



# City of Richmond

## Report to Development Permit Panel Planning and Development Department

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**To:** Development Permit Panel

**Date:** April 19, 2011

**From:** Brian J. Jackson, MCIP  
Director of Development

**File:** DP 10-544504

**Re:** Application by Townline Gardens Inc. (dba The Gardens Joint Venture) for  
Development Permit for a portion of 12011 Steveston Highway and 10800 No. 5  
Road

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### Staff Recommendation

That a Development Permit be issued which would:

1. Permit the development of 'The Gardens' – Phase 1 consisting of 2 mixed-use residential/commercial buildings containing a total of 182 apartment dwelling units with a total floor area of 20,335 m<sup>2</sup> (14,472 m<sup>2</sup> residential and 5,863 m<sup>2</sup> commercial) for a portion of 12011 Steveston Highway and 10800 No. 5 Road on a site rezoned Commercial Mixed Use (ZMU18) – The Gardens (Shellmont).

Brian J. Jackson, MCIP  
Director of Development

BJJ:bg  
Att. 6

## Staff Report

### Origin

Townline Gardens Inc. (The Gardens Joint Venture) has applied to the City of Richmond for permission to develop 'The Gardens' – Phase 1, consisting of 2 mixed-use residential/commercial buildings containing a total of 182 apartment dwelling units with a total floor area of 20,335 m<sup>2</sup> (14,472 m<sup>2</sup> residential and 5,863 m<sup>2</sup> commercial) over a common parking structure for approximately 649 cars. The development, subject of this report occupies a portion of 12011 Steveston Highway and 10800 No. 5 Road on a site that is being rezoned (RZ 08-450659) to Commercial Mixed Use (ZMU18) – The Gardens (Shellmont) and forms part of the larger comprehensive mix-use development proposal known as 'The Gardens'.

A Servicing Agreement (SA) is required for 'The Gardens' as part of the Rezoning Considerations to address Steveston Highway and No. 5 Road frontage upgrades as well as water, storm and sanitary required upgrades and specific site service connections.

'The Gardens' - Phase 1 development site is currently vacant with the exception of preload material over a portion of the site. See attached **Schedule A** for the location of the Phase 1 Development Site.

### Development Information

Please refer to attached Development Application Data Sheet (**Attachment 1**) for a comparison of the proposed development data with the relevant Bylaw requirements. Also refer to **Plans # 1** for the Site Plans, **Plans # 2** for the Parking Plans, **Plans # 3** for Landscape Plans, **Plans # 4** for Building Elevations and **Plans # 5** for Building Floor Plans, all of which form part of the subject Development Permit DP 10-544504.

### Background

The subject site is the former Fantasy Gardens property situated on the north side of Steveston Highway, between Highway 99 and No. 5 Road. The site has been in disuse for a number of years and preliminary site preparations for redevelopment are currently underway. Development surrounding the subject site is as follows:

To the north, are future development phases of 'The Gardens' development site, the ALR 'Agricultural-Park' and beyond, the area is characterized by large properties designated to permit assembly uses fronting No. 5 Road with farming to the rear (fronting Highway 99);

To the east, Highway 99 runs along the entire east edge of 'The Gardens' development site, separated from the site by a large drainage ditch (within the highway right-of-way) and a tall, evergreen hedge (within the subject site). East of the highway, properties are predominantly agricultural use;

To the south, Steveston Highway runs along the entire south edge of 'The Gardens' development site and beyond is an important employment centre, including the "Ironwood" commercial area west of No. 5 Road (made up of two large shopping centres), a large business park to its south and a mixed commercial/business park area east of No. 5 Road with frontage along Highway 99 and Steveston Highway, immediately across from the proposed Phase 1 development site. Ironwood offers a broad range of convenience retail, restaurants, and services, including a branch library; and

To the west, No. 5 Road runs along the west edge of 'The Gardens', and beyond is a stable, single-family residential neighbourhood, with a limited number of small, townhouse projects that front onto No. 5 Road.

### **Rezoning and Public Hearing Results**

The Public Hearing for the rezoning of this site was held on October 19, 2009. At the Public Hearing, the following concerns about rezoning the property were expressed, which are followed by the staff comments highlighted in ***bold italic***.

While no objections to the proposed development were raised, there was discussion regarding the traffic in this area and concern was expressed regarding:

- the creation of a new signalized intersection on No. 5 Road to permit full traffic movement to the development site. Reference was made to congested traffic already present in this area and it was suggested that commercial traffic use Rice Mill Road;
- that Williams Road be developed to go over Highway 99 to No. 6 Road; that clover leafs be developed at the existing overpass;
- that staff avoid planting bulbs or grass on the raised medians, due to backup of traffic during the maintenance of these medians;
- that evergreen trees should be planted to avoid leaves blocking gutters in the Fall; and
- that staff avoid planting trees directly under power lines.

During the discussion that ensued, Council requested that transportation staff provide information on the status of Rice Mill Road access and that Parks staff be requested to consider planting perennials in the medians and perform plant maintenance outside of work hours.

Staff worked with the applicant to address these concerns and comment as follows:

#### **Signalized Intersection at No. 5 Road**

***The new No. 5/Road 'A' intersection and associated traffic signal will facilitate full movement for access/exit to the proposed 'The Gardens' development and the ALR Agricultural Park. In association with this signalized intersection the applicant has agreed, as part of the SA, to the following additional road upgrades:***

- ***a new southbound to eastbound left-turn lane at the Steveston/No. 5 Road intersection;***
- ***a new westbound to southbound left-turn lane at the Steveston/No. 5 Road intersection;***
- ***and***
- ***a new westbound to northbound right-turn lane at the Steveston/No. 5 Road intersection.***

***These traffic and intersection improvements will provide additional road capacity intended to accommodate both commercial and general purpose traffic.***

#### **Planting on Medians**

***Parks staff indicate that centre median landscape maintenance along high traffic routes such as Steveston Highway is generally conducted at off-peak traffic hours during the day but given the traffic volumes along this corridor, all landscape maintenance work on the Steveston Highway centre medians in the Ironwood Sub-Area will now be performed at night and consideration will be given to lower maintenance landscape plantings in the vicinity.***

#### **Rice Mill Road Status**

***Transportation Engineering has indicated that Rice Mill Road is under the jurisdiction of the Ministry of Transportation and that access from Rice Mill Road to Highway 99 and vice versa is restricted to Ministry of Transportation vehicles.***



## Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan and is in compliance with the Commercial Mixed Use (ZMU18) provisions of Richmond Zoning Bylaw 8500. No zoning variances have been requested.

## Advisory Design Panel Comments

The Advisory Design Panel was supportive of this development permit application moving forward to the Development Permit Panel. A copy of the relevant excerpt from the Advisory Design Panel (ADP) minutes from December 22, 2010 is attached for reference (**Attachment 2**). The design responses from the Applicant are included immediately following the specific ADP comments and are identified in '*bold italics*'.

## Analysis

### Conditions of Adjacency

This proposal complies with the development permit guidelines and the required building setbacks. 'The Gardens' overall development proposal includes a 3.0 m wide ALR buffer strip along its north boundary, which will be provided as part of the proposed Phase 1 and a 15.0 m wide Riparian Management Area (RMA) along Highway 99; design/enhancement of this area will be determined as part of development applications for future development Phases.

The proposed Phase 1 development relates to its immediate context as follows:

North Edge: Phase 1 (Building A and Building B) of 'The Gardens' will ultimately abut future buildings of this multi-phased development (Building D, E1 and E2). A proposed internal east-west privately owned, publicly accessible street, "The Village Street", which includes a pedestrian courtyard and auto-court at the west end plus a landscaped traffic circle at the east end, will be the main organizing element of 'The Gardens' site development. In Phase 1, this Village Street parallels the north side of Buildings A and B development site, creating the separation from buildings in subsequent phases of 'The Gardens' development.

The Village Street is located on the roof deck of the parking podium and will be bisected by the north-south leg of Road 'A', a publicly accessible street that provides direct access to the Village Street from Steveston Highway and a link to No. 5 Road. This east-west central street will be fronted by Retail Commercial Units (RCU's) at the base of both, Buildings A and B, with 3 levels of residential apartments above. Buildings A and B present a wide sidewalk/promenade and short-term surface parking along their retail commercial frontage on the south side of the central Village Street. The intersection of the central Village Street with the Road 'A', at the northeast and northwest corners of Buildings A and B respectively, will be raised and will also include raised pedestrian crossings.

East Edge: A future restaurant (Building C) is planned on the east side of Building B; this building will be the subject of a separate development permit. There is a north-south drive aisle and parking proposed along the east side of Building B that will serve as loading area and parking for the future Building C. Aspects related to the separation/relationship between Buildings B and C (future restaurant) and height of Building C will be addressed during the development permit process for Building C.



Landscape and enhancement of the 15m wide RMA along the Highway 99 will also be addressed in future development permits as part of the RMA Landscape Enhancement and Maintenance Plan.

South Edge: Frontage improvements are proposed along the north side of Steveston Highway via the required Servicing Agreement. These off-site improvements include a new right turn only stacking/deceleration vehicle lane, a treed and grassed boulevard at the back of curb and a 3.5m wide shared pedestrian/bike greenway.

Building A, in its lower level, includes retail commercial uses and the glazed indoor amenity space for all residents of 'The Gardens' that are fully exposed to Steveston Highway, providing interest and animating the building frontage at street level. The on-site landscape treatment along Steveston Highway consists primarily of a landscaped bank that slopes up to the parking structure concealing the parking structure walls. The proposed landscape design along Steveston Highway that includes sloping and terraced landscape planting areas in a rectilinear pattern with low shrubs and groundcovers plus an informal row of trees is anchored with a pedestrian urban plaza and water feature at the Steveston/No. 5 Road intersection.

West Edge: The No. 5 Road frontage includes continuous retail commercial frontage that extends to the outdoors on a raised deck above the sidewalk level. This frontage also proposes a sloped and terraced landscape treatment that steps down from parking podium/western deck of Building A to the street. Treatment of the interface between the street and the raised deck/central Village Street and associated courtyards includes a system of angled stairs and ramps that connect street level with the CRU's level over the parking podium roof. The street level pedestrian, urban plaza, gateway public space, at the northeast corner of the Steveston Highway and No. 5 Road intersection includes a water feature and terraced landscape plantings while the northeast corner of Phase 1 site includes a pedestrian plaza on the parking podium deck, as part of the courtyard that anchors the western end of the central Village Street.

### ***Urban Design and Site Planning***

#### **General Comments:**

The site, located at the gateway entrance to Richmond from Highway 99 provides a unique urban living experience while still reflecting the garden, water and agricultural history of the area. The buildings are organized along both sides of a central East-West spine, the Village Street that concentrates residential, open space and retail uses and reinforces a strong pedestrian scale where pedestrians have dominance over vehicles. Trees, awnings, retail frontage, wide sidewalks, raised crossings, landscaping, planting and colour and textured pavement contribute to the proposed strong pedestrian character at the internal core of the proposed development. This same character is also expressed at street level along Steveston Highway and No. 5 Road frontages.

Key aspects of the urban design and site planning for 'The Gardens' development in general, and proposed Phase 1 development in particular, are highlighted below:

1. Urban Building Blocks: The site plan (overall plan and Phase 1) is based on a simple grid of medium-density buildings, each one organized around an outdoor amenity central courtyard, separated by standard width streets into 4 blocks that are framed by two major perimeter streets and the Highway.
2. Building Mass: Mass of the residential component of Building A, in Phase 1, takes the form of a C around an outdoor amenity space courtyard on the roof of the retail commercial uses in the lower level. This roof terrace courtyard is open towards the south and its depth helps to provide some distance from most of the residential units to the busy Steveston Highway,

minimizing the traffic noise. For the same reason, the residential component of Building B is also organized in the same manner; however, because this building is also located closer to the Highway 99 off-ramp, this central outdoor amenity space courtyard is also semi-enclosed by a lower, single loaded residential wing that extends along a portion of its south side.

3. Gateway at Steveston Highway and No. 5 Road Corner: In addition to the urban plaza at the SW corner of Phase 1 development, which is characterized by its large open space, fountain and terraced planters, the southwest corner of Building A presents glass walls, with some touches of colour, that angle up toward the angular soffit that cap the southwest corner of Building A to strongly anchor that important corner and establish the Gateway role of the proposed development in the area.
4. Terraced Streetwall along Steveston Highway & No. 5 Road: The proposed Phase 1 development features an integrated architectural and landscape treatment with horizontally layering along the outer perimeter frontages of the site to create a more pedestrian-friendly street experience, to better define the street wall and to complement the lower-rise neighbouring buildings. These layers include:
  - Perimeter Greenway: Off-site along existing street fronts, a shared pedestrian/bike route including a broad path, green landscaped boulevard and row of street trees. Parallel, but within the site, a decorative, retaining wall, landscaped terraces, second row of informally planted trees, to help screen the parking podium and soften the transition between the street and the fronting buildings.
  - Mid-Rise Streetwall: Buildings with a strong horizontal expression typically 19.0 m high, as measured from the fronting greenway, generally setback 6.0 m or more from the property line and typically characterized by durable materials such as brick masonry and concrete hardi-panels
  - Varied Building Roof Tops: Above the streetwall, a varied building roof top or cornice treatment to add individual distinction and visual interest to each building.
5. Pedestrian-Scaled Circulation: Along the north side of Phase 1, the internal Village Street will ultimately create an 'Urban Village' atmosphere with CRU's at grade with wide well landscaped sidewalks/walkways, a limited amount of convenience, short-term surface parking, and defined by well articulated, pedestrian-scaled residential and commercial frontages. A variety of decorative pedestrian paving materials are incorporated into the design including cast-in-place concrete, concrete unit pavers and wood decking to reinforce this pedestrian oriented character. The modern architectural character includes common elements between the buildings but with enough variation to create visual interest at a pedestrian pace along the street. Phase 1 will include construction of the east-west street plus 75% of both the west pedestrian courtyard and auto-court as well as the landscaped traffic circle at the east end.
6. Integration with an Urban-Agricultural Showcase: Council adopted the Conceptual Development Plan for the ALR 'Agricultural-Park' on January 24, 2011, which provides approximately 12.2 acres of ALR 'Agricultural-Park' as a community amenity. When completed in future phases of development, 'The Gardens' proposed development will integrate with the ALR 'Agricultural Park' through a wide Mews/linear open space as an extension of the Village Street space to the north.
7. A Green Development Strategy: Sustainability measures incorporated into 'The Gardens' include the following:



- all buildings will meet or exceed LEED Silver equivalent status;
- a geothermal heat exchange system using excess heat from the grocer's refrigeration units;
- a stormwater management concept whereby stormwater from the development site will be directed to the ALR 'Agricultural-Park', stored and used for irrigation;
- Transportation Demand Measures (TDM) including provision of two co-op car-share vehicle spaces, two bus shelters and end of trip bicycle facilities that will encourage alternative means of transportation and use of public transit;
- energy star appliances and glazing, operable windows, energy efficient lighting, low flow fixtures, low emission and recycled building materials wherever possible.

8. City in a Garden: Extensive tree planting and the green landscape perimeter treatment creates a 'garden-like' character by providing a typically wider building separation and by treatment of the intervening space with a combination of landscape plantings reflecting a large scale agricultural pattern creating a green landscaped verge to the perimeter of the site.

Separation between the Phase 1 buildings and future buildings across the Village Street in 'The Gardens' development site will vary from approximately 17.0 m to 35.0 m creating variety along the east-west Village Street and providing privacy for fronting residential units above. Precise separation of buildings in future phases will be determined at the time of the Development Permit review process for those future developments.

9. Parking & Services: 'The Gardens' presents a balanced approach between parking and loading for a transit- and pedestrian-oriented development. The proposed development minimizes parking and loading impacts as well as traffic conflicts with the surrounding road network through a series of existing roadway and intersection improvements. The interior site streets create safe and attractive pedestrian-oriented streetscapes.

Access to the Site and to Parking: There is restricted vehicle access to the site from Steveston Highway, where right-in only access is allowed on Road A to the central Village Street and surface parking, and from/to No. 5 Road (right-in and right-out only) that provides access to the below grade parking levels. There will be a full movement signalized intersection on No. 5 Road at the northwest corner of 'The Gardens' development site that will provide access/egress to all parking on the development site and to the ALR Agricultural Park.

'The Gardens' Off-street Parking: The majority of parking has been provided in a one -storey structure below the lowest habitable storey to minimize the bulk of the parking structure and the visual impact of parking on the surrounding streetscapes and pedestrian areas as well as maximize opportunities for an accessible ground plane. A small, lower level parking area is provided under Building B taking advantage of the soil conditions and site grading.

'The Gardens' Parking Requirements: As part of the rezoning (RZ 08-450659), the required number of parking was reduced based on the provision of TDM (see 'Green Development Strategy' above for list of TDM). Based on the preliminary development concept plan for the whole 'The Gardens' site, the proposed parking layout and loading requirements have been satisfactorily met. The preliminary development concept for the whole site includes 1,106 parking spaces, including 86 short-term surface parking spaces, while 992 spaces are estimated to be required.

Phase 1 Off-Street Parking: The proposed Phase 1 development provides 278 residents parking spaces, including 6 HC spaces (subject to the provision of TDM, 247 spaces are required, including 5 HC spaces). In addition, all the required combined commercial/visitor parking required for the complete 'The Gardens' proposal has been provided in Phase 1 of the

development (371 combined commercial/visitor parking spaces provided, while 302 spaces are required).

**Bicycle Parking:** The Proposed Phase 1 development meets the bicycle parking requirement by including 287 Class 1 spaces and 68 Class 2 bicycle parking spaces while 244 Class 1 and 58 Class 2 spaces are required.

**Loading and Unloading Spaces:** Of the total 11 loading spaces required for the whole development of 'The Gardens', 8 loading spaces (5 medium and 3 large) have been provided in Phase 1. Loading facilities for Building A and Building B are located away from main pedestrian areas on the Village Street, along the one-way section of Road 'A'. In addition, the actual loading docks are contained within the respective buildings to minimize the impact on adjacent site areas.

**TDM:** The development concept proposes a series of Transportation Demand Measures (TDM) that include:

- a transit- and pedestrian-oriented development,
- a mixed land-use development to help reduce vehicle trips,
- a new signalized intersection along No.5 Road,
- multi-purpose greenways along both Steveston Highway and No. 5 Road,
- two new bus stops (one on No. 5 Road and cash-in-lieu for another bus stop to be located on Highway 99 south of Steveston Highway),
- traffic calming measures within the development (upgrading paving, bollards, and raised crossings),
- end of trip bicycle facilities in Building A,
- shared parking (visitors and commercial) for more efficient use of spaces and two cooperative car share vehicle spaces along Road 'A' at the northeast corner of Building B.

### ***Architectural Form and Character***

General Comments: The building design generally conforms to the OCP Development Permit Guidelines. The architectural expression of proposed buildings in Phase 1 is well articulated, with a variety of building forms and via the use of high quality materials and layered detailing. The architectural form and massing of the Phase 1 proposed development incorporates significant features that enhance the perimeter expression of this project, including:

- a distinctive treatment of the building mass and facades at the southwest corner of Building A, at the Steveston Highway/No. 5 Road intersection;
- breaks or jogs along the street fronting buildings for visual interest;
- strong horizontal expression including flat roofs that reduce the apparent height of the buildings and provide a modern architectural character;
- layering of building systems (glazing, balconies and external structural members) that add detail, visual relief and a greater three-dimensional quality to the buildings;
- incorporation of high quality materials such as brick cladding;
- screening of parkade walls with berms and landscape plantings;
- Rooftop mechanical elements that will be screened from views from adjacent streets and residential units in the development; and
- Signage in compliance with the relevant bylaws that has been appropriately scaled along the building facades.



Architectural Elements: Enhancements to the liveability and pedestrian scale of Ironwood and 'The Gardens' area include:

1. Key Landmarks: Phase 1 of 'The Gardens' incorporates distinctive architectural features such as the unique, tilted glass wall and extensive roof overhang on the SW corner of Building A, varied building mass and use of other structural member such as glulam timbers, steel canopies and wood trellises that enhance the project identity at this prominent location as a south gateway to Richmond.
2. Steveston Highway/No. 5 Road Intersection: The proposed development creates a pedestrian-friendly environment including CRU's fronting publicly accessible, on-site streets and open spaces, specifically at the 'Gateway Terrace' (Steveston/No. 5 Road intersection). Liveability of this corner is reinforced by the large raised deck in front of the CRU's that front No. 5 Road and overlooks the urban plaza at the corner.
3. No. 5 Road Frontage: The provision of two large pedestrian plazas that incorporate public art with a fountain in one and a connecting staircase/ramp system plus landscape terraces with sloping ground plane plantings, invite pedestrians to the internal Village Street and create a streetfront that is responsive and appropriate to the residential uses on the opposite side of the street.
4. Steveston Highway Frontage: Along this high-traffic frontage Buildings A and B in Phase 1 of 'The Gardens' incorporate many features that enhance the streetfront presence of this development including the corner plaza, pedestrian-friendly shop frontages with clear glazing, public art, the prominent location of the shared indoor amenity space.

The varied building mass, building setbacks, the outdoor amenity landscaped deck over the retail components at ground level, that create gaps in the residential streetwall above the retail/commercial podium level, the use of brick masonry and concrete detailing, projecting illuminated and diffuse glass/sign panels on the south side of Building B, decorative lighting, restrained commercial signage and recessed balconies in the residential component all contribute to the richness and articulation of the buildings frontage toward the abutting streets.

5. Residential Noise Mitigation: The Applicant has provided an acoustical report, which states that by incorporation of the acoustical recommendations, 'The Gardens' will meet the OCP interior design noise level criteria. In addition, the design of the residential components of the buildings around a raised southward opened courtyard (toward Steveston Highway) minimizes the number of apartments directly exposed to traffic noise.
6. Residential Indoor Amenity Space: The 505m<sup>2</sup> shared indoor amenity space provided prominently fronting Steveston Highway in Phase 1 is intended to serve the entire 'The Gardens' development and appropriate easements over the amenity space are required as part of this development permit for the benefit of all future development phases. This indoor amenity space includes an indoor gym, fitness room, lounge area or great room, smaller meeting room and washrooms for a complete range of activities.
7. Residential Outdoors Amenity Space: Proposed Phase 1 includes 675.0 m<sup>2</sup> of common outdoor residential amenity space (319.0 m<sup>2</sup> in Building A and 356.0 m<sup>2</sup> in Building B), which is less than the 1,092m<sup>2</sup> (182 x 6 m<sup>2</sup>/unit) recommended in the OCP Development Permit Design Guidelines. However the entire 'The Gardens' development will ultimately provide approximately 12.2 acre as ALR 'Agricultural Park' as well as a public open space within a future phase of the development site, which should afford adequate outdoor recreation opportunities for all residents within the overall development.

### ***Landscape Design and Open Space Design***

General Comments: As part of the rezoning requirements, the applicant will transfer approximately 12.2 acres as an ALR 'Agricultural-Park' to the City that will include a play area, trails, community gardens, agricultural opportunities and various garden areas.

'The Gardens' will minimize development impacts on the ALR by incorporating the required continuous 3.0 m wide ALR buffer strip on a development permit by development permit basis in order to address the urban-agricultural interface area. This transition is proposed to be softened as follows:

- **Landscaped Parking Podium:** The landscape transition between the ALR and the parking podium of future buildings will be addressed by gradual landscaped slopes and planting in subsequent development permits.
- **Varied Building Setbacks & Heights:** The building setbacks are to be variable and building heights modulated to diminish the appearance of a solid streetwall when viewed from the perimeter of the site.

The required continuous 3.0 m ALR buffer including appropriate landscape treatment will be provided along the north edge of the development site and the required 15.0 m RMA will be provided along Highway 99 as a 'no-build zone'. The landscape treatment will be addressed by the development permit of future phases with frontage along any portion of the RMA.

There are no existing trees on Phase 1. PROP ROW's associated with Phase 1 will provide public access to all commercial uses through a series of internal streets and walkways to be maintained by the Owner (**Attachment 3**).

All perimeter street fronts have been appropriately landscaped with berms and planting to conceal parking podium walls.

A combined pedestrian-bike greenway will be provided along Steveston Highway and No. 5 Road as well as two public pedestrian plazas, one at the Steveston/No. 5 Road intersection including public art and a water feature; another pedestrian plaza along No. 5 Road is provided at the retail podium level with a second water feature.

The shared indoor amenity space is provided in Building A fronting on Steveston Highway. Phase 1 outdoor amenity space is provided by the two roof deck courtyards that include a simple lawn landscaping treatment. A small children play area provided on Building A raised outdoor landscaped courtyard.

Landscape Elements: The proposed development incorporates improvements to enhance the livability of 'The Gardens' for residents, workers and visitors and encourages a positive relationship between nearby residents through the provision of an open space network that includes the following:

Special Greenway Treatment: The proposed greenway treatment along both Steveston Highway and No. 5 Road creates an important community amenity and helps to provide a buffer for noise, visual screening, pollution and other impacts. This Greenway includes:

- Within the public road right-of-way, a wide landscaped boulevard at the back of curb, complete with street-lighting (with banner arms and electrical receptacles), a row of street trees (as per City requirements)
- A 3.5m wide shared pedestrian/bikeway with separate routes for pedestrians and bicycles, and
- Within a 1.5m wide strip along the street fronting of the property, a terraced landscape treatment complete with low-growing shrubs, groundcovers and an informally planted second row of trees



together with stairs and ramps (to access the interior of 'The Gardens' Village Street, at the core of the proposed development), other street furnishings such as benches and a bus shelter near the Steveston/No. 5 Road intersection.

A Landscaped Open Space Grid: The adjacent street frontages of Phase 1 have been ringed with a grid-like pattern of soft landscape planting reminiscent of the rectilinear aerial pattern view of agricultural land. The perimeter landscape general slopes up toward the buildings presenting a lush green pediment for the building and completely screening the parking structure under the buildings.

Public Outdoor Space: Complete development of 'The Gardens' will provide a range of publicly accessible, passive and active open space areas and features to bolster the recreational opportunities within the proposed development, the immediate area and Richmond in general. Complete development of the site will establish effective linkages between the development and the dedicated ALR 'Agricultural-Park'. Ultimately, the bulk of the outdoor amenity space will be provided by the approximately 12.2 acre ALR 'Agricultural Park'.

'The Gardens' overall site plan also proposes a public linear park/Mews within the total development site that will connect to the Agricultural Park. The open space network of 'The Gardens' will ultimately feature three separate connections to the future ALR 'Agricultural-Park', including the continuous 3.0 m wide ALR buffer strip.

Important outdoor public spaces associated with Phase 1 development:

Gateway Terrace: Phase 1 of 'The Gardens' provides a public plaza at the Steveston/No. 5 Road intersection, including a terraced water feature behind the plaza, defined by seat walls that wrap around the corner. This plaza is connected via a wide stairway and ramp system to an upper pedestrian plaza and the Village Street at the northwest corner of Building A on the parking podium level, which includes another water feature. The parking podium/Village Street level provides access to the retail plaza surrounding Buildings A and the large grocery store that will possibly occupy the lower level of Building B. See **Attachment 3** for a plan showing the proposed publicly accessible pedestrian corridors within the Phase 1 development site.

- **Transit Link:** There is a direct link between the proposed bus shelter on No. 5 Road and the retail plaza level on the parking podium that is well lit and defined with landscape plantings.
- **Seasonal Focus:** The proposed landscape design includes a 'Seasonal Plaza' on the parking podium at the west end of the internal east-west retail Village Street for businesses and residents to celebrate the changing seasons through festive features such as a communal Christmas Tree, seasonal plantings, banners and tree lights. In addition, this plaza space can also accommodate, on special occasions, such outdoor activities as a street market and/or outdoor dining.

### ***Affordable Housing***

Consistent with the rezoning considerations, the affordable house units will be provided in future development phases. Appropriate legal agreements are required in association with this Development Permit to ensure the provision of 5% of the total residential floor area for affordable housing units in the future.

### ***Public Art***

The Public Art Plan calls for a Phase 1 contribution of \$125,725.00 (60% of this amount for the Steveston/No. 5 Road pedestrian plaza and 40% for public art along Steveston Highway). The Public Art Plan was presented to and supported by the Richmond Public Art Advisory Committee on March 15, 2011. See **Attachment 4** for the relevant Richmond Public Art Advisory Committee minutes and **Attachment 5** for Phase 1 Public Art Plan.

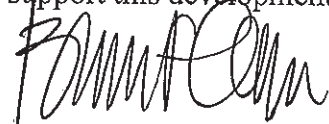
***Crime Prevention Through Environmental Design (CPTED)***

The following aspects have been considered in the proposed development design:

- The landscape design allows clear views of surrounding areas at the podium (commercial) and the courtyard (residential) levels through low shrub or groundcover planting and tree selections that permit views under the canopies
- The proposed development will be well lit at access points, along streets, commercial frontages and along all publicly accessible walkways.
- The design incorporates vision glass along commercial frontages, significant overlook areas from the residential units above and glazed doors in stairwells and parkade lobbies.
- The parking structure will be well lit and painted white to enhance visibility. The on-site mixed land uses help to ensure more eyes on the street at all times.
- The delineation of public, semi-public and private spaces has been incorporated to encourage a feeling of territoriality that will afford users and residents a sense of ownership and responsibility for these spaces.
- The building interiors will also be well lit. The design calls for non-glare, vandal resistant light fixtures that incorporate both motion-sensors and daylight-sensors into the design.

**Conclusion**

'The Gardens' – Phase 1 proposes a unique compact mixed-use transit-oriented development with a clear urban character within Richmond's suburban area that enhances pedestrian-friendly streetscapes and that addresses the gateway significance of the site, including a green landscape setting that is reminiscent of the former agricultural uses in the proximity of Highway 99, and provides opportunities for urban agriculture and supporting key sustainability objectives. Staff support this development permit application and recommends approval.



Brian Guzzi, CIP/CSLA  
Senior Planner (Urban Design)  
(4393)

BG:cas

- Attachment 1 Development Permit Data Sheet
- Attachment 2 Advisory Design Panel Excerpt of the Minutes and Applicant Responses
- Attachment 3 Publicly Accessible Pedestrian Walkways within the Phase 1 Development Site
- Attachment 4 Advisory Public Art Excerpted Minutes
- Attachment 5 Phase 1 Public Art Plan

- Plan # 1 Site Plans
- Plan # 2 Parking Plans
- Plan # 3 Landscape Plans
- Plan # 4 Building Elevations
- Plan # 5 Building Floor Plans

(over)



The following are to be met prior to forwarding this application to Council for approval:

1. Registration of a consolidated Phase 1 lot (Lot A) registered in the Land Title Office.
2. Receipt of cash in lieu (\$22,000.00) for 1 bus shelter to be located on site (along Highway 99 south of Steveston Highway).
3. Receipt of a Letter-of-Credit for the Phase 1 component of the overall Public Art Plan in the amount of \$125,725.00 (based on the estimate in the Public Art Plan).
4. Receipt of a Letter-of-Credit for landscaping in the amount of \$1,978,695.00 (based on a cost estimate from the registered Landscape Architect).
5. Registration of an appropriate cross access easement over all future development phases of the entire proposed development site to provide access to Phase 1 parking areas for the benefit of the Phase 1 development site.
6. Registration of a cross access easement over the entire indoor amenity space and all associated interior hallways of Building A within the Phase 1 development site to provide pedestrian access to and use of the indoor amenity space within Building A located on the Phase 1 development site for the benefit of all future development phases.
7. Registration of all appropriate Public Rights of Passage – Rights of Ways (PROP-ROW's) for pedestrian access to all publicly accessible walkways and commercial spaces within the Phase 1 development site for the benefit of general public access (reference **Attachment 3**).
8. Registration of a cross access easement over the commercial parking area and all associated vehicle access lanes within the Phase 1 development site for the benefit of all future development phases for the purposes of both commercial and visitor vehicle parking.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).



