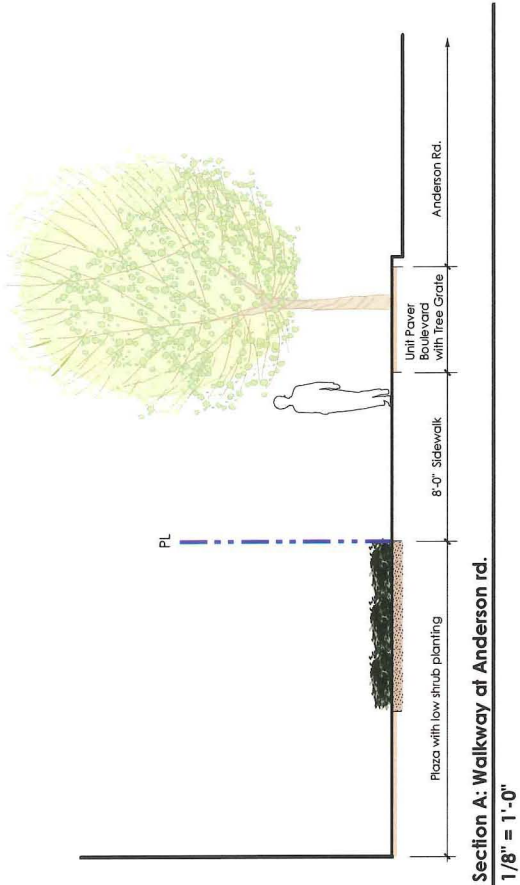
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Project No.:

13048

Sheet No.:

L-4 of 7



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07	Dec 15-14	Re-issued for DP
06	Dec 04-14	Re-issued for DP
05	Sept 24-14	Re-issued for DP
04	July 18-14	Re-issued for DP
03	May 06-13	Re-issued for DP
02	Sept 04-13	Issued for DP
01	Aug 07-13	Issued for Review
no.:	date:	item:

Revisions:

dk
Durante Kreut Ltd.
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Project:

8151 Anderson Road

JAN 29 2015

Drawn by: EN

Checked by: AL

Date: August 07, 2013

Scale: 1/4" = 1'-0"

Drawing Title:

Landscape Sections

DP 13-645286
#14e

Project No.:

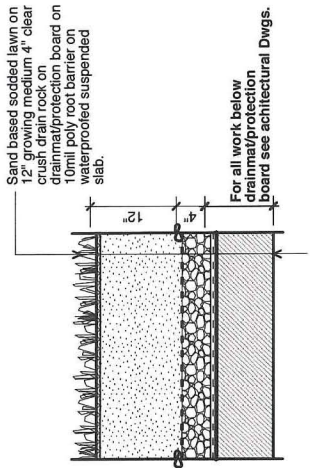
13048

Sheet No.:

Section E: Level 7 Terrace with Extensive Green Roof

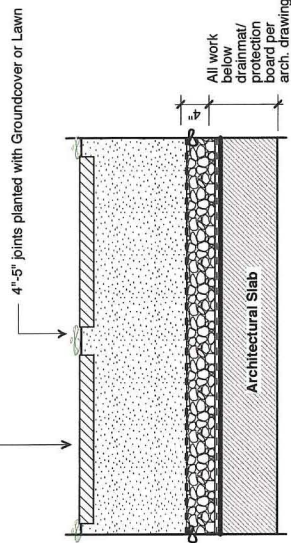
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L-5 of 7



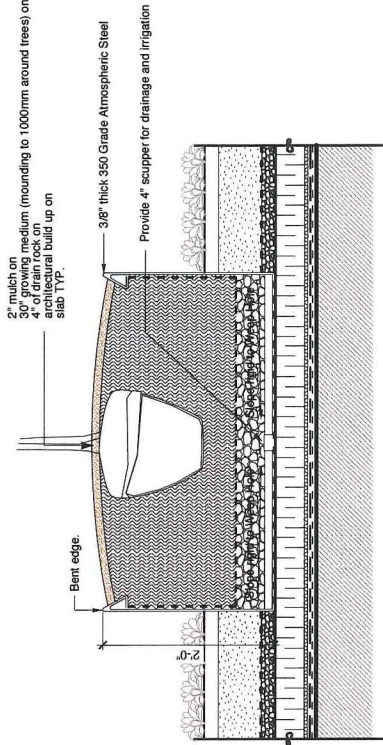
LD-1 Sodded Lawn on Slab
Scale 1" = 1'-0"

2ft Square Hydrapressed paving slabs, set min 12" above finish grade (refer to Sheet LD2 & LD3 for layout) set on 1" Type III growing medium on filter cloth on 4" drain rock on 10mil poly root barrier on waterproofed suspended slab.

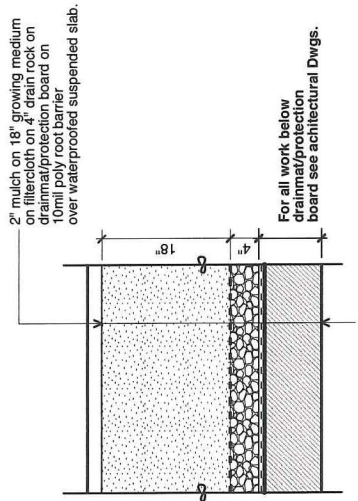


LD-5 Natural Stepping Stones with Planted Joints
Scale 1" = 1'-0"

Typical Metalwork Notes:
1. Submit shop drawings for all metalwork FOR REVIEW AND APPROVAL PRIOR TO ASSEMBLY in accordance with Section 05500.
2. All metalwork to be galvanized to meet the requirements of the manufacturer's instructions for powder coating. Field touch ups and on site welding.
3. Metal Planter to be all-weld construction. Ensure all weld faces are ground smooth.

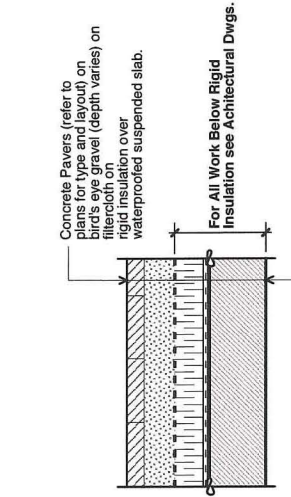


LD-9 Atmospheric Steel Planters with Trees
Scale 1" = 1'-0"

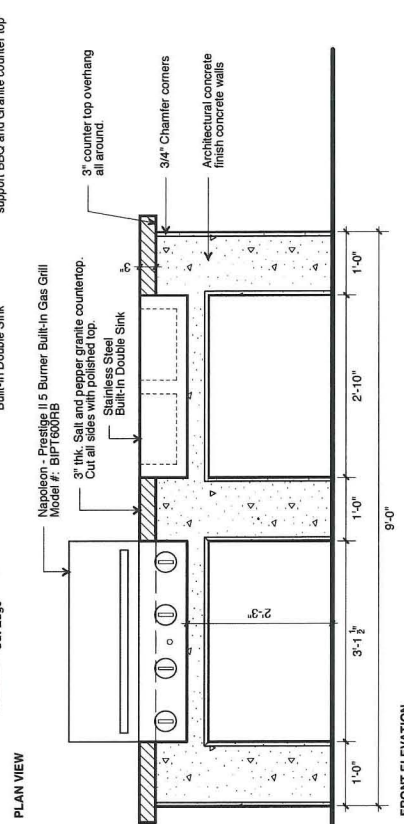
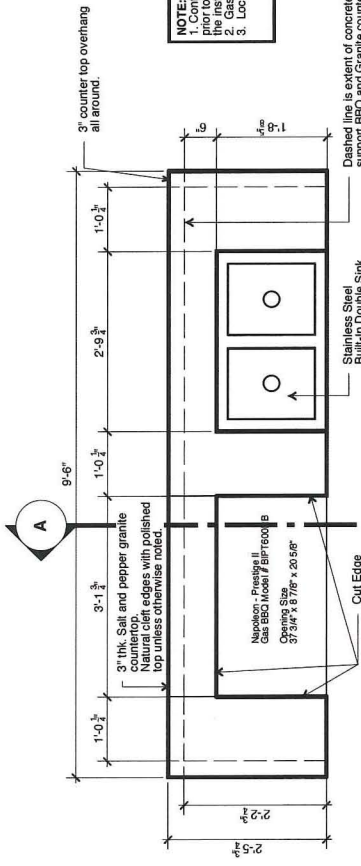


LD-2 Shrub Planting on Slab - Over Parkade
Scale 1" = 1'-0"

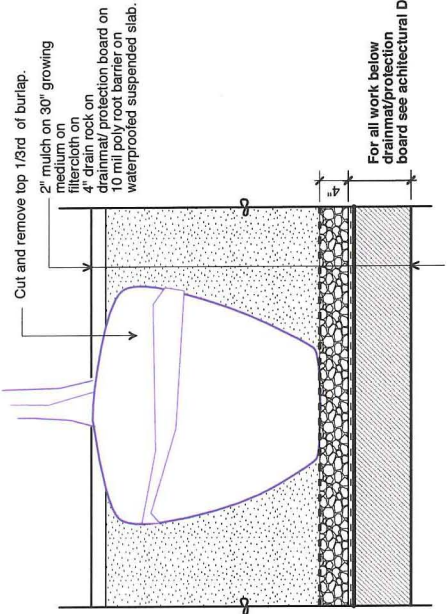
Note:
Cut all Pavers as required to maintain 12" above finish grade (refer to Sheet LD2 & LD3 for layout) set on 1" Type III growing medium on filter cloth on 4" drain rock on 10mil poly root barrier on waterproofed suspended slab. Saw, no guillotine cuts will be allowed.



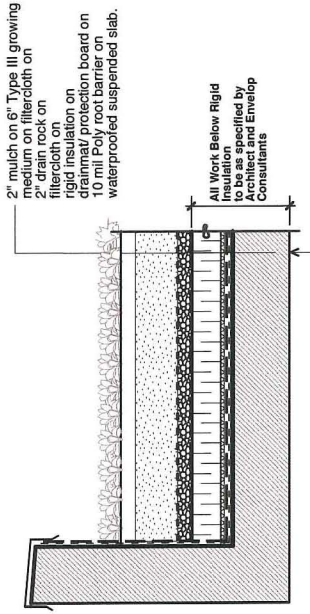
LD-6 Hydrapressed Concrete Paving Slab on Inverted Roof Slab
Scale 1" = 1'-0"



LD-10 Outdoor Kitchen with Gas BBQ
Scale 3/4" = 1'-0"

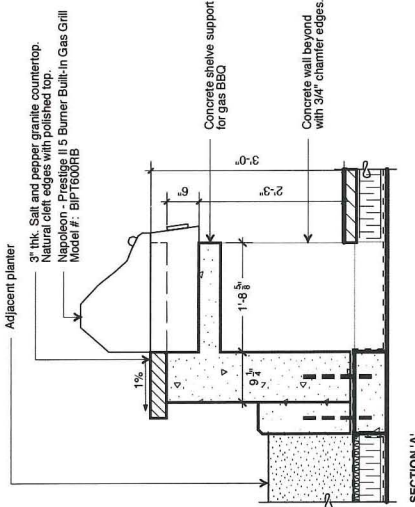


LD-3 Typical Tree Planting on Slab - Over Parkade
Scale 1" = 1'-0"



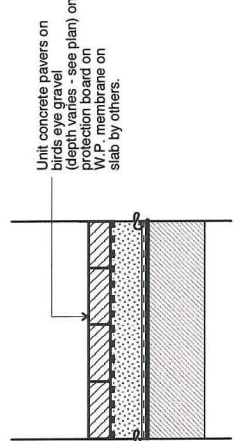
LD-7 Low Soil Planter on Slab - Inverted Roof
Scale 1" = 1'-0"

NOTE:
1. Contractor to review the BIP1600RB Assembly Procedure prior to forming and pouring BBO opening and base, to ensure that the BBO opening and base is formed and poured in accordance with the BIP1600RB Assembly Procedure.
2. Gas BBQ shall be as specified or pre-approved equivalent.
3. Locate valve box with regulator within planting bed.

