



City of Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: November 17, 2025

From: Joshua Reis
Director, Development

File: DP 23-031545

Re: **Application by Carson Lei for a Development Permit at 8680 Alexandra Road**

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the subdivision of 8680 Alexandra Road into two lots and permit construction of two commercial buildings on proposed Lot B on a site zoned "Auto-Oriented Commercial (CA)"; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 applicable to Lot B to:
 - a) Reduce the minimum interior side yard setback from 3.0 m to 1.5 m; and
 - b) Reduce the minimum rear yard setback from 3.0 m to 2.0 m.

Joshua Reis
Director, Development
(604-247-4625)

JR:ak
Att. 4

Staff Report

Origin

Ka Seng (Carson) Lei, on behalf of 332 Holding Ltd. (Directors: San Zi Li, Zhi Hong Li, Li Fang Feng, I Chai Lei, Ka Seng Lei, Weng Mui Ng), has applied to the City of Richmond for permission to subdivide 8680 Alexandra Road into two lots ("Lot A" and "Lot B"), and facilitate the construction of two commercial buildings of up to two-storeys in height on the eastern lot (Lot B) and zoned "Auto-Oriented Commercial (CA)". No construction is proposed on Lot A at this time. Vehicle access is proposed to be from Alexandra Road. The property is currently vacant. A location and aerial map of the site is provided in Attachment 1.

A Servicing Agreement (SA) is required as a condition of subdivision and includes, but is not limited to, the following improvements:

- New water, storm, and sanitary service connections.
- Frontage improvements along Alderbridge Way and Alexandra Road, including new sidewalk, landscaped boulevards and new curb and gutter.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 2) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the North: Across Alexandra Road are two properties zoned "Auto-Oriented Commercial (CA)". Each lot contains a two-storey commercial building fronting onto Alexandra Road.

To the East: Abutting the subject site is a lot zoned "Auto-Oriented Commercial (CA)" containing a single-storey childcare building.

To the South: Across Alderbridge Way is a lot zoned "Residential/Limited Commercial (RCL1)" containing five, four-storey low-rise apartments.

To the West: Abutting the subject site is a lot zoned "Auto-Oriented Commercial (CA)" containing a five-storey hotel building that is the subject of an active Development Permit (DP) application for exterior renovations (DP 24-043129).

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the urban design issues and other staff comments identified as part of the review of the subject DP application. In addition, it complies with the intent of the applicable sections of the Official Community Plan (OCP) and is generally in compliance with the "Auto-Oriented Commercial (CA)" zone, except for the zoning variance noted below.

Zoning Compliance/Variations (staff comments in ***bold italics***)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 applicable to proposed Lot B to:

- 1) Reduce the minimum interior side yard (west) setback from 3.0 m to 1.5 m.

Staff supports the proposed variance for the following reasons:

- *The reduced interior side yard setback enables a more efficient site circulation and parking layout and allows for two off-site trees to be retained on the neighbouring property at 8740 Alexandra Road. It also supports pedestrian connections along all sides of the proposed buildings, including a future 3.0 m wide publicly accessible north-south connection that bisects the subject site between Lot A and Lot B.*
- *An interim 1.5 m walkway is to be constructed between the west property line and Building 1. Building 1 proposes to provide glazing, entryways into commercial units and continuous canopies along the west building facade. The pedestrian path is framed as an active and visually engaging pedestrian corridor that benefits from weather protection and passive surveillance.*

- 2) Reduce the minimum rear yard (south) setback from 3.0 m to 2.0 m.

Staff supports the proposed variance for the following reasons:

- *The reduced setback enables the proposed development to provide two buildings within a functional site design. Accommodating two buildings on site strengthens the development's street presence and activation on both road frontages (Alexandra Road and Alderbridge Way).*
- *The applicant is voluntarily providing an approximately 4.5 m wide road dedication along the entire south property line, and the southern building has been landscaped and designed to activate the Alderbridge Way frontage. The resulting distance from the back of the curb to the building face is approximately 7.0 m.*

Advisory Design Panel Comments

The Advisory Design Panel (ADP) reviewed this proposal on September 4, 2025, and supported the proposed development moving forward to DP Panel, subject to the applicant giving consideration to the comments identified by Panel members. A copy of the relevant excerpt from the ADP meeting minutes is attached for reference (Attachment 3). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '***bold italics***'.

Analysis***Conditions of Adjacency***

- The subject site is located within the City Centre Area Plan (CCAP), Aberdeen Village, and is designated as "Urban Centre T5 (25.0 m)" within the "Commercial Reserve – Mid Rise" sub area. The sub-area promotes street-oriented commercial uses such as retail and restaurants. The proposed development complies with the plan designations, anticipating uses such as retail shops and restaurants, which support street frontage activation in keeping with adjacent commercial uses north of Alderbridge Way.

- The subject site fronts both Alexandra Road to the north and Alderbridge Way to the south. The proposed massing of the two commercial buildings on Lot B responds to the double fronting characteristic of the subject site and provides a transition between the commercial uses to the north and the residential to the south.
- The buildings have been sited to provide an east-west transition, providing separation from the daycare centre to the east while continuing to support pedestrian-friendly commercial uses westwards, including via a 3.0 m north-south pedestrian walkway.
- The proposed development would facilitate subdivision of the property into two lots. Development at this time is only proposed on the eastern lot (Lot B). No development is currently proposed for the west portion (Lot A). A concept plan demonstrating future development potential of Lot A post subdivision had been included for illustration purposes only. Any redevelopment of Lot A would require a separate DP.
- Development within the City Centre is subject to potential impacts from other developments that may be approved within the City Centre (e.g., loss of views, shading, overlook and reduced privacy, noise and ambient light). To ensure future owner/tenants understanding of these potential impacts, registration of a legal agreement on Title is required prior to DP issuance and requires that the owner provide an acknowledgement of the same in all purchase and sale agreements.
- The subject site is located within Aircraft Noise Sensitive Development (ANSI) Policy Area, "Area 1A", which prohibits new aircraft noise-sensitive land uses (e.g. residential uses, childcare facilities, hospitals and schools for kindergarten to grade 12). The proposed development complies with these restrictions. Registration of a restrictive covenant on Title is required prior to DP issuance to ensure owner awareness of potential aircraft noise impacts and the City's indemnification.

Urban Design and Site Planning

- The proposed development on Lot B consists of 14 units within two buildings. A two-storey building (Building 1) is positioned along the proposed future west property line and fronts Alexandra Road to the north. A single-storey building (Building 2) fronts Alderbridge Way to the south to provide streetscape activation. Both buildings are separated by a surface parking area, with vehicle access provided from Alexandra Road.
- Prior to DP issuance, the applicant will provide the following voluntary road dedications to accommodate road frontage improvements:
 - A 4.5 m wide road dedication along the entire south property line on Alderbridge Way.
 - A 1.0 m x 1.0 m corner cut at the northeast corner of the subject site to facilitate sidewalk connectivity to the east.
- A SA is required as a condition of subdivision for the design and construction of frontage improvements along the entire subject property, including, but not limited to:
 - Along Alderbridge Way: 3.5 m wide multi-use path, 1.5 m landscaped boulevard and 0.15 m curb and gutter.
 - Along Alexandra Road: 2.0 m wide sidewalk, 1.5 m landscaped boulevard, and 0.15 m curb and gutter.

- A 3.0 m wide pedestrian path is proposed to bisect the subject property, connecting Alexandra Road to Alderbridge Way. Of the 3.0 m, 1.5 m will be provided on Lot B abutting the west property line, and 1.5 m will be located along Lot A's east property line. Prior to DP issuance, the applicant is required to register on Title a Public Right-Of-Passage (PROP) Statutory Right-of-Way (SRW) over the 3.0 m pedestrian path. As part of this DP, the applicant will be constructing Lot B's 1.5 m portion of the pedestrian path. Prior to the development of Lot A, which will include constructing a 1.5 m path along the east property line, metal picket fencing is proposed to delineate the two lots. This fencing will be removed by the owner of Lot B upon ultimate completion of the path.
- Street facing building facades have been designed to a pedestrian scale with appropriate glazing and low canopies to provide strong urban edges.
- Pedestrian access is provided along all building faces, accompanied with continuous canopies for weather protection. Paths are at minimum 1.5 m wide, with doors designed to swing inwards to maintain clear pedestrian circulation.
- Additional opportunities for pedestrian activation are provided on the second level of Building 1. An outdoor corridor running along the building's east facade and an outdoor amenity terrace at the south end of the building animate the upper level and encourages natural surveillance for the driveway and surface parking area below.
- A waste and recycling room is proposed at the southern end of Building 1 for the shared use of the entire Lot B development.
- The applicant has designed the buildings to be compliant with the Flood Plain Designation and Protection Bylaw (2.9 m GSC – Area A). Prior to DP issuance, a flood indemnity covenant is required to be registered on Title.
- Prior to subdivision approval, the applicant will also amalgamate the City owned road parcel south of the subject site to form part of Alderbridge Way to help provide a more orderly property boundary configuration.

Transportation

- A total of 40 parking spaces are proposed in the surface parking area located between Buildings 1 and 2.
- The proposed parking rates are consistent with the Zoning Bylaw parking requirements, subject to the provisions of Transportation Demand Management (TDM) measures which are required to be secured through the registration of legal agreements prior to DP issuance. The following TDMs are proposed:
 - Provision of Level 2 electrical vehicle charging capabilities for 22 on-site parking spaces.
 - Provision of additional two Class 1 and three Class 2 bicycle parking spaces, exceeding the Zoning Bylaw required amount.
 - Provision of one e-bike charging space within one of the Class 1 bicycle lockers.
 - Provision of a 3.0 m wide north-south PROP SRW to facilitate pedestrian connection between Alderbridge Way and Alexandra Road.

- A total of six Class 1 and eight Class 2 bicycle parking spaces are proposed on site. Class 2 bicycle racks are located along Alexandra Road and Alderbridge Way while Class 1 bicycle lockers are located between Buildings 1 and 2.
- Consistent with Zoning Bylaw requirements, a medium loading space is proposed between Buildings 1 and 2 near the waste and recycling room.

Architectural Form and Character

- The proposed development features a contemporary commercial design defined by clean building lines, flat roof profiles and a simple massing arrangement. The buildings are designed to frame the internal parking area while maintaining a strong street presence on Alexandra Road to the north and Alderbridge Way to the south.
- The use of glazing, composite panels and metal panel flashing in warm neutral colours (pearl white and grey beige) contributes to a modern and visually cohesive building appearance. To help maintain visual porosity along the west façade of Building 1, registration of a legal agreement on Title limiting window covering, tinting, and frosting is required prior to DP issuance.
- Horizontality is emphasized with subtle variations in façade treatment and canopies to break up the building massing and enhance visual interest. Entrances and glazing are oriented toward public frontages and pedestrian walkways to support an active interface.
- Separate sign permits are required for future signage proposed on-site.

Tree Management

- The applicant submitted a Certified Arborist Report which has been reviewed by the City's Tree Preservation Officer and Parks staff. The report identifies 10 bylaw trees on-site and four off-site trees, including:
 - Two on-site trees (tag # 6 and 9) are significant black locust trees with a diameter at breast height (dbh) of 127 cm and 92 cm respectively. The Project Arborist reviewed and observed these trees to be in poor condition, exhibiting large wounds and bark inclusions. These trees are located within the future road dedication area along Alderbridge Way.
 - The remaining eight bylaw sized trees on-site (tag #5-14) are observed to be in fair to poor condition, exhibiting phototropic lean, failing branches and wounded stems. These trees are located within the future road dedication area along Alderbridge Way.
- City staff have confirmed the Project Arborist's observations and noted the trees to be poor candidates for retention, exhibiting leaning, signs of internal decay, broken branches and large wounds on various trees. Removal and compensation are recommended by Parks staff. Prior to DP issuance, the applicant is to provide the compensation amount of \$19,192.00 to the City's Tree Compensation Fund.
- Two off-site trees (tag #1 and 2) on the east neighbouring property are proposed to be protected and retained. The applicant is required to install tree protection fencing prior to any construction activities.

- Two off-site trees (tag #3 and 4) on the east neighbouring property are in poor condition with decay on several stems. The applicant is proposing to remove these two off-site trees and has acquired permission from the neighbouring owner.
- The applicant is proposing to plant 15 trees on site, including the four replacement trees associated with the removal of two off-site trees (tag #3 and 4).
All new on-site trees are proposed to be a minimum of 4.0 m in height for conifers and 8 cm caliper for deciduous trees. A mix of coniferous and deciduous trees planting along the north, west, and south property lines are proposed for variation and seasonal interest. The SA which the applicant will be required to enter into as a condition of subdivision, will include the planting of street trees within the landscaped boulevards as part of frontage improvements.

Landscape and Open Space Design

- A variety of trees and low shrub planting along Lot B's north and south property lines contribute towards a landscape edge and assist in softening the building frontages.
- Additional planting proposed between the two buildings along the western property line, on the second level patio of Building 1, and around pathway intersections on the subject site, collectively promote a courtyard like character and enhances the pedestrian realm. In addition, two existing trees located on the neighbouring property at 8740 Alexandra Road and adjacent to the subject site's east property line, are to be retained.
- Pavers are used throughout the site to increase permeability and highlight key pedestrian pathways.
- 1.2 m (4 ft.) high metal picket fencing is proposed along the east and west property lines to define the subject site while maintaining visual porosity between properties. Upon the completion of the future north-south 3.0 m wide pedestrian path, the fencing along the west property line of Lot B will be removed.
- High efficiency irrigation will be provided in all landscaped areas of the development.
- Prior to DP issuance, the applicant is required to submit a landscape security in the amount of \$249,623.00 to ensure that the proposed landscaping works are completed as proposed.

Crime Prevention Through Environmental Design

- Low-glare downward-facing lighting is proposed around the building perimeters and is incorporated within the landscaping to enhance security and prevent dark corners.
- The proposed design incorporates high amounts of glazing and an external second-level corridor to activate the pedestrian realm, providing transparency and enhancing natural surveillance.
- Low shrub planting and picket-style fencing have been used to help provide clear sightlines.

Sustainability

- The proposed development will be constructed to Step Code 2 with EL-2, consistent with City energy requirements. The applicant has submitted written confirmation from their Certified Energy Advisor indicating that this project can achieve the outlined energy targets.

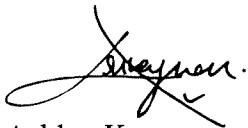
- The subject site is located within the City Centre District Energy Utility (DEU) service area. Prior to DP issuance, a standard DEU covenant will be registered on Title requiring the developer to:
 - Grant a SRW necessary for supplying DEU services to the proposed development.
 - Design the proposed development with capacity to connect to and be serviced by a DEU.
 - Establish a connection to the DEU once it becomes available.
- The following sustainability features will be incorporated into the proposed development and will be secured through legal agreements registered on Title prior to DP issuance:
 - Water and energy conservation features, including low-flow plumbing systems and motion sensor interior lighting.
 - Rough-in for future rooftop photovoltaic infrastructure as an alternative energy source for future tenants.

Conclusions

This proposal is for a commercial development consisting of two buildings of up to two storeys in height on the eastern portion of 8680 Alexandra Road and to facilitate the subdivision of the property into two lots (Lot A and B). Vehicle access is proposed to be from Alexandra Road.

The proposed development complies with the requirements of the “Auto-Oriented Commercial (CA)” zone, except for the variances discussed. As the proposed development would meet applicable policies and DP Guidelines, staff recommend that the DP be endorsed, and issuance by Council be recommended.

The list of DP Considerations is included in Attachment 4, which has been agreed to by the applicant (signed concurrence on file).



Ashley Kwan
Planner 1
(604-276-4173)

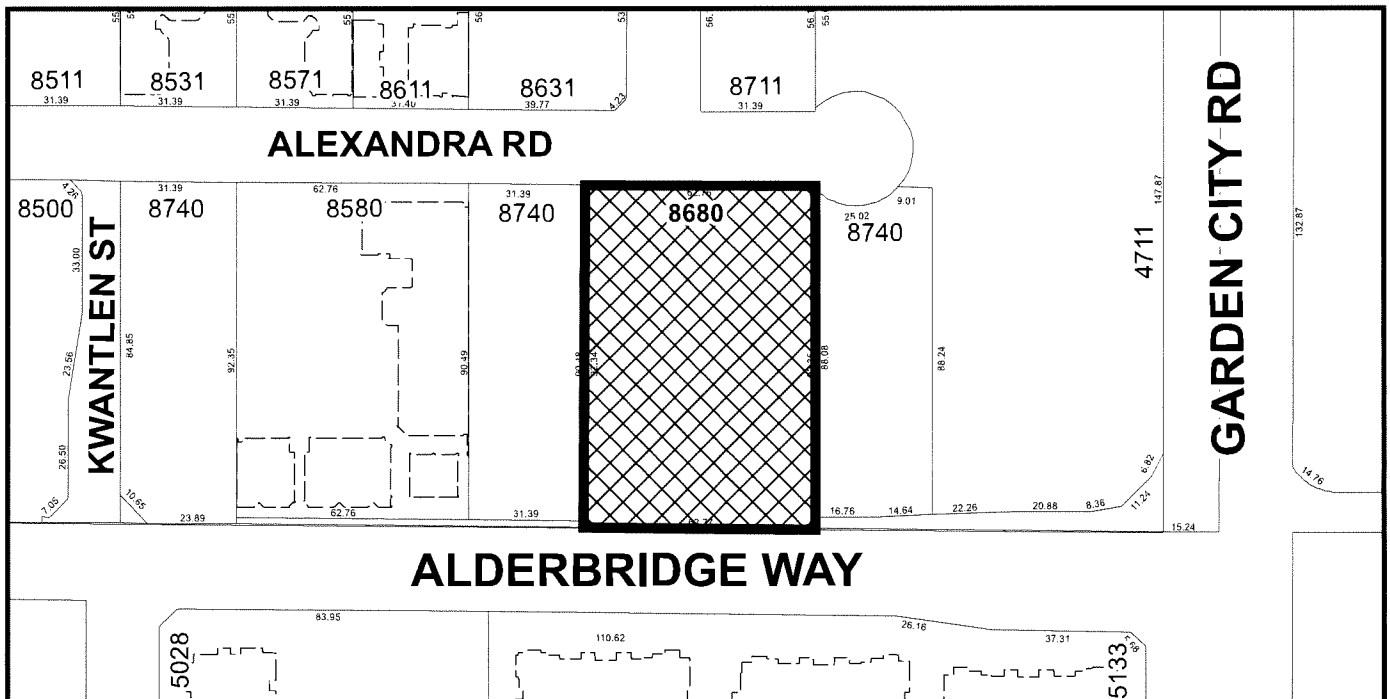
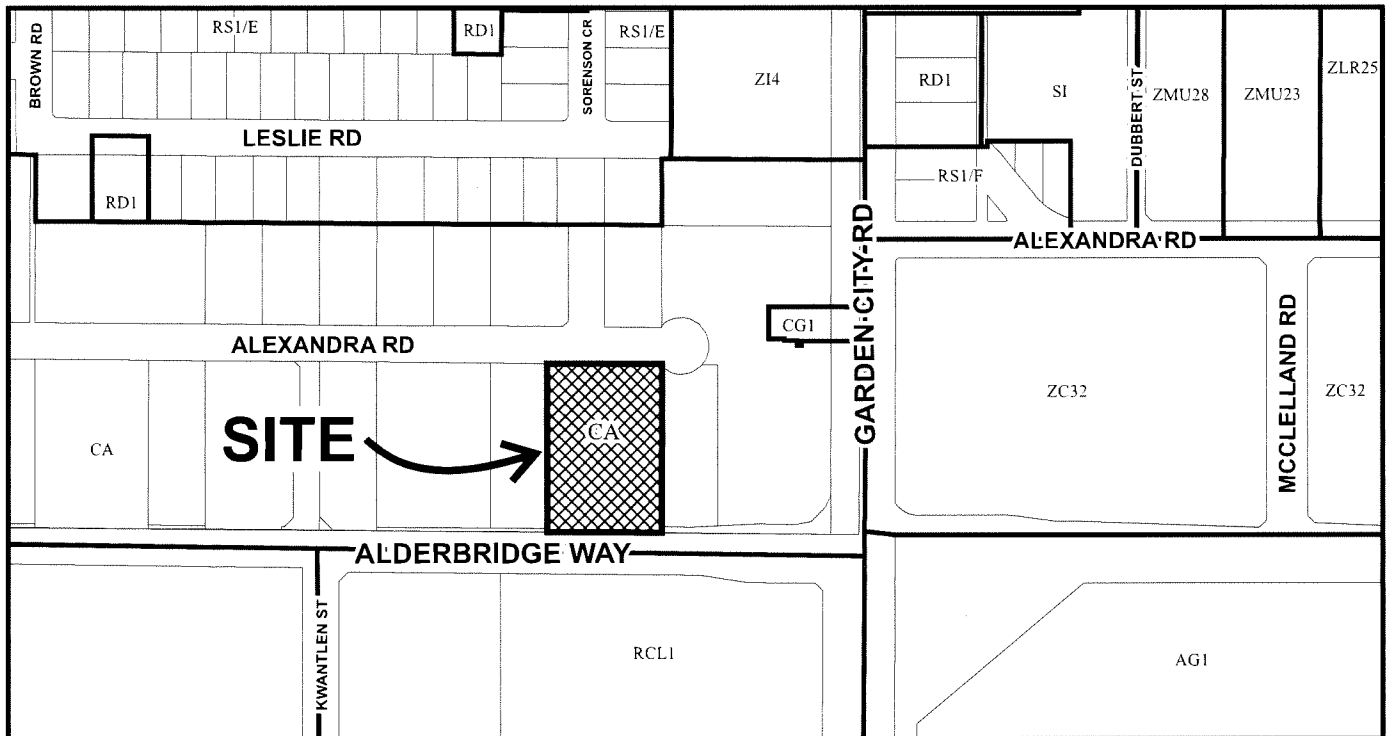
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- Att.
- 1: Location Maps
 - 2: Development Application Data Sheet
 - 3: Advisory Design Panel Meeting Minutes
 - 4: Development Permit Considerations



