



**To:** Planning Committee  
**From:** Wayne Craig  
Director, Development

**Date:** November 2, 2021  
**File:** RZ 20-905210

**Re:** Application by Enrich Custom Homes Ltd. for Rezoning at 8231 No. 3 Road from the "Single Detached (RS1/E)" Zone to the "Compact Single Detached (RC2)" Zone

**Staff Recommendation**

That Richmond Zoning Bylaw 8500, Amendment Bylaw 10309, for the rezoning of 8231 No. 3 Road from the "Single Detached (RS1/E)" zone to the "Compact Single Detached (RC2)" zone, be introduced and given First Reading.

Wayne Craig  
Director, Development  
(604-247-4625)

WC/na  
Att. 6

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Affordable Housing	<input checked="" type="checkbox"/>	

## Staff Report

### Origin

Enrich Custom Homes Ltd. (Gloria Kwok) has applied to the City of Richmond, on behalf of the owner, Su Chen, for permission to rezone 8231 No. 3 Road from the “Single Detached (RS1/E)” zone to the “Compact Single Detached (RC2)” zone, to permit the property to be subdivided to create two single-family lots, each with a secondary suite and vehicle access from the rear lane (Attachment 1). The proposed subdivision is shown in Attachment 2. The proposed site plan is shown in Attachment 3.

### Findings of Fact

A Development Application Data Sheet providing details about the development proposal is attached (Attachment 4).

#### Subject Site Existing Housing Profile

There is an existing owner-occupied single-family dwelling on the subject property, which is proposed to be demolished. The applicant has confirmed that there are no existing secondary suites in the dwelling.

### Surrounding Development

Development immediately surrounding the subject site is as follows:

To the North: A single-family dwelling on property zoned “Single Detached (RS1/E)” with a rezoning application currently under staff review for two compact single-family lots with vehicle access from the rear lane (RZ 20-908348). The proposed rezoning for this site will be presented to Council for consideration via a separate staff report at the conclusion of the staff review.

To the South: A single-family dwelling on property zoned “Compact Single Detached (RC1)”.

To the East: Across No. 3 Road, properties zoned “Single Detached (RS1/E)” and “Two-Unit Dwellings (RD1)”.

To the West: Across the lane, multiple properties zone “Single Detached (RS1/E)” fronting Sunnyholme Crescent.

### Related Policies & Studies

#### Official Community Plan

The subject property is located in the Broadmoor planning area, and is designated “Neighbourhood Residential” in the Official Community Plan (OCP). The proposed rezoning and subdivision is consistent with this designation.

### Arterial Road Policy

The subject property is designated “Arterial Road Compact Lot Single Detached” on the Arterial Road Housing Development Map. The Arterial Road Land Use Policy requires all compact lot developments to be accessed from the rear lane only. The proposed rezoning and ensuing development are consistent with this Policy.

Prior to final adoption of the rezoning bylaw, the applicant must submit a Landscape Plan, prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development, and deposit a Landscape Security based on 100% of the cost estimate provided by the Landscape Architect, including installation costs. The Landscape Plan should comply with the guidelines of the OCP’s Arterial Road Policy and include any required replacement trees identified as a condition of rezoning.

### Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on title is required prior to final adoption of the rezoning bylaw.

### **Public Consultation**

A rezoning sign has been installed on the subject property. Staff have not received any comments from the public about the rezoning application in response to the placement of the rezoning sign on the property.

Should the Planning Committee endorse this application and Council grant First Reading to the rezoning bylaw, the bylaw will be forwarded to a Public Hearing, where any area resident or interested party will have an opportunity to comment. Public notification for the Public Hearing will be provided as per the *Local Government Act*.

### **Analysis**

This redevelopment proposes to rezone and subdivide one existing single-family property into two new compact single-family lots, each with a secondary suite, and vehicular access from the rear lane. This rezoning and subdivision is consistent with the lot fabric and vehicular access of the adjacent lots on No. 3 Road. Similar applications to rezone and subdivide properties have been approved in years past to the south of the subject property.

### Tree Retention and Replacement

The applicant has submitted a Certified Arborist’s Report; which identifies on-site and off-site trees, assesses tree structure and condition, and provides recommendations on tree retention and removal relative to the proposed development. The Report assesses one bylaw-sized tree on the subject property, one street tree on City property, and a Cedar hedgerow composed of 13 trees on neighbouring property (8211 No. 3 Road). Additionally, there is an undersized Japanese Maple tree, two hedges in the existing rear yard and a hedge on the neighbouring property (8233 No. 3 Road).

The City’s Tree Preservation Coordinator and City Parks staff have reviewed the Arborist’s Report and provided the following comments:

- One bylaw-sized tree on-site, tag# 828 (Apple tree 28 cm caliper), is in declining health due to being uprooted in the past and should be removed and replaced.
- Replacement trees should be specified at 2:1 ratio as per the OCP.
- One untagged non-bylaw sized Japanese Maple tree located on site is in good condition and should be relocated. The Landscape Plan required prior to rezoning final adoption will identify where the Japanese Maple tree will be relocated to.
- The hedgerow in the rear yard along the proposed shared property line in the rear yard is in fair condition. Further review of how the hedgerow may be retained will be done as part of the Landscape Plan.
- The hedgerow in the southwest corner is over-grown and in fair condition but needs to be removed to facilitate rear lane access to the site.
- 13 Cedar hedgerow trees (tag# 830, 830, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841) located on an adjacent neighbouring property at 8211 No. 3 Road are identified to be retained and protected. Further assessment of the hedgerow will be done as part of the rezoning application for the property at 8211 No. 3 Road to determine if the hedgerow will be retained. Tree protection is to be provided as per City of Richmond Tree Protection Information Bulletin Tree-03 including tree protection fencing.
- The off-site hedgerow on the neighbouring property to the south at 8233 No. 3 Road is to be retained.
- One City tree tag# 829 (Cherry tree multi-stem 22 cm caliper) is in fair condition and should be retained and protected. Off-site improvements are to be worked around the retained tree. A Tree Survival Security of \$10,000.00 will be required.

*Tree Replacement*

The applicant wishes to remove one on-site tree (tag# 828). The 2:1 replacement ratio would require a total of two replacement trees. The applicant has agreed to plant a minimum of two trees on each lot proposed. Two trees are required to meet City requirements for new subdivisions for a total minimum of four trees to be provided. The required replacement trees are to be of the following minimum sizes, based on the size of the trees being removed as per Tree Protection Bylaw No. 8057.

No. of Replacement Trees	Minimum Caliper of Deciduous Replacement Tree	Minimum Height of Coniferous Replacement Tree
4	8 cm	4 m

*Tree Protection*

One City tree (tag# 829), a non-bylaw sized Japanese Maple, a hedgerow composed 13 Cedar trees on neighbouring property at 8211 No. 3 Road and a hedgerow on the neighbouring property to the south at 8233 No. 3 Road are to be retained and protected. The applicant has submitted a tree protection plan showing the trees to be retained and the measures taken to protect them during development stage (Attachment 5). To ensure that the trees identified for retention are protected at development stage, the applicant is required to complete the following items:

- Prior to final adoption of the rezoning bylaw, submission of a Tree Survival Security of \$10,000.00 for the retention and protection of the trees noted.
- Prior to final adoption of the rezoning bylaw, submission to the City of a contract with a Certified Arborist for the supervision of all works conducted within or in close proximity to tree protection zones. The contract must include the scope of work required, the number of proposed monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the Arborist to submit a post-construction impact assessment to the City for review.
- Prior to demolition of the existing dwelling on the subject site, installation of tree protection fencing around all trees to be retained. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and remain in place until construction and landscaping on-site is completed.

