




To: Development Permit Panel
From: Wayne Craig
Program Coordinator, Development
Date: September 4, 2012
File: DP 12-597695
Re: Application by Am-Pri Construction Ltd. for a Development Permit at 7691, 7711
and 7731 Bridge Street

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of a 34 Townhouse Units at 7691, 7711 and 7731 Bridge Street on a site zoned "Medium Density Townhouses (RTM2)"; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - a) Reduce the required side yard setback along the south property line from 3.0 metres to 1.50 meters for Building 7 to enable the retention of a mature tree on the Bridge Street frontage of the site; and
 - b) Permit resident parking to be provided in a tandem parking configuration for all 34 units.


Wayne Craig
Program Coordinator, Development

Attachment 1 Development Application Data Sheet
Attachment 2 Advisory Design Panel Comments

Staff Report

Origin

Am-Pri Construction Ltd. has applied to the City of Richmond for permission to develop 34 townhouse units at 7691, 7711 and 7731 Bridge Street on a site zoned “Medium Density Townhouses (RTM2)”. The site currently contains a Single Detached House on each site.

The site is being rezoned from “Single Detached (RS1/F)” to “Medium Density Townhouses (RTM2)” for this project under Bylaw 8803 (RZ 11-563568).

A separate Servicing Agreement is required for road upgrades and frontage improvements to Bridge Street as well as a Public Walkway along the northern edge of the site that was established through a Public Right-of-Way (ROW) during the rezoning stage. The Servicing Agreement will also include service connections for water, storm upgrades and sanitary sewer.

Development Information

Please refer to attached Development Application Data Sheet (**Attachment 1**) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the North: At 7671 Bridge Street, a Single Detached Dwelling on land zoned “Single Detached (RS1/F)”.

To the East: Across Bridge Street, a 32 unit, three storey Townhouse complex at 9688 Keefer Avenue on land zoned “Town Housing (ZT50) – South McLennan (City Centre)”.

To the South: A 22 unit, two and three storey Townhouse complex at 7771 Bridge Street on land zoned “Town Housing (ZT60) – North McLennan (City Centre)”.

To the West: A 29 unit, two and three storey Townhouse complex at 7788 Ash Street on land zoned Town Housing (ZT16) – South McLennan and St. Albans Sub-Area (City Centre)”.

Rezoning and Public Hearing Results

During the rezoning process, staff identified the following design issues to be resolved at the Development Permit stage. (Staff comments are provided in ***bold italics***):

1. Design of the outdoor amenity area, including the play area.
The applicant has proposed an outdoor amenity area that should be well received by the residents. The location, at the entrance of the complex, just to the north of the main entrance contains a children’s play area, benches, open lawn space and a mature tree that is to be retained. The surrounding landscaping and metal fencing provides a good separation and adequate screening from the street that will as the landscape matures, improve over time.

2. Landscaped design of the public walkway along the northern edge of the property to be designed and constructed by the applicant.
The applicant is proposing a simple but effective public walkway created by a 6.0 metre wide ROW along the northern edge of the property that will connect Bridge Street and eventually Armstrong Street to the west, and will serve as a walkway, the main access to the townhouse units in Buildings 1 and 2, and general access to the townhouse complex. The proposed access point off Bridge Street is well identified with permeable pavers.
3. Overall appropriateness of the landscaping plan.
The applicant has provided a good landscaping proposal for the complex, with a variety of plantings to engage interest. A good combination of new tree and shrub plantings will allow for good definition of private and semi-private spaces within the townhouse complex. The applicant has done a very good job incorporating the existing trees that have either been retained and kept in the same location, or retained and moved to another part of the site. The new plantings surrounding the perimeter of the complex will soften the appearance of the three (3) storey units from the road and the public walkway. Internally, an abundant number of trees along the internal path between Buildings 5 and 6 will have the opportunity to flourish and create a pleasant green node.
4. Manoeuvrability of larger vehicles (SU-9) within the site.
The applicant has submitted a plan that illustrates and Transportation confirms appropriate manoeuvrability for larger vehicles such as SU-9 within the site.
5. Form and Character of the townhouse units, including design features in highly visible sections and how they address adjacent properties.
The applicant has submitted elevations and a context rendering that show that the proposed development has similar characteristics and is an adequate fit with the adjacent townhouses in the area.
6. Provide a sense of territory for pedestrian use and movement within the site.
The applicant has done a good job in providing pedestrians the opportunity to move within the site that is separate from vehicle traffic. Units fronting Bridge Street and the public walkway have their entrances directly off either the sidewalk or the walkway. Internally, a walkway is located between Buildings 5 and 6 provides connections directly to these units.
7. Identify unit(s) to allow easy conversion for Universal access.
The applicant is proposing the unit directly west of the outdoor amenity area be built to accommodate easy conversion for universal access. Provisions made for this unit is a closet that can be easily framed to convert to allow for a future lift, doors that are wide enough, and adequate space is available to manoeuvre to accommodate wheelchair access.

The Public Hearing for the rezoning of this site was held on October 17, 2011. At the Public Hearing, the following concerns about rezoning the property were expressed:

Owners of 7671 Bridge Street indicated that they share the north property line of the subject site. They wished to know if the developer will be installing a fence along the entire length of this property line to give some privacy from the public walkway.

Staff worked with the applicant to address these issues in the following ways:

The applicant is proposing a six (6) foot high wood fence that runs along the entire length of the property line. A six (6) foot high wood fence is consistent with the treatment along common property lines. In addition several trees are proposed to be planted along the public walkway that will contribute to add privacy.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan and is generally in compliance with the Zoning Bylaw 8500 except for the zoning variances noted below.

Zoning Compliance/Variations (staff comments in bold)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Reduce the required side yard setback along the south property line from 3.0 metres to 1.50 meters for Building 7 to enable the retention of a mature tree on the Bridge Street frontage of the site.

Staff supports the proposed variance to enable the retention of a significant tree that is located within the outdoor amenity area. This tree will help give the complex an instant sense of maturity. The provision of the 6.0 m public ROW at the north of the site which provides an amenity for residents and the community, results in the Building No. 7 being 'pushed' further south on the site.

- 2) Permit resident parking to be provided in a tandem parking configuration for all 34 units.

Staff supports the proposed variance as the configuration will not interfere with the movement of other vehicles in the complex. A restrictive covenant is to be registered to ensure that the garage space does not get converted to resident use.

Advisory Design Panel Comments

The Advisory Design Panel was supportive of the proposal and identified areas for design development and consideration. The applicant addressed staff and Panel comments and made refinements to the design proposal. A copy of the relevant excerpt from the Advisory Design Panel Minutes from May 9, 2012 is attached for reference (**Attachment 2**). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '**bold italics**'.

Analysis

Conditions of Adjacency

- The subject site is located in the South McLennan area, where there has been significant redevelopment over the past several years, with many of the surrounding parcels being redeveloped from Single Detached Houses to Townhouses. The proposed development will be of similar height and massing to these recent developments and conforms to the guidelines set out in the neighbourhood plan.
- The public walkway along the northern edge of the property was at one time to be an extension of Keefer Avenue, connecting Bridge Street and Armstrong Street. It was determined during the review of a rezoning application (RZ 11-563568) for a seven (7) lot single family subdivision that this connection was no longer needed to facilitate vehicle movement within the neighbourhood. Instead of completely cutting off pedestrian access between Bridge Street and Armstrong Street, an amendment to the neighbourhood plan to change Keefer Avenue to a trail/walkway was established through Bylaw 8803. The implementation occurred as part of the rezoning application (RZ 11-563568) for this proposal and 6.0 metre wide Public ROW along the northern edge of the subject site to facilitate a public connection between these two streets, in addition to providing access to the individual townhouse units fronting it was secured. As the current proposal will not provide the needed connection to Armstrong Street until the property to the north redevelops, the walkway will be maintained by the townhouse strata until the formal connection is established.