City of Richmond

Attachment 1



DP 23-031545

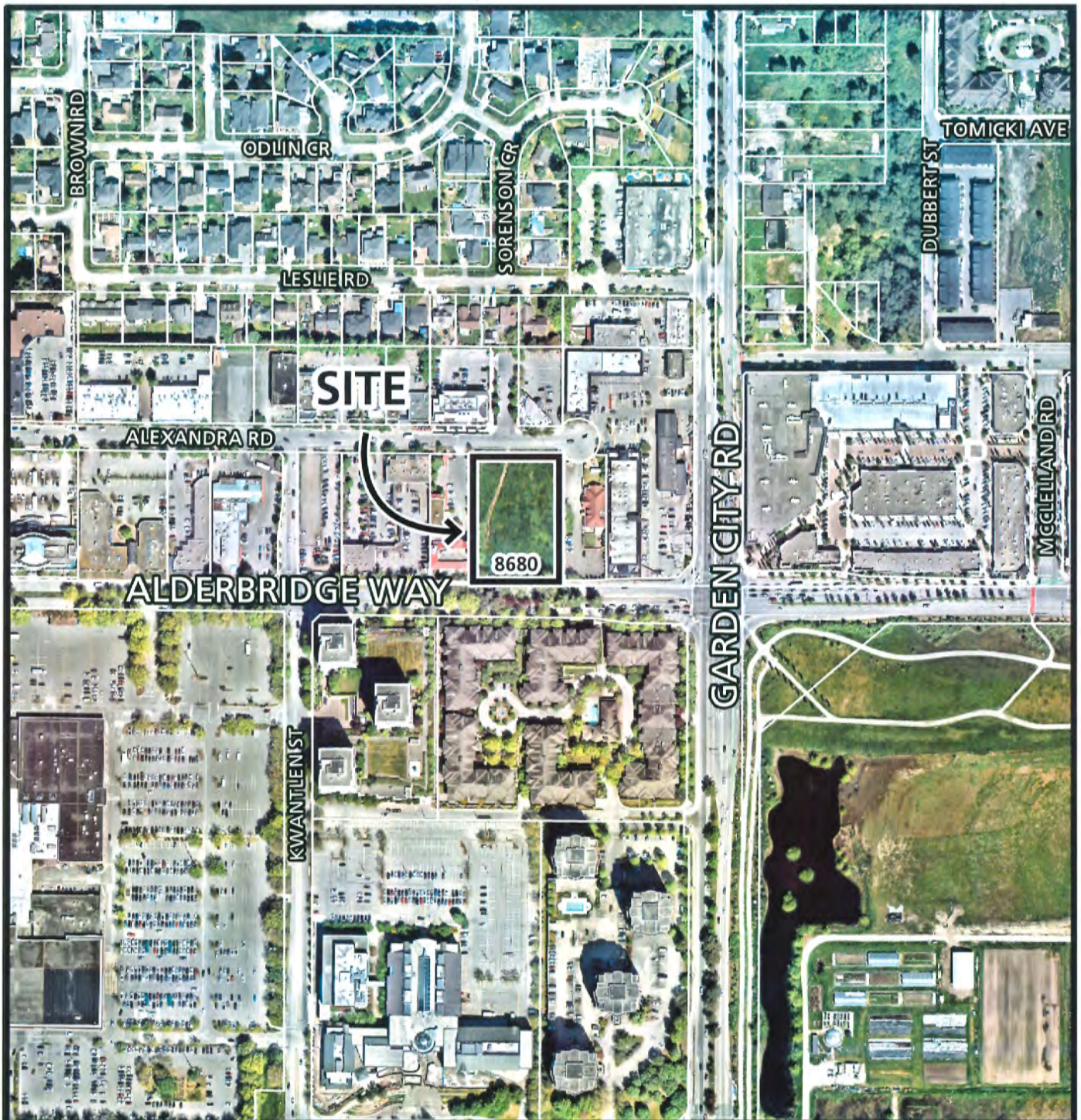
Original Date: 11/22/23

Revision Date:

Note: Dimensions are in METRES



City of
Richmond



DP 23-031545

Original Date: 11/06/25
Revision Date:

Note: Dimensions are in METRES



DP 23-031545

Attachment 2

Address: 8680 Alexandra Road

Applicant: Carson Lei

Owner: Carson Lei

Planning Area(s): City Centre - Aberdeen Village

Floor Area Gross: 1,529.8 m² (16,467 ft²) Floor Area Net: 1,376.9 m² (14,821 ft²)

	Existing	Proposed
Site Area:	5,795.9 m ² (62,387 ft ²)	Lot A: 2,758.2 m ² (29,689 ft ²) Lot B: 2,758.3 m ² (29,690 ft ²)
Land Uses:	Vacant	Lot A: No Change Lot B: Commercial, Retail, Restaurant
OCP Designation:	Commercial	No Change
Zoning:	Auto-Oriented Commercial (CA)	No Change
Number of Lots:	1	2
Number of Units:	0	Lot A: 0 Lot B: 14

	Bylaw Requirement	Proposed (Lot B)	Variance
Floor Area Ratio:	0.5	0.5	None Permitted
Lot Coverage:	Max. 50%	32%	None
Setback – Front Yard (North):	Min. 3.0 m	3.2 m	None
Setback – Side Yard (East):	Min. 3.0 m	3.0 m	None
Setback – Rear Yard (South):	Min. 3.0 m	2.0 m	Variance Requested
Setback – Side Yard (West):	Min. 3.0 m	1.5 m	Variance Requested
Height (m):	Max. 12 m	11.52 m	None
Off-street Parking Spaces – Regular/Small:	Regular: 40 Small: max 50%	Regular: 20 Small: 18 (45%)	None
Off-street Parking Spaces – Accessible:	Min. 2% (2)	2	None
Total off-street Spaces:	40 with TDMs	40 with TDMs	None
On-Site Loading Space	Medium Loading: 1	Medium Loading: 1	None
Bicycle Parking Spaces:	Class 1: 4 Class 2: 5	Class 1: 6 Class 2: 8	None

Excerpt from the Minutes from
The Design Panel Meeting

Thursday, September 4, 2025 – 4:00 p.m.
Remote (Teams) Meeting

**DP 23-031545 - COMMERCIAL DEVELOPMENT (TWO BUILDINGS) IN THE CITY
CENTRE AREA PLAN**

ARCHITECT: Pacific West Architecture
LANDSCAPE ARCHITECT: Homing Landscape Architecture
PROPERTY LOCATION: 8680 Alexandra Road

Applicant's Presentation

Architect Patrick Yang, Pacific West Architecture, Inc., and Landscape Architect Eason Li, Homing Landscape Architecture, presented the project and answered queries from the Panel.

Panel Discussion

Comments from Panel members were as follows:

- appreciate the applicant's presentation of the project; *Noted with thanks.*
- consider flipping Building 1 on Lot B and shift the building to the east side of the property so that the front rather than the back of the building would be facing the front of the future building on Lot A; this would create a more open space for Lot A, provide a better view of Building 1 on Lot B from Lot A, resolve concerns regarding the west elevation of Building 1, and eliminate CPTED issues on the west edge and the narrow pedestrian pathway along the west property line; consider installing a transparent fence to help address the grade change along the west property line; ***Flipping Building 1 was considered but the original design has been retained for the following reasons:***
 - ***Elevation design has been refined to activate the west building face in the interim and in the future. If building 1 isn't back facing Lot A, it's back will be facing neighbor 8740 Alexandra site on the other side. It is hard to say Lot A is more important than the 8740 lot. The location of the SRW between proposed Lot A and B provide the current orientation of Building 1 with opportunities for additional glazing along the west side of the building face, which has now been provided.***

- *The exterior design of the west elevation is significantly improved, including larger window areas, placement of continuous weather protection, roof overhang, and signages, more variable materials and colours. All of this will contribute to a more welcoming and vivid west elevation design, and a better pedestrian experience. Secondary entries for those units have been provided.*
- *Lot B has been designed to enhance the required SRW which bisects the site. If the site design was to be flipped, it will be awkward to have the pedestrian walkway between the two parking areas and will cause safety issues for pedestrians.*
- *The current design has also taken into consideration the neighbouring trees to east. Reorienting the building would likely require their removal or substantial regrading, which would negatively affect the current site ecology and the interface with the neighbouring property.*
- consider eliminating the break on the canopy above the second floor terrace of Building 1 to provide a continuous canopy that matches the continuous terrace; *A continuous canopy now wraps around the building perimeter and the top of the whole first floor is on the same level. Refer to the updated building elevation and 3D.*
- clarify the intended function/use of the roof deck on the south side of the second floor of Building 1 to avoid the space being used as a storage area for CRUs; *This roof deck is now a public patio on the same level of the 2nd floor, which will be accessible for all people. The adjacent mechanical roof and equipment are physically and visually separated from the patio by a continuous louver screen, ensuring that the two uses operate independently without conflict in function, access, or views. In addition, planting is introduced between the two areas to provide a soft green buffer, further enhancing visual comfort and contributes to the overall quality of the outdoor environment.*
- simplify the use of materials for Building 1 to differentiate the first floor from the second floor of the building; the teak coloured architectural panel backdrop for the CRU signage is not necessary as it would break up the building façade too much; *The panels that backdrop for the CRU signage are removed to maintain a linearly well articulated and clean expression. The overall colour and materials have been simplified while continuing to provide articulation.*
- the use of the teak coloured architectural panel in Building 2 is underdeveloped as it stops in the middle of the east and west elevations; *Teak coloured panels are removed. The design strategy of Building 2 is now similar with Building 1.*

- consider reducing the width of Building 2 to allow for wider pedestrian pathways along both sides of the building for pedestrians coming from Alderbridge Way; consider mimicking the width and design of the more successful east pedestrian walkway for the west pedestrian walkway; ***The path and landscape strip east of building 2 have been swapped to provide a landscape buffer between the subject site and the neighbouring property. Building 2's width has been reduced to provide a 8' wide pathway west of Building 2, creating a safer, more welcoming and visually prominent entrance to the public walkway.***
- consider installing a green roof on Building 2 to enhance the site's stormwater management and to provide visual interest to neighbouring taller buildings; ***At grade landscaping has been further refined. Although a green roof has advantages of improving stormwater management and creating long-term environmental and ecological value, it brings a lot of potential problems such as regular check and maintenance, risks of water leakage and damage, much heavier load on roof structure. Instead, both Building 1 and Building 2 have been designed with pre-ducting to support future solar PV rooftop installations, in alignment with the city's energy initiatives.***
- consider making the roof deck patio accessible by increasing the height of the mechanical and garbage and recycling rooms attached to Building 1 to match the height of the ground floor CRUs; ***Roof deck now has the same elevation as the CRUs of the second floor.***
- investigate ways to provide a better view of Building 1 on Lot B from the future development on Lot A including flipping Building 1 on Lot B; would prefer flipping Building 1 on Lot B as the existing trees along the east property line would soften the east edge and improve the visibility of the CRUs' signage in Building 1 from Alexandra Road; also consult with the City regarding feasible options for the site's vehicular entrance on Alexandra Road; ***Building 1 remains the same location but a range of design improvements have been implemented to enhance the west elevation, including larger window areas, continuous weather protection canopies, extended roof overhangs, varied materials and colors, and improved CRU signage visibility. These updates collectively create a more articulated, welcoming, and pedestrian-friendly west façade while maintaining the building's original orientation.***
- the roof deck patio on top of the mechanical and garbage and recycling rooms poses a CPTED concern as it is difficult to provide surveillance from the ground or from the second floor CRUs; a building section shown on the package provided by the applicant indicates that the patio is on the same floor level as the second level CRUs; consider following this section plan to provide accessibility to the patio from adjacent CRUs and enhance its usability; ***The roof deck has now been raised to be on the same elevation as the CRUs of the second floor.***

- review the position, location and width of some of the doors of the CRUs in relation to the sidewalk and adjacent pedestrian travel paths, particularly on Level 2 of Building 1, as the proposed outward swinging doors of CRU units are pinching the accessible pedestrian path of travel; considering inseting these doors to provide a more accessible pedestrian path of travel and improve the elevation and rhythm of entrances along publicly accessible façades; *Doors are now swinging inward. Based on BCBC 3.3.1.11(3), doors forming part of a means of egress are only required to swing in the direction of exit travel when the area they serve has an occupant load exceeding 60 persons. The largest CRU in this project has an area of 123.3 m², resulting in an occupant load of approximately 33 persons (based on 3.7 m²/person per Table 3.1.17.1 of BCBC). Therefore, all CRU entry and rear doors are permitted to swing inward without compromising life safety or code compliance.*
- consider installing wheel stops for the outdoor parking stalls on the north side of Building 2 to avoid parked cars encroaching into the adjacent narrow sidewalk and accessible pedestrian pathway; widening the sidewalk would also be helpful to enhance accessibility on the site; *Wheel stops will be installed, please refer to the site plan. The width of the pathway to the north of Building 2 remains 5' (1.5 m), consistent with the minimum accessible clear width of 1.5 m noted in the DP guidelines.*
- the use and placement of teak coloured architectural panels on the east and west elevations of Building 2 and the back of Building 1 appears random; provide more clarity to the intention of their use/placement in relation to the doors to improve the treatment of these elevations; *The panels that backdrop for the CRU signage are removed to maintain a linearly well articulated and clean expression. Now the overall color is simplified but clearly differentiate the first and second floor. Warm, related tones differentiate the two floors while keeping a cohesive look. Composite panels are used for easy installation and maintenance, with smooth finishes below and corrugated above to add subtle texture and improve street-level articulation.*
- consider using different colours or treatment for the staircases to highlight their locations; *The staircases are open and finished in light grey to contrast with the façade. Spatial lighting has been installed on every wall, ensuring full illumination with no dark corners, including behind the stairs, enhancing visibility and safety in line with CPTED principles.*
- considering identifying a location for grease traps to avoid grease coming from on-site restaurant operations going into the surface parking area; *An underground grease interceptor location has been identified on the site plan. Future grease interceptors for restaurant kitchen fixtures will be contained within each unit.*

- the pedestrian route along the western edge of the site is narrow and feels like the backside of the building; considering installing a gate on either side to address this CPTED concern; ***The design of the west building elevation is significantly improved to provide a safer and nicer pedestrian experience. Larger windows and well-positioned doors have been introduced to increase transparency and visual connectivity between the pedestrian route and the commercial units. These improvements improve sightlines, natural surveillance, and overall legibility of the space.***
- provide clarity on the use cementitious panels as they are not shown on the drawings with any EasyTrim reveal which is preferable than having aluminum trims; ***EasyTrim reveal lines are provided and shown on building elevation and 3D.***
- agree with the Panel comment that the planning and layout of the site needs further consideration; flipping Building 1 might conflict with existing trees but overall, would contribute to creating an improved urban environment that would benefit Lot A and Lot B, and provide an opportunity for a contiguous surface parking area and an integrated stormwater management for both lots; ***Building 1's location remains the same as before, but additional design enhancements have been made to achieve better site and building design outcomes, including larger window areas, continuous weather protection canopies, extended roof overhangs, varied materials and colors, and improved CRU signage visibility. These updates collectively create a more articulated, welcoming, and pedestrian-friendly west façade while maintaining the building's original orientation***
- the use and placement of materials looks segmented; the smaller teak coloured architectural panels over the canopy above the unit entrances does not match the flat roof design of the building; consider a more cohesive and contemporary design with simpler lines; review the use and placement of materials and colours (e.g. the use of lighter colour for the upper level and darker colour for the lower level) on Building 1; also consider a continuous canopy without breaks at the stairs above the CRUs on the second floor of Building 1; ***The panels that backdrop for the CRU signage are removed to maintain a linearly well articulated and clean expression. CRU signage will be subject to a separate permitting process. Now the overall color is simplified, but the first and second floor are clearly differentiated by colour.***

- appreciate the courtyard feel created between Building 1 and Building 2; however, it is counter to the pedestrian pathway adjacent to the planter along the east side of Building 2; should Building 1 be flipped, the pedestrian pathway along the west side would provide a stronger connection from Alderbridge Way to Alexandra Road through the alley of trees; consider utilizing the pedestrian path between Lots A and B to act as a median and landscaped for stormwater management; *The building 1 location remains the same but it has a more welcoming and functional design along the west elevation, including larger window areas, continuous weather protection canopies, extended roof overhangs, varied materials and colors, and improved CRU signage visibility. Considering the required pathway, parking space length, and drive aisle width, there is insufficient space to provide an alley of trees as recommended. Instead, the pedestrian experience from Alderbridge Way has been improved by reducing the width of building 2 to provide a landscape strip adjacent to the west side of building 2 for a wider, more open pedestrian experience.*
- consider raising the height of the patio on top of the mechanical and garbage recycling rooms to match the height of the terrace for the second floor CRUs so it can be accessed via the elevator servicing the second floor CRUs; the landscaped patio will provide a nice outdoor amenity for the CRUs and enhance their rentability; *Roof deck now has the same elevation as the CRUs of the second floor.*
- reconsider the proposed six-foot tall solid fence along the east property line as it appears harsh and abrupt without any landscape buffer; consider instead a more transparent fence that would incorporate CPTED principles for natural surveillance and allow visibility to low level CRU signage to enhance both the commercial and security aspects of the development; also investigate opportunities to create a median with planting on the surface parking area; *The 6' high fence is changed to be 4' high metal picket fence for better transparency, please refer to landscape plan L1.0 and detail 5 on L3.0. Proposed landscaping has maximized the opportunities of creating planting beds on the parking area. A lot of native and low-maintenance plants are used in the planting beds to ensure the planting beds on the parking area can be easily maintained in a good condition.*
- the proposed development is well-planned in terms of functionality, but the site plan appears geared more for vehicle parking than a pedestrian-oriented development; the landscaping along pedestrian routes needs to be improved to enhance the pedestrian experience on the site; and *The south entrance of the main public walkway along the west property line has been widened to 8', creating a more welcoming and visually prominent pedestrian entrance. The more than 4' wide planting bed between the public walkway and the surface parking area is composed of trees, and decorative and native shrubs, which creates a safe, pleasant and comfortable walking experience.*

- consider more articulation/details to the building elements, e.g. the guardrails, overhangs and canopies, to enhance the overall quality and feel of the buildings considering their simple massing; also review the design and placement of signage for CRUs to make them more pedestrian oriented. *The continuity of the guardrails, canopies, and roofs have been improved in relation to the pattern of finish materials, windows, and signages. Together, they represent a stronger linear rhythm for the overall look of the building facades.*

Panel Decision

It was moved and seconded

That DP 23-031545 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Advisory Design Panel.

CARRIED

Opposed: Winston Chong



Development Permit Considerations

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 8680 Alexandra Road

File No.: DP 23-031545

Prior to approval of the Development Permit, the developer is required to complete the following:

1. **(Road Dedication – Alderbridge Way and Alexandra Road)** Voluntary dedication of land along the subject site's south property line (Alderbridge Way) and a corner cut on Alexandra Road to facilitate frontage improvements. Exact road dedication shall be confirmed through design of the Servicing Agreement* works. Approximate road dedications are as follows:
 - a) Along Alderbridge Way: 4.5 m wide along the entire south frontage.
 - b) Alexandra Road: 1 m x 1m corner cut at the northeast corner of the site.
2. **(Public-Right-of-Passage SRW)** Granting of a 3.0 m wide Statutory Right-of-Way for Public-Right-of-Passage, bisecting the property north to south. 1.5 m is to be provided along the east property line of proposed Lot A and 1.5 m is to be provided along the west property line of proposed Lot B. Construction and maintenance is to be conducted by the respective owners of each future lot at the owners' cost. The agreement should also identify that the fencing along the west property line of Lot B is to be removed by the owner of Lot B upon completing construction of the portion of the pedestrian path on Lot A.
3. **(Landscape Security)** Receipt of a Letter of Credit for landscaping in the amount of \$249,623.00 (based on the costs estimate provided by a CSLA registered landscape Architect including 10% contingency).
4. **(City Tree Compensation)** City acceptance of the developer's voluntarily cash contribution of \$19,192.00, as determined by Parks Services, to the City's Tree Compensation Fund for the removal of ten City trees (tag# 5-14).
5. **(Tree Protection)** Installation of appropriate tree protection fencing around all trees (tag #3-4) to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
6. **(Arborist Contract)** Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
7. **(Aircraft Noise – Indemnity)** Registration of an aircraft noise indemnity covenant on title (Area 1A).
8. **(Flood Indemnity)** Registration of a flood indemnity covenant on title.
9. **(CCAP Future Development Impacts)** Registration of a legal agreement on title stipulating that the development is subject to potential impacts due to other development that may be approved within the City Centre including without limitation, loss of views in any direction, increased shading, increased overlook and reduced privacy, increased ambient noise and increased levels of night-time ambient light, and requiring that the owner provide written notification of this through the disclosure statement to all initial purchasers, and erect signage in the initial sales centre advising purchasers of the potential for these impacts.
10. **(Glazing)** Registration of a legal agreement on Title limiting window covering, tinting, and frosting on the west façade of Building 1 to maintain transparency and views onto the adjacent west pedestrian path.
11. **(Sustainability Measures)** Registration of a legal agreement on Title ensuring the development provides the following sustainability measures, to the satisfaction of the Director, Building Approvals:
 - a) Energy reducing light sensors for building interior lighting.
 - b) Water conservation features including water conserving plumbing fixtures and high efficiency irrigation.
 - c) Rooftop pre-ducting for solar photovoltaic or other alternative energy system.

12. **(Transportation Demand Management)** City's acceptance of the developer's offer to voluntarily contribute towards various transportation-related improvements and secure parking in compliance with the Zoning Bylaw requirements and Transportation Demand Management (TDM) parking reductions.
- a) Additional bicycle parking – Registration of a legal agreement on Title to ensure the provision of a minimum 6 Class 1 and 8 Class 2 bicycle parking spaces on site. Facilities to be constructed and maintained by the owner.
 - b) E-bike Parking – Registration of a legal agreement on Title to ensure the provision and maintenance of one publicly accessible energized outlet within the Class 1 bicycle parking area. Facilities to be constructed and maintained by the owner.
 - c) Electrical (EV) Charging – Registration of legal agreement(s) on the subject property requiring that the owner provides, installs, and maintains Level 2 electrical vehicle (EV) charging infrastructure for a minimum of 22 spaces on the property for the use of the building's tenants and visitors.
13. **(DEU)** Registration of a restrictive covenant and statutory right of way and/or alternative legal agreement(s), to the satisfaction of the City, securing the owner's commitment to connect to District Energy Utility (DEU) and granting the statutory right of way(s) necessary for supplying the DEU services to the building(s), which covenant and statutory right of way and/or legal agreement(s) will include, at minimum, the following terms and conditions:
- a) No building permit will be issued for a building on the subject site unless the building is designed with the capability to connect to and be serviced by a DEU and the owner has provided an energy modelling report satisfactory to the Director of Engineering.
 - b) A City Centre DEU Bylaw, which applies to the subject site, has been adopted by Council and was adopted prior to the issuance of any development permit for the subject site; however, no building permit will be issued for a building on the subject site unless:
 - (1) the City receives a professional engineer's certificate stating that the building has the capability to connect to and be serviced by a DEU;
 - (2) the owner designs, to the satisfaction of the City and the City's DEU service provider, Lulu Island Energy Company Ltd. (LIEC), a low carbon energy plant to provide any combination of heating, cooling, and/or domestic hot water heating to the building(s), as directed by the City's service provider (LIEC), to be constructed and installed on the site, with the capability to connect to and be serviced by a DEU;
 - (3) The owner enters into a Service Provider Agreement for that building with the City and/or the City's DEU service provider, LIEC, executed prior to depositing any Strata Plan with LTO and on terms and conditions satisfactory to the City; and
 - (4) the owner enters into an asset transfer agreement with the City and/or the City's DEU service provider on terms and conditions satisfactory to the City to transfer ownership of the low carbon energy plant to the City or as directed by the City, including to the City's DEU service provider, at no cost to the City or City's DEU service provider, LIEC, on a date prior to final building inspection permitting occupancy of the first building on the site. Such restrictive covenant and/or asset transfer agreement shall include a warranty from the owner with respect to the on-site DEU works (including the low carbon energy plant) and the provision by the owner of both warranty and deficiency security, all on terms and conditions satisfactory to the City;
 - c) The owner agrees that the building(s) will connect to a DEU when a DEU is in operation, unless otherwise directed by the City and the City's DEU service provider, LIEC.
 - d) If a DEU is available for connection and the City has directed the owner to connect, no final building inspection permitting occupancy of a building will be granted unless, and until:
 - (1) the building is connected to the DEU;
 - (2) the owner enters into a Service Provider Agreement for that building with the City and/or the City's DEU service provider, LIEC, executed prior to depositing any Strata Plan with LTO and on terms and conditions satisfactory to the City; and
 - (3) prior to subdivision (including Air Space parcel subdivision and Strata Plan filing), the owner grants or acquires, and registers, all Statutory Right-of-Way(s) and/or easements necessary for supplying the DEU services to the building.