### Affordable Housing Strategy

Consistent with the Affordable Housing Strategy, the applicant has proposed a secondary suite in both new dwellings; each being a minimum of 34.8 m<sup>2</sup> (375 ft<sup>2</sup>) and having minimum one bedroom each. Parking for each secondary suite will be accessed by the lane, adjacent to each garage. Prior to final adoption of the rezoning bylaw, the applicant must register a legal agreement on title to ensure that no final Building Permit inspection is granted until the secondary suite on Lot A and Lot B is constructed to the satisfaction of the City in accordance with the BC Building Code and the City's Zoning Bylaw.

### Transportation and Site Access

Vehicular access to No. 3 Road is not permitted. Registration of a restrictive covenant on title will be required to ensure vehicle access to the site at future development stage is from the rear lane only, with no access permitted to or from No. 3 Road (servicing road). Secondary suite parking will also be provided as required by Zoning Bylaw 8500.

### Site Servicing and Frontage Improvements

At Subdivision stage, the applicant must enter into a Servicing Agreement for the design and construction of the required site servicing and off-site improvements, including lane upgrades, as described in Attachment 6. Provision of a 0.6 m wide road dedication is required to facilitate sidewalk improvements and boulevard realignment. Additionally, a 3.0 m wide right-of-way (ROW) along the entire east property line will be required prior to adoption of the rezoning bylaw for containing inspection chambers and water meters. All frontage works will be required to work around trees identified for retention.

At Subdivision stage, the applicant is also required to pay the current year's taxes, Development Cost Charges (City, Metro Vancouver and TransLink), School Site Acquisition Charges, Address Assignment Fees, and enter into a Servicing Agreement for site servicing and frontage improvements, including the rear lane, as described in Attachment 6.

**Financial Impact**

None.

**Conclusion**

The purpose of this application is to rezone 8231 No. 3 Road from the “Single Detached (RS1/E)” zone to the “Compact Single Detached (RC2)” zone, to permit the property to be subdivided to create two single-family lots with secondary suites with vehicle access from the rear lane.

The proposed rezoning and subdivision are consistent with the applicable plans and policies affecting the subject.

The list of rezoning considerations is included in Attachment 6, which has been agreed to by the applicant (signed concurrence on file).

It is recommended that Richmond Zoning Bylaw 8500, Amendment Bylaw 10309 be introduced and given First Reading.

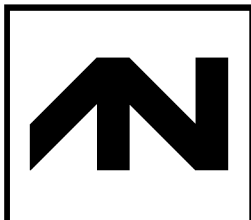
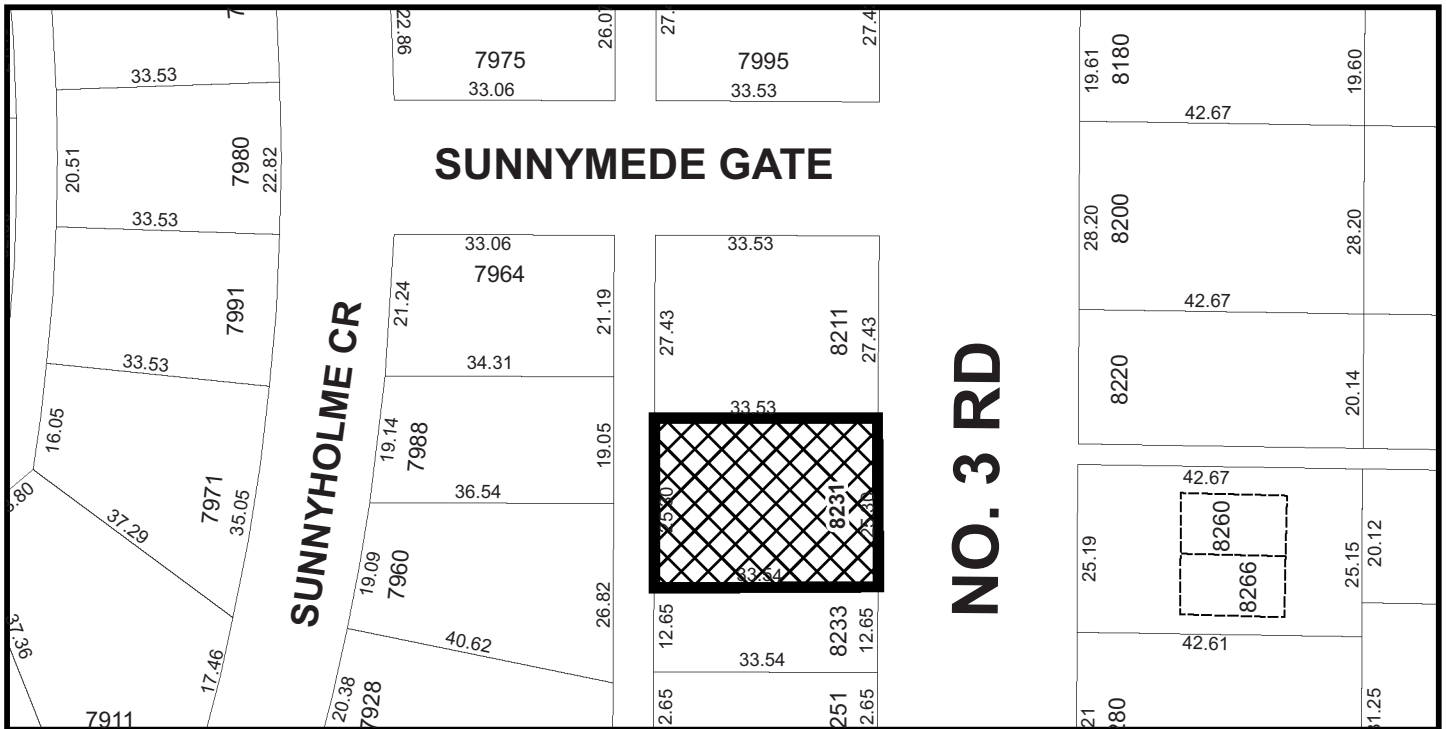
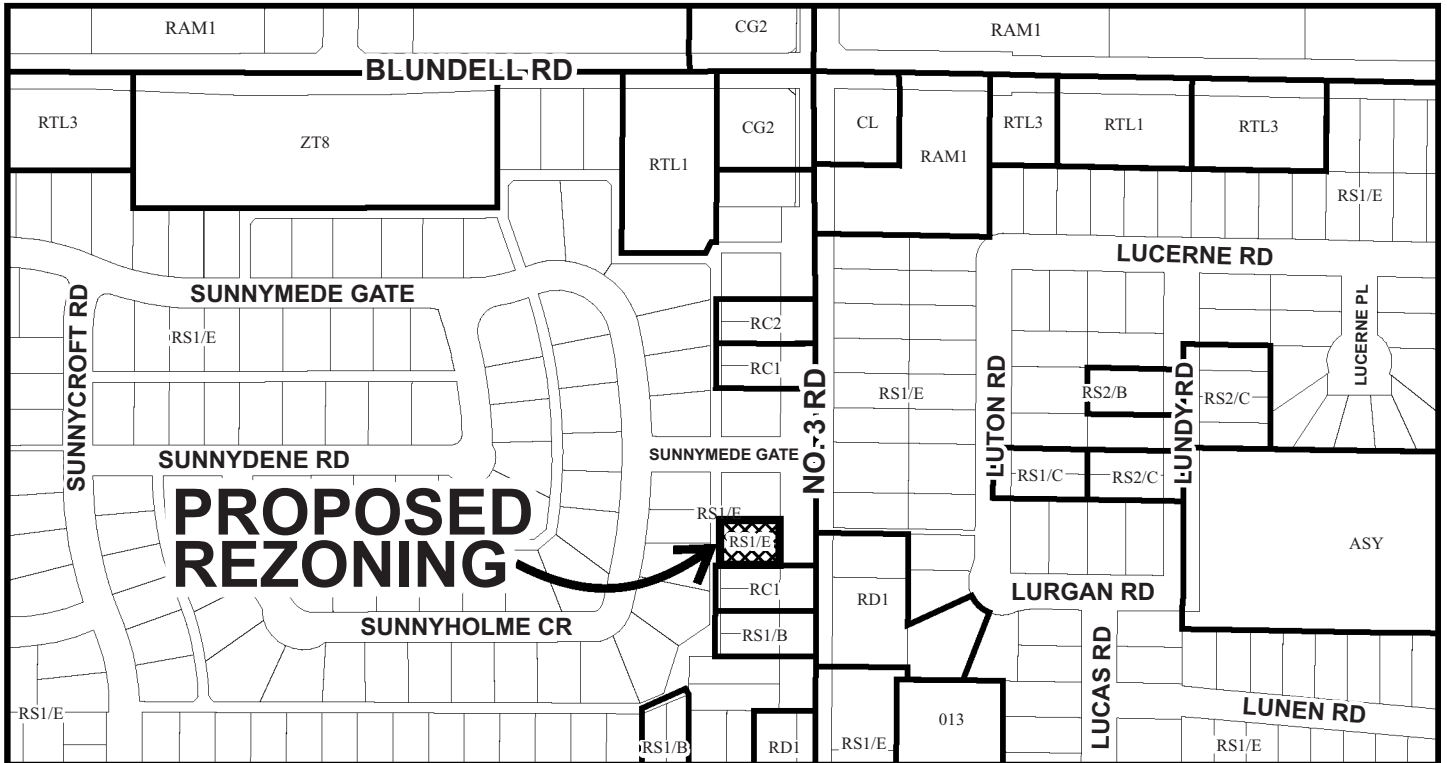


