



To: Planning Committee
From: Victor Wei,
Director, Transportation
Terry Crowe,
Manager, Policy Planning
Date: October 10, 2017
File: 08-4045-20-04/2017-
Vol 01
Re: **Proposed Changes: Steveston Area Plan, Village Heritage Conservation
Policies, Design Guidelines and Long-Term Bayview, Moncton and Chatham
Street Visions**

Staff Recommendation

1. That the report titled “Proposed Changes: Steveston Area Plan, Village Heritage Conservation Policies, Design Guidelines and Long-Term Bayview, Moncton and Chatham Street Visions” dated October 10, 2017 from the Director, Transportation and Manager, Policy Planning be received for information;
2. That Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9775, be introduced and given first reading;
3. That Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9775, having been considered in conjunction with:
 - a. the City's Financial Plan and Capital Program; and
 - b. the Greater Vancouver Regional District Solid Waste and Liquid Waste Management Plans;is hereby found to be consistent with said program and plans, in accordance with section 477(3)(a) of the Local Government Act; and
4. That Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9775, having been considered in accordance with Section 475 of the Local Government Act and the City's Official Community Plan Bylaw Preparation Consultation Policy 5043, is found not to require further consultation.
5. That the recommended Long-Term Streetscape Visions for Bayview, Chatham and Moncton Streets based on community feedback obtained from the public consultation held in July 2017 be endorsed to guide future street frontage improvements along these roadways as part of new developments and City capital projects.

- 6. That staff be directed to report back with an implementation strategy for the Bayview, Chatham and Moncton Street recommended streetscape visions including updated and more detailed cost estimates, boulevard surface finish, timing, and funding sources.
- 7. That the boundary of the 30 km/h speed limit on Chatham Street be extended from 3rd Avenue west to 7th Avenue to provide consistency along the length of the street.

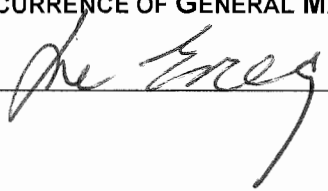

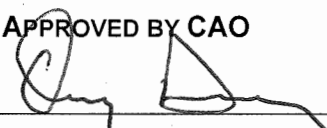


Victor Wei, P. Eng.
 Director, Transportation
 (604-276-4131)



Terry Crowe
 Manager, Policy Planning
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Att. 13

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Finance Department	<input checked="" type="checkbox"/>	
Parks	<input checked="" type="checkbox"/>	
Arts, Culture & Heritage	<input checked="" type="checkbox"/>	
Engineering	<input checked="" type="checkbox"/>	
Building Approvals	<input checked="" type="checkbox"/>	
Development Applications	<input checked="" type="checkbox"/>	
REVIEWED BY STAFF REPORT / AGENDA REVIEW SUBCOMMITTEE	INITIALS: 	APPROVED BY CAO 

Staff Report

Origin

At its regular meeting held on June 12, 2017, Council endorsed proposed changes to the design and heritage policies in the Steveston Area Plan, and a long-term streetscape visions for Bayview Street, Moncton Street and Chatham Street for the purpose of carrying out public consultation, and directed staff to report back on the outcome of the consultation in October 2017.

This report:

- presents the results of consultations with the general public and stakeholders;
- proposes recommendations to amend design and heritage policies of the Steveston Area Plan based on the consultation feedback and staff's analysis; and
- proposes recommended long-term streetscape visions based on the consultation feedback and staff's analysis.

This report supports Council's 2014-2018 Term Goal #2 A Vibrant, Active and Connected City:

- 2.3. *Outstanding places, programs and services that support active living, wellness and a sense of belonging.*

This report supports Council's 2014-2018 Term Goal #3 A Well-Planned Community:

- 3.2. *A strong emphasis on physical and urban design.*
- 3.3. *Effective transportation and mobility networks.*

This report supports Council's 2014-2018 Term Goal #9 A Well-Informed Citizenry:

- 9.1. *Understandable, timely, easily accessible public communication.*
- 9.2. *Effective engagement strategies and tools.*

Findings of Fact

Public Consultation Engagement

From July 14 to 30, 2017, the City sought input from the community and stakeholders regarding proposed changes to the design and heritage policies in the Steveston Area Plan, and a long-term streetscape vision for Bayview Street, Moncton Street and Chatham Street.

Outreach activities to raise awareness of the consultation included:

- Media release and local newspaper advertisement in the Richmond News;
- City of Richmond website and social media including LetsTalkRichmond.ca; and
- Distribution of posters in Steveston Village.

Feedback was primarily gathered via an online survey on LetsTalkRichmond.ca with paper surveys available at two open houses held at Steveston Community Centre on July 20 and July 22 (see Attachments 1 and 2 for the open house display boards, and Attachments 3 and 4 for the open house surveys). Each open house recorded approximately 90 attendees. Direct meetings with stakeholders included the Richmond Heritage Commission (July 19), the Steveston Harbour Authority (July 26), and the Steveston Group of 20/20 (September 14).

Analysis

Part A – Land Use and Design-Related Issues

1. Public Consultation Results and Staff Recommendations

A total of 195 design and heritage policies surveys were completed (167 on-line and 28 paper). Listed below are the survey results and the staff recommendation for each question in the design and heritage policies survey.

Question 1		
The current density allowed on Moncton Street is a maximum of 1.2 floor area ratio (FAR), and the maximum building height is 2 storeys or 9 m. However, 1 in 3 buildings may be up to a maximum of 3 storeys and 12 m. Which option do you support?		
Options		Survey Response
1	No change in the maximum density and maximum height.	18.1%
2	Reduce maximum density from 1.6 FAR to 1.2 FAR, and require all buildings to have a maximum height of 2 storeys and 9 m (recommended in May 30 staff report) .	81.9%

Staff Recommendation: Amend the Heritage (Section 4.0) and Development Permit Guidelines – Village Core Area (Section 9.0) of the Steveston Area Plan and accompanying land use, density and building height maps to reflect Option 2 above.

Question 2		
The current density allowed on Bayview Street (north side) is a maximum of 1.6 floor area ratio (FAR), and the maximum building height is 3 storeys, or 12 m, over parkade structure. Which option do you support?		
Options		Survey Response
1	No change in the maximum density and maximum height as described above.	17.7%
2	A reduction in density and height as follows: <ul style="list-style-type: none"> • Maximum density of 1.2 FAR • North side lot depth, up to 2 storeys over parkade (appears 3 storeys). • South side lot depth, up to 2 storeys over parkade (appears 2 storeys) (recommended in May 30 staff report) .	82.3%

Staff Recommendation: Amend the Development Permit Guidelines (Section 9.0) in the Steveston Area Plan specific to the Steveston Village Core Area and accompanying land use, density and building height maps to reflect Option 2 above.

Question 3		
<p>In the design guidelines for the Village Core (including Bayview Street north side), wood is the primary material for exterior cladding (i.e. siding). However, the wood for exterior cladding is restricted to horizontal siding. Historically, the wood used on buildings in Steveston Village included wood shingles, board-and-batten, and vertical shiplap, and these materials were allowed in the “Sakamoto Guidelines” that the City used for the Village Core before 2009. Which option do you support?</p>		
Options		Survey Response
1	No change to the primary material for exterior cladding (i.e. horizontal wood siding only).	7.7%
2	Expand the primary materials for exterior cladding to include wood shingles, board-and-batten and vertical ship lap, in addition to horizontal wood siding (recommended in May 30 staff report) .	92.3%

Staff Recommendation: Amend the Development Permit Guidelines – General and Village Core Area (Section 9.0) in the Steveston Area Plan to reflect Option 2 above.

Question 4		
<p>In the design guidelines for new buildings and additions, for the Village Core (including Bayview Street north side), the primary material for exterior cladding (i.e. siding) is wood. Glass, concrete, stucco, and metal that complements the wood siding may be used as secondary material(s) for exterior cladding. Which option do you support?</p>		
Options		Survey Response
1	No change to the secondary materials for exterior cladding (i.e. siding).	9.0%
2	No brick and no metal allowed. For façade upgrades, replace brick with similar brick.	5.3%
3	No brick and no metal allowed. For façade upgrades, replace brick with similar brick or different brick.	2.7%
4	No brick and no metal allowed. For façade upgrades, replace brick with similar brick, different brick or a better material.	2.1%
5	No metal but brick is allowed if different from the Hepworth Building. For façade upgrades, replace brick with a similar brick or different brick.	6.4%
6	No metal but brick is allowed if different from the Hepworth Building. For façade upgrades, replace brick with similar brick, different brick, or a better material (recommended in May 30 staff report) .	74.5%

Staff Recommendation: Amend the Development Permit Guidelines –Village Core Area (Section 9.0) in the Steveston Area Plan to reflect Option 6 above.

Question 5		
<p>In the design guidelines for the Village Core and the Riverfront, window frames that are wood are encouraged. Vinyl window assemblies are discouraged but allowable. Which option do you support?</p>		
Options		Survey Response
1	No change to materials for window treatments (i.e. wood or vinyl is allowed).	24.7%
2	Windows with wood frames or metal frames are allowed. Vinyl is prohibited (recommended in May 30 staff report) .	75.3%

Staff Recommendation: Amend the Development Permit Guidelines – Village Core and Riverfront Area (Section 9.0) in the Steveston Area Plan to reflect Option 2 above.

The proposed Steveston Area Plan amendments do not permit exclusively vinyl window frames and related assemblies in Steveston Village Core and Riverfront Area. However, the proposed guidelines would allow for the use of contemporary materials that offer a compatible look to wood or metal to be considered.

Question 6		
Solar panels, and other renewable energy infrastructure (e.g. air source heat pump), may be mounted on heritage buildings and non-heritage buildings in Steveston Village. No changes are proposed to the guidelines for heritage buildings. The design guidelines to manage the visibility of solar panels on non-heritage properties with a flat roof include a requirement for the panels to be located back from the building edges. There are no design guidelines for other renewable energy infrastructure on flat roofs, and no design guidelines for solar panels or other renewable energy infrastructure on new or existing pitched-roof buildings. Which option do you support?		
Options		Survey Response
1	No changes to existing design guidelines.	10.9%
2	New design guidelines that require any false parapets to be slightly taller on new flat-roofed buildings, and allow solar panels to be affixed flush to pitched roofs (recommended in May 30 staff report) .	89.1%

Staff Recommendation: Amend the Development Permit Guidelines – Village Core Area (Section 9.0) in the Steveston Area Plan to reflect Option 2 above.

Question 7		
Barrier railings for rooftop living spaces, which provide safety, on new and existing buildings should blend with the special character of the historic district. Currently there are no design guidelines for barrier railings in the Village Core. Rooftop living spaces are not possible in the Riverfront sub-area (Bayview Street south side) where roofs are pitched not flat. Which option do you support?		
Options		Survey Response
1	No changes to existing design guidelines.	8.9%
2	New design guidelines for barrier railings to be simple in design, and primarily consist of glazed panels to minimize visibility from streets and nearby rooftop patios on adjacent and surrounding buildings (recommended in May 30 staff report) .	91.1%

Staff Recommendation: Amend the Development Permit Guidelines – Village Core Area (Section 9.0) in the Steveston Area Plan to reflect Option 2 above.

Question 8		
<p>Managing the visibility of an access point for individual rooftop living spaces (i.e. roof decks and gardens) can be achieved through blending the hatch or 'pop-up' stair entries (that the building code requires) with the overall architecture of the new building or the existing building. There are currently no design guidelines for hatch ('pop-up') entries to individual rooftop living space. Which option do you support?</p>		
Options		Survey Response
1	No changes to existing design guidelines as described above.	6.4%
2	Prohibit all hatch stair entries.	3.7%
3	Prohibit all hatch stair entries unless they are not more than 1.83 m (6 ft.) in height, well-integrated with the architecture and setback 1.0 m or more from all roof edges (recommended in May 30 staff report) .	66.3%
4	Allow hatch stair entries if well-integrated with the overall architecture, and setback from all roof edges.	23.5%

Staff Recommendation: Amend the Development Permit Guidelines – Village Core Area (Section 9.0) in the Steveston Area Plan to reflect Option 3 above.

Question 9		
<p>Managing the visibility of one or more access points for communal rooftop living space (i.e. roof deck and garden) can be achieved through blending the structure for the access stairs or elevator shaft (two shafts may be required to meet the building code) with the overall architecture or the new building or the existing building. There are no design guidelines to reduce the visibility of access stairs or an elevator shaft for communal rooftop living spaces. Which option do you support?</p>		
Options		Survey Response
1	No changes to existing design guidelines as described above.	3.7%
2	Prohibit all elevator shafts and access stairs.	4.8%
3	Prohibit access points unless they are less than 2.2 m for elevator shafts, and 3.17 m for access stairs, well-integrated with the architecture, and setback 1.0 m or more from all roof edges (recommended in May 30 staff report) .	69.3%
4	Allow structures for elevator shafts and access stairs if well-integrated with the overall architecture, and setback from all roof edges.	22.2%

Staff recommendation: Amend the Development Permit Guidelines – Village Core Area (Section 9.0) in the Steveston Area Plan to reflect Option 3 above.

Question 10		
<p>The current density allowed on Bayview Street (south side) is a maximum of 1.6 floor area ratio (FAR), and the maximum building height is 3 storeys, or 12 m, over parkade structure. Which option do you support?</p>		
Options		Survey Response
1	No change in the maximum density and maximum height as described above (recommended in May 30 staff report) .	54.7%
2	Reduced density or reduced height.	45.3%

Staff recommendation: No changes proposed to the Steveston Area Plan.

Question 11		
The overall design vision for Bayview Street (south side) includes "Cannery-like" pitched roofed buildings, but flat roofs are allowable. Which option do you support?		
Options		Survey Response
1	No changes to existing design guidelines.	16.9%
2	Pitched roofs only to fully align with the design vision. Flat roofs are prohibited (recommended in May 30 staff report) .	83.1%

Staff Recommendation: Amend the Development Permit Guidelines – Riverfront Area (Section 9.0) in the Steveston Area Plan to reflect Option 2 above.

Question 12		
The overall design vision for Bayview Street (south side) includes retention of existing large lots. Which option do you support?		
Options		Survey Response
1	No changes to existing large lots (recommended in May 30 staff report) .	74.9%
2	Through the redevelopment process, allow the subdivision of the existing larger lots into relatively small lots.	25.1%

Staff Recommendation: Amend the Development Permit Guidelines – Riverfront Area (Section 9.0) in the Steveston Area Plan to reflect Option 1 above.

Question 13		
The overall design vision for Bayview Street (south side) includes large and small buildings on existing large lots. Which option do you support?		
Options		Survey Response
1	No changes (i.e. a mix of large and small buildings) (recommended in May 30 staff report) .	71.4%
2	Small buildings on small lots. No more new large "Cannery-like" buildings.	28.6%

Staff Recommendation: Amend the Development Permit Guidelines – Riverfront Area (Section 9.0) in the Steveston Area Plan to reflect Option 1 above.

Question 14		
<p>The City has the long-term objective of completion of the waterfront boardwalk, between 3rd Avenue and No. 1 Road, which is part of the Parks Trail System, and to complete pedestrian connections from Bayview Street to the riverfront. The Steveston Area Plan is currently unclear on how developers will contribute to the boardwalk and paths in the application review process. Which option do you support?</p>		
Options		Survey Response
1	No changes (i.e. no City policy on developer contributions).	6.7%
2	Developer contributions to the waterfront boardwalk and pedestrian paths are required through rezoning and development permit application review process (recommended in May 30 staff report).	93.3%

Staff Recommendation: Amend the Natural and Human Environment (Section 6.0) in the Steveston Area Plan to reflect Option 2 above.

Question 15		
<p>The Steveston Area Plan does not include a full set of design policies and guidelines for the waterfront boardwalk, between 3rd Avenue and No 1. Road, which is part of the Parks Trail System, or new and existing pedestrian connections, from Bayview Street to the riverfront. Which option do you support?</p>		
Options		Survey Response
1	No change to existing design policies and guidelines.	6.7%
2	New design guidelines that include, but are not limited to, a set of dimension standards for details, such as boardwalk and path widths, setbacks to accommodate hanging signage, and surface treatments (recommended in May 30 staff report).	93.3%

Staff Recommendation: Amend the Natural and Human Environment (Section 6.0) in the Steveston Area Plan and add accompanying maps and diagrams to reflect Option 2 above.

Question 16		
<p>To help support the vitality and conservation of Steveston Village, existing policy allows up to 33% reduction in on-site vehicle parking from the zoning regulations. However, there are impacts on the availability of street parking to be taken into consideration. Which option do you support?</p>		
Options		Survey Response
1	No change to the policy for on-site parking requirements (i.e. 33% reduction).	24.6%
2	Decrease the allowable parking reduction from up to 33% to up to 13% for new residential development (recommended in May 30 staff report).	75.4%

Staff Recommendation: Amend the Heritage (Section 4.0) and Transportation (Section 5.0) in the Steveston Area Plan to reflect Option 2 above.

The recommended amendment to the Steveston Area Plan to reflect the change in Option 2 also includes policies to provide direction on all parking reduction considerations to help achieve the City’s heritage conservation and management objectives in the Steveston Village Heritage Conservation Area, which have been applied in varying forms to redevelopments in the Steveston Village Core Area since 2009. The recommended parking reduction policies to be included in the Steveston Area Plan are summarized as follows:

- Consideration of parking reductions to be assessed through the applicable required development application,
- For development of new residential uses, a 13% reduction from applicable Zoning Bylaw parking requirements can be considered,
- For development of new commercial uses, a 33% reduction from applicable Zoning Bylaw parking requirements can be considered, and
- Required on-site residential visitor parking and other non-residential use parking (i.e., commercial) may be shared.

In accordance with Zoning Bylaw regulations specific to on-site parking, if the application of a parking reduction at the identified rate results in a fractional figure, it is rounded up to the nearest whole number.

2. Stakeholder Consultation

In addition to the public open house sessions in July, staff also engaged with stakeholders to consult on the Steveston Area Plan recommended changes and long-term streetscape visions for Bayview, Moncton and Chatham Street as outlined in the report reviewed and endorsed by Council in June 2017.

Steveston Harbour Authority

Staff met directly with the Steveston Harbour Authority (SHA) on July 26, 2017. The SHA forwarded a letter to the City following this consultation session (Attachment 5). A summary of the SHA comments is provided as follows:

- No issues with the proposed changes and/or clarifications pertaining to density, building height exterior finishing and rooftop structures.
- Concerns noted about the proposal for a contiguous riverfront walkway along the Steveston Village Riverfront Area, which could pose conflicts to the use and operation of the existing public fish sales dock area.
- Concerns about identifying the development potential for lots in the Steveston Village Riverfront Area, which are federally owned and managed by the SHA, and used to directly support the industry operating out of the harbour.

In response to comments from the SHA, staff propose to continue to work collaboratively with the SHA to ensure that their concerns are addressed and that they can continue the safe and secure operations of the harbour for the commercial fishing fleet. Staff recommended that the amendments to the Steveston Area Plan, as reflected in the public consultation survey results and outlined in this report, remain, as they will not negatively impact SHA operations.

Additional comments in the SHA's letter that were not part of the topics being addressed in the proposed land use and streetscape vision change included:

- Translink's long-term plans for a possible Steveston bus loop/exchange and its potential to negatively impact SHA supporting land along Chatham Street, and
- The City's identification of SHA's harbour infrastructure (e.g., piers, floats) in the Steveston Village Riverfront Area as heritage resources, may potentially negatively impact the SHA's operation of the harbour.

A proposed upgraded bus exchange in Steveston is to be included in TransLink's Phase 3 (Years 6-10) initiative which is part of the Mayors' Council 10-Year Vision and will also be identified in TransLink's draft Southwest Area Transport Plan which is anticipated over the next 5 years when Translink is anticipated to provide more details. The current and proposed changes to the Steveston Area Plan do not lessen the SHA's authority or ability to provide needed services along the Riverfront to support the commercial fishing fleet. More information and additional details on transit infrastructure proposed in Steveston by TransLink will come once work on Phase 3 of the 10-Year Vision commences, which is anticipated over the next 5 years. The current Steveston Area Plan allows for and supports SHA operations and use of the riverfront in support of the commercial fishing fleet.

Richmond Heritage Commission

Staff presented the proposed Steveston Village Conservation Area changes and Long-Term Streetscape Visions to the Richmond Heritage Commission (RHC) as part of the stakeholder consultation. The RHC was supportive of the staff recommended changes.

Steveston 20/20

On September 14, 2017, at the Steveston 20/20 Group's invitation, City staff presented the proposed Steveston Area Plan changes. At the meeting, the Group provided feedback on the Streetscape Options only for each street but did not complete a City survey. As the Steveston 20/20 Group itself declined to comment, it was left for the individual Steveston 20/20 Group's members to comment, if they wished by September 20, 2017.

Only one Steveston 20/20 Group member commented and can be found in Attachment 6.

Individual/Stand-alone Letters

Staff received one stand-alone letter from Oris Consulting (Attachment 7) communicating that the proposed changes to the Steveston Area Plan are generally supported and will benefit the area as a whole. The proposed changes would allow Village site specific factors to be considered on a case by case basis (e.g., roof top access structures). Staff also received a letter from Vancouver Coastal Health (Attachment 8) who were supportive of the long-term streetscape visions which support healthy communities.

3. Other Staff Recommendations

Establishing Geodetic Reference Points in the Steveston Village Core and Riverfront Areas

Staff recommend clarifying the following Geodetic Point reference elevations in the Steveston Area Plan, to ensure that the current street and ground elevations are recognized and retained, to achieve uniform building heights and safety, as Village development occurs. The clarified points do not change the maximum permitted heights of buildings.

- For properties in the Steveston Village Core, north of Bayview Street, the higher elevation of 1.4 m GSC or an existing adjacent sidewalk shall be referenced. The proposed 1.4 m GSC baseline is the elevation at the intersection of 3rd Avenue and Moncton Street which is a unique, historic feature of the Village Core that should be retained.
- For properties located in the Steveston Village Riverfront Area, south of Bayview Street, the higher elevation of 3.2 m GSC or existing adjacent sidewalks (e.g., the sidewalk in front 3531 Bayview Street ranges from 3.2m to 3.4m) shall be used.

Protected Heritage Properties – Renewable Energy Infrastructure

Staff recommend the continued use of the 2009 Council adopted Parks Canada, “Standards and Guidelines for the Conservation of Historic Places in Canada” document which established best practices for how the City will conserve the 17 protected Village heritage properties.

The Parks Canada, “Standards and Guidelines for the Conservation of Historic Places in Canada” document includes sustainability guidelines for the installation of renewable energy infrastructure (e.g., solar panels, air source heat pumps). Staff examined the visibility of placing renewable energy building infrastructure on flat and pitched roofs of the protected heritage properties from the street. The analysis indicates that it may be possible to install solar panels on flat and front-gable roofed buildings, if the panels are tucked behind false parapets and away from roof edges for facades along the street or lanes.

The recommendation supports owner and developer voluntary installation of renewable energy infrastructure (e.g., solar panels, air source heat pumps), while continuing to protect the 17 identified Village heritage properties through the application of the Parks Canada, “Standards and Guidelines for the Conservation of Historic Places in Canada”.

For clarity, in the Steveston Village Heritage Conservation Area, the Parks Canada, “Standards and Guidelines for the Conservation of Historic Places in Canada” document applies to the 17 protected heritage properties, to conserve the exteriors of the buildings.

For the remaining non-heritage properties contained in the Steveston Village Heritage Conservation Area, the policies and guidelines contained in the Steveston Area Plan (including recommended changes in this report) shall apply.

This approach would ensure the maximum flexibility in finding solutions for each of the 17 identified Village heritage properties, which is a principle of the City’s adopted Parks Canada’s

National Standards and Guidelines, when managing modifications and additions to existing buildings and new development in the area.

View Corridors and Location of Pedestrian Connections – Bayview Street to the Waterfront

Staff recommend not changing the current Steveston Area Plan DPA/HCA Riverfront Sub-Area guidelines which are intended to address views and pedestrian connectivity from Bayview Street to the waterfront. The existing guidelines identify the desired outcomes that new development should achieve while allowing flexibility for designers to respond to the site-specific conditions and context.

Sakamoto Guidelines

Staff recommend maintaining the spirit and intent of the Sakamoto Guidelines, which have been an integral part of the Steveston Area Plan since 1989. The Sakamoto Guidelines were originally developed to assist in the restoration of the facades of existing heritage buildings in Steveston Village, as well as other non-heritage buildings. As part of the proposed bylaw amendments that reflect the most recent stakeholder and public consultation, major elements of the Sakamoto Guidelines are still included in the design guidelines of the Steveston Area Plan. Certain elements have been updated including the use of certain building materials, incorporating solar panels, and rooftop living spaces.

Staff have prepared Bylaw 9775 which would incorporate the above recommendations into the design and heritage policies of the Steveston Area Plan.

Part B – Streetscape Vision for Bayview, Chatham and Moncton Street

1. Public Consultation Results

A total of 120 streetscape surveys were completed (93 on-line and 27 paper). The Steveston 20/20 Group provided feedback on the streetscape options only for each street but did not complete a City survey. A stand-alone letter was also received from Vancouver Coastal Health that expressed its preferred streetscape option for each street. For those who completed the City survey, the majority of respondents (63%) live within one kilometre of Steveston Village and of those, 28% live within 400 metres of the Village. A further 34% live in Richmond beyond one kilometre of the Village. Given respondents' proximity to Steveston Village, they regularly visit the area: 65% visit more than three times per week and a further 22% visit one to three times per week. The prevalent modes of travel are walking (53%), vehicle as a driver or passenger (34%) and cycling (9%). Listed below are the survey results and the staff recommendation for the question in the streetscape survey regarding the preferred option for each street.

Bayview Street

Question 4		
I have the following comments on Options 1 through 3 for Bayview Street		
Option	I think these features are important	I think these features are NOT important
Option 1 (Enhanced Pedestrian Realm on North Side Only)	<ul style="list-style-type: none"> Improved pedestrian realm (26%) Maintain on-street parking (18%) Consider directional bike lanes/paths (7%) Consider closing Bayview Street to vehicle traffic (5%) Addition of benches and landscaping (4%) 	<ul style="list-style-type: none"> Improved pedestrian realm as existing sidewalk is wide enough (11%) Addition of benches and landscaping (10%) Maintaining existing parking spaces (10%)
Option 2 (Enhanced Pedestrian Realm on North & South Sides)	<ul style="list-style-type: none"> Improved pedestrian realms (18%) Maintain on-street parking (10%) Addition of benches and landscaping (6%) Consider closing Bayview Street to vehicle traffic (3%) 	<ul style="list-style-type: none"> Loss of on-street parking (10%) Improved pedestrian realm as existing sidewalk on south side is wide enough (9%) Widen pedestrian realm on north side only (3%)
Option 3 (Enhanced Pedestrian Realm on North & South Sides plus Bikeway)	<ul style="list-style-type: none"> Cycling facilities (28%) Improved pedestrian realms (28%) Consider directional bike lanes/paths (7%) Maintain on-street parking (6%) 	<ul style="list-style-type: none"> Cycling facilities (15%) Improved pedestrian realm as existing sidewalk widths are sufficient (7%) Loss of on-street parking (6%)

Question 5		
I prefer the following streetscape vision for Bayview Street		
Options		Survey Response ¹
Status Quo	No changes to existing streetscape	11%
1	Enhanced Pedestrian Realm on North Side Only: no change to the existing curbs, wider pedestrian realm on north side (7.5 m) and retention of on-street parking on south side	25%
2	Enhanced Pedestrian Realm on North & South Sides: wider pedestrian realm on north side (7.5 m), remove on-street parking on south side and move south curb to the north by 2.5 m, and wider pedestrian realm on the south side (up to 4.75 m)	11%
3	Enhanced Pedestrian Realm on North & South Sides plus Bikeway: wider pedestrian realm on north side (6.0 m), move north curb to the north by 1.5 m, remove on-street parking on south side and move south curb to the north by 1.0 m, wider pedestrian realm on the south side (3.25 m), and two-way protected on-street cycling facility on south side (3.0 m)	32%
Don't Know/ No Response		7%
Other (i.e., close Bayview Street to vehicle traffic; convert Bayview Street to one-way vehicle traffic, keep on-street parking while widening on the south side only or on both sides; provide bike lanes while also keeping on-street parking)		14%

¹ Members of the Steveston 20/20 Group expressed the highest interest in Option 3 (11 of 16 responses or 69%) followed by Option 1 (7 of 13 responses or 54%) and Option 2 (two of 16 responses or 12.5%).

Staff Recommendation: Option 3, which originally comprised shifting both curbs, wider pedestrian realms on the north and south sides, the removal of on-street parking on the south side, and the provision of a two-way protected cycling facility on the south side, with the following modifications to address concerns identified by survey respondents:

- *Passenger Loading*: to mitigate the loss of on-street parking on Bayview Street that may impact visitors with mobility challenges seeking access to the waterfront, the existing parking lay-by on the north side near No. 1 Road would be retained and converted to a passenger loading zone to allow short-term pick up and drop off (e.g., 15 minute time limit). An additional lay-by on the north side for passenger loading would be established to the west between Second Avenue and Third Avenue. The pedestrian realm on the north side would be narrowed by approximately 2.5 m at these locations to accommodate the lay-bys.
- *Accessible Parking Space*: the existing on-street parking on Bayview Street includes one designated accessible parking space. To mitigate the loss of this parking space, additional accessible parking spaces would be designated on First Avenue and Second Avenue as close as possible to Bayview Street.
- *Design of Cycling Facility*: modification of the proposed two-way on-street protected cycling facility on the south side to directional bike lanes on either side of the street, which would provide more convenient access for cyclists, minimize confusion for pedestrians at crossings, and be consistent with the proposed cycling facilities on Chatham Street. Both the westbound and eastbound bike lanes would be located on the street as there is insufficient right-of-way to accommodate off-street facilities while maintaining adequate width for the pedestrian realm. An on-street cycling facility is considered acceptable given the lower vehicle speeds of 30 km/h.

The recommended modified Option 3 would result in the loss of 17 on-street parking spaces, which represents a relatively small proportion (10%) of the overall public parking available in the immediate vicinity of Bayview Street. Parking demand could be accommodated when on-street public parking immediately adjacent to the Steveston Village core is included (e.g., Chatham Street west of 3rd Avenue has sufficient capacity of approximately 54 spaces to fully accommodate future parking demand).

Attachment 9 illustrates a typical cross-section and plan view for the recommended modified Option 3 for Bayview Street. Attachment 10 indicates that recommended streetscape option could be implemented along the majority of both sides of the street (yellow shaded areas) with the exception of two areas where there would be private property impacts (pink shaded areas).

The current cost estimate (2017\$) for the recommended improvements is \$1.6 million. Staff propose to bring forth a future report detailing the implementation strategy for the recommended improvements including updated and more detailed cost estimates, boulevard surface finish (e.g., brick or concrete stamped to simulate bricks), timing, and funding sources. For any in-stream development applications where the frontage works have already been completed or designed, the modification of the public realm to be consistent with the recommended streetscape vision would be undertaken via the proposed implementation strategy.

Chatham Street

Question 6		
I have the following comments on Options 1 and 2 for Chatham Street		
Option	I think these features are important	I think these features are NOT important
Option 1 (Enhanced Pedestrian Realm on North & South Sides)	<ul style="list-style-type: none"> Improved pedestrian realms (20%) Maintaining on-street parking (16%) Addition of trees, benches and landscaping (8%) Vehicle access from the rear lane on the north side (7%) Need for cycling facilities (7%) 	<ul style="list-style-type: none"> Improved pedestrian realms as existing widths are sufficient (16%) Addition of benches not needed (5%) Shorter crossing distances (2%)
Option 2 (Enhanced Pedestrian Realm on North & South Sides plus Bike Paths)	<ul style="list-style-type: none"> Provision of cycling facilities (39%) Improved pedestrian realms (17%) Maintaining on-street parking (10%) Addition of trees, benches and landscaping (5%) Vehicle access from the rear lane on the north side (5%) 	<ul style="list-style-type: none"> Provision of cycling facilities (16%) Improved pedestrian realms as existing widths are sufficient (8%) Shorter crossing distances (2%) Addition of trees, benches and landscaping (2%)

Question 7		
I prefer the following streetscape vision for Chatham Street		
Options		Survey Response ²
Status Quo	No changes to existing streetscape	18%
1	Enhanced Pedestrian Realm on North & South Sides: no change to the existing curbs, wider pedestrian realms on north side (7.0 m) and south side (6.4 m), and retention of on-street parking on both sides	17%
2	Enhanced Pedestrian Realm on North & South Sides plus Bike Paths: shift north and south curbs into the roadway by 1.25 m each, wider pedestrian realms on north and south sides as in Option 1, retention of on-street parking on both sides, and delineated off-street directional cycling paths	51%
Don't Know/ No Response		11%
Other		3%

Staff Recommendation: Option 2, which comprises shifting the north and south curbs into the roadway, wider pedestrian realms on both sides, and delineated off-street directional cycling paths.

A 30 km/h speed limit is currently in place for the Steveston Village core bounded by No. 1 Road, Bayview Street, 3rd Avenue, and Chatham Street. Staff recommend extending the boundary of the 30 km/h speed limit on Chatham Street from 3rd Avenue west to 7th Avenue to

² Members of the Steveston 20/20 Group expressed the highest interest in Option 2 (8 of 16 responses or 50%) followed by Option 1 (three of 16 responses or 19%).

provide consistency along the length of the street. Following implementation, staff will continue to monitor vehicle speeds to determine if further traffic calming measures are needed.

The recommended streetscape vision Chatham Street also includes curb bulges at each intersection; the temporary curb bulges on Chatham Street at 4th Avenue would be replaced with new bulges. Staff would ensure that the design of new bulges can accommodate the turning movements of trucks and buses. Attachment 11 illustrates a typical cross-section for Chatham Street. Attachment 12 indicates that recommended streetscape option could be implemented along the both sides of the street (yellow shaded areas) with the exception of areas where there would be private property impacts (pink shaded areas) or the extent of implementation would be limited due to the presence of driveways (green shaded areas).

The current cost estimate (2017) for the recommended improvements is \$3.2 million. Staff propose to bring forth a future report detailing the implementation strategy for the recommended improvements including updated and more detailed cost estimates, boulevard surface finish (e.g., brick or concrete stamped to simulate bricks), timing, and funding sources. For any in-stream development applications where the frontage works have already been completed or designed, the modification of the public realm to be consistent with the recommended streetscape vision would be undertaken via the proposed implementation strategy.

Moncton Street

Question 8		
I have the following comments on Option 1 for Moncton Street		
Option	I think these features are important	I think these features are NOT important
Option 1 (Modified Curb Bulges and Blvd Surface plus 2 New Mid-Block Crossings)	<ul style="list-style-type: none"> Modified curb bulges with ramps (16%) Additional mid-block crossings (13%) Maintain on-street parking (9%) 	<ul style="list-style-type: none"> Additional mid-block crossings (8%) Modified curb bulges with ramps due to less protection for pedestrians (7%) Modified curb bulges with ramps not needed (6%)

Question 9		
I prefer the following streetscape vision for Moncton Street		
Options		Survey Response ³
Status Quo	No changes to existing streetscape	31%
1	Modified Pedestrian Realm: modify curb bulges (remove unit pavers and add asphalt ramps) and boulevard, add mid-block crossings	42%
Don't Know/ No Response		15%
Other (i.e., close Moncton Street to vehicle traffic; provide ramps but no curb bulges; provide a widened pedestrian realm; convert Moncton Street to one-way)		12%

³ Members of the Steveston 20/20 Group expressed the highest interest in Option 1 (11 of 16 responses or 69%).

Staff Recommendation: Option 1, which comprises the removal of unit pavers and provision of asphalt ramps with a rollover curb at the curb bulges, replacement of the boulevard surface (e.g., brick or concrete stamped to simulate bricks), addition of new mid-block crossings, and retention of on-street parking on both sides. In addition, wooden bollards (similar to that in place at Moncton Street-No. 1 Road) would be added at the edge of the ramps to enhance pedestrian safety in response to concerns expressed by respondents.

Attachment 13 provides a rendering of the modified curb bulges and boulevard surface.⁴ The current cost estimate (2017\$) for the recommended improvements is \$1.1 million. Staff propose to bring forth a future report detailing the implementation strategy for the recommended improvements including updated and more detailed cost estimates, boulevard surface finish (e.g., brick or concrete stamped to simulate bricks), timing, and funding sources. For any in-stream development applications where the frontage works have already been completed or designed, the modification of the public realm to be consistent with the recommended streetscape vision would be undertaken via the proposed implementation strategy.

2. Steveston Interurban Tram

At its September 11, 2017 meeting, Council approved the allocation of \$50,000 from Council Contingency to undertake a feasibility study, including a business case and transportation and engineering analysis, of operating the Steveston Interurban Tram between the existing tram building at No.1 Road and Moncton Street and the Gulf of Georgia Cannery. As noted in the staff report on the topic, none of the recommended long-term streetscape options would preclude a future operating tram. For example, if the tram were to operate on Bayview Street, the tracks could be laid within the vehicle portion of the roadway in combination with: (1) conversion of Bayview Street to one-way (i.e., the tram and vehicles each operate on one-half of the street); or (2) removal of the bike lanes and the re-allocation of that space to the tram with cyclists then operating with vehicle traffic, which could be accommodated given the 30 km/h speed limit. Staff will work with the feasibility study team to ensure that all users are accommodated within any potential tram route.

3. One-Way Street System in Steveston Village

As noted above, some survey respondents and open house attendees suggested consideration of a one-way street system in the Steveston Village core utilizing Moncton and Bayview Streets between No. 1 Road and 3rd Avenue to form an east-west couplet. Feedback from the Steveston 20/20 Group also indicated interest in a one-way street system (13 of 16 responses) that would comprise westbound only on Moncton Street and eastbound only on Bayview Street.

Staff have previously investigated potential one-way street systems for Steveston Village and, most recently, sought public feedback on a proposed one-way street system in June 2006 as part of a consultation process on parking options in Steveston Village. As the feedback results did not indicate strong support for converting selected two-way streets to one-way streets, staff recommended the status quo, which was endorsed by Council. At the time, staff noted that the

⁴ Note that the rendering does not show the bollards recommended by staff; these would be included as part of the detailed design of the improvements.

existing road patterns functioned well and establishing more one-way streets could impact the exposure and access to businesses on those streets and lead to more vehicle circulation within the Village. None of the recommended long-term streetscape options would preclude a future one-way street system in Steveston Village should there be an interest in pursuing this concept pending the outcome of the tram feasibility study.

Consultation

Staff have reviewed the proposed 2041 OCP amendment bylaw with respect to the *Local Government Act* and the City’s OCP Bylaw Preparation Consultation Policy No. 5043 requirements. Table 4 clarifies this recommendation. Public notification for the public hearing will be provided as per the *Local Government Act*.

Table 4 – OCP Public Consultation Summary

Stakeholder	Referral Comment
Provincial Agricultural Land Commission	No referral necessary, as they are not affected.
Richmond School Board	No referral necessary, as they are not affected.
The Board of the Greater Vancouver Regional District (GVRD)	No referral necessary, as they are not affected.
The Councils of Adjacent Municipalities	No referral necessary, as they are not affected.
First Nations (e.g., Sto:lo, Tsawwassen, Musqueam)	No referral necessary, as they are not affected.
TransLink	No referral necessary, as they are not affected.
Port Authorities (Port Metro Vancouver and Steveston Harbour Authority)	No referral necessary, as they are not affected.
Vancouver Airport Authority (VAA) (Federal Government Agency)	No referral necessary, as they are not affected.
Richmond Coastal Health Authority	No referral necessary, as they are not affected.
Community Groups and Neighbours	Community Groups (e.g., Group of 20/20, Steveston Harbour Authority) and Neighbours will have the opportunity to comment regarding the proposed OCP amendment (and proposed Zoning Bylaws) at Planning Committee, Council and at a Public Hearing.
All Relevant Federal and Provincial Government Agencies	No referral necessary, as they are not affected.

Financial Impact



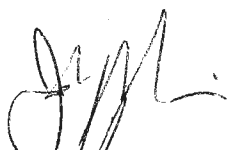
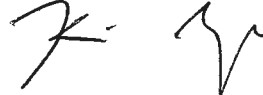
With respect to the recommended long-term streetscape visions, staff propose to report back with an implementation strategy for the improvements including updated and more detailed cost estimates, timing and funding sources.

Conclusion

The recommended design and heritage policies in the Steveston Area Plan and the long-term streetscape design concepts for Bayview Street, Chatham Street and Moncton Street reflect the

public feedback received, are supportive of the heritage character of Steveston and improve the public realm with wider sidewalks and boulevards, more benches and street trees, increased accessibility, and opportunities for active transportation to reduce reliance on private auto trips to the Village. These long-term visions will help provide clarity and guidance for future development to realize the community’s vision for these key streets in the Steveston Village area.

It is recommended that Bylaw 9775 be introduced and given first reading.

			
Joan Caravan Transportation Planner (604-276-4035)	Sonali Hingorani Transportation Engineer (604-276-4049)	John Hopkins Senior Planner (604-276-4279)	Kevin Eng Planner 2 (604-247-4626)

JC/SH/JH/KE:cas

- Att. 1: Open House Boards: Steveston Area Plan Update and Streetscape Concepts
- 2: Open House Boards: Long-Term Streetscape Visions for Bayview Street, Chatham Street and Moncton Street
- 3: Open House Survey: Steveston Area Plan Update – Design and Heritage Policies Survey
- 4: Open House Survey: Long-Term Streetscape Visions for Bayview Street, Chatham Street & Moncton Street: Public Feedback Form
- 5: Letter from Steveston Harbour Authority dated August 22, 2017
- 6: Survey Results from Steveston 20/20 Group Member dated September 26, 2017
- 7: Letter from Oris Consulting Ltd. dated July 28, 2017
- 8: Letter from Vancouver Coastal Health dated July 28, 2017
- 9: Typical Cross Section and Plan View of Recommended Streetscape Design for Bayview Street
- 10: Bayview Street: Timing of Implementation of Recommended Streetscape Improvements
- 11: Typical Cross Section of Recommended Streetscape Design for Chatham Street
- 12: Chatham Street: Timing of Implementation of Recommended Streetscape Improvements
- 13: Rendering of Recommended Streetscape Design for Moncton Street

Welcome To This Open House

Why are we here?