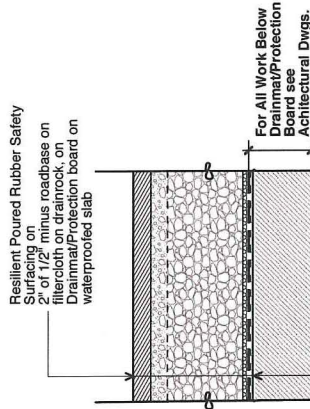


SECTION 'A'

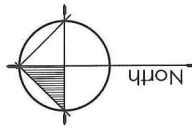
Note: Refer to Specifications for paving pattern and colour.



LD-4 Unit Concrete Pavers on Slab
Scale 1" = 1'-0"



LD-8 Resilient Rubber Tile Safety Surfacing - on Slab
Scale 1" = 1'-0"



08	Jan 29-15	Re-issued for DP
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06	Dec 04-14	Re-issued for DP
05	Sept 24-14	Re-issued for DP
04	July 18-14	Re-issued for DP
03	May 06-14	Re-issued for DP
02	Sept 04-13	Issued for DP
01	Aug 07-13	Issued for Review
no.:	date:	item:

Revisions:



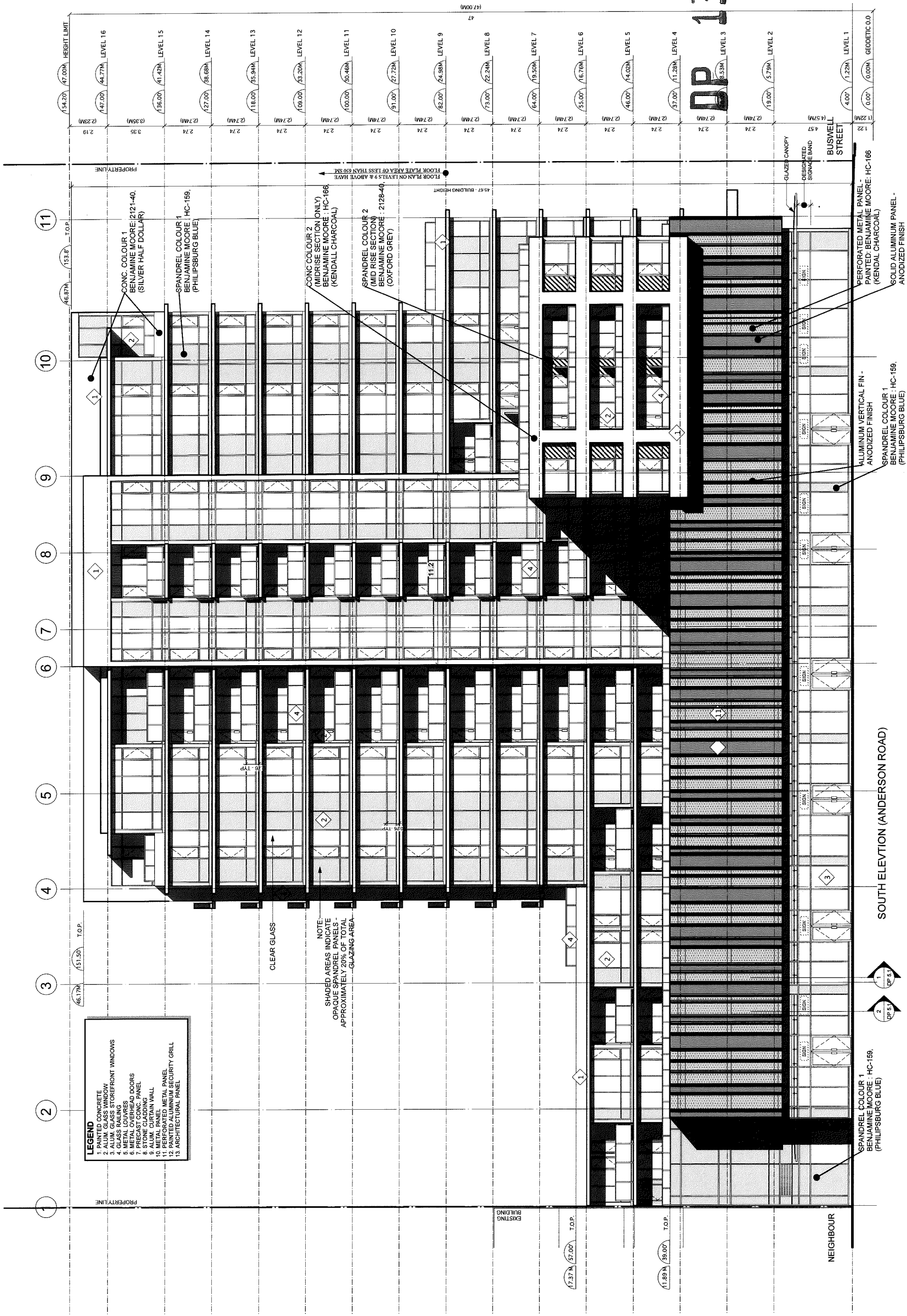
Project:
8151 Anderson Road

JAN 29 2015

Drawn by:	SF
Checked by:	AL
Date:	May 01, 2014
Scale:	As Noted
Drawing Title:	

Landscape Details

DP 13-645286
#149
Project No.: 13048
Sheet No.:



date	revisions
SEP 4, 2013	ISSUED FOR DRP
JUL 16, 2014	ISSUED FOR DRP
SEP 24, 2014	ISSUED FOR DRP
DEC 8, 2014	ISSUED FOR DRP
JAN. 29, 2015	ISSUED FOR DRP

consultants

JAN 29 2015

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project		8151 ANDERSON ROAD RICHMOND	
title		SOUTH ELEVATION - ANDERSON ROAD	
project number	file name	scale	drawing number
21212		1 : 100	DP 3.1
drawn sl	checked bc	<p>Client approved. This drawing is submitted for and will not be reviewed for compliance with the applicable regulations. It is not intended to be used for construction without the approval of the relevant authority.</p>	

This drawing was created using an Apple Macintosh® computer.

date	revisions
SEP 4, 2013	ISSUED FOR DP
JUL 18, 2014	ISSUED FOR DP
SEP 24, 2014	ISSUED FOR DP
DEC 6, 2014	ISSUED FOR DP
JAN 28, 2015	ISSUED FOR DP

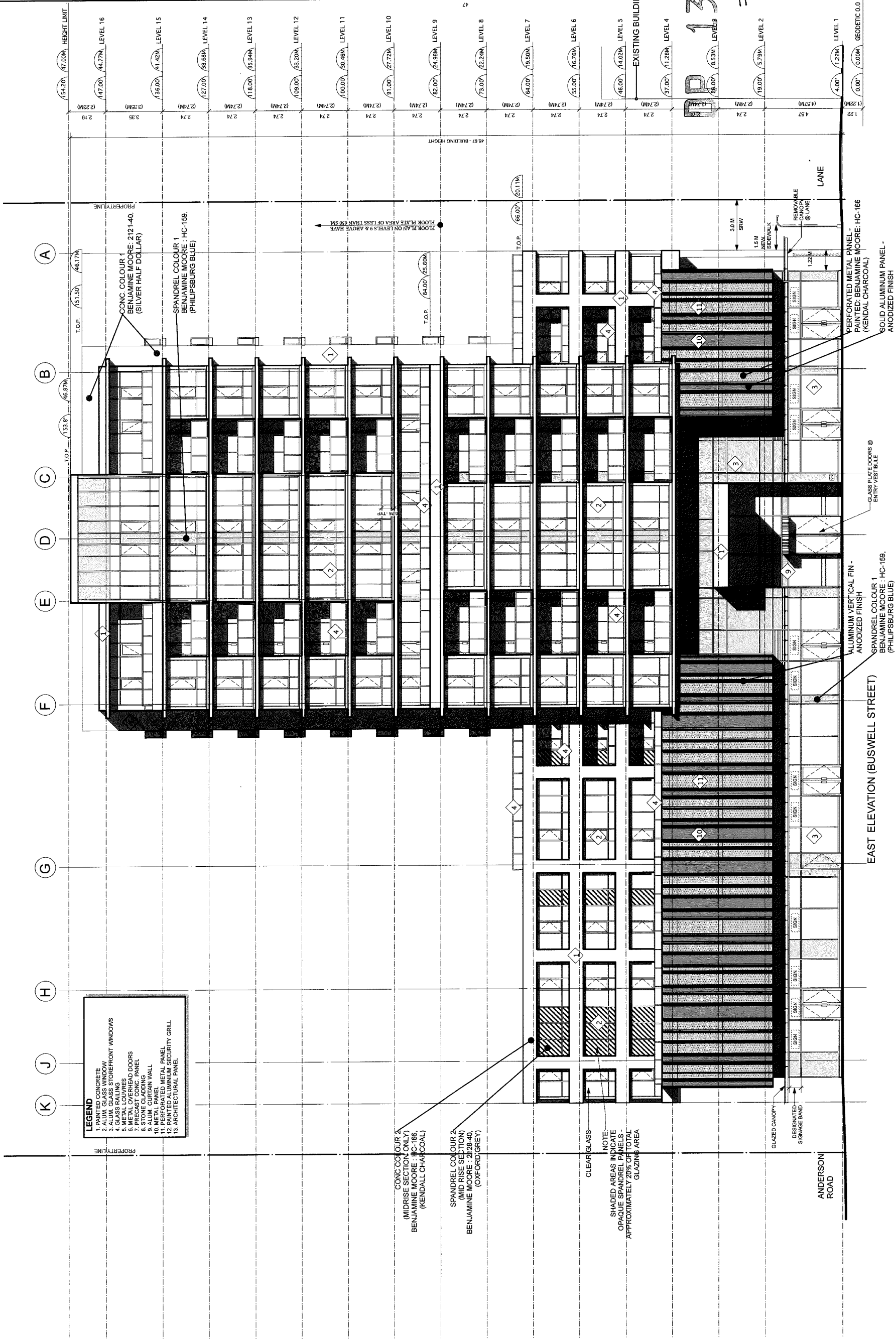
consultants

JAN 29 2015

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v6j 1t5

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fax (604) 685-1889

project	8151 ANDERSON ROAD RICHMOND		
title	EAST ELEVATION - BUSWELL STREET		
project number	21212	checked ac	scale 1 : 100
drawn sl			drawing number DP 3.2