**DP 10-544504**

**Attachment 1**

Address: 12011 Steveston Highway and 10800 No. 5 Road

Applicant: Townline Gardens Inc. (dba The Gardens Joint Venture) Owner: Townline Gardens Inc.

Planning Area(s): Shellmont – Ironwood Sub-Area Plan

Floor Area Gross: 20,335 m<sup>2</sup> Floor Area Net: 18,872 m<sup>2</sup>

	Existing	Proposed
<b>Site Area:</b>	Consolidated Lot A (Phase 1) = 17,145.7 m <sup>2</sup>	Consolidated Lot A (Phase 1) = 17,145.7 m <sup>2</sup>
<b>Land Uses:</b>	Vacant	Residential / Commercial
<b>OCP Designation:</b>	Mixed Use / Limited Mixed Use	Mixed Use / Limited Mixed Use
<b>Zoning:</b>	Commercial Mixed Use (ZMU18) – The Gardens (Shellmont)	Commercial Mixed Use (ZMU18) – The Gardens (Shellmont)
<b>Number of Units:</b>	0	182

	Bylaw Requirement	Proposed	Variance
<b>Floor Area Ratio:</b>	Total Site Permitted FAR = 1.43 (1.0 plus 0.3 FAR Affordable Housing bonus plus 0.13 FAR Community Use bonus)	Total Development FAR = 1.29 Phase 1 FAR = 1.19	none permitted
<b>Lot Coverage:</b>	Max. 45%	36%	none
<b>Setbacks – Steveston Hwy:</b> – No 5 Road: – Highway 99: – Rear Yard: – Interior Roads: – Parkade - below: – ALR: – Riparian:	Min. 6 m Min. 6 m Min. 15 m Min. 3 m Min. 3 m Min. 1.5 m Min. 3 m Min. 12 m	Building B: Min. 7.7 m Building A: Min. 9.55 m na na Bldg. A: 4.9 m & Bldg. B: 5.4 m 1.52 m 3.05 m 15 m	none
<b>Setback – Rear Yard:</b>	Min. 3 m	na	none
<b>Height (m):</b>	Max. 20 m (not more than 4 storeys) Max. 25 m (not more than 6 storeys) if located more than 90 m from No. 5 Road	Building A 19 m (4 storeys) Building B – 19.5 m (4 storeys)	none
<b>Lot Size:</b>	Min. 3,000 m <sup>2</sup>	17,145.7 m <sup>2</sup>	none
<b>Phase 1 - Off-street Parking Residential/Commercial:</b>	247 Residential + 302 Commercial = 549	278 Resi. + 371 Com. = 649	none
<b>Off-street Loading Spaces:</b>	5 Medium + 3 Large Spaces	5 Medium + 3 Large Spaces	none
<b>Total Off-Street Parking:</b>	690 Residential + 302 Commercial = 992	713 Resi. + 393 Com. = 1,106	none
<b>Accessible Parking Spaces</b>	10	15	none
<b>Tandem Parking Spaces</b>	not permitted	none	none
<b>Bike Parking – Residential (Resi.)/Commercial (Com.):</b>	228 Class 1 + 36 Class 2 - Resi. 16 Class 1 + 22 Class 2 - Com.	262 Class 1 + 36 Class 2 - Resi. 25 Class 1 + 32 Class 2 - Com.	none
<b>Amenity Space – Indoor:</b>	Min. 100 m <sup>2</sup>	505 m <sup>2</sup>	none
<b>Amenity Space – Outdoor:</b>	Min. 1,092 m <sup>2</sup> (182 units x 6 m <sup>2</sup> / unit)	ALR = approximately 12.2 acres Phase 1 = 675 m <sup>2</sup>	none

**Excerpt from the Minutes from  
Richmond Advisory Design Panel Meeting**

**Wednesday, December 22, 2010 – 4:00 p.m.  
Rm. M.1.003  
Richmond City Hall**

‘The Gardens’ – Phase 1 (DP 10-544504) was presented to the Richmond Advisory Design Panel (ADP) on December 22, 2010. The ADP supported this application moving forward to the Development Permit Panel subject to the following recommendations. The ADP comments are included below followed by the Applicant’s Response in ***bold italics***.

**Panel Decision**

It was moved and seconded

That the Advisory Design Panel supports DP 10-544504 moving forward to the Development Permit Panel subject to:

- A. The applicant making the following improvements to the project design:
  1. design development to the east, west and south ground level elevations of Building B with consideration to visual penetration into the commercial volume, added glazing, pedestrian friendliness on the east façade, improved animation, opportunity for landscape to soften hard edges with vertical green elements and green screen particularly on the south and east elevations; ***The ground level elevations of Building B have been revised to include and expand glazing on the north west corner and the north east corner, planting and vertical green screens aligned with building features along the east wall, and a tree lined boulevard and sidewalk on the west side.***
  2. design development to the East-West spine from No. 5 Road (which is referred to as the Village Street) and its continuation to the sidewalk at No. 5 Road with consideration to integration of the ramps and stairs treatment giving emphasis to the pedestrian experience, consideration of landscaping materials and opportunities for tree planting, reconsidering the roundabout or the materials of the roundabout to de-emphasize the vehicular and emphasize the pedestrian priority; ***We have replaced the roundabout with a traditional intersection with raised unit paved crosswalk areas that emphasizes the pedestrian priority. By doing this we have been able to reduce the functional width of the southern portion of Road ‘A’, gain a wider sidewalk on the west side, and provide a boulevard separation between vehicular traffic and pedestrian flow through the site on both the west and east sides of Road ‘A’. This major change has allowed us to claim more of the space for pedestrians and greening strategies. Additional trees have been added along the north-south segment of Road ‘A’, at the intersection and along the east-west spine road in front of Building B.***
  3. design development to the expression of the indoor amenity space, wayfinding and access with consideration to weather protection and relationship with interior and exterior spaces; ***The entryway to the indoor amenity space has been recessed and an outdoor amenity space to the west of the amenity entrance has been claimed along Steveston Highway. Bike racks and a seating area have been added for the amenity space user. The glazing has been expanded along the Steveston frontage of the amenity and weather protection***



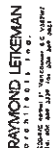
*exists along a portion of the facade. Signage associated with wayfinding to and from the amenity will be designed at a later stage. A widened sidewalk and relocated boulevard east of Building A now allows the resident to walk to the indoor amenity space more comfortably.*

4. design development to resolve the location and function of garbage and recycling disposal onsite; *Each residential building will have its own garbage disposal and recycling rooms in the parkade. Garbage will be serviced privately. Recycling bins will be brought up to a special holding area located in Building A's expanded loading area (along Road 'A') and will be serviced by the City from this point. Commercial garbage and recycling will be serviced at the loading areas for Buildings A and B.*
5. design development to provide village character in the interior elevations with consideration to i) wood detailing or colour treatments that are used elsewhere in the project, or ii) landscaping including paving landmarks, vertical landscape elements, and trees; *We have adjusted the parking layout and road to allow for a greater number of trees and landscaping along the east-west and north south internal road, and added distinctive raised crosswalk paving at the junction of these two internal roads. The pedestrian crossing at the lobby for Building B was widened to enhance the spacing of the trees along the food store frontage and to better align with the access to the green space between Buildings E1 and E2. Public art will further support the village character of this interior space. Glazed commercial openings with integrated overhead awning treatments have been placed on the north commercial street level frontage. Clear glazing is intended at the food store entry, along the length of the internal moving ramp and at the northeast commercial corner to provide a visual connection into the food store. Effort will be made with the future tenant to maintain as much clear glazing along the balance of the north frontage as possible to enhance the village character along the street.*
6. design development to ensure that pedestrian, vehicle and truck circulation onsite is reviewed by traffic consultant including i) concern re: bus stop location in proximity to the corner, ii) potential conflict of truck loading and vehicle and pedestrian access from Steveston Highway, and iii) truck turning in roundabout; *Our traffic consultant has prepared a detailed traffic study for the site and provided transportation engineering advice all through the design process. The current design is acceptable from a transportation engineering standpoint and no potential conflicts have been identified between loading, vehicle and pedestrian access. The bus stop has been shifted northward by 5m based on further consultation with TransLink. The elimination of the roundabout has streamlined the path for large trucks through the site and has allowed us to improve the pedestrian flow along Road 'A' by widening the boulevard and sidewalks with the extra space gained by decreasing the width of this portion of the internal road.*
7. design development to emphasize the importance of the gateway location at the corner with consideration to view from the East, lighting, colour, height, design features, and texture; *We have borrowed from the distinctive form and character of the expansive glazing form and treatment on the south west corner of the building to design the adjacent residential corner to address the views of the project from the east. The residential corner further gestures eastward through the use of projecting bays and a corresponding extended roof overhang. Public art will also form an integral part of this gateway location either being incorporated into the corner plaza and/or the building itself.*

8. design development to the plaza design at the corner and consider the possibility that that the likely animation or use would come from the west edge CRU and not at the corner and in view thereof i) reconsider stairs and ramps, ii) reconsider treatment at the corner, and iii) consider strengthening the water feature; *The stairs and sloped walkway have been redesigned to allow for larger landing space between flights and legs of the walkway. The water feature has been reduced slightly to allow for the extension of the sloped walkway into the space at grade.*
9. design development to define the gateway treatment to the park to the North and consider how to frame and transition to park, which could be done either through integration or creating a strong transition; *This item will be designed as part of the subsequent phases of the project.*
- B. The applicant taking into consideration the following comments:
  10. consider moving the density of townhouses in Building B along Steveston Highway to a subsequent phase; *We have retained these townhouses along Steveston Highway in Building B because it allows us to differentiate this building from Building A (from a massing and urban design standpoint) and it creates a quieter open space at the residential level since it is sheltered from the adjacent traffic noise.*
  11. consider the impact of traffic noise on the south-facing courtyards and consider introducing glazed elements to provide acoustic protection along Steveston Highway; *We have added a glazed railing system along the open areas of both Buildings A and a portion of Building B. Introducing large glazed walls would negate the design intent of the building to provide a viewing deck over the adjacent gateway area and would be counter to our approach to provide variety in massing and urban design along this corridor.*
  12. consider integrating public art in landscaping and project design which could provide focal point or landmark; *This approach is outlined in our public art plan – the provision has been made for both landmark public art as well as integrated public art flowing through the site.*
  13. consider access and potential isolation of CRU No. 7; *This CRU has been lowered so it is accessible. The location of this CRU will be suitable for a tenant that requires high visibility.*
  14. consider use of sliding doors to washrooms in the unit designs to increase accessibility and free up floor area (applies to all units) particularly the ensuite access through walk-in closets (between the bedroom and the bathroom); *Townline does not typically provide pocket doors to their tendency to break down and necessitate repairs over the long run. Also, our units are generally designed to maximize floor space such that the wall that the pocket doors would need to slide into is encumbered by other elements in the unit such as plumbing for the unit.*
  15. consider extending the agricultural character streetscape landscape treatment beyond the property line and out to the street edge with street trees to provide traffic calming. *This item was discussed with City staff and it is not their preference to have shrub planting along the boulevard in this location due to future maintenance concerns.*







and is subject to the same "no third transaction" rule as "covered" and "uncovered" securities. The Commission has stated that the "no third transaction" rule is designed to prevent a person from circumventing the rule by selling securities to a third party.

1

August 20, 2010  
Issued for Development Permit  
October 15, 2010  
Re-scheduled for Development Permit  
Issued for ADOP Review  
February 8, 2011  
Re-scheduled for Development Permit  
March 11, 2011  
Development Permit Classification  
April 15, 2011  
Re-scheduled for Development Permit



Townline Homes

# Phase One

No. 5 Road & Stevenson Highway  
Richmond, B.C.

Building B  
Main Floor Plan

16-10  
10/15/2011

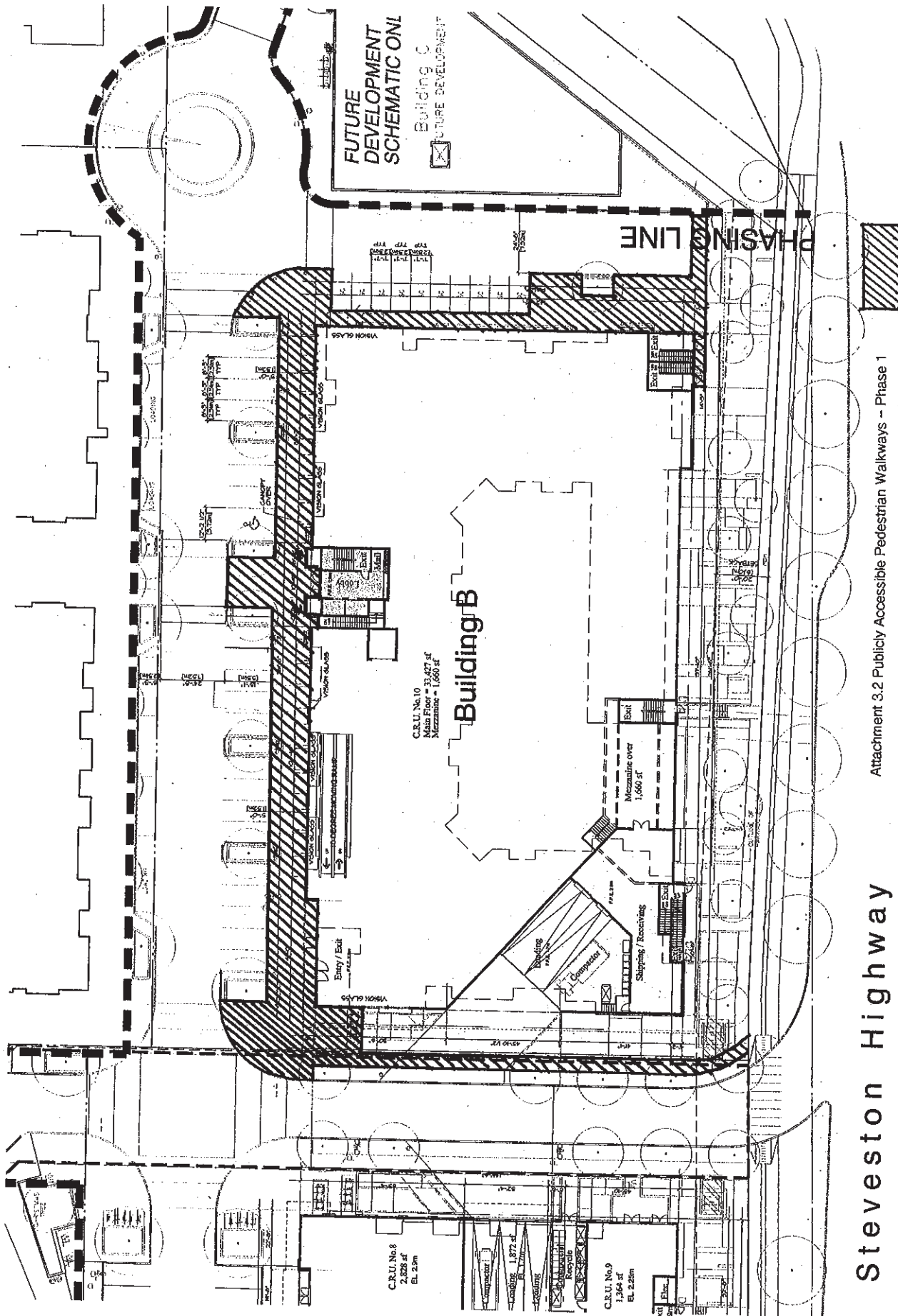
Attachment 3.2 Publicly Accessible Pedestrian Walkways - Phase 1

# Steveston Highway

## Building B Main Floor Plan



SK-5.0





Excerpt from the Minutes from  
**Richmond Public Art Advisory Committee Meeting**

Wednesday, March 15, 2011 – 4:00 p.m.

Rm. M.1.003

Richmond City Hall4

‘The Gardens’ Public Art Plan was presented to the Richmond Public Art Advisory Committee (RPAAC) on March 15, 2011. From the minutes of this presentation it was noted that:

- The upper portion of this project will be retained as a garden.
- Extensive public workshops have occurred to get a feel for what the community wanted for this site.
- During this consultation process the following themes emerged; improving the “gateway” image, the garden city theme, including a botanical garden, and having a mixed-use village.
- The Guiding Principles for this project include delivering this site in accordance with the Richmond Official Community Plan, following the guidelines for public art, leaving a legacy for Richmond, no loss of agricultural land, transform to an attractive “front door”, executing with high creativity and excellence, and including projects that are fun, interactive and easy to understand.
- Discussion ensued on the different levels of understanding (of public art) within the community and the possible inclusion of some form of assistance to broaden the understanding of these pieces.
- This will be a 5-phase project; however, most of the public art will be occurring in phase 1. It was noted that the themes that have been outlined for this phase could also be used for future public art. It was also noted that each phase will likely have a piece of public art associated with it.
- The budget for phase 1 is \$125,000.
- Phase 1 proposes allocating 60% of the budget public art at the corner of Steveston Highway at No. 5 Road and 40% to the Steveston Highway frontage. There may be flexibility in the allocation, which will be finalized at the Artist Call stage. It was noted the preference for using this corner for building a landmark and capturing a sense of identity.
- The Committee recommends having one artist call for the two opportunities in preference to two separate calls. That would allow artists to make proposals for one or both sites. It was recommended that the term “gateway” be featured in the call.

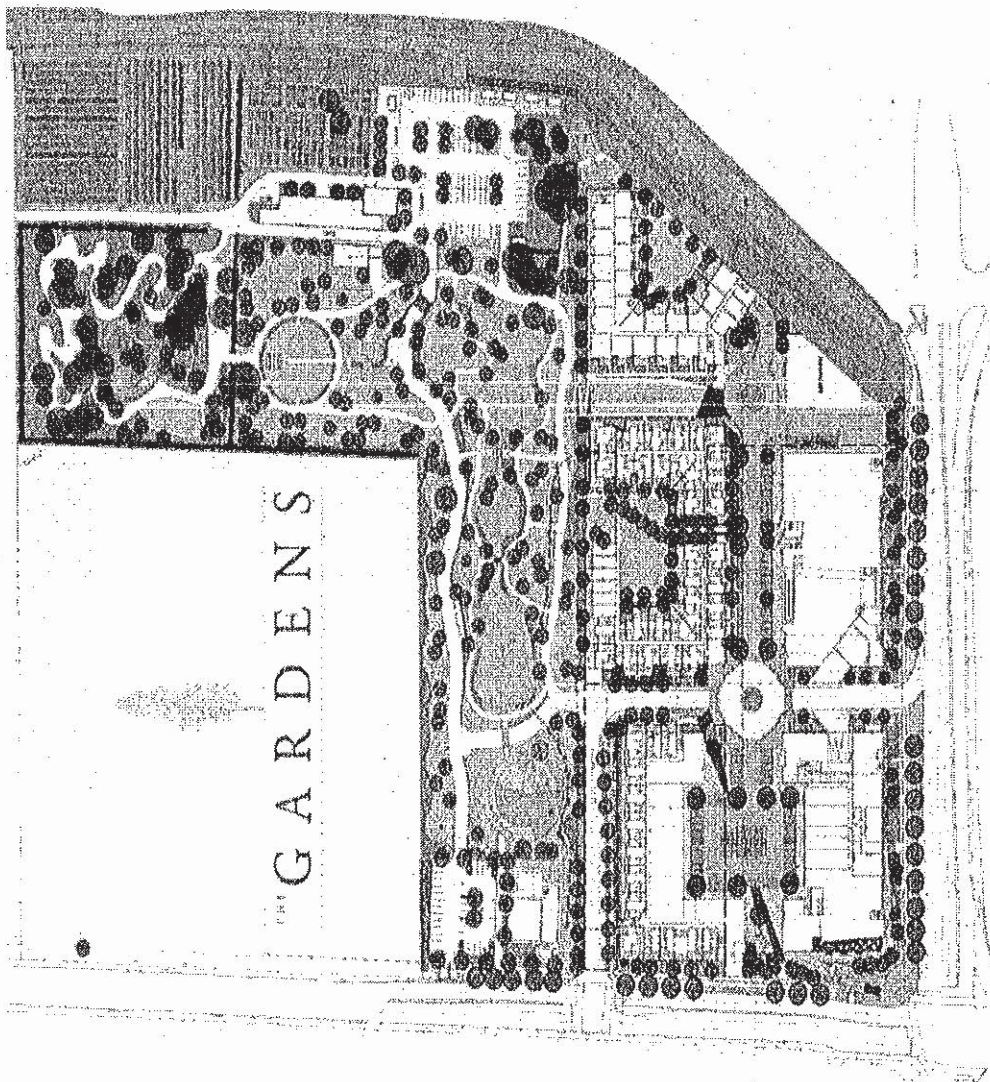
It was moved and seconded

*That the Richmond Public Art Advisory Committee endorse the public art concept presented for The Gardens on March 15<sup>th</sup> 2011 and recommended that a combined call for artist/artist teams for the two opportunities in Phase 1 be made.*

**CARRIED (unanimously)**

# The "Gardens" Public Art Plan

Richmond, BC  
March 15, 2011





## VISION

The extensive public workshops for this project indicated that the public had a vision for this site. The vision was:

- To improve the appearance of this
- gateway to Richmond by greening this
- entrance to the City as part of the
- Garden City theme;
- To ensure public access to the old
- botanical garden;
- Functional and attractive improvement to
- the intersection of No. 5 Road;
- A mixed use village with a garden theme
- and a strong sense of place.



# GUIDING PRINCIPLES

The guiding principles for the Public Art Plan is to create and deliver an inclusive program that will successfully embody the spirit of the neighbourhood. The guiding principles are:

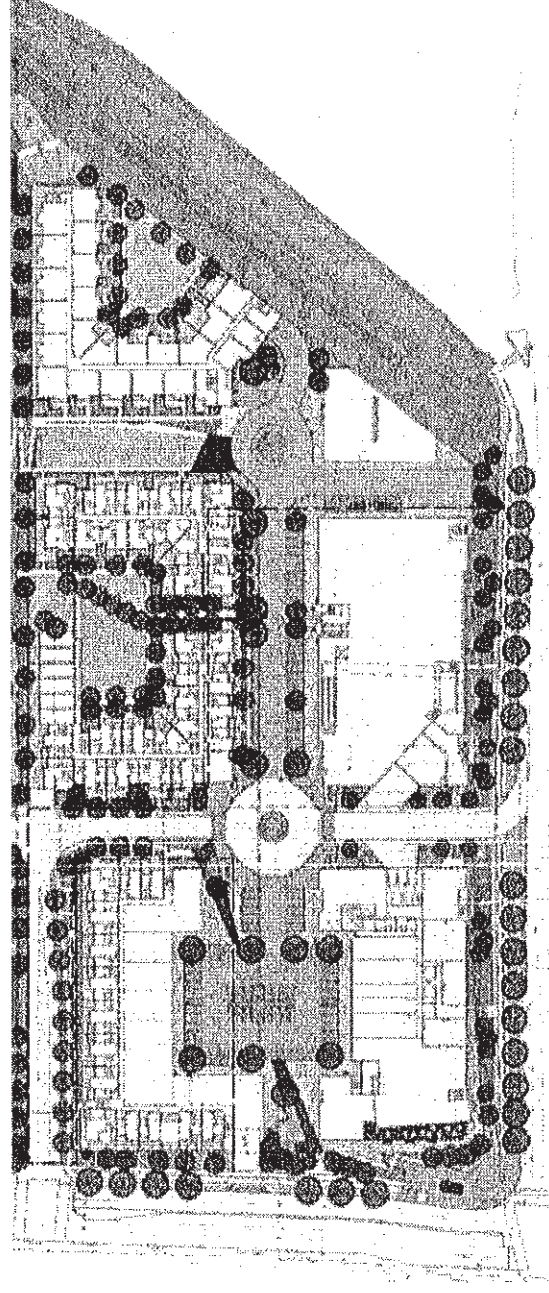
- To plan and deliver the site in accordance with the City of Richmond 's Official Community Plan and the vision developed with the community;
- To provide a legacy for Richmond;
- To ensure that there is no net loss of agricultural land
- To transform this area to an attractive frontdoor entrance ;
- To ensure that there are not any big/box format super stores;
- To create projects of the highest level of creativity and excellence;
- To commission the best artists, locally, regionally, nationally and internationally;
- To consider projects that are fun, interactive and easy to understand.



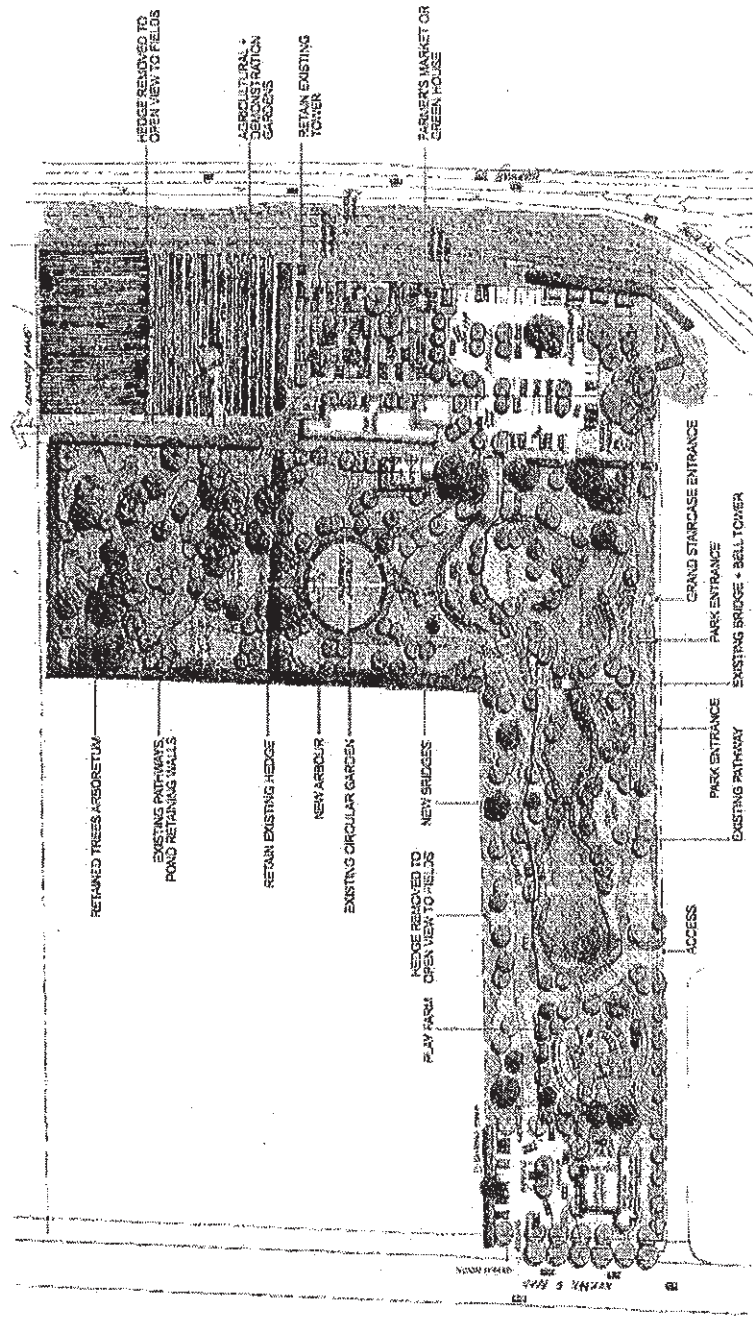


# THEMES

- The Garden City - "Green" theme as the unifying character - return the gardens to the community.
- Transformation of this southerly gateway to Richmond from a "back door" to a "front door entrance" showcasing and celebrating urban agriculture and gardening.
- Down to Earth - the 100 metre diet - Food production as part of infrastructure.
- Creation of a mixed use village with a variety of uses and styles woven together with a strong pedestrian environment to create a neighbourhood.

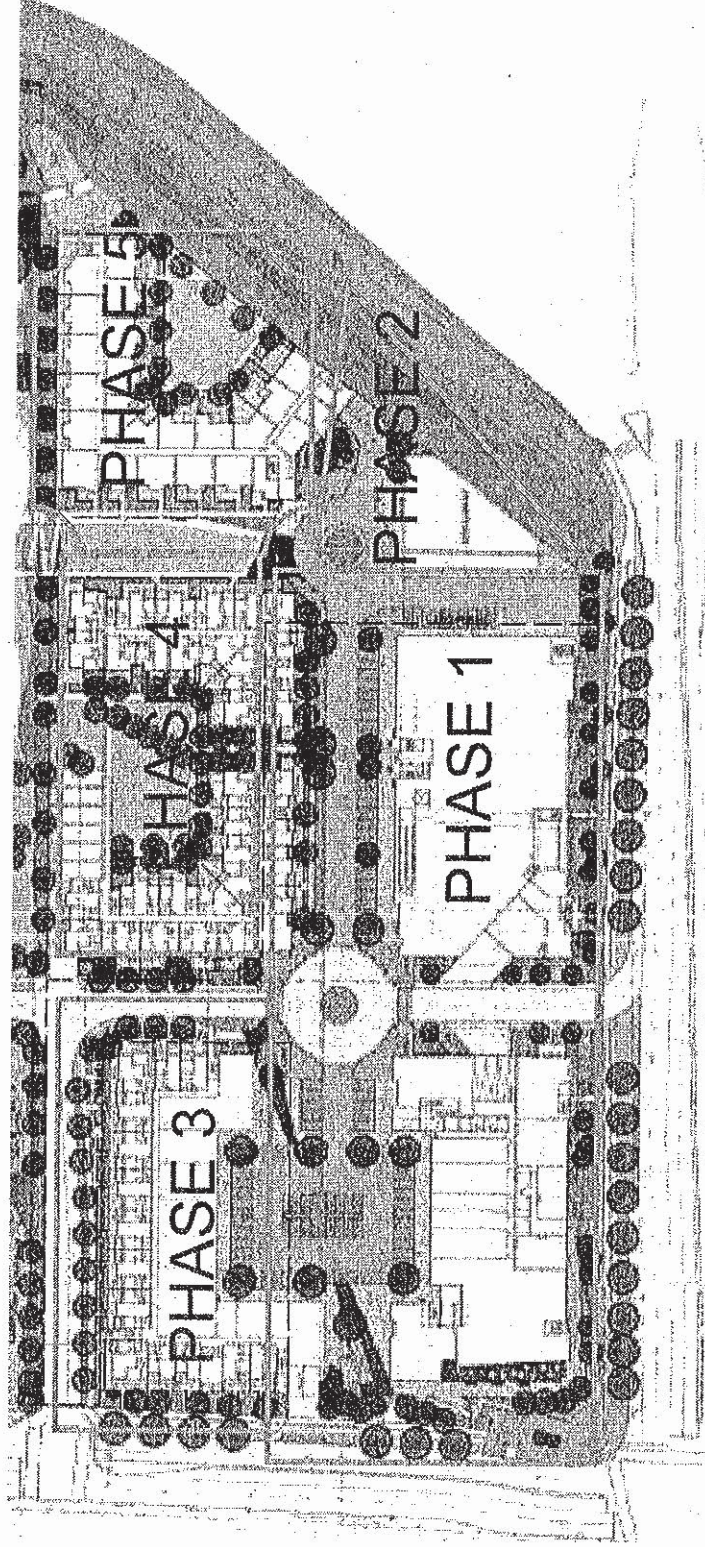


- A healthy and spiritual place to learn, discover and connect with people and the earth
- A seamless collection of gardens.
- An additional theme may reference the geology of the site.





