



City of
Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: October 4, 2023

From: Wayne Craig
Director, Development

File: DP 17-790086

Re: **Application by Jhujar Construction Ltd. for a Development Permit at 9291 and 9311/9331 No. 2 Road**

Staff Recommendation

That a Development Permit be issued which would permit the construction of 12 townhouse units and one secondary suite at 9291 and 9311/9331 No. 2 Road on a site zoned "Low Density Townhouses (RTL4)" with vehicle access provided via 9211 No. 2 Road.

Wayne Craig
Director, Development
(604-247-4625)

WC:el
Att. 4

Staff Report

Origin

Jhujar Construction Ltd. (Director: Harjan S. Johal and Satnam Johal) has applied to the City of Richmond for permission to develop 12 townhouse units at 9291 and 9311/9331 No. 2 Road with vehicle access through a Statutory Right-of-Way (SRW) registered on Title of the adjacent property to the north at 9211 No. 2 Road. One of the 12 townhouse units is proposed to contain a ground-level secondary suite and two of the 12 units will be designed in accordance with the convertible unit guidelines.

The site is being rezoned from “Single Detached (RS1/E)” and “Two-Unit Dwellings (RD1)” zones to “Low Density Townhouses (RTL4)” zone for this project under Bylaw 9749 (RZ 15-716773), which received third reading following the Public Hearing on October 16, 2017. The site is currently vacant. A Servicing Agreement for frontage beautification works and service connections is required prior to Building Permit issuance.

Development Information

Please refer to the attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the north: A ten-unit townhouse complex on a lot zoned “Low Density Townhouses (RTL4)”.

To the east: Across No. 2 Road, a four-storey senior’s apartment building (three-storeys over parking) on a lot zoned “Medium Density Low Rise Apartments (RAM1)” and a church on a lot zoned “Assembly (ASY)”.

To the south: Existing single-family homes on lots zoned “Single Detached (RS1/E), which are identified for townhouse development under the Arterial Road Land Use Policy.

To the west: Existing single-family dwellings on lots zoned “Single-Detached (RS1/B)”.

Rezoning and Public Hearing Results

The Public Hearing for the rezoning of this site was held on October 16, 2017. No concerns regarding the rezoning application were expressed at the Public Hearing.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan (OCP) and is generally in compliance with the “Low Density Townhouses (RTL4)” zone.

Advisory Design Panel Comments

The Advisory Design Panel (ADP) has reviewed the project and supports it. A copy of the relevant excerpt from the Advisory Design Panel Minutes from Wednesday, October 19, 2022, is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '*bold italics*'.

Analysis

Conditions of Adjacency

- The proposed height, siting and orientation of the buildings generally address the massing of the surrounding residential developments.
- While three-storey units are proposed along No. 2 Road, the building height is stepped down to two-storeys along the side property lines to provide a transition to the adjacent single-family home to the south and the two-storey end units of the townhouse development to the north.
- Two-storey units in duplex and detached unit form are proposed along the rear (west) property line to minimize privacy and overlook concerns.
- The site grade along the north property line will be raised to match the existing grade of the adjacent townhouse development to the north; no new retaining wall will be required.
- Along the rear (west) property line, the existing site grade will be maintained to provide an appropriate transition to the adjacent single-family properties to the west, and to accommodate tree retention on-site and on the neighbouring properties.
- The site grade along the south property line will be raised, except for the area at the southwest corner of the site adjacent to a protected tree. The maximum height of the proposed retaining wall is approximately 1.0 m. The heights of the wood fences proposed on top of the retaining wall will be reduced to approximately 1.2 m in order to ensure the overall privacy screen height (retaining wall plus wood fence) is no taller than 1.8 m.
- Three hedgerows and two trees located along the west and south property lines will be retained; a variety of trees will also be planted along the rear and side property lines to enhance the interfaces between the proposed townhouse development and the existing adjacent residential developments. Further details in the Landscape Design section below.
- Perimeter drainage will be required as part of the Building Permit to ensure storm water is managed and addressed through the development and will not impact the neighbouring properties.

Transportation and Site Access

- Vehicle access is proposed through an existing Statutory Right-of-Way (SRW EPP51370, CA5001638-CA5001643) registered over the drive aisle of the neighbouring townhouse development to the north at 9211 No. 2 Road; no direct vehicle access to No. 2 Road is permitted from the subject site.
- This access arrangement was secured through the rezoning and development applications for 9211 No. 2 Road, which received final adoption by Council on March 29, 2016 (RZ 12-620563).
- A legal opinion prepared by the applicant's lawyer confirms that the City can rely on this SRW. This legal opinion has also been reviewed by the City's Law Department.

- Staff requested that the applicant discuss the use of the SRW with the adjacent strata, in order to address any concerns with the shared use of the driveway, as well as day-to-day issues such as maintenance and wayfinding signage.
- The applicant has made offers to cost share but no formal agreement has been reached between the developer and the neighbouring strata. Discussions are ongoing. A summary report prepared by the developer's solicitor is presented in Attachment 3.
- Should the Development Permit application proceed, staff would continue to encourage discussions between the two parties on the shared use of the driveway through the Building Permit process.
- Properties to the south of the subject site are also designated for townhouse development. A Statutory Right-of-Way (SRW) allowing access to/from the adjacent future development sites through the subject site (internal drive aisle) has been secured at rezoning.
- Signage indicating that the driveway on the subject site may connect to the future adjacent townhouse developments will be installed at the south end of the drive aisle so that future residents/owners/strata of the subject development are aware that they may be required to provide access to the south.
- Servicing Agreement is required as a condition of Building Permit issuance and includes, but is not limited to, a new 1.5 m concrete sidewalk at the property line and minimum 1.5 m wide landscaped boulevard with street trees behind the existing curb.

Urban Design and Site Planning

- The layout of the townhouse units is oriented around the north-south internal manoeuvring aisle, providing access to the unit garages.
- Units along No. 2 Road will have direct pedestrian access from the street, and units along the rear (west) property line will have access from the internal drive aisle.
- One of the units will contain a ground-level secondary suite (studio) of approximately 37.4 m² (403 ft²) in size. No additional parking stall is required for the proposed secondary suite since the required parking spaces for the unit containing a secondary suite are provided in a side-by-side arrangement.
- All townhouse units will have two vehicle parking spaces in a double-car garage. The proposal will feature four units with a total of eight spaces in a tandem arrangement (33 per cent of total required residential parking spaces), which is consistent with the maximum 50 per cent of tandem parking provision of Richmond Zoning Bylaw 8500. A restrictive covenant to prohibit the conversion of the tandem garage area into habitable space has been secured at rezoning.
- A total of three visitor parking spaces, including one van accessible parking space, will be provided throughout the site. The number of visitor parking spaces proposed is in compliance with the minimum bylaw requirement.
- Both internal and external bicycle parking spaces have been incorporated into the proposal and are in compliance with the Zoning Bylaw requirements.
- The provision of private outdoor spaces complies with the Development Permit Guidelines of the OCP. All units will have private outdoor spaces consisting of a front or a rear yard; the three-storey units will also have a covered deck on the second floor facing the internal drive aisle.

- Outdoor amenity space is centrally located on the subject site. The size and location of the outdoor amenity space is appropriate in providing open landscape and amenity space convenient to all units.
- No indoor amenity space is proposed on site. A \$12,000.00 cash-in-lieu contribution has been secured as a condition of rezoning approval, consistent with the OCP.
- A covered mailbox kiosk is proposed at the entrance to the outdoor amenity area.
- A walkway is proposed through the outdoor amenity space to provide direct pedestrian access from No. 2 Road to the internal drive aisle of the site.
- The required garbage, recycling and organic waste storage enclosure has been incorporated into the design of the building proposed at the northwest corner of the site (Building #1) to minimize the visual impact.

Architectural Form and Character

- The exterior form and materials are reminiscent of heritage-style architecture. The building's character includes several elements common to a heritage design, including classic hip roof form with gables and dormers on the upper floor.
- A pedestrian scale is generally achieved along the public sidewalk and internal drive aisle through the inclusion of variation in building projections, recesses, variety of materials/colours and landscape features.
- The impact of blank garage doors has been mitigated with panel patterned doors, transom windows and planting islands along the drive aisle.
- The proposed building materials (asphalt roof shingles, Hardi panel siding, face brick, stucco, wood fascia/trim) are generally consistent with the OCP Guidelines and compatible with the existing single-family character of the neighbourhood.
- A palette of earthy colours with white as an accent colour on the main cladding materials, are generally consistent with the heritage design of the project.

Landscape Design and Open Space Design

- Tree preservation was reviewed at rezoning stage:
 - Significant efforts were made to ensure the retention of one 40 cm calliper English Oak tree (Tag# 681) located along the site's No. 2 Road frontage which will be retained and protected in a tree well proposed in the front yard of the site. A survival security in the amount of \$10,000.00 will be required prior to Development Permit Issuance.
 - One hedgerow (five stems) identified as (tag# 687) located along the rear property line of the site will be retained and protected within the rear yard of Building #2. A survival security in the amount of \$25,000.00 will be required prior to Development Permit Issuance.
 - Eight trees and two hedgerows were identified for removal at the Rezoning stage and have been removed under Tree Permits T3 15-717055 due to their poor condition (either dead, dying, had been previously topped or exhibited structural defects).
 - Based on the 2:1 tree replacement ratio stated in the OCP, 16 replacement trees are required for the removal of eight bylaw-sized trees.
 - The applicant is proposing to plant 38 replacement trees on-site, including 15 conifers and 23 deciduous trees.

- One tree (Tag #N03) and two hedgerows (Tag #N02 & #N04) located on the adjacent properties to the west, as well as one tree (Tag # N01) located on the adjacent property to the south, are to be retained and protected.
- Tree protection fencing is required to be installed as per the Arborist Report recommendations prior to any construction activities (including demolition) occurring on-site.
- The proposal provides for a pedestrian-oriented streetscape fronting No. 2 Road with a landscaped edge treatment including brick pilasters, aluminum fencing and gates, as well as new trees and sodded lawn in the front yards.
- All units along the rear (west) property line will have a private yard with a raised deck and a sodded lawn area.
- Various hedges, shrubs and ground covers, as well as perennials and grasses have been selected to ensure the landscape treatment remains interesting throughout the year.
- Landscaping including Japanese cherry trees will be provided along the north-south internal drive aisle to soften the internal streetscape.
- A children's play area is proposed for the outdoor amenity area and play equipment has been chosen to fit into the outdoor amenity space and to provide different play opportunities. A bench is also provided for caregivers.
- Surface parking spaces and designated walkways on-site will be treated with permeable pavers for better water infiltration and variety in paving surfaces.
- An on-site irrigation system is proposed to ensure continued maintenance of live landscaping.
- Lighting plans are included in the DP plans, which indicates various lighting provided throughout the site. Wall mounted sconce lighting will be provided at each unit entry and by the garage doors. Bollard lighting will be provided throughout the site in the common areas including the outdoor amenity space and visitor parking areas, etc.
- In order to ensure that the proposed landscaping works are completed, the applicant is required to provide a landscape security of \$310,750.00 in association with the Development Permit.

Crime Prevention Through Environmental Design

- A combination of planting and fencing is proposed along the perimeter of the site to define the public, semi-public and semi-private realms.
- All pedestrian access points to the site, including access to the townhouse units, are to be gated to provide a visual and physical separation from public circulation paths.
- All walkways, outdoor amenity spaces and visitor parking stalls can be viewed directly from windows within the project, providing visual surveillance from the townhouse units.
- All exterior lighting will be down-directed or ground-oriented to eliminate the impact on the townhouse units and adjacent properties.

Sustainability

- The Step Code Target for the townhomes is Level 3 with Low Carbon Energy System (LCES). To achieve this requirement, the following items will be included in the design:
 - High-performance envelope;
 - Low window-to-wall ratio;
 - Low-E double pane glazing;

- Heat Recovery Ventilators (HRV); and
- Heat pumps for heating and domestic hot water.
- Level 2 EV charging will be provided in each garage as per Richmond Zoning Bylaw 8500.

Accessible Housing

- The proposed development includes two convertible units that are designed with the potential to be easily renovated to accommodate a future resident in a wheelchair. The potential conversion of this unit will require the installation of a chair lift (where the staircase has been dimensioned to accommodate this in the D1 units) in the future if desired.
- All of the proposed units incorporate aging-in-place features to accommodate mobility constraints associated with aging. These features include:
 - Stairwell handrails.
 - Lever-type handles for plumbing fixtures and door handles.
 - Solid blocking in washroom walls to facilitate future grab bar installation beside toilets, bathtubs and showers.

Conclusions

As the proposed development would meet applicable policies and Development Permit Guidelines, staff recommend that the Development Permit be endorsed, and issuance by Council be recommended.



Edwin Lee
Planner 2
(604-276-4121)

EL:he

- Att. 1: Development Application Data Sheet
2: Excerpt from the Minutes of the Advisory Design Panel Meeting – October 19, 2022
3: Summary Letter – Negotiation with Neighbour Strata with respect to use of SRW
EPP51370
4: Development Permit Considerations



DP 17-790086

Attachment 1

Address: 9291 and 9311/9331 No. 2 Road

Applicant: Jhujar Construction Ltd.

Owner: Jhujar Construction Ltd.

Planning Area(s): Blundell

Floor Area Gross: 2,084.7 m²

Floor Area Net: 1,452.9 m²

	Existing	Proposed
Site Area:	2,482 m ²	2,452 m ²
Land Uses:	Single-Family Residential / Two-Family Residential	Multiple-Family Residential
OCP Designation:	Low-Density Residential	No Change
Zoning:	Single Detached (RS1/E) and Two-Unit Dwellings (RD1)	Low Density Townhouses (RTL4)
Number of Units:	3	12

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.60	0.59	none permitted
Lot Coverage – Building:	Max. 40%	40%	none
Lot Coverage – Non-porous Surfaces:	Max. 65%	63%	none
Lot Coverage – Landscaping:	Min. 25%	26%	none
Setback – Front Yard (m):	Min. 6.0 m	6.2 m	none
Setback – North Side Yard (m):	Min. 3.0 m	3.0 m	none
Setback – South Side Yard (m):	Min. 3.0 m	3.0 m	none
Setback – Rear Yard (m):	Min. 3.0 m	5.1 m	none
Height (m):	Max. 12.0 m (3 storeys)	11.42 m (3 storeys) along No. 2 Road & 8.31 m (2 storeys) along west property line	none
Lot Width:	Min. 50.0 m	50.25 m	none
Lot Depth:	Min. 35.0 m	45.12 m	none
Off-street Parking Spaces – Regular (R) / Visitor (V):	2 (R) and 0.2 (V) per unit	2 (R) and 0.2 (V) per unit	none
Off-street Parking Spaces – Total:	24 (R) and 3 (V)	24 (R) and 3 (V)	none

Tandem Parking Spaces:	Max. 50% of proposed residential spaces in enclosed garages (24 x Max. 50% = 12)	8	none
Small Car Parking Spaces	None when fewer than 31 spaces are provided on site	0	none
Handicap Parking Spaces:	Min. 2% of parking stalls required when 3 or more visitor stalls are required (27 x 2% = 1 space)	1	none
Bicycle Parking Spaces – Class 1 / Class 2:	1.25 (Class 1) and 0.2 (Class 2) per unit	1.3 (Class 1) and 0.3 (Class 2) per unit	none
Off-street Bicycle Parking Spaces – Total:	15 (Class 1) and 3 (Class 2)	16 (Class 1) and 4 (Class 2)	none
Amenity Space – Indoor:	Min. 70 m ² or Cash-in-lieu	Cash-in-lieu	none
Amenity Space – Outdoor:	Min. 6 m ² x 12 units = 72 m ²	90 m ²	none

Excerpt from the Minutes from
The Design Panel Meeting

Wednesday October 19, 2022 – 4:00 p.m.
Remote (Webex) Meeting

1. **DP 17-790086 – 12 UNIT TOWNHOUSE PROPOSAL WITH VEHICLE ACCESS VIA THE ADJACENT SITE TO THE NORTH**

ARCHITECT: DF Architecture Inc.

LANDSCAPE ARCHITECT: Hapa Collaborative

PROPERTY LOCATION: 9291 and 9331 No. 2 Road

Applicant's Presentation

Zubin Billimoria, DF Architecture Inc., David Rasouli, Alphatec Energy Inc., and Alex Fu, Hapa Collaborative, presented the project and answered queries from the Panel.

Panel Discussion

Comments from Panel members were as follows:

- commend the landscape team for their efforts to come up with a well thought out landscape design for a small townhouse development;
- providing three walkways between No. 2 Road and the internal drive aisle is excessive due to the small size of the proposed development; consider retaining just the main pedestrian walkway through the proposed common outdoor amenity area and utilize the space for the other two proposed walkways for installation of additional soft landscaping in lieu of the proposed concrete paving treatment for the walkway;

Walkway along the North property line has been eliminated and replaced with the landscaping. 3ft wide maintenance access to the electrical room has been provided.

Walkway connecting to the sidewalk previously along the South property line has been revised and a 5ft. wide Firefighters access has been provided to access the main entrance of the unit D1 in building 4. A landscaping buffer has been provided at the South property line along this Firefighters access.

- use of space would have been more efficient and the building appearance would have been better if the three two-storey buildings in the rear were consolidated into one continuous building with five units;

According to the OCP, 14.4.13.C, rear yard building form should be limited to two (2) units in the townhouse cluster (duplex).

- review the provision of a lock-off unit in Unit B2 in Building No. 3 as it compromises the design of Unit B2, resulting in a very tight living, dining and kitchen areas and limits the opportunity to provide an adequate number of windows to the unit, thus reducing daylight penetration into the unit; consider eliminating the lock-off unit and integrating the space into Unit B2 to improve its livability;

The lock-off unit has been redesigned to improve livability of the unit by increasing the kitchen and living area of the unit due to reconfiguration of the washroom, relocation of washer/dryer, moving the washroom and providing a larger closet.

- concern about the proposed location of heat pumps due to their potential noise impacts is moot as newer models of heat pumps are very quiet and compact and could be located more efficiently in the project as they will not pose noise concerns;

The heat pump units have been moved out of the South side yard for building 4. For building 5 (North side yard), 3 out of 4 condenser units have been moved to the decks on upper floor of the building. One unit stays in the side yard but has been moved away from the property line as much as possible with adequate Landscape buffer to mitigate acoustic concerns.

For buildings 1,2 and 3 (rear yard), the condenser units have been moved to the patio of these units and landscape buffers have been introduced for all the patio to mitigate the noise.

- appreciate the drawings and site layout presented by the applicant;
- support the Panel comment that the provision of three pedestrian walkways is not necessary; the north and middle walkways would be sufficient and ensure that these walkways are accessible for people with disabilities; the space for the south walkway could be utilized for additional soft landscaping;

Walkway along the north property line has been eliminated and replaced with the landscape. 3ft wide maintenance access to the electrical room has been provided.

Walkway connecting to the sidewalk previously along the South property line has been revised and a 5ft. wide Firefighters access has been provided to access the main entrance of the unit D1 in building 4. A landscaping buffer has been provided at the South property line along this Firefighters access.

- agree with the Panel comment that the proposed location of heat pumps should not be a concern as newer models of heat pumps do not pose noise concerns;

See responses above.

- look at the proposed location of the heat pump for Unit B1 to ensure that it will not block the window at the back of the unit;

Heat pumps will not block these windows as they are placed on the patio and the heat pump is 2'6" high. There is a 2'2" gap between the sill of the window and top of the heat pump.

- the roof massing appears heavy; the dark colour of dormers make them look heavy; consider a lighter colour for the dormers;

Color and material for all dormers has been revised. Stucco siding with same color has been used for the dormers making the overall massing look much lighter. Battens and trims have been eliminated except for the trims around the windows to give an accentuated appeal to the overall design. Trim colors have also been revised to a lighter color.

- not concerned with the decorative pickets on top of the entry porches as they look nice;

Noted

- north elevation of Building 4 and south elevation of Building 5 should be treated as a front façade;

Elevations for building 4 & 5 facing the outdoor amenity have been revised by adding a band to give the building a distinct top, middle and base. Also, the dormers facing the amenity area have been revised with the same treatment as the dormer of other buildings.

- consider installing additional glazing on the north elevation of Building 1 to provide passive surveillance opportunity over the vehicle entrance to the development and the visitor parking area;

North elevation of building 1 has been revised with overall massing being reduced by using a lower slope for the roofs and addition of another window on this side to improve passive surveillance opportunity over the vehicular entrance to the development and the visitor parking area.

- review the accessibility of the washer and dryer for Unit D1 (Convertible Unit) on the second floor; ensure that they are accessible to people with disabilities, particularly people in wheelchairs;

This has been considered and changes have been made.

- like the proposed landscaping around the existing oak tree and wood deck that cantilevers over the tree protection zone; confirm with the project arborist whether the sod lawn around the tree could be trampled on; if the tree roots need to be protected, the wood deck could be extended over;

The suggestion of extending wood platform in the ADP comments cannot be implemented, because there is not much available space near the existing tree, most of the areas are in the root protection zone.

- appreciate the creative use of space around the existing oak tree; similar treatment could be extended to the children's play area through installation of wood platforms, extended wood beam and wood natural features in lieu of the slide to make the whole outdoor amenity space cohesive and interesting;

A wooden playhouse is proposed in the Children's play area as the main play equipment which can continue the natural interestingness to the children's play area.

- consider smaller sodded lawns at the rear yards of units but with more planting; consider increasing the size of the decks at the back if there are no private roof decks provided;

More planting area and trees have been added in the rear yards.

- consider increasing the density of planting, e.g. installing evergreen trees, along the west property line to provide more screening from adjacent properties and more greenery during winter;

Five Serbian Spruce as evergreen trees have been added along the west property line to provide more privacy and greenery.

- there are a lot of trees on the planting strip immediately adjacent to the buildings facing No. 2 Road; consider providing a drain strip along the buildings and relocating the trees to the sidewalk edge to avoid conflict between the buildings and the trees' canopies and roots;

Trees were adjacent to the buildings have been moved to the sidewalk edge.

- review the proposed planting of Japanese Skimmia on the south side as they do not grow well under the sun; ensure that shade plants are on the north side and sun plants are on the south side;

All Skimmias on the south side have been replaced with sun plants like Nandina domestica 'Gulf Stream' and Kinnikinnick. All plants on the north side are adaptive to the shade environment.

- support Panel comments regarding appreciation for applicant's efforts to incorporate existing trees into the proposed development;

- review the play value of the proposed play equipment and consider installing natural play elements; the landscaping for the whole outdoor amenity area needs to be cohesive;

A timber play panel is proposed, which can work well with the landscaping to create more naturalizing and cohesive feeling.

- appreciate the lighting plan for the proposed development;
- consider adding more variety to the proposed planting palette;

Two shrub species have been added into the planting palette.

- review the use of corrugated metal roof for the mail box shelter as it may impact the overall aesthetic of the proposed development; and

Corrugated metal sheet won't be visible from the ground level as the slope is so small. In addition, the pressure treated plywood layer under the corrugated will prevent the corrugated metal from being seen from the ground level.

- ensure there is no conflict between the proposed hedging and fencing for units facing No. 2 Road.

Confirmed, no conflict between the proposed hedging and fencing for units facing No. 2 Road.

(The following comments were submitted by Kelly Riopelle and read into the record by Chris Lee)

- review dormer heights on No. 2 Road and make more similar to internal drive aisle side of Buildings 1 and 4; review width of dormers to reduce their mass; do not support dark colour to camouflage them into dark asphalt shingled roof;

Color and material for all dormers has been revised. Stucco siding with same color has been used for the dormers making the overall massing look much lighter. Battens and trims have been eliminated except for the trims around the windows to give an accentuated appeal to the overall design. Trim colors have also been revised to a lighter color.

- review pickets above entry porches and delete if possible as they are not serving practical value;

The decorative pickets on top of the entry porches have been removed from all building to keep them consistent.

- the applicant is encouraged to use full brick versus face brick only and to return around corners instead of transitioning into Quoin veneer; elevations feel very busy otherwise; the applicant is also encouraged to represent brick courses in scaled elevation accurately; the brick and Quoin IXL veneer presently appear sized at CMU block 8" x 4" or larger;

Use of Face Brick 3 ½" x 7 ½" by Mutual Materials and Quoin IXL were the client's preference.

- consider further development of north elevation of Building 4 and south elevation of Building 5 to address level 1 pedestrian walkway; support the brick treatment specified, but explore having a brick that wraps the No. 2 Road elevation around the corners with a full brick versus a veneer.

Elevations for building 4 & 5 facing the outdoor amenity have been revised by adding a band to give the building a distinct top, middle and base. Also, the dormers facing the amenity area have been revised with the same treatment as the dormer of other buildings.



August 15, 2023

FILE NO.: 102688
REPLY TO: Brian J. P. Corcoran
DIRECT LINE: 604-231-5103
E-MAIL: bcorcoran@pllr.com

BY EMAIL

City of Richmond
6911 No. 3 Road
Richmond, BC V6Y 2C1

Attention: Edwin Lee, Planner

Dear Sirs:

**Re: Jhujar Construction Ltd. (the “Owner”)
Rezoning Application No. RZ 15-716773
Lands situated at 9291 and 9311/9331 No. 2 Road, Richmond (the “Proposed
Development”) – Negotiation with Neighbour located at 9211 No. 2 Road,
Richmond, B.C. for use of SRW CA5001638**

We are the solicitors for the Owner with respect to the Proposed Development. You have requested an update on our extended negotiations with respect to a cost sharing agreement to use the existing drive aisles of 9211 No. 2 Road, Richmond (the “Neighboring Development”).

The following is a summary of our discussions with the Neighbouring Development to date:

- On March 15, 2023, our firm sent correspondence to Pacifica First Property Management Ltd. (“Pacifica”), who are the property management company of the Neighbouring Development. Our letter indicated the City of Richmond’s requirement that the Owner enter into a Cost Sharing Agreement with the Neighbouring Development in exchange for access to the drive aisles of the Neighbouring Development for the purpose of accessing No. 2 Road.
- On March 15, 2023, we received a response from Pacifica that William Wong was the property manager and to contact William directly.
- On March 15, 2023, we followed up directly with William Wong outlining our request.
- On March 16, 2023, William Wong responded that he had forwarded our email and documents to the Strata Council of the Neighbouring Development.

- On March 16, 2023, I responded to William Wong's email asking for a timeline as to when the Strata Council would be able to meet with my client.
- On March 16, 2023, William Wong responded that he could not provide a timeline for a response from the Strata Council at this time.
- On March 30, 2023, William Wong responded and asked for our firm to provide a written proposal and legal advice as a starting point for the negotiation.
- On March 30, 2023, I wrote to William Wong that we cannot provide legal advice to the Strata Council but we will provide a form of agreement.
- On May 19, 2023, I provided a draft form of Shared Lane Access Agreement and Cost Sharing Agreement (the "Agreement") to William Wong which indicated that the Owner would share all costs of maintenance and upkeep of the drive aisles 50/50 with the Neighbouring Development.
- On May 23, 2023, William Wong indicated he forwarded my email and Agreement to the Strata Council.
- On July 25, 2023, I sent a follow up email to William Wong inquiring as to the status of the Strata Council's review of the Agreement.
- On July 25, 2023, William Wong responded and apologized for the delay and indicated that a new Strata Council for the Neighbouring Development was elected in June and it would take some time for the new Strata Council to be caught up on the matter.
- On July 26, 2023, William Wong responded with the Strata Council's requests that we provide documents/maps and plans for the proposed development along with confirmation that the Owner would pay for the Strata Council's legal fees and disbursements with respect to the negotiations of an agreement. There was some erroneous information from the Strata Council included which indicated the Strata Council's believe that the Owner was purchasing additional lots south of the Owner's Proposed Development.
- On August 14, 2023, I responded to William Wong to confirm that the Owner is not purchasing any further lots with respect to this development. I further clarified that we are only making this access request as it is a requirement by the City of Richmond and is contemplated by the fact that the City of Richmond placed a Statutory Right of Way on the Neighbouring Development's drive aisles for just this reason. I provided a copy of a schematic of the Proposed Development together with a ParcelMap Report showing the Owner's 2 lots immediately south of the Neighbouring Strata. In addition, I confirmed that the Owner would cover reasonable legal fees of the Neighbouring Strata with respect

to the review of the Agreement. Finally, I indicated that the continued delays are severely hampering the Owner's ability to move forward with the Proposed Development.

- On August 14, 2023, William Wong responded and indicated that he had forwarded my email and attachments to the Strata Council.
- On August 14, 2023, I emailed William Wong to again stress the urgency of the matter.
- On August 14, 2023, William Wong replied indicating that he understood time is an issue and that he had asked the Strata Council to appoint a representative to deal with both me and the Strata Council's own legal counsel to speed up the communication process.

I have attached all of the above correspondence and attachments for reference. It is also my understanding that the Owner originally reached out to the Neighbouring Strata directly as early as May, 2019 to try and come to an agreement with respect to the cost sharing and access agreement. It is our opinion that the Owner has made all reasonable efforts to reach a cost sharing agreement with the Neighbouring Strata and as a result of continued delays and requests by the Strata Council, the Proposed Development has been unfairly stalled resulting in potential damages to the Owner. We respectfully request that, based on the forgoing, the Owner's Proposed Development be forwarded to the City's Development Permit Panel in the interim while we continue to work with the Neighbouring Strata to enter into a cost sharing and access agreement.

We trust you will find the above to be in order. Should you have any questions or concern, please do not hesitate to contact the undersigned.

Yours truly,

PRYKE LAMBERT LEATHLEY RUSSELL LLP

Per:



Brian J. P. Corcoran

BC:msd
cc:Client
Encls.



Address: 9291 and 9311/9331 No. 2 Road

File No.: DP 17-790086

Prior to approval of the Development Permit, the developer is required to complete the following:

1. **(Tree Survival Security)** Submission of a Tree Survival Security to the City in the amount of \$35,000 for the 40 cm calliper English Oak tree (Tag# 681) and hedgerow (tag# 687, five stems) to be retained on site. The City will release 50% of the security after construction and landscaping on the proposed development are completed, inspections are approved, and an acceptable post-construction impact assessment report is received. The remaining 50% of the security would be released one (1) year later subject to inspection. A legal agreement is required to accompany the Tree Survival Security to set the terms for its use and release.
2. **(Tree Protection Fencing)** Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
3. **(Landscaping Security)** Receipt of a Letter of Credit for landscaping in the amount of \$310,750.00 (based on the costs estimate provided by a CSLA registered landscape Architect including 10% contingency). A legal agreement is required to accompany the Landscape Security to set the terms for its use and release.

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Enter into a Servicing Agreement* for the design and construction of engineering infrastructure improvements and frontage beautification works. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to:

Water Works

- a. Using the OCP Model, there is 442 L/s of water available at a 20 psi residual at the hydrant at 9260 No.2 Rd. Based on your proposed development, your site requires a minimum fire flow of 220 L/s.
- b. The Developer is required to:
 - Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm the development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage and Building designs.
 - Install a fire hydrant at No.2 Road frontage to service the proposed townhouse development. Coordination with the City's Fire Department to confirm the location of the proposed hydrant is required.
- c. At Developers cost, the City is to:
 - Cut and cap at main, the existing water service connections to both 9291 and 9311 No.2 Rd.
 - Install 1 new water service connection off of the 200mm PVC watermain along No.2 Rd.

Storm Sewer Works

- a. At Developer's cost, the City is to:
 - Check the size and condition of the existing storm service connection on lot 9291's frontage. Upgrade to the service connection pipe may be required if it is inadequately sized or in poor condition.
 - Cut, cap, and abandon the existing storm service connection lead and dispose existing inspection chamber at the northeast corner of Lot 9311.
 - Install a new Type III inspection chamber and remove the existing storm inspection chamber STIC51279.

Initial: _____

Sanitary Sewer Works

- a. At Developers cost, the City is to:
- Remove the existing IC and sanitary lead that services Lot 9291.
 - Install a 150mm service connection complete with an inspection chamber and tie-in to existing manhole SMH3304 located at the northwest corner of 9311 No 2 Road. Connection shall utilize the existing opening at the southeast face of SMH3304.
- b. The Developer is required to:
- Not start building construction until the rear yard sanitary connection is completed by city crews.

Frontage Improvements

- a. The Developer is required to:
- Construct a new 1.5 m wide concrete sidewalk immediately along the east property line of the site. The new sidewalk is to connect to the existing sidewalk north and south of the subject site. The alignment of the sidewalk may be changed for tree protection purposes.
 - Remove the existing sidewalk next to the curb and backfill the area to provide a grass/tree boulevard between the new sidewalk and the existing curb.
 - The existing driveways to provide access to the subject site from No. 2 Road are to be closed permanently. Remove the existing driveway crossings and replace with barrier curb/gutter, boulevard and sidewalk per standards described above. The developer of this site is responsible for the design and construction of curb/gutter, sidewalk and boulevard as part of the driveway closure works in addition to other required frontage improvements.
 - Coordinate with BC Hydro, Telus and other private communication service providers when:
 - i. Undergrounding Hydro service lines.
 - ii. Relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
 - iii. Determine if above ground structures are required and coordinate their locations on-site with the architect and private utility companies (e.g. PMT, LPT, Shaw cabinets, Telus, Kiosks, etc).

General Items

- a. The Developer is required to:
- Provide additional ROW to accommodate the proposed service connections. Details to be determined during the SA process.
 - Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
 - Provide a pre-load plan and geotechnical assessment of impact to existing surrounding utilities and recommendations to mitigate the impact.
2. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
3. Incorporation of accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
4. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated

Initial: _____

fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.
All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.
The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.
- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contains prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed _____

Date _____



No. DP 17-790086

To the Holder: Jhujar Construction Ltd.
Property Address: 9291 and 9311/9331 No. 2 Road
Address: c/o DF Architecture Inc.
10851 Shellbridge Way Unit 350,
Richmond, British Columbia, V6X 2W9

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #34 attached hereto.
4. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$310,750.00 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit
No. DP 17-790086

To the Holder: Jhujar Construction Ltd.
Property Address: 9291 and 9311/9331 No. 2 Road
Address: c/o DF Architecture Inc.
10851 Shellbridge Way Unit 350,
Richmond, British Columbia, V6X 2W9

7. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF

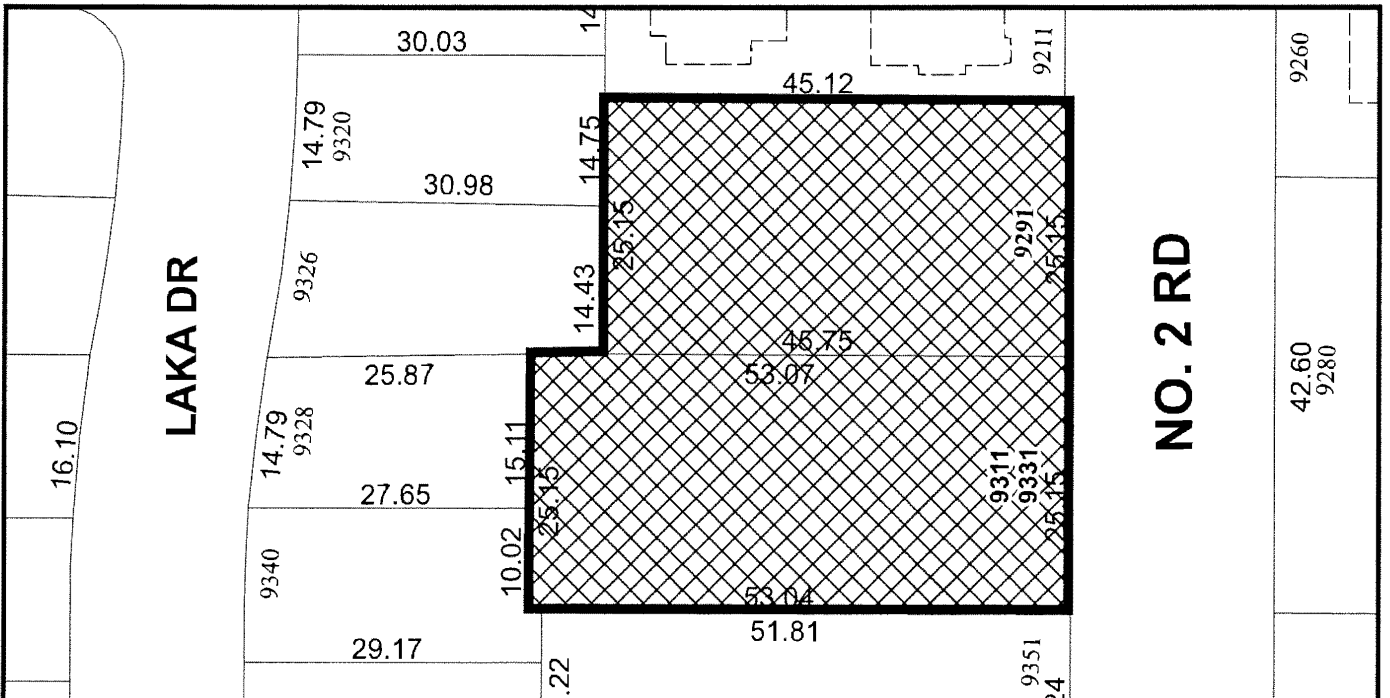
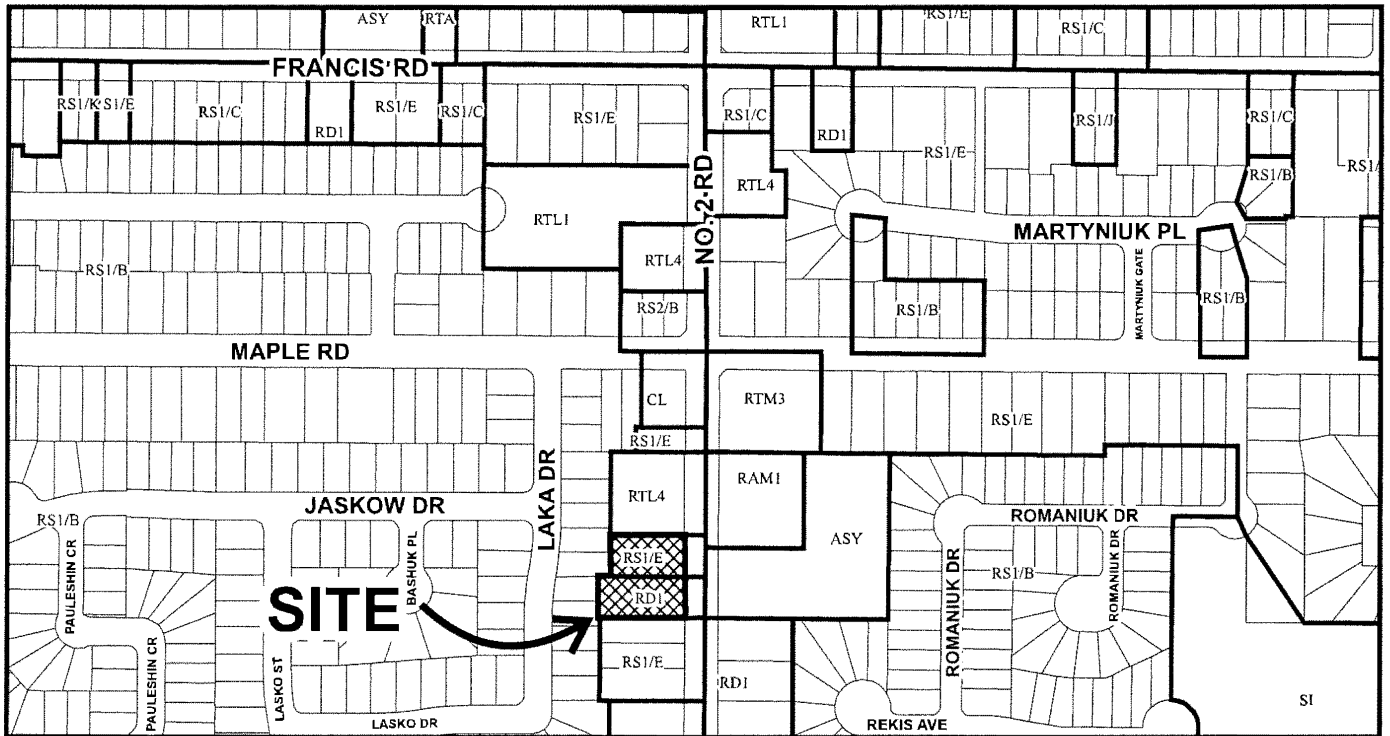
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF

MAYOR



City of Richmond



DP 17-790086 SCHEDULE "A"

Original Date: 10/27/17

Revision Date: 09/18/23

Note: Dimensions are in METRES

TOWNHOUSE DEVELOPMENT

CIVIC ADDRESS: 9291 & 9311 NO. 2 ROAD, RICHMOND B.C.

