

### Report to Development Permit Panel

Planning and Development Department

To: Development Permit Panel

Date: May 13, 2013

From: Wayne Craig

Re:

File:

DP 12-626615

Director of Development

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Application by Robert Ciccozzi Architecture Inc. for a Development Permit at

7680 and 7720 Alderbridge Way

### Staff Recommendation

That a Development Permit be issued which would:

- 1. Permit the construction of a mixed-use development that includes 237 residential units and 457 m<sup>2</sup> (4915 sq. ft.) of commercial space at 7680 and 7720 Alderbridge Way; and
- 2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
  - a) Reduce the required commercial parking aisle width from 7.5 m to 6.7 m.

Wayne Craig

Director of Development

Att.

### Staff Report

### Origin

Robert Ciccozzi Architecture Inc. has applied to the City of Richmond for permission to develop 237 residential units distributed in three (3) multi-story residential buildings ranging from seven (7) to 11 stories, 457 m<sup>2</sup> (4915 sq. ft.) of commercial space and two-level parkade and podium enclosed by townhouses, located at 7680 and 7720 Alderbridge Way.

Currently, this 0.99 ha (2.45 acre) development site, now occupied by the Grimms Fine Foods plant, is composed of two (2) lots bounded by Lansdowne Road, Alderbridge Way and Cedarbridge Way.

The site is being rezoned from "Industrial Retail (IR1)" to "Residential/Limited Commercial (RCL2)" for this project under Bylaw 8946 (RZ 11-593705).

There is also a Servicing Agreement (SA 13-630300) for water, sanitary and storm and street frontage works within these road allowances and the Lansdowne Linear Park (as discussed below) that needs to be entered into and secured before adoption of the zoning amendment bylaw.

### **Development Information**

Please refer to the attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

### Background

Development surrounding the subject site is as follows:

To the North: Across Alderbridge Way, there is a mix of older warehouses and light industrial uses. Most recently, a rezoning application by Onni Construction Ltd. at 7731 and 7771 Alderbridge Way (RZ 11-585209) has been approved to rezone that site to "High Density Low Rise Apartments (RAH2)" to allow for the construction of four (4) six-storey, wood-frame buildings containing 659 dwellings.

To the West: Also across Alderbridge Way, as it curves northeast, the former "V-Tech" light industrial building is located on a site that is now zoned "Industrial Retail (IR1)."

To the East: Across Cedarbridge Way, there are light industrial, office and retail uses on sites now zoned "Industrial Retail (IR1)" with one (1) site which is under a rezoning application for a 160-unit development at 5600 Cedarbridge Way (RZ 12-620370).

To the South: Across the narrow, half-width section of Lansdowne Road there is a site with car storage, light industrial, office and retail uses which is now zoned "Industrial Retail (IR1)."

### Rezoning and Public Hearing Results

The Public Hearing for the rezoning of this site was held on November 19, 2012. At the Public Hearing, one (1) letter was received from a local resident that expressed concerns about view loss and traffic as well as with noise during the constructions phase. In this regards, the City requires traffic management plans and site preparation plans to address site excavation and to be provided by developers at the time of building permit. The City Centre Transportation Plan and the upcoming Translink City Centre Transit Plan are designed to address road and transit improvements related to the growth of the City Centre.

During the rezoning process, staff identified the following design issues which have been resolved at the Development Permit stage:

- Improving the green wall and water features on the Lansdowne elevation of the building to better integrate these features into the overall building form and into the adjacent planned Lansdowne Linear Park.
- Increasing spacing between the front patios of the townhouse units and adjacent sidewalk within the planned Lansdowne Linear Park.
- Changing the building articulation of the north-west elevation of Building A to improve privacy and increase light penetration to the affordable housing units within this building.
- Refining the balcony railings, concrete overhangs and ledges on the upper floors of each of the three (3) buildings above the podium level.
- Including additional architectural elements that visually associate the amenity building to the green wall and waterfall on the south side of the podium, including a trellis with climbing plants and additional windows.
- Increasing the size and improving form of the canopies for each of the building lobbies facing the streets.
- Making refinements to the podium landscape by adjusting the relationships of the pool and amenity building with the adjacent buildings, landscaped areas, seating areas and the private patios.

### Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and staff comments identified during the Development Permit review process. In addition, it complies with the intent of the "Mixed Use" designation of the City's Official Community Plan (OCP) and the "Urban Centre T5 (25 m)" designation within the City Centre Area Plan's (CCAP) "Lansdowne Village Specific Land Use" map.

Of note, the CCAP includes the proposed Lansdowne Linear Park extending from No. 3 Road to Elmbridge Way, running along the southern 10 m (33 ft.) of the proposed development site. The proposed development's edge condition along its south side has been designed to achieve a good relationship to the future Linear Park as discussed further below.

Although the proposed development exceeds the 25 m (82 ft.) height identified in the CCAP, the proposal meets the related CCAP policy. The CCAP states that buildings may exceed the 25 m (82 ft.) height identified for a site, provided that the form of development contributes towards a varied, attractive skyline, does not compromise privacy or sunlight to public area areas and provides improved views over what would otherwise be provided. Staff are supportive of the proposed heights for Buildings A and B because this facilitates the development to include buildings of varied heights, allowing the development to meet the 2.0 FAR density set out for the area in the CCAP. In addition, this condition contributes to the following development features:

- Increased common outdoor amenity space on larger podium garden that occupies 3,996 m<sup>2</sup> (approximately 1.0 acre) or 45% of the net development site; and
- Building heights of seven (7) to 11 storeys for Buildings A, B and C contributing to spacing between these buildings facing the Lansdowne Linear Park on the south side of development, and by providing breaks in the building masses facing the street frontages.

### Zoning Compliance/Variances (staff comments in bold)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

1) Reduce the required commercial parking aisle width from 7.5 m to 6.7 m.

Planning and Transportation staff support the proposed variance given the small parking 19-space lot needed for the limited commercial 4915 sq. ft. (457 m²) floor area, the short aisle sections subject to the variance and configuration of the aisles that provide for additional room for vehicles backing out.

### **Advisory Design Panel Comments**

The Advisory Design Panel (ADP) reviewed the proposed development at their meeting on February 20, 2013 and was generally supportive of the proposed development design. A copy of the relevant excerpt from the Advisory Design Panel Minutes is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific ADP comments and is identified in 'bold italics'.

### **Analysis**

### Conditions of Adjacency

### Surrounding Development

The development's triangular site is bounded on the south side by a current narrow half-width section of Lansdowne Road, and respectively by the full-width (with two (2) travel and two(2) parking lanes) Alderbridge and Cedarbridge Ways to the west and east that each curve to form a point at the northern end of the site. Conditions of adjacency for the proposed development are discussed in relation to the current light-industrial buildings (with typical heights of one (1) to

two (2) stories) in the area and the future development of the area envisioned by the CCAP. The CCAP policies support the development of a compact, engaging and urban Lansdowne Village with typical building heights of 25 m. (82 ft.) or six (6) stories and densities of 2.0 FAR under the Urban Centre (T5) land-use designation.

Specifically, the proposed development addresses the conditions of adjacency as follows.

- The varied building heights and massing of the three (3) buildings ranging from seven (7) to 11 stories provide for improved views to the Fraser River overlooking the low-rise six (6) storey buildings such as Onni's Riva development located to the north of Alderbridge Way. Furthermore, the proposed development's varied building heights above six (6) stories provide for a variety of building massing in the area that combines well with the six (6) storey building forms with a comparable 2.0 FAR.
- The proposal also achieves a good relationship between the proposed development and the
  public streetscape and future residential buildings planned for the surrounding area. In
  particular, the street-oriented townhouses with front entries opening out onto the majority of
  the Alderbridge and Cedarbridge Way street frontages will establish the desired urban,
  pedestrian-oriented character of the streetscape.
- The location of the three (3) buildings at each corner anchors the development, yet reduces the overall mass of the proposed development and provides for potential view corridors for future adjacent developments.
- The proposed Buildings A and B, respectively at eight (8) and 11 stories are positioned so as to provide view corridors and good building separation from a proposed 14 storey tower located at across Cedarbridge Way, currently under rezoning application RZ 12-620370.

### Lansdowne Linear Park:

The CCAP designates a 10m (33ft.) wide strip of land for a linear park within the development site that extends along the north side of Lansdowne Road from No. 3 Road to Elmbridge Way. The City is currently undertaking the Lansdowne Corridor Plan (LCP) which will include a general Concept and a more detailed Implementation Plan with a "kit of parts" for the Lansdowne Linear Park and adjacent streetscape. While the LCP had been planned to be completed in January 2013, it is now anticipated that the LCP will be completed and brought forward to Council for consideration later in 2013.

The subject development will include the creation of the first phase of the linear park, with an area of approximately 1,184 m<sup>2</sup> (11,989 ft<sup>2</sup>) that will be transferred by the applicant to the City at no cost as a condition of rezoning. Further discussion of the development's relationship to the Lansdowne Road and Park is included below.

### Urban Design and Site Planning

This development includes with three (3) higher buildings (relatively low towers) located in such a way as to anchor each corner of the site that occupies a full triangular block. Specific urban design features include:

### Three Towers Anchoring the Corners of the Site

- Buildings A and B follow the curved Alderbridge Way frontage and are located at the
  southwest corner and northern point of the triangular development. The buildings include
  eight (8) and 11 storey towers respectively, with lower six (6) storey wings extending
  towards each other along the Alderbridge Way frontage. These building wings are joined by
  a wide, elevated solid curved concrete trellis above the podium level to create a sense of
  connection.
- Building C, located at the southeast corner of the site at the intersection of Lansdowne Road and Cedarbridge Way, is a wider slightly curved, seven (7) storey building that frames the curved Cedarbridge Way.

### Central Podium Parkade Enclosed by Street-Oriented Townhouses

- Two (2) storey street-oriented townhouses extend along the street frontages of the majority of the bottom of the three (3) buildings and the central podium that includes the two-level parkade. Each townhouse includes individual entry doors to the street and front patios that add rhythm and enhance street life on the curved Alderbridge and Cedarbridge Way sidewalks and Lansdowne Linear Park onto which they face. These townhouses are setback 3.5 m (11.5 ft.) from Cedarbridge Way, 4.0m (12.1 ft.) from the Linear Park and 4.33 m (14.2 ft.) from Alderbridge Way. The front patios facing Alderbridge and Cedarbridge Ways range from 1.0 m (3.33 ft.) to 1.2 m (4.0 ft.) above the sidewalk level in conformance to the CCAP and OCP DP guidelines.
- Entry lobbies for each of the three (3) taller buildings also front onto Alderbridge and Cedarbridge Ways, introducing desirable breaks in the development's street walls which are largely composed of the townhouse units.
- The (2) parkade entrances and enclosed loading areas are located on the Cedarbridge and Alderbridge Way frontages. The City's proposed sub-grade Minoru sanitary pump station is located within the Cedarbridge building setback adjacent to the first residential parkade entrance; the development's electricity transformer bank is located off of Alderbridge Way adjacent to the second parkade entrance for the residential and commercial parking areas. The respective locations of the pumps station and transformer bank adjacent to the parkade entrances and enclosed loading zones is preferable to being located in front of the street-oriented townhouses or commercial space.

### Lansdowne Linear Park Frontage

The proposed development forms an important edge to the Lansdowne Linear Park. City Parks and Planning staff have provided guidance on the character of the landscape and building elevations along the Lansdowne Linear Park.

In this regard, the applicant has focussed on three (3) distinct sections of the development's south frontage along the Linear Park. There are:

- Retail Frontage (West): The 457 m<sup>2</sup> (4,915 sq. ft.) commercial space located at the southwest corner of the development includes extensive window walls and continuous canopies along its Lansdowne and Cedar frontages. The adjacent ground plain on-site and immediately adjacent in the proposed Linear Park is composed of decorative paving designed for a possible outdoor seating and formally-treated trees that will convey a more urban character.
- Pedestrian Gathering Area and Water Feature (Centre): There is a widening of the Lansdowne Linear Park with the gap between Buildings A and C that creates a recessed section of the parking podium. This gap is enhanced with the inclusion of a pond and green wall, overlooked by the indoor amenity area located atop the podium. This gap between Buildings A and C forms a focal point on the development's south facade that is inset several metres from the adjacent commercial and residential sections of the development. This section of the development creates a quieter green environment with a large two (2) storey high green wall and a waterfall that enters the pond.

To increase the sense of a natural pond, the grade within the adjacent park has been gradually increased so that the adjacent gathering area within the park is located slightly above the pond. In summary, this gathering area also provides a buffer between the active street-oriented commercial space to the west and the residential townhouses to the east.

• Residential Townhouses (East): Two-storey residential townhouses are located on the eastern one-third of the site up to the intersection of Lansdowne Road and Cedarbridge Way. These townhouses are setback 4.0m (13.1 ft.) from the linear park with front entries leading onto patios located above landscape retaining walls ranging from 0.60 m (2.0 ft.) to 1.0m (3.3 ft.) above a landscaped strip and adjacent sidewalk within the park.

### Architectural Form and Character

The proposed building design incorporates some features of the "International Style" of architecture. Major architectural form and character elements are discussed below.

### Towers with Rounded and Angular Forms

- Buildings A, B and C are provided with extensive wide, rounded concrete slab overhangs and balconies that give a strong horizontal building expression towards each of their street elevations. In addition, the three (3) building's walls have a light appearance with extensive window walls composed of clear glass and opaque blue glass spandrel panels.
- Buildings A and B also have angular wings with curtain walls composed of clear glass and "Maple Sugar" (mustard colour) metal spandrel panels respectively facing Lansdowne Road and Cedarbridge Way. The vertical shape of the windows and spandrel panels along with the smooth building facades increase the sense of verticality of these building components. The strength of the angular tower elements is in juxtaposition with the adjacent curvilinear building forms of the elevations facing outwards to the streets, contributing to an interesting articulation of the higher building's mass.

• The lower six (6) storey wings of Buildings A and B facing Alderbridge Way include green roofs with sedum ornamental grasses and ground covers as was as large private patios.

### Interior-Facing Angular Facades of the Towers

 The three (3) main buildings each have angular facades facing inwards towards the landscaped podium with curtain walls composed of clear glass and "Maple Sugar" (mustard colour) metal spandrel panels with inset balconies with glass clad railings with horizontal mullions.

### Street-Oriented Townhouse Units

- At street level, two-storey townhouse units surround the majority of the site perimeter as
  discussed above to help create an animated streetscape. The townhouses have a strong
  angular repeating form that contrasts to the lighter, rounded forms of each of the three (3)
  larger building forms. The concrete townhouses are also predominantly painted "Maple
  Sugar" with smooth-face ebony brick applied to sections of the townhouse forms that
  contributes to a finer grain of architectural expression at the pedestrian level.
- All townhouse units have their lower levels elevated by 1.2 m (4.0 ft.) to 1.5m (5.0 ft.) above the street, allowing stepped down terraces/patios for connecting outdoor space from 1.0 m (3.3 ft.) to 1.2m (4.0 ft.) above the street. Concrete and pisa stone landscape walls stepping up to these patios are setback 0.5m (1.66 ft.) from the back of the sidewalk. The glass railings end entry gates have also been provided with horizontal mullions for additional light and transparency.
- The townhouses provide layering of intermediate and semi-private spaces between the private (townhouse) and the public (street), with landscaping to soften the building edges fronting the sidewalks. In summary, the townhouses reflect and frame the curvilinear streets, helping to create the pedestrian-oriented environment envisioned for the area.

### Landscape Design and Open Space Design

The landscape and open space components are outlined by geographic area a follows.

### Lansdowne Linear Park - Southern Elevation of the Project

- A section of the 10m (33 ft.) wide Lansdowne Linear Park is included within the proposed development with pedestrian and cycle-oriented routes on the south side of the project, following the City's conceptual design for the Linear Park.
- The proposed development fronts onto the section of the Lansdowne Linear Park that is intended to have a less urban and more residential environment than the sections of the park and street to the east and west of the development. Within this section of the Lansdowne

Corridor, the development includes three (3) distinctly-themed sections as discussed above and as follows: (1) To the east, an urban, hard-surfaced patio and trees facing the commercial space; (2) in the centre, a softer, greener environment focused on the enlarged pond with the waterfall and greenwall that rises out of pond; and (3) to the west, an urban residential character with townhouses facing a more natural Linear Park.

• A linear bioswale have been included along the western-most one-third of the Linear Park.

### Podium Level Landscape

The podium level includes a wide range of passive and active common on-site outdoor recreation uses that relate well to the two-storey amenity building on the podium level which include:

- An outdoor pool surrounded by patio area with an adjacent hot tub.
- An outdoor patio seating area with an outdoor kitchen and BBQ located between the amenity building and the feature green wall and cascading waterfall that drop to street-level by the Lansdowne Linear Park.
- As far as active sports areas, there is a sport court that can be used for a variety of informal net sports and a two (2) to five (5) year old play area.
- More passive recreation areas include a Tai Chi lawn, community garden plots, a fire pit and outdoor seating areas.
- Extensive green roofs are specified for approximately half of the roof area of the lower six (6) storey wings of Buildings A and B and most of the roof of the seven (7) storey Building C.
- The podium-level units in each of the three (3) buildings are provided with large patios, substantially larger than the 18.5m (200 ft<sup>2</sup>) minimum area recommended within the Development Permit Area guidelines.