# PHASING



Public Art will be installed in Phase 1

# BUDGET

Total Estimated Contribution +/- = \$125,724.39

## Proposed Contribution Allocation:

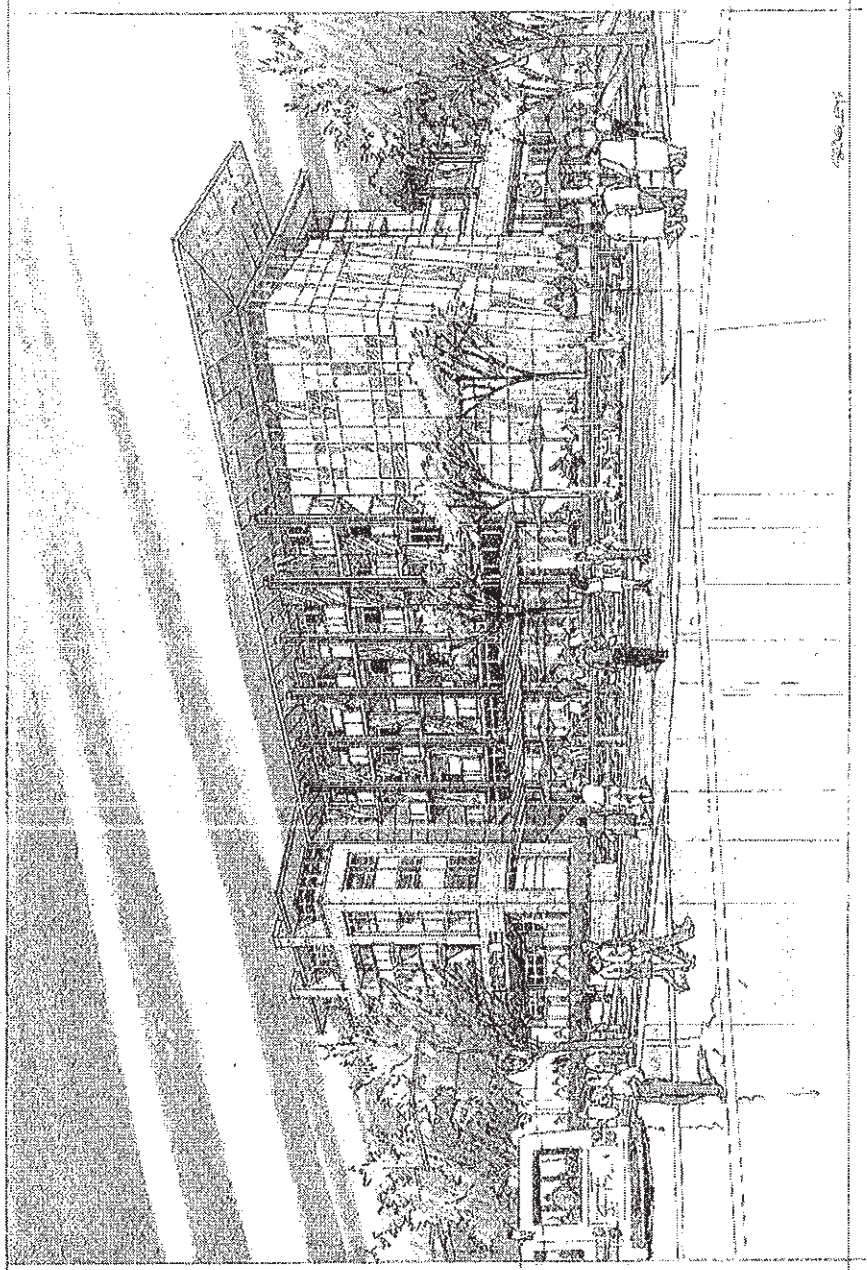
- 60% Towards art located at the corner of No. 5 Road and Steveston Highway.
- 40% Towards art located prominently along Steveston Highway.
- No more than 10% the of total contribution will be used on a public art consultant to assist Townline and Bob Rennie and Associates.

In Phase One of this multi-phased development, the art will be located at the corner of No. 5 Road and Steveston Highway as well as prominently integrated into a building along Steveston Highway. It is anticipated that 60% of the contribution will be spent on the corner of No. 5 Road and 40% on the building along Steveston Highway.

Note: Density does not include Affordable Housing or Daycare



# Thank you





# City of Richmond

## Development Permit

**No. DP 10-544504**

To the Holder: TOWNLINE GARDENS INC. (DBA THE GARDENS JOINT VENTURE

Property Address: 12011 STEVESTON HIGHWAY AND 10800 NO. 5 ROAD

Address: 120-13575 COMMERCE PARKWAY,  
RICHMOND, BC V6V 2L1

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #5 attached hereto.
4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$1,978,695.00 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
7. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof



To the Holder: TOWNLINE GARDENS INC. (DBA THE GARDENS JOINT VENTURE

Property Address: 12011 STEVESTON HIGHWAY AND 10800 NO. 5 ROAD

Address: 120-13575 COMMERCE PARKWAY,  
RICHMOND, BC V6V 2L1

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This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO. ISSUED BY THE COUNCIL THE  
DAY OF , .

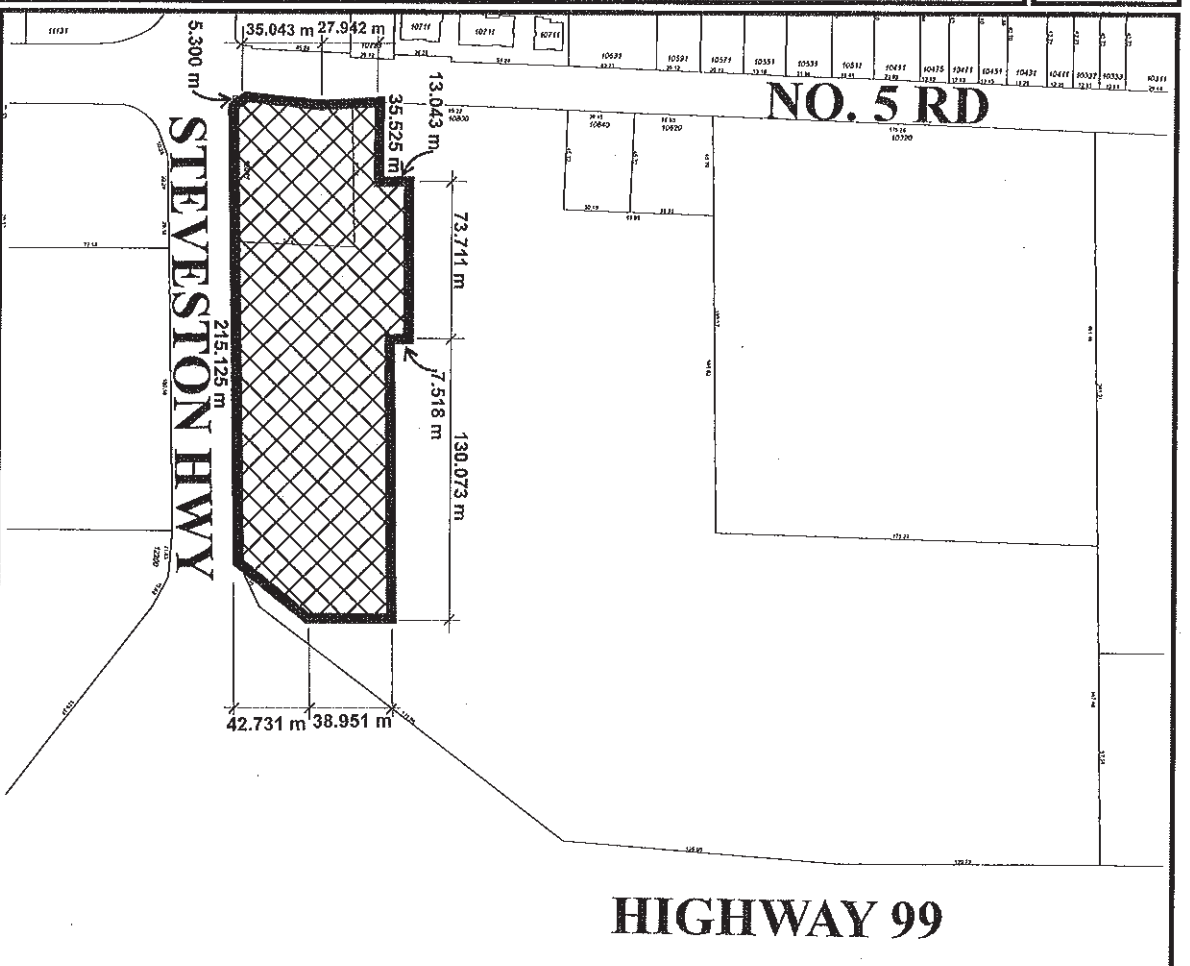
DELIVERED THIS DAY OF , .

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MAYOR



# City of Richmond



## DP 10-544504 SCHEDULE "A"

Original Date: 09/21/10

Revision Date: 04/27/11

Note: Dimensions are in METRES





Revised:

August 26, 2010	Issued for Development Permit
December 15, 2010	Re-issued for Development Permit
February 8, 2011	Re-issued for Development Permit
March 11, 2011	Development Permit Classifications
April 15, 2011	Re-issued for Development Permit

#

APR 20 2011

1057504



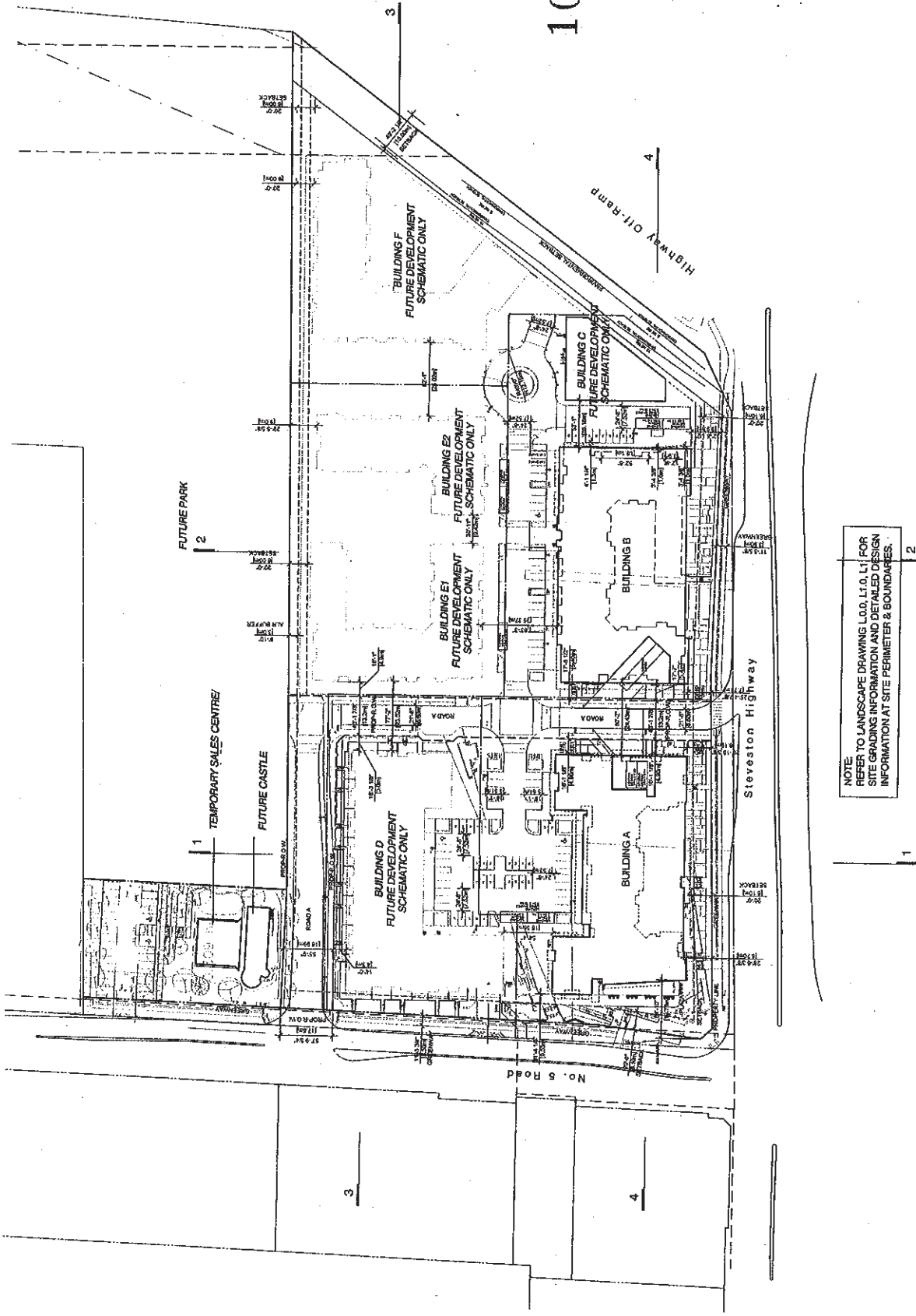
## Townline Homes

"GARDENS  
Phase One

No. 5 Road & Stevenson Highway  
Richmond, S.C.

Overall Site Plan -  
Main Floor  
1" = 50'-0"  
April 15, 2011

SK-2.0



# Overall Site Plan - Main Floor





RAYMOND LEIKEMA  
ARCHITECTS, INC.  
1000 10th Avenue, Suite 200  
St. Paul, MN 55102  
Tel: 651.224.1234 Fax: 651.224.1235  
www.raymondleikema.com

Revisions:  
August 26, 2009  
Revised for Development Permit  
December 15, 2010  
Revised for Development Permit  
February 18, 2011  
Revised for Development Permit  
March 11, 2011  
Development Permit Application  
April 15, 2011  
Revised for Development Permit

#1.2

APR 29 2011

10544504



Townline Home

GARDENS  
Phase One

No. 5 Road & Stevenson Highway  
Northwood, B.C.

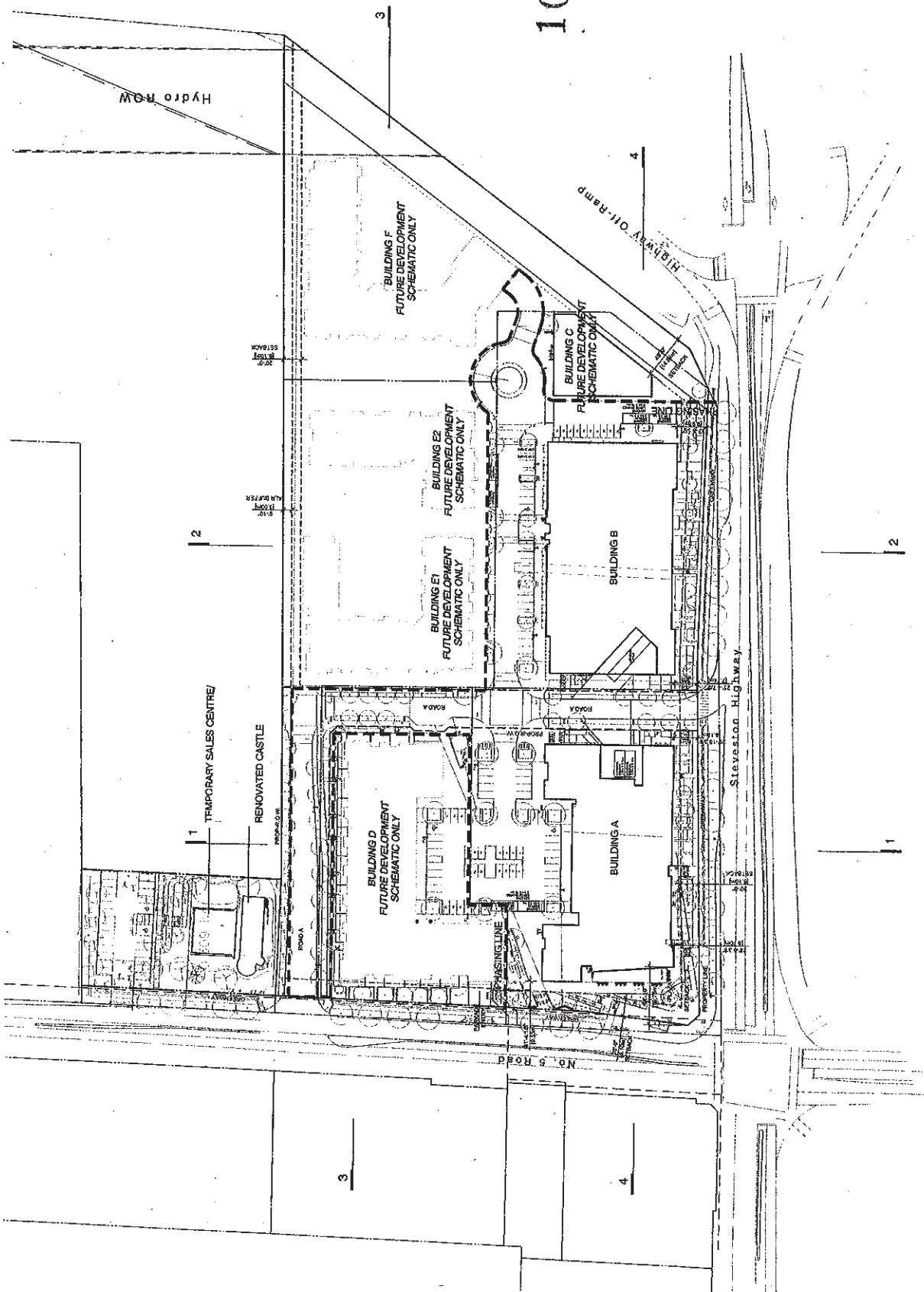
Phase One  
Site Plan - Main F  
1:1 = 50' 0"  
April 15, 2011



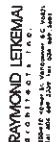
# Phase One Site Plan - Main Floor



SK-3.2







• *John and Don: Our Champion the 10-Speed All-Velocipeder* (1938) illustrated by John and Don: Incorporating various kinds of machinery.

August 26, 2010  
Reviewed for Development Permit

December 15, 2010  
Reviewed for Development Permit  
Submitted for ADP Review

February 8, 2011  
Reviewed for Development Permit

March 11, 2011  
Development Permit Clarifications

July 15, 2011  
Reviewed for Development Permit

3.  
#

APR 20 2011

105450



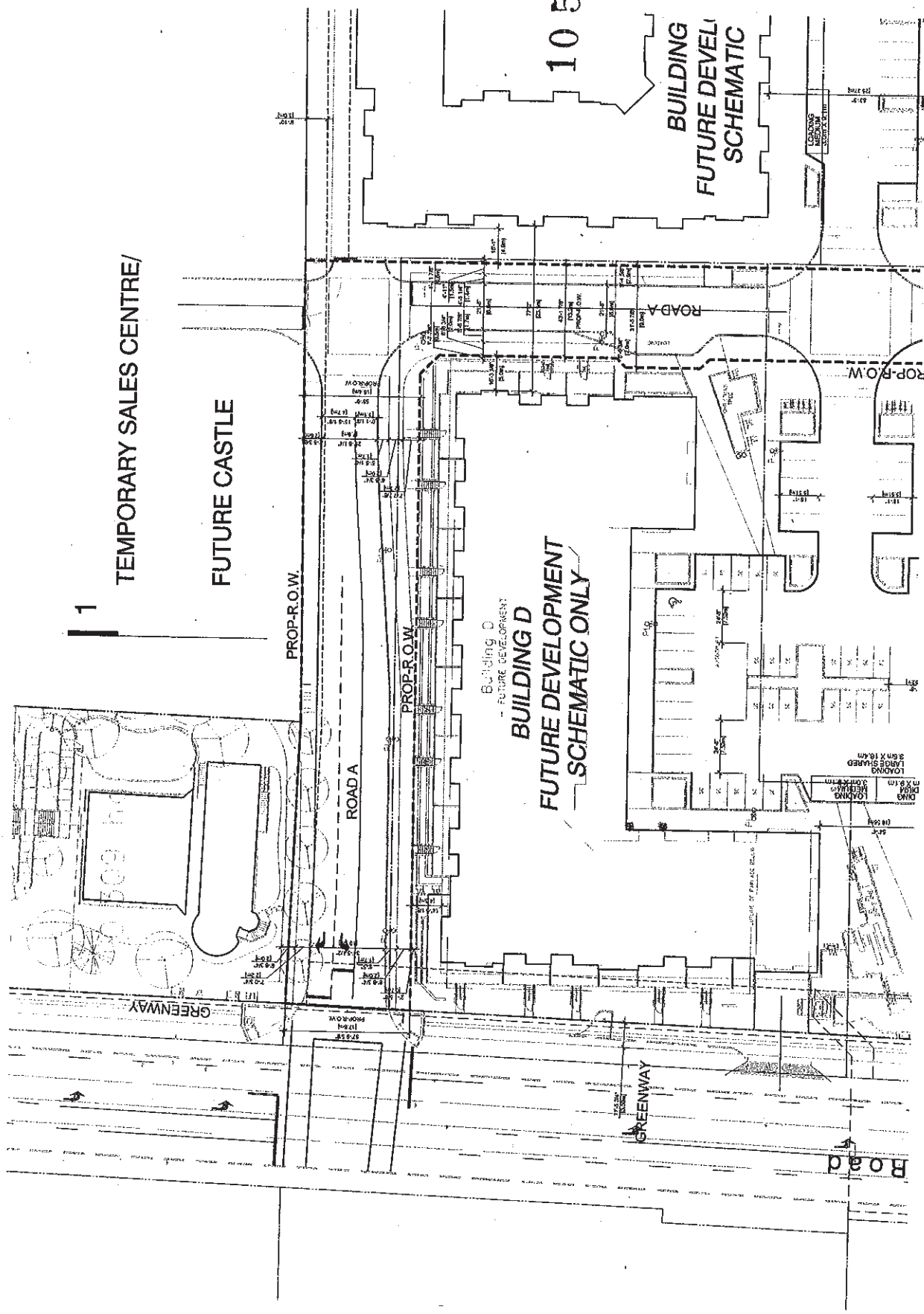
### Townline Home:

GARDENS  
Phase One

Box 5 Road & Steveston Highway,  
Richmond, B.C.

Overall Site Plan -  
Road A  
= 20'-0"  
April 15, 2011

SK-2.0.



# Overall Site Plan - Road A



RAYMOND LETKEMA  
CONSULTANTS INC.  
10000 14th Avenue, Suite 200  
Edmonton, Alberta T6C 3K1  
Tel: 780.443.2222 Fax: 780.443.2223  
www.letkema.com

August 16, 2010  
Approved for Development Permit  
October 14, 2010  
Revised for Development Permit  
Issued for ADP Review  
February 8, 2011  
Approved for Development Permit  
March 11, 2011  
Development Permit Conditions  
April 13, 2011  
Revised for Development Permit

#1.4

APR 20 2011

10544504



Townline Homes

GARDENS  
Phase One

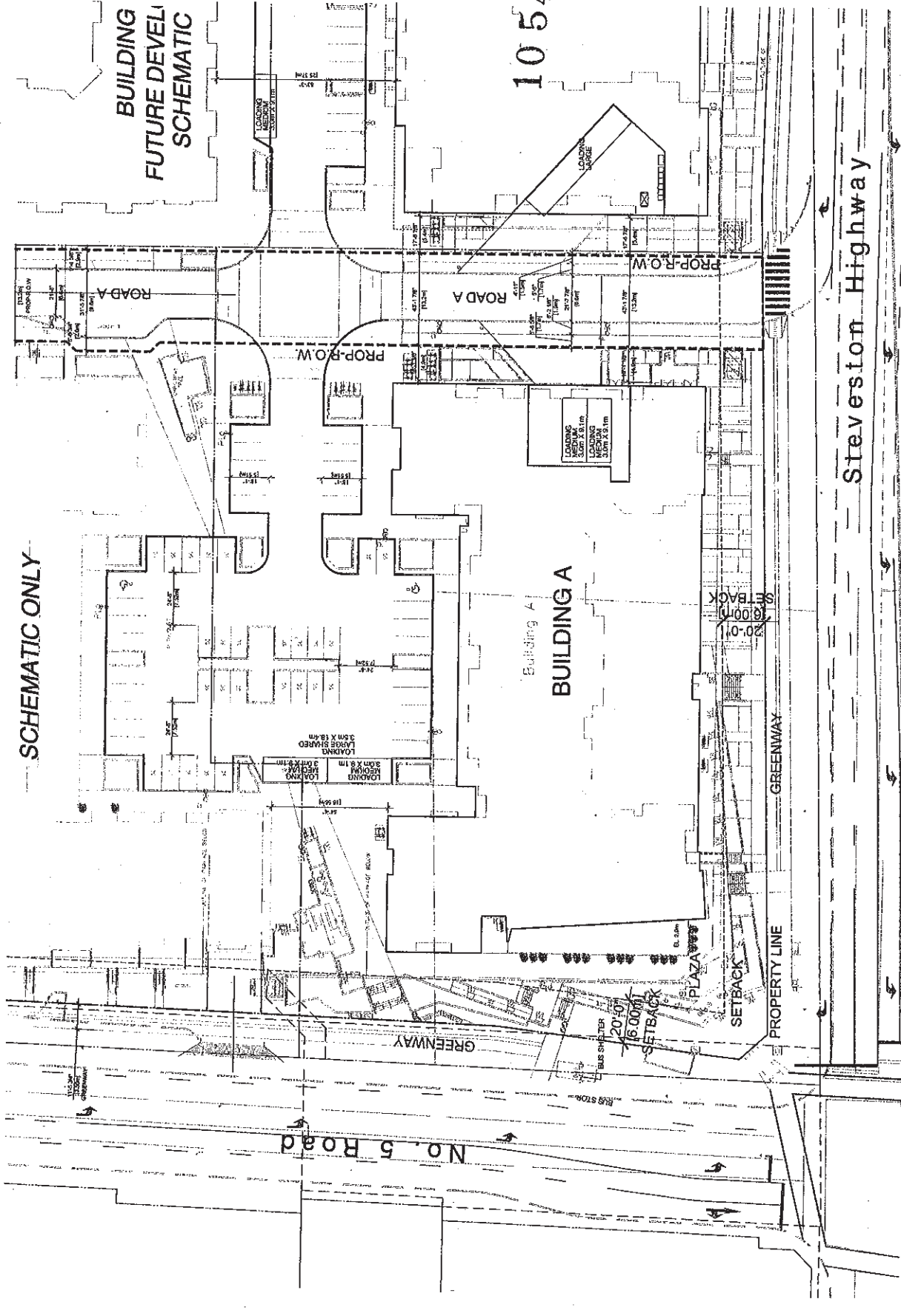
No. 5 Road & Steveston Highway  
Edmonton, AB

Overall Site Plan -  
Road A  
1" = 20' 0"  
April 15, 2011

































































































# Overall Site Plan - Road A

SK-2.0.1







Reviewers	August 26, 2010 (Issued for Development Permit)	December 15, 2010 (Re-issued for Development Permit Issued for ALUP Review)	February 8, 2011 (Re-issued for Development Permit)	March 11, 2011 (Development Permit Clarifications)	April 15, 2011 (Re-issued for Development Permit)
<p>   David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith    David J. Smith  </p>					

12#

100-443887-100

074504



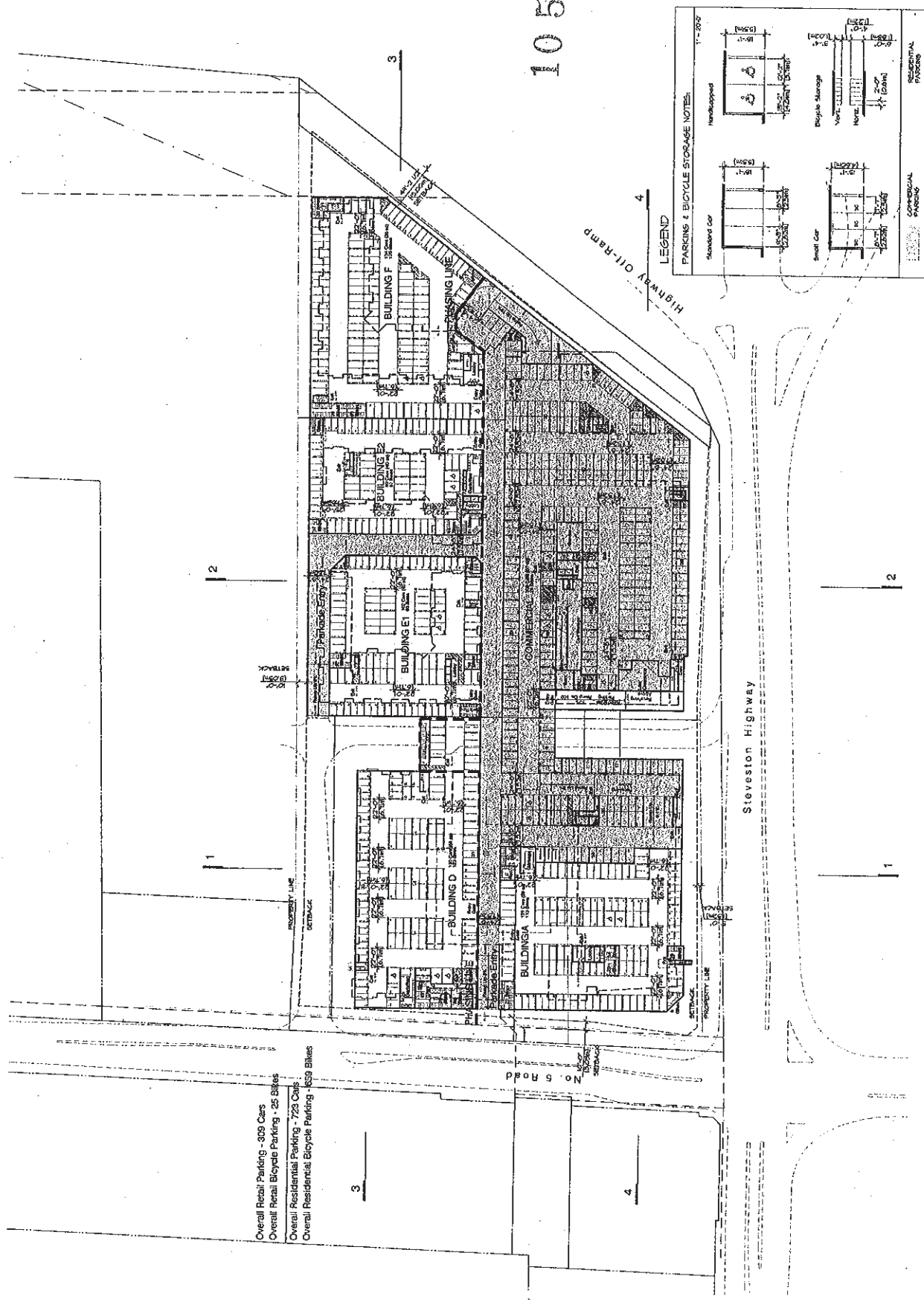
Townline Homes

GARDENS  
Phase One

No. 5 Road & Stevenson Highway  
Richmond, B.C.