LEGAL DESCRIPTION:
 1) NORTH HALF LOT 17 EXCEPT: PART SUBDIVIDED BY PLAN 60236;
 2) SOUTH HALF LOT 17 EXCEPT: PART SUBDIVIDED BY PLAN 70853; BLOCK 'B' SECTION 25 BLOCK 4 NORTH RANGE 7 WEST NEW WESTMINSTER DISTRICT PLAN 1353

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A-103	SITE OVERLAYS
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A-301 - A-310	BUILDING ELEVATIONS
A-327	CONVERTIBLE UNIT
A-401 - A-402	SITE SECTIONS
A-501 - A-502	STREETSCAPE

PROJECT DRIECTORY:

ARCHITECTURAL:

DF ARCHITECTURE INC.
 1205 - 4871 SHELL ROAD,
 RICHMOND, B.C. V6X 3Z6
 CONTACT: JESSIE ARORA
 T 604 284 5194
 jessie@dfarchitecture.ca
 info@dfarchitecture.ca

ARBORIST:

ARBOTECH (ACL GROUP)
 6286 203 STREET A125,
 LANGLEY TWP, BC V2Y 3S1
 604 275 3484

ENERGY:

ALPHATEC ENERGY INC.
 201 - 275 2ND STREET W,
 NORTH VANCOUVER, BC V7M1C9
 604 771 5886

LANDSCAPE:

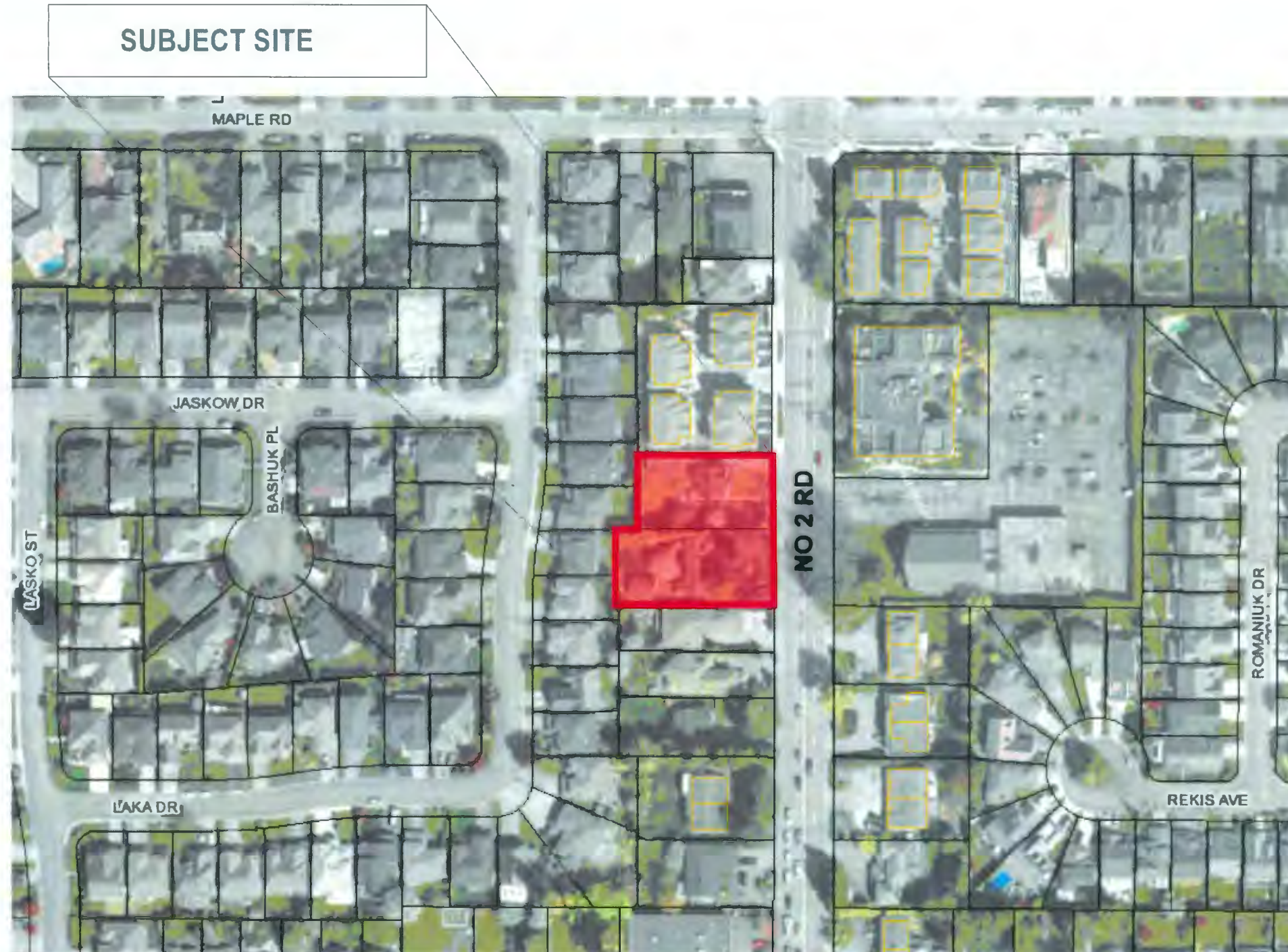
HAPA
 Landscape Architecture
 Urban Design
 403 - 375 West Fifth Avenue
 Vancouver BC, V5Y 1J6
 604 909 4150
 hapacobo.com

CIVIL:

Core Concept Consulting Ltd.
 Landscape Architecture
 Urban Design
 220 - 2639 vinking Way,
 Richmond BC, V6V 3B7
 604 249 5041
 coreconceptconsulting.com

ACOUSTIC:

BROWN STRACHAN ASSOCIATES
 1020 MAINLAND STREET,
 VANCOUVER, BC V6B2T5
 604 689 0514



NO.	DATE	DESCRIPTION
2023-10-03	G	DP RESUBMISSION FOR OPP
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-08	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-08	B	SUBMISSION FOR ADP 17-790086
2022-06-20	A	DP RESUBMISSION



350-10851 SHELLBRIDGE WAY
 RICHMOND BRITISH COLUMBIA
 CANADA V6X 2W9
 T: (604) 284-5194 F: (604) 284-5131
 info@dfhl.com

PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DRAWN: WA	
CHECKED: YA	
SCALE: N.T.S.	
JOB No: R04-033	
DATE: 2023	
SHEET TITLE:	

COVER SHEET

DRAWING NO.:	
A-001	G

DP 17-790086

PLAN #1

October 4, 2023

PROJECT DATA			
CIVIC ADDRESS :	9291 & 9311 NO.2 ROAD, RICHMOND, BC		
LEGAL DESCRIPTION :	NORTH HALF LOT 17 EXCEPT : PART SUBDIVIDED BY PLAN 60236 SOUTH HALF LOT 17 EXCEPT : PART SUBDIVIDED BY PLAN 70853; BLOCK 'B' SECTION 25 BLOCK 4 NORTH RANGE 7 WEST NEW WESTMINISTER DISTRICT PLAN 1353		
LOT INFO			
GROSS SITE	26,715.0 SQFT	2,482 SQM	
DEDICATIONS	319 SQFT	29.6 SQM	
NET AREA	26,396.0 SQFT	2,452 SQM	
ZONING			
EXISTING	RTL 3		
PROPOSED	RTL 3		
SETBACKS			
	REQUIRED	PROPOSED	
NORTH (ELEC. RM - BLDG 5)	3.00 M	3.00 M	
NORTH (TO BLDG. 1)	3.00 M	4.40 M	
NORTH (TO GARBAGE ENCLOSURE)	3.00 M	3.30 M	
SOUTH	3.00 M	3.00 M	
EAST (NO. 2 ROAD)	6.00 M	6.20 M	
WEST	3.00 M	5.10 M	
HEIGHT (REFER TO ELEVATIONS FOR BUILDING HT. DIMENSIONS)			
ALLOWED	3 STOREY(12M)		
PROPOSED	2 STOREY/3 STOREY		
SITE COVERAGE			
ALLOWED	10558.4 SQFT	40.0%	
PROPOSED(ON NET AREA)	10,570.0 SQFT	40.0%	
FLOOR AREA RATIO(FAR)			
ALLOWED	15837.6 SQFT	0.60	
PROPOSED (ON NET AREA)	15,638.6 SQFT	0.59	

AMENITY CALCULATION			
OUTDOOR AMENITY	NO. OF UNITS	AREA PER UNIT	AREA
REQUIRED	12	6 SQM.	72 SQM.
PROVIDED			90.8 SQM

FAR CALCULATION										
BUILDING	LVL 1	LVL 2	LVL 3	GROSS AREA	EXCLUSIONS				NET AREA	NET AREA(SQM)
					GARAGE DEDUCTION	GARBAGE/ELEC./BALCONIES	STAIRS+ENTRY DEDUCTION	STAIRS DEDUCTION UPPER FLOOR		
BUILDING 1	2,055.08 SFT	1,622.88 SFT	0.00 SFT	3,677.96 SFT	714.66 SFT	75.00 SFT	113.36 SFT	110.70 SFT	2,664.24 SFT	247.5 SQM
BUILDING 2	974.08 SFT	754.33 SFT	0.00 SFT	1,728.41 SFT	443.50 SFT		61.36 SFT	22.36 SFT	1,201.19 SFT	111.6 SQM
BUILDING 3	2,636.25 SFT	1,435.00 SFT	0.00 SFT	4,071.25 SFT	776.32 SFT		86.70 SFT	75.68 SFT	3,132.55 SFT	291.0 SQM
BUILDING 4	2,137.50 SFT	2,128.15 SFT	1,344.09 SFT	5,609.74 SFT	1,236.99 SFT	127.00 SFT	229.02 SFT	112.17 SFT	3,904.56 SFT	362.7 SQM
BUILDING 5	2,760.33 SFT	2,736.45 SFT	1,855.33 SFT	7,352.11 SFT	1,930.70 SFT	166.00 SFT	355.08 SFT	164.27 SFT	4,736.06 SFT	440.0 SQM
TOTAL				22,439.47 SFT					15,638.60 Sq.Ft.	1452.9 SQM
									FAR PROPOSED	0.59

OFF STREET PARKING				
REQUIRED PARKING	NO. OF UNITS	REQUIRED CARS PER D.U	NO. OF CARS	REQUIRED TOTAL
RESIDENTIAL REQUIRED	12	2	24	24
VISITORS CAR PARK REQUIRED	12	0.2	2.4	3
TOTAL NO. OF PARKING REQUIRED				27
REQUIRED ACCESSIBLE PARKING				
	2% OF TOTAL REQUIRED		0.52	1
PROPOSED PARKING				
PROPOSED TANDEM RESIDENT PARKING				10
PROPOSED DOUBLE GARAGE RESIDENT PARKING				14
PROPOSED VISITORS PARKING		VAN ACCESSIBLE		1
		STANDARD VISITOR		2
TOTAL PROPOSED PARKING				27
NOTE: All resident parking spaces to be equipped with level 2 ev charging outlets as per bylaw requirements.				
NOTE: Maximum 50% of residential parking can be provided in tandem arrangement. Currently 10 of 24 spaces(42%) are proposed as tandem spaces.				
BICYCLE PARKING				
REQUIRED BIKE PARKING	NO. OF UNITS	REQUIRED BIKES PER	NO. OF BIKES	SAY
CLASS 1 BIKE PARKING	12	1.25	15	15
CLASS 2 BIKE PARKING	12	0.2	2.4	2
PROPOSED BIKE PARKING				
PROPOSED BIKE PARKING		CLASS 1		16
		CLASS 2		5

2023-10-03	C	DP RESUBMISSION FOR DPP
2023-09-12	F	DP RESUBMISSION
2023-07-28	I	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_DP17-790086
2022-06-20	A	DP RESUBMISSION



350-10851 SHELLBRIDGE
RICHMOND BRITISH COLUMBIA
CANADA V6X 2W9
T 604(284)5194 F(604)284-5131
inf@df.ca

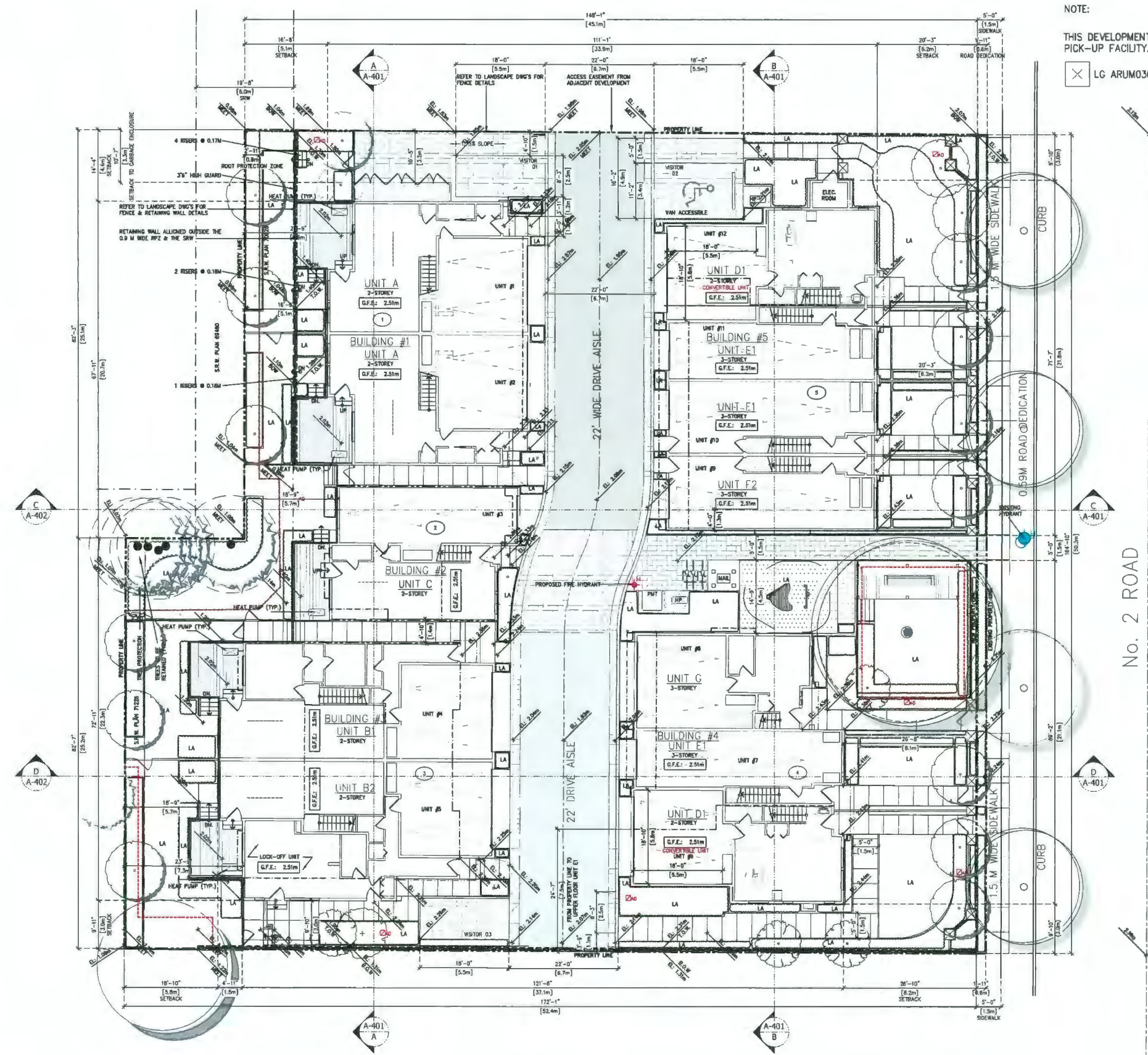
PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DATE:	WA
CHECKED:	YA
SCALE:	AS SHOWN
JOB NO.:	RCM-033
DATE:	SEPT 2023
SHEET TITLE:	

DATA SHEET



NOTE:
 THIS DEVELOPMENT IS NOT DESIGNED TO ALLOW FOR DOOR TO DOOR GARBAGE AND RECYCLING PICK-UP FACILITY.
 X LG ARUM036GSS5 HEAT PUMP.

NOTES:



LEGEND:
 LA - LANDSCAPE
 PMT - PAD MOUNTED THERMOSTAT
 H - FIRE HYDRANT

2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP-DP17-790086
2022-06-20	A	DP RESUBMISSION



350-10851 SHELLBRIDGE
 RICHMOND BRITISH COLUMBIA
 CANADA V6C 2W9
 T: 604 284-5194 F: 604-284-5131
 n | d | h | w | e | l | c | o | m | e

PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DRAWN BY:	WA
CHECKED BY:	YA
SCALE:	3/32" = 1'-0"
JOB NO.:	RCM-033
DATE:	SEPT 2023
SHEET TITLE:	

SITE PLAN

1 SITE PLAN
 A-100 SCALE: 3/32" = 1'-0"

TOTAL CONVERTIBLE UNITS PROPOSED : 2 (UNIT D1- BUILDINGS 4 & 5)



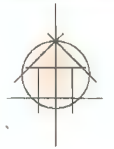
DP 17-790086

PLAN #3

October 4, 2023

DRAWING NO.:	A-100	G
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NOTES:



LEGEND

RED LINE INDICATES PATHS FROM HYDRANTS TO MOST REMOTE SUITE
 MAXIMUM 90m PER BCBC (2018) 3.2.5.5.(2)(b)

NOTE: MAX. DISTANCE FROM FIRE TRUCK TO UNIT ENTRY IS 45m
 PER BCBC (2018) 3.2.5.5.(2)(c)

MAX. DISTANCE FROM FIRE HYDRANT TO FIRE TRUCK PLUS THE
 UNOBSTRUCTED PATH OF TRAVEL FROM FIRE TRUCK TO THE
 UNIT ENTRY IS NOT MORE THAN 90m PER BCBC (2018) 3.2.5.5.(2)(b)

EXTENT OF FIRE TRUCK ACCESS

2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP DP17-790086
2022-08-20	A	DP RESUBMISSION



350-10851 SHELLBRIDGE
 RICHMOND, BRITISH COLUMBIA
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 T 504.284.5194 F 604.284.5131
 info@df.ca

PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

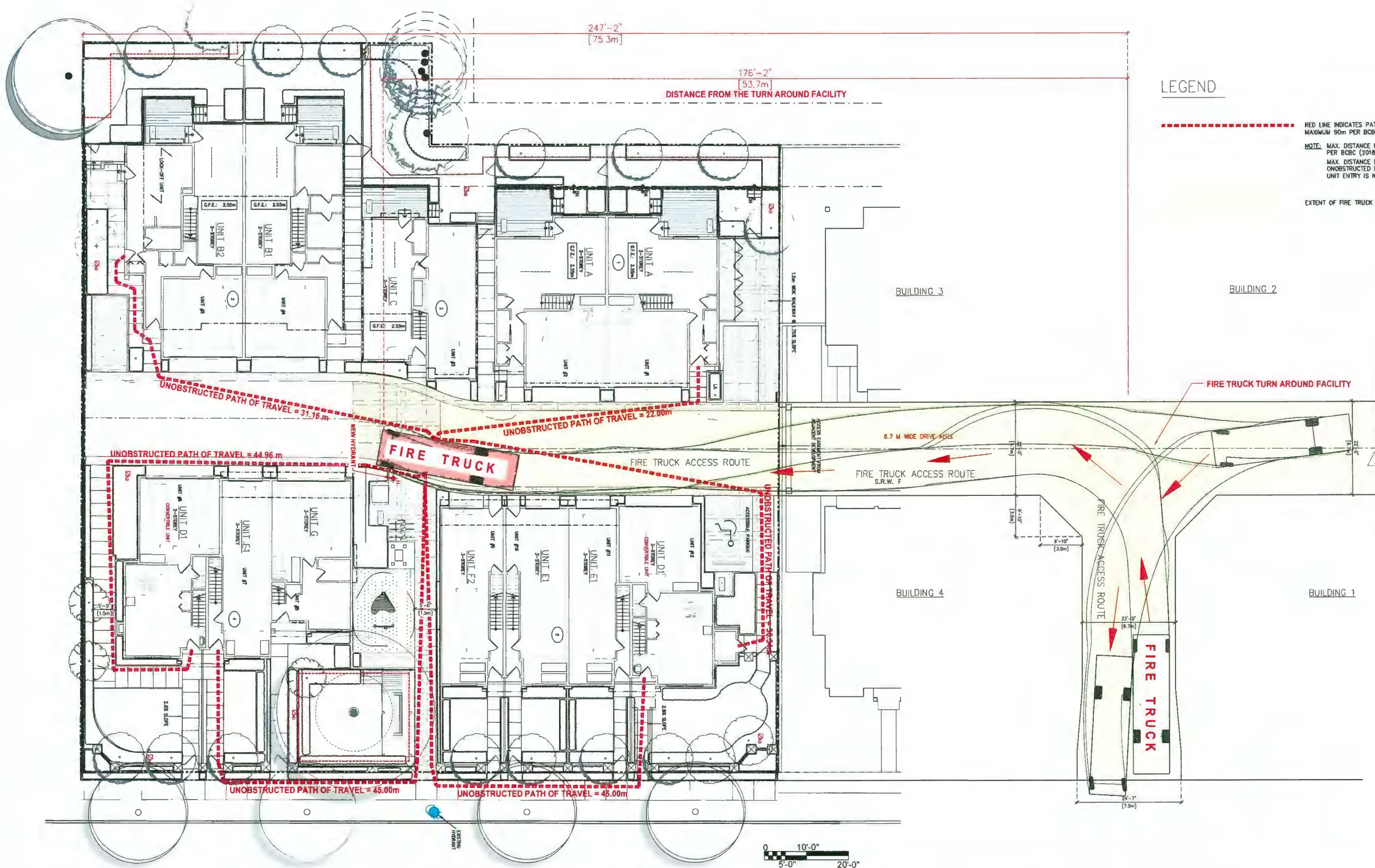
CLIENT:
SATNAM JOHAL

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 ARCHITECT'S WRITTEN CONSENT.

DRAWN	WA
CHECKED	YA
SCALE	3/32" = 1'-0"
JOB No.	RCM-033
DATE	SEPT 2023
SHEET TITLE	

FIRE ACCESS PLAN

DRAWING NO.:	A-101	G
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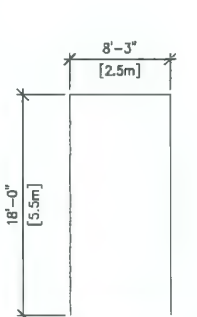
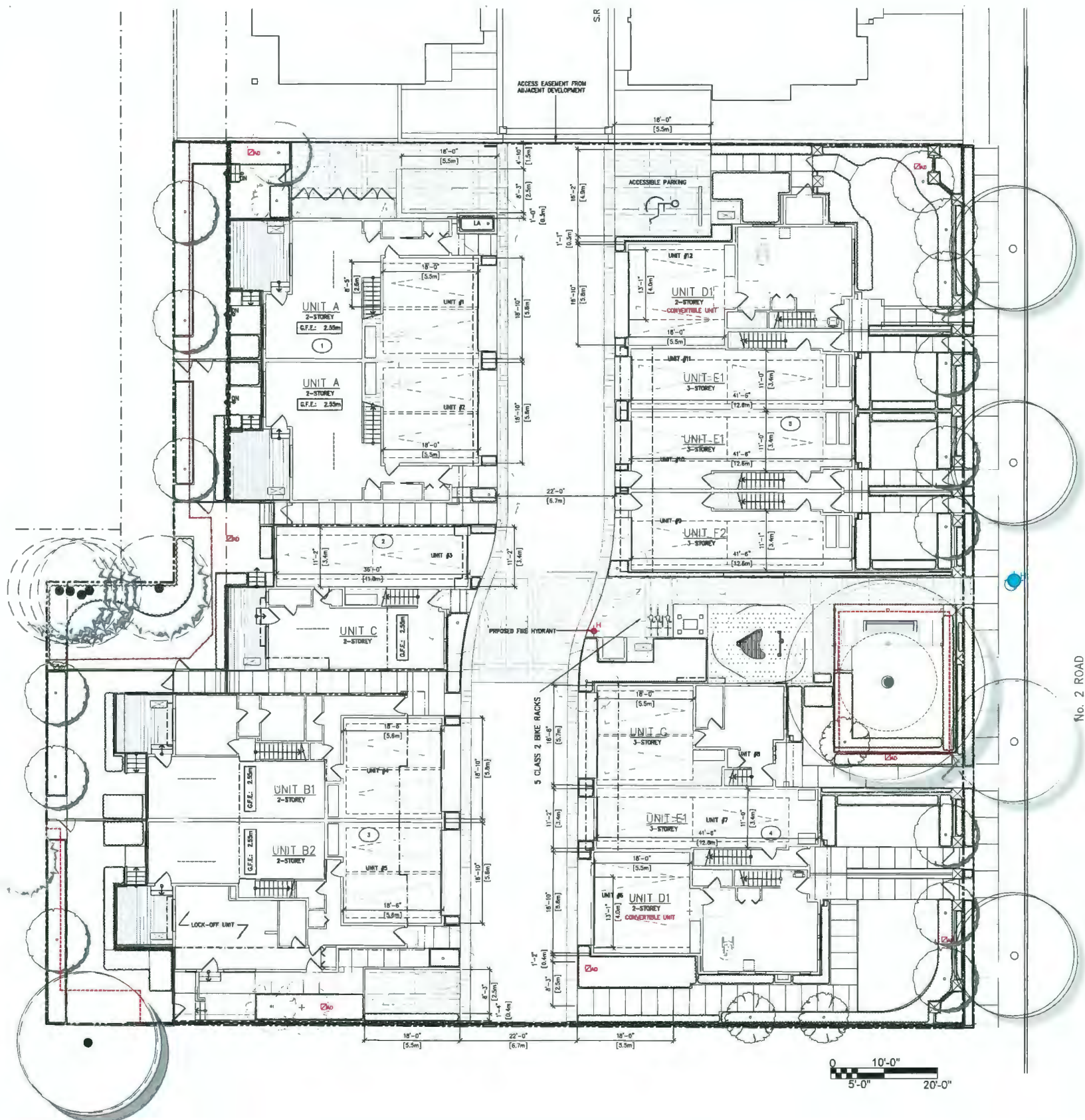


1 FIRE ACCESS PLAN
 A-101 SCALE: 3/32" = 1'-0"

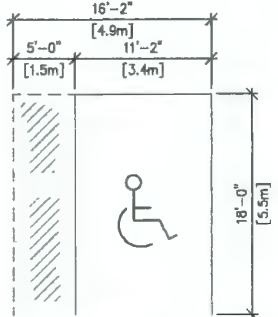
DP 17-790086

PLAN #4

October 4, 2023

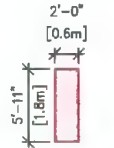


STANDARD PARKING SPACE
LENGTH - 5.5M
WIDTH - 2.5M



ACCESSIBLE SPACE
LENGTH - 5.5M
WIDTH - 3.4M
SHARED AISLE - 1.5M

BICYCLE PARKING



CLASS-1 & CLASS-2
BICYCLE PARKING SPACE
LENGTH - 1.8M (HORIZONTAL)
WIDTH - .6M

NOTE: DEVELOPMENTS WITH LESS THAN 31 PARKING SPACES REQUIRE 100% OF PARKING TO BE STANDARD SIZED SPACES. NO SMALL CAR SPACES ARE PROPOSED.

MAXIMUM 50% OF RESIDENTIAL PARKING CAN BE PROVIDED IN TANDEM ARRANGEMENT CURRENTLY 10 OF 24 SPACES (42%) ARE PROPOSED AS TANDEM SPACES

ALL RESIDENTIAL PARKING SPACES (EXCLUDING VISITOR PARKING) IS TO BE PROVIDED WITH LEVEL 2 EV CHARGING OUTLETS (208-240V AC & CURRENT OF 16A-80A)

THIS DEVELOPMENT IS NOT DESIGNED TO ALLOW FOR DOOR TO DOOR GARBAGE AND RECYCLING PICK-UP FACILITY.

NOTES:



REVISION	DATE	DESCRIPTION
2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	F	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-08	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_DP17-790086
2022-06-20	A	DP RESUBMISSION



350-10861 SHELLBRIDGE
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info@dfa-architecture.ca

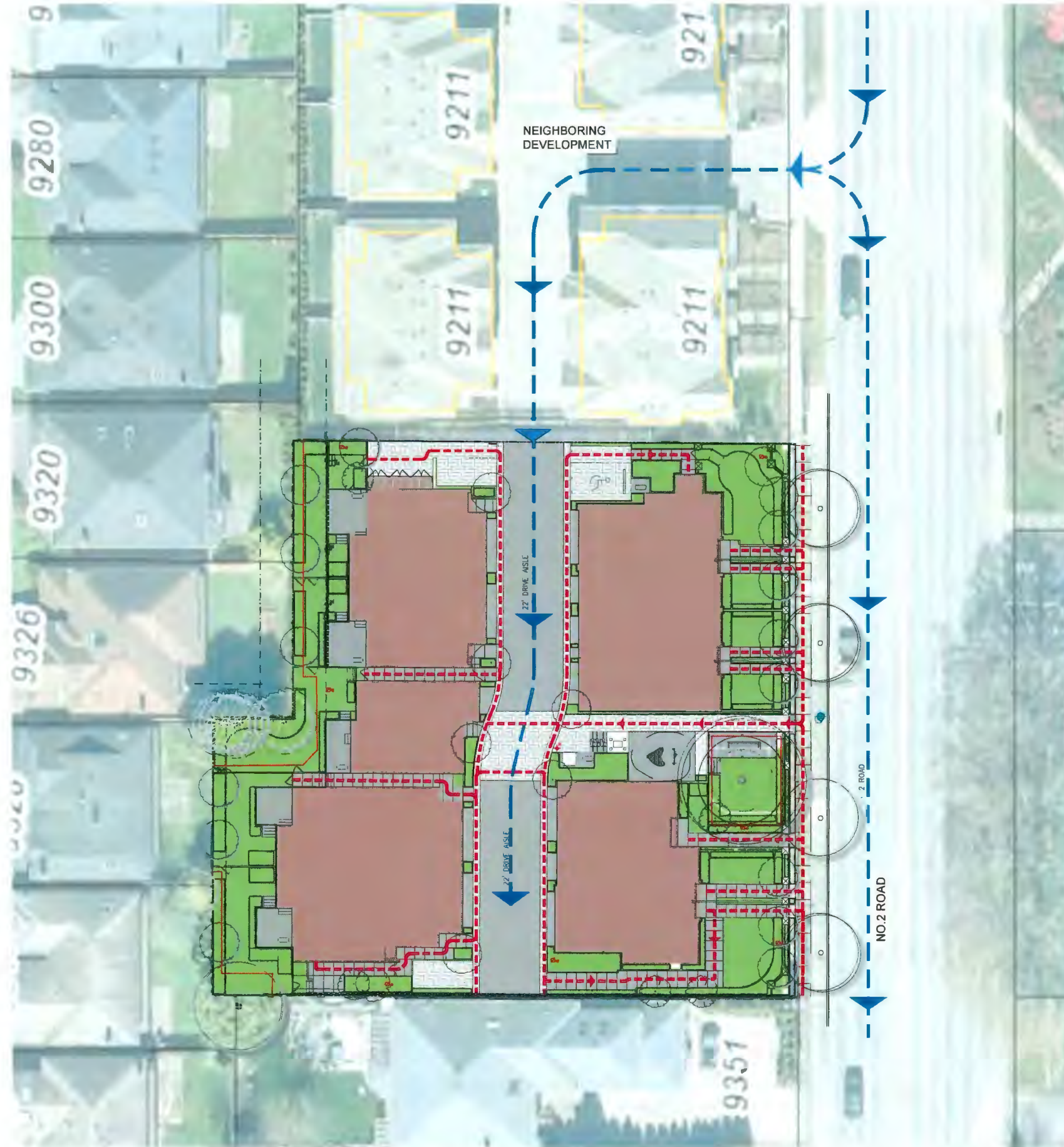
PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DESIGNER:	WA
CHECKED:	YA
SCALE:	3/32" = 1'-0"
JOB No.	ROH-033
DATE:	SEPT 2023
SHEET TITLE:	

1 PARKING PLAN
A-102 SCALE: 3/32" = 1'-0"



 VEHICULAR ACCESS
 PEDESTRIAN ACCESS

NOTES:



2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_DP17-790086
2022-06-20	A	DP RESUBMISSION

df

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PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DESIGN: BA
CHECKED: YA
SCALE: 1/16" = 1'-0"
JOB No.: RCH-033
DATE: SEPT 2023
SHEET TITLE:

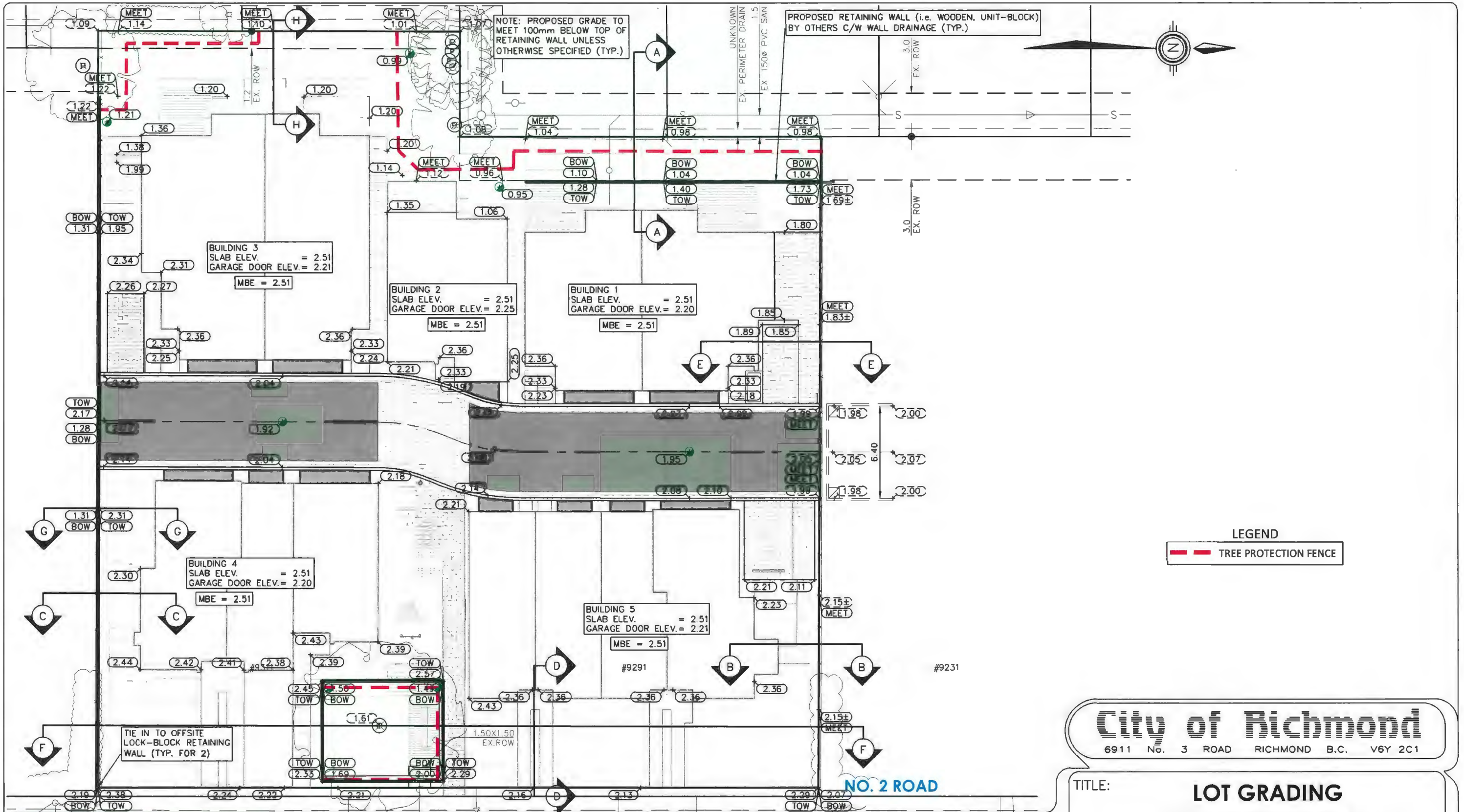
CONTEXT PLAN

DP 17-790086

PLAN #6

October 4, 2023

DRAWING NO.:
A-106
G



CoreConcept CONSULTING LTD.
 tel : 604.249.5040 fax: 604.249.5041
 #220-2639 Viking Way, Richmond, BC, V6V 3B7
 www.coreconceptconsulting.com

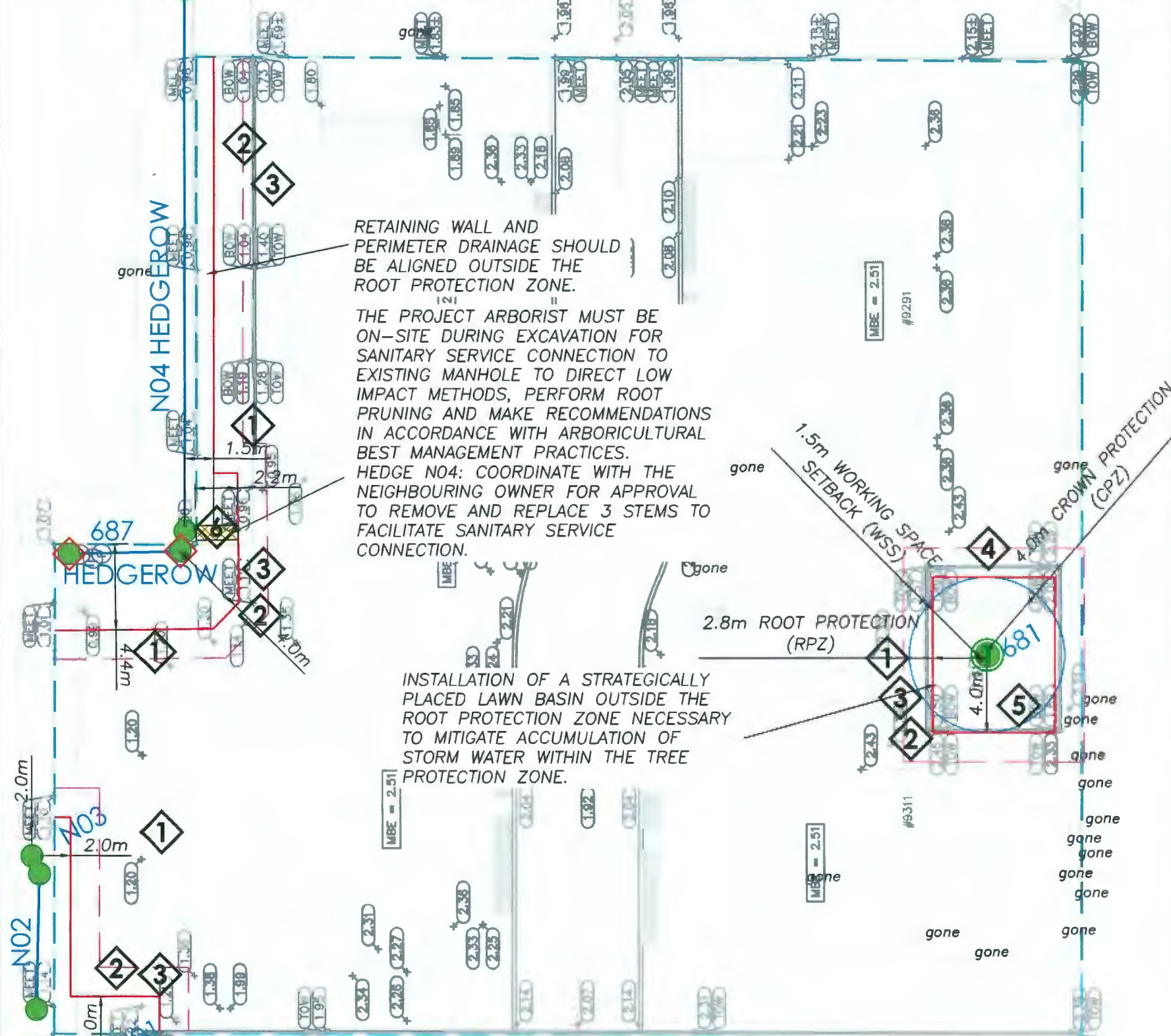
AMAN DHALIWAL
 Core Concept Consulting Project Manager
 DP-17-790086
 DWG. 10F

REV	DATE	BY	CHK	DESCRIPTION
8.	28 SEP 2023	GSG	TS	LOT GRADING ISSUED FOR DP
7.	20 SEP 2023	GSG	TS	LOT GRADING ISSUED FOR DP
6.	20 JUL 2023	GSG	TS	LOT GRADING ISSUED FOR DP
5.	13 MAR 2023	NMM	TS	LOT GRADING ISSUED FOR DP
4.	27 JUN 2022	JB	TS	LOT GRADING ISSUED FOR DP
REV	DATE	BY	CHK	DESCRIPTION
				REVISIONS

PLAN #7

October 4, 2023

TREE RETENTION AND PROTECTION DETAIL - PROJECT DESIGN BASE PRESCRIPTION FOR EXISTING TREES RELATED TO THE PROPOSED DEVELOPMENT



NOTE - TREE PROTECTION ZONES:
THE PROJECT ARBORIST MUST BE ON-SITE DURING ANY WORKS (I.E. GRADING, EXCAVATION FOR THE NEW BUILDING FOUNDATIONS, TREE/STUMP REMOVAL, EXCAVATION FOR INSTALLATION OF UTILITIES AND SERVICE CONNECTIONS, PRUNING, PREPARATION FOR INSTALLATION OF ANY HARDSCAPE FEATURES, LANDSCAPE FINISHING) WITHIN OR DIRECTLY ADJACENT (I.E., WITHIN 2.0 m) TO THE TREE PROTECTION ZONE (TPZ) TO UNDERTAKE ROOT PRUNING, DIRECT LOW IMPACT METHODS AND MAKE RECOMMENDATIONS IN ACCORDANCE WITH ARBORICULTURAL BEST MANAGEMENT PRACTICES. PERIMETER DRAINAGE AND RETAINING WALLS MUST NOT BE INSTALLED WITHIN THE

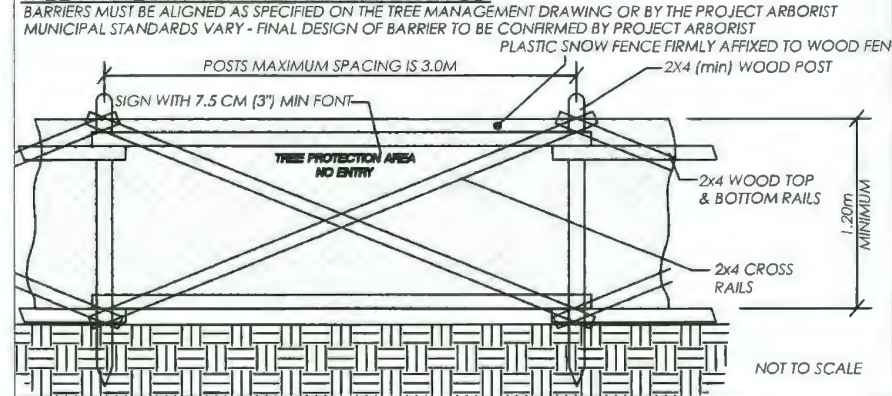
LEGEND-TREE RETENTION:

- TAGGED TREES:
(see arborist report and details herein for further details)
- XXX denotes ARBORIST TAG NUMBER or ID REFERENCE (see tree inventory and assessment list)
 - XXX denotes SURVEY TAG NUMBER or ID REFERENCE (if applicable)
 - approx denotes APPROXIMATE LOCATION for an un-surveyed tree (if contemplated to be retained we recommend that the tree be accurately located before design is finalized)
 - denotes RETENTION tree (protection measures as specified)
 - ✕ denotes REMOVAL tree (municipal permit or approvals required as applicable)
 - ✕ denotes HIGH RISK tree to be REMOVED or MODIFIED (municipal permit or approvals required as applicable)
 - ⊗ denotes UNDERSIZE TREE that is proposed to be REMOVED (if to be retained consult this office for protection measures) (if to be removed a permit may be required from municipality)

LEGEND-TREE PROTECTION:

- Tree Protection Zones (TPZ) are comprised of a CPZ, RPZ and WSS as detailed below and with limitations and restrictions as described herein and in the arborist report.
 - Tree locations noted as APPROXIMATE should be surveyed accurately before finalizing design and installing tree protection measures.
 - All tree protection setbacks are measured from the centre of tree trunk and/or from the reference property line or site features, as applicable.
 - The project arborist should be consulted as to barrier installation to provide onsite direction as well as to inspect and provide sign off once they are constructed.
 - Barriers aligned with dimensions of farm/utility lines at ESA setbacks require surveying to mark those reference lines prior to the barrier installation and inspection.
 - Temporary interim barriers for demolition or site preparation phase may require adjustment to the final barrier alignment after the pre-construct or work is complete.
- denotes CROWN PROTECTION ZONE - CPZ (excision zone - no aerial encroachment of buildings, vehicles, cranes, etc)
 - denotes ROOT PROTECTION ZONE - RPZ (minimum alignment for TREE PROTECTION BARRIERS - no soil disturbance within)
 - denotes WORKING SPACE SETBACK - WSS (zone outside RPZ where construction work requires advance approval and/or on-site supervision by the Project Arborist)
 - denotes SPECIAL MEASURES required (the Project Arborist is required to direct or implement measures as noted herein as detailed in letter of undertaking - appendix E of arborist report)
 - denotes INTERIM TREE BARRIER

TREE PROTECTION BARRIER DETAIL - SAMPLE:



PROJECT:	PROPOSED TOWNHOUSE DEVELOPMENT		
ADDRESS:	9311 & 9291 NO. 2 ROAD, RICHMOND		
CLIENT:	JHUJAR CONSTRUCTION		
CITY REF:		ACL FILE:	15241
PLOT SIZE:	11"X17"	REV #:	8
		DATE:	SEP 29, 2023

Condensed Tree Inventory and Assessment Data:

Tag/ID	# of Trees	Survey	Loc	Common name, (Botanical)	Dia TPZ	Ht	Spr	LCR	Class	Priority	Action	CPZ	RPZ
681	1	Y	ON	English oak (Quercus robur)	46	15	6	80	O	1	RETAIN	6	2.8 x 4.0
687	5	Y	ON	Western redcedar (Thuja plicata)	29	10	4.0	90	O	LOW	RETAIN	4.0	
N01	1	Y	OFF	Southern magnolia evergreen (Magnolia grandiflora)	30	9	4.0	70	O	2	PROTECT	4.0	3.0
N02	1	Y	OFF	Eastern white cedar (Thuja occidentalis)	15	6	1.0	100	O	LOW	PROTECT	1.0	1.0
N03	1	Y	OFF	English laurel (Prunus laurocerasis)	20	7	3.0	80	O	LOW	PROTECT	3.0	2.0
N04	1	Y	OFF	Eastern white cedar (Thuja occidentalis)	15	5	1.0	100	O	LOW	PROTECT	1.0	1.5

TREE PROTECTION STANDARD MEASURES:

As a general protocol relative to this LOU the Client is required to; maintain compliance with the Tree Protection Specifications (see Appendix D in the reference document), to keep the PA updated with city approvals and scheduling, as well as to consult with the PA to review or attend the site at the following project milestones and/or prior to and during certain work activities on the site as follows:

- A. Barrier Installation:**
To direct and inspect the tree protection barrier installation and provide barrier sign-off report.
- B. Pre-Treatment of Trees:**
To undertake or direct tree treatments (i.e. pruning, enhancements to the growing site and special measures) to prepare the trees and assist them in sustaining and adapting to the rigours of construction.
- C. Pre-Construction:**
To attend a pre-construction meeting, assist in implementing contractor mobilization to the site and to direct/supervise pre-construction works in and around TPZ's, such as but not limited to:
 - site access and egress,
 - service and utility decommissioning,
 - civil enabling works,
 - demolition of building or hardscapes,
 - ESC installation,
 - invasive plant treatments or management,
 - site clearing and tree removal (low impact removal of vegetation and stumps within TPZ),
 - preloading or site grading, and
 - installation of site hoarding, site office and temporary power.
- D. Construction Phase:**
To attend and assist with specific activities within or in close proximity of a TPZ to direct or undertake low impact methods and materials, to perform root pruning and to direct tree and root protection, concurrent with the following:
 - site grading (cuts or fills),
 - excavations,
 - trenching or overhead work for services and utility installation,
 - forming and concrete placing,
 - scaffolding, craning or manlift operation,
 - framing and building envelope finishing,
 - all landscape works (see below), and
 - at certain times as identified in the Special Measures section (see below).
- E. Regular Inspections and On-Call:**
To attend on a regular monthly schedule or at an interval determined by the PA relative to the construction progress and timing of certain works and to be available from time to time to review design details that the Client deems may present conflicts with a TPZ and/or to assist in resolving project constructability challenges.
- F. Access within TPZ:**
To review and direct worker or equipment access into a tree protection zone (TPZ) whenever contemplated or desired for any reason, and only when absolutely necessary. The PA has a toolbox of various solutions for cost effective methods to enable certain types and scopes of access within a TPZ while minimizing the impacts to the tree.
- G. PRIOR TO REMOVING BARRIERS - Landscape Finishing:**
To review landscape drawings and subcontractor work plan including TPZ related limitations on methods and materials in advance of commencing those works and to provide on-site direction and guidance for the associated preparation works and construction of PA approved hard and soft landscape elements as follows:
 - sidewalks/paths,
 - patios/decks/benches,
 - retaining walls,
 - fencing,
 - irrigation/electrical conduit
 - soil placement/planting and grass or turf installation.

Note that certain landscape features may be excluded, may require an arboricultural aeration system, or will be limited to specific materials and methods that meet tree and root protection requirements. Planting of any plants, shrubs or hedges within a TPZ is restricted to small pot sizes such as "4 inch" and "#1", but these may vary depending on tree species and proximity to the tree. The "pocket planting" standards will apply via minimizing planting hole size, digging into existing grade where that work will avoid damage to woody roots, and backfilled with minimal addition of growing medium.
- H. In-Progress Documentation:**
To provide site review reports at certain milestones and issued to; the owner/developer, the general contractor and the prime consultant, as well as any other parties the Client wishes to include, throughout the construction phase.
Note that if non-compliance is observed, the PA is required by the city to report the non-compliance to them in the form of an impact and mitigation assessment which may require investigative work by the PA.
- I. Post Construction Sign-Off:**
To attend at the completion of the construction and landscape works to inspect and prepare a city required sign-off report confirming that tree protection was compliant for the duration of the project and that all tree protection measures were implemented as specified. This sign-off will assist the Client in applying for the release of city held tree protection bonds or securities (if applicable).
Note that if the project failed to notify the PA of certain work that required our supervision or direction, then the work was not compliant and our report will be prepared accordingly. In some cases of non-compliance, the city may require the removal of hard or soft landscape features in order to determine impacts to trees, soil or roots. If the impacts imperil the health or structural condition of a tree the city may withhold or defer reimbursement of bonds or securities, or take other action.

TREE PROTECTION SPECIAL MEASURES:

The following items within a TPZ require PA direction, treatment or supervision/monitoring. See the Tree Management Drawing for additional references to locations where special measures are required.

1. **TREE # 681, 687, N01, N02, N03, and N04 - Building Site Preparation:**
The PA must be on site concurrently with the excavation (site stripping) activities adjacent to the tree protection zones to identify tree roots, provide root protection measures and/or undertake root pruning treatments as necessary. Also when pre-load or fill placement is planned, the PA must be contacted to review any measures, including temporary retaining walls and/or drainage.
2. **TREE # 681, 687, N01, N02, N03, and N04 - Perimeter Drainage, Site Filling and Retaining Wall Construction:** The alignment for the perimeter drainage and associated retaining wall must be outside of the RPZ (as noted above). The installation must be reviewed in advance by the PA and in certain cases special measures and supervision will be required. Certain encroachments into the TPZ may be enabled at the discretion of the PA via the use of an aeration and drainage system intended to keep underlying roots viable. Details of those measures will be specified as field instructions issued by the PA as the work proceeds under their direction.
3. **TREE # 681, 687, N01, N02, N03, and N04 - Root Pruning for Site Excavation:**
The PA must be on site concurrently with the excavation adjacent to the tree protection zone to identify tree roots, provide root protection measures and/or undertake root pruning treatments as necessary.
4. **TREE # 681 - Root Pruning for Services Adjacent to TPZ:**
The PA must approve the method of excavation (i.e. excavator, hydro-vac, air-vac, air spade etc) and also must be on site concurrent with trenching to identify tree roots, provide root protection measures and/or undertake root pruning treatments as necessary.
5. **TREE # 681 - Landscape Finishing:** The project arborist must be on-site prior to commencing with and during preparation for installation of any hardscape features or grading within or directly adjacent to the tree protection zone to direct low impact methods. Certain materials may be restricted to mitigate impacts as directed by the project arborist. Strategically placed lawn basins must be placed outside of the root protection zone to mitigate accumulation of storm water in the root zone.

TREE PROTECTION SPECIAL MEASURES:

The following items within a TPZ require PA direction, treatment or supervision/monitoring. See the Tree Management Drawing for additional references to locations where special measures are required.

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4. **TREE # 681 - Root Pruning for Services Adjacent to TPZ:**
The PA must approve the method of excavation (i.e. excavator, hydro-vac, air-vac, air spade etc) and also must be on site concurrent with trenching to identify tree roots, provide root protection measures and/or undertake root pruning treatments as necessary.
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6. **Tree N04 - Sanitary Sewer Service Connection:** The project arborist must be on site prior to commencing with and during excavation for sanitary service connection into the existing manhole in the adjacent private land to the west. Seek approval from the neighbouring owner to remove and replace estimated quantity of 3 stems to facilitate excavation.

DRAWING USE AND COORDINATION:

- This drawing relies on information and drawings supplied by the client or their consultants. Refer to original drawings from the consultants (i.e. surveyor, engineer, architect or other design professionals) for accurate locations and dimension of site features.
- All tree protection measures specified herein should be included and coordinated with the designs for the project, including but not limited to; architectural, landscape, civil and geo-technical. It is the responsibility of each design professional to understand and review the tree protection measures and determine any conflicts. If conflicts are identified, the design professional and/or the client should bring those to the attention of the project arborist from this office to review and resolve.
- Tendering and contracts for site preparation, land clearing, civil works and/or construction should include specifications for tree protection measures to be implemented as per this drawing and any reference documents.
- It is the responsibility of the owner or their agent to obtain all necessary approvals for the tree retention and removal scheme presented herein. Any changes that the municipality requests should be brought to the attention of the project arborist from this office to review and resolve.
- Some existing trees may not be shown on this drawing (i.e. undersize or bylaw exempt trees, or grouped trees). It is the responsibility of the contractor(s) to confirm that all necessary municipal approvals are in place, and to determine the full scope of tree removal work. Only the trees shown to be retained and protected are to remain on site, unless otherwise directed by the owner.
- Trees and stumps to be removed from within the tree protection zone (including CPZ, RPZ and WSS) are to be removed as directed and with on-site supervision from an arborist from this office.
- Stump grinding may be required for the removal of trees within the tree protection zone, at the discretion of an arborist from this office.
- Certain tree removals in proximity of retained trees or power lines may require assistance from a suitably qualified professional, such as but not limited to;
 - ISA Certified Arborist (tree removal, rigging, pruning and other tree service work) working to ANSI A300 and ANSI Z133 Standards and Best Management Practices.
 - Certified Utility Arborist (tree removal, power line clearance, etc.) working to ANSI A300 and ANSI Z133 Standards and Best Management Practices and following OHS and safety protocols.

APPENDIX C: TREE MANAGEMENT DRAWING SHEET 2 OF 2

arborgroup.ca
ARBORTECH CONSULTING
ACL GROUP
A125 - 6284 203 Street, Edmonton, Alberta T6E 6E3 | 780-427-5138 | info@arborgroup.ca

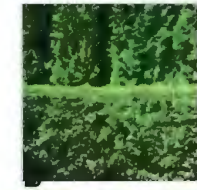
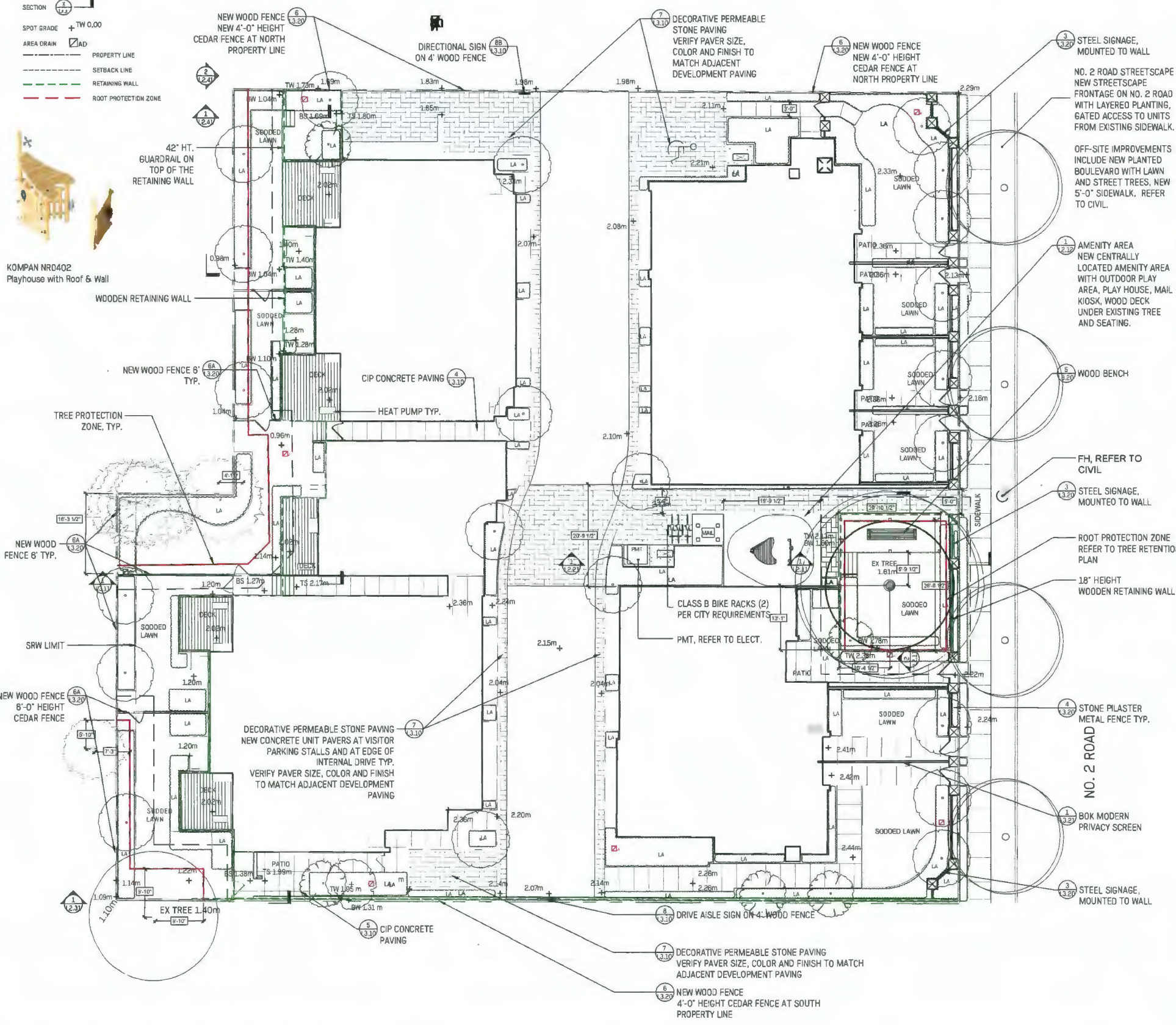
PROJECT:	PROPOSED TOWNHOUSE DEVELOPMENT		
ADDRESS:	9311 & 9291 NO. 2 ROAD, RICHMOND		
CLIENT:	JHUJAR CONSTRUCTION		
CITY REF:		ACL FILE:	15241
PLOT SIZE:	11"X17"	REV #:	8
		DATE:	SEP 29, 2023

DP 17-790086 PLAN #9

October 4, 2023

GENERAL LEGEND

- DETAIL NUMBER
- SHEET ON WHICH DETAIL IS SHOWN
- SECTION
- SPOT GRADE + TW 0.00
- AREA DRAIN
- PROPERTY LINE
- SETBACK LINE
- RETAINING WALL
- ROOT PROTECTION ZONE



Landscape Rationale

Privacy of Adjacent Homes:
 The landscape plan for the property provides a hierarchy of fencing, planting and retaining walls to distinguish public and private areas of the site, and preserve the privacy of adjacent residential homes. Fencing is proposed up to 8'0" in height for all side yard and rear yard conditions to maintain privacy. Additionally cedar hedging and additional landscape planting is proposed on the rear yard west property line to satisfy privacy concerns.

Streetscape Treatment
 The streetscape treatment along No. 2 Road includes a lawn boulevard at the back of curb consistent with adjacent new development to the north and south, along with street tree planting, per the City of Richmond recommendation. Brick pilasters, aluminum fencing and gates are proposed at the property line edge to provide definition of public and private spaces, and low fencing is proposed between limited common property yard areas. Special paving is used at the edges of the interior road to limit the visual impact of asphalt and add a decorative treatment to the lane for pedestrians. Columnar trees are proposed along the drive aisle where possible.

Private Yards
 Each private yard is contained with a combination of low aluminum fencing along No. 2 Road, low wood fencing between units, and higher wood fencing at the property lines. A combination of evergreen and deciduous plant materials are proposed for perimeter planting, foundation planting and accent planting for each unit. Refer to the planting plan. Each unit includes at least one deciduous tree for each limited common property zone.

Outdoor Amenity Area
 The outdoor amenity area includes a combination of play equipment limited to a Play house meeting CSA standards for fall height and fall areas, refer to the plan. This is combined with a broader amenity zone that includes a wood deck that cantilevers over the tree protection zone of the oak tree, and a new tire swing in the oak tree that expands play potential into the lower landscape area, while being completely contained from the street. Other amenities include bench seating, decking, special paving, bike parking and the mail kiosk.

Direct access from the amenity area is provided via the pathway to the south. Gated separation from the sidewalk is recommended.

- Sustainable Landscaping**
- Design low volume irrigation by installing low volume nozzles and subsurface drip system to reduce water use and increase soil moisture. Install moisture sensor to reduce over watering.
 - Choose more drought tolerant plants to reduce water use.

HAPA

Landscape Architecture
 Urban Design
 403 - 375 West Fifth Avenue
 Vancouver BC, V5Y 1J6
 604 909 4150
 hapacoba.com

NOT FOR CONSTRUCTION

No.	Description	Date
17	Re-issue for DP	Oct 03 2023
16	Re-issue for DP	Sep 12 2023
15	Issue for DP	Jul 20 2023
14	Issue for DDP	NOV 18 2022
13	Re-issue for ADP	Oct 06 2022
12	Re-issue for ADP	Sep 27 2022
11	Re-issue for Review	Apr 27 2022
10	Re-issue for Review	Oct 29 2020
9	Re-issue for Review	Oct 16 2020
8	Re-issue for Review	Feb 19 2020
7	Re-issue for Review	Mar 15 2019
6	Re-issue for ADP	Dec 3 2018
5	Re-issue for DP	Oct 12 2018
4	Issue for DP	Oct 17 2017
3	Re-issue for Rezoning	Jul 26 2017
3	Re-issue for Rezoning	Jun 8 2017
2	Issue for Rezoning	Mar 13 2017
1	Issue for Rezoning	Nov 17 2015

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**Multitunit Development
 9291 & 9311 No 2 Rd**

Richmond BC

**Landscape
 Materials Plan**

Date	June 10 2021	Drawing Number
Project No.	15044	
Scale	AS SHOWN	L1.11
Drawn/Checked	MC/JF	

DP 17-790086

PLAN #10

October 4, 2023

Plant Schedule

SYM	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE	COMMENTS
Tree					
Ps	8	<i>Prunus serrulata</i> 'Amanogawa'	Japanese Cherry	8.0 cm Cal., B&B	Full, well branched
Sj	15	<i>Styrax japonicus</i>	Japanese Snowbell	8.0 cm Cal., B&B	Full, uniform branching
Apr	5	<i>Acer circinatum</i>	Vine Maple	2.4 m ht. (8'-0" ht.)	B&B, Nursery grown, minimum 3 stems
POR	10	<i>Picea omarika</i>	Serbian Spruce	2.4 m ht. (8'-0" ht.)	B&B, Well branched
Shrub and Grass					
Cm	107	<i>Carex morrowii</i> 'Ice Dance'	Evergreen Sedge	#1 Pot, 600mm o.c.	Full
Cc	30	<i>Cholysia ternata</i>	Mexican Orange	#3 Pot, 900mm o.c.	Full
Db	130	<i>Aucuba japonica</i> 'Rozannie'	Compact Japanese aucuba	#3 Pot, 750mm o.c.	Full
Hm	159	<i>Hemerocallis</i> 'Mac the Knife'	Red Daylily	#1 Pot, 450mm o.c.	Full
Ho	28	<i>Berberis buxifolia</i> 'Nana'	Dwarf Magellan Barberry	#3 Pot, 900mm o.c.	Full
Sk	114	<i>Skimmia rubella</i>	Japanese Skimmia	#2 Pot, 500mm o.c.	Full
Th	88	<i>Thuja occidentalis</i> 'Smaragd'	Emerald Cedar	1.2m Ht., 600mm o.c.	Sheared, full
Tm	104	<i>Taxus x media</i> 'Hicksii'	Hick's Yew	1.2m Ht., 500mm o.c.	Sheared, full
Vm	486	<i>Vinca minor</i>	Periwinkle	#1 Pot, 450mm o.c.	Full 15cm (6") leads.
Auu	84	<i>Arctostaphylos uva ursi</i>	Kinnikinnick	#1 Pot, 380mm o.c.	Minimum 3 leads, nursery grown
Auu	71	<i>Nandina domestica</i> 'Gulf Stream'	Compact Heavenly Bamboo	#2 Pot, 300mm o.c.	Well established

1. Provide all plant material meeting BCSLA / BCNLA Standard, latest edition.
2. All street trees are to be planted in accordance with City of Richmond requirements for species, soil volume and installation. Confirm with Richmond Parks for species and selection.
3. No substitutions or adjustments without written approval by the landscape architect.
4. One (1) hose bib will be provided for each residence on the driveway side of each unit, coordinated between architect and mechanical engineer.
5. Including high efficiency drip irrigation system.

NOT FOR CONSTRUCTION

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2	Issue for Rezoning	Mar 13 2017
1	Issue for Rezoning	Nov 17 2015

No.	Description	Date

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Multiunit Development
9291 & 9311 No 2 Rd

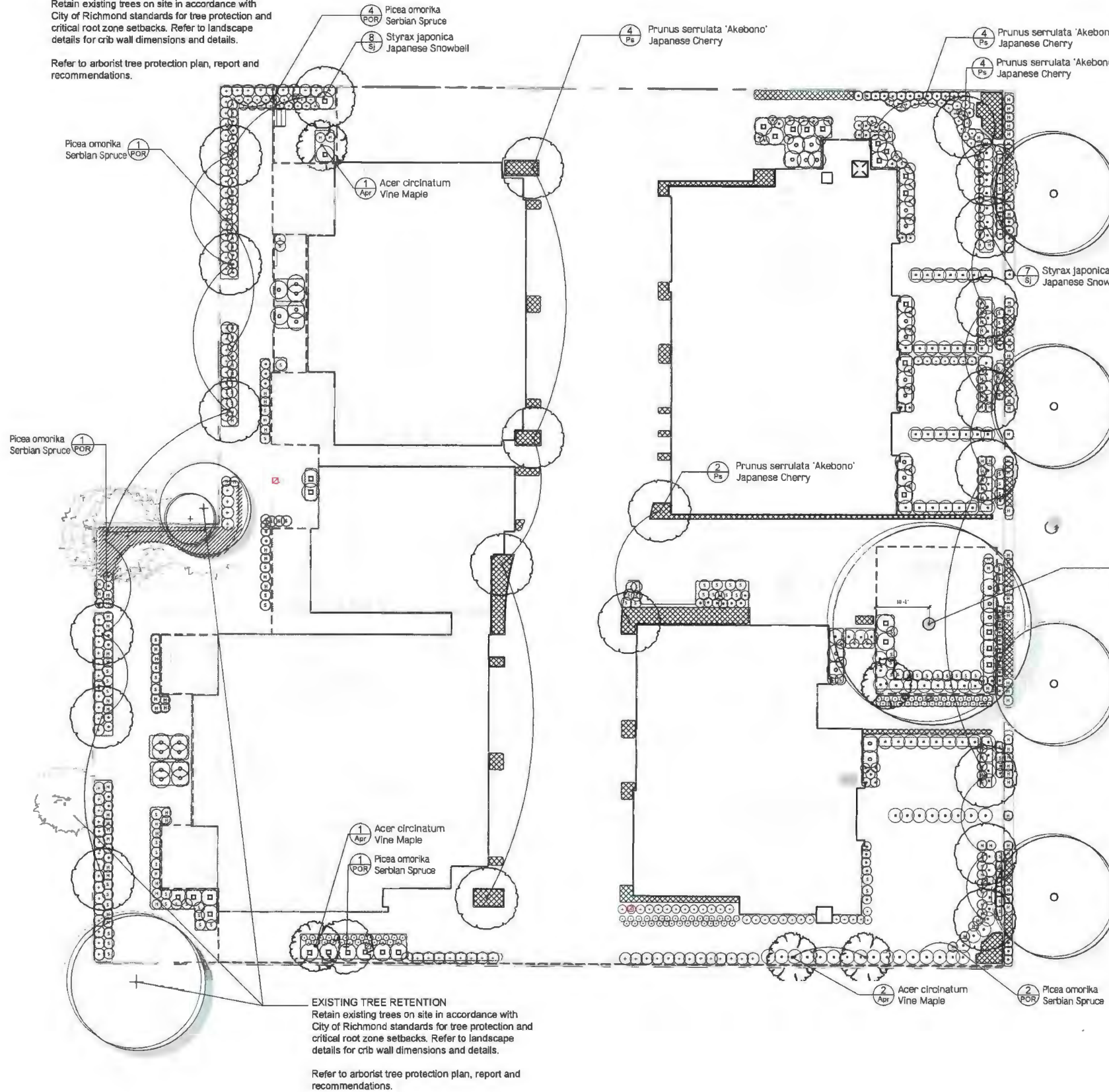
Richmond BC

Landscape
Planting Plan

Date	June 10 2021	Drawing Number	
Project No.	15044		
Scale	as shown		L1.21
Drawn/Checked	MC/JF		

EXISTING TREE RETENTION
Retain existing trees on site in accordance with City of Richmond standards for tree protection and critical root zone setbacks. Refer to landscape details for crib wall dimensions and details.

Refer to arborist tree protection plan, report and recommendations.

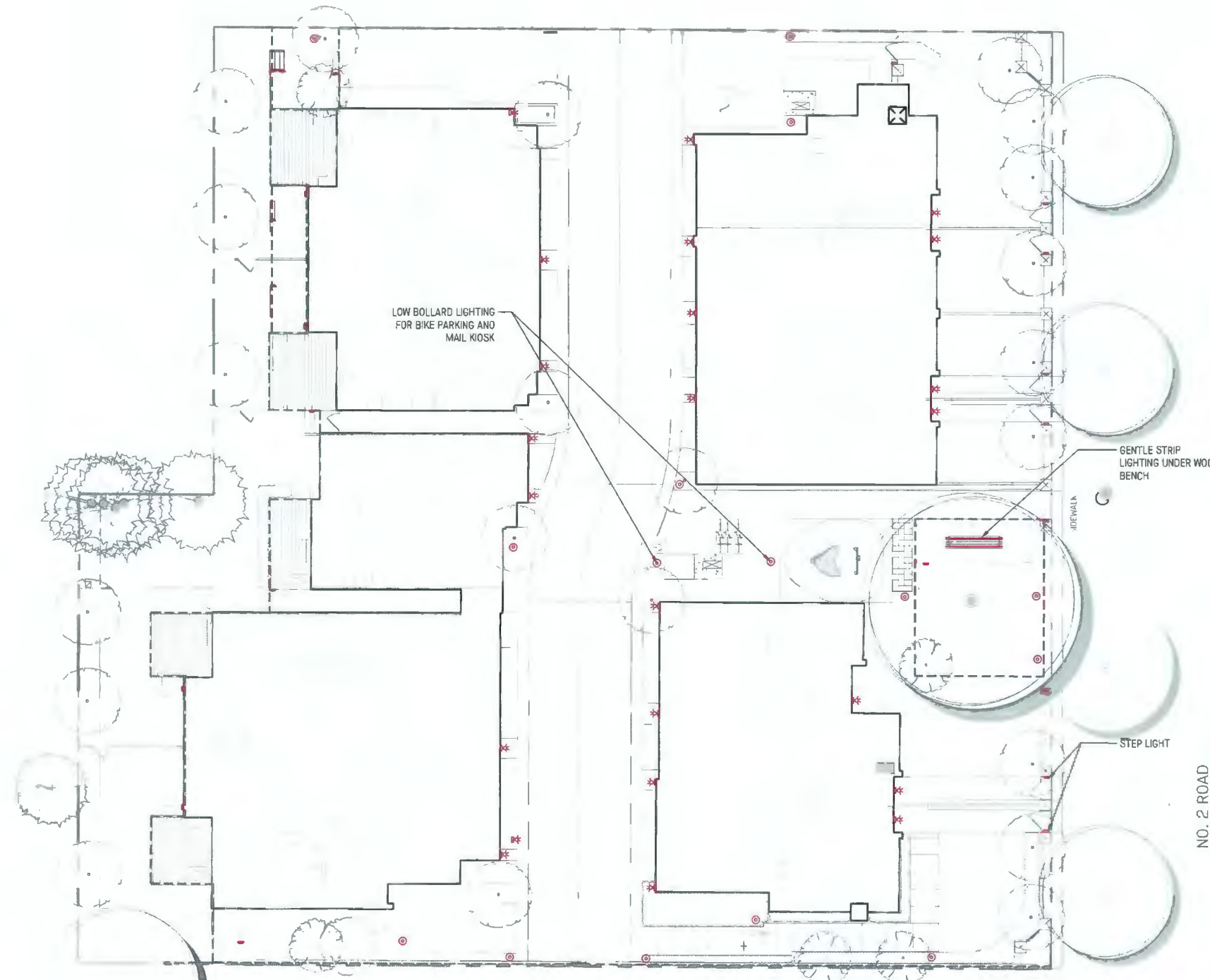


EXISTING TREE RETENTION
Retain existing trees on site in accordance with City of Richmond standards for tree protection and critical root zone setbacks. Refer to landscape details for crib wall dimensions and details.

Refer to arborist tree protection plan, report and recommendations.

EXISTING TREE RETENTION

Refer to arborist tree protection plan, report and recommendations.



EXAMPLE LIGHT:

STEPLIGHT
 BRAND: BEGA
 MODEL: Recessed wall luminaire 33051
 Shielded

REPRESENTATIVE LIGHT IMAGE



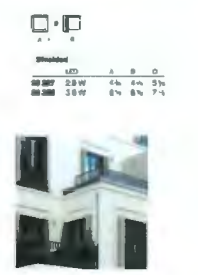
BOLLARD LIGHT
 BRAND: BEGA
 MODEL: Bollard 77221
 Shielded

REPRESENTATIVE LIGHT IMAGE



WALL MOUNTED SCENCE LIGHTING
 BRAND: BEGA
 MODEL: Impact-resistant wall luminaire Shielded 33 327

REPRESENTATIVE LIGHT IMAGE



LIGHTING SYMBOLS

SYMBOL	QTY	LIGHT TYPE
—	2 x 10'	STRIP UNDERLIGHTING
⊙	14	BOLLARD LIGHT
✱	23	WALL MOUNTED SCENCE LIGHTING
⬇	28	STEPLIGHT

NOTES

- REFER TO ELECTRICAL DRAWINGS;
- ALL DIMENSIONS ARE TO CENTRE OF LIGHT;
- PROVIDE LED LIGHT SOURCES FOR ALL LANDSCAPE LIGHTING;
- WITH THE EXCEPTION OF TREE UPLIGHTS, PROVIDE LIGHT FIXTURES IN ACCORDANCE WITH DARK SKY GUIDELINES;
- IMAGES OF LIGHT FIXTURES ARE REPRESENTATIVE ONLY, MANUFACTURE OF ACTUAL INSTALLED PRODUCTS MAY VARY.

HAPA
 Landscape Architecture
 Urban Design
 403 - 375 West Fifth Avenue
 Vancouver BC, V5Y 1J6
 604 909 4150
 hapacoba.com

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Multitnit Development
9291 & 9311 No 2 Rd

Richmond BC

Landscape
Lighting Plan

IRRIGATION NOTES:

- IRRIGATION DESIGN IS FOR INFORMATION ONLY. PLANS ARE NOT TO BE CONSTRUCTED FROM.
- LANDSCAPE CONTRACTOR TO PROVIDE COMPLETE DESIGN-BUILD SERVICES FOR ALL AUTOMATIC SITE IRRIGATION WORKS.
- IRRIGATION CONTRACTOR PERFORMING THE WORK MUST HAVE A MIN. (5) FIVE YEARS DOCUMENTED EXPERIENCE AND A MEMBER IN GOOD STANDING OF THE IABC (IRRIGATION INDUSTRY ASSOCIATED OF BC).
- IRRIGATION TO CONFORM TO ALL LOCAL PLUMBING AND ELECTRICAL CODE REQUIREMENTS.
- IRRIGATION TO CONFORM TO ALL IABC STANDARD AND UNIVERSITY OF BRITISH COLUMBIA STANDARDS.
- COORDINATE WITH CIVIL CONTRACTOR TO PROVIDE PVC SLEEVING WHERE REQUIRED.
- REFER TO PLAN FOR HOSE BIB LOCATIONS.
- PROVIDE SHOP DRAWINGS FOR IRRIGATION LAYOUT TO LANDSCAPE ARCHITECT FOR REVIEW AND APPROVAL PRIOR TO FABRICATION AND INSTALLATION. INCLUDE SLEEVING, DRIP LINE, PIPE SIZE, HEAD LAYOUT, VALVE BOXES, ETC., AND ALL IRRIGATION COMPONENT SPECIFICATIONS. VALVE BOXES TO BE LOCATED IN LOW-VISIBLE, PLANTED AREAS ONLY AND LOCATIONS ARE TO BE APPROVED PRIOR TO INSTALL.
- HIGH EFFICIENCY IRRIGATION TECHNOLOGY IS TO BE USED.
- TEST SYSTEM PRIOR TO COMPLETION OF LANDSCAPE WORKS TO ENSURE NO LEAKAGE AND SPECIFIED PSI IS MET. PROVIDE WRITTEN NOTICE THAT PRESSURE TESTING HAS MET STANDARDS (LEAKS SHALL NOT BE REPAIRED WITH PATCHING).
- PROVIDE COVERAGE AND CONTROLLER TEST WHEN SYSTEM IS COMPLETE AND MAKE ADJUSTMENTS AS REQUIRED.
- PROVIDE AS-BUILT RECORD DRAWINGS TO SCALE SHOWING LOCATIONS OF ALL CONCEALED COMPONENTS, PIPING AND CONDUIT.
- MAINTAIN AND OPERATE IRRIGATION PER MANUFACTURER'S RECOMMENDATIONS THROUGHOUT THE WARRANTY PERIOD. WINTERIZATION AS REQUIRED.
- PROVIDE (3) THREE COPIES OF INSTRUCTION MANUALS FOR OPERATION AND MAINTENANCE OF SYSTEM AND CONTROLS, SEASONAL ACTIVITY AND SHUTDOWN AND MANUFACTURER'S PARTS CATALOGUE.
- INSTRUCT OWNER'S PERSONNEL IN THE OPERATION AND MAINTENANCE OF THE SYSTEM, INCLUDING ADJUSTING OF SPRINKLER HEADS. USE OPERATION AND MAINTENANCE MANUAL AS BASIS FOR DEMONSTRATION.