Urban Design and Site Planning

- The townhouse units are arranged in four (4) and five (5) unit clusters located throughout the site with most of the clusters having a north-south alignment that would provide adequate sun exposure to the units.
- A 6.0 metre wide Public ROW is located along the northern edge of the site. The intent of the ROW is to provide a pedestrian throughway, connecting Bridge Street from the east to the recently constructed portion of Armstrong Street to the west, as well as providing resident access to the units along the north side of the site which will be fronting the ROW. This walkway also provides separate pedestrian access to the townhouse site.
- The main vehicular access to the site is off Bridge Street, accessing the site by the main east-west internal drive aisle. Secondary drive aisles that branch off from the main internal driveway provide access to the majority of the units.
- All units fronting Bridge Street and the public walkway have a direct pedestrian link to these public circulation routes.
- The outdoor amenity area is located directly north of the main entrance, and provides areas for seating, lawn space and a child's play structure, suitable for children from ages 2 to 5.

Architectural Form and Character

- The proposed form and massing of the buildings is consistent with the style of the neighbourhood and meets the intent of the guidelines set out in the neighbourhood plan.
- The choice of materials used on the facade of the buildings is typical of the area, and includes the various uses of hardiplank panels that provides the elevations with a good base, middle and top feature, as indicated in the urban design guidelines of the area plan. The use of brick at the base of the elevations for units fronting both Bridge Street and the public ROW adds to the interest and architectural expression of the buildings.

- The colour choices for the proposal are considered appropriate to the urban context and architectural character of the proposal and meet the intent of the area plan.

Transportation

- Vehicle access is off Bridge Street, slightly north of the centre of the property. No lands were dedicated for road improvement but frontage improvements such as sidewalk, treed and grassed boulevard with curb and gutter will be undertaken by the developer through the Serving Agreement.
- All 34 units have an attached garage to serve the parking needs of the residents. All attached garages will be in a tandem configuration as the rezoning application process was well underway prior to Council's referral to staff to examine tandem parking arrangements was given. The number of proposed parking stalls meets the requirements of the Zoning Bylaw, and a restrictive covenant will be registered prior to the issuance of the Development Permit to ensure these tandem stalls are not converted into habitable living spaces.
- Visitor parking is scattered throughout the site, giving visitors easy access to the units in the complex. One stall is provided for those with disabilities and is located between Buildings 5 and 6. The number of visitor stalls meets the requirements of the Zoning Bylaw.

Landscape Design and Open Space Design

- The submitted landscape plan identifies a mixture of soft and hard landscaping that defines a consistent treatment of edges throughout the site. The applicant has done a good job in being able to retain a total of four (4) trees and will relocate two (2) other trees from the southern edge to the south east corner of the site, as well as the north west corner. The most prominent tree is located at the front of the property and is incorporated as an important component of the outdoor amenity area. One tree being retained is located at the southern edge of the property between Buildings 5 and 6. The remaining trees are retained at the western edge of the site, off of Building 4. All these trees have been incorporated into the proposed landscaping design and will complement the proposed layout.
- The hard surfacing is supplied by a combination of permeable pavers along the entire length of the main east-west internal drive aisle with asphalt lanes on the secondary aisles. Permeable pavers are predominant in the internal pedestrian walkways and help identify visitor parking spaces. The amount of permeable paving within the hard surfacing is about 50%.
- The applicant has provided a lighting plan to illustrate how the site is to be illuminated during evening hours. The fixtures themselves will provide good illumination but will avoid illumination spilling onto adjacent properties.
- An Arborist report was submitted at the rezoning application stage and was reviewed by City staff. The report identified 30 on-site trees that are affected by this development and called for the removal of these trees due to its poor condition and its location within the development footprint. City Staff concur with the report and will obtain a security deposit from the developer to ensure survival of the proposed landscaping.
- In accordance with City Policy, the applicant must provide at least 60 trees to meet the 2:1 replacement ratio policy. In response, the applicant meets this requirement by proposing 62 trees. The table below summarises this requirement.

Number of trees to be removed	Number of trees to be retained or relocated	Required number of trees to be planted	Proposed number of trees to be planted	Surplus (Deficit)
30	6	60	62	2 Surplus

Amenity Space

- The applicant is not proposing any indoor amenity space with this application. Instead, the applicant made a voluntary contribution in lieu of providing indoor amenity space through the rezoning process.
- The applicant is providing an outdoor amenity space, located next to the main vehicle entrance to the site. The size meets the minimum area requirements outlined in the neighbourhood plan and provides adequate space to house seating area, lawn space and a child play structure. A large mature tree has been retained in this area to provide shade and is a bonus feature to the area.

Garbage and Recycling

- The garbage and recycling area is located along the main drive aisle, next to Building 6. The number of bins as well as the accessibility to the enclosure meets the requirements of Environmental Programs.

Affordable Housing

- The applicant is not providing any affordable housing units, but did make a voluntary contribution to the Affordable Housing Reserve Fund in accordance with policy. This contribution was secured during the rezoning stage.

Sustainability Indicators

- The proposed plan is reusing existing single-family sites to increase living density within the neighbourhood, utilizing existing and proposed infrastructure more efficiently.
- The residential units will provide energy efficient appliances and water saving faucets.
- The site is close to a major transit corridor and close to bus stops for convenience of use.
- A permeable paving system covering approximately 50% of the overall paved area within the complex are proposed in areas where concrete pavers are located.
- The proposed landscaping provides a variety of planting that are appropriate for the geographical area are low maintenance and will improve over time.
- Retention or relocation of six (6) existing on site trees lessens the disruptions of the site.

Universal Access

- All units will be providing aging in place features such as backer blocks for the easy installation of future grab bars, door lever handles instead of door knobs and sufficient door openings to allow access for wheelchairs.
- To provide design flexibility and to allow for better movement to those with limited mobility, the applicant has provided a unit plan for unit type A1a in Building 1, which would allow for the easy conversion for residents who require use of a wheelchair. Items that have been taken into consideration in designing this unit’s layout are:
 - Wider doors for easier access to the unit.

- Closet space that can be removed to accommodate an elevator to provide access for occupants to different floors of the unit
- Proper design of the kitchen and bathroom layout for wheelchair mobility.
- Additional backer blocks behind the finished walks for the future installation of grab bars.

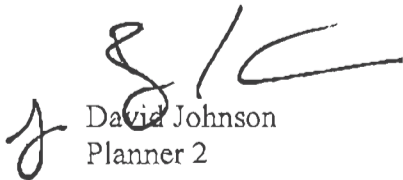
(Dimensions and notations reflecting these provisions are to be shown on the Building Permit drawings)

Crime Prevention Through Environmental Design

- The proposed plan provides good edge treatment features such as low fencing, landscaping and paving materials to separate public and private space, giving the site a good sense of territoriality.
- The submitted lighting plan will provide good evening illumination without extending unwanted light onto adjacent properties.
- The site design allows for good sight lines through the development site for open observation, including surveillance over the outdoor amenity area. There are enough windows looking out onto all common areas that will facilitate casual surveillance.

Conclusions

Am-Pri Construction Ltd. Has applied to the city of Richmond for permission to develop 34 townhouse units at 7691, 7711 and 7731 Bridge Street. The proposed development has gone through some design adjustments in coordination with staff and the result is a design proposal that addresses the design guidelines for the area. Staff supports this Development Permit application and recommends approval as the proposed design should fit well within the streetscape and the character of the neighbourhood.



David Johnson
Planner 2

DJ:cas

The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter-of-Credit for landscaping in the amount of \$74,252.60 (based on total floor area of 37,126.3 square feet).
- Registration of a legal agreement to prohibit the conversion of tandem parking area into habitable space.

Prior to future Building Permit issuance, the developer is required to complete the following:

- The applicant is required to obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).