Initial: _____

- e) If a DEU is not available for connection, no final building inspection permitting occupancy of a building will be granted until:
- (1) the City receives a professional engineer's certificate stating that the building has the capability to connect to and be serviced by a DEU; and
 - (2) the owner grants or acquires any additional Statutory Right-of-Way(s) and/or easements necessary for installing and constructing DEU infrastructure and supplying DEU services to the building, registered prior to any subdivision of the subject site (including Air Space parcel subdivision and strata plan filing).
14. **(Fees)** Payment of all fees in full for the cost associated with the Development Permit Panel Meeting Notices, consistent with the City's Consolidated Fees Bylaw No 8636.

Prior to Subdivision, the developer must complete the following requirements:

1. **(Road Dedication)** Dedication of Road Parcel (Plan 50405).
2. **(Fees)** Payment of fees required prior to subdivision approval, including but not limited to
 - a) Payment of current years' property taxes. If approval is sought on or after September 1st in any year, payment of the following year's estimates taxes per City practice.
 - b) Payment of address assignment fee.
3. Enter into a Servicing Agreement* for the design and construction of the following works. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to:
 - a) **Water Works:**
 - (1) Using the OCP Model, there is 225 L/s of water available at a 20 psi residual at the Alexandra Road frontage. Based on your proposed development, your site requires a minimum fire flow of 200 L/s.
 - (2) At Developer's cost, the Developer is required to:
 - (a) Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage building designs.
 - (b) Review hydrant spacing on all road frontages and install new fire hydrants as required to meet City spacing requirements for the proposed land use.
 - (c) Provide rights-of-ways for the water meters. Minimum right-of-way dimensions to be the size of the meter box (from the City of Richmond supplementary specifications) + any appurtenances (for example, the bypass on W2n-SD) + 0.5 m on all sides. Exact right-of-way dimensions to be finalized via the servicing agreement process.
 - (3) At Developer's cost, the City will:
 - (a) Complete all tie-ins for the proposed works to existing City infrastructure.
 - (b) Cut, cap & remove the existing water service connections and meters.
 - (c) Install two new water service connections off of the existing water main fronting Alexandra Rd complete with water meters to service the proposed subdivision.
 - b) **Storm Sewer Works:**
 - (1) At Developer's cost, the Developer is required to: Provide an erosion and sediment control plan for all on-site and off-site works, to be reviewed as part of the servicing agreement design
 - (2) At Developer's cost, the City will:
 - (a) Complete all tie-ins for the proposed works to existing City infrastructure.
 - (b) Cut, cap & remove the existing storm service connection and ICs
 - (c) Install two new storm service connections on Alexandra Road complete with inspection chambers to service the proposed development.
 - c) **Sanitary Sewer Works:**

Initial: _____

- (1) At Developer's cost, the Developer is required to: Confirm the condition and capacity of the existing sanitary service connections at the northwest and northeast corners of 8680 Alexandra Road through CCTV inspection and sizing calculations. Reuse the existing sanitary connections if they are in proper condition and has adequate capacity. If they are not in proper conditions and do not have adequate capacity, install two new service connections (by City crews at developer's cost) complete with IC to service the proposed subdivision.
- (2) At Developer's cost, the City will:
 - (a) Complete all tie-ins for the proposed works to existing City infrastructure.
 - (b) If the existing sanitary connections at the northwest and northeast corners of 8680 Alexandra Rd are not in proper condition and do not have adequate capacity, cut, cap & remove existing sanitary service connections and ICs at the northwest and northeast corners of the site.

d) Street Lighting:

- (1) At Developer's cost, the Developer is required to: Review street lighting levels along all road and lane frontages, and upgrade as required.

e) Frontage Improvements:

- (1) At Developer's cost, the Developer is required to construct the following along the full frontage of the subject site:
 - (a) Along Alderbridge Way, from North to South, starting at the new south property line:
 - (i) Minimum 3.5 m wide multi-use pathway
 - (ii) Minimum 1.5 m boulevard with street trees
 - (iii) 0.15 m curb and gutter to match the location of the existing curb and gutter.
 - (b) Along Alexandra Rd, from South to North, starting at the North property line:
 - (i) Minimum 2.0 m wide sidewalk
 - (ii) Minimum 1.5 m boulevard with street trees
 - (iii) 0.15 m curb and gutter.
 - (c) New frontage improvements are to be transitioned to existing treatments to the East/West of the site.
- (2) At Developer's cost, the Developer is required to construct a 1.5 m pathway along the eastern half of the SRW PROP that bisects the subject property.

f) General Items:

- (1) At Developer's cost, the Developer is required to:
 - (a) Coordinate with BC Hydro, Telus and other private communication service providers:
 - (i) To pre-duct for future hydro, telephone and cable utilities along all road frontages.
 - (ii) Before relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
 - (b) Locate/relocate all above ground utility cabinets and kiosks required to service the proposed development and proposed undergrounding works, and all above ground utility cabinets and kiosks located along the development's frontages, within the developments site (see list below for examples). A functional plan showing conceptual locations for such infrastructure shall be included in the development design review process. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements (e.g., statutory right-of-way dimensions) and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of statutory right-of-ways that shall be shown on the architectural plans/functional plan, the servicing agreement drawings, and registered prior to SA design approval:
 - BC Hydro PMT – 4.0 x 5.0 m
 - BC Hydro LPT – 3.5 x 3.5 m
 - Street light kiosk – 1.5 x 1.5 m

Initial: _____

- Traffic signal kiosk – 2.0 x 1.5 m
 - Traffic signal UPS – 1.0 x 1.0 m
 - Shaw cable kiosk – 1.0 x 1.0 m
 - Telus FDH cabinet – 1.1 x 1.0 m
- (c) Provide, prior to start of site preparation works or within the first servicing agreement submission, whichever comes first, a preload plan and geotechnical assessment of preload, dewatering, and soil preparation impacts on the existing utilities fronting the development site and provide mitigation recommendations.
- (d) Provide a video inspection report of the existing sanitary connection at the northeast and northwest corners of 8680 Alexandra Rd prior to start of site preparation works or within the first servicing agreement submission, whichever comes first. A follow-up video inspection, complete with a civil engineer's signed and sealed recommendation letter, is required after site preparation works are complete (i.e. pre-load removal, completion of dewatering, etc.) to assess the condition of the existing utilities and provide recommendations to retain, replace, or repair. Any utilities damaged by the pre-load, de-watering, or other ground preparation shall be replaced or repaired at the Developer's cost.
- (e) Submit a proposed strategy at the building permit stage for managing excavation de-watering. Note that the City's preference is to manage groundwater onsite or by removing and disposing at an appropriate facility. If this is not feasible due to volume of de-watering, the Developer will be required to apply to Metro Vancouver for a permit to discharge into the sanitary sewer system. If the sanitary sewer does not have adequate capacity to receive the volume of groundwater, the Developer will be required to enter into a de-watering agreement with the City wherein the developer will be required to treat the groundwater before discharging it to the City's storm sewer system.
- (f) Not encroach into City rights-of-ways with any proposed trees, retaining walls, or other non-removable structures. Retaining walls proposed to encroach into rights-of-ways must be reviewed by the City's Engineering Department.
- (g) Enter into, if required, additional legal agreements, as determined through the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
2. Incorporation of accessibility measures in Building Permit (BP) plans as determined via the Development Permit processes.
3. If applicable, payment of latecomer agreement charges associated with eligible latecomer works.
4. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

* This requires a separate application.

- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the

Initial: _____

Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- If the development will be constructed in phases and stratified, a Phased Strata Subdivision Application is required. Each phase of a phased strata plan should be treated as a separate parcel, each phase to comply with the Richmond Zoning Bylaw 8500 in terms of minimum lot area, building setback and parking requirements. Please arrange to have the City's Approving Officer review the proposed phased boundaries in the early DP stages. To allow sufficient time for staff review and preparation of legal agreements, the application should be submitted at least 12 months prior to the expected occupancy of development.
- If the development intends to create one or more air space parcels, an Air Space Parcel Subdivision Application is required. To allow sufficient time for staff review and preparation of legal agreements, the application should be submitted at least 12 months prior to the expected occupancy of development.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contains prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed

Date



No. DP 23-031545

To the Holder: Carson (Kaseng) Lei

Property Address: 8680 Alexandra Road

Address: 7551 Petts Road, Richmond, BC, V7A 1J9

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500", as applicable to proposed Lot B, is hereby varied to:
 - a) Reduce the minimum interior side yard setback from 3.0 m to 1.5 m; and,
 - b) Reduce the minimum rear yard setback from 3.0 m to 2.0 m.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #21 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$249,623.00. to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit

No. DP 23-031545

To the Holder: Carson (Kaseng) Lei

Property Address: 8680 Alexandra Road

Address: 7551 Petts Road
Richmond, BC, V7A 1J9

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

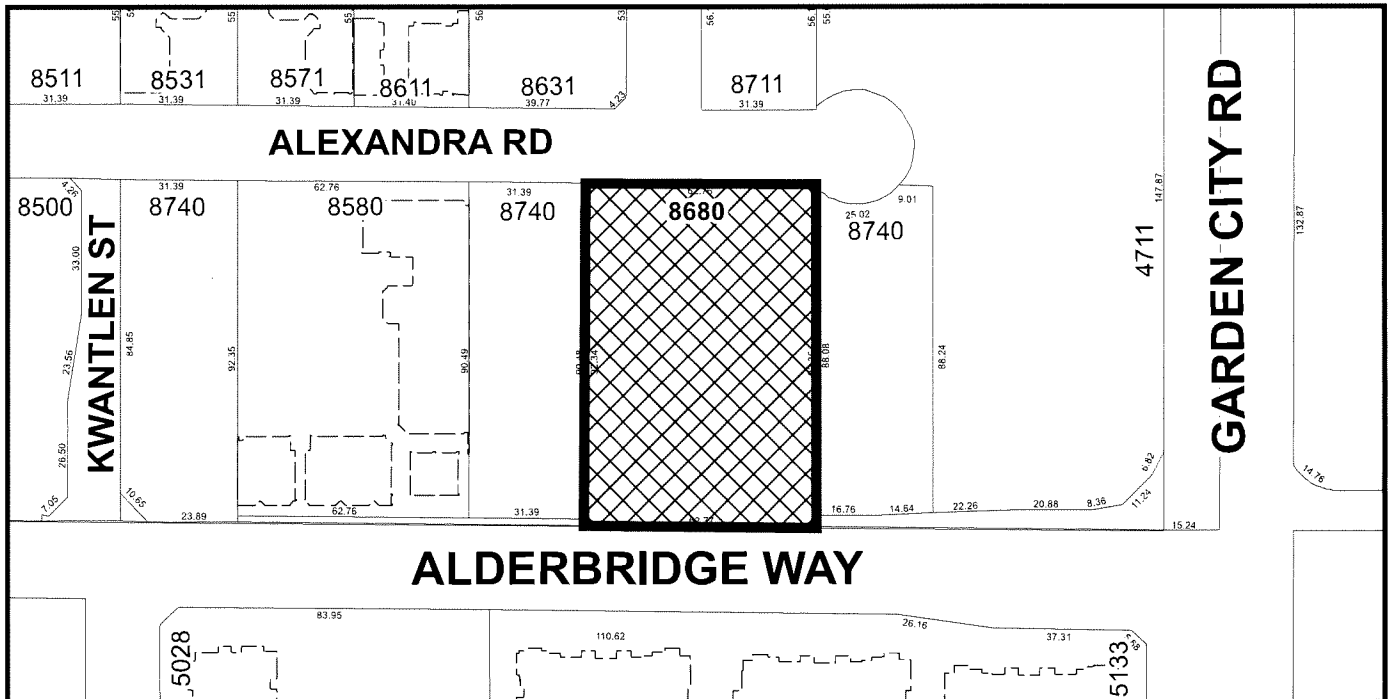
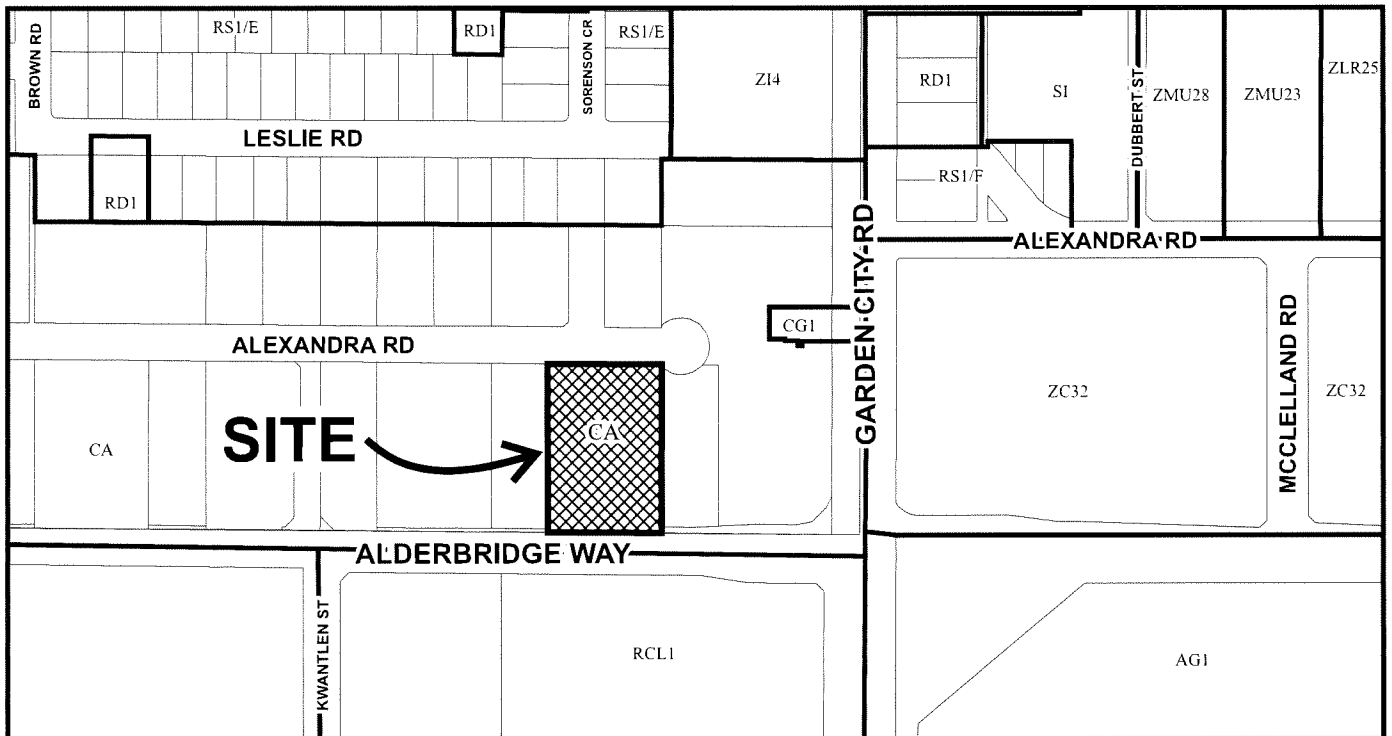
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond



DP 23-031545
SCHEDULE "A"

Original Date: 11/22/23

Revision Date:

Note: Dimensions are in METRES

NEW COMMERCIAL DEVELOPMENT

CIVIC ADDRESS: LOT B 8680 ALEXANDRA RD, RICHMOND BC

LEGAL DESCRIPTION: LOT 1 SEC 33 BLK 5N RG 6W PL EPP38189 LOT 1, BLOCK 5N, PLAN EPP38189, SECTION 33, RANGE 6 W, NEW WESTMINSTER LAND DISTRICT, & SEC 34



DESIGN TEAM

ARCHITECT

PACIFIC WEST ARCHITECTURE Inc.
1200 West 73rd Ave(Airport Square)
Suite 940, Vancouver B.C. V6P 6G5
Tel: (604)-558-3064
Email: info@pwaarchitecture.com

ENERGY

JRS ENGINEERING
300 - 4595 Canada Way
Burnaby B.C. V5G 1J9
Tel: (604)-320-1999
Email: info@jrsengineering.com

SURVEYOR

J.C. TAM AND ASSOCIATES
115 - 8833 Odlin Crescent
Richmond B.C. V6X 3Z7
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LANDSCAPE ARCHITECT

HOMING LANDSCAPE ARCHITECTURE
1423 W11th Avenue
Vancouver B.C. V6H 1K9
Tel: (778)-323-3536
Email: hominglandscape@gmail.com

MECHANICAL

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Unit 201 - 5489 Byrne Road
Burnaby B.C. V5J 3J1
Tel: (778)-323-1368
Email: stephen@liaengineering.com

CIVIL

PARC ENGINEERING Ltd.
200 - 100 Park Royal
West Vancouver B.C. V7T 1A2
Tel: (604)-765-7271
Email: nathan@parcengineering.ca

ELECTRICAL

WGW ENGINEERING Ltd.
2800 Douglas Road
Burnaby B.C. V5B 4R6
Tel: (604)-626-2930
Email: info@wggweng.com

ARBORIST

DAVEY RESOURCE GROUP
#200 - 8208 Swenson Way
Delta B.C. V4G 1J6
Tel: (604)-499-6568
Email: peter.rennie@davey.com

TRAFFIC

Innovative Traffic Engineering
101A – 1952 Kingsway Avenue
Port Coquitlam, BC, V3C 6C2
Tel: +1 (604) 936 6190 Ext:228
Email: achan@cts-bc.com

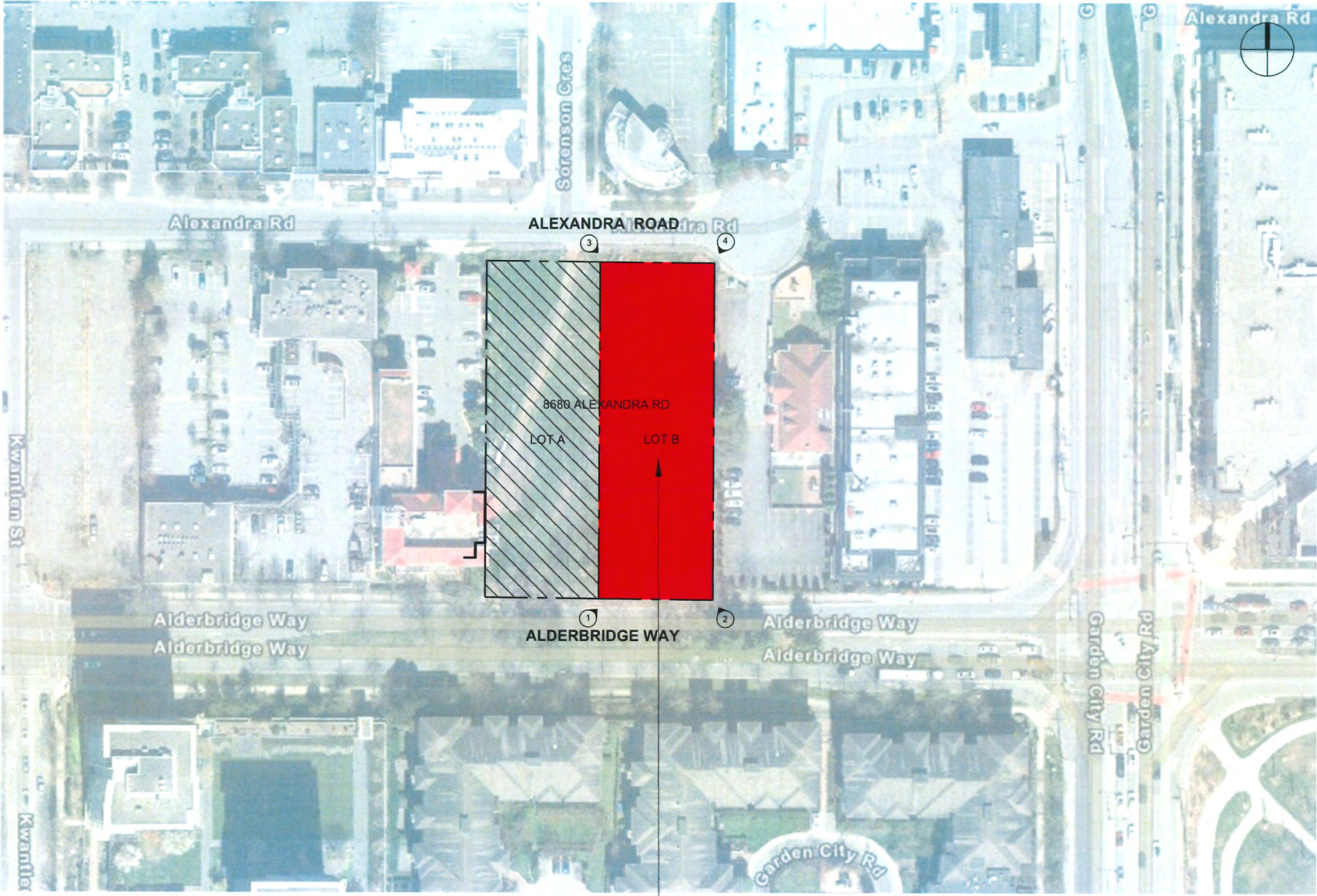
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A 005	FIRE ACCESS PLAN
A 006	LOT COVERAGE PLAN
A 007	LOT A POSSIBLE SITE PLAN
A 008	SHADOW STUDIES
A 101	BUILDING 1 FLOOR AND ROOF PLAN
A 102	BUILDING 2 FLOOR AND ROOF PLAN
A 103	FLOOR AREA OVERLAY
A 201	BUILDING 1 ELEVATIONS
A 202	BUILDING 1 ELEVATIONS
A 203	BUILD
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A 205	BUILD
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A 302	3D PERSPECTIVE
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A 400	MATERIAL BOARD

DP 23-031545
November 17, 2025
Plan #1



2025-11-04



1 LOCATION PLAN
Scale: N.T.S.

SUBJECT PROPERTY:
LOT B, 8680
ALEXANDRA RD.,
RICHMOND, BC



2 VIEW 1



3 VIEW 2



5 VIEW 4



**pacific
west
architecture**

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Office: 604 558 3064
Fax: 604 267 7056
Email: info@pwaarchitecture.com
www.pwaarchitecture.com