NOT FOR CONSTRUCTION

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No.	Description	Date
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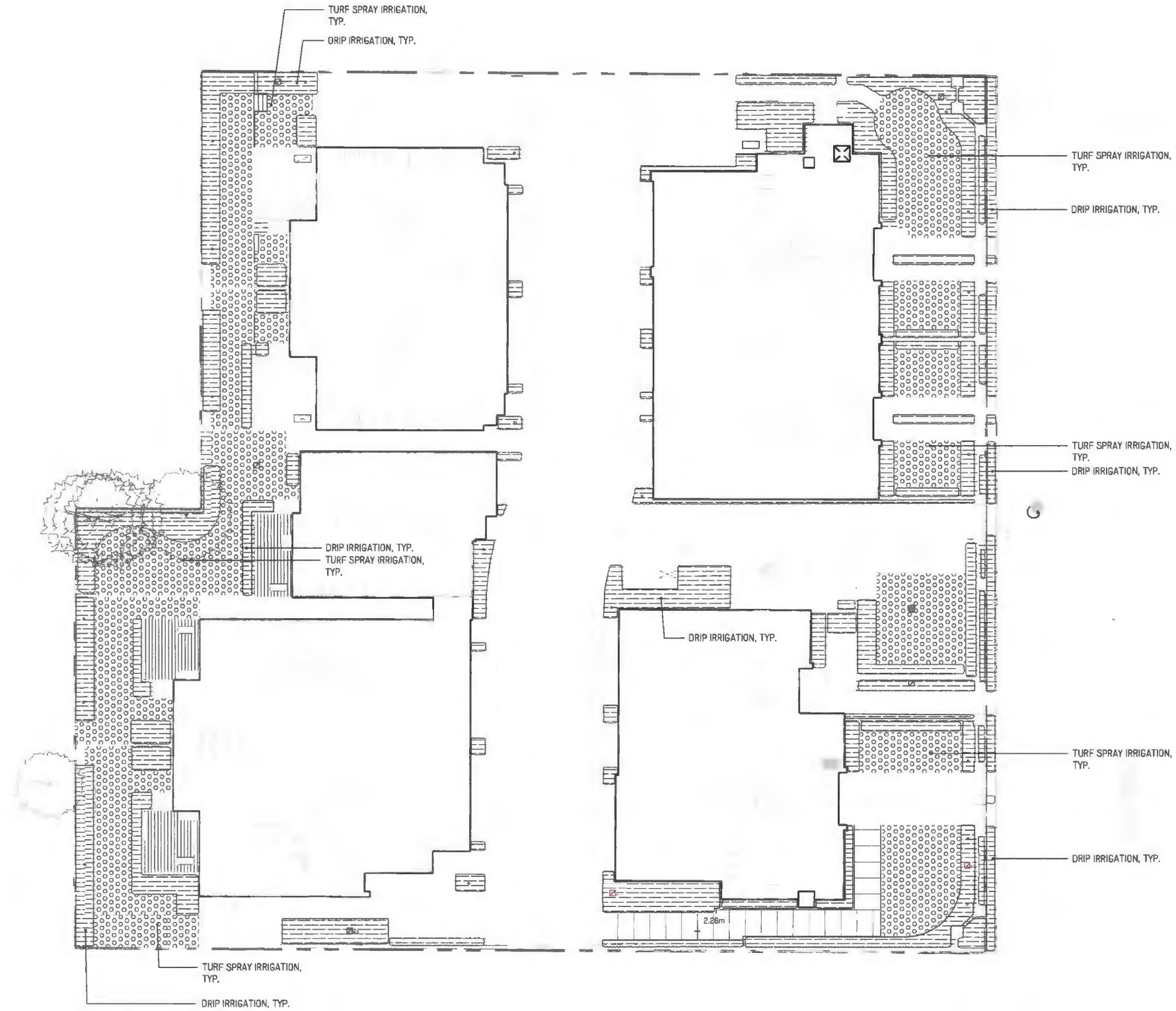
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**Multitunit Development
9291 & 9311 No 2 Rd**

Richmond BC

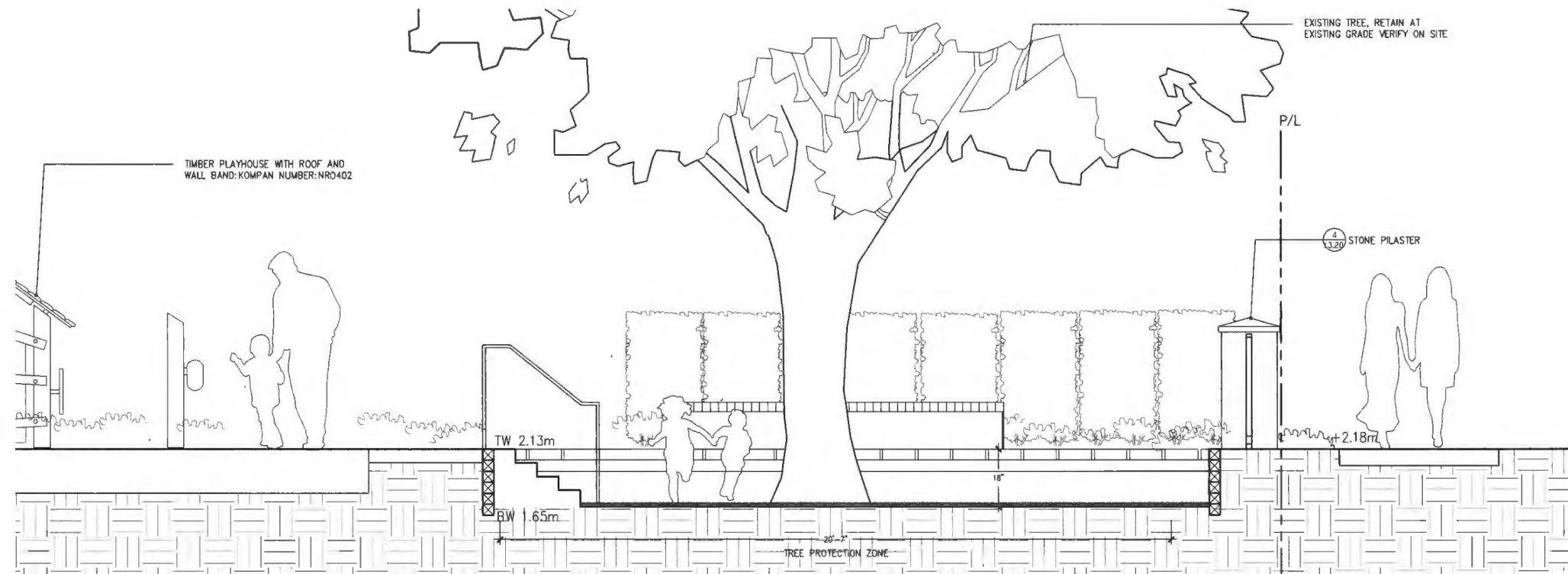
**Landscape
Irrigation Plan**

Date	June 10 2021	Drawing Number
Project No.	15044	
Scale	as shown	L1.41
Drawn/Checked	MC/JF	

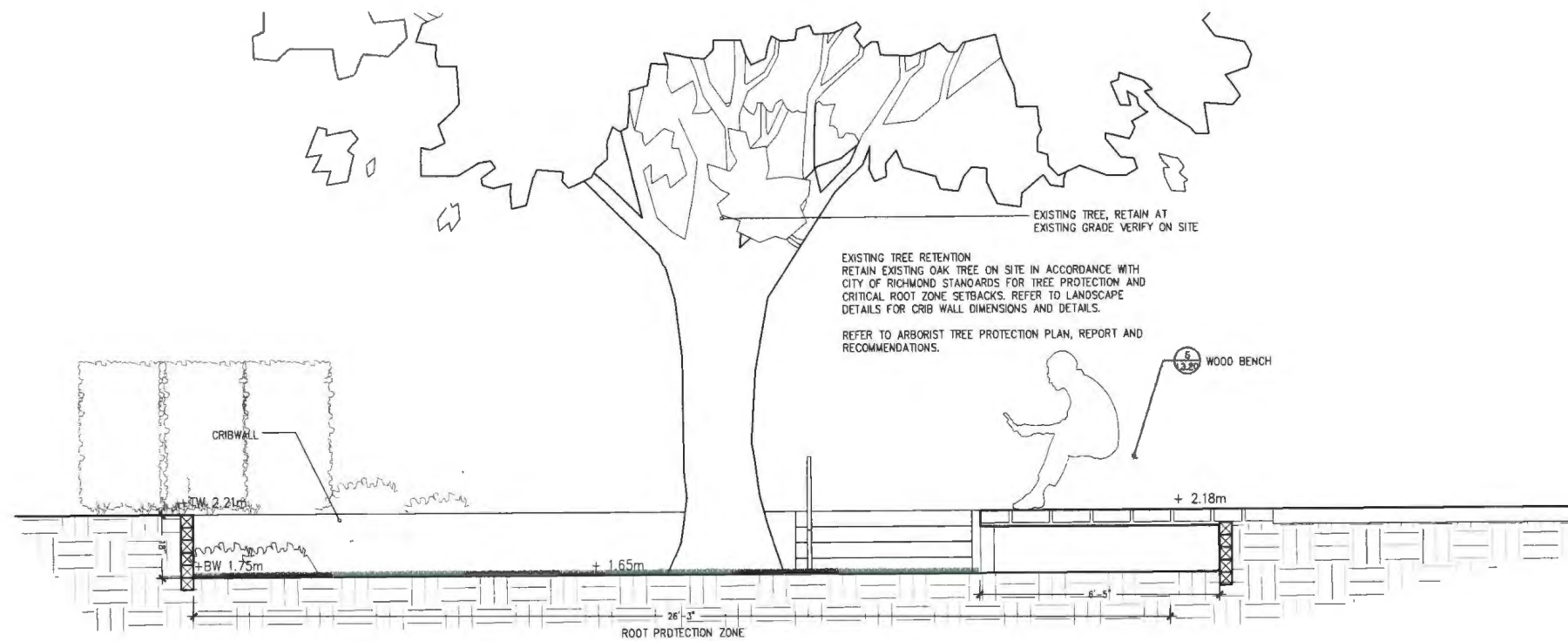


IRRIGATION SCHEDULE

SYMBOL	DESCRIPTION	QTY	PRECIP	PSI	GPM
	Drip	3 707 s.f.	1.44 in/h	30	55
	Turf Spray	3 081 s.f.	1.5 in/h	35	48

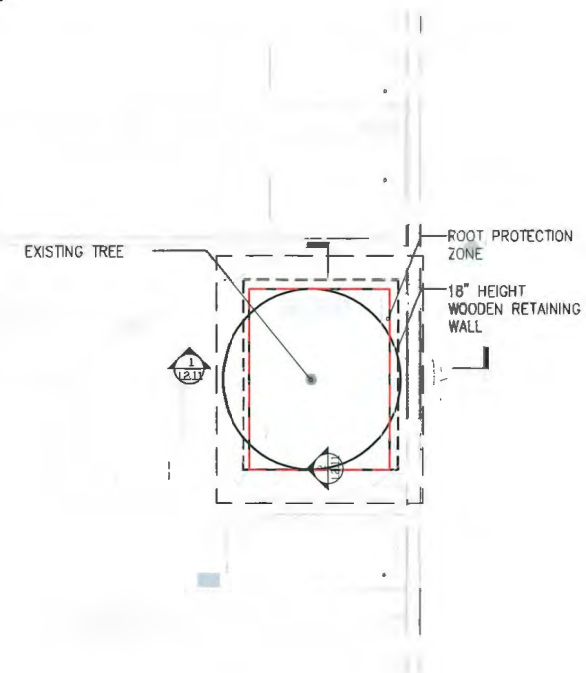


1 Tree Protection (East to West Profile)
L2.11 1/2" - 1' 0"



2 Tree Protection (South to North Profile)
L2.11 1/2" - 1' 0"

KEY MAP



NOT FOR CONSTRUCTION

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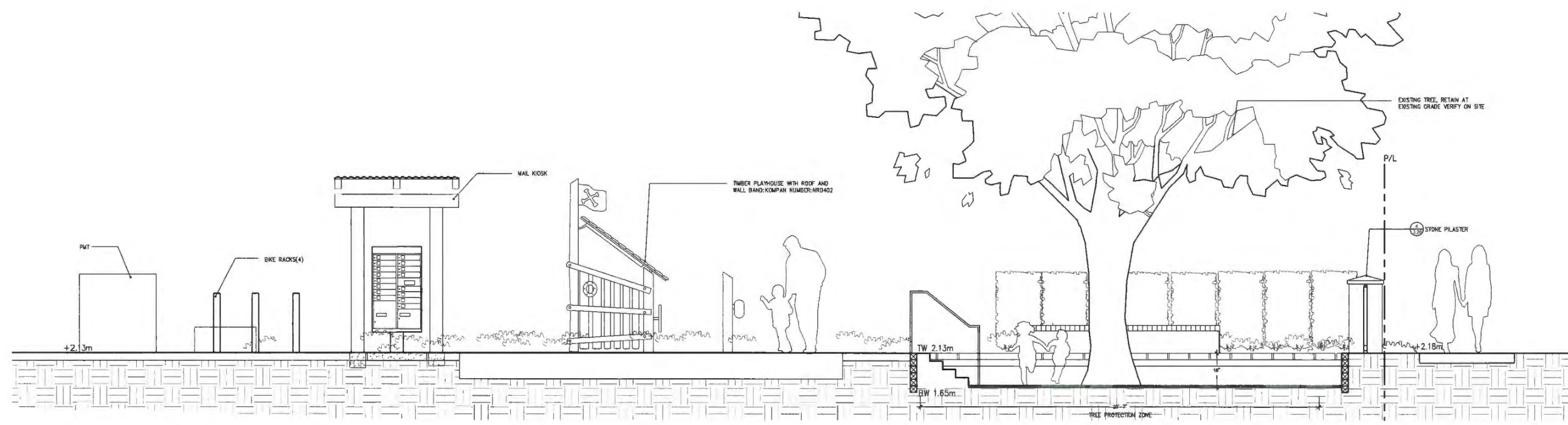
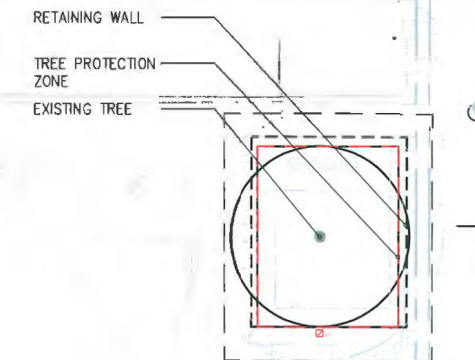
No.	Description	Date
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Multituit Development
9291 & 9311 No 2 Rd
Richmond BC

Landscape Sections

KEY MAP



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No.	Description	Date
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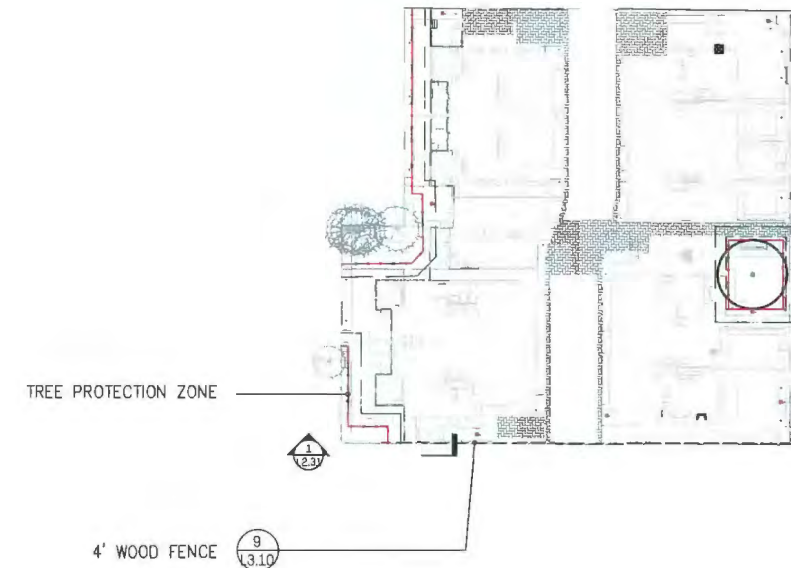
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Multituit Development
9291 & 9311 No 2 Rd
Richmond BC

AMENITY AREA SECTION

1 Section Through Amenity Area and Wood Deck
L2.21 3/8" = 1' 0"

KEY MAP



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No. Description Date

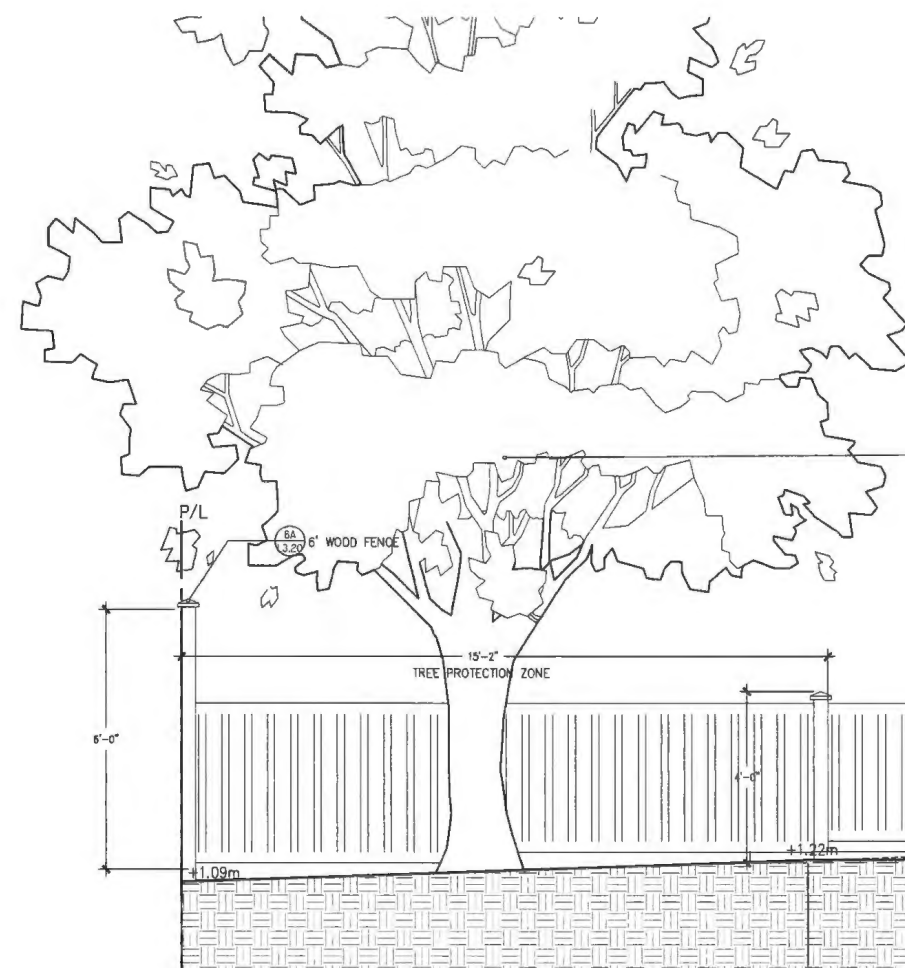
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Multitunit Development 9291 & 9311 No 2 Rd

Richmond BC

Section South Side

Date	June 10 2021	Drawing Number
Project No.	15044	L2.31
Scale	as shown	
Drawn/Checked	MJC/JF	



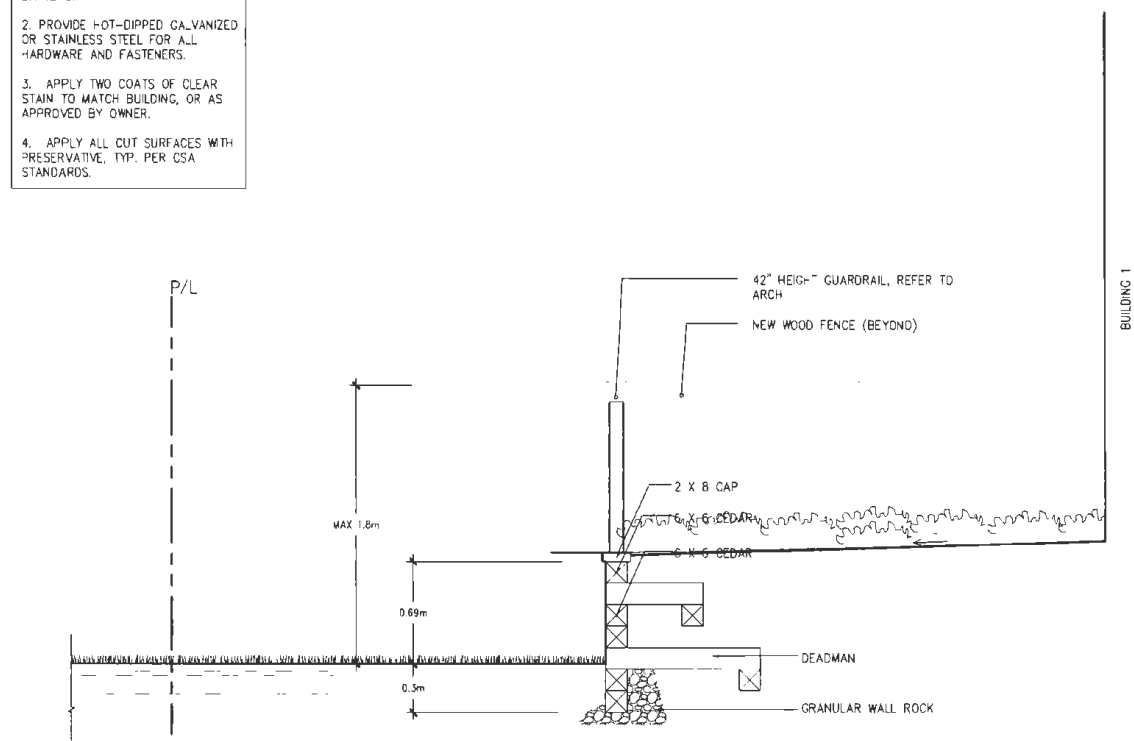
EXISTING TREE RETENTION
RETAIN EXISTING MAGNOLIA GRANDIFLORA TREE
(TREE NO.4) ON SITE IN ACCORDANCE WITH CITY
OF RICHMOND STANDARDS FOR TREE PROTECTION
AND CRITICAL ROOT ZONE SETBACKS. REFER TO
LANDSCAPE DETAILS FOR CRIB WALL DIMENSIONS
AND DETAILS.

REFER TO ARBORIST TREE PROTECTION PLAN,
REPORT AND RECOMMENDATIONS.

EXISTING TREE, RETAIN AT
EXISTING GRADE VERIFY ON SITE

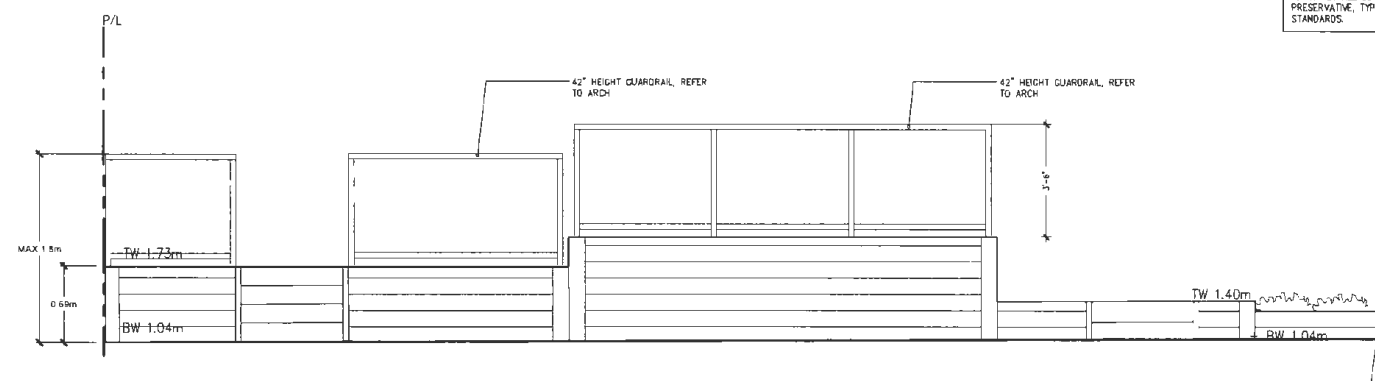
1 Wood fence (South)
L2.31 1/2" = 1' 0"

1. PROVIDE PRESSURE-TREATED LUMBER FOR ALL WOOD MEMBERS, OR CEDAR FOR BOARDS AND BATTENS.
2. PROVIDE HOT-DIPPED GALVANIZED OR STAINLESS STEEL FOR ALL HARDWARE AND FASTENERS.
3. APPLY TWO COATS OF CLEAR STAIN TO MATCH BUILDING, OR AS APPROVED BY OWNER.
4. APPLY ALL CUT SURFACES WITH PRESERVATIVE, TYP. PER CSA STANDARDS.

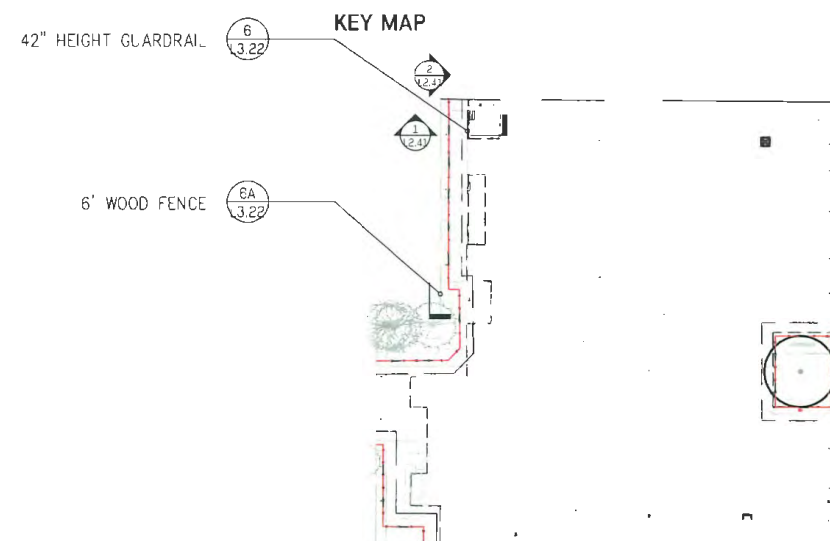


1 DETAIL & SECTION OF HIGH TIMBER RETAINING WALL AND FENCE ON WEST SIDE OF BUILDING 1 (EAST TO WEST)
L2.41 1/2" = 1' 0"

1. PROVIDE PRESSURE-TREATED LUMBER FOR ALL WOOD MEMBERS, OR CEDAR FOR BOARDS AND BATTENS.
2. PROVIDE HOT-DIPPED GALVANIZED OR STAINLESS STEEL FOR ALL HARDWARE AND FASTENERS.
3. APPLY TWO COATS OF CLEAR STAIN TO MATCH BUILDING, OR AS APPROVED BY OWNER.
4. APPLY ALL CUT SURFACES WITH PRESERVATIVE, TYP. PER CSA STANDARDS.



2 ELEVATION OF HIGH TIMBER RETAINING WALL AND GUARDRAIL ON WEST SIDE OF BUILDING 1 (NORTH TO SOUTH)
L2.41 3/8" = 1' 0"



NOT FOR CONSTRUCTION

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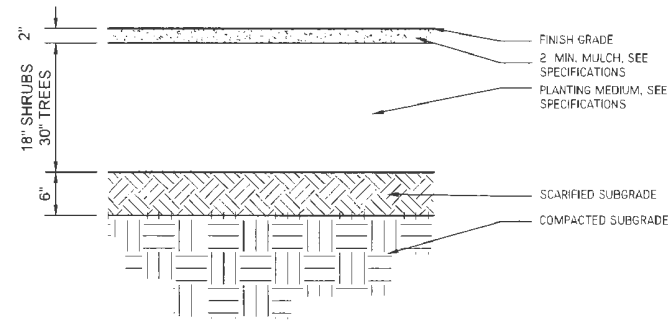
No. Description Date

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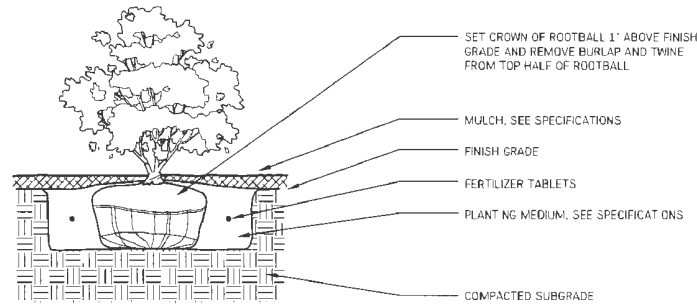
Multinuit Development
9291 & 9311 No 2 Rd

Richmond BC

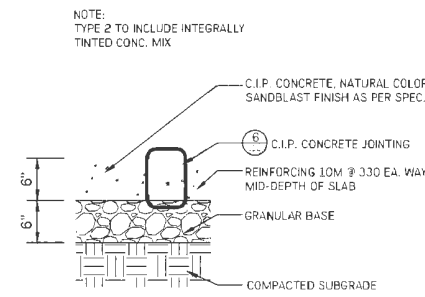
Section West Side



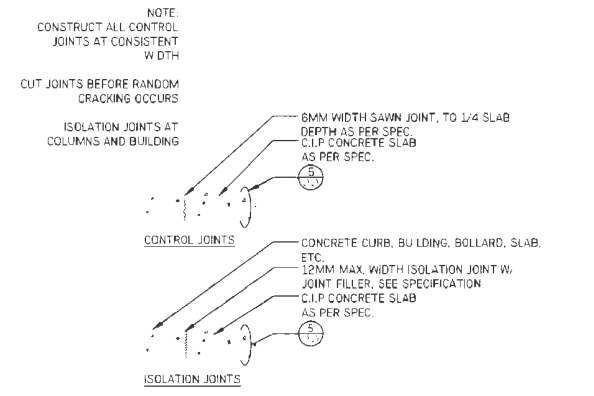
1 Soil profile Typ.
L3.10 1' - 1' 0"



2 Shrub Planting Typ.
L3.10 1' - 1' 0"



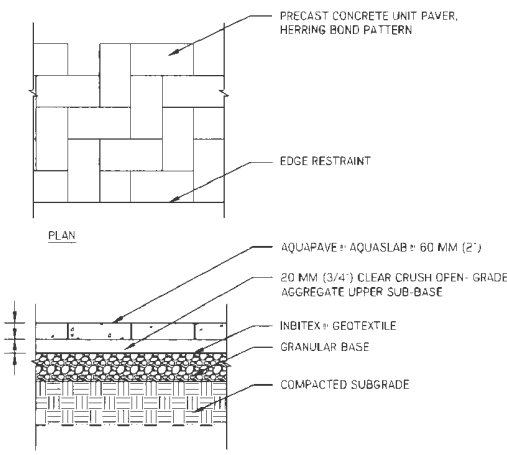
5 C.I.P. Concrete Typ.
L3.10 1' - 1' 0"



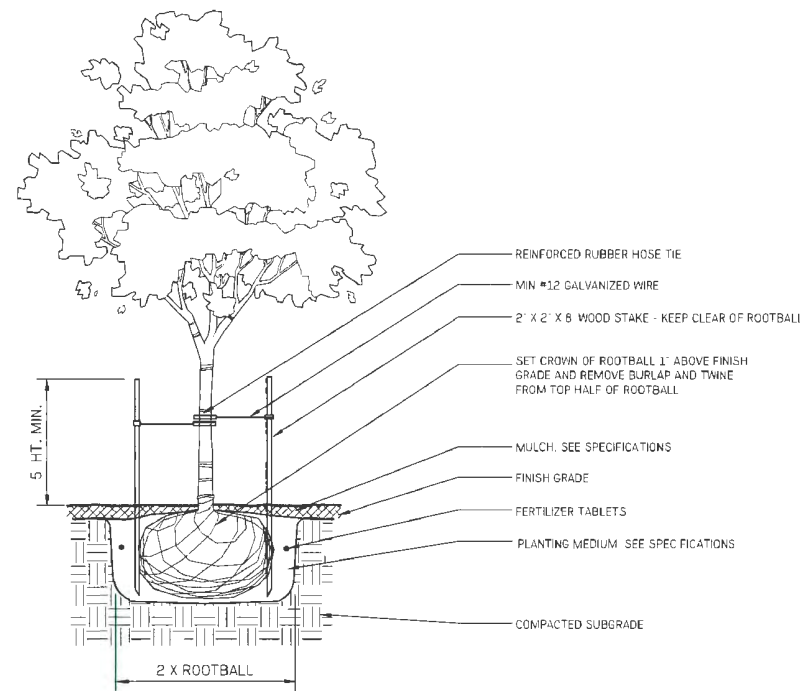
PAVING NOTES:
 DECORATIVE PERMEABLE STONE PAVING
 VERIFY PAVER SIZE, COLOR AND FINISH TO MATCH ADJACENT DEVELOPMENT PAVING
 STANDARD AQUAPAVE PERMEABLE PAVER
 SUPPLIED BY ABBOTSFORD CONCRETE PRODUCTS
 WWW.PAVINGSTONES.COM OR APPROVED EQUAL.
 1. THICKNESS: 60MM
 2. 4" X 8" SIZE
 114 X 228 X 60 MM DEPTH STANDARD
 3. COLOR AND FINISH: SHADOW / NATURAL SAND
 4. PATTERN: HERRING BOND PATTERN. REFER TO PLAN AND DETAIL.
 CURVED PATH PAVER LAYOUT: TO BE HERRING BOND ALIGNED WITH PATH CURVE AS SHOWN
 LAY OUT PAVERS AT ONE FIRST THEN CONTINUE TO PATH INTERIOR
 HERRING BOND TO CURVE WITH PATHWAY CURVES



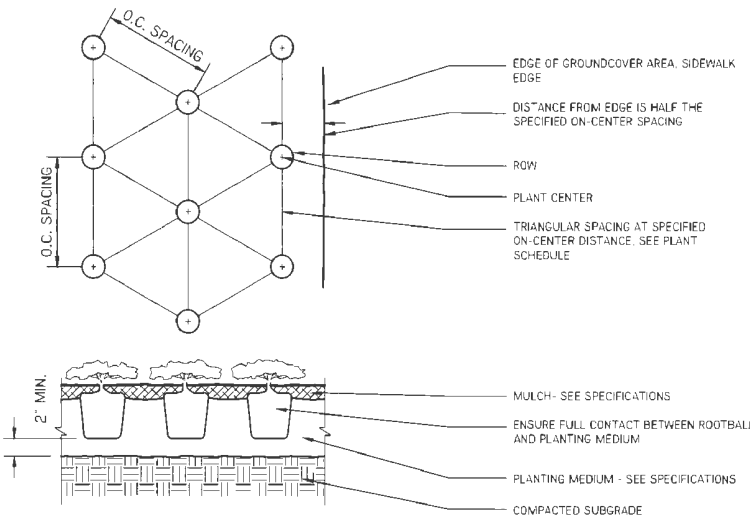
ABBOTSFORD CONCRETE PRODUCTS



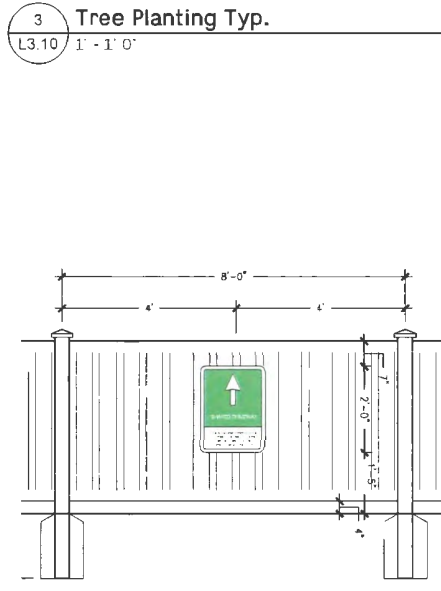
7 Permeable Stone Pavers
L3.10 1' - 1' 0"



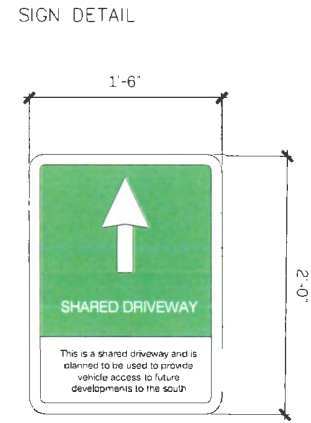
3 Tree Planting Typ.
L3.10 1' - 1' 0"



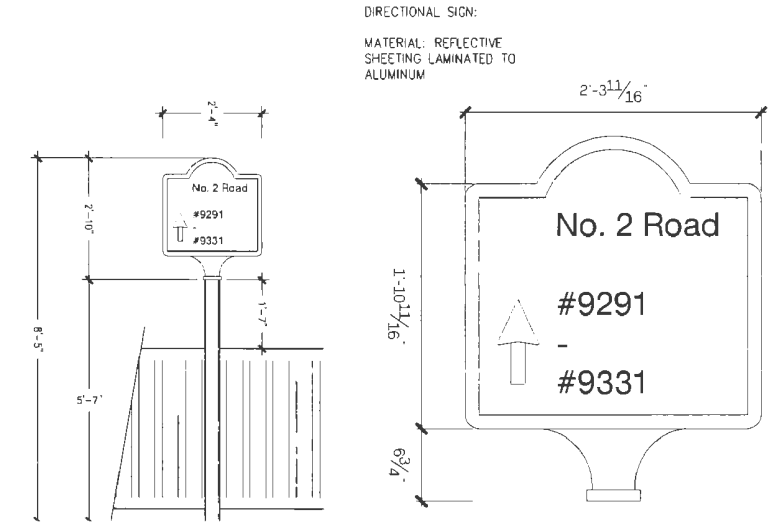
4 Groundcover Spacing Typ.
L3.10 1' - 1' 0"



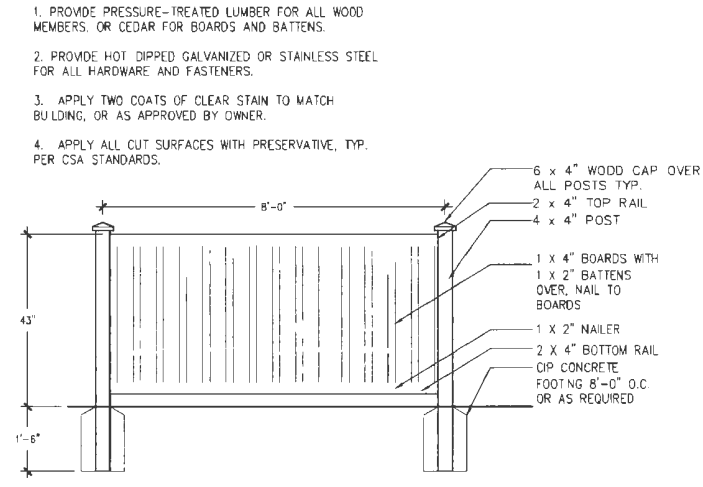
8 Drive Aisle Sign on 4' Wood Fence
L3.10 1' - 1' 0"



DRIVE AISLE SIGN:
 WORD:
 "This is a shared driveway and is planned to be used to provide vehicle access to future developments to the south"
 MATERIAL: REFLECTIVE SHEETING LAMINATED TO ALUMINIUM
 SIGN SIZE (W x H):
 18" X 24"



9 Directional Sign on 4' Wood Fence
L3.10 1' - 1' 0"

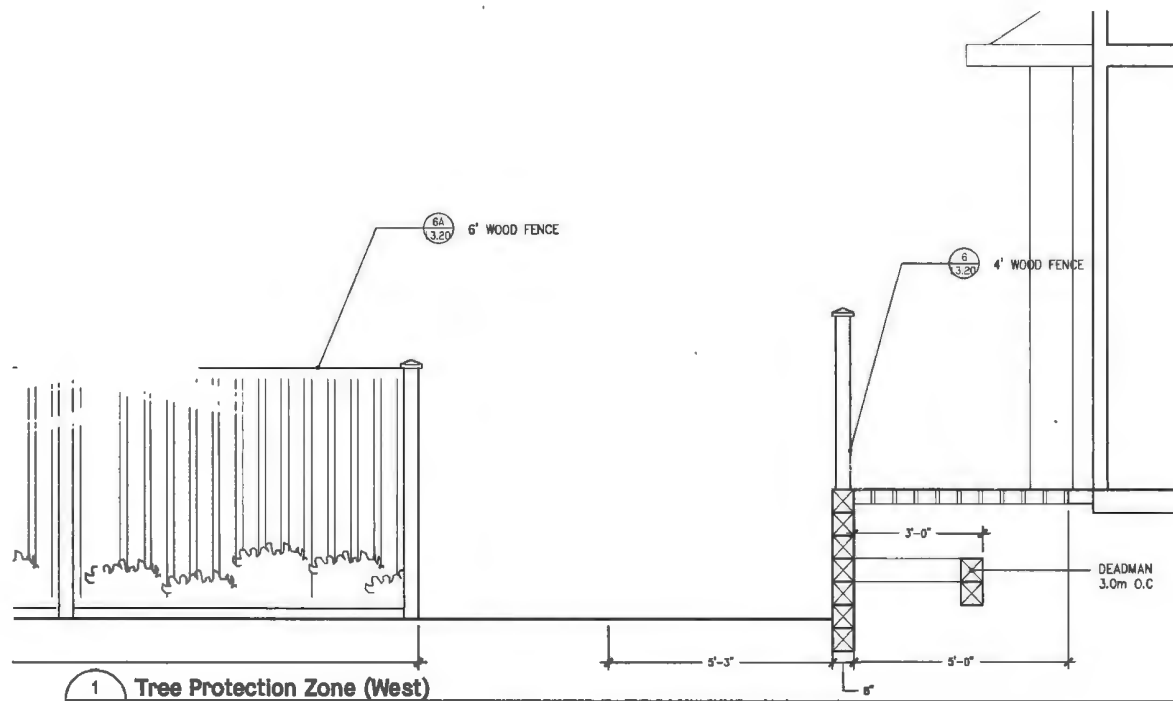


9 43" Height Wood Fence
L3.10 1/2" - 1' 0"

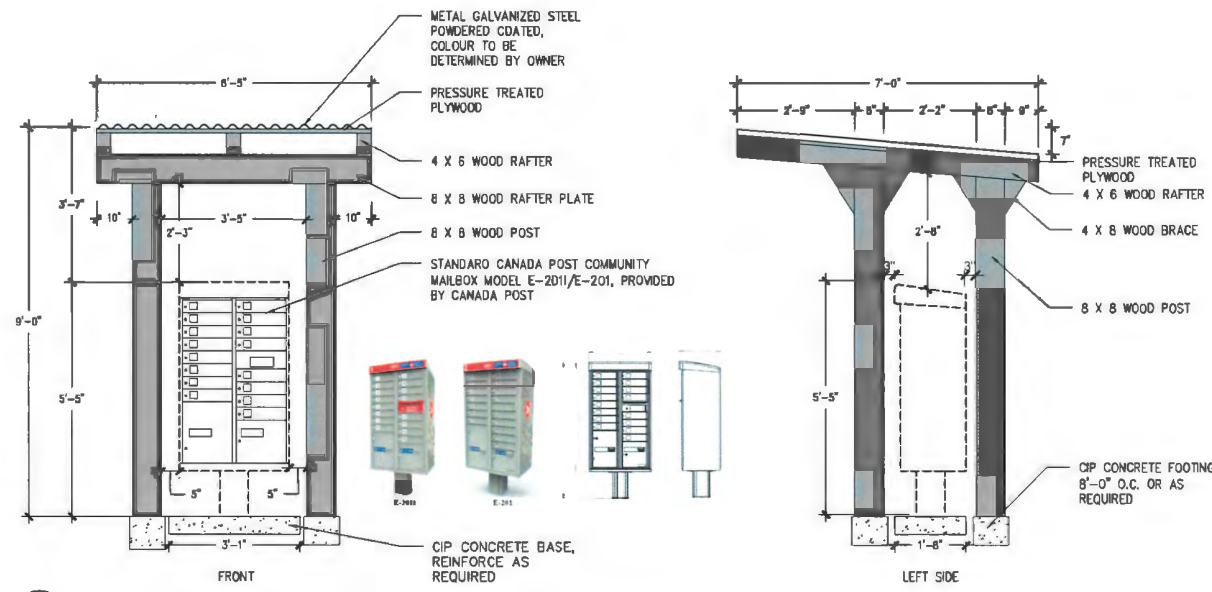
NOT FOR CONSTRUCTION

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Multiunit Development
9291 & 9311 No 2 Rd
 Richmond BC
 Landscape Details



1 Tree Protection Zone (West)
L3.20 1/2" - 1' 0"

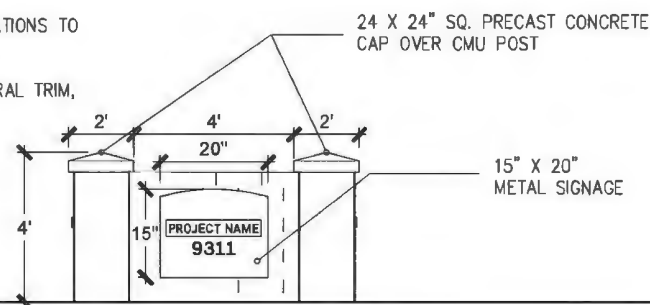


2 Custom Shelter for Standard Canada Post Community Mailbox
L3.20 1/2" - 1' 0"

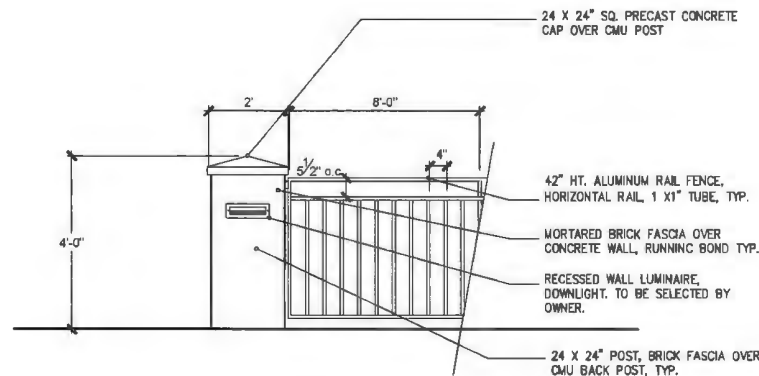
NOTES:

PROVIDE CONCRETE FOUNDATIONS TO 450MM DEPTH TYP.