Since the Steveston Area Plan was updated in 2009, there have been some concerns in the community about how new development fits into the special character of Steveston.

The public realm is an important part of the uniqueness of Steveston, and streetscape concept visions for Bayview, Chatham and Moncton Streets are long-term objectives.

On June 12, 2017, Council directed staff to:

- Undertake public consultation on proposed changes to the design and heritage policies in the Steveston Area Plan, and streetscape concepts for Bayview Street, Chatham Street and Moncton Street.
- Complete engagement by July 31, 2017
- Report back in October 2017 on feedback and recommendations.

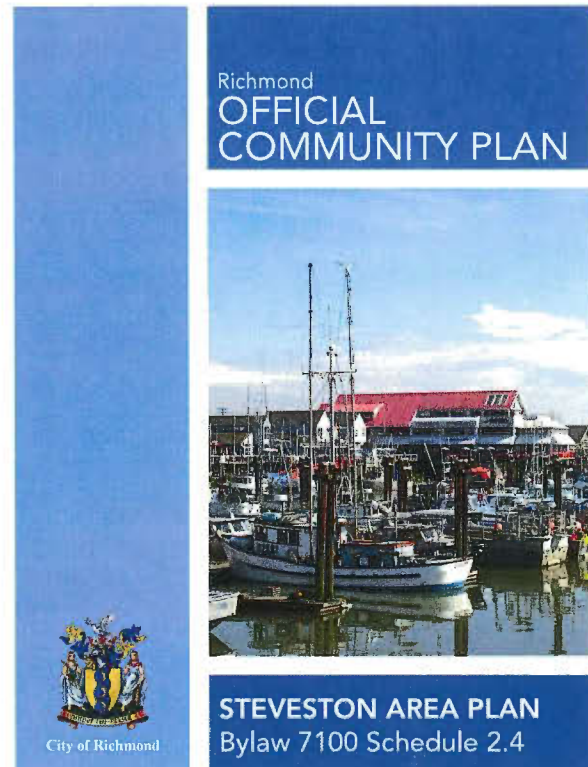
Today's Open House is an opportunity to:

- Learn more about design and heritage policies in the Steveston Area Plan.
- Review options and proposed changes to design and heritage policies in the Plan.
- Review options for streetscape concepts for Bayview Street, Chatham Street and Moncton Street.
- Ask questions and give feedback.

More information

www.richmond.ca

communityplanning@richmond.ca



Have Your Say

- Talk to City staff
- Fill out a Let's Talk Richmond survey today and drop it off with staff or mail it back to us (to the address on the form).
- Complete a Let's Talk Richmond survey at www.richmond.ca
- Stay informed through visiting the project website following the links from the homepage at www.richmond.ca

Context: How Is Change To Properties Managed In Steveston Village?

Steveston Village is the area within the boundaries generally between 3rd Avenue to the west, No. 1 Road to the east, Chatham Street to the north, and Bayview Street and the riverfront to the south.

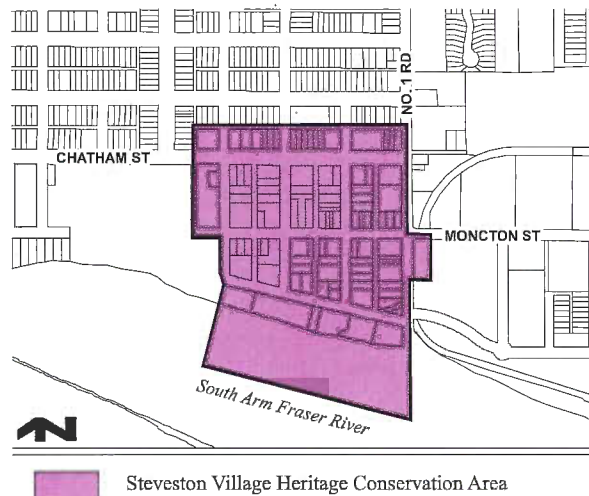
Changes to buildings, structures, landscaping and land in Steveston Village are managed through a Heritage Conservation Area (HCA) and a Development Permit Area (DPA).

Steveston Village Heritage Conservation Area (HCA)

The purpose of the HCA is to conserve the heritage value and special character of Steveston Village through HCA guidelines.

For changes to 17 protected heritage properties, ("identified heritage resources" on the bottom map), the City uses *The National Standards and Guidelines for the Conservation of Historic Places in Canada*.

The HCA guidelines that apply to all other properties in Steveston Village are the same as the DPA guidelines.



Steveston Village Development Permit Area (DPA)

The purpose of the DPA is to manage the appearance of new development, and façade upgrades (over \$50,000), to fit within the special character of Steveson Village.

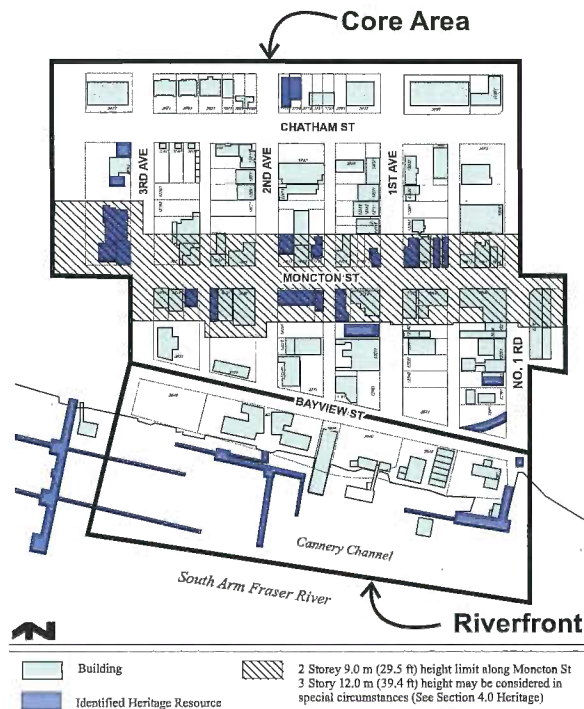
The DPA has two-sub-areas:

- Village Core
- Riverfront Precinct

The entire DPA has general guidelines, and there are additional special guidelines for each of the sub-areas.

The design vision for the Village Core is relatively small lots, and buildings that reflect the historical mixed-use.

This contrasts to the vision for the Riverfront Precinct which is larger 'Cannery-like' buildings and larger lots.



Land Use Density and Heights in the Village Core

What are the issues?

- There have been recent community concerns about the size, scale and height of Moncton Street development and a preference for two-storey buildings has been raised.
- There have been similar concerns about the size, scale and height of development along the north side of Bayview Street, and a desire for lowering the building height has been raised.
- There is some lack of clarity about technical aspects of how to measure the building heights in Steveston Village.

What is included in the Steveston Area Plan today?

Moncton Street

Maximum density: 1.2 FAR.

Maximum height: Up to 2 storeys and 9 m and eligibility for 1 in 3 buildings to be 3 storeys and 12 m.

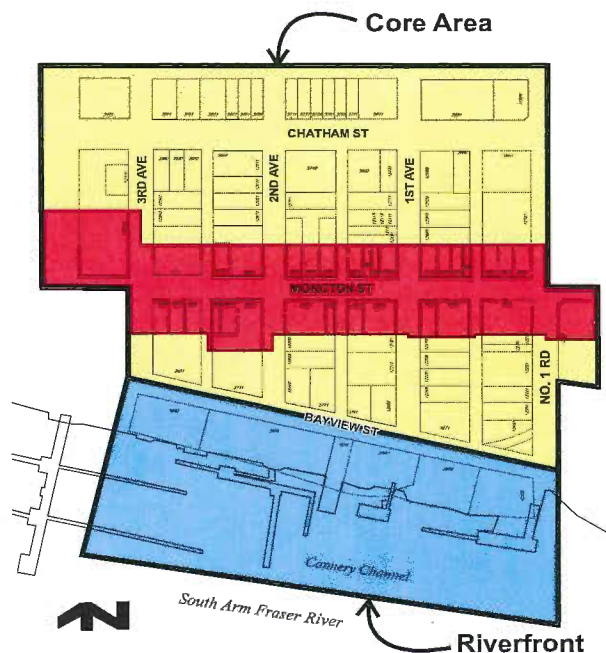
Bayview Street (north side)

Land Use Density: 1.6 FAR.

Building Height: 3 storeys over parkade.

Density & heights in Steveston Village

	Maximum FAR	Maximum Storeys	Maximum Building Height
Core Area, generally	1.6	3	12 m
Moncton Street	1.2	2	9 m
Riverfront Area	1.6	3	20 m GSC



Have Your Say

Tell us what you support.



Moncton Street

- **Option 1:** No change.
- **Option 2:** Reduced height: 1.2 FAR and 2 storeys and 9 m. ***staff recommendation***

Bayview Street (north side)

- **Option 1:** No change.
- **Option 2:** Reduced density and height: 1.2 FAR; and

For the north 50% of any lot depth, up to 2 storeys over parkade (looks like 3 storeys).

For the south 50% of any lot depth, up to 2 storeys over parkade (looks like 2 storeys).

staff recommendation

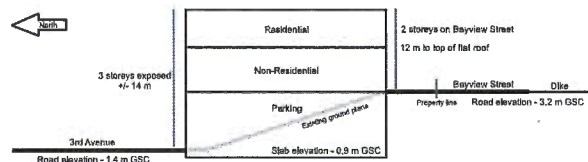
Add comments here

Technical measurement of building height

To provide clarity for designers, engineers and property owners, staff are recommending the use of "geodetic points" for height measurements.

A **geodetic point** is a reference point on the earth from which to calculate the height of buildings and structures (e.g. parkades). It provides consistency in determining the height of buildings and structures.

How to measure (geodetic) height



Design Guidelines for Exterior Cladding and Window Treatments

What are the issues?

- The materials for exterior cladding and window treatments should fit with the special character of Steveston Village.

What is included in the Steveston Area Plan today?

General guidelines for Steveston Village Core & Riverfront

Exterior cladding:

- Horizontal wood siding with complementary glass, concrete, stucco and metal for siding.
- Brick is allowed.
- Vinyl siding is prohibited.

Window treatments:

- Wood frames are encouraged.
- Vinyl frames are discouraged but not banned.

* Choices of exterior cladding and windows for the 17 heritage properties must be in keeping with unique features of each building.

Exterior Cladding: primary finishes

Wood is the primary material for new buildings but is currently limited to horizontal siding.

Staff recommend that siding choices include vertical ship lap, board-and-batten, and wood shingles which were used historically and in the earlier Sakamoto Guidelines until 2009.



Have Your Say

Tell us what you support.

Village Core (includes north Bayview)
Exterior cladding: secondary finishes

- **Option 1:** No change.
- **Option 2:** For new buildings and additions, no brick and no metal allowed. For façade upgrades, *replace brick with similar brick.*
- **Option 3:** For new buildings and additions, no brick and no metal allowed. For façade upgrades, *replace brick with similar brick or different brick.*
- **Option 4:** For new buildings and additions, no brick and no metal allowed. For façade upgrades, *replace brick with similar brick or different brick or other better material.*
- **Option 5:** For new buildings and additions, no metal but brick is allowed if different from the Hepworth building. For façade upgrades, *replace brick with similar brick or different brick.*
- **Option 6:** For new buildings and additions, no metal but brick is allowed if different from the Hepworth building. For façade upgrades, *replace brick with similar brick or different brick or better material.* ***staff recommendation***

Add comments here



Have Your Say

Tell us what you support.

Window treatments

- **Option 1:** Wood, vinyl and metal frames are allowed.
- **Option 2:** Wood and metal frames are allowed. Vinyl is prohibited. ***staff recommendation***

Add comments here

Brick in the Village Core

The Hepworth Building is the only heritage property with brick masonry.

There are 13 non-heritage buildings with brick features in a variety of colours and textures. Some of the brick is painted.



Please fill out the Feedback form as you view the display boards.



Design Guidelines for Rooftop Structures

What are the issues?

- Minimizing the visibility of solar panels, and other renewal energy infrastructure (i.e. air source heat pumps), that is mounted on the exterior of new and existing buildings is important to help retain the special character of Steveston Village.
- Barriers around rooftop living spaces, which provide safety, should blend with the special character of the Village.

Solar panels and other renewable energy infrastructure (e.g. air source heat pumps)

The National Standards and Guidelines for the Conservation of Historic Places in Canada, which apply to the 17 protected heritage properties, require solar panels, and other infrastructure, to not be visible from the street.

Existing design guidelines for non-heritage properties include a requirement for solar panels on flat roofs to be located back from the building edges. There are no guidelines for other infrastructure (e.g. air source heat pumps), or pitched roofs.

Rooftop barrier railings

Like solar panels and other renewal energy infrastructure, barrier railings for rooftop living spaces in Steveston Village should fit into the special character of the historic area.

There are no existing design guidelines for barrier railings.

Have Your Say

Tell us what you support.

- **Option 1:** No changes to existing design guidelines.
- **Option 2:** New additional design guidelines that require false parapets on new flat-roofed buildings to be slightly higher and to allow solar panels affixed on pitched roofs. ***staff recommendation***



Have Your Say

Tell us what you support.

- **Option 1:** No changes to existing design guidelines.
- **Option 2:** New design guidelines for barrier railings to be simple in design, and primarily consist of glazed panels to minimize visibility from streets and nearby rooftop patios. ***staff recommendation***



Add comments here



Solar panels behind a false parapet on a flat roof



Barrier railings for a rooftop patio (Victoria, BC)

Design Guidelines for Rooftop Structures

What are the issues?

- There have been recent community concerns about the visibility of elevator shafts for communal rooftop living spaces and hatch (or 'pop-up') entries for individual rooftop living spaces.
- Managing the visibility of rooftop access points is important to retain the special character of Steveston Village, and can be achieved through blending hatch or 'pop-up' stair entries, access stairs, or elevator shafts, with the overall architecture.

Hatch or 'pop-up' entries

There are no existing design guidelines for hatch (or 'pop-up') stair entries for individual rooftop living spaces.

Elevator shafts and access stairs

There are no existing design guidelines for structures for access stairs or elevator shafts for communal rooftop living spaces.

Have Your Say

Tell us what you support.

- **Option 1:** No changes to existing design guidelines.
- **Option 2:** Prohibit all hatch stair entries.
- **Option 3:** Prohibit all hatch stair entries unless they are not more than 1.83 m (6ft.) in height, well-integrated with the architecture and setback 1.0 m or more from all roof edges.
staff recommendation
- **Option 4:** Allow hatch stair entries if well-integrated with the overall architecture, and set back from all roof edges.

Add comments here

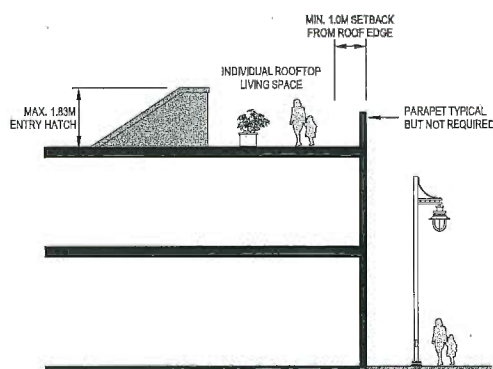


Have Your Say

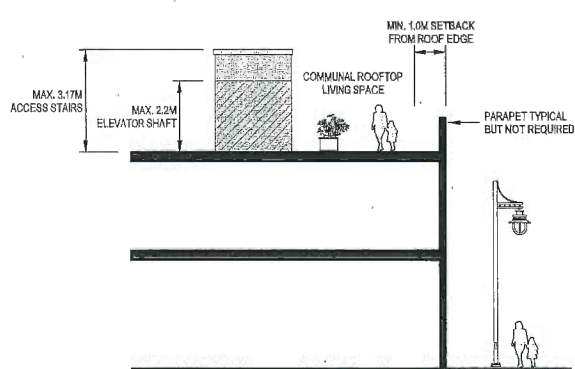
Tell us what you support.

- **Option 1:** No changes to existing design guidelines.
- **Option 2:** Prohibit all elevator shafts and access stairs.
- **Option 3:** Prohibit all structures unless they are not more than 2.20 m (7.2 ft.) for elevator shafts, and 3.17 m (10.4 ft.) for access stairs, well-integrated with the architecture and setback 1.0 m or more from all roof edges.
staff recommendation
- **Option 4:** Allow structures for elevator shafts and access stairs if well-integrated with the overall architecture, and set back from all roof edges.

Add comments here



Cross-section of hatch entry



Cross-section of access stairs and elevator shafts

Design Vision for Riverfront Precinct

What are the issues?

- The City is seeking to reconfirm if the community supports the current density and heights on south Bayview Street.
- There has been a lack of clarity about whether flat roofs should be allowable along the south side of Bayview Street.

Density and heights on Bayview Street (south)

Have Your Say

Tell us what you support.

- **Option 1:** 1.6 FAR and 3 storeys (no change).
staff recommendation
- **Option 2:** Reduced density or reduced height.

Add comments here



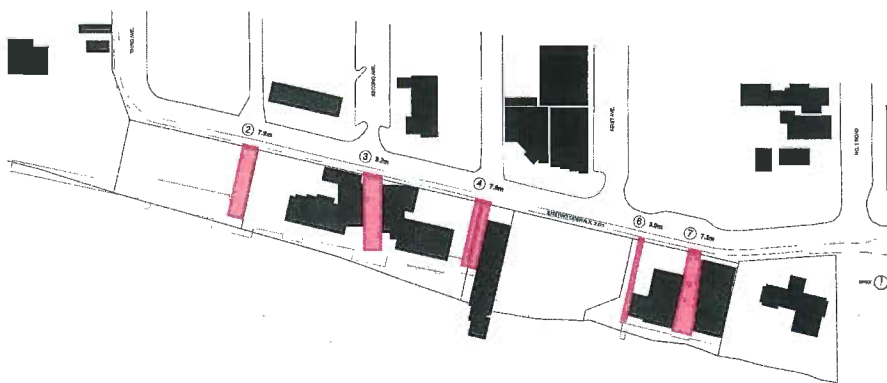
Roofs types on Bayview Street (south)

Have Your Say

Tell us what you support.

- **Option 1:** Flat roofs, or pitched, roofs (no change).
- **Option 2:** Pitched roofs. Flat roofs are prohibited.
staff recommendation

Add comments here



Properties along Bayview Street (south)

■ EXISTING CONNECTION AND EXISTING WIDTH

Existing Condition



Model of existing buildings on Bayview Street (south)

Design Vision for Riverfront Precinct

What are the issues?

- There has been some interest in the recent past in the subdivision of large lots on the south side of Bayview Street, between 3rd Avenue and No. 1 Road, into smaller lots with smaller buildings.

Lot sizes on Bayview St. (south)

Have Your Say

Tell us what you support.

- Option 1:** Large lots (no change).
staff recommendation
- Option 2:** Small lots.

Add comments here



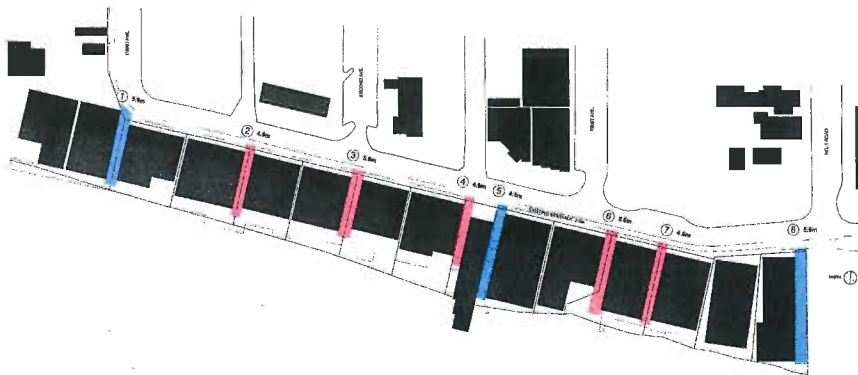
Building sizes on Bayview St. (south)

Have Your Say

Tell us what you support.

- Option 1:** Large & small buildings (no change).
staff recommendation
- Option 2:** Small buildings.

Add comments here



Small Lots – potential creation of new lots

**Illustration is theoretical – not proposed redevelopments*

- EXISTING CONNECTION AND FUTURE WIDTH (MINIMUM)
- FUTURE CONNECTION AND FUTURE WIDTH (MINIMUM)

Small Lot Development



Massing model of buildings on potential small lots

**actual development would not result in fully built out lots due to zoning regulations (e.g. setbacks) and meeting design guidelines*

Design Vision for Riverfront Precinct

What are the issues?

- There is a need to provide clarity on how the City will complete the waterfront boardwalk and pedestrian connections from Bayview Street, with respect to developer contributions, and the overall design of the City walkways.

Have Your Say

Tell us what you support.

Developer contributions toward the walkways

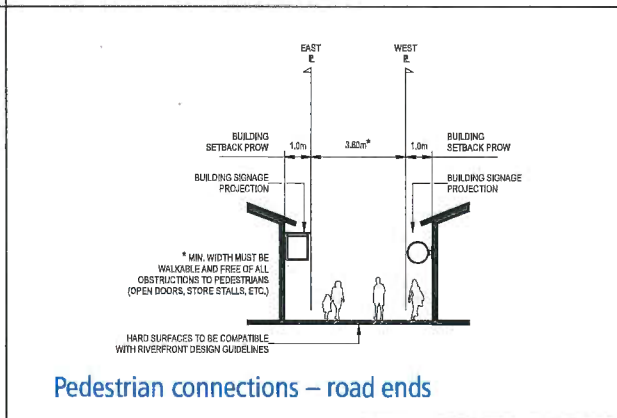
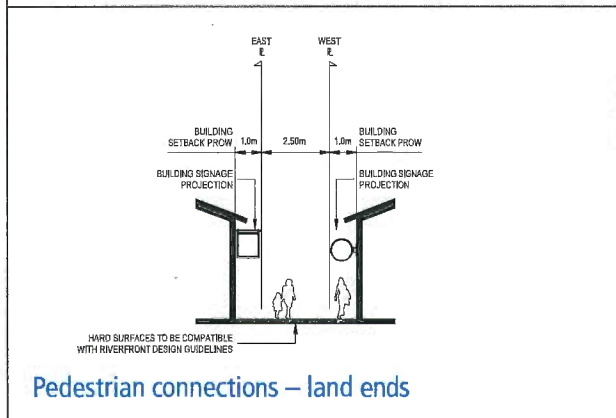
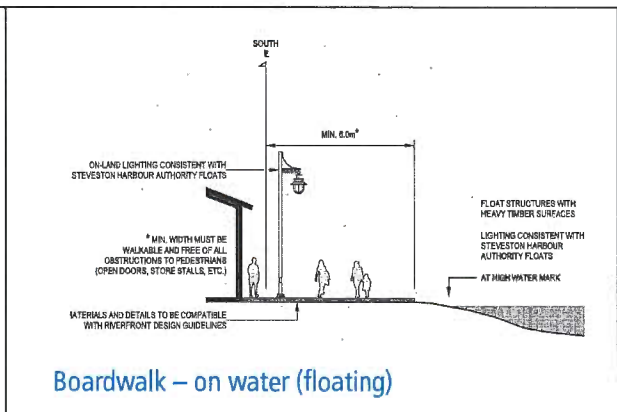
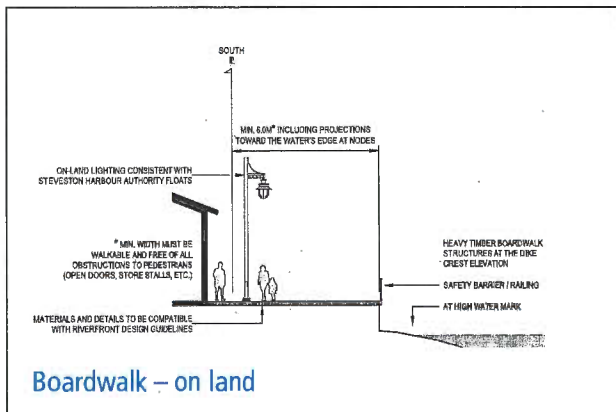
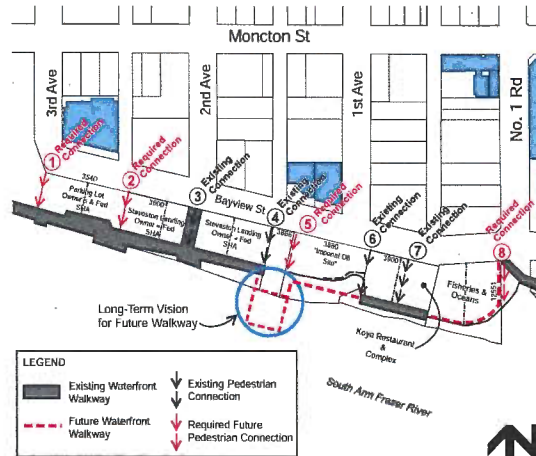
- Option 1:** No City policy (no change).
- Option 2:** Developer contributions to be required through the rezoning and development permit application process. ***staff recommendation***

Design guidelines for the boardwalk and paths

- Option 1:** No design guidelines (no change).
- Option 2:** Design guidelines including but not limited to the cross sections that are shown on this board. ***staff recommendation***



Existing and future riverfront walkways



On-Site Parking Requirements

What are the issues?

- Address the need to maintain an adequate supply of on street parking in Steveston Village.
- Consider a smaller on-site vehicle parking reduction for future residential developments.

What is included in the Steveston Area Plan (SAP) today?

Where a rezoning application is required for new developments in Steveston Village, the SAP allows up to a 33% reduction in on-site vehicle parking from the City's Zoning Bylaw requirements.

OPTION 1

Existing Parking Rates for Steveston Village	
Existing Conservation Strategy Parking Rate (Up to 33% Reduction from Zoning Bylaw Parking Requirements)	
Residential	1.0 stall/ dwelling Unit
Retail	2.0 stalls/ 100 sq.m
Restaurant	6.0 stalls/ 100 sq.m

OPTION 2

Proposed Parking Rates for Steveston Village	
Proposed New Parking Rates	
Residential	1.3 stalls/ dwelling Unit
Retail	2.0 stalls/ 100 sq.m
Restaurant	6.0 stalls/ 100 sq.m

- Allows more future residents to park on site

Have Your Say

Tell us what you support.

On-Site Parking Requirements: Steveston Village

- **Option 1:** No change. Maintain up to 33% on-site parking reduction for all uses
- **Option 2:** Decrease allowable parking reduction from up to 33% to up to 13% for residential use

Add comments here



LONG-TERM STREETSCAPE VISIONS FOR BAYMEW STREET, CHATHAM STREET AND MONCTON STREET

What is a "Streetscape"

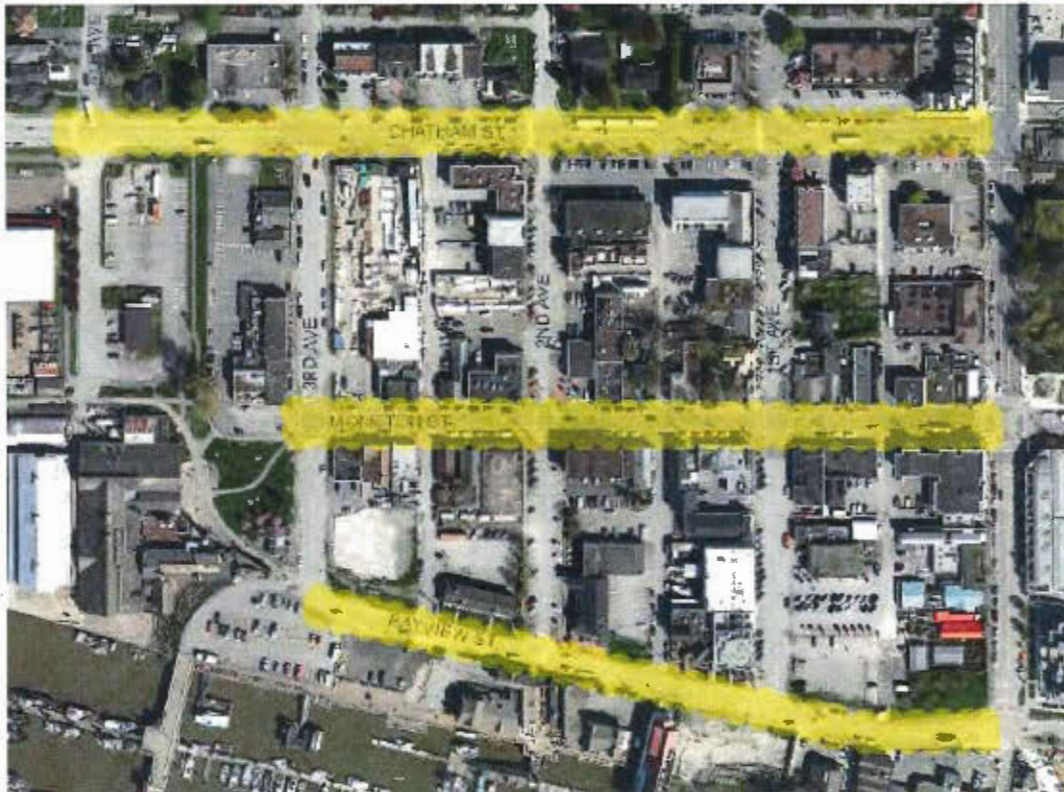
The elements of a street including the road, adjoining buildings, sidewalk and open spaces, street furniture, trees, and other elements that combine to form the street character.

Why We Need Long-Term Streetscape Visions

- A planning tool to help guide future development
- Support implementation of the *Steveston Village Conservation Strategy*

Streetscape Design Objectives

- Support and be respectful of the heritage of Steveston Village
- Allow the buildings to stand out in front of a less complex streetscape
- Use of simple materials with a minimum of street furniture
- Enhance pedestrian areas and encourage more walking, cycling and transit use



Scope of Streetscape Study.

Your Opinions are Important to Us

Community feedback is an important component when considering changes to the streetscapes of Baymew Street, Chatham Street and Moncton Street in Steveston Village.

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETScape VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

Results of Public Consultation in April-May 2013

- Majority support for wider and improved pedestrian realms on Bayview Street and Chatham Street with no additional on-street parking
- Recommended streetscape visions consistent with the *Steveston Village Conservation Strategy* and community feedback were presented to City Council in July 2013
- Staff were directed to undertake further analysis of streetscape features

The Next Several Boards Detail:

- Existing conditions on Bayview Street, Chatham Street and Moncton Street
- Potential revised streetscape options for each street
- The pros and cons of each option
- The estimated cost of implementation and funding source



Bayview Street looking west

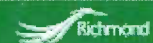


Chatham Street looking west



Moncton Street looking west

Please fill out the Feedback form as you view the display boards.

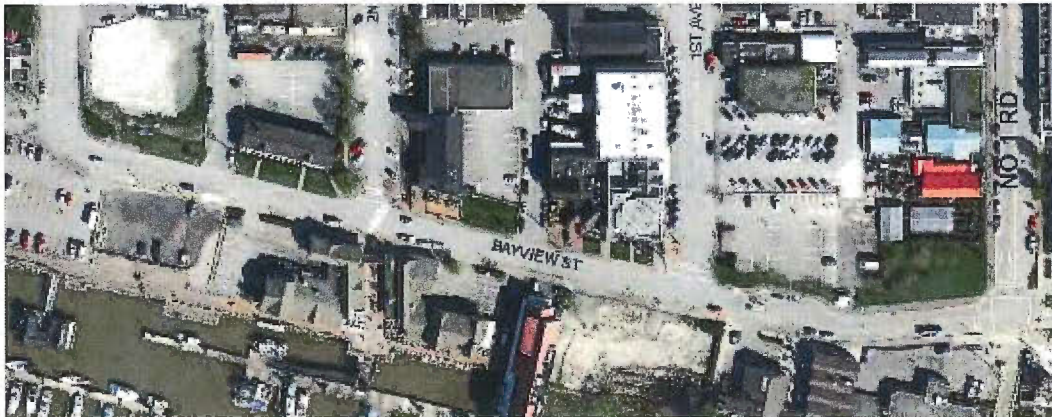


LONG-TERM STREETScape VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

BAYVIEW STREET

Existing Conditions

- 2.0 metre to 3.0 metre wide sidewalk on south side
- 1.5 metre to 2.0 metre wide sidewalk on north side plus 5.5 metre to 6.0 metre wide green space
- Total of 17 parallel parking spaces: 14 spaces on south side and 3 spaces on north side



Aerial View of Bayview Street



Street View of Bayview Street Looking East to 2nd Avenue

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETSCAPE VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

BAYVIEW STREET

Option 1: Enhanced Pedestrian Realm on North Side Only

- Maintain location of north and south curbs
- Widen pedestrian realm (combined sidewalk and boulevard) up to 7.5 metres wide on north side
- Add benches, pedestrian lighting and landscaping on the north side
- Pedestrian realm on south side remains unchanged
- Maintain total of existing 17 parallel parking spaces (14 on south side and 3 on north side)

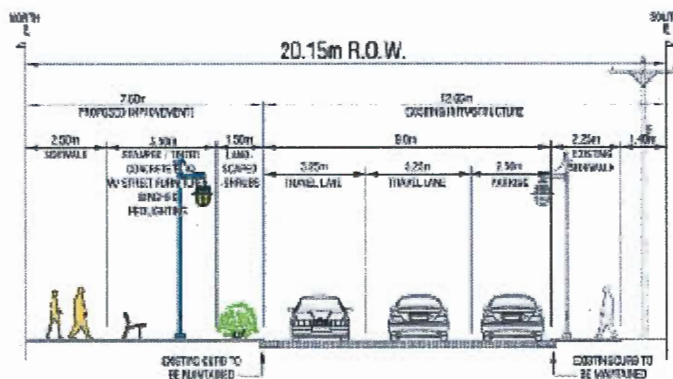


Pros

- Improved pedestrian realm on north side
- Wider pedestrian area on north side (by 1.0 m) versus Option 3
- Provides better buffer between pedestrians and moving traffic

Cons

- No pedestrian realm improvements on south side versus Options 2 and 3
- No cycling facilities versus Option 3



Estimated Cost

\$500,000

Potential Funding Source

Roads Development Cost Charges Program

Question 4:

I think the following features of Option 1 for Bayview Street are important: _____

I think the following features of Option 1 for Bayview Street are not important: _____

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETSCAPE VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

BAYVIEW STREET

Option 2: Enhanced Pedestrian Realm on North and South Sides

- Maintain location of north curb
- Widen pedestrian realm up to 7.5 metres wide on north side as in Option 1
- Remove on-street parking on south side and move south curb to the north by 2.5 metres
- Widen pedestrian realm up to 4.75 metres on the south side
- Add benches, pedestrian lighting and landscaping on the north and south sides



Pros

- Improved pedestrian realm on north and south sides
- Provides better buffer between pedestrians and moving traffic

Cons

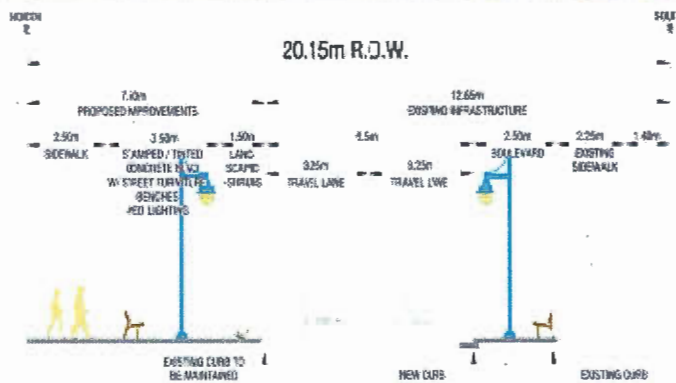
- Removal of on-street parking on south side
- No cycling facilities versus Option 3

Estimated Cost

\$1,500,000

Potential Funding Source

Roads Development Cost Charges Program



Question 4:

I think the following features of Option 2 for Bayview Street are important: _____

I think the following features of Option 2 for Bayview Street are not important: _____

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETSCAPE VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

BAYVIEW STREET

Option 3: Enhanced Pedestrian Realm on North and South Sides plus Continuous Bikeway

- Move north curb to the north by 1.5 metres and widen pedestrian realm up to 6.0 metres on north side
- Remove on-street parking on south side and move south curb to the north by 1.0 metres
- Widen pedestrian realm up to 3.25 metres on the south side
- Reallocate 3.0 m on the south side of the road for a two-way protected cycling facility
- Add benches, pedestrian lighting and landscaping on the north and south sides

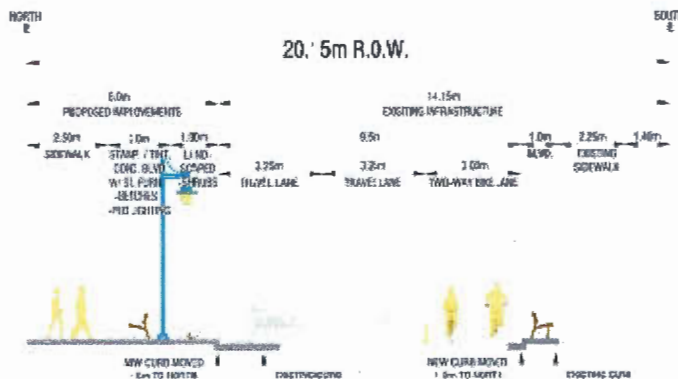


Pros

- Improved pedestrian realm on north and south sides
- Provides better buffer between pedestrians and moving traffic
- Protected cycling facility that connects to off-street pathways at either end

Cons

- Removal of on-street parking on south side
- Pedestrian realms on north and south sides not as wide as Options 1 or 2 (by 1.5 m)



Estimated Cost

\$1,600,000

Potential Funding Source

Roads Development Cost Charges Program

Question 4:

I think the following features of Option 3 for Bayview Street are important

I think the following features of Option 3 for Bayview Street are not important

Question 5:

I prefer the following streetscape vision for Bayview St

- | | |
|-------------------------------------|---|
| <input type="checkbox"/> Status Quo | <input type="checkbox"/> Option 3 |
| <input type="checkbox"/> Option 1 | <input type="checkbox"/> Other (please specify) |
| <input type="checkbox"/> Option 2 | <input type="checkbox"/> Don't Know / Unsure |

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETSCAPE VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

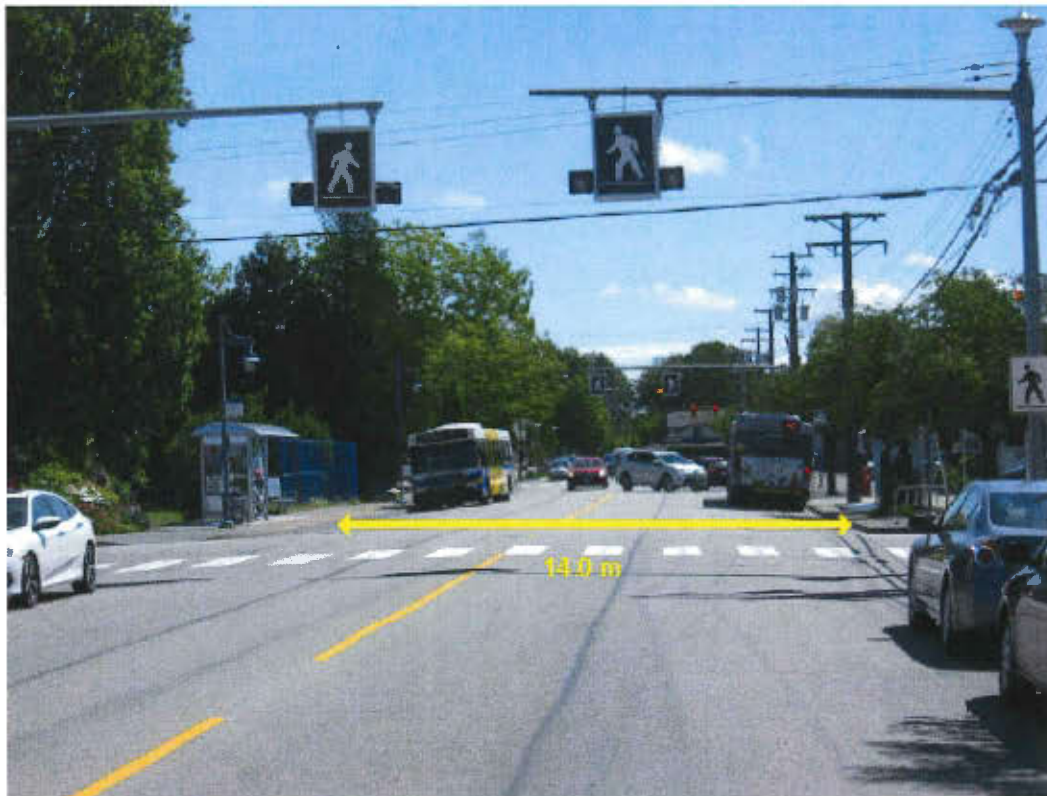
CHATHAM STREET

Existing Conditions

- 2.0 metre to 4.0 metre wide sidewalk and boulevard on north side
- 1.5 metre to 5.0 metre wide sidewalk and boulevard on south side
- Total of 23 parallel parking spaces: 14 spaces on north side and 9 spaces on south side



Aerial View of Chatham Street



Street View of Chatham Street Looking East to 2nd Avenue

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETScape VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

CHATHAM STREET

Option 1: Enhanced Pedestrian Realm on North and South Sides

- Maintain location of north and south curbs
- Widen pedestrian realms (sidewalk and boulevard) up to 6.4 metres on north side and 7.0 metres on south side
- Add benches, pedestrian lighting and landscaping on the north and south sides
- Maintain total of existing 23 parallel parking spaces
- As development occurs on north side, pursue opportunities to relocate driveways to rear lane

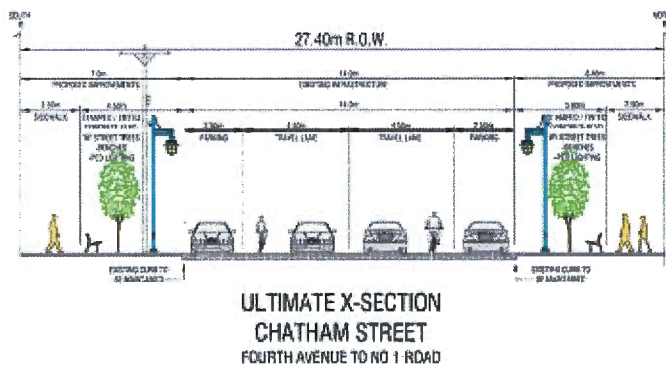


Pros

- Improved pedestrian realm on north and south sides
- Provides better buffer between pedestrians and moving traffic

Cons

- Longer crossing of Chatham Street for pedestrians versus Option 2
- Cyclists not protected from adjacent vehicles versus Option 2



Estimated Cost

\$2,600,000

Potential Funding Source

Roads Development Cost Charges Program

Question 6:

I think the following features of Option 1 for Chatham Street are important: _____

I think the following features of Option 1 for Chatham Street are not important: _____

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETSCAPE VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

CHATHAM STREET

Option 2: Enhanced Pedestrian Realm on North and South Sides plus Cycling Paths

- Move north and south curbs into the roadway by 1.25 metres each
- Widen pedestrian realms (sidewalk and boulevard) up to 5.65 metres on north side and 6.25 metres on south side
- Add benches, pedestrian lighting and landscaping on the north and south sides
- Delineate off-street cycling path on north and south sides
- Maintain total of existing 23 parallel parking spaces
- As development occurs on north side, pursue opportunities to relocate driveways to rear lane

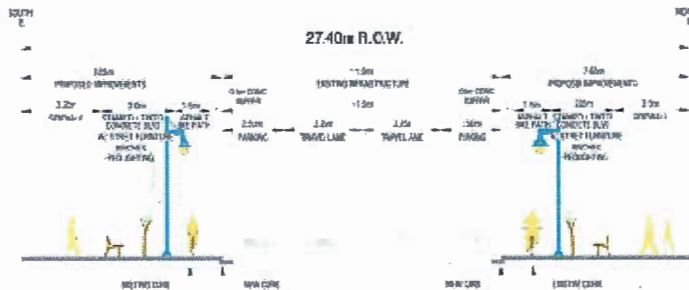


Pros

- Improved pedestrian realm on north and south sides
- Provides better buffer between pedestrians and moving traffic
- Shorter crossing of Chatham Street for pedestrians
- Cycling paths protected from adjacent vehicles

Cons

- Pedestrian realm (sidewalk and boulevard) on north and south sides not as wide as Option 1 (by 0.75 m)



Estimated Cost

\$3,200,000

Potential Funding Source

Roads Development Cost Charges Program

Question 6:

I think the following features of Option 2 for Chatham Street are important

I think the following features of Option 2 for Chatham Street are not important

Question 7:

I prefer the following streetscape vision for Chatham St

- Status Quo
- Option 1
- Option 2
- Other (please specify)
- Don't Know / Unsure

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETScape VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

MONCTON STREET

Existing Conditions

- Pedestrian realm comprises concrete sidewalk and boulevard with unit pavers
- Curb bulges at 1st, 2nd and 3rd Avenues
- Total of 46 parallel parking spaces: 21 spaces on north side including 2 loading zone spaces and 25 spaces on south side



Aerial View of Moncton Street



Street View of Moncton Street Looking East at 2nd Avenue

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETSCAPE VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

MONCTON STREET

Option 1: Modified Curb Bulges and Boulevard Surface with Two New Mid-Block Crossings

- Modify curb bulges with removal of unit pavers and provision of ramps with a rollover curb at 1st, 2nd and 3rd Avenues
- Add two new mid-block crossings with modified curb bulges at the lane between 1st and 2nd Avenues, and the lane between 2nd and 3rd Avenues
- Replace boulevard unit pavers with textured concrete as proposed for Bayview Street and Chatham Street
- Maintain location of north and south curbs
- Maintain total of existing 46 parallel parking spaces



Pros

- Better consistency of pedestrian realm with proposed streetscapes for Bayview Street and Chatham Street
- Additional crossing opportunities of Moncton Street for pedestrians

Cons

- Perception of less protection for pedestrians from turning vehicles
- May require additional physical protection (e.g., bollards) at rollover curb edge

Estimated Cost

\$1,100,000

Potential Funding Source

Roads Development Cost Charges Program

Question 8:

I think the following features of Option 1 for Moncton Street are important:

I think the following features of Option 1 for Moncton Street are not important:

Question 9:

I prefer the following streetscape vision for Moncton St:

- Status Quo
- Option 1
- Other (please specify)

Please fill out the Feedback form as you view the display boards.