Overall  
Parkade Plan  
1" = 50'-0"  
April 15, 2011

SK-2.1



# Overall Parkade Plan





**RAYMOND LEIKEMA**  
ARCHITECTS INC.

1000 West 1st Avenue, Suite 100  
Vancouver, BC V6C 1A5  
Tel: 604.681.1111 Fax: 604.681.1112  
www.leikema.com

Revisions:  
August 16, 2010 Issued for Development Permit  
December 15, 2010 Reissued for Development Permit  
February 1, 2011 Reissued for Development Permit  
March 11, 2011 Development Permit Application  
April 15, 2011 Reissued for Development Permit

#2.2

APR 29 2011

10544504



Townline Homes

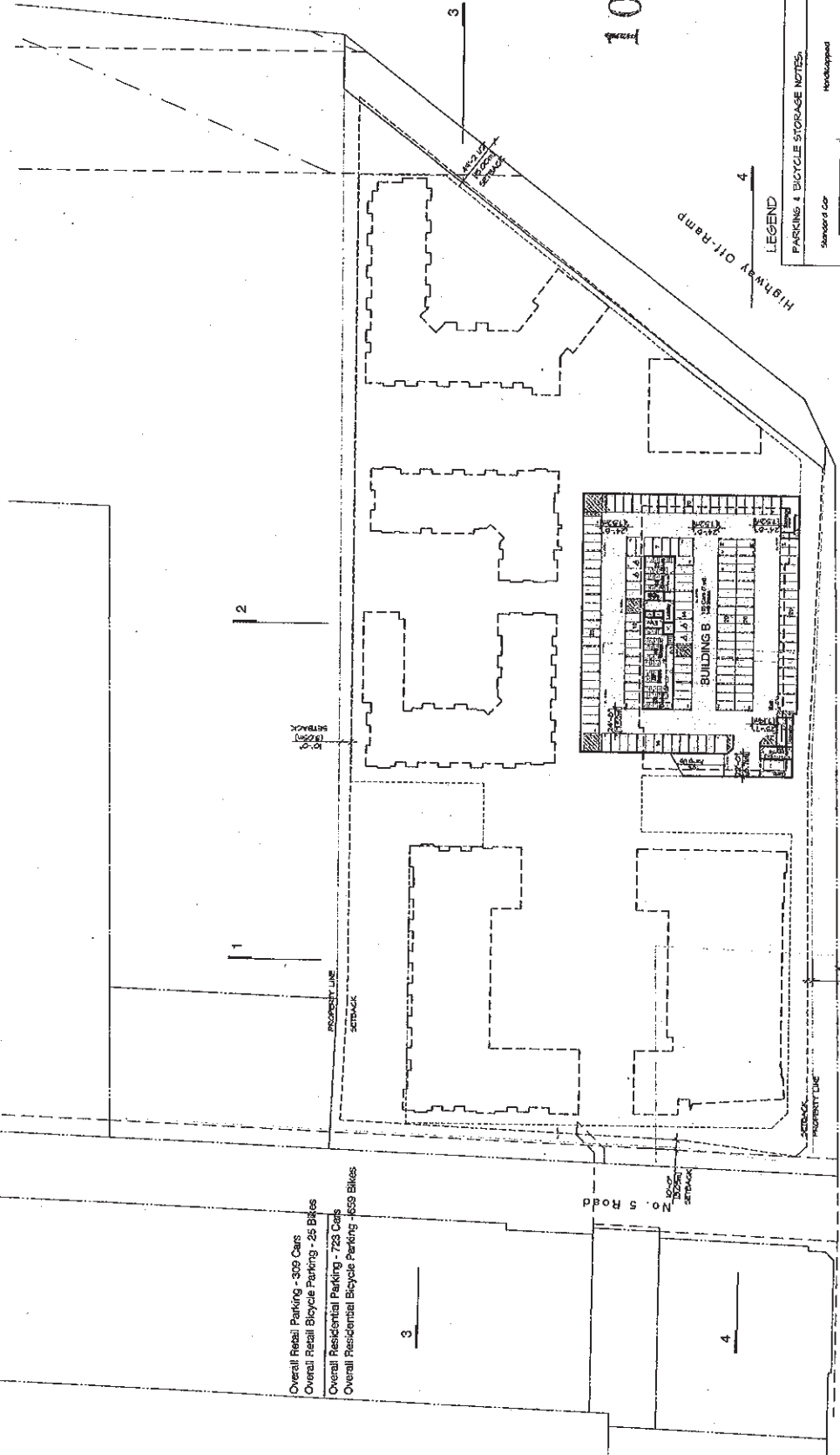
GARDENS  
Phase One

No. 5 Block 5 Steveston Highway  
Richmond, B.C.

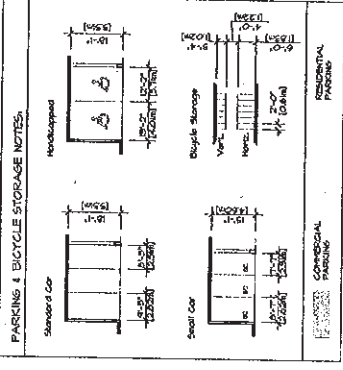
Overall  
P2 Parkade Plan

April 15, 2011

SK-2.2



Overall Retail Parking - 309 Cars  
Overall Retail Bicycle Parking - 25 Bikes  
Overall Residential Parking - 723 Cars  
Overall Residential Bicycle Parking - 659 Bikes



# Overall P2 Parkade Plan





**RAYMOND LEIKEMA & ASSOCIATES**  
ARCHITECTS

10000 Highway 101, Suite 200  
Steveston, BC V9A 5G5  
Tel: 250.866.1111 Fax: 250.866.1112  
www.raymondleikema.com

Project No. 2019  
Project Name: Steveston P2  
Development Permit  
Issued for Development Permit  
Issued for ALUP Review  
February 1, 2021  
March 11, 2021  
Development Permit  
Development Permit  
April 13, 2021  
Revised for Development Permit

#2,3

APR 20 2021

1054504



Townline Home

GARDENS  
Phase One

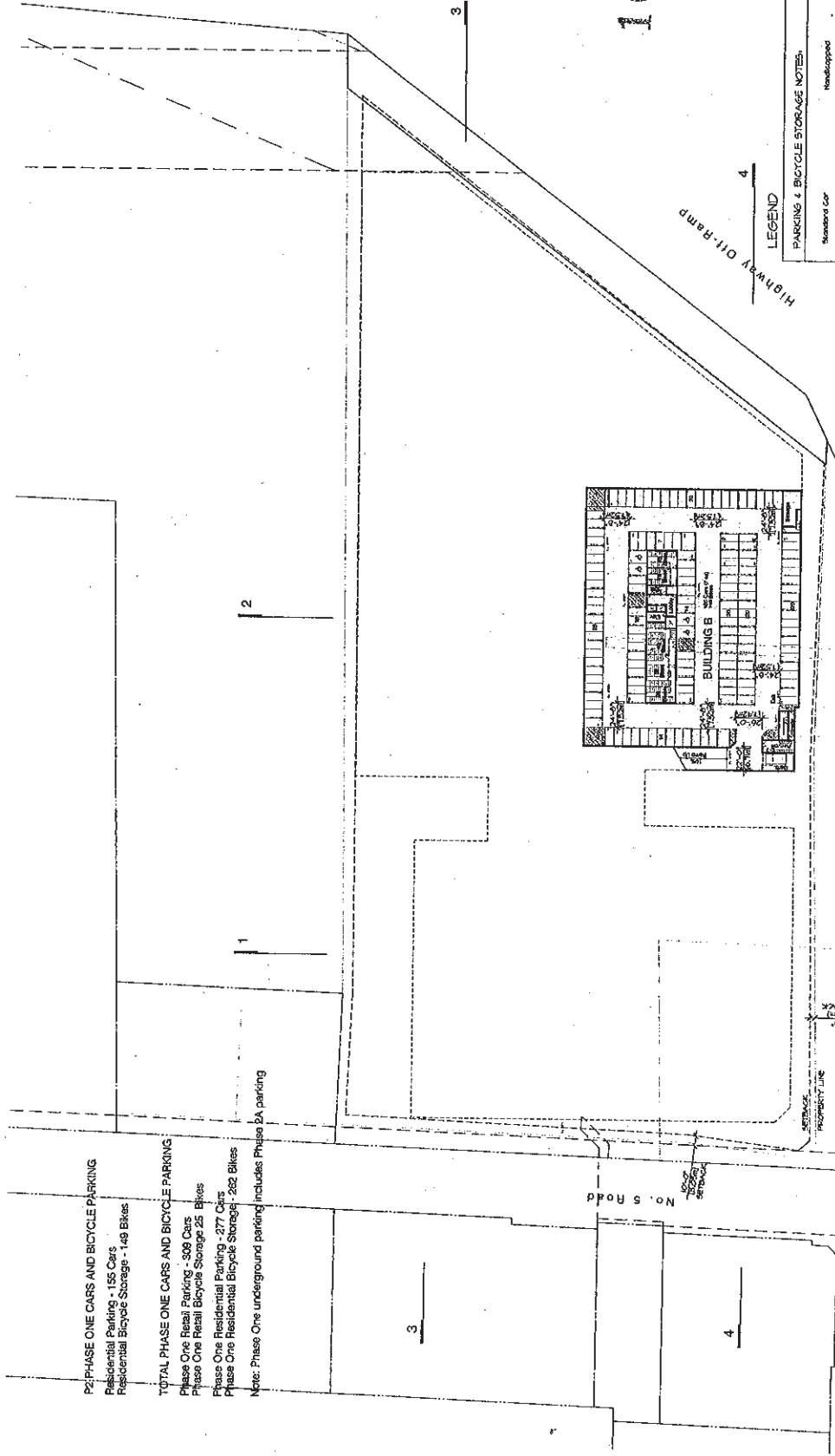
No. 5 Road & Steveston Highway  
Steveston, B.C.

Phase One  
P2 Parkade Plan  
11-09-21  
April 12, 2021

SK-3.0



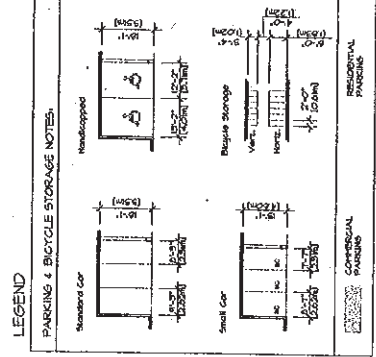
# Phase One P2 Parkade Plan



**P2 PHASE ONE CARS AND BICYCLE PARKING**  
Residential Parking - 155 Cars  
Residential Bicycle Storage - 148 Bikes

**TOTAL PHASE ONE CARS AND BICYCLE PARKING**  
Phase One Retail Parking - 308 Cars  
Phase One Retail Bicycle Storage - 25 Bikes  
Phase One Residential Parking - 277 Cars  
Phase One Residential Bicycle Storage - 282 Bikes

Note: Phase One underground parking includes Phase 2A parking





**RAYMOND LEFKEMA**  
 ARCHITECTS  
 1000 S. 1st Street, Suite 200  
 Richmond, B.C. V6Y 1A1  
 Tel: 604.278.2222 Fax: 604.278.2221

August 26, 2010  
 Issued for Development Permit  
 November 15, 2010  
 Issued for Development Permit  
 November 15, 2010  
 Issued for Development Permit  
 February 8, 2011  
 Re-submitted for Development Permit  
 March 11, 2011  
 Development Permit Consideration  
 April 15, 2011  
 Approved for Development Permit  
 Approved for Development Permit

#2.4

APR 20 2011

1054504



Townline Homes

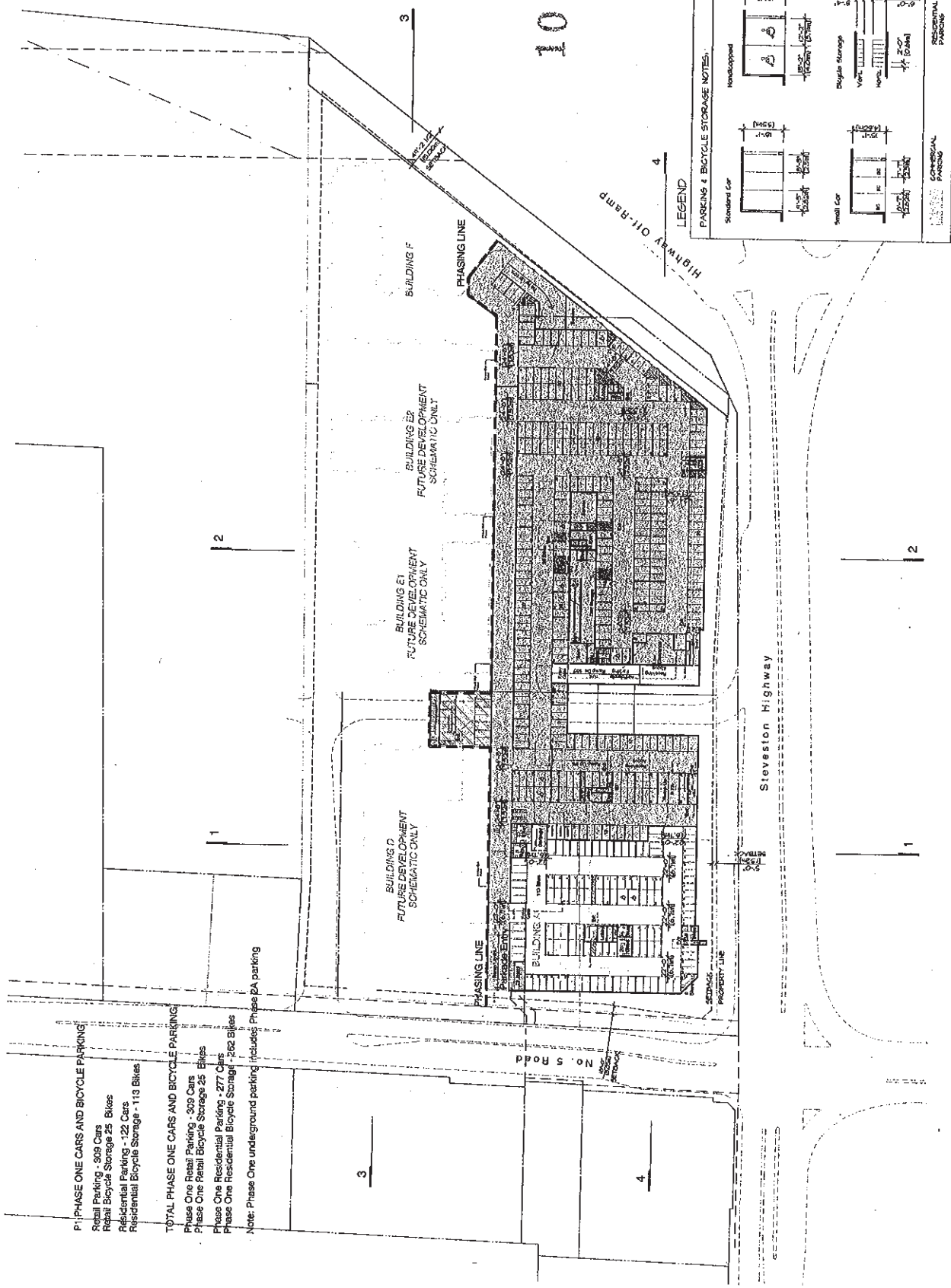
GARDENS  
 Phase One

No. 5 Road & Steveston Highway  
 Richmond, B.C.

Phase One  
 Parkade Plan

1" = 50'-0"

SK-3.1



# Phase One Parkade Plan







P+A

Project: 10544504  
Location: 10544504  
Scale: 1/8" = 1'-0"

#3.2

APR 20 2011

10544504

- 1. Release for DP
- 2. Release for Review
- 3. Release for DP
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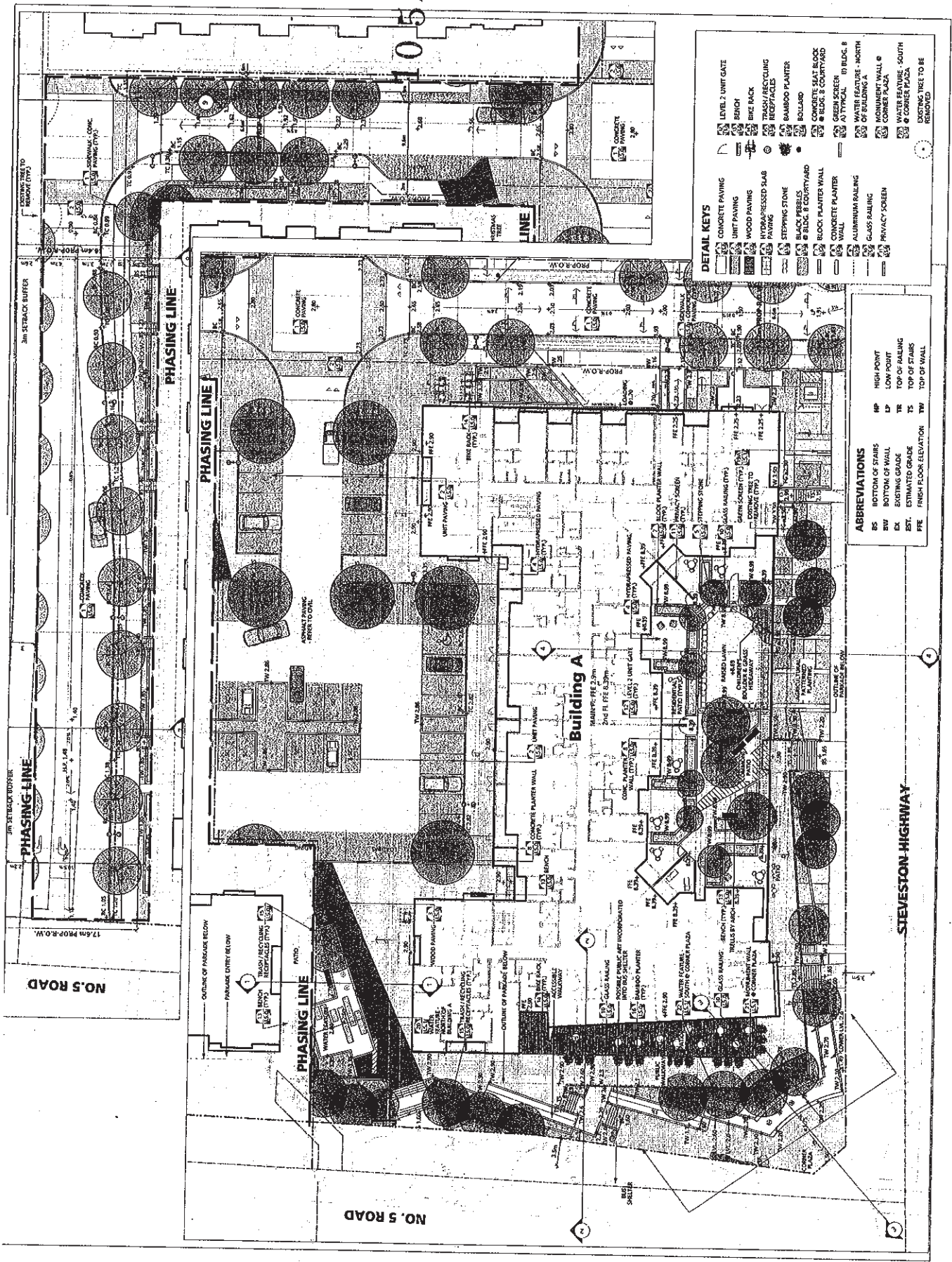


Townline Homes

**GARDENS**  
The Gardens - Phase 1  
Building A / Road A  
Key Plan

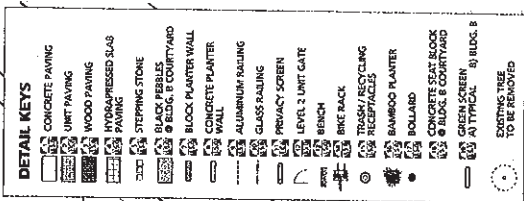
Project No: 10544504  
Scale: 1/8" = 1'-0"  
Sheet No: 10544504

L1.0



- DETAIL KEYS**
- CONCRETE PAVING
  - UNIT PAVING
  - WOOD PAVING
  - HYDRA-PRESSED SLAB
  - STEPPING STONE
  - BLACK PEBBLES
  - CONCRETE PLANTER
  - ALUMINUM RAILING
  - GLASS RAILING
  - PRIVACY SCREEN
  - WATER FEATURE - NORTH
  - WATER FEATURE - SOUTH
  - WATER FEATURE - CORNER PLAZA
  - WATER FEATURE - CORNER PLAZA
  - EXISTING TREE TO BE REMOVED

- ABBREVIATIONS**
- HP HIGH POINT
  - BP BOTTOM OF STAIRS
  - SW BOTTOM OF WALL
  - EX EXISTING GRADE
  - EST. ESTIMATED GRADE
  - FEE FINISH FLOOR ELEVATION
  - TMW TOP OF WALL





43#

AD 235

ॐ नमो भगवते वासुदेवाय

8	Reissue for DP	04/15
7	Issue for Review	03/25
6	Issue for DP Amendment	02/08
5	Reissue for DP	12/15
4	Issue for DP	09/26
3	Issue for Review	08/09
2	Issue for Review	03/04
1	Issue for Review	07/28
Revision No.		0



## Townline Homes

Product Y/1hr

GARDENS

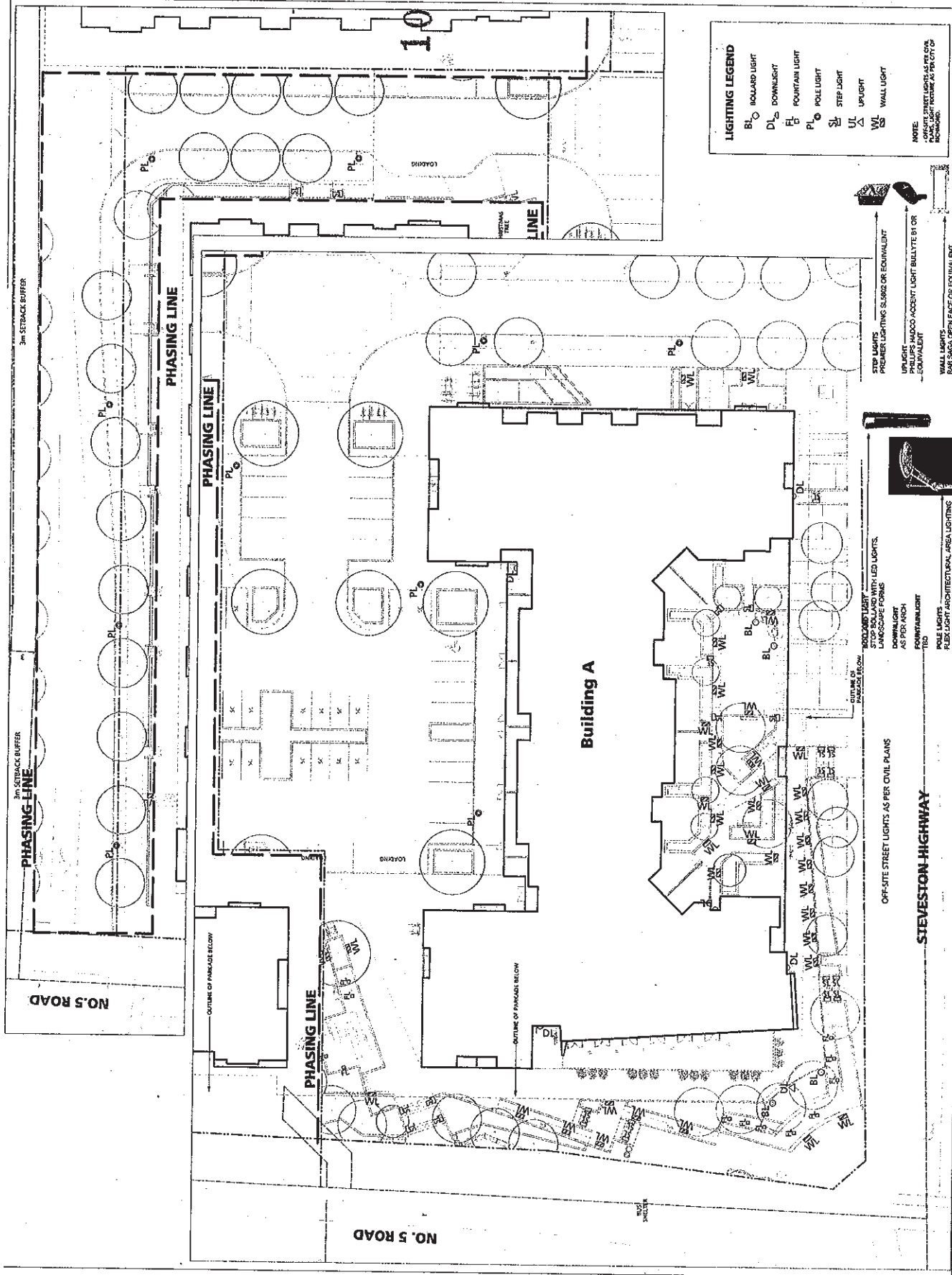
**The Gardens - Phase 1**  
Drawing Title:  
**Building A / Road A**  
**Lighting**

Project Number

Checked by: \_\_\_\_\_

Scale:	Job No.:	10-01
	1:200	

003





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1054504

8	Reissue for DP	04/15
7	Issue for Review	03/05
6	Issue for DP Amendment	02/08
5	Reissue for DP	12/15
4	Issue for DP	08/26
3	Issue for Review	08/09
2	Issue for Review	08/04
1	Issue for Review	07/28

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## Townline Homes

Project Title:

GARDENS

## The Gardens - Phase 1


## Building & Planning

## Lighting

Project Number: \_\_\_\_\_ Drawn By: \_\_\_\_\_

Checked By: \_\_\_\_\_

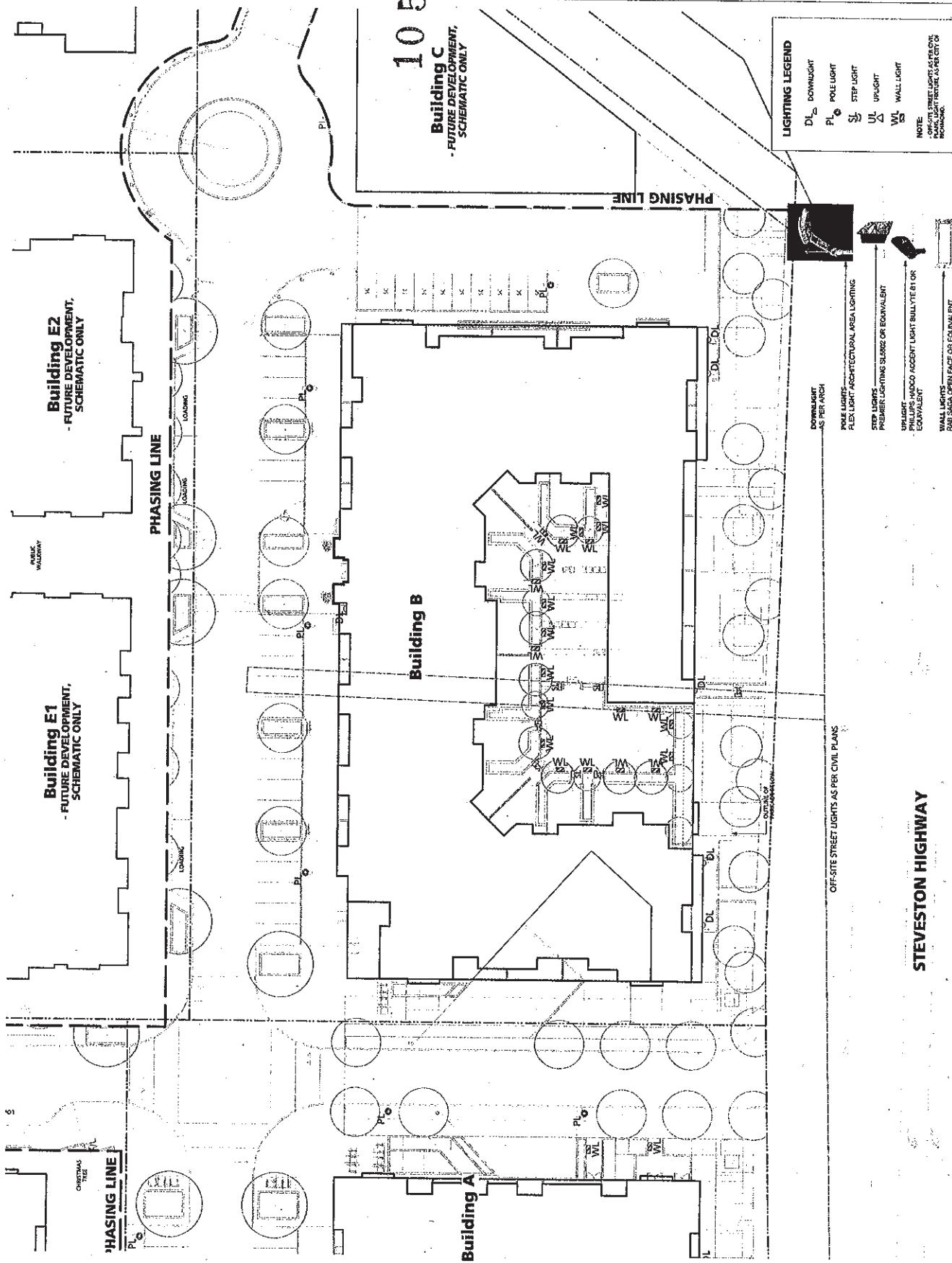
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Summer	Job No.:	10-01:
	1:200	

Sheet No.:

## L2.1





RAYMOND LEIKEMAN  
ARCHITECTS, INC.  
10000 Highway 100, Suite 100  
Houston, Texas 77036  
Tel: 281.461.1000 Fax: 281.461.1001

P+A

Project: 10544504  
Date: 04/20/01  
Scale: 1/8" = 1'-0"

#3.6

APR 20 2001

10544504

- 1. Review for Review 04/20/01
- 2. Review for Review 04/20/01
- 3. Review for Review 04/20/01
- 4. Review for Review 04/20/01
- 5. Review for Review 04/20/01
- 6. Review for Review 04/20/01
- 7. Review for Review 04/20/01
- 8. Review for Review 04/20/01
- 9. Review for Review 04/20/01
- 10. Review for Review 04/20/01