Nathan Andrews  
Planning Technician  
(604-247-4911)

NA:blg

Attachments:

- Attachment 1: Location and Aerial Map
- Attachment 2: Survey and Proposed Subdivision Plan
- Attachment 3: Conceptual Development Plans
- Attachment 4: Development Application Data Sheet
- Attachment 5: Tree Retention Plan
- Attachment 6: Rezoning Considerations



RZ 20-905210

Original Date: 10/28/20

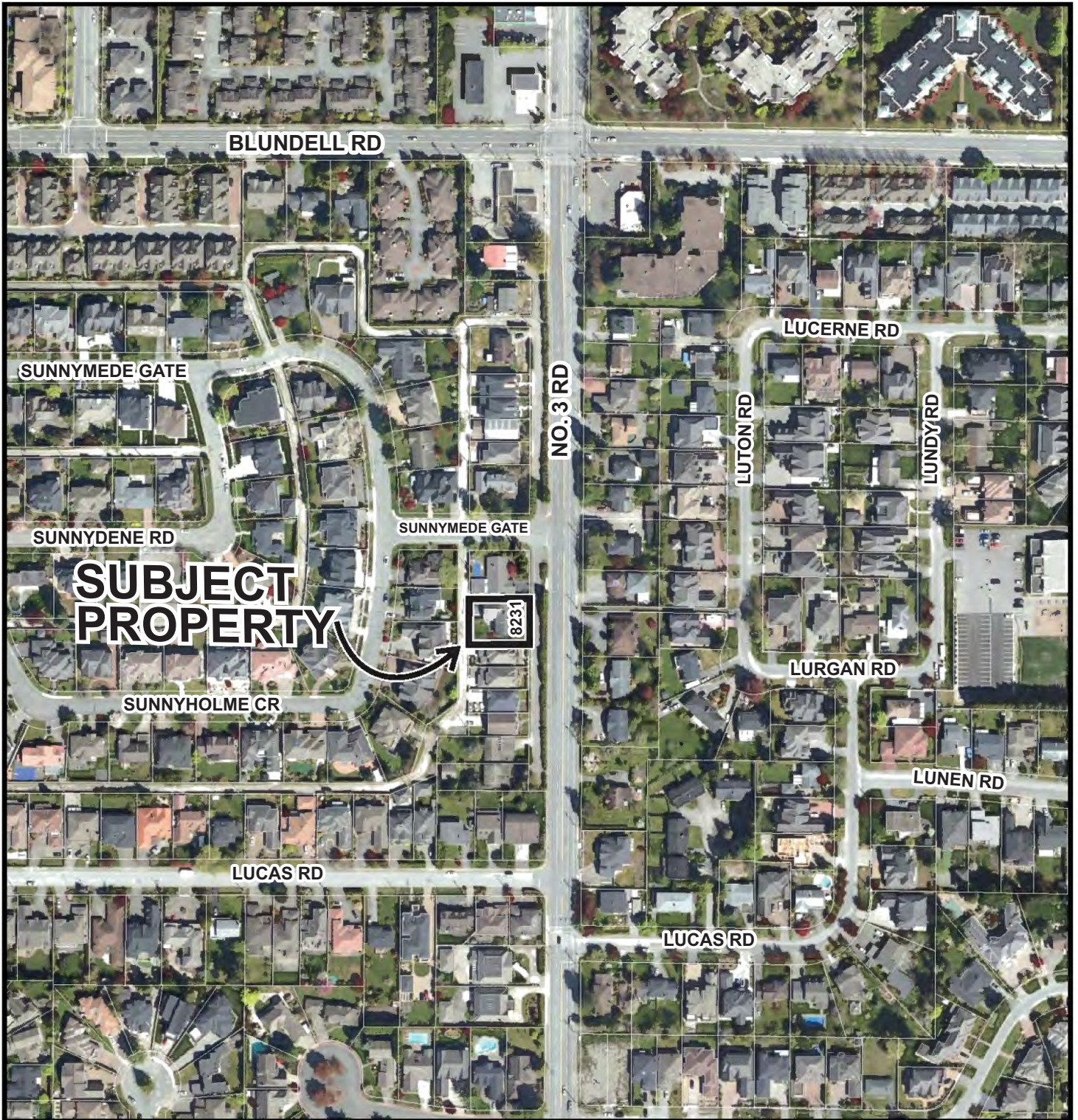
Revision Date:

Note: Dimensions are in METRES





# City of Richmond



**RZ 20-905210**

Original Date: 10/28/20

Revision Date:

Note: Dimensions are in METRES



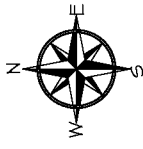
**TOPOGRAPHIC SURVEY AND PROPOSED SUBDIVISION OF  
 LOT 27 SECTION 20 BLOCK 4 NORTH RANGE 6 WEST  
 NEW WESTMINSTER DISTRICT PLAN 21352**