LONG-TERM STREETScape VISIONS FOR BAYVIEW STREET, CHATHAM STREET AND MONCTON STREET

 **Have Your Say - What Options Do You Support?**

Bayview Street				
STATUS QUO	OPTION 1 Enhanced Pedestrian Realm on North Side Only	OPTION 2 Enhanced Pedestrian Realm on North and South Sides	OPTION 3 Enhanced Pedestrian Realm on North and South Sides plus Continuous Bikeway	OTHER (Please Specify)

Chatham Street			
STATUS QUO	OPTION 1 Enhanced Pedestrian Realm on North and South Sides	OPTION 2 Enhanced Pedestrian Realm on North and South Sides plus Cycling Paths	OTHER (Please Specify)

Moncton Street		
STATUS QUO	OPTION 1 Modified Curb Bulges and Boulevard Surface with Two New Mid-Block Crossings	OTHER (Please Specify)

Please fill out the Feedback form as you view the display boards.





Introduction

The City of Richmond is seeking comments from the community on options for changes to design and heritage policies in the Steveston Area Plan. For more information on key issues, existing policies, and options, please view the Open House Boards on the website to answer the survey and add comments (www.letstalkrichmond.ca/svapupdate2017/documents).

We thank you for taking the time to fill out this survey. Your input will be included in results that staff will report back to Council in October 2017, and will inform staff review of preferred options, as well as the Council decision on changes to the Steveston Area Plan.

Please send your survey to Helen Cain, Planner 2, Policy Planning, through:

Email: communityplanning@richmond.ca

Fax: 604 276 4052

Mail or drop off: City of Richmond, 6911, No. 3 Road, Richmond, BC

The deadline to submit surveys and other comments is **July 30, 2017**.

For more information, please contact Helen Cain at 604-276-4193 or communityplanning@richmond.ca.

Land Use Density and Building Heights in the Village Core

Please refer to Open House Board #3 for more information on the issues and illustrations.

- 1. The current density allowed on *Moncton Street* is a maximum of 1.2 floor area ratio (FAR), and the maximum building height is 2 storeys or 9 m. However, 1 in 3 buildings may be up to a maximum of 3 storeys and 12 m.**

Which option do you support?

1. No change in the maximum density and maximum height as described above.

Staff Recommendation

2. Reduce maximum density from 1.6 FAR to 1.2 FAR, and require all buildings to have a maximum height of 2 storeys and 9 m.

Comments: _____

- 2. The current density allowed on *Bayview Street (north side)* is a maximum of 1.6 floor area ratio (FAR), and the maximum building height is 3 storeys, or 12 m, over parkade structure.**

Which option do you support?

1. No change in the maximum density and maximum height as described above.

Staff Recommendation

2. A reduction in density and height as follows:
 Maximum density of 1.2 FAR
 North side lot depth, up to 2 storeys over parkade (appears 3 storeys).
 South side lot depth, up to 2 storeys over parkade (appears 2 storeys).

Comments: _____

Design Guidelines for Exterior Cladding and Window Treatments

Please refer to Open House Boards #4 and #5 for more information on the issues and illustrations.

3. In the design guidelines for the *Village Core* (including Bayview Street north side), wood is the primary material for exterior cladding (i.e. siding). However, the wood for exterior cladding is restricted to horizontal siding. Historically, the wood used on buildings in Steveston Village included wood shingles, board-and-batten, and vertical shiplap, and these materials were allowed in the "Sakamoto Guidelines" that the City used for the Village Core before 2009.

Which option do you support?

1. No change to the primary material for exterior cladding (i.e. horizontal wood siding only).

Staff Recommendation

2. Expand the primary materials for exterior cladding to include wood shingles, board-and-batten and vertical ship lap, in addition to horizontal wood siding.

Comments: _____

4. In the design guidelines for new buildings and additions, for the *Village Core* (including Bayview Street north side), the primary material for exterior cladding (i.e. siding) is wood. Glass, concrete, stucco, and metal that complements the wood siding may be used as secondary material(s) for exterior cladding.

Which option do you support?

- 1: No change to the secondary materials for exterior cladding (i.e. siding).
- 2: **No brick** and **no metal** allowed. For façade upgrades, *replace brick with similar brick.*
- 3: **No brick** and **no metal** allowed. For façade upgrades, *replace brick with similar brick or different brick.*
- 4: **No brick** and **no metal** allowed. For façade upgrades, replace brick with similar brick, different brick or a better material.
- 5: **No metal** but **brick is allowed if different from the Hepworth Building.** For façade upgrades, *replace brick with a similar brick or different brick.*

Staff Recommendation

- 6: **No metal** but **brick is allowed if different from the Hepworth Building.** For façade upgrades, *replace brick with similar brick, different brick, or a better material.*

Comments: _____

5. In the design guidelines for the *Village Core* and the *Riverfront*, window frames that are wood are encouraged. Vinyl window assemblies are discouraged but allowable.

Which option do you support?

- 1: No change to materials for window treatments (i.e. wood or vinyl is allowed).

Staff Recommendation

- 2: Windows with wood frames or metal frames are allowed. Vinyl is prohibited.

Comments: _____

Design Guidelines for Rooftop Structures

Please refer to Open House Boards #6 and #7 for more information on the issues and illustrations.

6. **Solar panels, and other renewable energy infrastructure (e.g. air source heat pump), may be mounted on heritage buildings and non-heritage buildings in Steveston Village. No changes are proposed to the guidelines for heritage buildings. The design guidelines to manage the visibility of solar panels on non-heritage properties with a flat roof include a requirement for the panels to be located back from the building edges. There are no design guidelines for other renewable energy infrastructure on flat roofs, and no design guidelines for solar panels or other renewable energy infrastructure on new or existing pitched-roof buildings.**

Which option do you support?

- 1: No changes to existing design guidelines.

Staff Recommendation

- 2: New design guidelines that require any false parapets to be slightly taller on new flat-roofed buildings, and allow solar panels to be affixed flush to pitched roofs.

Comments: _____

7. **Barrier railings for rooftop living spaces, which provide safety, on new and existing buildings should blend with the special character of the historic district. Currently there are no design guidelines for barrier railings in the Village Core. Rooftop living spaces are not possible in the Riverfront sub-area (Bayview Street south side) where roofs are pitched not flat.**

Which option do you support?

- 1: No changes to existing design guidelines.

Staff Recommendation

- 2: New design guidelines for barrier railings to be simple in design, and primarily consist of glazed panels to minimize visibility from streets and nearby rooftop patios on adjacent and surrounding buildings.

Comments: _____

8. **Managing the visibility of an access point for individual rooftop living spaces (i.e. roof decks and gardens) can be achieved through blending the hatch or 'pop-up' stair entries (that the building code requires) with the overall architecture of the new building or the existing building. There are currently no design guidelines for hatch ('pop-up') entries to individual rooftop living space.**

Which option do you support?

- 1: No changes to existing design guidelines as described above.
 2: Prohibit all hatch stair entries.

Staff Recommendation

- 3: Prohibit all hatch stair entries unless they are not more than 1.83 m (6 ft.) in height, well-integrated with the architecture and setback 1.0 m or more from all roof edges.
 4: Allow hatch stair entries if well-integrated with the overall architecture, and setback from all roof edges.

Comments: _____

9. **Managing the visibility of one or more access points for communal rooftop living space (i.e. roof deck and garden) can be achieved through blending the structure for the access stairs or elevator shaft (two shafts may be required to meet the building code) with the overall architecture or the new building or the existing building. There are no design guidelines to reduce the visibility of access stairs or an elevator shaft for communal rooftop living spaces.**

Which option do you support?

- 1: No changes to existing design guidelines as described above.
- 2: Prohibit all elevator shafts and access stairs.

Staff Recommendation

- 3: Prohibit access points unless they are less than 2.2 m for elevator shafts, and 3.17 m for access stairs, well-integrated with the architecture, and setback 1.0 m or more from all roof edges.
- 4: Allow structures for elevator shafts and access stairs if well-integrated with the overall architecture, and setback from all roof edges.

Comments: _____

Design Vision for the Riverfront Precinct

Please refer to Open House Boards #8 through #11 for more information on the issues and illustrations.

10. **The current density allowed on Bayview Street (south side) is a maximum of 1.6 floor area ratio (FAR), and the maximum building height is 3 storeys, or 12 m, over parkade structure.**

Which option do you support?

Staff Recommendation

- 1: No change in the maximum density and maximum height as described above.
- 2: Reduced density or reduced height.

Comments: _____

11. **The overall design vision for Bayview Street (south side) includes “Cannery-like” pitched roofed buildings, but flat roofs are allowable.**

Which option do you support?

- 1: No changes to existing design guidelines.

Staff Recommendation

- 2: Pitched roofs only to fully align with the design vision. Flat roofs are prohibited.

Comments: _____

12. **The overall design vision for Bayview Street (south side) includes retention of existing large lots.**

Which option do you support?

Staff Recommendation

- 1: No changes to existing large lots.
- 2: Through the redevelopment process, allow the subdivision of the existing larger lots into relatively small lots.

Comments: _____

13. **The overall design vision for Bayview Street (south side) includes large and small buildings on existing large lots.**

Which option do you support?

Staff Recommendation

- 1: No changes (i.e. a mix of large and small buildings).
- 2: Small buildings on small lots. No more new large "Cannery-like" buildings.

Comments: _____

14. **The City has the long-term objective of completion of the waterfront boardwalk, between 3rd Avenue and No. 1 Road, which is part of the Parks Trail System, and to complete pedestrian connections from Bayview Street to the riverfront. The Steveston Area Plan is currently unclear on how developers will contribute to the boardwalk and paths in the application review process.**

Which option do you support?

- 1: No changes (i.e. no City policy on developer contributions).

Staff Recommendation

- 2: Developer contributions to the waterfront boardwalk and pedestrian paths are required through rezoning and development permit application review process.

Comments: _____

15. **The Steveston Area Plan does not include a full set of design policies and guidelines for the waterfront boardwalk, between 3rd Avenue and No 1. Road, which is part of the Parks Trail System, or new and existing pedestrian connections, from Bayview Street to the riverfront.**

Which option do you support?

- 1: No change to existing design policies and guidelines.

Staff Recommendation

- 2: New design guidelines that include, but are not limited to, a set of dimension standards for details, such as boardwalk and path widths, setbacks to accommodate hanging signage, and surface treatments.

Comments: _____

On-Site Parking Requirements

Please refer to Open House Board #12 for more information on the issues and illustrations.

16. **To help support the vitality and conservation of Steveston Village, existing policy allows up to 33% reduction in on-site vehicle parking from the zoning regulations. However, there are impacts on the availability of street parking to be taken into consideration.**

Which option do you support?

- 1: No change to the policy for on-site parking requirements (i.e. 33% reduction).

Staff Recommendation

- 2: Decrease the allowable parking reduction from up to 33% to up to 13% for new residential development.

Comments: _____



**City of
Richmond**

**Long-Term Streetscape Visions for
Bayview Street, Chatham Street & Moncton Street:
Public Feedback Form**
6911 No. 3 Road, Richmond, BC V6Y 2C1

The City is continuing a planning process to develop long-term streetscape visions for Bayview Street, Chatham Street and Moncton Street in Steveston Village.

The purpose of this City initiative is to inform you, seek your input on the important elements that should be included in the planning concepts and identify your preferred vision for each street.

Your views will be considered by Council.

1. I live:

- In Richmond within 400 m of Steveston Village
- In Richmond beyond 1 km of Steveston Village
- In Richmond between 400 m and 1 km of Steveston Village
- Outside of Richmond

2. I visit Steveston Village:

- Frequently (more than 3 times per week)
- Slightly Often (once per month)
- Very Often (1–3 times per week)
- Not at All Often (1–10 times per year)
- Moderately Often (2–3 times per month)
- Other (please specify) _____

3. I travel to Steveston Village most often by:

- Vehicle as a Driver or Passenger
- Walking
- Bicycle
- Scooter
- Transit
- Other (please specify) _____

4. I have the following comments on Options 1 through 3 for Bayview Street (Boards 4–6):

Option 1 (Board 4)

I think these features are important:

I think these features are **NOT** important:

Option 2 (Board 5)

I think these features are important:

I think these features are **NOT** important:

Option 3 (Board 6)

I think these features are important:

I think these features are **NOT** important:

5. I prefer the following streets cape vision for Bayview Street:

- Status Quo
- Option 1
- Option 2
- Option 3
- Don't Know/Unsure
- Other (please specify) _____

6. I have the following comments on Options 1 and 2 for Chatham Street (Boards 8–9):

Option 1 (Board 8)

I think these features are important:

I think these features are **NOT** important:

Option 2 (Board 9)

I think these features are important:

I think these features are **NOT** important:

7. I prefer the following streets cape vision for Chatham Street:

- Status Quo Option 1 Option 2 Don't Know/Unsure
 Other (please specify) _____

8. I have the following comments on Option 1 for Moncton Street (Board 11):

Option 1 (Board 11)

I think these features are important:

I think these features are **NOT** important:

9. I prefer the following streets cape vision for Moncton Street:

- Status Quo Option 1 Don't Know/Unsure
 Other (please specify) _____

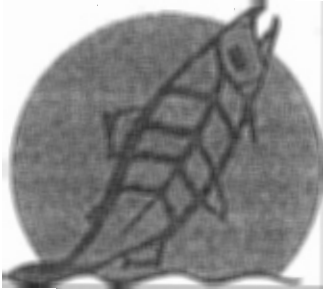
10. I heard about this public engagement opportunity via (check all that apply):

- Newspaper ad (Richmond News) LetsTalkRichmond.ca email sent to me Poster in City facility Twitter
 News story in local newspaper City of Richmond website (richmond.ca) Word of mouth Facebook

Please fill out the survey form and return it to the City by **Sunday, July 30, 2017**.

- Mail it to the City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1 to the attention of Joan Caravan, Transportation Planner; or
- Fax it to the City of Richmond at 604-276-4052 (fax); or
- Email it to the City of Richmond at joan.caravan@richmond.ca; or
- Fill it out online at the City's website and at www.letstalkrichmond.ca; or
- Leave it in the drop off boxes provided at this Public Open House.

Thank you for your participation



August 22, 2017

STEVESTON HARBOUR AUTHORITY

12740 Trites Road, Richmond, B.C. V7E 3R8 604-272-5539 Fax 604-271-6142

Terry Crowe, Manager, Policy Planning
City of Richmond
TCrowe@richmond.ca

Dear Mr. Crowe,

RE: STEVESTON AREA PLAN ("SAP")

Further to our meeting on July 26, 2017, the following are Steveston Harbour Authority's (SHA) comments regarding the SAP.

Density, Height, Exterior Finishes & Rooftop Structures

The SHA has no issues with the changes proposed by City staff. We do appreciate the City's efforts in clarifying the rules with respect to height.

Riverfront Walkway

While we generally do not oppose the proposal to complete the riverfront walkway spanning from Britannia Heritage Shipyards all the way to 3rd Avenue, we do have two concerns with the proposed drawings as they currently stand:

1. The proposed walkway around the Blue Canoe/Catch building would come too close to our public fish sales float, restricting berthage access to the entire northeast side of the dock. This float is extremely busy during certain parts of the year and losing area for moorage is not acceptable to us, particularly after having spent millions of dollars on the new floats in the past two years.
2. SHA is concerned with the walkway connecting directly to the sales float, as it increases liability for DFO with the increased public access. It also may be detrimental to the fishermen trying to make a living by selling their catch as increased foot traffic may deter potential customers from purchasing seafood on the float, which is the primary purpose of the float.

As such, we cannot support the walkway in its current proposed form but we do look forward to reviewing a revised drawing, as discussed at our meeting.

Chatham Street Parking Lot

We have several issues with the proposed use of the Chatham Street parking lot as a bus loop for Translink's operations:

1. This lot currently generates significant revenue for the SHA that is used to fund dredging of the Cannery Channel, building maintenance and other capital projects in the harbour.
2. The lot is important to the community of Steveston as the space is used to support community events.
3. SHA has medium-term plans to develop the lot and surrounding area to support the commercial fishing industry.

The SHA is not interested in a bus loop on any of our properties and we have reiterated this conclusion to Translink multiple times over the past several years.

Steveston Harbour Infrastructure - Heritage Resources

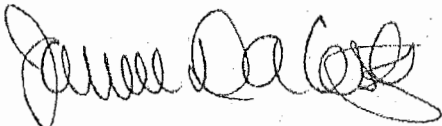
Upon consultation with the Department of Fisheries and Oceans, Small Craft Harbours (SCH) we have several additional concerns that were not discussed at the meeting:

1. SHA's No. 1 Road pier, public fish sales float and 3rd Avenue floats have been all been included in your maps as "heritage resources" (page 3 of your PowerPoint presentation). As discussed at the meeting, none of SHA's infrastructure should be identified as heritage properties as it may impede the operation of the commercial fishing harbour. As you are aware the SHA exists solely to provide safety, security and service to the commercial fishing fleet.
2. The City is proposing future development on the waterfront (page 14 & 15 of the PowerPoint) which clearly include properties owned by SCH and managed by SHA. SHA in no way supports this objective as all property managed by the SHA will be used to support industry.

Please note that we have raised all of these issues with DFO and they are aware of these matters.

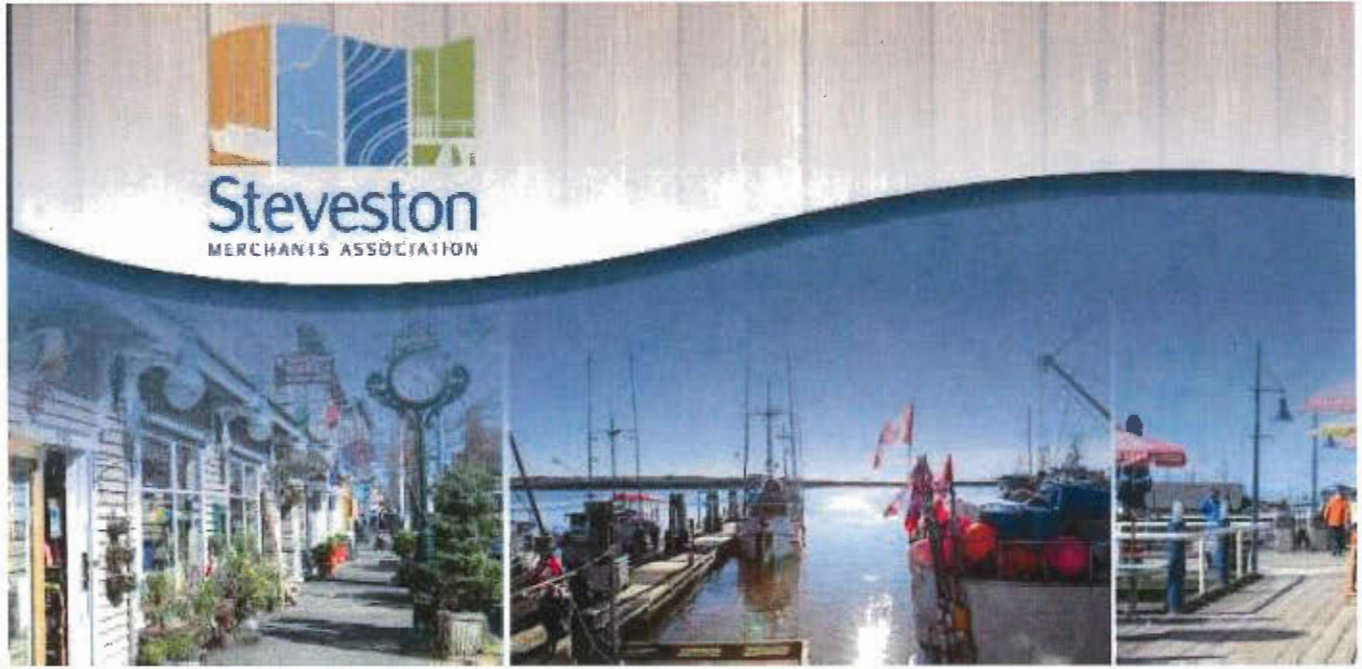
If you have any questions please feel free to contact me at 604-272-5539 or via email at jaime@stevestonharbour.com.

Yours truly,



Jaime Da Costa, General Manager
Steveston Harbour Authority

CC: Robert Kiesman, Board Chairman
Tina Atva, Senior Planning Coordinator
Donna Chan, Manager, Transportation Planning
Sonali Hingorani, Transportation Engineer
Helen Cain, Heritage Planner



Constant Contact Survey Results

Survey Name: Steveston Streetscape Survey

Response Status: Partial & Completed

Filter: None

9/26/2017 7:56 AM PDT

One Way Traffic Idea: This option is not on the proposal by the city but we want to know if you are interested in considering this.

Plan one-way traffic on
 Moncton Street (heading west) and Bayview Street (heading east) creating a loop. This would allow for substantially wider side walks, benches/tables for sitting, natural greenery, separate bike lane on Bayview Street connecting dyke path to Onni Development.

Answer	0%	100%	Number of Response(s)	Response Ratio
Yes, interested in this idea			13	81.2 %
No, not interested in this idea			3	18.7 %
Other			0	0.0 %
Totals			16	100%

BAYVIEW STREET

Option 1: Enhanced Pedestrian Realm on North Side Only Maintain location of north and south curbs. Widen pedestrian realm (combined sidewalk and boulevard) up to 7.5 metres wide on north side. Add benches, pedestrian lighting and landscaping on the north side. Pedestrian realm on south side remains unchanged. Maintain total of existing 17 parallel parking spaces (14 on south side and 3 on north side).

Answer	0%	100%	Number of Response(s)	Response Ratio
Yes, interested in this idea			7	53.8 %
No, keep Bayview Street as it is			4	30.7 %
Other			1	7.6 %
Totals			13	100%

BAYVIEW STREET

Option 2: Enhanced Pedestrian Realm on North and South Sides. Maintain location of north curb. Widen pedestrian realm up to 7.5 metres wide on north side as in Option 1. Remove on-street parking on south side and move south curb to the north by 2.5 metres. Widen pedestrian realm up to 4.75 metres on the south side. Add benches, pedestrian lighting and landscaping on the north and south sides.

Answer	0%	100%	Number of Response(s)	Response Ratio
Yes, interested in this idea			2	12.5 %
No, not interested in this idea			8	50.0 %
Other			0	0.0 %
No Response(s)			6	37.5 %
Totals			16	100%

BAYVIEW STREET




Option 3: Enhanced Pedestrian Realm on North and South Sides plus

Continuous Bikeway. Move north curb to the north by 1.5 metres and widen pedestrian realm up to 6.0 metres on north side. Remove on-street parking on south side and move south curb to the north by 1.0 metres. Widen pedestrian realm up to 3.25 metres on the south side. Reallocate 3.0 m on the south side of the road for a two-way protected cycling facility. Add benches, pedestrian lighting and landscaping on the north and south sides.

Answer	0%	100%	Number of Response(s)	Response Ratio
Yes, interested in this idea			11	68.7 %
No, not interested in this idea			4	25.0 %
Other			0	0.0 %
No Response(s)			1	6.2 %
		Totals	16	100%


CHATHAM STREET

Option 1: Enhanced Pedestrian Realm on North and South Sides. Maintain location of north and south curbs. Widen pedestrian realms (sidewalk and boulevard) up to 6.4 metres on north side and 7.0 metres on south side. Add benches, pedestrian lighting and landscaping on the north and south sides. Maintain total of existing 23 parallel parking spaces. As development occurs on north side, pursue opportunities to relocate driveways to rear lane.

Answer	0%	100%	Number of Response(s)	Response Ratio
Yes, interested in this idea			3	18.7 %
No, not interested in this idea			9	56.2 %
Other			0	0.0 %
No Response(s)			4	25.0 %
		Totals	16	100%

CHATHAM STREET

Option 2: Enhanced Pedestrian Realm on North and South Sides plus Cycling Paths. Move north and south curbs into the roadway by 1.25 metres each. Widen pedestrian realms (sidewalk and boulevard) up to 5.65 metres on north side and 6.25 metres on south side. Add benches, pedestrian lighting and landscaping on the north and south sides. Delineate off-street cycling path on north and south sides. Maintain total of existing 23 parallel parking spaces. As development occurs on north side, pursue opportunities to relocate driveways to rear lane.

Answer	0%	100%	Number of Response(s)	Response Ratio
Yes, interested in this idea			8	50.0 %
No, not interested in this idea			7	43.7 %
Other			0	0.0 %
No Response(s)			1	6.2 %
Totals			16	100%

MONCTON STREET

Option 1: Modified Curb Bulges and Boulevard Surface with Two New Mid-Block Crossings. Modify curb bulges with removal of unit pavers and provision of ramps with a rollover curb at 1st, 2nd and 3rd Avenues. Add two new mid-block crossings with modified curb bulges at the lane between 1st and 2nd Avenues, and the lane between 2nd and 3rd Avenues. Replace boulevard unit pavers with textured concrete as proposed for Bayview Street and Chatham Street. Maintain location of north and south curbs. Maintain total of existing 46 parallel parking spaces.

Answer	0%	100%	Number of Response(s)	Response Ratio
Yes, interested in this idea			11	68.7 %
No, not interested in this idea			3	18.7 %
Other			1	6.2 %
No Response(s)			1	6.2 %
Totals			16	100%

There was a survey out this summer regarding Land Use Density and Building Heights in the Village Core; Design Guidelines for Exterior Cladding and Window Treatments; Design Guidelines for Rooftop Structures; Design Vision for the Riverfront Precinct; On-Site Parking Requirements. This is an extensive survey. Please read this link and reply directly to the city if you have feedback to be included in their report. [Steveston Area Plan Update](#)

1 Response(s)



Oris Consulting Ltd
12235 No 1 Rd,
Richmond, BC
V7E 1T6

July 28, 2017

City of Richmond
6911 No. 3 Rd
Richmond, BC
V6Y 2C1

RE: Steveston Area Update Plan

Dear Sir or Madam,

We have reviewed the proposed changes to the Steveston Area Plan and for the most part think they will be a great addition to the current guidelines. We have made a few notes below on a couple of areas we believe should be looked at in further details.

Rooftop decks Steveston Area Plan

In reference to the proposed updated Steveston Area plan, Oris believes that providing guidelines around the height of rooftop hatches, along with stair and elevator access is a positive step towards greater clarity and should be introduced.

Our concerns, however, are around the implementation of this. The Steveston Area plan considers that sites within the township that are designated as 3-storeys within the plan, have a maximum height of 12m. Given that the frontage along these streets must include commercial uses the minimum height of the first storey is 14-16' floor to floor. With 2 stories of residential on top of this at 10' floor to floor, the building will be a minimum height of 11m to the rooftop.

As these sites are built to the property lines to provide the required parking and commercial space, no room for outdoor space for residential owners can be provided at grade. We believe outdoor living space is essential to residents living in the village.

Recent changes in the building code are shifting towards making rooftop hatches for individual unit owner's unachievable, leaving common stairs and elevators as the only options. We also believe these rooftop areas should be made accessible to all owners, including those with mobility issues. Given the minimum height requirements for buildings from floor to floor this will ensure that most new developments will be looking for a height exemption, as to achieve the elevator access will cause the height of the building to be at 13-14m in a localized area. We believe that by allowing this doesn't detract from what Steveston Village owners and visitors are looking for. The suggestion to set these decks and rooftop access points back from the building edge by 1m is an excellent way to help limit overlook and should be implemented.

We understand that as each site develops this will be a localized condition and will need to be reviewed as such. We request that the requirement within the report for these items to not be seen within 90m be



www.orisconsulting.ca

modified for development within 90m of the dyke. It isn't possible to achieve given that the access stairs or elevator access cannot fit within the zoning height limit of 12m and the elevated grade on the dyke opens sightlines that are not available from the street grade. We would suggest that the sightlines be taken from the street level grade that prevails through most of the village.

Secondly, we believe the addition of more exterior finish types will help to provide more variety in the township and create a richer more vibrant village. Metal windows for the store fronts of buildings will provide an appearance consistent with the historical character of the area. However, we feel that vinyl windows should not be prohibited for the residential levels as long as they can be made to fit in with the Steveston Village vision. Wood are historically more accurate, however they need greater maintenance for the homeowner and isn't something that should be mandated. Properly detailed vinyl windows appear identical to wood windows viewed from the ground to the second floor.

Kind Regards,

Nathan Curran

Oris Consulting Ltd



Health Protection
Environmental Health

#325 - 8100 Granville Avenue
Richmond, BC V6Y 3T6
Tel: (604) 233-3147 Fax: (604) 233-3175

July 28, 2017

Joan Caravan
Transportation Planner
City of Richmond
6911 No. 3 Road
Richmond BC V6Y 2C1

Dear Ms. Caravan:

RE: Long-Term Streetscape Visions for Bayview Street, Chatham Street & Moncton Street

Healthy communities are places that are safe, contribute to a high quality of life, provide a strong sense of belonging and identity, and offer access to a wide range of health-promoting amenities, infrastructure, and opportunities for all residents. It is well documented that a community's built environment, defined as the human-made surroundings that provide the setting for human activity, can have a significant influence on the physical and mental health of its residents.

Proposed streetscape visions for were reviewed by Vancouver Coastal Health - Richmond Health Protection's Healthy Built Environment Team. Please consider our support for the following visions:

- *Bayview Street: Option 3*
- *Chatham Street: Option 2*

These visions prioritize safety and promote active transportation such as walking and biking. The proposed streetscapes increase perception of safety, offer attractive features such as benches and landscaping, which encourage use of active transportation. Active transportation has been shown to improve social connectivity, physical activity, mental health and quality of life. Furthermore, by making active transportation the more convenient and safe choice in the area, the reduction of car traffic will provide additional benefits of reduced traffic noise and improved ambient air quality.

Vancouver Coastal Health looks forward to reviewing future documents associated with the project. If you have any further questions or comments, please contact me at 604-233-3106 or via email at elden.chan@vch.ca

Sincerely,

A handwritten signature in blue ink, appearing to read 'Elden Chan', is written over a light blue horizontal line.

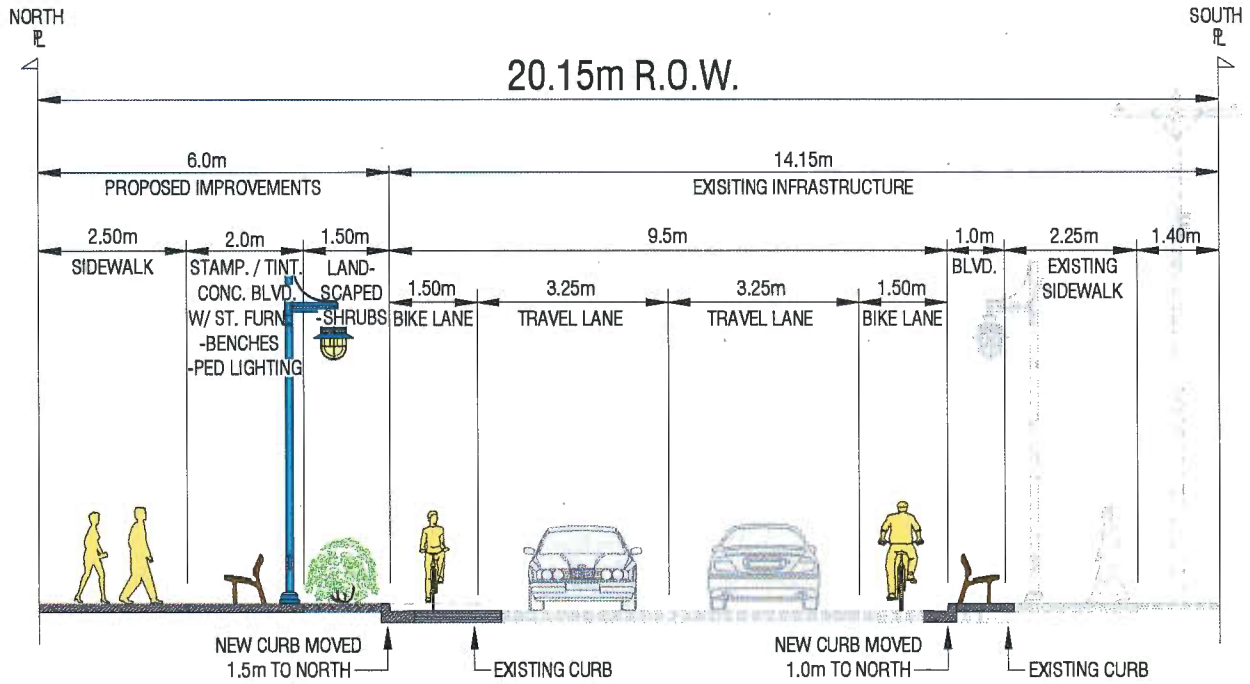
Elden Chan
Environmental Health Officer | Healthy Built Environment
Vancouver Coastal Health

CC: Dalton Cross, Senior Environmental Health Officer

Envh0115449

CNCL - 519

Typical Cross-Section of Recommended Streetscape Design for Bayview Street



RECOMMENDED ULTIMATE CROSS-SECTION

BAYVIEW STREET
THIRD AVENUE TO NO 1 ROAD

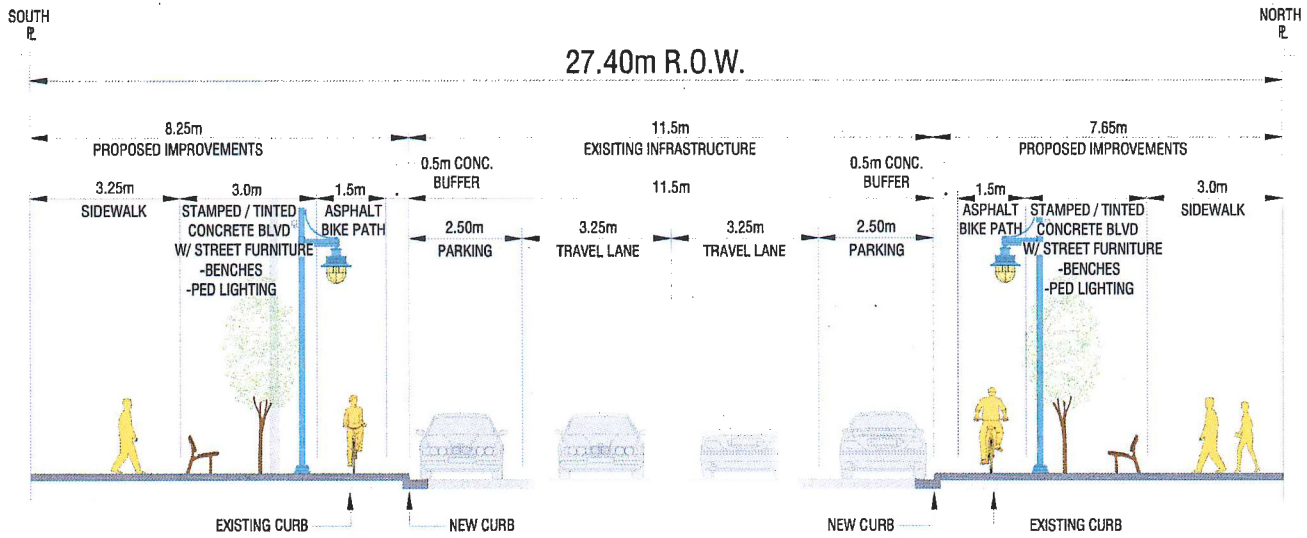
Plan View of Recommended Streetscape Design for Bayview Street



Bayview Street: Timing of Implementation of Recommended Streetscape Improvements

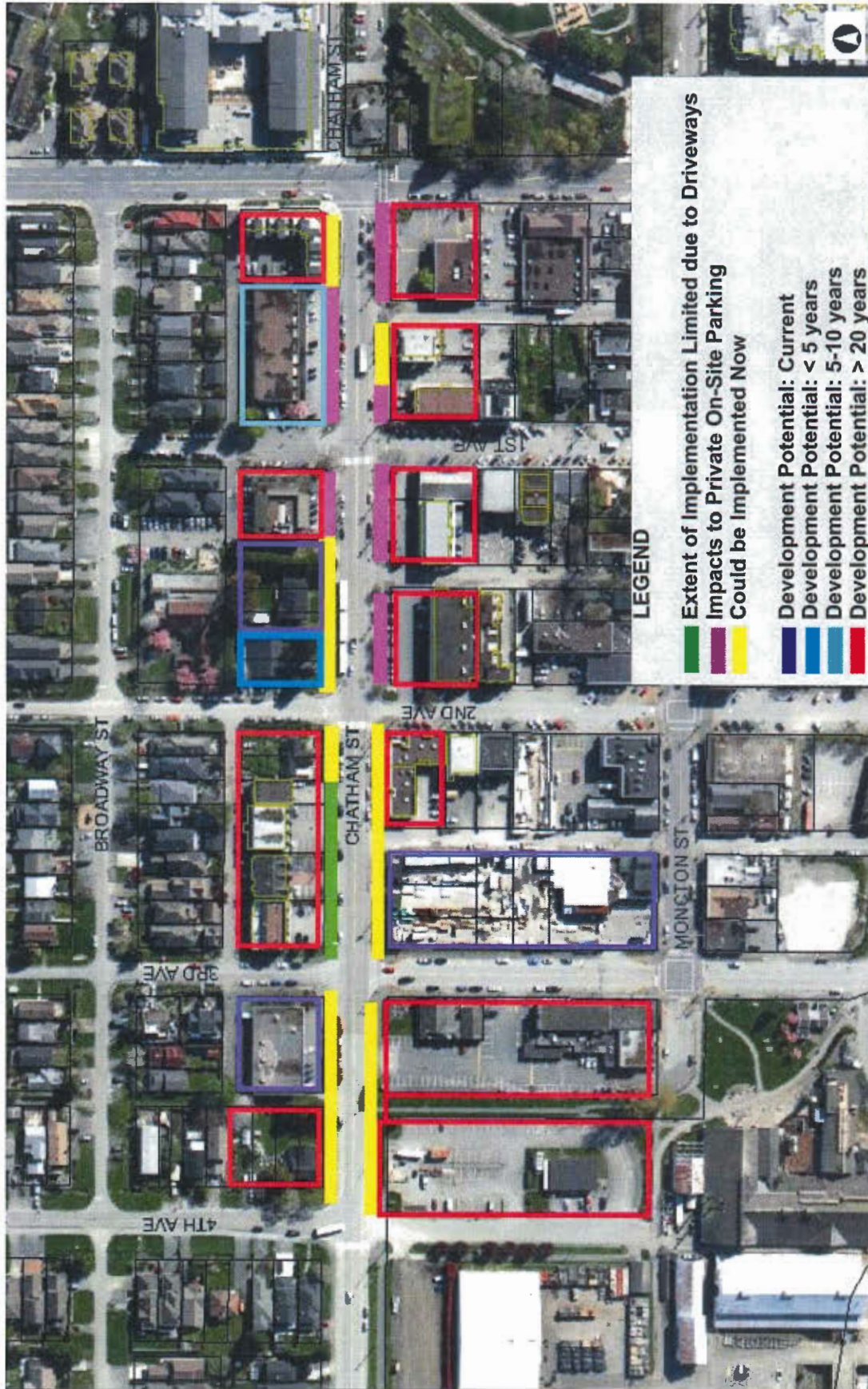


Typical Cross-Section of Recommended Streetscape Design for Chatham Street



RECOMMENDED ULTIMATE CROSS-SECTION CHATHAM STREET FOURTH AVENUE TO NO 1 ROAD

Chatham Street: Timing of Implementation of Recommended Streetscape Improvements





Moncton Street: Recommended Modification of Curb Bulges

Note: The rendering does not include the recommended addition of bollards to provide pedestrian protection, which will be included as part of the detailed design of the improvements.