The division was created under an Apple Macintosh computer.

notes

revisions

SEP 4, 2013	ISSUED FOR DP
JUL 18, 2014	ISSUED FOR DP
SEP 24, 2014	ISSUED FOR DP
DEC 8, 2014	ISSUED FOR DP
JAN 29, 2015	ISSUED FOR DP

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JAN 29 2015

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project
8151 ANDERSON ROAD
RICHMOND

file
NORTH ELEVATION -
LANE

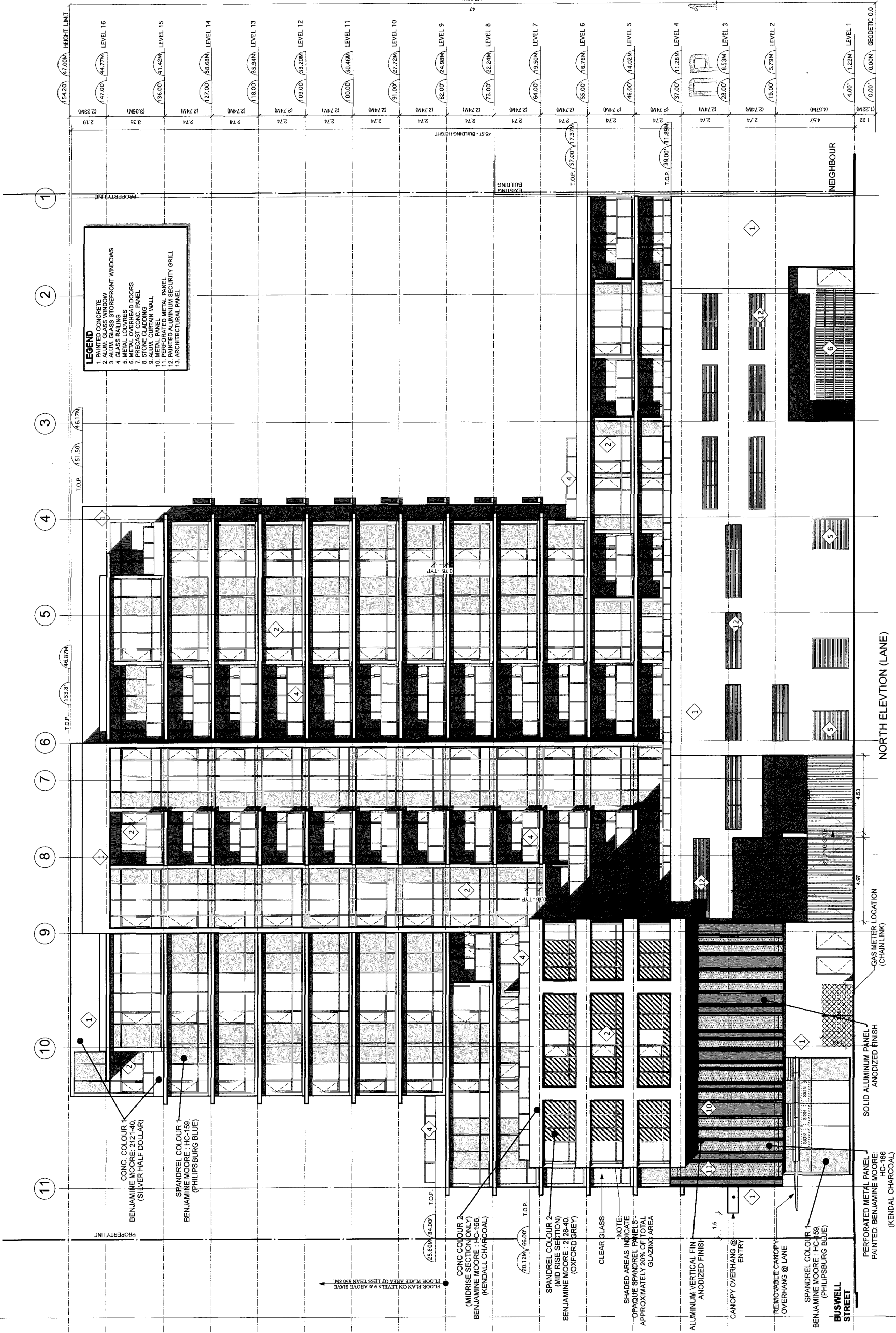
project number
21212

file name

checked
ac

scale
1 : 100

drawing number
DP 3.3



8151 ANDERSON RD, RICHMOND, BC
15 STORY, MIXED USE BUILDING

PROJECT DATA:	
1. CIVIC ADDRESS:	8151 ANDERSON ROAD, RICHMOND, B.C.
2. LEGAL DESCRIPTION:	A SEC 9 BLK4N RG&W PL 6789 (J96195E).
3. ZONING CLASSIFICATION:	CDT-1
4. SITE AREA:	3484.4 m ²
5. FLOOR AREA RATIO (F.A.R.):	
Allowable F.A.R.:	
3.0 (base) + 0.15 (aff. housing) =	<u>3.15</u>
Proposed F.A.R.:	
2.831 (9,864.1 m ² Res.)+0.338(1,176.7 m ² Comm.)+0.027(94.9 m ² BUHU) =	<u>3.142</u>
6. AFFORDABLE HOUSING :	
Req'd Aff. Housing : 0.05 x 9,864.1 SM=493.2 SM	
Proposed Aff. Housing: 8 Units, 506.4 SM	
7. BASIC UNIVERSAL HOUSING UNITS (BUHU):	
Req'd BUHU: (0.05 x 103 Res. Units) +(1 x 8 Aff. Housing Units) =	6+8= <u>14</u>
Proposed BUHU=	<u>55</u>
8. AGING IN PLACE FEATURES:	
Proposed: 100%	<u>111</u>
9. SITE COVERAGE:	
Allow. Lot Coverage: <u>90%</u>	
Proposed: 2,815 m ² (Coverage) / 3,464 m ² (Lot Area) =	<u>81.3 %</u>
10. CAR PARKING:	
Resident parking required (1 x 103 units)+(0.9 x 8 AFF.H. Units) =	<u>111</u>
Visitor parking required (0.2x111 units) =	<u>23</u>
Retail parking required on first 2 floors: (1177 /100 X 3.75) =	<u>45</u>
Total parking required* : 111 + 45 =	<u>156</u>
Total parking provided :	<u>162</u>
LEVEL 1: 0 Small + 18 Standard + 1 Accessible =	19
LEVEL 2: 31 Small + 33 Standard + 1 Accessible =	65
LEVEL 3: 40 Small + 35 Standard + 3 Accessible =	78
Totals: 71 Small + 86 Standard + 5 Accessible =	162
* 12 visitor parking spaces are shared with retail parking, hence not counted towards total.	
11. BICYCLE PARKING:	
Class I Req'd: [111 Res. X 1.25] + [(1,177 M ² Retail/100) X 0.27] =	<u>143</u>
Class I provided: (88 on Level 2 + 63 on Level 3, all Horizontal stalls)=	<u>151</u>
Class II Req'd: [111 Res. X 0.2] + [(1,177 M ² Retail/100) X 0.4] =	<u>28</u>
Class II provided:	<u>28</u>
12. LOADING / GARBAGE REMOVAL:	
RESIDENTIAL(7.13.6.1): 111 Units:	1 Medium (SU-9)
COMMERCIAL(7.13.6.2): Gross Area=1,176.7 SM	1 Medium (SU-9)
(One loading bay shared by garbage/recycling removal bay)	

PROJECT DATA

8151 Anderson Rd.	1 BR AREA	1+ DEN AREA	2 BR AREA	3 BR AREA	CIRC. AREA	RETAIL AREA	NET RES. AREA	GROSS RES. AREA	AFF. H AREA	Univ. Access	BIKE C-1	BIKE C-2	CAR	TOTALS (SM)		
Level 1	0	0	0	0	0	158.25	1176.7	0	158.25	0	0	28	19	1334.99		
Level 2	0	0	0	0	0	0	0	0	0	0	88	65	0	0		
Level 3	0	0	0	0	0	0	0	0	0	0	63	78	0	0		
Level 4	2	118.12	8	516	6	494.65	0	132.98	0	1128.67	1261.65	3	190	8	0	1261.65
Level 5	4	227.4	8	519	7	567.47	0	125.85	0	1313.97	1439.82	3	190	10	0	1439.82
Level 6	4	217.86	2	140.59	8	671.0	0	91.14	0	1029.45	1120.59	2	126	6	0	1120.59
Level 7	0	0	0	3	249	4	444.31	48.59	0	693.21	741.8	0	2	0	0	741.8
Level 8	2	96.54	0	7	583.0	0	49.67	0	681.54	731.21	0	3	0	0	0	731.21
Level 9	0	0	0	5	417.28	2	195.30	36.76	0	612.58	649.34	0	0	3	0	649.34
Level 10	0	0	0	7	598.41	0	0	34.84	0	598.41	633.25	0	0	3	0	633.25
Level 11	0	0	0	7	598.41	0	0	34.84	0	598.41	633.25	0	0	3	0	633.25
Level 12	0	0	0	7	598.41	0	0	34.84	0	598.41	633.25	0	0	3	0	633.25
Level 13	0	0	0	7	598.41	0	0	34.84	0	598.41	633.25	0	0	3	0	633.25
Level 14	0	0	0	7	598.41	0	0	34.84	0	598.41	633.25	0	0	3	0	633.25
Level 15	0	0	0	0	0	0	4	564.64	31.36	0	596	0	0	4	0	596
Site Area																3484.4
Total Unit Count	12	18	71	10				9016.11	8	51					111	
Net Res. Area															9016.11	
Gross Res. Area								9864.91							9864.91	
Gross Total Area		661.92	1175.59	5974.35	1204.25	848.8	1176.7								11041.65	
FAR Calculation	(13.0 Base) + (0.15 Affordable Housing) = 3.15 x (Site Area: 3484.4) = 10,975.9 SM															
Allowable Area (With BUHU Density Exclusion)	(51 Basic Universally Accessible Units x 1.66 SM / Unit = 94.9 SM)															
Affordable Housing	(0.05 x Gross Residential Area = 0.05 x 9,864 SM = 493.2 SM)															
Parking Req'd by 7.7.2.1):	{103 Res. Units. x 1.5 = 154.5 -(55 AFF Units x 0.9 = 12 = 8) -(1176 sm Comm. x 3.75 = 45)} = 208															
Parking by City Centre Zone1	{111 Units x 0.2 = 22.2 - 23 Residential Visitor parking to be shared with Commercial}															
	{103 Res. Units. x 1.0 = 103 -(8 AFF Units x 0.9 = 12 = 8) -(1200 sm Comm. x 3.75 = 45)} = 156															
'123 Res. visitor spaces shared with retail parking)																
Total Parking Provided																
Electric Vehicle (EV) parking stalls	'required: 20 % of 156=32 total --- provided: 5 on Ground level+ 14 on 2nd floor+ 19 on 3rd floor= 38 total)															
Future EV parking stalls	'required: 25 % of 156=39 total --- provided: 30 on 2nd floor+ 16 on 3rd floor= 46 total)															
Total Bike- Class I	(111 Res. Units. x 1.25 = 139) -(1176 SM retail / 100= 12 x 0.27=3.18=0)=143															
Total Bike- Class II	(111 Res. Units. x 0.2=22.2) -(1177 SM retail / 100= 12 x 0.4=4.8=0)=28															
											151	28			151	
															162	
															506.4	
															208	
															156	
															162	
															506.4	
															38	
															46	
															151	
															28	
															28	

revisions	date	revisions
SEP. 4, 2013	ISSUED FOR DP	
JUL. 18, 2014	ISSUED FOR DP	
SEP. 24, 2014	ISSUED FOR DP	
DEC. 8, 2014	ISSUED FOR DP	
DEC. 16, 2014	ISSUED FOR DP	
JAN. 29, 2015	ISSUED FOR DP	
consultants		
JAN 29 2015		

ANDREW CHEUNG
ARCHITECTS INC.