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Townline Homes

GARDENS

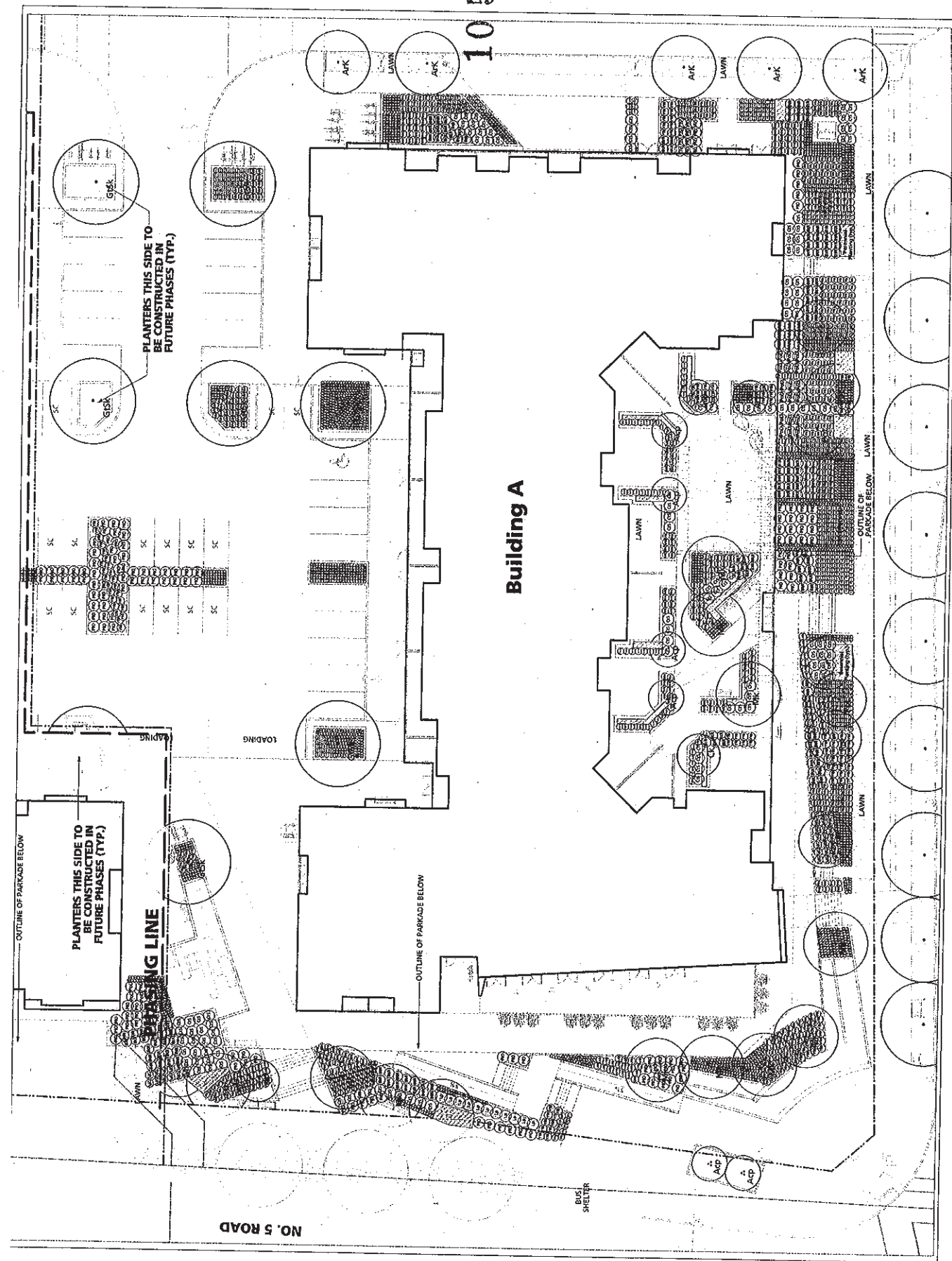
The Gardens - Phase 1

Building A

Planting Plan

Project No: 10544504  
Drawing Title: Building A Planting Plan  
Scale: 1/8" = 1'-0"

L3.0





RAYMOND LEIKEMAN  
ARCHITECT, INC.  
25000 Avenue 24, Westborough, MA 01581  
(508) 851-1000 Fax: (508) 851-1001

P+A

Project: #3.7  
Location: 10544504  
Scale: 1/8" = 1'-0"

#3.7

APR 20 2001

10544504

- 7. Release for DP
- 6. Issue for Review
- 5. Issue for Review
- 4. Release for DP
- 3. Issue for Review
- 2. Issue for Review
- 1. Issue for Review

04/15/01  
03/20/01  
03/20/01  
03/20/01  
03/20/01  
03/20/01  
03/20/01



Townline Homes

GARDENS

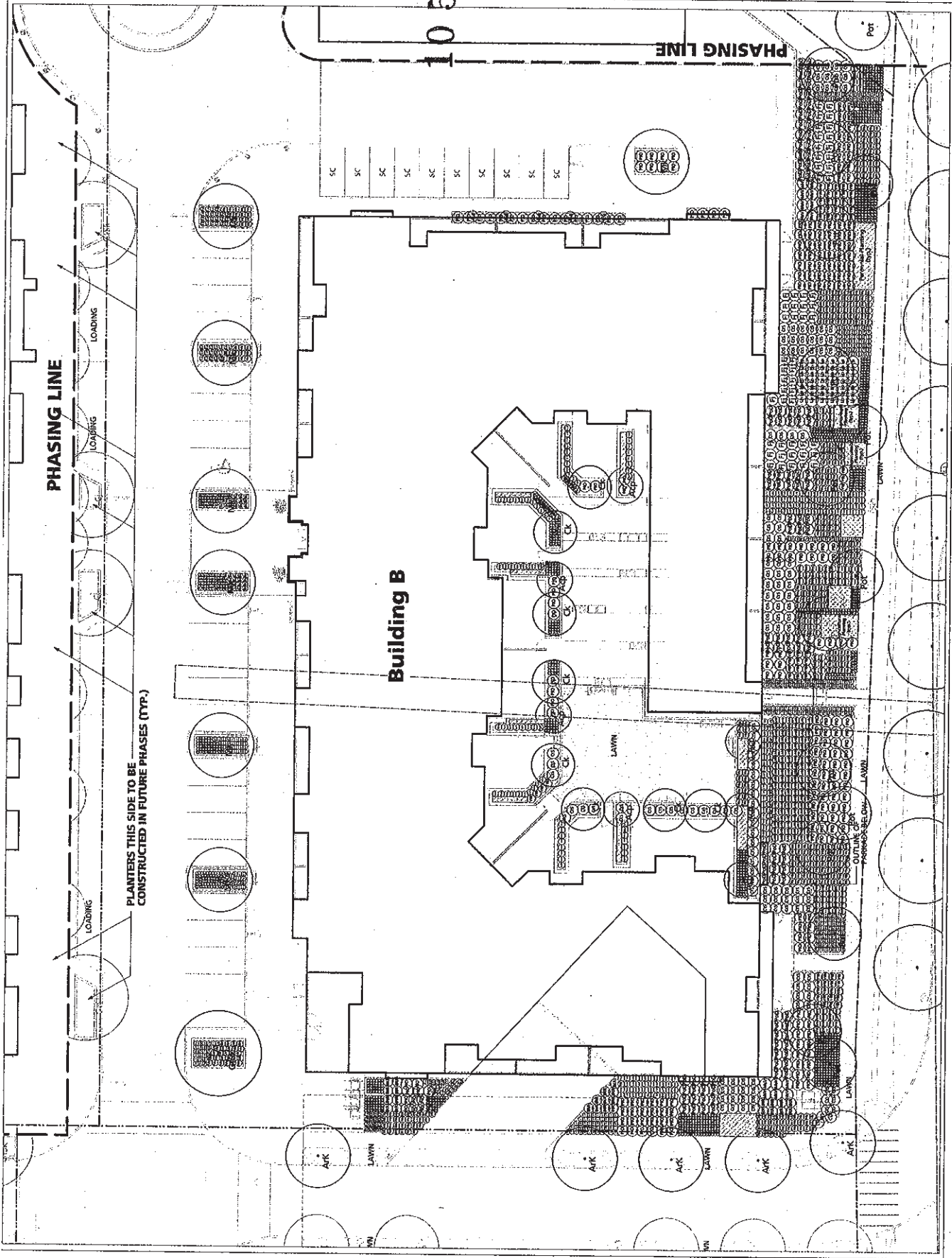
The Gardens - Phase 1

Building B

Planting Plan

Project No.: 10544504  
Drawing No.: 10544504  
Scale: 1/8" = 1'-0"

L3.1





20.  
#

103 07 2019

10-2044504

7	Reissue for DP	04/1
6	Issue for Review	03/2
5	Issue for DP Amendment	02/10
4	Reissue for DP	7/21
3	Issue for DP	08/2
2	Issue for Review	03/10
1	Issue for Review	08/10

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## Townline Homes

**Project Title**

GARDENS

## The Gardens - Phase

**Tides:**

## Agricultural Buffers

### Plant List

[illegible]

North: 

Drawn by:  Y. CHEN

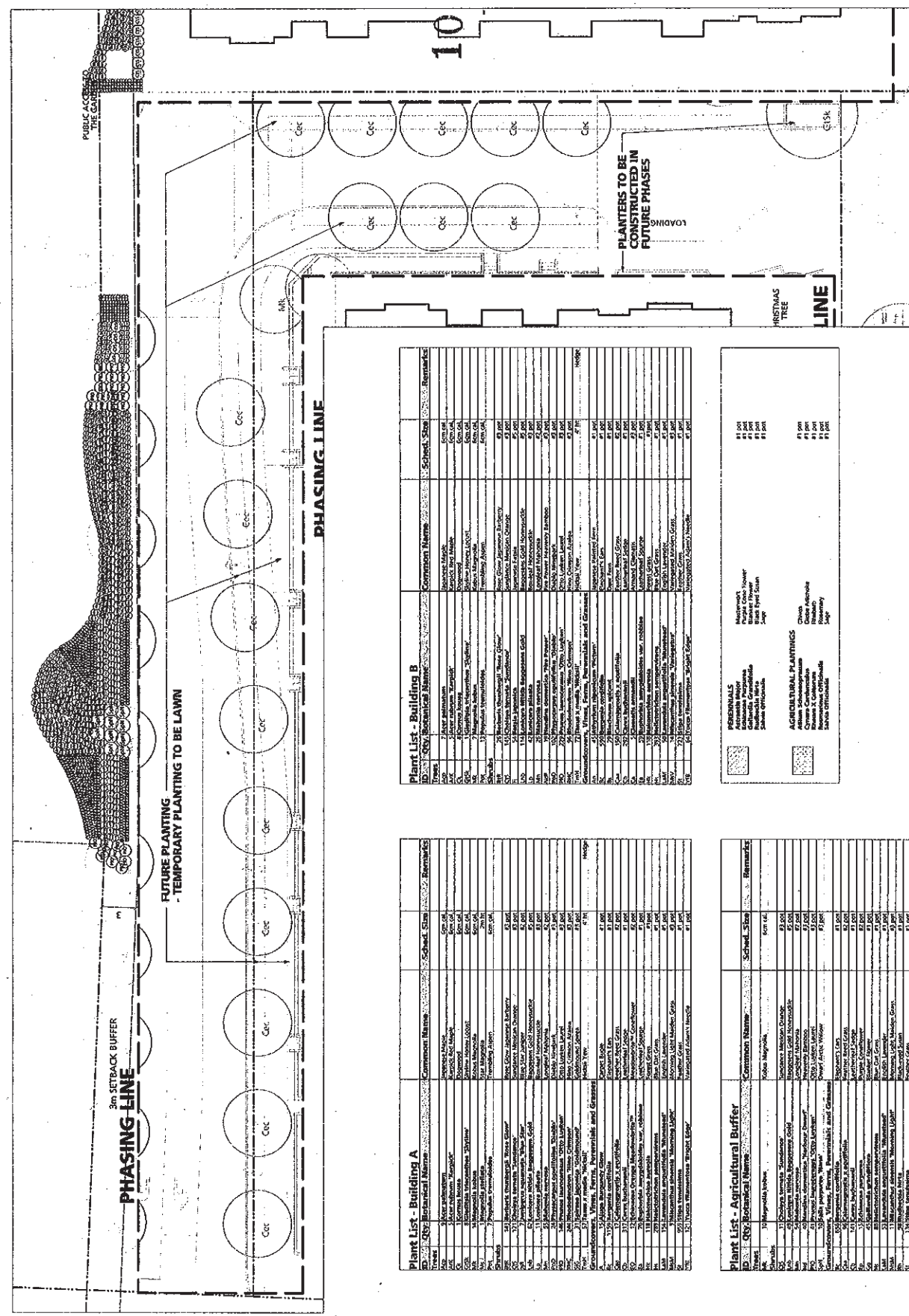
Checked By: \_\_\_\_\_

Job title	1:150	10:1
...	...	...

2

### 13.

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Plant List - Building A							
ID	DB	Botanical Name	Common Name	Sched.	Size	Remarks	
Trees							
1		<i>Quercus virginiana</i>	White Oak		60 ft		
2		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
3		<i>Pinus taeda</i>	Pinus Str. 100 ft		60 ft		
4		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
5		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
6		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
7		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
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10		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
11		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
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23		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
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25		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
26		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
27		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
28		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
29		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
30		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
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46		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
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59		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
60		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
61		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
62		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		
63		<i>Pinus strobus</i>	Pinus Str. 100 ft		60 ft		

[illegible]

Plant List - Building B		Common Name	Scient. Name	Height	Remarks
Trains	12	12	12	12	12
1	1	1	1	1	1
2	2	2	2	2	2
3	3	3	3	3	3
4	4	4	4	4	4
5	5	5	5	5	5
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[illegible]

- |   |                        |       |
|---|------------------------|-------|
| 3 | Reissue for DP         | 02/15 |
| 7 | Issue for Review       | 03/25 |
| 5 | Issue for DP Amendment | 02/08 |
| 5 | Reissue for DP         | 12/15 |
| 3 | Issue for DP           | 06/26 |
| 3 | Issue for Review       | 06/09 |
| 2 | Issue for Review       | 08/04 |
| 1 | Issue for Review       | 07/28 |



## Townline Homes

**Project Title:**

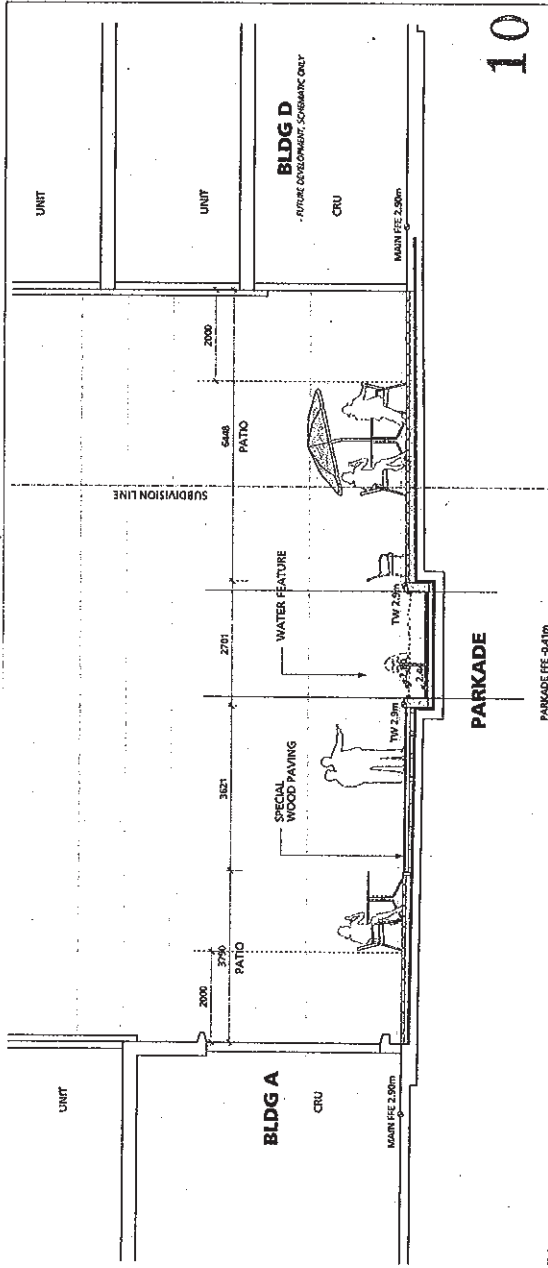
GARDENS

## The Gardens - Phase 1

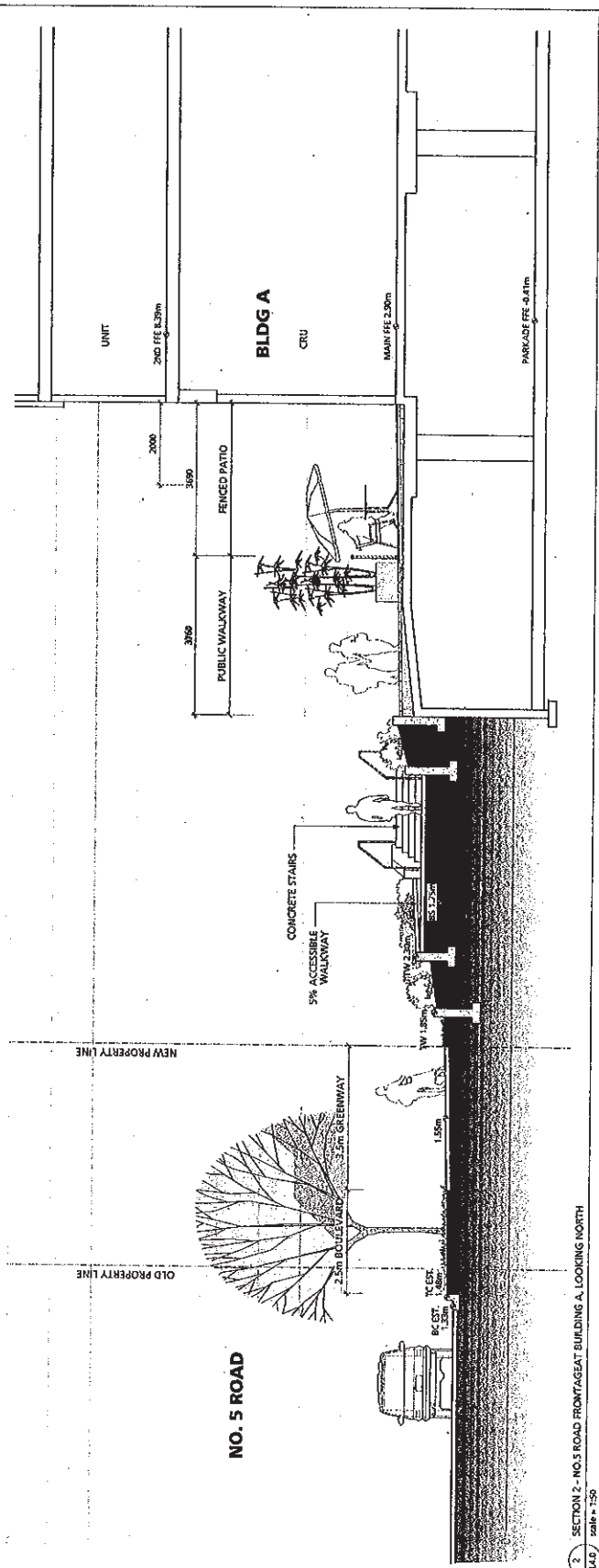
Job Title: **Landscape Sections**

NAME: \_\_\_\_\_  
 as shown?  
 Sub No.: \_\_\_\_\_  
 Checked by: \_\_\_\_\_  
 Date: \_\_\_\_\_

L4.0



1 SECTION 1 - PLAZA BMW BUILDING A AND D, LOOKING WEST



2 SECTION 2 - NO. 5 ROAD FRONTAGE AT BUILDING A, LOOKING NORTH  
U.A.O. scale = 1:50

**P+A**

**Perry • Associates**  
Suite 200-7558 W 4th St  
Vancouver, B.C. V6L 1R1

**Landscaping Associates**  
T 484.328.8113  
F 484.728.8116  
terra@terraassociates.com

#310

622 02 299

1054504

8	Issue for DP	00/1
7	Issue for Review	03/2
6	Issue for DP Amendment	02/0
5	Relative for DP	12/1
4	Issue for DP	08/2
3	Issue for Review	08/0
2	Issue for Review	08/0
1	Issue for Review	07/2

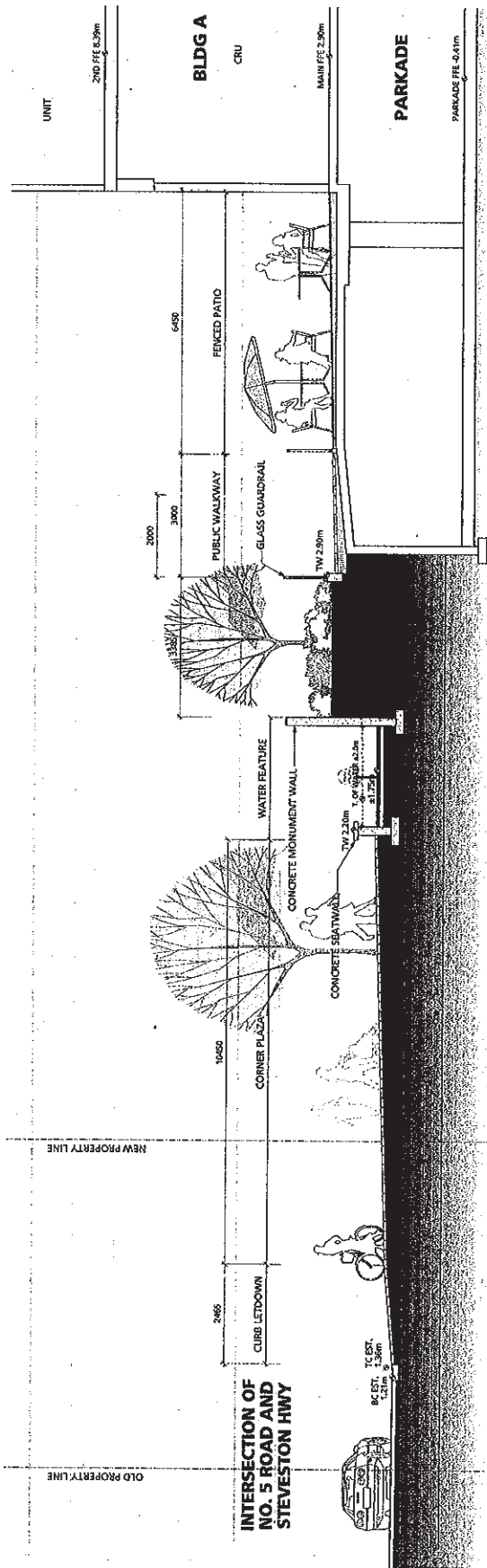


**Townline Homes**

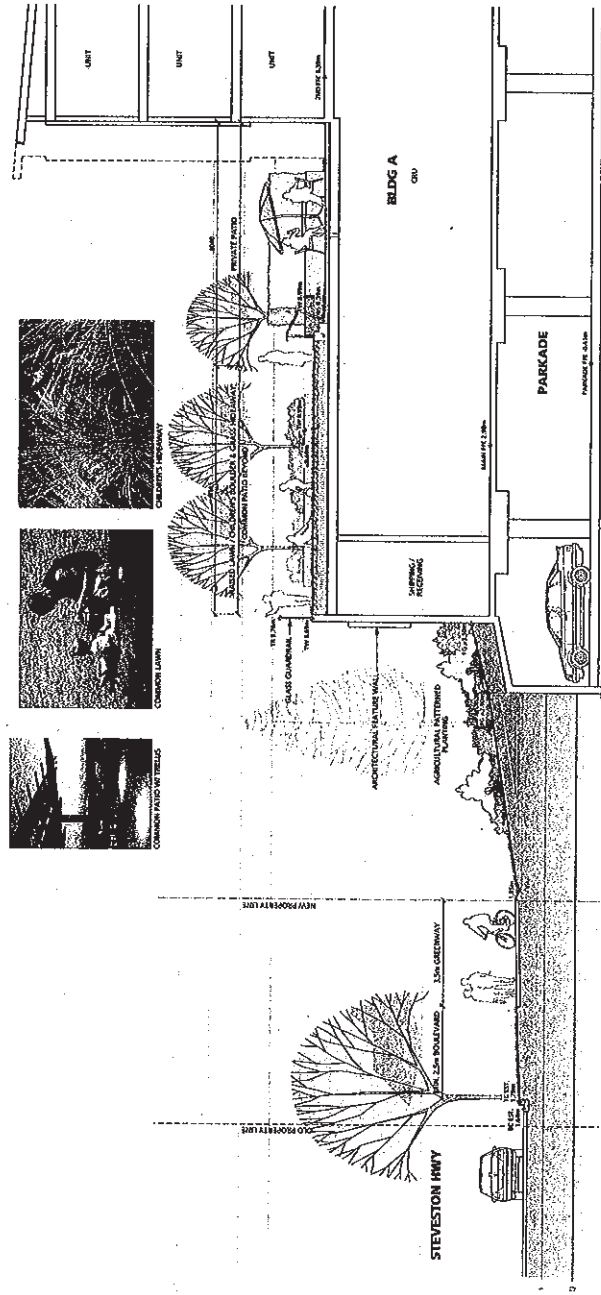
**The Gardens - Phase**  
**Landscape Section**

Project North:	Drawn By:	JU
	Checked by:	
Scale:	Job No.:	10-0
	as shown	
Sheet No.:		

#### L4.1



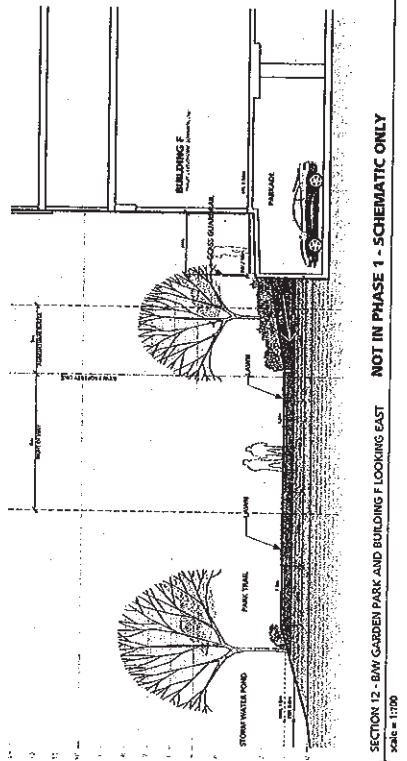
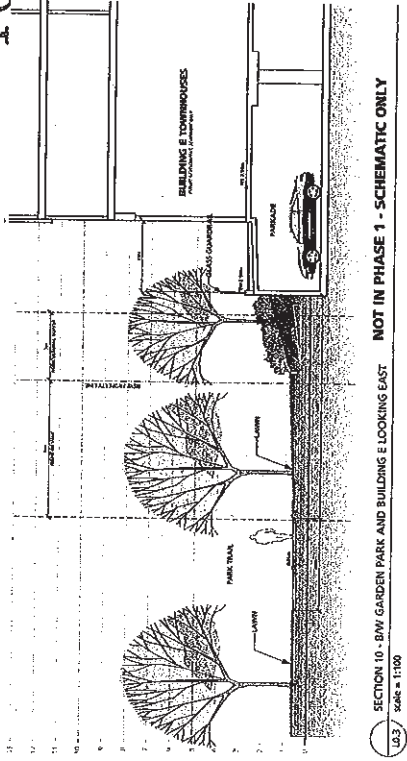
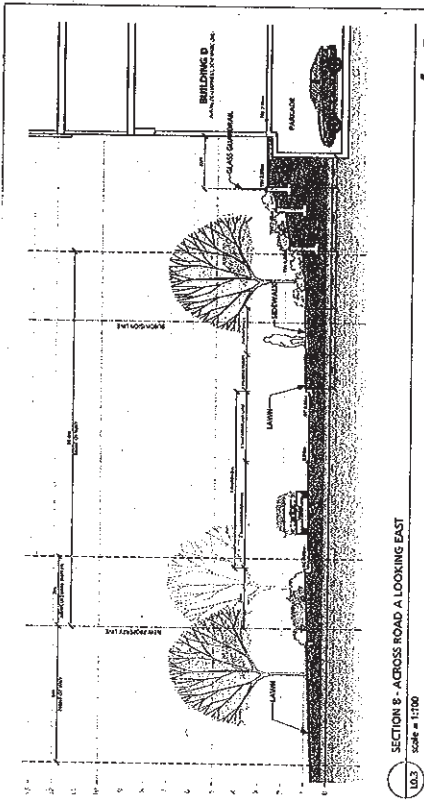
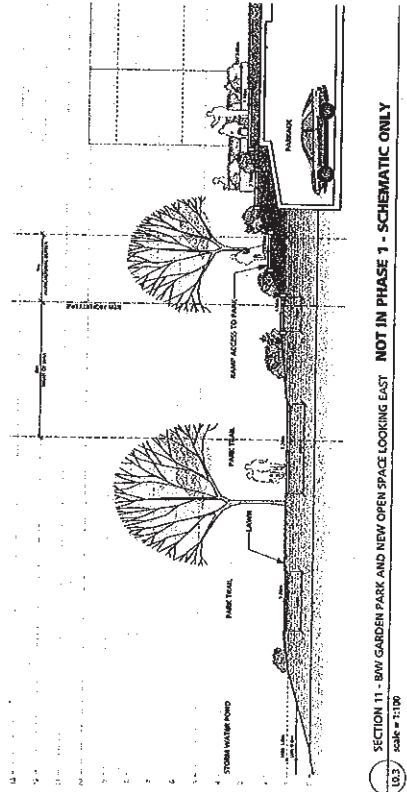
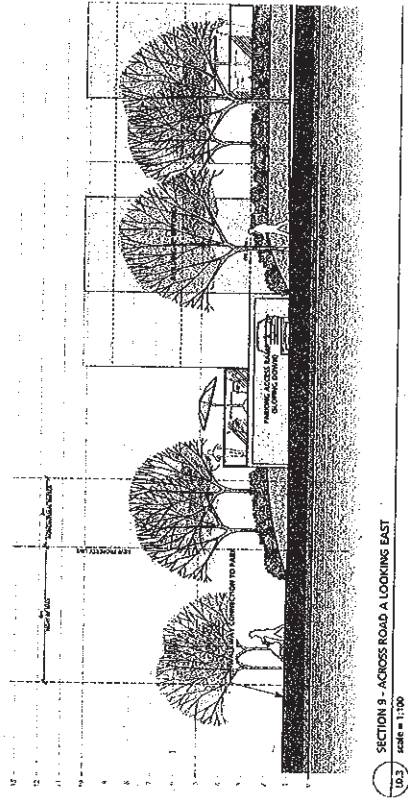
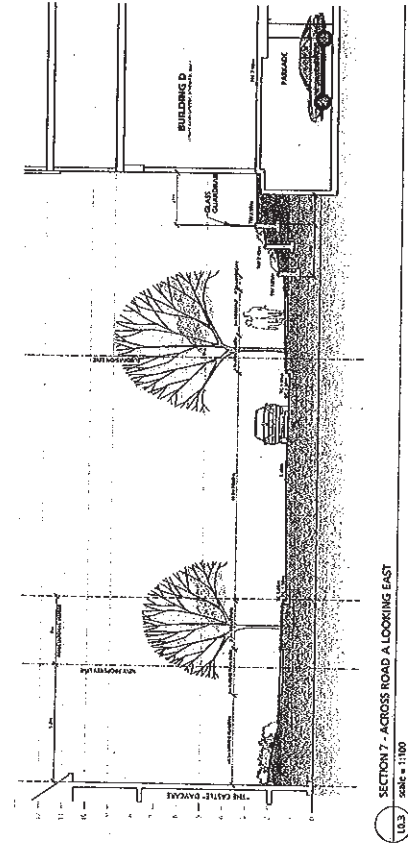
3 SECTION 3 - CORNER PLAZA AT INTERSECTION OF STEVESTON HWY. AND NO.5 ROAD, LOOKING NORTHWEST



4 SECTION 4 - BUILDING A PODIUM AND STEVESTON HWY. FRONTAGE, LOOKING WEST  
U.A.1 scale = 1:75







**P+A**

**Perry • Associates**  
Landscaping Architecture  
Site Planning  
760-798-4115  
561-504-4115  
www.perryassoc.com

313 #

11-2-59

105420

- |   |                        |      |
|---|------------------------|------|
| 4 | Reissue for DP         | 0475 |
| 3 | Issue for Review       | 0325 |
| 2 | Issue for DP Amendment | 0208 |
| 1 | Reissue for DP         | 1215 |
|   | Revision No.           | 0    |