Moncton Street: Recommended Textured Concrete Boulevard



**Richmond Official Community Plan Bylaw 7100
Amendment Bylaw 9775
Steveston Area Plan (Schedule 2.4)**

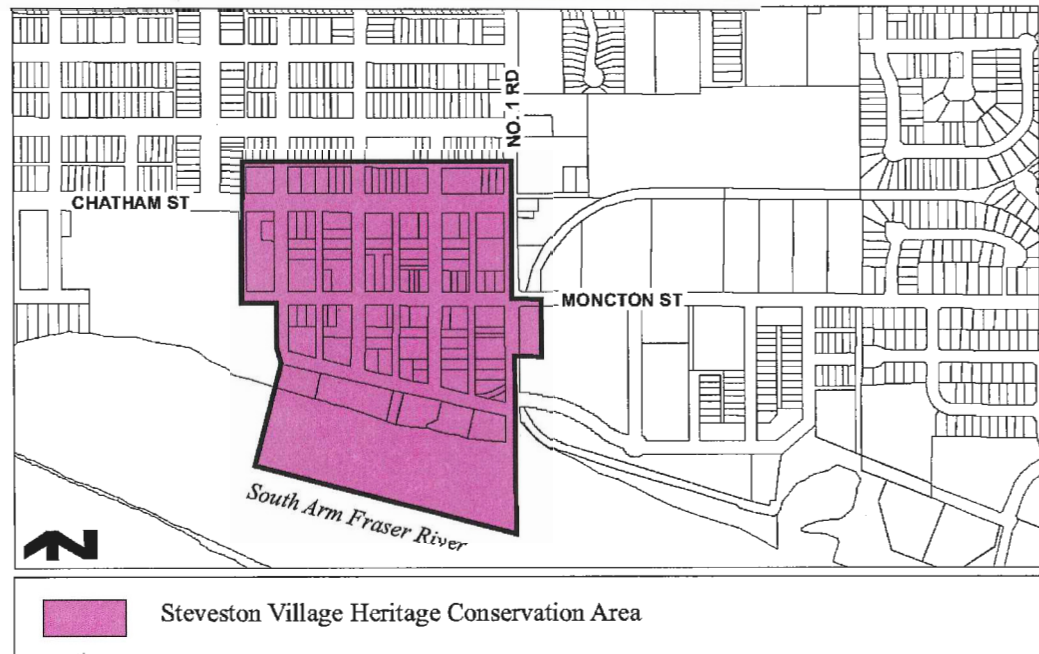
The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Official Community Plan Bylaw 7100 is amended by repealing and replacing and/or adding text and accompanying diagrams to various sections of the Steveston Area Plan (Schedule 2.4) as follows:
 - i) Adding the following text into Section 3.2.3 Steveston Village Node:
 - “h) Promote public access to the waterfront between 3rd Avenue and No. 1 Road through new pedestrian connections from Bayview Street and upgrades to the existing pedestrian paths.
 - i) Work toward uninterrupted connectivity along the waterfront between 3rd Avenue and No. 1 Road through extensions and improvements to walkway infrastructure and surfaces.”
 - ii) Repeal and replace the following text in Section 4.0 Heritage – Policies for Steveston Planning Area:
 - “k) To assist in achieving heritage conservation, consider utilizing a variety of regulatory and financial incentives through the applicable development application requirements (i.e., rezoning, development permit and/or heritage alteration permit), including but not limited to new zones, reduced parking, loading and unloading requirements, density bonusing and density transfer as well as consider using a variety of legal tools (i.e., heritage revitalization agreements, heritage covenants, phased development agreements).
 - Note: Supporting policies and guidelines are contained in the Heritage (Section 4.0), Transportation (Section 5.0), Natural and Human Environment (Section 6.0) and Development Permit Guidelines (Section 9.0) in the Steveston Area Plan.”
 - iii) Repeal and replace the following text in Section 4.0 Heritage – Policies for Steveston Village Node:
 - “l) Along Moncton Street the maximum building height shall be two-storeys and 9 m in height to ensure the size and scale of Moncton Street development is consistent with the village node.”

- iv) Adding the following text into Section 5.0 Transportation and accompanying diagram:

“Objective 6: Consider on-site parking reduction opportunities to help achieve the City’s heritage conservation and management objectives for the Steveston Village Heritage Conservation Area, in recognition that Steveston Village (Core and Riverfront Areas) is a complete and compact community well serviced by public transit offering a wide range of services to residents, visitors and employees.

Steveston Village Heritage Conservation Area Map



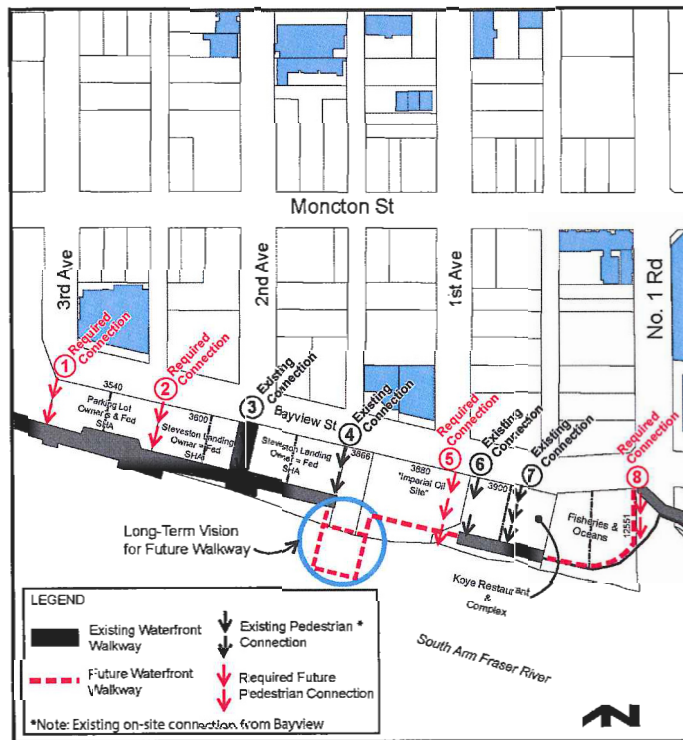
Policies:

- a) Consideration of parking reductions to be assessed through the applicable required development application.
- b) For development of new residential uses, a 13% reduction from applicable Zoning Bylaw parking requirements can be considered.
- c) For development of new commercial uses, a 33% reduction from applicable Zoning Bylaw parking requirements can be considered.
- d) Required on-site residential visitor parking and other non-residential use parking (i.e., commercial) may be shared.”

- v) Adding the following text into Section 6.0 Natural & Human Environment and accompanying diagrams:

“Objective 6: Work toward public accessibility for pedestrians to and along the waterfront between 3rd Avenue and No. 1 Road through pathways that connect Bayview Street to the water’s edge, and completion of a continuous boardwalk.

Existing and Future Riverfront Walkways and Connections

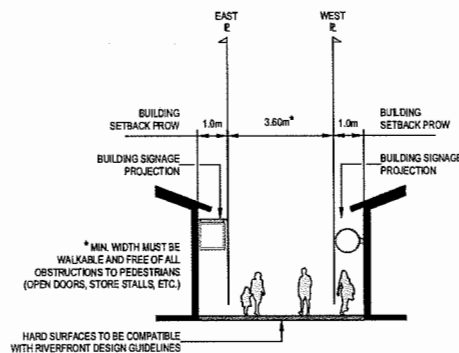


Policies:

- a) Work with the Federal Government, Steveston Harbour Authority and other property owners to establish new pedestrian connections at the following street and lane ends.
 - Pedestrian connections at road ends at the south foot of No. 1 Road, 1st Avenue and 3rd Avenue will meet the following guiding principles for universal accessibility and urban design:
 - Create a public right-of-passage with a minimum width of 5.6 m including 1.0 m setbacks from adjacent buildings
 - Building signage projections up to 1.0 m into any building setback and detailed as per Steveston Development Permit Area Design Guidelines

- A minimum of 5.6 m of the above minimum 5.6 m public right-of-passage must be free and clear of obstructions, including but not limited to: building projections (except for signage), doors, patios, store stalls.
- Accessible hard surfaces with materials compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
- Pedestrian connections materials and surface treatments designed to be safe and accessible for all users.
- Undertake enhancements to existing pedestrian connections in accordance with these guidelines where appropriate.

Pedestrian Connections at Road Ends



X-SECTION NORTH - SOUTH WALKWAYS

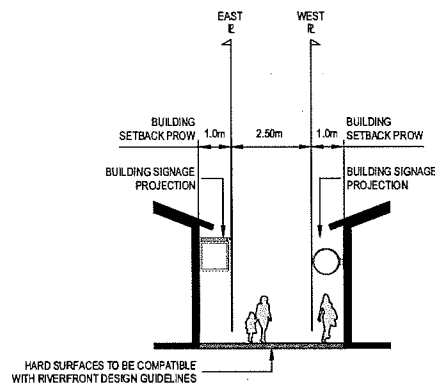
SOUTH FOOT OF:
NO.1 ROAD
1ST AVENUE
2ND AVENUE
3RD AVENUE

- Connections at the lane ends between No. 1 Road and 1st Avenue, between 1st Avenue and 2nd Avenue; and between 2nd Avenue and 3rd Avenue, will meet the following guiding principles for universal accessibility and urban design:
 - Create a public right-of-passage with a minimum width of 4.5 m including 1.0 m setbacks from adjacent buildings
 - Building signage projections up to 1.0 m into any building setback and detailed as per Steveston Development Permit Area Design Guidelines
 - A minimum of 4.5 m of the above minimum 4.5 m public right-of-passage must be free and clear of obstructions, including but not

limited to: building projections (except for signage), doors, patios, store stalls.

- Accessible hard surfaces with materials compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
- Pedestrian connections materials and surface treatments designed to be safe and accessible for all users.
- Undertake enhancements to existing pedestrian connections in accordance with these guidelines where appropriate.

Pedestrian Connections at Lane Ends

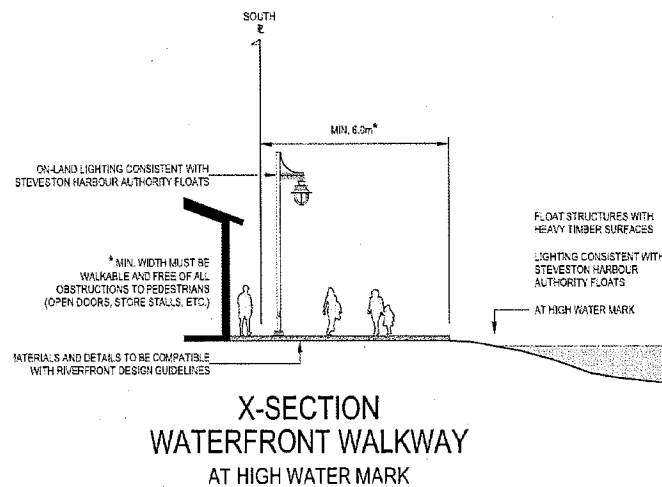


X-SECTION
NORTH - SOUTH WALKWAYS
 SOUTH FOOT OF LANE ENDS BETWEEN:
 NO.1 ROAD & 1ST AVENUE
 1ST AVENUE & 2ND AVENUE
 2ND AVENUE & 3RD AVENUE

- b) Work with the Federal Government, Steveston Harbour Authority and other property owners to establish waterfront walkway connections at, and above, high water mark.
- Walkway sections that are situated at high water mark elevation will meet the following guiding principles for universal accessibility and urban design:
 - Minimum 6.0 m in width.
 - Connected to walkways above, at the street end nodes, with gangways to create accessible access points.
 - Float structures with heavy timber surfaces.

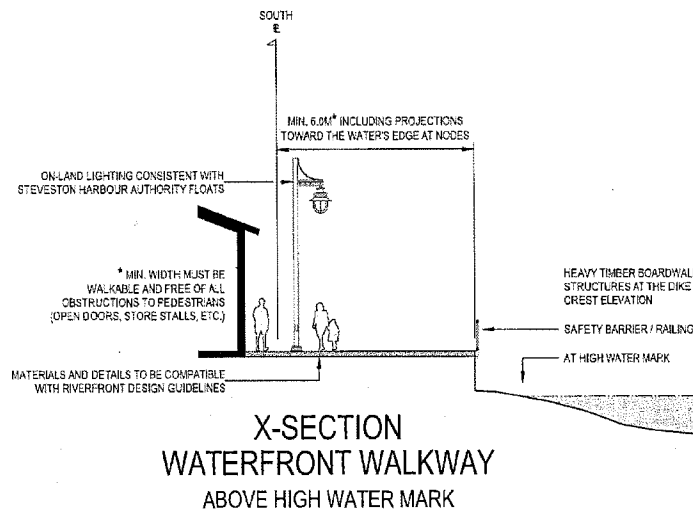
- Materials and details compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
- Waterfront walkway materials and surface treatments designed to be safe and accessible for all users.
- Lighting to enable nighttime use consistent with Steveston Harbour Authority floats.
- Undertake enhancements to existing waterfront walkway connections in accordance with these guidelines where appropriate.

Waterfront Walkway at High Water Mark



- Walkway sections that are situated above high water mark elevation will meet the following guiding principles for universal accessibility and urban design:
 - Minimum 6.0 m in width including projections toward the water’s edge at nodes (i.e. both street end and lane end connections).
 - Heavy timber boardwalk structures at the dike crest elevation.
 - Materials and details compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
 - Waterfront walkway materials and surface treatments designed to be safe and accessible for all users.
 - Lighting, seating and other site furnishings, as appropriate, at nodes.
 - Undertake enhancements to existing waterfront walkway connections in accordance with these guidelines where appropriate.

Waterfront Walkway Above High Water Mark

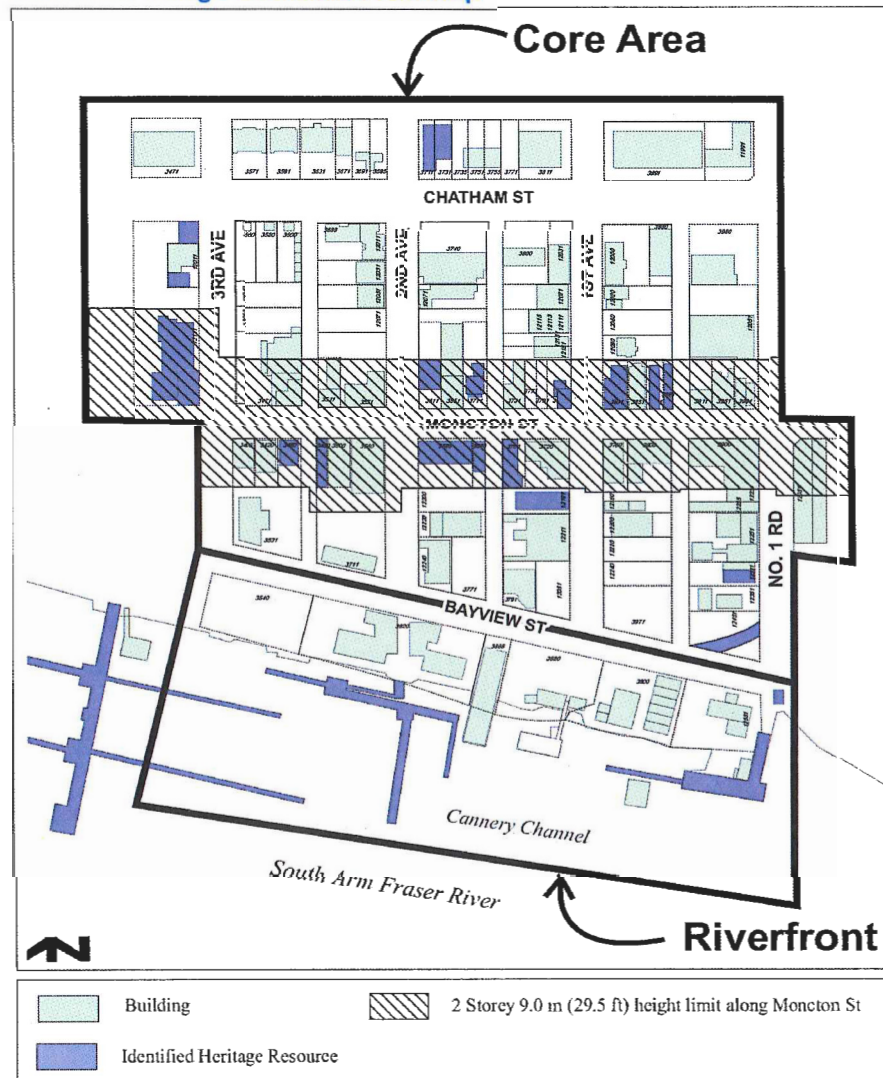


- c) Work with Steveston Harbour Authority to connect the waterfront walkway to existing structures as follows:
- Piers at the south foot of No. 1 Road and 3rd Avenue:
 - Increase the accommodation of pedestrian volume, circulation, resting and viewing points, while removing any obstructions to access to the water for harbour-related activities.
 - Add seating and other site furnishings in accessible locations (e.g. pier ends) to further enable people to observe harbour activities.
 - Floats:
 - Extend the length of publicly accessible floats.
 - Increase the number of connections from the land side.
 - Parking lot at 3rd Avenue:
 - Dedicate a pedestrian route to the waterfront boardwalk and pier.
 - Develop a bridge crossing to the Gulf of Georgia Cannery waterside deck.
- d) In scenarios where waterfront walkways deadend as an interim condition, ensure developments provide suitable universally accessible on-site connections from these points to Bayview Street.

- e) Developers through rezoning, development permit and/or heritage alteration permit applications shall be required to provide their portion of the continuous, universally accessible, riverfront walkway through:
 - Ensuring public access to the riverfront walkway and pathway connections in perpetuity through the necessary legal agreements.
 - Design and construction of the riverfront walkway and pathway connections by the developer in accordance with the design guidelines contained in the Steveston Area Plan.”

vi) In Section 9.3 Additional Development Permit Guidelines: Character Area Guidelines, repeal and replace the Steveston Village Character Area Map as follows:

Steveston Village Character Area Map



- vii) Inserting the following text to Section 9.3.2.1 Steveston Village General Guidelines: Shifts in Scale:
- “e) Existing elevations in the Village Core (at Moncton Street and 3rd Avenue), measured at 1.4 m GSC (Geodetic Survey Datum of Canada) is a historic feature in the Steveston Village Character Area to be retained:
- For properties in the Steveston Village Core, north of Bayview Street, the higher elevation of 1.4 m GSC or of the existing adjacent sidewalk shall be used and referenced in the development.
 - For properties in the Steveston Village Riverfront Area, south of Bayview Street, the higher elevation of 3.2 m GSC or of the existing adjacent sidewalk shall be used and referenced in the development.”
- viii) Repeal and replace the following text in Section 9.3.2.1 Steveston Village General Guidelines: Roofscapes, Exterior Walls, and Finishes as follows:
- “g) Using horizontal siding as the primary exterior cladding materials, complemented by a judicious use of glass, concrete, stucco and delicate timber details. Siding is encouraged to include historical treatments such as ship lap, flat lap horizontal wood, board-and-batten, and wood shingles. In keeping with the special heritage character of the two sub-areas, the use of metal exterior cladding or architectural detailing is not permitted in the Village Core except to replace existing metal materials with similar metal finishes in any existing building. The use of brick is not permitted in the Riverfront precinct except to replace any existing brick with similar brick.”
- ix) Repeal and replace the following text in Section 9.3.2.2 Area B: Steveston Village Sub Area Guidelines (Steveston Village Core Area – Massing and Height) as follows:
- “a) Reinforce a continuous commercial storefront streetwall with harmonious height of buildings, parapets, canopies and fascias. Building height should typically be no more than three storeys and may be varied to provide visual interest to the streetscape roofline (e.g., stepping from two to three-storey, except along Moncton Street where building heights are to be limited at two storeys.
- g) Make use of roofs as outdoor living spaces except for the roof decks with 3.0 m of the street property line; use the 3.0 m zone as a water collection area or inaccessible landscape area where no element or mature plant material is higher than 1.05 m above roof deck level.
- h) Building facades facing streets, or within 10 m (32.8 ft.) of a street, should have parapets at least 1.2 m above roof deck level.

- i) Solar panels may be affixed to flat roofs up to a height of 1.20 m and placed in any section of the roof deck that is a minimum distance of 1.0 m back from the roof edge. On a sloped roof, panels must be affixed flush to the roof and may not be more than 0.2 m above the roof surface.
- j) To encourage use of roof top decks as outdoor living spaces and architecturally integrate individual and communal rooftop deck access points into the building, such structures are not permitted unless all of the following criteria are met:
 - For individual unit roof top deck access:
 - Hatch access points (i.e., also known as pop-ups) should not exceed 1.83 m in height, as measured from the roof deck and be well integrated with the overall design of the building and setback from all roof edges to a minimum distance of 1.0 m.
 - Evaluate individual roof top deck access structures to ensure they are not visible from the streets and other public vantage points (i.e., lanes) generally from a distance of 90 m, taking into account any site specific context.
 - For communal (i.e., resident shared) roof top deck amenities:
 - Stair structures should not exceed 3.17 m in height for access as measured from the roof deck. Elevator lifts to facilitate accessibility to rooftop decks may require additional height to accommodate mechanical equipment, which would be reviewed as part of the required development application.
 - Stair and elevator structures should be well integrated with the overall design of the building and setback from all roof edges to a minimum distance of 1.0 m.
 - Evaluate communal rooftop deck access structures to ensure they are not visible from the streets and other public vantage points (i.e., lanes) generally from a distance of 90 m, taking into account any site specific context.
- k) On Bayview Street (north side), to achieve a suitable transition in built form moving north from Bayview Street to Moncton Street:
 - For the north 50% of any lot depth, a density of 1.2 F.A.R. and 3 storeys maximum building height (containing a parkade structure and two storeys above) is supported.
 - For the south 50% of any lot depth (nearest to Bayview Street which is the dyke) a density of 1.2 F.A.R. and 2 storeys building height as viewed

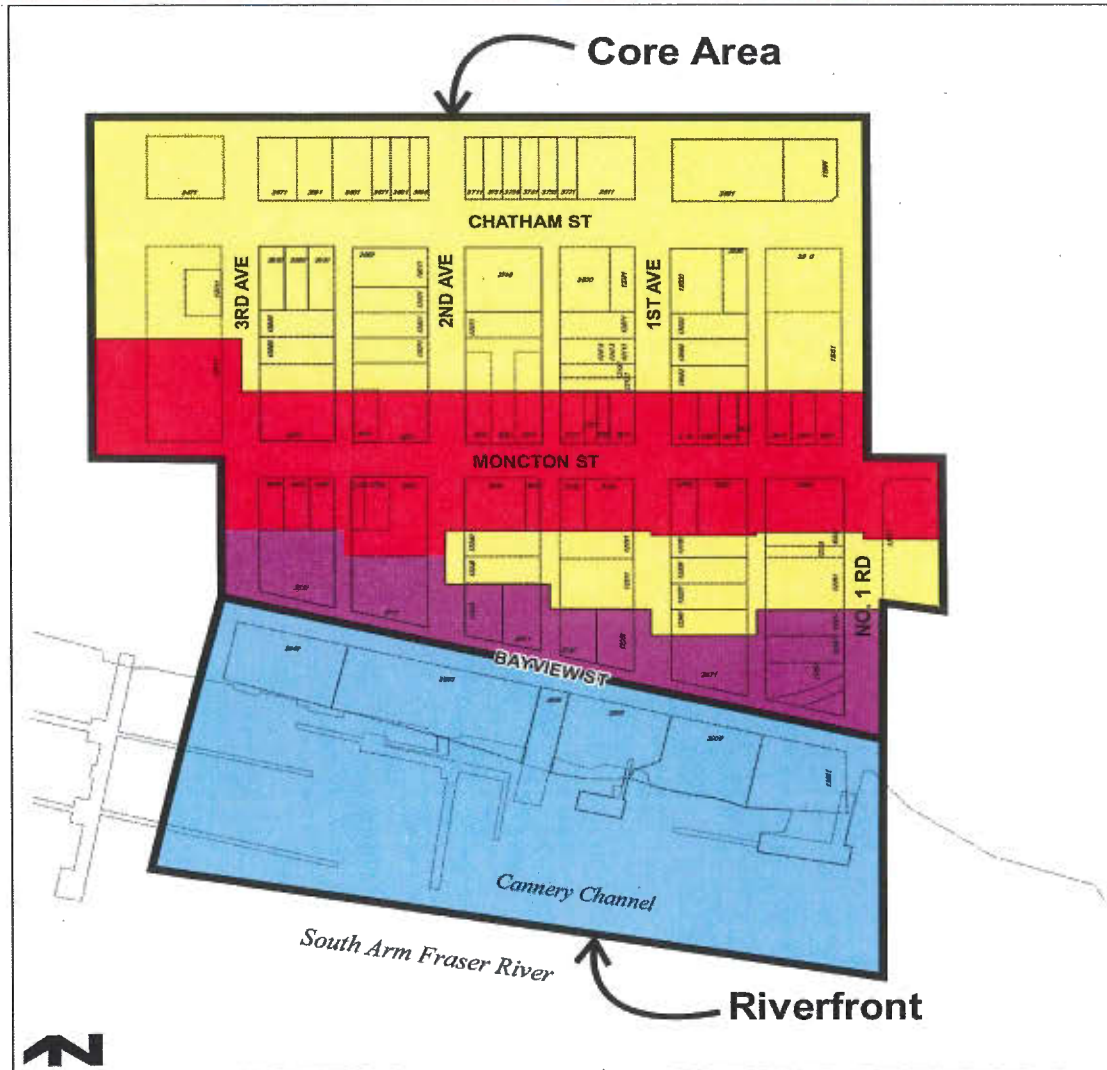
from Bayview Street is supported as the parkade structure below the two storeys will predominantly be concealed by the grade difference.”

- x) Repeal and replace the following text in Section 9.3.2.2 Area B: Steveston Village Sub Area Guidelines (Steveston Village Core Area – Architectural Elements) as follows:
 - “b) High quality materials that weather gracefully. Preferred cladding materials to be historic materials such as horizontal wood siding, board and batten, vertical channel board, wood shingles, 150 mm wide by 19 mm wood trim boards, or contemporary materials that provide effect (e.g., cementitious beveled board that replaces the appearance of bevelled wood siding). The use of brick is permitted as a secondary treatment for architectural elements and detailing in new buildings and new additions if that brick is clearly distinguishable from the Hepworth Building’s brick in colour and texture. For façade improvements to existing buildings, any brick that is removed should be replaced with similar brick, or a different brick or materials that would improve the aesthetics of the building and the area character. Stucco is prohibited. The use of brick or metal for exterior cladding or architectural detailing is not permitted, except to replace existing brick or metal materials with suitable brick, or similar metal, finishes in any existing building.
 - c) Metal or wood framed windows are preferred or contemporary materials that offer a compatible look. Exclusively vinyl framed windows are not permitted. Imitation divided lights should be avoided.
 - d) Roof top deck barrier railings are to be simple in design and consist primarily of transparent glazed panels at a minimum height that complies with British Columbia Building Code requirements but also mitigates their visibility from the street or from neighbouring roof top deck areas.”
- xi) Insert the following text into Section 9.3.2.2 Area B: Steveston Village Sub Area Guidelines (Steveston Village Riverfront – Settlement Patterns) and renumber clauses accordingly:
 - “b) Retain the existing large lot configuration along the Riverfront Area to accommodate a mix of large ‘cannery-like’ buildings and smaller buildings in accordance with the Steveston Village Riverfront Area guidelines.”
- xii) Repeal and replace the following text into Section 9.3.2.2 Area B: Steveston Village Sub Area Guidelines (Steveston Village Riverfront – Massing and Height) as follows:
 - “a) Typically be simple buildings blocks with broad gable roofs of approximately 12/12 pitch, augmented by subordinate portions with shed roofs having shallower pitches seamlessly connected to the main roof form. Flat roofs are not permitted.”

- xiii) Repeal and replace the following text into Section 9.3.2.2 Area B: Steveston Village Sub Area Guidelines (Steveston Village Riverfront – Architectural Elements) as follows:
- “a) Contribute to an interesting and varied roofscape which combines extensive use of shed and gable forms with very limited use of symmetrical hip, feature roofs, and dormers.
 - e) Employment of architectural elements which enhance enjoyment of the river, the sun, and the view and provide opportunities for private open space, especially in the case of residential uses where french balconies and similar features are encouraged. Roof decks are not permitted.
 - m) Metal or wood framed windows are preferred or contemporary materials that offer a compatible look. Application of exclusively vinyl framed windows in buildings is not supported. Vinyl siding is not permitted. Cementitious boards may be considered. The use of brick for exterior cladding or architectural detailing is not permitted, except to replace existing brick materials with suitable brick finishes in any existing building.”

- xiv) Repeal and replace the Steveston Village Land Use Density and Building Height Map as follows:

Steveston Village Land Use Density and Building Height Map



	Maximum FAR	Maximum Storeys	Maximum Building Height
Core Area, generally	1.6	3	12 m *
Core Area, Bayview Street (North)	1.2	3**	Varies due to grade elevation difference north of Bayview Street
Moncton Street **	1.2	2	9 m *
Riverfront Area	1.6	3	20 m GSC ***

* Maximum building height may increase where needed to improve the interface with adjacent existing buildings and streetscape, but may not exceed the maximum storeys.

** Three storey building height for buildings along the north side of Bayview Street shall include two storeys over a parkade structure.

*** Maximum building height may not exceed the height of the Gulf of Georgia Cannery, which is approximately 22 meters GSC.

- 2. This Bylaw may be cited as **“Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9775”**.

FIRST READING

PUBLIC HEARING

SECOND READING

THIRD READING

ADOPTED

CITY OF RICHMOND
APPROVED by LE
APPROVED by Manager or Solicitor

MAYOR

CORPORATE OFFICER



To: Mayor and Councillors
From: Terry Crowe
Manager, Policy Planning
Date: November 24, 2017
File:
Re: Proposed Steveston Area Plan Bylaw Changes: Village Heritage Conservation Policies and Design Guidelines

Purpose

The purpose of this memo is to respond to direction provided by Planning Committee on November 21, 2017, as follows:

- (1) *That staff be directed to:*
 - (a) *incorporate both the "Sakamoto Guidelines for Area Revitalization (1987)" and "Sakamoto Guidelines for Façade Improvements (1989)" in their entirety, into the Steveston Area Plan;*
 - (b) *incorporate design guidelines that would require solid non-transparent barrier railings for rooftop structures;*
 - (c) *incorporate design guidelines that would restrict the use of brick only for the replacement of existing brick façades throughout the Village; and*
 - (d) *remove the proposed policy to establish a bridge connection to the Gulf of Georgia site;**and to make the necessary changes to Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9775;*
- (2) *That new developments in the Riverfront Area south of Bayview Street be restricted to a maximum of two storeys and a maximum density of 1.2 FAR; and*

Response

City staff have made Planning Committee's requested changes to Official Community Plan Bylaw 7100, Amendment Bylaw 9775 to address Items (1)(a) through (d) above (Attachment 1).

As well, staff have prepared proposed Official Community Plan Bylaw 7100, Amendment Bylaw 9797 to address Planning Committee's Item (2) above (Attachment 2). Both proposed Bylaws are being forward to you for consideration by Council at its on Monday, November 27, 2017 meeting.

For clarification, please contact me at 604-276-4139

Terry Crowe
Manager, Policy Planning

TTC:ke

Att.2

pc: Joe Erceg, MCIP, General Manager, Planning and Development
Wayne Craig, Director, Development



**Richmond Official Community Plan Bylaw 7100
Amendment Bylaw 9775
Steveston Area Plan (Schedule 2.4)**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Official Community Plan Bylaw 7100 is amended by repealing and replacing and/or adding text and accompanying diagrams and materials to various sections of the Steveston Area Plan (Schedule 2.4) as follows:
 - i) Adding the following text into Section 3.2.3 Steveston Village Node:
 - “h) Promote public access to the waterfront between 3rd Avenue and No. 1 Road through new pedestrian connections from Bayview Street and upgrades to the existing pedestrian paths.
 - i) Work toward uninterrupted connectivity along the waterfront between 3rd Avenue and No. 1 Road through extensions and improvements to walkway infrastructure and surfaces.”
 - ii) Repeal and replace the following text in Section 4.0 Heritage – Policies for Steveston Planning Area:
 - “k) To assist in achieving heritage conservation, consider utilizing a variety of regulatory and financial incentives through the applicable development application requirements (i.e., rezoning, development permit and/or heritage alteration permit), including but not limited to new zones, reduced parking, loading and unloading requirements, density bonusing and density transfer as well as consider using a variety of legal tools (i.e., heritage revitalization agreements, heritage covenants, phased development agreements).
 - Note: Supporting policies and guidelines are contained in the Heritage (Section 4.0), Transportation (Section 5.0), Natural and Human Environment (Section 6.0) and Development Permit Guidelines (Section 9.0) in the Steveston Area Plan.”
 - iii) Repeal and replace the following text in Section 4.0 Heritage – Policies for Steveston Village Node:
 - “l) Along Moncton Street the maximum building height shall be two-storeys and 9 m in height to ensure the size and scale of Moncton Street development is consistent with the village node.”

- iv) Adding the following text into Section 5.0 Transportation and accompanying diagram:

“Objective 6: Consider on-site parking reduction opportunities to help achieve the City’s heritage conservation and management objectives for the Steveston Village Heritage Conservation Area, in recognition that Steveston Village (Core and Riverfront Areas) is a complete and compact community well serviced by public transit offering a wide range of services to residents, visitors and employees.

Steveston Village Heritage Conservation Area Map



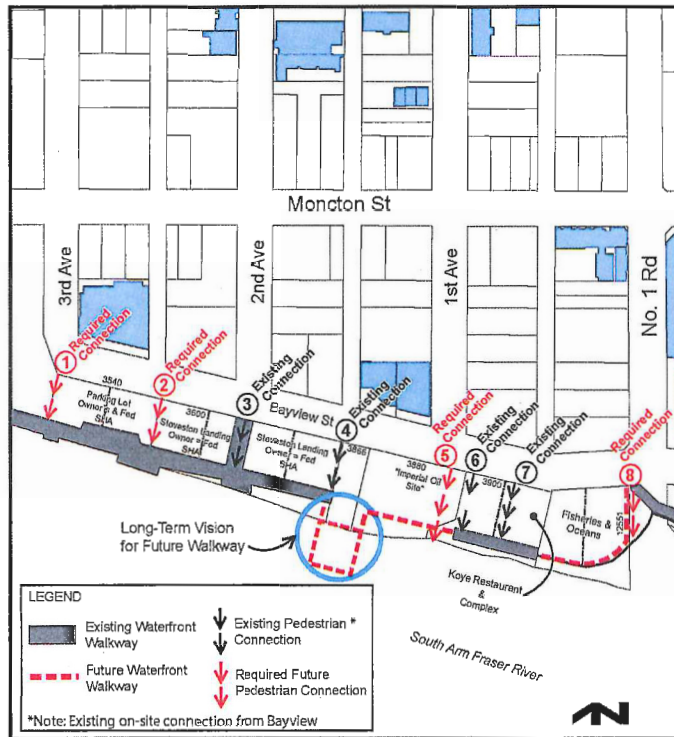
Policies:

- a) Consideration of parking reductions to be assessed through the applicable required development application.
- b) For development of new residential uses, a 13% reduction from applicable Zoning Bylaw parking requirements can be considered.
- c) For development of new commercial uses, a 33% reduction from applicable Zoning Bylaw parking requirements can be considered.
- d) Required on-site residential visitor parking and other non-residential use parking (i.e., commercial) may be shared.”

- v) Adding the following text into Section 6.0 Natural & Human Environment and accompanying diagrams:

“Objective 6: Work toward public accessibility for pedestrians to and along the waterfront between 3rd Avenue and No. 1 Road through pathways that connect Bayview Street to the water’s edge, and completion of a continuous boardwalk.

Existing and Future Riverfront Walkways and Connections

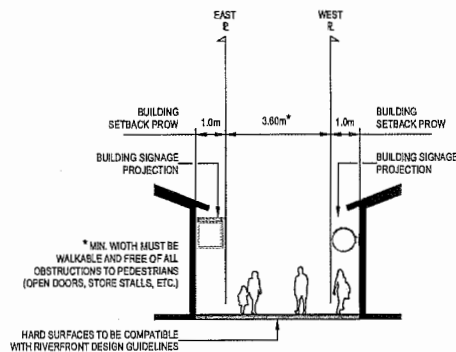


Policies:

- a) Work with the Federal Government, Steveston Harbour Authority and other property owners to establish new pedestrian connections at the following street and lane ends.
 - Pedestrian connections at road ends at the south foot of No. 1 Road, 1st Avenue and 3rd Avenue will meet the following guiding principles for universal accessibility and urban design:
 - Create a public right-of-passage with a minimum width of 5.6 m including 1.0 m setbacks from adjacent buildings
 - Building signage projections up to 1.0 m into any building setback and detailed as per Steveston Development Permit Area Design Guidelines

- A minimum of 5.6 m of the above minimum 5.6 m public right-of-passage must be free and clear of obstructions, including but not limited to: building projections (except for signage), doors, patios, store stalls.
- Accessible hard surfaces with materials compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
- Pedestrian connections materials and surface treatments designed to be safe and accessible for all users.
- Undertake enhancements to existing pedestrian connections in accordance with these guidelines where appropriate.

Pedestrian Connections at Road Ends



X-SECTION NORTH - SOUTH WALKWAYS

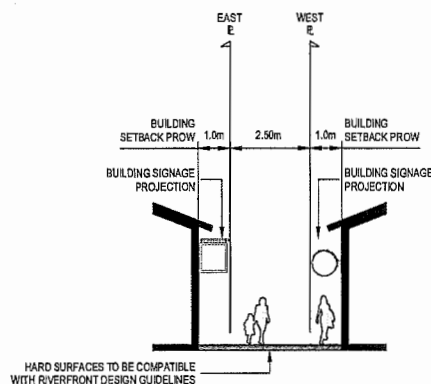
SOUTH FOOT OF:
NO.1 ROAD
1ST AVENUE
2ND AVENUE
3RD AVENUE

- Connections at the lane ends between No. 1 Road and 1st Avenue, between 1st Avenue and 2nd Avenue; and between 2nd Avenue and 3rd Avenue, will meet the following guiding principles for universal accessibility and urban design:
 - Create a public right-of-passage with a minimum width of 4.5 m including 1.0 m setbacks from adjacent buildings
 - Building signage projections up to 1.0 m into any building setback and detailed as per Steveston Development Permit Area Design Guidelines
 - A minimum of 4.5 m of the above minimum 4.5 m public right-of-passage must be free and clear of obstructions, including but not

limited to: building projections (except for signage), doors, patios, store stalls.

- Accessible hard surfaces with materials compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
- Pedestrian connections materials and surface treatments designed to be safe and accessible for all users.
- Undertake enhancements to existing pedestrian connections in accordance with these guidelines where appropriate.