#8231 NO. 3 ROAD,  
 RICHMOND, B.C.  
 P.I.D 004-881-702

**PH - 28**

© copyright  
 J. C. Tam and Associates  
 Canada and B.C. Land Surveyor  
 115 - 8833 Odlin Crescent  
 Richmond, B.C. V6X 3Z7  
 Telephone: 214-8928  
 Fax: 214-8929  
 E-mail: office@jctam.com  
 Website: www.jctam.com  
 Job No. 7489  
 FB-386 P59-61  
 Drawn By: MY/TH

**DWG No. 7489-TOPO-02**



**SCALE: 1:200**



ALL DISTANCES ARE IN METRES AND DECIMALS  
 THEREOF UNLESS OTHERWISE INDICATED

**LEGEND:**

- (d) denotes deciduous
- CB denotes catch basin
- LS# denotes lamp standard
- WR denotes round catch basin
- WV denotes water valve
- denotes power pole
- denotes water meter
- CO denotes cleanout
- T denotes top of wall
- ST denotes stair elevation

**NOTE:**

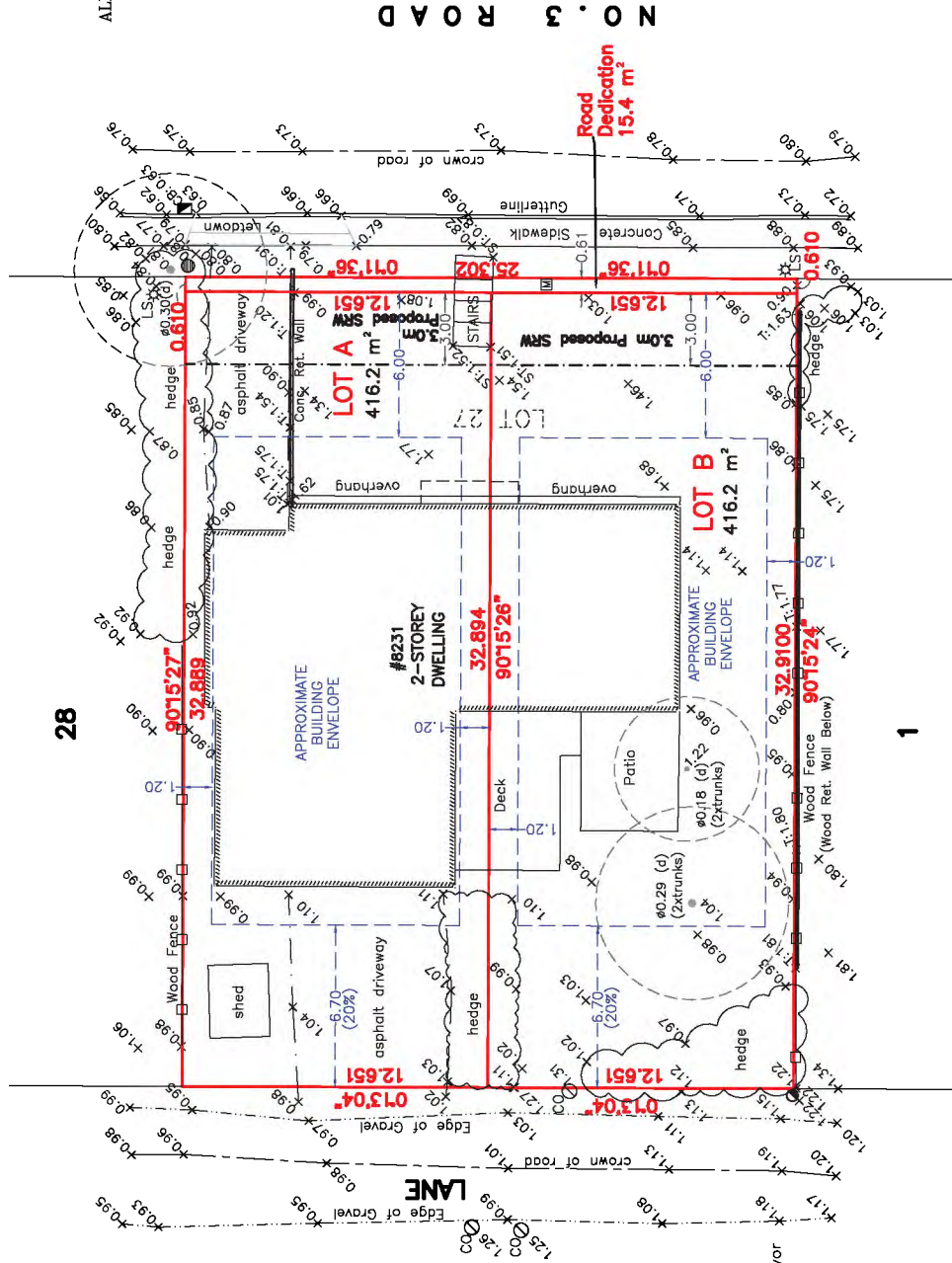
Elevations shown are based on  
 City of Richmond HPN  
 Benchmark network.  
 Benchmark: HPN #191  
 Control Monument 02H2453  
 Elevation: 1.664m  
 Benchmark: HPN #204  
 Control Monument 02H2452  
 Elevation: 1.559m

**CERTIFIED CORRECT:**  
 LOT DIMENSION ACCORDING TO  
 FIELD SURVEY.

JOHNSON C. TAM, B.C.L.S.

**MAY 6th, 2020**

ATTACHMENT 2



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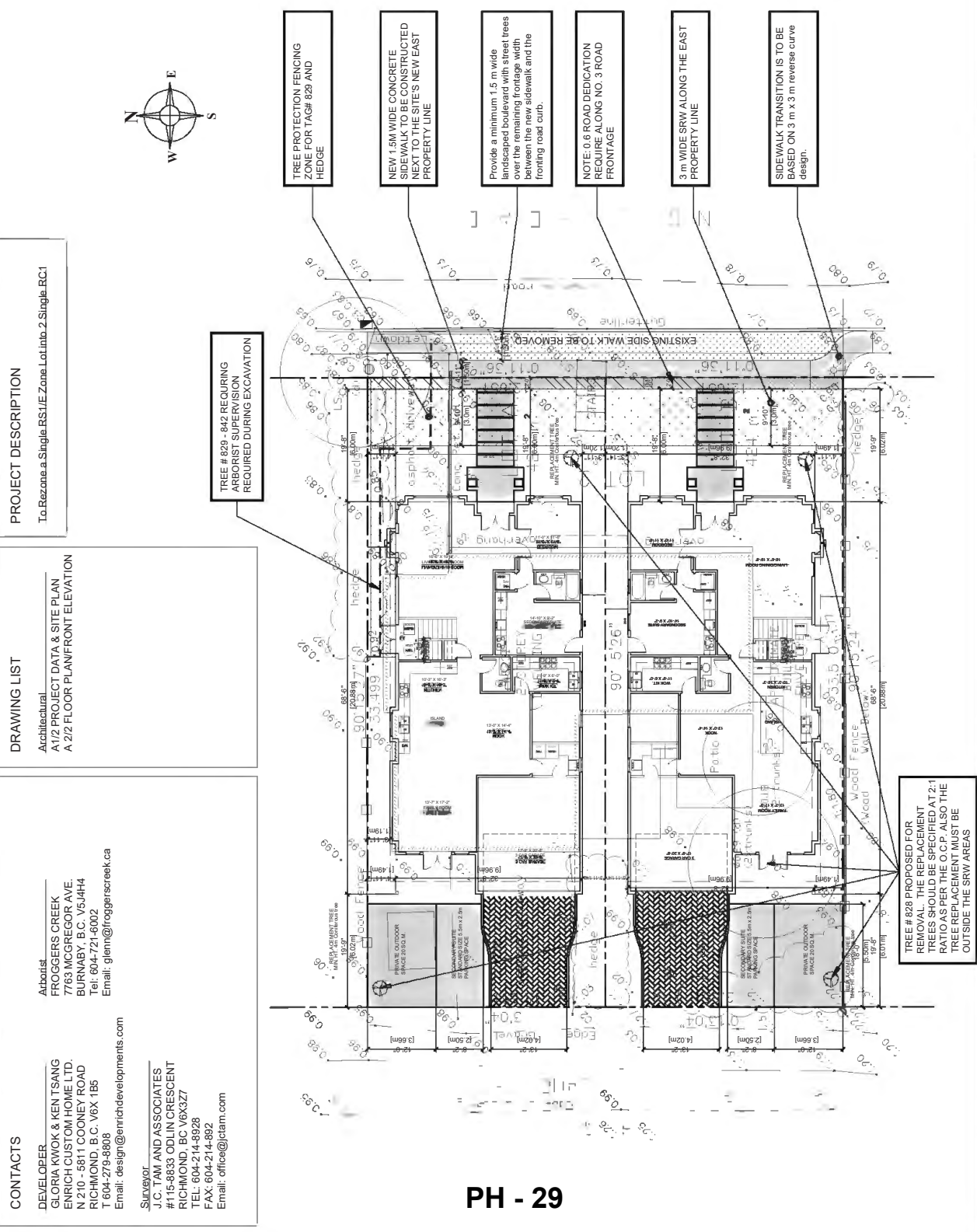
1