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## Townline Homes

Project Title

GARDENS

## The Gardens - Phase 1

## Drawing Title

### Possible Public Art Locations

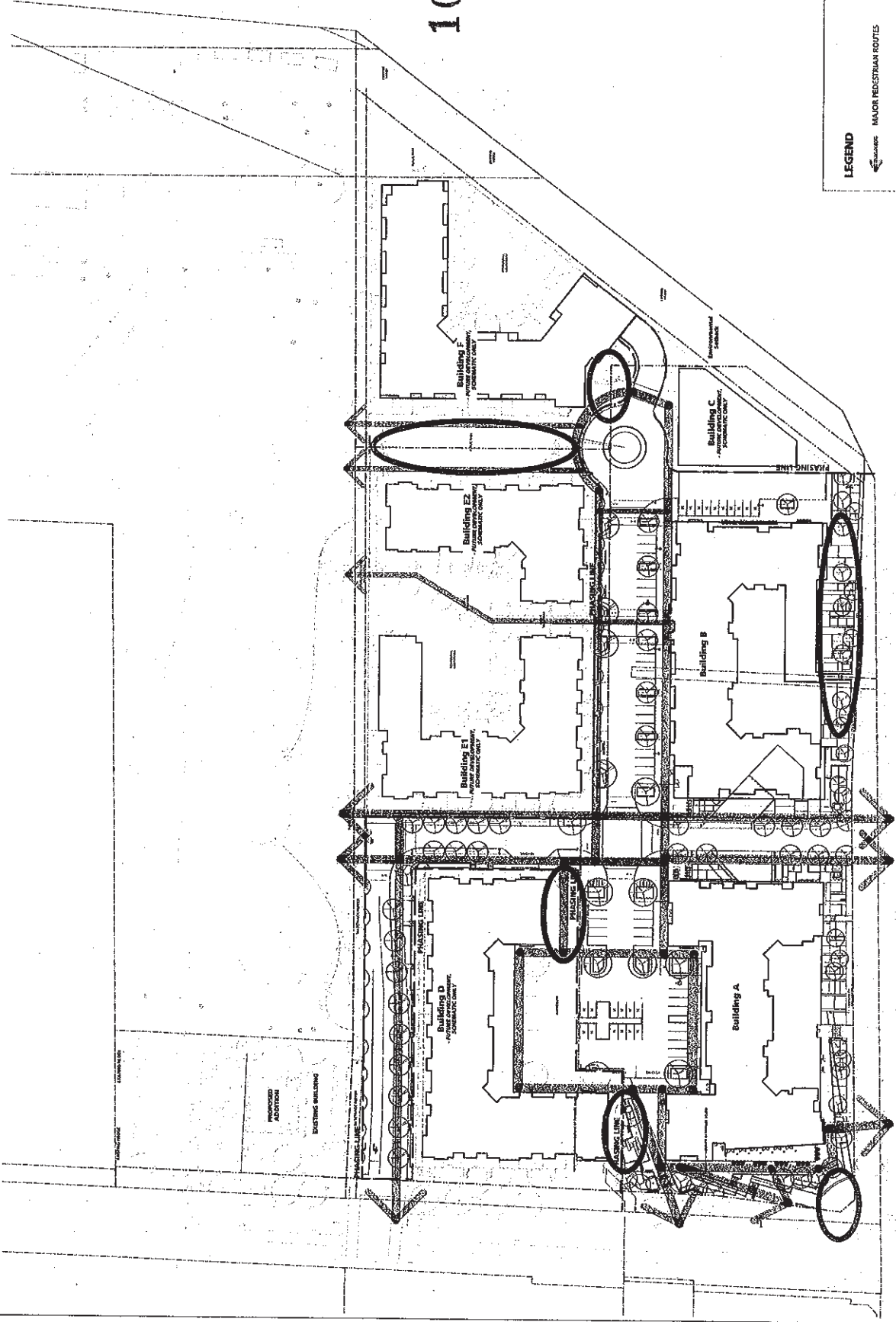
Received March 12, 1999

Grain (g)	Y
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20	20
30	30
40	40
50	50
60	60
70	70
80	80
90	90
100	100

[illegible]

Street No.: 1500 10-012

## LO.4



### LEGEND



### MAJOR PEDESTRIAN ROUTES



POSSIBLE PUBLIC ART LOCATIONS  
- TO BE DETERMINED DURING  
PUBLIC ART PROCESS







**RAYMOND LERAMAN**  
ARCHITECTS, INC.  
2000 10th Street, N.W., Washington, D.C. 20007  
Tel: (202) 462-1200 Fax: (202) 462-1201

**P+A**

Project: The Gardens - Phase I  
Location: Washington, D.C.  
Client: Townline Homes  
Architect: Raymond LeRaman Architects, Inc.  
Scale: 1/8" = 1'-0"

10544504

REVISED

#3.15

- 0. Return for DP
- 1. Return for Review
- 2. Return for Review
- 3. Return for Review
- 4. Return for Review
- 5. Return for Review
- 6. Return for Review
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- 95. Return for Review
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- 97. Return for Review
- 98. Return for Review
- 99. Return for Review
- 100. Return for Review

THE GARDENS - PHASE I  
LANDSCAPE DETAIL  
Drawing Title: The Gardens - Phase I  
Drawing Date: 10/01/01  
Drawing By: R  
Drawing For: R  
Drawing Scale: 1/8" = 1'-0"



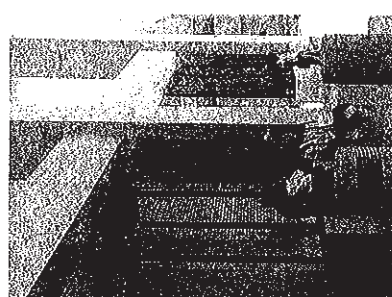
**Townline Homes**

**GARDENS**

**The Gardens - Phase I  
Landscape Detail**

Project Name: The Gardens - Phase I  
Location: Washington, D.C.  
Client: Townline Homes  
Architect: Raymond LeRaman Architects, Inc.  
Scale: 1/8" = 1'-0"

**L5.1**



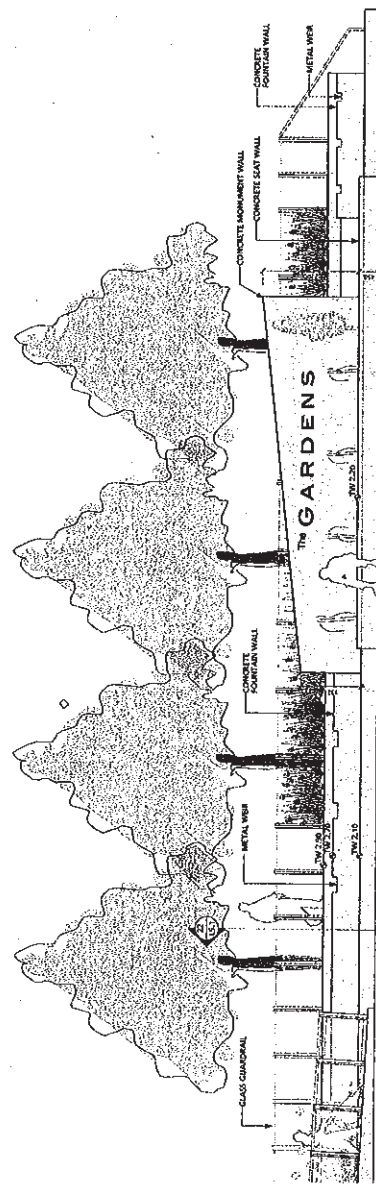
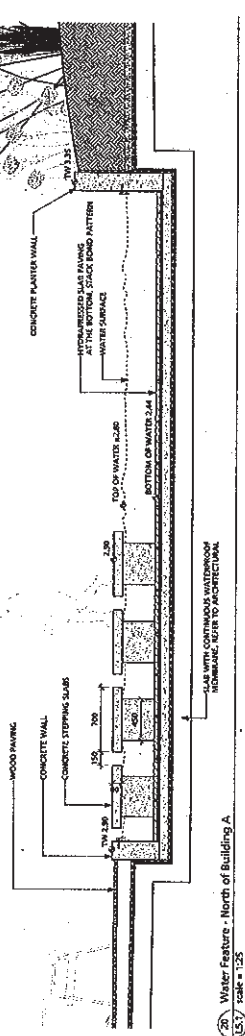
**A) TYPICAL**  
Green Screen  
Scale: 1/8" = 1'-0"



**B) EAST OF BUILDING B**  
Scale: 1/8" = 1'-0"



**Concrete Seat Block @ Bldg. 8 Courtyard**  
Scale: 1/8" = 1'-0"



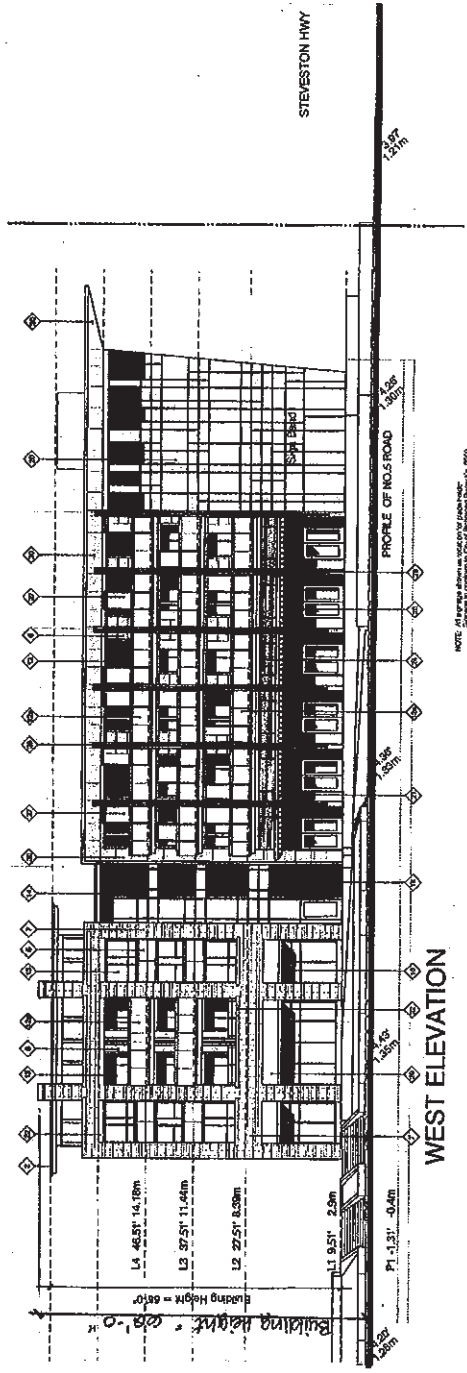
**Water Feature - South of Corner Plaza**  
Scale: 1/8" = 1'-0"

**Water Feature - North of Building A**  
Scale: 1/8" = 1'-0"

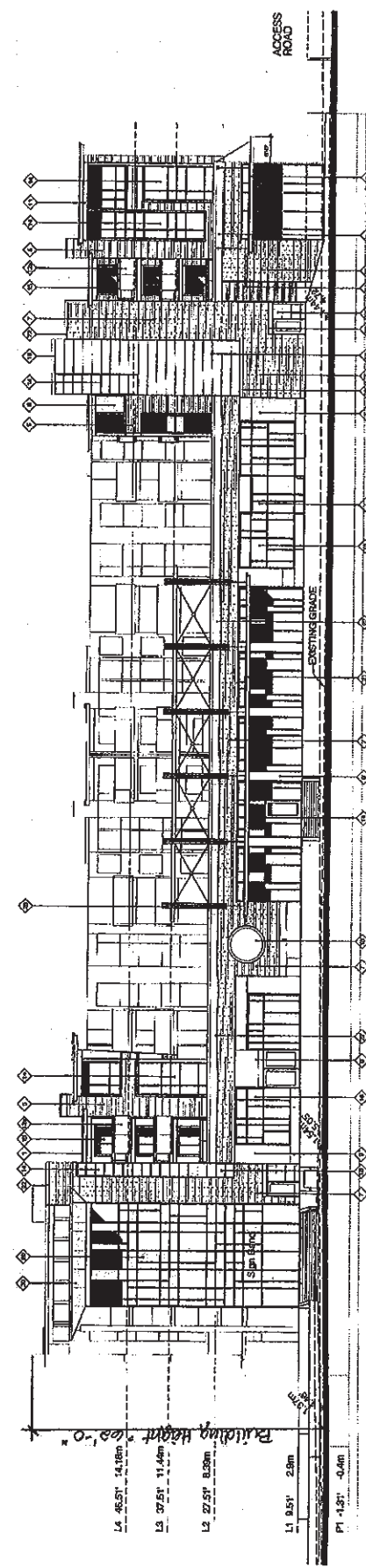
**Water Feature - South of Corner Plaza**  
Scale: 1/8" = 1'-0"

**Water Feature - North of Building A**  
Scale: 1/8" = 1'-0"

Building Height  
\* measured from the average elevation  
along the Steveston Hwy & No. 5 Rd. sidewalks



WEST ELEVATION



SOUTH ELEVATION ~ STEVESTON

# Elevations

SK-4.5

## Material Legend

1	200 Series Steel Joist
2	200 Series Steel Joist
3	200 Series Steel Joist
4	200 Series Steel Joist
5	200 Series Steel Joist
6	200 Series Steel Joist
7	200 Series Steel Joist
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12	200 Series Steel Joist
13	200 Series Steel Joist
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41	200 Series Steel Joist
42	200 Series Steel Joist
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45	200 Series Steel Joist
46	200 Series Steel Joist
47	200 Series Steel Joist
48	200 Series Steel Joist
49	200 Series Steel Joist
50	200 Series Steel Joist

**RAYMOND LITKEMAN**  
Architect  
1000 Steveston Hwy  
Steveston, BC V9A 1G1  
Tel: 250-761-1111  
Fax: 250-761-1112  
www.raymondlitkeman.com

August 20, 2010  
Approved for Development Permit  
October 12, 2010  
Approved for Development Permit  
February 6, 2011  
Approved for Development Permit  
February 11, 2011  
Approved for Development Permit  
April 13, 2011  
Approved for Development Permit

#4.1

APR 27 2011

10544504



Townline Homes

GARDENS  
Phase One

No. 5 Road & Steveston Highway  
Steveston, BC

Building A  
Elevations  
Scale: 1/8" = 1'-0"  
April 13, 2011

ACCESS ROAD





**RAYMOND LECKMAN**  
ARCHITECTS, P.C.  
100 West 10th St., Suite 100  
Stevenson, MD 21153  
Tel: 410.326.1000 Fax: 410.326.1001  
www.raymondleckman.com

**Permits**  
August 28, 2010 Approved Permit  
December 14, 2010 Approved Permit  
December 14, 2010 Approved Permit  
February 8, 2011 Approved Permit  
March 14, 2011 Approved Permit  
April 15, 2011 Approved Permit  
Approved for Development Permit

#4.2

APR 20 2011

10544504



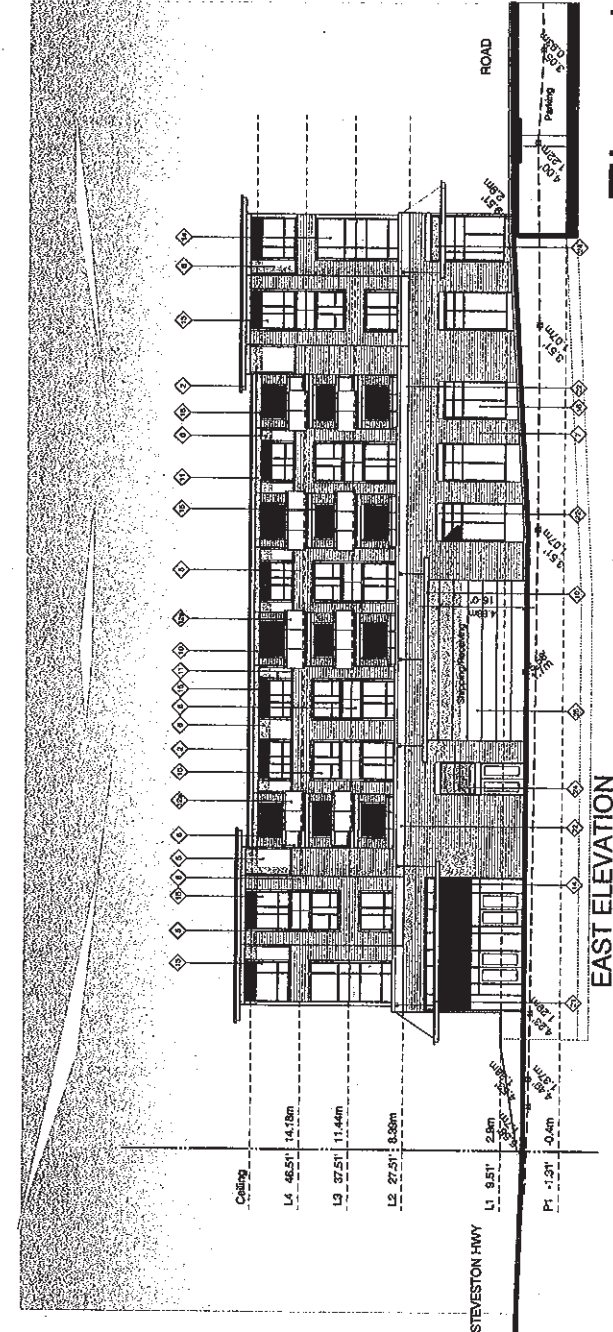
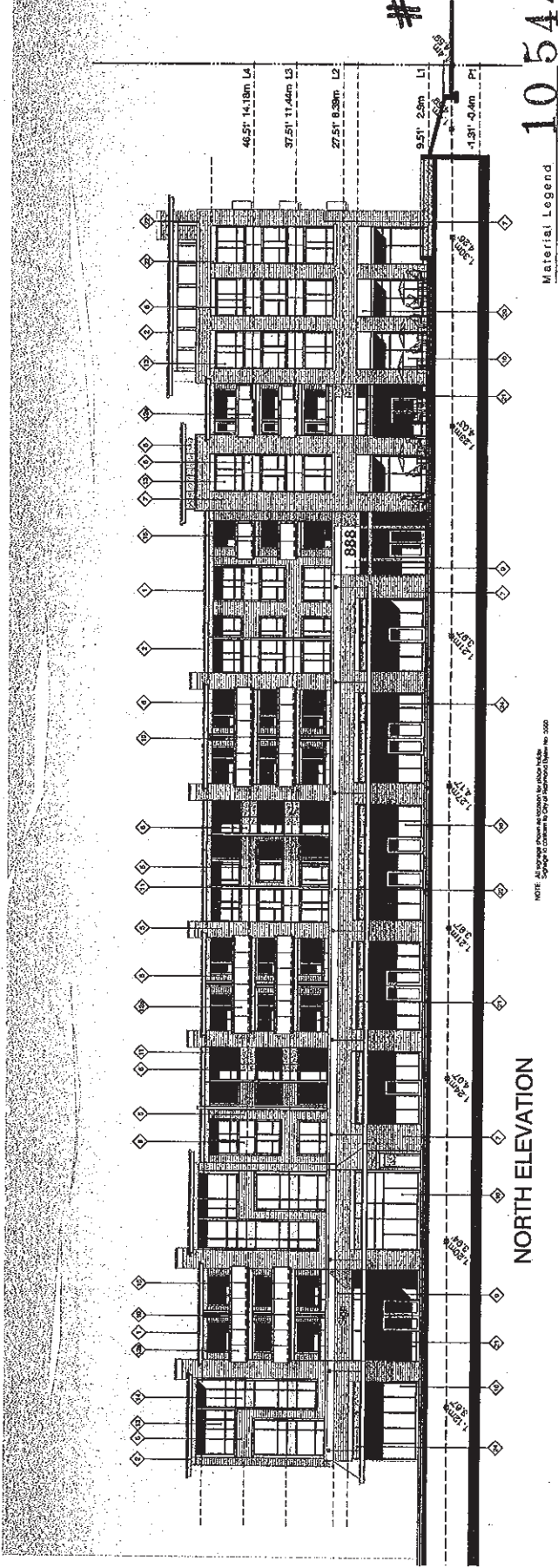
Townline Homes

GARDENS  
Phase One

No. 5 Road & Stevenson Highway  
POTOMAC, D.C.

**Building A  
Elevations**  
320' x 120'  
APR 15, 2011

SK-4.6



Elevations



WEST ELEVATION / SECTION - TERRACE



RAYMOND LEROMAIN  
ARCHITECTS INC.  
10000 Old Dominion Road, Suite 200  
Richmond, VA 23234  
Tel: 804.781.1234 Fax: 804.781.1235  
www.raymondleromain.com

Project:  
April 15, 2011  
Issued for Development Permit  
December 15, 2010  
Revised for Development Permit  
Issued for Development Permit  
January 15, 2011  
March 15, 2011  
Development Permit Clarifications  
April 15, 2011  
Issued for Development Permit  
Issued for Development Permit

#4.4

APR 28 2011

10544504



Townline Homes

GARDENS  
Phase One

No. 4 Road & Stevenson Highway  
Richmond, B.C.

Building A  
Elevations  
3/20" = 1" = 0'  
April 15, 2011

SK-4.8

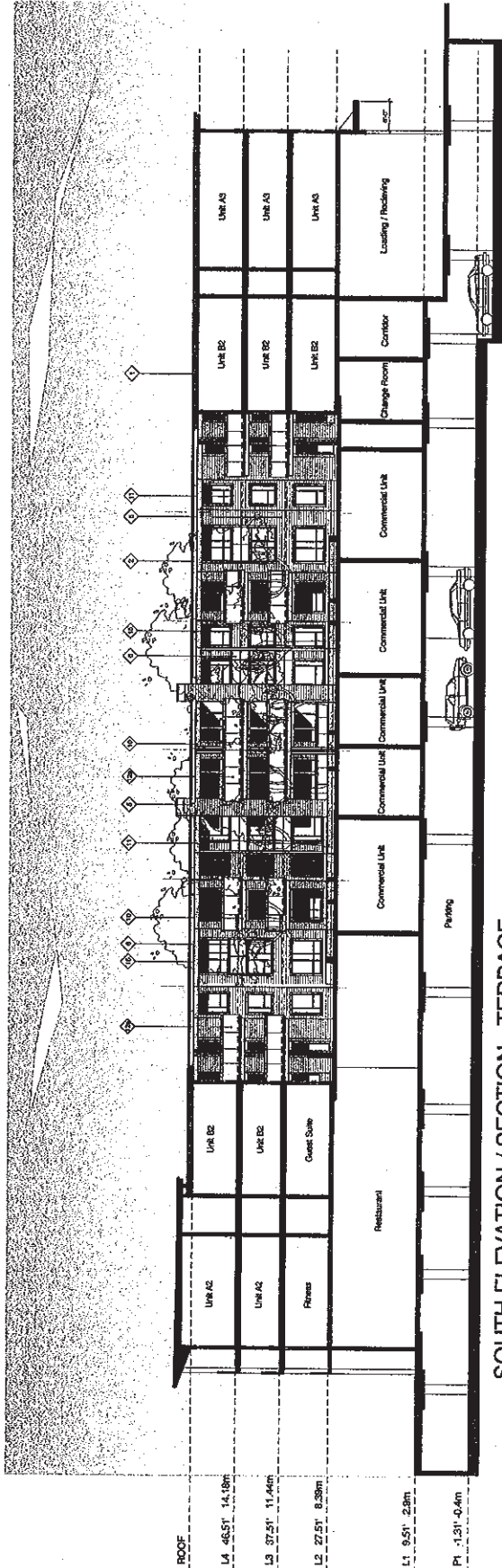
Elevations

SOUTH ELEVATION / SECTION - TERRACE

# Material Legend

1	Roof
2	2nd Floor Concrete
3	2nd Floor Walls
4	2nd Floor Windows
5	2nd Floor Doors
6	2nd Floor Ceilings
7	2nd Floor Floors
8	2nd Floor Stairs
9	2nd Floor Balconies
10	2nd Floor Terraces
11	2nd Floor Parking
12	2nd Floor Loading
13	2nd Floor Stairs
14	2nd Floor Balconies
15	2nd Floor Terraces
16	2nd Floor Parking
17	2nd Floor Loading
18	2nd Floor Stairs
19	2nd Floor Balconies
20	2nd Floor Terraces
21	2nd Floor Parking
22	2nd Floor Loading
23	2nd Floor Stairs
24	2nd Floor Balconies
25	2nd Floor Terraces
26	2nd Floor Parking
27	2nd Floor Loading
28	2nd Floor Stairs
29	2nd Floor Balconies
30	2nd Floor Terraces
31	2nd Floor Parking
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33	2nd Floor Stairs
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50	2nd Floor Terraces
51	2nd Floor Parking
52	2nd Floor Loading
53	2nd Floor Stairs
54	2nd Floor Balconies
55	2nd Floor Terraces
56	2nd Floor Parking
57	2nd Floor Loading
58	2nd Floor Stairs
59	2nd Floor Balconies
60	2nd Floor Terraces
61	2nd Floor Parking
62	2nd Floor Loading
63	2nd Floor Stairs
64	2nd Floor Balconies
65	2nd Floor Terraces
66	2nd Floor Parking
67	2nd Floor Loading
68	2nd Floor Stairs
69	2nd Floor Balconies
70	2nd Floor Terraces
71	2nd Floor Parking
72	2nd Floor Loading
73	2nd Floor Stairs
74	2nd Floor Balconies
75	2nd Floor Terraces
76	2nd Floor Parking
77	2nd Floor Loading
78	2nd Floor Stairs
79	2nd Floor Balconies
80	2nd Floor Terraces
81	2nd Floor Parking
82	2nd Floor Loading
83	2nd Floor Stairs
84	2nd Floor Balconies
85	2nd Floor Terraces
86	2nd Floor Parking
87	2nd Floor Loading
88	2nd Floor Stairs
89	2nd Floor Balconies
90	2nd Floor Terraces
91	2nd Floor Parking
92	2nd Floor Loading
93	2nd Floor Stairs
94	2nd Floor Balconies
95	2nd Floor Terraces
96	2nd Floor Parking
97	2nd Floor Loading
98	2nd Floor Stairs
99	2nd Floor Balconies
100	2nd Floor Terraces

NOTE: All changes shown are in accordance with the latest  
approved by the City of Richmond, B.C. 2011



SOUTH ELEVATION / SECTION - TERRACE



#4.5

APR 27 2011

10544504



Townline Homes

"GARDENS"  
Phase One

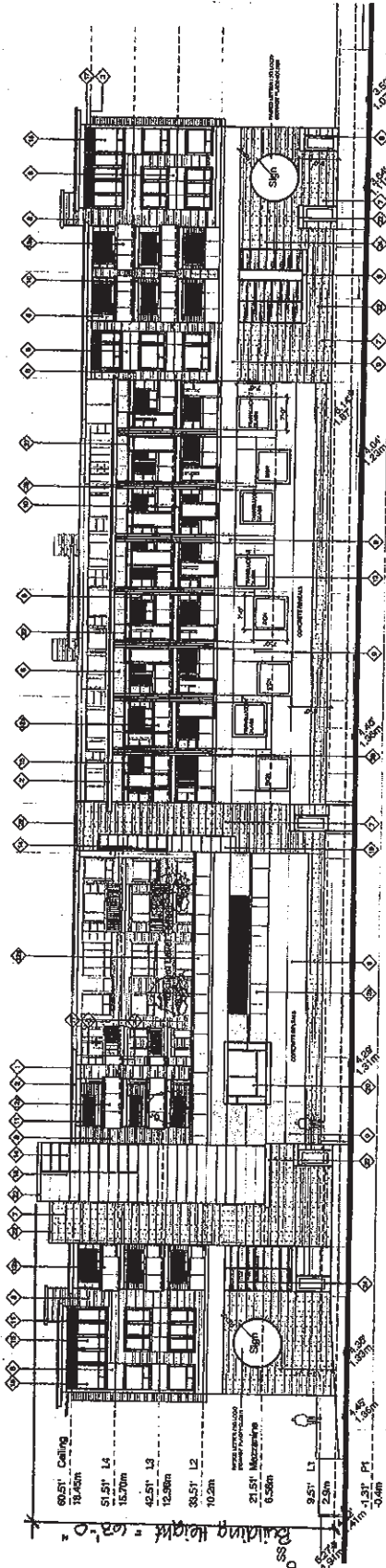
No. 5 Road & Steveston Highway  
Rexford, N.C.

Building B  
Elevations

32' x 10'  
April 15, 2011

SK-5.5

Building Height  
→ measured from the average elevation  
along the Steveston Hwy sidewalk

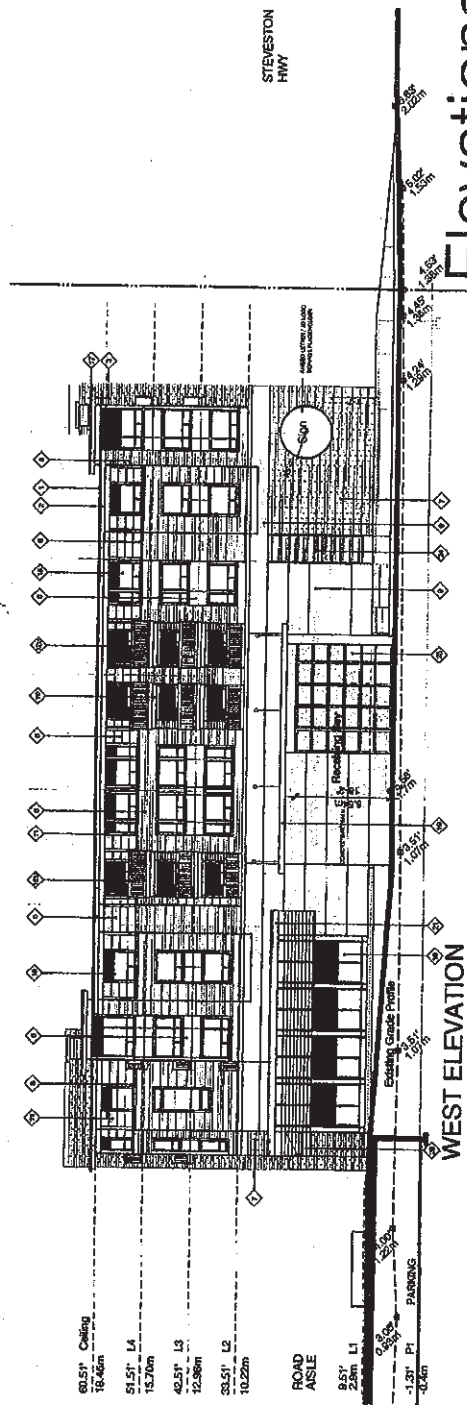


SOUTH ELEVATION ~ STEVESTON

NOTE: All heights are measured to the top of the building.

Material Legend

- 1. Red Brick
- 2. White Brick
- 3. Dark Siding
- 4. Light Siding
- 5. Dark Siding
- 6. Light Siding
- 7. Dark Siding
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- 97. Dark Siding
- 98. Light Siding
- 99. Dark Siding
- 100. Light Siding



WEST ELEVATION

Elevations

41

022029

1054504



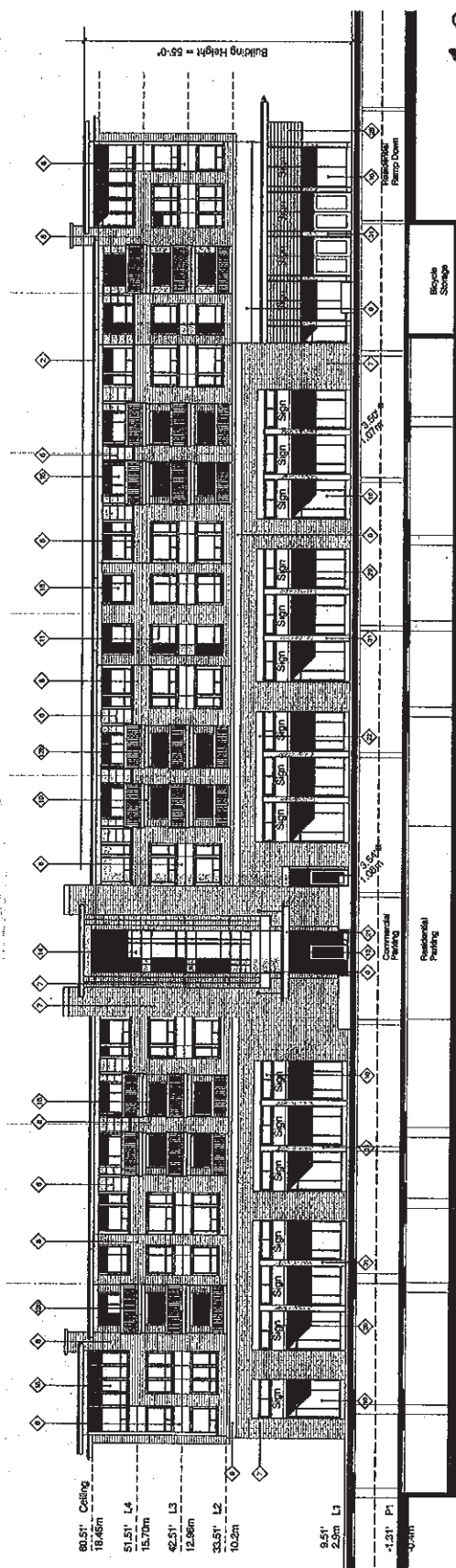
## Townline Home

GARDENS  
Phase OneNo. 5 Road & Steveston Highway  
Richmond, B.C.