Pedestrian Connections at Lane Ends

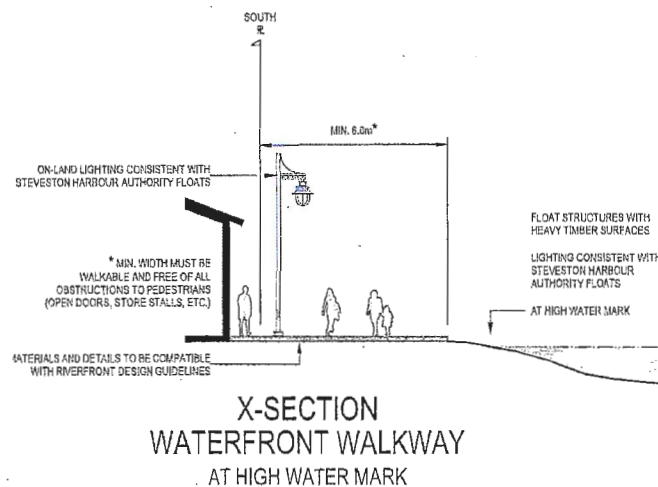


X-SECTION
NORTH - SOUTH WALKWAYS
 SOUTH FOOT OF LANE ENDS BETWEEN:
 NO.1 ROAD & 1ST AVENUE
 1ST AVENUE & 2ND AVENUE
 2ND AVENUE & 3RD AVENUE

- b) Work with the Federal Government, Steveston Harbour Authority and other property owners to establish waterfront walkway connections at, and above, high water mark.
- Walkway sections that are situated at high water mark elevation will meet the following guiding principles for universal accessibility and urban design:
 - Minimum 6.0 m in width.
 - Connected to walkways above, at the street end nodes, with gangways to create accessible access points.
 - Float structures with heavy timber surfaces.

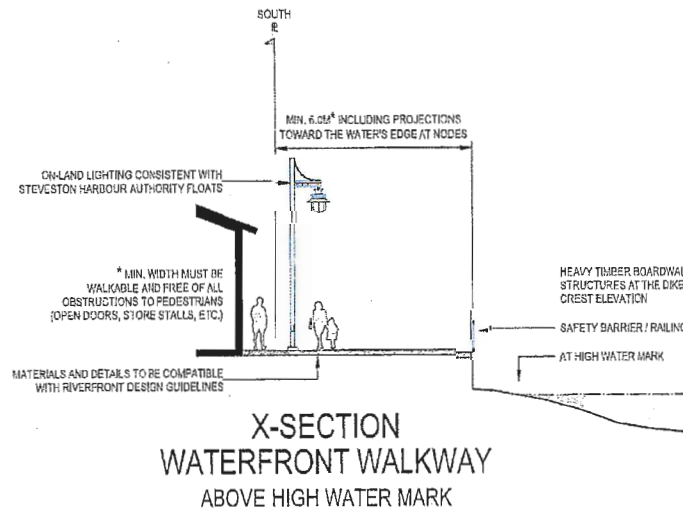
- Materials and details compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
- Waterfront walkway materials and surface treatments designed to be safe and accessible for all users.
- Lighting to enable nighttime use consistent with Steveston Harbour Authority floats.
- Undertake enhancements to existing waterfront walkway connections in accordance with these guidelines where appropriate.

Waterfront Walkway at High Water Mark



- Walkway sections that are situated above high water mark elevation will meet the following guiding principles for universal accessibility and urban design:
 - Minimum 6.0 m in width including projections toward the water’s edge at nodes (i.e. both street end and lane end connections).
 - Heavy timber boardwalk structures at the dike crest elevation.
 - Materials and details compatible with “Steveston Village Riverfront” Development Permit Area design guidelines (see: Section 9.3.2.2.b).
 - Waterfront walkway materials and surface treatments designed to be safe and accessible for all users.
 - Lighting, seating and other site furnishings, as appropriate, at nodes.
 - Undertake enhancements to existing waterfront walkway connections in accordance with these guidelines where appropriate.

Waterfront Walkway Above High Water Mark

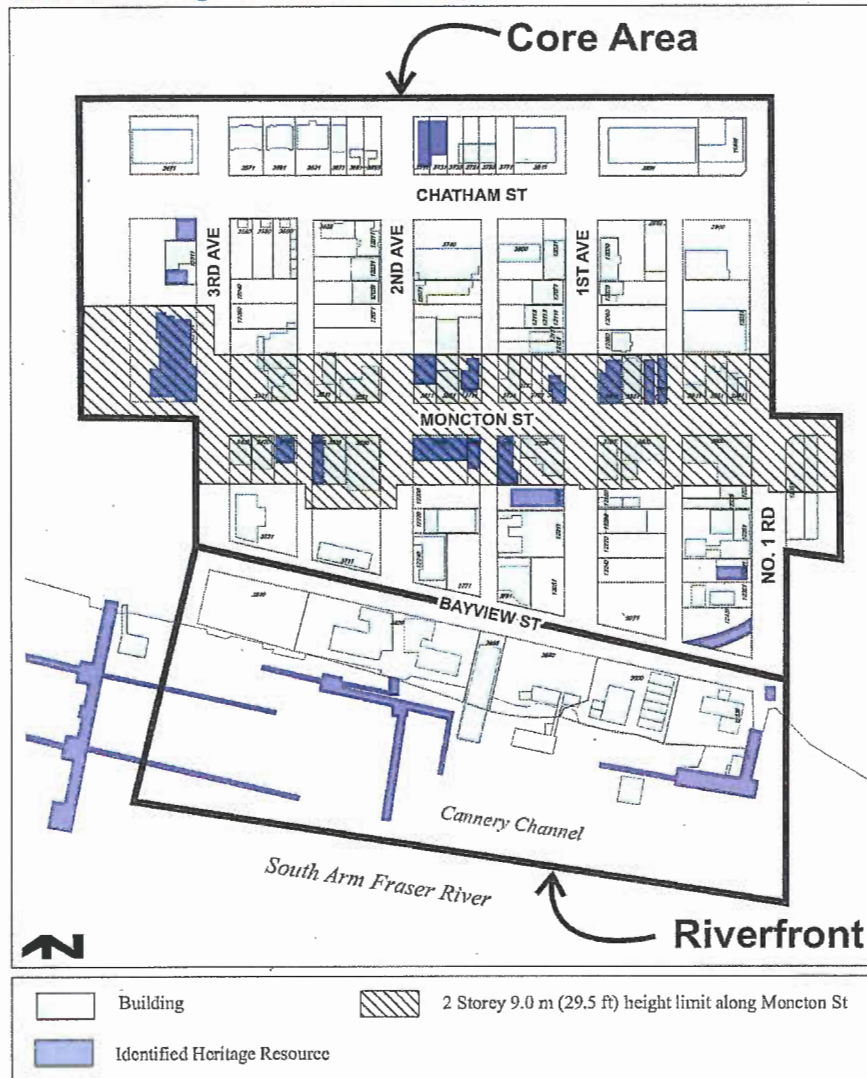


- c) Work with Steveston Harbour Authority to connect the waterfront walkway to existing structures as follows:
- Piers at the south foot of No. 1 Road and 3rd Avenue:
 - Increase the accommodation of pedestrian volume, circulation, resting and viewing points, while removing any obstructions to access to the water for harbour-related activities.
 - Add seating and other site furnishings in accessible locations (e.g. pier ends) to further enable people to observe harbour activities.
 - Floats:
 - Extend the length of publicly accessible floats.
 - Increase the number of connections from the land side.
 - Parking lot at 3rd Avenue:
 - Dedicate a pedestrian route to the waterfront boardwalk and pier.

- d) In scenarios where waterfront walkways deadend as an interim condition, ensure developments provide suitable universally accessible on-site connections from these points to Bayview Street.
- e) Developers through rezoning, development permit and/or heritage alteration permit applications shall be required to provide their portion of the continuous, universally accessible, riverfront walkway through:
- Ensuring public access to the riverfront walkway and pathway connections in perpetuity through the necessary legal agreements.
 - Design and construction of the riverfront walkway and pathway connections by the developer in accordance with the design guidelines contained in the Steveston Area Plan.”
- vi) Inserting the following text at the end of Section 9.1 Application and Intent:
- “The “Sakamoto Guidelines for Design Criteria for the Steveston Revitalization Area” and the “Sakamoto Guidelines for Steveston Downtown Revitalization Area Façade Guidelines” are contained in Appendix 2 to the Steveston Area Plan for reference purposes and are applicable to the Steveston Village Core and Riverfront Area. These documents can be interpreted flexibly and are to be used in coordination with the Development Permit Steveston Village Sub Area Guidelines (Core and Riverfront Area) when reviewing development proposals.”
- vii) Amending the Steveston Area Plan table of contents to add Appendix 2 (Sakamoto Guidelines for Design Criteria for the Steveston Revitalization Area; Sakamoto Guidelines for Steveston Downtown Revitalization Area Façade Guidelines).
- viii) Amending the Steveston Area Plan to add the Sakamoto Guidelines for Design Criteria for the Steveston Revitalization Area and Sakamoto Guidelines for Steveston Downtown Revitalization Area Façade Guidelines (contained in Schedule A attached to and forming part of Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9775)

- ix) In Section 9.3 Additional Development Permit Guidelines: Character Area Guidelines, repeal and replace the Steveston Village Character Area Map as follows:

Steveston Village Character Area Map



- x) Inserting the following text to Section 9.3.2.1 Steveston Village General Guidelines: Shifts in Scale:

“e) Existing elevations in the Village Core (at Moncton Street and 3rd Avenue), measured at 1.4 m GSC (Geodetic Survey Datum of Canada) is a historic feature in the Steveston Village Character Area to be retained:

- For properties in the Steveston Village Core, north of Bayview Street, the higher elevation of 1.4 m GSC or of the existing adjacent sidewalk shall be used and referenced in the development.

- For properties in the Steveston Village Riverfront Area, south of Bayview Street, the higher elevation of 3.2 m GSC or of the existing adjacent sidewalk shall be used and referenced in the development.”
- xi) Repeal and replace the following text in Section 9.3.2.1 Steveston Village General Guidelines: Roofscapes, Exterior Walls, and Finishes as follows:
- “g) Using horizontal siding as the primary exterior cladding materials, complemented by a judicious use of glass, concrete, stucco and delicate timber details. Siding is encouraged to include historical treatments such as ship lap, flat lap horizontal wood, board-and-batten, and wood shingles. In keeping with the special heritage character of the two sub-areas, the use of metal exterior cladding or architectural detailing is not permitted in the Village Core except to replace existing metal materials with similar metal finishes in any existing building. The use of brick is not permitted in the Core or Riverfront Area except to replace any existing brick with similar brick.”
- xii) Repeal and replace the following text in Section 9.3.2.2 Area B: Steveston Village Sub Area Guidelines (Steveston Village Core Area – Massing and Height) as follows:
- “a) Reinforce a continuous commercial storefront streetwall with harmonious height of buildings, parapets, canopies and fascias. Building height should typically be no more than three storeys and may be varied to provide visual interest to the streetscape roofline (e.g., stepping from two to three-storey, except along Moncton Street where building heights are to be limited at two storeys.
- g) Make use of roofs as outdoor living spaces except for the roof decks within 3.0 m of the street property line; use the 3.0 m zone as a water collection area or inaccessible landscape area where no element or mature plant material is higher than 1.05 m above roof deck level.
- h) Building facades facing streets, or within 10 m (32.8 ft.) of a street, should have parapets at least 1.2 m above roof deck level.
- i) Solar panels may be affixed to flat roofs up to a height of 1.20 m and placed in any section of the roof deck that is a minimum distance of 1.0 m back from the roof edge. On a sloped roof, panels must be affixed flush to the roof and may not be more than 0.2 m above the roof surface.
- j) To encourage use of roof top decks as outdoor living spaces and architecturally integrate individual and communal rooftop deck access points into the building, such structures are not permitted unless all of the following criteria are met:
- For individual unit roof top deck access:

- Hatch access points (i.e., also known as pop-ups) should not exceed 1.83 m in height, as measured from the roof deck and be well integrated with the overall design of the building and setback from all roof edges to a minimum distance of 1.0 m.
 - Evaluate individual roof top deck access structures to ensure they are not visible from the streets and other public vantage points (i.e., lanes) generally from a distance of 90 m, taking into account any site specific context.
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 - Evaluate communal rooftop deck access structures to ensure they are not visible from the streets and other public vantage points (i.e., lanes) generally from a distance of 90 m, taking into account any site specific context.
- k) On Bayview Street (north side), to achieve a suitable transition in built form moving north from Bayview Street to Moncton Street:
- For the north 50% of any lot depth, a density of 1.2 F.A.R. and 3 storeys maximum building height (containing a parkade structure and two storeys above) is supported.
 - For the south 50% of any lot depth (nearest to Bayview Street which is the dyke) a density of 1.2 F.A.R. and 2 storeys building height as viewed from Bayview Street is supported as the parkade structure below the two storeys will predominantly be concealed by the grade difference.”
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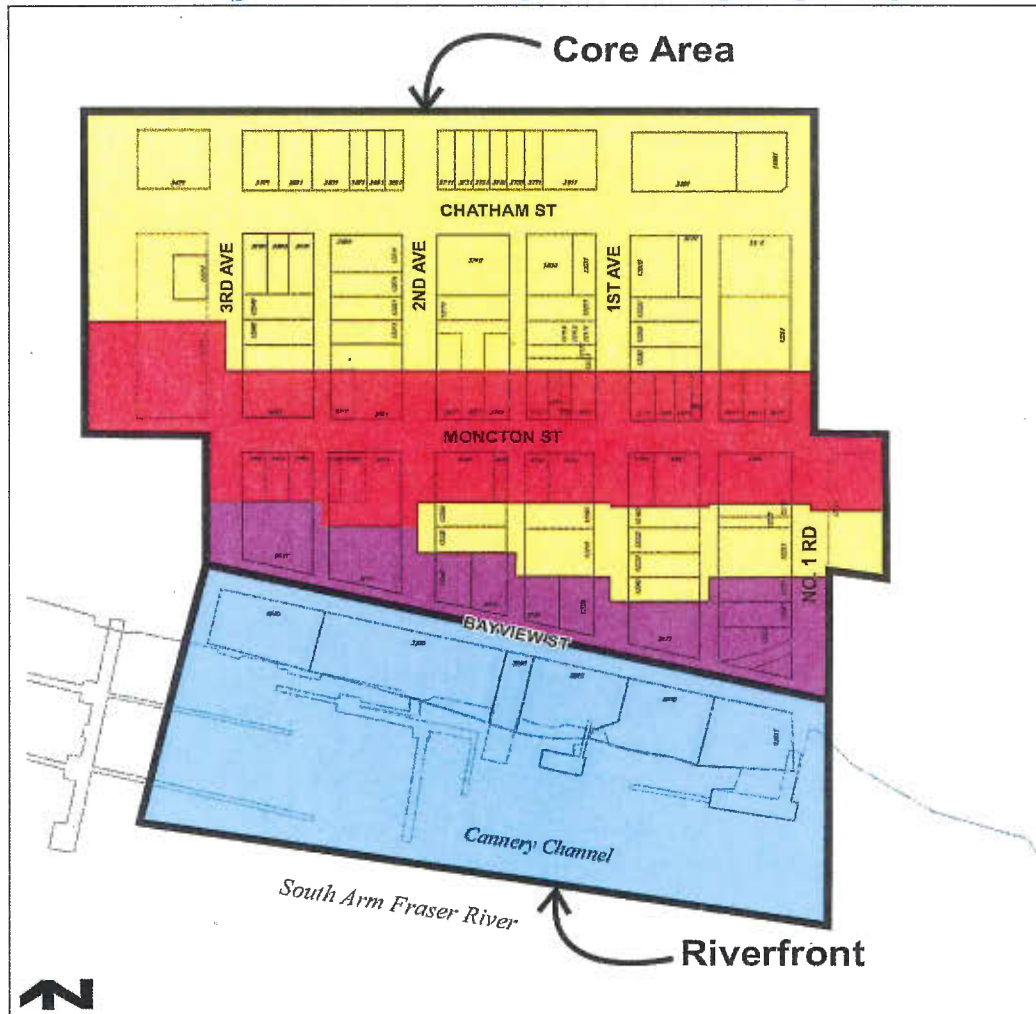
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*** Maximum building height may not exceed the height of the Gulf of Georgia Cannery, which is approximately 22 meters GSC.

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SECOND READING

THIRD READING

ADOPTED

CITY OF RICHMOND
APPROVED by
KE
APPROVED by Manager or Director
[Signature]

MAYOR

CORPORATE OFFICER

Design Criteria for the STEVESTON REVITALIZATION AREA



Prepared by the Richmond Planning Department

Community Development -
Richmond

DES

CNCL - 554

DESIGN CRITERIA FOR THE STEVESTON REVITALIZATION AREA

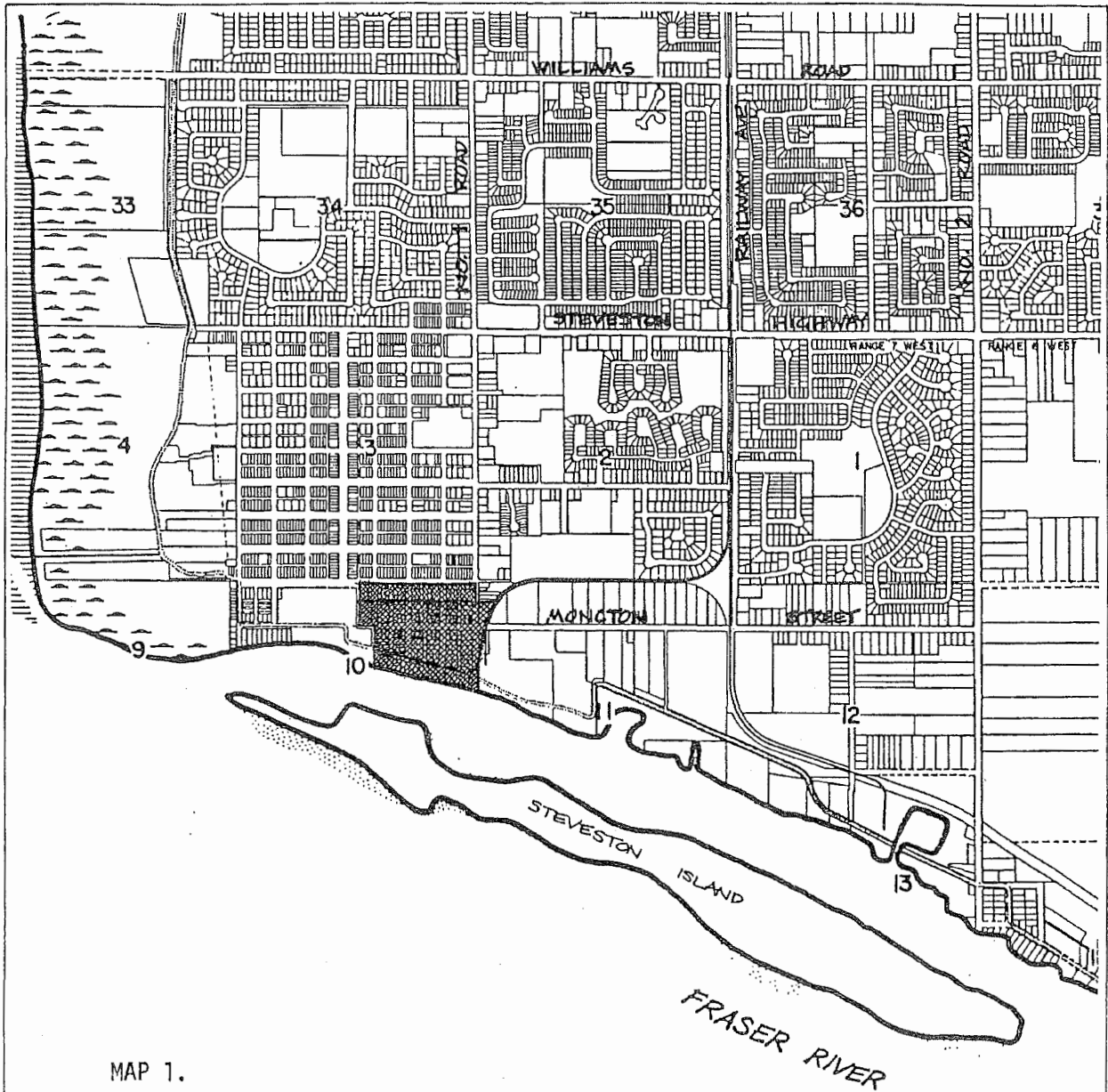
Prepared by the Richmond Planning Department

December, 1987

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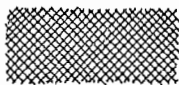
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MAP 1.

ATTACHMENT '2'
 DEVELOPMENT
 PERMIT
 AREA MAP



REVITALIZATION
 AREA

STEVESTON



0 250
 METRES

JANUARY 1987

DESIGN CRITERIA FOR STEVESTON

INTRODUCTION

These design criteria are a supplement to the development permit guidelines in the Steveston Area Plan, Attachments 2 and 3. The Steveston Area Plan forms part of the Official Community Plan for Richmond. The map on page 1 shows the applicable area.

The development permit guidelines have been prepared in accordance with the Municipal Act of the Province of British Columbia, and every person who intends to construct a building or alter the land in the areas shown on the development permit map (attachment 2) must first obtain a development permit. The Permit is issued by Council subject to the guidelines described in the Steveston Area Plan. The guidelines are repeated in this document in bold type, and must be adhered to. The design criteria in this document will assist developers to understand and respond to the special conditions in the Steveston Area.

The Richmond Zoning By-law, Screening By-law,* Parking By-law,* Building Code, and Sign By-law will all affect the design of buildings in Steveston. The criteria in this document expand on both development permit guidelines and the Screening By-law regulations, therefore a separate Screening Permit is not required. A Building Permit and Sign Permit will be required after the Development Permit is approved.

1. HERITAGE BUILDING VARIANCES

Because this area is a heritage area, owners of recognized heritage buildings may have special opportunities and obligations. Buildings shown on Map 2 as potential heritage buildings may be considered for variances to the Zoning By-law (including parking requirements) and Screening By-law regulations. In order to receive the variances, applicants will be required to adhere to the form, character and building finish criteria in this document, and have a Heritage Designation By-law approved for their building.** For a list of the potential heritage buildings, refer to Appendix 5. (Buildings on this list may be removed subject to the consultant work being undertaken in 1988.)

2. DOWNTOWN REVITALIZATION AND FACADE IMPROVEMENT GRANTS

Because Steveston is also a Downtown Revitalization Area, building owners are eligible for Facade Improvement Grants. The grants are provided by the B.C. Downtown Revitalization Program and administered by the Municipality. The grants are intended to assist owners to upgrade their store fronts in accordance with local criteria, as specified under guidelines #4 in this report. Financial and procedural details regarding the grants are provided in Appendix 1.

* draft

** pursuant to the Heritage Conservation Act

3. ADMINISTRATIVE REQUIREMENTS

HOW TO APPLY FOR A DEVELOPMENT PERMIT

You will need a Development Permit if you plan to develop in the Steveston Downtown Revitalization Area.

You can obtain an application form for a Development Permit at the counter in the Planning Department. The general requirements, including a letter of intent, owner's signature, and fees are on the application form.

Before making a formal application, you may want to read this report and check servicing requirements with the Engineering Department. Planning staff will assist you with any questions regarding the application form, design criteria or general planning for the area.

PLANS AND OTHER INFORMATION REQUIRED

A complete set of preliminary architectural drawings is recommended, accompanied by a letter describing the project in full. This information is important because planning staff, the Design Panel, Council, and people on neighbouring properties will use the information to evaluate your development. Plans should include:

1. a Site Plan showing the street, surrounding properties, parking, landscaping and all major buildings. Dimensions should be sufficient to determine compliance with or variances to the Zoning By-law. Calculations should indicate parking.

Context photos, and a plan and street elevation showing adjacent buildings are requested by the Design Panel.

2. Preliminary architectural plans should indicate general interior layouts, main front entrances, balconies, outdoor living areas, amenity areas, awnings, canopies, signs, exterior elevations and exterior facade finish materials.
3. Building sections or elevations should be in sufficient detail to determine heights and bulk. Elevations should show exterior finish materials and door and window finish materials. A colour scheme is requested by the Design Panel.
4. Preliminary landscape plans should indicate required landscaping, screening, fencing, street furniture and all existing trees on the site.

HOW THE PROCESS WORKS

Development Permits are issued by Council at regular Council meetings. The process is generally as follows:

Step 1: The applicant consults with the Planning Department and obtains an application form.

Step 2: The applicant's architect prepares preliminary plans based on the Criteria for Development Permits published by the Municipality.

Step 3: The applicant submits the application form, fee, plans, and other required documentation to the Planning Department.

Step 4: The Planning Department obtains feedback from relevant Municipal departments and agencies. Planning staff will, along with the Design Panel, review the plans to determine compliance with the Criteria. The architect may make a presentation to the Design Panel.

Municipal staff will also determine the need for variances to the Zoning By-law or Screening By-law.

Step 5: Planning staff will contact the applicant if any changes to the plans are required.

The applicant's architect or landscape architect may need to revise drawings at this stage.

Step 6: When plans are sufficient, planning staff will prepare a report to Council. The completed permit and plans will be attached to the report. The Municipal Clerk will give ten days notice as required by the Municipal Act, so that affected property owners can speak at the Hearing-in-Public.

Step 7: Council will hold a Hearing-in-Public and will then consider issuance of the Development Permit, usually the same day, at a regular Council meeting.

Step 8: Staff will register the Permit on the title at the Land Registry Office.

Later, staff will inspect the completed project to determine compliance with the terms of the Permit.

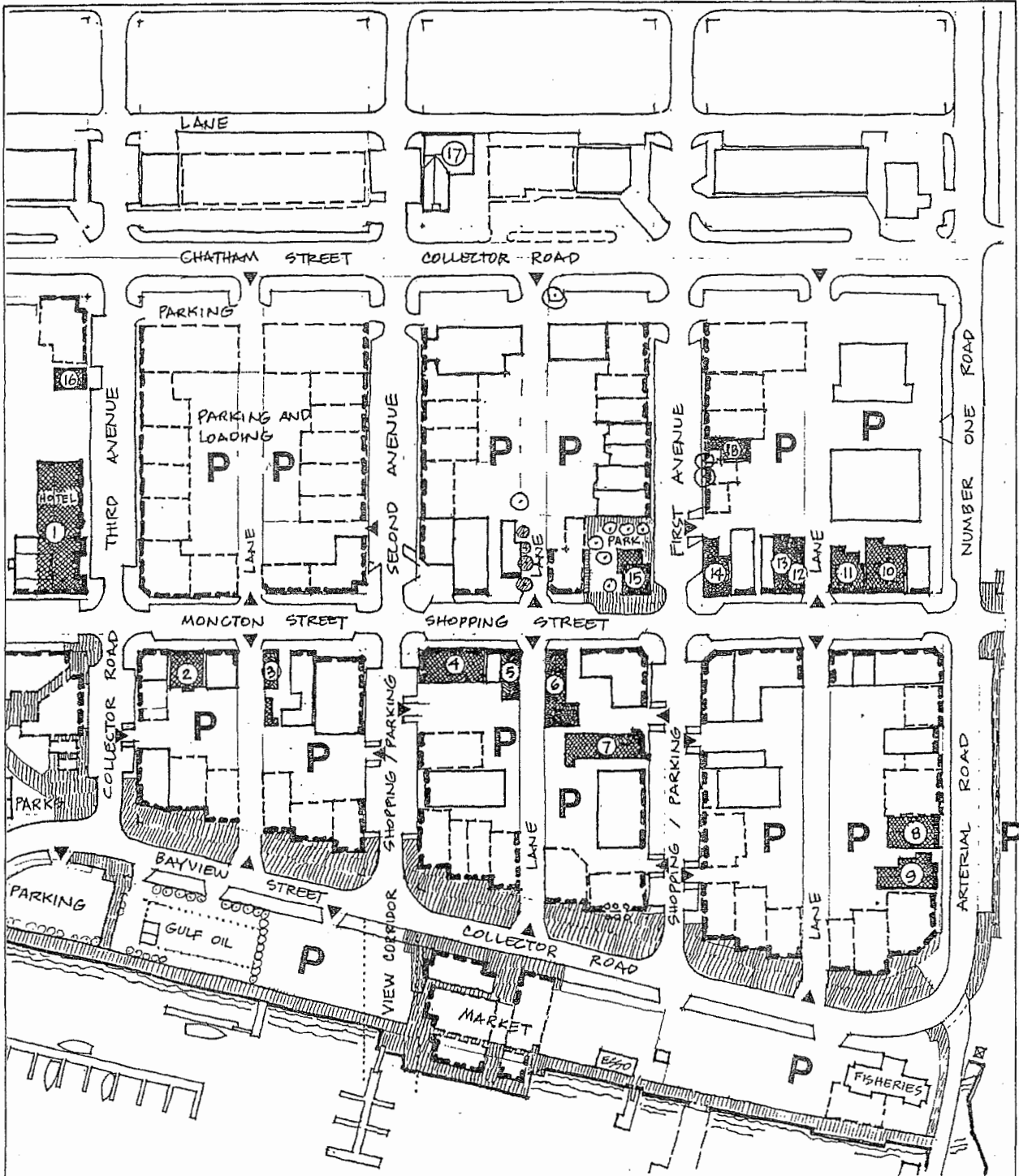
STEVESTON DOWNTOWN DESIGN CONCEPT

The design concept plan is intended to lend cohesiveness to the Revitalization Area criteria. The concept plan illustrates the important relationships between present and future buildings, streets, parking and access lanes.

The design concept shows the extent of street improvements for the foreseeable future. Number One Road, Bayview Street, Third Avenue and Chatham Street function primarily to move traffic into and out of the area. Motorists will also use Moncton to gain access, but its main function is as a shopping street with space for short term customer parking. First and Second Avenue and most lanes have extensive parking and loading and provide the main access to parking lots and shops.

The design concept also shows the approximate location and massing of new buildings. This plan is not intended to be fixed in stone, but shows the preferred street setbacks and land expected to be developed for parking. Because the concept encourages a filling-in of empty spaces and requires a continuous commercial frontage along shopping streets, the area will become more attractive to window shoppers.

Existing buildings which have heritage potential are shown on the design concept. These are the buildings where some relaxation of Zoning and Screening regulations will be considered.



STEVESTON DOWNTOWN DESIGN CONCEPT

- PROPOSED BUILDING MASSING
- ⑰ POTENTIAL HERITAGE BUILDINGS. SEE APPENDIX 5.
- - - COMMERCIAL FACADE / CANOPIES
- ▶ P PARKING / ACCESS
- ||||| PROPOSED BOARDWALK

MAP 2

FEB. 1988

STEVESTON DOWNTOWN REVITALIZATION AREA

DESIGN GUIDELINES

1. The distinctive character of the original buildings should be preserved and restored in keeping with the styles of the era. Pre-1930 building often had false fronts, gable roofs, and canopies.

There are two distinctive types of buildings in Steveston, the commercial buildings on the Moncton Street vicinity and the industrial buildings on the waterfront. The two types are discussed and illustrated separately on the following pages. See Appendix 2 for a sketch of building types.



Second Avenue

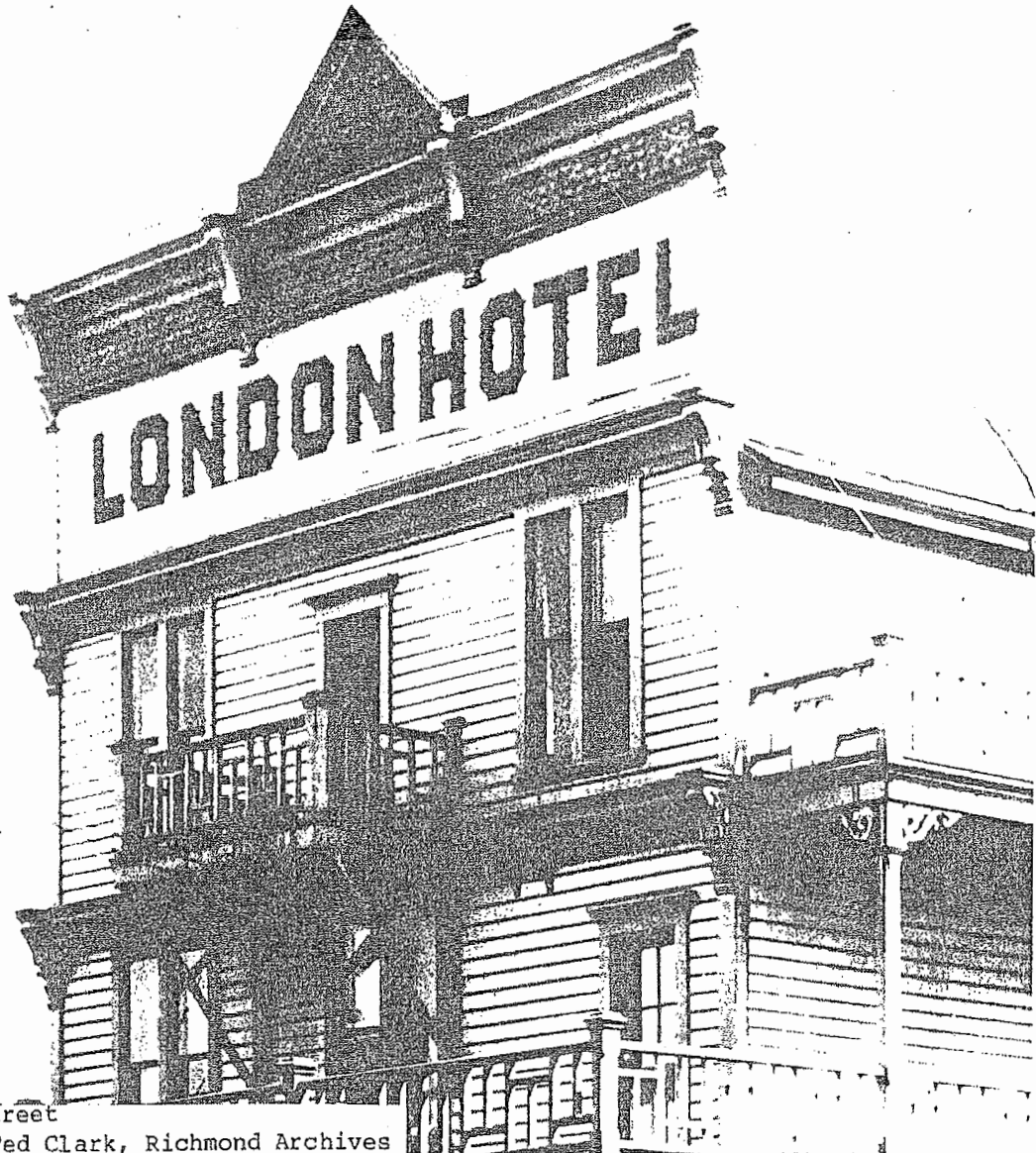
Source: Vancouver City Archives.

1.1 Traditional buildings on Moncton Street and vicinity

Today several buildings remain on Moncton Street dating from the 1920's and 1930's.

We can see from archival photographs that buildings from the turn of the century had a distinctive decorated false-front style.

Early wooden buildings, which did not survive the fire of 1918, were generally two or three storeys in height, with more elaborate ornamentation than the 1920's commercial buildings. The turn-of-the-century building typically had balconies, decorated handrails, and decorative trim. The sidewalks in front of older buildings were often protected from the weather by canopies, usually supported on carved posts with decorated brackets. These old buildings had gabled roofs with rectilinear or ornamented false fronts facing the street, and were usually one or two storeys in height.



Moncton Street

Source: Ted Clark, Richmond Archives

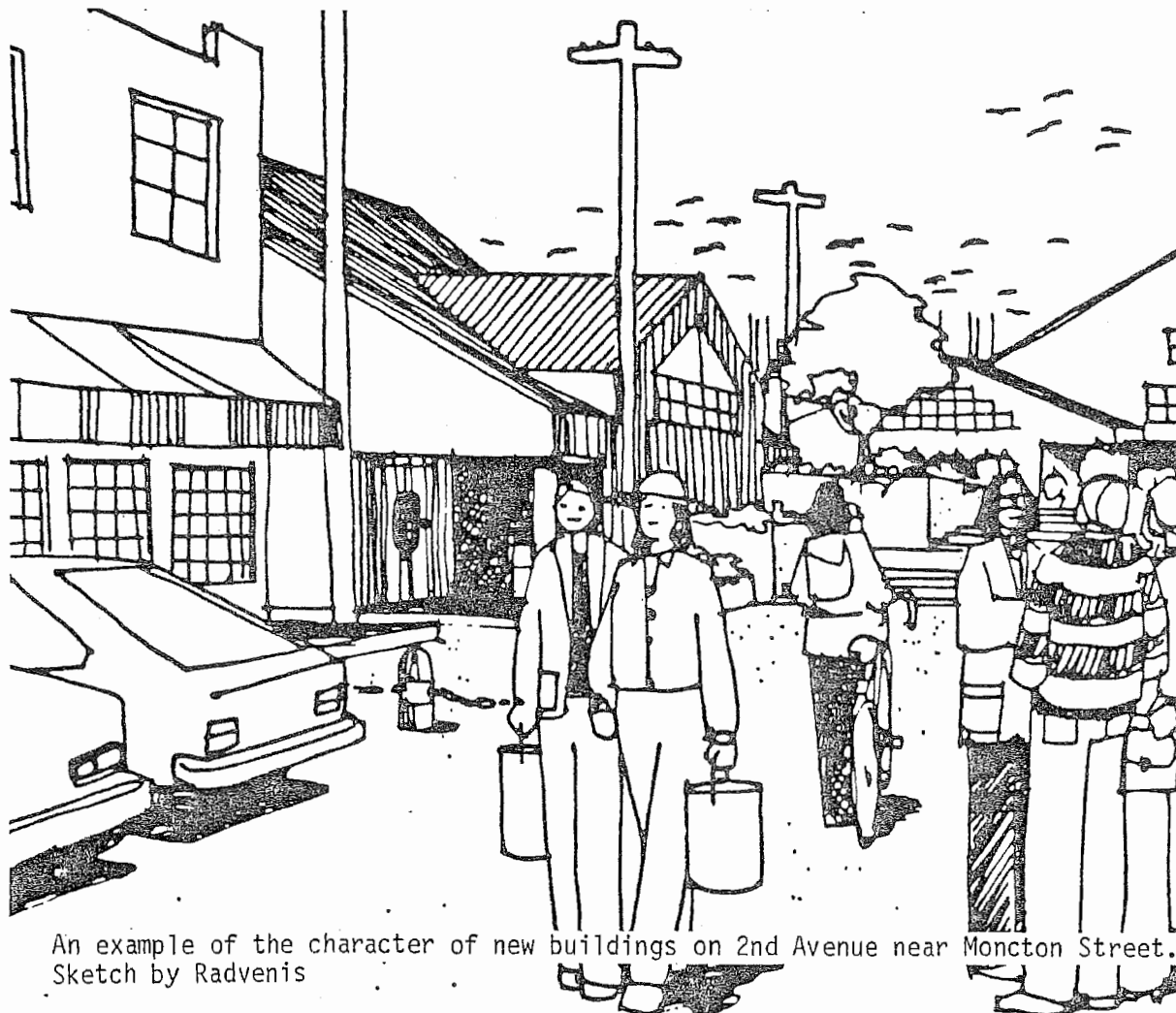
Existing buildings, if they are renovated or restored, should be based on traditions illustrated in this document. The community would like to see the following elements preserved or restored:

- . gabled roofs and false fronts
- . decorative brackets, balconies and posts
- . canopies
- . painted wooden horizontal siding or shingles
- . wooden vertical windows or bay windows

New buildings

New buildings in the area should be designed to compliment the tradition established by existing older buildings. To do this, new buildings should be of two or three stories in height, should have features of interest to shoppers, and should have simple, pedestrian scaled signs. Finish materials should be compatible with traditional materials. Replica buildings should be faithful to the buildings illustrated in this report or seen in other old photographs.

For details of building style, refer to Appendix 2.



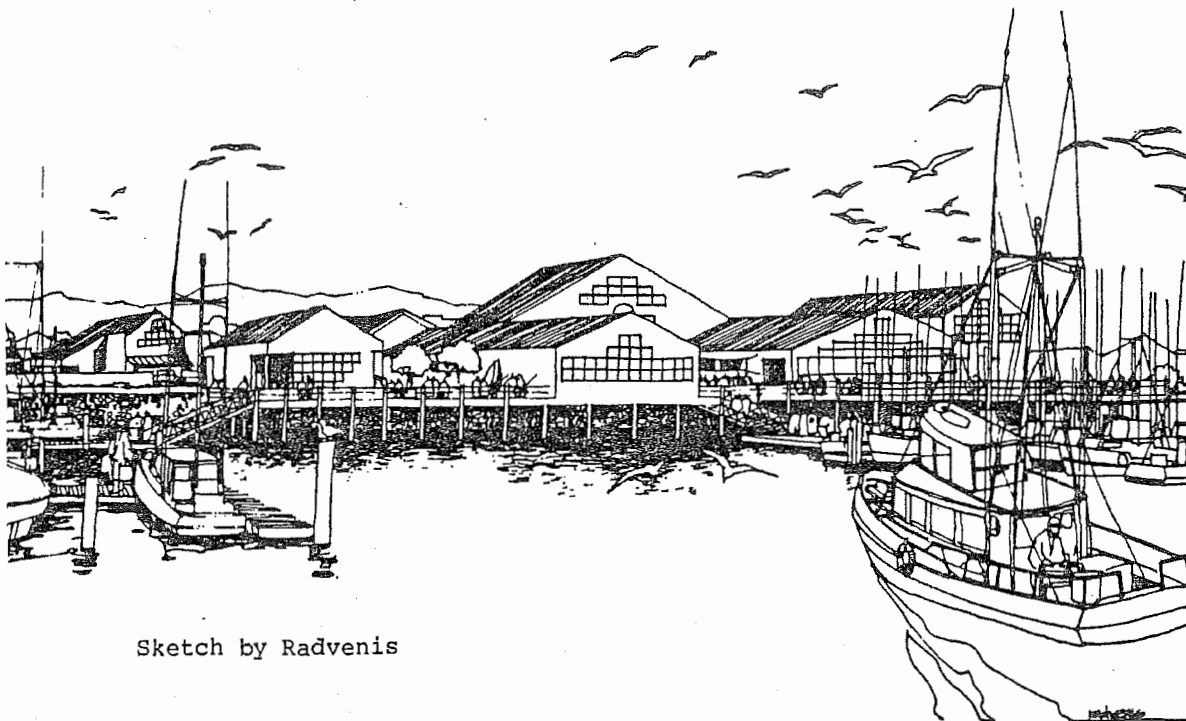
An example of the character of new buildings on 2nd Avenue near Moncton Street. Sketch by Radvenis

1.2 Traditional buildings on the Bayview Street waterfront

B.C. coastal industrial architecture has traditionally considered fairly large structures with peaked roofs having ridge boards perpendicular to the shoreline. Some structures later evolved into a "L" shaped plan.