PROJECT DESCRIPTION:	
CIVIC ADDRESS:	8291 No. 3 Road RICHMOND, B.C.
LEGAL ADDRESS:	TOPOGRAPHIC SURVEY OF LOT 127 SEC. 20, BLK 4 N. ROE. S.V. N.W.D., PLAN 21552 PID: 005-585-792
ZONE:	REZONE FROM RS1E TO R2
GROSS AREA include Road Dedication	846.00 SQ.M (8,127.80 SQ.FT.)
GROSS AREA - 0.6m Road Dedication	832.82 SQ.M (8,843.38 SQ.FT.)
SITE SIZE PER LOT - 0.6m Road Dedication	436.41 SQ.M (4,632.19 SQ.FT.)
FAR	PERMITTED 0.60 x 4.482 (9 x 2,893.31 SQFT) PROPOSED 2,893.31 SQFT
MAIN FLOOR	PROPOSED 1,989.87 SQFT
UPPER FLOOR	PROPOSED 1,100.28 SQFT
GARAGE	PERMITTED 538.21 SQFT PROPOSED 509.50 SQFT
LOT COVERAGE	PERMITTED 4,482.19 X 0.45 = 2,018.98 SQFT PROPOSED 2,018.98 SQFT
IMP. AREA	PERMITTED 4,482.19 X 0.70 = 3,137.53 SQFT PROPOSED 3,137.53 SQFT
OUTDOOR AREA	PERMITTED 281.83 SQFT PROPOSED 281.83 SQFT
ACC. BLDG.	PERMITTED 107.00 SQFT PROPOSED NA
LANDSCAPING REQUIREMENT	
MINIMUM 20% LANDSCAPING PER LOT	4,482.19 SF X 20% = 896.44 SF
PROPOSED	900 SF

**NOTES:**

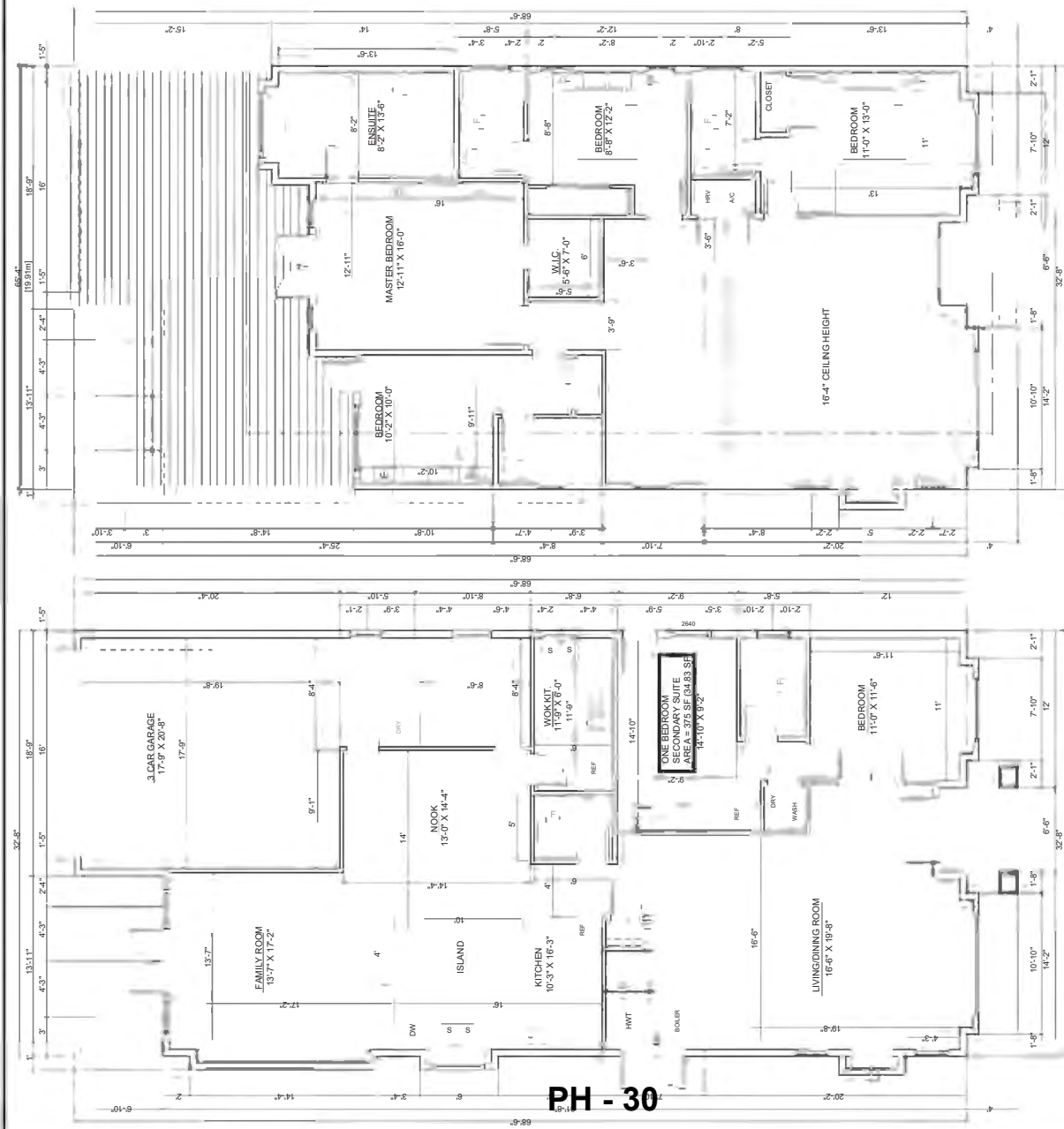
1. WRITTEN DIMENSIONS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS.
2. CONTRACTOR SHALL VERIFY ALL ON SITE CONDITIONS PRIOR TO COMMENCEMENT OF THE WORK
3. ALL WORK SHALL CONFORM TO THE REQUIREMENT OF BRITISH COLUMBIA'S 2018 EDITION OF THE REQUIREMENT OF BRITISH COLUMBIA'S 2018 EDITION OF THE REQUIREMENT OF BRITISH C.S.A. 34581 LATEST EDITION.
4. ALL CONCRETE SHALL CONFORM TO THE REQUIREMENT OF C.S.A. 34581 LATEST EDITION.
5. CONCRETE STRENGTH AT 28 DAYS, MINIMUM 3,000 PSI.
6. ALL FRAMING AND MAILING SHALL CONFORM TO B.C. BUILDING CODE PART 9 AND DESIGN TO C.S.A. LATEST EDITION.
7. ALL FRAMING LUMBER SHALL BE DOUGLAS FIR @2 OR BETTER.
8. WOOD TRUSSES SHALL BE DESIGNED AND SEALED BY P.E. REGISTERED IN B.C.
9. ALL PLYWOOD SHALL BE DOUGLAS FIR CONFORM TO C.S.A. 0 921
10. OWNER IS TO BE ENTIRELY RESPONSIBLE FOR ALL STRUCTURAL ENGINEERING REQUIREMENT.
11. ALL DIMENSIONS ARE TO BE CHECKED ON SITE BEFORE WORK COMMENCES. THERE IS NO RESPONSIBILITY OF THE DESIGNER. THE OWNER CONTRACTOR/BUILDER SHOULD INFORM THE DESIGNER BEFORE COMMENCEMENT OF CONSTRUCTION FOR ANY DISCREPANCY.