### Building B Elevations

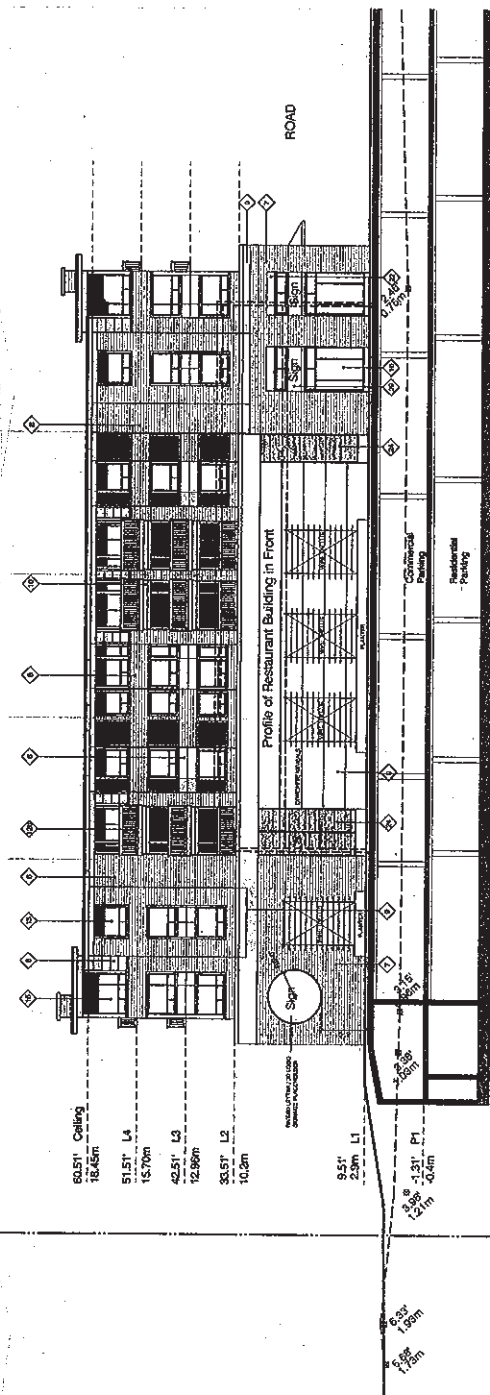
132-1107  
April 15, 2011

SK-5.6



NORTH ELEVATION

**NOTE:** All signage shown as location for place holder.  
 Conditions in California California Department of Transportation



EAST ELEVATION

# Elevations

Material Legend	10
1	255 Not Specified
2	Delmar 2nd ed. 12-13 Years, Revised
3	VanC, Adolescent
4	VanC, PreAdolescent
5	Adolescent Skills
6	Adolescent Skills
7	Adolescent Skills
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99	Adolescent Skills
100	Adolescent Skills



RAYMOND LERAMAN  
ARCHITECT P.C.  
1000 West 10th Street, Suite 100  
Wichita, KS 67203  
Tel: 316.262.1234  
Fax: 316.262.1235  
www.rlearchitect.com

Colors represent suggested approximations of colors as they may appear in print and may vary from the actual colors of the materials used in the construction of the building.

Permitted:  
August 28, 2010  
Issued for Development Permit  
November 15, 2010  
Revised for Development Permit  
Revised for ADGP Review  
February 1, 2011  
Issued for Development Permit  
March 11, 2011  
Revised for Development Permit  
April 15, 2011  
Revised for Development Permit

#4.7

APR 20 2011

10541504



Townline Homes

GARDENS  
Phase One

No. 5 Road & Stevenson Highway  
Pawnee, KS

Building B  
Elevations

3/20/11 - 1/17  
April 15, 2011

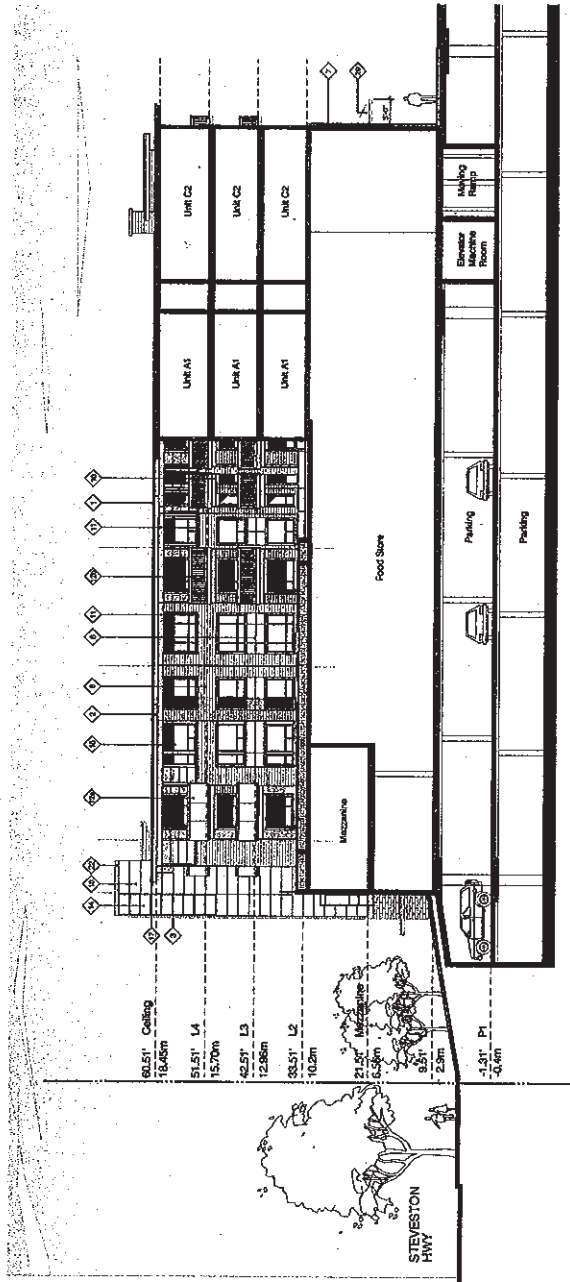
# Elevations

## SOUTH ELEVATION / SECTION ~ TERRACE

SK-5.7

Material Legend

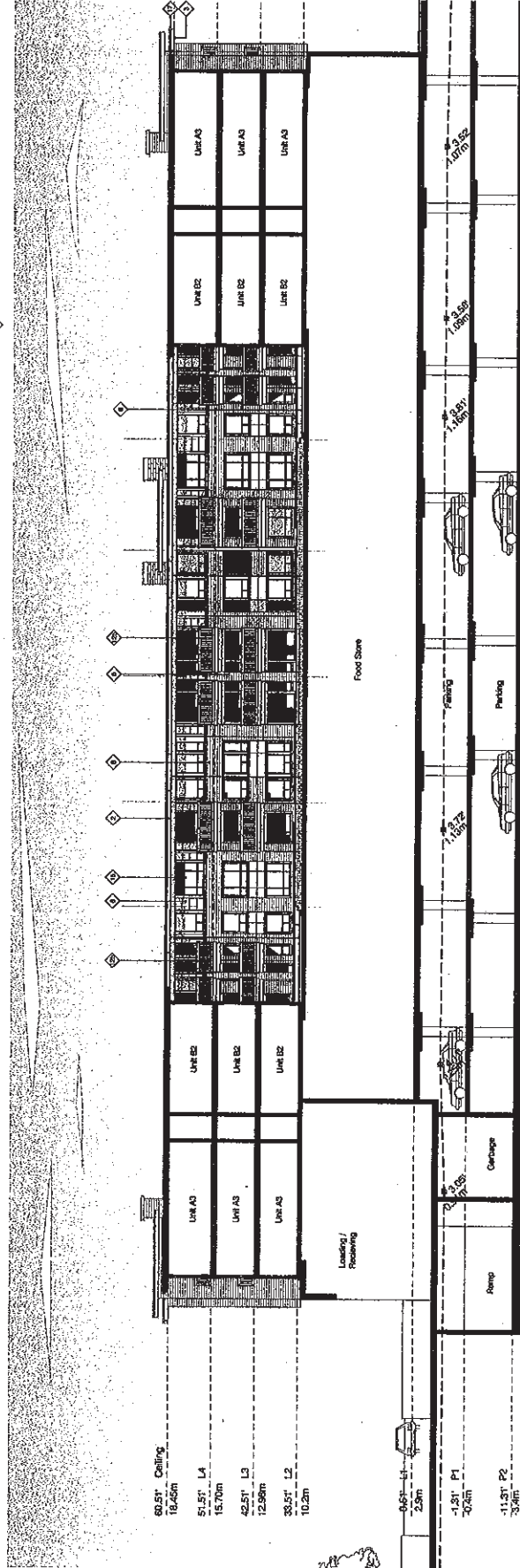
1	Red	255 Red Brick
2	Red	255 Red Brick
3	Red	255 Red Brick
4	Red	255 Red Brick
5	Red	255 Red Brick
6	Red	255 Red Brick
7	Red	255 Red Brick
8	Red	255 Red Brick
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97	Red	255 Red Brick
98	Red	255 Red Brick
99	Red	255 Red Brick
100	Red	255 Red Brick



## EAST ELEVATION / SECTION ~ TERRACE

NOT: All elevations shown are subject to change without notice.

NOT: All elevations shown are subject to change without notice.



## SOUTH ELEVATION / SECTION ~ TERRACE





1	Med.	Self-Help Medication
2	Med. Tests	Develop Job on 2x 12" Metal, Thread
3	Med. Tests	Verify, Adjust
4	Med. Tests	Verify, Adjust
5	Med. Tests	Verify, Adjust
6	Med. Tests	Verify, Adjust
7	Med. Tests	Verify, Adjust
8	Med. Tests	Verify, Adjust
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99	Med. Tests	Verify, Adjust
100	Med. Tests	Verify, Adjust



RAYMOND LEIKEMAN  
ARCHITECTS, P.C.  
10000 Old Dominion Road, Suite 200  
Richmond, VA 23234  
Tel: 804.771.1100 Fax: 804.771.1101  
www.leikemanarchitects.com

Project: Phase One  
Location: 10500 Old Dominion Road, Suite 200  
Richmond, VA 23234  
Date: 10/15/2011

Project: Phase One  
Location: 10500 Old Dominion Road, Suite 200  
Richmond, VA 23234  
Date: 10/15/2011

Project: Phase One  
Location: 10500 Old Dominion Road, Suite 200  
Richmond, VA 23234  
Date: 10/15/2011

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Richmond, VA 23234  
Date: 10/15/2011

Project: Phase One  
Location: 10500 Old Dominion Road, Suite 200  
Richmond, VA 23234  
Date: 10/15/2011

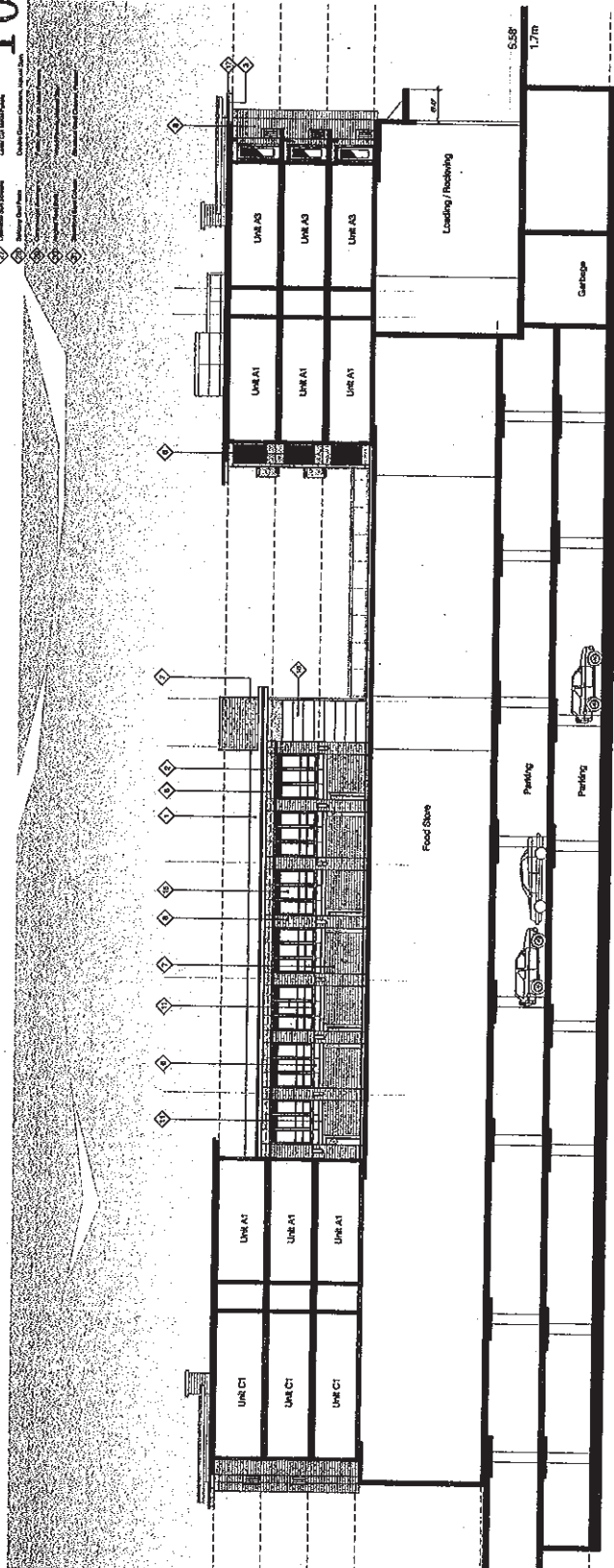
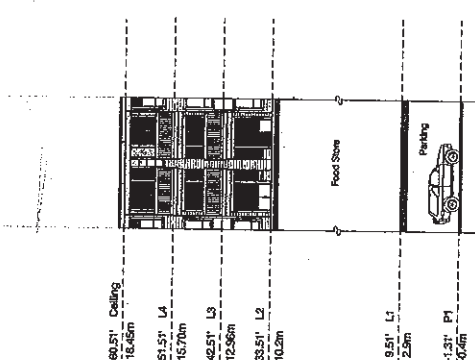
Project: Phase One  
Location: 10500 Old Dominion Road, Suite 200  
Richmond, VA 23234  
Date: 10/15/2011

# Material Legend

- 1. 200 Red Machine
- 2. 200 Red Machine
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- 100. 200 Red Machine

NOTE: All material elevations are based on the existing ground level. Elevation of existing ground level is shown on the site plan.

## EAST & WEST INSIDE CORNER ELEVATIONS/ SECTION ~ TERRACE



## NORTH ELEVATION / SECTION ~ TERRACE

## Elevations

SK-5.9



Townline Homes

GARDENS  
Phase One

No. 5 Road & Station Highway  
Richmond, VA

Building B  
Elevations  
10/15/2011



RAYMOND LEROMAIN  
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1000 N. 10th St., Suite 100  
Tomball, TX 77375  
Tel: 281.358.7338 Fax: 281.358.7339  
www.raymondleromain.com

Permitted for Development Permit  
August 18, 2010  
Revised for Development Permit  
December 15, 2010  
Revised for Development Permit  
February 10, 2011  
Revised for Development Permit  
February 10, 2011  
Revised for Development Permit  
March 11, 2011  
Development Permit: Clarifications  
April 15, 2011  
Revised for Development Permit

#4.10

APR 20 2011

10544504



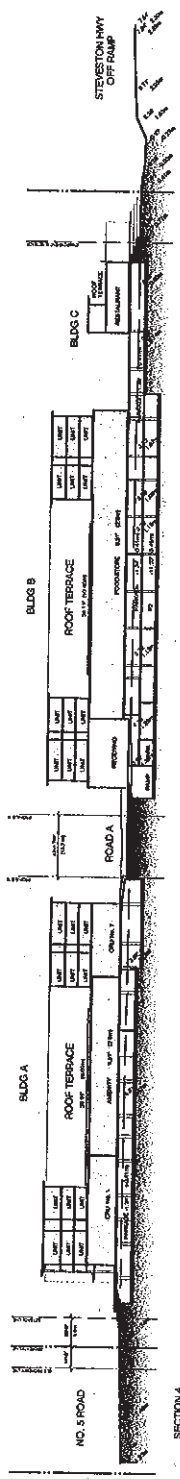
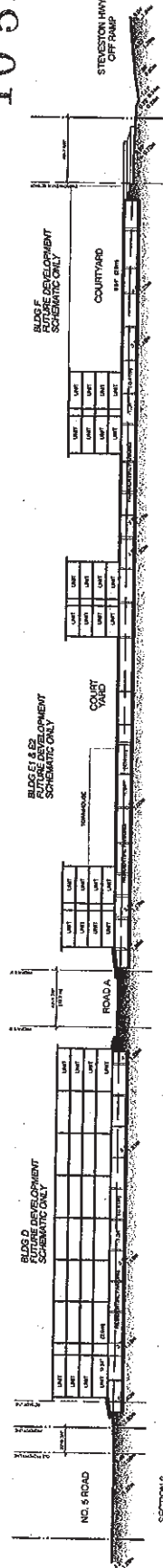
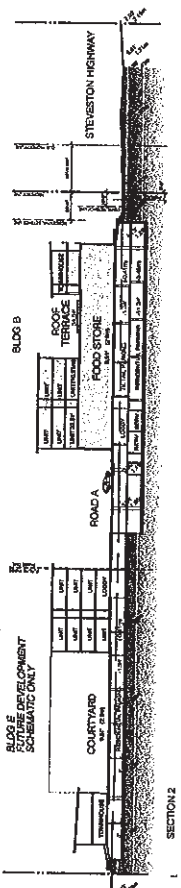
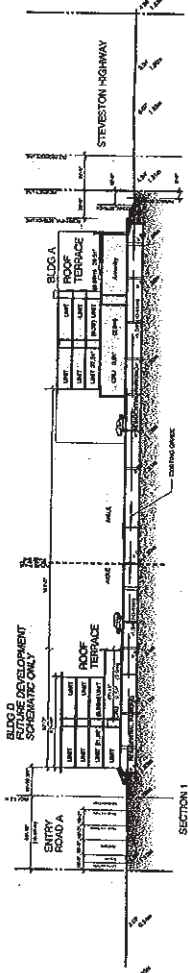
Townline Homes

GARDENS  
Phase One  
No. 5 Road & Stevenson Highway  
Houston, TX

Overall  
Site Sections  
10/20/10  
April 15, 2011

# Site Sections

SK-2.3







Resident	August 26, 2010 Issued for Development Permit
	December 15, 2010 Re-issued for Development Permit Issued for ALDP Review
	February 6, 2011 Re-issued for Development Permit
	March 11, 2011 Development Permit Cancellation
	April 15, 2011 Re-issued for Development Permit

#15

11-22-54

057450

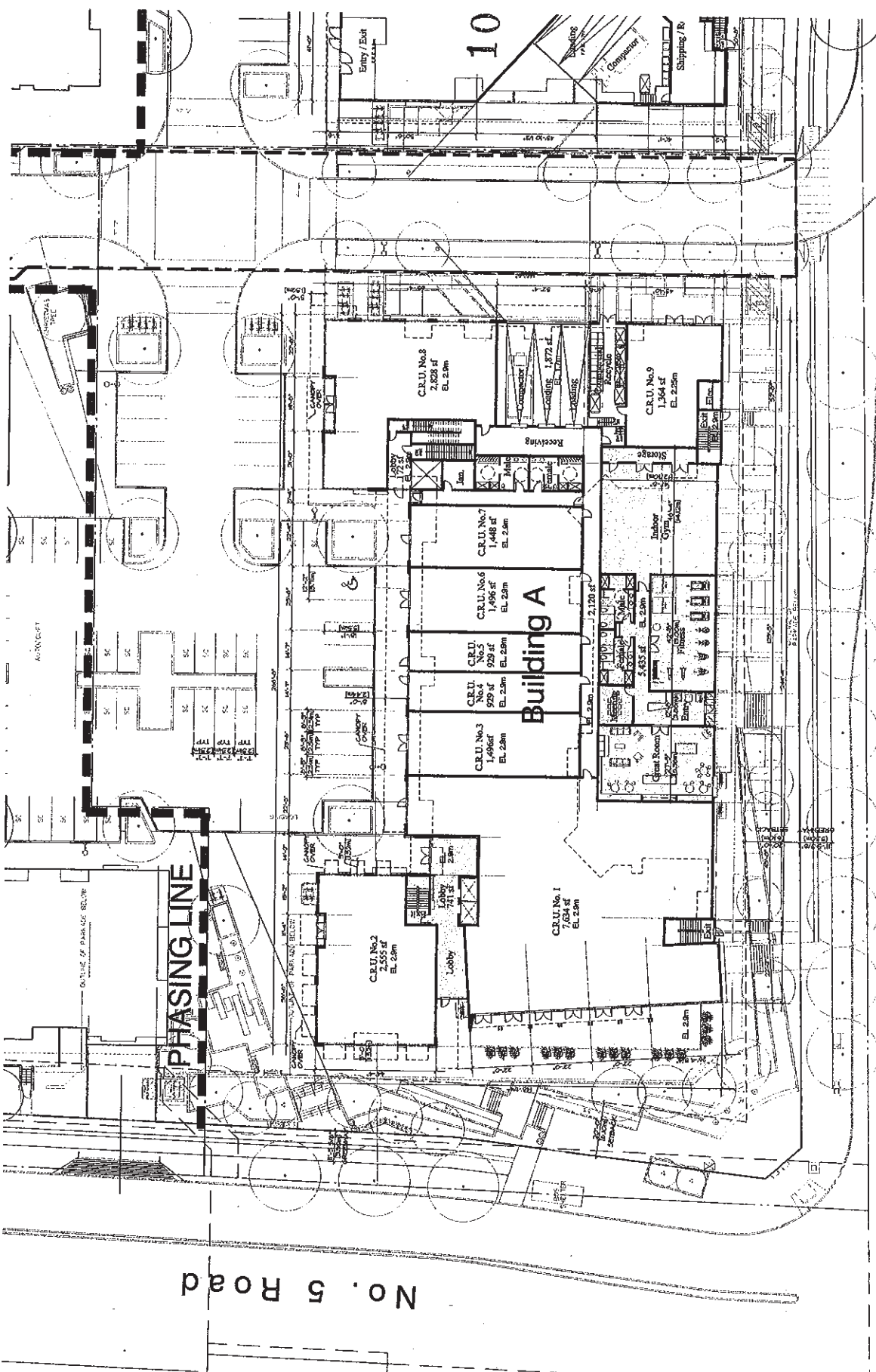


Phase One

No. 5 Road & Stavection Highway  
Richmond, B.C.

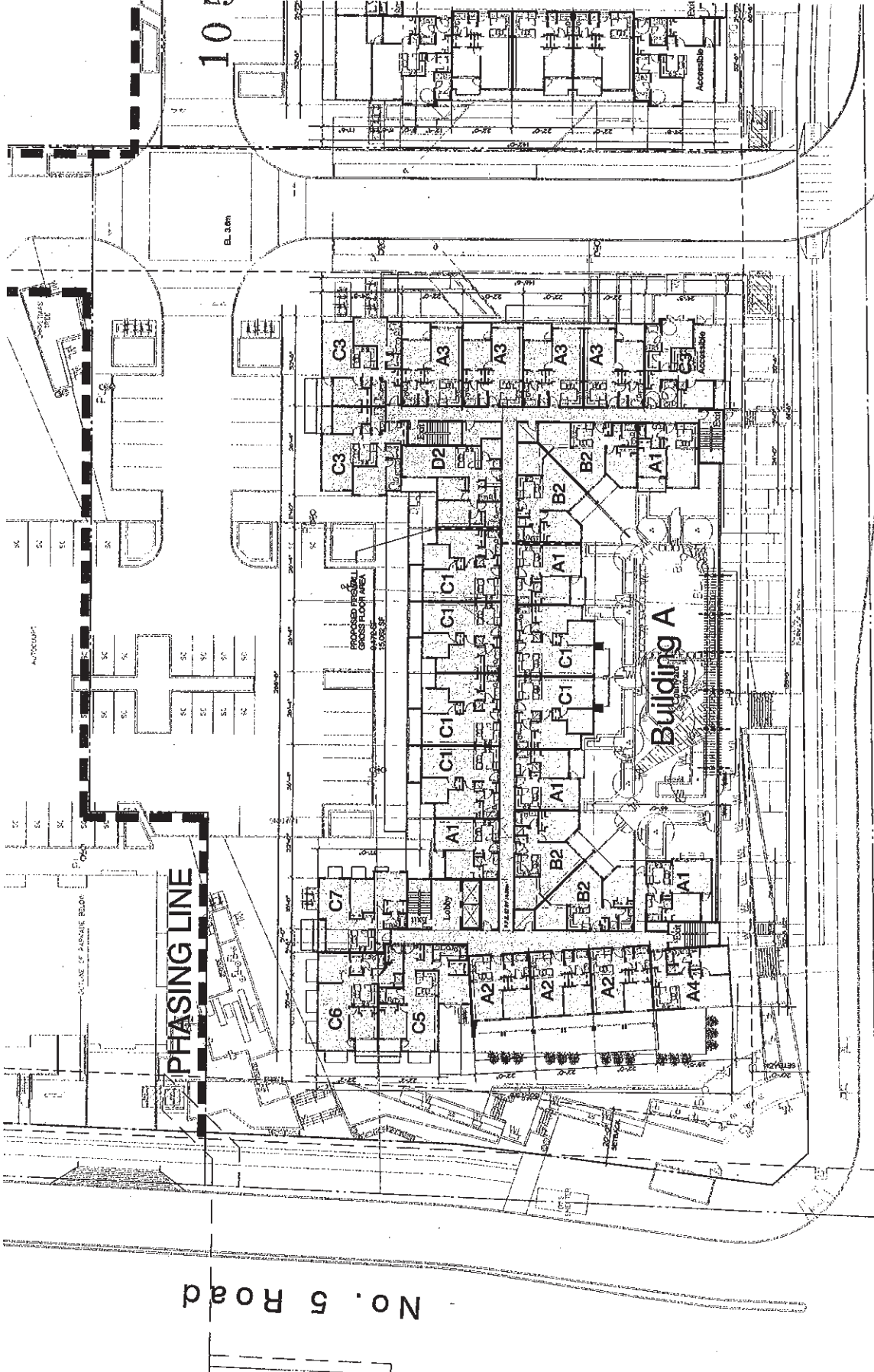
Building A  
Main Floor Plan

SK-4.0



# Steveston Highway

# Building A Main Floor Plan



Steveston Highway

Building A 2nd Floor Plan











RAYMOND LETEYMAN  
ARCHITECT  
1000 West 1st Avenue, Suite 100  
Portland, Oregon 97204  
Tel: 503.222.1111 Fax: 503.222.1112

10544507

August 20, 2010  
Issued for Development Permit  
December 15, 2010  
Revised for Development Permit  
February 1, 2011  
Revised for Development Permit  
March 11, 2011  
Revised for Development Permit  
April 15, 2011  
Revised for Development Permit

APR 20 2011

#5.5



Townline Homes

GARDENS  
Phase One

No. 5 Road & Steveston Highway  
Richmond, B.C.