Originally, all structures had board and batten siding but in recent years most waterfront buildings have been clad in metal.

These buildings traditionally had small-panel windows, with a vertical format.



Sketch by Radvenis

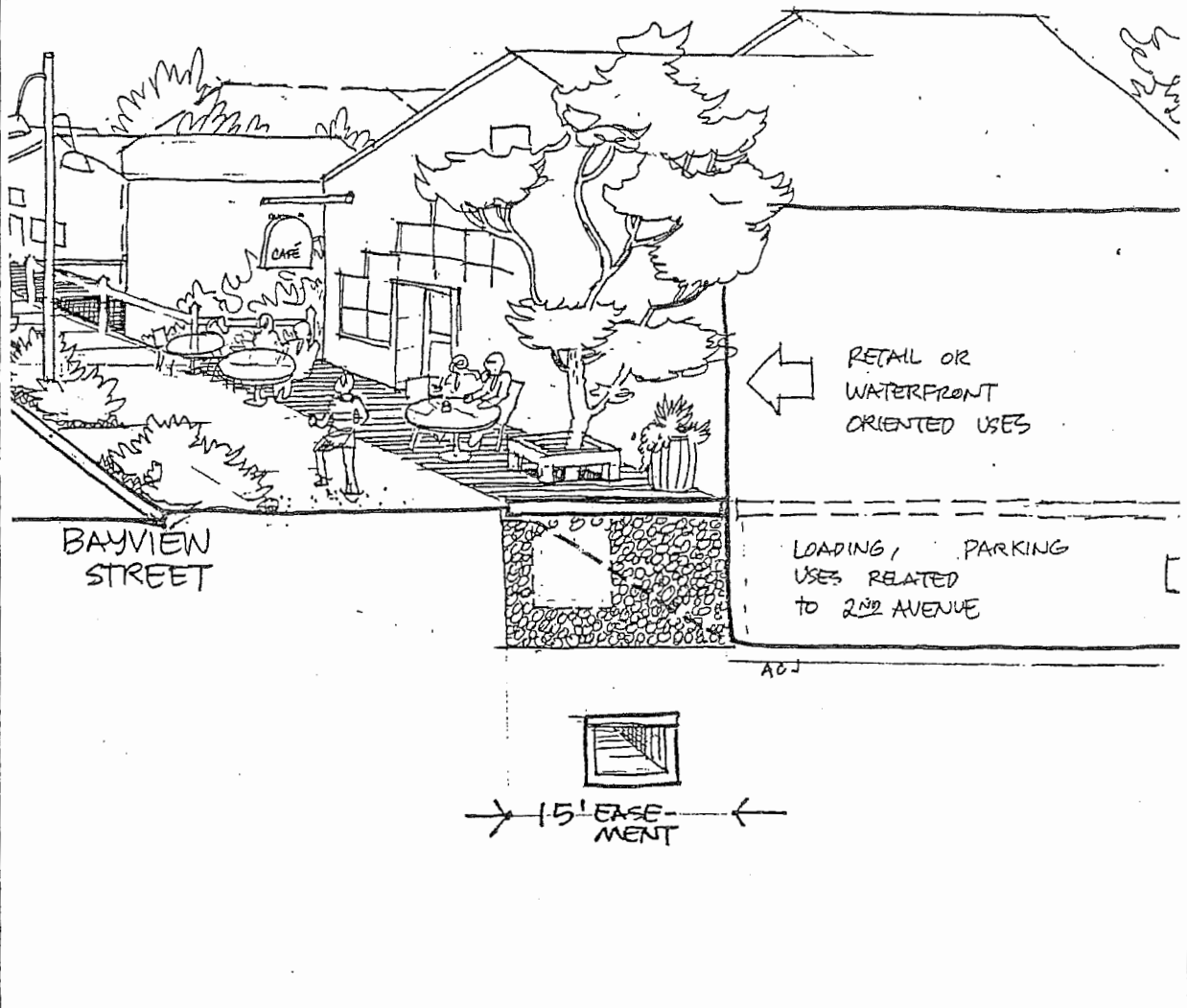
New buildings on Bayview Street

Siting of new buildings on Bayview Street or the waterfront should be with a consideration of views of the water, both for people in the new building and for people on the street. It is desirable to maintain unobstructed views of the water from all north-south streets. New buildings on Bayview Street may have a more industrial character than buildings on Moncton Street, but should not exceed three stories in height, measured from the dyke elevation. A form and character similar to waterfront cannery structures would be acceptable.

Entrances to buildings along Bayview street or the waterfront should be with a consideration of views of the water, both for people in the new buildings and for people on the street. It is desirable to maintain unobstructed views of the water from all north-south streets.

Entrances to buildings along Bayview street have traditionally been constructed of wood. Wooden boardwalks or porches with wooden handrails are therefore recommended.

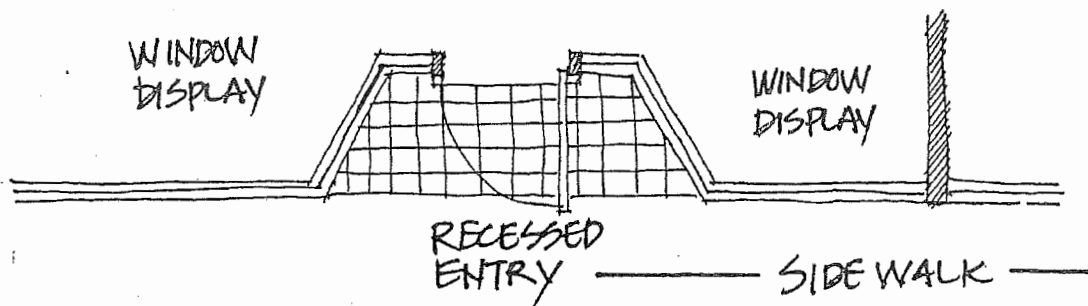
Traditionally, Bayview Street had a row of buildings facing a waterfront boardwalk. The buildings have long since been destroyed by fire. The ditch inside the dyke has been replaced by a buried culvert and a 15' easement inside the property line. Buildings cannot be built over these easements, however a boardwalk is recommended as a link between the buildings and the reconstructed Bayview Street.



2. The continuity of the commercial frontage should be maintained by having a minimum street setback, consistent with older commercial streets.

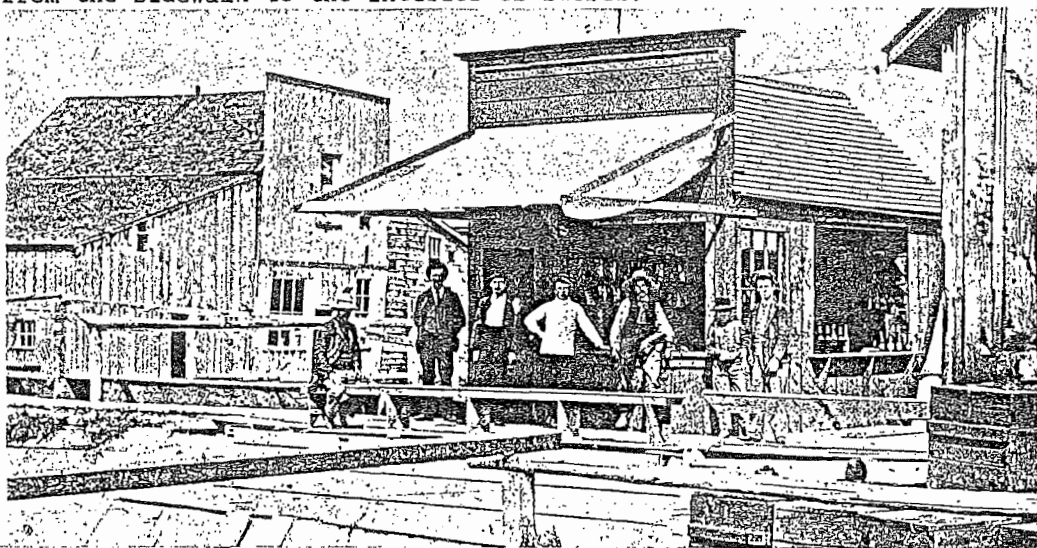
The intent of this guideline is to make it easier and more interesting for shoppers to move from store to store. The natural flow of pedestrians along the public sidewalk makes this an appropriate location for buildings. Extensive landscaping, parking, loading or storage should not be located next to sidewalks on commercial properties. (See the Design Concept for recommended commercial frontages.)

Shops should have recessed entries, as was common in older buildings in Steveston. Recessed entries increase the amount of window display area, add to the interest of the facade, and allow shop doors to open outward, safely without obstructing the sidewalk.



- 2.1 Store fronts should have windows facing commercial streets wherever possible, for the interest of passers-by.

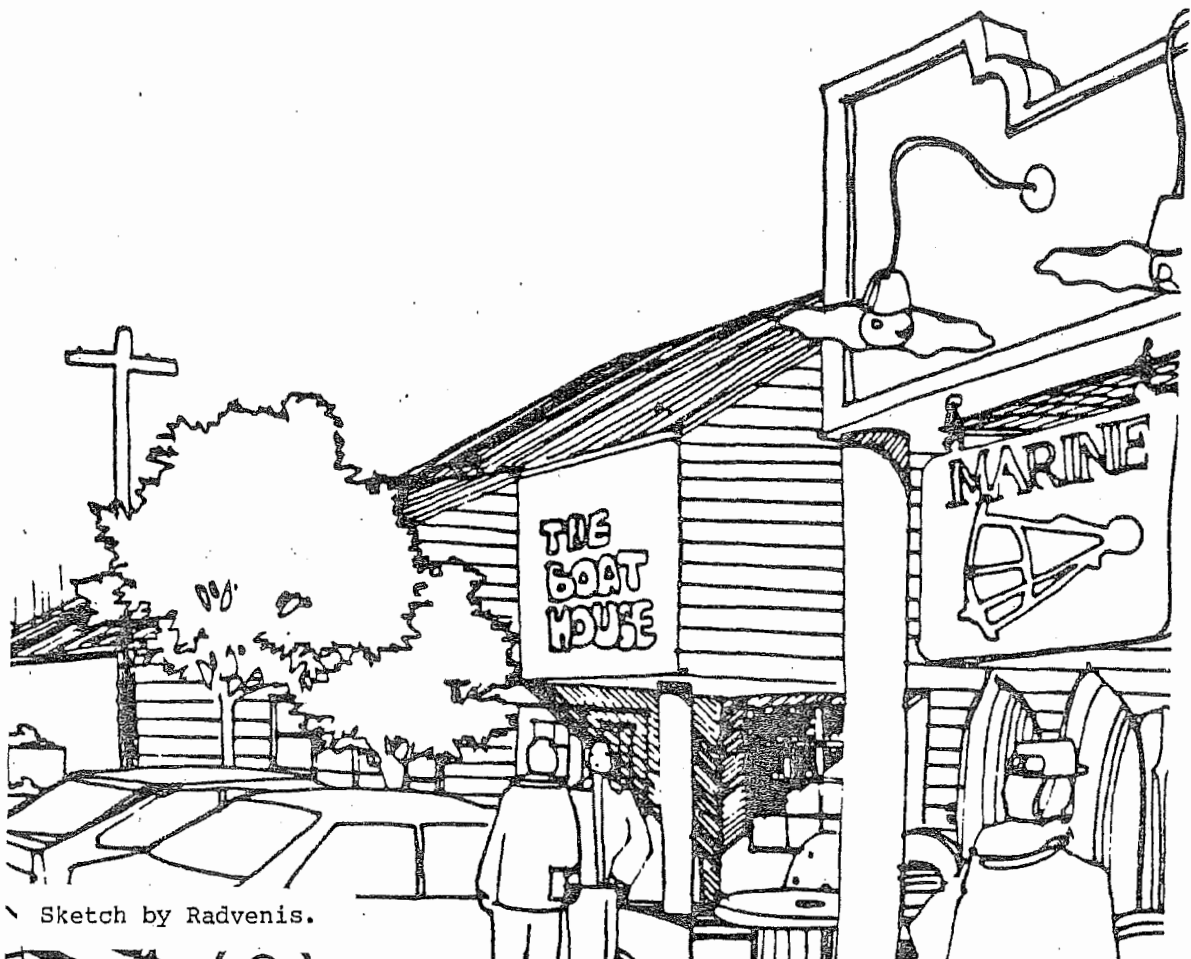
Because this is a shopping area and the guidelines encourage continuity of commercial frontage, it is important that all shops present an interesting facade to the street. Windows allow merchants to create displays which communicate the nature of the business to potential customers passing by on the sidewalk. Windows make a visual transition from the sidewalk to the interior of stores.



A dyke-front store in Steveston c. 1900 had windows and open-air counter to display and sell "groceries".

2.2 Canopies or awnings should be provided, to protect people on the sidewalks from rain and snow.

Given our climate, sidewalks should be sheltered as much as possible. The traditional method in Steveston was canopies supported on posts, or protecting canvass awnings.

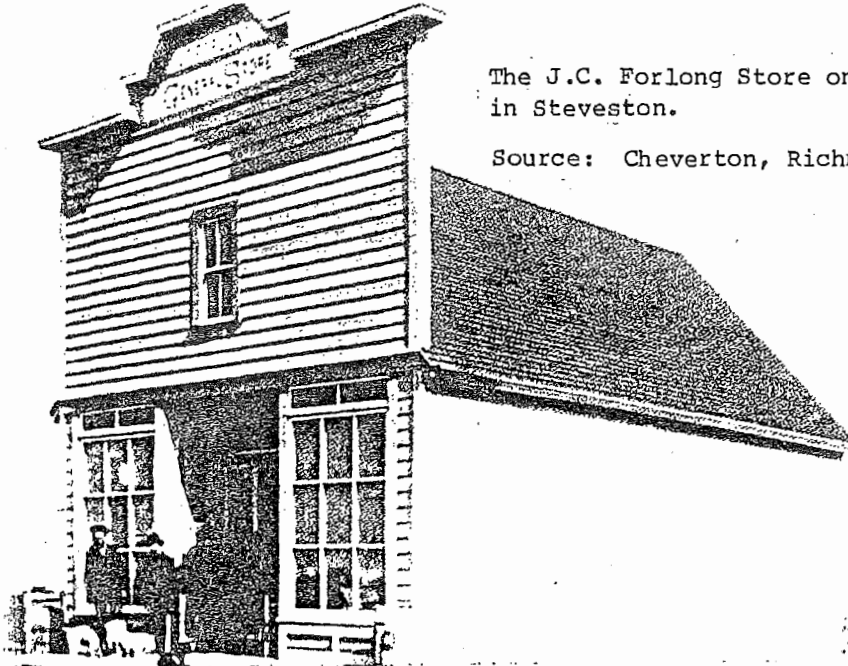


Sketch by Radvenis.

Canopies projecting over public sidewalks are a special case. Canopies supported on posts should have the posts located on private property. Canopies, or parts of buildings which project over public property must conform to all codes and the owner must sign an Easement and Indemnity Agreement with the Municipality. An illustration of canopy requirements is provided in Appendix 3. New canopies may be eligible for grants from the Facade Improvement Grant Program (Appendix L.).

3. New buildings should not exceed three storeys in height.

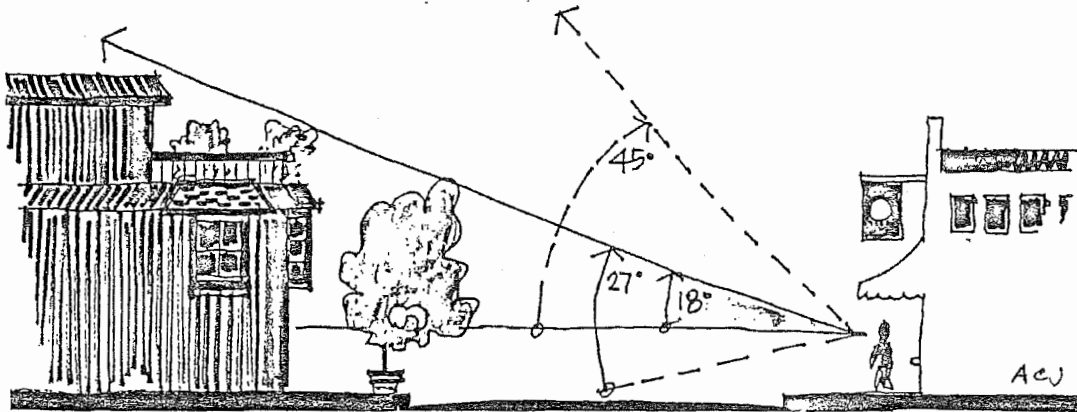
Buildings in Steveston have traditionally been one to three storeys in height. This situation was partly the result of wood frame building technology of the day, but coincidentally resulted in a pleasing relationship between buildings and the street.



The J.C. Forlong Store on Second Avenue in Steveston.

Source: Cheverton, Richmond Archives.

This small scale building in relation to a typical street is sometimes referred to as "human scale".



Human eyes can normally perceive a vertical field of vision of about 27° , or 18° above the horizon. This means that a person will feel most comfortable viewing a two storey building across a typical street. Some image of the whole remains up to 45° from the horizon. A building is considered to be of a human scale if it can be comfortably viewed at a glance. Therefore, new buildings should have a setback such that there is a height: distance ratio, taken from the opposite side of a street or park, of between 1:1 and 1:2.

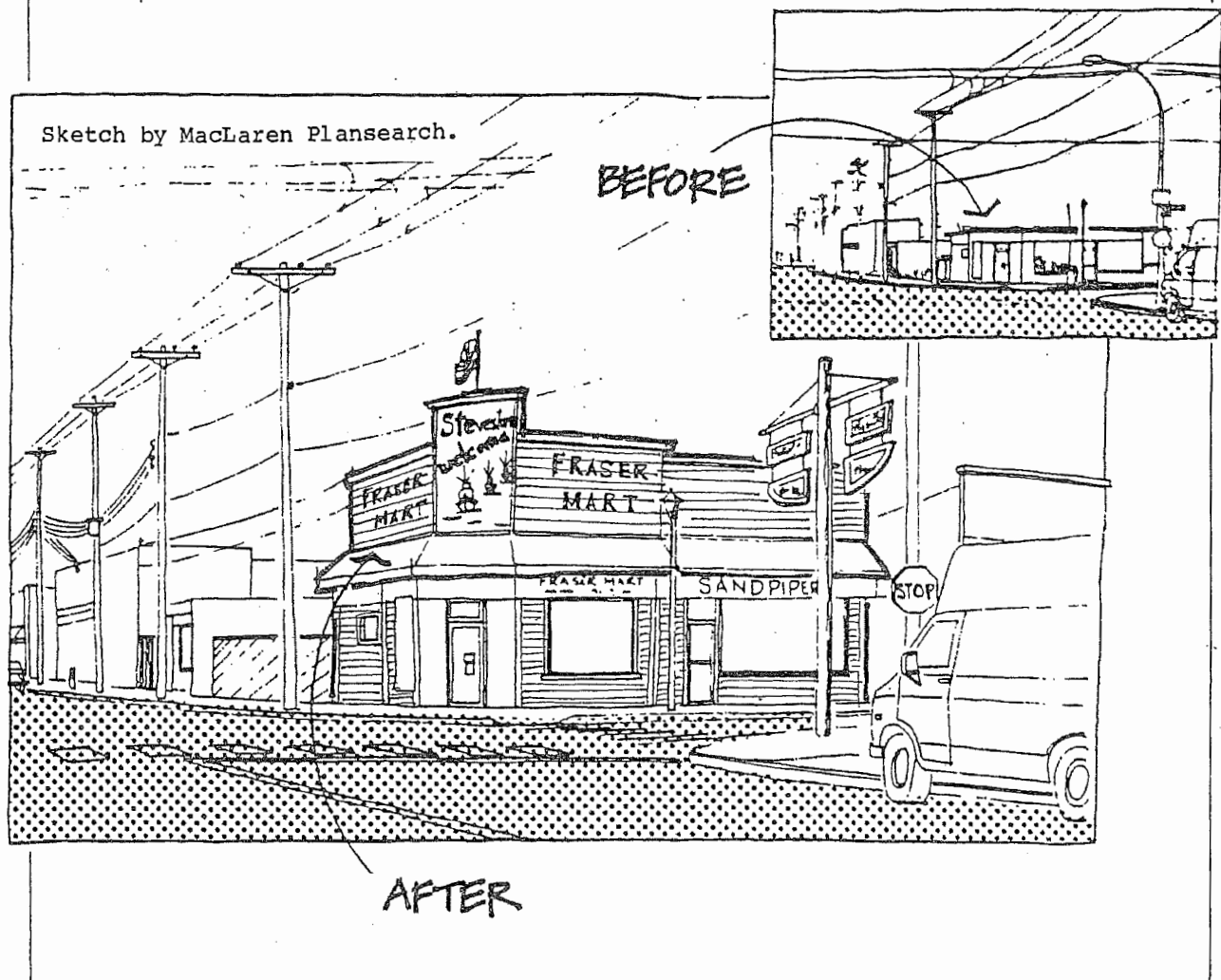
Conversely, in some cases spacing between buildings is too great, and there is no feeling of enclosure on the street. This is the opposite extreme of the "boxed in" feeling, and just as undesirable.

4. Exterior finish of buildings facing commercial streets should utilize traditional materials, or materials which are compatible with existing natural finishes.

Older buildings in the Steveston Commercial District were finished with wood. The newer buildings are generally stucco or, more recently painted concrete block. Only a few buildings survived the 1918 fire, one being the brick "Hepworth block". Other buildings of the period generally had painted shiplap or wooden shingle siding.

Finish materials for new or renovated buildings should be compatible with traditional materials, for example, wood or brick. The hand-made character of finish and decoration could be carried on with careful detailing, and some modern and machine-made materials can be successfully incorporated. Finish materials, windows, doors, hand rails and decorative elements can take up the form, character or rhythm of nearby older buildings without imitating them.

See Appendix 2 for examples of building finish and details.



5. Parking should be located at the rear of buildings, or in communal lots.

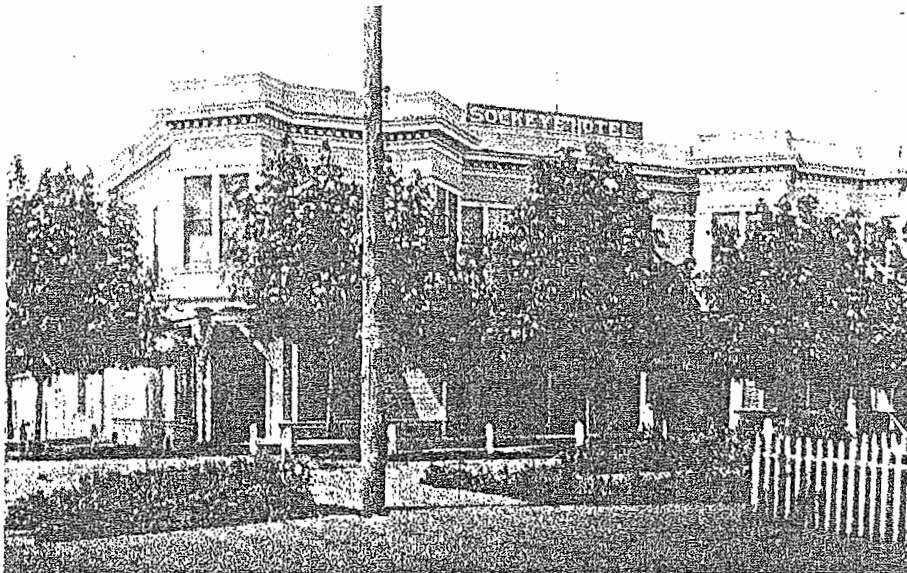
This guideline dovetails with other guidelines aimed at maintaining the vitality of the commercial street, while at the same time providing adequate customer and employee parking. There are three aspects to municipal parking policy for Steveston:

1. spaces should be provided on the street immediately in front of shops for short term customer parking, including loading zones for fishermen.
2. communal parking and loading should be provided off of lanes, at the rear of commercial buildings and on municipal parking lot(s) for long term parking, employee parking, and fishermen parking
3. parking lots should not be located in front of shops because they would inhibit pedestrian access.

A proposed parking layout for Steveston is shown on Map 2.

6. Signs for identification of businesses and activities should be in keeping with the historic nature of the town.

Signs in the early 1900's were usually painted on wood, either directly on the siding or on boards fastened to the fascia or suspended under a canopy. Occasionally a larger establishment, such as the Sockeye Hotel, would display a roof sign.



Roof sign on the Sockeye Hotel (now the Steveston Hotel).
Source: Vancouver Public Library Collection.

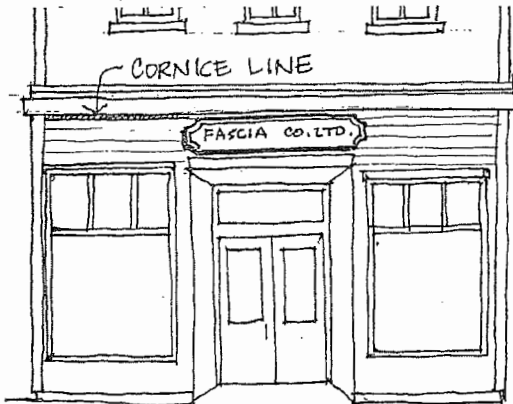
Signs should be made to be viewed mainly from the sidewalk. In some cases signs may also be designed to be viewed from the water, or from slow moving vehicles.

The following types of signs are recommended:



MARQUEE SIGNS

Are easily seen by persons walking on the sidewalk, especially under canopies. It is expected that these will replace projecting signs as new canopies are built.



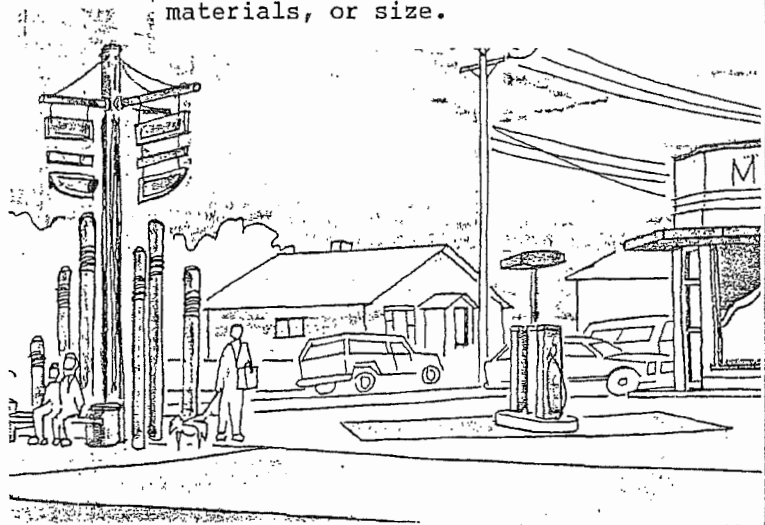
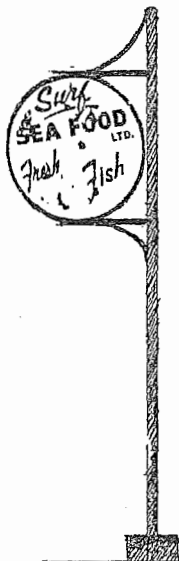
FASCIA SIGNS

Are traditional signs in Steveston and are usually made of painted wood or metal. External illumination by spot light is most appropriate.

Fascia signs should be located so as not to obscure building details. For example, fascia signs should be located below the cornice, as shown in the sketch.

FREESTANDING SIGNS

These may need to be specially designed for Steveston since modern "standard" signs are generally not appropriate in form, materials, or size.





CANOPY SIGNS

These are also an effective replacement for the old projecting signs. They may be incorporated into a balcony or porch style sidewalk covering.



PROJECTING SIGNS

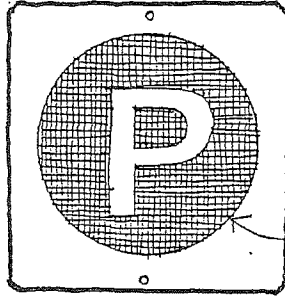
Are permitted on private property only. New signs will probably not be permitted to project over public sidewalks or lanes. Some existing projecting signs may remain, as long as they are in safe condition.



ROOF SIGNS

These signs are only recommended for industrial uses or hotels, as was the custom in the past in Steveston.

Source:
Richmond Archives



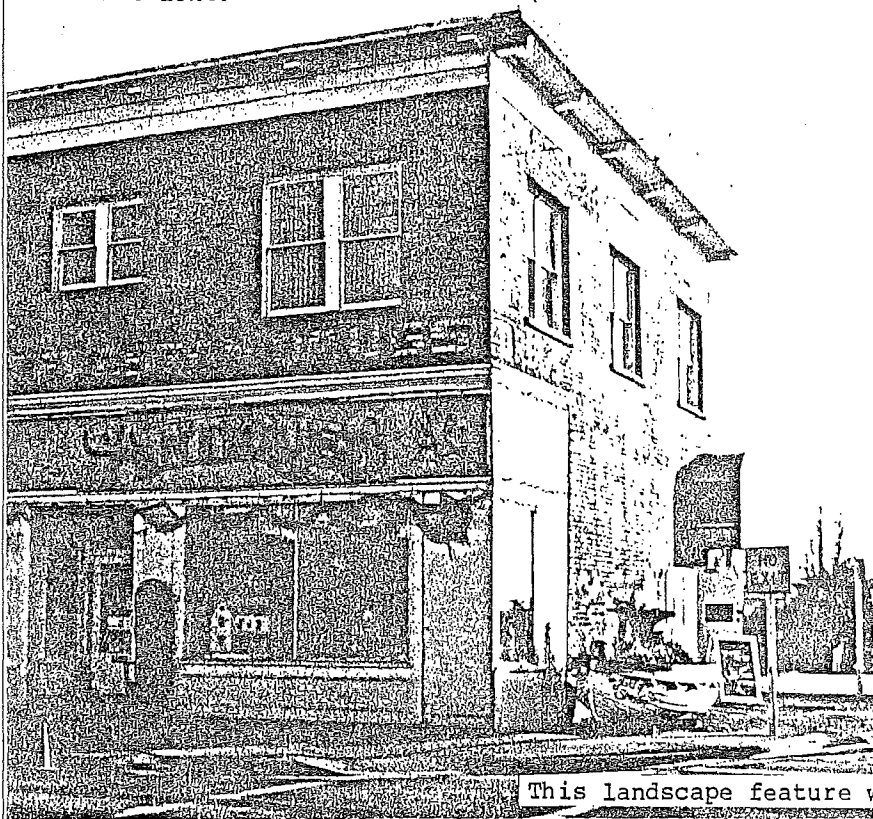
PARKING OR INFORMATION SIGNS

These will be permitted, especially to designate communal areas and parking lots shown on the plan.

Before deciding on types and details of signs, applicants should consult the Richmond Sign By-law. For example, certain signs will not be permitted. These include: readograph, third party advertising and other signs specifically prohibited by the Sign By-Law.

7. Development and redevelopment should include new pedestrian amenities, landscaping, site improvements and screening, where appropriate. This criterion refers to improvements on private property, since the Municipality will be responsible for improving street furniture as part of the Downtown Revitalization Program.

Although many buildings will have virtually no setback from the street, there may still be room for improvements at the rear of buildings, in parking areas, in window boxes, in entry recesses or in small front setbacks.



This landscape feature was a private initiative.

New pedestrian amenities could include benches, cafe tables and chairs, handrails, fountains, sculpture, porches and bicycle racks.

Landscaping could include wooden window boxes, wooden or clay pots, or barrels with flowers, hanging flower baskets or even old rowboats filled with annuals. Developers of every new building or renovation are encouraged to include some plants as described here. Perennial flowers generally require little maintenance. Annual flowers can be changed with the season. Regular maintenance of annuals is recommended, and one advantage of this small-scale potted landscaping is that the owners can remove them when their usefulness is expended. Examples of annuals are: pansies, daisies, nasturtiums or kale. A list of Perennials is provided in Appendix 4.

No large trees or shrubs should be planted on the street frontage for two reasons. Firstly there is not enough room for large growing plants. Secondly, for approximately the last 60 years, there have been very few trees in the Steveston Downtown area, and people have accepted this as a tradition.

Extensive landscaping, tree planting and screening are encouraged at the rear of buildings. The Screening By-law requires screening of parking lots from the public street. Curbs, bumpers or bollards should be provided to separate parked cars from pedestrians.

SOURCES

1. The Corporation of the Township of Richmond, Steveston Area Plan, and amendments.
2. Ross, Leslie, Richmond, Child of the Fraser, 1979.
3. Sakamoto, B., Sproule, J.K., and Hancock, J. of J.K. Sproule Architecture/Urban Design, Steveston: A Fishing Village, 1983.
4. Baldwin, P., MacLaren Plansearch Ltd., Downtown Revitalization Plan for Steveston, 1986.
5. Freschette R.1 Steveston '83 Landscape Concept, Report for the Richmond Planning Department, unpublished, 1983.

ACKNOWLEDGEMENTS

Photos: Richmond, Child of the Fraser by Leslie Ross

Richmond Archives

Vancouver Public Library

Mr. Ted Clark

Mrs. Jas Cheverton

Appendix 1

FACADE IMPROVEMENT GRANTS

The Provincial Government has designated Steveston as a Downtown Revitalization Area, which entitles shop owners to "Private Premises Facade Improvement Grants". The grants are administered by the Municipality as part of the approved design concept for Steveston. Grants are to be distributed to owners or applicants who have improved the facades of their buildings. Improvements must be to exterior walls that face public streets, land, or parking areas; or private land or parking areas that the public has access to. The grants are given after improvements have been completed and certain criteria met.

Calculation of the Grant

The grant amount is 20% of the cost of the private ground floor facade improvements up to a maximum of \$200 per metre. If a building has frontage on a side street or other public passageway, or parking area, up to 10% of the cost or \$100 per metre can be added to the grant amount.

Grant Administration

The grant is administered through the municipal building inspection process and the grant application is the actual municipal building permit. Since some types of improvements, such as cleaning and repainting, do not normally require a building permit, the Municipal Council must have indicated its agreement to have staff undertake the administration of building facade grants at municipal cost. Building permit fees are not charged for improvements which would not normally require a permit, although the owner or applicant must submit a letter stating plans and costs, and use the permit as the grant application form. The owner or an applicant (if the owner has agreed in writing to the works) presents a description or drawings of the works, as required, to the Building Inspector, who then notes the anticipated cost of the improvements on the permit. The Building Inspector also certifies on the permit that the qualifying requirements have been met, namely:

- . a Resolution of Council to permit grant administration through the building inspection process; and
- . written confirmation from the Municipal Clerk that the municipality has approved either a design or promotion and marketing concept for the downtown area.

The Building Inspector ensures that the planned works are for facade beautification and improvement, that they conform to other Municipal by-laws and are being made to existing properties. Changes to building interiors other than for window displays visible from the outside, or normal maintenance, do not qualify. Facade improvements can, of course, be carried out while other more extensive work is being done and the Building Inspector must exercise judgement as to the proportion of the work which is part of the Facade Program.

The Building Inspector also confirms the calculation of building frontage and notes this on the permit and sends a copy of the annotated, issued permit to the Ministry of Municipal Affairs.

If there are questions about a grant application, the Ministry will contact the Building Inspector within 21 days of receiving the permit copy. Otherwise it should be assumed that a grant will be payable on completion of the works.

Final Approval

Once the facade improvements have been completed and passed final inspection, the actual costs of the improvements and the Building Inspector's certification of completion should be noted on a copy of the building permit and forwarded to the Ministry of Municipal Affairs. The Building Inspector is responsible for determining what the final costs are and should be guided by the invoices, time sheets, etc., which the applicant provides. If the applicant has done some of the work, the inspector estimates what his labour would have cost and includes this in the total costs.

If improvement works have been of the type that do not normally require a building permit or Inspections, the owner or applicant has the responsibility of informing the inspector when the improvements have been completed. The Inspector then confirms that the improvements have been made and, as above, confirms their cost.

The final permit form sent to the Ministry should be a copy of the original so that the applicant's name, address and permit number are consistent on all copies.

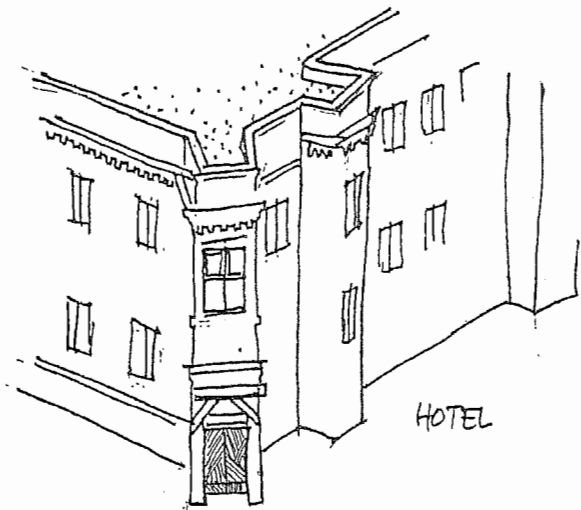
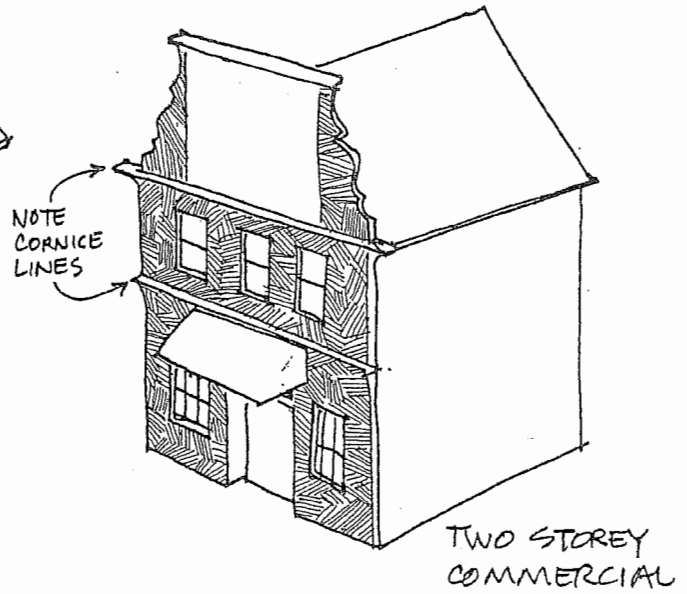
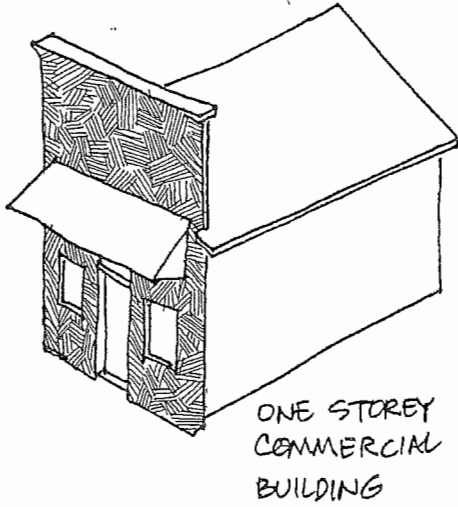
The Municipality, or an organization that it has approved for this purpose, may, if owners give their consent, undertake central contract administration for private facade improvements. This does not, however, affect the fact that grants are calculated on an individual basis.*

* This information is taken from Downtown Revitalization, a Guide, Ministry of Municipal Affairs, Province of B.C. and a Guide to the use of Development Permits in Downtown Revitalization, prepared for the Ministry of Municipal Affairs, B.C. (draft) 1987.

APPENDIX 2,

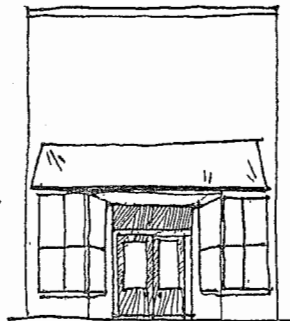
EXAMPLES OF TYPICAL BUILDING FORM AND TRADITIONAL FACADE DETAILS.