DP 13-645086

reference

Project	8151 ANDERSON ROAD RICHMOND
PROJECT DATA	
PROJECT NUMBER	File name
21212	
drawn	checked
si	ac
scale	NA
drawing number	
DP 1.1	

notes

revisions

SEP. 4, 2013
ISSUED FOR DP
SEP. 23, 2014
ISSUED FOR DP
DEC. 8, 2014
ISSUED FOR DP
DEC. 16, 2014
ISSUED FOR DP
JAN. 23, 2015
ISSUED FOR DP

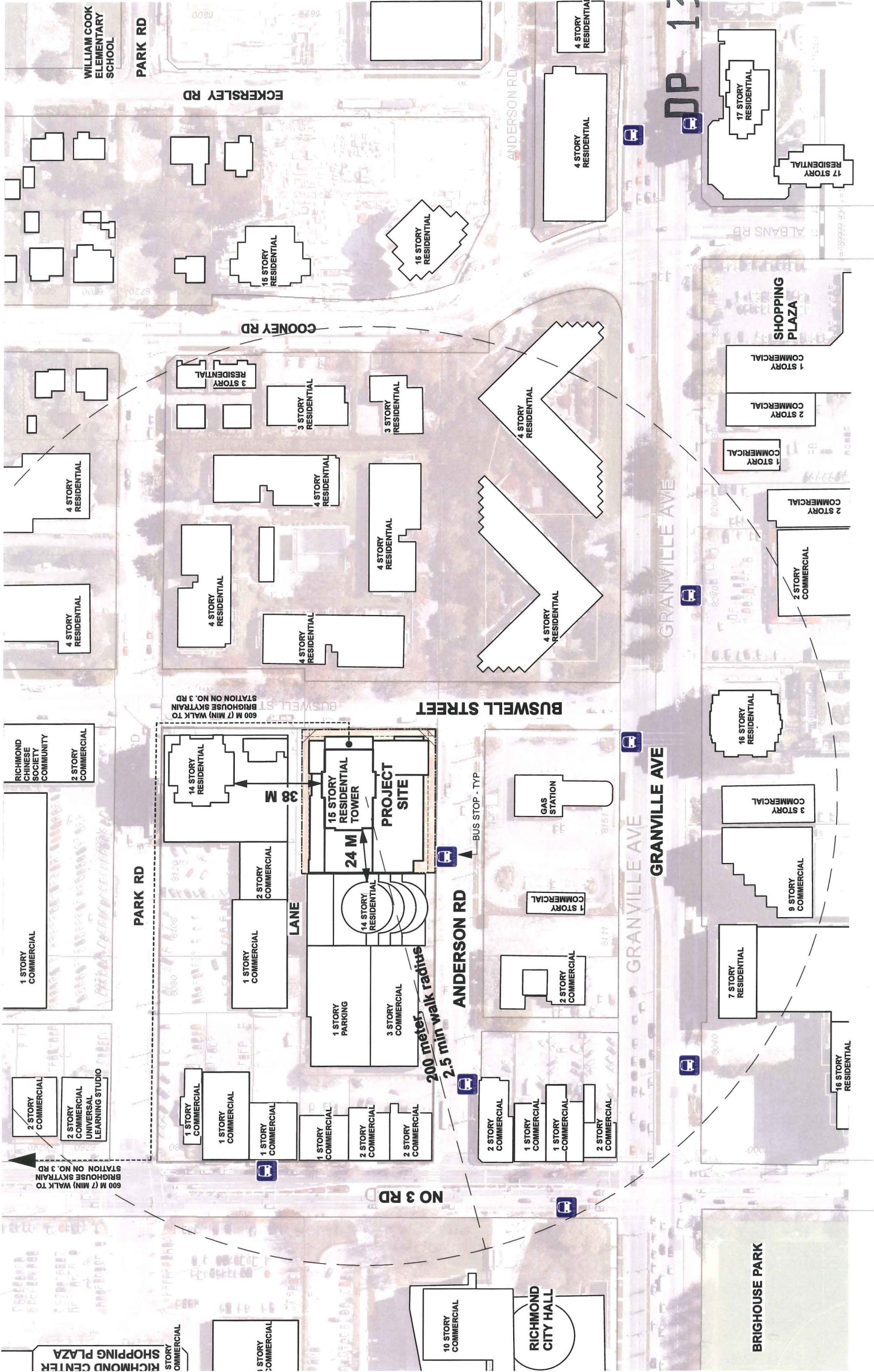
consultants

JAN 29 2015

ANDREW CHEUNG
ARCHITECTS INC.

DP 13-645286
reference

project		8151 ANDERSON ROAD RICHMOND	
title		CONTEXT PLAN	
project number	21212	file name	
drawn	al	checked	ac
scale	1:750	drawing number	DP 1.2



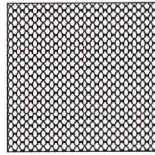
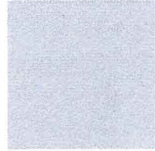
MATERIAL BOARD