BRICK: MATCH ARCHITECTURAL TRIM, TYP.



3 Metal Signage Mounted to Wall
L3.20 3/4" - 1' 0"



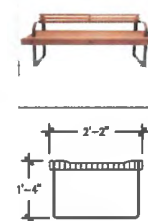
4 Stone Pilaster Metal Fence Typ.
L3.20 1/2" - 1' 0"

NOTES:

PROVIDE CONCRETE FOUNDATIONS TO 450MM DEPTH TYP.

METAL: PAINTED ALUMINUM TO MATCH ARCHITECTURAL TRIM, REFER TO DRAWINGS.

BRICK: MATCH ARCHITECTURAL TRIM, TYP.



5 Wood Bench Typ.
L3.20 1/2" - 1' 0"

BENCH NOTES:

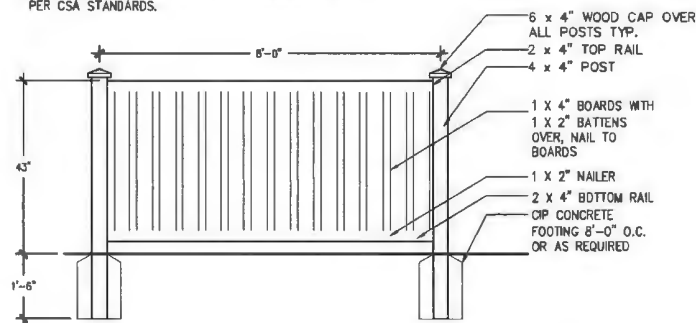
DESIGN -SERIES J - D3-5, BY FRANCES ANDREW SITE FURNISHINGS LTD. OR APPROVED EQUAL.

Wood Info: Ipe hardwood. Wood members are finished to 1-1/2" (3.7 cm) x 3" (7.6 cm) and 1-1/2" (3.7 cm) x 2-1/2" (6.2 cm) with 1/4" (0.63 cm) radius on edges and ends. Ipe is smooth sanded and sealed.

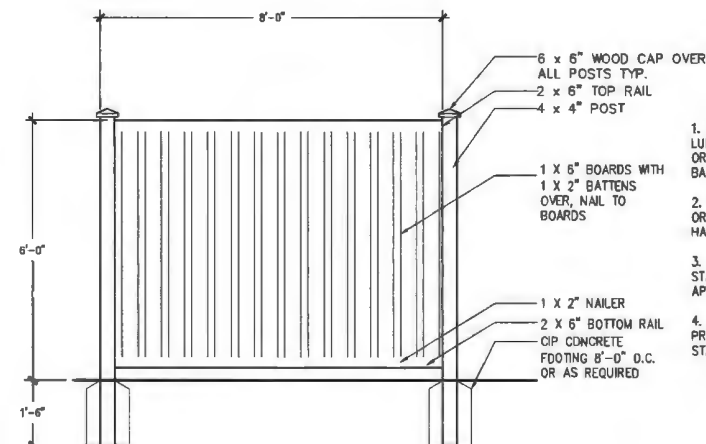
Metal Info: Mild steel, wheel-abraded to bare metal then precisely fabricated to its final shape. Metal is coated with polyester powder, electrostatically applied and baked at a high temperature to produce a mar-resistant finish.

Hardware Info: Nut design and bolt-through construction create a single, solid unit. A special wrench for vandal resistant hardware is provided with each bench. All hardware is professionally finished with our baked-on polyester powder.

2. PROVIDE HOT-DIPPED GALVANIZED OR STAINLESS STEEL FOR ALL HARDWARE AND FASTENERS.
3. APPLY TWO COATS OF CLEAR STAIN TO MATCH BUILDING, OR AS APPROVED BY OWNER.
4. APPLY ALL CUT SURFACES WITH PRESERVATIVE, TYP. PER CSA STANDARDS.

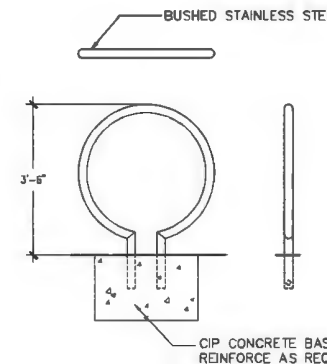


6 Wood Fence 43" Typ.
L3.20 1/2" - 1' 0"



6A Wood Fence 6" Typ.
L3.20 1/2" - 1' 0"

1. PROVIDE PRESSURE-TREATED LUMBER FOR ALL WOOD MEMBERS, OR CEDAR FOR BOARDS AND BATTENS.
2. PROVIDE HOT-DIPPED GALVANIZED OR STAINLESS STEEL FOR ALL HARDWARE AND FASTENERS.
3. APPLY TWO COATS OF CLEAR STAIN TO MATCH BUILDING, OR AS APPROVED BY OWNER.
4. APPLY ALL CUT SURFACES WITH PRESERVATIVE, TYP. PER CSA STANDARDS.



7 Bike Rack
L3.20 1/2" - 1' 0"

NOTE:

REFER TO SPEC FOR DIMENSIONS AND INSTALLATION. "RING" PRODUCT FROM LANDSCAPE FORMS O.A.E. PROVIDE SHOP DRAWING.

NOT FOR CONSTRUCTION

17	Re-Issue for DP	Oct 03 2023
16	Re-Issue for DP	Sep 12 2023
15	Issue for DP	Jul 20 2023
14	Issue for DDP	Nov 18 2022
13	Re-Issue for ADP	Oct 06 2022
12	Re-Issue for ADP	Sep 27 2022
11	Re-Issue for Review	Apr 27 2022
10	Re-Issue for Review	Oct 29 2020
9	Re-Issue for Review	Oct 16 2020
8	Re-Issue for Review	Feb 19 2020
7	Re-Issue for Review	Mar 15 2019
6	Re-Issue for ADP	Dec 3 2018
5	Re-Issue for DP	Oct 12 2018
4	Issue for DP	Oct 17 2017
3	Re-Issue for Rezoning	Jul 28 2017
3	Re-Issue for Rezoning	Jun 8 2017
2	Issue for Rezoning	Mar 13 2017
1	Issue for Rezoning	Nov 17 2015

INC. Detail on DTA

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Multiunit Development
9291 & 9311 No 2 Rd

Richmond BC

Landscape Details
Walls, Furnishings

NOT FOR CONSTRUCTION

17	Re-Issue for DP	Oct 03 2023
16	Re-Issue for DP	Sep 12 2023
15	Issue for DP	Jul 20 2023
14	Issue for DDP	NOV 18 2022
13	Re-Issue for ADP	Oct 06 2022
12	Re-Issue for ADP	Sep 27 2022
11	Re-Issue for Review	Apr 27 2022
10	Re-Issue for Review	Oct 29 2020
9	Re-Issue for Review	Oct 16 2020
8	Re-Issue for Review	Feb 19 2020
7	Re-Issue for Review	Mar 15 2019
6	Re-Issue for ADP	Dec 3 2018
5	Re-Issue for DP	Oct 12 2018
4	Issue for DP	Oct 17 2017
3	Re-Issue for Rezoning	Jul 26 2017
3	Re-Issue for Rezoning	Jun 8 2017
2	Issue for Rezoning	Mar 13 2017
1	Issue for Rezoning	Nov 17 2016

No.	Description	Date
-----	-------------	------

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**Multiunit Development
9291 & 9311 No 2 Rd**

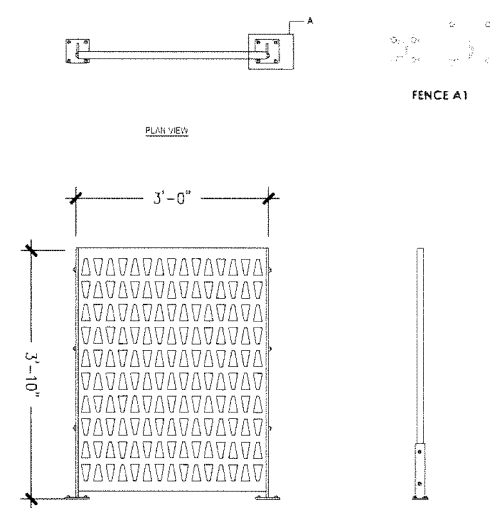
Richmond BC

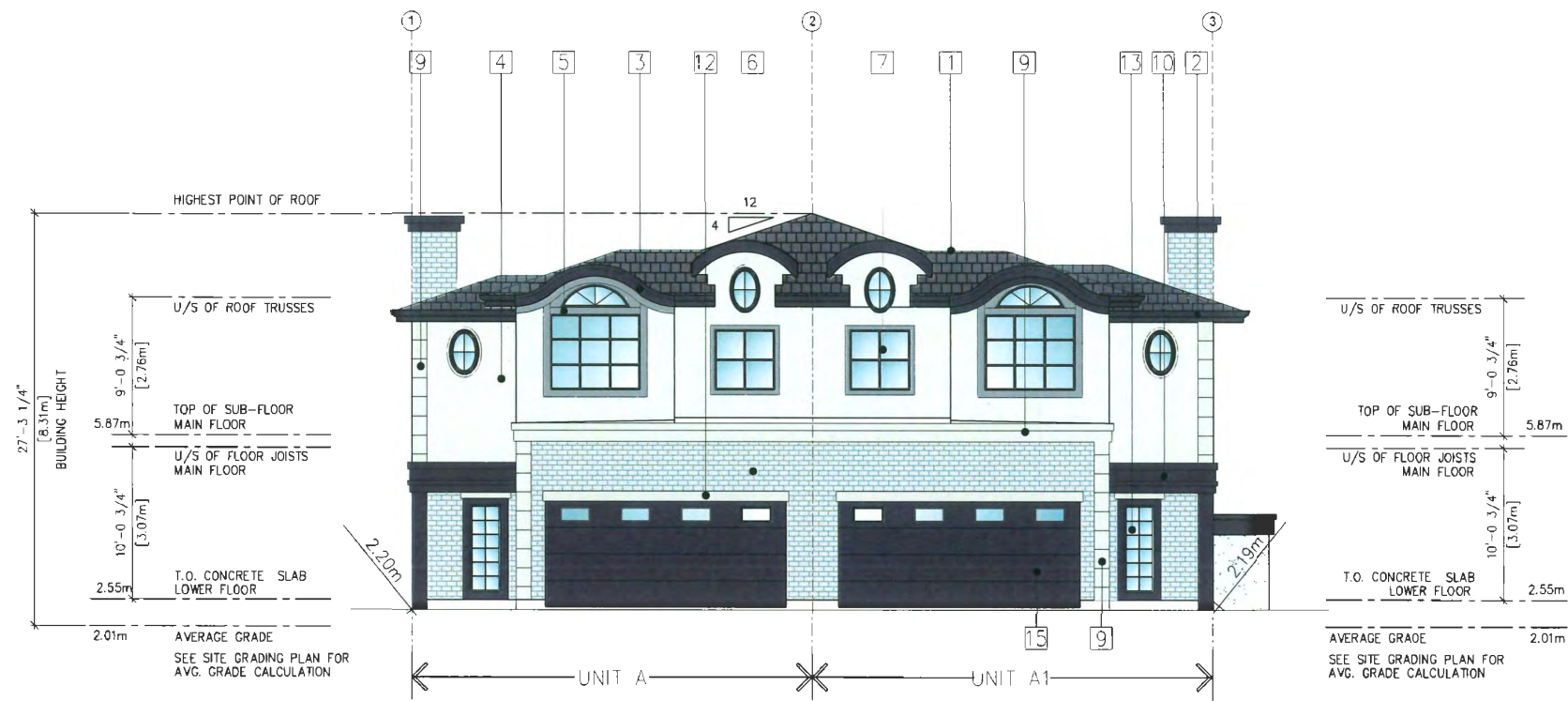
Landscape Details
Walls, Furnishings

Date	June 10 2021	Drawing Number
Project No.	15044	
Scale	as shown	L3.21
Drawn/Checked	MC/JF	

BOK MODERN PRIVACY SCREEN PANEL NOTES:

1. DETAIL IS FOR INFORMATION ONLY, CONTRACTOR TO SUBMIT SHOP DRAWINGS FOR LANDSCAPE ARCHITECT APPROVAL PRIOR TO STARTING CONSTRUCTION
2. CONTRACTOR TO LAYOUT PRIVACY PANEL FOR LANDSCAPE ARCHITECT APPROVAL PRIOR TO INSTALLATION
FENCE PANELS TO BE SURFACE MOUNTED, REFER TO ARCH FOR CURB DETAIL
3. PATTERN SHOWING FOR REFERENCE ONLY. REFER TO SPECIFICATIONS FOR PRODUCT INFORMATION AND PROPOSED PANEL PATTERN

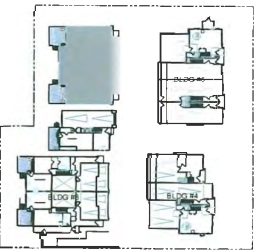




1 EAST ELEVATION
A-301 SCALE 3/16" = 1'-0"

SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE STORM AF-700
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR : PEWTER BRAND : MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS (12" X 12") & HEADER BAND
COLOR : EGGSHELL BRAND : QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL HARDIE TRIMS
COLOR : BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"

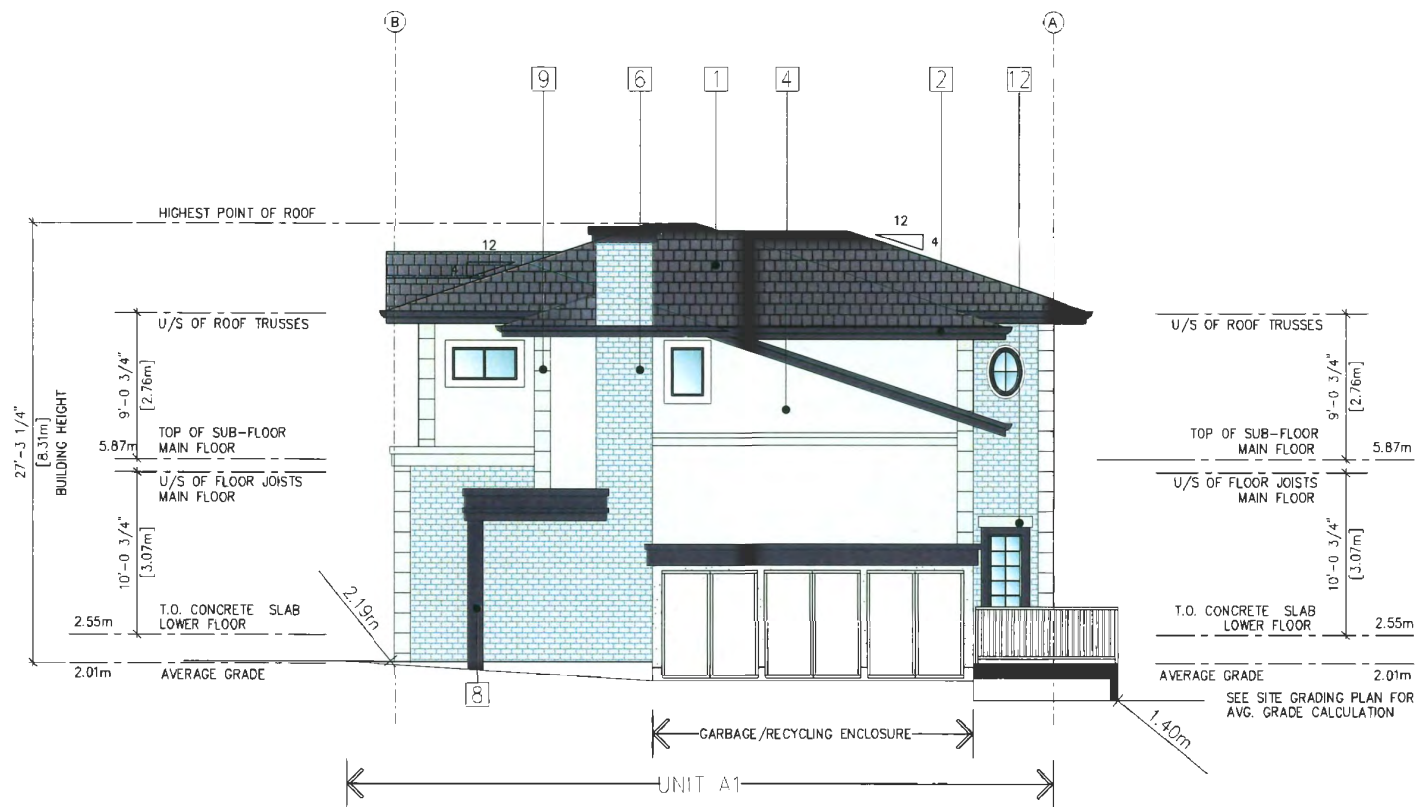


KEY PLAN

2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-26	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP DP17-790086
2022-06-20	A	DP RESUBMISSION

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



2 NORTH ELEVATION
A-301 SCALE 3/16" = 1'-0"



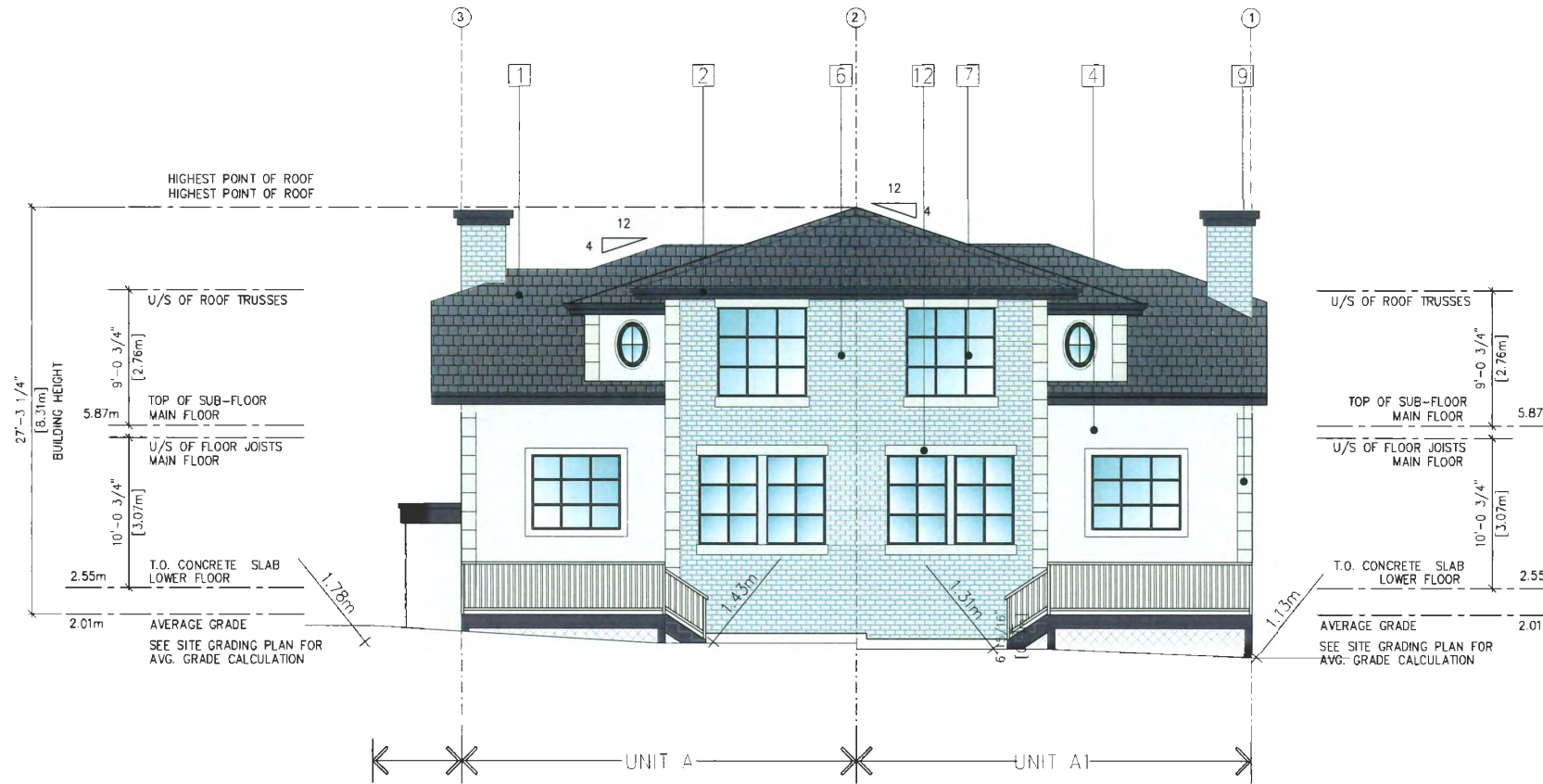
PRODUCT
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT
SATNAM JOHAL

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DRAWN	JA
CHECKED	JA
SCALE	3/16" = 1'-0"
JOB NO	REN-033
DATE	SEPT 2023
SHEET TITLE	

BLDG 1 ELEVATIONS



SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-200
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR : PEWTER BRAND : MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS (12" x 12") & HEADER BAND
COLOR : EGGSHELL BRAND : QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR : BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"



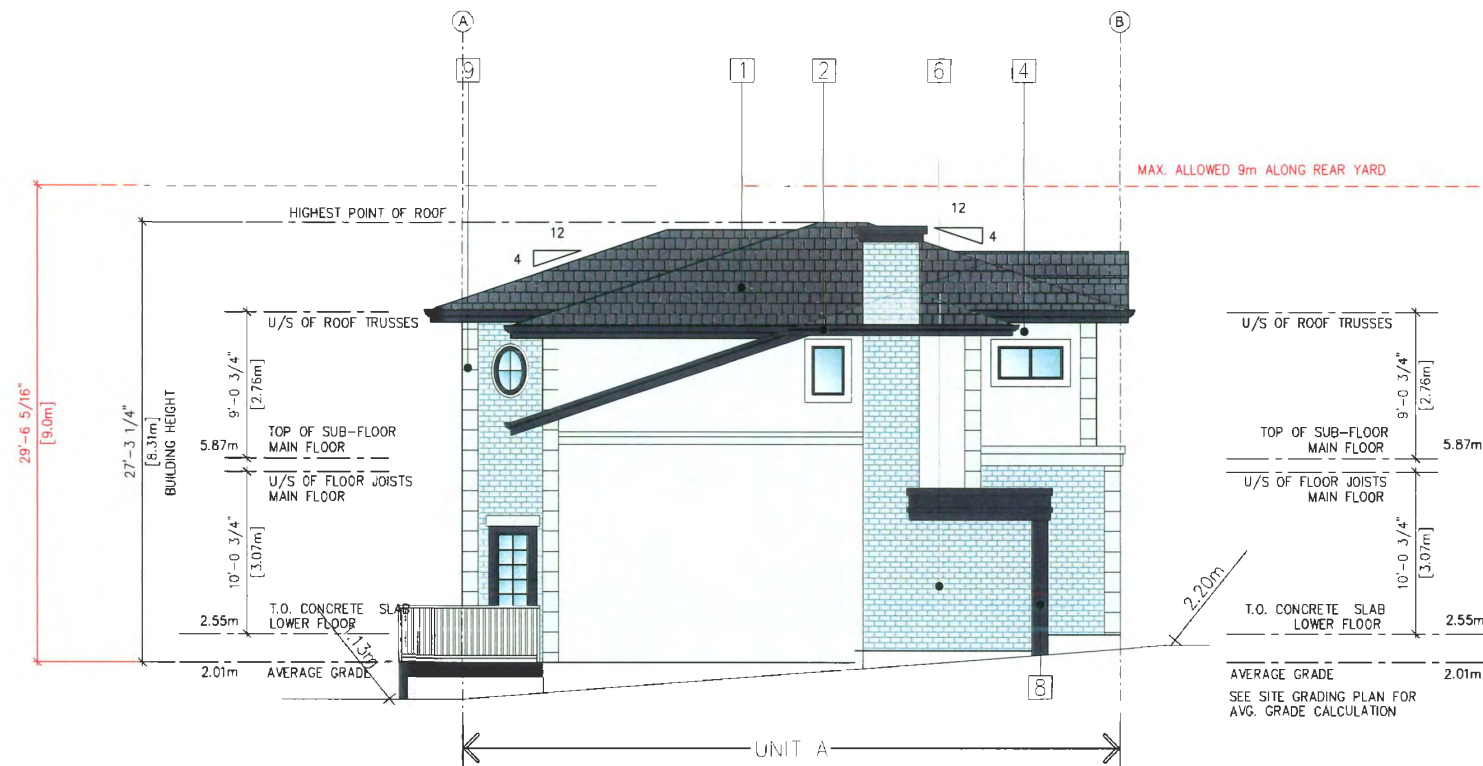
KEY PLAN

2023-10-03	G	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP 17-790086
2022-06-20	A	DP RESUBMISSION

1 WEST ELEVATION
A-302 SCALE: 3/16" = 1'-0"

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



2 SOUTH ELEVATION
A-302 SCALE: 3/16" = 1'-0"



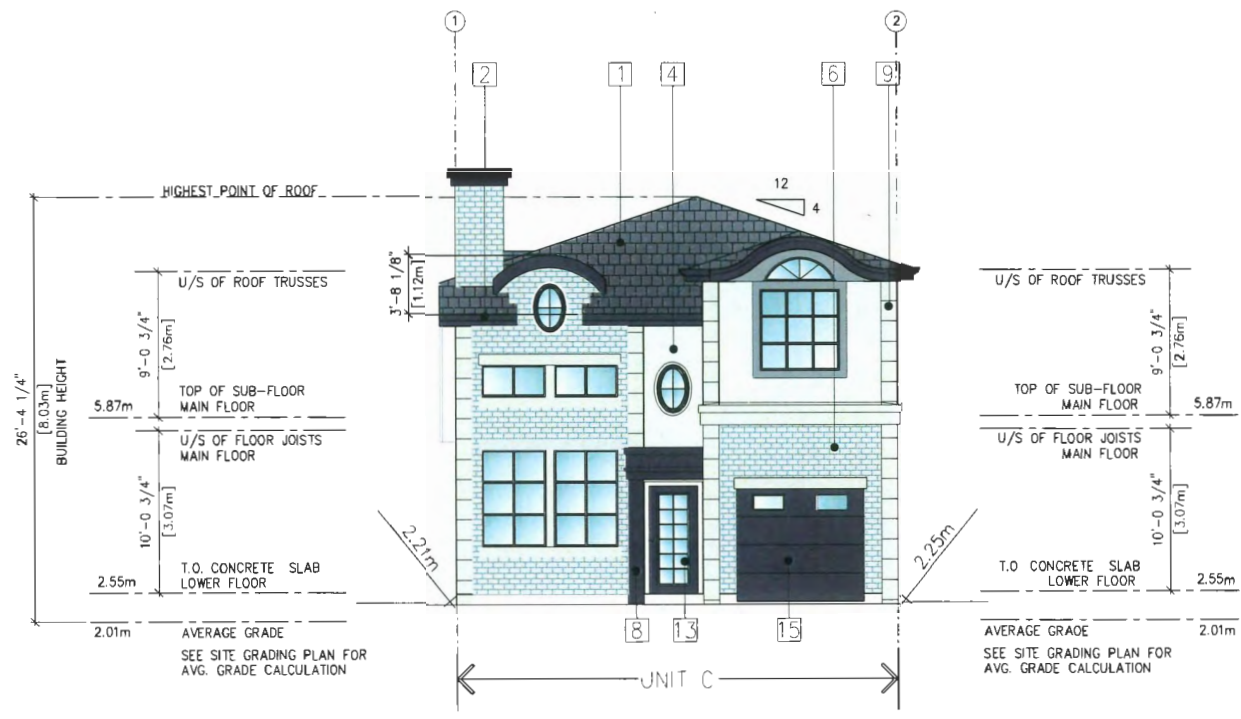
PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DRAWN BY	
CHECKED BY	
SCALE	3/16" = 1'-0"
JOB NO	PCH-033
DATE	SEPT 2023
SHEET TITLE	

BLDG 1 ELEVATIONS



1 EAST ELEVATION
SCALE 3/16" = 1'-0"

SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR : PEWTER BRAND : MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS (12" X 12") & HEADER BAND
COLOR : EGGSHELL BRAND : QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR : BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"

NOTES

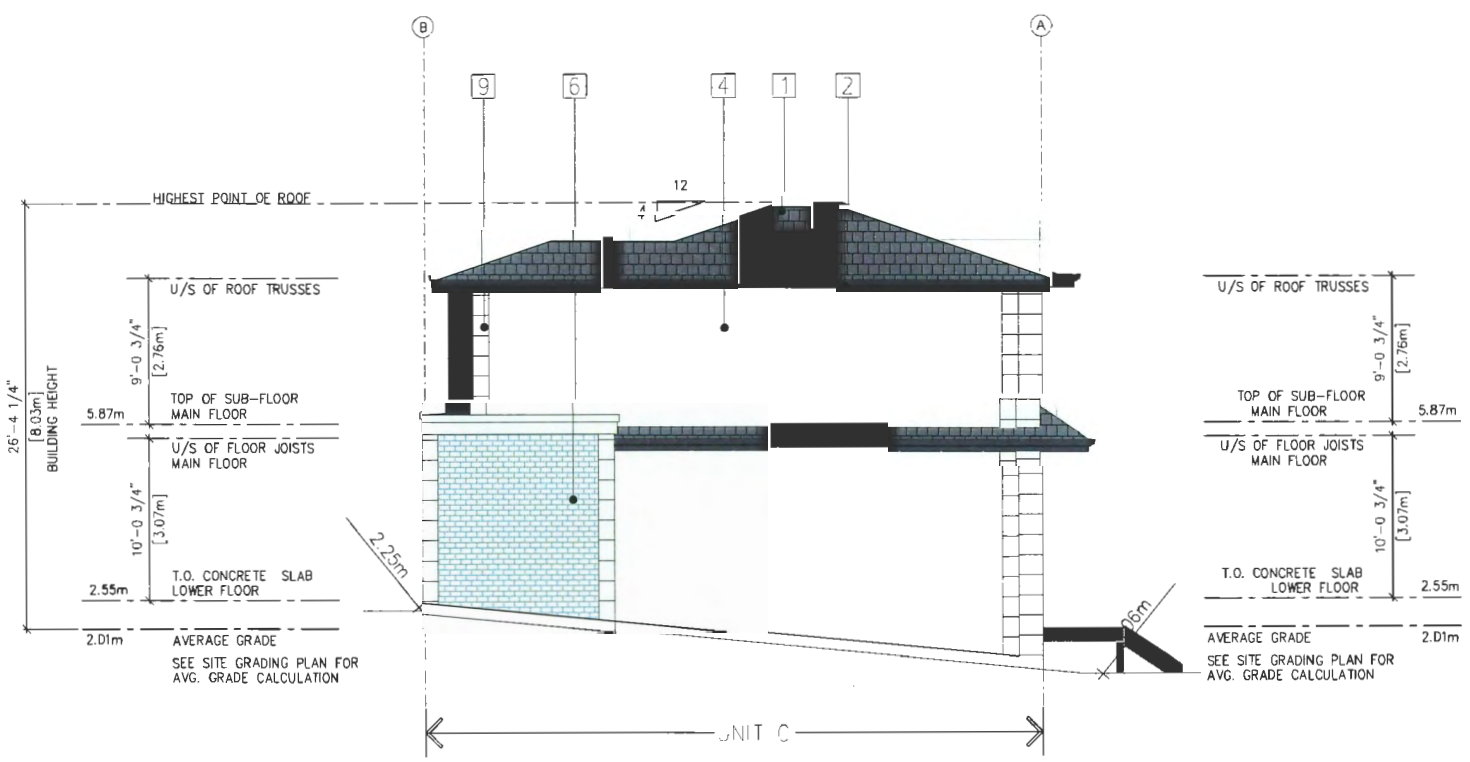


KEY PLAN

2023-10-03	C	OP RESUBMISSION
2023-09-12	T	OP RESUBMISSION
2023-07-28	E	OP RESUBMISSION
2023-03-27	D	OP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_OP17-790086
2022-06-20	A	OP RESUBMISSION

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
	AVERAGE SITE GRADE	2.01m



2 NORTH ELEVATION
SCALE 3/16" = 1'-0"



RICHMOND BR
CANADA
504-284-5194

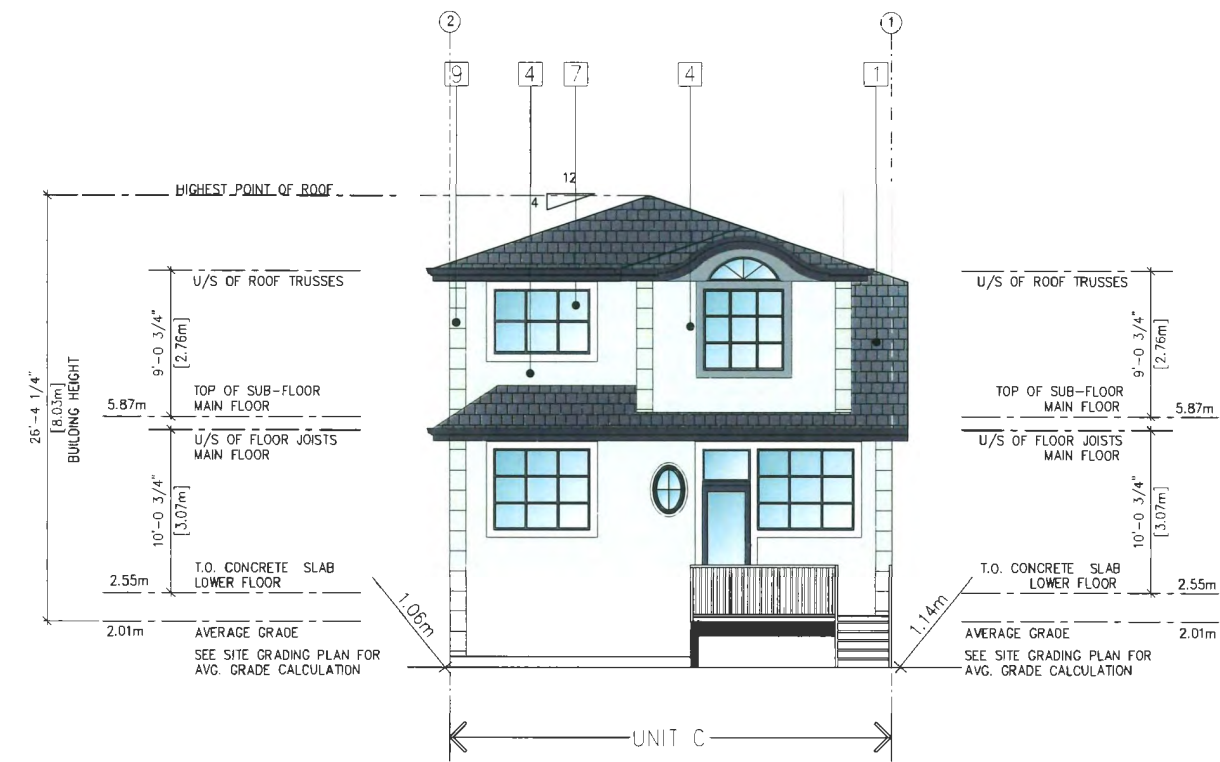
PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DRWEN	AA
CHECKED	FA
SCALE	3/16" = 1'-0"
JOB NO	924-033
DATE	SEPT 2023
SHEET TITLE	

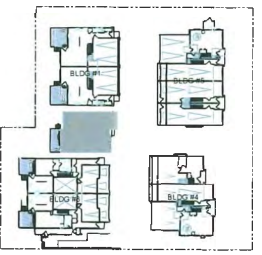
BLDG 2 ELEVATIONS



1 WEST ELEVATION
A-304 SCALE 3/16" = 1'-0"

SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR : PENTEX BRAND : MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS (12" X 12") & HEADER BAND
COLOR : EGGSHELL BRAND : QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR : BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"



KEY PLAN

2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-02-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_DP17-790086
2022-08-20	A	DP RESUBMISSION



PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

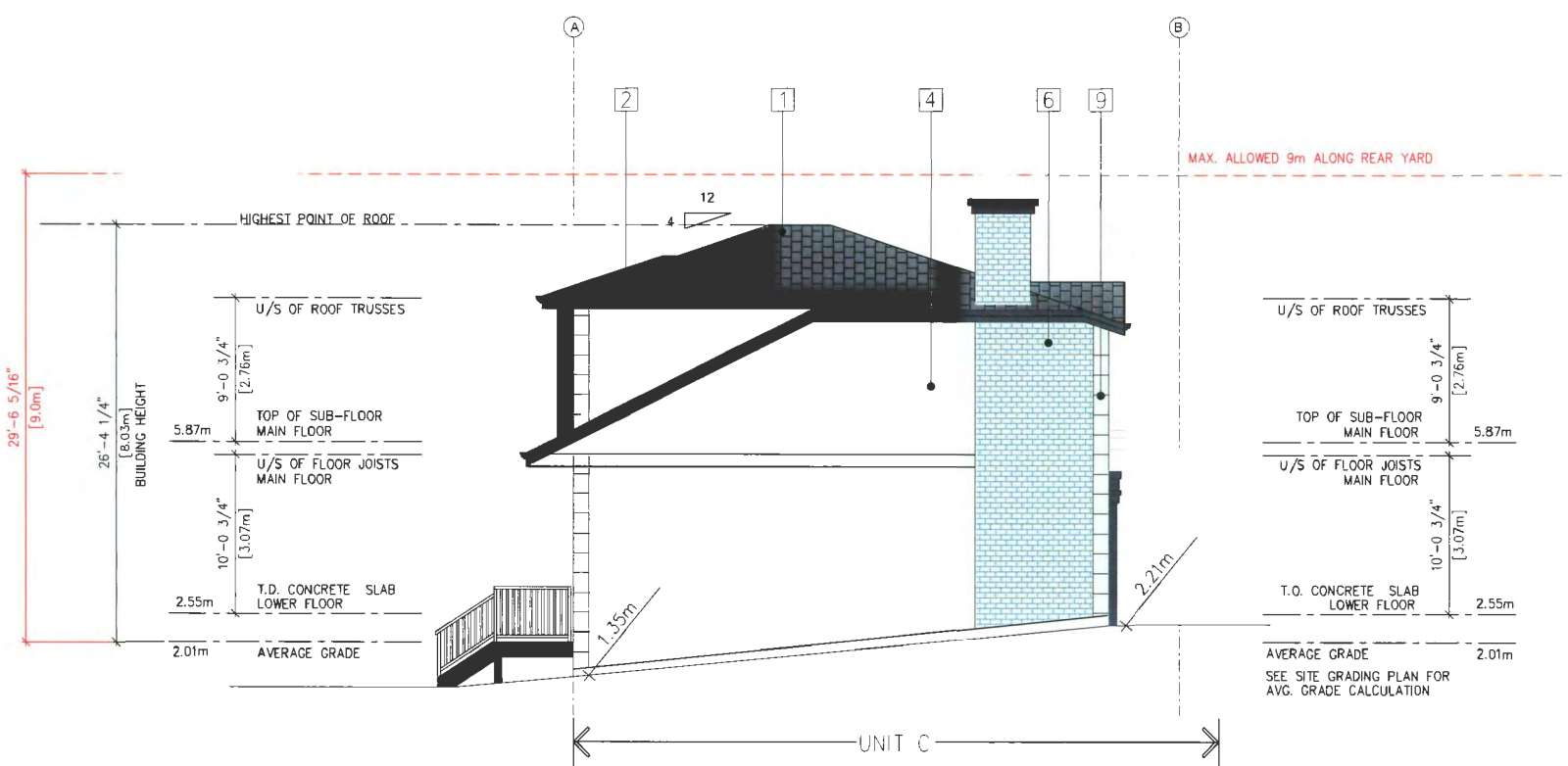
CLIENT:
SATNAM JOHAL

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DRAWN:	BA
CHECKED:	JA
SCALE:	3/16" = 1'-0"
JOB NO. / REV:	053
DATE:	SEPT 2023
SHEET TITLE:	

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
	AVERAGE SITE GRADE	2.01m



2 SOUTH ELEVATION
A-304 SCALE 3/16" = 1'-0"

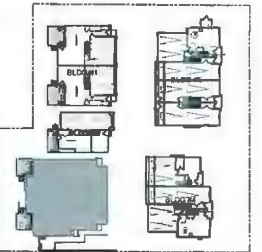
BLDG 2 ELEVATIONS

NOTES:



SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE MC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR : PEWEE BRAND : MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS (12" X 12") & HEADER BAND
COLOR : EGGSHELL BRAND : QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR : BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3"-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"