1. GENERAL FORM

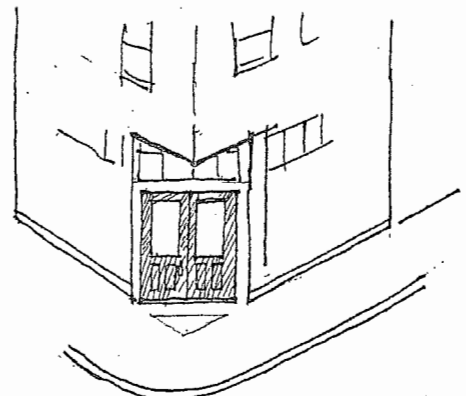


2. ENTRANCES

EARLY ENTRIES WERE LOCATED IN THE CENTRE OF THE FACADE

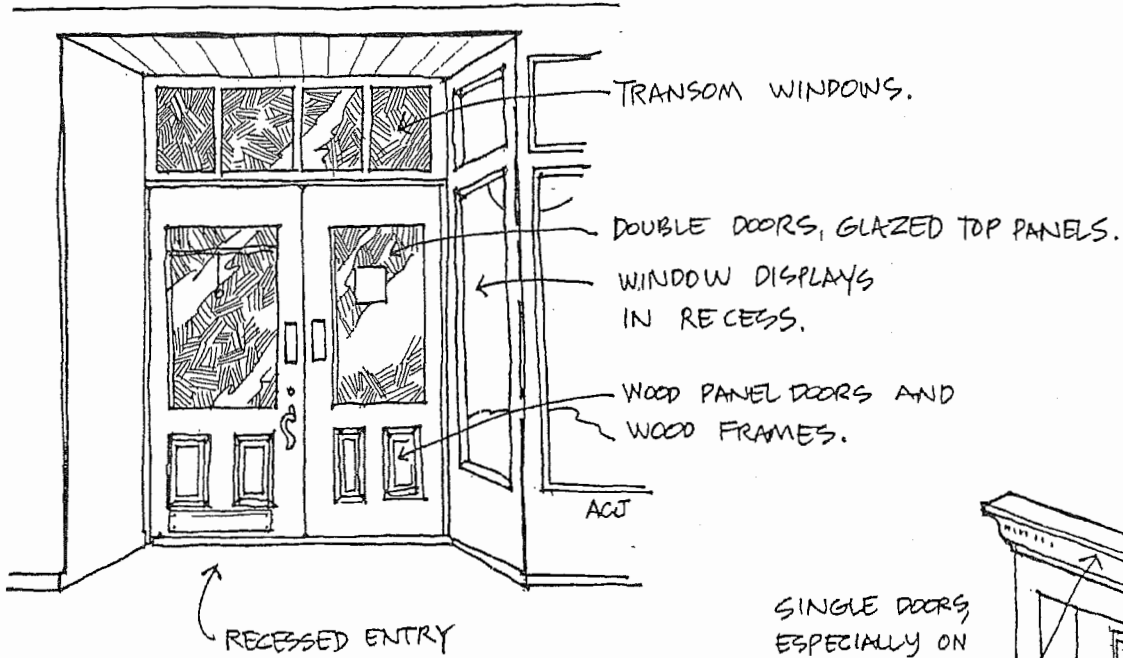


OR...
LATER ON, SOME ENTRIES WERE LOCATED ON THE STREET CORNERS...

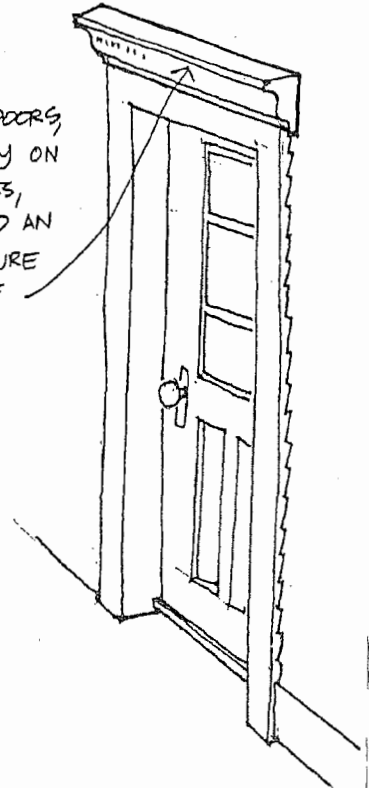


APPENDIX 2. CONT'D. EXAMPLES

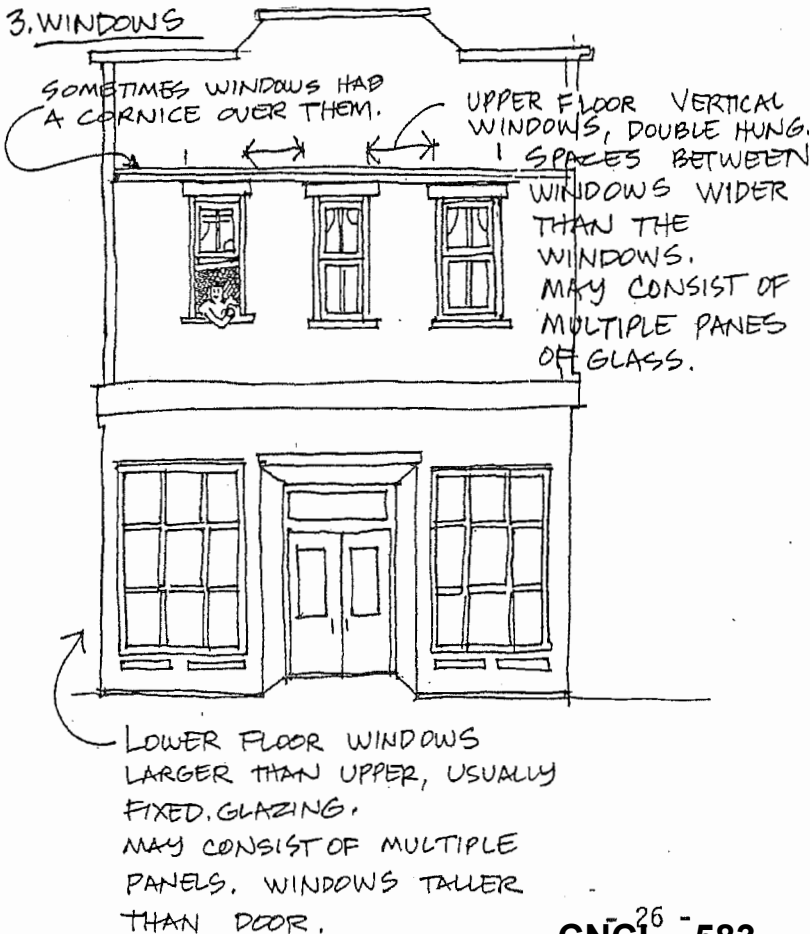
ENTRANCE DOORWAY DETAILS



SINGLE DOORS, ESPECIALLY ON BALCONIES, OFTEN HAD AN ENTABLATURE OR SHELF OVER.



3. WINDOWS

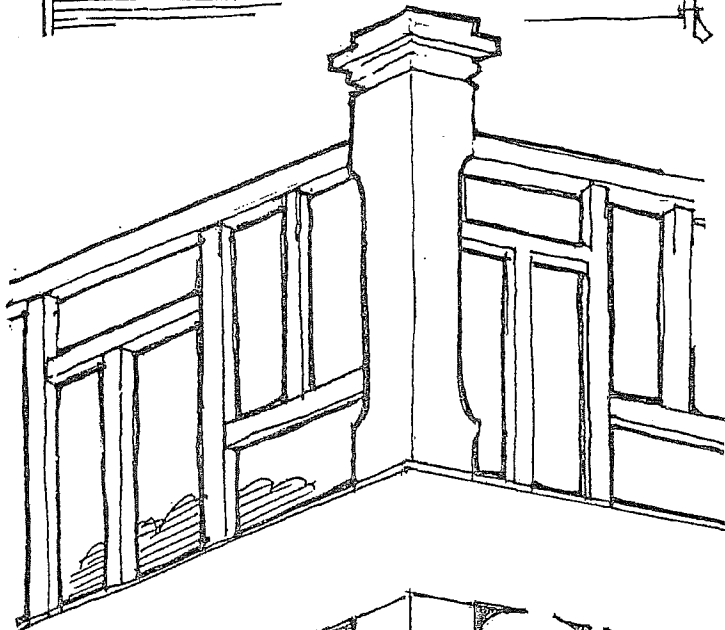
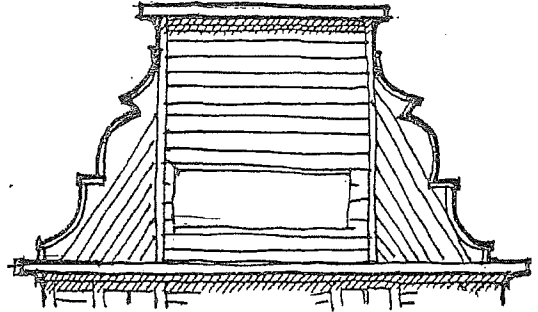
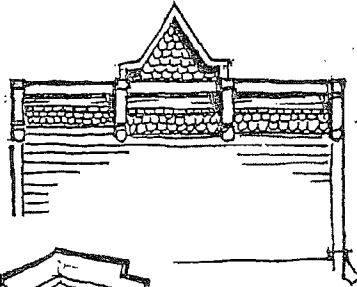
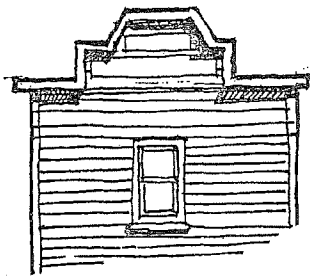


APPENDIX 2. CONT'D

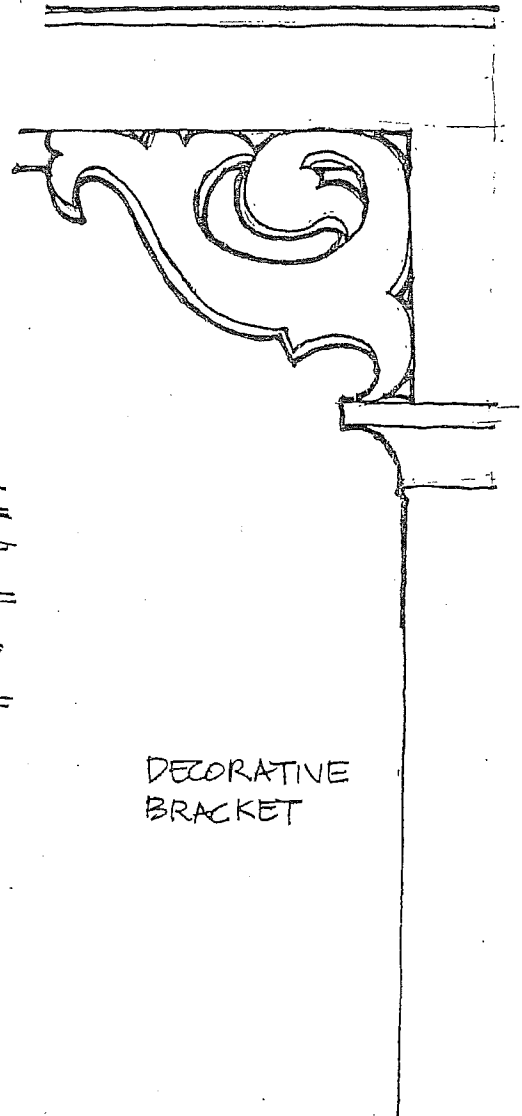
3. DETAILS

SIDING, TRIM, CORNICES, POSTS AND BRACKETS WERE ALMOST EXCLUSIVELY MADE OF WOOD.

SOME EXAMPLES OF FALSE FRONTS OR PARAPETS:

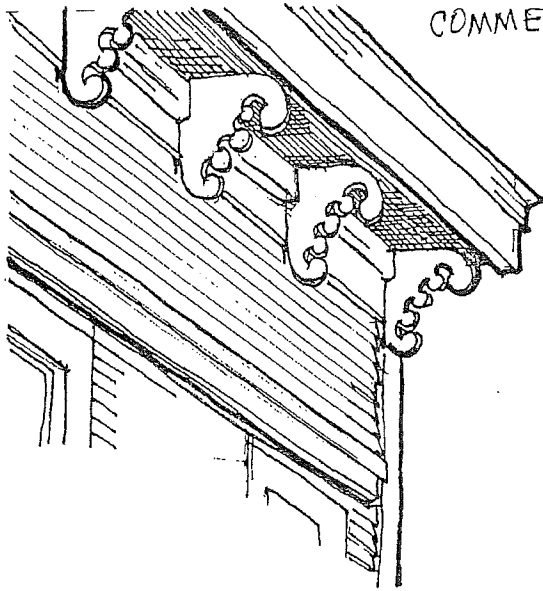


CORNER POST
BRACKET AND
HAND RAIL
DETAILS

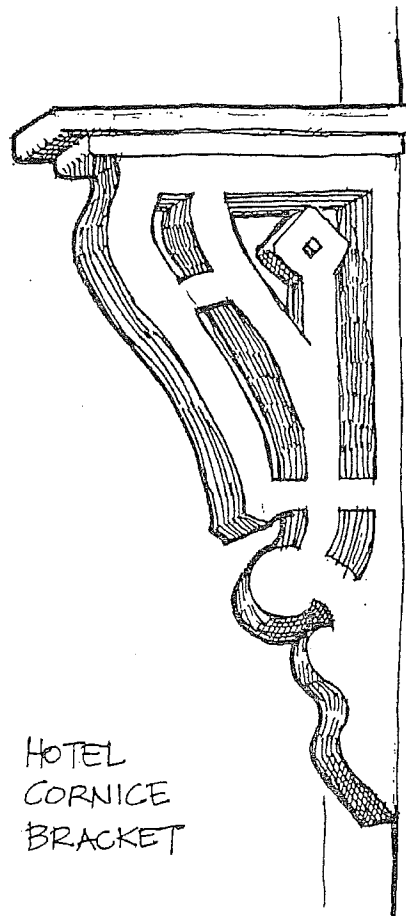


DECORATIVE
BRACKET

3. DETAILS
CORNICES



COMMERCIAL BUILDING

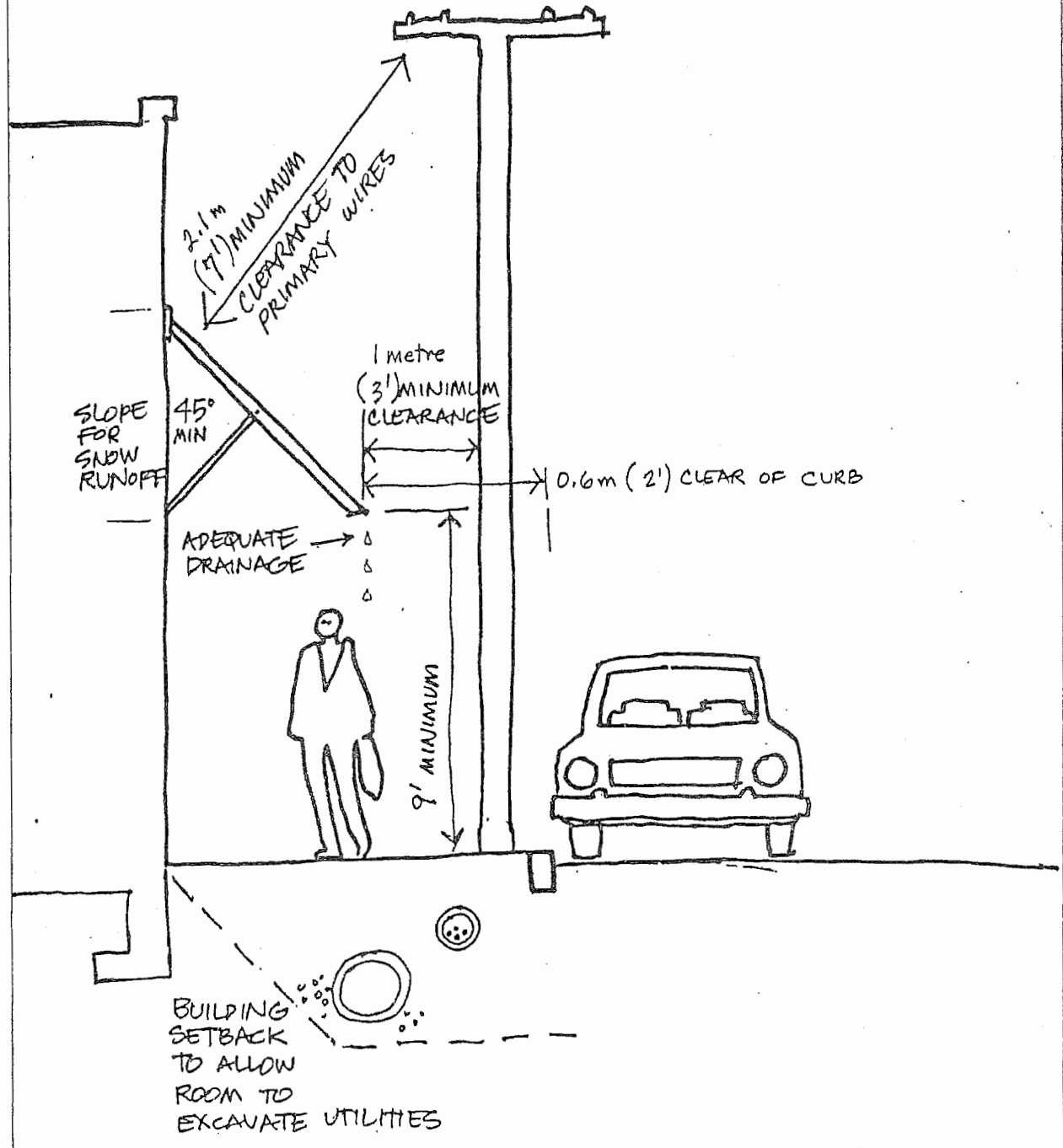


HOTEL
CORNICE
BRACKET

CRITERIA FOR CANOPIES

± 7'

SIDEWALK SHOULD
HAVE ADEQUATE WIDTH
FOR PEDESTRIANS



PERENNIAL FLOWERS

APPENDIX 4

SCIENTIFIC & COMMON NAME	HEIGHT	EVRN.	NATIVE	SCENT	FAVORED	BANK	DYKE	STREET	FEATURE	PLANTED	SCREEN	OTHER PARTICULARITIES										
												PLANTING TYPE	SCREEN	PLANTED	FEATURE	STREET	DYKE	BANK	FAVORED	SCENT	FAVORED	BANK
Aquilegia formosa - Western Columbine	3'																	Various Colors				
Alyssum saxatile - Basket of Gold	1'																	Yellow Flowers				
Arenaria alpina - Rockcress	6"																	White Flowers				
Arenaria sp. - Sandwort	3"																	White Flwrs, Shade Lover				
Armeria maritima - Common Thrift	10"																	Pink Flowers				
Artemisia stelleriana - Dusty Miller	30"																	Yellows Flwrs, White Lv.				
Asclepias tuberosa - Butterfly Weed	3'																	Attracts Butterflies				
Cerastium tomentosum - Snow-In-Summer	6"																	Silver Gray Leaves				
Chrysanthemum carinatum - Chrysanthemum	2'																	Various Colors				
Chrysanthemum frutescens - Marguerite	2'																	White & Pink Flowers				
Coreopsis grandiflora - Coreopsis	2'																	Attracts Birds				
Cortaderia selloana - Pampas Grass	6'																	Yellowish Heads				
Dianthus sp. - Pinks	2'																	Various Colors				
Doronicum cordatum - Leopard's Bane	2'																	Shade Lover				
Echinops exaltatus - Globe Thistle	4'																	Blue Heads				
Erigeron glaucus - Sea Side Daisy	10"																	Lavender Flowers				
Erigeron speciosus - Fleabane	2'																	Lavender Flowers				
Eschscholzia californica - California Poppy	18"																	Orange Flowers				
Gaillardia grandiflora - Blanket Flower	4'																	Red, Orange Flowers				
Gypsophila paniculata - Baby's Breath	3'																	Various Colors				
Helianthemum nummularium - Sun Rose	6"																	Various Colors				
Hemerocallis lilloasphodelus - Day Lily	3'																	Yellow				
Heuchera micrantha - Coral-Bells	2'																	Greenish White Flwrs				
Iberis sempervirens - Evergreen Candytuft	1'																	Pure White Flwrs.				
Iris douglasiana - Douglas Iris	2'																	Blue Flowers				
Lavandula angustifolia - English Lavender	3'																	Lavender Flowers				
Lilium columbianum - Columbia Lily	3'																	Orange Flowers				
Limonium latifolium - Sea Lavender	2'																	Lavender Flowers				
Lythrum salicaria - Loosestrife	5'																	Requires Moisture				
Santolina chamaecyparissus - Lavender Cotton	2'																	Yellow Heads				
Veronica sp. - Speedwell	30"																	Various Colors				
Yucca whipplei - Our Lord's Candle	3'																	White Flowers				

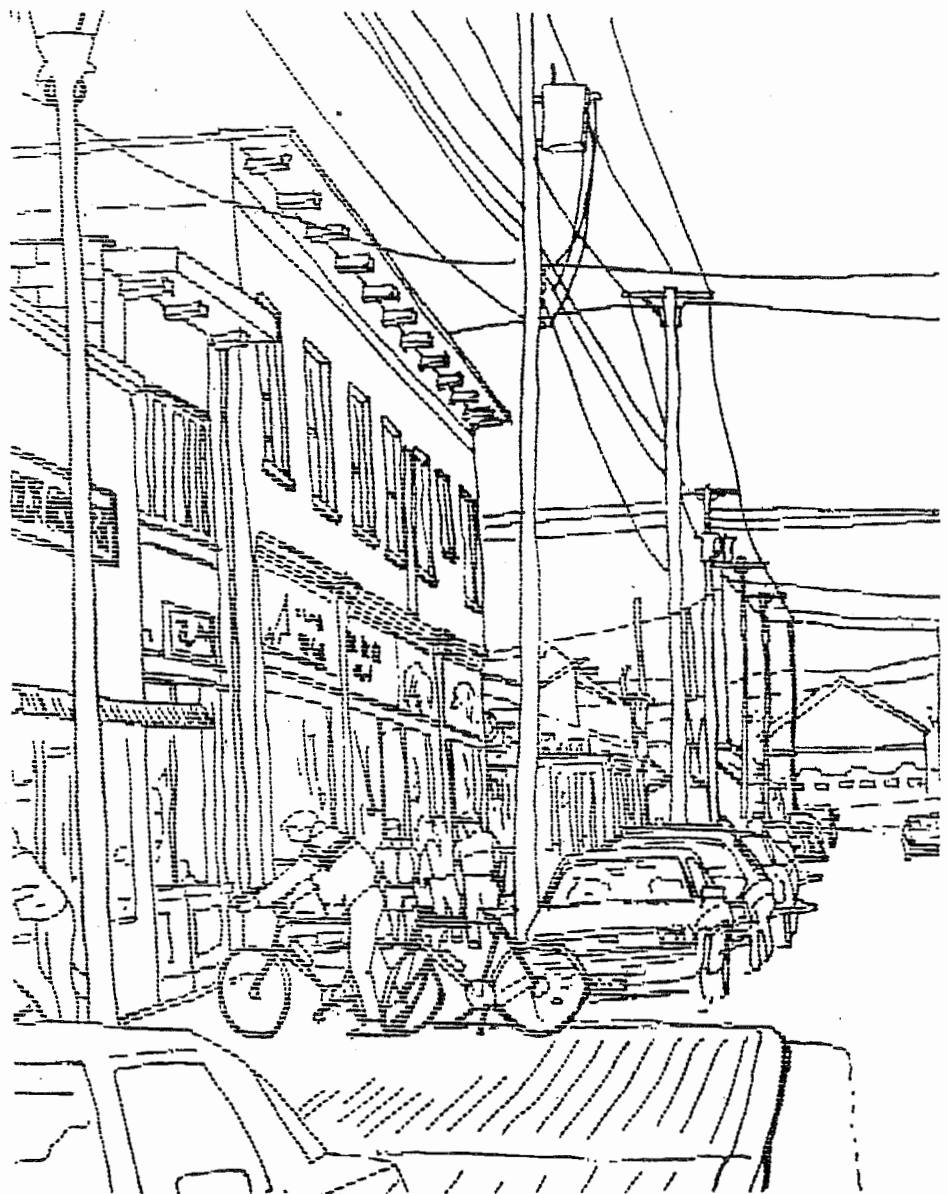
POTENTIAL HERITAGE BUILDINGS

MAP
KEY
NO.

1. 12111 3rd Avenue Steveston Hotel - Eastern Portion
2. 3420 Moncton Street - Steveston Danish Bakery
3. 3480 Moncton Street - Bookstore/retail, pre - 1925, 3 buildings.
4. 3580 Moncton Street. "Hepworth Block", pre 1918
5. 3680 Moncton Street. Marine Grocery, pre 1920e
6. 3700 Moncton Street-Redden Net Co., pre 1925e
7. 12160 First Ave-"Steva Theatre" Eastern Portion
8. 12251 Number One Rd-"Eashope", South-east building
9. 12311 Number One Road-Steveston Furniture
10. 3951 Moncton Street-Store
11. 3911 Moncton Street-Hiro's Grocery
12. 3891 Moncton St.-Store/dwelling, pre 1915e
13. 3871 Moncton St.-Store
14. 3831 Moncton St. Store
15. 3771, 3791, 3811 Moncton St.-Museum-Post Office, 1907-8. DESIGNATED.
16. 12011 Third Ave.-Municipal Building, 1925-32e DESIGNATED.
17. 3731 Chatham St.-Steveston Bicycle "Church", 1894.
18. 12020 First Avenue - former bakery - west portion

Steveston Downtown Revitalization

Corporation of the
Township of Richmond



Facade Guidelines

CNCL-589

**STEVESTON REVITALIZATION
AREA FACADE GUIDELINES**

Prepared by
Bud Sakamoto, Steveston Coordinator
in consultation with Patricia Baldwin
March 1989

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1. INTRODUCTION

Steveston was born in 1889 when William Herbert Steves laid out a section of his farm into town lots. Immediately development began with the following decade, the 1890's, turning Steveston into a "boomtown" with fishermen flocking in on weekends to make it not only a boisterous place, but also one of the most important cannery centres on the entire coast. From the beginning, Steveston was changing with fires playing a major role by ravaging the town. When wooden frame buildings which stood side by side caught fire, many buildings were destroyed before the fire was put out. Buildings were reconstructed with similar character and the town continued to function as a centre for the fishing industry.

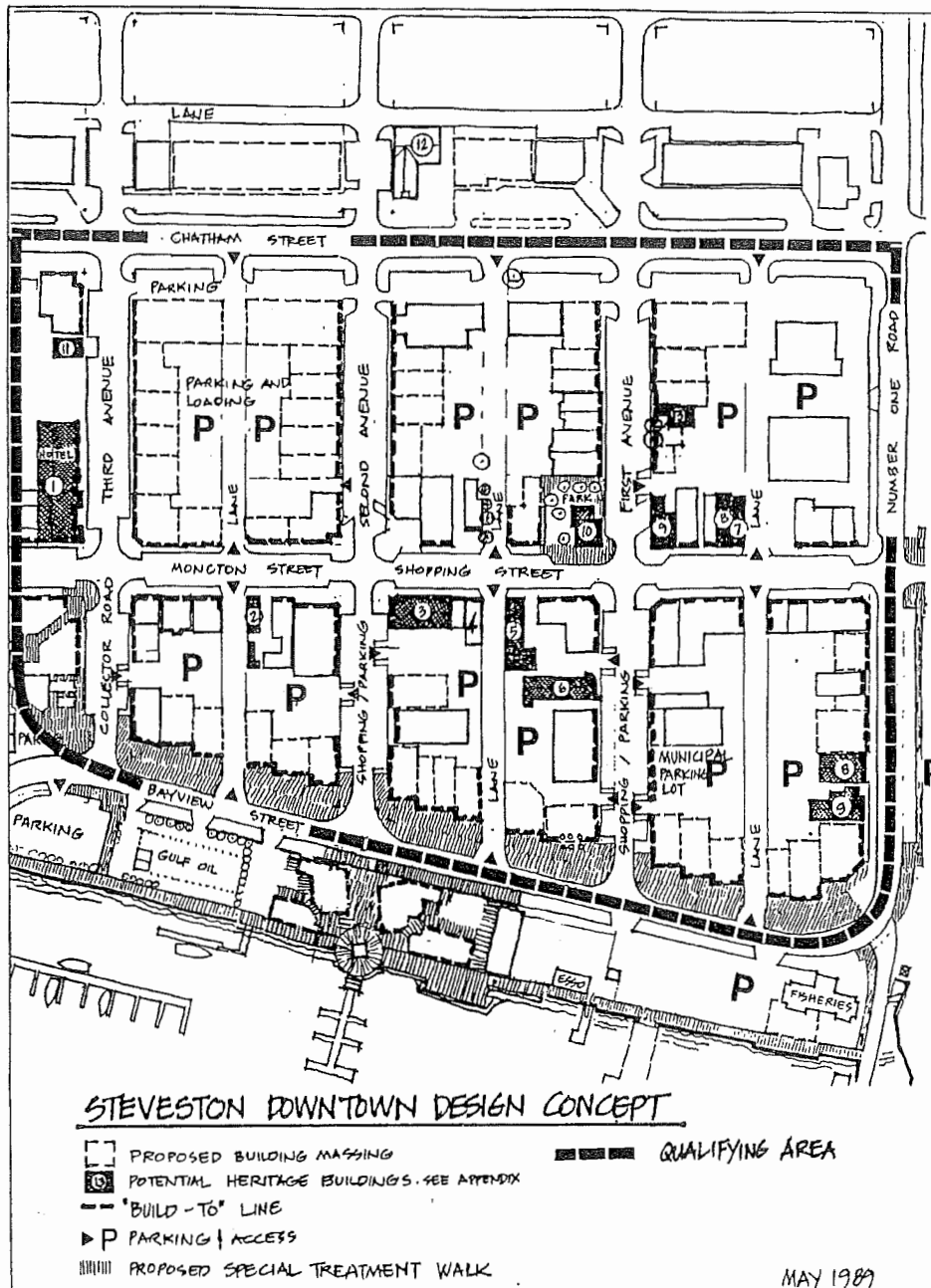
During the 1950's and 1960's, zoning bylaws encouraged demolition of older buildings and the construction of characterless concrete block structures. Steveston was then still an isolated area and the fishing industry dominated the area.

Today, there is renewed interest in Steveston. The importance of the operating fishing industry still remains, but the encroaching urban development is placing a new focus on the area. The Corporation of the Township of Richmond, through the Steveston Downtown Revitalization Committee, is committed to the fishing industry and the development of the area as a local and fishing service centre. Improvements to the street and sidewalks have been carried out as part of the Downtown Revitalization Program with an image of a working fishing town.

In the revitalization, an important component is the improvements to the store fronts. The purpose of the Facade Improvement Guidelines is to provide design guides and standards for maintaining continuity in the improvements being carried out. The Guidelines are a simplistic interpretation of Steveston's architectural past to provide a design theme for the area's improvements. The hope is for submissions of appropriate and imaginative design schemes which are beyond the scope of the Guidelines. These guidelines do not apply to new buildings. For new construction, "Design Guidelines for the Steveston Downtown Revitalization Area" should be obtained.

2. STORE FRONT FACADE GRANTS

Grants are available to both tenants and property owners who improve the facades of existing buildings. To qualify, the building must be in the Steveston Downtown Revitalization Area (see attached map) which is bounded by Chatham Street, No. 1 Road, Bayview Street and Third Avenue, including the west side of Third Avenue.



STORE FRONT FACADE GRANTS (continued)

Grants are available for improvements to exterior walls that face a public street, land or parking area, or private land or parking area that has public access. The grants are paid after improvements are completed and the design criteria of the Guidelines have been met. The grant policy for individual shops are as follows:

- FRONT A 20% grant or \$200 per metre whichever is the least.
- SIDE A 10% grant or \$100 per metre whichever is the least. It is, however, at the discretion of the Municipality to recommend a special grant of 20%, to a maximum of \$200 per metre, be awarded for corner shops with a front facing a front street and a side facing a pedestrian oriented shopping street, containing a full advertising display window. The 10% grant applies to a pedestrian oriented side street that does not have a display window.
- REAR A 10% grant or \$100 per metre whichever is the least. It is noted that the rear may be parking oriented with rear entrances from the parking area into the shops. Special grants may be considered, however, special application/documentation must be forthcoming prior to approval in individual claims.

3. STORE FRONT FACADE GUIDELINES

3.1. Designated Heritage Buildings

Guideline: Restore designated heritage buildings.

Restoration applies only to officially designated buildings and to the improvements to the exterior of the building to as closely as possible to details and quality of the original constructed building. Only two designated heritage buildings exist in Steveston (see previous map).

3.2. Potential Heritage Buildings

Guideline: Improve potential heritage buildings to minimize change and to retain the heritage character.

The original buildings of the early "boomtown" days have long been lost. The heritage buildings that remain date back to the early part of this century. These buildings are considered potential heritage buildings.

The appearance of the potential heritage buildings should be returned to the time of early construction by removing later added exterior material, replacing missing details or repairing deteriorated materials. Adaption of construction and the use of available similar material may be considered provided the appearance is not drastically altered. The intention is the maintenance of the character of the building and not a faithful restoration as reconstruction.

Steveston is a historic town. The owners and tenants of potential heritage buildings have special opportunities and obligations.

3.3. Improvement of Infill Building

Guideline: Develop an identifiable store front for all businesses by reflecting a special character to indicate the type of business or merchandise being sold.

Most of infill buildings have been built during the 1950's and 1960's. They are concrete block structures and, in most instances, lack an identifiable feature. The store front provides the first impression of the business, identifies the premise and indicates the type of business. It provides a strategic draw for customers and an improvement to the business. It is legitimate subliminal advertising.

STORE FRONT FACADE GUIDELINES (continued)

3.4. Sympathetic Design Overview

Guideline: Improvements to store fronts should be in context of the streetscape. Relationships such as building height, store front parapet height, and canopy and fascia heights should be maintained for scale and continuity of the street and buildings.

The term "sympathetic design" refers to the concept of viewing an individual building facade within the context of its surroundings. To achieve an attractive and successful business area, the "streetscape" should be viewed as a complete unit rather than a series of individual isolated store fronts.

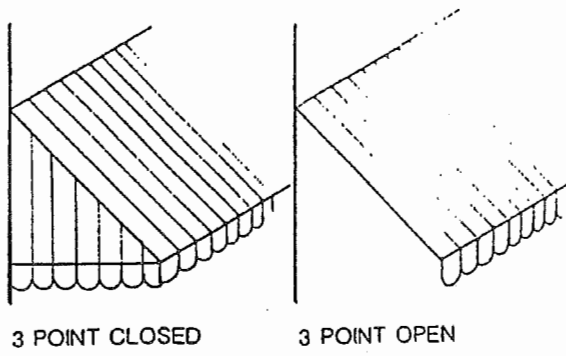
3.5. Canopies

- Guidelines:**
- (a) The minimum height of a canopy over pedestrian areas shall be 2.75 metres (9.0 feet).
 - (b) The minimum clearance of the canopy shall be 0.6 metres (2.0 feet) from the curb and 0.9 metres (3.0 feet) from the utility pole.
 - (c) The required clearance to primary electrical power lines shall be 2.5 metres (8.0 feet), (see attached drawings).

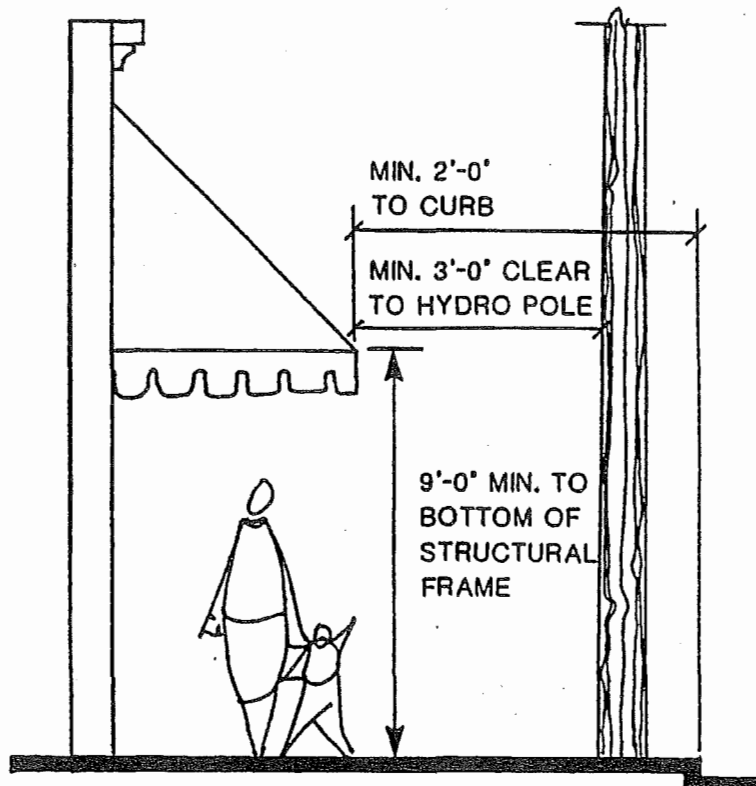
Canopies can be either an awning or a fixed structure. Awnings are fabric and frame which are attached to the face of the building. Canopies should extend out to protect pedestrians from inclement weather.

- Guidelines:**
- (a) Awning frame may be rigid welded or retractable style and the fabric shall be 100% polyester with a acrylic finish and not vinyl.
 - (b) The shape of the awning may be either 3 point style with a valance or 4 point with a fascia of not more than 15 cm (6 inches).
 - (c) The color of the awning shall be suitable to the overall color scheme of the building and streetscape.

Unacceptable awning styles are quarter-barrel, half domes and projecting quarter sphere. Vinyl fabrics are not acceptable.



CANOPY-AWNING TYPES



CRITICAL DIMENSIONS FOR
AWNINGS AND CANOPIES

STORE FRONT FACADE GUIDELINES (continued)

3.5. Canopies (continued)

Fixed canopies are structurally integrated features of a building face and are either cantilevered, hung or supported on a post. Any post supporting a fixed canopy is to be located on private property.

- Guidelines:
- (a) Fixed canopies may be flat or sloping roofs extending over walkways.
 - (b) Sloping canopies shall be covered with wood cedar shingles.
 - (c) Any supporting post shall be round or square wood with simple details or shaping and may be decorated with wooden brackets.

Unacceptable materials are metal, corrugated fibreglass and concrete (posts).

3.6. Windows

- Guidelines:
- (a) In the store front improvement, the display window should be designed to respect the historic rhythm and be part of the overall facade.
 - (b) The window on the upper floors should form a historic rhythm different from the picture windows and be within a proportion of the overall facade.
 - (c) The upper floor windows should be framed.

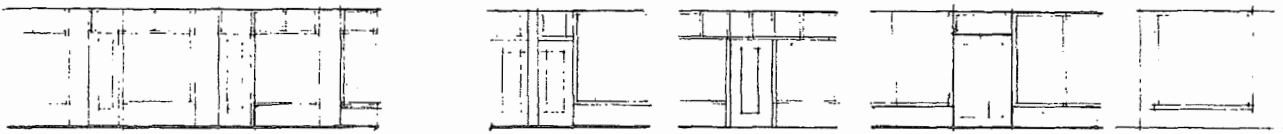
The store fronts are designed to display the business with the "picture" windows being an important feature. At street level, the windows of the store front shows the merchandise and allows visual access into the shop while at the same time forming the wall that separates the inside from the outside.

The design of the windows with transoms, mullions, opaque or translucent glass and multiple glass panes form important patterns in the overall store front facade. The lower portion usually referred to as the "bulkhead", is part of the designed window. The picture window creates store front rhythm and the streetscape.

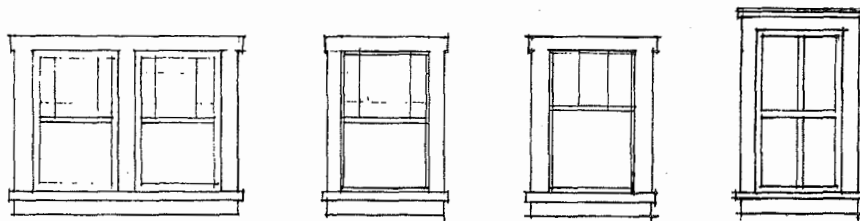
STORE FRONT FACADE GUIDELINES (continued)

3.6. Windows (continued)

Acceptable picture windows are as follows:



Historically, the pattern of the windows on the upper floor is different from the picture windows. They form a rhythm which is in keeping with the overall facade. Acceptable upper floor window patterns are as follows:

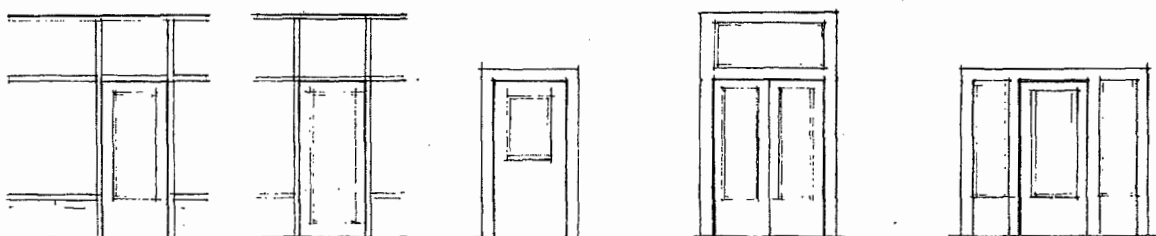


The window frames may be wood, white or coloured aluminum or steel and the glass may be clear or grey tinted. All other colored or mirror finish glass is unacceptable.

3.7. Doors

Guidelines: (a) Doors should be designed to be part of the overall store front character and should have glass panels.

(b) Acceptable doors are as follows:



STORE FRONT FACADE GUIDELINES (continued)

3.7. Doors (continued)

- (c) Acceptable doors are solid wood, wood panel and aluminum frame. Doors without glazing and metal doors are not acceptable.

3.8. Signage

- Guidelines:
- (a) Signs for the building should be an integral part of the facade design.
 - (b) Signs consistent with the Sign By-law should be approved along with the facade design.

Often signs are attached to the building as an afterthought. They are part of carrying out business, but are neglected until the business is about to open.

The prerequisite of a good sign is a clear message and legibility. A balance where neither the building or the sign dominates is needed for the building and the signs to be read. The importance of one well located sign over many signs needs to be stressed. Signs conceived independently can create a discordant image of the downtown and a rash of street signs results in the loss of the purpose of signage. For Steveston, the signs need to be oriented to slow moving traffic and predominantly to pedestrians.