### Crime Prevention Through Environmental Design (CPTED)

The project addresses a number of CPTED principles as follows:

- The project's street frontages include townhouse units that provide for casual surveillance over the adjacent public sidewalks.
- The commercial/restaurant space located at the southwest corner of the site provides for increased pedestrian activity along its frontage, and the amenity area overlooking the Lansdowne Linear Park, provides for more intense surveillance over the park than provided by the residential units.

- The outdoor amenity space on the podium will be visually open, well illuminated, and offer multiple egress options with secured ingress from the public street below.
- Within the parkade, all parking spaces will be well illuminated and overhead security gates will separate resident parking from visitor/commercial parking area.

### Public Art

- The developer has offered to voluntarily provide \$159,223 to Richmond's Public Art Program to facilitate the commissioning of public art.
- The proposed adjacent Lansdowne Linear Park includes the "Walk of Art" in the City Centre Public Art Plan where the City may commission works of public art.

### Accessible Housing and Aging in Place

- The proposed development includes 14 Affordable Housing Units that are provided with the Basic Universal Housing features (detailed in the Development Permit plans) and that are designed to be easily renovated to accommodate a future resident in a wheelchair. These units are required to incorporate all of the accessibility provisions listed in the Basic Universal Housing Features Section 4.16 of Zoning Bylaw 8500, and are permitted a density exclusion of 1.86 m<sup>2</sup> (20 ft<sup>2</sup>) per unit.
- All of the proposed development's 237 units also incorporate Aging-In-Place features to accommodate mobility constraints associated with aging, which include:
  - Stairwell hand rails.
  - o Lever-type handles for plumbing fixtures and door handles.
  - o Solid blocking in washroom walls to facilitate future grab bar installation beside toilets, bathtubs and showers.

### Affordable Housing

Following the City's Affordable Housing Policy, the development will be including 14 affordable housing (low-end market rental) to the satisfaction of the City. The affordable housing units will have a total combined habitable area of at least 5% of the residential floor area ratio (FAR) permitted which equates to 972 m<sup>2</sup> (10,465 ft<sup>2</sup>). The units are denoted on the Development Permit plans and summarized on the following table.

Location	1 Bedroom & Den	2 Bedroom	Total
Building A	4		4
Building B	4		4
Building C	4		4
Townhouses		2	2
Overall Total	12	2	14

### On-Site Vehicle Parking and Loading

- The proposed project includes a total of 295 parking spaces with 252 resident spaces, 43 visitor spaces (which includes the 17 shared commercial parking spaces) with the permitted TDM parking reduction of 10% set out within Bylaw 8500 with the provision of TDM measures by the development. These include, providing:
  - o 120V electric plug-ins for 20% of all parking stalls; and
  - o 120V electric plug-ins for electric bikes, one (1) for every 40 bicycle storage racks (if there are fewer than 40 bicycle racks in a storage compound, one (1) 120V electric plug-in is required for each compound).
  - o An interim 2 m (6.6 ft.) wide asphalt walkway along the east side of Alderbridge Way, from Lansdowne Road to a lane just north of Elmbridge Way.
- There will be registration of a covenant on title ensuring that the shared residential visitor and commercial parking is not assigned to any specific residential unit / commercial unit nor be designated by the owner or operator for the exclusive use of employees, specific businesses, and/or others.
- There also will be registration of a restrictive covenant allowing only right-in/right-out turning from the driveway on Alderbridge Way.
- The proposed development has also accommodated the required two (2) SU9 (medium-sized trucks) off-street loading spaces within the parkade located below the podium.

### Street Frontage Works & Servicing Highlights

### Lansdowne Park and Road:

The applicant will be building the Lansdowne Linear Park under the Servicing Agreement and transferring it to the City as a freehold parcel under the Rezoning Conditions. This rezoning condition also stipulates that, while a landscape plan for the park will be required at the Development Permit stage, the final landscape plan may be completed under the Servicing Agreement to ensure that that the park meets the final Lansdowne Corridor Plan. In this regard, the applicant has been required to provide a \$400,000 landscape security prior to rezoning adoption. City Parks and Transportation staff have been involved in the review of the Lansdowne Linear Park and adjacent street widening and frontage works and to ensure that the Development Permit meets the LCP's draft concept objectives and the Functional Road Plan requirements for the development.

The applicant will also dedicate land for widening of Lansdowne Road to accommodate the following (from south to north) to include: an eastbound travel lane, a left-turn lane/painted median, 6.45 m (21.5 ft.) wide westbound vehicle travel lanes and a minimum 2.0 (6.6 ft.) m wide sidewalk, 3 m wide two-way bicycle path adjacent to the proposed park.

### Alderbridge Way:

The developer will design and complete road widening to accommodate the following: a 2.0 m (6.6 ft.) wide concrete sidewalk; a 1.65 m (5.5 ft.) wide landscaped boulevard and road widening

to accommodate two eastbound/northbound lanes and one left-turn/painted median lane with the remaining space to the for the westbound traffic lanes.

### Cedarbridge Way:

The developer will design and complete road widening to accommodate the following: a 1.65 m (5.5 ft.) wide landscaped boulevard and a 2.0 m (6.6 ft.) wide concrete sidewalk.

### Sustainability

The development has been designed to meet the Canadian Green Building Council LEED 2009 Silver criteria. In this regard, Econ-Integration has provided a LEED Scorecard confirming how the project meets LEED Silver criteria. The proposed project includes at least 56 points which is within the LEED Silver category of 50 to 59 points out of the maximum 110 points under the LEED Project Checklist attached to the Development Permit.

The CCAP requires two LEED criteria to be met, including Heat Island Effect: Roof Credit and Storm Water Management: Treatment Credit which have both been met.

Other LEED measures within the development include:

- Transportation features are proposed to encourage the use of alternate travel modes, including bike storage for residents, visitors, convenient on-site delivery/passenger loading in the parkade and Electric vehicle charging stations for 20% of the residential and commercial parking spaces.
- A large part of the development site includes green roofs and permeable, absorbing landscaping on the podium.
- The development's podium landscape is also designed to be able to provide for runoff directed to Lansdowne Linear Park's soft landscape (subject to finalization of the design of the park with the City under the Servicing Agreement).
- Water efficient landscaping with planting that is drought tolerant and includes many native species, reducing water use.
- Domestic water use reduction, including use of low flow fixtures will be specified to conserve water.
- Optimize energy performance with high-performance mechanical and cooling systems.
- Construction waste management, including diverting waste to recycling.
- Use of low-emitting interior finishing materials such as adhesives, sealants, paints and carpets.

A letter of assurance will be required prior to issuance of an occupancy permit confirming that the building has met the LEED Silver score.

### District Energy Utility (DEU) Ready

As a rezoning condition, there will be registration of a restrictive covenant that ensures that no building permit will be permitted to be issued until the Developer enters into legal agreements to:

- Design and construct the buildings to facilitate a hook-up to a DEU system (e.g., hydronic water-based heating system); and
- Enter into a Service Provision Agreement and statutory right-of-way that will provide for DEU connection and associated access to the subject development when a DEU is complete.

### Noise Management

The applicant has provided sealed letters by registered professionals qualified in acoustics and mechanical engineering respectively from Brown Strachan Associates and SRC Engineering Consultants Ltd. dated April 22, 2013. These letters confirm how the building design accommodates the measures needed to satisfy the "Aircraft Noise Management" standards set out in the OCP and the noise management covenant being registered as a rezoning consideration. As the site is situated in a transitional industrial area and may be subject to noises not typical of other residential neighbourhoods over the short term, a covenant is also being registered on title during the rezoning phase, in this regard.

### Conclusions

The subject development is consistent with the OCP and CCAP Development Permit Area guidelines and land-use policies as well as the RCL2 zoning.

In particular, the proposed development's three (3) mid-rise building forms allows for a large podium landscape garden and varied building massing. Furthermore, the townhouses facing all three (3) adjacent streets will relate well to the adjacent public sidewalks, achieving the streetscape and character objectives for the subject development and future adjacent developments envisioned by the CCAP.

In addition, the unique form and character of the subject development the dedication and development of the first section of the 10 m (33 ft.) wide Lansdowne Linear Park and adjacent widening of Lansdowne Road are significant aspects to this project.

Overall, the proposed development will contribute to creating the desired character of Lansdowne Village and broader City Centre area. On this basis, staff supports issuance of the Development Permit for this development.

Mark McMullen

Senior Coordinator-Major Projects

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The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter-of-Credit for on-site landscaping in the amount of \$813,230 (based on estimates provided by the landscape architect including 15% contingency).
- The owner entering into a restrictive covenant to be registered on title that stipulates that the development is subject potential loss of views in any direction due to other developments that may be approved in the City Centre and requiring that the owner provide written regarding potential this potential loss of views.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<a href="http://www.richmond.ca/services/ttp/special.htm">http://www.richmond.ca/services/ttp/special.htm</a>).



# Development Application Data Sheet Development Applications Division

DP 12-626615 Attachment 1

Address:	7680 a	and 7720 Alderbridge		
Applicant:		Ciccozzi Architecture Inc.	Owner;	Amacon (Alderbridge) Development Corp.
Planning Ar	ea(s):	City Centre – Lansdowne Village		
Floor Area (	Gross:	19,905.6 m <sup>2</sup>	Floor Area Net:	19,879.8 m²

	Existing	Proposed
Site Area:	10,892 m²	8,829.9 m <sup>2</sup> net area 9,939.9 m <sup>2</sup> including park for FAR calc. under Byław 8946
Land Uses:	Industrial / Warehouse	Mixed-Use Commercial / Residential
OCP / CCAP Designation:	Mixed-Use / Urban Centre T5 (25 m)	Mixed-Use / Urban Centre T5 (25 m)
Zoning:	"Industrial Retail (IR1)"	"Residential/Limited Commercial (RCL2)"
Number of Units:	None	237

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	2.0	2.0	none permitted
Lot Coverage:	Max. 90%	45.4%	none
Setback-Front Yard - Lansdowne:	Min. 3.0 m	3.0 m	none
Setback-Side Yard - Alderbridge:	Min. 3.0 m	4.33 m	none
Setback-Side Yard - Cedarbridge:	Min. 3.0 m	3.0 m	none
Height (m):	Max. 35 m	34.62 m	None
Lot Size:	2400 m²	8829 m²	none .
Off-street Parking Spaces – Regular/Commercial;	241 spaces for 225 market residence; 11 spaces for 14 affordable units, and 43 visitor spaces with 19 shared commercial spaces with the 10%TDM reduction	241 spaces for 225 market residence; 11 spaces for 14 affordable units, and 43 visitor spaces with 19 shared commercial spaces with the 10%TOM reduction	Variance for commercial parking alste from 7.5 m to 6.7 m
Off-street Parking Spaces – Accessible:	2% (6)	2% (6)	none
Total off-street Spaces:	295	295	none
Tandem Parking Spaces	50%	17.6% (52 spaces)	none
Amenity Space – Indoor:	Min. 474 m <sup>2</sup>	474 m²	none
Amenity Space - Outdoor:	Min. 2296 m <sup>2</sup>	3,996 m²	none

## Excerpt from the Minutes from The Design Panel Meeting

Wednesday, February 20, 2013 – 4:00 p.m. Rm. M.1.003 Richmond City Hall

The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in 'bold italics'.

APPLICANT: Rob Ciccozzi Architects

PROPERTY LOCATION: 7720 Alderbridge Way

### **Applicant's Presentation**

Architect Rob Ciccozzi, Principal, Robert Ciccozzi Architecture, Inc., and Mark van der Zalm, Principal, van der Zalm + Associates, Inc., presented the project on behalf of the applicant.

### Remarks of the Chair

The Chair pointed out that in the preliminary presentation of the project last year, a significant discussion took place in the Panel regarding (a) the need for the City to come up with and present to the Panel a comprehensive plan for the Lansdowne Linear Park, (b) concern associated with development proceeding in advance of developed overall public realm design of adjacent space(s), and (c) the lack of involvement of outside consultants and the Advisory Design Panel in the design of the City's public realm compared to other cities in Metro Vancouver. He added that the Panel reiterates its previous request for more information and looks forward to the presentation of the comprehensive/overall plan for the Lansdowne Linear Park to the Panel and the overall public realm in general.

### Panel Discussion

Comments from the Panel were as follows:

The whole project works; appreciate the form and character and colour; footprint on top of the podium with three individual buildings is great; appreciate the subtleties of the curves.

### We appreciate the positive comments.

Appreciate the two (2) levels of parking inside which are completely hidden.
 Noted.

 Applicant could have further developed the landscaping of midrise roofs for semi-shared and semi-private uses they are visible from the higher floors/units.

The following towers have been proposed with greenroofs as per drawing L-05 within the Landscape package:

Building A: 6th Floor only will contain an extensive greenroof and gravel maintenance paths. (Approx 83m2 softscape)

Building B - 6th Floor only will contain an extensive greenroof and gravel maintenance paths. (Approx 124m2 softscape)

Building C - 7th Floor will contain an extensive greenroof and gravel maintenance paths. (Approx 383m2 softscape)

Upper podium - Approximately 4000m2 of intensive green roof (hardscape).

Would like to see more commercial shops; pushing the loading bay further in will create an opportunity for small shops to wrap around the loading bay area; will create a neighborhood feel in the development.

The suggestion is acknowledged however we note that the parking levels are currently designed to capacity (including a 10% TDM reduction in parking) and although we proposed alternate (off site) loading options, the City requires the on-site loading areas within the structure; therefore expansion of the commercial area is not feasible.

Would be nice to see efficient access between the bike lane/bike lockers coming out from the buildings.

An additional access door was added in the P1 level on the south façade (adjacent to the commercial area) with direct access to the Lansdowne frontage. This provides more convenient access to adjacent bike rooms from the bike lanes.

 Provision of 14 affordable units is not enough considering the size of the project.

The City requirement for affordable housing is 5% of market housing, which is met with the 14 Units provided.

 Ensure accessibility of the exercise and fitness area in the amenity building and the public congregation area on the podium; washrooms in these areas should include handicapped stalls.

The facilities in the Amenity building including washrooms, along with the outdoor amenity areas will be designed to meet the requirements for accessibility.

 Consider outward-opening doors/ pocket doors for powder rooms and ensuite washrooms in the residential units to facilitate the movements of the elderly and handicapped residents.

The affordable apartment units have been designed to the Richmond Adaptable Design Standards (Section 4.16) for accessible housing, with all required clearances to facilitate movement within the suites.

Like the scheme and the height variation of the buildings.

We appreciate the positive comments.

 Relationship to the ground plane on the three (3) sides of the development is excellent.

We appreciate the positive comments.

Response to the curvatures on the site is excellent.

We appreciate the positive comments.

- Buildings A and B have deep inside corners; however, they present opportunities for private landscaping.
   All the corners contain private patios for each unit. The units facing the upper podium also contain private patios, separated by a raised block wall/planter. Please see L-02 for further details.
- Framing of the townhouses is a bit heavy; needs a finer grain treatment.

The robust detailing at the Townhouse entries was intentional to create a strong presence and identity at street level. We do note that finer detailing within the mass is achieved with a pattern of reveals within the concrete façade.

• Curious on the rationale for the bright colours on the corners which is not a usual corner treatment.

The rectilinear vertical elements at the corners were created to ground the overall design with their continuation to the ground plane and juxtaposition against the curved elements. The colour selected reinforces the contrast between the tower elements while identifying with the strong Townhouse entry feature; thus creating continuity at the pedestrian level.

 Very well resolved project; form and character and massing are great; ground plane works well.

We appreciate the positive comments.

Consider further development on the ground plane along Cedarbridge Way to complement the work done on the Lansdowne Road and Alderbridge Way sides; blank wall at the loading bay and parkade access seems to be a forgotten piece.  Applicant has responded well to all issues raised by the Panel in the previous presentation of the project.

### We appreciate the positive comments.

 Project works well as a stand-alone project; however, more context information would help the Panel evaluate how the subject development will fit in and contribute to the diversity of the architectural character of this district.

### We note the comment and will add context into a 3D massing sketch.

 Very interesting mix of curvilinear building forms with rectilinear forms on townhouses and inner tower elements; works very well.

### We appreciate the positive comments.

• Overall, the scheme hangs together very well as a "whole block" development.

### We appreciate the positive comments.

Weakest piece of the project is the ribbon that connects the two (2) towers on Alderbridge Way at the podium level; further design development is needed to tie the two (2) towers together, e.g. trellis feature.

This element was further discussed after the close of the panel presentation. Rob Ciccozzi pointed out the strength of the ribbon connecting the two (2) towers was specifically proportioned to create 'movement' – and when viewed from below (as demonstrated in street level shots from the 3D model) it was agreed that the mass and scale of the ribbon was very appropriate when viewed as intended at pedestrian level.

 Amenity building changes are good improvements both for making this a special architectural treatment as well as improving the programming at the podium level that includes a mix of activities.

### We appreciate the positive comments.

Careful investigation done by the landscape architect on the project's interface with the Linear Park is appreciated; would like to see the overall scheme/design of the Lansdowne Linear Park.