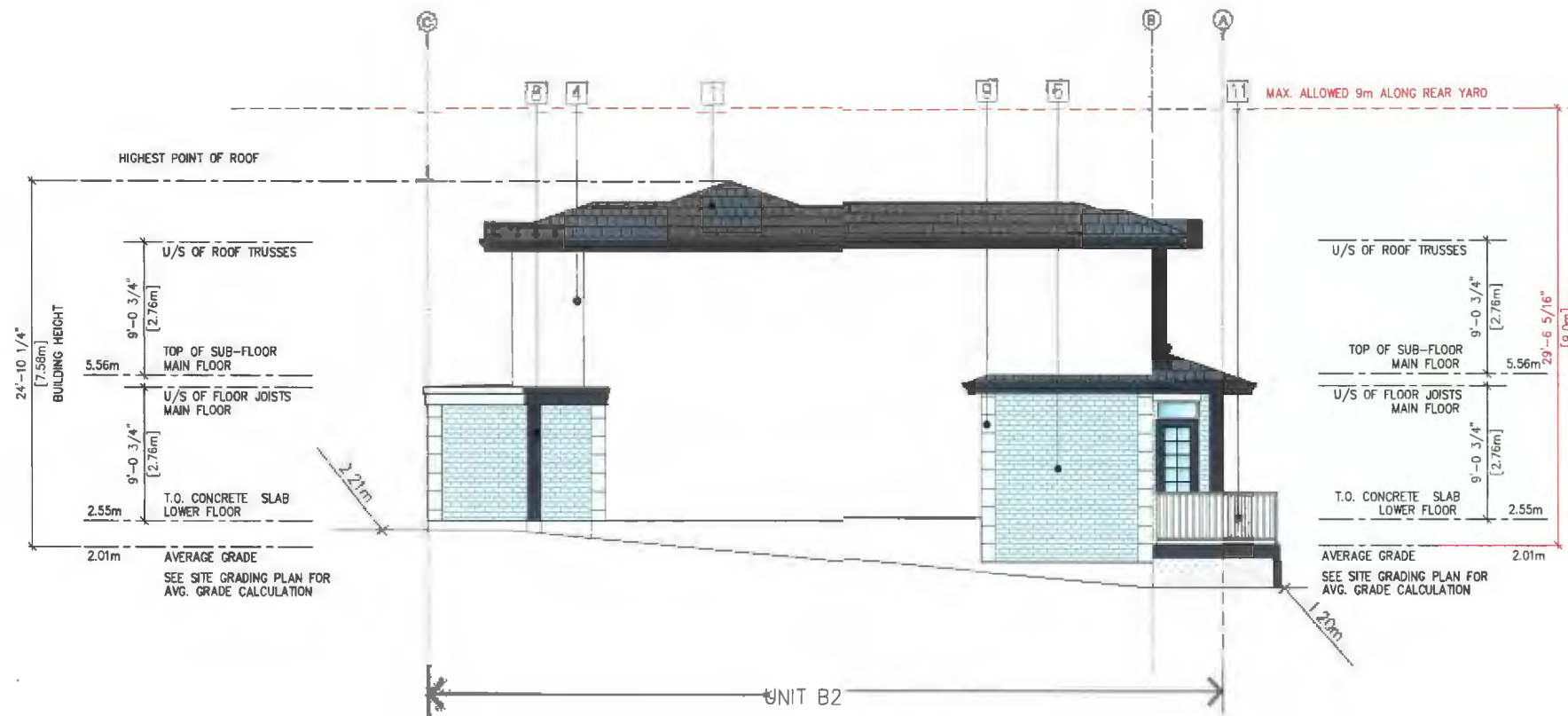
KEY PLAN

2023-10-03	G	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-26	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_DP17-790086
2022-06-20	A	DP RESUBMISSION

1 EAST ELEVATION
A-305
SCALE : 3/16" = 1'-0"

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



2 NORTH ELEVATION
A-305
SCALE : 3/16" = 1'-0"



10851 SHELLBRIDGE
RICHMOND BRITISH COLUMBIA
CANADA BC V6V 2G9
T 604-284-5194 F 604-284-5131
df inc

PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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OWNER: SA
CHECKED: YA
SCALE: 3/16" = 1'-0"
JOB NO.: R04-053
DATE: SEPT 2023
SHEET TITLE:

BLDG 3 ELEVATIONS

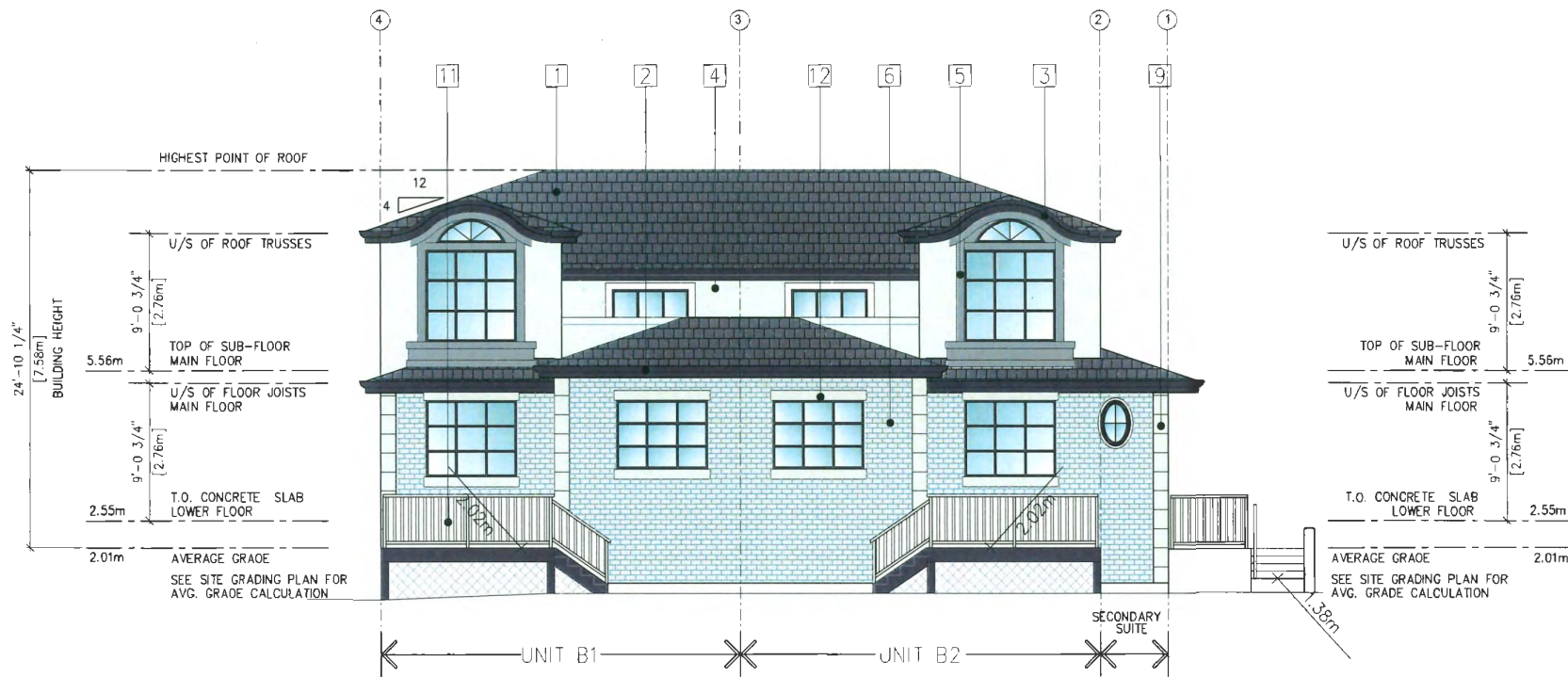
DRAWING NO.:	G
A-305	

DP 17-790086

PLAN #25

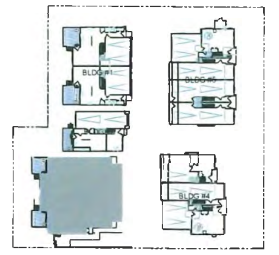
October 4, 2023

NOTES:



SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR : PEWTER BRAND : MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS (12" X 12") & HEADER BAND
COLOR : EGGSHELL BRAND : QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR : BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"



KEY PLAN

2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-25	E	DP RESUBMISSION
2022-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER AOP COMMENTS
2022-10-06	B	SUBMISSION FOR AOP DP17-790086
2022-06-20	A	DP RESUBMISSION

1 WEST ELEVATION
A-306 SCALE 3/16" = 1'-0"



PROJECT
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

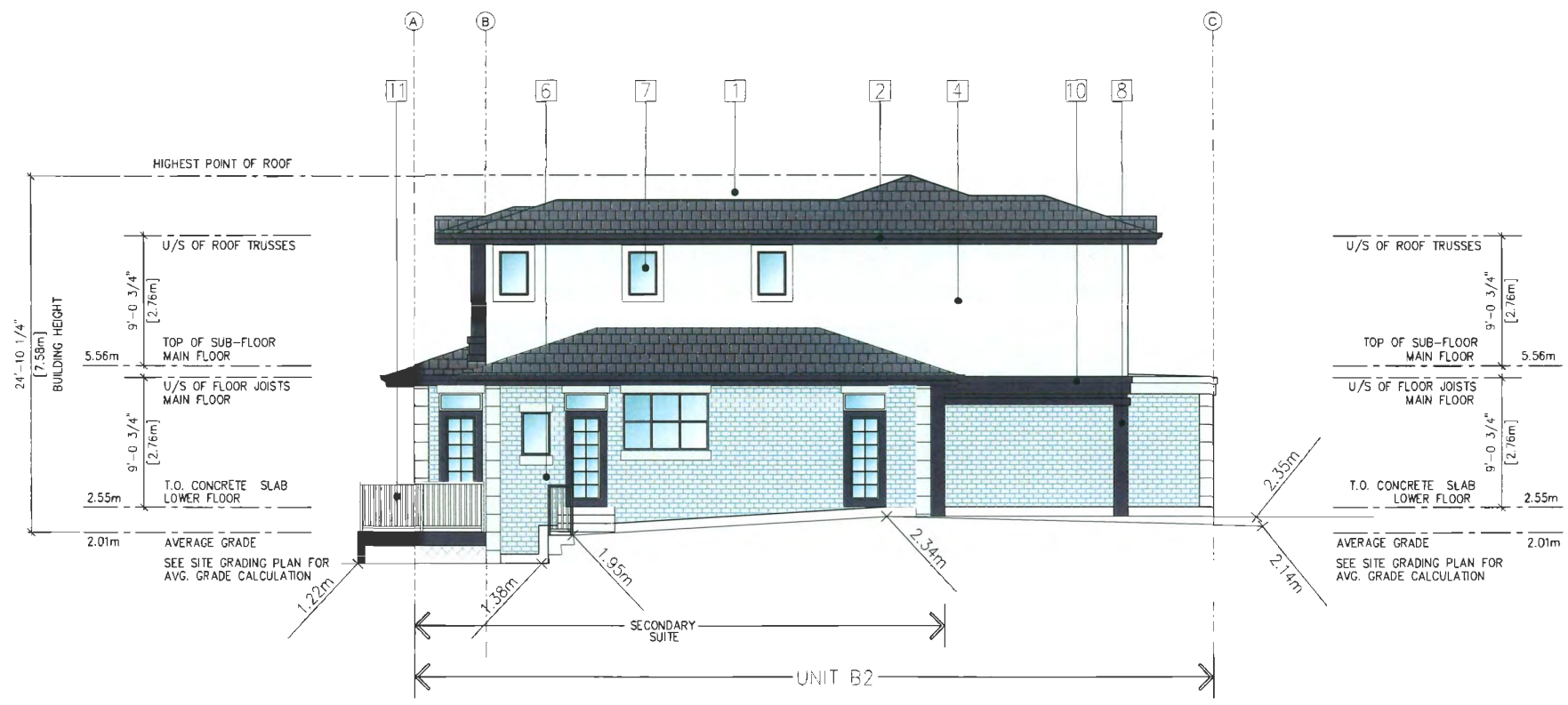
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DRAWING NO.	
CHECKED BY	
SCALE	3/16" = 1'-0"
JOB NO - RCH 033	
DATE	SEPT 2023
SHEET TITLE	

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



1 SOUTH ELEVATION
A-306 SCALE 3/16" = 1'-0"

BLDG 3 ELEVATIONS



SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE 14-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR: FEWTER BRAND: MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUIN CORNERS (12" X 12") & HEADER BAND
COLOR: EGGSHELL BRAND: QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR: BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3"-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"



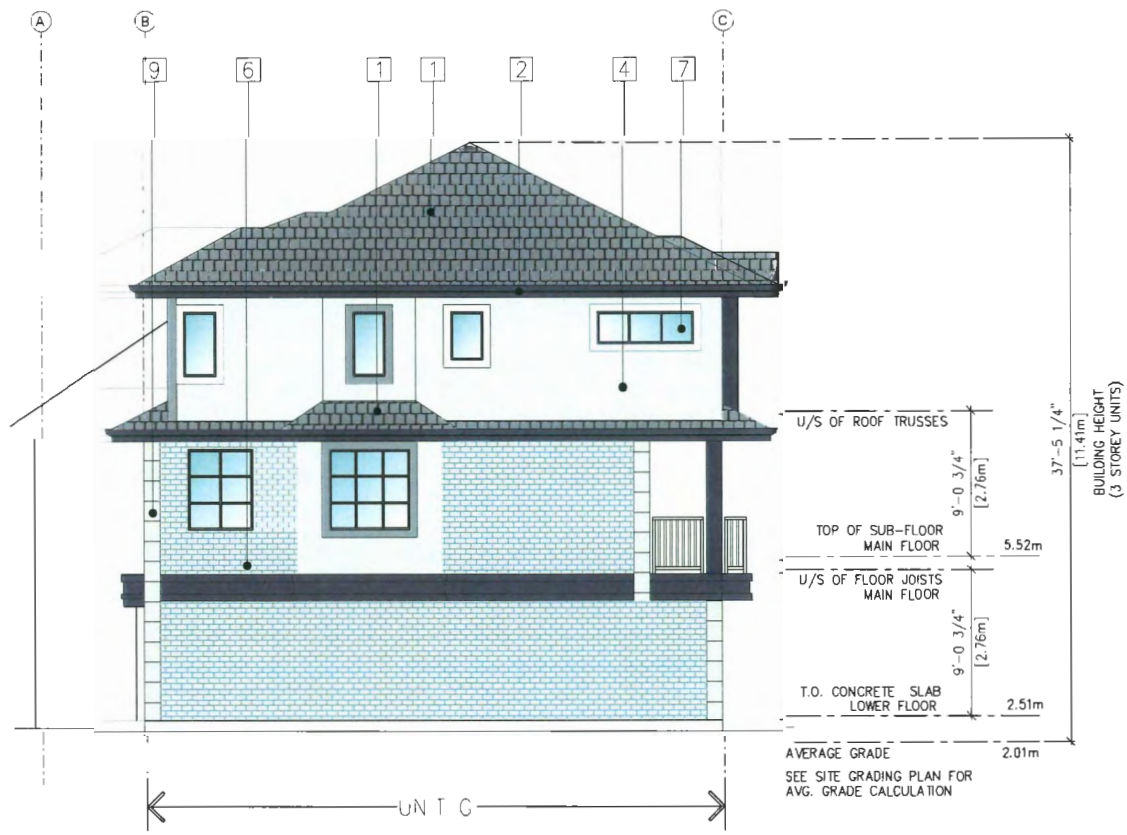
KEY PLAN

REVISION	DATE	BY	DESCRIPTION
1	2022-10-03	DF	DP RESUBMISSION FOR DPP
2	2023-09-12	DF	DP RESUBMISSION
3	2023-07-28	DF	DP RESUBMISSION
4	2023-03-27	DF	DP RESUBMISSION
5	2022-11-09	C	RESUBMISSION AFTER AEP COMMENTS
6	2022-10-06	H	SUBMISSION TOP ADP_DP17-790086
7	2022-06-29	A	DP RESUBMISSION

1 WEST ELEVATION
A-307 SCALE 3/16" = 1'-0"

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



2 NORTH ELEVATION
A-307 SCALE 3/16" = 1'-0"



PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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CHECKED BY	
SCALE	3/16" = 1'-0"
JOB NO.	REC-033
DATE	OCT 2023
SHEET TITLE	

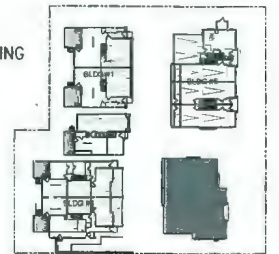
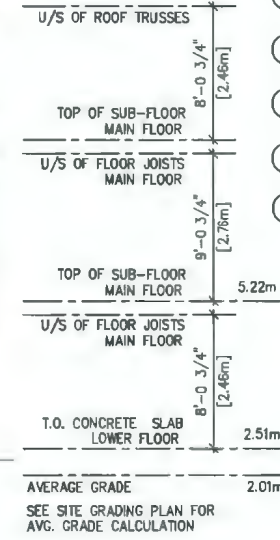
BLDG 4 ELEVATIONS

NOTES:



SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR: PEWTER BRAND: MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS (12" X 12") & HEADER BAND
COLOR: EGGSHELL BRAND: QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR: BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"



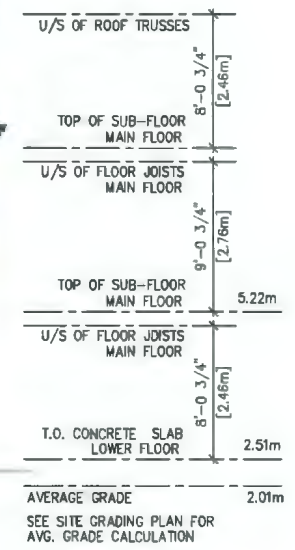
KEY PLAN

2023-10-03	G	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_BP17-790086
2022-06-20	A	DP RESUBMISSION

1 EAST ELEVATION
A-308 SCALE: 3/16" = 1'-0"

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



2 SOUTH ELEVATION
A 308 SCALE: 3/16" = 1'-0"



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PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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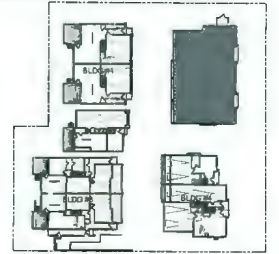
DRAWN: YA
CHECKED: YA
SCALE: 3/16" = 1'-0"
JOB NO.: RCH-053
DATE: SEPT 2023
SHEET TITLE:

BLDG 4 ELEVATIONS

NOTES:

SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR: PEWTER BRAND: MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PRE-FINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑨ HEWN QUOIN CORNERS-(12" X 12") & HEADER BAND
COLOR: EGGSHELL BRAND: QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR: BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR -- SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR -- PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR -- PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"



KEY PLAN

2023-10-03	C	DP RESUBMISSION
2023-09-11	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2023-11-08	C	RESUBMISSION AFTER ADP COMMENTS
2021-10-08	B	SUBMISSION FOR ADP_DP17-790086
2021-06-20	A	DP RESUBMISSION



350-10851 SHELLBRIDGE
RICHMOND BRITISH COLUMBIA
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PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DRAWN: WA
CHECKED: WA
SCALE: 3/16" = 1'-0"
JOB No.: R04-033
DATE: SEPT 2023
SHEET TITLE:

BLDG 5 ELEVATIONS

DRAWING NO.:
A-309
G



1 WEST ELEVATION
A-309 SCALE: 3/16" = 1'-0"

AVERAGE GRADE CALCULATION

	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



2 NORTH ELEVATION
A-309 SCALE: 3/16" = 1'-0"

DP 17-790086

PLAN #29

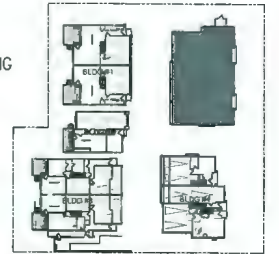
October 4, 2023



SCHEDULE OF FINISHES

- ① ASPHALT SHINGLE ROOFING
- ② PREFINISHED ALUMINUM GUTTER ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ③ 1x4 WOOD TRIM ON 2x10 WOOD FASCIA
BENJAMIN MOORE 2128-20 "ABYSS"
- ④ STUCCO SIDING
BENJAMIN MOORE HC-170 "STONINGTON"
- ⑤ HARDIE-PANEL SIDING WITH BATTENS
BENJAMIN MOORE STORM AF-700
- ⑥ FACE BRICK (3-1/2" x 7-1/2")
COLOR : PEWTER BRAND : MUTUAL MATERIALS
- ⑦ VINYL FRAMED WINDOWS WITH 2x6 HARDIE TRIM
PREFINISHED BLACK VINYL FRAME
- ⑧ 12x12 BUILT-UP COLUMN W/ HARDIE PANEL SIDING
BENJAMIN MOORE 2128-20 "ABYSS"

- ⑨ HEWN QUOIN CORNERS (12" X 12") & HEADER BAND
COLOR : EGGSHELL BRAND : QUALITY STONE VENEER INC
- ⑩ 2X4, 2X6 & 2X10 HORIZONTAL AHRDIE TRIMS
COLOR : BENJAMIN MOORE 2128-20 "ABYSS"
- ⑪ 3'-6" HIGH ALUMINUM PICKET GUARDRAIL
PRE-FINISHED BLACK
- ⑫ 8" CONCRETE HEADER & SILL
- ⑬ ENTRY DOOR - SOLID CORE METAL WITH TEMPERED GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑭ SERVICE DOOR - PAINTED METAL
BENJAMIN MOORE 2128-20 "ABYSS"
- ⑮ GARAGE DOOR - PAINTED METAL WITH GLAZING
BENJAMIN MOORE 2128-20 "ABYSS"

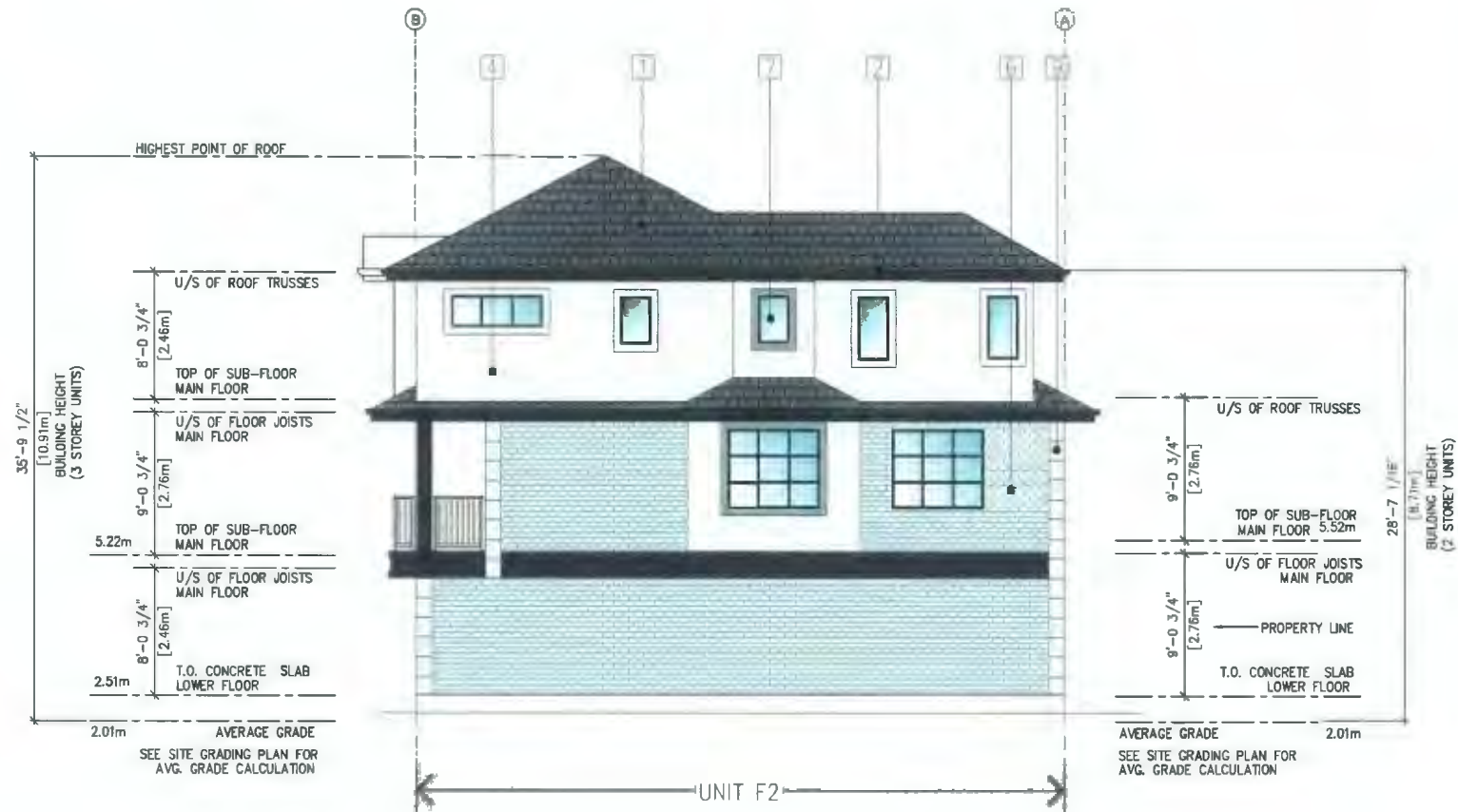


KEY PLAN

NO.	DATE	BY	DESCRIPTION
2023-10-03	G	DP	RESUBMISSION
2023-09-12	F	DP	RESUBMISSION
2023-07-28	E	DP	RESUBMISSION
2023-03-27	D	DP	RESUBMISSION
2023-11-08	C		RESUBMISSION AFTER ADP COMMENTS
2023-10-06	B		SUBMISSION FOR ADP_0P17-790086
2022-08-20	A	DP	RESUBMISSION

1 EAST ELEVATION
A-310
SCALE: 3/16" = 1'-0"

AVERAGE GRADE CALCULATION		
	CORNER GRADES	AVG
BUILDING #1:	NW 2.02m, NE 2.18, SE 2.23m, SW 2.02m	2.11m
BUILDING #2:	NW 1.06m, NE 2.25m, SE 2.21m, SW 1.35m	1.72m
BUILDING #3:	NW 1.20m, NE 2.24m, SE 2.25m, SW 1.36m	1.76m
BUILDING #4:	NW 2.18m, NE 2.39m, SE 2.44m, SW 2.20m	2.30m
BUILDING #5:	NW 2.21m, NE 2.36m, SE 2.43m, SW 2.21m	2.30m
SITE	NW 1.67m, NE 2.29m, SE 2.38m, SW 1.09m	1.86m
AVERAGE SITE GRADE		2.01m



2 SOUTH ELEVATION
A-310
SCALE: 3/16" = 1'-0"



350-10851 SHELLBRIDGE
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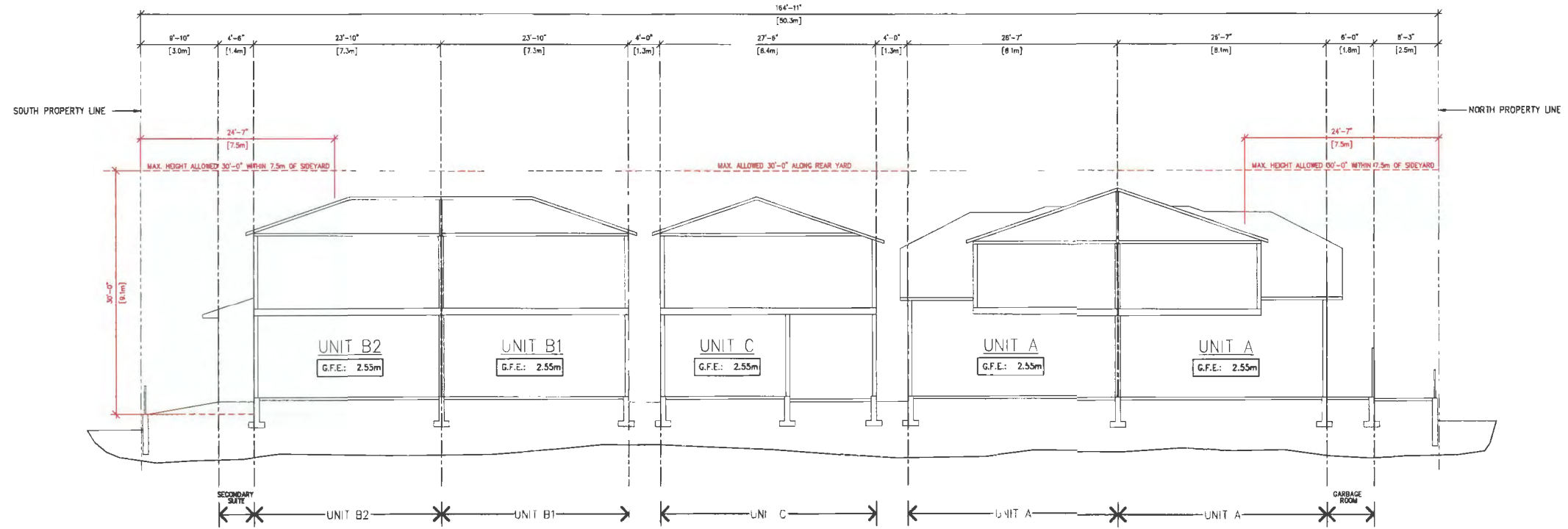
PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

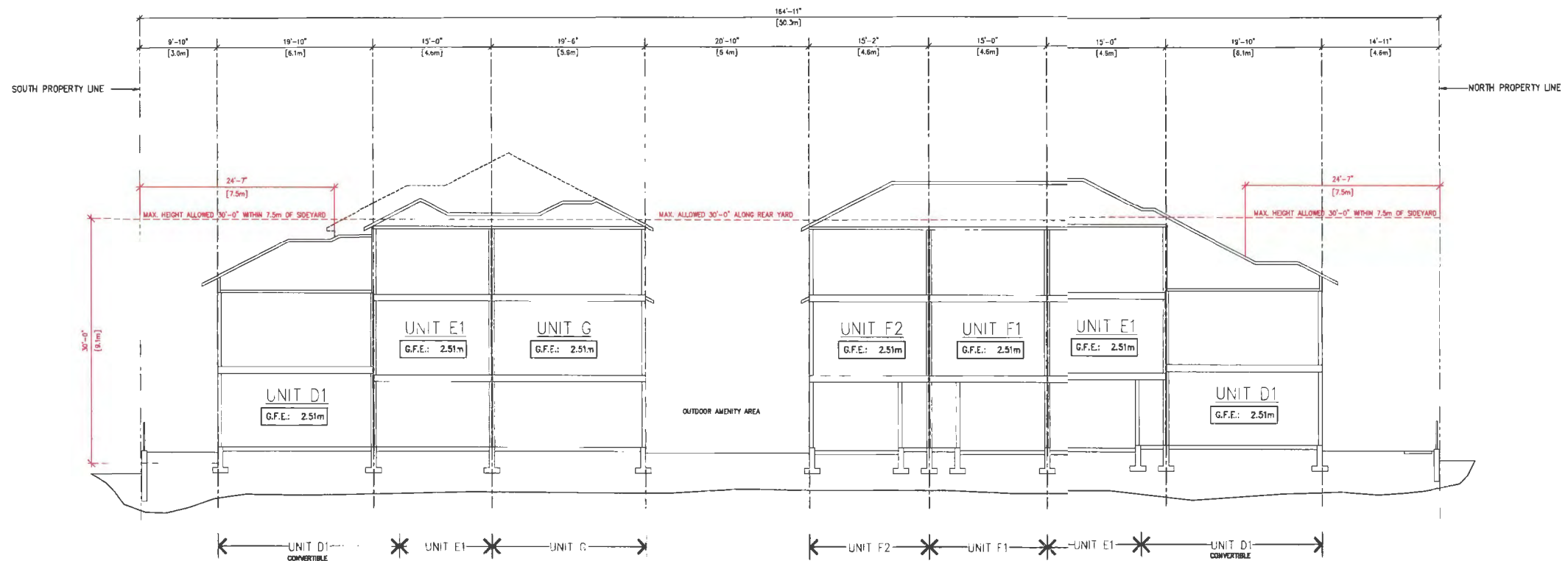
DRAWING NO.	
CHECKED BY	
SCALE	3/16" = 1'-0"
JOB NO.	RDH-033
DATE	SEPT 2023
SHEET TITLE	

BLDG 5 ELEVATIONS

NOTES:



1 SECTION A-A
A-401 SCALE: 1/8" = 1'-0"



2 SECTION B-B
A-401 SCALE: 1/8" = 1'-0"

NO.	DATE	DESCRIPTION
2023-10-03	G	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_DP17-790086
2022-06-20	A	DP RESUBMISSION



PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
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DRAWN: WA
CHECKED: VA
SCALE: 1/8" = 1'-0"
JOB NO: RCH-033
DATE: SEPT 2023
SHEET TITLE:

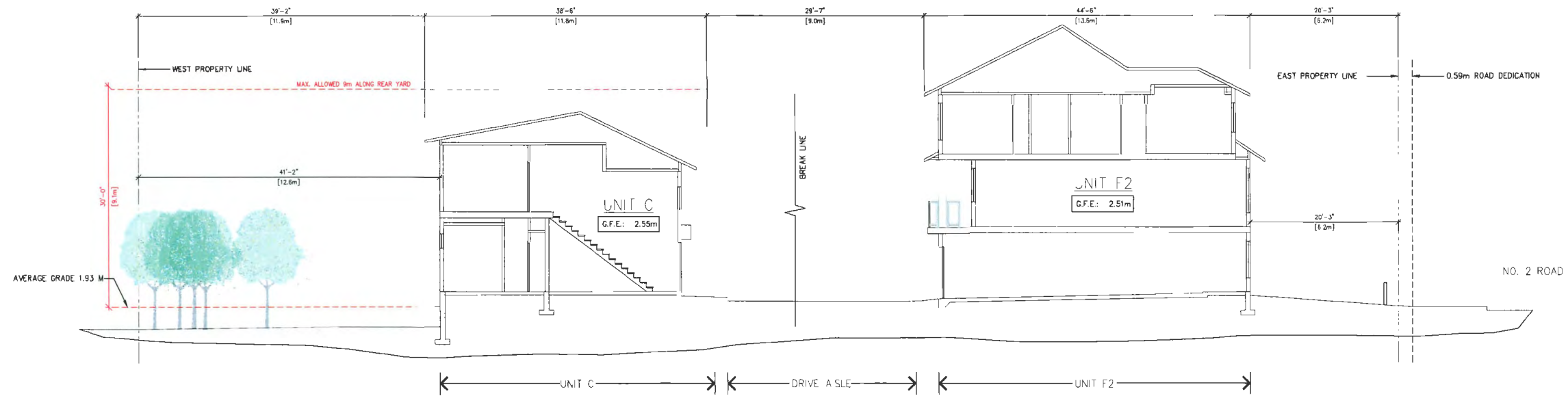
SITE SECTION

DP 17-790086

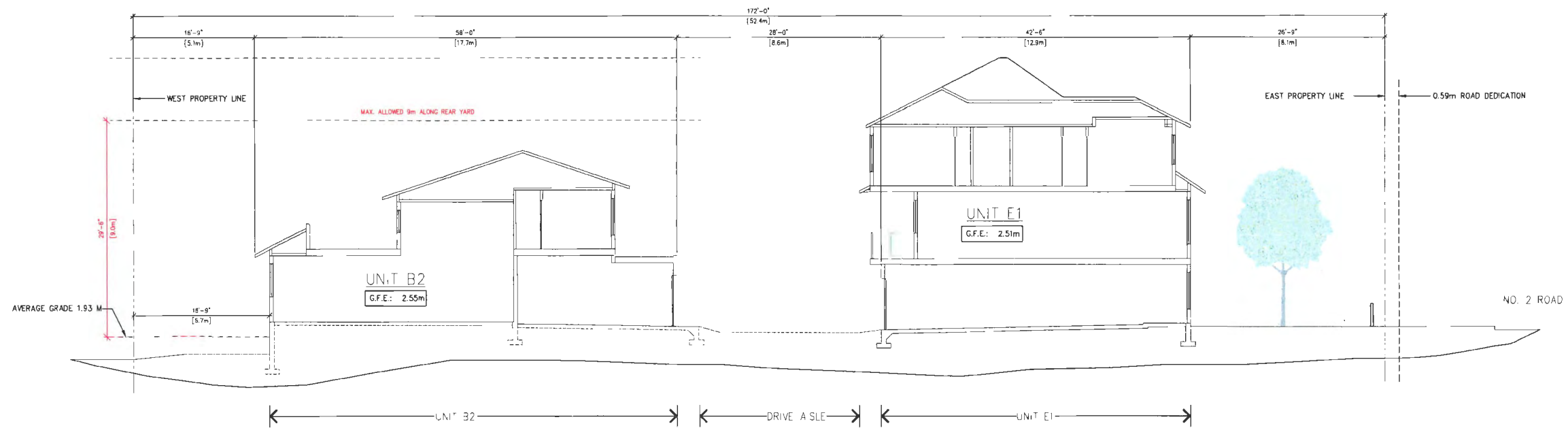
PLAN #31

October 4, 2023

DRAWING NO:	A-401	G
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1 SECTION C-C
A-402 SCALE 1/8" = 1'-0"



2 SECTION D-D
A-402 SCALE 1/8" = 1'-0"

2023-10-03	G	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	F	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-08	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-08	W	SUBMISSION FOR ADP OP17-790086
2022-06-20	A	DP RESUBMISSION



10851 SHELLBRIDGE WAY
RICHMOND, BRITISH COLUMBIA
CANADA V6X 2W5
(4284)5194 F (604)284-5131

PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DESIGN	WA
CHECKED	WA
SCALE	1/8" = 1'-0"
JOB NO	RDH-032
DATE	SEPT 2023
SHEET TITLE	

SITE SECTION

DRAWING NO.	A-402	G
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NOTES:



1 EAST STEETScape (NO. 2 RD)
A-501 SCALE: 3/16" = 1'-0"



2 EAST STEETScape (DRIVE AISLE)
A-501 SCALE: 3/16" = 1'-0"

2023-10-03	G	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP_DP17-790086
2022-08-20	A	DP RESUBMISSION



350-10851 SHELLBRIDGE
RICHMOND BRITISH
CANADA V6V 2W9
T 284-5194 F (604)284-
inf df 1 1/2 1/2

PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT:
SATNAM JOHAL

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DRAWN: WA
CHECKED: YA
SCALE: 3/16" = 1'-0"
JOB NO. RD-033
DATE: SEPT 2023
SHEET TITLE:

STREETSCAPE

DP 17-790086

PLAN #33

October 4, 2023

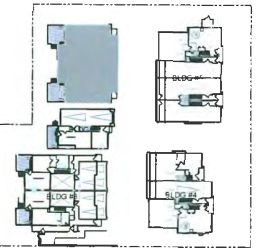
DRAWING NO.:
A-501
G

NOTES

ABOVE IN PLACE FEATURES:

- STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT.
- LEVER HARDWARE ON DOORS
- BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS

NOTE: UNITS WILL INCLUDE PRE-DUCTING FOR SOLAR HEATING. LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



KEY PLAN

2023-10-03	C	DP SUBMISSION
2023-09-12	F	DP SUBMISSION
2023-07-26	F	DP SUBMISSION
2023-03-27	D	DP SUBMISSION
2022-11-09	C	REVISION AFTER ADP COMMENTS
2022-10-08	B	SUBMISSION FOR ADP_DP17-790086
2022-06-20	A	DP SUBMISSION