1 WINDOW GLASS
FLOAT GLASS
COLOUR: GREY

3 CONC @ MIDRISE/
PERFORATED METAL
SCREEN @ PARKADE

5 SPANDREL GLASS PANEL 1
- TOWER & ABOVE ENTRY
LOBBY AREAS.

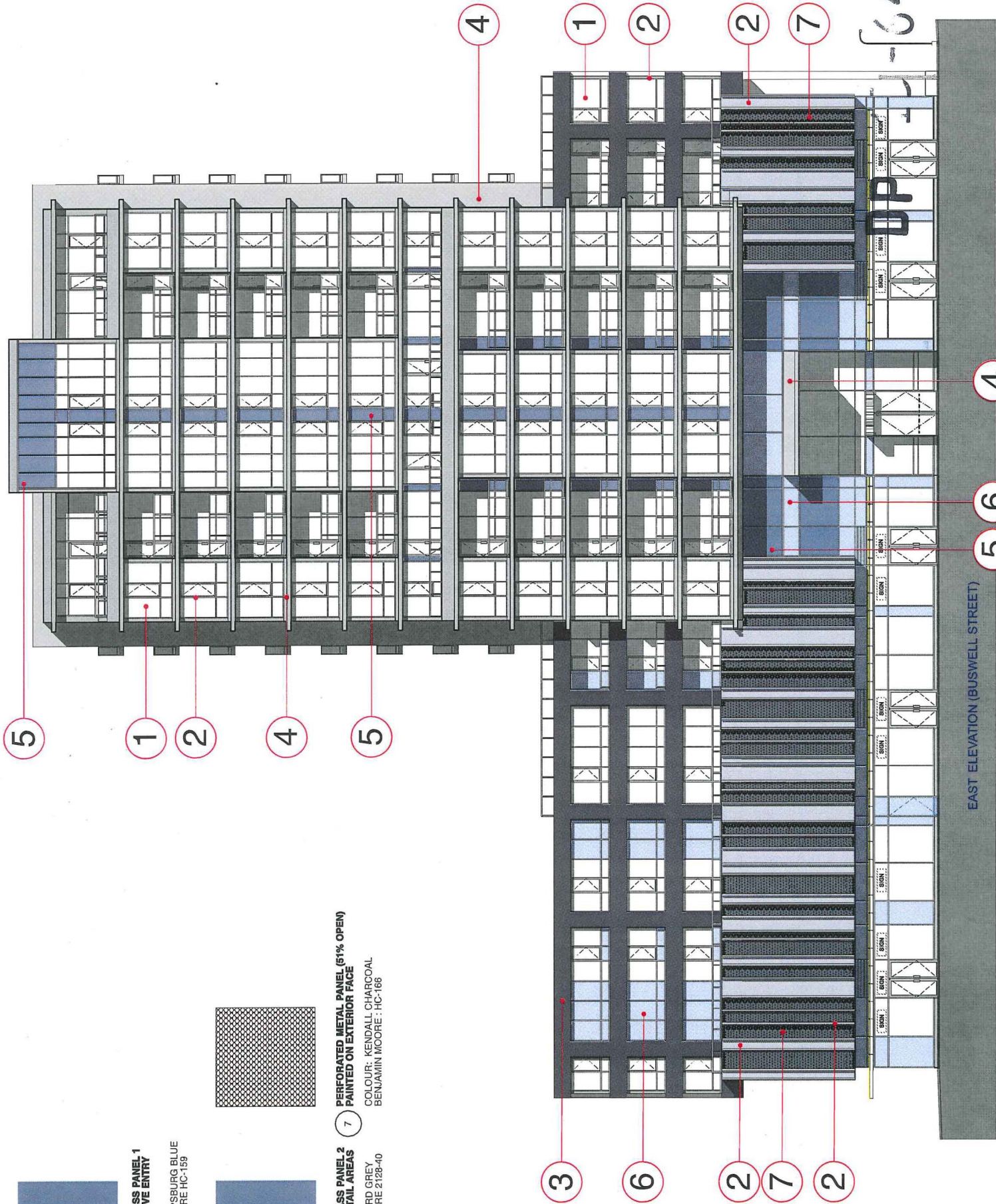


2 WINDOW MULLION/
SOLID METAL PANEL
COLOUR: STANDARD SILVER
STARLINE WDW GX200Q-D1010

4 CONC @ HIGHRISE/LANE
CONC. CANOPY @ ENTRY
COLOUR: SILVER HALF DOLLAR
BENJAMIN MOORE 2121-40

6 SPANDREL GLASS PANEL 2
- MIDRISE & RETAIL AREAS
COLOUR: OXFORD GREY
BENJAMIN MOORE 2128-40

7 PERFORATED METAL PANEL (51% OPEN)
PAINTED ON EXTERIOR FACE
COLOUR: KENDALL CHARCOAL
BENJAMIN MOORE: HC-166



DP 9.1 - MATERIAL BOARD

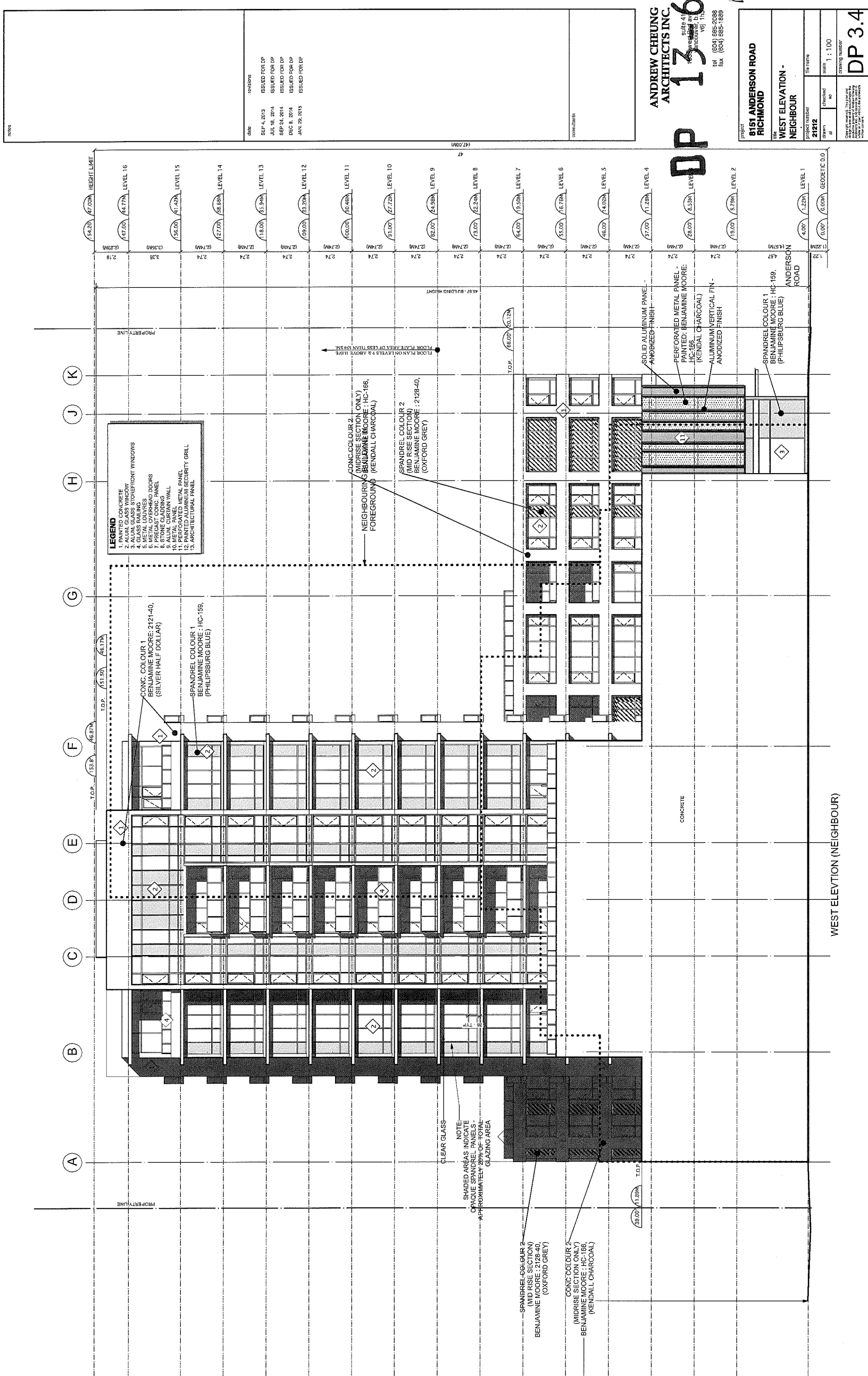
8151 ANDERSON ROAD, RICHMOND, B.C.

East Elevation (Buswell Street)

ANDREW CHEUNG ARCHITECTS INC.

#410, 1639 WEST 2ND AVE.
VANCOUVER, B.C. V6J 1H3
TEL: 604 655-2088, FAX: 604-655-1889

FEB 04 2015



**ANDREW CHEUNG
ARCHITECTS INC.**

13645286
CHIEF, INC.
suite 41
103 west 2nd av
vancouver, b.c.
v6j 1h6

reference

FEB 03 2015

DP 3.4

NOTES

DATE

SEP 4, 2013

ISSUED FOR DP

DATE

JUL 18, 2014

ISSUED FOR DP

DATE

SEP 24, 2014

ISSUED FOR DP

DATE

DEC 8, 2014

ISSUED FOR DP

DATE

JAN 28, 2015

ISSUED FOR DP

REVISIONS

CONSULTANTS

JAN 29 2015

ANDREW CHEUNG
ARCHITECTS INC.
13-645286
reference

Project

8151 ANDERSON ROAD
RICHMOND

Section

SECTION A

Drawn

21212

Checked

21212

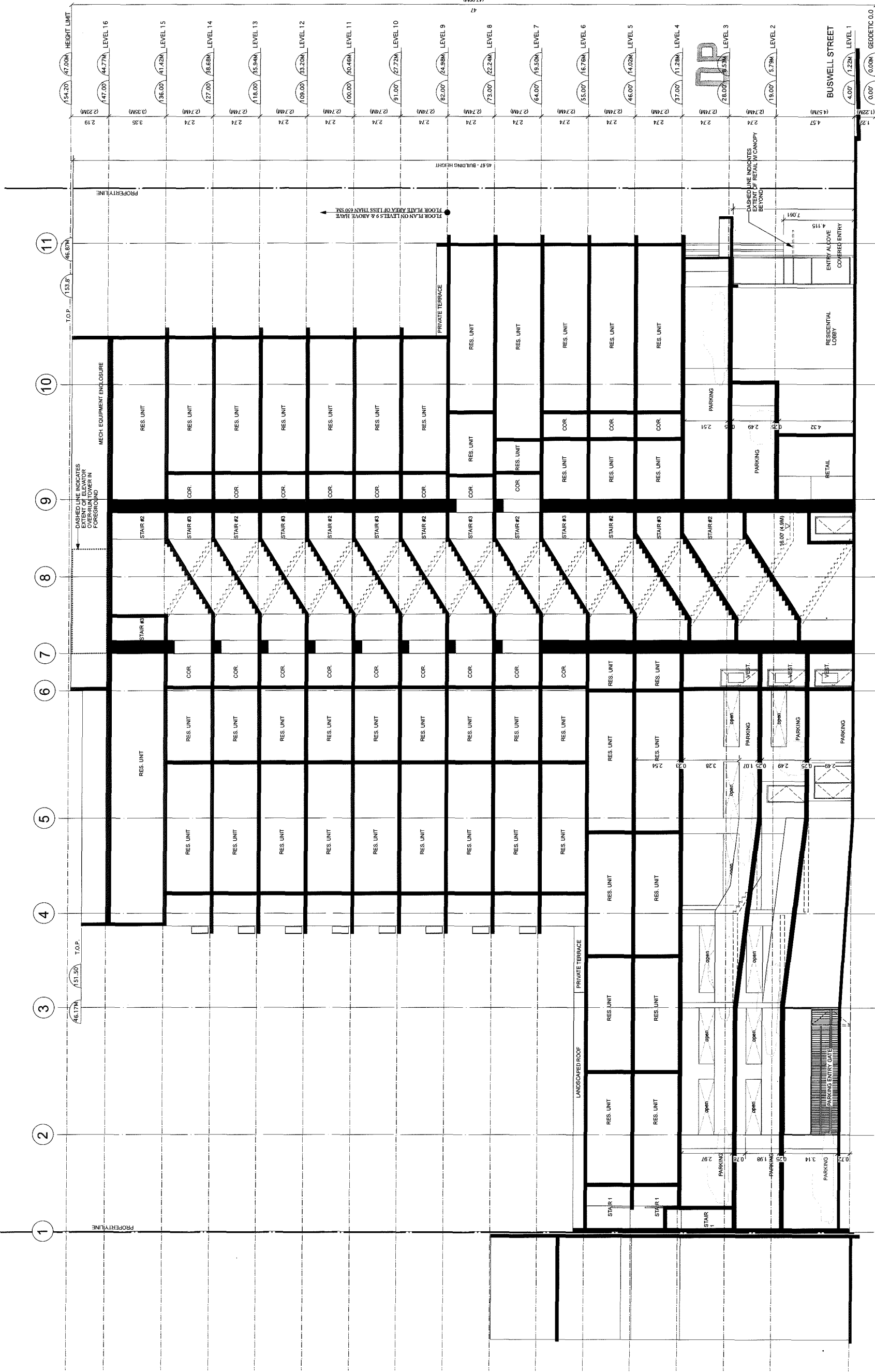
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Drawing number