Lansdowne linear Park is to be designed is to be completed and built under the Servicing Agreement after the City of Richmond's further review under the Lansdowne Corridor Process. The applicant will not be part of this work hereafter, apart from integrating both onsite and offsite hardscape and softscape.

Well resolved project; upper level podium has a lot of programs but is cohesively put together.

### We appreciate the positive comments.

 Urban agriculture area on the podium may lack sun exposure based on shadow analysis provided by the applicant.

### Revised. Please see drawing L-04 with new planter box locations.

• Consider rolling up the landscape treatment on the podium level to better integrate the raised swimming pool.

Please refer to LS-02 for a cross section. The landscaping does roll up to the pool. (Please note the model did not show the full detail of the finished grades).

• Sport court is interesting but investigate if the materials (e.g. grass) around it will stand up to use and outdoor exposure.

Revised. The sport court will now be artificial turf up to the edge of the concrete path.

• Long term maintenance of planters up on the green wall would be challenging for the future strata management; consider other alternatives such as vines planted in a good soil volume at the bottom.

An irrigation system will be incorporated within each planter. Gravel/pebbles over filter fabric will be added to the top surface layer to suppress weed growth.

 Concur with comments regarding the concern on the maintenance of the green wall; consider/investigate other methods that are not design dependent; appreciate that the landscape architect is moving away from water intensive and more sustainable systems.

Proposed plants are Carex, which will require pruning once a year in Feb/March. Carex are particularly hardy. This is a low maintenance requirement. The strata would be responsible for the occasional maintenance to the planters.

Landscaping on the podium is interesting and tied together well; however, look at the number of water features on the podium in view of the maintenance issues that might be encountered by the future strata.

Revised. Water feature within the asian garden has been deleted and water feature outside tower B has been deleted. The remaining water feature will require minimal maintenance. Please refer to www.aquascape.ca for further details on the low maintenance/energy requirements of these small water features.

 Applicant has responded well to the previous comments of the Panel during the preliminary presentation of the project.

We appreciate the positive comments.

International style and curvilinear design of the towers is more eye-catching than the standard Richmond towers.

We appreciate the positive comments.

 Like the idea of carrying the eyebrow/ ribbon across the two (2) buildings facing Alderbridge Way; however, consider a different material or dimension on the street level or in the podium.

Revised. The 'eyebrow' element has been strengthened by creating a thickening at these locations.

Agree with the comment that the corners are brighter instead of darker; however, they rise above and break up the roofline.

Refer to comments for rationale on colour choice on the corner elements. We also appreciate the positive comments.

Nice change in the amenity roof; distinct roof of the amenity building can be seen from the street and the podium; like the move of the applicant to pull back the amenity building from the edge of the podium; lay-out of the club is well thought out.

We appreciate the positive comments.

Landscaping is well developed; a lot of activities going on at the podium level; consider long term maintenance costs when management will be taken over by the future strata; look at the possibility of creating hard surfaces that do not require a lot of maintenance in lieu of some soft landscaping that may not be necessary.

Revised. Please refer to L0-4 following extensive revisions post ADP to provide additional lawn areas and planting areas. The asian garden, the courtyard and water feature outside tower two, two large landscape areas either side of the amenity area have been significantly revised to accommodate more softscape.

Current design of the green wall requires a lot of maintenance; however, if the design will not work particularly during winter, it will still create interest due to the presence of certain elements, i.e. stainless steel cables, wall planters, and the water feature to the side; water feature should be designed with consideration that it may be turned off and drained.

Acknowledged.

### Panel Decision

It was moved and seconded

That DP 12-626615 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Panel.

CARRIED



### **Development Permit**

No. DP 12-626615

To the Holder:

ROBERT CICCOZZI ARCHITECTURE INC.

Property Address:

7680 AND 7720 ALDERBRIDGE WAY

Address:

2339 COLUMBIA ST SUITE 200 VANCOUVER 8C V5Y 3Y3

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.

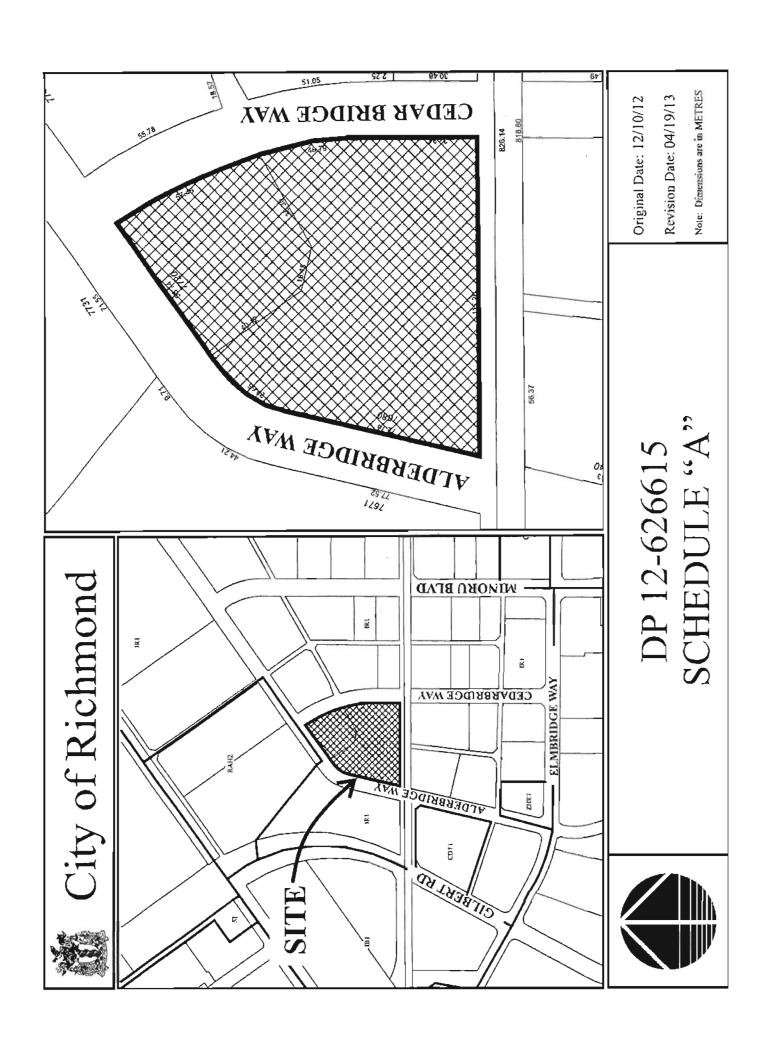
- 2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
- 3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
  - a) Reduce the required commercial parking aisle width from 7.5 m to 6.7 m.
- 4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans # DP 12-626615-1 to # DP 12-626615-58 attached hereto.
- 5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
- 6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$813,230 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
- 7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

### Development Permit No. DP 12-626615

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ROBERT CICCOZZI ARCHITECTURE INC.

To the Holder:



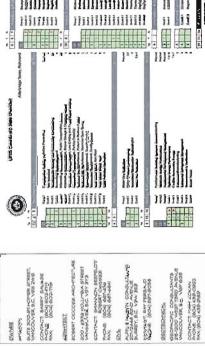
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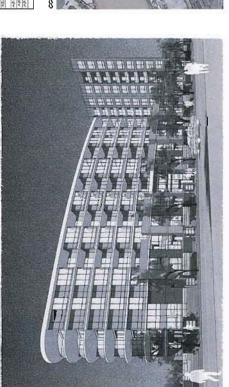


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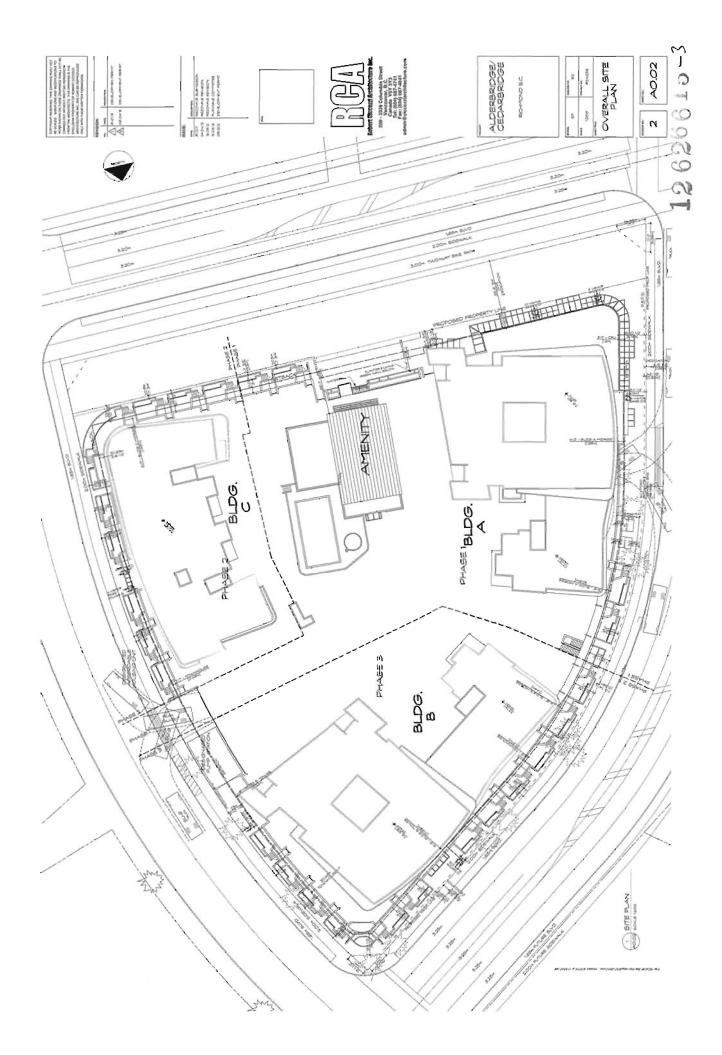
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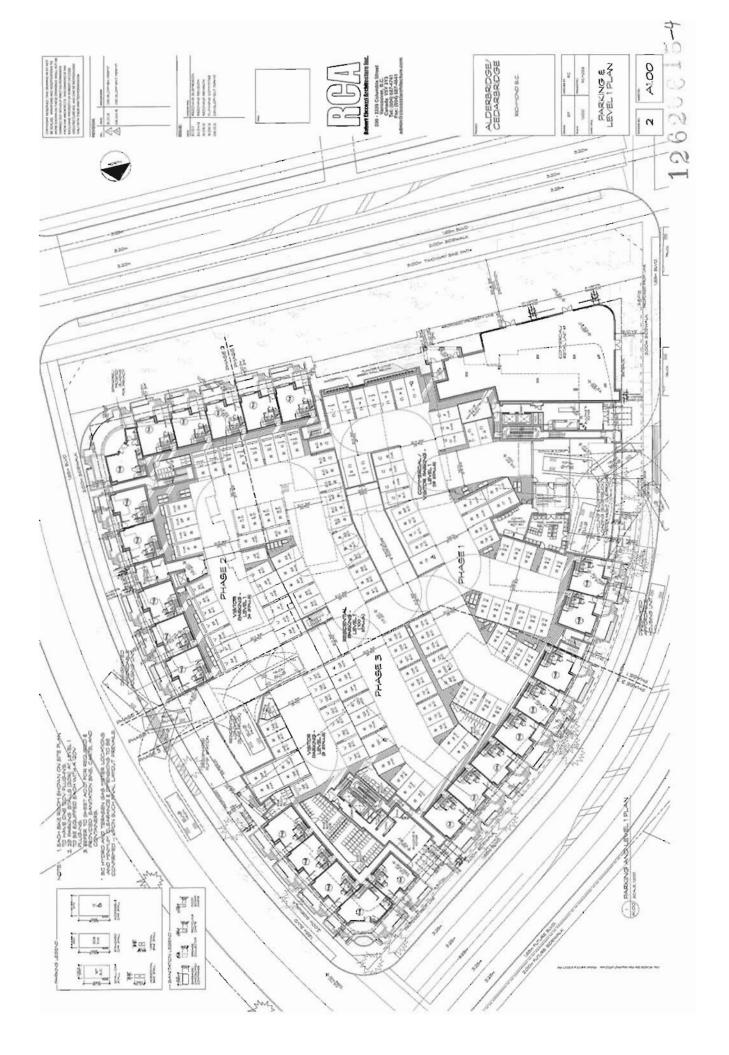
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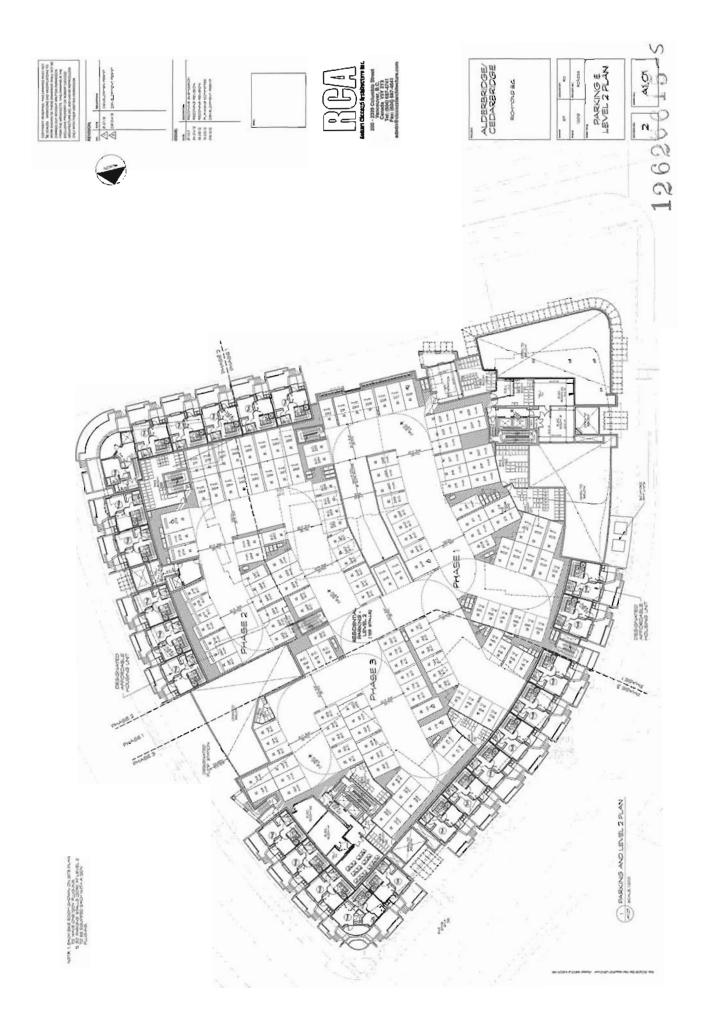
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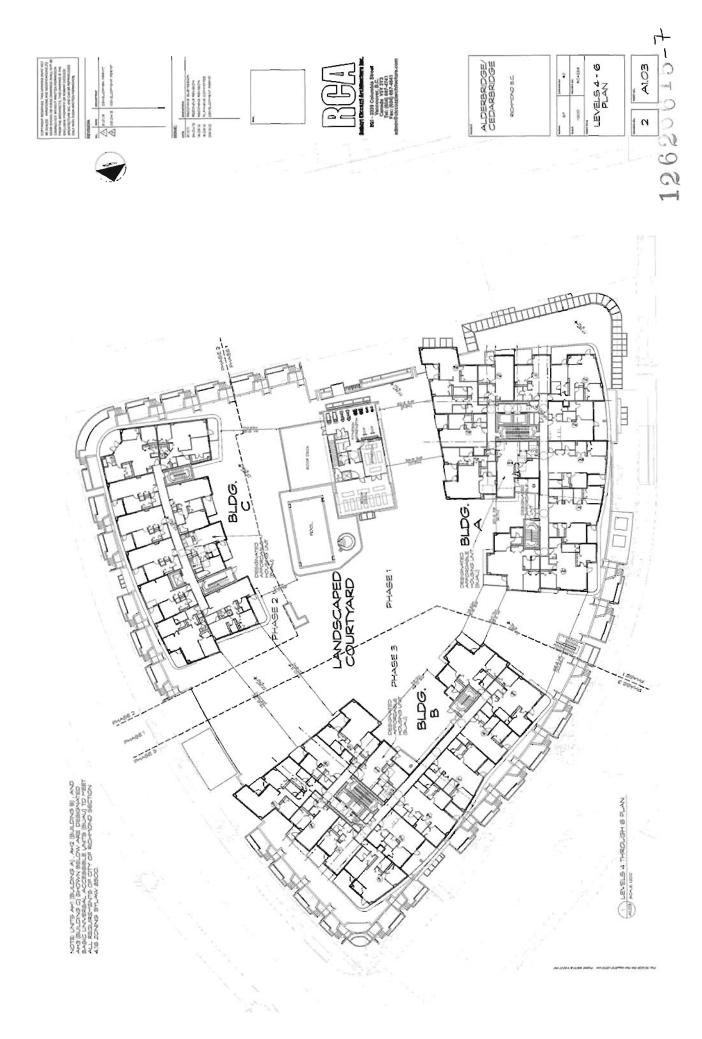
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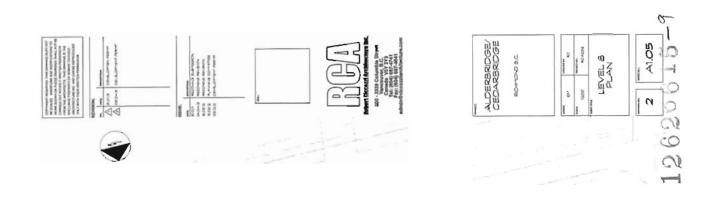