DP 12-597695

Attachment 1

Address: 7691, 7711 and 7731 Bridge Street

Applicant: Am-Pri Construction Ltd.

Owner: Same

Planning Area(s): McLennan South (Schedule 2.10D)

Floor Area Gross: 3,449.1 m²

	Existing	Proposed
Site Area:	5,309.5 m ²	5,309.5 m ²
Land Uses:	Single Detached	Townhouses
OCP Designation:	Residential, 2 ½ storeys typical (3 storeys maximum), predominately Triplex, Duplex, Single Family 0.55 base FAR	No change
Zoning:	Single Detached (RS1/F)	Medium Density Townhouses (RTM2)
Number of Units:	1 unit per lot	34 units over a consolidated lot

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Site Area = 5,309.5 m ² (0.65 FAR) = 3,451.2 m ²	3,450.9 m ²	none permitted
Lot Coverage:	Max. 40%	40%	none
Setback – Front Yard:	Min. 6.0 m	6.0 m	none
Setback – Side Yard: (north)	Min. 3.0 m	8.0 m	none
Setback – Side Yard: (south)	Min. 3.0 m	1.50 m	1.50 m
Setback – Rear Yard:	Min. 3.0 m	4.50 m	none
Height (m):	Max. 12.0 m	10.75 m	none
Off-street Parking Spaces – Regular:	48	68	none
Off-street Parking Spaces – Visitor:	7	7	none
Total off-street Spaces:	55	75	none
Tandem Parking Spaces	not permitted	34 tandem 68 parking stalls	34 tandem 68 parking stalls

Amenity Space – Indoor:	Min. 70 m ²	Cash-in-lieu secured at rezoning	none
Amenity Space – Outdoor:	Min. 204 m ²	214 m ²	none

Excerpt from the Minutes from
The Design Panel Meeting

Wednesday, May 9, 2012 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall

Comments from the Panel:

Investigate further the pedestrian experience on the public walkway at the northern edge of the property; neither straight nor curve; appears walk and not defined;

-The pedestrian public walkway has been redesigned to be more curvilinear. (L3.0)

Adjacent project on Bridge Street streetscape steps down toward the subject development (i.e., toward the property line of building No. 7); however, Building No.7 form is straight up; transition needs more resolution; needs to be brought down visually;

-A secondary roofline with a large overhang has been incorporated at the second level of Building 7. This roof line ties into the 2nd floor roof of the adjacent building and brings the horizontal line of the adjacent building's 2nd floor roof across the front of building 7. In addition, the unit adjacent to the townhouse to the south incorporates a large porch that relates to the large porch of the adjacent building. (See A 4.0)

Either use brick for entire base of buildings or get rid of it;

-Buildings 1, 2, and 7 incorporates brick for the entire base facing Bridge St. and the public pedestrian walkway. (See A4.0-4.2)

Roof slopes are a bit shallow given the blocky nature of the project; look at varying the pitch of dormers;

-Roof slopes have been increased to enhance the scale of the roof and to match the roof slopes of adjacent buildings. (A4.1-4.5)

Bay windows need more proportion and depth; can either put brackets underneath to accentuate the bay or drop the soffit of the bay to provide more mass

-Bay windows widened to improve massing. Brackets also incorporated to bay windows on second floor to enhance the massing. (A4.1-4.5). Second floor roof incorporated to further emphasize the hierarchy between the lower two levels and top floor.

Project fits nicely into its context; nice to see the public walkway at the northern edge of the project; however, design development is needed with regard to the orientation of the pathway; should respond to the long and narrow condition of the ROW;

-Pedestrian walkway has been redesigned to be more curvilinear and provide additional interest along its length.

Consider planting or ground cover massing on the north side of the pathway; provide lighting in ROW;

-We have considered. However, the grass would be the easiest to maintain as it would be handed over to the City in the future. - Ito & Associates.

Unify mailbox, signage, gate to play area, and seating into one structure at the amenity area to provide opportunity for social interaction among residents;

-The mail box, sign, gate have been designed as one structure. However, the project sign and the gate to the play area remained as separate structures due to the distance between and the retention of tree #6793 in the amenity area.

Extend permeable concrete pavers throughout the main drive aisle to introduce visual texture through the very narrow and long roadway;

-Permeable concrete pavers extended to run through the main internal drive way. (See L3.0)

Articulation of the building facades helps break up the overall mass but the proportions do not work well and there are marked divisions between the base and upper floors; need more investigation; tie-in with surrounding context;

-Bay windows widened to improve massing. Brackets also incorporated to bay windows on second floor to enhance the massing. (A4.1-4.5). Second floor roof incorporated to further emphasize the hierarchy between the lower two levels and top floor. Second floor roof also ties into partial 2nd floor roof of adjacent townhouse development.

Roof slopes are a bit shallow; not well integrated with what is going on with the facade; need more articulation to better reflect the facade;

-Roof slopes increased to enhance scale of roof. Secondary roof with brackets along the 2nd floor integrated to create better articulation with the facade. (See A4.1)

Consider opportunity for a gate to provide connection from playground in the amenity area to the public ROW;

-Gate provided from playground in amenity area to connect with public pedestrian walkway (See L3.0)

Per Plan#5, proposed ground floor and upper floor location of the potential vertical lift work well; however, proposed location of the potential vertical lift on the second floor is not appropriate as it is in the middle of a living room; consider moving it to a corner of a room;

-Vertical lift re-oriented vertically to provide access away from living room. (See A5.3)

Ensure that convertible washrooms will have enough circulation space;

-Convertible washroom showing dimensions to ensure there is circulation space. (See A5.3)

Like the strong attempt to integrate the amenity area into the public walkway; not sure who will maintain the lawn in the public walkway; concern on the maintenance of the lawn if it is the City;

-Public walkway to be maintained by the city.

Interface of the public walkway with the private townhouses could be an issue; consider moving the public walkway further to the north;

-The public walkway has been redesigned to increase the distance to some of the units.

However, the distance to some the units remain as previous in order to satisfy the comment to make the walk more curve linear.

The minimum distance from property line to the public walkway would be still similar to a city sidewalk to many multiple residential complexes in the city.

Consider location of public art at the entrance to the public walkway adjacent to the amenity area

-Public art to be incorporated into the entrance of the public walkway adjacent to the amenity area as per liaison between owner and Arts Department from the city.



No. DP 12-597695

To the Holder: AM-PRI CONSTRUCTION LTD.
Property Address: 7691, 7711 AND 7731 BRIDGE STREET
Address: C/O KEVIN PERALTA
YAMAMOTO ARCHITECTURE INC.
2386 OAK STREET, VANCOUVER, BC V6H 4J1

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - a) Reduce the required side yard setback along the south property line from 3.0 metres to 1.50 meters for Building 7 to enable the retention of a mature tree on the Bridge Street frontage of the site ; and
 - b) Permit resident parking to be provided in a tandem parking configuration for all 34 units.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #16 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$74,252.60 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit

No. DP 12-597695

To the Holder: AM-PRI CONSTRUCTION LTD.
Property Address: 7691, 7711 AND 7731 BRIDGE STREET
Address: C/O KEVIN PERALTA
YAMAMOTO ARCHITECTURE INC.
2386 OAK STREET, VANCOUVER, BC V6H 4J1

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO. ISSUED BY THE COUNCIL THE
DAY OF , .

DELIVERED THIS DAY OF , .

