



To: Development Permit Panel

Date: October 9, 2012

From: Wayne Craig,
Director of Development

File: DP 12-615424

Re: Development Permit DP 12-615424 – Follow-Up from September 26, 2012

Background

At the September 26, 2012 meeting of the Development Permit Panel, a Development Permit application for Onni Contracting Ltd.'s 659-unit project at 7731 and 7771 Alderbridge Way was considered. In regard to that application, the following recommendation was carried by the Panel:

That Onni Contracting Ltd.'s DP 12-615424 (7731 and 7771 Alderbridge Way) be referred back to staff, and brought forward for consideration by the Development Permit Panel at its October 10, 2012 meeting, to be held at 3:30 p.m. in the Council Chambers, City Hall, in order for:

- 1. The applicant to revisit the treatment, in relation to the parkade and other elements, for the Alderbridge Way frontage, and determine whether something more could be done;*
- 2. Staff to prepare more information regarding:*
 - (a) the northwest corner of the subject site; and*
 - (b) the plan and timeline for the City's Parks and Transportation plans within the adjacent Gilbert Road allowance;*
- 3. Staff to examine the effect of the disproportionate 25% reduction in visitor parking on the development; and*
- 4. The applicant and staff to provide more information regarding the interface with the property to the west of the subject site.*

Follow-Up on Recommendation

In addressing the above recommendation, staff have requested further design changes and information from the applicant and further reviewed the above-noted aspects of the development. To assist the DP Panel with this consideration, this Memorandum should be reviewed with the initial Staff Report to the September 26, 2012 DP Panel meeting (which have also been included in the October 10, 2012 meeting agenda). The plans attached to this Memorandum replace and supplement the plans attached the draft permit in the September 26, 2012 Staff Report, and are numbered accordingly.

1. Alderbridge Way, Cedarbridge Way and River Road Frontages

The applicants have worked to change the Alderbridge Way frontage conditions to address concerns from the DP Panel. The applicants have also provided further enlarged sections along Alderbridge Way (Buildings 1 and 4), Cedarbridge Way (Buildings 1 and 2) and also River Road (Buildings 2 and 3) to better illustrate the street edge conditions with the requested changes. Specifically, as requested, the applicants have made design changes to Alderbridge Way and Cedarbridge Way edge conditions for Buildings 1 and 4 (Amended DP Plans 10A, 10B, 11C to 11G, 48 and 51) as follows by:

- Ensuring that all of the patio terraces do not exceed 1.5m (4.9 ft.) above the adjacent public sidewalks along Alderbridge Way. This was achieved, in part, by lowering the slab elevation of the covered bicycle parking at the front edge of the building in relation to the adjacent covered vehicle parking area;
- Reducing the height of the landscape wall adjacent to the Alderbridge Way sidewalk from 0.75m (2.5 ft.) down to 0.45m (1.5 ft.) (seat height);
- Stepping the lower landscape wall in an increased number of places adjacent to Cedarbridge Way that more closely mirroring the change in the elevation of the street;
- Including sloped landscaping between the lower walls adjacent to the sidewalk and the upper walls adjacent to the patio terraces to reduce the apparent height of the higher wall;
- Lowering, pushing back and further angling the corner sections of these retaining walls from the public sidewalk at the intersections of Alderbridge Way with: the North-South Greenway on the western side of the site, Cedarbridge Way at the middle of site and the East Lane;
- Significantly improving the location and the stairs leading from the Alderbridge sidewalk to several of the units.

Also, as requested by staff, the applicants have made design changes to the River Road edge conditions for Buildings 2 and 3 (Amended DP Plans 11A and 11B) as follows by:

- Reducing the height of the landscape wall adjacent to the River Road sidewalk from 0.75m (2.5 ft.) down to 0.50m (1.7 ft.);
- Including sloped landscaping between the lower walls adjacent to the sidewalk and the upper walls adjacent to the patio terraces to reduce the apparent height of the higher wall;
- Pushing back and further angling the corner sections of these retaining walls from the public sidewalk at the intersection of River Road with Cedarbridge Way;
- Improving the location and the stairs leading from the River Road sidewalk to several of the units;
- Confirming that both the patio terraces and adjacent units are located at the same level not more than 1.3m (4.25 ft.) above the public sidewalk.

2 (a) Northwest Corner of the Development Site

The applicant has included better section views that extend from the development site's property line through to the Gilbert Road allowance to the ultimate curb and current edge of pavement of the road (Amended DP Plans 7 and 40). These sections also show the use of brick for the exposed on-site terrace walls adjacent to the Gilbert Road allowance. The applicants have also included a 2.0m (6.6 ft.) interim planting strip within the adjacent Gilbert Road allowance area consisting of

a combination of shrubs and groundcover to screen the approximate 0.6 m (2.0 ft.) to 0.9 m (3.0 ft.) high portion of the parkade wall. This landscaping would be included within the Development Permit landscape security. This planting would be the responsibility of the subject development's owners under Boulevard Maintenance Bylaw 7174. The City would continue to maintain the remaining area of the Gilbert Road allowance due to its unusually large size and prominent location. Under the development's Servicing Agreement, the applicant will be responsible for finished grading and grassing of sections of the Gilbert Road allowance disturbed during their construction of the development and the Gilbert Road improvements.

2(b) Plans for the Gilbert Road Allowance

The development of the greenway on the east side of the very-wide unused Gilbert Road allowance, a prominent gateway to the City Centre, remains to be finalized by the City. Given that there will be approximately 20 m (66 ft.) of open space between the project property line and the future Gilbert Road curb in this high visibility area, a plaza, pedestrian and cycling paths, lighting, significant tree planting are envisioned to be included within this area to be planned and constructed by the City. There is also a major \$350,000 Landmark Public Art piece proposed to be included under the project's Public Art Plan for this area.

Staff have confirmed that there is currently no funding allocation in the 5-Year Capital Plan for Parks development of this area of the Gilbert Road allowance. Staff further discussed the use of the \$100,000 TDM funding received from the applicant allocated for construction of an interim multi-use pathway from Lansdowne Road to River Road within the Gilbert Road allowance. Staff confirm that these funds are sufficient to construct a 3.0m (10 ft.) wide paved bike/pedestrian pathway in this section of the Gilbert Road allowance. Such a pathway would be constructed by the City closer to the Gilbert Road edge, with any remaining funds applied to the construction of an interim mini-plaza at the north terminus of this pathway at the intersection of Gilbert Road and River Road.

3. Visitor Parking Reduction

Transportation staff have provided the following review for the 0.15 visitor stall per unit parking rate. This can be explained by industry experience and various site-specific considerations.

Industry Experience: A comprehensive study on the residential parking supply and demand in strata apartments in the region (Parking Facility Survey and Household Survey) was recently completed by Metro Vancouver. The study surveyed 80 apartment complexes across the region, including nine sites in Richmond. Of the 80 sites surveyed, 64 sites are located within close proximity to TransLink's Frequent Transit Network bus services and/or SkyTrain stations. The nine apartment complexes surveyed in Richmond are all located within the service coverage area of a frequent bus service (e.g. #403 on No. 3 Road) and/or a Canada Line station. The study has a number of key findings regarding residential parking supply and demand. In particular, the study found that visitor parking facilities in the region may be over supplied. Observed visitor parking demand rates were below 0.1 stall per apartment unit, compared to the typical municipal requirement of 0.2 visitor stall per unit.

Site-Specific Considerations: The subject development is located within 800 m (one-half mile) of the Canada Line Lansdowne station and within 400 m to transit services on No. 3 Road and the C-94 community shuttle. The site-specific considerations are given as follows:

- Transit Services: the subject development would be served by several transit routes in close proximity, including the Canada Line, frequent bus services along No. 3 Road and C-94 community shuttles, thereby making transit an attractive travel option.
- Active Transportation: in addition to the on-site greenways (east/west through the site and north/south along the east edge of the site), the development would be connected to major cycling/pedestrian greenway/corridor along Gilbert Road and new River Road in the former CP corridor, thereby promoting walking and cycling as a viable travel option.
- TDM measures: the subject development is providing a \$100,000 contribution, as part of the TDM measures to support the parking rate reduction, to construct a cycling/pedestrian greenway on the eastside of Gilbert Road connecting the development to Lansdowne Road. Other TDM measures include requiring that 20% of the resident parking spaces be pre-wired for electric vehicle charging, that one (1) electric charging outlet be provided for every 40 bicycle parking spaces and a \$25,000 contribution be provided for a bus shelter on River Road.
- Bylaw Requirements: the overall on-site parking supply proposed would still meet the bylaw requirements in terms of overall parking supply (including 10% reduction for the provision of TDM measures), i.e., 1.26 stalls per dwelling unit for both residents and visitors.

This parking reduction was approved by City Transportation and results in having 51 visitor parking spaces located within the parkade on the proposed Lot 1 and 49 visitor spaces located within the parkade on the proposed Lot 2. Visitor parkade intercoms and parkade entrance signage will be provided allowing for visitor parking sharing for both Buildings 1 and 2 on Lot 1 and Buildings 3 and 4 on Lot 2 so each parkade provides the 0.15 visitor spaces per unit.

4. Interface with the Development Site to West

As noted in the initial Development Permit Panel report, there is variance requested to reduce the side yard setback from 1.5m (4.9 ft.) to 0.0m for a partially below-grade parking structure. This condition is largely the result of gradually raising the grade along the North-South greenway along the west side of the site up to the level of the intersecting East-West greenway at the centre of the proposed development site. It is expected that the future development of the site to the west will butt into the parkade and meet the grade of the North-South greenway on the subject site as the second half of the greenway (5.0m) will also be provided on the adjacent property (Amended DP Plan 7A).


The edge on the subject property was to include a lock-block wall covered with parged concrete rising from street level at both the northern (River Road) and southern (Alderbridge Way) to meet the 2.5m (8.2 ft) parkade wall that abuts the centre section of the west property line. To improve this interface, the applicants have replaced the interim lock-block walls at the northern and southern ends of the greenway path with a poured-in-place concrete wall with reveals that will read as part of the adjacent concrete parkade wall.

Staff and the applicant have spoken with the owners of the site to the west. The owners of the site, Richmond Holdings, have confirmed they have recently hired an architect to review the development potential for their property. They now also have confirmed they understand the future plans for their property edge and the subject property and provided written support for the subject application.

Conclusion

In response to the September 26, 2012 DP Panel recommendation, the applicant has made a number of design changes to the orientation of the walls and terraces facing the Alderbridge and Cedarbridge Way frontages as well as the River Road frontage. Also, the replacement of the lock block wall with a poured-in-place concrete wall along the west property line and additional plantings on the Gilbert Road allowance adjacent to the site has improved the interim interface of the development to west.

Given the above, staff recommends that the proposed Development Permit DP 12-615424 be approved for issuance.


Wayne Craig,
Director of Development

WC:mm

Att.



City of
Richmond

Report to Development Permit Panel

Planning and Development Department

To: DPP Mtg. Oct. 10, 2012

To: DPP Mtg. Sept. 26, 2012

Date: September 9, 2012

File: DP 12-615424

To: Development Permit Panel

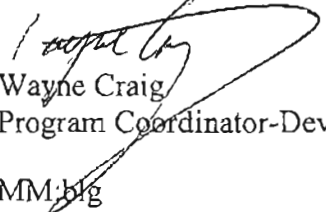
From: Wayne Craig
Program Coordinator-Development

Re: Application by Onni Contracting Ltd. for a Development Permit at
7731 and 7771 Alderbridge Way

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of a 659-unit project in four (4), six-storey wood frame buildings over two (2) concrete parking structures located at 7731 and 7771 Alderbridge Way; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - (a) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures on the proposed Lots 1 and 2 from 3.0 m to 0.0 m respectively along Cedarbridge Way and Gilbert Road;
 - (b) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the west property line of the proposed Lot 1 and the east property line of the proposed Lot 2;
 - (c) Reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15 spaces/dwelling unit for the development as a whole; and
 - (d) Relax the requirement for the provision of on-site loading spaces for two (2) WB-17 loading spaces.


Wayne Craig
Program Coordinator-Development

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Att.

Staff Report

Origin

Onni Contracting Ltd. has applied to the City of Richmond for permission to develop a 659-unit project in four (4), six-storey wood frame buildings over two (2) concrete parking structures. Two (2) buildings will be located on top of one (1) large single storey parkade on proposed Lot 1 (West) and Lot 2 (East) on each side of a newly dedicated section of Cedarbridge Way.

The site is currently under application for rezoning from "Industrial Retail (IR1)" to "High Density Low Rise Apartments (RAH2)" (RZ 11-585209) to facilitate this Development Permit application. The applicable Zoning Amendment Bylaw No. 8884 has received third reading with the applicant currently completing the Rezoning Considerations.

As part of the Rezoning Considerations for the development, a Servicing Agreement is being registered on title that will secure the servicing and road works to be completed. No-build covenants are also required to ensure that the necessary servicing works and streets are constructed at the necessary times prior to issuance of building and occupancy permits for the four (4) proposed buildings.

Development Information

Of the 659 units proposed, Buildings 1 and 2, located on top of a parkade to the west of Cedarbridge Way, contain 340 units. Buildings 3 and 4, located on second parkade on the east of Cedarbridge Way, contain 319 units. Lot 1 is proposed to be developed first under a two (2) phase strata for Buildings 1 and 2. Subsequently, Lot 2 is proposed to be developed under a separate two (2) phase strata for Buildings 3 and 4. Please refer to the attached Development Application Data Sheet (**Attachment 1**) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

The existing two (2) lots within the subject site of 2.87 ha. (7.09 acres) were created in 1969 as part of the Brighthouse Industrial Estate subdivision along Alderbridge Way. Of note, the western lot was the site of the long-standing Stacey's Furniture World and the eastern lot now includes a Tim Horton's amongst numerous other smaller commercial and light industrial tenants.

The existing and proposed development surrounding the subject consolidated site is described as follows:

To the North, lies the former CPR line property which is now owned by the City and will form part of New River Road. Further to the north, one (1) large light industrial building is located on a site zoned as "Industrial Business (IB1)." This site is designated within the CCAP as part of a large future Riverfront Park.

To the South, lies Alderbridge Way with the former Grimm's sausage factory site on the south side of the street. This site is now zoned "Industrial Retail (IR1)" and is the subject of a current rezoning application to rezone the site to a "Residential Limited Commercial (RCL)" zone allow for a higher density, mixed-use development.

To the East, two (2) light industrial / retail buildings are located on site to the east of an adjacent lane and on a site zoned "Industrial Retail (IR1)".

To the West, the Gilbert Road approach to the Dinsmore Bridge forms the north-west boundary of the subject site. The remainder of the site is bounded by the former "V-Tech" building site which is zoned "Industrial Retail (IR1)."

Rezoning and Public Hearing Results

Public Hearings for the rezoning of this site were held on May 22, 2012 and June 18, 2012. At the Public Hearings, the following concerns about rezoning the property were expressed.

There were concerns raised by Tim Horton's Canada (TDL Group Corp.) and local franchise owner, Mr. Mike Raspberry, regarding the applicant's consultation with them on the existing lease for the Tim Horton's located in part of the large existing building at 7771 Alderbridge Way. As result of this concern, the following resolution PH/12 5-6 was made at the May 22, 2012 Public Hearing:

That, in relation to this rezoning, as a further condition of fourth reading of the Bylaw, that any leases registered on title, including the lease in favour of Tim Hortons Canada, would be discharged.

There was also a concern raised by raised in letters from by Ebco Industries Ltd. regarding view corridors mentioned in the Rezoning Staff Report on the project. Staff explained that this report text concerned the context site and potential views towards the north and was not granting any view corridors over the Ebco property. A letter was also sent to Ebco Industries in follow-up.

Following the May 22, 2012 Public Hearing, new information was brought forward by the applicant to staff and Council leading to the above resolution PH/12 5-6 being rescinded and with a second Public Hearing being held and Third Reading being given on June 18, 2012. At this second hearing, similar concerns were raised by TDL Group Corp. and Mike Raspberry and Ebco Industries. A representative from the Jones New York store at 7771 Alderbridge Way also noted they had not been consulted by the applicant. While the tenant concerns are private property matters, staff and Council urged the applicant to communicate with its tenants concerning the future plans for buildings on the project site.

After the second Public Hearing, Onni sent a letter to all of its tenants updating them on the development process, advising that 7731 Alderbridge Way (proposed Buildings 1 and 2 on Lot 1) is scheduled to be re-developed first. The letter also stated that re-development of 7771 Alderbridge Way (proposed Buildings 3 and 4 on Lot 2) was contingent on the rate of market absorption of the first two buildings.

Staff Comments

The proposed development scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, the proposal complies with the intent of the applicable sections of the Official Community Plan (OCP) and the CCAP which designates the site as “Urban Centre (25 m)” and includes the Sub-Area B.2 Mixed-Use Mid-Rise Residential and Limited Commercial Development Permit guidelines.

The proposed application is in compliance with the “High Density Low Rise Apartments (RAH2)” zone of City of Richmond Zoning Bylaw except for the zoning variances noted below.

Zoning Compliance/Variances (staff comments in **bold**)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures on the proposed Lots 1 and 2 from 3.0 m to 0.0 m respectively along Cedarbridge Way and Gilbert Road.

(Staff supports the proposed variance for the parkade setbacks on Lots 1 and 2 to the new Cedarbridge Way as the sections of parkade walls located at 0.0 m. are largely located below the Cedarbridge Way street grade where the east-west greenway crosses the street. Short sections of these parkade walls rise up to approximately 0.75 m (2.5 ft.) to meet adjacent terraced planter (see Architectural plan A0.7 and Landscape Plan L3.1).

Similar adjacent terraced planter walls are located between Building 2 and the Gilbert Road allowance property line on the proposed Lot 1 (see Architectural plans A3.04 and A3.06). There will be approximately 20 m (66 ft.) of slightly lower open space between the project property line and the future curb line within the Gilbert Road allowance. As discussed below in this report, the City will be constructing a plaza, pedestrian and cycling paths and locating a major public art work adjacent to this property line within the Gilbert Road allowance. This area will be raised to the level of Gilbert and River Roads which at a similar level to the base of the project's landscape walls which are located in front of the parkade.

On both Cedarbridge Way and Gilbert Road, lowered parkade ceilings over bike parking and the front vehicle parking have been used to assist in accommodating these planters and hiding the parkade structures.)

- 2) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the west property line of the proposed Lot 1 and the east property line of the proposed Lot 2;

(Staff supports the proposed variance for the parkade wall and interim retaining wall under Buildings 1 and 2 on the west side of the proposed Lot 1. The proposed retaining wall and parkade wall range from 0.0 m to approximately 2.5 m (8.2 ft.) in height above grade see Architectural plans A3.04 and A3.06).

This condition is largely the result of gradually raising the grade along the North-South greenway along the west side of the site up to the level of the intersecting East-West greenway at the centre of the proposed development site. Currently, there is an approximately 6.0 m (20 ft.) high concrete building located on the subject site with no setback to west property line. This building will be removed when the subject development commences. It is expected that the future development of the site to the west will butt into the parkade and meet the grade of the North-South greenway on the subject site as the greenway will also be provided on the adjacent property.)