DP 4.1

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SECTION A - A

NOTES

revisions

SEP 4, 2013
JUL 18, 2014
SEP 24, 2014
DEC 8, 2014
JAN 28, 2015

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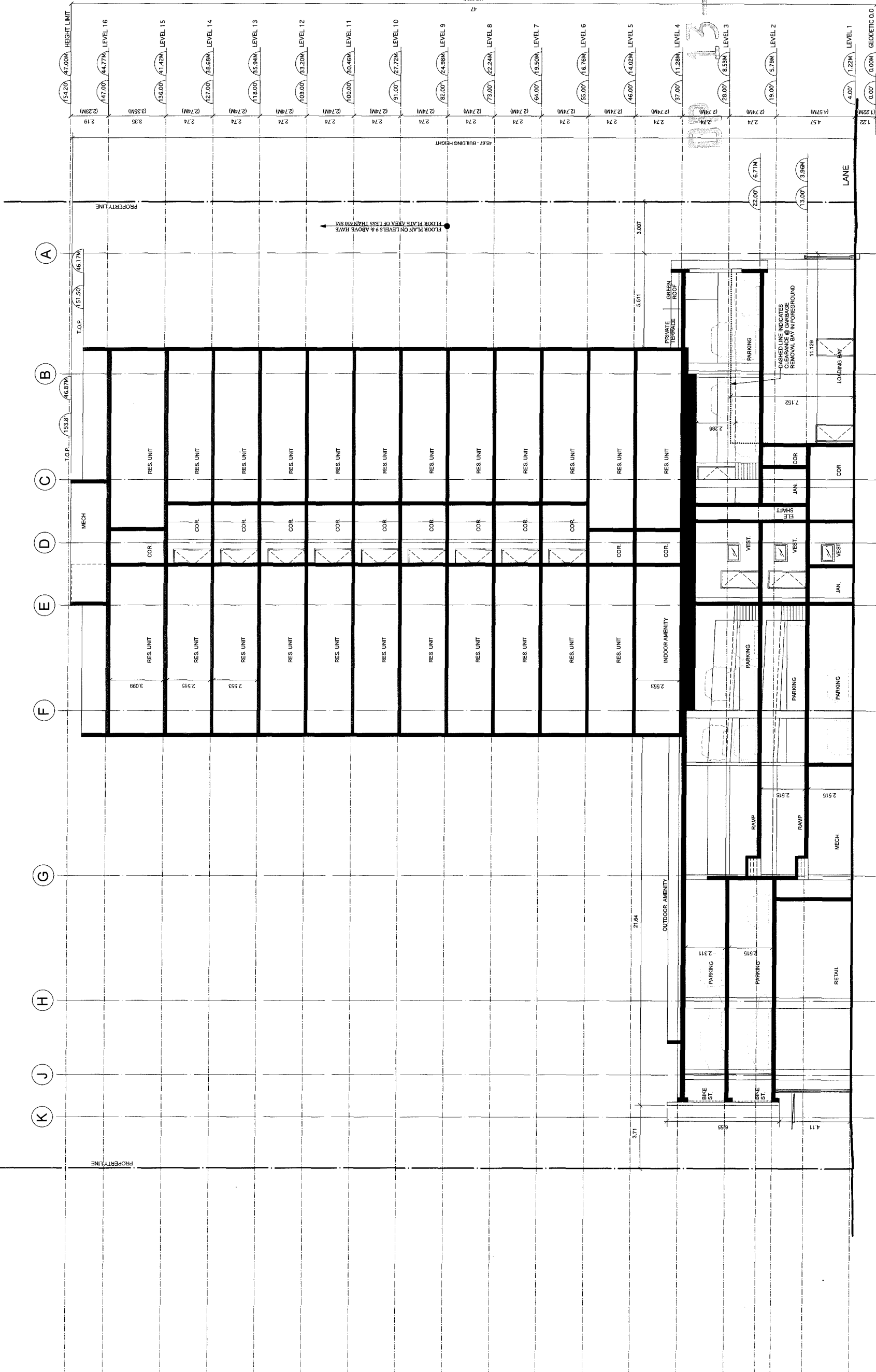
JAN 29 2015

ANDREW CHUNG
64 ARCHITECTS INC

reference

Project	
8151 ANDERSON ROAD RICHMOND	
Section	
SECTION B	
Project number	21212
Scale	1 : 100
Drawing number	DP 4.2

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VIEW FROM ANDERSON ROAD

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13-645286
reference

NOTES	
CONSULTANTS	
JAN 29 2015	
REVISIONS	
DATE	REVISIONS
SEP. 4, 2013	ISSUED FOR DP
JUL. 18, 2014	RE-ISSUED FOR DP
JAN. 29, 2015	RE-ISSUED FOR DP

PROJECT	
8151 ANDERSON ROAD RICHMOND	
NAME	
3D PERSPECTIVE VIEWS ANDERSON ROAD	
PROJECT NUMBER	
21212	
SHEET NUMBER	
1	
DRAWN	
N. T. S.	
CHECKED	
AC	
SCALE	
N. T. S.	
DRAWING NUMBER	
DP 6.1	



VIEW FROM CORNER OF BUSWELL STREET AND ANDERSON ROAD

notes	
revisions	
date	SEP. 1, 2013 ISSUED FOR DP
JUL. 18, 2014	RE ISSUED FOR UP
JAN. 29, 2015	RE ISSUED FOR DP
consultants	
JAN 29 2015	

13-64
reference
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fax (604) 685-1889

project	
8151 ANDERSON ROAD RICHMOND	
title	
3D PERSPECTIVE VIEWS - CORNER OF BUSWELL STREET AND ANDERSON ROAD	
project number	file name
21212	
drawn	checked
at	by
scale	
N. T. S.	
drawing number	
DP 6.2	



CORNER OF BUSWELL STREET AND LANE VIEW

DP

13-645200
reference

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Project		8151 ANDERSON ROAD RICHMOND	
Title		3D PERSPECTIVE VIEWS - BUSWELL STREET AND LANE VIEW	
Project number		21212	
Drawn by	checked by	Scale N.T.S.	
Drawing number		DP 6.3	

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Notes

Revisions
SEP. 1, 2013 ISSUED FOR DP
JUL. 18, 2014 RE ISSUED FOR DP
JAN. 23, 2015 RE ISSUED FOR DP

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JAN 29 2015



STREET LEVEL VIEW - BUSWELL AND LANE



RESIDENTIAL ENTRANCE VIEW

NOTES

DATE
SEP. 4, 2013
JUL. 18, 2014
JAN. 29, 2015

REVISIONS
ISSUED FOR DP
RE-ISSUED FOR DP
RE-ISSUED FOR DP

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JAN 29 2015

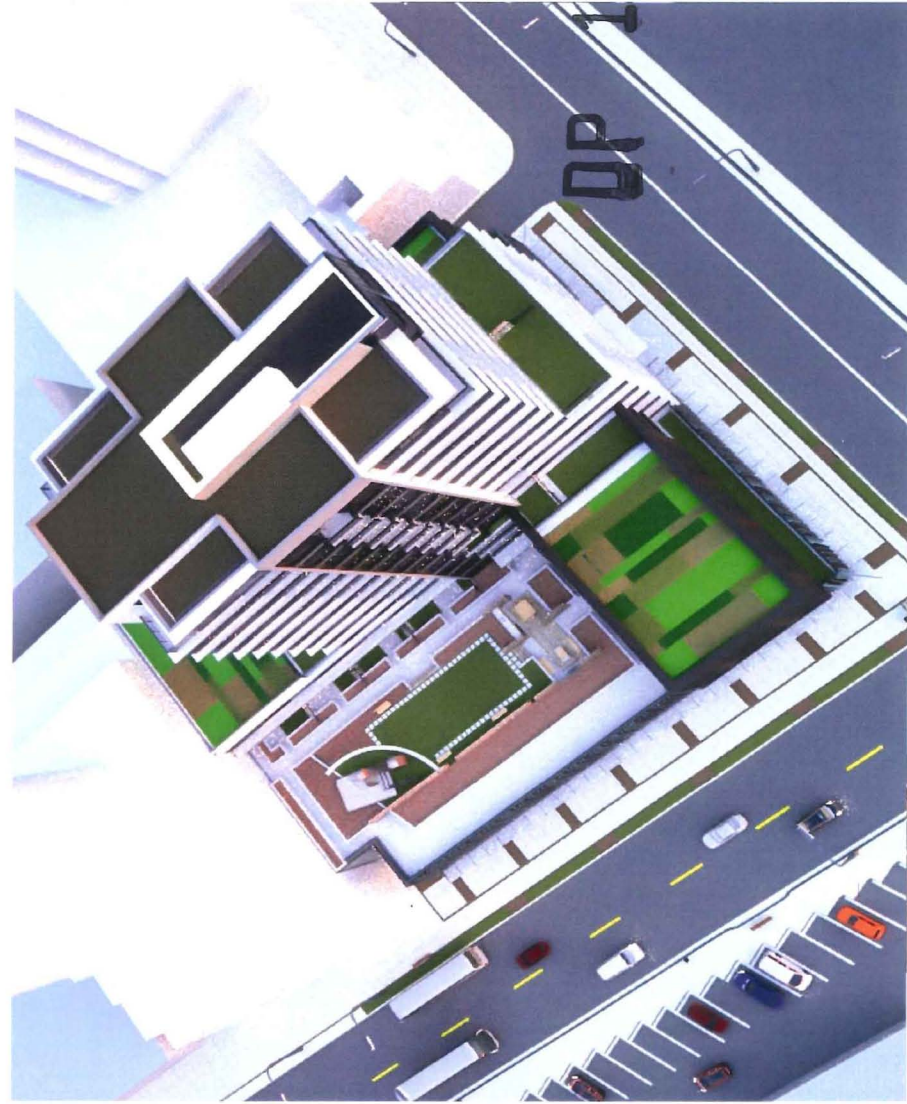
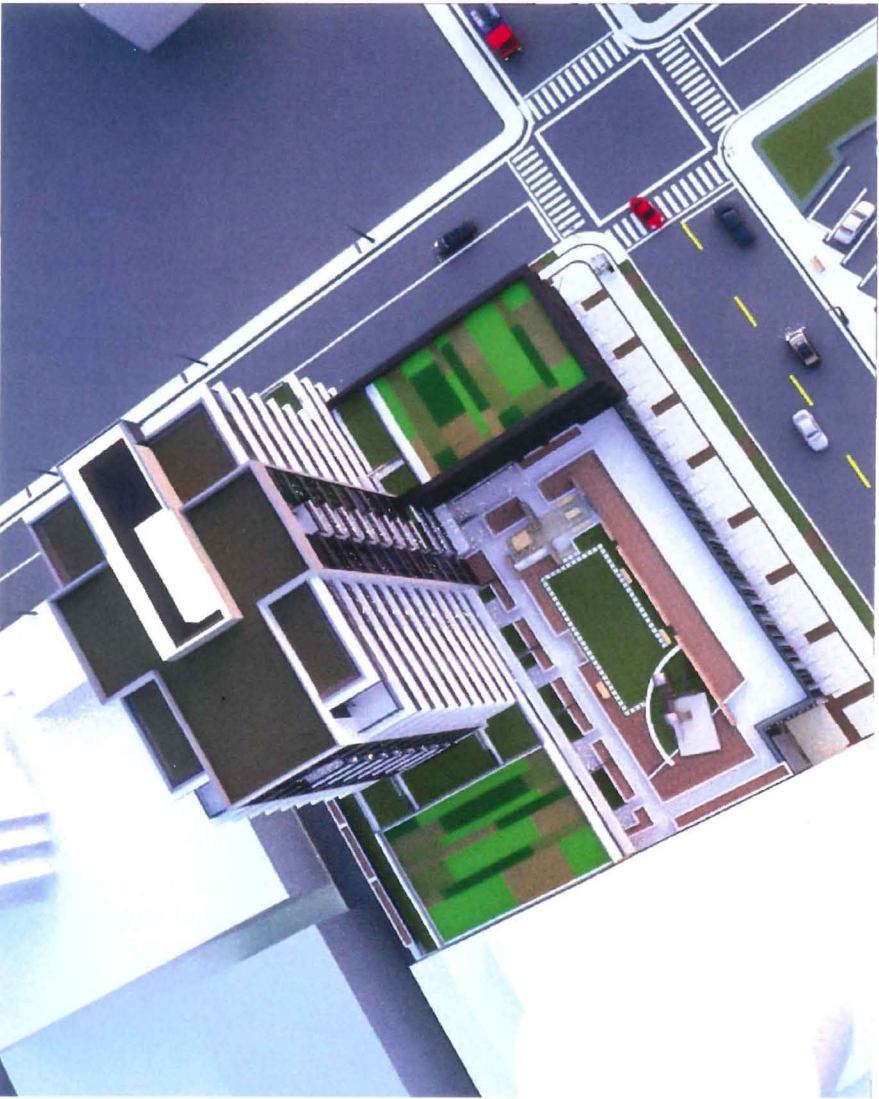
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DP 13-64520
reference