MAYOR

NOTE: All required resident parking to be provided in tandem garages

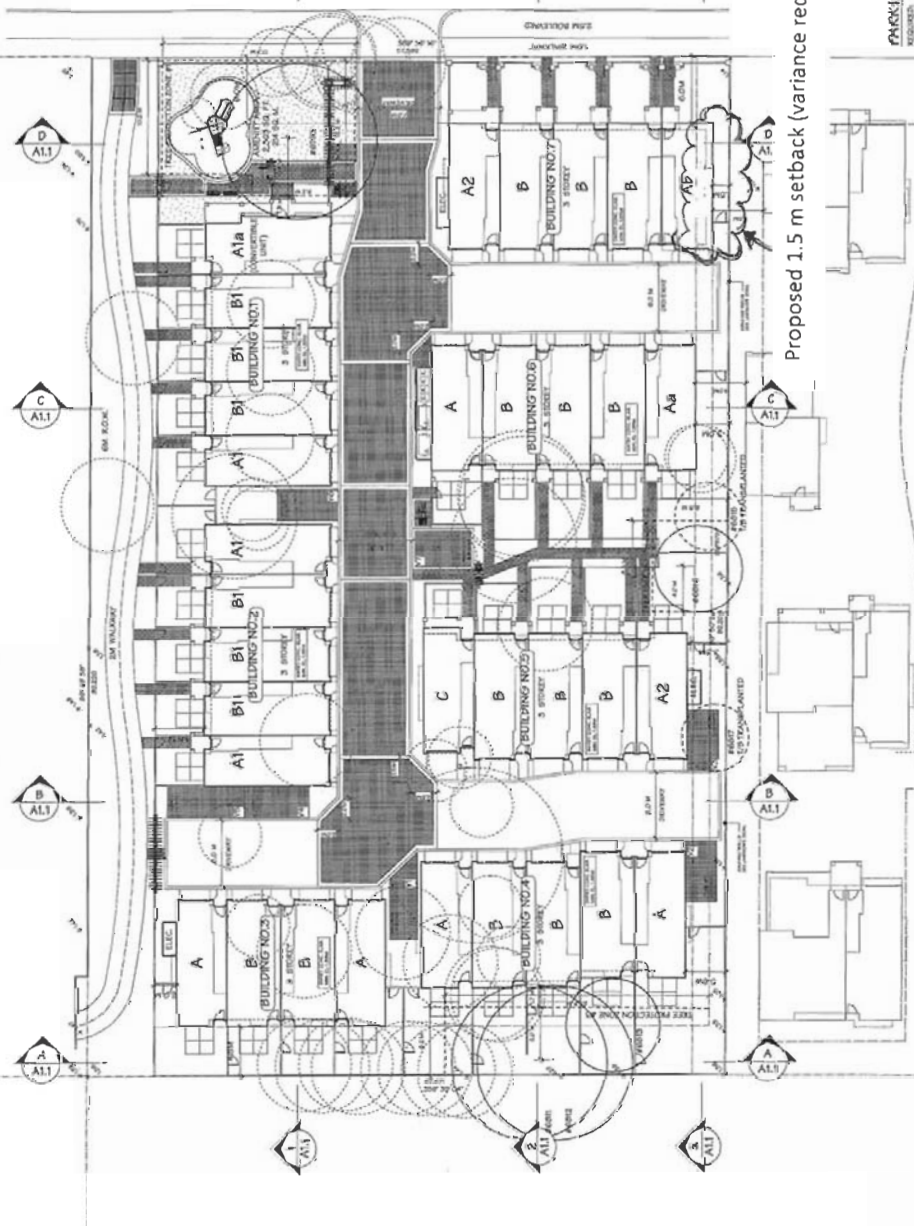
KEEPER AVE.



CONTEXT PLAN
SCALE: 1:500

12597695
Rev. 05/1

Proposed 1.5 m setback (variance required)



PARKING:

1.5 SPACES / 24 UNITS	60 SPACES (RESIDENTS)
0.5 SPACES / 24 UNITS	15 SPACES (VISITORS)
0.5 SPACES / 24 UNITS	15 SPACES (TOTAL)
2 CAR GARAGE SPACES / 24 UNITS	60 SPACES
1 OPEN VISITOR PARKING SPACES / 24 UNITS	15 SPACES
15 SPACES (TOTAL)	75 SPACES
MIN. TOTAL SPACES TO BE PROVIDED	75 SPACES
REQUIRED - STANDARD SPACES	60 SPACES
REQUIRED - BI-CYCLE SPACES	15 SPACES
REQUIRED - BI-CYCLE RACKS	15 SPACES

STATISTICS:

CITY ADDRESS:	774 BRIDGE STREET
LOCAL DESCRIPTION:	RESIDENTIAL
PROPOSED ZONING:	R-1
SITE AREA:	10,000 SQ FT
LOT COVERABLE:	10,000 SQ FT
MAX ALLOWED:	10,000 SQ FT
PROPOSED:	10,000 SQ FT
MAX HEIGHT:	10.0 M
PROPOSED:	10.0 M

FA.R.1

MAX ALLOWED FLOOR AREA (G.M.R.)	10,000 SQ FT
PROPOSED FLOOR AREA:	10,000 SQ FT
UNIT A1 (2 BDR)	1,000 SQ FT
UNIT A2 (2 BDR)	1,000 SQ FT
UNIT B1 (2 BDR)	1,000 SQ FT
UNIT B2 (2 BDR)	1,000 SQ FT
UNIT C1 (2 BDR)	1,000 SQ FT
UNIT C2 (2 BDR)	1,000 SQ FT
ADDITIONAL AREA:	0 SQ FT
OVERALL FLOOR AREA:	10,000 SQ FT

AMENITY AREA:

BIKE STORAGE	15 BIKES
BIKE REPAIR	15 BIKES
BIKE REPAIR	15 BIKES
BIKE REPAIR	15 BIKES
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AMENITY AREA:

BIKE STORAGE	15 BIKES
BIKE REPAIR	15 BIKES
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AMENITY AREA:

BIKE STORAGE	15 BIKES
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PLAN #1

A1.0

05-12-2019

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Yamamoto Architecture Inc.

774 BRIDGE STREET

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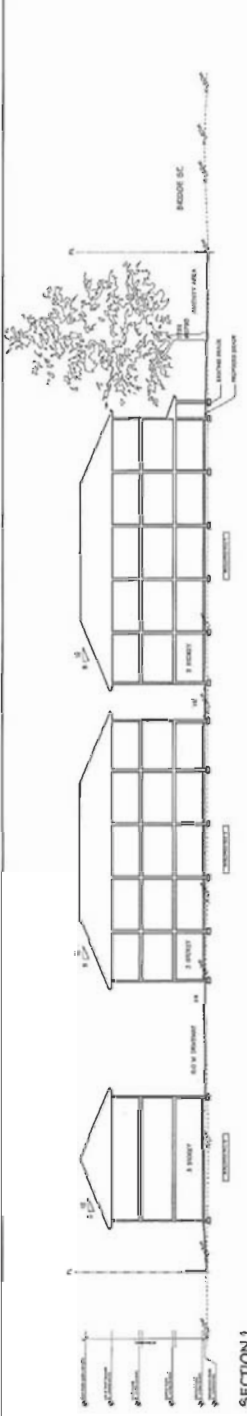
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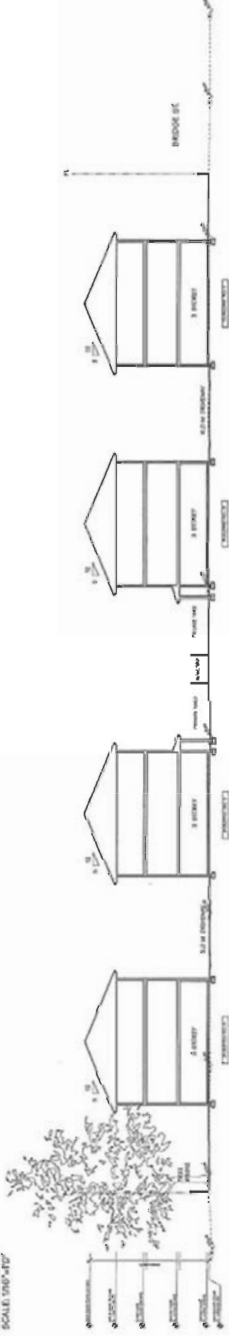
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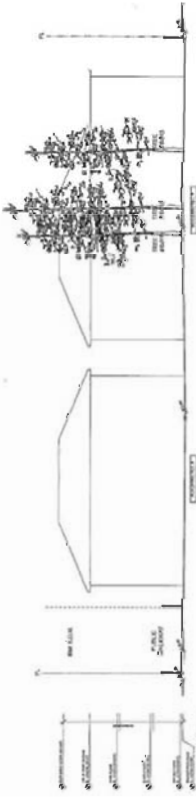
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SCALE 1/8"=1'-0"