(The second variance is for the parkade wall under Buildings 3 and 4 on the east side of the proposed Lot 2 adjacent to a 6.0m (20.0 ft.) lane. The wall ranges between approximately 2.0 m (8.2 ft.) to 3.0 m (9.8 ft.) in height above grade of the adjacent east lane. This condition is an interim condition as a future pedestrian path will be constructed within the east lane at a higher grade rising up to the level of the intersecting East-West greenway. An ultimate grading plan is included within the Development Permit plans that address this future raised pathway with planter walls not exceeding 1.2 m (4.0 ft.) against the final pedestrian pathway elevation (see Architectural plans A3.04 and A3.05). Staff have also discussed this proposed change of grade with the owners of the future development site to the east who will be required to construct the pathway over the current lane alignment as discussed above.)

- 3) Reduce the required visitor parking from 0.20 spaces /dwelling unit to 0.15 spaces /dwelling unit for the development as a whole.

(Staff supports this visitor parking reduction beyond the maximum 10% TDM reduction of 10% to 0.18 spaces/dwelling unit set out under the zoning bylaw. Of note, the TDM Rezoning Considerations allowed for an overall 7.5% reduction in total parking requirements (resident and visitor parking combined) which would allow for flexibility in allocating the 7.5% parking reduction between the resident and visitor parking. As the visitor parking is proposed to be reduced by 25% under the Development Permit (more than the 10% TDM maximum), a formal variance is required. The applicant has agreed to interconnect the visitor parkade intercoms and include parkade entrance signage allowing for visitor parking sharing for Buildings 1 / 2 and Buildings 3 / 4 so each parkade provides the 0.15 visitor spaces for the two buildings located above. This parking reduction was approved by City Transportation and results in having 51 visitor parking spaces located within the parkade on the proposed Lot 1 and 49 visitor spaces located within the parkade on the proposed Lot 2. The TDM package is discussed below in this report.)

- 4) To relax the requirement to provide two (2) large WB-17 loading spaces for the site.

(Staff supports the proposed variance as there will be four (4) medium SU9 loading spaces within the development site with one (1) space being located next to each building's lobby entrance off of Cedarbridge Way. While most moving and delivery trucks will be accommodated by the SU9 spaces on-site, City Transportation has also provided that they will offer temporary on-street loading permits near the building lobbies upon-request on occasion when very large semi-trailers need to load.

Advisory Design Panel Comments

The Advisory Design Panel reviewed the application on July 18, 2012. A copy of the relevant excerpt from the Advisory Design Panel Minutes is attached for reference (**Attachment 2**). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '*bold italics*'.

Analysis

Conditions of Adjacency

The developer will construct an entirely new River Road from Gilbert Road to the east side of the subject development site where the former CPR line is now located. This section will include two (2) eastbound and two (2) westbound lanes along with an adjacent bi-direction separate bike path and sidewalk immediately to the north of the development.

On the north side of the new River Road, the site containing existing light industrial buildings is designated for the new Riverfront Park in the CCAP. Upon creation of this park, the development will be afforded views of the park, and better views of the Middle Arm of the Fraser River and North Shore mountains in the distance.

There will be widening of the Alderbridge Way to the south, with new sidewalks and treed boulevards being constructed as part of the subject development.

To the west, the existing "V-Tech" building will remain until sometime in the future when the site is redeveloped. As a Greenway, connecting Alderbridge Way with Gilbert Road, is designated in the CCAP along the west property line of the site, the west-half of this greenway of at least 5.0 m (16.5 ft.) will be landscaped and included within a Statutory Right of Way (SROW) to complement the half width of the north-south greenway now being provided on the subject development. To the northwest, the applicant is required to construct the full curb to curb widening of Gilbert Road northward of approximately where this greenway intersects Gilbert Road.

To provide for an additional pedestrian pathway linking Alderbridge Way with New River Road within the wide block between Cedarbridge Way and Minoru Boulevard, Planning, Transportation, Parks and Engineering staff have agreed to close the northerly three-quarters of the lane immediately to the east of the site. With the current application, there will be reconstruction of the southern part of the current lane to the east along with registration of SROW for a sidewalk. The remaining northerly two-thirds of the lane will remain in its current state for the time being. At the time of redevelopment of the current warehouse building immediately to the east, this remaining section will be closed and re-grade and turned into another pedestrian link which will intersect the major East-West greenway crossing the subject and adjacent site. Lastly, Cedarbridge Way will be constructed as part of this application through the development site from Alderbridge Way to New River Road.

Urban Design and Site Planning

The proposed development is composed of a family of four (4) buildings centered on the extension of Cedarbridge Way from Alderbridge Way to the new River Road extension.

Buildings' Orientation: The four (4) buildings have a similar U-shaped building form three (3) sides around the garden courtyards for each building that rises between four (4) to six (6) storeys

above street grade. Differentiation amongst the buildings has been achieved by mainly varying the orientation of the “U” shaped buildings and courtyard openings while differentiating the materials and small-scale facade articulation between Buildings 1 and 4 facing Alderbridge Way and Buildings 2 and 3 facing the New River Road.

View Corridors: View corridors are particularly important due to the proposed riverfront park being developed immediately to the north, and the distant mountain views to the north over the existing warehouses which occupy the site of the future Riverfront Park. The spacing between the buildings across Cedarbridge Way allows for good view corridors north-south and sunlight penetration.

East-West Greenway: The CCAP’s envisioned east-west greenway connects the Oval Village local commercial and major recreational destinations to the Aberdeen Village Commercial and Arts District. The applicant has addressed these components to the satisfaction of Planning, Transportation and Parks Department staff.

The separation between the buildings averages approximately 22 m (72.5 ft.) along the greenway, leaving sufficient area for ground floor patios and common strata property on each side. The greenway will include a 3.5 m (11.5 ft.) wide hard-surfaced public path that extends from the east to the west boundaries of the development, crossing Cedarbridge Way in the centre of the development site. The greenway also includes landscaping on both sides of the hard-surface pathway.

North-South Greenway: This greenway, extending from north to south on the west side of the proposed west Lot 1, will include a 3.5 m (11.5 ft.) hard-surfaced public path which intersects the west end of the east-west greenway. The greenway also includes landscaping on the east of the hard surface path within the 5.0m (16.5 ft.) SROW for east half of the greenway being constructed for the subject development. The future development of the “V-Tech” site to the west will be required to provide the remaining landscaping within a further a 5.0m (16.5 ft.) SROW, thus completing the west side of the north-south greenway.

Gilbert Road Boulevard and Greenway: The development of the greenway on the east side of the very wide unused Gilbert Road allowance, a prominent gateway to the City Centre, remains to be finalized. Given that there will be approximately 20 m (66 ft.) of open space between the project property line and the future Gilbert Road curb in this high visibility area, a plaza, pedestrian and cycling paths, lighting, significant tree planting and a major \$350,000 Landmark Public Art piece is included under the project’s Public Art Plan.

Parking and Transportation Demand Management (TDM): The proposed project includes a total of 871 parking spaces with 462 spaces (including 51 visitor spaces) in the parkade on Lot 1 for Buildings 1 and 2, and 409 spaces (including 49 visitor spaces) within the parkade on Lot 2 for Buildings 3 and 4 (See Architectural plans for full parking statistics). While there are 192 proposed tandem parking spaces, there will be a covenant register on title ensuring that each pair of tandem parking spaces is assigned to the same dwelling unit.

While there is one (1) parkade under Buildings 1 / 2 on Lot 1 and one (1) parkade under Buildings 3 / 4 on Lot 2, each building has its own parkade entrance and adjacent partitioned visitor parking area. Buildings 1 and 2 on Lot 2 have parkade entrances from the west side of Cedarbridge Way. The parkade for Building 3 is located on the east side of Cedarbridge Way while the entrance to the Building 4 parkade is from the east lane near Cedarbridge Way.

The parking includes a combined visitor and resident parking reduction of 7.5% below the parking requirements set out in Zoning Bylaw 8500 subject to the provision of TDM measures included in the Rezoning Considerations (see attached Development Reference Plans). This combined parking TDM reduction includes visitor parking being reduced by 25% from 0.20 to 0.15 spaces/unit on each of Lot 1 (Buildings 1 & 2) and Lot 2 (Buildings 3 & 4) as discussed in the variance section above.

In lieu of this reduction, the City accepts the Developer's offer to voluntarily:

- Contribute \$100,000 to the City for the construction of a 3.0 m (9.8 ft.) bike/pedestrian pathway along the east side of Gilbert Road from the southern end of the applicant's required frontage improvements to Lansdowne Road. (Not eligible for DCC credits.)
- Contribute \$25,000 to the City for a City Centre-type bus shelter. (Not eligible for DCC credits.)
- Enter into an agreement with the City to ensure that the electrical vehicle and bicycle plug-ins be provided as a condition of issuance of the City building and occupancy permits for each building. This will include provision of 20% of the total resident parking spaces in each parkade with electric service for vehicle plug-ins with conduits, circuits breakers and wiring (actual outlets to be provided later by strata owners). Provision of one (1) standard 120 volt electric plug-in for every forty (40) resident bicycle parking spaces.

It should be noted that there will be also on-street parking provided on Cedarbridge Way throughout the day and off-peak on-street parking on Alderbridge Way and River Road over the short to intermediate term.

Bicycle Parking: The proposed project includes a total of 860 resident bicycle parking spaces with 424 resident spaces in the parkade and 68 surface visitor spaces for Buildings 1 and 2; and 436 resident spaces within the parkade and 64 surface visitor spaces for Buildings 3 and 4. The resident and visitor bicycle parking provided meets the minimum requirements of Zoning Bylaw 8500.

Loading Space Requirements: Section 7.13 of Zoning Bylaw 8500 requires that one medium-sized (1) SU9 off-street loading space be provided for each building and one large (1) off-street WB 17 (17 m trucks) loading space be provided for every two (2) buildings. The applicant has accommodated the four (4) required SU9 loading spaces on either side of the East-West greenway junction with Cedarbridge Way. Given the low frequency of use of such large WB17 trucks in a purely residential project, staff supports a variance to relax this requirement as discussed above.

Recycling and Garbage: Each parkade services a pair of buildings and includes one (1) large recycling / garbage room and garbage compactor meeting the size and access requirements of the City's Sanitation Division. However, given the long distance between these rooms and loading bays where recycling trucks will pick-up and given there are over 300 units serviced by each room, the Sanitation Division is requesting that the applicant provide suitable golf carts for recycling movement for each building. The applicant is required to post a security with the City for the provision of these golf carts prior to Development Permit issuance.

Architectural Form and Character

Key Street Walls: This development expresses itself with continuous street walls on Alderbridge Way and New River Road which are two (2) of the major curvilinear streets in the City Centre. The development's units face streets with stairs and entrance doors with design techniques to deliver a townhouse character. As well, the use of stepped patio and landscaped terraces reduce the appearance of the grade difference.

Street Frontage Elevations: The current Alderbridge Way elevation is lower at 1.5 m (4.9 ft.) compared to the New River Road which is located at 2.6 m (8.6 ft.) geodetic elevation. This elevation difference results in a 2.5 m (8.2 ft.) grade difference between Alderbridge Way and the first floor of the adjoining units. To address this grade difference, the patios have been located 1.5 m (4.9 ft.) above street level instead of at the unit ground floor level of 2.5 m (8.2 ft.) above the street. To achieve this grade transition, the stairs have been broken into two (2) sections. Also, cladding materials have been extended over the upper part of the parkade that lies between the unit's floors and the patios, further reducing the appearance of the grade different between street and unit levels. Terraced planters have been located between the unit patios and street level.

The grade difference of approximately 1.4 m (4.6 ft.) on New River Road and Gilbert Road presents far less of a challenge and has been well addressed through the use of terraced planters located between the unit patios and street level. Lastly, Cedarbridge Way begins at the lower Alderbridge Way grade rising to being level with the ground floor of the buildings and the East-West greenway and then falling again to the River Road grade.

Individual tower-like elements have been included at corners of all four (4) buildings to anchor the buildings at the intersections of Cedarbridge Way with Alderbridge Way and the east-west greenway. Also, a prominent tower element has been included at the north-west corner of Building 2 given the prominent view of this building from the adjacent Gilbert and River Roads.

Building Height and Rooflines: Each of the four (4) buildings rises to six (6) storeys in height. The buildings include terraces downward to as low as four (4) storeys to provide for a variety of building form and larger decks for some of the units on the top two (2) floors of each building. The use of inverse gable or butterfly roofs and higher ceilings for the sixth floor in each building provides variety to building masses while also maintaining continuity within the family of buildings in the proposed development.

Building Materials and Articulation: While the buildings are similar, the use of varied architectural building frames, balcony forms, materials, and wall articulation have been utilized to provide for further differentiation between the buildings. Facing Alderbridge Way, Building 1 has a fibre-cement cladding materials with darker brown and grey colours while Building 4 includes both fibre-cement metal panelling with generally brighter colours, including orange.

Facing River Road, Buildings 2 and 3 are designed with more of a mid-20th modernist style with bolder frames, more simple articulation and use of lighter coloured fibre panelling that appears to read as stone in Building 2 and read as concrete in Building 3.

In summary, the massing, articulation, and materials give these wood-frame buildings the appearance of concrete buildings with varied characters.

On-Site Landscape and Amenity Space Design

On-Site Outdoor Amenity: As noted above, the “U” shape buildings provide for large semi-private courtyards while maintaining highly visible smaller water features which provide variety to gardens and create attractive transitions from the semi-private courtyards to adjacent public streets. The typical width of the courtyards from building face to building face is approximately 35 m (115 ft.) which provides privacy and ample room for on-site outdoor amenities and patios for each ground floor unit.

The applicant has responded to staff’s concern about having enlarged play areas included within the courtyards of Buildings 1, 2 and 4 on either side of Cedarbridge Way. Multi-purpose amenity/BBQ areas are provided for the Buildings 1 and 2 courtyards while community garden plots are provided in the Building 2, 3 and 4 courtyards.

The OCP includes on-site open space guidelines for active uses including socializing, children’s play and related use. The additional CCAP guidelines provide for on-site walkways, planting, garden plots, etc. The development includes approximately 5,176 m² (55,714 ft.²) of such on-site socializing areas.

Common Indoor Amenity: A shared indoor amenity space of approximately 951 m² (10,235 ft²) within Building 1, which includes an indoor swimming pool, will open up on a large common courtyard patio. There will also be small meeting rooms of approximately 21m² (230 ft²) in each of Buildings 3 and 4. There will be registration of a reciprocal access easement on the proposed Lots 1 and 2 to ensure that the large proposed indoor recreation space in Building 1 is accessible to all building residents and is constructed in the first phase.

Private Outdoor Amenity: As provided under the OCP DPA guidelines on the minimum private amenity space requirements, the architect has confirmed that each ground level unit has been provided with at least 24 m² (258 ft.²) of patio space and each upper floor unit has been provided with at least 6 m² (65 ft.²) of deck space.

Trees: Of note, while there are no trees on the subject site, staff have requested and reviewed an arborist’s report confirming that the proposed buildings and north-south greenway with a retaining wall (discussed earlier in the report) will not adversely affect several significant trees on the adjacent property to the west.

Summary: In summary, staff feels that the applicant has gone a long way to developing a wood-frame project that has the modern, urban character desired for the City Centre and which responds well to the CCAP’s DPA design guidelines. Particularly, the applicant has responded well to staff and the ADP recommendations to apply high quality, durable materials and undertake modifications to the detailed design of the buildings.

Crime Prevention Through Environmental Design

Casual surveillance of the public streets is provided by the ground-orientated units with individual patios between 1.2 m (4.0 ft.) and 1.5 m (5.0 ft.) above the public sidewalks and front doors facing the streets. The average 22.0 m (72.5 ft.) spacing between buildings along the east-west greenway provides for sufficient building separation, but also provides for surveillance from the units on the first four (4) floors of the adjacent buildings which are placed at an average setback of 6.0 m (20 ft.) from each side of the 10 m (33 ft.) greenway right-of-way.

Proposed street lighting and on-site lighting fixtures provide for safe pedestrian circulation from streets and along both of the east-west and north-south greenways. The City Centre's modern "Type 8" lamp standards with "Hestia" style light fixtures are provided on both the greenways and streets. These fixture are outfitted to provide for white light and in such a way to prevent ambient light pollution that conflicts with neighbouring apartment units.

Special attention has been given to open-up the greenway intersections where they intersect with public streets. In particular, the intersection of the east-west greenway with Cedarbridge Way has been opened up to create mini-plazas on each side of Cedarbridge Way by shifting planters and loading spaces away from the greenway. These mini plazas include the four (4) SU9 loading which can read and appear as part of the mini plazas when not occupied by loading trucks.

Public streets and greenways and the semi-public spaces (building courtyards) have been well defined and highly visible from the private patios and decks. Symbolic barriers have been incorporated through changes in vegetation, grade and landscape features. (e.g. low walls, fences and trellises).

Accessibility

The proposed 528 Basic Universal Accessible Housing units are each designated throughout the buildings plans on the attached Development Permit architectural plans which confirm the units meeting all of the accessibility requirements under Section 4.16 of Zoning Bylaw 8500. Compliance with the design requirements will be re-confirmed through the Building Permit process.

Affordable Housing

Following the City's Affordable Housing Policy, the applicant will be providing 40 affordable housing (low-end market rental) to the satisfaction of the City. The affordable housing units will have a total combined habitable area of at least 5% of the residential floor area ratio (FAR) permitted (not including hallways and common areas) with a minimum 30,931 ft.² combined habitable area to be provided in the assigned buildings planned to be developed in Buildings 1, 3 and 4 as follows:

Location	1 Bedroom	2 Bedroom	Total
Building 1	4	5	9
Building 3	0	8	8
Building 4	8	15	23
Overall Total	12	28	40

The affordable housing units are located on in Buildings 1, 3 and 4 and are designated on the attached Development Permit Architectural Plans which is included under Affordable Housing Bylaw 8936 considered by Planning Committee on September 5, 2012.

Noise Related Aspects of the Proposal

A restrictive covenant is being registered requiring that prior to the Development Permit being issued, the applicant is required to provide reports by a registered professional qualified in acoustics identifying the measures needed to satisfy the “Noise Management” standards set out in the OCP. As the site is temporarily situated in a transitional industrial area and may be subject to noises not typical of other residential neighbourhoods, a covenant was registered on title during the rezoning phase, on this regard.

Public Art

The City has accepted the applicant’s offer to voluntarily provide \$440,411 to Richmond’s Public Art Program. This amount includes a cash contribution of \$139,700 to be provided to the City’s Public Art Reserve Fund for a Landmark Art piece and providing a security to the City for \$300,711 for other Public Art with locations generally shown on sheet L.1.8 of the landscape plans. The applicant has submitted a detailed Public Art Plan which will be reviewed and need to be accepted by Planning and Public Art staff.

District Energy Utility (DEU)

The applicant has agreed to commit to connecting to the proposed City Centre DEU. The DEU terms will be finalized as a Rezoning Consideration include:

- Design and construction of the development’s buildings to facilitate hook-up to a DEU system (e.g., hydronic water-based heating system); and
- Entering into a Service Provision Agreement(s) and statutory right-of-way(s) and/or alternative legal agreements, to the satisfaction of the City.