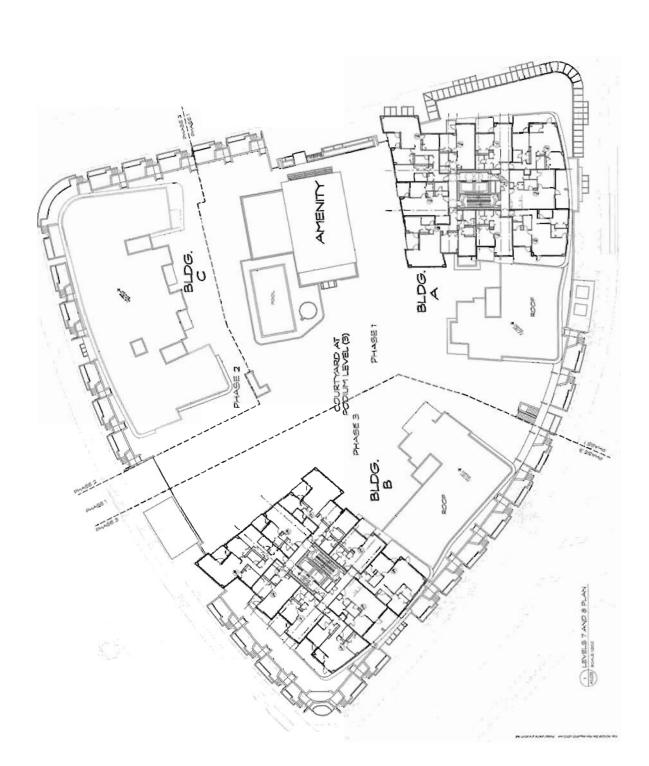


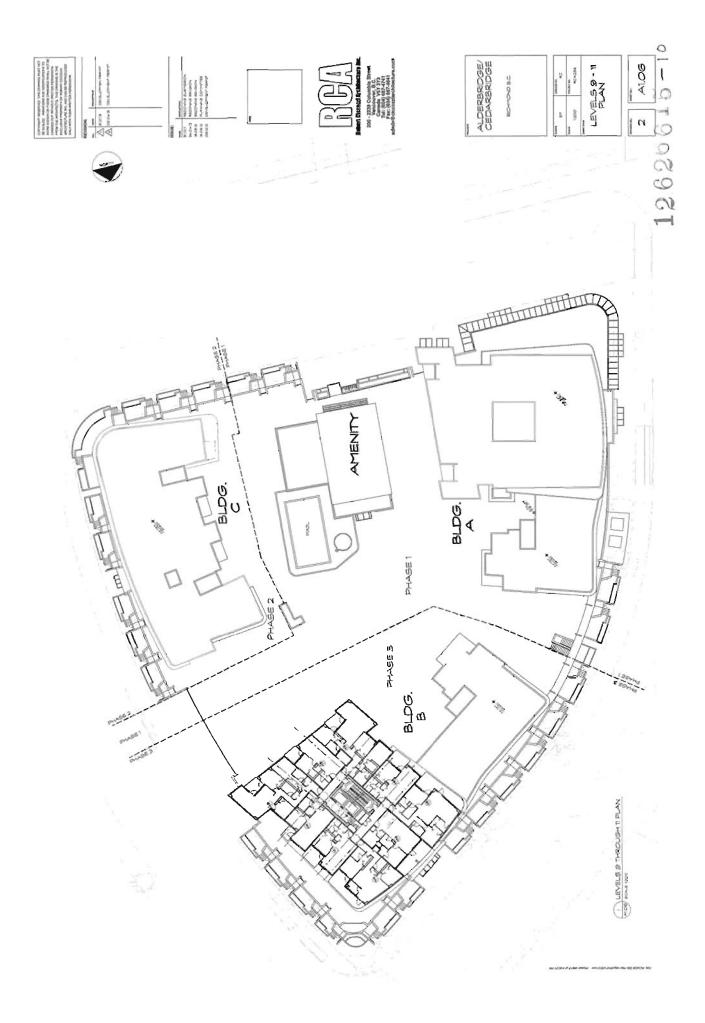


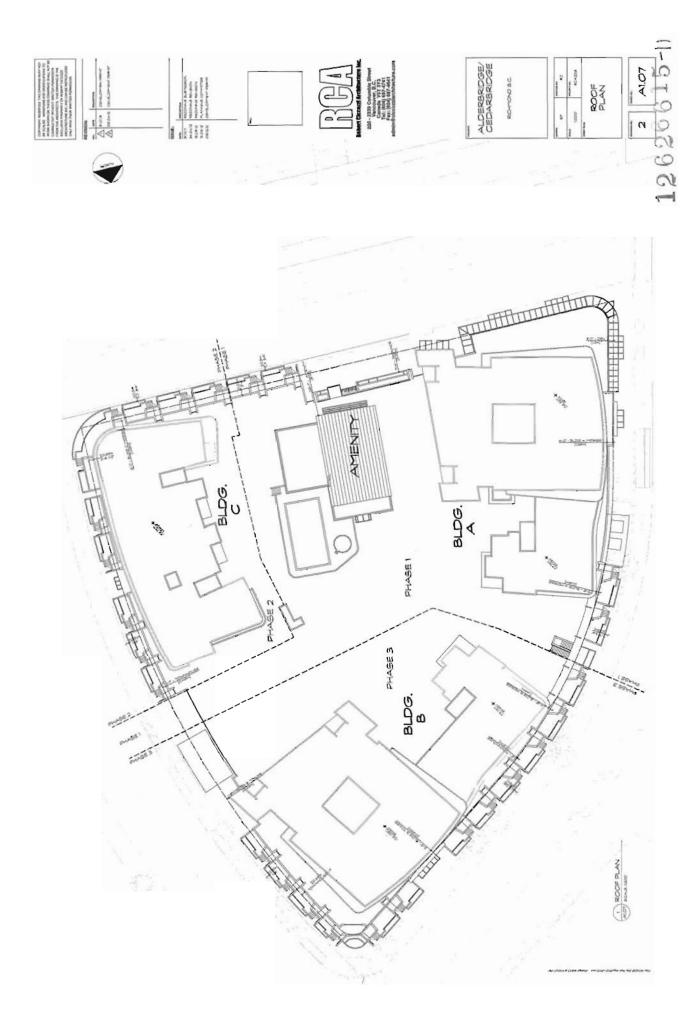


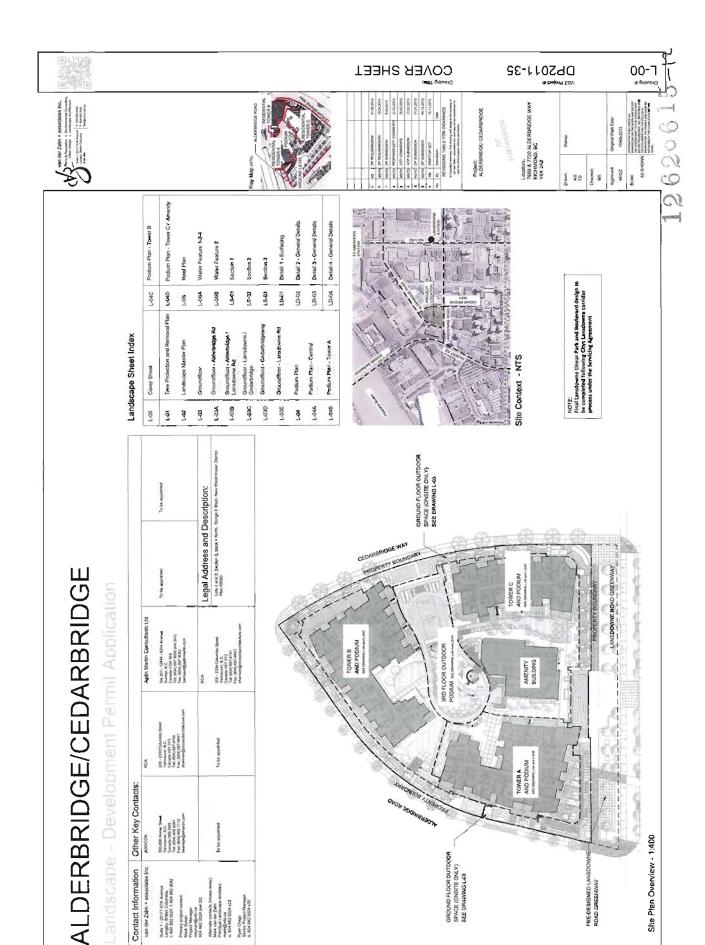












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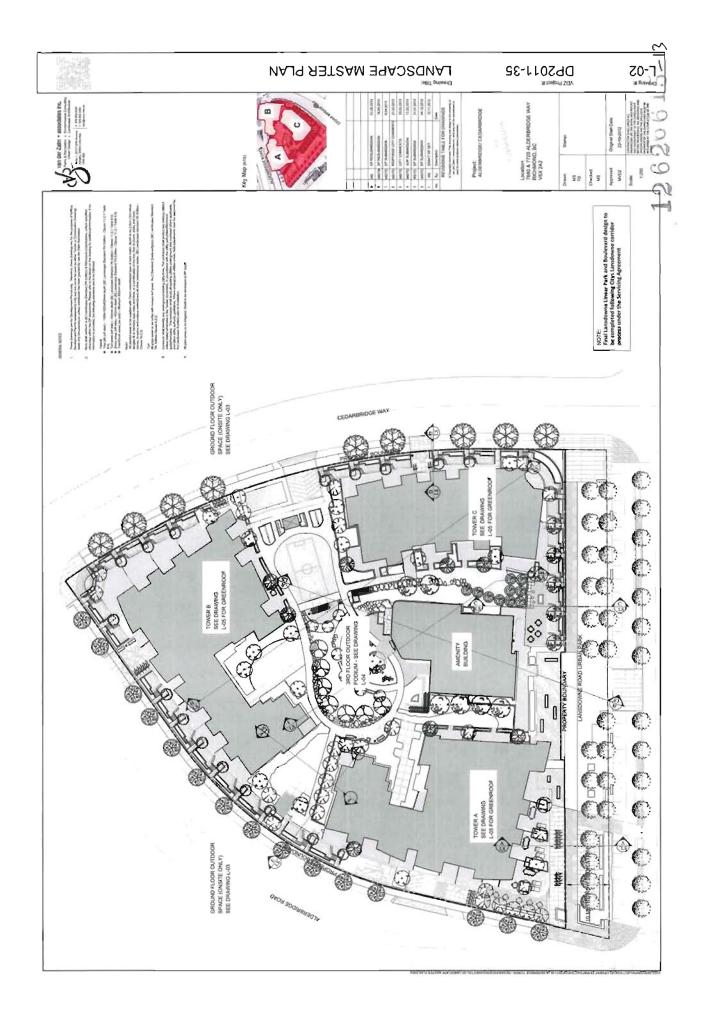
Site Plan Overview - 1:400

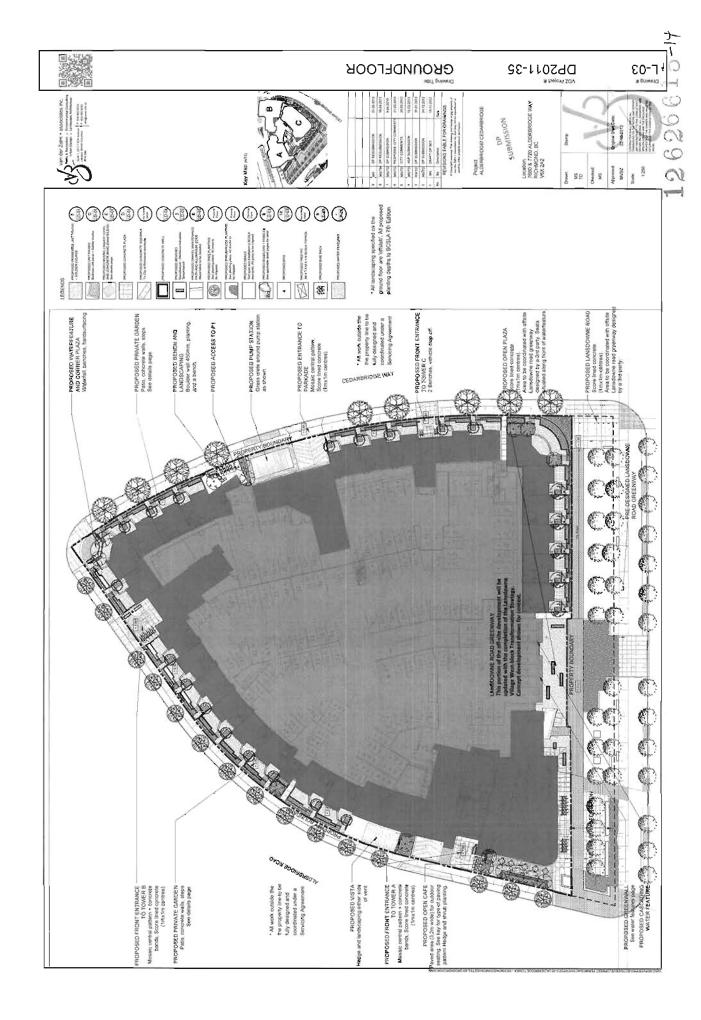
PRE-DESIGNED LANSD ROAD GREEHWAY

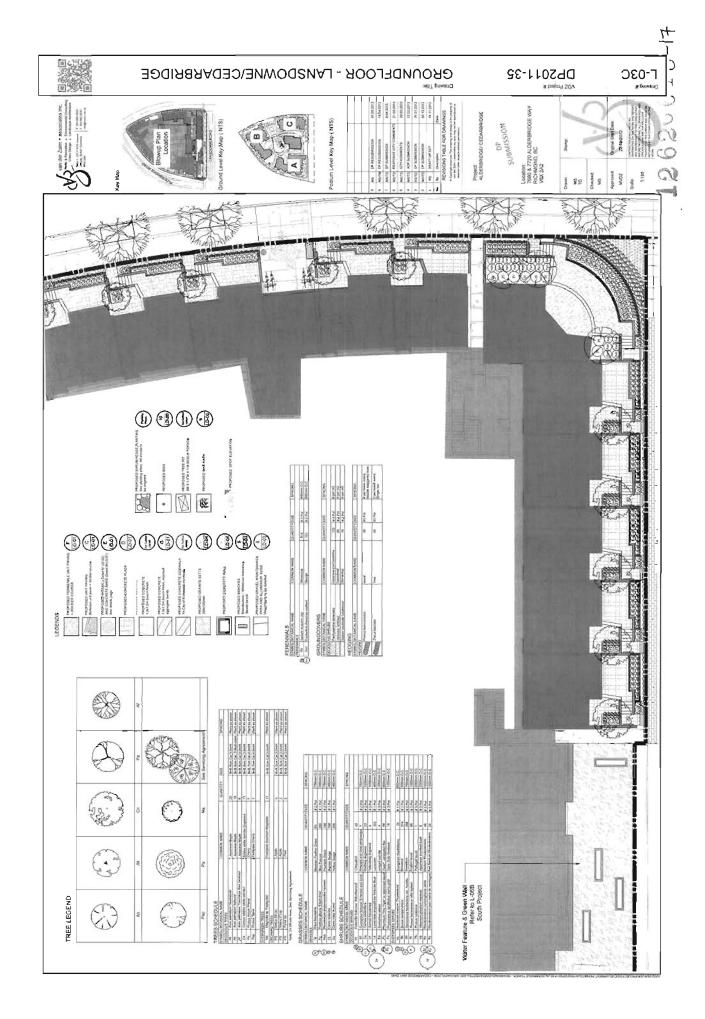
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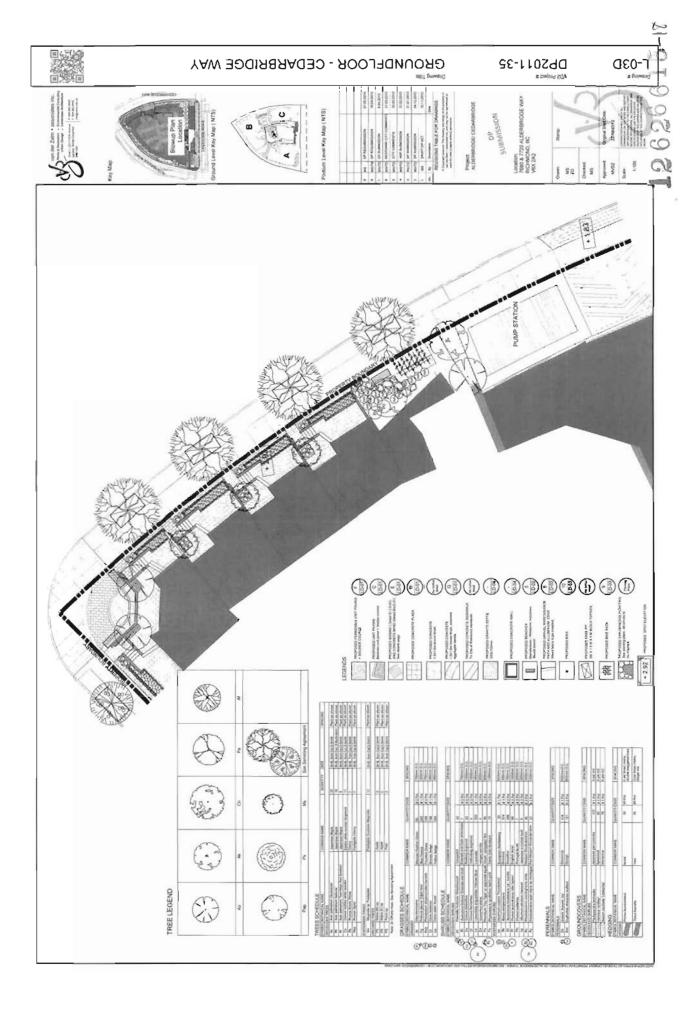
Contact Information Other Key Contacts:

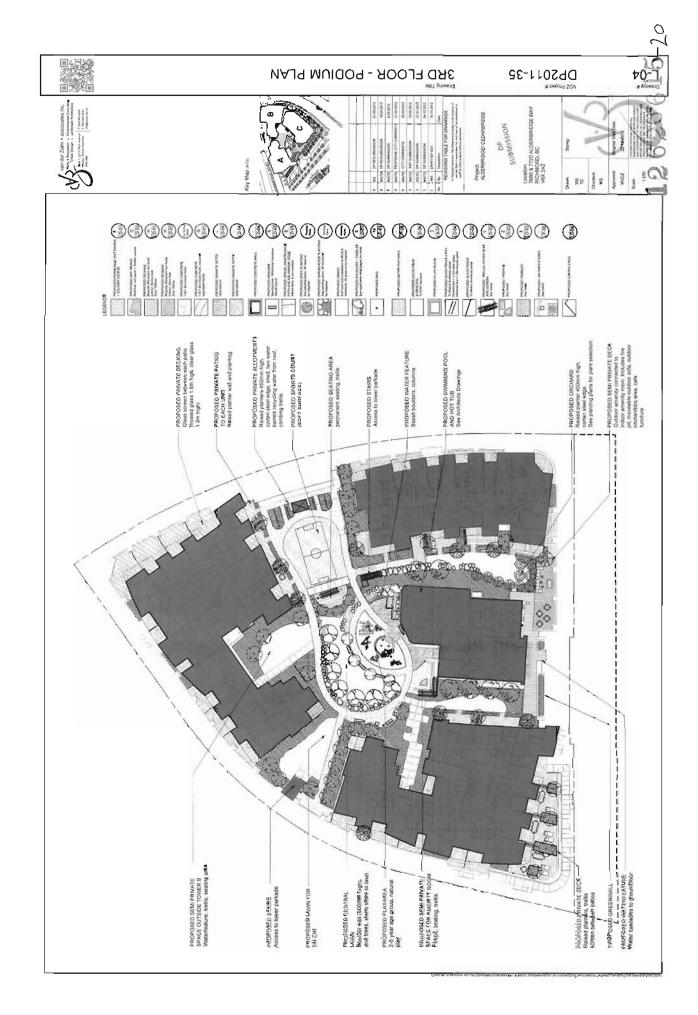
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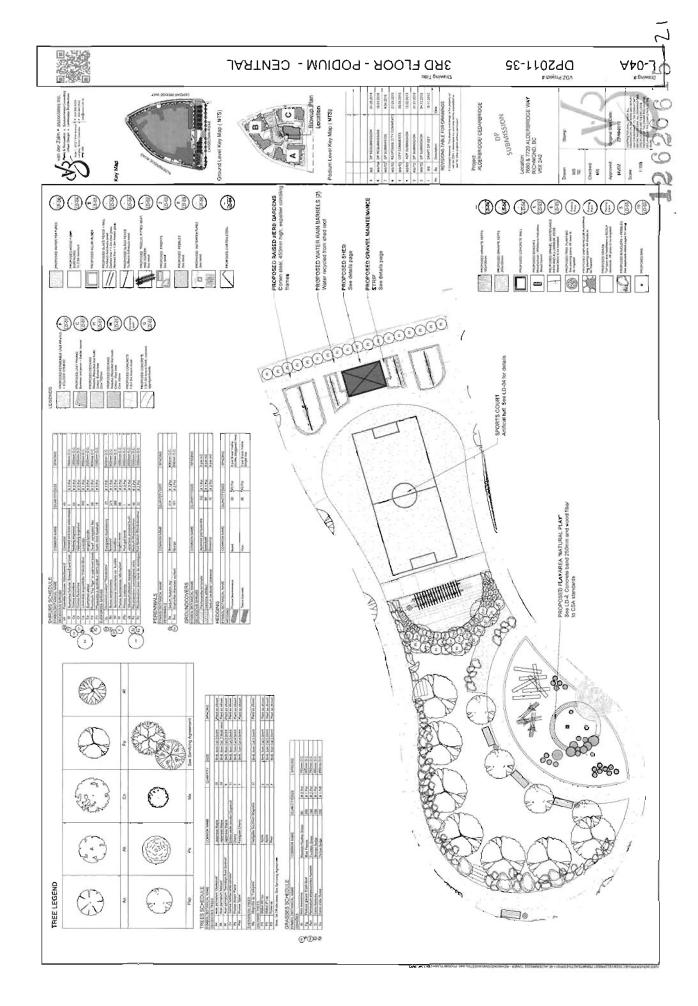