Building A  
Roof Plan  
1/8" = 1'-0"  
April 15, 2011

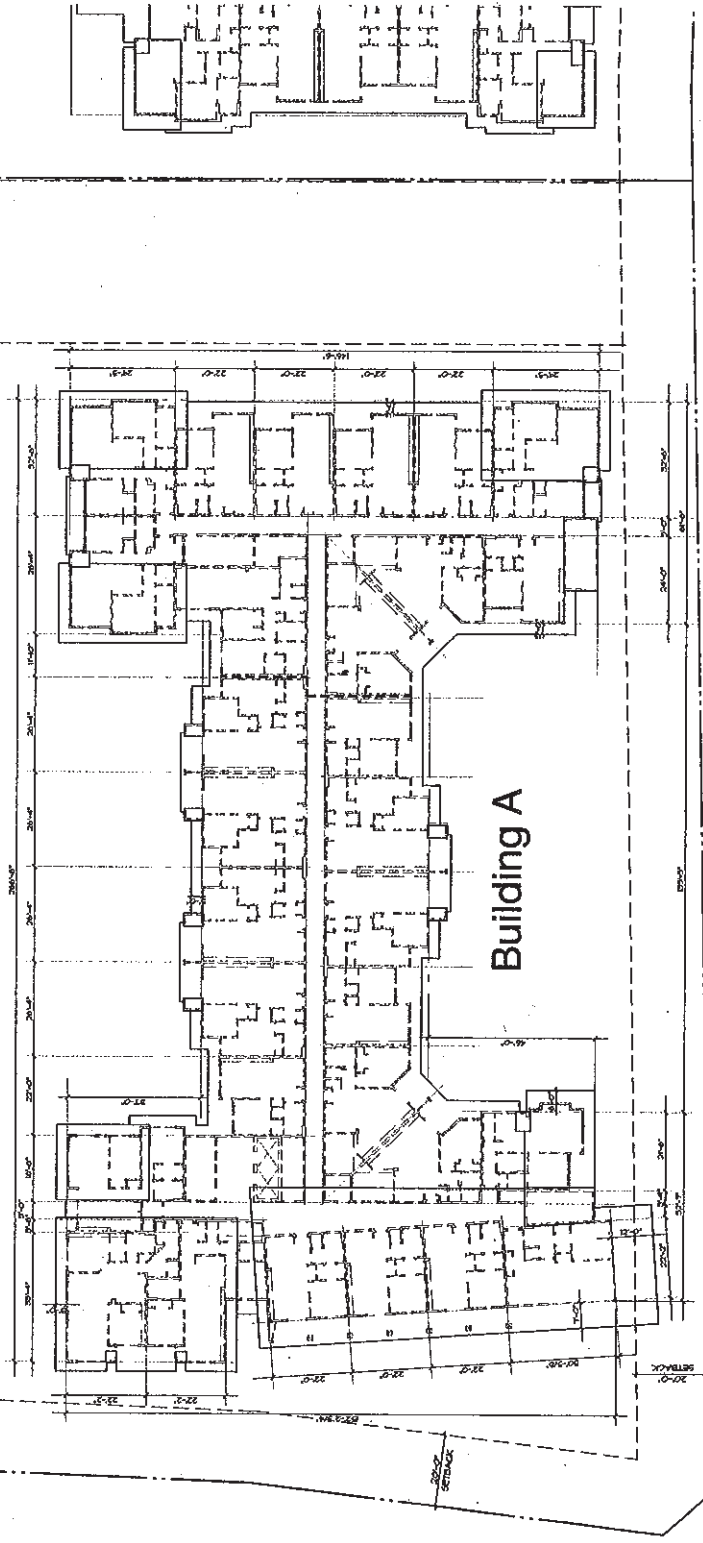


SK-4.4

Steveston Highway

Building A Roof Plan

No. 5 Road





**RAYMOND LERIVAI**  
 Architect  
 200-511-1111  
 200-511-1111  
 200-511-1111

August 28, 2001  
 Approved for Development Permit  
 December 12, 2001  
 Approved for Development Permit  
 February 1, 2001  
 Approved for Development Permit  
 March 11, 2001  
 Approved for Development Permit  
 April 13, 2001  
 Approved for Development Permit

#5.6

APR 20 2001

1054150



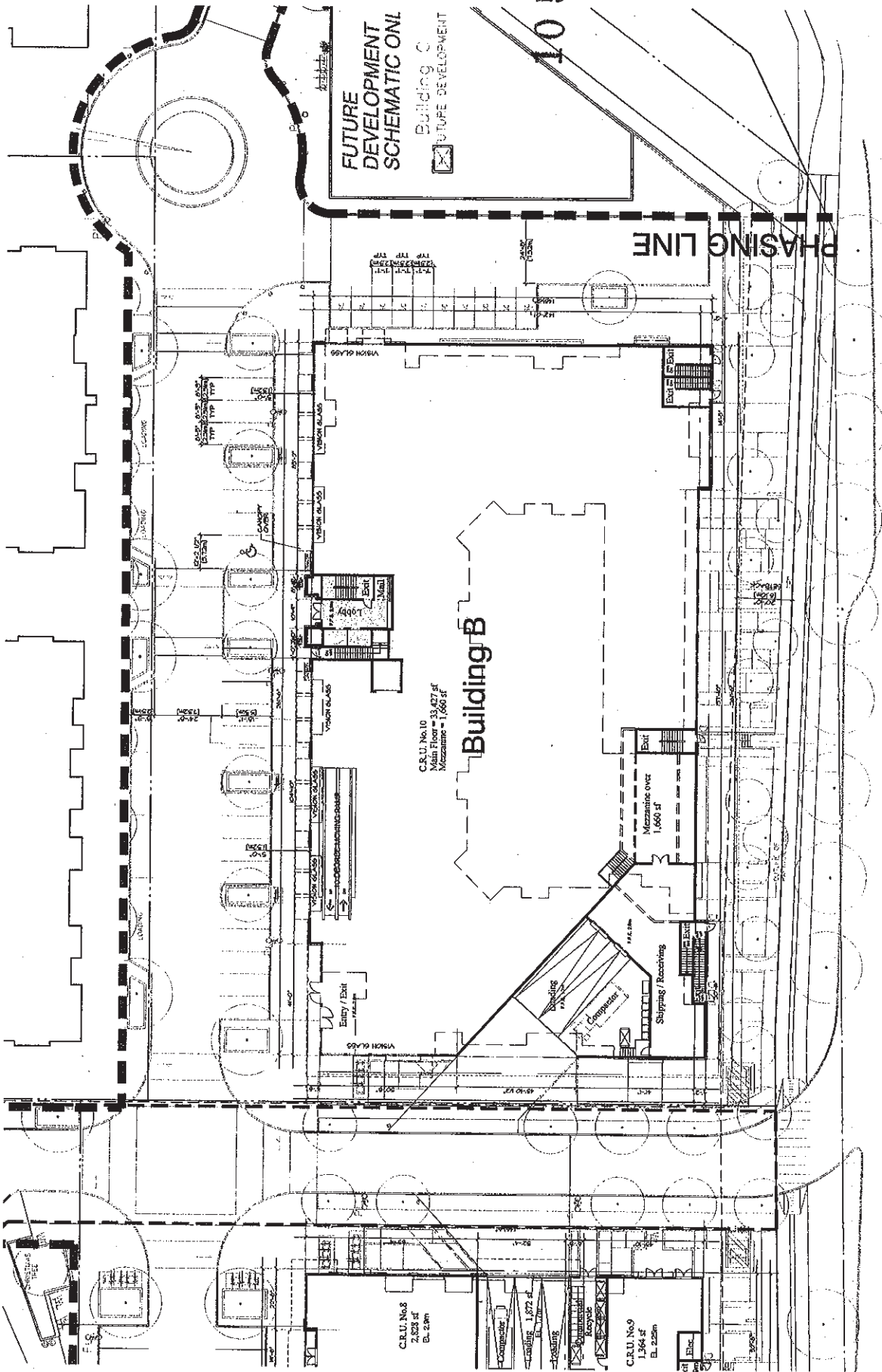
Townline Homes

GARDENS  
 Phase One

No. 5 Road & Steveston Highway  
 Richmond, B.C.

Building B  
 Main Floor Plan  
 1/8" = 1'-0"  
 April 13, 2001

SK-5.0



Steveston Highway

Building B Main Floor Plan







RAYMOND LERAMAN  
ARCHITECT, INC.  
10000 Steeles Ave. East, Unit 100  
Richmond, B.C. V6X 1K7  
Tel: 604.273.1111  
Fax: 604.273.1112  
www.raymondleraman.com

Project: 10000 Steeles Ave. East, Unit 100  
Richmond, B.C. V6X 1K7  
December 15, 2010  
Revised for Development Permit  
Revised for Development Permit  
Revised for Development Permit  
March 11, 2011  
Development Permit: Conditions  
April 15, 2011  
Revised for Development Permit

#5.7

APR 20 2011  
105450

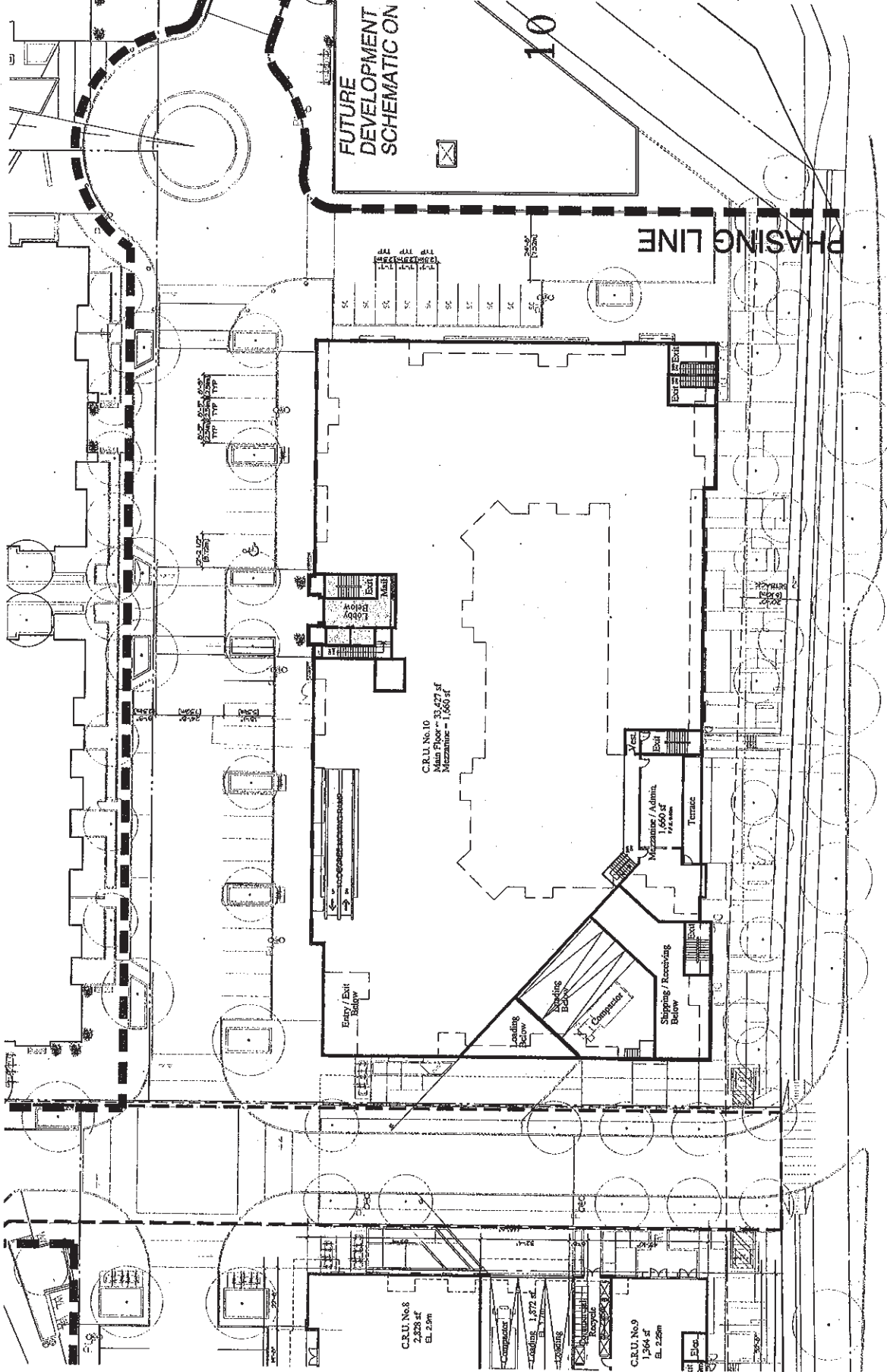


Townline Homes

GARDENS  
Phase One

No. 5 Street & Steveston Highway  
Richmond, B.C.

Building B  
Mezzanine Floor Plan  
V6X 1K7  
April 15, 2011



Steveston Highway

# Building B Mezzanine Floor Plan



SK-5.0.1



**RAYMOND LECKMAN**  
 ARCHITECTS  
 200-500 West 12th Avenue, Suite 100  
 Vancouver, B.C. V6H 1A1  
 Tel: 604-681-1111 Fax: 604-681-1112  
 Website: www.raymondleckman.com

**Project:**  
 August 26, 2010  
 Revised for Development Permit  
 December 12, 2010  
 Revised for Development Permit  
 Issued for A-20 Review  
 February 8, 2011  
 Revised for Development Permit  
 March 11, 2011  
 Development Permit Conditions  
 April 15, 2011  
 Revised for Development Permit

#5.0

APR 29 2011

10544504

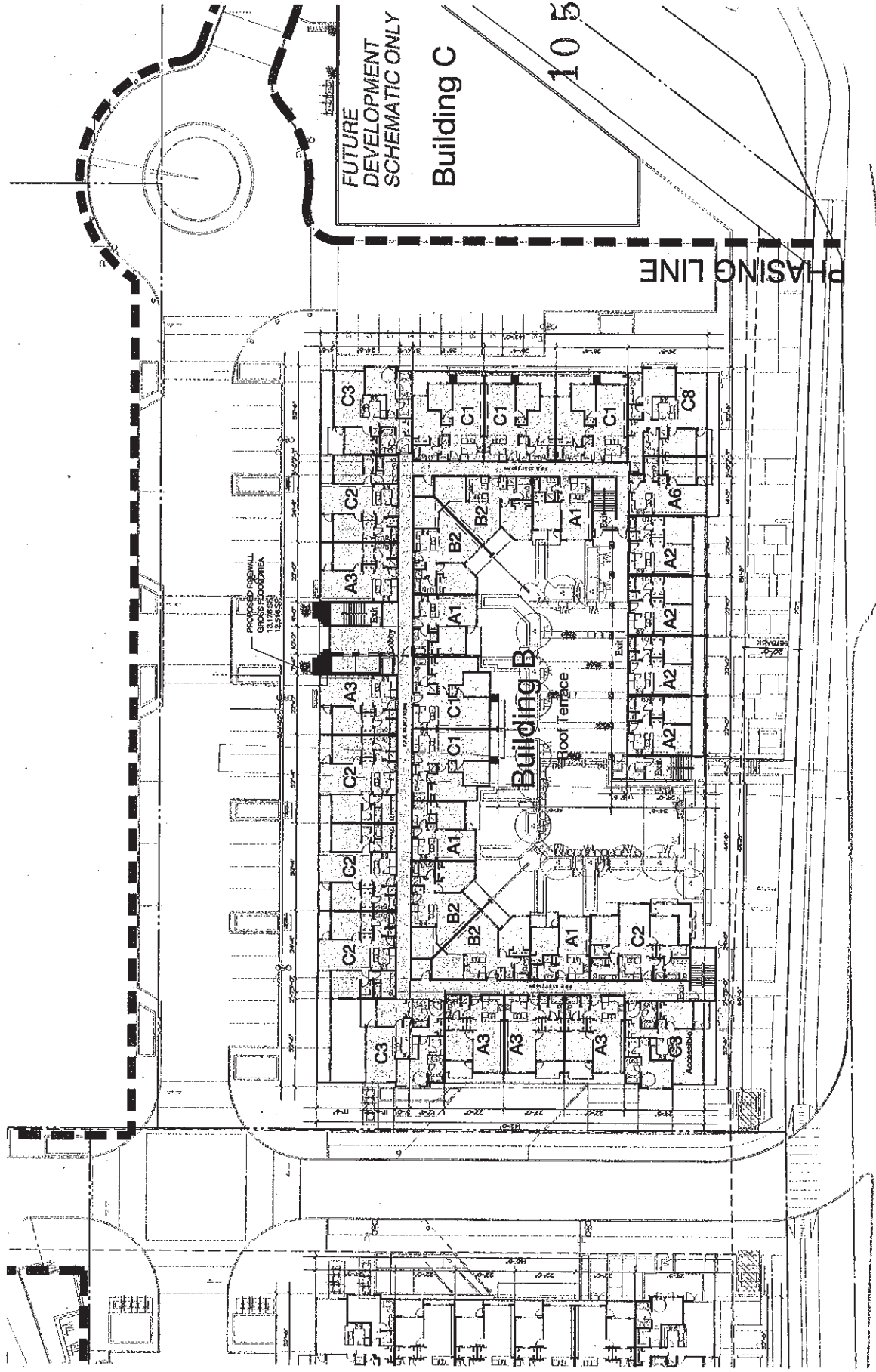


Townline Homes

GARDENS  
 Phase One

No. 5 Road & Steveston Highway  
 Richmond, B.C.

Building B  
 2nd Floor Plan  
 1/8" = 1'-0"  
 April 15, 2011



Steveston Highway

# Building B 2nd Floor Plan



SK-5.1



August 26, 2010  
Issued for Development Permit

December 15, 2010  
Re-issued for Development Permit

February 8, 2011  
Re-issued for Development Permit

March 11, 2011  
Development Permit Classification

April 15, 2011  
Re-issued for Development Permit

1127 02 2019

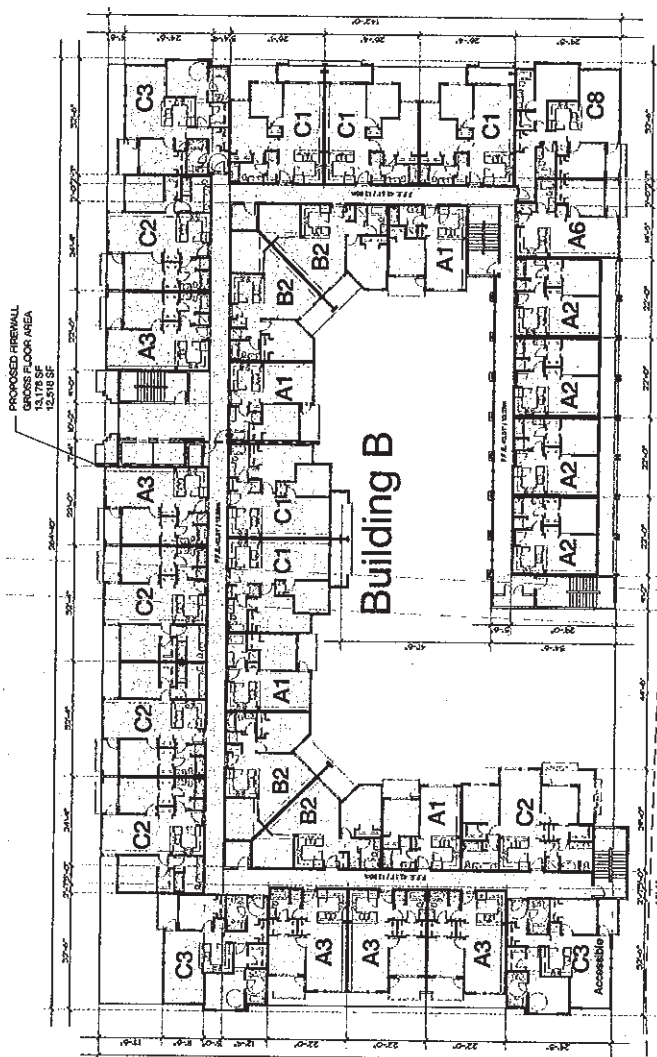


Phase One

No. 5 Road & Steveston Highway  
Richmond, B.C.

Building B  
3rd Floor Plan

SK-5.2



**FUTURE  
DEVELOPMENT  
SCHEMATIC ONLY**

## Building C

1057501  
APR 26 2011

# Steveston Highway

# Building B 3rd Floor Plan







Recovery	August 26, 2010 Issued for Development Permit
	December 15, 2010 Re-issued for Development Permit Issued for ALDP Review
	February 8, 2011 Re-issued for Development Permit
	March 17, 2011 Development Permit Confirmed
	April 15, 2011 Re-issued for Development Permit

05#

APR 25 1967

~~1054504~~



**Townline Homes**

# Phase One

No. 5 Road & Stoveston Highway  
Richmond, B.C.

Building B  
4th Floor Plan  
1/8" = 1'-0"  
April 15, 2011

# Steveston Highway

# Building B 4th Floor Plan



3-5-K-S



August 26, 2010  
No-issued for Development Permit

December 15, 2010  
No-issued for Development Permit  
No-issued for ALDP Review

February 8, 2011  
No-issued for Development Permit

March 11, 2011  
Development Permit Cancellations

April 15, 2011  
No-issued for Development Permit

## Building C

5-11

2007-01-01

1054704



# Phase One

No. 5 Road & Stevenson Highway  
Richmond, B.C.

### Building B Roof Plan

1945-1946

# Steveston Highway

# Building B Roof Plan



SK-5.4



RAYMOND LEIKEMAN  
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10000 West Lake Ave., Suite 200  
West Lake, Ohio 44140  
Tel: 440.333.1234  
Fax: 440.333.1235  
www.rleikeman.com

Project:  
August 22, 2018  
Approved for Development Permit  
October 15, 2018  
Revised for Development Permit  
Issued for ALP Review  
February 13, 2019  
Revised for Development Permit  
March 11, 2019  
Development Permit Conditions  
April 13, 2019  
Revised for Development Permit

#5.12

APR 20 2019

10544504



Townline Homes

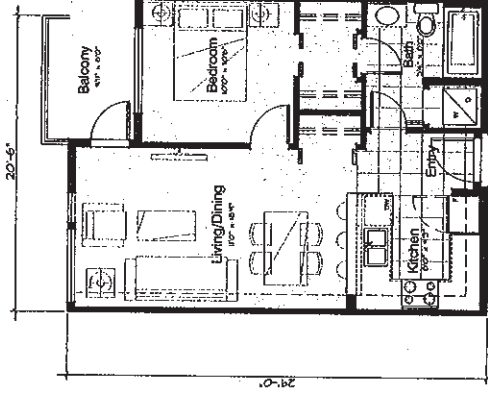
GARDENS  
Phase One

No. 5 Road & Stevenson Highway  
Barnhart, IL

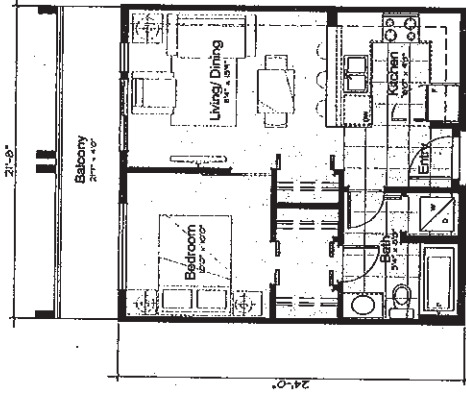
Unit Plans  
1/4" = 1'-0"  
April 15, 2019

# Unit Plans

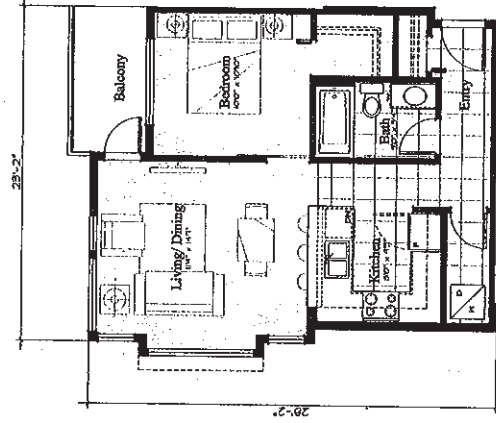
SK-6.0



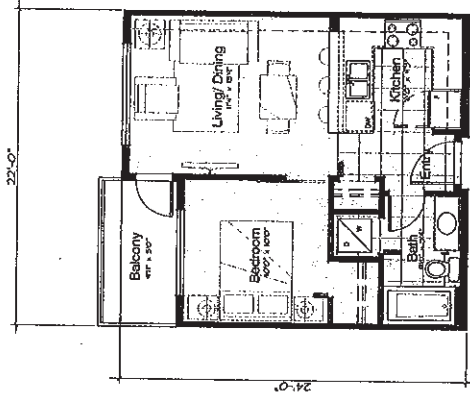
**Unit A3**  
1 Bedroom Standard  
Area: 585 sf



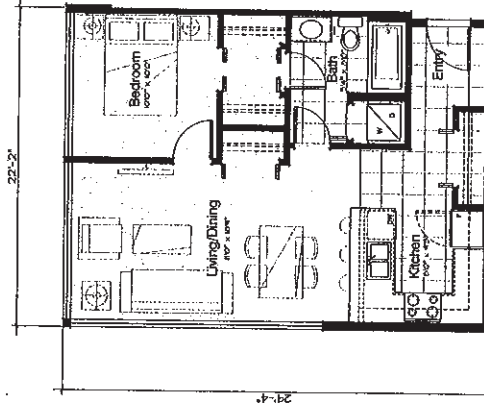
**Unit A2**  
1 Bedroom  
Area: 520 sf



**Unit A5**  
1 Bedroom  
Area: 602 sf



**Unit A1**  
1 Bedroom  
Junior  
Area: 481 sf



**Unit A4**  
1 Bedroom Standard  
Area: 546 sf





**RAYMOND LETZEMAN**  
 ARCHITECTS, INC.  
 100 West 10th Street, Suite 200  
 New York, NY 10011  
 Tel: 212 333 3333 Fax: 212 333 3334  
 www.raymondletzeman.com

August 20, 2010 Approved Permit  
 December 15, 2010 Reissued for Development Permit  
 Reissued for ALP/Station  
 February 6, 2011 Approved Permit  
 March 14, 2011 Development Permit  
 Development Permit Conditions  
 April 15, 2011 Reissued for Development Permit

#5.13

APR 20 2011

10544504



Townline Homes

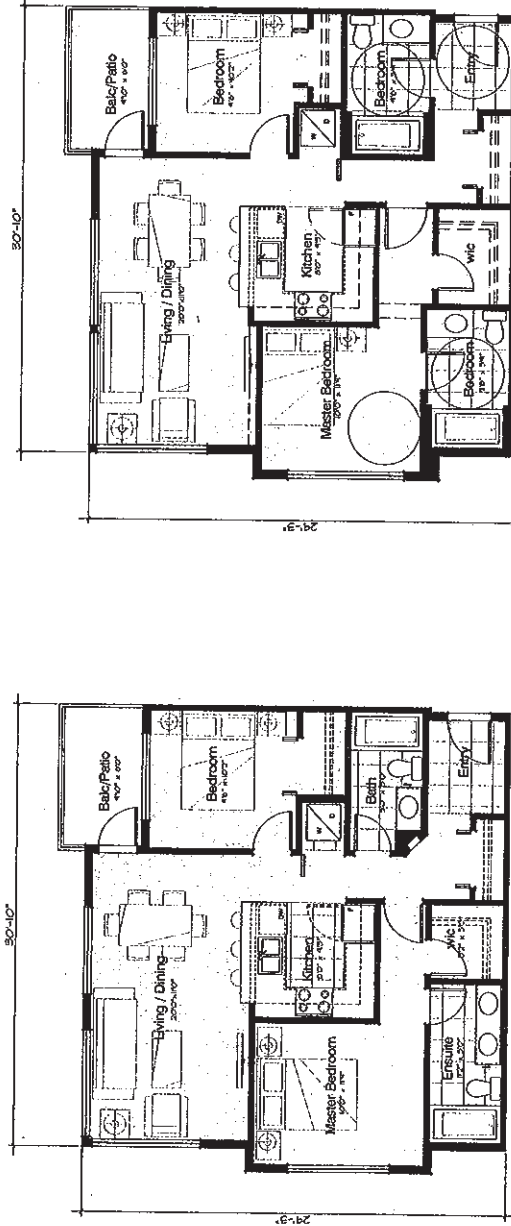
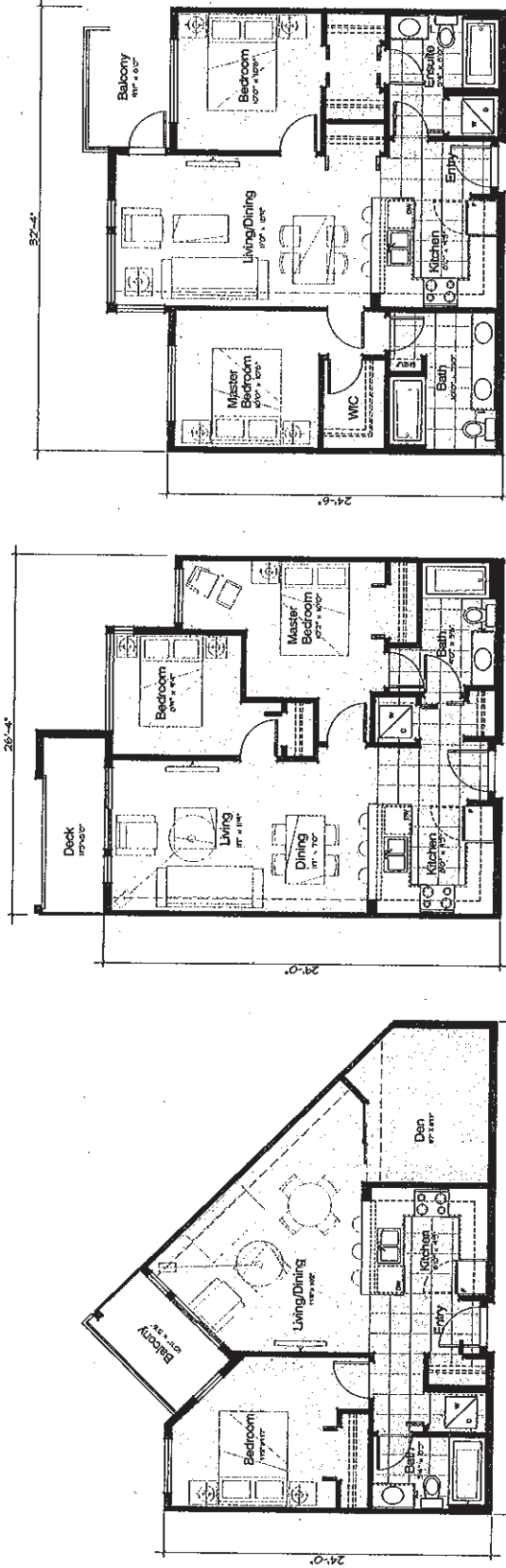
GARDENS  
 Phase One

No. 5 Road & Sheppard Highway  
 Richmond Hill, ON

Unit Plans  
 1/8" = 1'-0"  
 April 15, 2011

SK-6.1

# Unit Plans



145#

15-00000

057707



Townline Homes

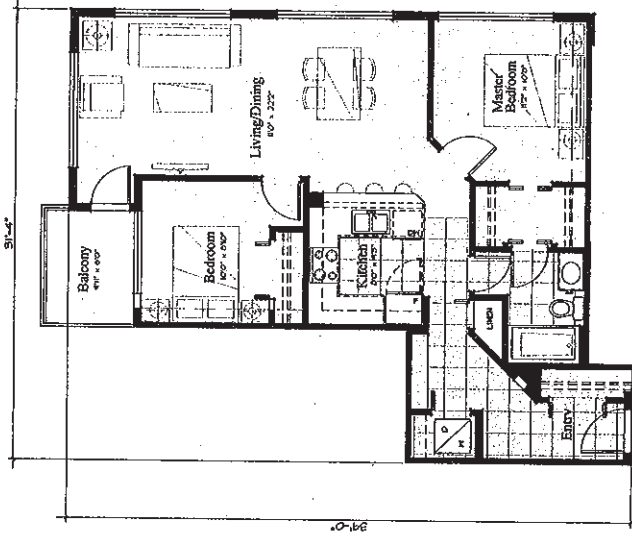
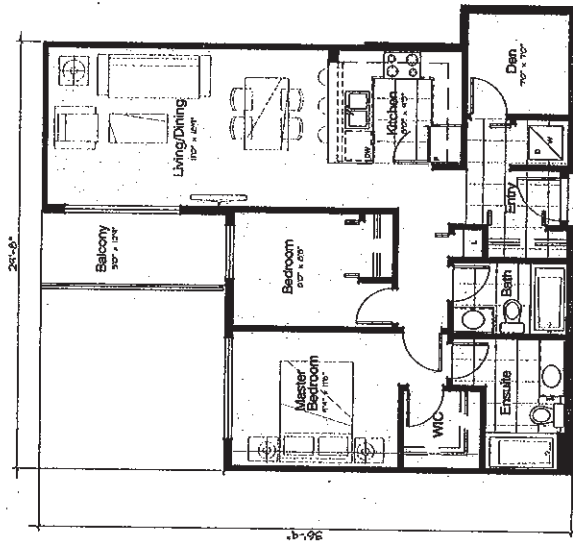
Phase One

No. 5 Road & Sleveston Highway  
Richmond, B.C.

Unit Plans  
7/4<sup>th</sup> - 7/4<sup>th</sup>  
April 15, 2011

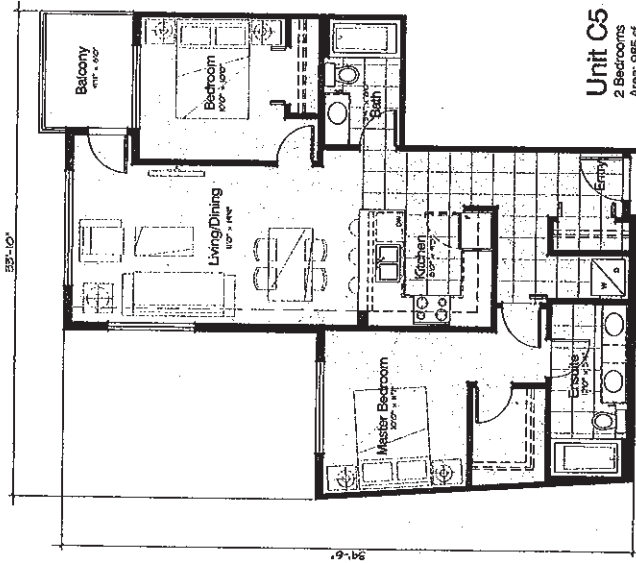
SK-6.2

**Unit C6**  
2 Bedrooms  
Area: 898 sf

Unit D2  
2 Bedrooms  
Area: 872 sf

## Unit Plans

**Unit C5**  
2 Bedrooms  
Area: 985 sf



**Unit C7**  
2 Bedrooms  
Area: 829 sf