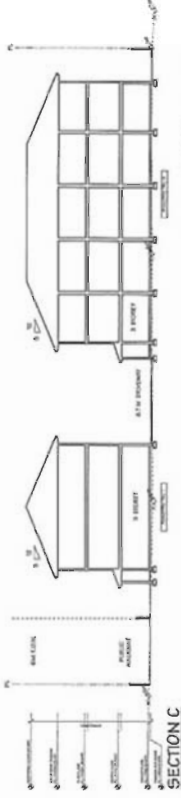
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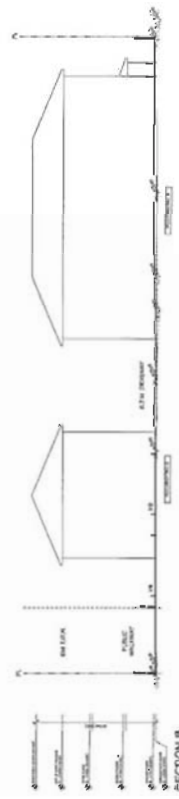
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SCALE 1/8"=1'-0"



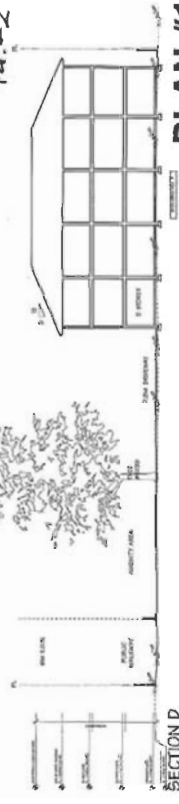
SECTION A
SCALE 1/8"=1'-0"



SECTION C
SCALE 1/8"=1'-0"



SECTION B
SCALE 1/8"=1'-0"



SECTION D
SCALE 1/8"=1'-0"

NO.	DATE	DESCRIPTION
1	10/15/2010	100% PERMITS
2	10/15/2010	90% PERMITS
3	10/15/2010	80% PERMITS
4	10/15/2010	70% PERMITS
5	10/15/2010	60% PERMITS
6	10/15/2010	50% PERMITS
7	10/15/2010	40% PERMITS
8	10/15/2010	30% PERMITS
9	10/15/2010	20% PERMITS
10	10/15/2010	10% PERMITS

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14.0075 TOWNWOODS, DENV.

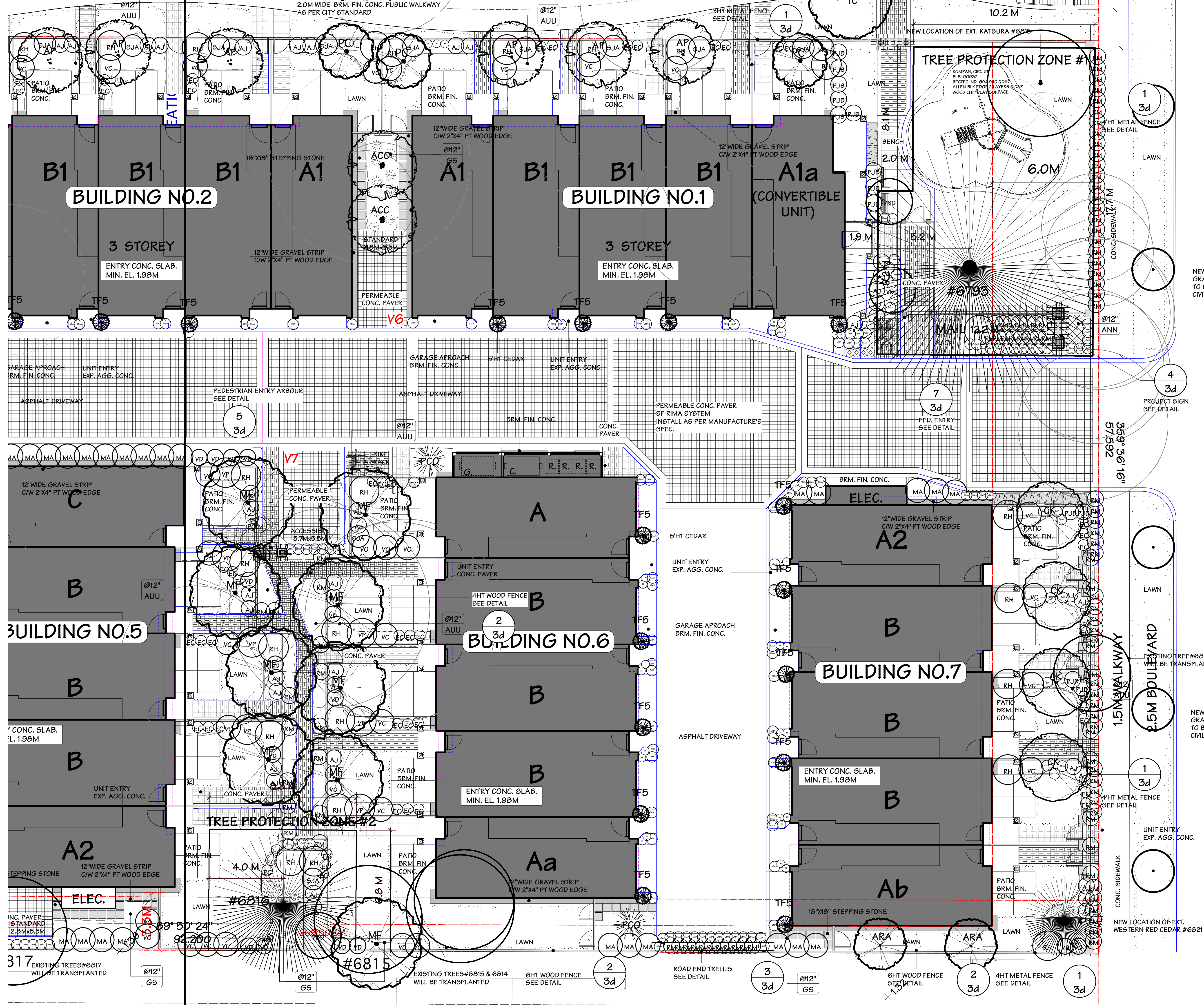
Yamamolo
Architecture Inc.