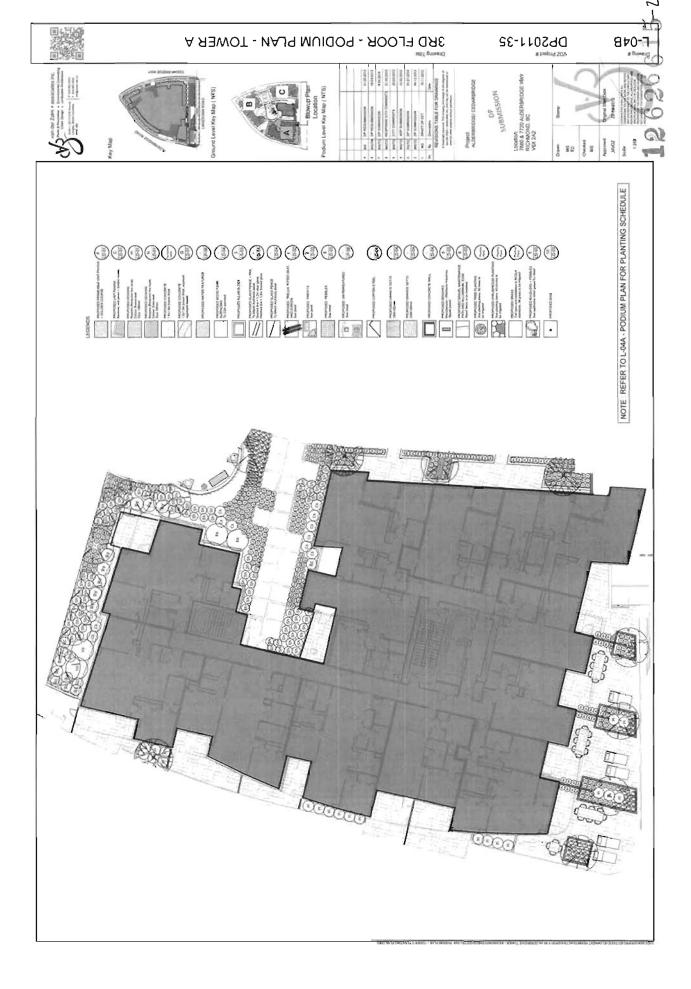


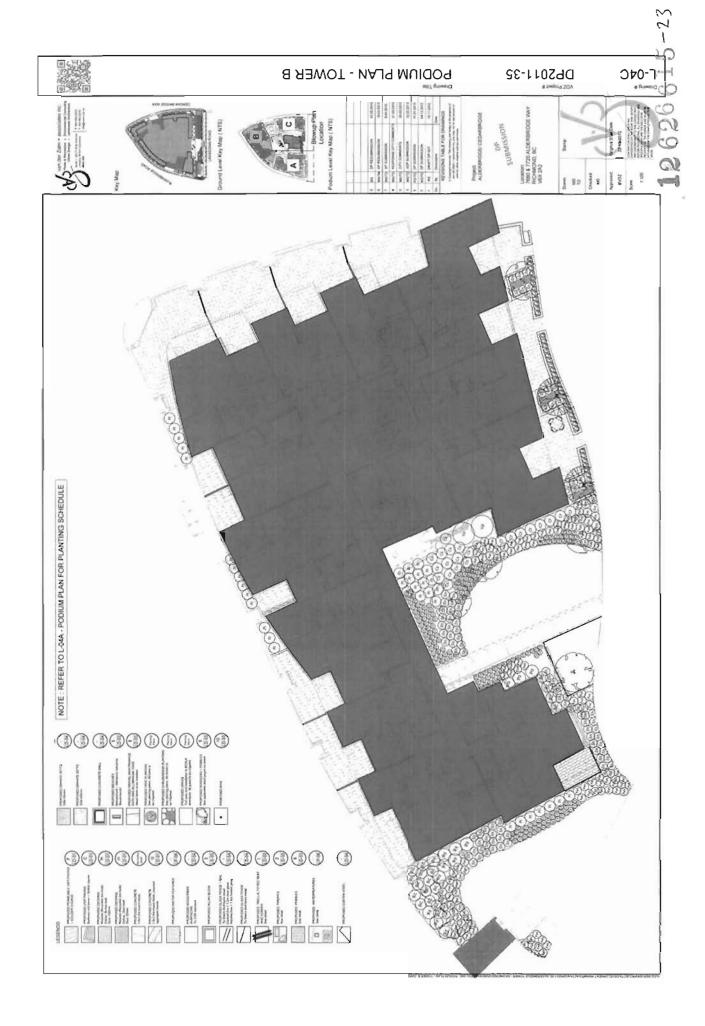


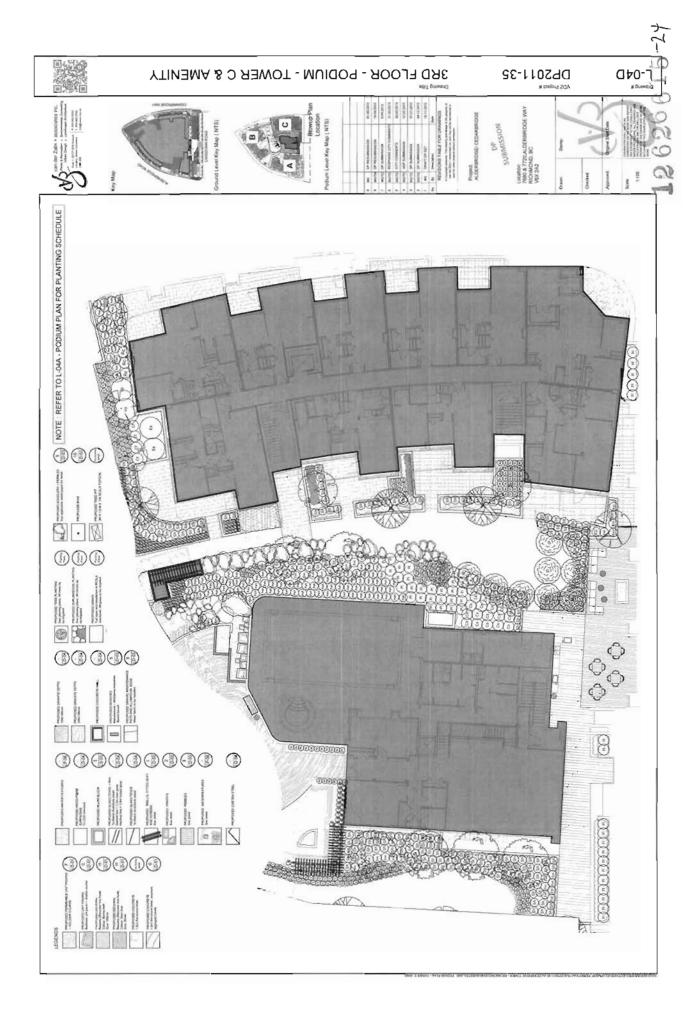


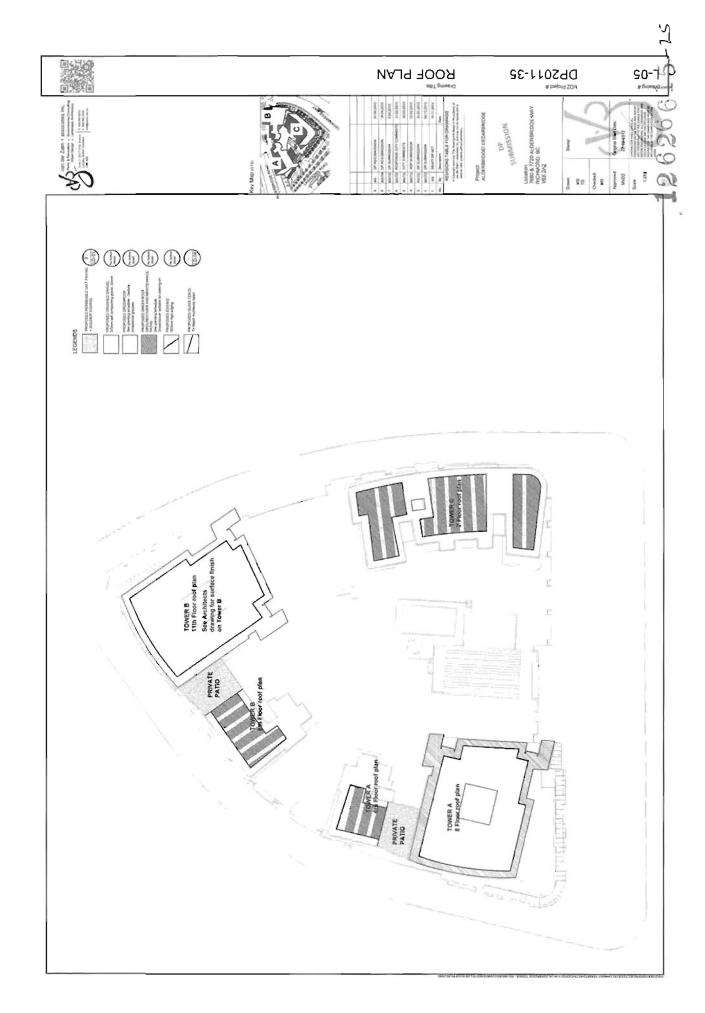


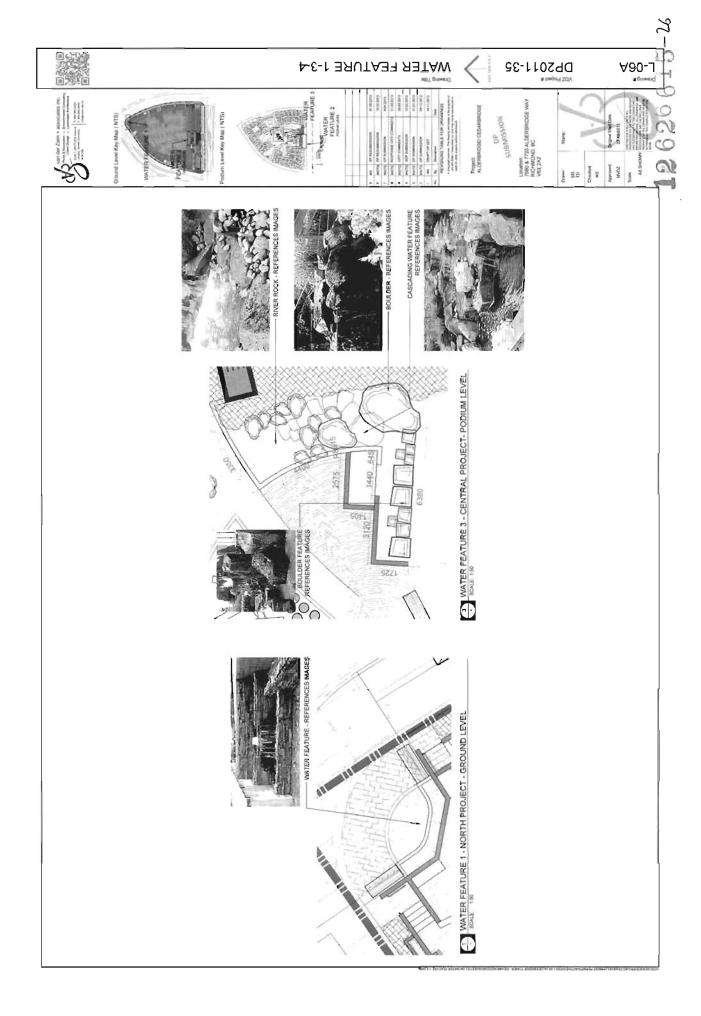




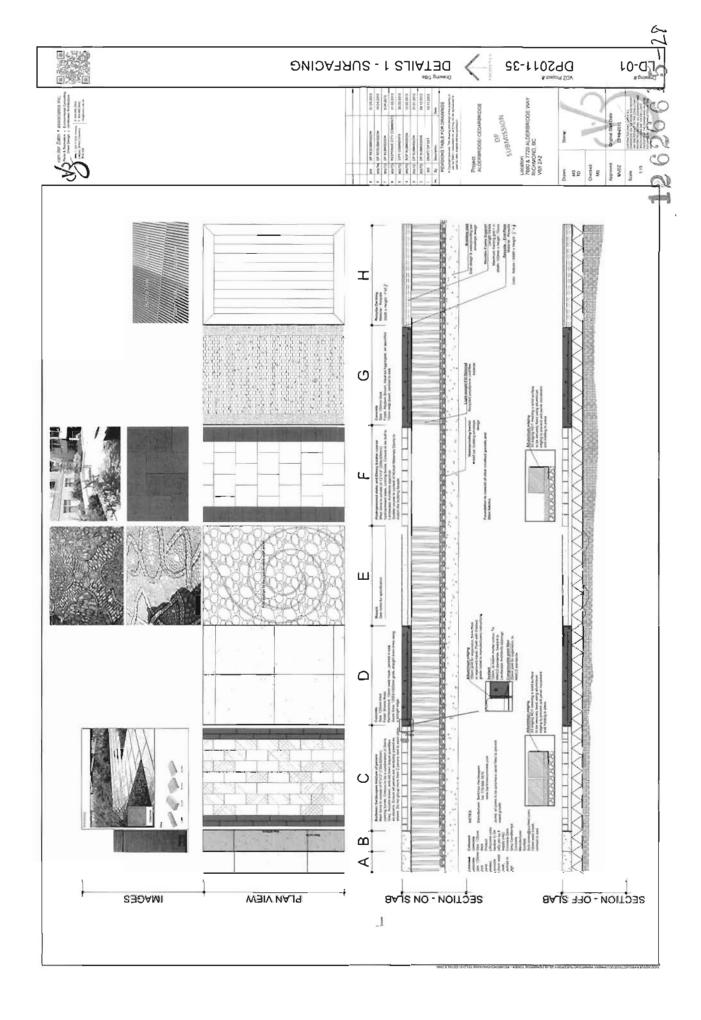


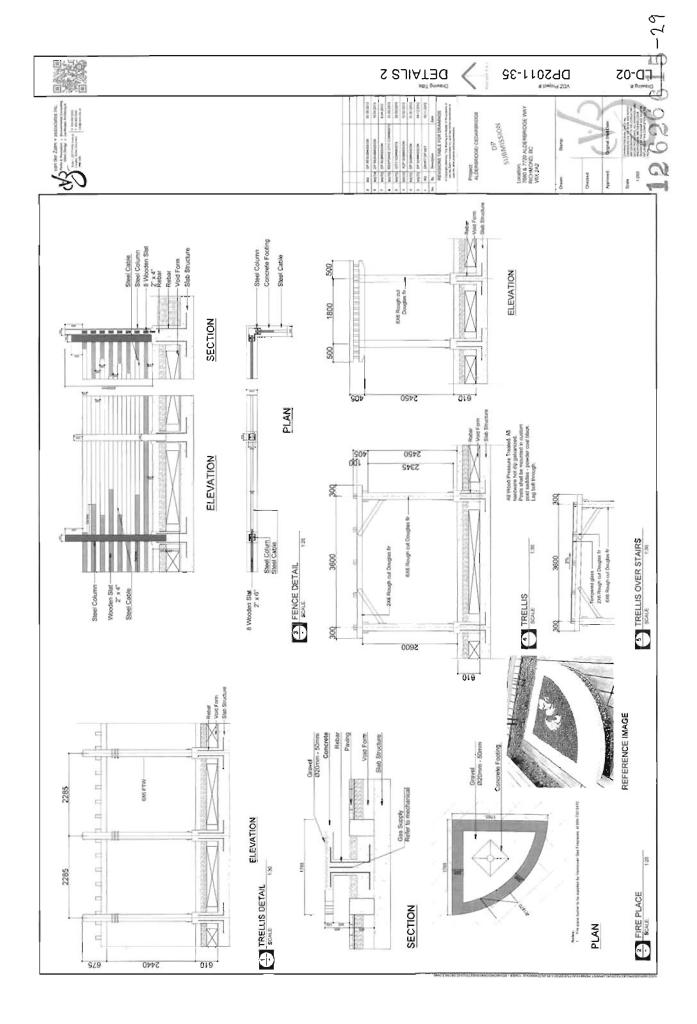


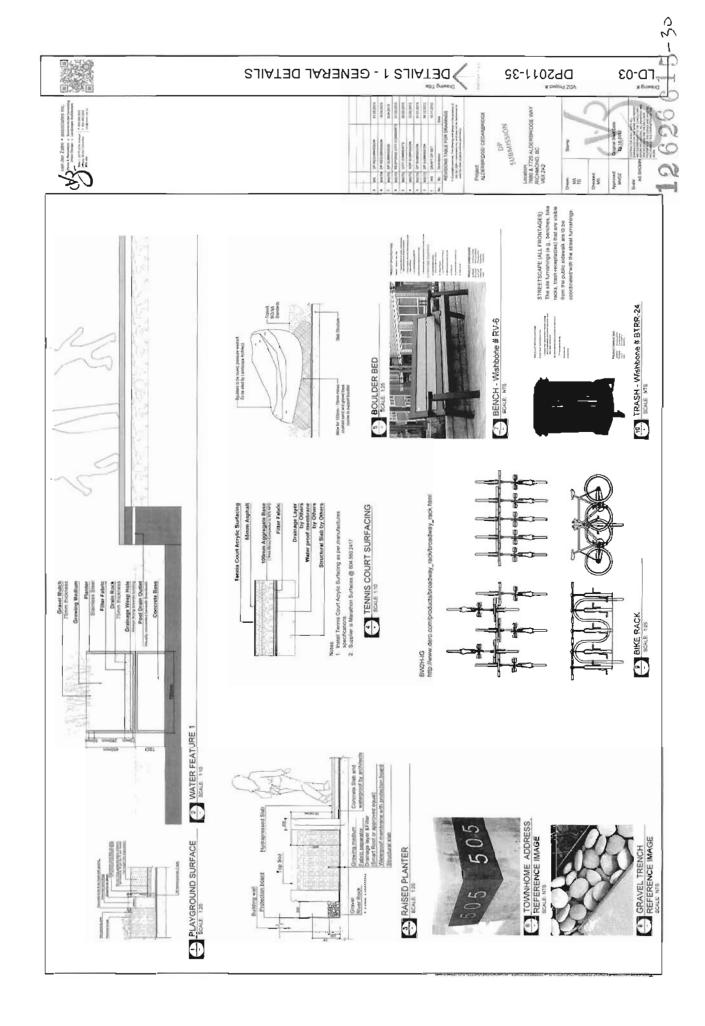


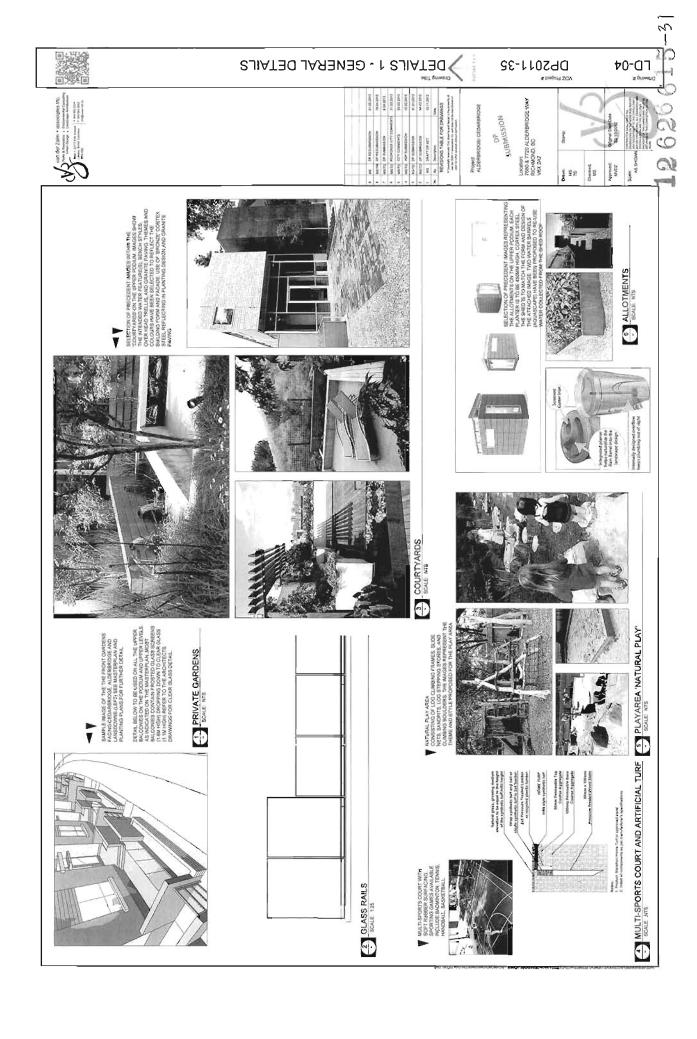


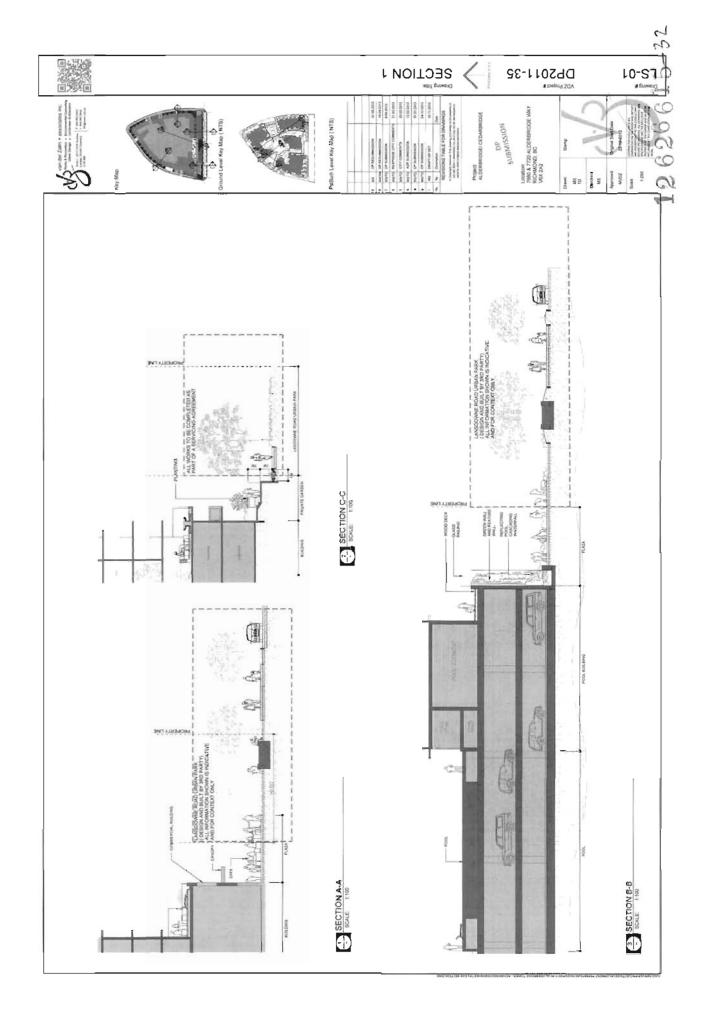
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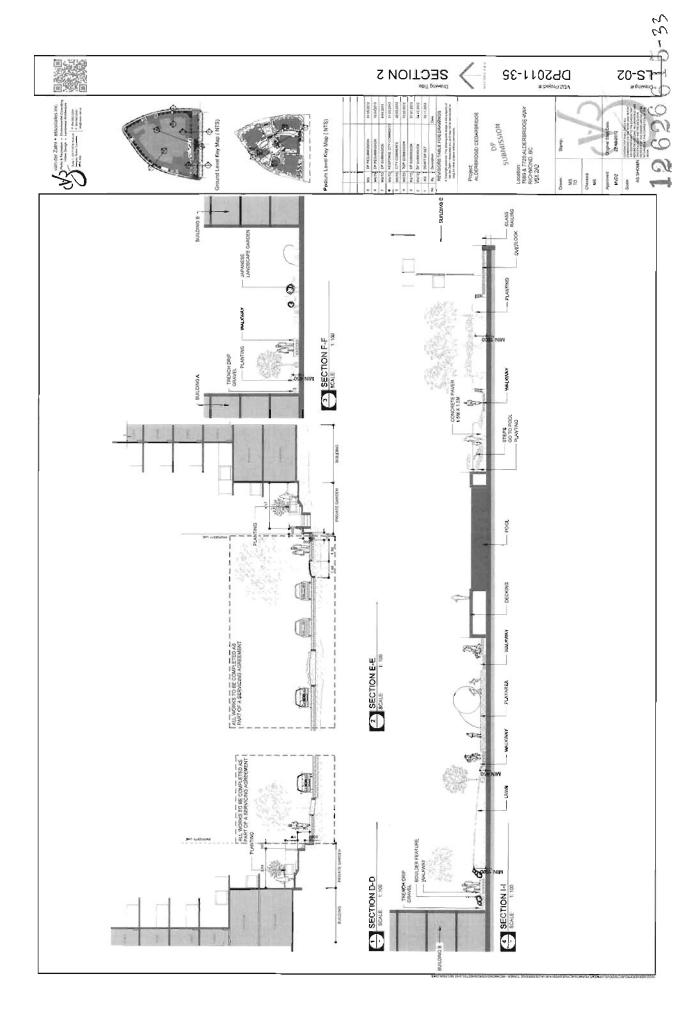