REVISIONS		
1		
ISSUES		DATE
8	ISSUED FOR DPP	NOV 04, 2025
7	REISSUED FOR DP APPLICATION / ADP MEETING	JUL 22, 2025
6	REISSUED FOR DP SUBMISSION	JUN 26, 2025
5	REISSUED FOR DP APPLICATION / ADP MEETING	APR 4, 2025
4	REISSUED FOR DP APPLICATION	JAN 10, 2025
3	REISSUED FOR DP APPLICATION	DEC 31, 2024
2	REISSUED FOR DP APPLICATION	SEP 9, 2024
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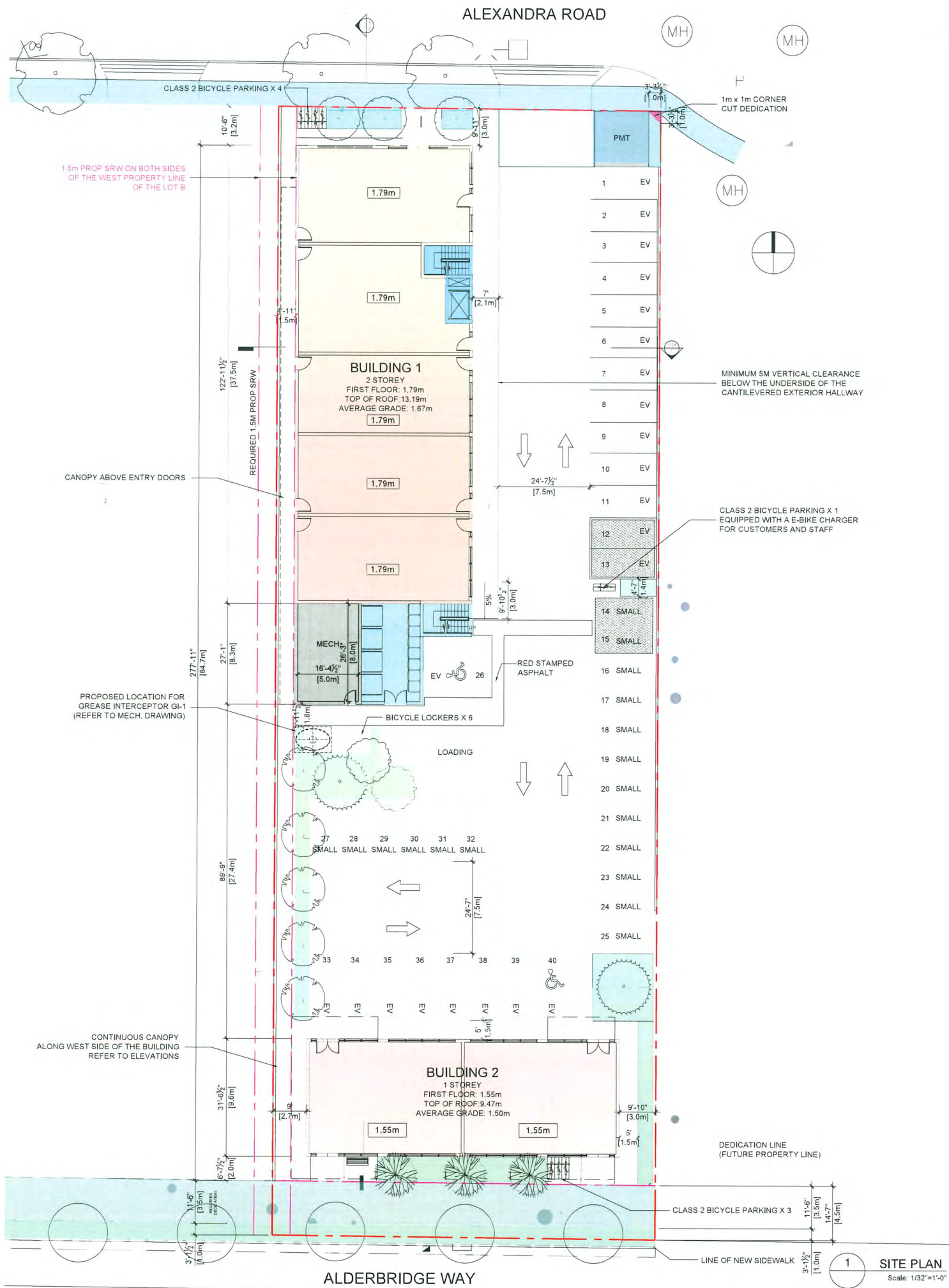
DP 23-031545
November 17, 2025
Plan #2

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

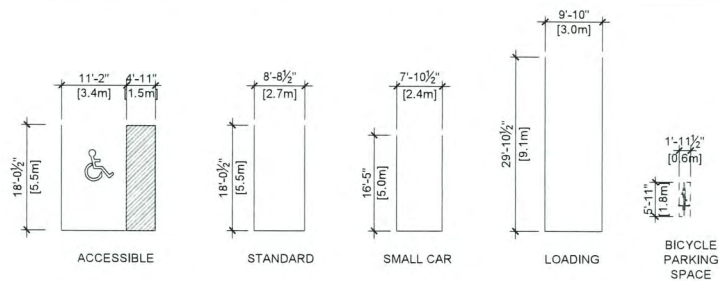
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SITE STREETVIEWS

DRAWING No
A002
PAGE 7



Zoning Analysis Table					
Civic Address	8680 ALEXANDRA RD				
Legal Description	LOT 1 SEC 33 BLK 5N RG 6W PL EPP38189 LOT 1, BLOCK 5N, PLAN EPP38189, SECTION 33, RANGE 6 W, NEW WESTMINSTER LAND DISTRICT, &				
Current Zoning	CA				
Lot B Area	2,898m ² (31,193.8 sq.ft)				
Proposed Zoning	CA				
CRITERIA	CA ZONE REQUIREMENTS		PROPOSED	VARIANCE	
Minimum Lot Area	no requirement (Zoning bylaw 10.3.8)		2,758.3m2(29,690 sq.ft) after dedication	None	
Height	12m (39.97 ft)		Building 1: 2 Storey, 11.52m Building 2: 1 Storey, 7.97m	None	
Front Yard (north)	3m (9.84 ft)		3.0m	None	
Rear Yard (south)	3m (9.84 ft)		2.0m	YES	
Side Yard (west)	3m (9.84 ft)		1.5m	YES	
Side Yard (east)	3m (9.84 ft)		3.0m	None	
Lot coverage	50%		Building 1: 641.7m2 Building 2: 246.4 m2 Total: 888.1 m ² /32%(after dedication)	None	
FAR	0.5		Building 1: first floor: commercial: 525.6 m ² (5658 sq.ft) elevator+stairways: 29.5 m ² (317 sq.ft) garbage room: 40 m ² (432 sq.ft) second floor: commercial: 525.6 m ² (5658 sq.ft) stairways: 9.7 m2 (104 sq.ft) Building 2: commercial: 246.4 m2 (2652 sq.ft) Total: 1376.9 m ² (14821 sq. ft) FAR: 0.50 (after dedication)	None	
Minimum Parking Requirements	First floor: 4.2 spaces per 100.0 m2 of gross leasable floor area 4.2 x 783.9 m2/100=32.92 Upper floor: General Retail 3.0 space per 100m2 of GFA, up to 350m2, plus 4.0 space for each additional 100m2, with 5% reduction 3.0 x 350m2/100 + 4.0 x(525.6-350)m2/100 x 95%=16.65 Total: 49.57 TDM reduction: 80% x 49.57=39.66	Total Required: 40	Regular size: 20 Small car: 18 Accessible: 2 Total: 40 22 parking spaces will have Lv 2 EV charge capacity	None	
Accessible Parking	min. 2 % when 11 or more		2	2	None
Parking Stall Dimension	Standard: 2.65m W x 5.5 m L (8.53 ft. W x 18.04 ft. L) Small: 2.4m W x 5m L (7.87 ft. W x 15.75 ft. L) Accessible: 2.5m W x 5.5m L (11.15 ft. W x 23.95 ft. L)+ Shared Aisle: 1.5m Accessible Van: 3.4m W x 5.5m L (11.15 ft. W x 23.95 ft. L)+ Shared Aisle: 1.5m		Small : max 50% when 31 or more	Small Car: 18 (45%)	None
Drive Aisle Width	7.5m		7.5m		None
Loading requirement	1 Medium size on-site designated		1 Medium size on-site designated		None
Loading space size	Medium: 9.1m L x 3m W x 3.8m H		Medium: 9.1m L x 3m W x 3.8m H		None
Bicycle Parking	Class 1: 0.27 / 100.0 m ² gross leasable area greater than 100 m2 0.27 x 1341.1/100=3.6	4	6		None
	Class 2: 0.4 /100.0 m ² gross leasable area greater than 100 m ² 0.4 x 1341.1/100=5.4	5	8 1 of which will be equipped with e-bike charger		None
Bicycle Dimension	1.8m (horizontal)/1.0m (vertical) L x 0.6m W x 1.9m H		1.8m (horizontal)/1.0m (vertical) L x 0.6m W x 1.9m H		

NOTE: FOR BUILDING PERMIT APPLICATIONS SUBMITTED TO THE CITY SHALL COMPLY WITH STEP 2 WITH EL-2 ENERGY COMPLIANCE



2 TYP. PARKING AND LOADING SIZE
Scale: 1/32"=1'-0"



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5	REISSUED FOR DP APPLICATION / ADP MEETING
4	REISSUED FOR DP APPLICATION
3	REISSUED FOR DP APPLICATION
2	REISSUED FOR DP APPLICATION
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DATE	
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JUN 26, 2025	
APR 4, 2025	
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DP 23-031545
November 17, 2025
Plan #3

PROJECT
LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

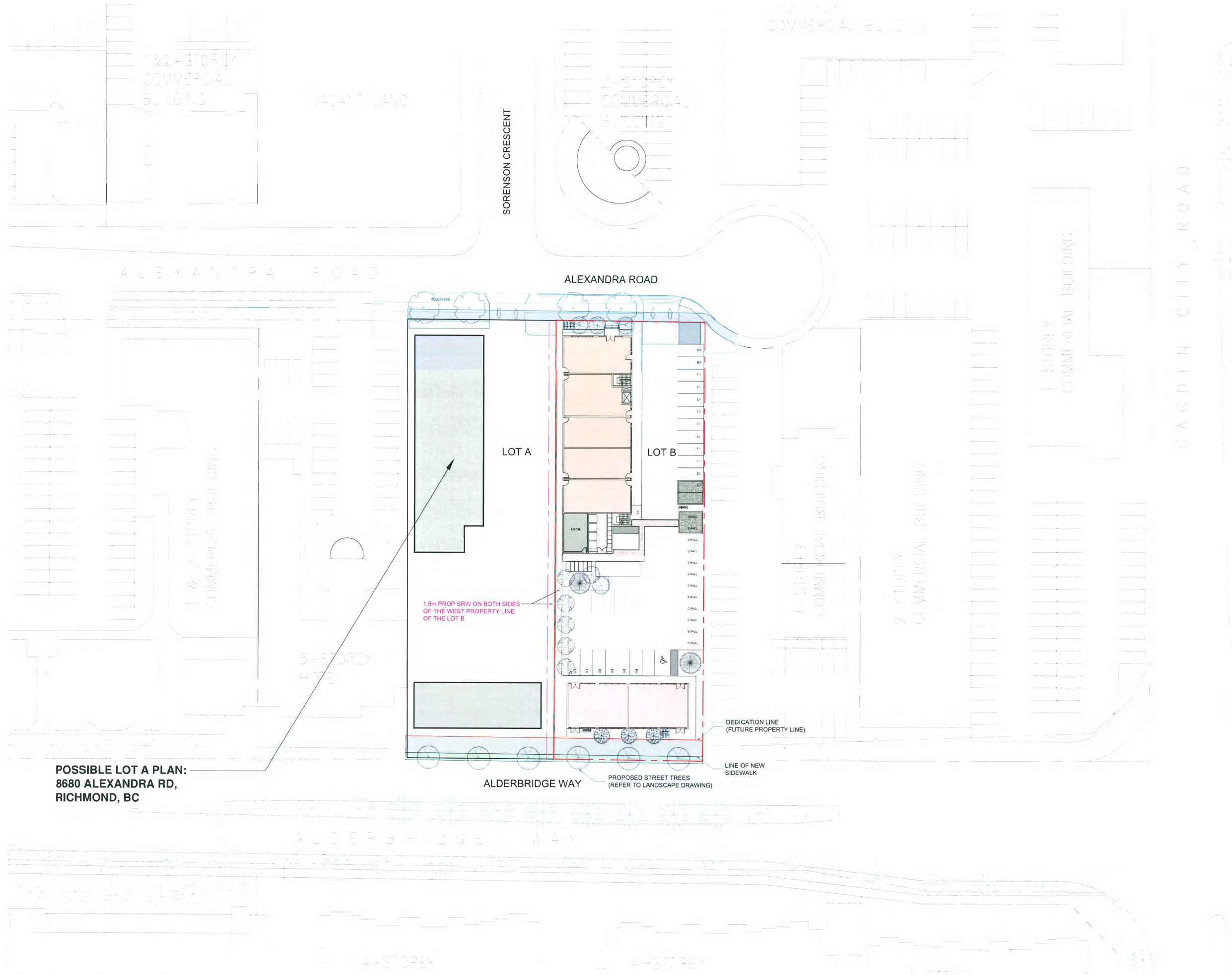
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SITE PLAN & STATISTICS

DRAWING No.

A004

PAGE 9



POSSIBLE LOT A PLAN:
8680 ALEXANDRA RD,
RICHMOND, BC

1.5m PROP SRW ON BOTH SIDES
OF THE WEST PROPERTY LINE
OF THE LOT B

DEDICATION LINE
(FUTURE PROPERTY LINE)

LINE OF NEW
SIDEWALK

PROPOSED STREET TREES
(REFER TO LANDSCAPE DRAWING)

ALDERBRIDGE WAY



PWA

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REGISTERED ARCHITECT
XU YANG

BRITISH COLUMBIA

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DP 23-031545

November 17, 2025

Plan #4

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

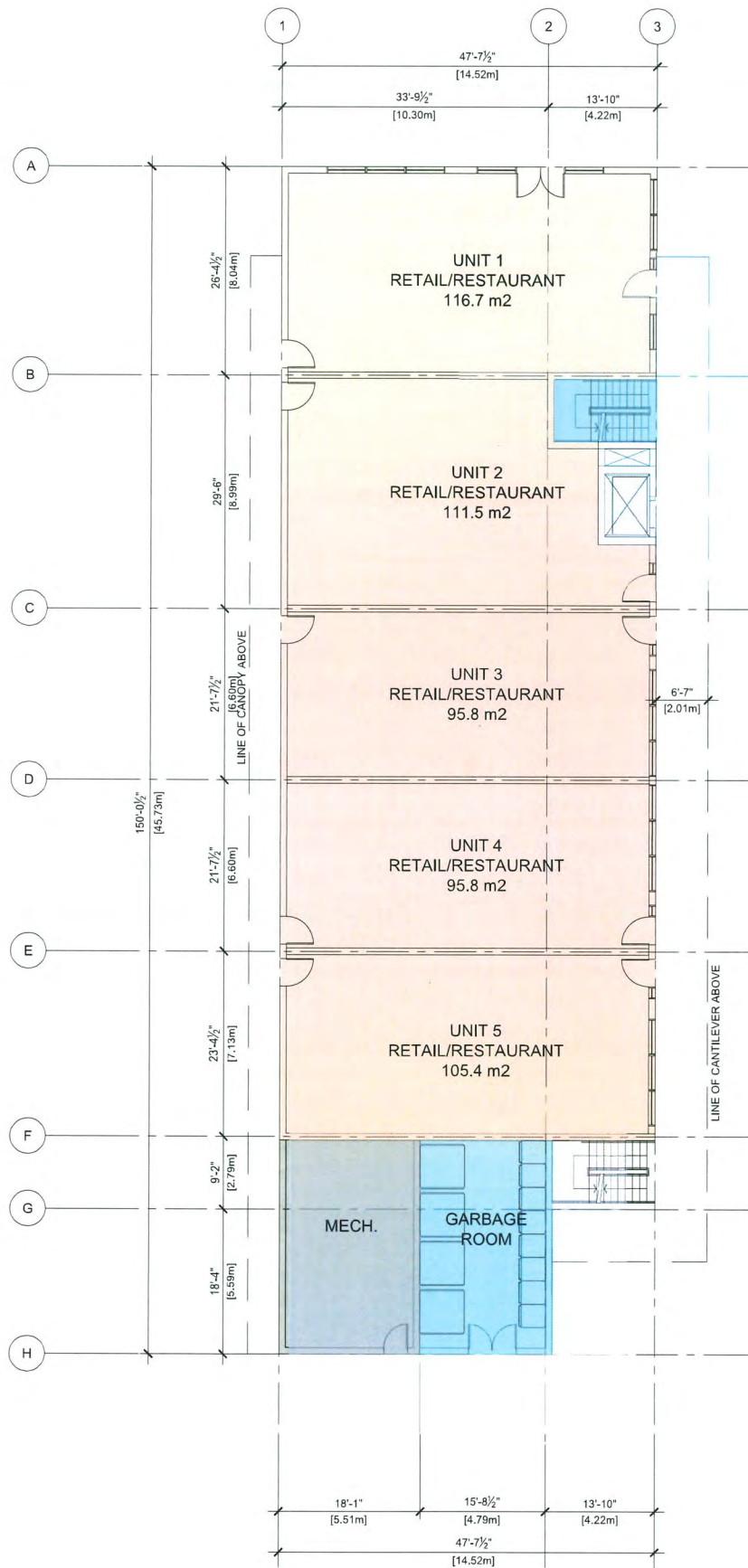
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LOT A POSSIBLE
SITE PLAN

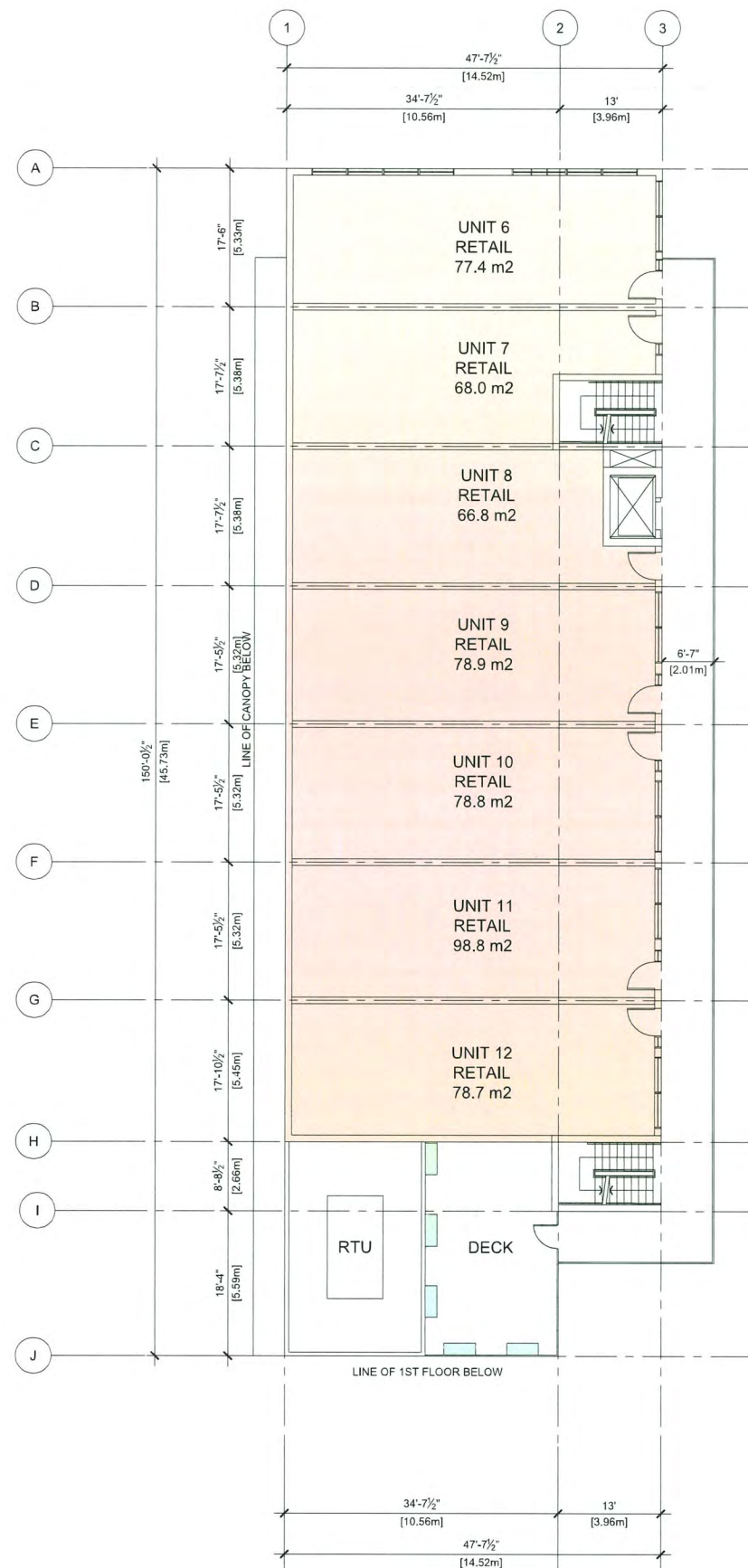
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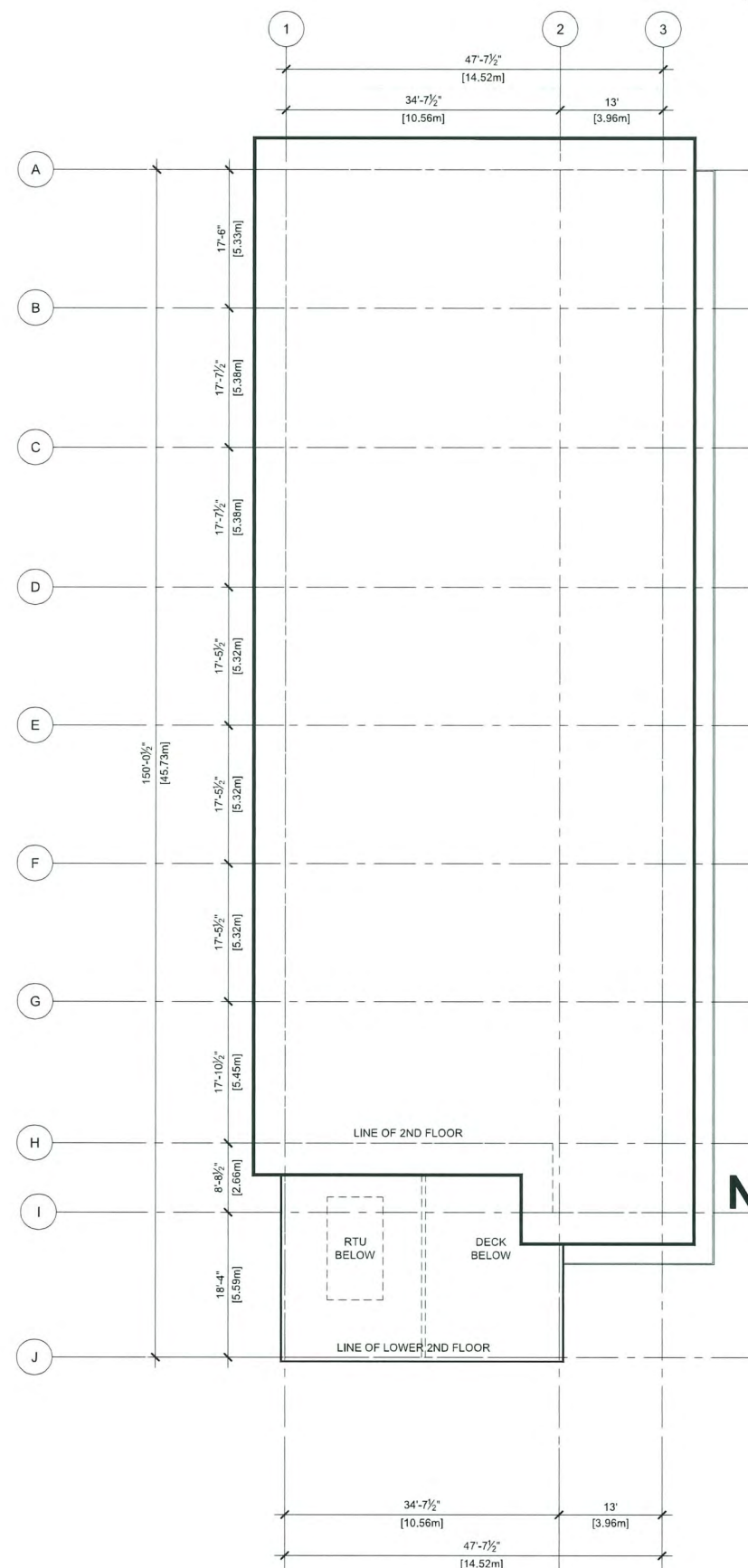
PAGE 10



1 BUILDING 1 - FIRST FLOOR PLAN
Scale: 3/64"=1'-0"



2 BUILDING 1 - SECOND FLOOR PLAN
Scale: 3/64"=1'-0"



3 BUILDIGN 1 - ROOF PLAN
Scale: 3/64"=1'-0"



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November 17, 2025
Plan #5

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

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BUILDING 1
FLOOR AND ROOF PLAN

DRAWING No.