DP 12597695
Pg. 002

100% PERMITS
90% PERMITS
80% PERMITS
70% PERMITS
60% PERMITS
50% PERMITS
40% PERMITS
30% PERMITS
20% PERMITS
10% PERMITS

NO.	DATE	DESCRIPTION
1	10/15/2010	100% PERMITS
2	10/15/2010	90% PERMITS
3	10/15/2010	80% PERMITS
4	10/15/2010	70% PERMITS
5	10/15/2010	60% PERMITS
6	10/15/2010	50% PERMITS
7	10/15/2010	40% PERMITS
8	10/15/2010	30% PERMITS
9	10/15/2010	20% PERMITS
10	10/15/2010	10% PERMITS

PLAN #1a



BRIDGE STREET

- Aug. 7, 2012:**
Public walkway revised, details modified
Issued for DP
- Apr. 30, 2012:**
Public walkway indicated along n. pl.
Issued for DP
- Apr. 26, 2012:**
Bldg. & Hard landscape modified
Pitg. adjusted.
Issued for DP
- Dec. 9, 2011:**
Road layout, bldg. modified
hard lands. pitg. adjusted.
Issued for DP
- Nov. 8, 2011:**
Issued for DP

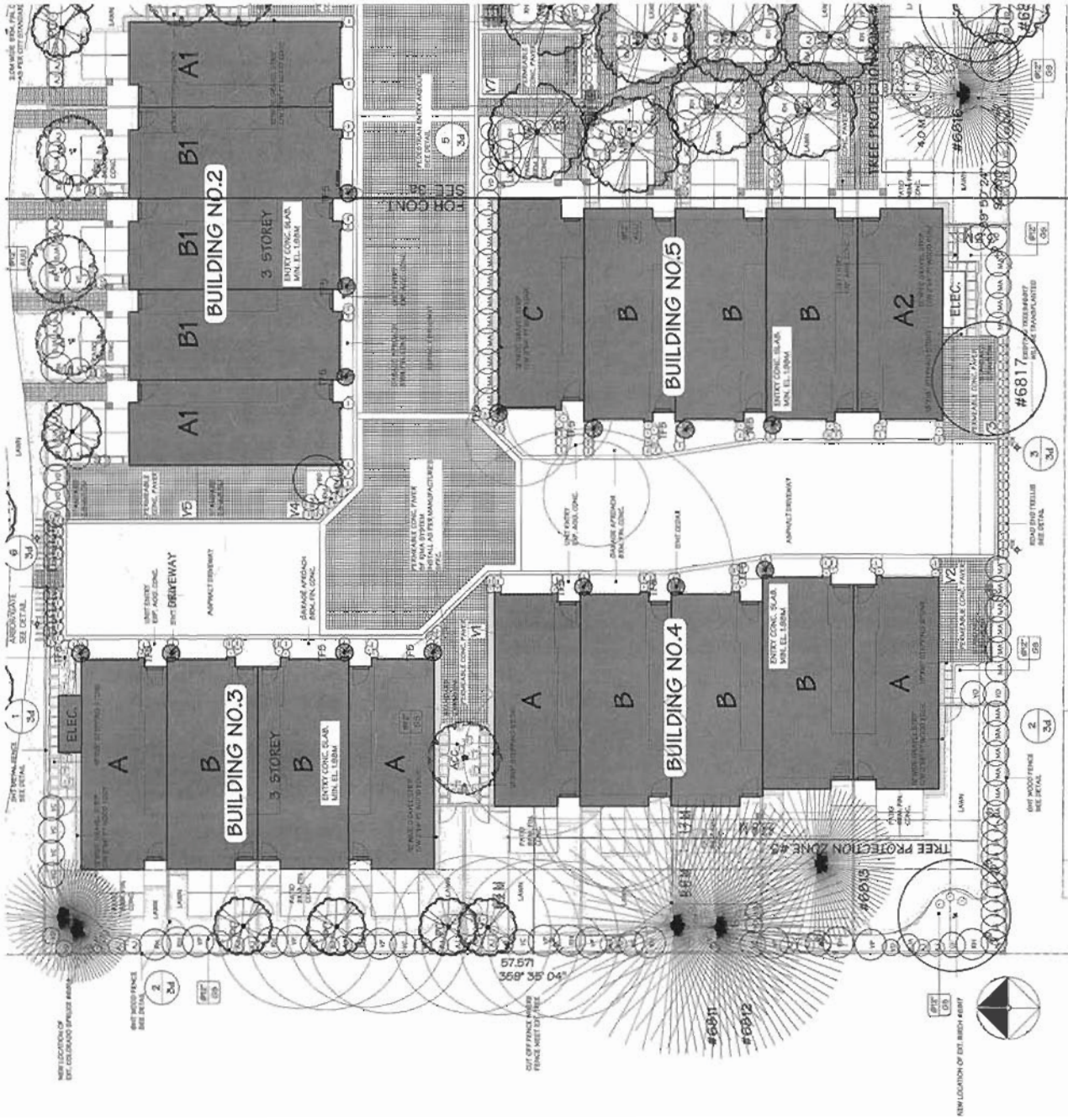
REVISIONS / ISSUES

ITO
& ASSOCIATES
Landscape Architects
1942 E 5th Ave.
Vancouver, BC V5N 1M2
T/F: (604) 255 5009
Email: itovan@mac.com

Project:
9691,9711,9731
BRIDGE ST.
RICHMOND B.C.

Drawing Title:
LANDSCAPE PLAN

Scale: 1/8" = 1'-0"
Drawn: MI
Checked: TI
Date: JULY 19, 2011
Job: 11L13
Sheet:



ITO ASSOCIATES
 LANDSCAPE ARCHITECTS
 1342 E 29th Ave.
 Vancouver, B.C. V6K 1M2
 T: (604) 272-0009
 Fax: (604) 272-0000
 Email: info@itoassociates.com

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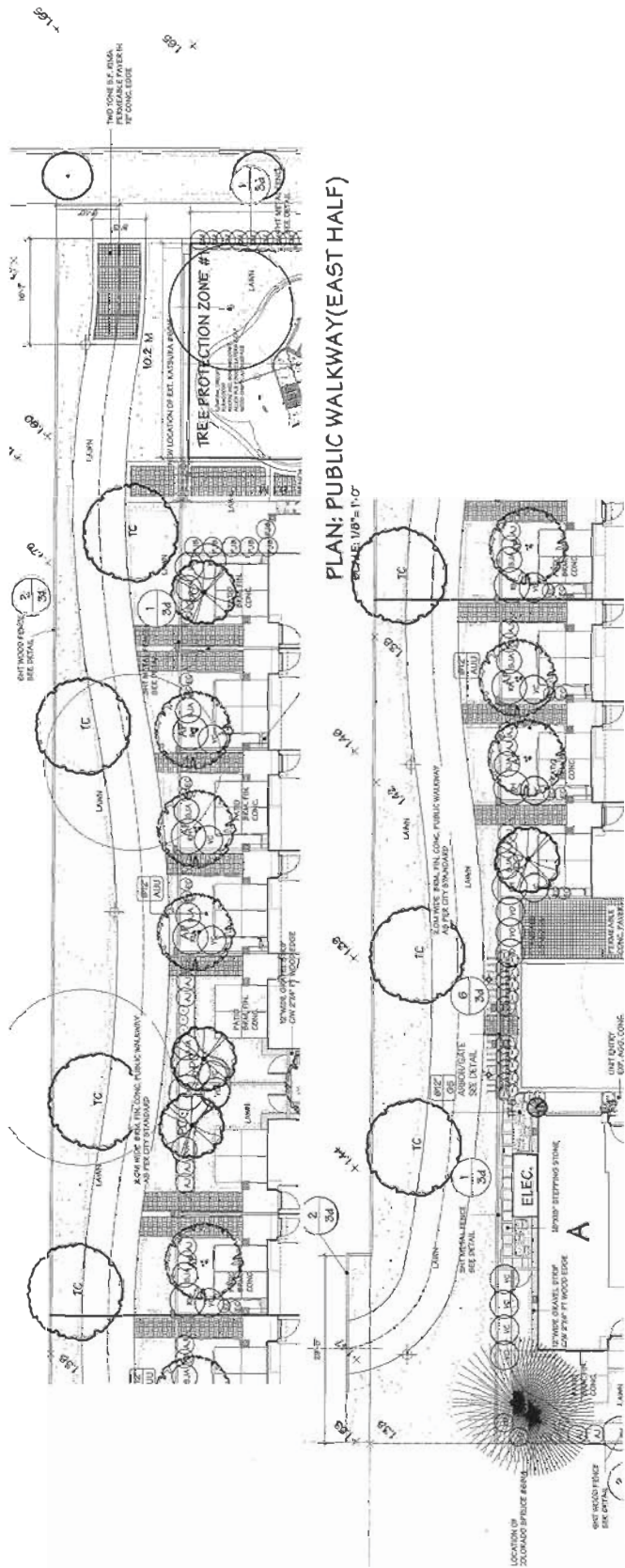
9691, 9711, 9731
 BRIDGE ST.
 RICHMOND B.C.