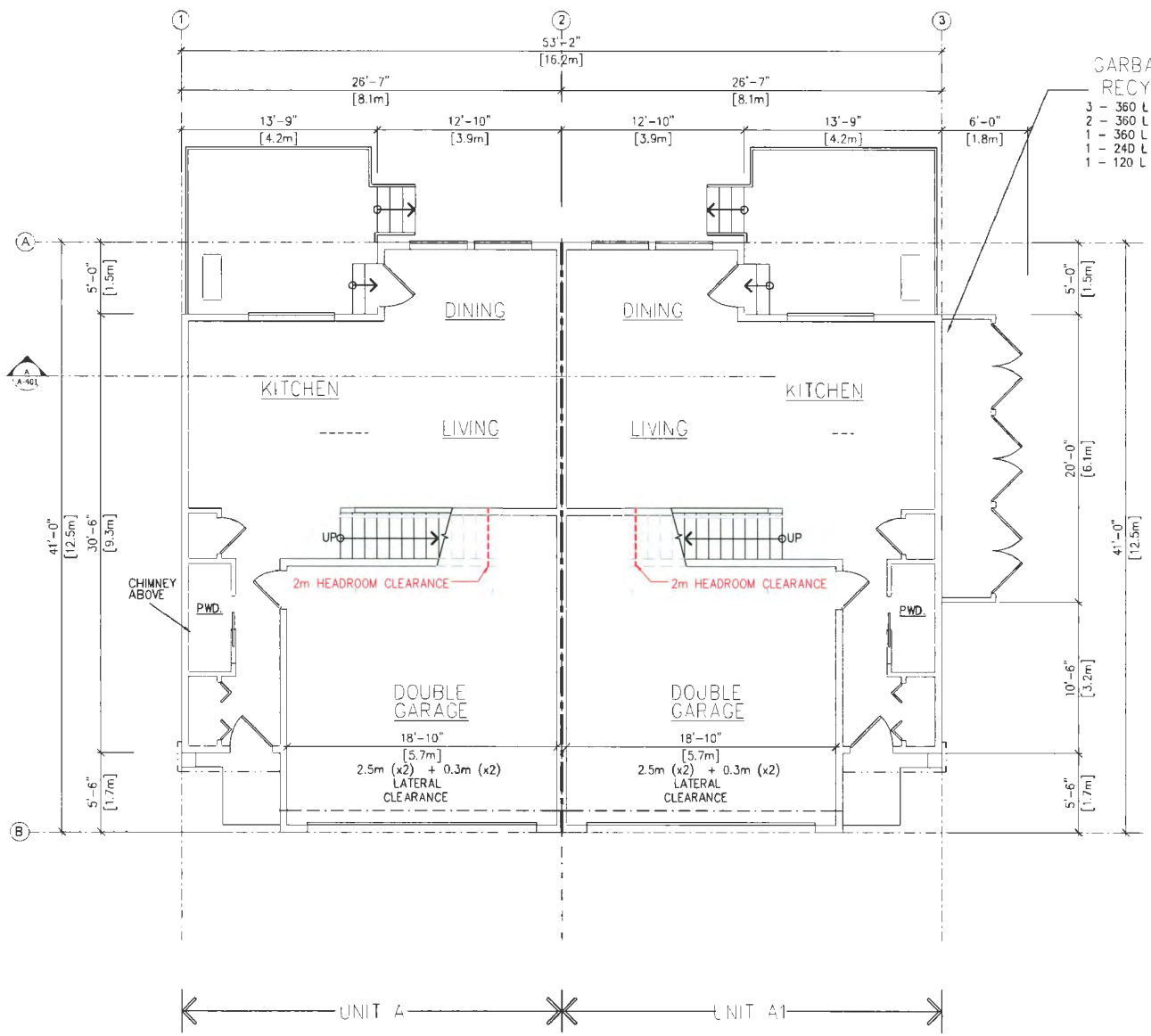


PROJECT
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

CLIENT
SATNAM JOHAL

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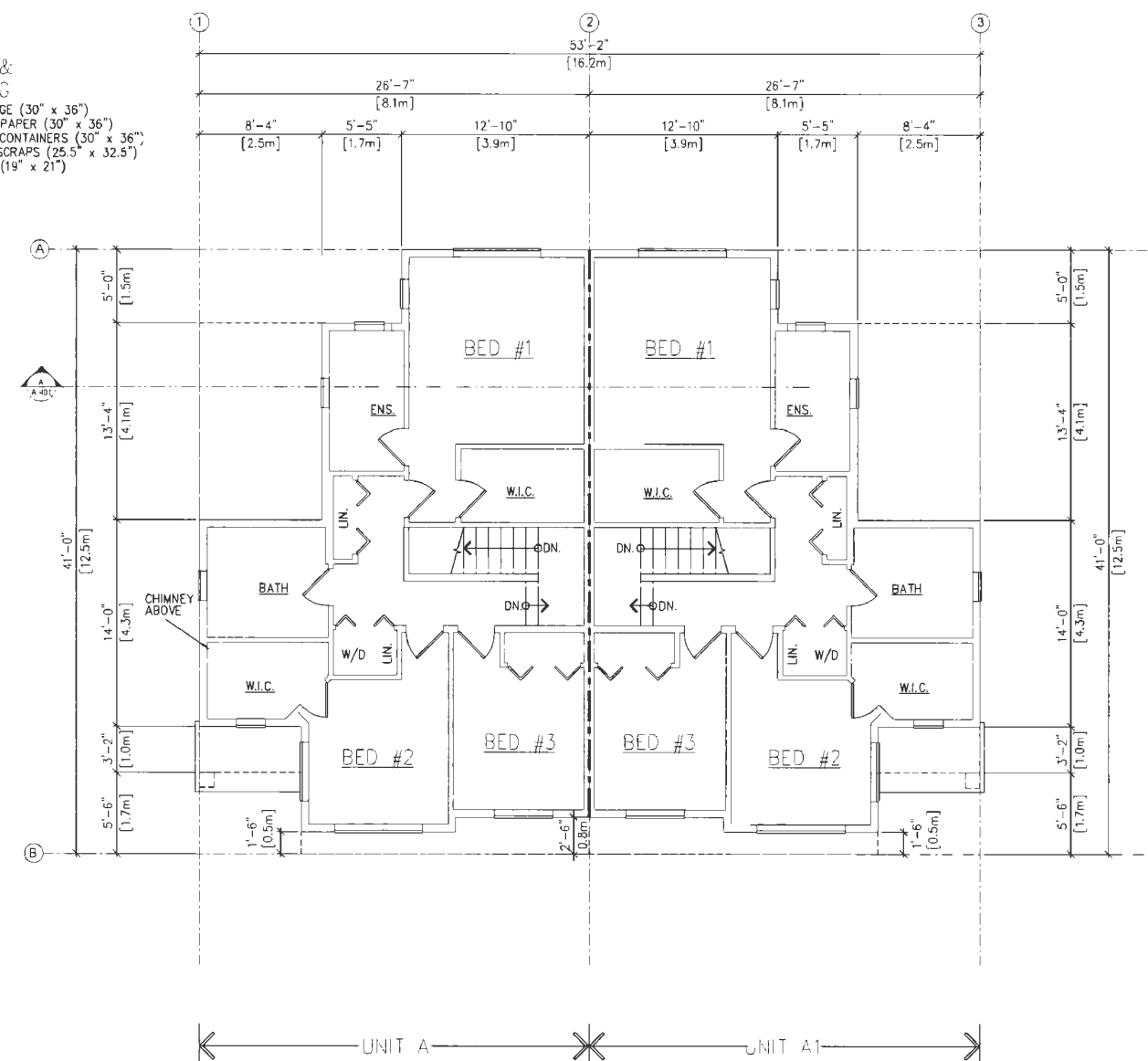
DRAWN	MS
CHECKED	VA
SCALE	3/16" = 1'-0"
JOB NO	PCA-033
DATE	SEPT 2023
SHEET TITLE	



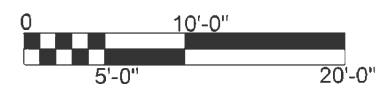
1
 MAIN FLOOR PLAN
 A-201 SCALE 3/16" = 1'-0"

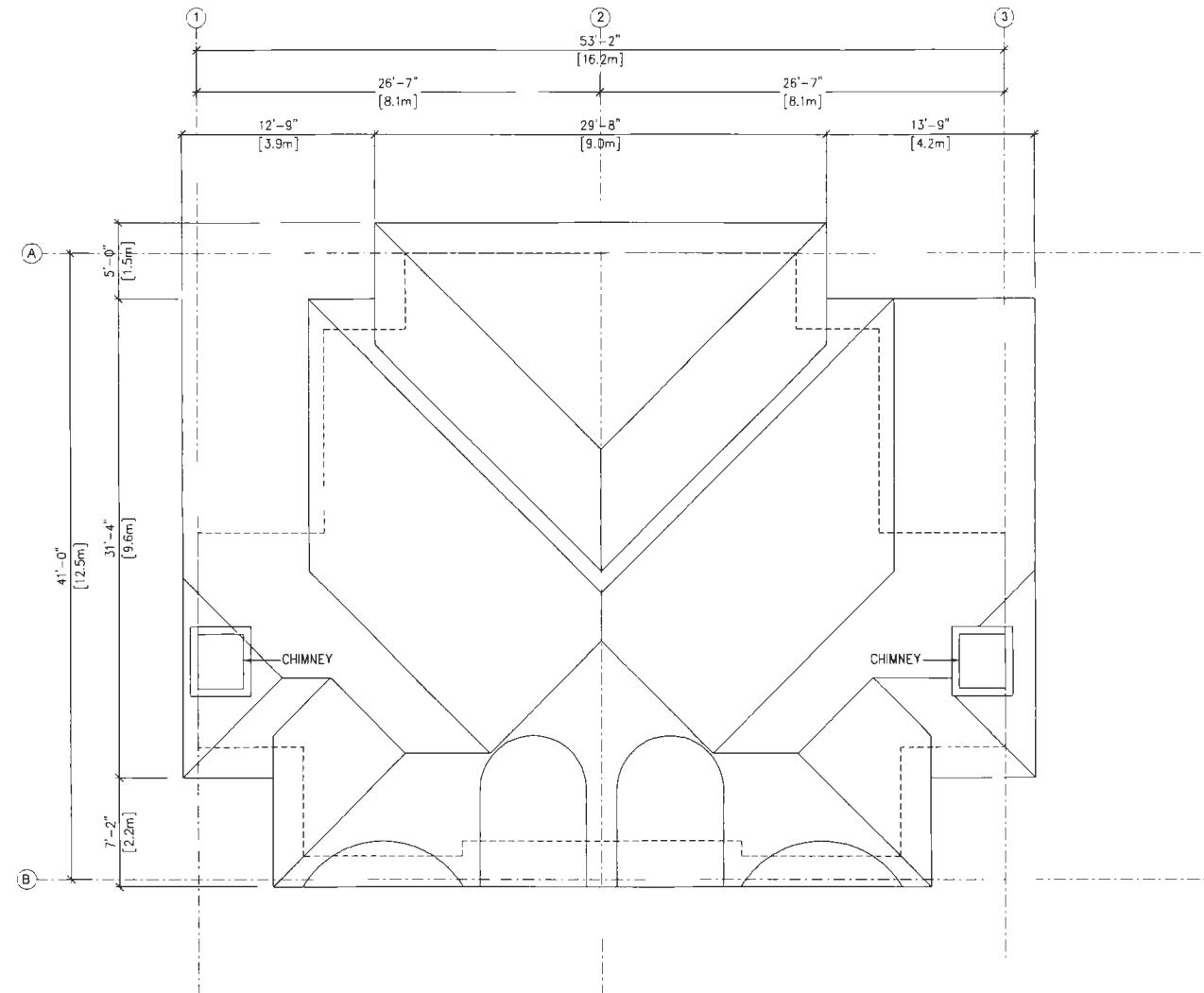
GARBAGE & RECYCLING

- 3 - 360 L GARBAGE (30" x 36")
- 2 - 360 L MIXED PAPER (30" x 36")
- 1 - 360 L MIXED CONTAINERS (30" x 36")
- 1 - 240 L FOOD SCRAPS (25.5" x 32.5")
- 1 - 120 L GLASS (19" x 21")



2
 LEVEL 2 PLAN
 A-201 SCALE 3/16" = 1'-0"

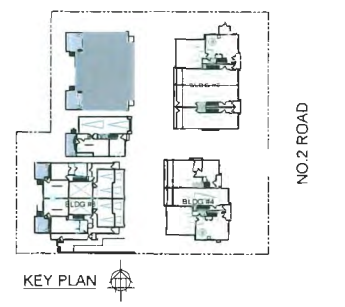




1 ROOF PLAN
A-202 SCALE 3/16" = 1'-0"

NOTES:
 ASING IN PLACE FEATURES:
 - STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT
 - LEVER HARDWARE ON DOORS
 - BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS

NOTE: UNITS WILL INCLUDE PRE-DUCTING FOR SOLAR HEATING. LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



DATE	DESCRIPTION	BY
2023-10-03	DP RESUBMISSION	
2023-09-12	DP RESUBMISSION	
2023-07-26	DP RESUBMISSION	
2023-03-27	DP RESUBMISSION	
2022-11-09	RESUBMISSION AFTER ADP COMMENTS	
2022-10-08	SUBMISSION FOR ADP DP17-790086	
2022-08-20	DP RESUBMISSION	



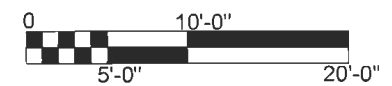
PROJECT
 12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

CLIENT
 SATNAM JOHAL

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DRAWN	MP
CHECKED	VA
SCALE	3/16" = 1'-0"
JOB NO.	PEH-033
DATE	SEPT 2023
SHEET TITLE	

BLDG 1 PLANS



DP 17-790086

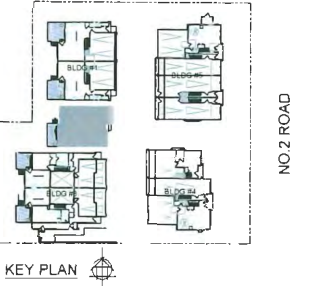
REFERENCE PLAN

October 4, 2023

DRAWING NO.	A-202	G
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NOTES:
ORIGINAL CLASS FEATURES:
 - STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT.
 - LEVER HARDWARE ON DOORS.
 - BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS.

NOTE: UNITS WILL INCLUDE PRE-BUILDING FOR SOLAR HEATING LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	F	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP DP17-790086
2022-06-20	A	DP RESUBMISSION

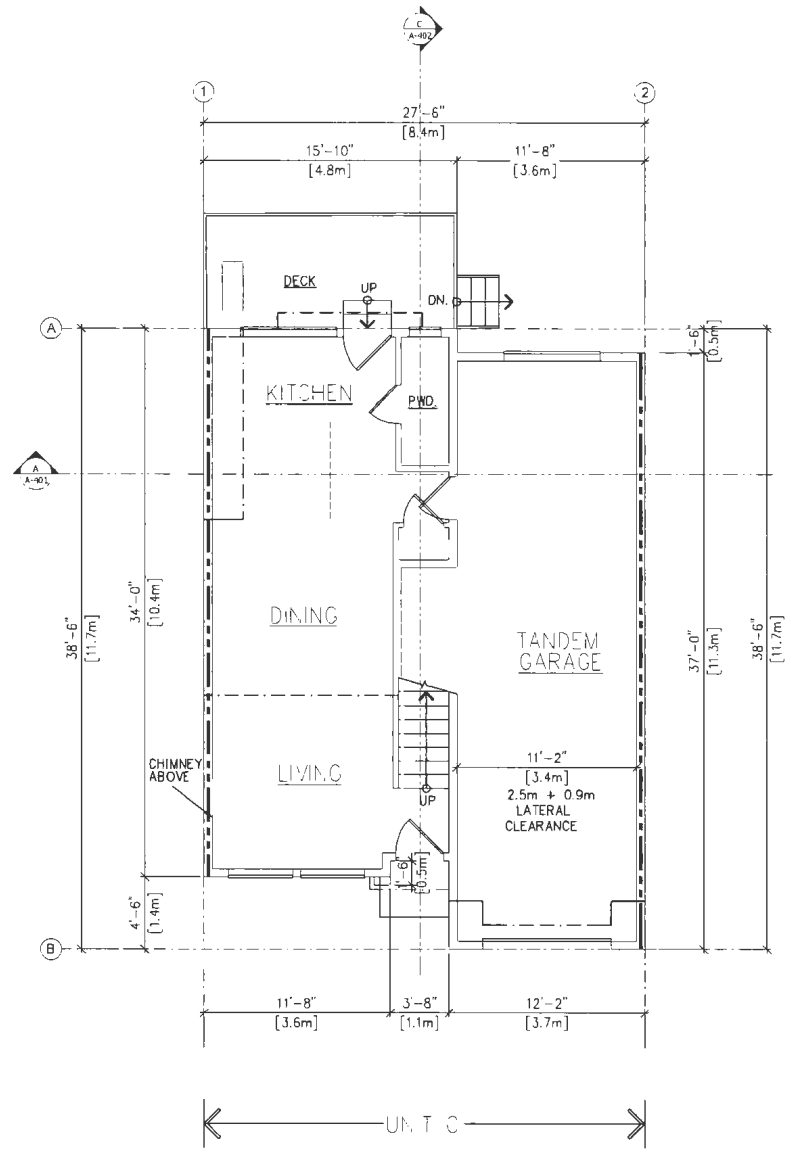


PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

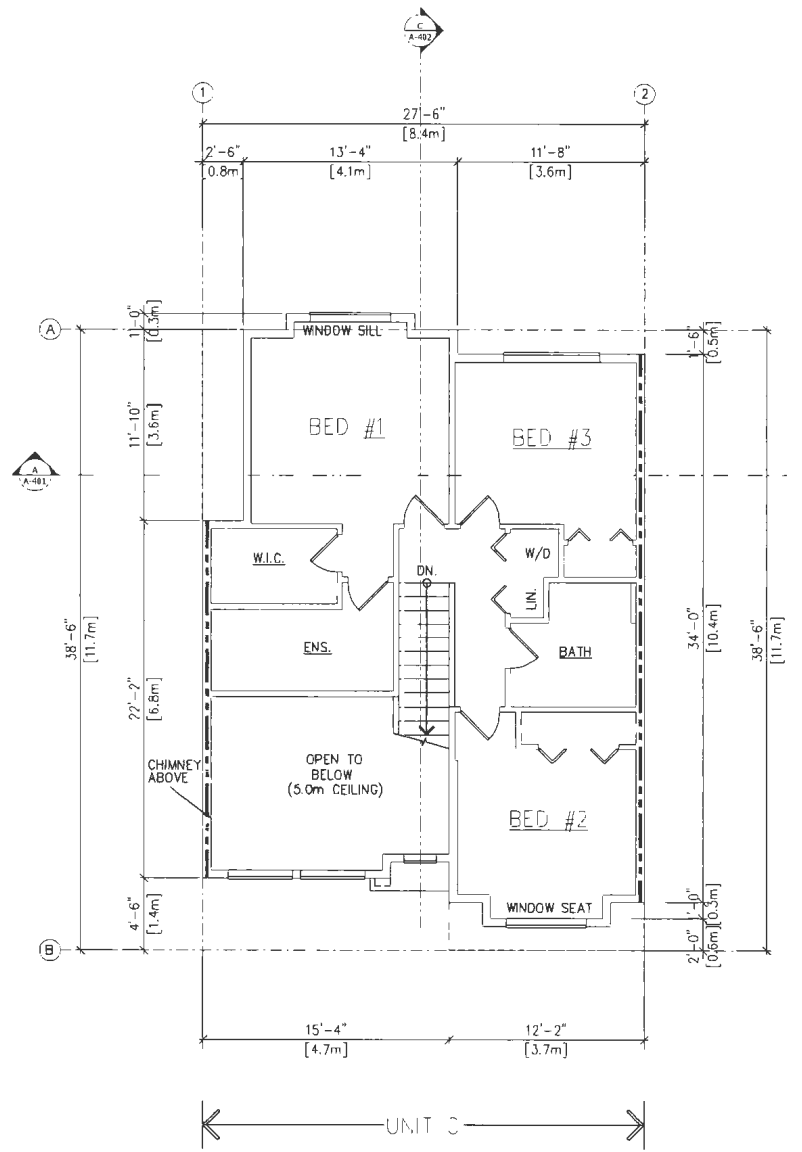
CLIENT:
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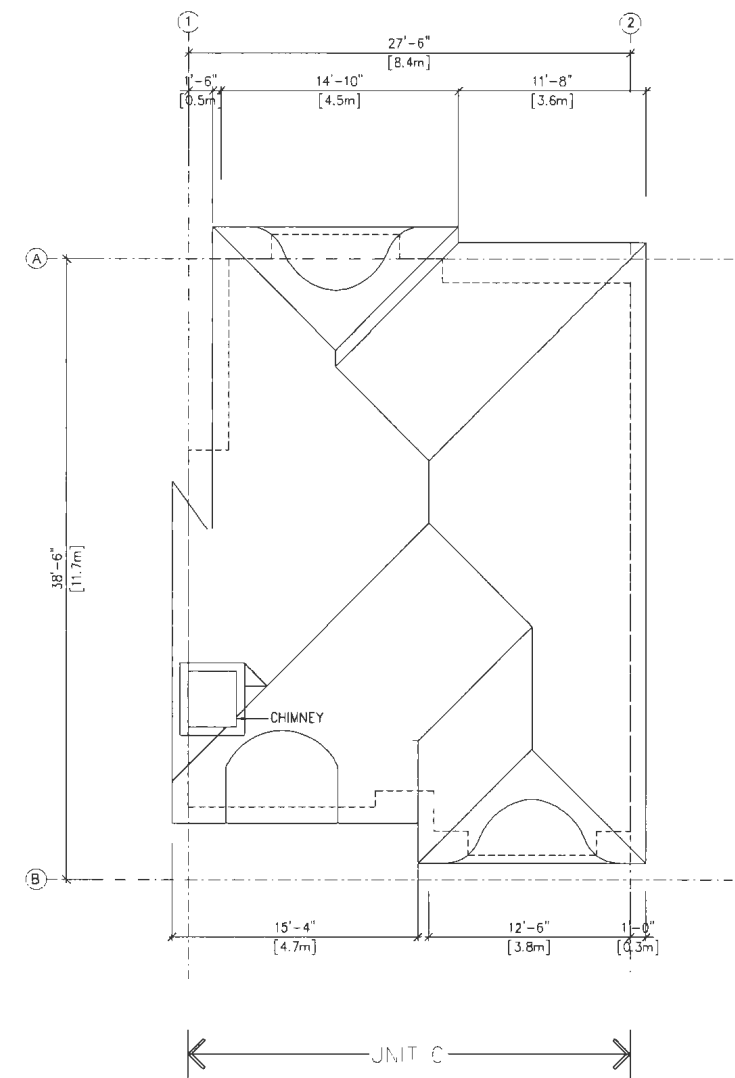
DRAWN	MA
CHECKED	MA
SCALE	3/16" = 1'-0"
JOB NO.	RCH-033
DATE	SEP 2023
SHEET TITLE	



1 MAIN FLOOR PLAN
 A-203 SCALE 3/16" = 1'-0"



2 LEVEL 2 PLAN
 A-203 SCALE 3/16" = 1'-0"

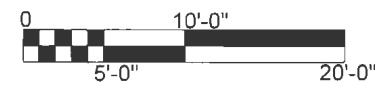


3 ROOF PLAN
 A-203 SCALE 3/16" = 1'-0"

DP 17-790086

REFERENCE PLAN

October 4, 2023



BLDG 2 PLANS

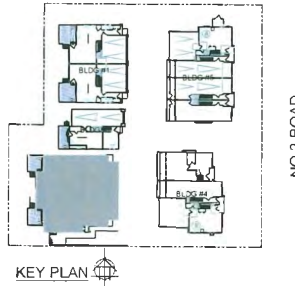
DRAWING NO.	A-203
	G

NOTES:

AS-BUILT IN PLACE FEATURES:

- STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT.
- LEVER HARDWARE ON DOORS
- BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS

NOTE: UNITS WILL INCLUDE PRE-DUCTING FOR SOLAR HEATING. LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



2023-10-03	C	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-05-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP, DP17-790086
2022-06-20	A	DP RESUBMISSION



PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

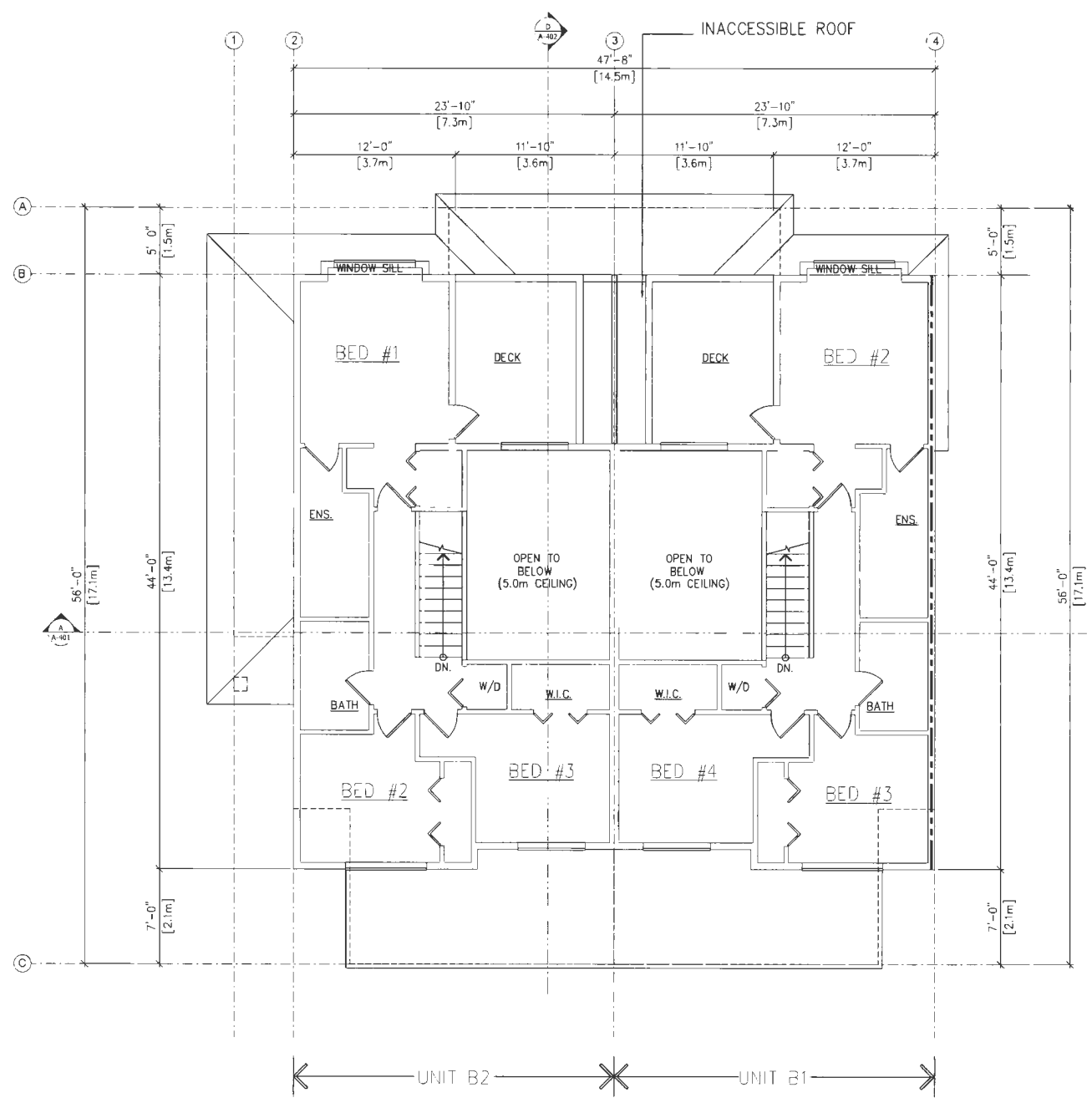
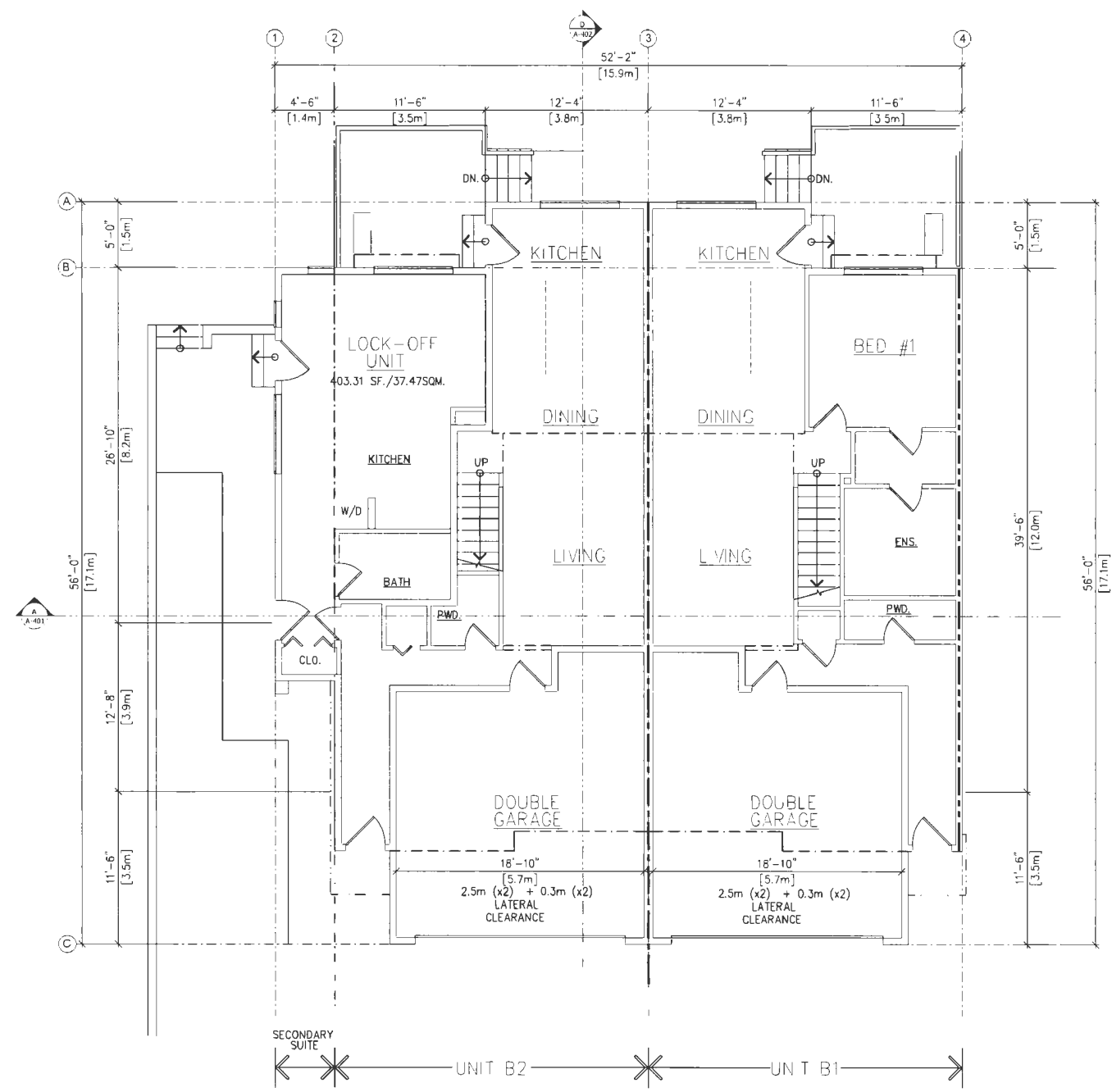
CLIENT:
SATNAM JOHAL

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DRAWN BY	
CHECKED BY	VA
SCALE	3/16" = 1'-0"
JOB NO	RC-033
DATE	SEPT 2023
SHEET TITLE	

BLDG 3 PLANS

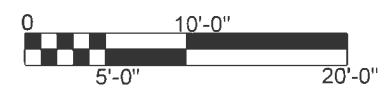
DRAWING NO.:	A-204	G
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DP 17-790086

REFERENCE PLAN

October 4, 2023

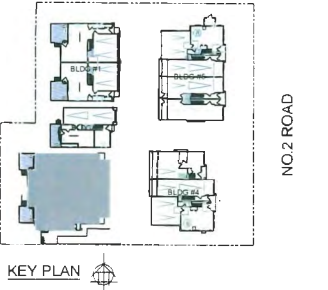


NOTES:

AS-BUILT IN PLACE FEATURES:

- STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT.
- LEVER HARDWARE ON DOORS
- BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS

NOTE: UNITS WILL INCLUDE PRE-DUCTING FOR SOLAR HEATING LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



REVISION	NO.	DATE	DESCRIPTION
	1	2022-05-20	DP RESUBMISSION
	2	2022-10-26	SUBMISSION FOR ADP-DP17-790086
	3	2022-11-08	RESUBMISSION AFTER ADP COMMENTS
	4	2022-03-27	DP RESUBMISSION
	5	2022-07-28	DP RESUBMISSION
	6	2023-09-12	DP RESUBMISSION
	7	2023-10-03	DP RESUBMISSION



PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

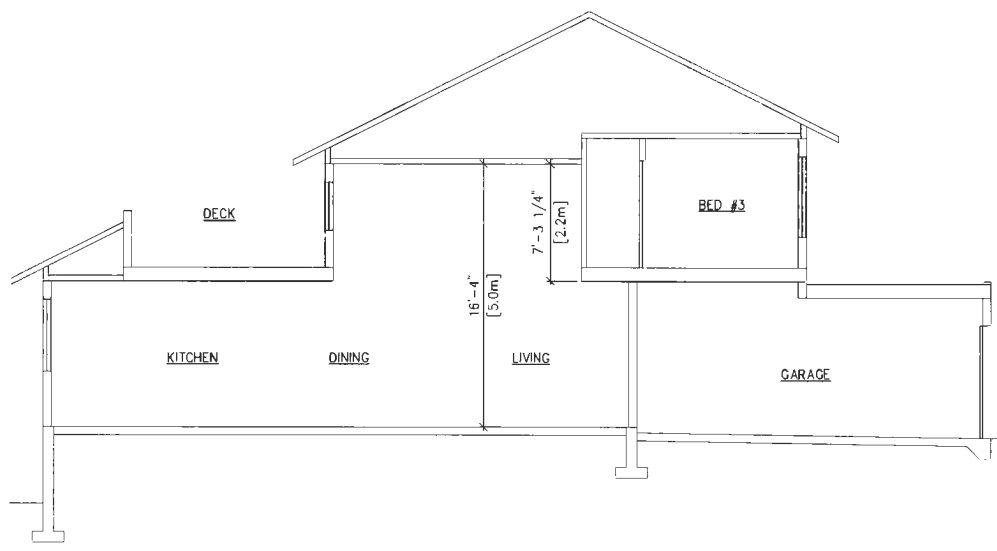
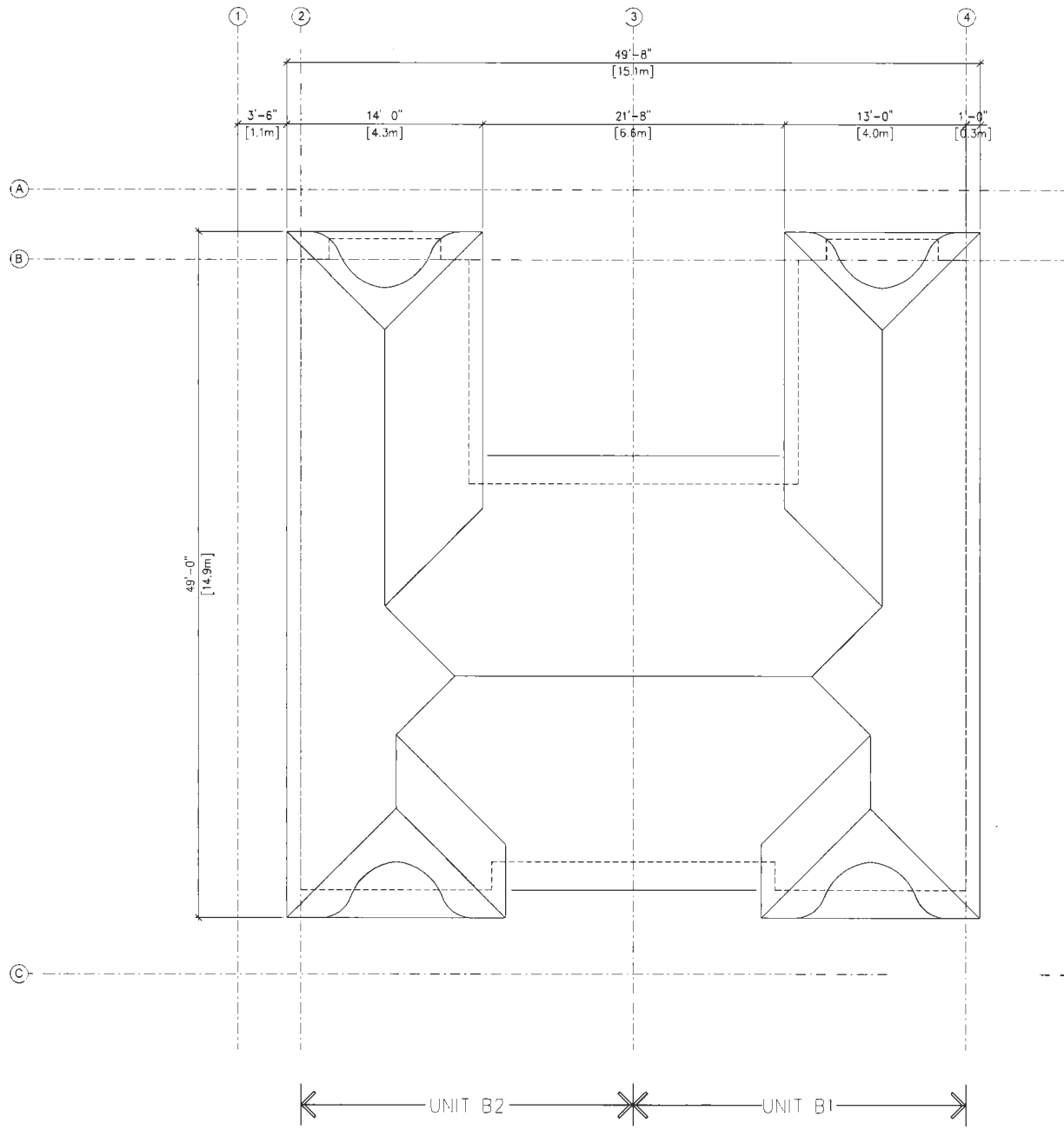
CLIENT:
SATNAM JOHAL

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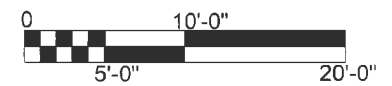
DESIGN:	JA
CHECKED:	JA
SCALE:	3/16" = 1'-0"
JOB NO:	RC4-022
DATE:	SEPT 2023
SHEET TITLE:	

BLDG 3 PLANS

DRAWING NO.:	A-205	G
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U/S OF ROOF TRUSSES	9'-0 3/4"	[2.8m]
TOP OF SUB-FLOOR MAIN FLOOR	5.87m	
U/S OF FLOOR JOISTS MAIN FLOOR	9'-0 3/4"	[2.8m]
T.D. CONCRETE SLAB LOWER FLOOR	2.55m	
AVERAGE GRADE SEE SITE GRADING PLAN FOR AVG. GRADE CALCULATION	2.30m	



DP 17-790086

REFERENCE PLAN

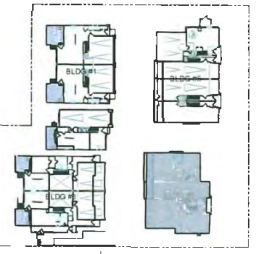
October 4, 2023

NOTES:

AGRS IN PLACE FEATURES:

- STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT.
- LEVER HARDWARE ON DOORS
- BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS

NOTE: UNITS WILL INCLUDE PRE-ROUTING FOR SOLAR HEATING LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



KEY PLAN

2023-10-31	G	DP RESUBMISSION
2023-08-12	F	DP RESUBMISSION
2023-03-28	E	DP RESUBMISSION
2023-03-21	D	DP RESUBMISSION
2022-11-06	C	RESUBMISSION AFTER ACP COMMENTS
2022-10-06	B	SUBMISSION FOR ACP DP17-790086
2022-04-26	A	DP RESUBMISSION

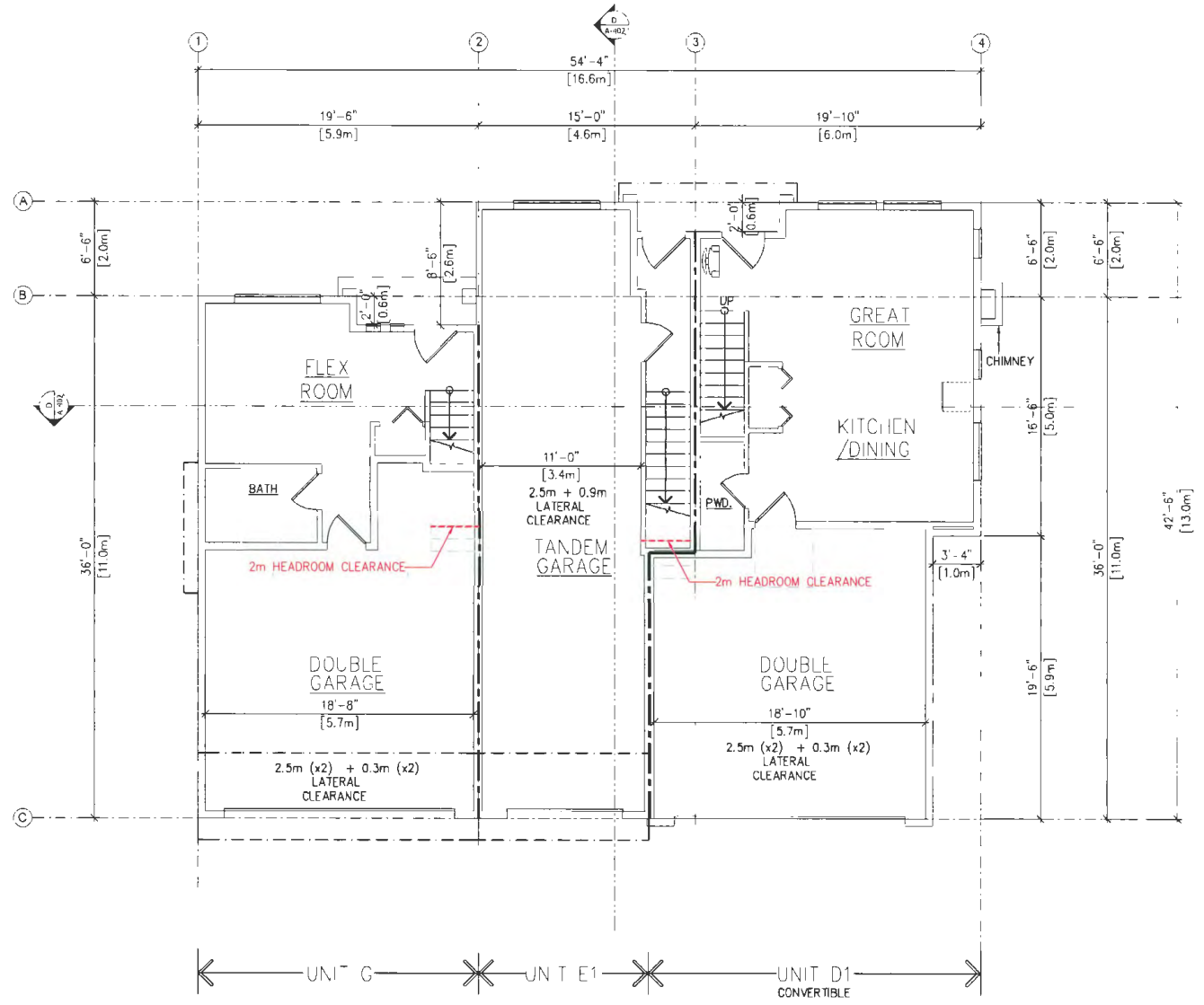


PROJECT
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

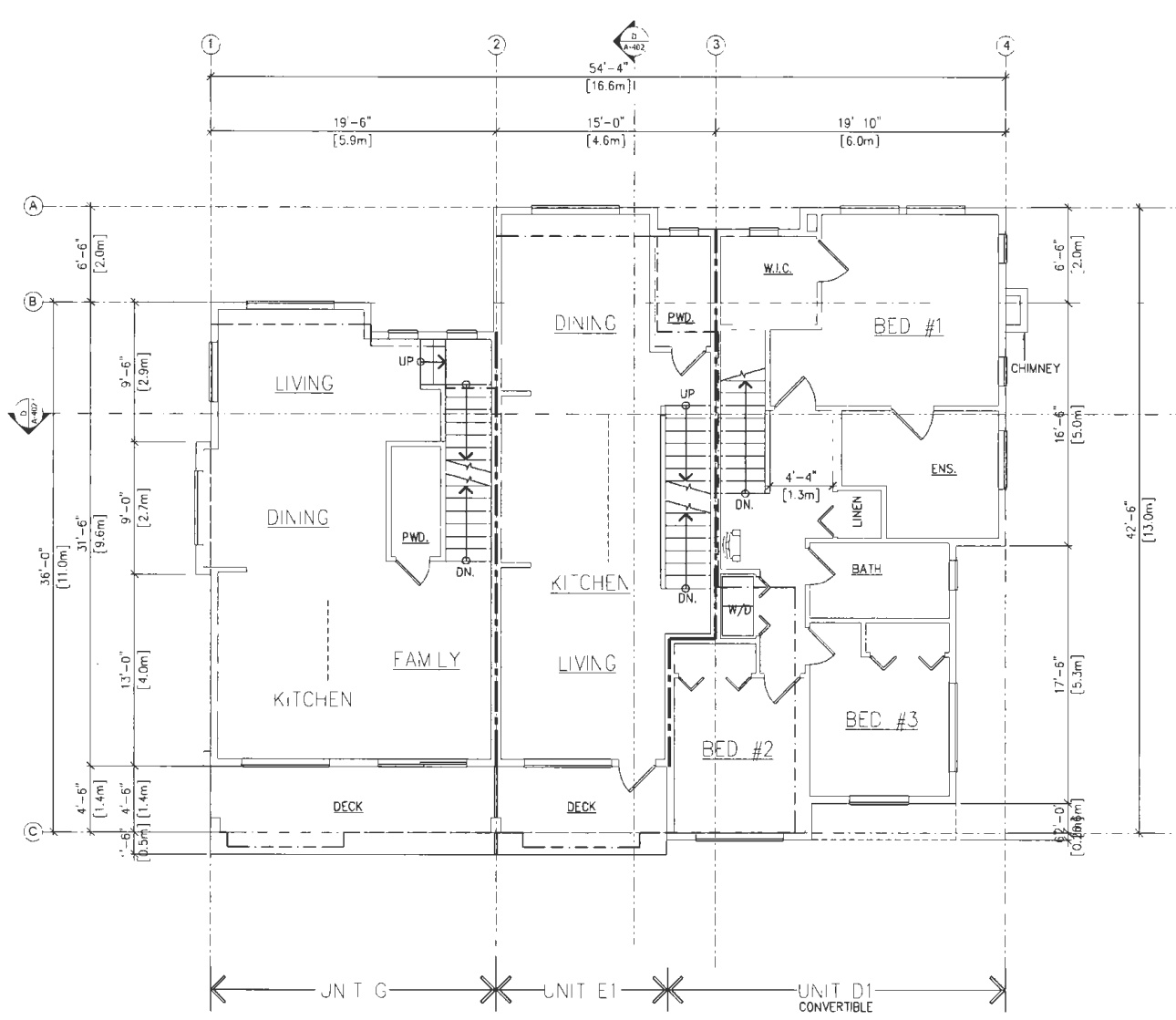
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DRAWN:	PA
CHECKED:	PA
SCALE:	3/16" = 1'-0"
JOB NO.:	REN-333
DATE:	SEPT 2023
SHEET TITLE:	