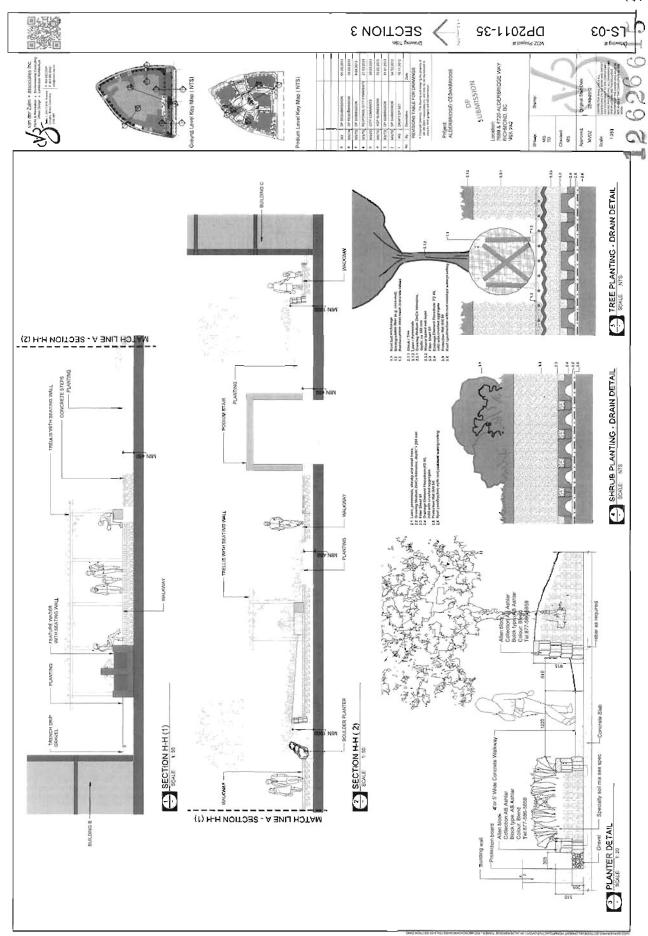


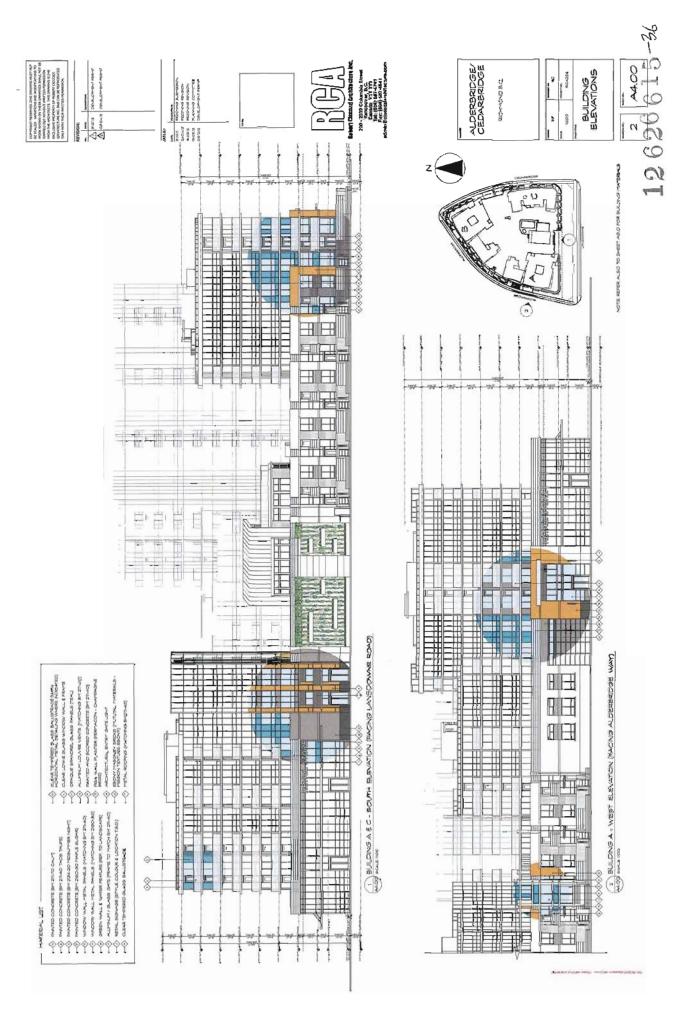


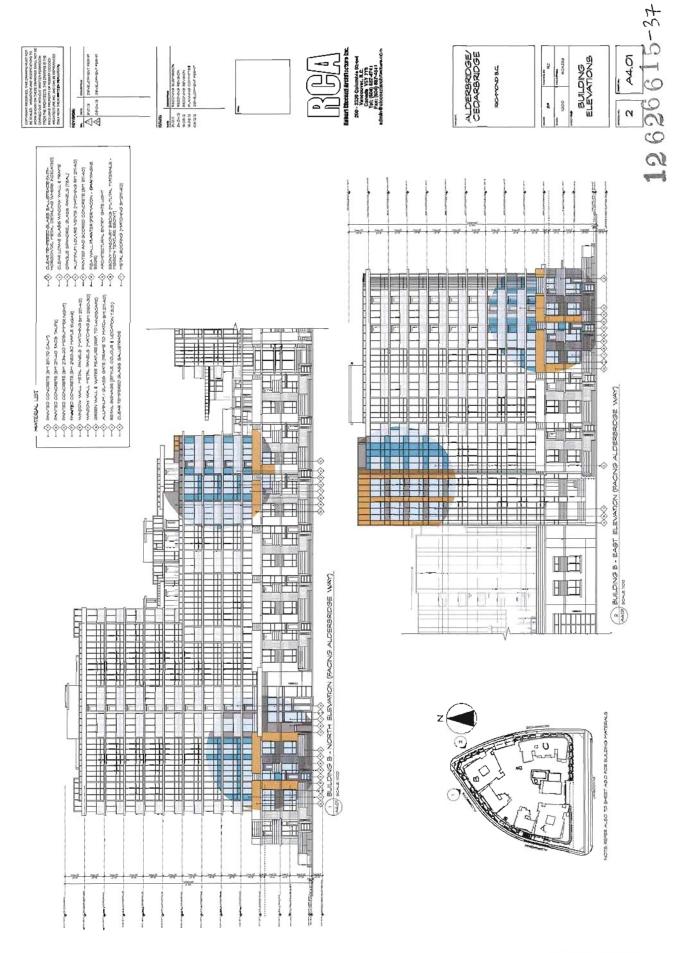


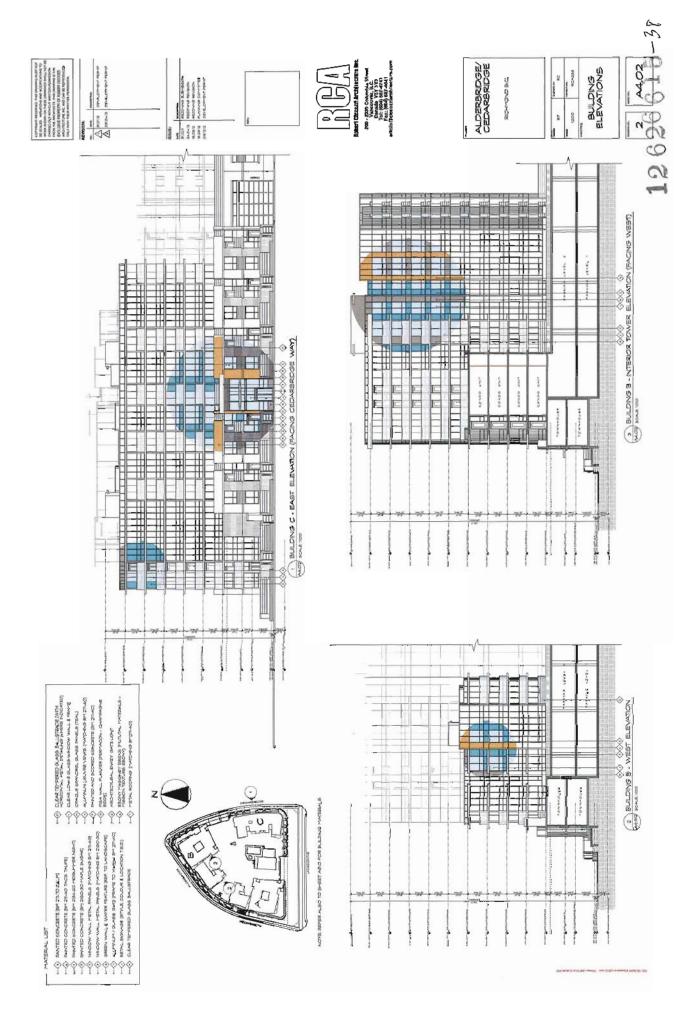


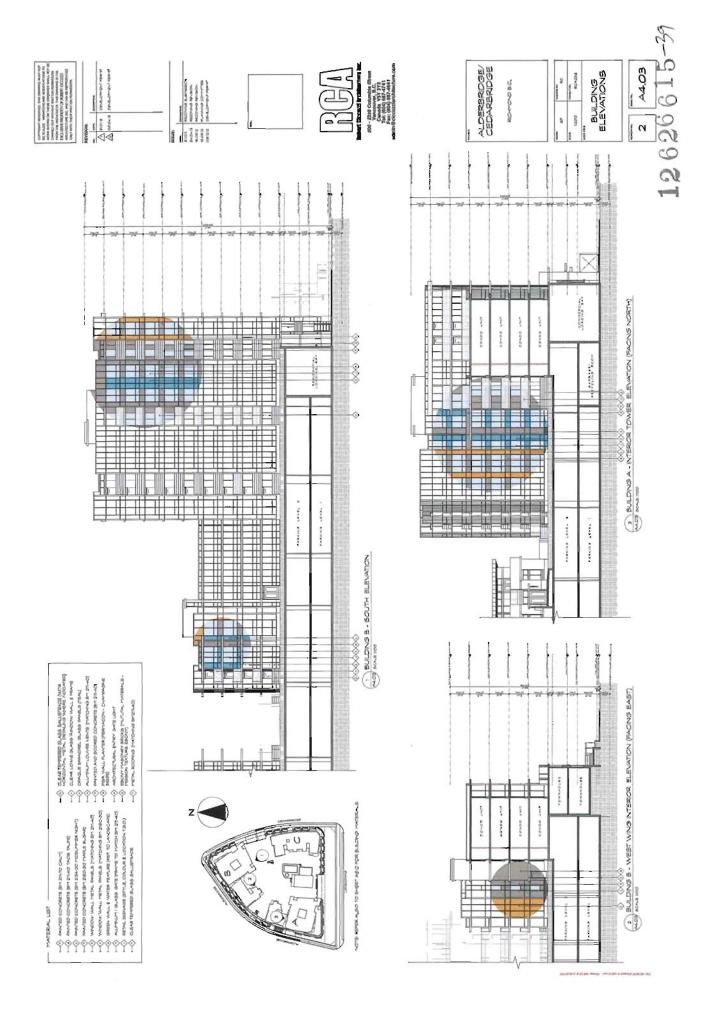


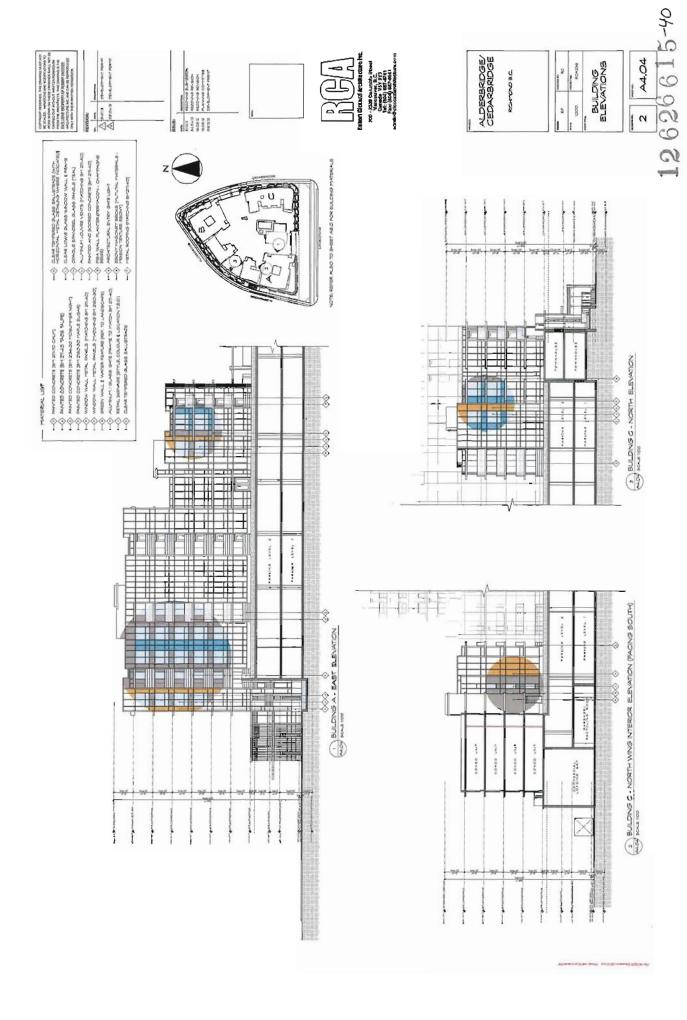


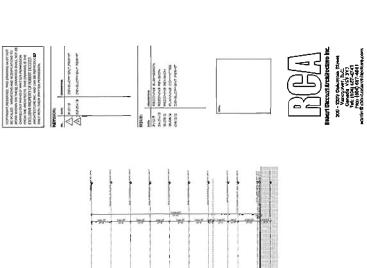














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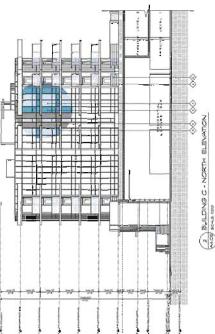


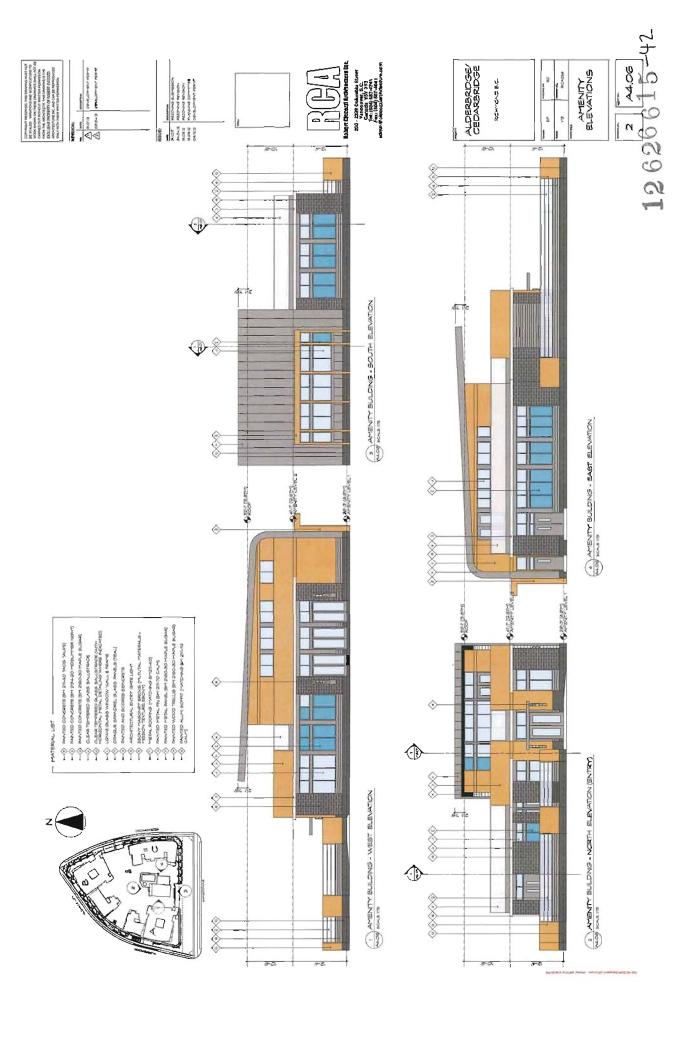
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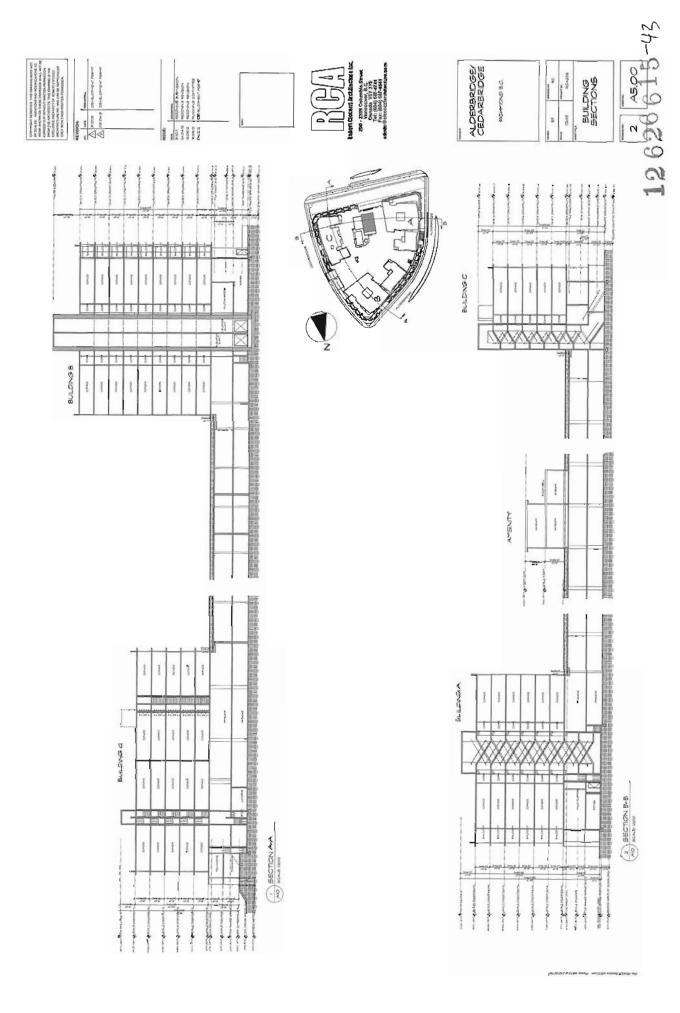
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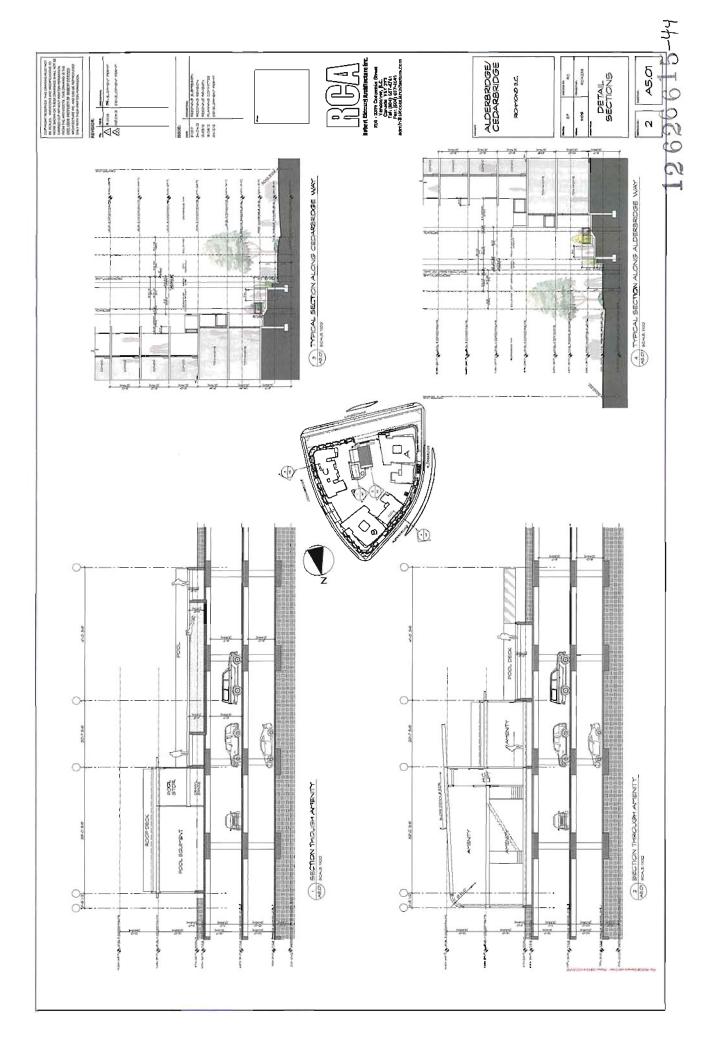
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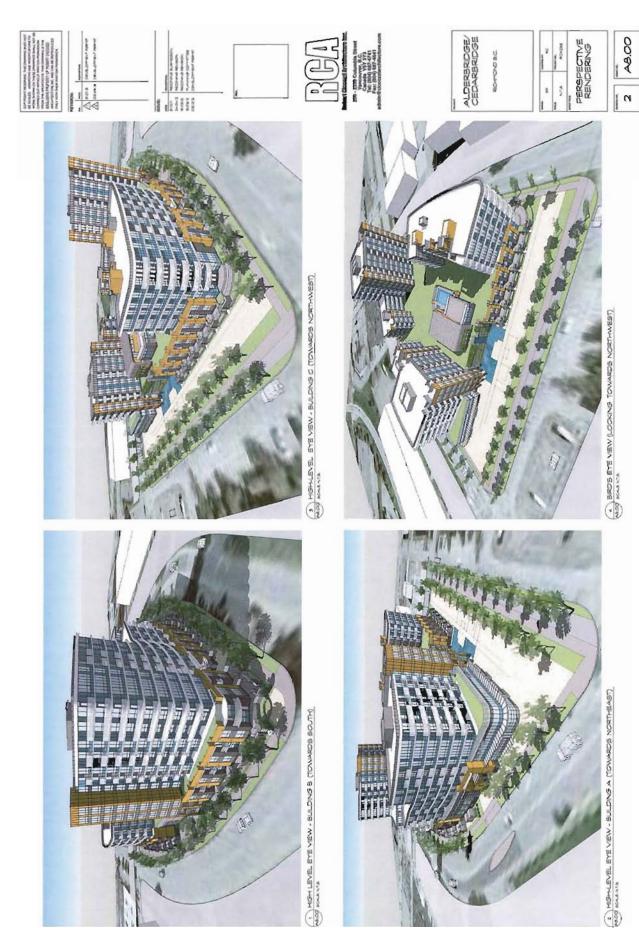
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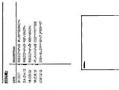