A101

PAGE 12



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DP 23-031545 November 17, 2025 Plan #6

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

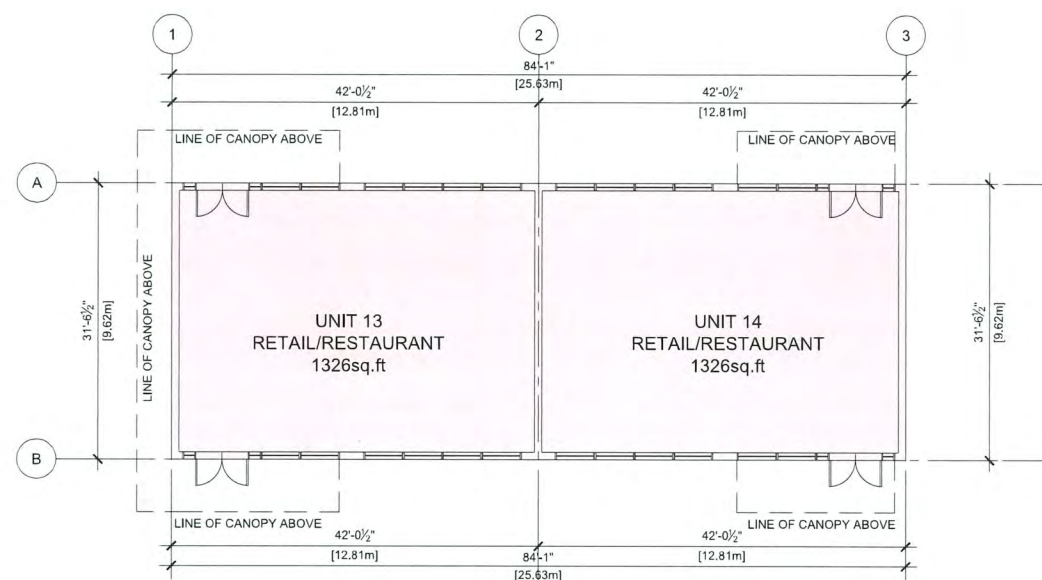
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BUILDIGN 2
FLOOR AND ROOF PLAN

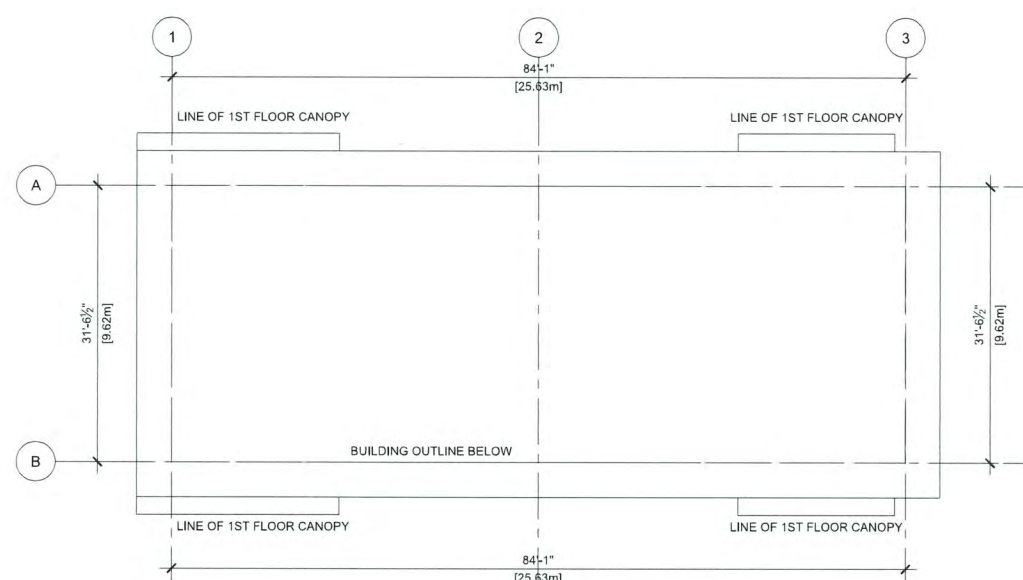
DRAWING No.

A102

PAGE 13



1 BUILDING #2 FLOOR PLAN
Scale: 3/64"=1'-0"



2 BUILDIGN #2 ROOF PLAN
Scale: 3/64"=1'-0"



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DP 23-031545
November 17, 2025
Plan #7

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

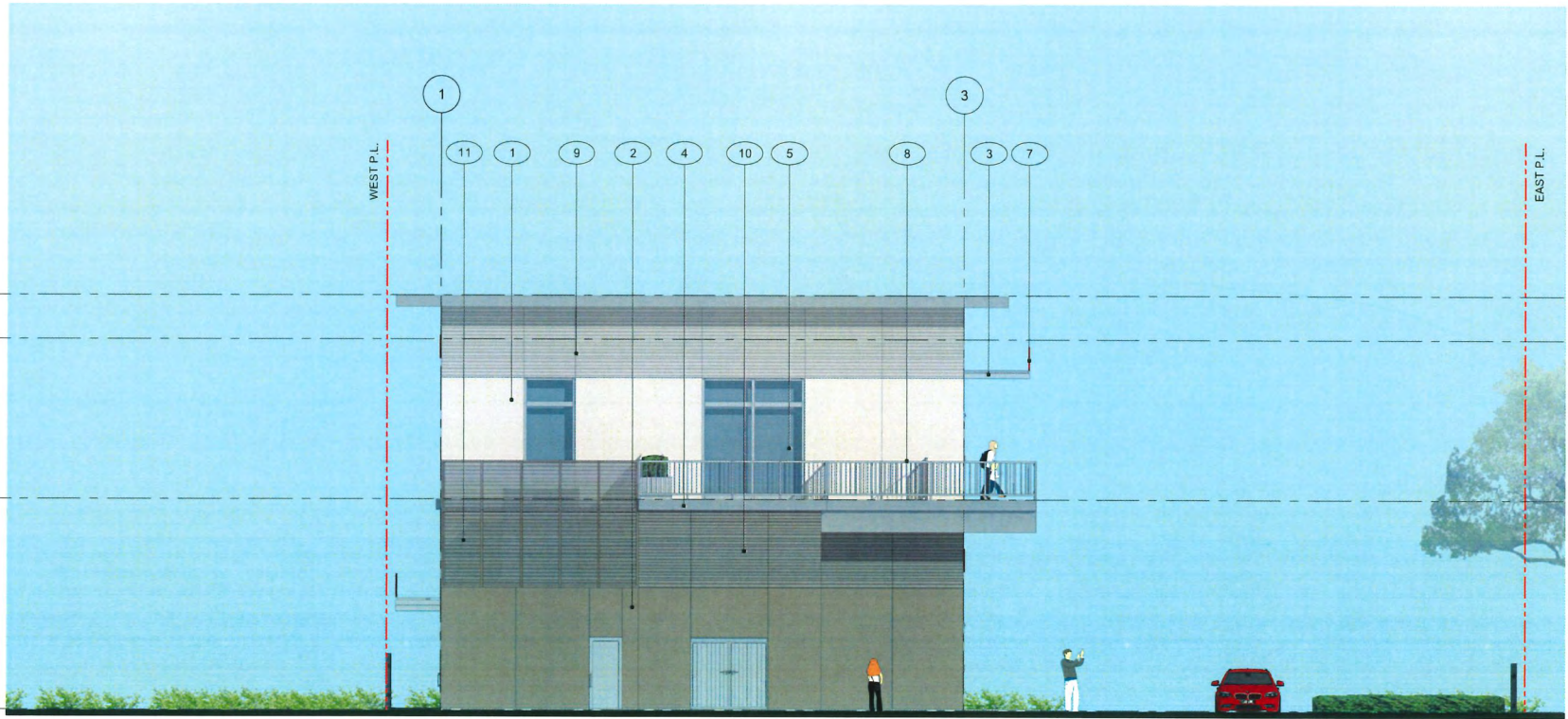
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**BUILDING 1
ELEVATIONS**

DRAWING No.

A201
PAGE 14



1 EAST ELEVATION
Scale: 1/16"=1'-0"



2 SOUTH ELEVATION
Scale: 1/16"=1'-0"

MATERIAL KEY

- 1 COMPOSITE PANEL, COLOR: PEARL WHITE
- 2 COMPOSITE PANEL, COLOR: GREY BEIGE
- 3 SAFETY GLASS CANOPY WITH METAL FRAME
- 4 METAL PANELING/FLASHING, SILVER METALLIC COLOR
- 5 CLEAR ANODIZED HIGH-PERFORMANCE GLAZING WITH PRE FINISHED LIGHT SILVER METALLIC COLOR ALUMINUM FRAME
- 6 METAL STOREFRONT DOOR, LIGHT SILVER METALLIC COLOR TO MATCH CURTAIN WALL
- 7 SIGNAGE
- 8 METAL RAILING, PAINTED - LIGHT GREY
- 9 CORRUGATED COMPOSITE PANEL, COLOR: PEARL WHITE
- 10 CORRUGATED COMPOSITE PANEL, COLOR: GREY BEIGE
- 11 METAL LOUVRE SCREEN

NOTE:
ALL SIGNAGE IS SUBJECT TO THE CITY'S SIGN BYLAW
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DP 23-031545
November 17, 2025
Plan #8

LOT B
8680 ALEXANDRA ROAD

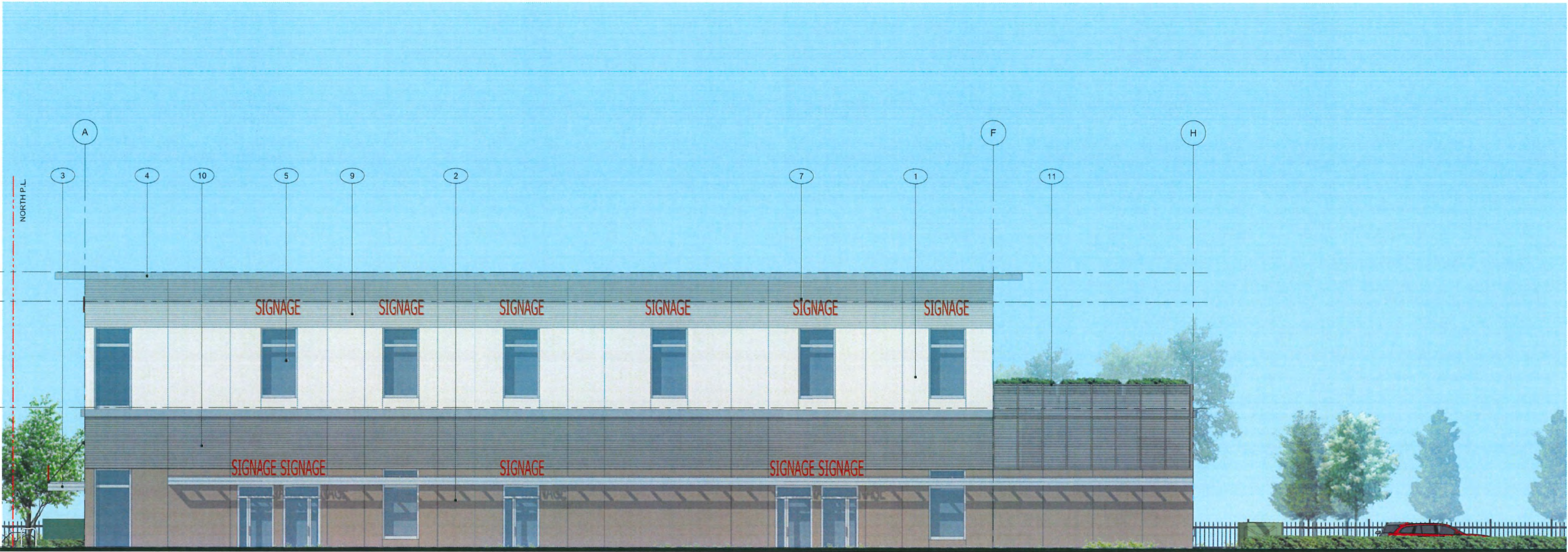
RICHMOND, BC

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**BUILDING 1
ELEVATIONS**

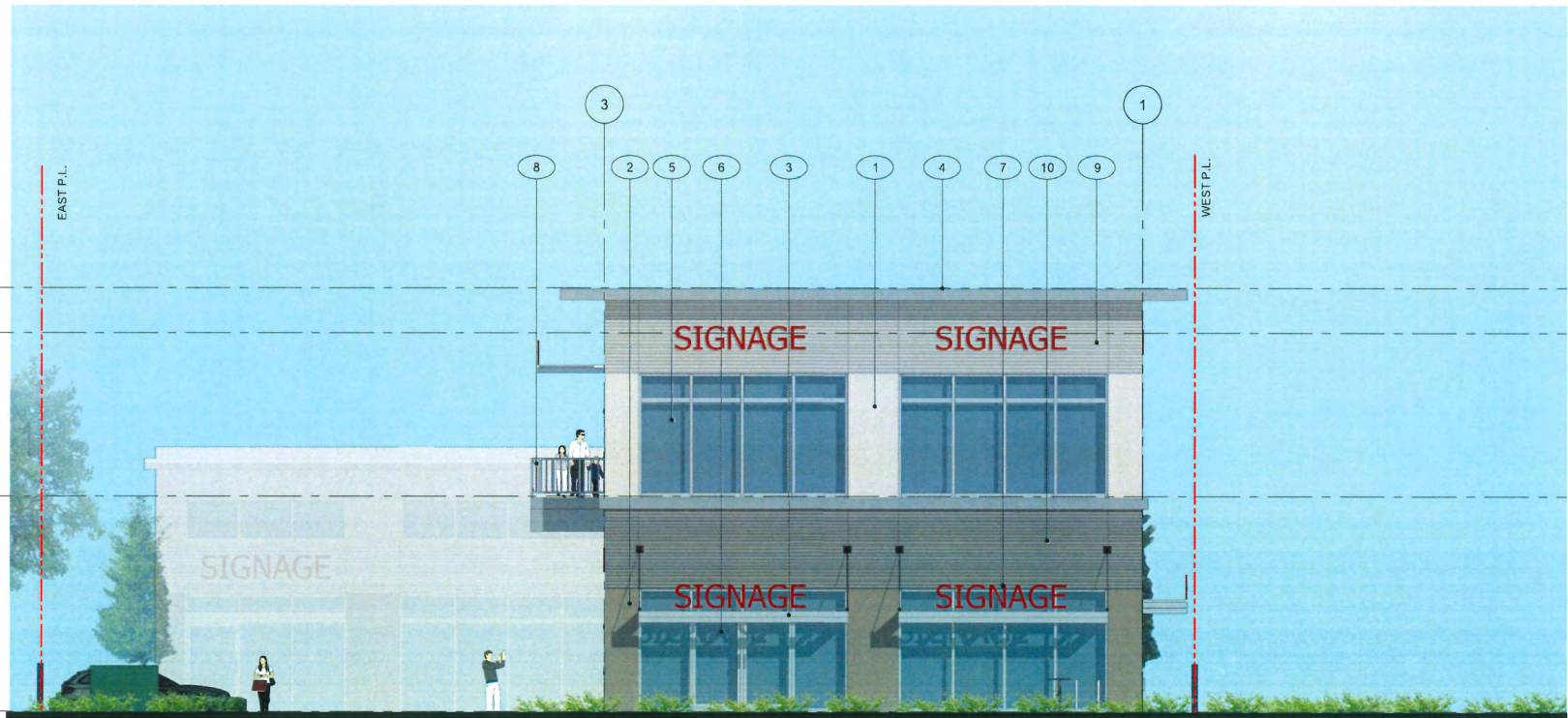
DRAWING No.

A202

PAGE 15



1 WEST ELEVATION
Scale: 1/16"=1'-0"



2 NORTH ELEVATION
Scale: 1/16"=1'-0"

MATERIAL KEY

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**DP 23-031545
November 17, 2025
Plan #9**

**LOT B
8680 ALEXANDRA ROAD**

RICHMOND, BC

DRAWING TITLE

**BUILDING 2
ELEVATIONS**

DRAWING No.

A203
PAGE 16



MATERIAL KEY

- 1 COMPOSITE PANEL, COLOR: PEARL WHITE
- 2 COMPOSITE PANEL, COLOR: GREY BEIGE
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DP 23-031545
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Plan #10

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

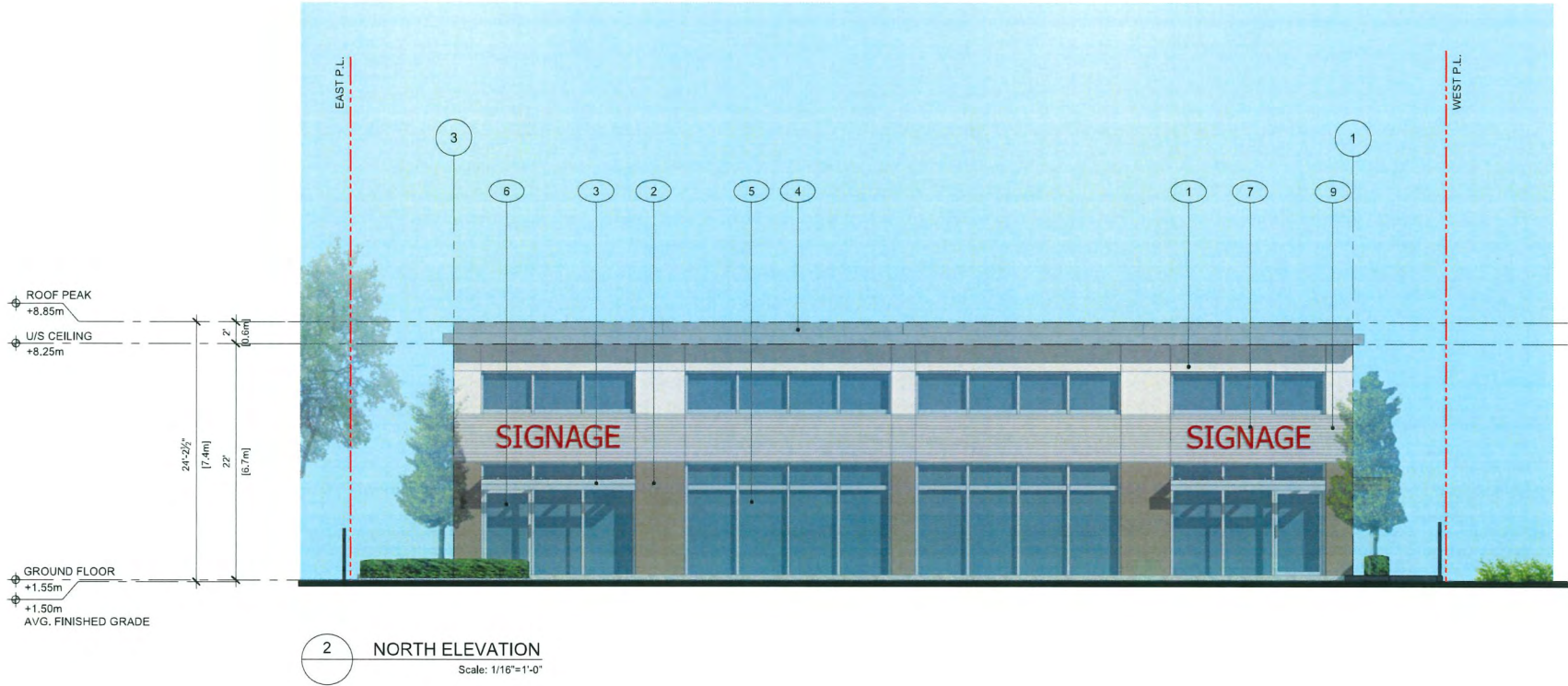
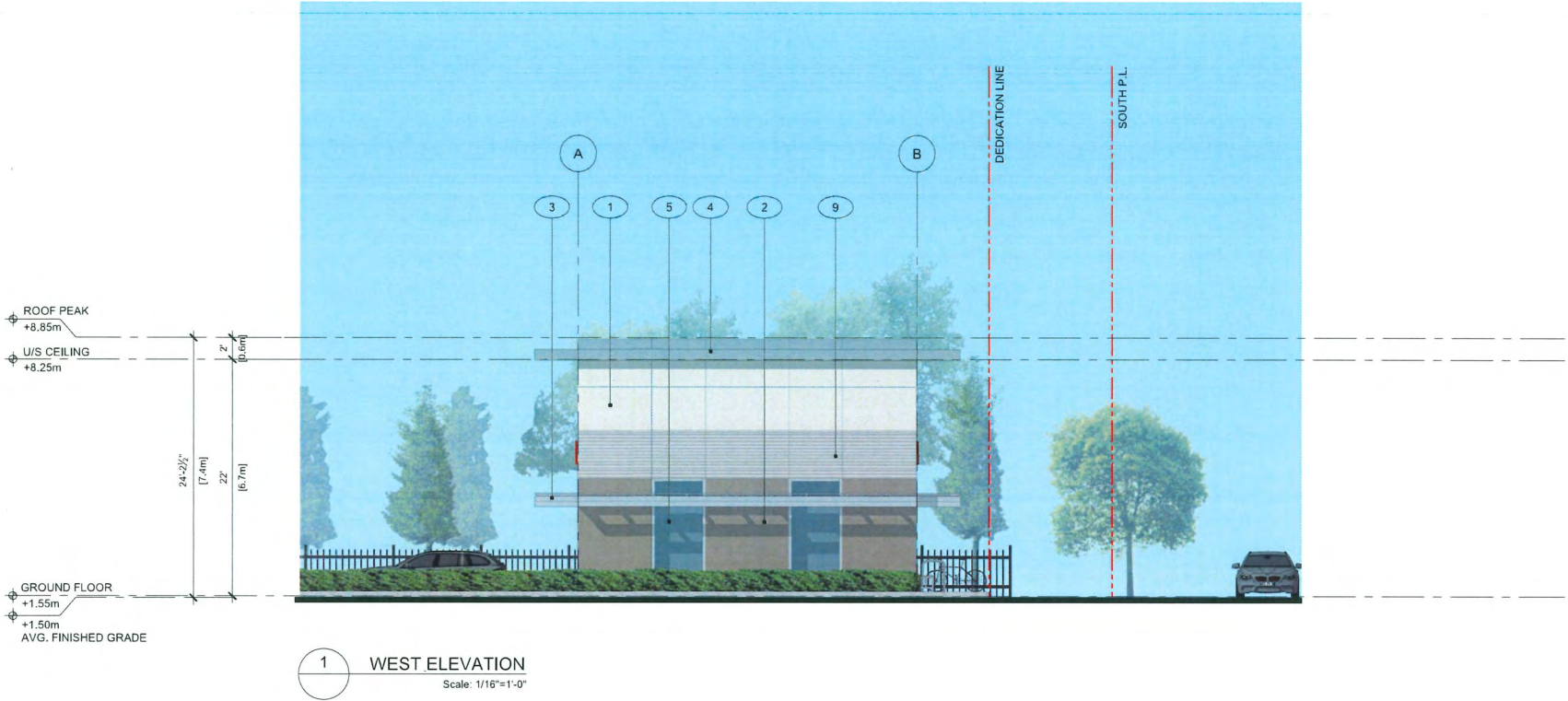
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**BUILDING 2
ELEVATIONS**

DRAWING No.