THESE PLANS CONFORM TO BCBC 2018



8231 NO. 3 ROAD  
RICHMOND, B.C.

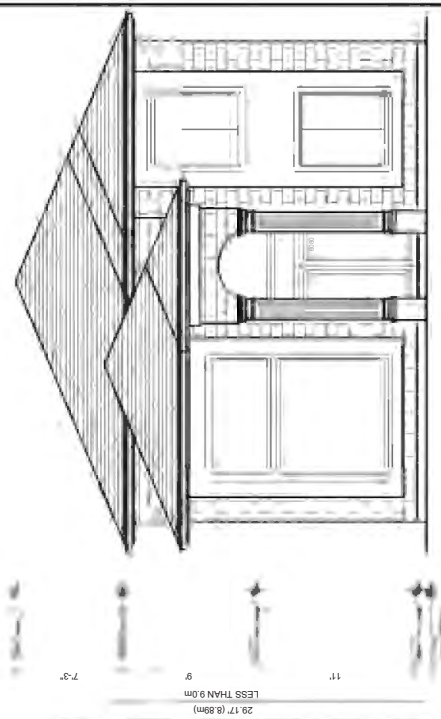
OCTOBER 24, 2021  
DESIGN PROPOSAL 2 LOT SUBDIVISION



**FLOOR PLAN**  
SCALE: 1/4" = 1'-0"  
FAR: 1,588.97 SF

**FLOOR PLAN**  
SCALE: 1/4" = 1'-0"  
FAR: 1,100.34 SF

**SOUTH ELEVATION**  
SCALE: 1/4" = 1'-0"



PH - 30

OCTOBER 24, 2021  
DESIGN PROPOSAL 2 LOT SUBDIVISION



ENRICH DEVELOPMENT GROUP  
N 210 - 5811 COONEY ROAD,  
RICHMOND, BC V6X 1B5  
T 604-279-8808 EMAIL: design@enrichdevelopments.com

THESE PLANS CONFORM TO BCBC 2018

8231 ND. 3RD  
RICHMOND, B.C.



**RZ 20-905210**

**Attachment 4**

Address: 8231 No. 3 Road

Applicant: Enrich Custom Homes Ltd.

Planning Area(s): Broadmoor

	Existing	Proposed
<b>Owner:</b>	Su Chen	To be determined
<b>Site Size (m<sup>2</sup>):</b>	848 m <sup>2</sup>	Lot A: 416 m <sup>2</sup> Lot B: 416 m <sup>2</sup>
<b>Land Uses:</b>	One Single Detached Dwelling	Two Single Detached Dwellings
<b>OCP Designation:</b>	Neighbourhood Residential	No change
<b>Area Plan Designation:</b>	N/A	No change
<b>Zoning:</b>	Single Detached (RS1/E)	Compact Single Detached (RC2)
<b>Number of Units:</b>	1	2
<b>Other Designations:</b>	Arterial Road Compact Lot Single Detached	No change

On Future Subdivided Lots	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.60 for lot area up to 464.5 m <sup>2</sup> plus 0.3 for area in excess of 464.5 m <sup>2</sup>	0.60	none permitted
Buildable Floor Area (m <sup>2</sup> ):*	Lot A: Max. 249.8m <sup>2</sup> (2,689.31ft <sup>2</sup> ) Lot B: Max. 249.8 m <sup>2</sup> (2,689.31ft <sup>2</sup> )	Lot A: Max. 249.8m <sup>2</sup> (2,689.31 ft <sup>2</sup> ) Lot B: Max. 249.8 m <sup>2</sup> (2,689.31 ft <sup>2</sup> )	none permitted
Lot Coverage (% of lot area):	Building: Max. 50% Non-porous Surfaces: Max. 70% Live Landscaping: Min. 20%	Building: Max. 50% Non-porous Surfaces: Max. 54% Live Landscaping: Min. 20%	none
Lot Size:	Min. 270 m <sup>2</sup>	416 m <sup>2</sup>	none
Lot Dimensions (m):	Width: 9.0 m Depth: 24.0 m	Width: 12.65 m Depth: 33.5 m	none
Setbacks (m):	Front: Min. 6.0 m Rear: Min. 6.0 m Side: Min. 1.2 m	Front: Min. 6.0 m Rear: Min. 6.0 m Side: Min. 1.2 m	none
Height (m):	9.0 m or 2.5 storeys	8.9 m	none



<b>On Future Subdivided Lots</b>	<b>Bylaw Requirement</b>	<b>Proposed</b>	<b>Variance</b>
Off-street Parking Spaces – Regular (R) / Secondary Suite (S):	2 (R) and 1 (S) per unit	2 (R) and 1 (S) per unit	none
Private Outdoor Space (m <sup>2</sup> ):	Min. 20 m <sup>2</sup> (min. 3.0 m width and depth) provided on the lot outside front yard	Min. 20 m <sup>2</sup>	none

Other: Tree replacement compensation required for loss of significant trees.

\* Preliminary estimate; not inclusive of garage; exact building size to be determined through zoning bylaw compliance review at Building Permit stage.