Acceptable signage is as follows:

Fascia Signs: These are flat rectangular signs placed above the store front (as the buildings main business identification). The message in the sign board should be restricted to the name of the business for the sake of clarity; but may include a very brief trade description. In place of sign boards, but in keeping with a similar intent and flavor, signs may be painted directly on to the building facade, generally on the upper storey.

Sign boards may be illuminated from the back or painted boards may be illuminated with fixtures which are in keeping with the facade character.

Window Signs: These are painted on the inside of the main display window. The message should be kept brief, usually to the name of the business; but may include a brief trade description.

STORE FRONT FACADE GUIDELINES (continued)

3.8. Signage (continued)

Projecting or Hanging Signs: Signs may be hung along the store front or perpendicular to the building face. The message should be kept brief and to the business name or logo.

Awning Signs: These signs are painted directly onto the face of canopy, front edge (valance or flounce) or side panel. These messages should be restricted to the name of the business and logo. Back lit awning signs are unacceptable. A Sign Permit will be required for awning signs.

3.9. Building Materials and Finishes

Guidelines: (a) Building materials added for store front improvements should be restricted to the following:

- ship lap or flat lap horizontal wood
- 4 inch lap bevel boards
- drop cove horizontal wood siding
- board and batten
- vertical channel board
- wood shingles for small areas and features
- gingerbread details
- smooth stucco

(b) Acceptable finishes are as follows:

- natural weather
- transparent and opaque stains
- paint

Materials and finishes which are not in keeping with the historic character of the town are unacceptable. These are as follows:

- veneered brick, terra cotta, or stone
- metal siding (aluminum and steel)
- vinyl siding
- textured stucco (California style)
- asbestos shingles and panels
- plywood
- enamel panels
- ceramic or glass tiles
- concrete

An existing concrete block wall may be painted provided the store front painting schedule is within a context of an overall design concept.

STORE FRONT FACADE GUIDELINES (continued)

3.10. Color Coordination

- Guidelines:
- (a) Color schemes for buildings should use only heritage colors.
 - (b) Color schedules for facade improvements shall be submitted with samples along with the color samples of the adjoining buildings.
 - (c) The appropriate use of colors can dramatically increase the visual impact of a building as well as the surrounding context. In selecting the color scheme, neighbouring buildings, building function, surface material color balance and color contrast should be considered. Acceptable colors are as follows:
 - natural colored wood
 - stained wood
 - heritage color of paint manufacturers
 - colors to accentuate architectural details

Unacceptable are extensive bright colors, use of pure white in large masses, monochromatic and monotone color schemes.

3.11. Lighting

Guideline: Lighting should be provided to illuminate the store front facades, windows and signs.

For Steveston, the street lighting provides illumination for the requirements of the street. Buildings, facades and signs are not conveniently highlighted from the street.

Designed illumination can highlight special features of the facade, well prepared signs, main entrances and tastefully prepared displays. For businesses which operate after dark, special care should be given to lighting.

For signage, lighted signs need not be limited to the standard internally lit plastic-face box. Alternatives may be more attractive, more effective and more affordable. Direct illumination of a sign with hooded lights or goose necked lamps is a traditional form of lighting. Other acceptable methods of lighting are concealed spotlights, recessed fixtures, exposed industrial lights and historical feature fixtures which are integrated into the design of the facade.

STORE FRONT FACADE GUIDELINES (continued)

3.11. Lighting (continued)

The plastic-face sign box is a fact of life today. If a box is to be used, effective designs should fit the sign into a framework and into the building facade. The background should be dark colored with light lettering and the plastic face should be matte finished to minimize the sheen.

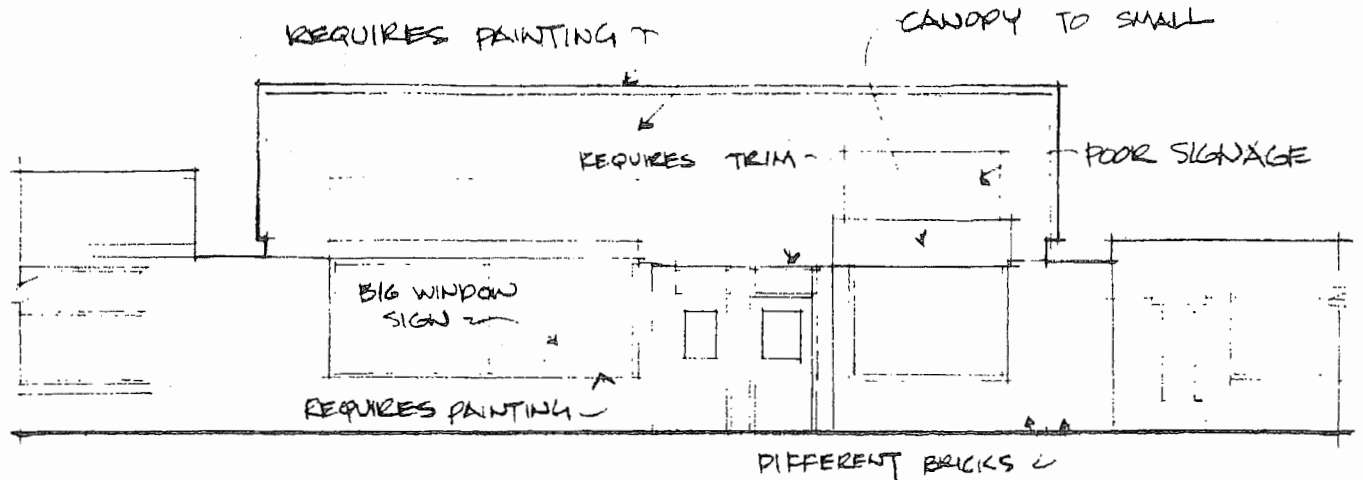
If neon is to be used, it should be for artistic design features and not for the purpose of signage.

Lights which are unacceptable are flourescent lights in display windows, mercury vapour and high pressure sodium lights

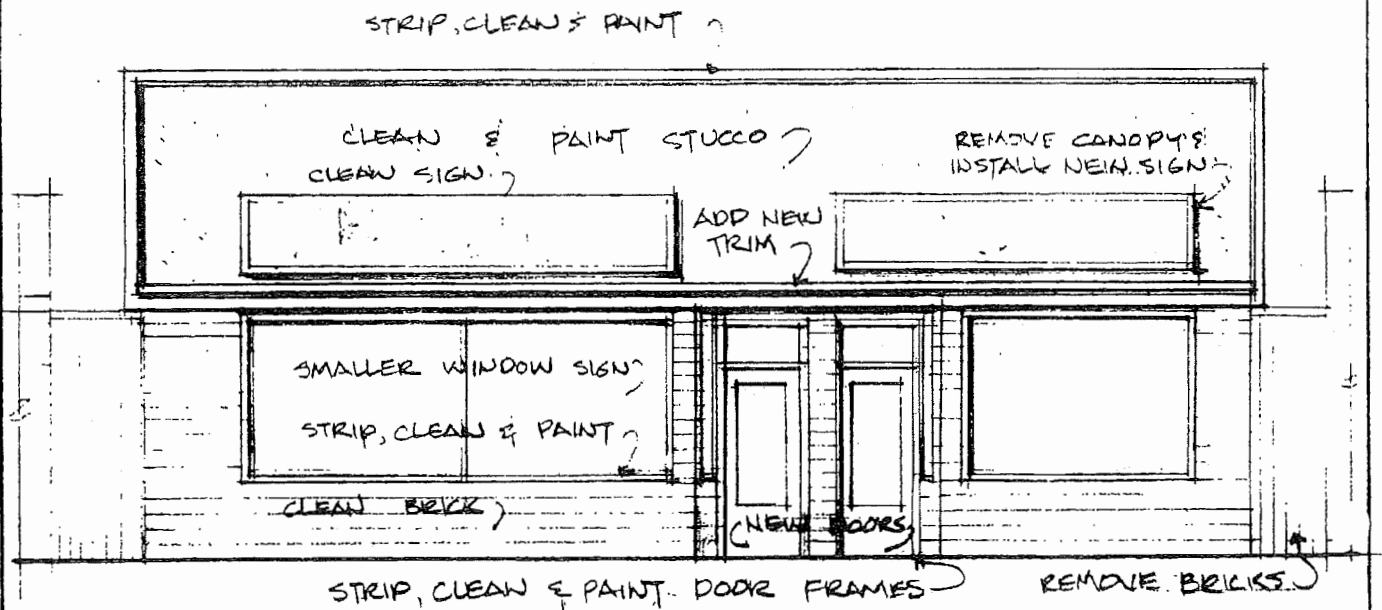
4. FACADE IMPROVEMENT EXAMPLES

The following pages provide examples of facade improvements in Steveston.

FACADE IMPROVEMENT EXAMPLE #4.1



ANALYSIS - EXISTING FACADE



FACADE RECOMMENDATIONS **GNCL - 605**

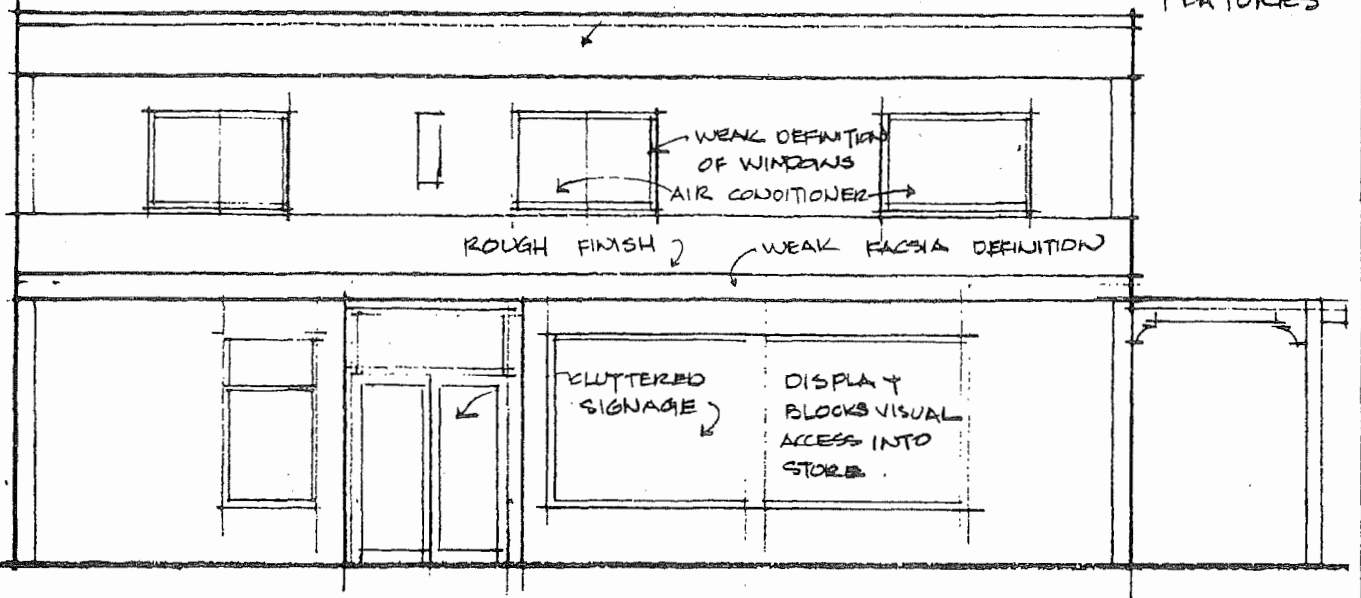
RICHMOND DINISH BAKERY
MONCTON ST. STEVESTON

FACADE IMPROVEMENT EXAMPLE #4.2

REQUIRES PAINTING -

FACADE NEED DEFINITION &

FEATURES

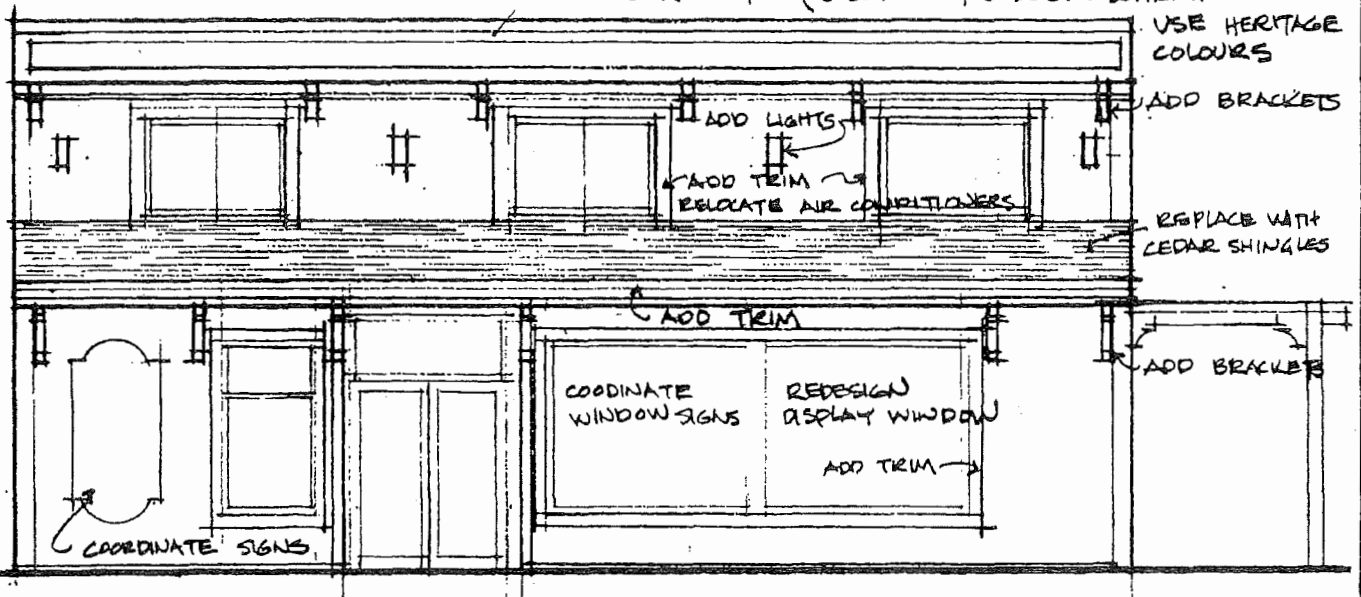


ANALYSIS - EXISTING FACADE

ADD TRIM

COORDINATE (3 COLOUR) COLOUR SCHEME

USE HERITAGE COLOURS

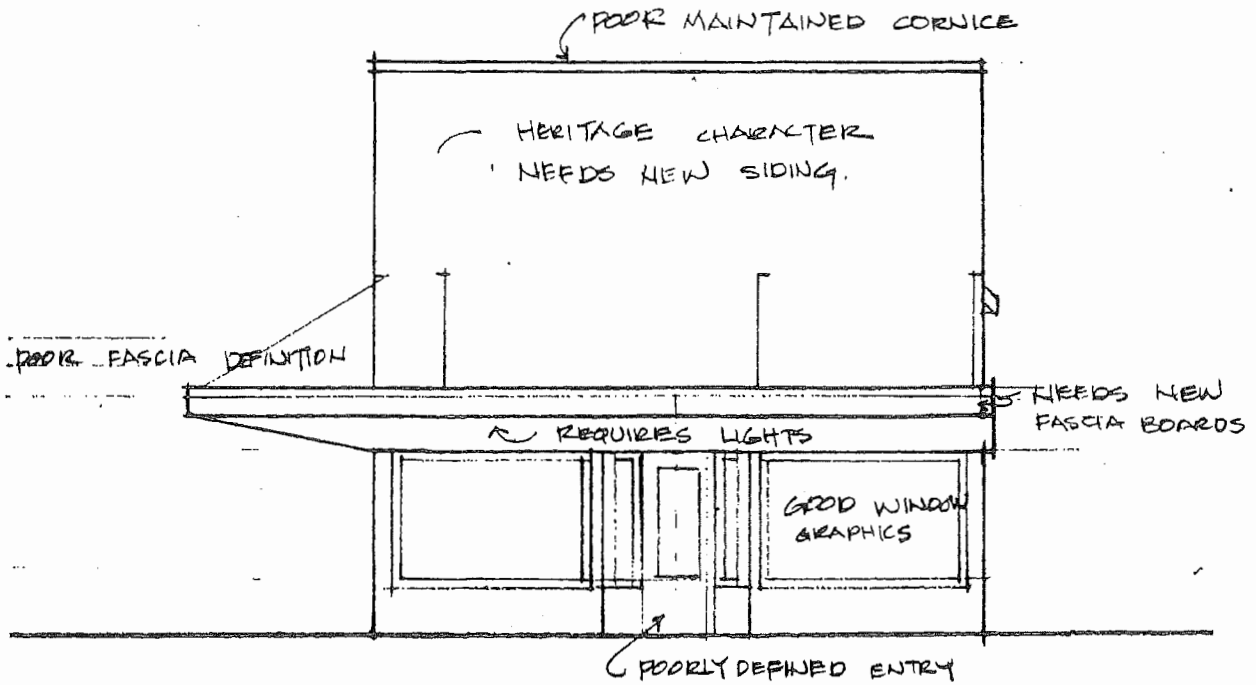


FACADE RECOMMENDATIONS

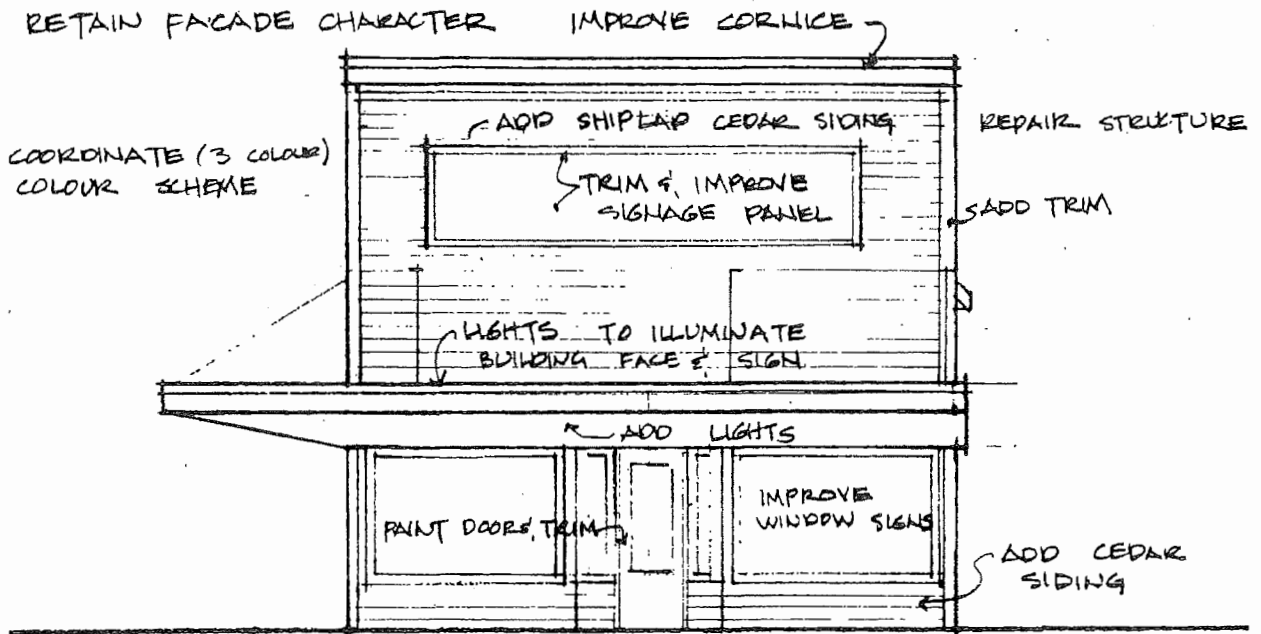
PACIFIC NET & TWINE

MONCTON STREET, STEVESTON

FACADE IMPROVEMENT EXAMPLE # 4.3

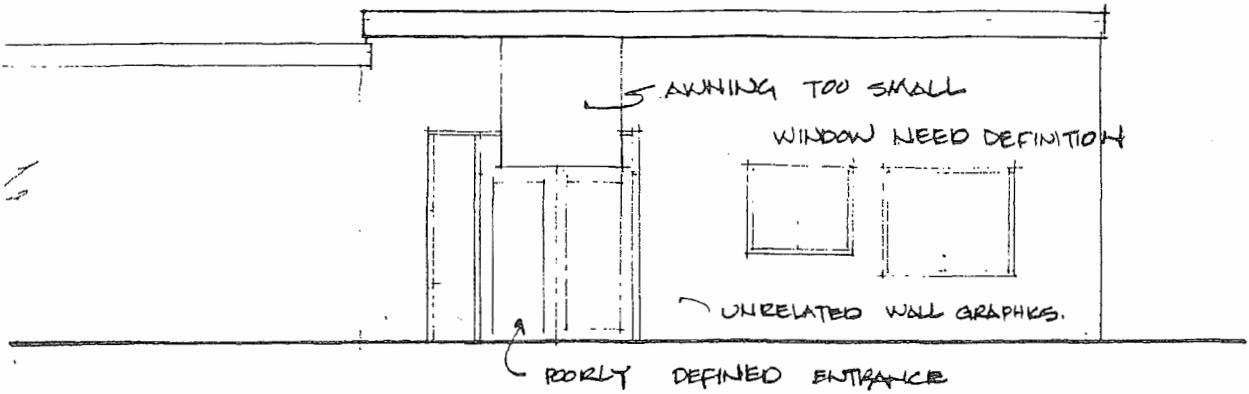


ANALYSIS - EXISTING FACADE



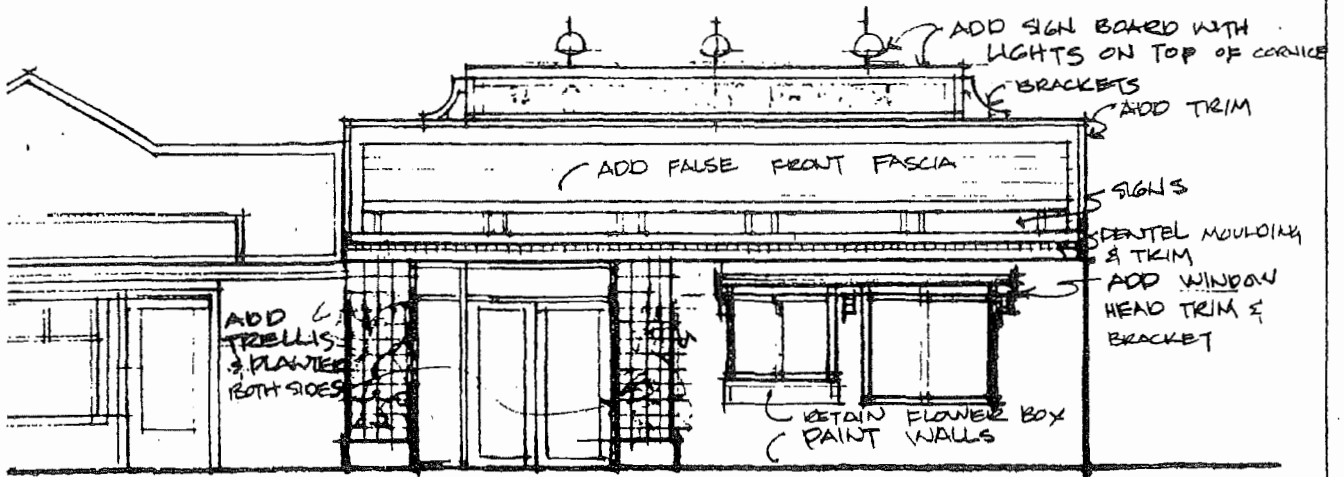
FACADE IMPROVEMENT EXAMPLE # 44

UN-DEFINED STOREFRONT



ANALYSIS - EXISTING FACADE

COORDINATE COLOR SCHEME.



FACADE RECOMMENDATION

COUNTRY MOUSE

CNCL - 608

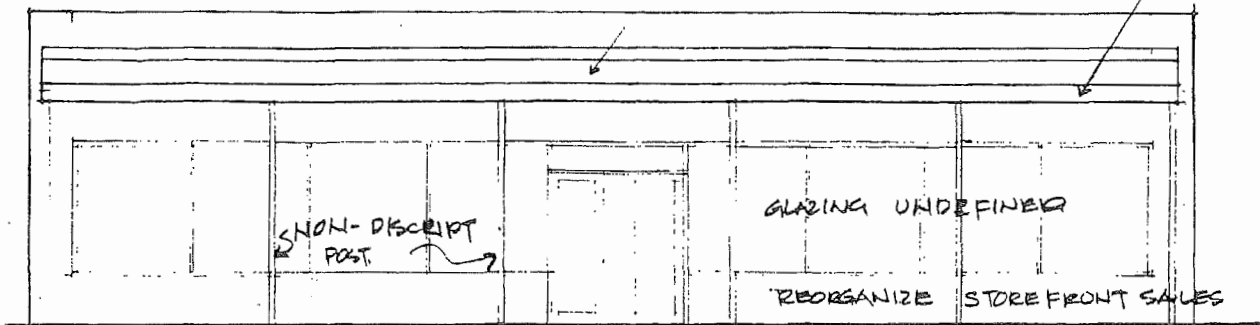
1ST AVENUE STEVESTON

FACADE IMPROVEMENT EXAMPLE # 4.5

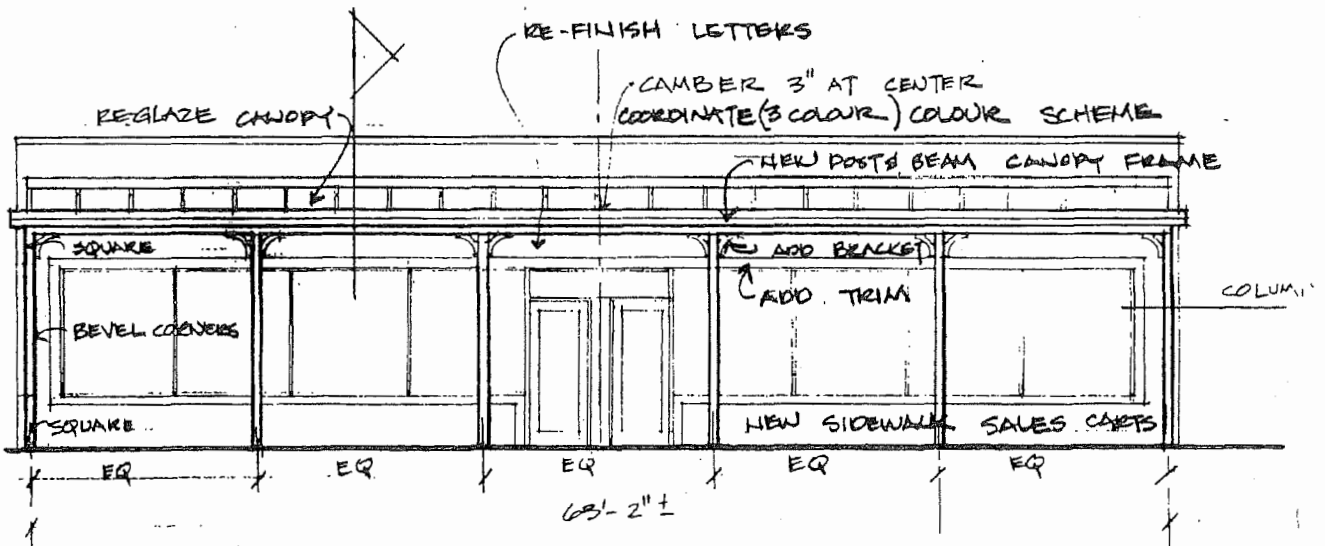
BUILDING NEEDS REPAINTING

DETERIORATED CANOPY

UNDEFINED CANOPY FASCIA



ANALYSIS - EXISTING FACADE

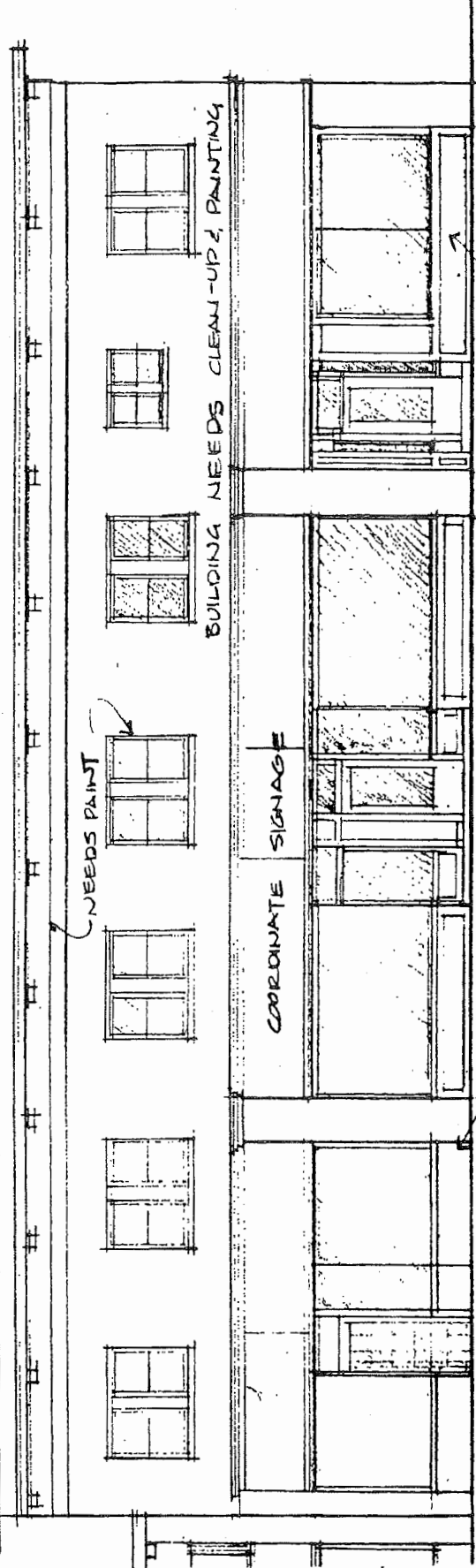


FACADE RECOMMENDATIONS

CNCL - 609

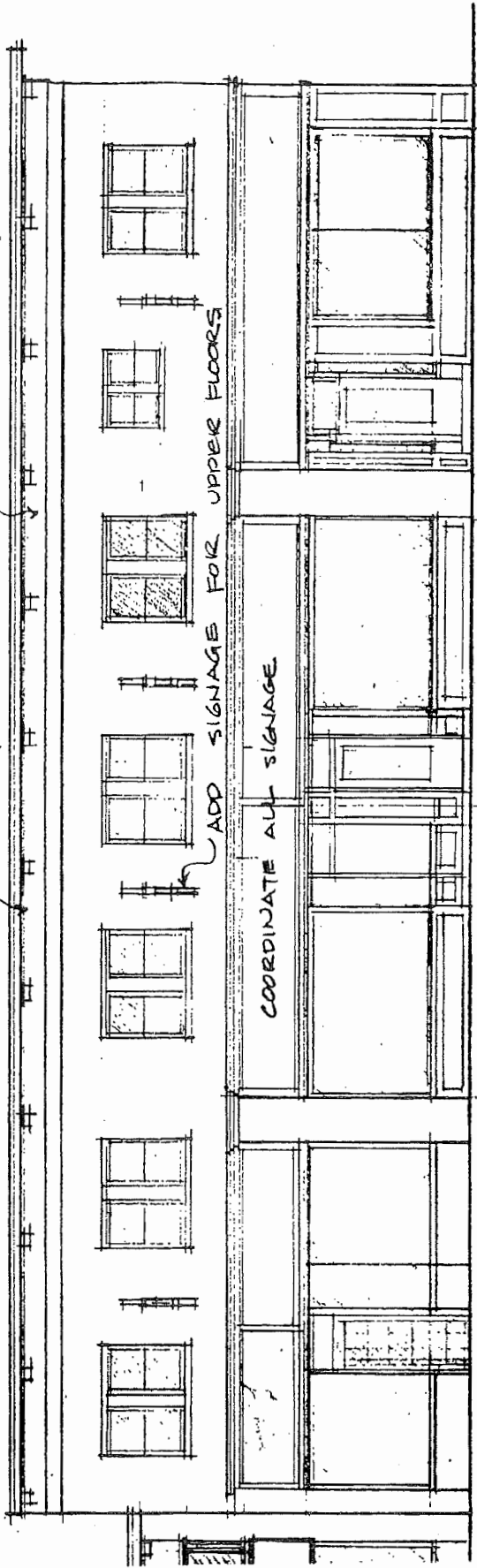
STEVESTON MARINE & HARDWARE
MONCTON STREET STEVESTON

FACADE IMPROVEMENT EXAMPLE 4.6



ANALYSIS - EXISTING FACADE

NEEDS PAINT
 BUILDING NEEDS CLEAN-UP & PAINTING
 COORDINATE SIGNAGE
 BULKHEAD NEED COORDINATION
 COORDINATE COLOURS ENHANCE HERITAGE CHARACTER



FACADE RECOMMENDATION

HEPWORTH BUILDING
 MONKTON STREET STEVESTON

APPENDIX 1

DESIGN REVIEW PROCEDURES

(a) Steps to Facade Improvement

The following steps should be followed for facade improvements:

- . Develop a clear idea of what image you want your business and store front to have. Write it down.
- . With the use of these guidelines, analyze your store front and with your business image in mind, select the features that are the most suited to your situation.
- . Translate your ideas into drawings which will be required for design approvals and for grant applications. It is strongly recommended that you hire an experienced professional designer. The drawings must show all proposed facade improvements to scale and include color chips, fabric samples and photographs or sketches of the building.
- . Present drawings to the Revitalization Review Committee. Store front improvements will be reviewed by the Revitalization Facade Review Committee. The committee may advise you on what other merchants and owners are doing with their store fronts in Steveston to help you coordinate plans and ideas. Please contact the Coordinator responsible for the Steveston area, or the designated Municipal Planner at 276-4082.
- . Make sure you follow the guidelines. You may be asked by the Revitalization Committee to revise and resubmit your drawings if the guidelines are not followed.
- . After the committee has given your submission design approval, fill out a special municipal Revitalization Development Permit Application and submit it along with your drawings and anticipated costs to the Planning Department at Municipal Hall. These documents will make up the grant application.

DESIGN REVIEW PROCEDURES (continued)

(b) Facade Grant Administration

- . Once plans have been submitted and a permit has been issued, the designated Municipal Planner records the anticipated costs of the improvement; certifies that the qualifying requirements have been met; confirms the frontage calculations; and ensures the work conforms to municipal bylaws and is being made to existing buildings. A copy of the approved permit is then sent to the Ministry of Municipal Affairs.
- . The grant is payable directly to the applicant (whether tenant or owner) upon completion of the work unless the Ministry contacts the Municipal Planner within 21 days of receiving the permit copy for further documentation or clarification.
- . The applicant should, upon request, provide invoices and timesheets for the construction to substantiate all costs claimed.
- . After the completion of construction and a final inspection, the Building Inspector certifies the completion on a copy of the building permit and forwards it to the Ministry.
- . The grant is then issued from Victoria directly to the applicant. The Municipality of Richmond will not be receiving the grant and then forwarding it to the applicant.

STEVESTON REVITALIZATION FACADE IMPROVEMENT APPLICATION

TELEPHONE: 278-5575

1. APPLICATION FOR PLAN REVIEW

Date: _____

PLEASE PRINT (to be completed by applicant)

Property address: _____ Unit No: _____

Legal description: _____

Registered tenant/owner: _____ Tel. No: _____

Tenant/Owner's mailing address: _____
(if different from above)

Contractor's business name: _____

Architect/Engineer: _____

PROPOSED WORK - CHECK ONE:

New _____, Add/Alter _____, Interior Finish _____, Repair _____,

Other (specify) _____

Tenant/Owner: _____

Nature of business: _____

Telephone: _____ (H) _____ (O)

2. Please provide a letter outlining the work in full.

3. Six sets of plans and sketches showing scope of work.

OFFICE USE ONLY

COMMENTS

Applicant Fee: \$ _____	Receipt No.: _____	_____
Roll No: _____	Richmond Key: _____	_____
Work Desc: _____	Class: _____	_____
Contractor's Business Licence No: _____	_____	_____
PERMIT NO. _____	_____	_____

APPENDIX 2

STEVESTON DOWNTOWN REVITALIZATION PLAN

In September 1980, the Ministry of Municipal Affairs initiated a program of urban design and beautification for the downtown business cores in cities and towns throughout British Columbia.

Local Steveston business representatives, municipal staff and members of Council from Richmond, formed a 'Downtown Revitalization Committee' in November, 1985 and designated an area of the Village of Steveston suitable for revitalization. The role of this committee has been to provide a community based presentation for the overall revitalization design. The purpose of the Steveston Revitalization program is to:

- . Retain and encourage the fishing fleet and related facilities and thus enhance Steveston's image as a 'Fishing Community'.
- . Maintain the variety of uses geared to local residences and the fishing industry.
- . Integrate urban design features based on the needs of the local residents and the fishing industry.
- . Enhance existing built features and physical qualities of Steveston to reinforce its uniqueness in Richmond and the Lower Mainland.
- . View tourism as a secondary industry.

Design improvements include public improvements to streets and sidewalk reconstruction; provision of additional street furniture; upgrading of lighting and installation of business signage.

APPENDIX 3

RICHMOND SIGNAGE BY-LAW (Extracts Only)

(Certified copies of the original by-law should be consulted for all interpretation and applications of the by-laws on this subject)

APPLICATION FOR SIGN PERMIT

A signed written statement marked 'Application for Sign Permit' must be prepared with the following information:

- . Street address of proposed site of sign.
- . Name and address of person or company for whose benefit the sign is being set-up and the name of the agent for that person or company.
- . Full name and address of sign company.

Prepare plans and specifications drawn in accordance with standard architectural practice and showing:

- . Dimensions and weight of sign.
- . The area of all sides of the structure used as sign.
- . The overall height of the sign and the amount of clearance beneath it; both as measured from finished grade.
- . The proposed location of the sign in relation to the boundaries of the lot it is to be situated upon.
- . The proposed location of the sign in relation to the face of the building or in front of which it is to be affixed.
- . If incandescent lamps are used, the number to be installed.
- . If gas tubing is used, the number of feet of illuminated tubing to be installed.
- . No part of the sign shall project beyond the top or sides of the wall to which it is affixed.
- . Prior to the issuance of a permit, the Building Inspector shall have considered the report of Design Panel pertaining to the sign.

RICHMOND SIGNAGE BY-LAW (Extracts Only) (continued)

Projecting Signs

- . A projecting sign may not project over municipal Property more than 5 feet 6 inches and not less than 10 feet 6 inches from the level of the sidewalk.
- . Projecting signs shall be in an area (including the area of all sides used as a sign) no greater than 3 square feet per foot of wall length to which they are affixed.
- . No part of a projecting sign shall be closer at any point than 8 feet from the nearest finished grade of the site upon which they are situated.
- . No part of any projecting sign shall be higher at any point than the top of the roof line or wall to which they are affixed provided, however, that in no case shall the top of the sign be higher than 25 feet from the nearest finished grade of the site upon which they are situated.

Marquee Signs

- . A marquee sign is affixed wholly beneath a permanent canopy perpendicular to the face of the building.
- . A marquee sign may extend up to 5 feet 6 inches over public property when affixed wholly beneath a marquee or walkway covering.
- . A marquee sign shall be no greater than 8 square feet (including the total area of all sides of the marquee device used as a sign).

APPENDIX 4

GENERAL CRITERIA FOR CANOPIES PROJECTING OVER MUNICIPAL SIDEWALKS

DEFINITION

Canopies include any projection designed to project over municipal sidewalks to protect pedestrians from the elements. Canopies may also be called awnings or marquees.

Canopies must meet Building Code requirements. Canopies must be supported by structural elements on private property because no posts or supports will be permitted on public property.

INDEMNITY

Owners of properties with canopies projecting over municipal property shall sign a Section 215 agreement indemnifying the Municipality.

PERMITS

Canopies shall be regulated by Development Permits and Building Permits.

LOCATIONS

Canopies will be permitted in all Development Permit Areas, subject to the Guidelines adopted in that area.

CLEARANCES

(See sketch)

- . 2.7 metres (9.0 feet) headroom
- . 1.0 metres (3.0 feet) to utility poles
- . 600 mm (0.68 feet) to curb
- . 2.5 metres (8.0 feet) to wires or metal fixtures

DRAINAGE/SNOW ACCUMULATION

Canopies shall be designed to safely shed snow and rain. A minimum slope of 45° is recommended.



**City of
Richmond**

Bylaw 9797

**Richmond Official Community Plan Bylaw 7100
Amendment Bylaw 9797
Steveston Area Plan (Schedule 2.4)**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Official Community Plan Bylaw 7100 is amended as follows:
 - i) Repeal and replace the following text in Section 9.3.2.2 Area B: Steveston Village Sub Area Guidelines (Steveston Village Riverfront – Massing and Height) as follows:
 - “c) With regard to building height:
 - i) Typically vary from one to two storeys and up to 20 m GSC at main roof ridge, to not be taller than the Gulf of Georgia Cannery;”

- ii) Repeal and replace a portion of the existing table in the Steveston Village Land Use Density and Building Height Map for the Riverfront Area as follows:

	Maximum FAR	Maximum Storeys	Maximum Building Height
Riverfront Area	1.2	2	20 m GSC***

2. This Bylaw may be cited as **“Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9797”**.

FIRST READING

PUBLIC HEARING

SECOND READING

THIRD READING

ADOPTED

_____	CITY OF RICHMOND
_____	APPROVED by
_____	<i>KE</i>
_____	APPROVED by Manager or Solicitor
_____	<i>[Signature]</i>

MAYOR

CORPORATE OFFICER