A204

PAGE 17



MATERIAL KEY

- 1 COMPOSITE PANEL, COLOR: PEARL WHITE
- 2 COMPOSITE PANEL, COLOR: GREY BEIGE
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RICHMOND, BC

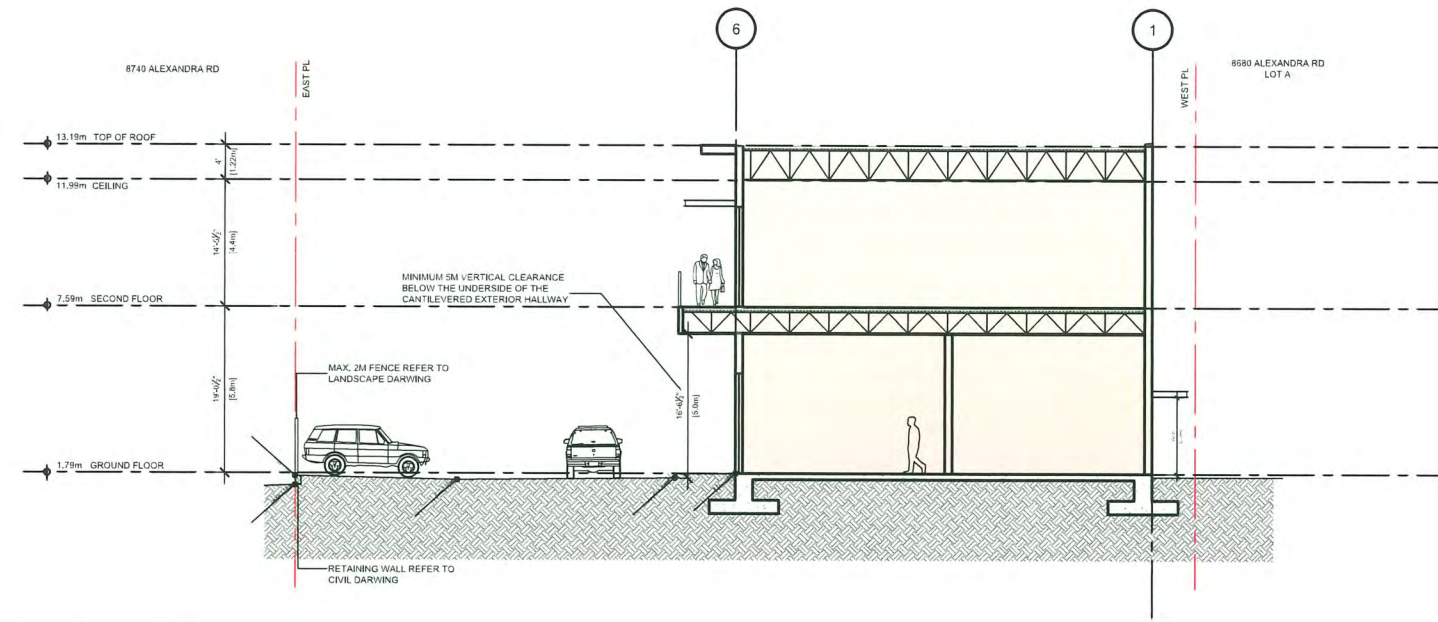
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**BUILDING
SECTIONS**

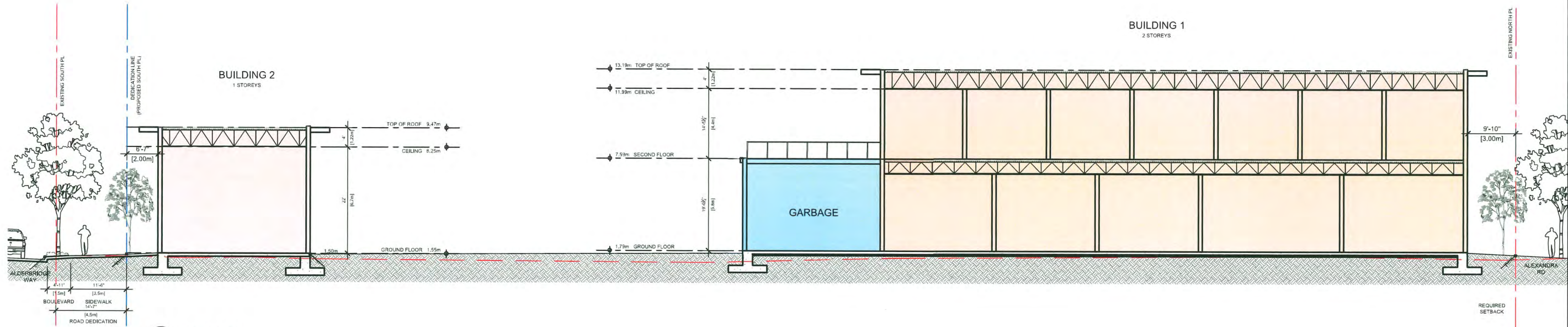
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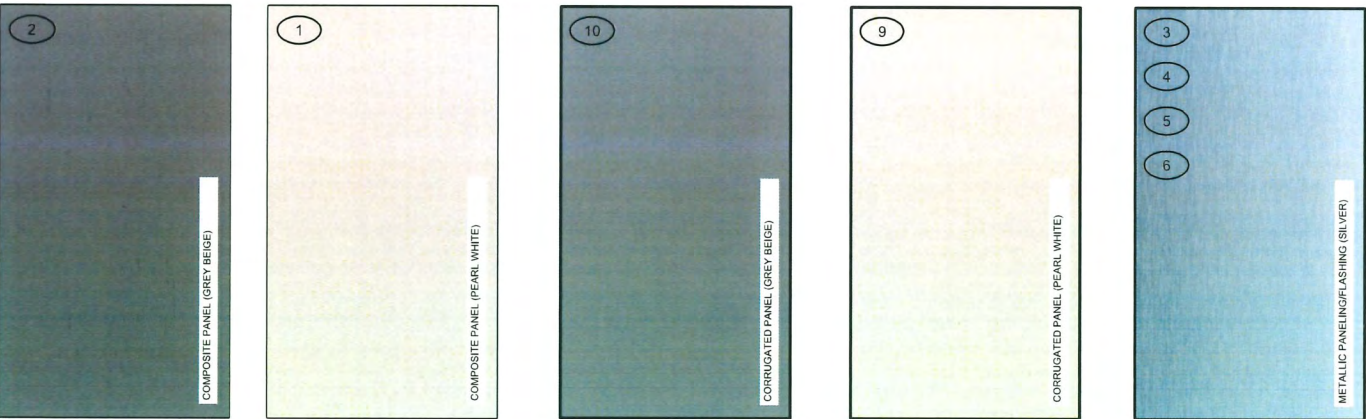


SECTION2
Scale: 3/64"=1'-0"

**DP 23-031545
November 17, 2025
Plan #11**



1 PERSPECTIVE-NORTHEASTERN
Scale: N.T.S



2 MATERIAL BOARD
Scale: N.T.S

MATERIAL KEY

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DP 23-031545
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Plan #12

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

DRAWING TITLE

MATERIAL BOARD

DRAWING No.

A400

PAGE 25

TREE MANAGEMENT LEGEND

EXISTING TREE TO BE RETAINED, REFER TO ARBORIST REPORT AND TREE MANAGEMENT PLAN PREPARED BY ARBORIST

EXISTING TREE AND TREE STUMP TO BE REMOVED, REFER TO ARBORIST REPORT AND TREE MANAGEMENT PLAN PREPARED BY ARBORIST, TO BE CONFIRMED PER ARBORIST ON-SITE WITH CITY OF RICHMOND ARBORIST PRIOR TO REMOVAL

TREE PROTECTION FENCING
TREE PROTECTION FENCING TO BE INSTALLED PER CITY OF RICHMOND STANDARDS AND TREE MANAGEMENT PLAN PREPARED BY ARBORIST

TREE MANAGEMENT NOTES

- DO NOT REMOVE OR RELOCATE ANY TREE, EXCEPT AS INDICATED ON PLANS.
- DO NOT ALTER EXISTING GRADE OR STORE MATERIALS UNDER THE DRIP LINE OR WITHIN TREE PROTECTION ZONE. EXCAVATION WITHIN DRIP LINES OF TREES ONLY WHERE INDICATED ON PLANS.
- INFORM ARBORIST WHEN ALL TREE BARRIERS HAVE BEEN INSTALLED. ARBORIST TO PROVIDE WRITTEN INSPECTION AND APPROVAL OF ALL BARRIERS AND SUBMIT INSPECTION REPORT TO CITY OF RICHMOND STAFF FOR REVIEW AND APPROVAL PRIOR TO DEMOLITION / MOBILIZATION CONSTRUCTION ACTIVITY.
- CONTRACTOR TO CONTACT CITY ARBORIST, PROJECT ARBORIST AND LANDSCAPE ARCHITECT 48 HOURS PRIOR TO ANY CONSTRUCTION WORK AFFECTING THE DRIP LINE OF TREES.
- CONSTRUCTION WORK WITHIN THE DRIP LINES OF TREES SHALL MINIMIZE DISTURBANCE TO TREES AND ROOTS.
- ALL ROOTS GREATER THAN 2cm IN DIAMETER SHOULD BE HAND PRUNED.
- ADEQUATELY CARE FOR TREES THROUGHOUT CONSTRUCTION, WATER THE RETAINED TREES REGULARLY.
- STUMPS TO BE CUT FLUSH TO GRADE AND DUG OUT IF NOTED TO BE REMOVED FROM SITE.

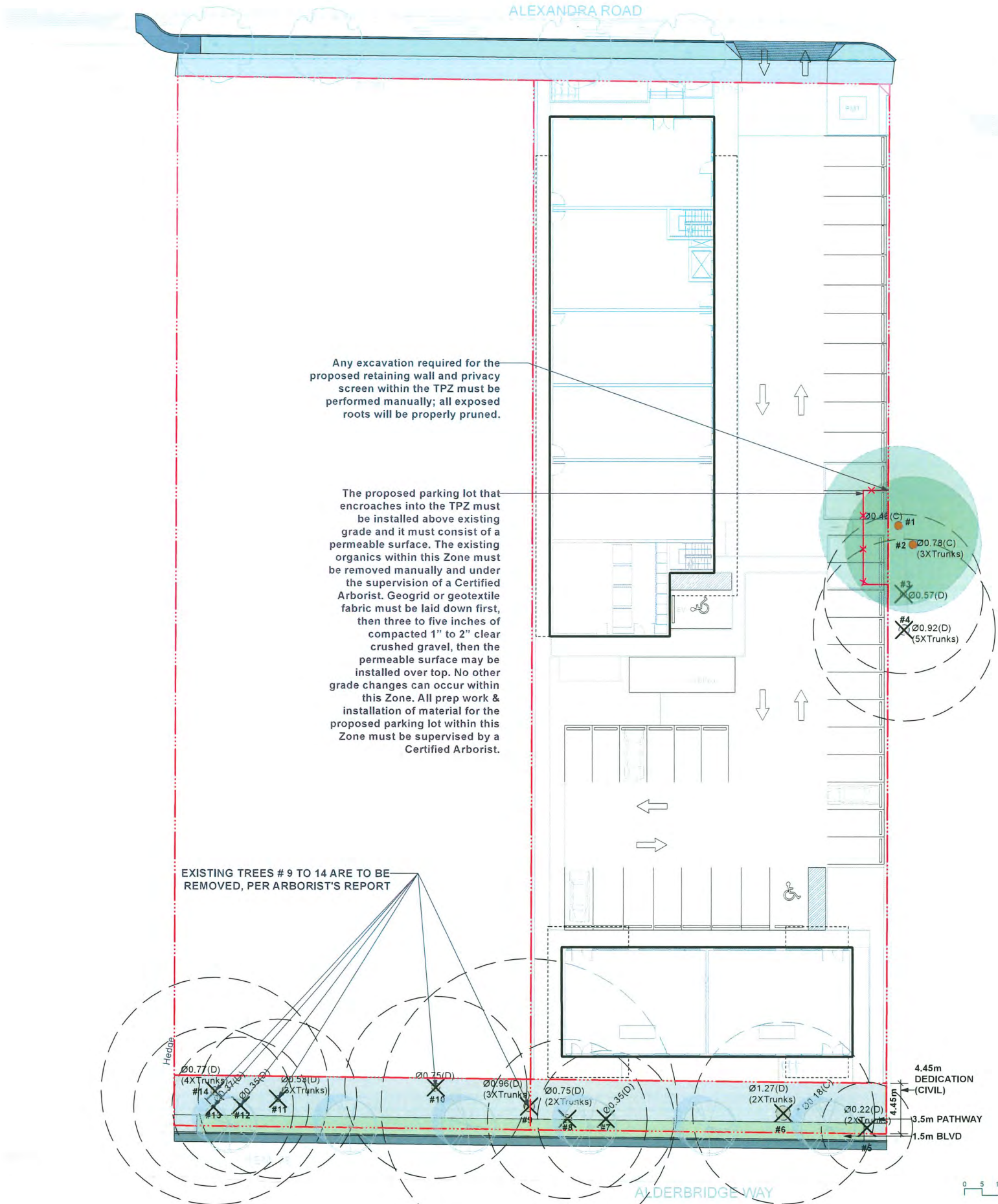
Tree Protection Zone

The Tree Protection Zone (TPZ) encompasses the drip line of the tree as illustrated in Fig. 1a and 1b.

Fig. 1a - Example of a drip line on a tree

Fig. 1b - Example of a drip line on a tree with one side pruned

Note: If the tree is on an adjacent property, the drip line must still be protected on your side of the property line. If the branches have been cut or pruned, the TPZ must protect the original drip line of the tree (Fig. 1b).



HOMING
LANDSCAPE
ARCHITECTURE

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Revisions

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H	2025-07-23	ISSUED FOR DP
I	2025-08-06	ISSUED FOR DP
J	2025-10-22	ISSUED FOR DP
K	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #13

NEW COMMERCIAL
DEVELOPMENT

PROJECT ADDRESS:
8680 ALEXANDRA RD, RICHMOND ,
BC, CANADA

PROJECT NUMBER: 23-20

SCALE: 1/16"=1'0" (1:192)

DRAWN BY: EL

REVIEWED BY: EL

Landscape Tree
Management Plan

REPLACEMENT TREES SUMMARY

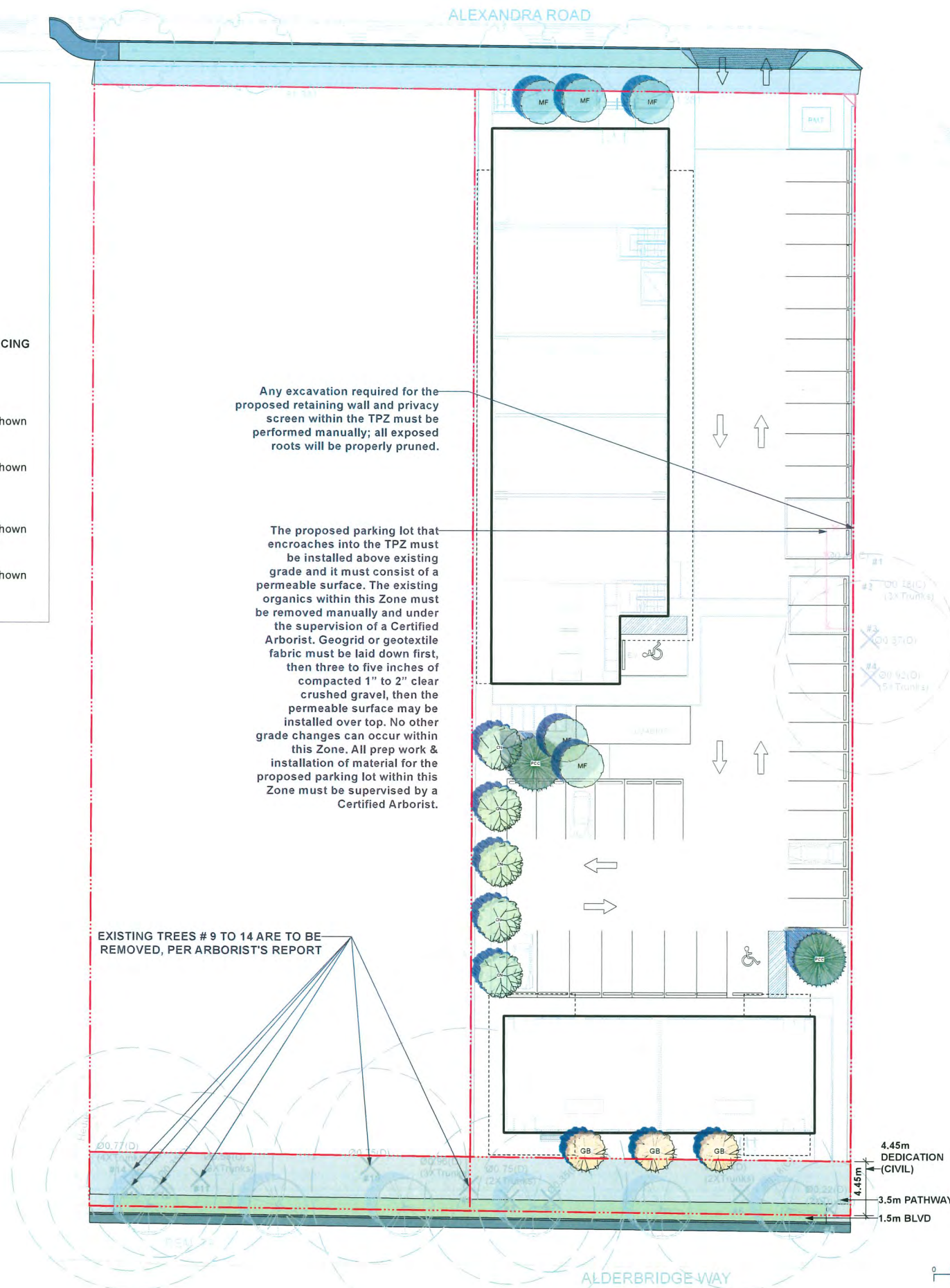
TOTAL 5 ONSITE TREES AND 2 OFFSITE TREES TO BE REMOVED, WHICH REQUIRES 14 REPLACEMENT TREES TO BE INSTALLED. ALL REPLACEMENT TREES WILL BE INSTALLED ON SITE, REFER TO THE PLANT LIST BELOW.

REPLACEMENT TREES LEGEND

PLANT LIST

QTY.	BOTANICAL NAME	COMMON NAME	SIZE	SPACING
TREES				

	5	Syringa reticulata 'Ivory Silk'	Japanese Tree Lilac	8cm cal., B&B	as shown
	3	Ginkgo biloba 'Princeton Sentry'	Princeton Sentry® Ginkgo	8cm cal., B&B	as shown
	2	Pinus contorta subsp contorta	Shore Pine	4.0m ht.	as shown
	5	Cornus nuttallii	Pacific dogwood	8cm cal., B&B	as shown





**HOMING
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ARCHITECTURE**

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Revisions		
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F	2025-03-19	ISSUED FOR DP
G	2025-06-25	ISSUED FOR DP
H	2025-07-23	ISSUED FOR DP
I	2025-08-06	ISSUED FOR DP
J	2025-10-22	ISSUED FOR DP
K	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #14

**NEW COMMERCIAL
DEVELOPMENT**

PROJECT ADDRESS:
8680 ALEXANDRA RD, RICHMOND ,
BC, CANADA

PROJECT NUMBER: 23-20
SCALE: 1/16"=1'0" (1:192)
DRAWN BY: EL
REVIEWED BY: EL

**Landscape
Replacement Tree
Plan**

LANDSCAPE LEGENDS

SYMBOL MATERIALS DETAILS

PEDESTRIAN CONCRETE UNIT PAVERS
CIP CONCRETE PAVING
1/L3.0

RED COLOR STAMPED ASPHALT PAVING
6/L3.0

CHARCOAL COLOR PERMEABLE PAVER
4/L3.0

LAWN
4/L3.1

PLANTING BED
2/L3.1

BIKE RACKS
3/L3.0

4' HIGH METAL PICKET FENCE
5/L3.0

PROPOSED REPLACEMENT TREES
1/L3.1

EXISTING TREES TO BE RETAINED

1. DO NOT SCALE DRAWINGS.
2. LAYOUT DRAWINGS ARE TO BE READ IN CONJUNCTION WITH ALL OTHER CONSULTANT DRAWINGS.
3. ALL EXISTING INFORMATION APPROXIMATE ONLY. VERIFY ALL EXISTING GRADES AND DIMENSIONS WITH SITE CONDITIONS. REPORT DISCREPANCIES TO CONSULTANT TEAM PRIOR TO PROCEEDING.
4. THE CONTRACTOR IS TO PROTECT ALL EXISTING UTILITIES, HARD SURFACES, STRUCTURES, WALLS, AND TREES FOR DURATION OF CONSTRUCTION.
5. THE LOCATION OF ALL PROPOSED HARDSCAPE AND SITE FURNISHINGS ARE TO BE FLAGGED OUT ONSITE BY THE CONTRACTOR AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
6. ALL OFF-SITE WORKS TO CONFORM TO CITY OF RICHMOND STANDARDS.
7. LAYOUT OF ALL OFF-SITE WORKS, INCLUDING FURNISHINGS, PATHWAYS, WALLS, PLANTING, ETC., TO BE CONFIRMED WITH CITY OF RICHMOND PRIOR TO INSTALLATION.
8. FENCING ALONG WEST PROPERTY LINE TO BE REMOVED UPON THE COMPLETION OF THE FULL SRW.