Sustainability

The applicant has committed to meet the Canadian Green Building Council LEED Silver 2009 criteria . In this regard, Stantec, providing LEED consulting services for the project, has provided the LEED Scorecard confirming how the project meets LEED Silver criteria. A letter of assurance will be required prior to issuance of an occupancy permit for each building confirming meeting the LEED Silver score. The proposed project includes at least 50 points with up to a possible 54 points within the LEED Silver category of 50 to 59 points.

The CCAP requires two LEED criteria to be met, including *Heat Island Effect: Roof Credit* and *Storm Water Management Credit* which have both been met. Other sustainability measures, contained within the development include:

- Permeable pavers have been applied in pedestrian pathways areas to allow for storm water infiltration, and less storm runoff;
- Water efficient landscaping with planting is drought tolerant and many are native species, reducing water use by 50%;

- Domestic water use reduction, including use of low flow fixtures will be specified to conserve water;
- Optimize energy performance with a 25% to 56% energy cost savings;
- Construction waste management, diverting 50% to 75% of waste to recycling; and
- Use of low-emitting interior finishing materials such as adhesives, sealants, paints and carpets.

Conclusions

Issues identified during the Development Permit Application design review process have been considered and addressed, including staff and Advisory Design Panel comments regarding adjacencies and landscape design. In summary, the development:

- Forms a distinctive, modern, wood-frame, high-quality, high-density yet low-rise part of the Lansdowne Village neighbourhood;
- Completes important sections of the major road network in the CCAP including New River Road east of Gilbert Road and the extension of Cedarbridge Way to New River Road; and
- Provides the first significant section of the major east-west and north-south greenways that will connect Lansdowne Village to the rest of the City Centre.

Therefore, staff recommends support for this Development Permit Application.



Mark McMullen
Senior Coordinator-Major Projects
(604-276-4173)

MM:blg

Attachment 1: Development Application Data Sheet

Attachment 2: Excerpt of Minutes from July 18, 2012 Meeting of the Advisory Design Panel

The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter-of-Credit for landscaping in the amount of \$826,437 based on an estimate provided by the landscape architect.
- Receipt of security for one (1) recycling golf cart for each of the two (2) parkade recycling/garbage rooms in an amount to secure purchase of such vehicles to the satisfaction of the City.

- The applicant is required to retain a registered professional who is qualified in the design and installation of an air condition, or an alternative cooling system that meets the ASHRAE 55-2004 standard, or subsequent updates as they may occur. Further, the retained registered professional is to certify that any required noise insulation measures have been installed according to the report's recommendations before the building may obtain an Occupancy Permit. Maximum noise levels (decibels) within the dwelling units must be as follows:

Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

Prior to future Building Permit issuance, the developer is required to complete the following:

- Address possible changes to the width, material and elevation of the greenway crosswalk at Cedarbridge Way under the Servicing Agreement to the satisfaction of the City's Planning, Transportation and Parks Departments.
- Incorporation of the above-noted acoustical report's recommendations into the BP drawings.
- Confirmation of accessibility measures under Section 4.16 of Zoning Bylaw 8500 being incorporated into BP drawings for the designated 528 Universal Accessible Units.
- Obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/ttp/special.htm>).

Attachment 1



City of Richmond

6911 No. 3 Road
Richmond, BC, V6Y 2C1
www.richmond.ca

Development Application Data Sheet

Development Applications Division

RZ 11-585209

Address: 7731 & 7771 Alderbridge Way

Applicant/Owner: Onni Contracting Ltd.

Owner: Onni 7731 Alderbridge Way Holding Corp. & 7771 Alderbridge Way Holding Corp.

Planning Area(s): City Centre Area (Lansdowne Village)

Floor Area: No change is proposed in maximum permitted floor area or density

	Bylaw Requirement	Proposed Development	Variance
Lot Size (Min.)	<ul style="list-style-type: none"> 2400 m² (25,833 ft²) 	<ul style="list-style-type: none"> Lot 1: 13,288.40 m² (143,040 ft²) Lot 2: 11,885.75 m² (127,937 ft²) 	<ul style="list-style-type: none"> None
Lot Coverage (Max.)	<ul style="list-style-type: none"> 60% for buildings 80% for building and non porous surfaces 	<ul style="list-style-type: none"> Lot 1: building footprint: 45% non-porous surfaces: 69.5% Lot 2: building footprint: 45% non-porous surface: 70.3% 	<ul style="list-style-type: none"> None
FAR	<ul style="list-style-type: none"> 1.2, up to 2.0 FAR of the total site with provision of 5% of total floor area for affordable housing units. 	<ul style="list-style-type: none"> 2.0 FAR of the total site with 20 m Cedarbridge dedication and 5% affordable housing as per the RAH2 zone and Zoning Amendment Bylaw No. 8884 text not deducted. 	<ul style="list-style-type: none"> None
Habitable Floor Elevation (Min.)	<ul style="list-style-type: none"> Residential: 2.9 m geodetic 	<ul style="list-style-type: none"> Residential: 4.064 m geodetic Local exception permitted for one (1) lobby per building. 	<ul style="list-style-type: none"> None
Height (Max.)	<ul style="list-style-type: none"> 25 m, but with specific areas allowing up to 35 m as outlined in CCAP. 	<ul style="list-style-type: none"> Varies, but less than 25 m above finished grade in all cases. 	<ul style="list-style-type: none"> None

	Bylaw Requirement	Proposed Development	Variance
Setbacks (Min.)	a) 3.0m @ Alderbridge P/L b) 1.5m @ East Lane P/L c) 3.0m @ River Road P/L d) 3.0m @ Cedarbridge P/L e) 3.0m @ Gilbert P/L f) 1.5m @ West Side P/L	a) 4.8 m for Building 1 and 5.3 m Building 4 @ Alderbridge from PROP b) 0.0m @ East Lane P/L for parkade c) 3.0m @ River Road P/L d) 0.0m @ Cedarbridge Way P/L for parkade e) 0.0m @ Gilbert Road P/L f) 0.0m @ West Side P/L for parkade Based on setback to PROP/SROW on Alderbridge Way; setbacks from the actual property lines are greater.	<ul style="list-style-type: none"> DVP for interior side west P/L on Lot 1 and east P/L on Lot 2 on from 1.5m to 0.0 m; exterior side for Gilbert Road for Lot 1 and exterior side for Cedarbridge Way for Lots 1 and 2
Off-Street Parking	<u>Lot 1: Parkade (Bldgs1/2): 475</u> Min Residents/affordable: 407 (max small car: 50%) Min Visitors: 0.2/unit: 68 (With maximum 7.5% TDM overall reduction) <u>Lot 2 Parkade: (Bldgs 3/4): 438</u> Min Residents/affordable: 374 (max small car 50%:) Min Visitors: 0.2/unit: 64 <i>(Before maximum 7.5% TDM overall reduction and 25% visitor reduction to 0.15 spaces/unit)</i>	<u>Lot 1: Parkade (Bldgs1/2): 462</u> Residents/Affordable: 411 (small car: 49%) Visitors: 51 <u>Lot 2: Parkade (Bldgs3/4): 409</u> Residents/Affordable: 360 (small car: 50%) Visitors: 49 <i>(With maximum 7.5% TDM overall reduction and 25% visitor reduction to 0.15 spaces/unit)</i>	DVP to reduce visitor parking to 0.15 spaces/dwelling unit.
Bicycle Parking	<u>Lot 1: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 425 Visitor (0.2/unit): 68 <u>Lot 2: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 425 Visitor (0.2/unit): 68 	<u>Lot 1: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 399 Visitor (0.2/unit): 68 <u>Lot 2: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 436 Visitor (0.2/unit): 64 	<ul style="list-style-type: none"> None
Loading	<ul style="list-style-type: none"> 2 medium; 2 large with one being provided for each building with sizes as per Section 7.10.2. To be on-site. 	<ul style="list-style-type: none"> Required 4 SU9 loading spaces with 1 provided for each of the four buildings. 	<ul style="list-style-type: none"> DVP to relax the requirement for 2 WB 17 spaces required.

Attachment 2

Excerpt from the Minutes from The Design Panel Meeting

Wednesday, July 18, 2012 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall

Panel Discussion

Comments from the Panel were as follows:

- like the roof design, i.e. the butterfly roofs with four different forms and facing different directions; appealing feel for the neighbouring towers and from River Road;

Butterfly roofs are a distinguishing characteristic of the project and serve to unify all four buildings.

- four forms are similar in height and mass and a lot of detail has been introduced to differentiate the four forms; differences are not just in colour and materials but also in forms and details; four forms have subtle differences; applicant has addressed a previous comment of the Panel to differentiate the building in terms of colour and texture;

We have worked with the Staff and the preliminary ADP comments to further differentiate the four buildings in terms of massing and material treatment.

- concern on heat gain on metal panel system and the use of dark colours in Building 4; however, appreciate the right balance in the use of bright colours in small and different areas in comparison to the two field colours;

Metal panels will be properly detailed to ensure heat gain is minimized and ventilation is provided behind panels.

- model presented by the applicant should have been a coloured model; use of materials should have been shown in the model to show the significant and minute details as it would be difficult to see them in the elevations and renderings;

3D views were supplied to suggest materiality of each of the buildings.

- there are a lot of community gardens in the project; like its use in the project but not in the smaller courtyards, i.e. in Buildings 2, 3 and 4; garden plots will diminish the usable space for residents due to their limited use during certain seasons of the year, e.g. during winter when they are mostly dormant and visually unappealing; consider scaling back the community gardens in the courtyards;

Community gardens have been slightly scaled back to make room for other amenities in two of the courtyards. Community gardens can be made to stay more attractive through strata regulations and the use of a management company.

- concern on the central intersection between the roadway and greenway due to truck loading; consider having more intricate pattern for the pavers, e.g. by using two different materials and textures;

Landscape paving patterns have been updated to provide more clear distinction between the different areas. Planting has also been used to establish separation.

- overall, support the project;

Noted

- hope to see aging in place features in the project; consider sliding door access to ensuite washrooms;

Aging in place features will be incorporated at BP stage including; blocking in bathrooms for grab bars, lever handles, pocket doors for washrooms.

- interesting model for the neighbourhood; building is interstitial type; inverted roof is interesting;

We feel that this project provides an alternate form and variety of housing typology to the tower and townhouse developments occurring in the area.

- agree with comment regarding the preponderance of plot gardens in the courtyards;

As mentioned above, community gardens have been reduced and other amenities expanded.

- no major concern with the courtyards; they are distinct from each other and have a lot of programmatic elements that make them useful within a small space; nice hierarchy of spaces in the courtyards;

Thank you

- there is opportunity to create a public realm in the center court that is significant and pedestrian-focused; however, location of loading presents a challenge to this opportunity; need to work with Transportation to figure out how to integrate loading at the center court in a more sensitive way; consider more detail in the crossroads to privilege the walking traffic, e.g. add bollards and designate loading on the street as loading at the center court is a suburban model for loading and not an urban model;

The centre court has been refined to help create good public spaces and incorporate loading. Loading bays have been located closer to the buildings to allow for a wider centre court. Bollards have been added to define the loading spaces.

- reconsider building garden plots as the greenway ROW feels domestic and tends to privatize the place; 4 x 8 ft. timber grids don't enhance visual appeal; consider other ways to achieve urban agriculture that are more civic, e.g. street trees bearing fruit, grape/kiwi vine on a trellis, etc.; need not depend on garden plots to animate the space; greenway should become a really critical walkway link in the emerging neighbourhood;

Greenway garden plots have been removed from the project.

- applicant has not provided information/vision regarding how the western terminus of the greenway might integrate with the properties to the west; similar suggestions for east lane integration; information was requested by the Panel during the rezoning stage; applicant is encouraged to suggest some ideas;

The West end of the greenway is designed to end at the intersection of the North-South greenway per City direction. The East end will terminate at the City East lane as a temporary solution. In the future the East lane will be converted to a greenway by others, which will include raising the elevation of the future greenway to meet the grade of the East-West greenway on the Onni development. This will allow the East West greenway to be extended eastward across neighbouring sites to the east as planned for in the CCAP.

- sections on Alderbridge Way are fairly well resolved; applicant has adequately handled a difficult scenario, i.e. a 2-meter grade change;

We have worked closely with staff to create a pedestrian friendly sidewalk along Alderbridge. This has been achieved by; setting back buildings 1 and 4 from Alderbridge further than the setback allows, providing stepped planters along the front of the parkade, and dropping the front patios of the Alderbridge fronting buildings down from 4.0m to 3.0m

- the project suffers from the model presented to the Panel; the model does not resemble the quality of its design; model should show colours and textures;

3D views were supplied to suggest materiality of each of the buildings. In future, a coloured model will be provided.

- each building works well as standalone but not together as one development; buildings are object buildings, i.e. unique and specialized buildings which stand out differently from each other;

Buildings are differentiated to address site specific conditions, including prominent corners at the northwest, and at the new intersections of Cedarbridge with Alderbridge and River Road. Per the preliminary ADP and previous direction from Staff, we have endeavoured to create more differentiation between the building material and massing. However, there are unifying elements that work to tie all four buildings together as one "precinct", including; butterfly roofs, corner tower element massing at the center of the site, cedarbridge and greenway lobby treatments, material treatment and massing of upper floors, and consistent landscape features.

- buildings are too similar, both in the use of materials and articulation; too much "sameness" in the project; elements are the same but articulated a little bit differently; not enough difference in mass, colour and texture;

See opposing comment above. We have introduced more differentiation between materiality and massing of each building. Since the formal ADP, we have created more distinct volumes within each of the four buildings. This serves to break down the lengths of the buildings into smaller volumes, and create more diverse compositions within each of the buildings.

- the use of the word "monolithic" is not appropriate to describe the buildings which are actually pedestrian friendly and have a residential character;

The word "monolithic" was incorrectly used to describe the heavier base of the buildings relative to the cleaner, lighter top floors.

- concrete look of wood-frame buildings would be difficult to achieve; flashing details would be prevalent; rooftop cantilevers don't work; eyebrow detail will require parapet and thus a deeper profile; waterproofing does not have termination;

Details were developed and presented at ADP that illustrate how we are achieving the appearance of the buildings. Flashing details will be minimized and integrated within the pattern of material panels. Eyebrow details are shown with flashing in places and parapets in other locations where the deeper profile is aesthetically warranted.

- massing of the building needs to be broken down; massing needs relief at certain points on the site; there is need for certain elements to become expressed; The Gardens by Townline is a precedent;

The buildings have been further broken down into distinct volumes. This serves to break down the lengths of the buildings into smaller elements, and create more diverse compositions within each of the buildings. Secondary balconies have been removed in locations to emphasize breaks in the massing similar to what was done at The Gardens project. Rooflines have been adjusted to reinforce corner tower elements.

- courtyards are sterile and enhance the monolithic aspect of the project; water features are contained, i.e. they don't flow from one courtyard to the other; given the monolithic nature of the project, it would have helped if the landscape would have been more grainy to soften the edges in and around the buildings;

Landscape designs have been adjusted to 'loosen up' and relax some of the formal patterns to help address this. Water feature designs don't link between courtyards, but have been designed to connect with people at public and private levels.

- project needs an extra push;

Since formal ADP and per previous responses above, we have further developed the buildings. The tower element on the northwest corner of building 2 has been improved with additional height and more expansive glazing. All four buildings have been improved with more breaks within the building length and differentiated massing elements.

- great presentation materials; comments by the Panel during the preliminary stage have been successfully addressed by the applicant;

Noted

- wood frame buildings do not appear as such as they have a concrete look; hope that this look will survive through the entire development application process; lightening up of the upper levels of the buildings, i.e. 4th, 5th and 6th levels adds to the design of the buildings; hope that this detail will be maintained; and

Though these are wood frame buildings, we will endeavour to carry out the look of the buildings as presented. Detail of the simplified upper levels will be maintained.

- applicant has done a good job in addressing the elevation changes at the street level in terms of landscaping; e.g. through stepping up.

Noted

At this point, Sara Badyal, Planner and Staff Liaison to the Panel, read the following comments of Hal Owens who left the meeting earlier:

- in terms of site context, sensitivity and connectivity to surrounding land uses are generally good; slope up at internal street is a bit steep still with high wall along pedestrian sidewalk; needs some detail at wall or planting;

Internal street is sloping at less than 5% as supported by Transportation. Stepped planters are proposed along the sidewalk that will reduce in height towards the intersection of Cedarbridge and the east-west Greenway.

- site layout is generally okay but not clear how is through access by cars to be discouraged;

Through access by cars is not discouraged. The central crossing of the Greenway and Cedarbridge is proposed to be paved to mark the crossing and link all four buildings with the greenway to the extent permitted by City transportation.

- building character, massing, roof form, facade articulation are generally okay;

Noted

- generally, the applicant has addressed the concerns from the previous presentation; however, prominent corner element is well integrated but still lacks an iconic character that announces this project;

The tower element on the northwest corner of building 2 has been improved with additional height and more expansive glazing. The additional glazing helps to further differentiate the corner element from the rest of the building.

- character of the building is essentially the same, i.e. there is little differentiation other than minor material/colour detail changes;

Additional work has been done to further differentiate the massing within each building, and between the buildings. Stacks of secondary balconies have been removed in key locations to break down the large buildings into smaller volumes. Material treatment of lower levels of each building is quite distinct, upper levels are treated similarly in order to create unity to the project.

- exterior finishes and colours are fairly good but not as robust as what is on the adjacent parcels in the Oval district; may cause this project to look like a “poor neighbour”;

Bright colours have been avoided except as accents. We feel that the more subtle palette will age better, and not be prone to ‘date-ing’ the project.

- landscape design has generally addressed most of the concerns from previous presentation;

Noted

- applicant should show where and how public art is being considered;

Onni has contributed funds at the request of the City for a landmark art piece to be located on City land at the corner of Gilbert and New River Road.

On the development site a number of potential locations for public art have been identified in the Detailed Public Art Plan prepared by ‘Id Public Art Consulting’ who are the public art consultants for the project. Areas that are hubs for pedestrian movement have been identified as potential artworks locations, such as areas along the east-west greenway, north south pedestrian link and around the lobby entrances that front onto the east-west greenway.

- consider enlarging the water features and tie into river garden or canal theme;

This option was previously explored, it was not supported by City staff as there was a need for more usable outdoor space. The elevation changes in the project would make the river or canal theme hard to see and understand at a pedestrian level. Instead, we have tried to make water accessible at a number of different levels.

- consider reducing small bits of colour and instead use a larger or bolder mass or element on each building to differentiate them.

Smaller colour elements on building 4 have been complemented with larger areas of colour at the Greenway lobby and prominent building corners. Mass of all buildings has been broken down into smaller elements to further differentiate each building and to emphasize tower and corner elements.