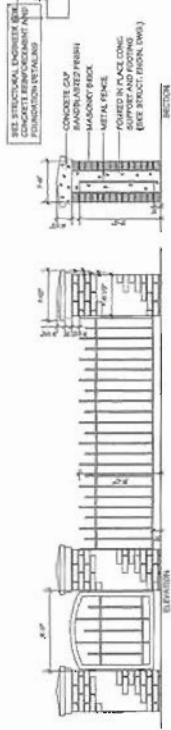
LANDSCAPE PLAN

DP 12597695
 Pg. 011

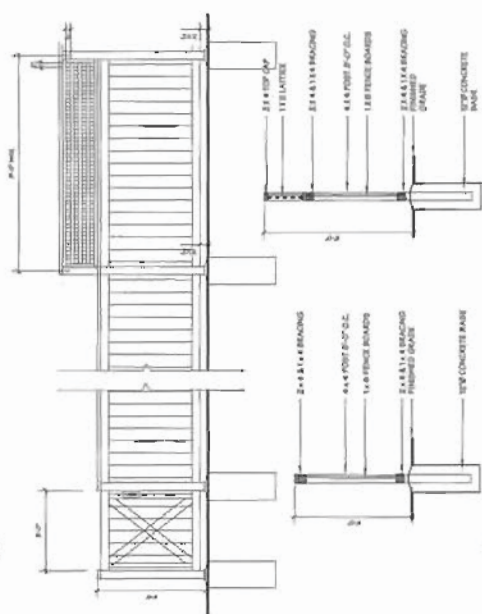
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Drawn by:	ML
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Sheet:	3b

3b

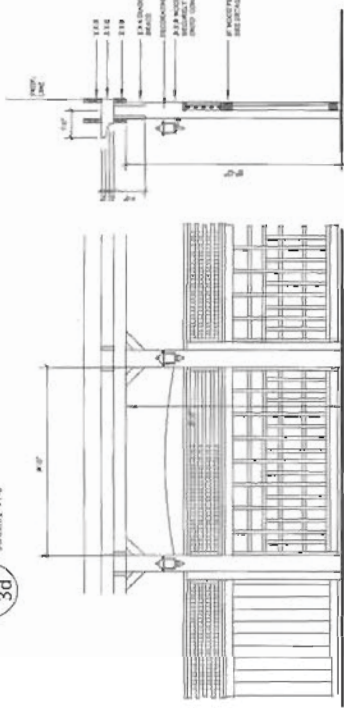




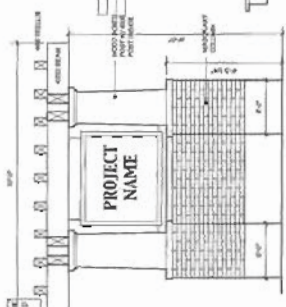
1 METAL FENCE/POST DETAIL
SCALE: 1/4" = 1'-0"



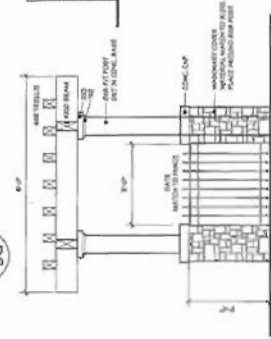
2 4' HT / 6' HT WOOD FENCE
SCALE: 1/4" = 1'-0"



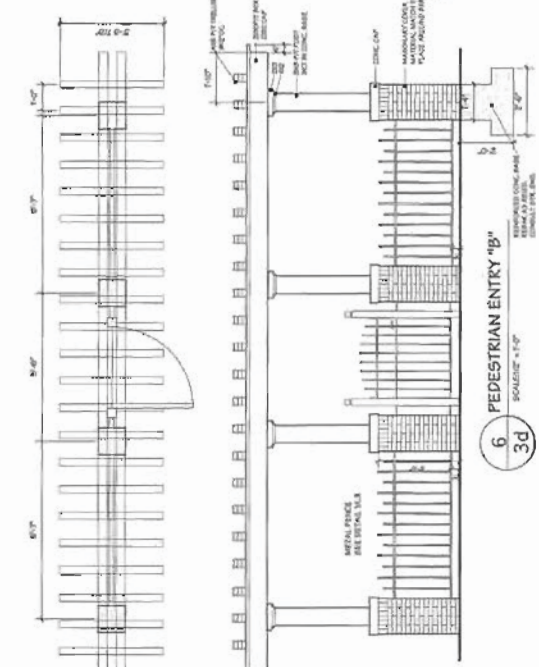
3 ROAD END TRELLIS
SCALE: 1/4" = 1'-0"



4 SIGN
SCALE: 1/4" = 1'-0"

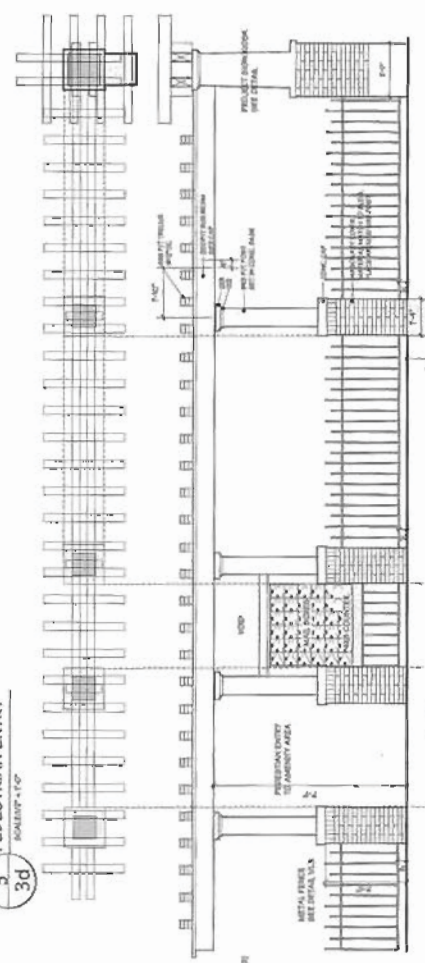


5 PEDESTRIAN ENTRY
SCALE: 1/4" = 1'-0"



6 PEDESTRIAN ENTRY
SCALE: 1/4" = 1'-0"

NOTE:
ALL WOOD SHALL BE PRESSURE TREATED PER B.S.S. AND STAINED WITH 2 COATS SOLID STAIN. COLOR TO BE DETERMINED AT A LATER DATE.
ALL WALLS AND OTHER METAL COMPONENTS SHALL BE GALVANIZED.