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H	2025-07-23	ISSUED FOR DP
I	2025-08-06	ISSUED FOR DP
J	2025-10-22	ISSUED FOR DP
K	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #15

8680 ALEXANDRA
ROAD

PROJECT ADDRESS:
8680 ALEXANDRA RD, RICHMOND ,
BC, CANADA

PROJECT NUMBER: 23-20

SCALE: 1/16"=1'0" (1:192)

DRAWN BY: EL

REVIEWED BY: EL

Landscape Layout
and Materials Plan

L1.0

PLANT LIST

QTY. BOTANICAL NAME COMMON NAME SIZE SPACING



TREES

5	 Syringa reticulata 'Ivory Silk'	Japanese Tree Lilac	8cm cal., B&B	as shown
3	 Ginkgo biloba 'Princeton Sentry'	Princeton Sentry® Ginkgo	8cm cal., B&B	as shown
2	 Pinus contorta subsp contorta	Shore Pine	4.0m ht.	as shown
5	 Cornus nuttallii	Pacific dogwood	8cm cal., B&B	as shown
3	 Fagus sylvatica 'Dawyck'	Dawyck beech	6cm cal., B&B	as shown
Street trees species to be finalized at servicing agreement application				
2	 Acer rubrum	Red Maple	6cm cal., B&B	as shown
Street trees species to be finalized at servicing agreement application				

SHRUBS

16	 Cornus sericea 'Kelsey'	Kelsey's Dwarf Red-Osier Dogwood	#2 pot	24" o.c
44	 Perfecto Mundo Double Pink azalea	Reblooming Azalea	#2 pot	24" o.c
16	 Rhododendron 'Crete'	Pink Rhododendron	#3 pot	36" o.c
15	 Sarcococca hookeriana humilis	Himalayan Sweet Box	#2 pot	24" o.c
42	 Spiraea bumulda 'Goldflame'	Goldflame Spiraea	#2 pot	24" o.c
12	 Symphoricarpos albus	Snowberry	#2 pot	36" o.c
60	 Blechnum spicant	Deer Fern	#1 pot	24" o.c
18	 Gaultheria shallon	Salal	#2 pot	24" o.c
26	 Vaccinium ovatum "Thunderbird"	Huckleberry	#3 pot	36" o.c
6	 Thuja occidentalis 'Smaragd'	Emerald Cedar	6' Height	24" o.c
4	 Daphne x transatlantica 'BLAFRA'	Eternal Fragrance Daphne	#2 pot	24" o.c

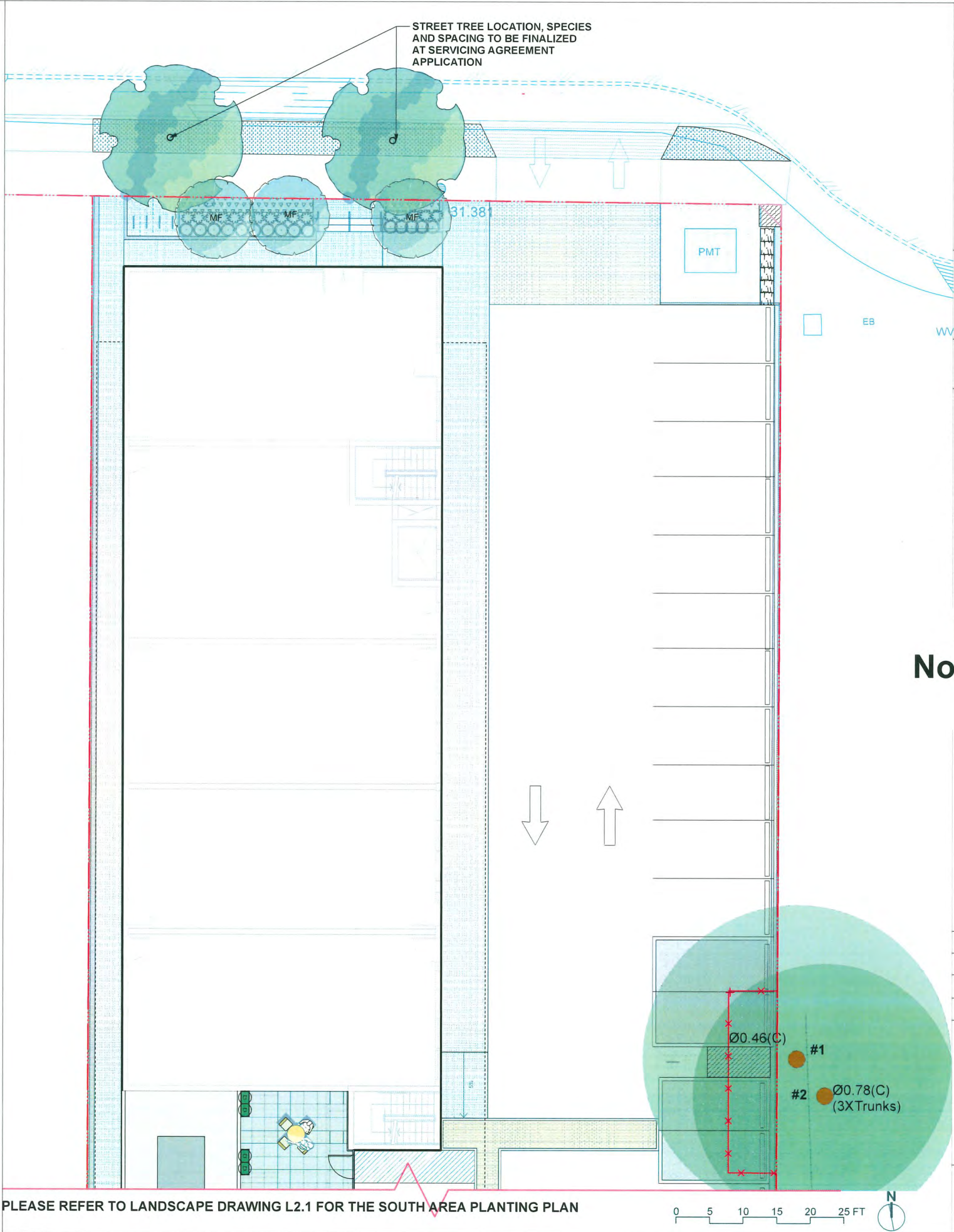
GROUND COVERS

83	 Oxalis oregana	Redwood Sorrel	4"(10cm) pot	12" o.c
548	 Arctostaphylos uva-ursi	Bearberry	4"(10cm) pot	12" o.c

LAWN

856	 ft2 Lawn
-----	--

- NOTES:
- 1) In case of discrepancy between plant numbers on this list and on the plan, the latter shall prevail.
 - 2) All planting shall be in accordance with CSLA Landscape Standard, latest edition.
 - 3) All planting beds to be installed with 2"(50mm) mulch per specifications.



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I	2025-08-06	ISSUED FOR DP
J	2025-10-22	ISSUED FOR DP
K	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #16

8680 ALEXANDRA
ROAD

PROJECT ADDRESS:
8680 ALEXANDRA RD, RICHMOND ,
BC, CANADA

PROJECT NUMBER: 23-20

SCALE: 1/8"=1'0" (1:96)

DRAWN BY: EL

REVIEWED BY: EL

Landscape
Planting Plan -
North Area

L2.0

PLANT LIST

QTY. BOTANICAL NAME COMMON NAME SIZE SPACING



TREES

5	 Syringa reticulata 'Ivory Silk'	Japanese Tree Lilac	8cm cal., B&B	as shown
3	 Ginkgo biloba 'Princeton Sentry'	Princeton Sentry® Ginkgo	8cm cal., B&B	as shown
2	 Pinus contorta subsp. contorta	Shore Pine	4.0m ht.	as shown
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3	 Fagus sylvatica 'Dawyck'	Dawyck beech	6cm cal., B&B	as shown
Street trees species to be finalized at servicing agreement application				
2	 Acer rubrum	Red Maple	6cm cal., B&B	as shown
Street trees species to be finalized at servicing agreement application				

SHRUBS

16	 Cornus sericea 'Kelsey'	Kelsey's Dwarf Red-Osier Dogwood	#2 pot	24" o.c.
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4	 Daphne x transatlantica 'BLAFRA'	Eternal Fragrance Daphne	#2 pot	24" o.c.

GROUND COVERS

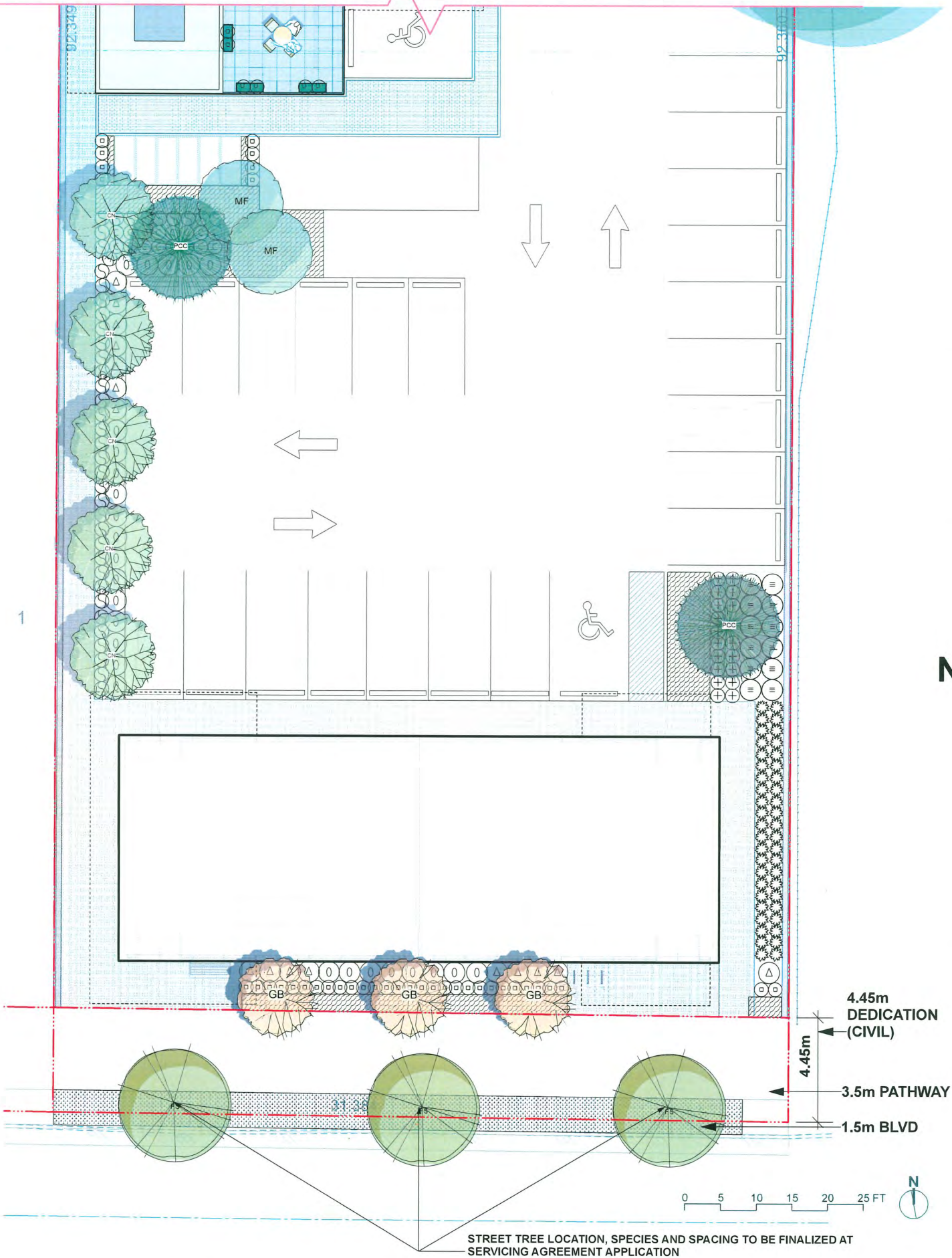
83	 Oxalis oregana	Redwood Sorrel	4"(10cm) pot	12" o.c.
548	 Arctostaphylos uva-ursi	Bearberry	4"(10cm) pot	12" o.c.

LAWN

856	 ft2 Lawn
-----	--

- NOTES:
- 1) In case of discrepancy between plant numbers on this list and on the plan, the latter shall prevail.
 - 2) All planting shall be in accordance with CSLA Landscape Standard, latest edition.
 - 3) All planting beds to be installed with 2"(50mm) mulch per specifications.

PLEASE REFER TO LANDSCAPE DRAWING L2.0 FOR THE NORTH AREA PLANTING PLAN



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J	2025-10-22	ISSUED FOR DP
K	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #17

**8680 ALEXANDRA
ROAD**

PROJECT ADDRESS:
8680 ALEXANDRA RD, RICHMOND ,
BC, CANADA

PROJECT NUMBER: 23-20

SCALE: 1/8"=1'0" (1:96)

DRAWN BY: EL


REVIEWED BY: EL

**Landscape
Planting Plan -
South Area**


L2.1

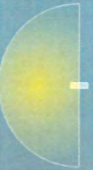
LANDSCAPE LIGHTING LEGENDS

SYMBOL MATERIALS





BOLLARD LIGHT



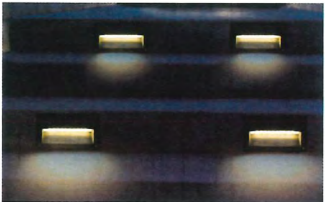



PARKING LOT POLE DOWNLIGHT WITH SHIELD



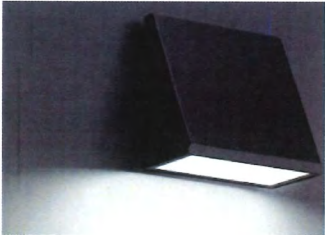


STEP LIGHT





WALL MOUNTED DOWNLIGHT



LIGHTING NOTES

1. CONCEPTUAL LANDSCAPE LIGHTING PLAN IS PROVIDED FOR INFORMATION ONLY. REFER TO ELECTRICAL ENGINEER'S DRAWINGS FOR CONSTRUCTION.
2. CONTRACTOR TO COORDINATE RECESSED FORMING, ELECTRICAL CONDUIT, AND LOCATION LOGISTICS WITH OTHER TRADES AS REQUIRED.
3. CONTRACTOR TO ENSURE ALL LIGHTING WIRE AND FIXTURES MUST BE INSTALLED TO MEET THE LOCAL ELECTRICAL REGULATIONS AND SAFETY REQUIREMENTS



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K	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #18

8680 ALEXANDRA
ROAD

PROJECT ADDRESS:
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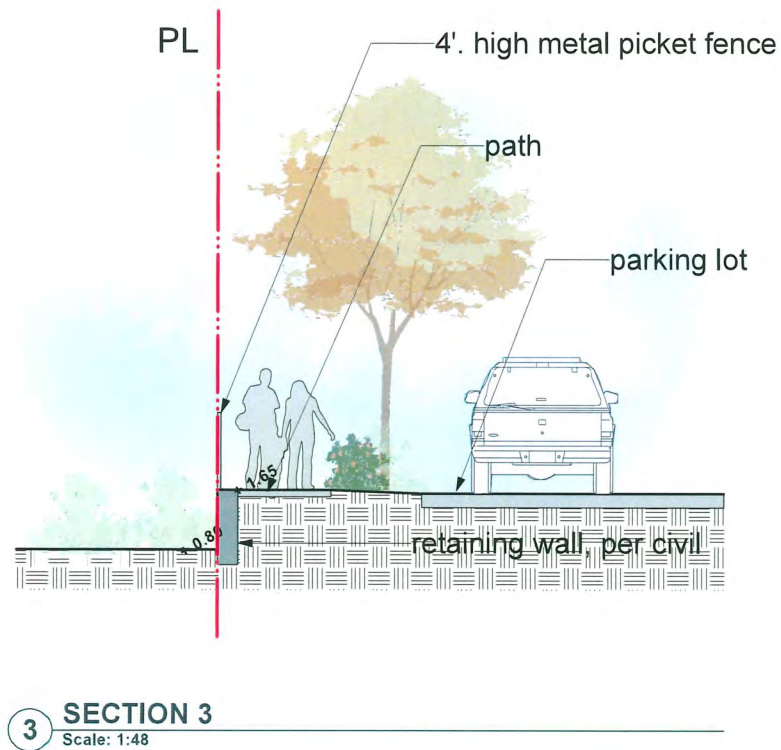
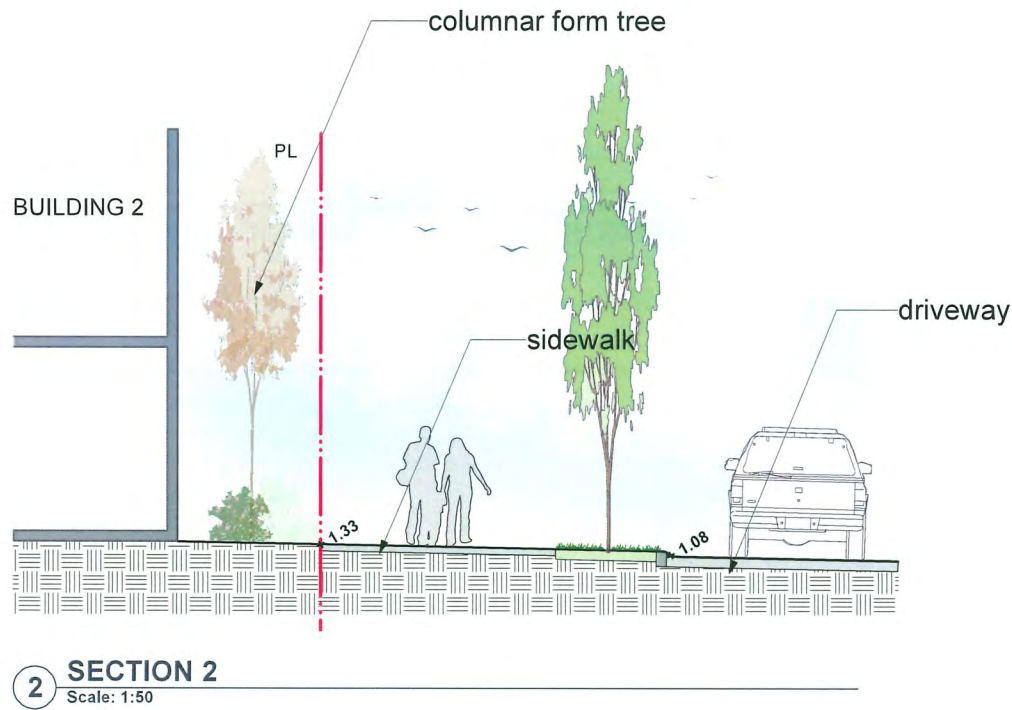
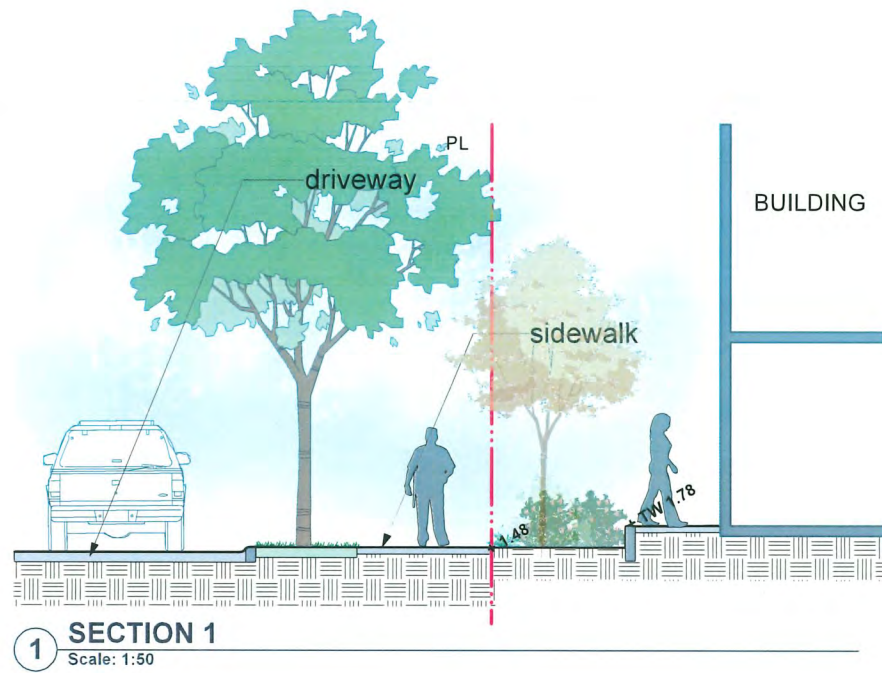
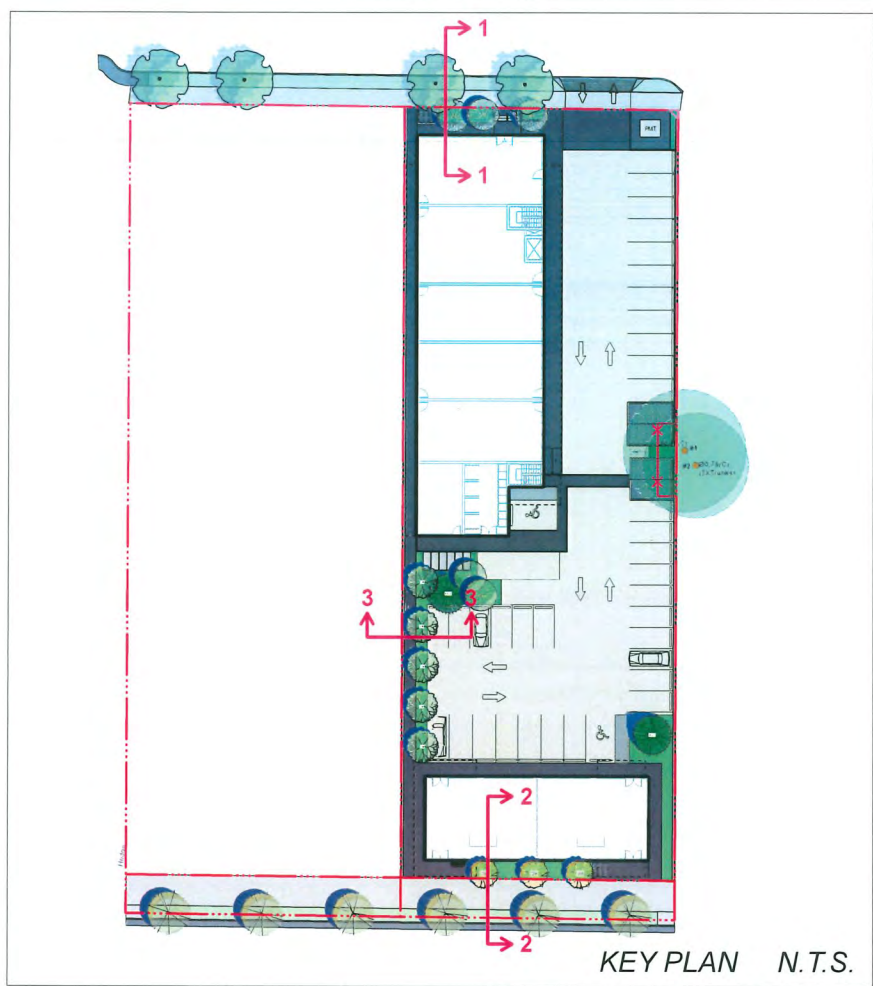
PROJECT NUMBER: 23-20

SCALE: 1/16"=1'0" (1:192)

DRAWN BY: EL

REVIEWED BY: EL

Conceptual
Landscape
Lighting Plan



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J	2025-10-22	ISSUED FOR DP
K	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #19

**8680 ALEXANDRA
ROAD**

PROJECT ADDRESS:
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BC, CANADA

PROJECT NUMBER: 23-20

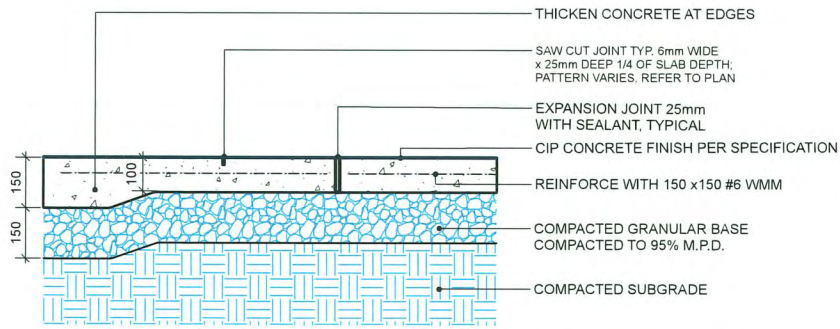
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DRAWN BY: EL

REVIEWED BY: EL

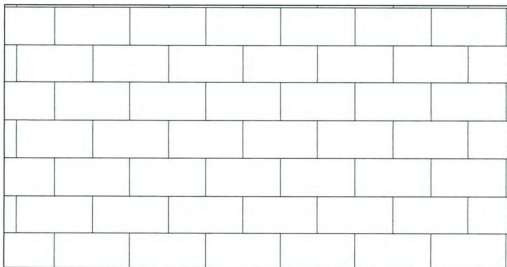
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L2.3

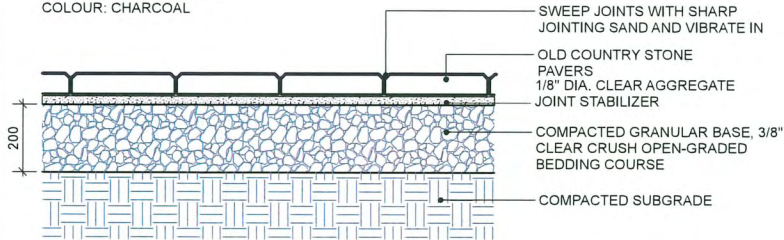


NOTES:
1. EXPANSION JOINTS 6m O.C. MAX, CONTROL JOINTS @ 1.5m O.C. ADJUST TO SUIT SITE LAYOUT PLAN.
2. ALL SCORELINES TO BE APPROVED BY LANDSCAPE ARCHITECT ON-SITE PRIOR TO INSTALLATION.