No. DP 12-615424

To the Holder: ONNI CONTRACTING LTD.
Property Address: 7731 AND 7771 ALDERBRIDGE WAY
Address: C/O MR. ERIC HUGHES, ONNI CONTRACTING LTD.
550 ROBSON STREET – SUITE 300
VANCOUVER, BC V6B 2B7

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - (a) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures on the proposed Lots 1 and 2 from 3.0 m to 0.0 m respectively along Cedarbridge Way and Gilbert Road;
 - (b) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the west property line of the proposed Lot 1 and the east property line of the proposed Lot 2;
 - (c) Reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15spaces/dwelling unit for the development as a whole; and
 - (d) Relax the requirement for the provision of on-site loading spaces for two (2) WB-17 loading spaces.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Architectural and Landscape Architecture Plans labelled Plans 1 to 74 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.

No. DP 12-615424

To the Holder: ONNI CONTRACTING LTD.
Property Address: 7731 AND 7771 ALDERBRIDGE WAY
Address: C/O MR. ERIC HUGHES, ONNI CONTRACTING LTD.
550 ROBSON STREET – SUITE 300
VANCOUVER, BC V6B 2B7

6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$826,437 ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

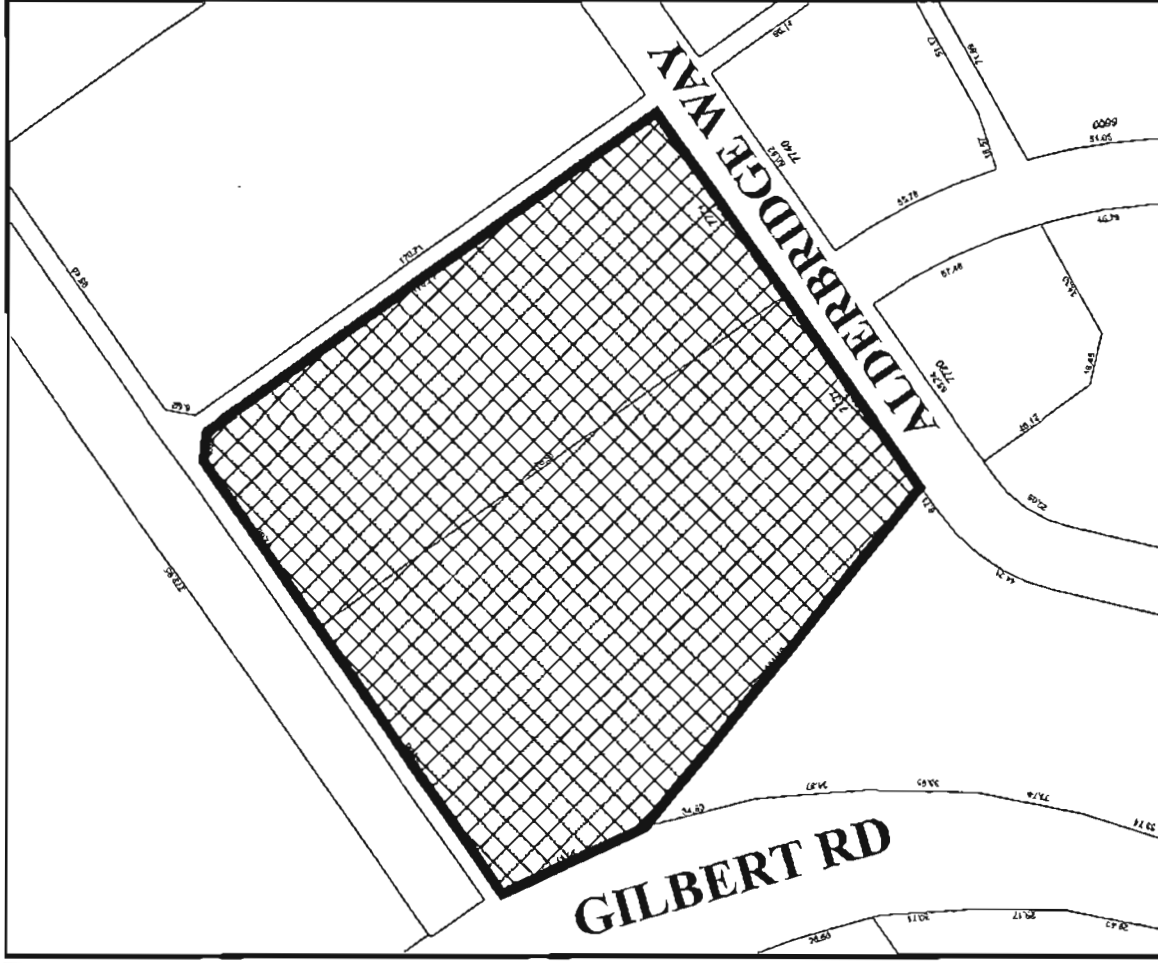
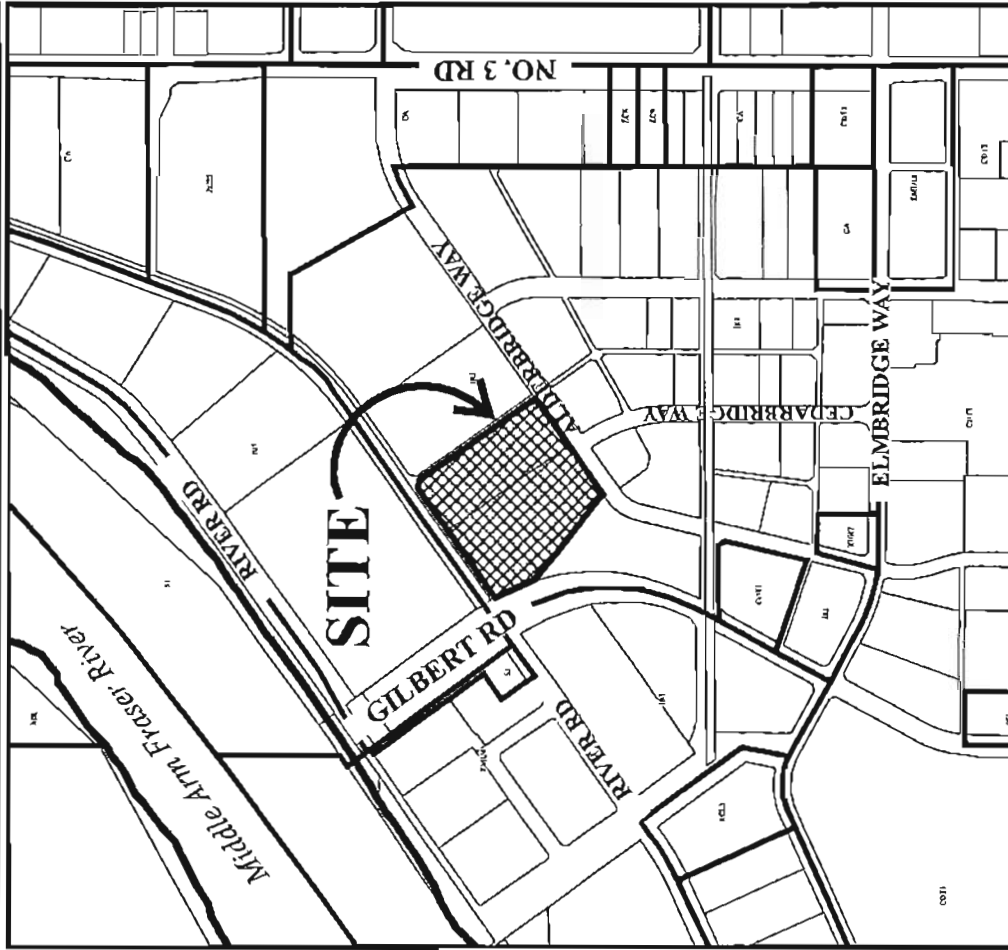
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond

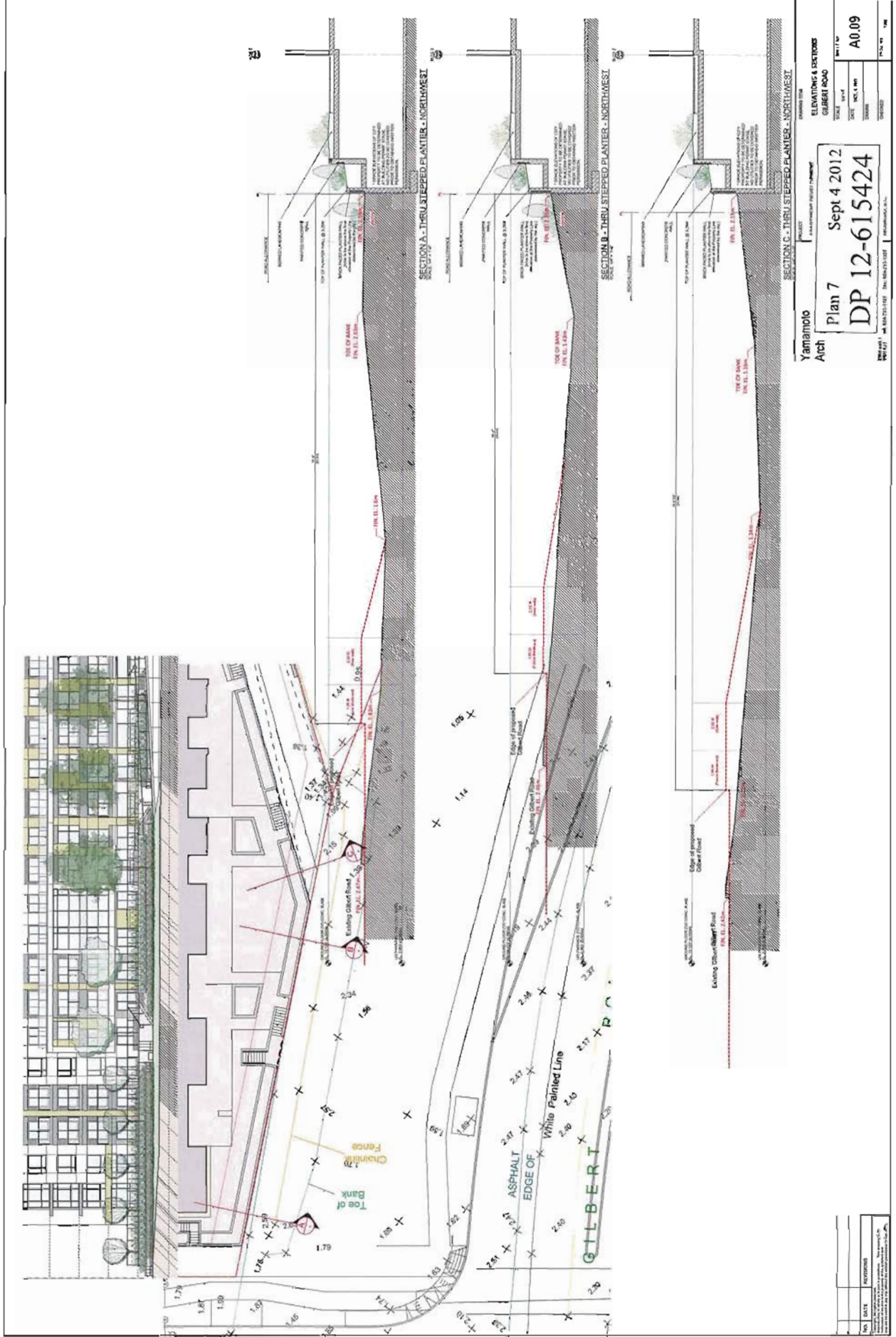


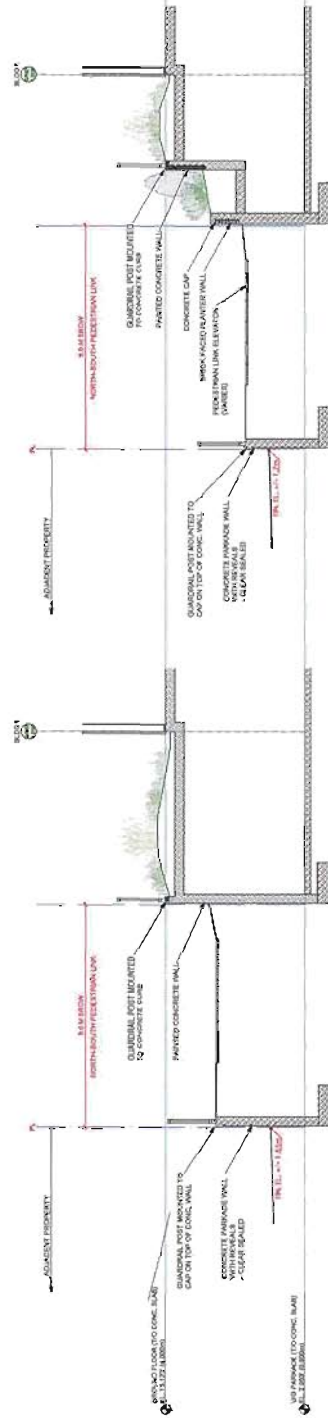
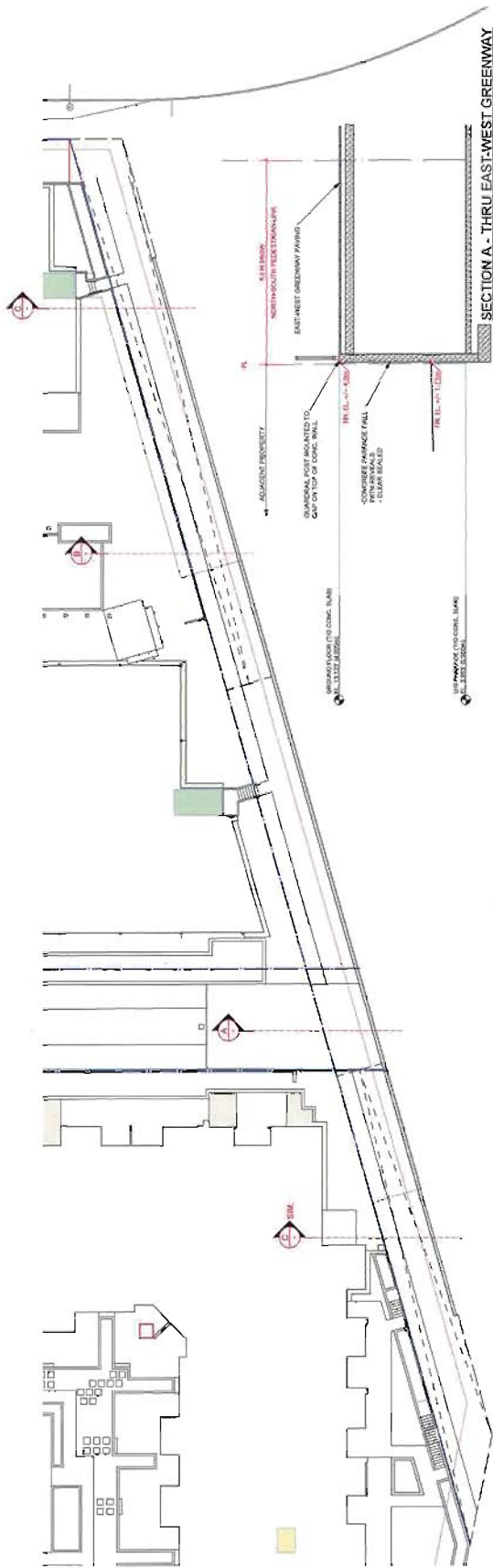
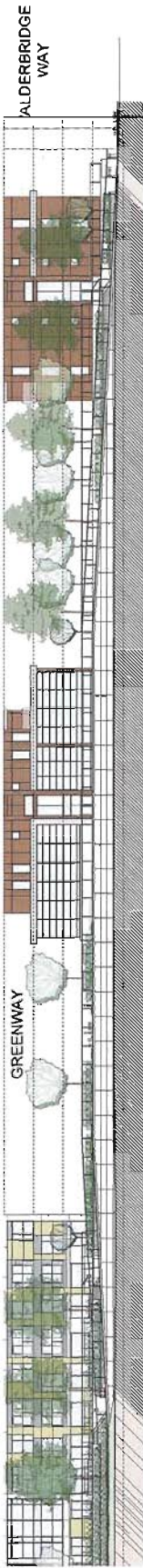
DP 12-615424 SCHEDULE "A"

Original Date: 07/11/12

Revision Date:

Note: Dimensions are in METRES





SECTION B - THRU EAST-WEST GREENWAY
SCALE: 1/4\"/>

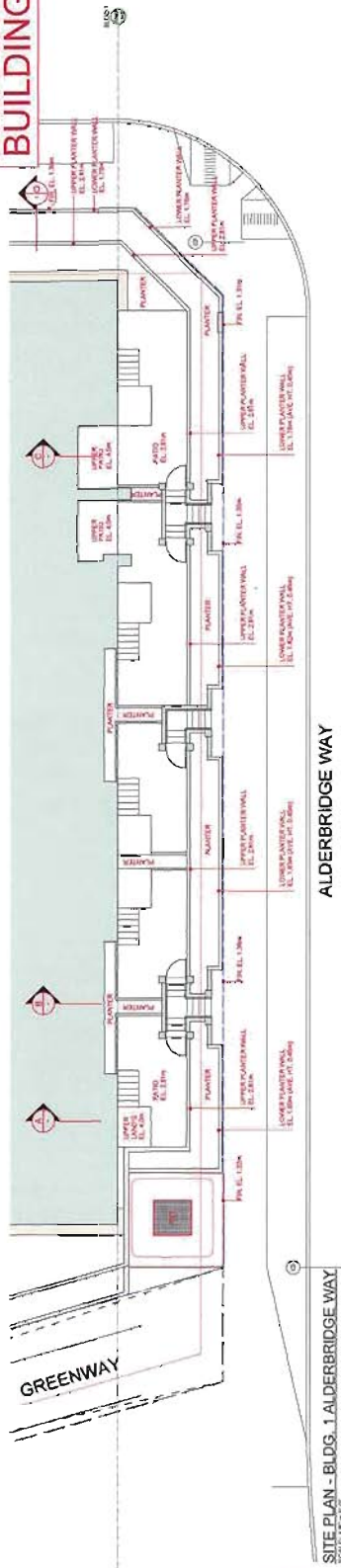
Plan 7A Sept 4 2012
DP 12-615424

PROJECT		Yamamoto Architecture Inc.	
ANALYST DEVELOPMENT		Yamamoto Architecture Inc.	
DRAWING TITLE		ELEVATIONS & SECTIONS	
SHEET NO.		A0.10	
DATE		SEP 4 2012	
BY		J. YAMAMOTO	
CHECKED		J. YAMAMOTO	