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v6j 1h3
(604) 685-2088
(604) 685-1889

8151 ANDERSON ROAD RICHMOND		scale N. T. S.	drawing number DP 6.4
3D PERSPECTIVE VIEWS - BUSWELL STREET LEVEL VIEW AND RESIDENTIAL ENTRANCE			
project number 21212	file name	checked ac	drawing sl
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OVERHEAD PERSPECTIVE VIEWS

10047

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SEP. 4, 2013	ISSUED FOR DP
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JAN. 29, 2015	RE-ISSUED FOR DP

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JAN 29 2015

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reference

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fax (604) 685-1889

Project
8151 ANDERSON ROAD
RICHMOND

3D PERSPECTIVE VIEWS -
OVERHEAD VIEWS

project number 21212		file name	
drawn	checked an	scale N. T. S.	drawing number DP 6.5
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STREET VIEW SHOWING PANELS DETAIL

Notes	
	<div style="text-align: center;"><i>revisions</i></div> <div>SEP. 4, 2013 ISSUED FOR DP JUL. 18, 2014 HE ISSUED FOR DP JAN. 29, 2015 RE-ISSUED FOR DP</div>
	JAN 29 2015 <small>Consultant</small>

**ANDREW CHEUNG
ARCHITECTS INC.**

6452
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Vancouver, B.C.
V6J 1N3

project	8151 ANDERSON ROAD RICHMOND		
title	3D PERSPECTIVE VIEWS - CLOSE-UP OF PARKADE PANELS		
project number	21212		
drawn at	checked at	scale N. T. S.	
drawing number		DP 6.6	



City of
Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: February 4, 2015

From: Wayne Craig
Director of Development

File: DV 14-670015

Re: **Application by Lansdowne Congregation of Jehovah's Witnesses for a
Development Variance Permit at 11014 Westminster Highway**

Staff Recommendation

That a Development Permit be issued which would vary the provisions of Richmond Zoning Bylaw 8500 to reduce the interior side yard (west property line) from 7.5 m to 2.3 m and interior side yard (east property line) from 7.5 m to 6.9 m in order to permit the rebuild of the existing assembly hall within the same building footprint on the "Assembly (ASY)" zoned site at 11014 Westminster Highway.


Wayne Craig
Director of Development

WC:ke

Att.

Staff Report

Origin

Lansdowne Congregation of Jehovah's Witnesses has applied to the City of Richmond for permission to vary the "Assembly (ASY)" zoning district in Richmond Zoning Bylaw 8500 to reduce the interior side yard from 7.5 m (24.6 ft.) to 2.3 m (7.5 ft.) on the west property line and 6.9 m (22.6 ft.) on the east property line to allow the existing assembly hall to be rebuilt on the existing foundation within the same building footprint at 11014 Westminster Highway. The subject site is located in the Agricultural Land Reserve (ALR). The site currently contains a one-storey assembly hall on the north portion of the site with off-street parking located behind the building. The project involves reconstruction of a new assembly hall building on the existing foundation with no other site modifications proposed.

Surrounding Development

- To the north, a property in the ALR on the north side of Westminster Highway zoned "School and Institutional (SI)";
- To the east, a property in the ALR and zoned "Agriculture (AG1)" that contains and existing single-family dwelling;
- To the south, a property (no access) in the ALR and zoned "Agriculture (AG1)".
- To the west, properties in the ALR and zoned "Agriculture (AG1)" that contain an existing watercourse (5 m designated Riparian Management Area) and existing rail line corridor.

Previous ALR Non-Farm Use Application (AG 11-566932)

The applicant submitted a previous ALR non-farm use application (AG 11-566932) in 2011 that proposed a redevelopment that demolished the existing building on the front portion of the site, developed a larger assembly hall (825 sq. m or 8,882 sq. ft.) on the rear portion of the existing Assembly (ASY) zoned property and utilize all remaining areas for supporting off-street parking, drive-aisles and on-site septic disposal area. The congregation choose not to proceed with this application, which has since been withdrawn.

The congregation has determined that the existing building footprint location and size can accommodate their long-term needs. A new building in this location is proposed.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

The site is zoned "Assembly (ASY)" and contains an existing 418 sq. m (4502 sq. ft.) one-storey assembly hall and off-street parking for the congregation, which was constructed in the late 1970's. To accommodate the changing space and programming needs of the congregation, the applicant is proposing to rebuild the assembly hall on the existing foundation and footprint of the existing building. Total floor area of the new one-storey building will be the same as the existing

hall at 418 sq. m (4,502 sq. ft.). No additional off-street parking is required for the proposal as the existing parking area, containing 55 stalls, meets zoning bylaw requirements and space needs of the congregation. As they are proposing to redevelop on the existing foundation of the assembly hall, a development variance application to reduce the interior side yard setback from 7.5 m (24.5 ft.) to 2.3 m (7.5 ft.) on the west side and 6.9 m (22.6 ft.) on the east side of the site and will enable the congregation to continue to use this site without having to expand the facility or parking into other areas of their property (contained in the ALR).

The subject site's total area is 6,751 sq. m (72,579 sq. ft.). Approximately one-half (3,400 sq. m or 36,597 sq. ft.) of site has already been modified by the existing building, parking area, drive-aisles, front and side yard areas. Therefore approximately one-half of the total area of the site already contains land uses related to the existing assembly hall. The proposed redevelopment does not increase the modified area.

Related Policies and Studies

2041 Official Community Plan (OCP)

The 2041 OCP's land use designation for the subject site is "Agriculture". The site is zoned "Assembly (ASY)" and the proposed redevelopment of the assembly hall complies with this zoning district, except for the interior side yard variances being requested as part of this Development Variance Permit application. Although there is no active farming being undertaken on the remaining portions of the subject site, the proposed redevelopment does not expand the existing building or parking footprint and the remaining area not being utilized for by assembly hall activities (approximately one-half of the total area of the site) can still be actively farmed if the congregation chooses to do so. As there will be no expansion of assembly related development into farmland, the proposal complies with the 2041 OCP land use designation.

The subject site also has an Environmentally Sensitive Area (ESA) designation over the southern portion of the site. No modification to this ESA is being proposed as part of this development. As a result, no ESA Development Permit is required.

Flood Plain Designation and Protection Bylaw

The proposed development must meet the requirements of the Flood Plain Designation and Protection Bylaw (8204). Registration of a Flood Plain Covenant on title is required prior to forwarding the Development Variance Permit application to Council.

Zoning Compliance/Variations (staff comments in bold)

The applicant requests to vary Section 13.3.6.2 of Richmond Zoning Bylaw 8500 to reduce the required interior side yard from 7.5 m (ft.) to 2.3 m (ft.) on the west property line and 6.9 m (ft.) on the east property line to allow the existing assembly hall to be rebuilt on the existing foundation and within the same building footprint at 11014 Westminster Highway:

Staff supports the proposed variance as:

- ***The proposed new assembly hall is based on the existing location of the building and does not result in any further setback encroachments or expansion of the building or***

parking footprint that would potentially result in further expansion into the vacant agricultural areas of the site.

- *The new building (one storey building; 418 sq. m or 4,502 sq. ft.) will be similar in size, massing and height when compared to the existing assembly hall.*
- *The existing site contains 55 off-street parking stalls. This parking area exceeds the current required parking regulations in the Zoning Bylaw and no additional parking is required for the new building.*
- *The proposal addresses the surrounding conditions of adjacency and maintains existing on-site landscaping/fencing as outlined in the forthcoming sections of this report.*
- *In order for a new assembly hall to be built in accordance with the setbacks, a longer narrower building would have to be constructed, resulting in further expansion of the building and parking on the back portion of the site, which is not desirable.*
- *The applicant has noted in the submission that they have consulted with the two neighbouring properties to the east about the proposed redevelopment and received a positive response from both neighbours. To the west, the site is separated by a rail allowance and public trail (Shell Road Trail).*

Consultation

Agricultural Advisory Committee Review

The proposal was reviewed and supported by the Richmond Agricultural Advisory Committee (AAC) on October 9, 2014 as follows (Please refer to Attachment 2 for an excerpt of AAC minutes):

That the Development Variance Permit Application for 11014 Westminster Highway be supported.

Carried Unanimously

Agricultural Land Commission Review

ALC staff have confirmed that redevelopment of the existing assembly building and supporting parking contained within the existing modified footprint of the subject site would not require approval from the ALC.

Analysis

Conditions of Adjacency

- The proposed new assembly hall maintains the existing adjacency conditions to the neighbouring property to the east (containing a single-family dwelling) and west (existing RMA designated watercourse and rail corridor).
- Shadowing and overlook impacts to neighbouring sites will be minimized as the proposed assembly hall is limited to one-storey massing with minimal overlook onto neighbouring sites.

- A setback of 6.9 m (22.6 ft.) is proposed on the interior side yard (east) that is adjacent to an existing single-family dwelling. Although a reduction of 0.6 m (2 ft.) is being requested, the 6.9 m (22.6 ft.) setback provides for sufficient separation between the neighbouring property and the one-storey assembly building. This setback area will primarily be utilized as the main drive-aisle to the site and access to the existing parking lot at the rear of the building.
- Existing grades will be maintained and as a result, no new or higher retaining walls around the site will be required. The new assembly hall building will be built to meet and/or exceed the minimum required floor construction level required.

Architectural Form and Character

- Rebuilding the existing assembly hall within the same building footprint enables the congregation to update the exterior appearance of the one storey congregation hall, while also reconfiguring the interior layout of the building to meet the applicant's current needs.
- Low pitch roof elements are incorporated into the architectural design of the assembly hall, which is consistent with the single-family residential dwellings along Westminster Highway.
- The proposed redevelopment also enables the incorporation of a variety of exterior cladding and materials consistent with residential development such as large wooden support beams, hardi-panel board and batten siding with cultured stone decorative elements.

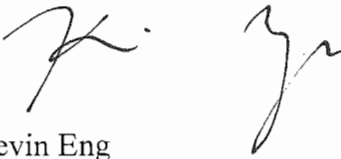
Landscape Design and Open Space Design

- The proposed redevelopment will be able to maintain existing open spaces and landscape buffering around the perimeter of the subject site, including the plantings located along the west property line next to the proposed building. The applicant explored the potential for additional landscape buffering to be added along the east edge of the site adjacent to the existing north-south running drive-aisle; however, there is not enough width to accommodate the necessary space needed for the driveway and a buffer strip adjoining the fence.
- The applicant notes that the existing wooden fence (1.8 m or 6 ft.) along the east property line will be replaced with a similar height and type fence.
- The existing on-site sanitary septic system is located in the front yard (north of the existing building). The applicant has confirmed that this existing on-site sanitary septic system will be maintained and be able to service the new building.
- The subject site also has an existing 5 m Riparian Management Area (RMA) designation that runs along the subject site's west property lines as a result of a canal on the neighbouring rail right-of-way properties to the west that will not be impacted as a result of this development.
- Based on the redevelopment, a majority of all trees on site will be retained. The submitted landscape plan identifies a small number of trees that are located in close proximity to the existing footprint of the building. Further review of these trees will be undertaken once the applicant has developed more detailed plans for the building. If the proposed new building requires removal of any trees, the applicant will be required to submit required tree permit applications to request their removal.
- A landscape bond will be secured as a Development Permit consideration to ensure complete of the works identified in the plan.