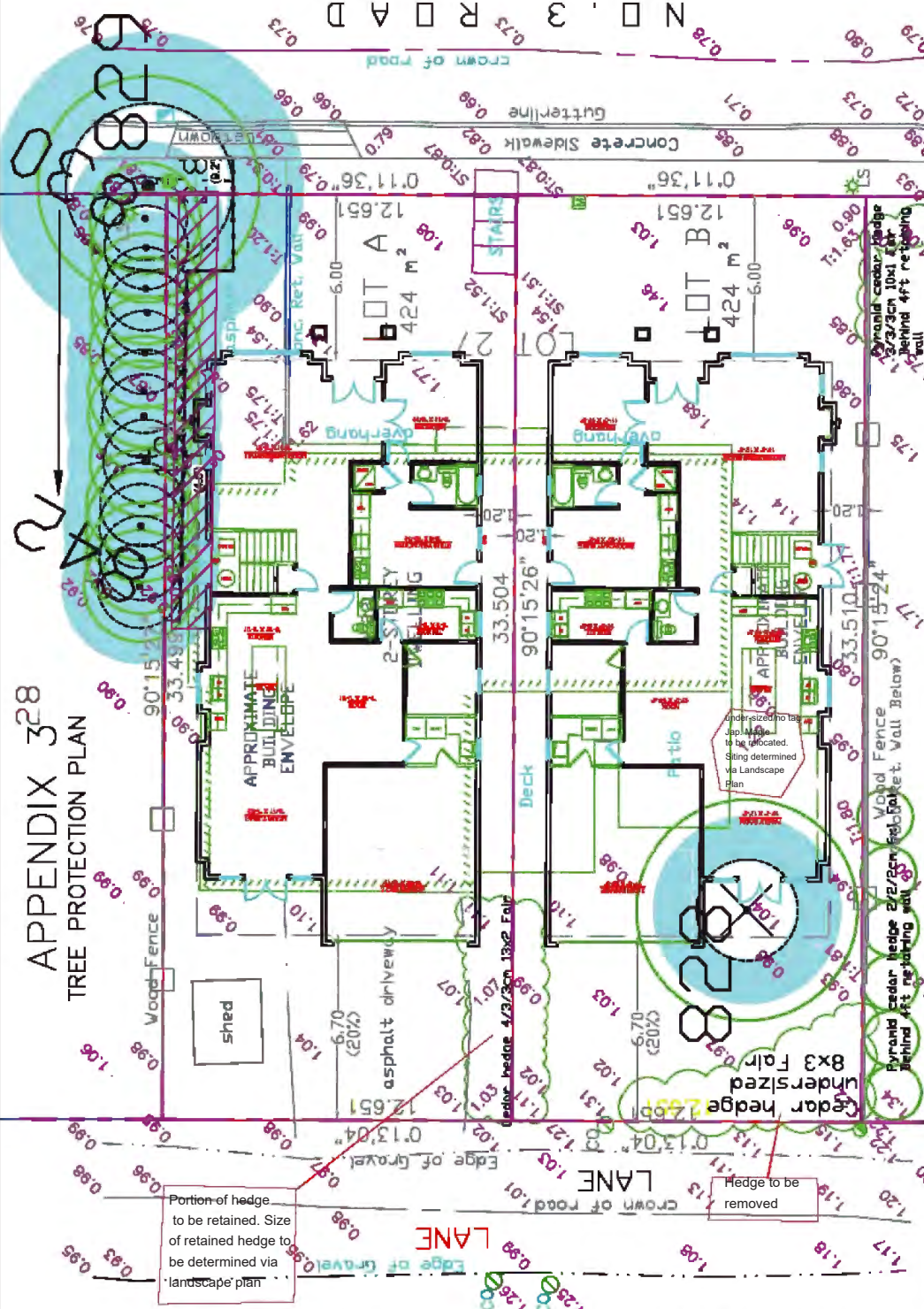
Froggers Creek  
Tree Consultants Ltd

7783 Midway Avenue, Burnaby BC V5J 4K4  
Telephone 604-781-8888 Fax 604-457-8879

6827 16 J. M. Armstrong St  
Vancouver BC

TREE PROTECTION ZONING  
THE DRAWING PLATS ALL TREES, PROPOSED FOR RETENTION, REMOVAL, OR COMPROMISE, SHALL BE IDENTIFIED WITH TREE PROTECTION ZONING IN RELATION TO PROPOSED LAYOUT

# APPENDIX 328 TREE PROTECTION PLAN



## TREE PROTECTION FENCING

Minimum Radial Distance from trunk

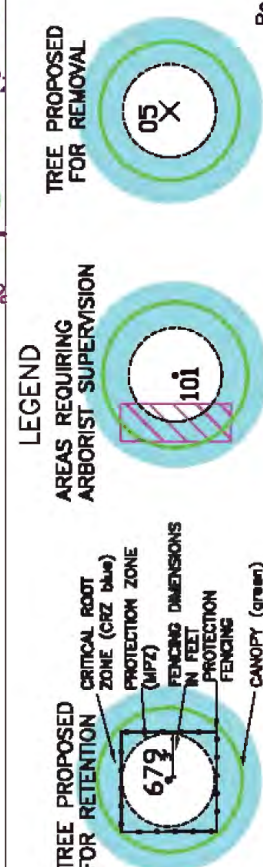
#	Type	DBH	Metres	Feet
829	Cherry	21/22/18cm	2.8m	9.2ft
830	Pyramid Cedar	23cm	1.4m	4.5ft
831	Pyramid Cedar	23cm	1.4m	4.5ft
832	Pyramid Cedar	23cm	1.4m	4.5ft
833	Pyramid Cedar	23cm	1.4m	4.5ft
834	Pyramid Cedar	23cm	1.4m	4.5ft
835	Pyramid Cedar	23cm	1.4m	4.5ft
836	Pyramid Cedar	23cm	1.4m	4.5ft
837	Pyramid Cedar	23cm	1.4m	4.5ft
838	Pyramid Cedar	23cm	1.4m	4.5ft
839	Pyramid Cedar	23cm	1.4m	4.5ft
840	Pyramid Cedar	23cm	1.4m	4.5ft
841	Pyramid Cedar	23cm	1.4m	4.5ft
842	Pyramid Cedar	23cm	1.4m	4.5ft

## TREE INVENTORY

Type	Action	DBH	MPZ
828 Apple	Remove	28cm	1.7m
829 Cherry	Retain	22/22/18cm	2.9m
830 Pyramid Cedar	Retain	23cm	1.4m
831 Pyramid Cedar	Retain	23cm	1.4m
832 Pyramid Cedar	Retain	23cm	1.4m
833 Pyramid Cedar	Retain	23cm	1.4m
834 Pyramid Cedar	Retain	23cm	1.4m
835 Pyramid Cedar	Retain	23cm	1.4m
836 Pyramid Cedar	Retain	23cm	1.4m
837 Pyramid Cedar	Retain	23cm	1.4m
838 Pyramid Cedar	Retain	23cm	1.4m
839 Pyramid Cedar	Retain	23cm	1.4m
840 Pyramid Cedar	Retain	23cm	1.4m
841 Pyramid Cedar	Retain	23cm	1.4m
842 Pyramid Cedar	Retain	23cm	1.4m

DBH=diameter, MPZ= protection zone

- NOTES:
1. SITE LAYOUT INFORMATION AND TREE SURVEY DATA PER SUPPLIED DRAWING
  2. REFER TO ATTACHED TREE PROTECTION REPORT FOR ADDITIONAL COMMENTS, TREE SPECIES, STEM DIAMETER, HEIGHT, CANOPY SPREAD AND CONDITION.
  3. ALL MEASUREMENTS ARE METRIC





**Address:** 8231 No. 3 Road

**File No.:** RZ 20-905210

**Prior to final adoption of Richmond Zoning Bylaw 8500, Amendment Bylaw 10309, the developer is required to complete the following:**