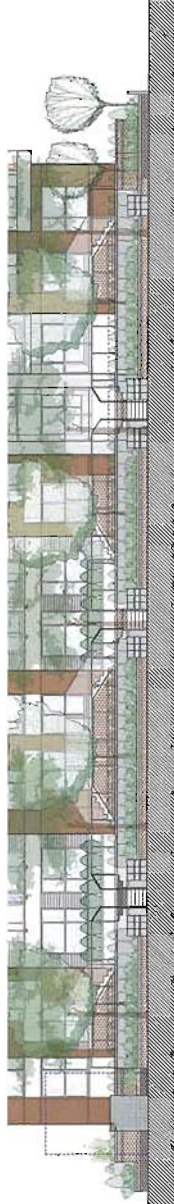
DATE	REVISION

BUILDING NO.1

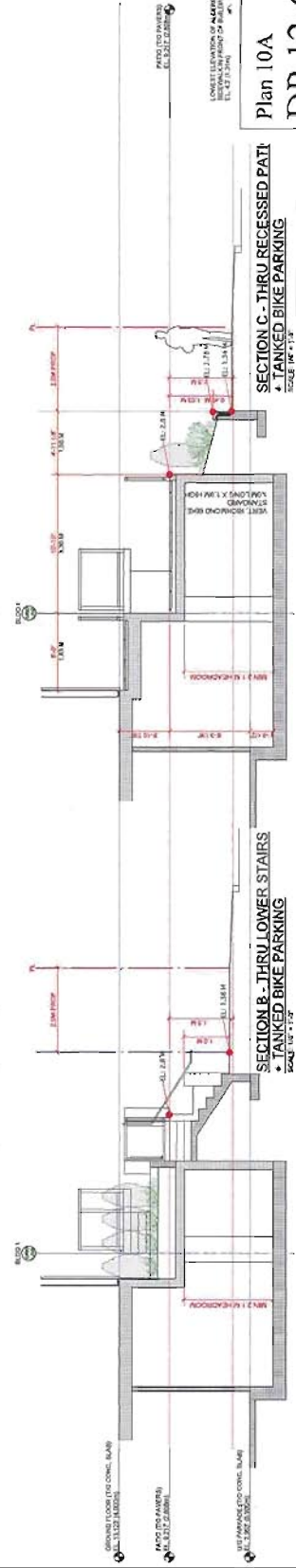
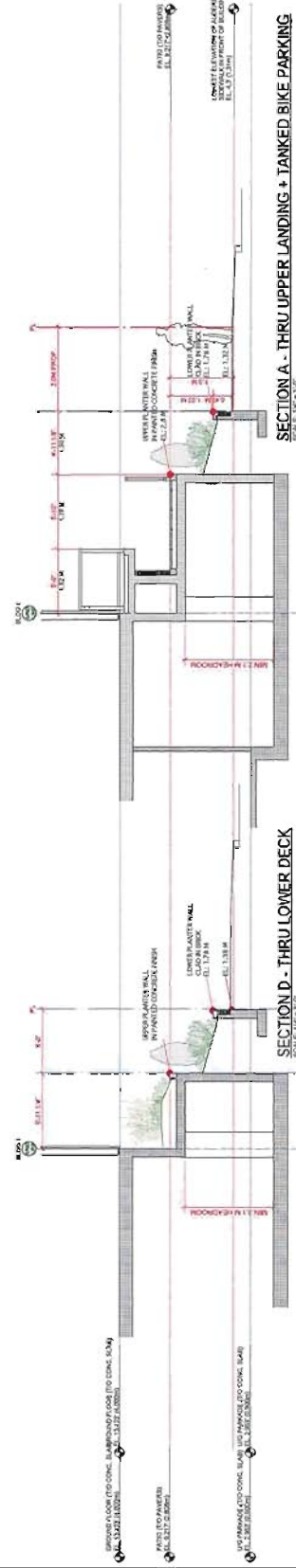
CEDARBRIDGE WAY



ALDERBRIDGE WAY



STREETSCAPE - BLDG, 1 ALDERBRIDGE WAY



NO.	DATE	REMARKS

PROJECT
APARTMENT BUILDING
CONCRETE

RIVA
7771, 7771 ALBERGHEE WAW

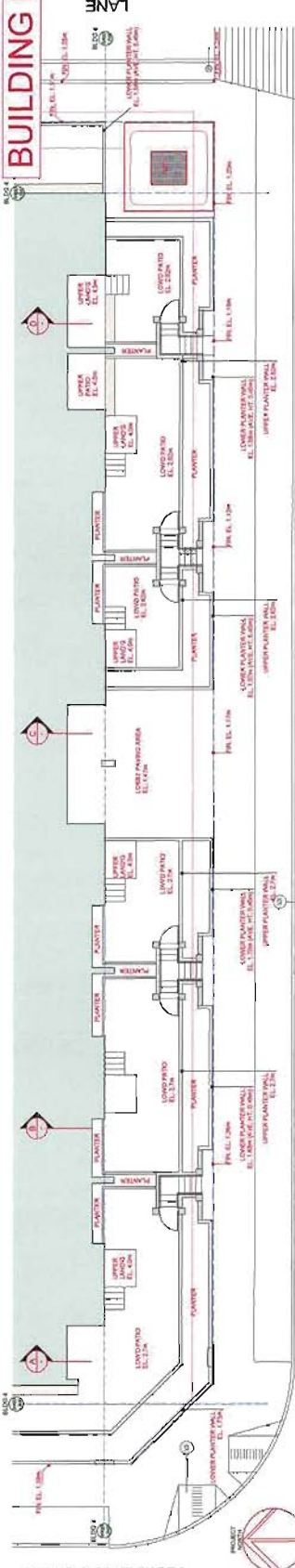
**Yamamoto
Architecture Inc.**

2204 Oak Street, Vancouver, B.C.
V6M 4A1 Tel: 604-731-1137 Fax: 604-731-1137

SCALE	1	TEST NO.
0.05		

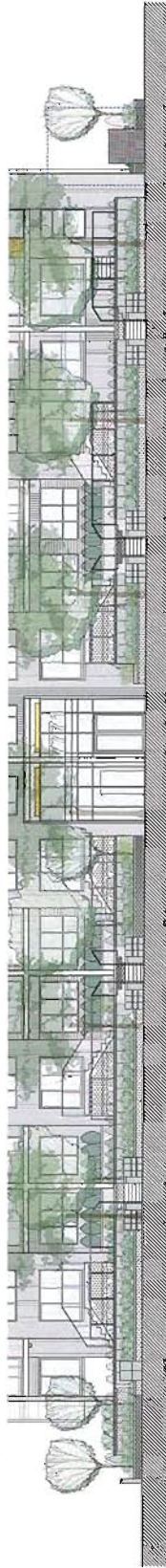
Plan 10A Sept 4 2012
DP 12-615424

BUILDING NO. 4

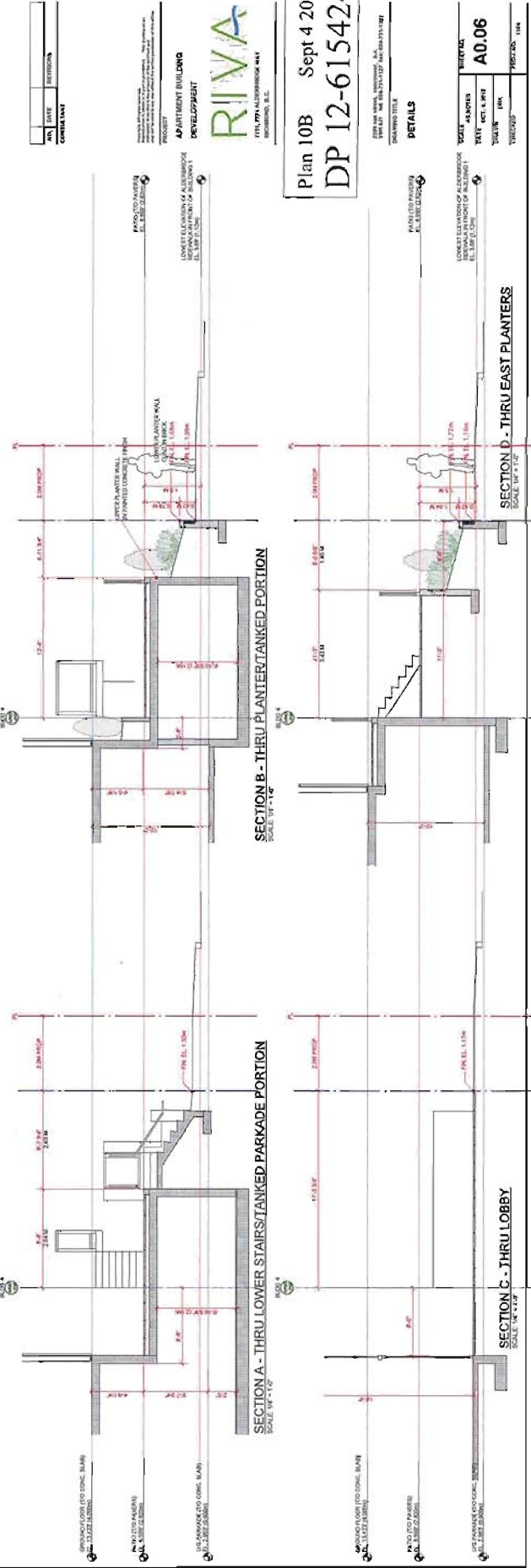


SITE PLAN - BLDG. 4 ALDERBRIDGE WAY
SCALE 1/8" = 1'-0"

ALDERBRIDGE WAY



STREETSCAPE - BLDG. 4 ALDERBRIDGE WAY
SCALE 1/8" = 1'-0"



SECTION B - THRU PLANTER/TANKED PORTION
SCALE 1/4" = 1'-0"

SECTION A - THRU LOWER STAIRS/TANKED PARKADE PORTION
SCALE 1/4" = 1'-0"

SECTION C - THRU LOBBY
SCALE 1/4" = 1'-0"

SECTION D - THRU EAST PLANTERS
SCALE 1/4" = 1'-0"

NO.	DATE	REVISION
1	09/04/12	CONSTRUCTION

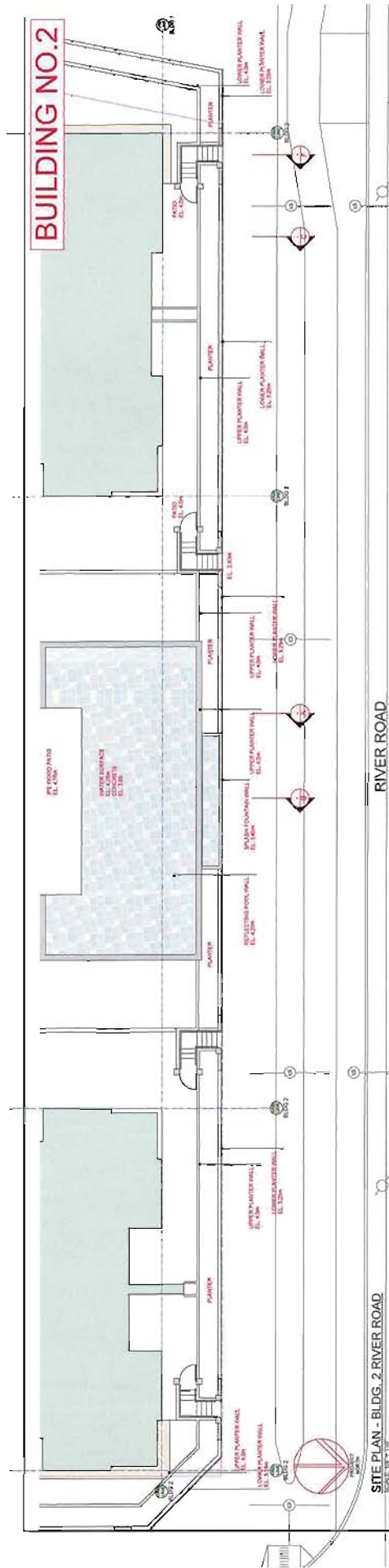
PROJECT: APARTMENT BUILDING DEVELOPMENT
RIVA
1000 W. 10TH STREET, SUITE 100
DENVER, CO 80202

Plan 10B Sept 4 2012
DP 12-615424

DETAILS
DRAWING TITLE
2000 W. 10TH STREET, SUITE 100
DENVER, CO 80202

SCALE	DATE	REVISION
A0.06	09/04/12	CONSTRUCTION

BUILDING NO.2

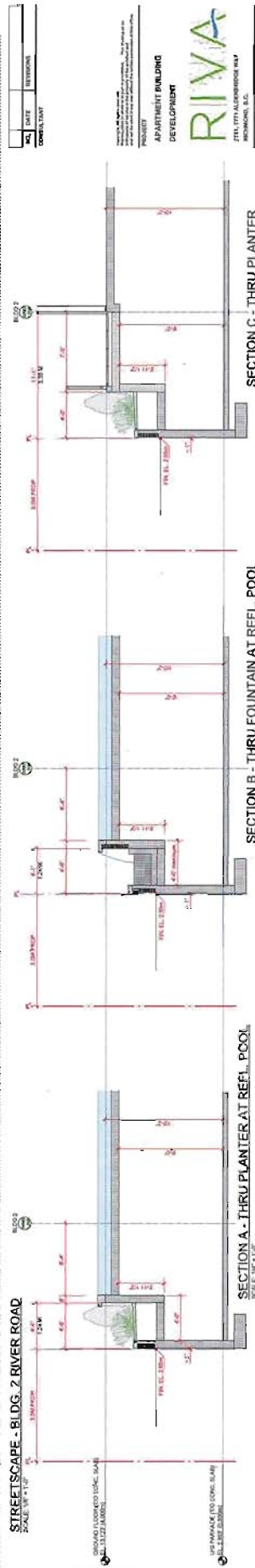


RIVER ROAD

SITE PLAN - BLDG. 2 RIVER ROAD



STREETSCAPE - BLDG. 2 RIVER ROAD



SECTION A - THRU PLANTER AT REF. POOL

SECTION B - THRU FOUNTAIN AT REF. POOL

SECTION C - THRU PLANTER

Plan 11A Sept 4 2012
DP 12-615424

PROJECT
APARTMENT BUILDING
DEVELOPMENT
RIVA
FIRM: RIVA ARCHITECTURE INC.
ARCHITECT: R.C.

DATE: 09/04/12
SCALE: 1/8" = 1'-0"

SECTION F - THRU STAIRS TO UNIT PATIO
SCALE: 1/8" = 1'-0"

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

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UPPER PLANTER WALL (SEE CONC. SLAB)

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UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

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UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

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UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

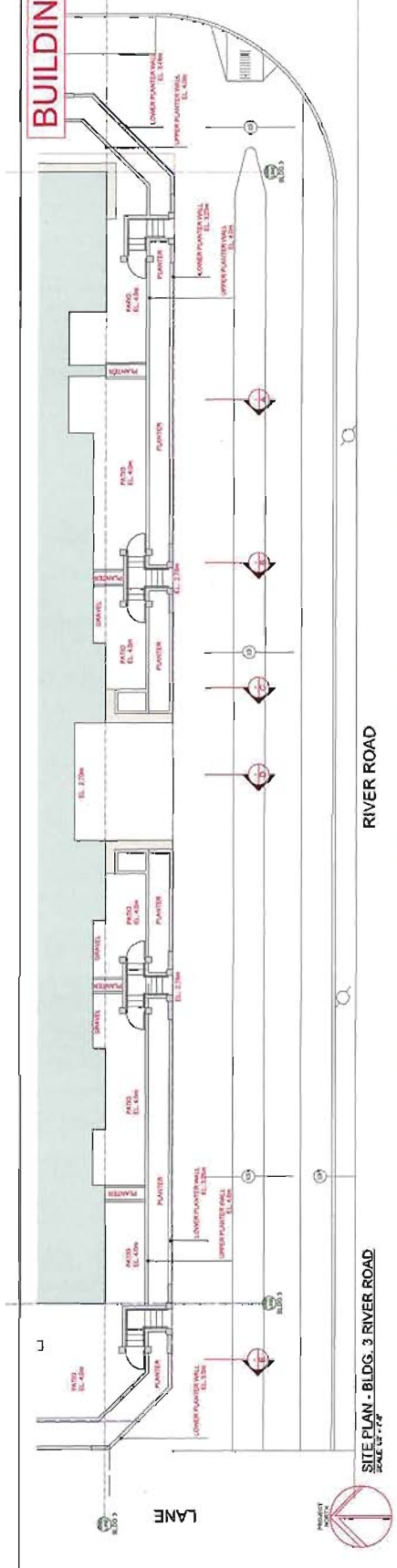
UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

UPPER PLANTER WALL (SEE CONC. SLAB)

BUILDING NO.3

CEDARBRIDGE WAY

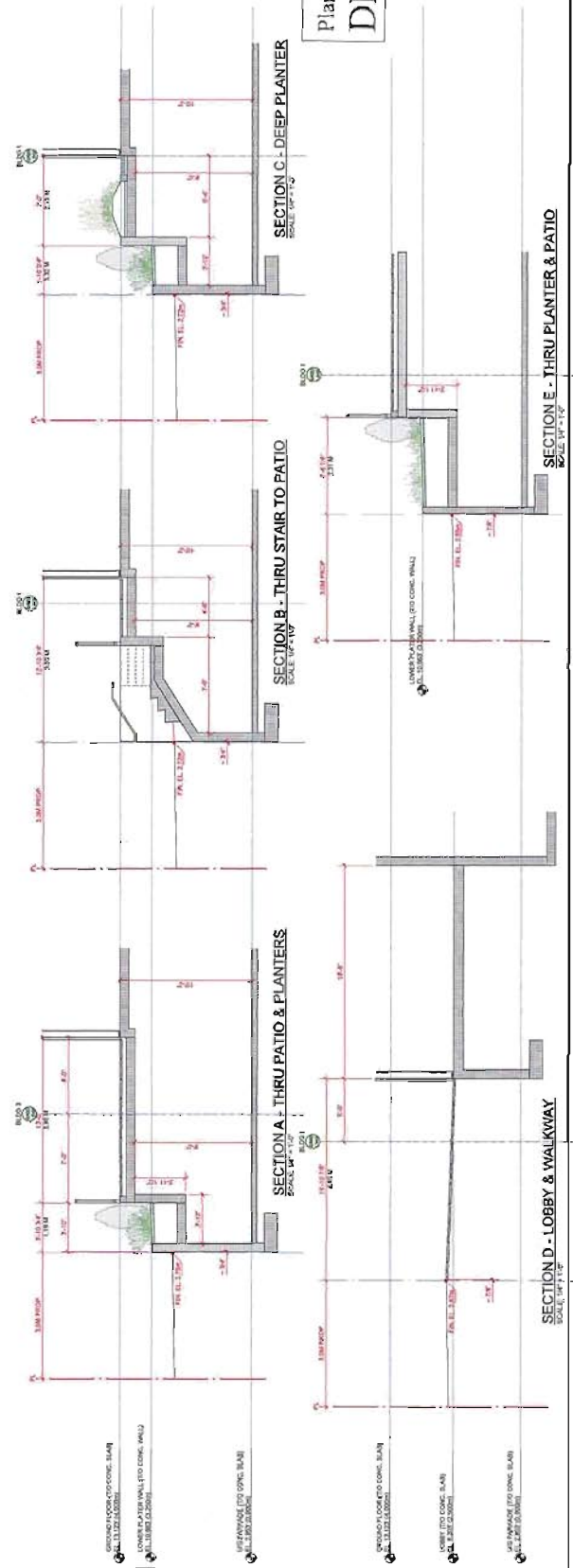


RIVER ROAD

SITE PLAN - BLDG. 3 RIVER ROAD
SCALE: 1/4" = 1'-0"



STREETSCAPE - BLDG. 3 RIVER ROAD
SCALE: 1/4" = 1'-0"



SECTION A - THRU PATIO & PLANTERS
SCALE: 1/4" = 1'-0"