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4 BIRDS BYE VIEW (LOOKING TOWARDS SOUTHEAST)









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(4) BIRDS EYE VIEW LOOKING TOWARDS SOUTHEAST)





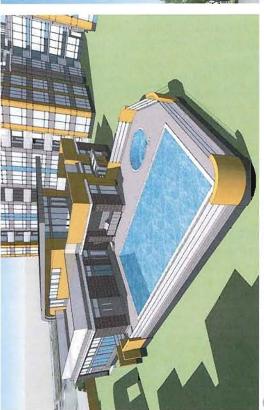
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TOWARDS SOUTH



3 STREET-LEVEL VIEW - BULDING A (TOWARDS NORTHEAST)



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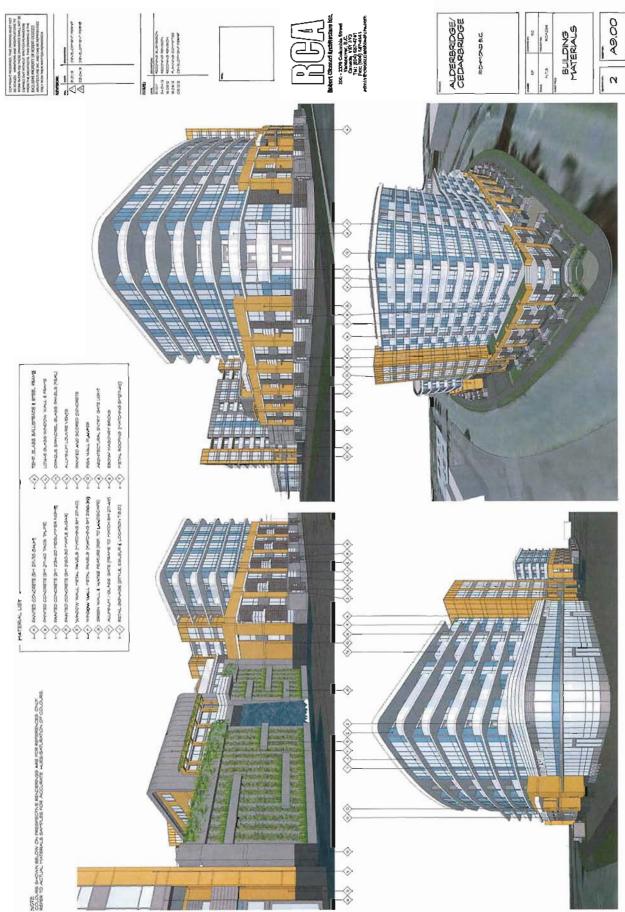


A STREET-LEVEL VIEW - GREEN WALL & WATER FEATURE









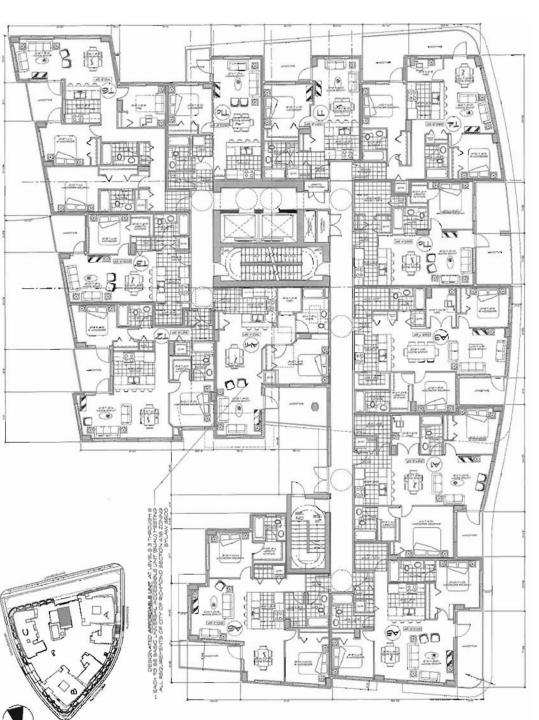
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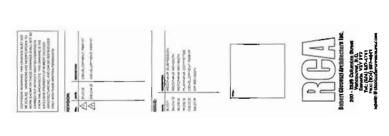


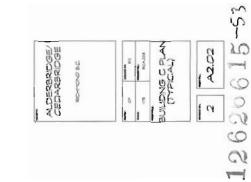
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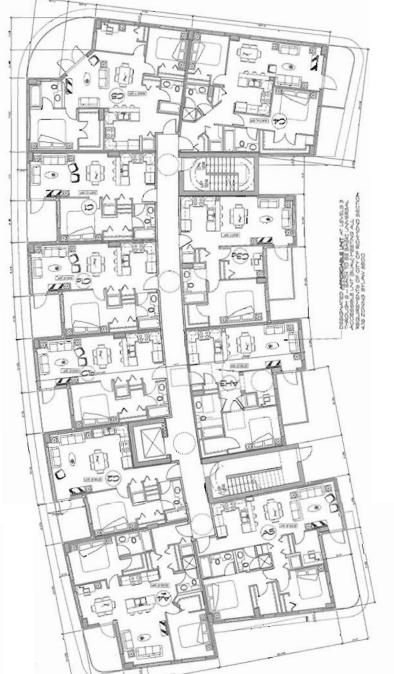
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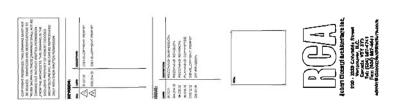


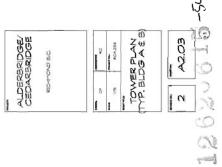


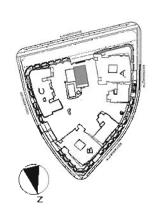


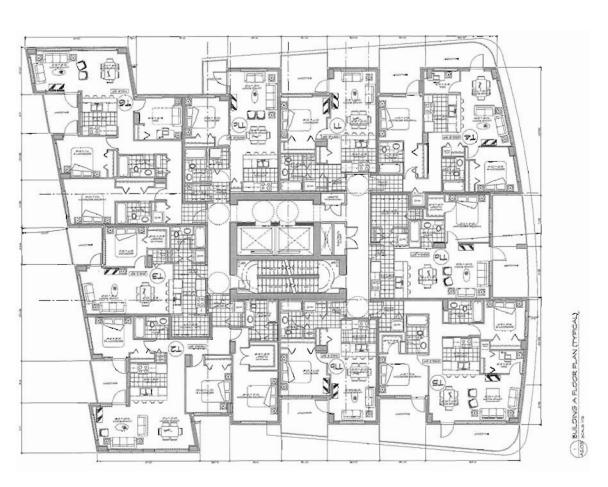


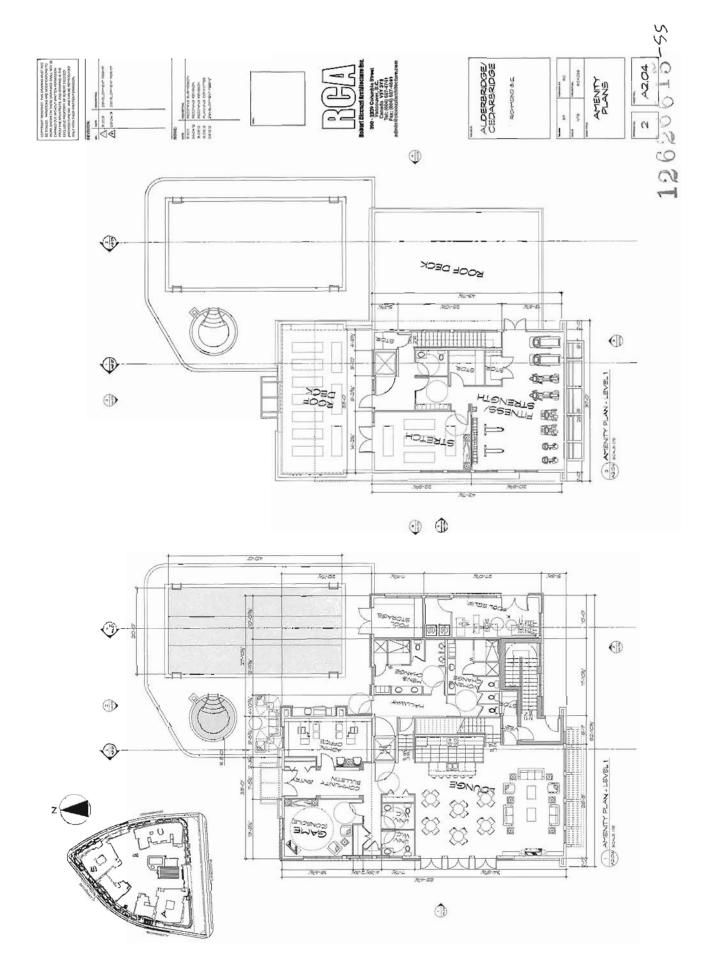
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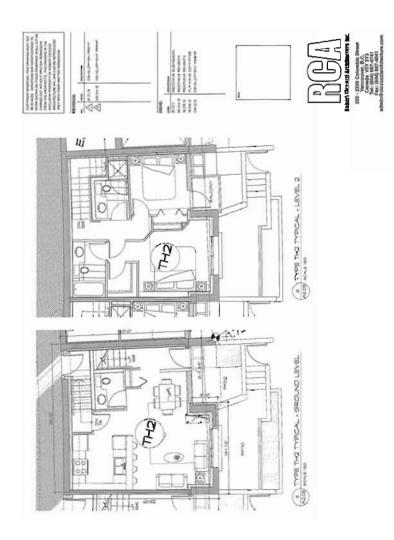


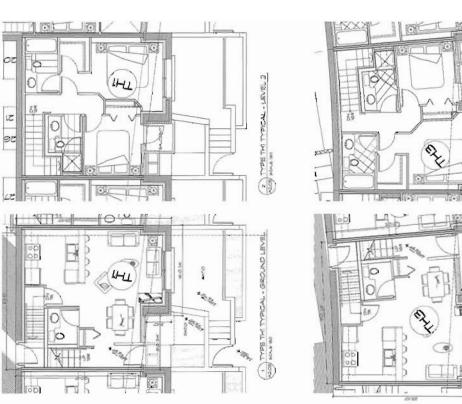


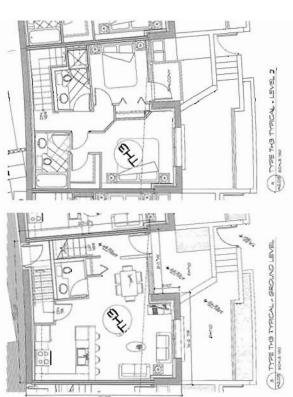


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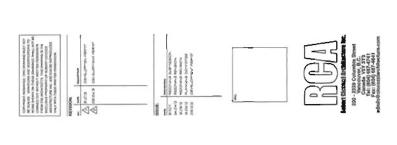
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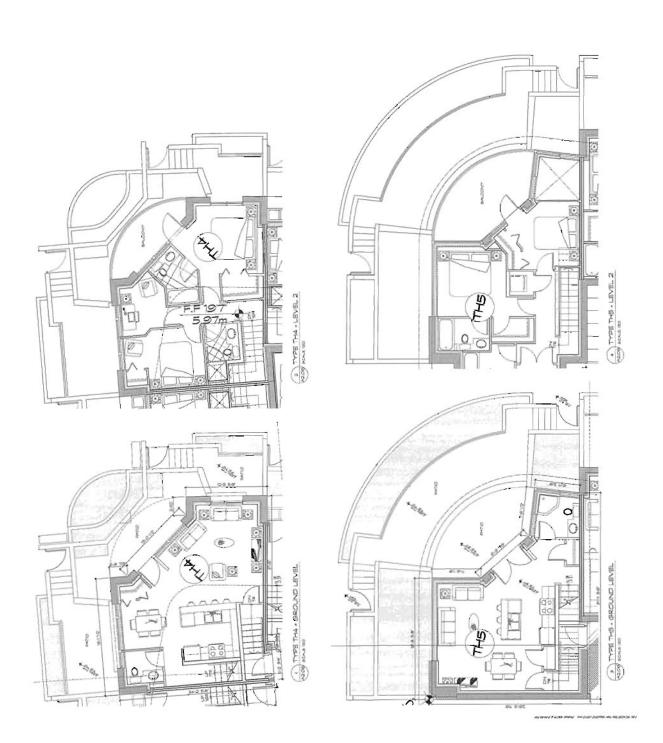
ALDERBRIDGE/ CEDARBRIDGE

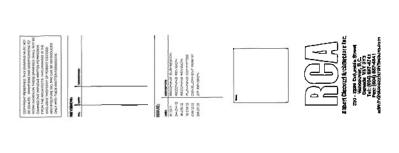




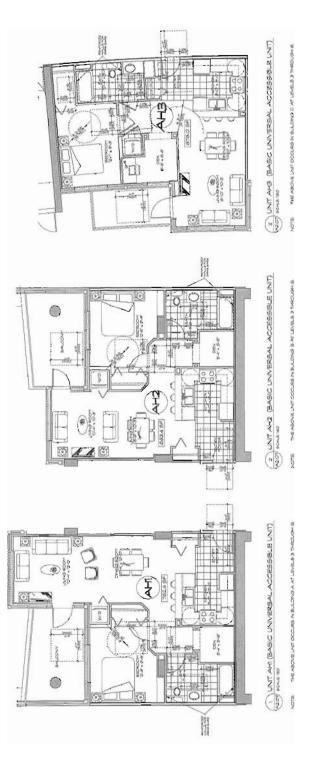
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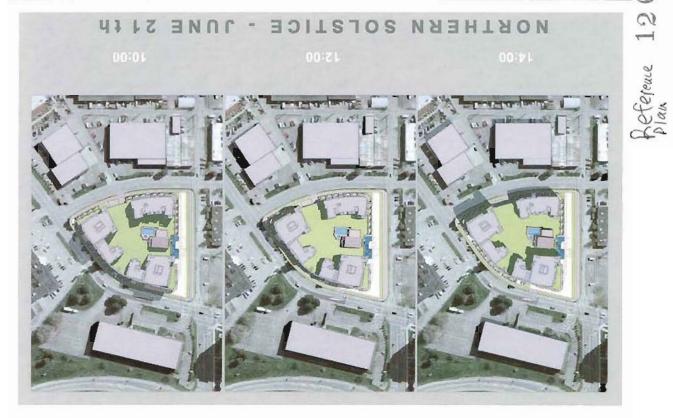












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