Conclusion

The applicant has applied to the City of Richmond for permission to vary Section 13.3.6.2 of Richmond Zoning Bylaw 8500 to reduce the required minimum interior side yard in the "Assembly (ASY)" zone from 7.5 m (24.6 ft.) to 2.3 m (7.5 ft.) on the west property line and 6.9 m (22.6 ft.) on the east property line to allow the existing assembly hall to be rebuilt within the same building footprint at 11014 Westminster Highway.

The proposal complies with the 2041 OCP and as confirmed by ALC staff, does not require an ALR application. Staff recommend support of the requested variance as the proposal enables the existing "Assembly (ASY)" zoned site to continue to be used by the congregation over the long-term without having to expand or further modify land in the ALR.



Kevin Eng
Planner 2

KE:cas

The following are to be met prior to forwarding this application to Council for approval:

- Submission of a landscape bond/letter of credit in the amount of \$6,680
- Registration of a flood plain covenant on title identifying a minimum Floor Construction Level of 2.9 m GSC.
- Installation of all required RMA protection fencing and measures to the satisfaction of Environmental Sustainability Staff. RMA protection fencing and measures will be required to be maintained and installed on the subject site for the duration of redevelopment activities.

Prior to future Building Permit issuance, the developer is required to complete the following:

- Obtain any necessary tree permits for required tree removals as part of the redevelopment.
- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).



DV 14-670015

Attachment 1

Address: 11014 Westminster Highway

Applicant: Lansdowne Congregation of Jehovahs Witnesses Owner: Same as applicant

Planning Area(s): East Richmond

Floor Area Gross: 418.2 m² Floor Area Net: 418.2 m²

	Existing	Proposed
Site Area:	6,751 m ²	6,751 m ²
Land Uses:	Existing assembly hall on north portion of the site with supporting off-street parking	Rebuilt assembly hall within existing building footprint. Existing parking areas and drive-aisles remain unchanged
OCP Designation:	Agriculture	No change
Zoning:	Assembly (ASY)	Assembly (ASY)

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	0.5 FAR	0.06 FAR	none permitted
Lot Coverage:	Max. 35%	6%	none
Setback – Front Yard:	Min. 6.0 m	18.1 m	none
Setback – Side Yard (west):	Min. 7.5 m	2.3 m	Variance requested
Setback – Side Yard (east):	Min. 7.5 m	6.9 m	Variance requested
Setback – Rear Yard:	Min. 7.5 m	140 m	none
Height (m):	Max. 12 m	8 m	none
Off-street Parking Spaces	42 stalls	55 stalls	none
Off-street Parking Spaces – Accessible:	2 stalls	5 stalls	none
Total off-street Spaces:	42 stalls	55 stalls	none

ATTACHMENT 2

Excerpt of Agricultural Advisory Committee Meeting Minutes October 9, 2014

Development Proposal – Development Variance Permit 11014 Westminster Hwy

Staff (Kevin Eng) provided a summary of the Development Variance Permit application to enable a new assembly hall to be built within the similar footprint of the existing building. The previous proposal to build a larger assembly hall on the south portion of the site was reviewed and supported by the AAC in 2012 but Council referred it back to staff for further review and the proponent decided not to pursue the previous proposal.

The applicant indicated that the proposal would use the existing exterior perimeter foundation. Additional piles will be put under the slab and a new concrete slab will be poured. The applicant also noted that there was a preliminary discussion with ALC and the ALC staff had indicated that, as long as the footprint does not change, approval from the ALC would not be required; Staff will formally refer the application to the ALC and confirm it.

A general question was asked by the Committee why a 7.5m side yard setback was required in the Assembly zone and staff explained that it was to minimize any potential negative impacts to neighbours. In this case, the impacts of the requested variances are expected to be minimal as the new building will be constructed on the same location and to the west is a railroad allowance (there is no building on the adjacent property).

That the Development Variance Permit Application for 11014 Westminster Highway be supported.

Carried Unanimously



City of Richmond

Development Variance Permit

No. DV 14-670015

To the Holder: Lansdowne Congregation of Jehovahs Witnesses

Property Address: 11014 Westminster Highway

Address: Tom J. Munro
2919 145A Street
Surrey, BC V4P 1P7

1. This Development Variance Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied by this Permit.
2. This Development Variance Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied as follows:
 - a) Vary the provisions of Richmond Zoning Bylaw 8500 to reduce the minimum interior side yard (Richmond Zoning Bylaw 8500 Section 13.3.6.2) from 7.5 m to 2.3 m on the west side of the site and 7.5 m to 6.9 m on the east of the site for an assembly hall building
 - b) The dimension and siting of buildings and structures on the land shall be as shown on Plan #1 to Plan #6 attached hereto.
4. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.
5. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse.

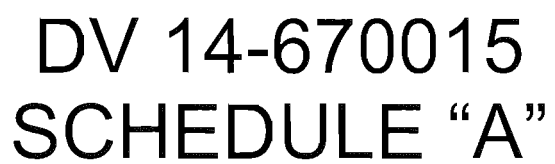
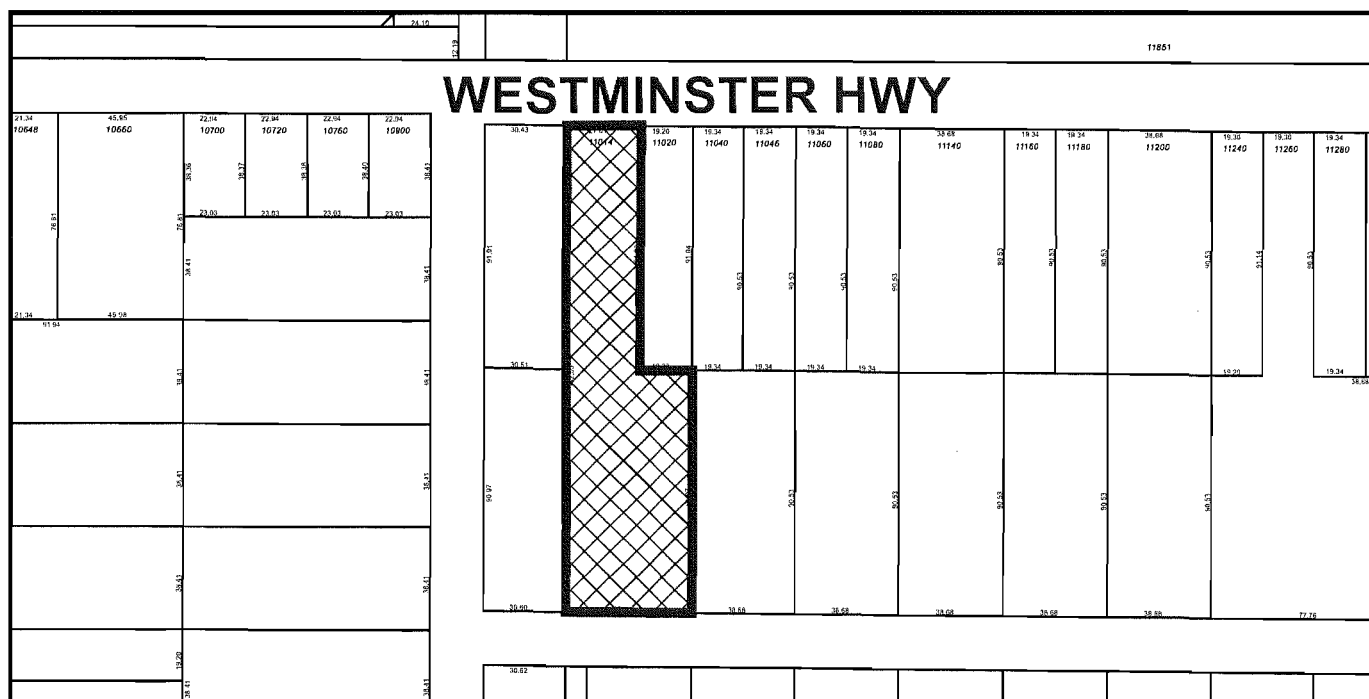
This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



Note: Dimensions are in METRES



SHEET TITLE:	
SITE PLAN	
DRAWING NUMBER:	REV. NUMBER:

OV 14-670015

DESIGN AND CONSTRUCTION OF THIS PROJECT SHALL BE IN ACCORDANCE WITH BC BUILDING CODE 2012, AND APPLICABLE MUNICIPAL BY-LAWS.

- [illegible]



DST ARCHITECTURE

63-2418 avon place
port coquitlam, bc
v3b 0c7

t.604.475.2727

dstarchitecture@gmail.com

LANSDOWNE 2014
 KINGDOM HALL OF
 JEHOVAH'S WITNESSES
 LANSDOWNE 2014

SHEET TITLE:

MAIN FLOOR PLAN

1 MAIN FLOOR PLAN
A2.0 SCALE: 1/4"=1'-0"

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FEB 04 2015

14-670015 DV

DRAWING NUMBER:	REV. NUMBER:
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A2.0

GENERAL NOTES:
1. DESIGN AND CONSTRUCTION OF THIS PROJECT SHALL

- [illegible]

1. The first step in the process of developing a business plan is to determine the purpose of the business.	
2. The second step is to conduct a market analysis to determine the size and growth potential of the market.	
3. The third step is to develop a marketing plan to determine how the business will reach its target market.	
4. The fourth step is to develop a financial plan to determine the costs of the business and the expected revenue.	
5. The fifth step is to develop an operational plan to determine the day-to-day activities of the business.	
6. The sixth step is to develop a management plan to determine the roles and responsibilities of the management team.	
7. The seventh step is to develop a risk management plan to determine the potential risks to the business and how to mitigate them.	
8. The eighth step is to develop a legal plan to determine the legal structure of the business and the legal requirements for operating in the industry.	
9. The ninth step is to develop a human resources plan to determine the staffing needs of the business and how to recruit and retain talent.	
10. The tenth step is to develop a technology plan to determine the technology needs of the business and how to implement and maintain it.	

SEAL:

DST ARCHITECTURE

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LANSDOWNE 2014
KINGDOM HALL OF
JEHOVAH'S WITNESSES

LANSDOWNE 2014

SHEET TITLE:

ROOF PLAN

1 ROOF PLAN
A2.1 SCALE: 1/4"=1'-0"

DRAWING NUMBER:	REV. NUMBER:
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A2.1