SECTION B - THRU STAIR TO PATIO
SCALE: 1/4" = 1'-0"

SECTION C - DEEP PLANTER
SCALE: 1/4" = 1'-0"

SECTION D - LOBBY & WALKWAY
SCALE: 1/4" = 1'-0"

SECTION E - THRU PLANTER & PATIO
SCALE: 1/4" = 1'-0"

NO.	DATE	REVISION
1	09/04/12	ISSUED FOR PERMIT

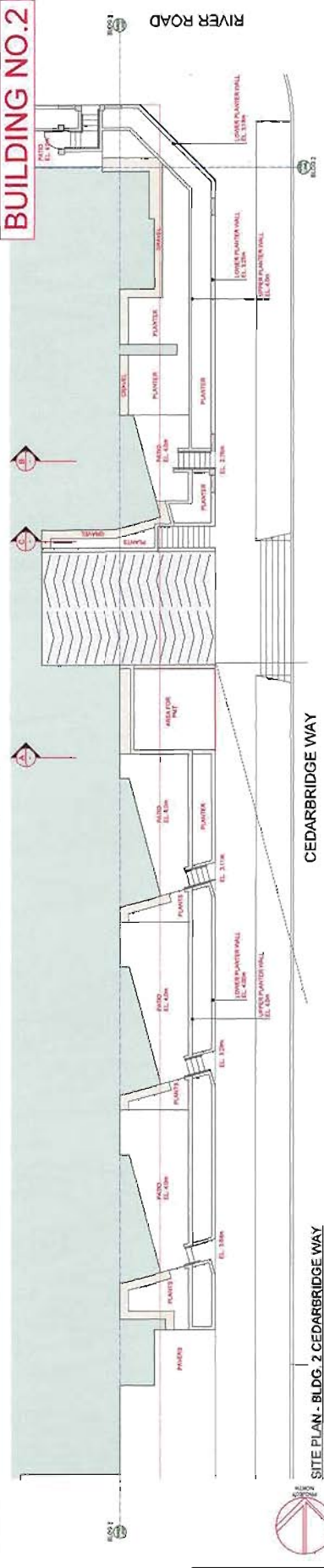
PROJECT: APARTMENT BUILDING DEVELOPMENT
RIVA
1700 N. RIVER ROAD, SUITE 100
MONTGOMERY, AL 36106

Plan 11B Sept 4 2012
DP 12-615424

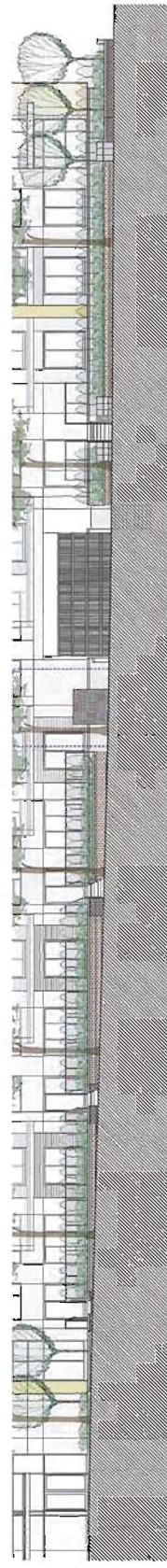
PROJECT TITLE: DETAILS
DRAWN BY: [Name]
CHECKED BY: [Name]

DATE: 09/04/12	BY: [Name]
DATE: 09/04/12	BY: [Name]
DATE: 09/04/12	BY: [Name]

BUILDING NO.2



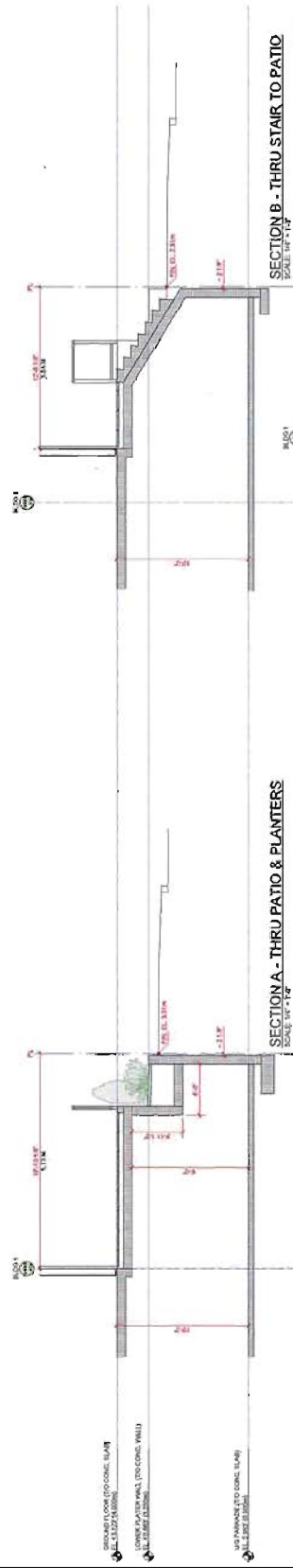
STREETSCAPE - BLDG. 2 CEDARBRIDGE WAY



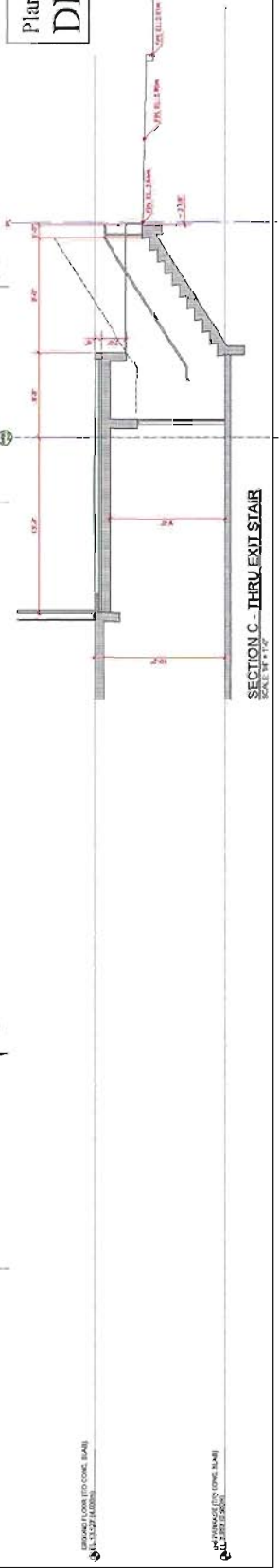
STREETSCAPE - BLDG. 2 CEDARBRIDGE WAY
SCALE: 1/8" = 1'-0"

NO.	DATE	REVISIONS
1	09/04/12	CONCEPT

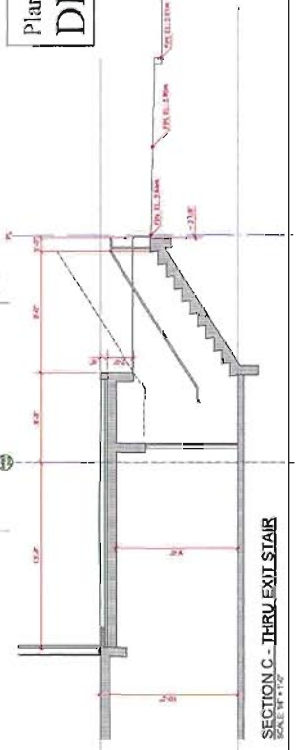
JACQUETTE BUILDING
 DEVELOPMENT
RIVA
 1771 17TH AVENUE, SUITE 100
 DENVER, CO 80202



SECTION A - THRU PATIO & PLANTERS
SCALE: 1/8" = 1'-0"



SECTION B - THRU STAIR TO PATIO
SCALE: 1/8" = 1'-0"



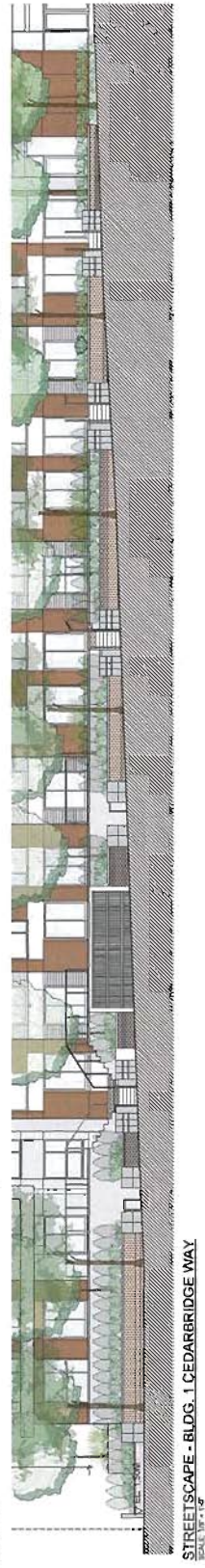
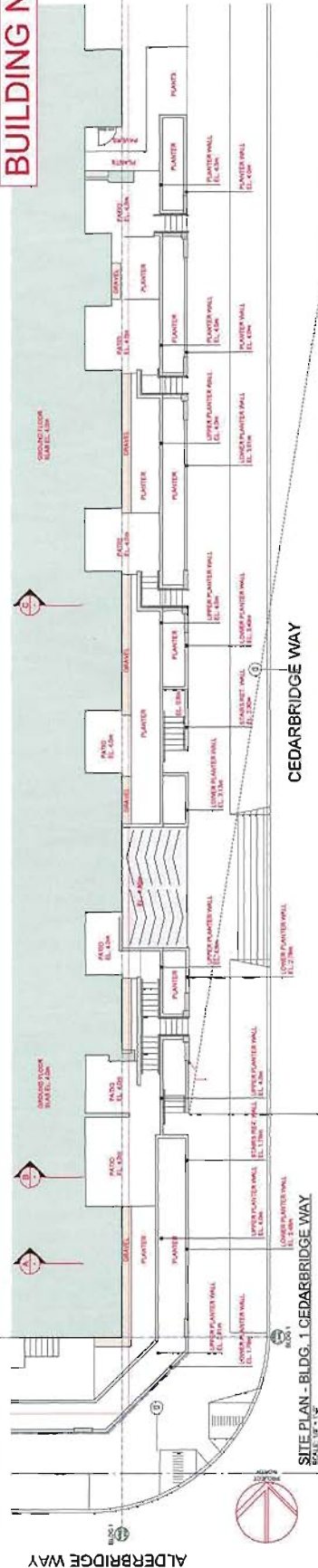
SECTION C - THRU EXIT STAIR
SCALE: 1/8" = 1'-0"

Plan 11C Sept 4 2012
 DP 12-615424

NO.	DATE	REVISIONS
1	09/04/12	CONCEPT

DATE	09/04/12
BY	11C
CHECKED	11C
DESIGNED	11C
PROJECT	11C

BUILDING NO.1



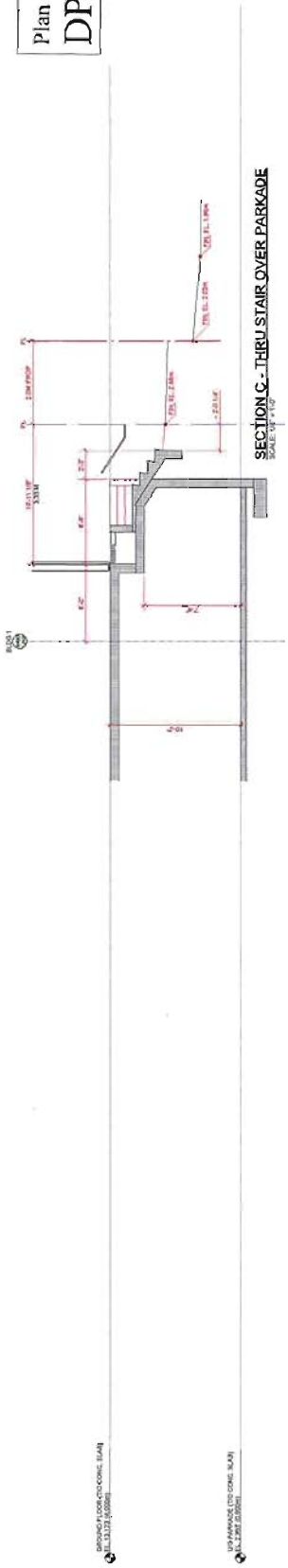
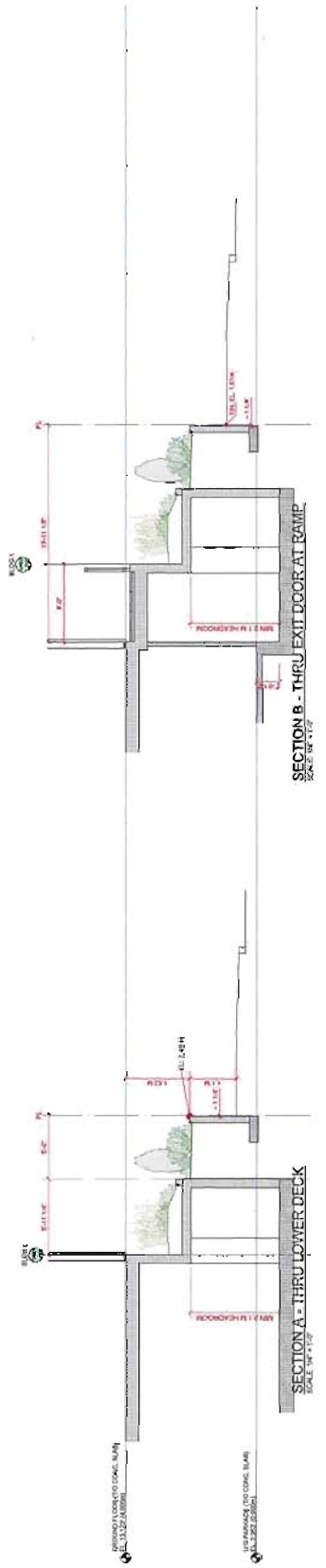
NO.	DATE	DESCRIPTION
1	09/04/12	ISSUED FOR PERMIT

APARTMENT BUILDING
DEVELOPMENT
RIVA
1711 17TH ALEXANDRIA HWY
FARMERSVILLE, VA 22434

Plan 11D Sept 4 2012
DP 12-615424

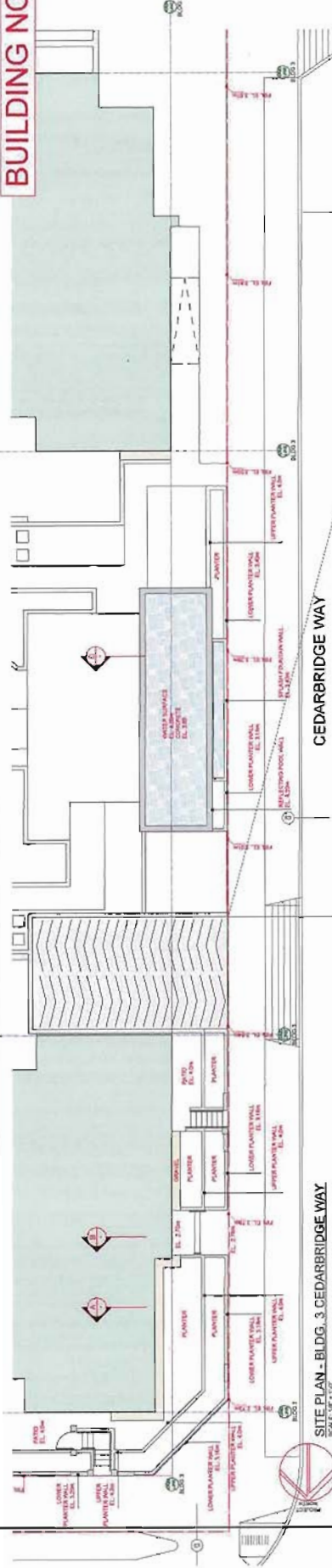
DETAILS

NO.	DATE	DESCRIPTION
1	09/04/12	ISSUED FOR PERMIT



SECTION C - THRU STAIR OVER PARKADE
Scale: 1/8" = 1'-0"

BUILDING NO.3



SITE PLAN - BLDG. 3 CEDARBRIDGE WAY
SCALE 1/8" = 1'-0"

CEDARBRIDGE WAY



STREETSCAPE - BLDG. 3 CEDARBRIDGE WAY
SCALE 1/8" = 1'-0"

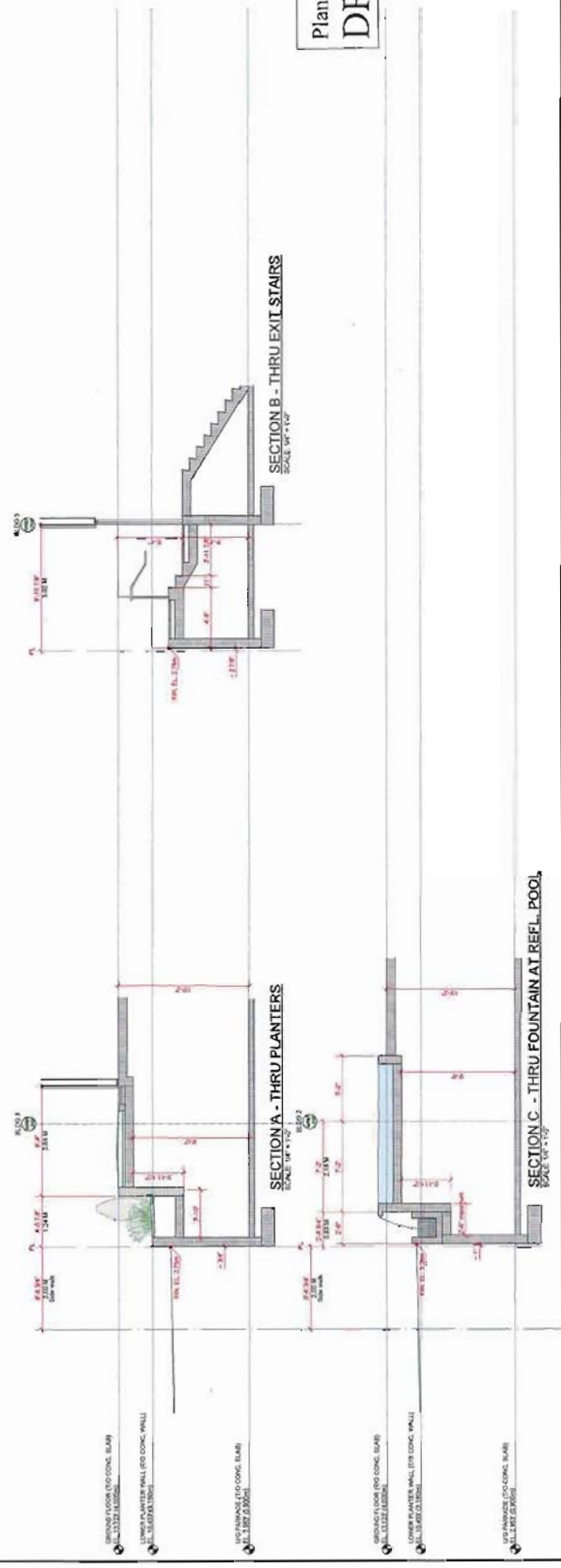
NO.	DATE	REVISIONS
1	08/14/12	CONTRACT

PROJECT
APARTMENT BUILDING
DEVELOPMENT
RIVA
RIVA DEVELOPMENT, LLC
1714 17TH AVENUE, SUITE 100
DENVER, CO 80202

Plan 11E Sept 4 2012
DP 12-615424

DATE: 08/14/12
DRAWN BY: JLM
CHECKED BY: JLM
SCALE: 1/8" = 1'-0"