1 CIP CONCRETE ON GRADE (TYPICAL)
Scale: 1:10

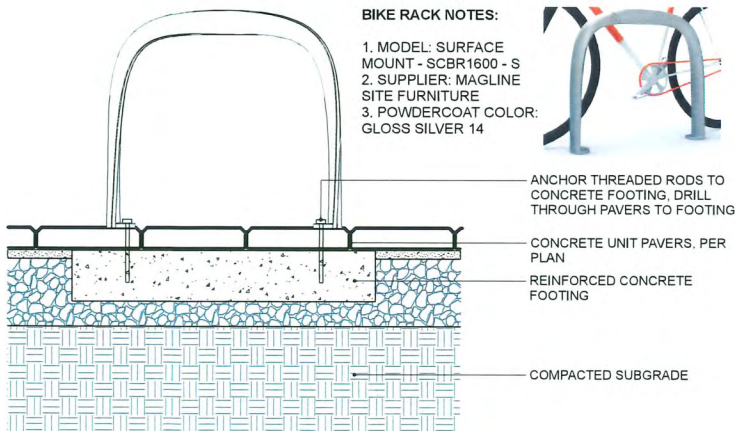


PLAN
CLASSIC STANDARD PAVERS - STANDARD SIZE BY
ABBOTSFORD CONCRETE (1-800-663-4091)
DIMENSIONS: STANDARD 225(L) x 112.5(W) x 60(T) mm (8-7/8" x
4-7/16" x 2-3/8")
PATTERN: RUNNING BOND
COLOUR: CHARCOAL



NOTE:
USE CONCRETE HIDDEN EDGE RESTRAINT WHEN PAVERS ARE
NOT ADJACENT TO A SOLID EDGE CONDITION.

2 CONCRETE UNIT PAVING ON GRADE
Scale: 1:10



NOTE:
PER SUPPLIER FOR SPECIFICATIONS AND INSTALLATIONS.

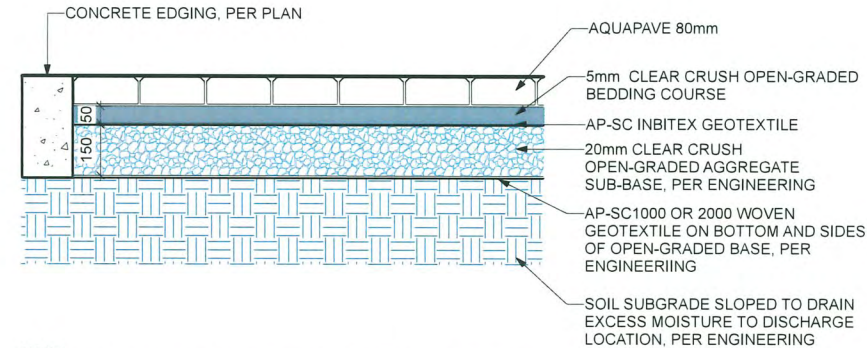
3 BIKE RACK
Scale: 1:10

AQUAPAVE STANDARD PAVER
BY ABBOTSFORD CONCRETE PRODUCTS

PATTERN: RUNNING BOND
LENGTH: 8-3/8" (221MM)
WIDTH: 4-5/16" (110MM)
THICKNESS: 3-1/8" (80MM)



COLOR: CHARCOAL



NOTE:
1. REFER TO ENGINEERING DRAWINGS FOR THE BASE MATERIALS INSTALLED UNDERNEATH THE PAVERS.
2. THE CONDITIONS UNDER THE PAVERS SHALL BE APPROVED BY ENGINEERS BEFORE PAVERS INSTALLATION.

4 AQUAPAVE PERMEABLE PAVER
Scale: 1:10



PRECEDENT

5 4' HIGH METAL PICKET FENCE
Scale: N/A



6 STAMPED ASPHALT PEDESTRIAN PATH
Scale: N/A



7 OUTDOOR BIKE LOCKER
Scale: N/A



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ARCHITECTURE

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CLARIFICATION AND APPROVAL BEFORE
PROCEEDING WITH WORKS.

Revisions

NO.	Date	Note
1	2023-10-06	ISSUED FOR DP
2	2024-07-10	ISSUED FOR DP
3	2024-08-06	ISSUED FOR DP
4	2024-08-23	ISSUED FOR DP
5	2025-01-09	ISSUED FOR DP
6	2025-03-19	ISSUED FOR DP
7	2025-06-25	ISSUED FOR DP
8	2025-07-23	ISSUED FOR DP
9	2025-08-06	ISSUED FOR DP
10	2025-10-22	ISSUED FOR DP
11	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #20

8680 ALEXANDRA
ROAD

PROJECT ADDRESS:
8680 ALEXANDRA RD, RICHMOND,
BC, CANADA

PROJECT NUMBER: 23-20

SCALE: AS SHOWN

DRAWN BY: EL

REVIEWED BY: EL

Hardscape &
Furnishing Details

L3.0



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5	2025-01-09	ISSUED FOR DP
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7	2025-06-25	ISSUED FOR DP
8	2025-07-23	ISSUED FOR DP
9	2025-08-06	ISSUED FOR DP
10	2025-10-22	ISSUED FOR DP
11	2025-11-06	ISSUED FOR DP

DP 23-031545
November 17, 2025
Plan #21

**8680 ALEXANDRA
ROAD**

PROJECT ADDRESS:
8680 ALEXANDRA RD, RICHMOND ,
BC, CANADA

PROJECT NUMBER: 23-20

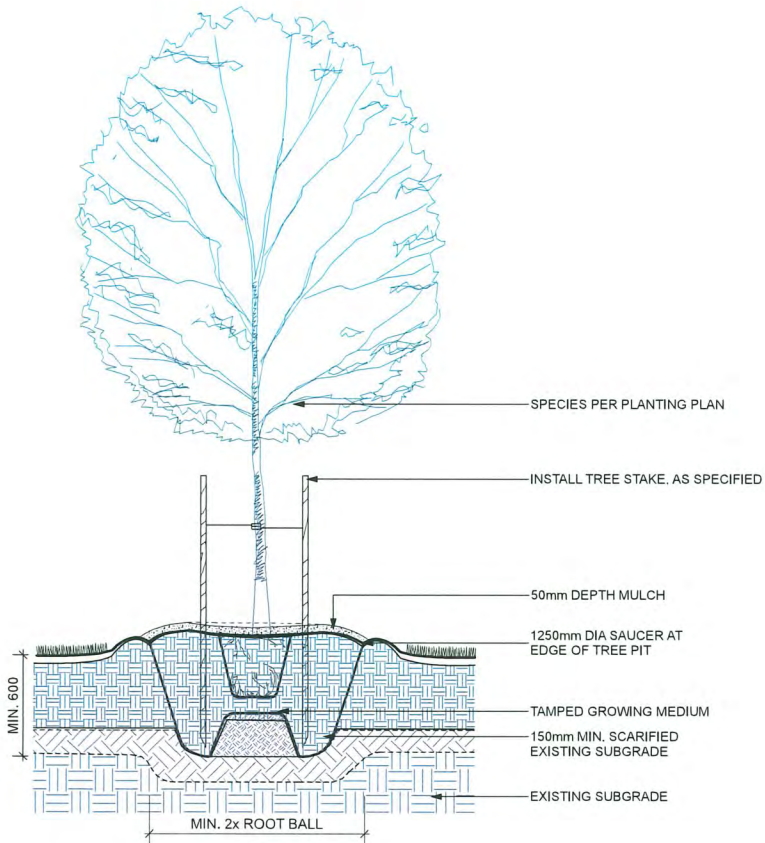
SCALE: AS SHOWN

DRAWN BY: EL

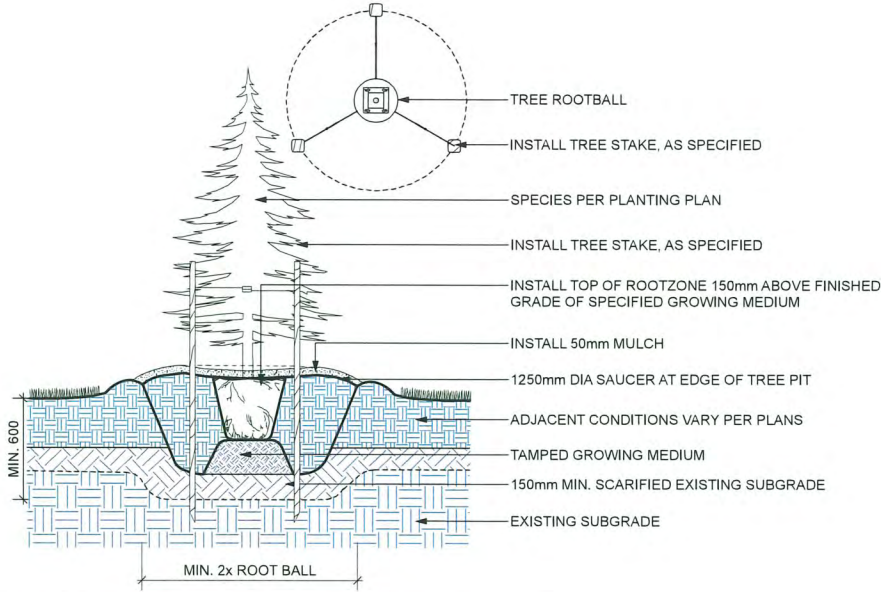
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Softscape Details

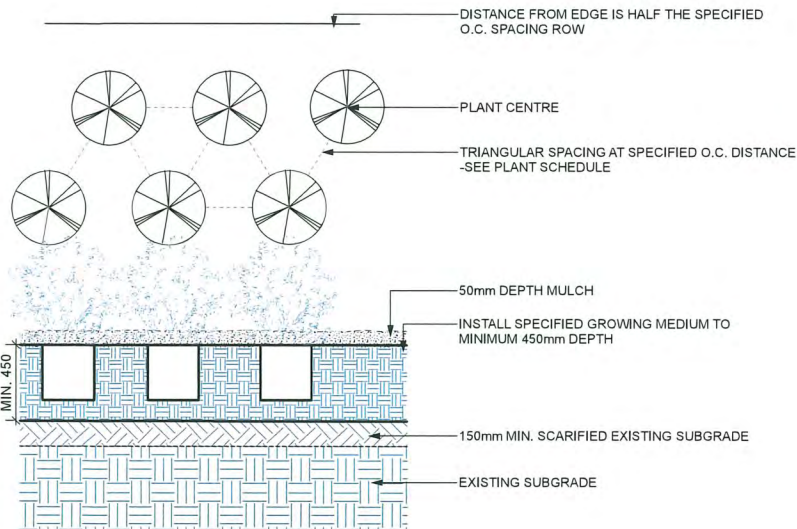
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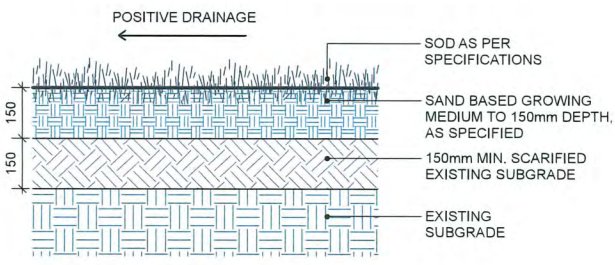
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Scale: 1:20



3 CONIFEROUS TREE PLANTING ON GRADE (TYPICAL)
Scale: 1:20



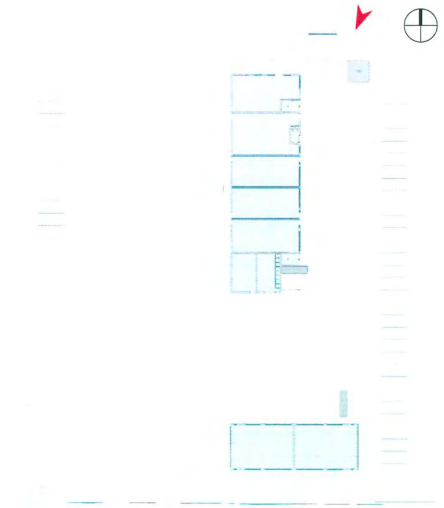
2 PLANTING ON GRADE (TYPICAL)
Scale: 1:20



4 SOD LAWN (TYPICAL)
Scale: 1:10



1 PRESPECTIVE - FROM NORTHEAST
Scale: NTS



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8 ISSUED FOR DPP	NOV 04, 2025
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2 REISSUE FOR DP APPLICATION	SEP 9, 2024
1 ISSUE FOR DP APPLICATION	OCT 11, 2023

PROJECT NUMBER	A434
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CHECKED BY	PY
DATE CHECKED	

DP 23-031545
November 17, 2025
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LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

DRAWING TITLE

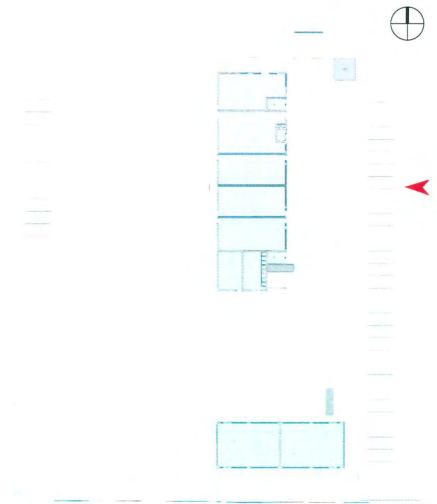
PRESPECTIVE

DRAWING No.

A301
PAGE 19



1 PRESPECTIVE - FROM EAST
Scale: NTS



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November 17, 2025
Reference Plan

LOT B
8680 ALEXANDRA ROAD

RICHMOND, BC

DRAWING TITLE

**PRESPECTIVE
BUILDING 1**

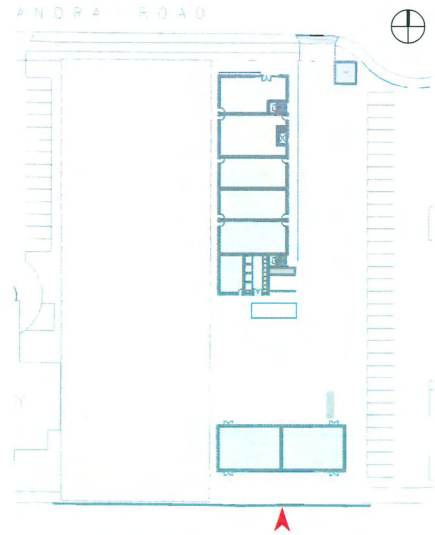
DRAWING No.

A302

PAGE 20



1 PRESPECTIVE - FROM SOUTH-WEST
Scale: NTS



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RICHMOND, BC
DRAWING TITLE
PRESPECTIVE BUILDING 1
DRAWING No.
A303 PAGE 21



1 3D VIEW 1
Scale: N.T.S



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DRAWING TITLE
3D VIEWS

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A304
PAGE 22



1 3D VIEW 2
Scale: N.T.S

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PROJECT NUMBER	A434
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PROJECT
LOT B
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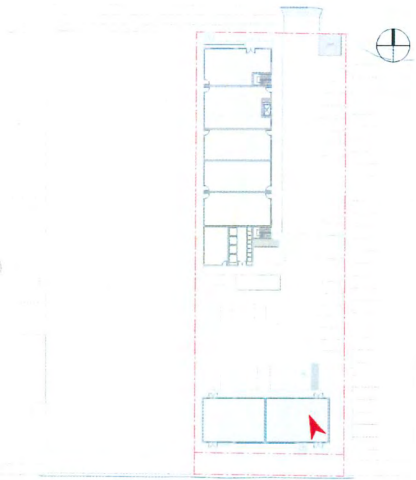
RICHMOND, BC

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DRAWING No.
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PAGE 23



1 3D VIEW 3
Scale: N.T.S



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3D VIEWS

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A306
PAGE 24



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DP 23-031545
November 17, 2025
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DRAWING TITLE

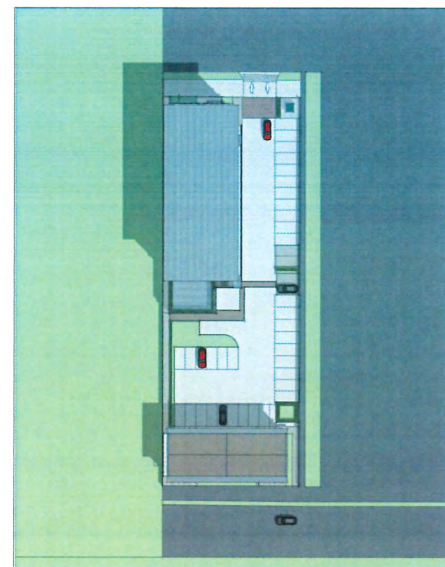
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STUDIES**

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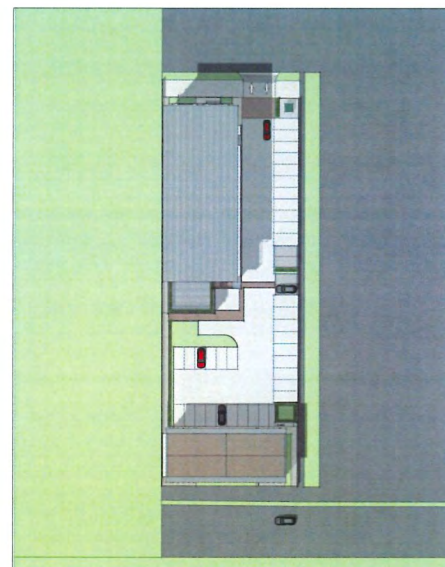
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PAGE 11

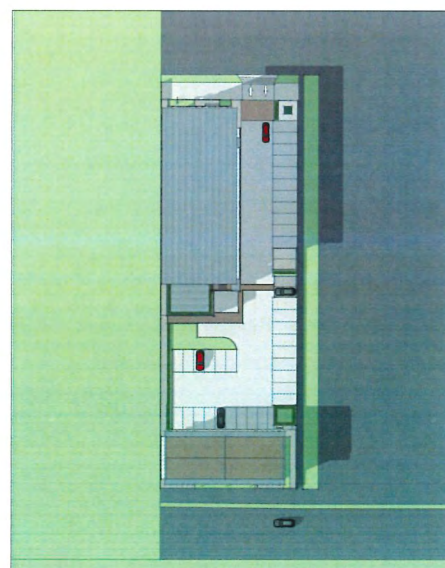
SPRING EQUINOX
MARCH 21 (DST)



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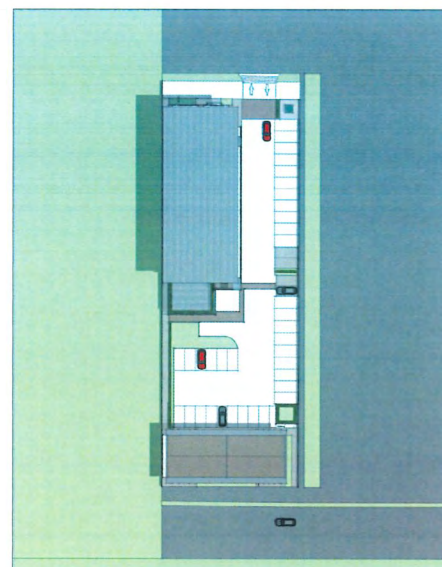


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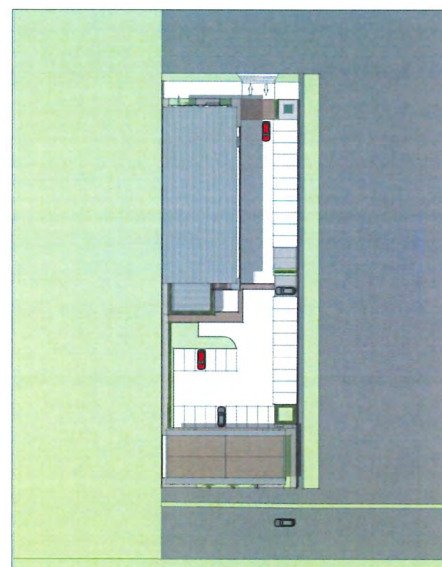


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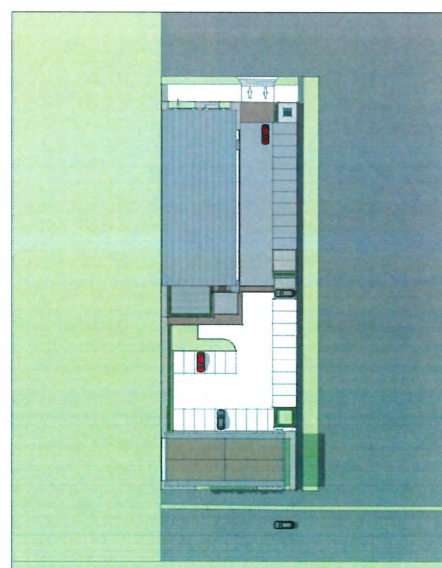
SUMMER EQUINOX
JUNE 21 (DST)



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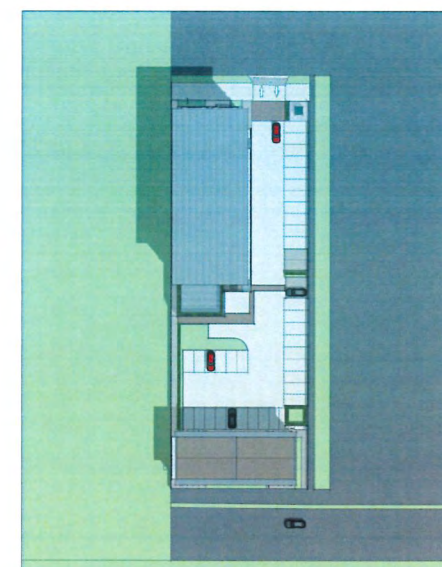


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04 PM
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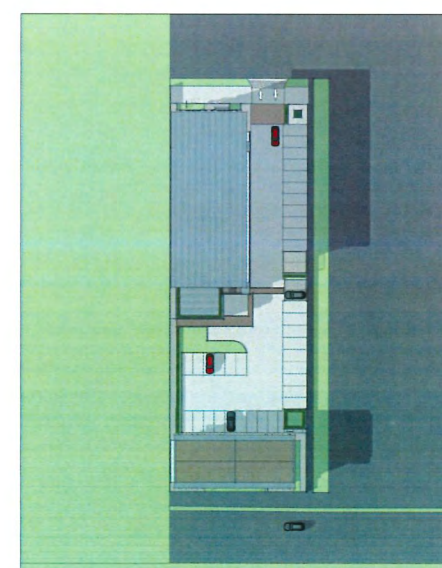
FALL EQUINOX
SEPTEMBER 22 (DST)



10 AM
(PDT: UTC -7:00)



02 PM
(PDT: UTC -7:00)



04 PM
(PDT: UTC -7:00)