7 AMENITY ENTRANCE/MAIL KIOSK
SCALE: 1/4" = 1'-0"



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Email: Ito@itoa.com

5891.9711.9731
BRIDGE ST.
RICHMOND B.C.

Sheet No. **DETAILS**

DATE	10/1/13
SCALE	1/4" = 1'-0"
NO.	13
REV.	13
DATE	11/1/13
NO.	13
REV.	13

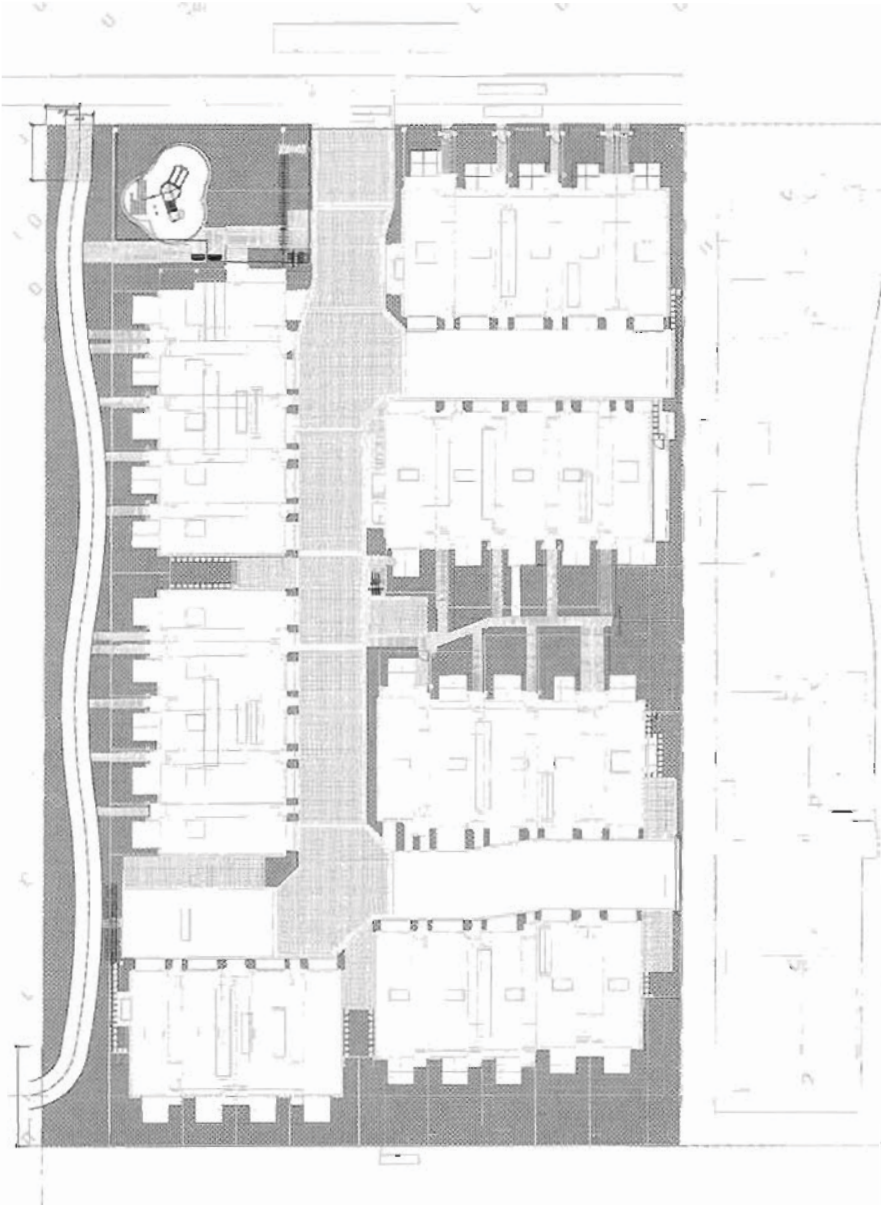
3d

12597695

DR

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LOT COVERAGE OF SOFT LANDSCAPE AREA

ITEM	AREA (SQ. FT.)
TOTAL SITE AREA	25,514.00
SOFT LANDSCAPE AREA	1,500.00
PLANTING	1,500.00
PERCENTAGE	5.88%

DP 12597695

Pa. # 14

3e

Aug. 7, 2017
 9681, 9711, 9731
 BRIDGE ST.
 RICHMOND B.C.

Aug. 24, 2017
 9681, 9711, 9731
 BRIDGE ST.
 RICHMOND B.C.

Aug. 24, 2017
 9681, 9711, 9731
 BRIDGE ST.
 RICHMOND B.C.

ITO
 & ASSOCIATES
 LANDSCAPE ARCHITECTS
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 Vancouver, BC V6H 1W2
 TEL: (604) 275-8009
 Email: Ito@itoa.com

9681, 9711, 9731
 BRIDGE ST.
 RICHMOND B.C.

Lot Coverage of
 Soft Landscape

DATE	DESCRIPTION
Aug. 7, 2017	Initial Design
Aug. 24, 2017	Revised Design
Aug. 24, 2017	Final Design



LIGHTING LEGEND

+	POLE LIGHT
○	WALL MOUNT
□	RECESSED LIGHT
△	SPOT LIGHT

PP 12597695
Pg. # 16

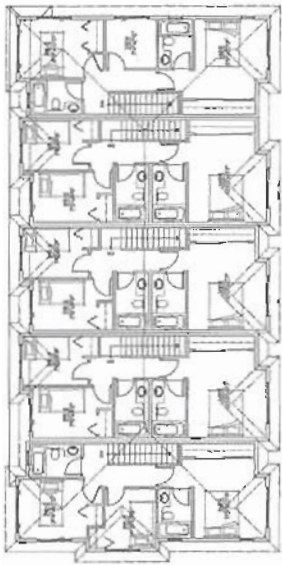
BRIDGE STREET

ITO
I.T.O. ASSOCIATES
Landscape Architects
1002 E 2nd Ave
Vancouver, BC V6H 1M0
Tel: (604) 255-5000
Fax: (604) 255-9900

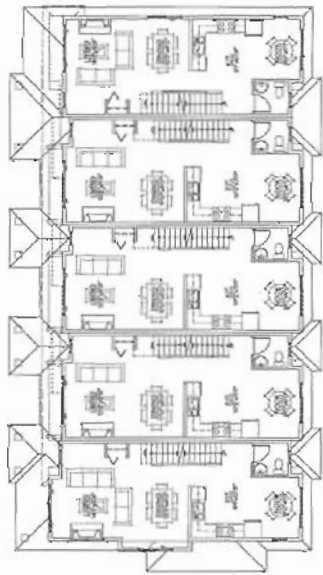
Project:
9891, 871, 19731
BRIDGE ST
RICHMOND B.C.

CONCEPTUAL
LIGHTING PLAN

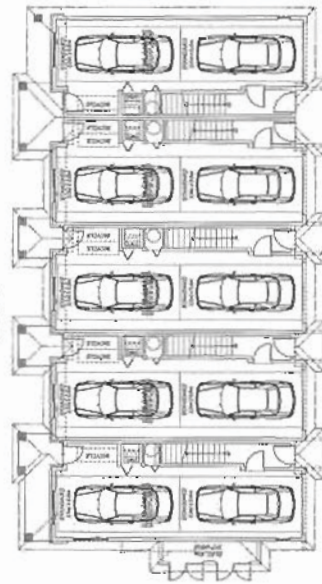
Date:	08/12/07
Client:	12597695
Drawn by:	ALJ/12/07
Scale:	AS IS



UPPER FLOOR PLAN
SCALE: 1/8" = 1'-0"



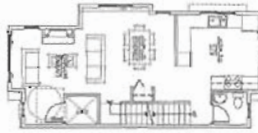
SECOND FLOOR PLAN
SCALE: 1/8" = 1'-0"



GROUND FLOOR PLAN
SCALE: 1/8" = 1'-0"



UPPER FLOOR PLAN
SCALE: 1/8" = 1'-0"



SECOND FLOOR PLAN
SCALE: 1/8" = 1'-0"



CONVERTIBLE UNIT PLAN
SCALE: 1/8" = 1'-0"

THIS UNIT IS TO BE USED AS A SINGLE-FAMILY RESIDENCE. IT IS NOT TO BE USED AS A COMMERCIAL OR INDUSTRIAL BUILDING.

DATE: 05.10.2018	12597695
PROJECT: 34 UNIT TOWNHOUSE DEVELOPMENT	
CLIENT: YAMAMOTO ARCHITECTURE INC.	
ARCHITECT: YAMAMOTO ARCHITECTURE INC.	
NO. 12597695	PLAN #50

34 UNIT TOWNHOUSE DEVELOPMENT

Yamamoto Architecture Inc.

FLOOR PLANS
12597695

REF. PG. 53
PLAN #50

12597695

PLAN #50