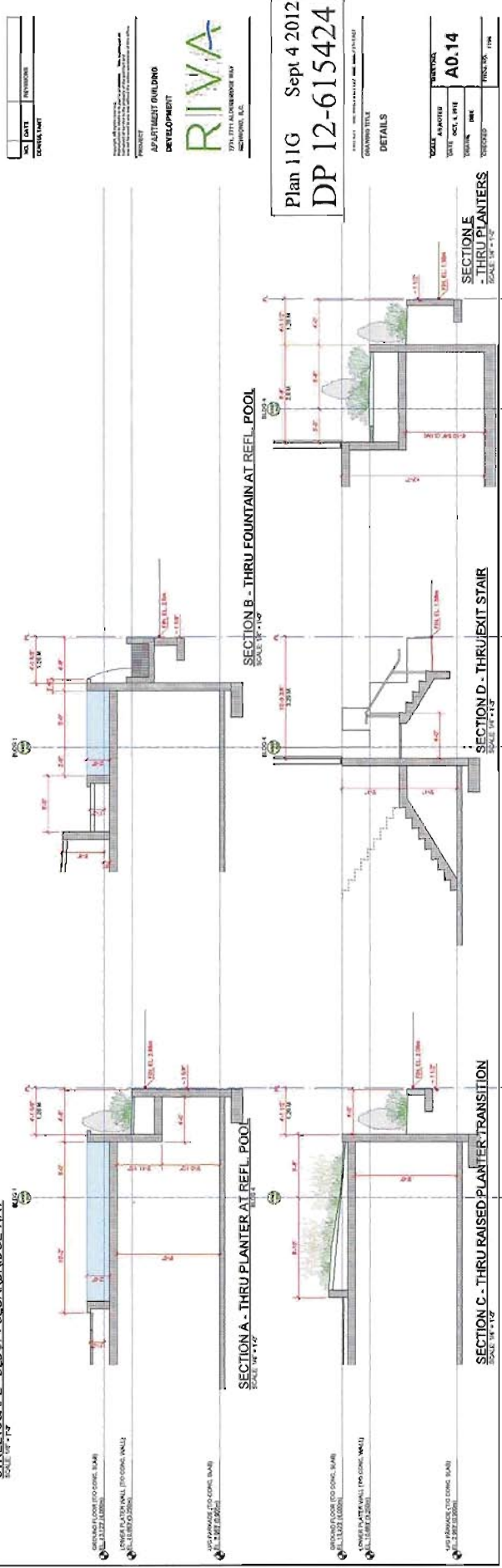
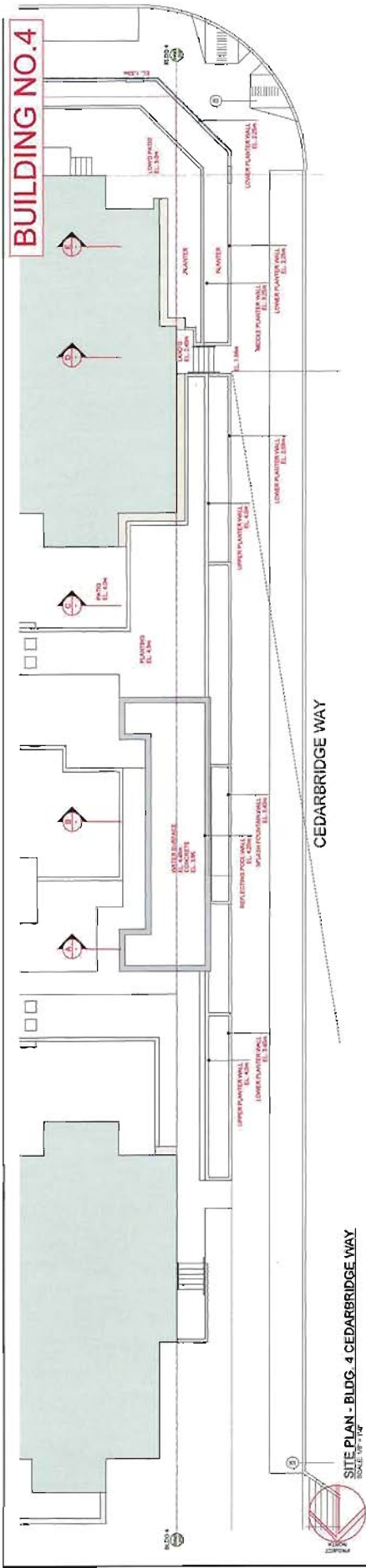
NO.	DATE	REVISIONS
1	08/14/12	CONTRACT



SECTION A - THRU PLANTERS
SCALE 1/8" = 1'-0"

SECTION B - THRU EXIT STAIRS
SCALE 1/8" = 1'-0"

SECTION C - THRU FOUNTAIN AT REFILL POOL
SCALE 1/8" = 1'-0"





Category	Number of entries	Category	Number of entries
Animal Husbandry	10	Engineering	10
Arts and Crafts	10	Health Sciences	10
Business and Economics	10	Humanities	10
Chemical Engineering	10	Information Technology	10
Chemical Sciences	10	Law	10
Computer Science	10	Life Sciences	10
Construction	10	Mathematics	10
Education	10	Medicine	10
Electrical Engineering	10	Physical Sciences	10
Environmental Engineering	10	Psychology	10
Environmental Sciences	10	Social Sciences	10
Food Science and Technology	10	Statistics	10
Health Sciences	10	Transportation	10
Humanities	10	Unlabeled	10
Information Technology	10		
Law	10		
Life Sciences	10		
Mathematics	10		
Medicine	10		
Physical Sciences	10		
Psychology	10		
Social Sciences	10		
Statistics	10		
Transportation	10		
Unlabeled	10		



SHARP & DIAMOND
CONCRETE ARCHITECTURE INC.
301 W. 4th Street
Suite 211
Seattle, WA 98101
Tel: 206.461.1000
Fax: 206.461.1001
www.sharpanddiamond.com

**Yamamoto
Architecture Inc.**

DEPARTMENT OF AGRICULTURE

RIVA

Plan 40 Sept 4 2012
DP 12-615424

Year 2000	400
Year 2001	500
Year 2002	600
Year 2003	700
Year 2004	800
Year 2005	900
Year 2006	1000
Year 2007	1100
Year 2008	1200
Year 2009	1300
Year 2010	1400
Year 2011	1500
Year 2012	1600
Year 2013	1700
Year 2014	1800
Year 2015	1900
Year 2016	2000
Year 2017	2100
Year 2018	2200
Year 2019	2300
Year 2020	2400
Year 2021	2500
Year 2022	2600
Year 2023	2700
Year 2024	2800
Year 2025	2900
Year 2026	3000
Year 2027	3100
Year 2028	3200
Year 2029	3300
Year 2030	3400
Year 2031	3500
Year 2032	3600
Year 2033	3700
Year 2034	3800
Year 2035	3900
Year 2036	4000
Year 2037	4100
Year 2038	4200
Year 2039	4300
Year 2040	4400
Year 2041	4500
Year 2042	4600
Year 2043	4700
Year 2044	4800
Year 2045	4900
Year 2046	5000
Year 2047	5100
Year 2048	5200
Year 2049	5300
Year 2050	5400
Year 2051	5500
Year 2052	5600
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Year 2056	6000
Year 2057	6100
Year 2058	6200
Year 2059	6300
Year 2060	6400
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Year 2062	6600
Year 2063	6700
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Year 2117	12100
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Year 2119	12300
Year 2120	12400
Year 2121	12500
Year 2122	12600
Year 2123	12700
Year 2124	12800
Year 2125	12900
Year 2126	13000
Year 2127	13100
Year 2128	13200
Year 2129	13300
Year 2130	13400
Year 2131	13500
Year 2132	13600
Year 2133	13700
Year 2134	13800
Year 2135	13900
Year 2136	14000
Year 2137	14100
Year 2138	14200
Year 2139	14300

LANDSCAPE SITE PLAN

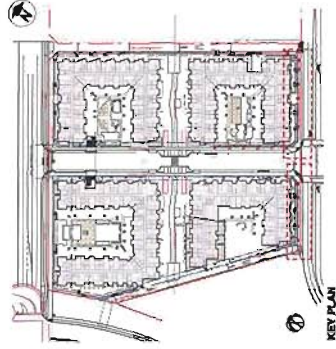
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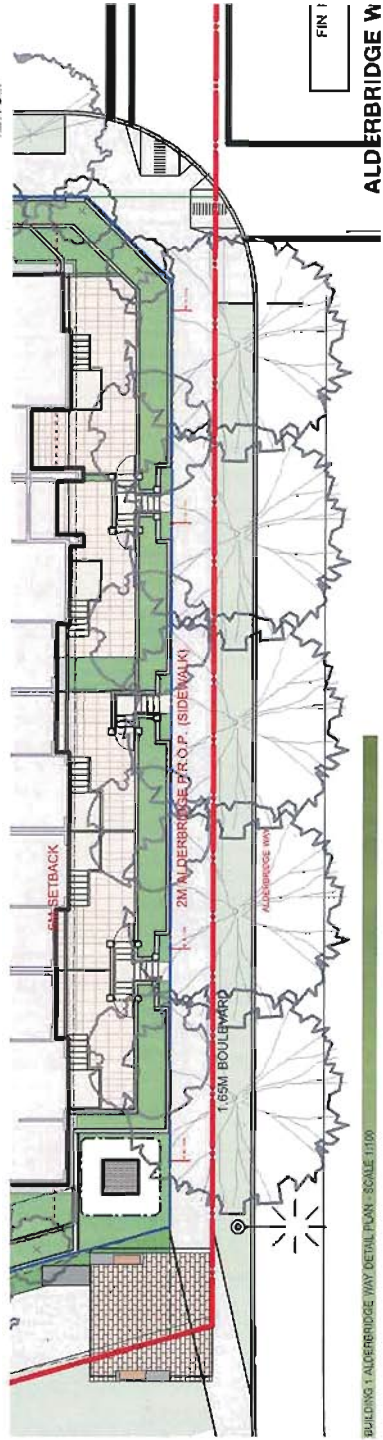
DRAWING LEGEND

- 1.1.1. LANDSCAPE SITE PLAN
- 1.1.2. LANDSCAPE PLAN
- 1.1.3. COURTYARD DETAIL PLAN - BUILDING 4
- 1.1.4. COURTYARD DETAIL PLAN - BUILDING 3
- 1.1.5. COURTYARD DETAIL PLAN - BUILDING 1
- 1.1.6. WEST COMMUNITY GARDEN GREENWAY
- 1.1.7. ON-SITE PRIVATE AND SHARED OPEN SPACE
- 1.1.8. PROPOSED PUBLIC ART PLAN
- 1.1.9. LANDSCAPE IRRIGATION PLAN
- 1.1.10. CENTRAL PLAZA DETAIL PLAN
- 1.2.1. PLANT MATERIALS
- 1.2.2. LANDSCAPE SECTIONS
- 1.2.3. LANDSCAPE SECTIONS
- 1.2.4. LANDSCAPE SECTIONS
- 1.2.5. LANDSCAPE ELEVATIONS AND DETAILS
- 1.4.1. PRECEDENT IMAGES
- 1.5.1. PLANT AREA ELEMENTS
- 1.5.2. LANDSCAPE DETAILS

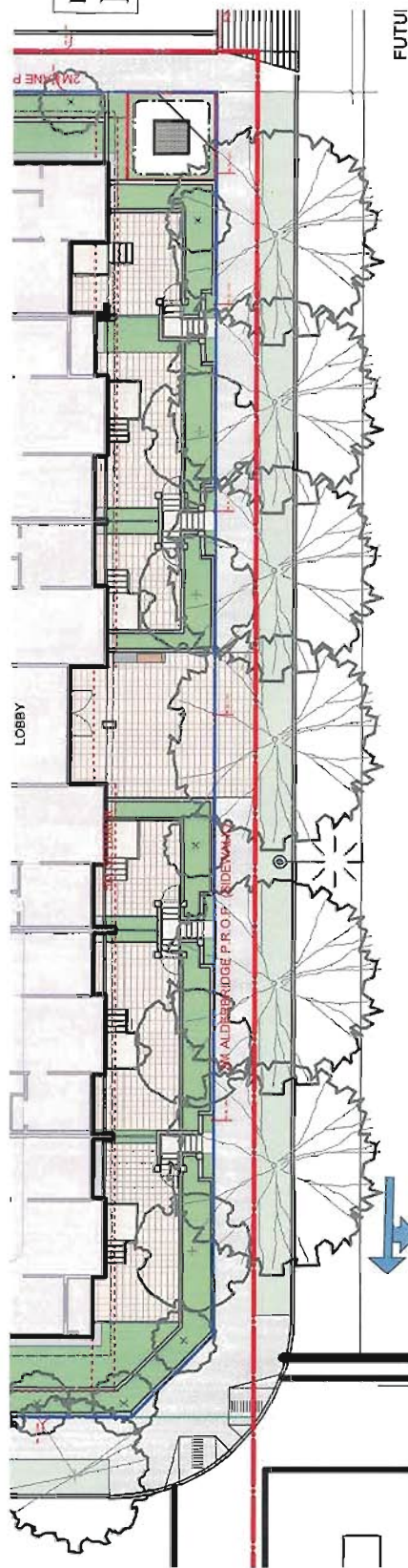




NOTES:



BUILDING 1 ALDERBRIDGE WAY DETAIL PLAN - SCALE 1:100



BUILDING 3 ALDERBRIDGE WAY DETAIL PLAN - SCALE 1:100

NO.	REVISION	DATE	BY	CHKD.
1	ISSUED FOR PERMIT	09/04/12	YAMAMOTO	SHARP
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SHARP & DIAMOND
ARCHITECTS
1000 10th Avenue
Suite 100
Vancouver, BC V6Z 1A1
Tel: 604.681.1111
Fax: 604.681.1112
www.sharpanddiamond.com

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Plan 48A Sept 4 2012
DP 12-615424

NO.	REVISION	DATE	BY	CHKD.
1	ISSUED FOR PERMIT	09/04/12	YAMAMOTO	SHARP
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www.sharpanddiamond.com

L1.11

NOTES:

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Yamamoto
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1000 15th Street, Suite 100
San Francisco, CA 94103
415.774.4477
www.sharpanddiamond.com

Plan S1 Sept 4 2012
DP 12-615424

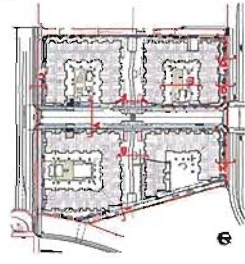
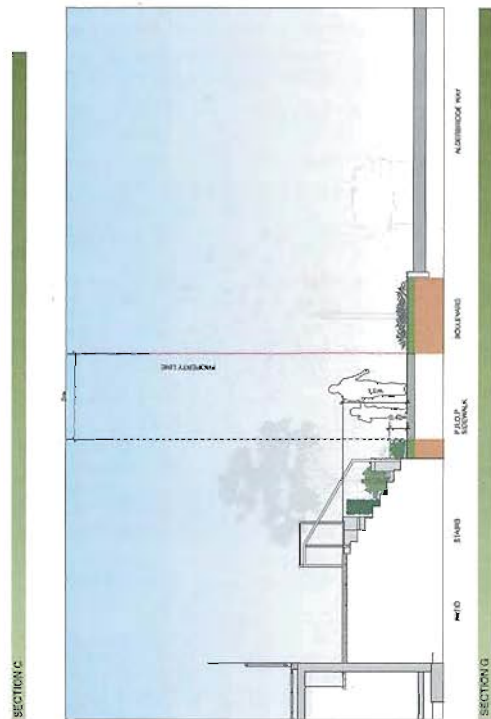
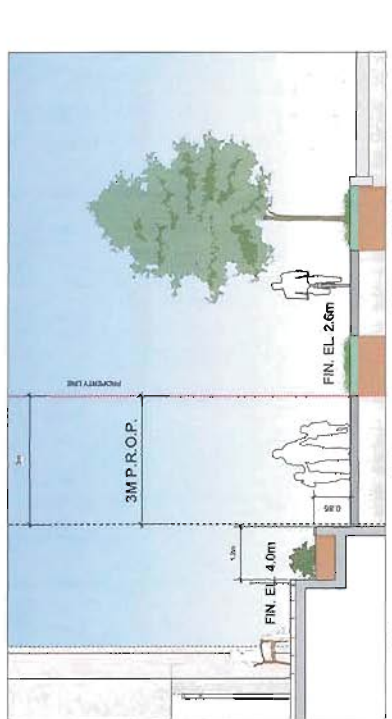
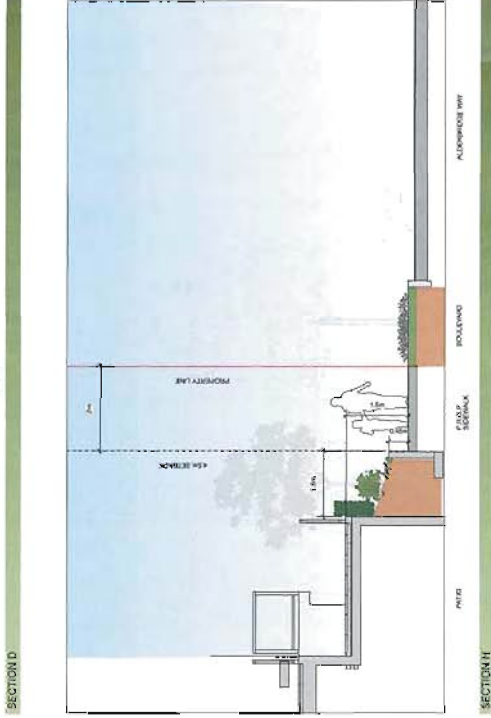