1 MAIN FLOOR PLAN
 A-206 SCALE 3/16" = 1'-0"



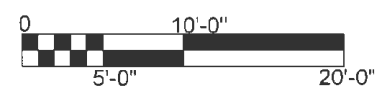
2 LEVEL 2 PLAN
 A-206 SCALE 3/16" = 1'-0"

BLDG 4 PLANS

DP 17-790086

REFERENCE PLAN

October 4, 2023



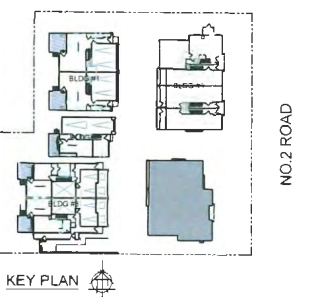
DRAWING NO.:	A-206
	G

NOTES

AGRS IN PLACE FEATURES:

- STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIT.
- LEVER HARDWARE ON DOORS
- BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS

NOTE: UNITS WILL INCLUDE PRE-ROUTING FOR SOLAR HEATING LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



DATE	DESCRIPTION
2023-10-03	G DP RESUBMISSION
2023-09-12	F DP RESUBMISSION
2023-07-26	E DP RESUBMISSION
2023-03-27	D DP RESUBMISSION
2022-11-09	C RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B SUBMISSION FOR POP 2P.7-790086
2022-06-20	A DP RESUBMISSION

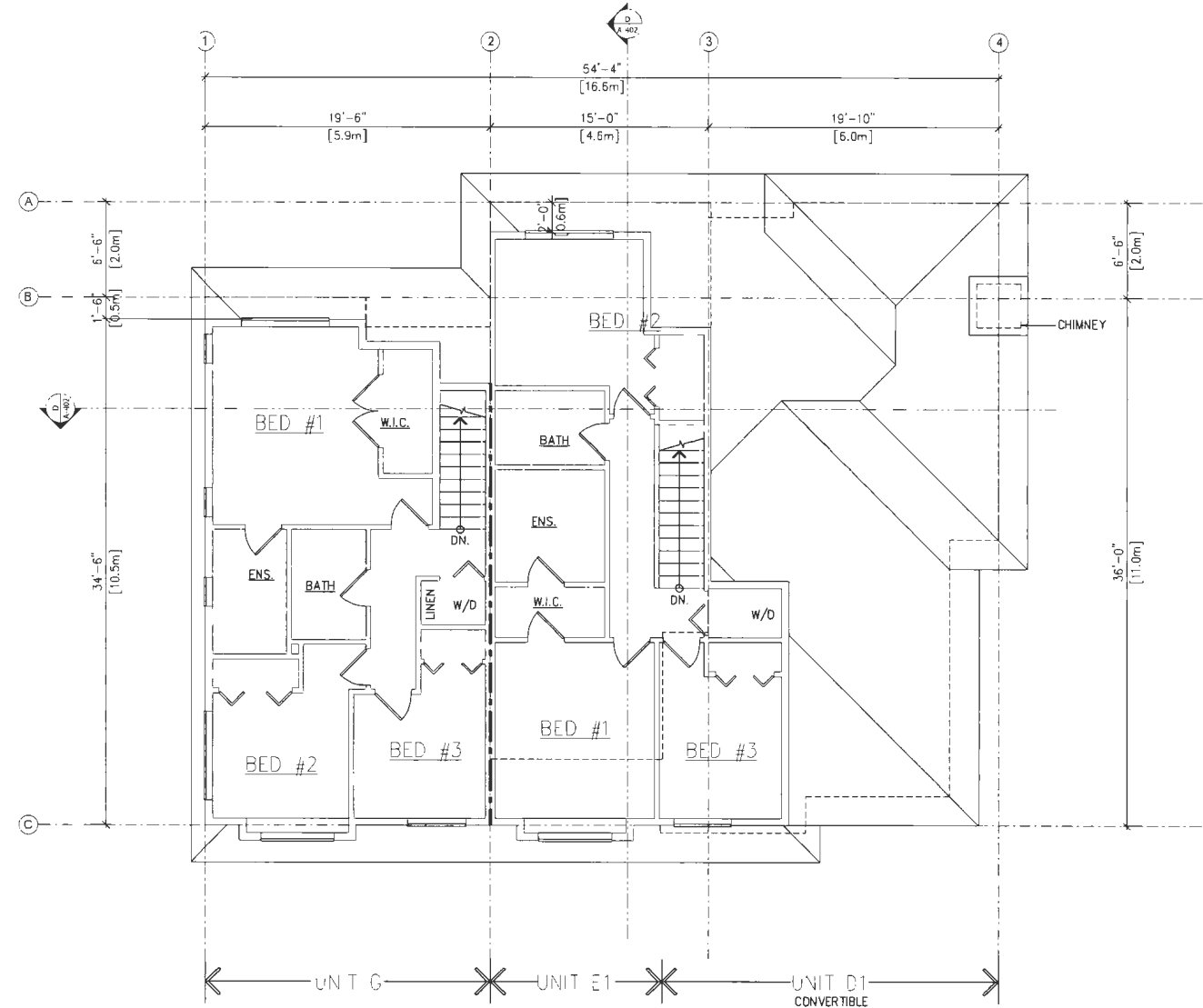


PROJECT
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B.C.

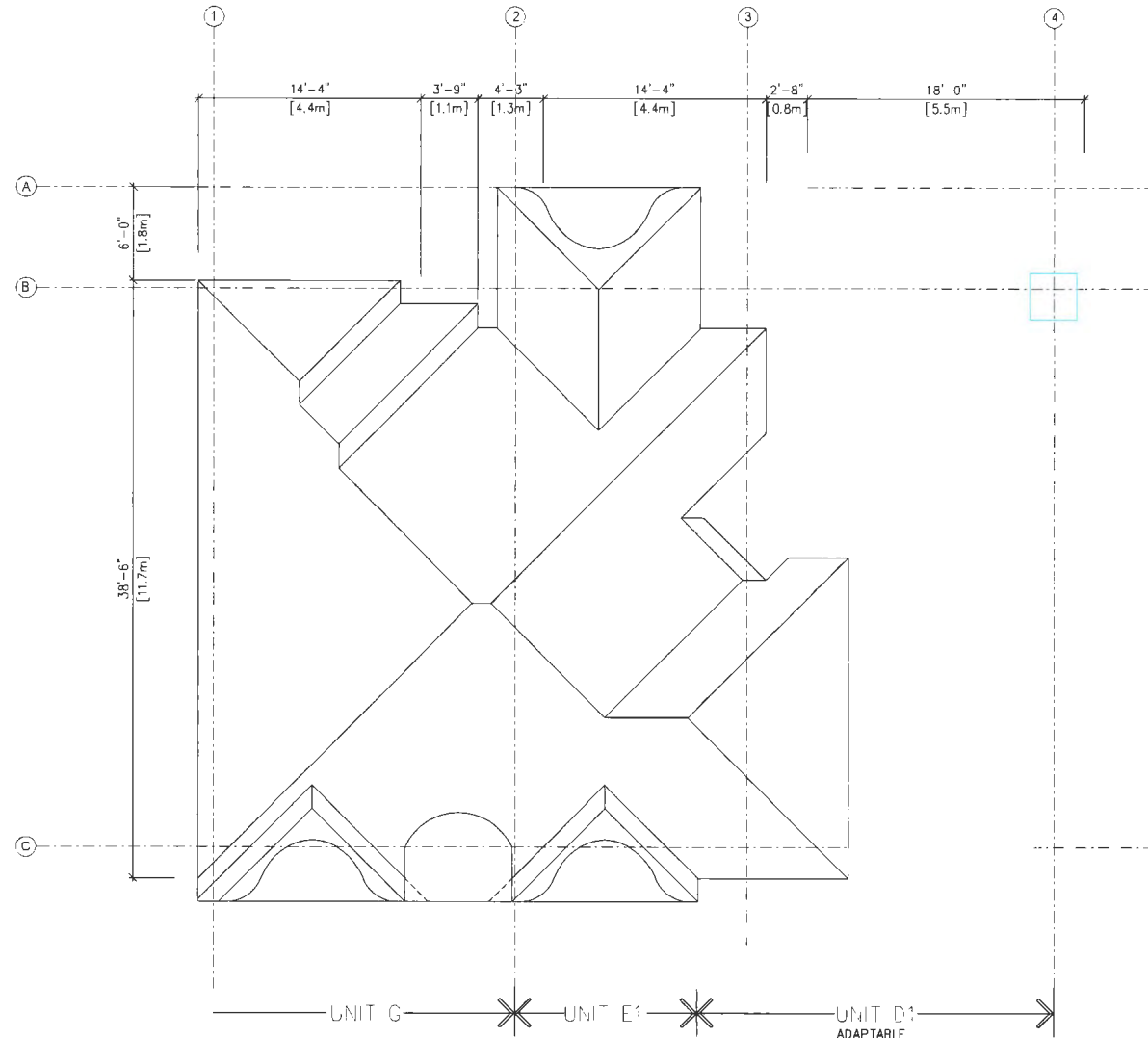
CLINT
SATNAM JOHAL

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DRAWN: NA
CHECKED: YA
SCALE: 3/16" = 1'-0"
JOB NO.: MCH-033
DATE: SEPT 2021
SHEET TITLE:



1 LEVEL 3 PLAN
 A-207 SCALE 3/16" = 1'-0"



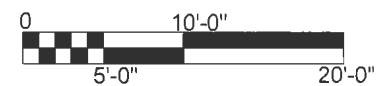
2 ROOF PLAN
 A-207 SCALE 3/16" = 1'-0"

BLDG 4 PLANS

DP 17-790086

REFERENCE PLAN

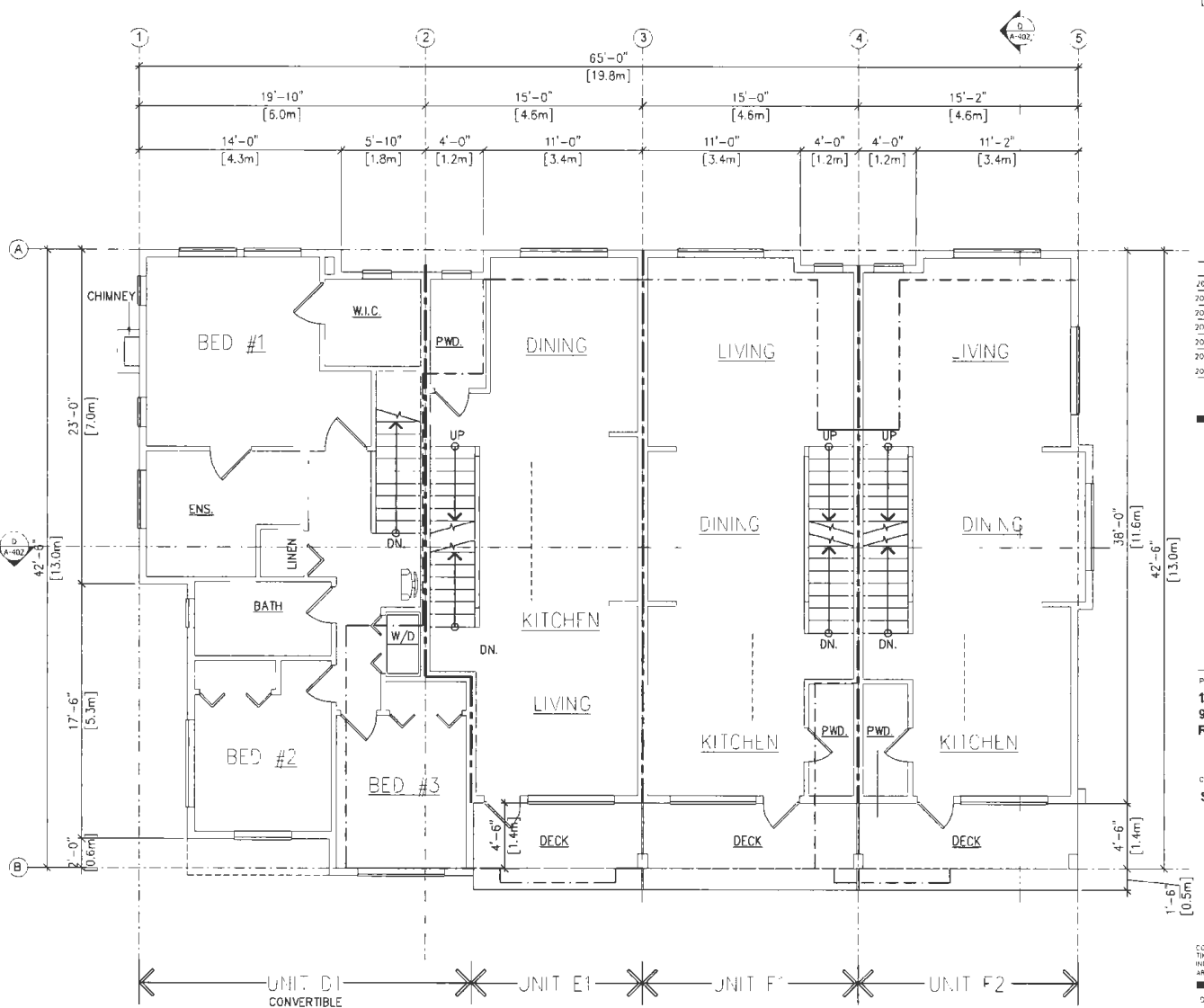
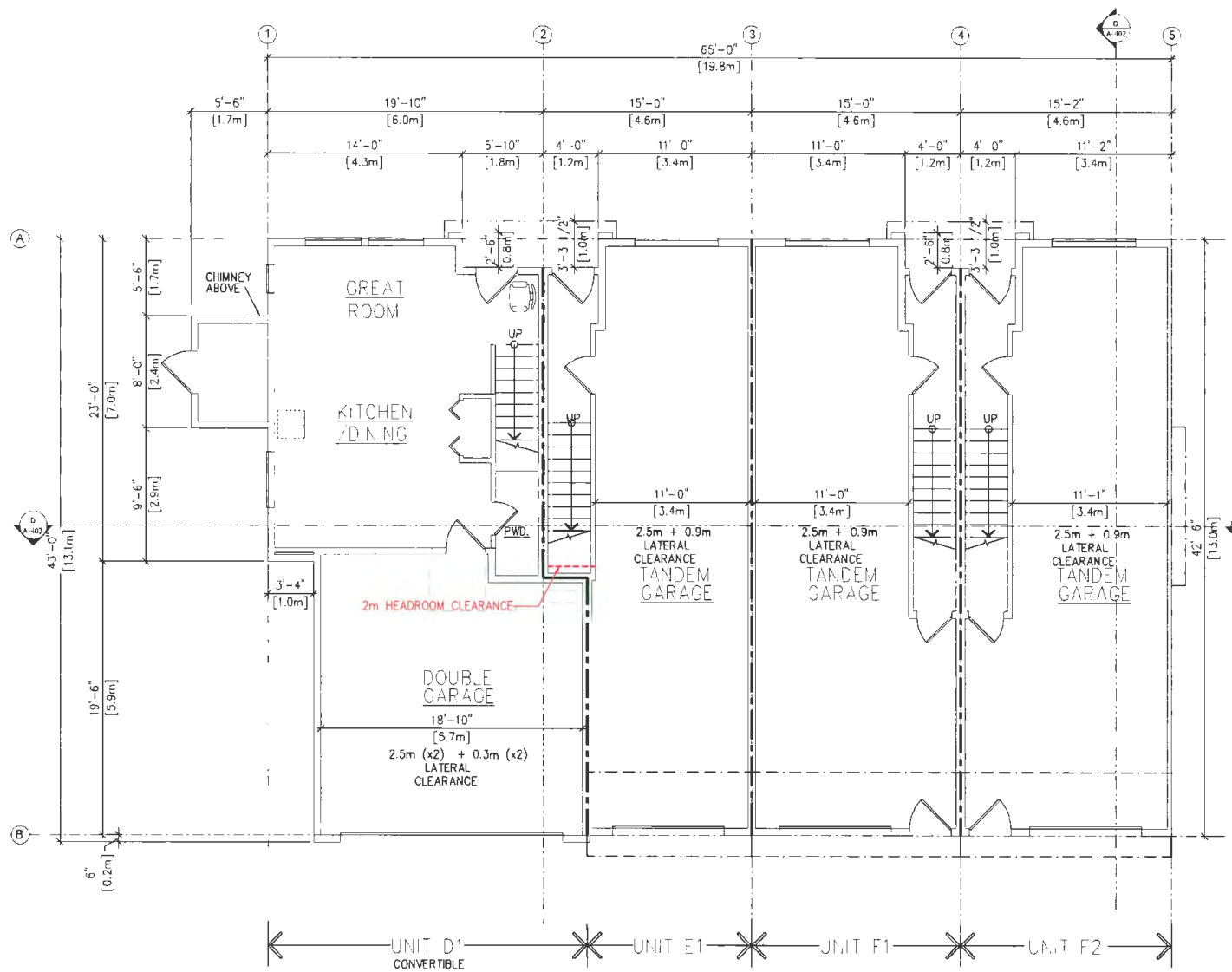
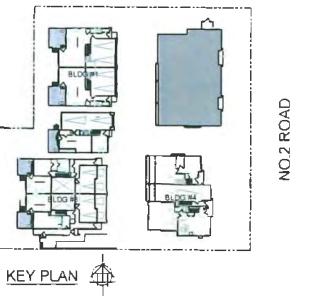
October 4, 2023



DRAWING NO.:	A-207
	G

NOTES:
ABOVE-GLASS FEATURES:
 - STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT.
 - LEVER HARDWARE ON DOORS.
 - BLOCKING IN PARTITIONS FOR FUTURE INSTALLATION OF GRAB BARS.

NOTE: UNITS WILL INCLUDE PRE-DUCTING FOR SOLAR HEATING; LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



2023-10-25	D	DP RESUBMISSION
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-09	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-06	B	SUBMISSION FOR ADP 0911-790086
2022-06-20	A	DP RESUBMISSION



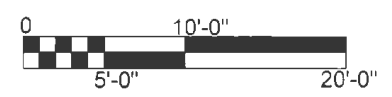
PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
 9291 & 9311 NO. 2 RD,
 RICHMOND, B. C.

CLIENT:
SATNAM JOHAL

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DATE:	19
CHECKED:	YA
SCALE:	3/16" = 1'-0"
JOB NO.:	REM-C53
DATE:	SEPT 2023
SHEET TITLE:	

BLDG 5 PLANS



DRAWING NO.:	A-208	G
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DP 17-790086

REFERENCE PLAN

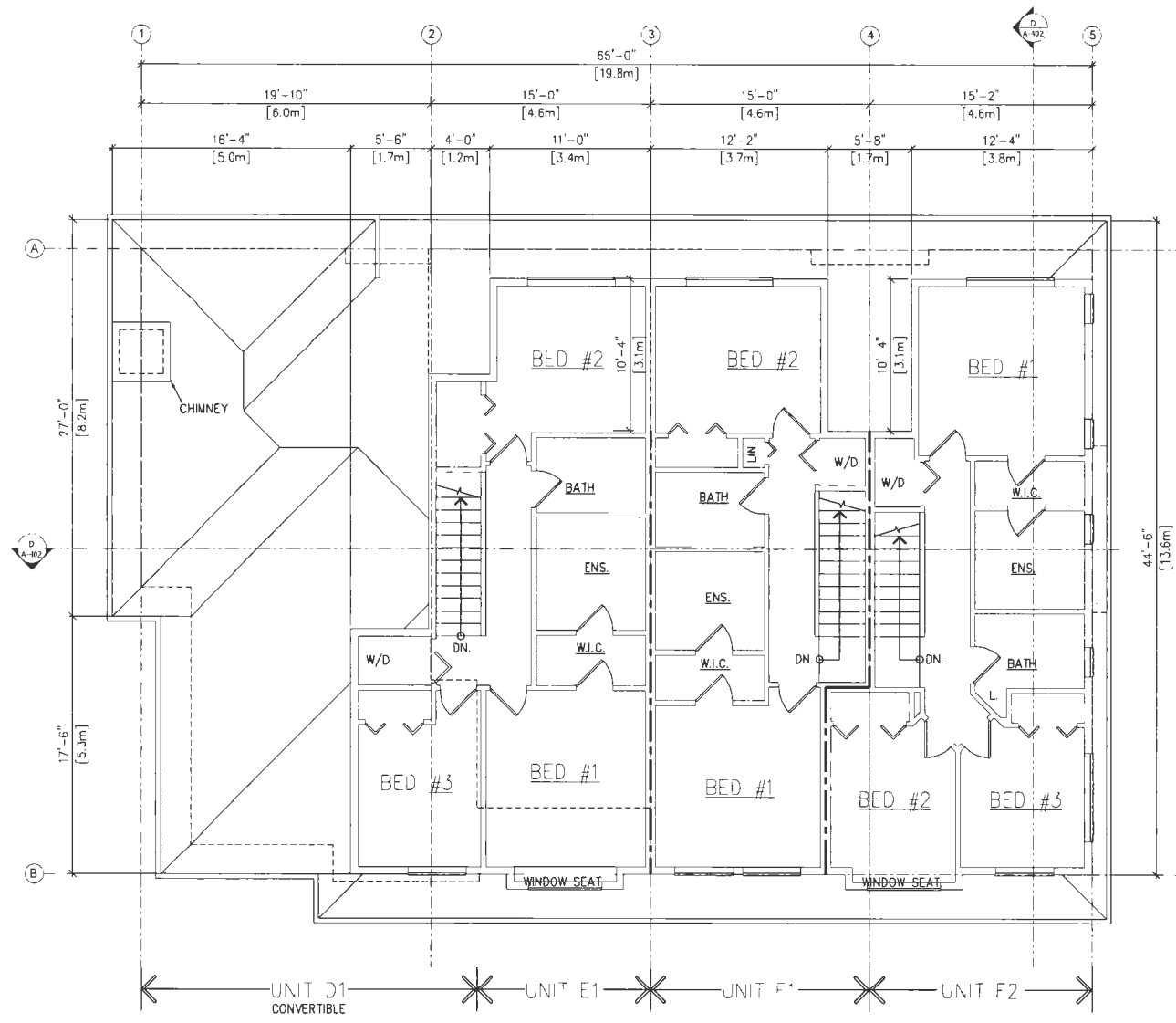
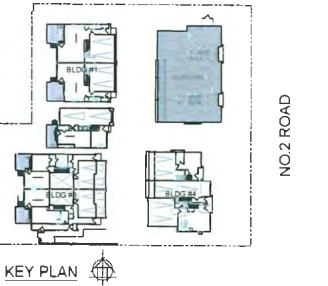
October 4, 2023

NOTES:

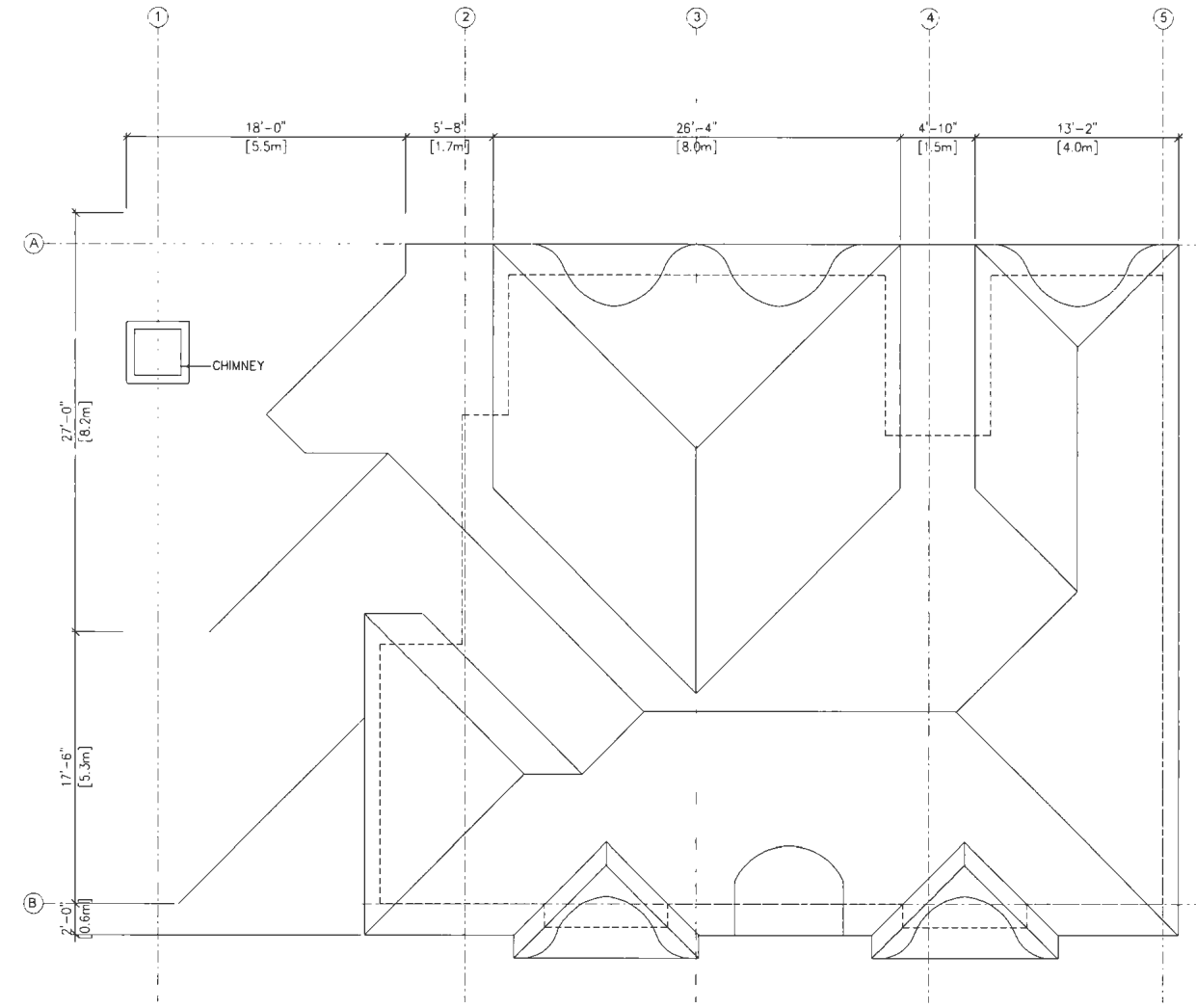
AS-BUILT IN PLACE FEATURES:

- STRAIGHT RUN OF STAIRS WITH BLOCKING FOR FUTURE INSTALLATION OF RESIDENTIAL STAIR LIFT.
- LEVER HARDWARE ON DOORS
- BLOCKING IN BATHROOMS FOR FUTURE INSTALLATION OF GRAB BARS

NOTE: UNITS WILL INCLUDE PRE-DUCTING FOR SOLAR HEATING LOCATION AND DETAILS TO BE DETERMINED BY THE MECHANICAL ENGINEER DURING THE BUILDING PERMIT STAGE.



1 LEVEL 3 PLAN
A-209 SCALE 3/16" = 1'-0"



2 ROOF PLAN
A-209 SCALE 3/16" = 1'-0"

2023-10-03	C	DP RESUBMISSION FOR DPP
2023-09-12	F	DP RESUBMISSION
2023-07-28	E	DP RESUBMISSION
2023-03-27	D	DP RESUBMISSION
2022-11-29	C	RESUBMISSION AFTER ADP COMMENTS
2022-10-26	B	SUBMISSION FOR ADP DP 17-790086
2022-06-20	A	DP RESUBMISSION



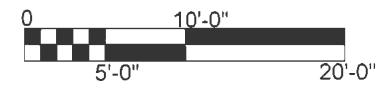
PROJECT
12 UNIT TOWNHOUSE DEVELOPMENT
9291 & 9311 NO. 2 RD,
RICHMOND, B.C.

CLIENT
SATNAM JOHAL

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SCALE	3/16" = 1'-0"
JOB NO	REH-013
DATE	SEPT 2023
SHEET TITLE	

BLDG 5 PLANS



DP 17-790086

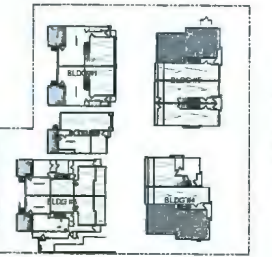
REFERENCE PLAN

October 4, 2023

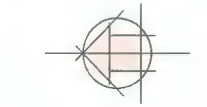
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Convertible Unit Guidelines for Townhouses

NOTES:



KEY PLAN



2023-10-03	DP RESUBMISSION
2023-09-14	DP RESUBMISSION
2023-07-28	DP RESUBMISSION
2023-03-27	DP RESUBMISSION
2023-11-09	RESUBMISSION AFTER ADP COMMENTS
2023-01-06	SUBMISSION FOR ADP-0P17-790086
2023-05-20	DP RESUBMISSION



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PROJECT:
12 UNIT TOWNHOUSE DEVELOPMENT
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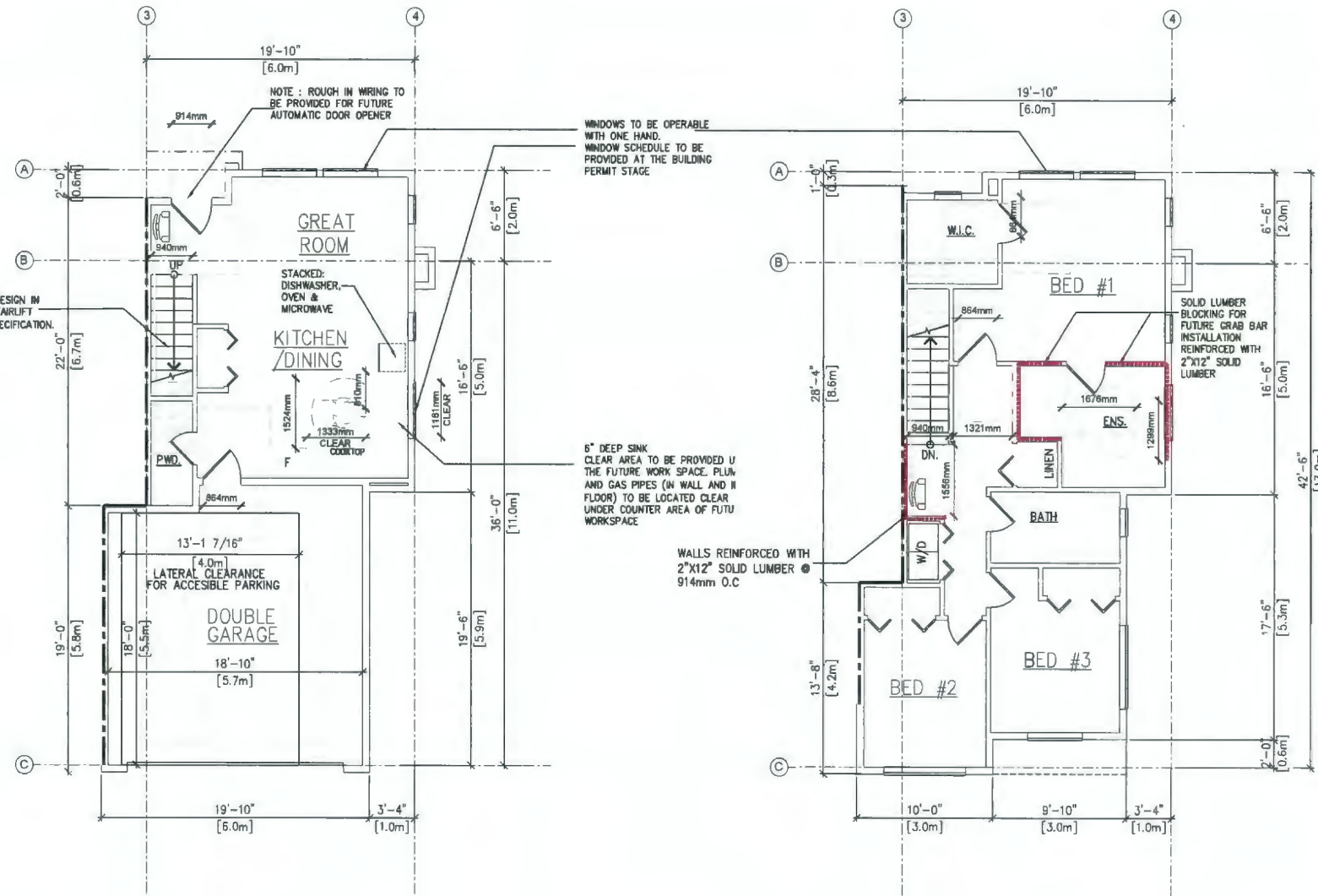
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SCALE	3/16" = 1'-0"
JOB NO.	RCM-033
DATE	SEPT 2023
SHEET TITLE	

CONVERTIBLE UNIT

DRAWING NO:
A-327
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Convertible Unit Guidelines	
Doors & Doorways	Entry doors are a minimum 863 mm but ideally 914 mm and have clear access. Entry door clear exterior floor space min. 1220 mm depth by door width plus 600 mm on latch side (not needed if rough in wiring provided for future automatic door opener). Interior doors to main living areas, 1 bathroom and 1 bedroom, min. 800 mm clear opening with flush thresholds max. 13 mm height. Demonstrate wheelchair access between the hallway and rooms and widen hallway and/or doorway(s) if necessary to secure access. Patio/balcony min. 860 mm clear opening. Note how accessed. All interior thresholds within units comply with BC Building Code. Lever-type handles for all doors.
Vertical Circulation	Stair lift, staircase width, framing support, and landings, as noted on floor plans in compliance with manufacturer specifications. OR Vertical lift, depressed slab area, and landings, as noted on floor plans in compliance with manufacturer specifications. Framing to accommodate shaft construction without impact to surrounding structure. At the top of all stairways, walls are reinforced with 2" x 12" solid lumber at 914 mm to centre.
Hallways	Min. 900 mm width.
Garage	Min. 1 accessible parking space with min. 4 m garage width. Access from garage to living area min. 800 mm clear opening.
Bathroom (Min. 1)	Toilet clear floor space min. 1020 mm at side and in front. Wall blocking for future grab bar installation at toilet, tub and shower. Reinforced with 2" x 12" solid lumber in all bathtub, shower, and toilet locations. Lever-type handles for plumbing fixtures. Pressure and temperature control valves are installed on all shower faucets. Cabinets underneath sink(s) are easily removed. Demonstrate bath and shower controls are accessible (layout or fixture placement). Clear area needed under future work space. Plumbing and gas pipes (in-wall and in-floor) located clear of under counter area of future work space (stove, sink & min. 810 mm wide counter). All pipes are brought in no higher than 304 mm to 355 mm to the centre of the pipe from floor level. Cabinets underneath sink are easily removed. 1500 mm turning diameter or turning path diagram. Lever-type handles for plumbing fixtures.
Kitchen	Min. 1 window that can be opened with a single hand (bathroom, kitchen, living room)
Windows	Placement locations of electrical outlets: beside window, bottom of stairways, beside toilet, above external doors (outside and inside), on front face of kitchen counter, within proximity of control centre for smart home options. Upgrade to four-plex outlets in master bedroom, home office, garage, and recreation room.
Outlets & Switches	



1 UNIT D1 LEVEL 1
A-327 SCALE: 3/16" = 1'-0"

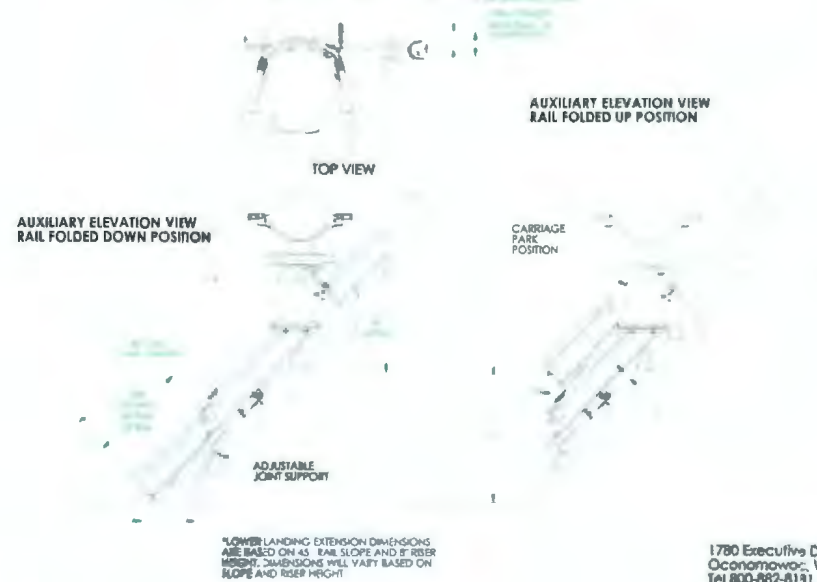
2 UNIT D1 LEVEL 2
A-327 SCALE: 3/16" = 1'-0"

SRE-3000
DISTANCE FROM WALL - SEAT SWIVELED



27 May 2013
Planning Committee Report: http://www.richmond.ca/agendafiles/Open_Planning_5-22-2C13.pdf

SRE-3000 FOLDING RAIL



TECHNICAL DATA/SPECIFICATION
MODEL: SRE-3000 ELAN STRAIGHT RAIL STAIRLIFT PRIVATE RESIDENCE ONLY
RATED LOAD: 300 LBS. ONE PASSENGER
POWER SOURCE: 24 VOLT DC COMPILED OF (3) 3 AMP/HR 12 VOLT SEALED BATTERIES
165 220 VOLT AC 0.8 AMP, 24 VOLT DC 1 AMP CONTINUOUS MONITORING CHARGER
DRIVE: 24 VOLT DC DIRECT DRIVE SELF-LOCKING POSITIVE GEARED MOTOR
2 POLE RATED 1/2 HP MAX. RIGHT ANGLE WORM GEARBOX
50mm (2.0") TORQUE MAX. 25.5 RPM SPEED MAX.
16 AMP CURRENT MAX.
SUSPENSION: RACE & PINION - INTEGRATED BOP GEAR RACK ON RAIL WITH SPUR GEAR ON THE OUTPUT SHAFT OF GEARBOX.
BRAKING: DYNAMIC MOTOR BRAKING THROUGH THE MOTOR CONTROLLER WORM GEAR. ELECTRO-MECHANICAL MOTOR BRAKE AND OVERSPEED PROTECTION.
CONTROLS: CONTINUOUS PRESSURE ROCKER SWITCH LOCATED ON THE HANDRAIL AND WALL MOUNTED CALL/SOUND CONTROLS LOCATED AT BOTH THE TOP AND BOTTOM LANDINGS
ANGLES: 22 TO 45 DEGREES (SPECIALLY UP TO 52 DEGREES) RIGHT OR LEFT HAND ORIENTATIONS.
FINAL LIMIT SWITCH: STANDARD
LOWER LANDING EXTENSION DIMENSIONS ARE BASED ON 45 DEGREE RAIL SLOPE ON 4" X 8" 1ST RISER METHOD. DIMENSIONS WILL VARY BASED ON SLOPE AND RISE/HIGHT

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ILS-01345
SHEET 1 OF 3
REV. 1 (6456)(9/01/17)(BHM)

ILS-01345
SHEET 2 OF 3
REV. 1 (6456)(9/01/17)(BHM)

ILS-01345 Folding Rail
SHEET 3 OF 3
REV. 1 (6456)(9/01/17)(BHM)

DRAWING NO:
A-327
G

DP 17-790086 REFERENCE PLAN October 4, 2023