1. 0.6 m wide road dedication along the entire east frontage.
2. Submission of a Landscape Plan, prepared by a Registered Landscape Architect, to the satisfaction of the Director of Development, and deposit of a Landscaping Security based on 100% of the cost estimate provided by the Landscape Architect, including installation costs. The Landscape Plan should:
  - comply with the guidelines of the OCP’s Arterial Road Policy and should not include hedges along the front property line;
  - include a mix of coniferous and deciduous trees;
  - include the dimensions of tree protection fencing as illustrated on the Tree Retention Plan attached to this report;
  - One untagged and non-bylaw sized Japanese maple tree located on site is in good condition and is to be relocated. The Landscape Plan required prior to rezoning final adoption should incorporate the undersized Japanese maple tree and the tree is to be hand-dug when relocated.
  - The hedgerow along the proposed shared property line in the rear yard is in fair condition and should be considered for partial retention. Further review of the hedgerow integration should be done as part of the Landscape Plan.
  - include the 4 required replacement trees with the following minimum sizes:

<b>No. of Replacement Trees</b>	<b>Minimum Caliper of Deciduous Tree</b>	or	<b>Minimum Height of Coniferous Tree</b>
4	8 cm		4 m

3. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
4. Submission of a Tree Survival Security to the City in the amount of \$10,000.00 for the 1 City tree and neighbouring hedgerow to be retained.
5. Registration of a flood indemnity covenant on title.
6. Registration of a Statutory Right-of-Way to provide a 3.0 m right-of-way along the development’s entire east property line, for the purpose of containing inspection chambers and water meters.
7. Registration of a legal agreement on Title to ensure that no final Building Permit inspection is granted until a 1-bedroom secondary suite of minimum 34.8 m<sup>2</sup> (375 ft<sup>2</sup>) is constructed on both of the future lots (Lot A and Lot B), to the satisfaction of the City in accordance with the BC Building Code and the City’s Zoning Bylaw.
8. Registration of a restrictive covenant on title to ensure vehicular access to the site at future development stage is from the rear lane only, with no access permitted to or from No. 3 Road.

**Prior to Demolition Permit Issuance, the developer must complete the following requirements:**

1. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.

**At Subdivision\* stage, the developer must complete the following requirements:**

1. At Subdivision stage, the applicant is required to pay the current year’s taxes, Development Cost Charges (City, Metro Vancouver and TransLink), School Site Acquisition Charges, Address Assignment Fees, and the costs associated with the completion of the site servicing and other improvements.



2. Enter into a Servicing Agreement\* for the design and construction of engineering infrastructure improvements. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to:

**Water Works:**

- a) Using the OCP Model, there is 881 L/s of water available at a 20 psi residual at the No 3 Road frontage. Based on your proposed development, your site requires a minimum fire flow of 95 L/s.
- b) At Developer's cost, the Developer is required to:
  - i) Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage building designs.
  - ii) Provide a 3.0 m-wide utility right-of-way along the entire east property line of the site (requirement to be completed prior to rezoning final adoption).
- c) At Developer's cost, the City will:
  - i) Cap and remove the existing water service connection and meter.
  - ii) Install a new service connection for each of the newly subdivided lots, complete with meter located onsite in proposed right-of-way.

**Storm Sewer Works:**

- d) At Developer's cost, the City will:
  - i) Cap and remove the existing storm connection and inspection chamber.
  - ii) Install a new storm connection complete with inspection chamber located onsite in the proposed right-of-way and dual service leads.

**Sanitary Sewer Works:**

- e) At Developer's cost, the City will:
  - i) Cap and remove the existing sanitary connection and inspection chamber.
  - ii) Install a new sanitary connection complete with inspection chamber and dual service leads.

**Frontage Improvements:**

- f) At Developer's cost, the Developer is required to:
  - i) Coordinate with BC Hydro, Telus and other private communication service providers:
    - (1) Before relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
    - (2) To locate all above ground utility cabinets and kiosks required to service the proposed development within the development site.
  - ii) Upgrade the rear lane along the development frontage to the City's standards per R-5-DS in the Engineering Design Specifications, complete with rollover curbs, asphalt, drainage, and lighting. The drainage shall be extended to the north to connect to the existing storm sewer in Sunnymede Crescent, complete with a new manhole at the tie-in.
  - iii) Complete other frontage improvements as per Transportation requirements.

Road frontage:

Across the No. 3 Road (service road) development frontage, the following improvements are required:

- Remove the existing sidewalk and construction a new 1.5 m wide concrete sidewalk along the site's east property line. The alignment of the sidewalk may have to be adjusted to go around trees identified for retention.



- Provide a minimum 1.5 m wide landscaped boulevard with street trees over the remaining frontage width between the new sidewalk and the fronting road curb.
- If the width of the exiting frontage is not sufficient for supporting these standards, road dedications would be required.
- Transition of frontage improvements:
- The new sidewalk and boulevard are also to be transitioned to meet the existing frontage treatments to the south of the subject site.
- The existing driveway along the development road frontage is to be closed permanently. The Developer is responsible for the removal of the existing driveway let-down and the replacement with barrier curb/gutter, boulevard and concrete sidewalk per standards described above.
- Reinstate/back-fill street signage and pavement marking affected by the frontage works.

Lane upgrade:

The existing lane along the subject site's west property line is to be upgraded to the following standards:

- 6.0 m right-of-way.
- 5.1 m wide pavement.
- Continuous rollover curb and gutter along both sides of the lane.
- Lighting.

The lane is to be upgraded as per City Engineering Design Specifications for Roadworks (Drawing R-6-DS) constructed as part of a Servicing Agreement.

Engineering will determine:

- The exact finished cross-section of the lane taking into account lighting and other utility requirements; and
- The requirement for repaving the existing driving surface in this section of the lane.

Access to lane:

The driveway let-down at the north end of the lane (Sunnymede Gate) is to be reconstructed to meet the upgraded lane cross-section noted above. The design standards for the driveway let-down are to meet those listed in the City Engineering Design Specifications for Roadworks (Drawing RD-9-DS).

A road functional plan is required to show the above noted frontage improvements. The plan must also show clear dimensions and any right-of-way and/or dedication requirements.

- Consult Parks on the requirements for tree protection/placement including tree species and spacing as part of the frontage works.
- Consult Engineering on lighting and other utility requirements as part of the frontage works.
- Per Zoning Bylaw requirements, the Developer is required to provide, for all residential parking spaces (excluding visitor parking), Level 2 EV charging outlets (208V to 240V AC and current of 16A to 80A).

**General Items:**

- g) At Developer's cost, the Developer is required to:
- i) Not encroach into City rights-of-ways with any proposed trees, retaining walls, or other non-removable structures. Retaining walls proposed to encroach into rights-of-ways must be reviewed by the City's Engineering Department.
  - ii) Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

**Prior to Building Permit Issuance, the developer must complete the following requirements:**

1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
2. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

**Note:**

- \* This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

Signed \_\_\_\_\_

Date \_\_\_\_\_



**Richmond Zoning Bylaw 8500  
Amendment Bylaw 10309 (RZ 20-905210)  
8231 No. 3 Road**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation of the following area and by designating it **“COMPACT SINGLE DETACHED (RC2)”**.

P.I.D. 004-881-702

Lot 27 Section 20 Block 4 North Range 6 West New Westminster District Plan 21352

2. This Bylaw may be cited as **“Richmond Zoning Bylaw 8500, Amendment Bylaw 10309”**.

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

OTHER CONDITIONS SATISFIED

ADOPTED

NOV 22 2021

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CITY OF RICHMOND
APPROVED by <i>[Signature]</i>
APPROVED by Director or Solicitor <i>[Signature]</i>

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CORPORATE OFFICER