LANDSCAPE SECTIONS

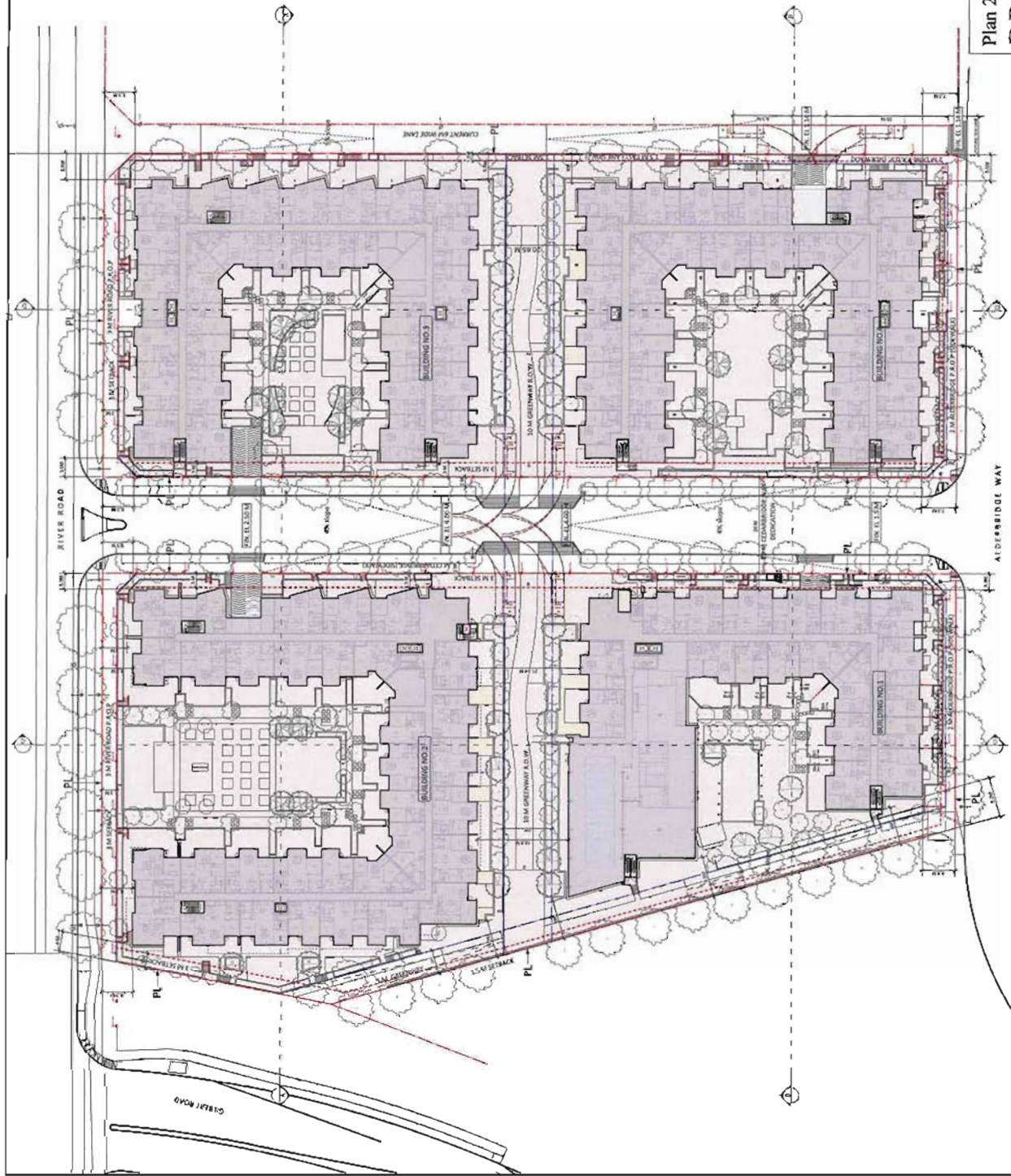
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PROJECT	12-615424
CLIENT	YAMAMOTO ARCHITECTURE INC.
LOCATION	1000 15TH STREET, SUITE 100, SAN FRANCISCO, CA 94103
SCALE	AS SHOWN
DESIGNER	SHARP & DIAMOND
DATE	09/04/12

LANDSCAPE SECTIONS

L3.2



KEY PLAN



LEGEND:
 PL - PROPERTY LINE AFTER
 CEMETARYWAY ACQUISITION
 VERIFICATION

Plan 2 Sept 4 2012
 DP 12-615424

PROJECT	APARTMENT DEVELOPMENT
CLIENT	YANAMOLO ARCHITECTURE INC.
DESIGNER	RIVA-ARCHITECTS INC.
DATE	SEPTEMBER 4, 2012
SCALE	1:1000
PROJECT NO.	12-615424
PROJECT NAME	APARTMENT DEVELOPMENT
PROJECT ADDRESS	1000 RIVER ROAD, WILLOWDALE, ONTARIO M2H 1A1
PROJECT OWNER	YANAMOLO ARCHITECTURE INC.
PROJECT CONTACT	YANAMOLO ARCHITECTURE INC.
PROJECT PHONE	416-291-1111
PROJECT FAX	416-291-1112
PROJECT EMAIL	info@yanamolo.com
PROJECT WEBSITE	www.yanamolo.com

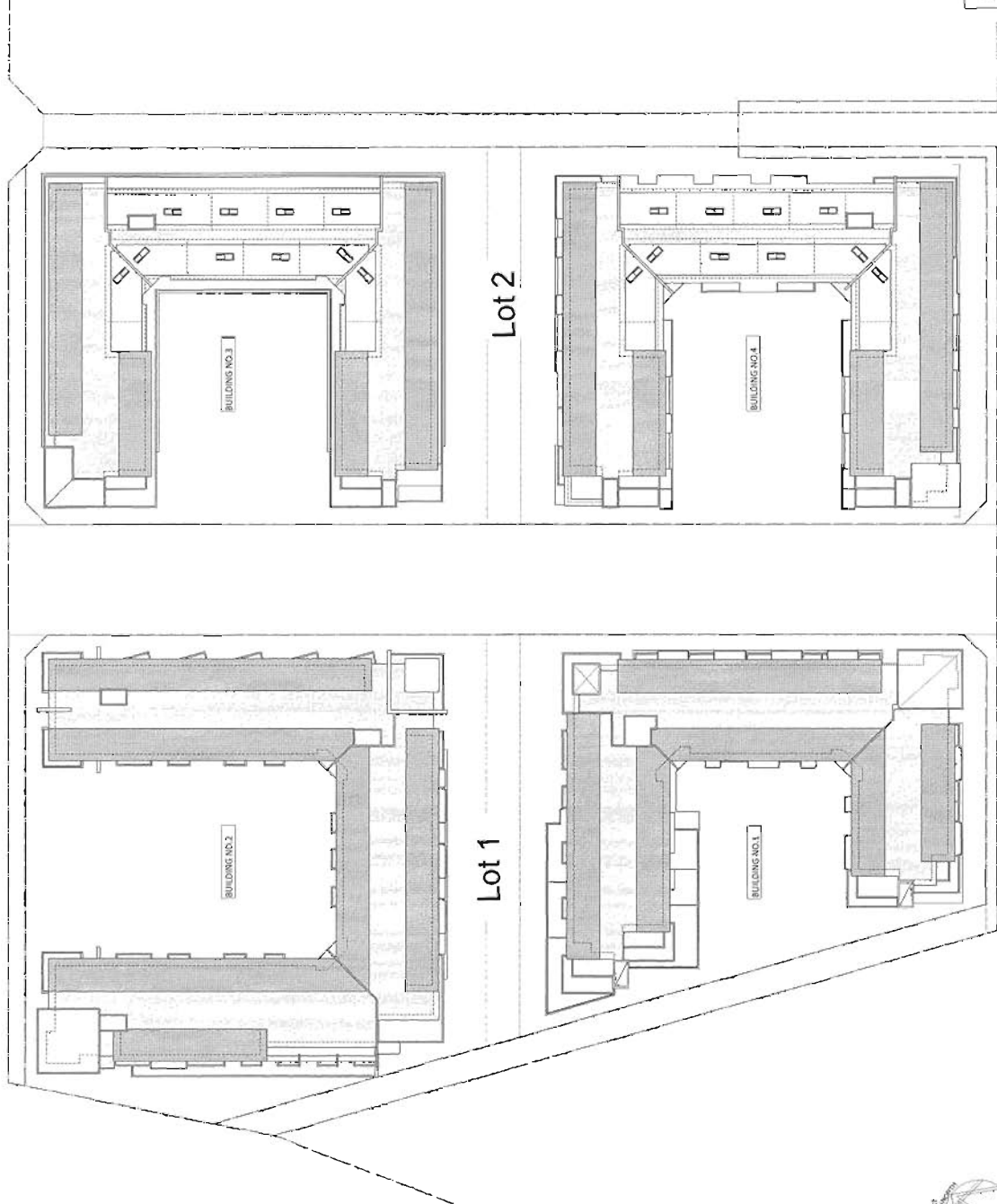
PROJECT
 APARTMENT DEVELOPMENT

RIVA-
 ARCHITECTS INC.

Yanamolo
 Architecture Inc.

1000 RIVER ROAD, WILLOWDALE, ONTARIO M2H 1A1
 PROJECT NO. 12-615424
 PROJECT NAME
 APARTMENT DEVELOPMENT

SCALE	1:1000
DATE	SEPTEMBER 4, 2012
PROJECT NO.	12-615424
PROJECT NAME	APARTMENT DEVELOPMENT
PROJECT ADDRESS	1000 RIVER ROAD, WILLOWDALE, ONTARIO M2H 1A1
PROJECT OWNER	YANAMOLO ARCHITECTURE INC.
PROJECT CONTACT	YANAMOLO ARCHITECTURE INC.
PROJECT PHONE	416-291-1111
PROJECT FAX	416-291-1112
PROJECT EMAIL	info@yanamolo.com
PROJECT WEBSITE	www.yanamolo.com



Plan 4 Sept 4 2012
 DP 12-615424

NO.	DATE	REVISION
1	NOV 15 2011	REVISED FOR PERMITS
2	NOV 15 2011	REVISED FOR PERMITS
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APARTMENT DEVELOPMENT
 RIVA
 1711 ALABAMA AVE
 RICHMOND, VA 23219

Yamamoto
 Architecture Inc.

1711 ALABAMA AVE, N.E.
 RICHMOND, VA 23219
 PROJECT TITLE
 ROOF PLAN

SCALE	1" = 32'
DATE	NOV 15 2011
DRAWN	AM
CHECKED	AM
PROJECT NO.	12-615424
SHEET NO.	00
DATE	NOV 15 2011
SCALE	1" = 32'



East Lane Elevation Showing Section Locations on A3.05 / A3.06

Yamamoto Architects

Plan 5

Sept 4 2012

DP 12-615424

EL ELEVATIONS

GILBERT / WALKWAY & LANE

DATE 9-4-12

SCALE 1/8" = 1'-0"

PROJECT A3.04

OWNER 3M

DESIGNER YAMAMOTO ARCHITECTS

DATE 9-4-12

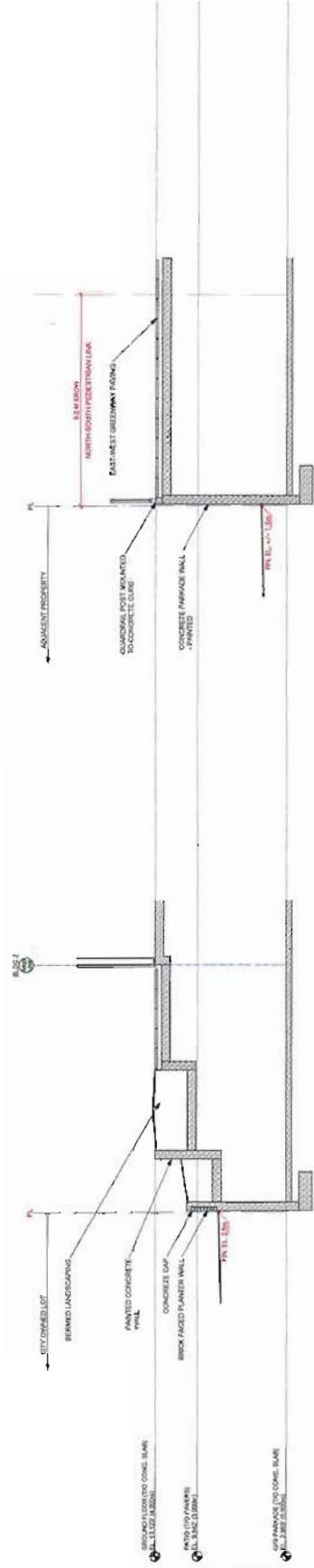
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PROJECT A3.04

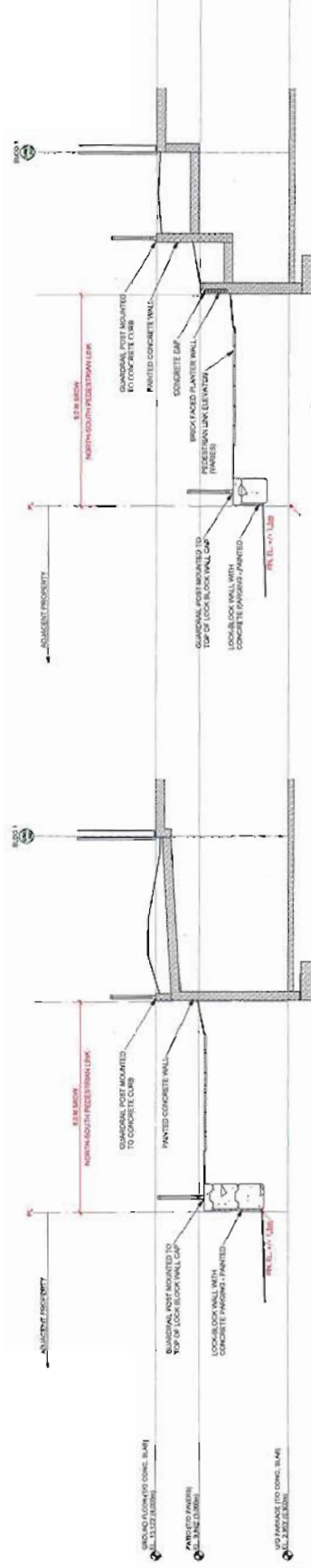
OWNER 3M

DESIGNER YAMAMOTO ARCHITECTS

NO.	DATE	BY	CHKD	DESCRIPTION
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2	09/04/12	YAMAMOTO	YAMAMOTO	REVISION

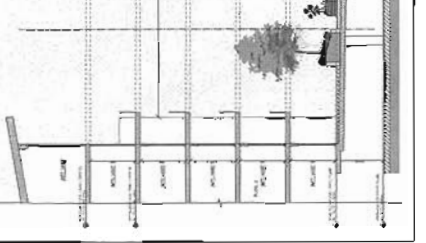
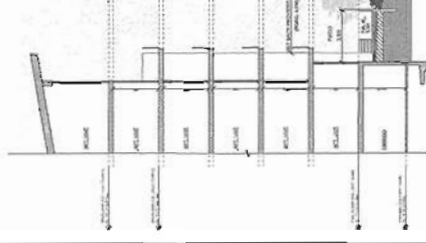


SECTION G - THRU EAST-WEST GREENWAY
SCALE 1/4" = 1'-0"



SECTION I - THRU EAST-WEST GREENWAY
SCALE 1/4" = 1'-0"

PROJECT		Yamamoto Archib	PROJECT	REPAIR/REPLACE PEDESTRIAN FARM		
DESIGNED		DATE	DRAWN			
CHECKED		DATE	APPROVED			
PROJECT TITLE		ELEVATIONS				
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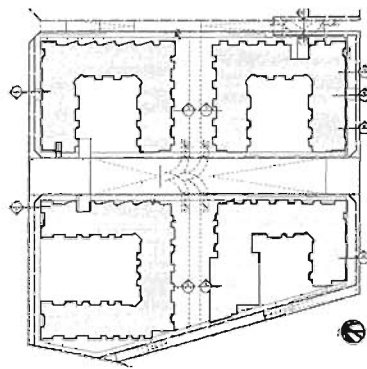


PRO-557

**Yamamoto
Architecture Inc.**

Plan 8 Sept 4 2012
DP 12-615424

SHEET NO.	A0.8	D0
DATE	JAN. 1977	
DESIGNER		
CHECKED		1104



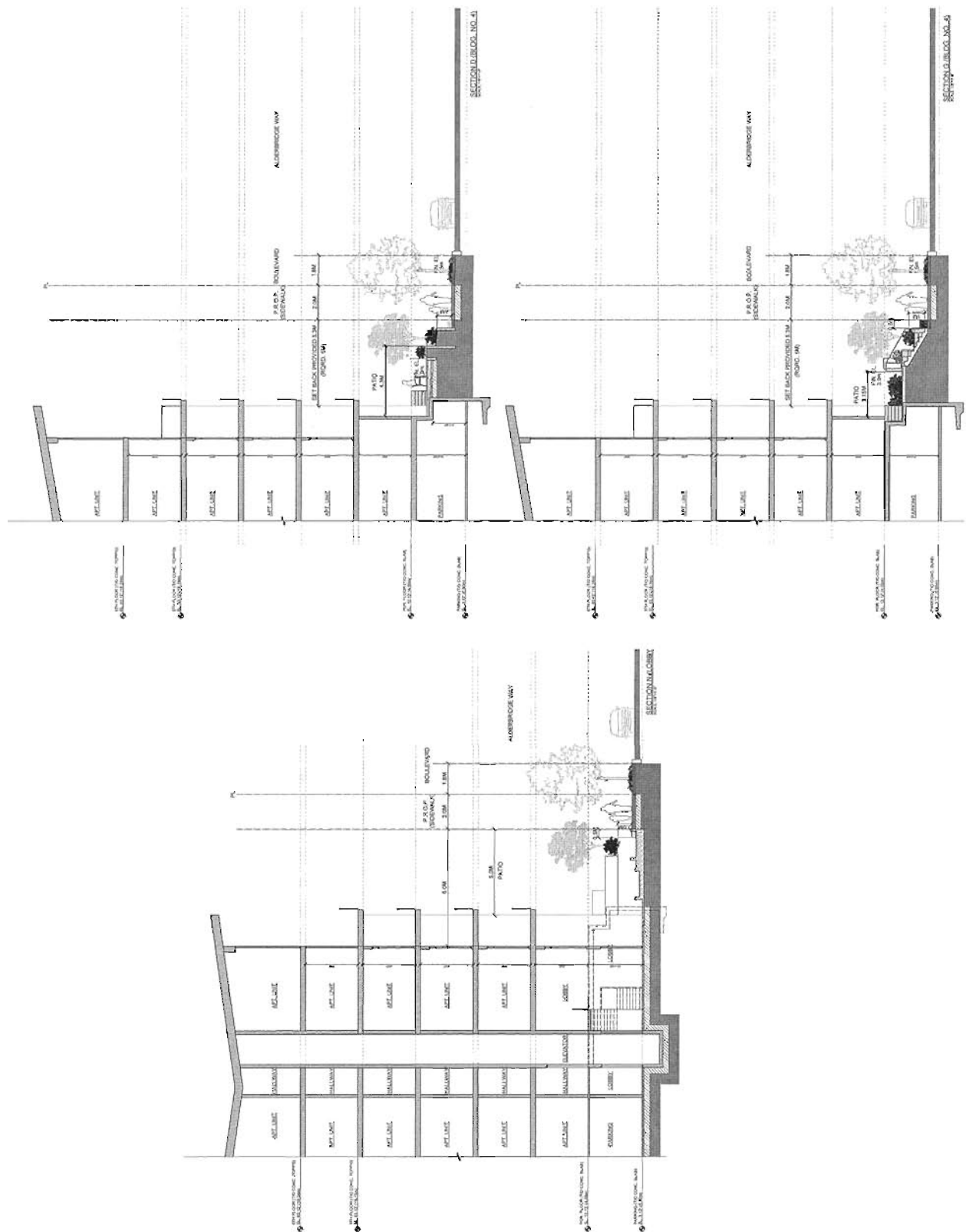
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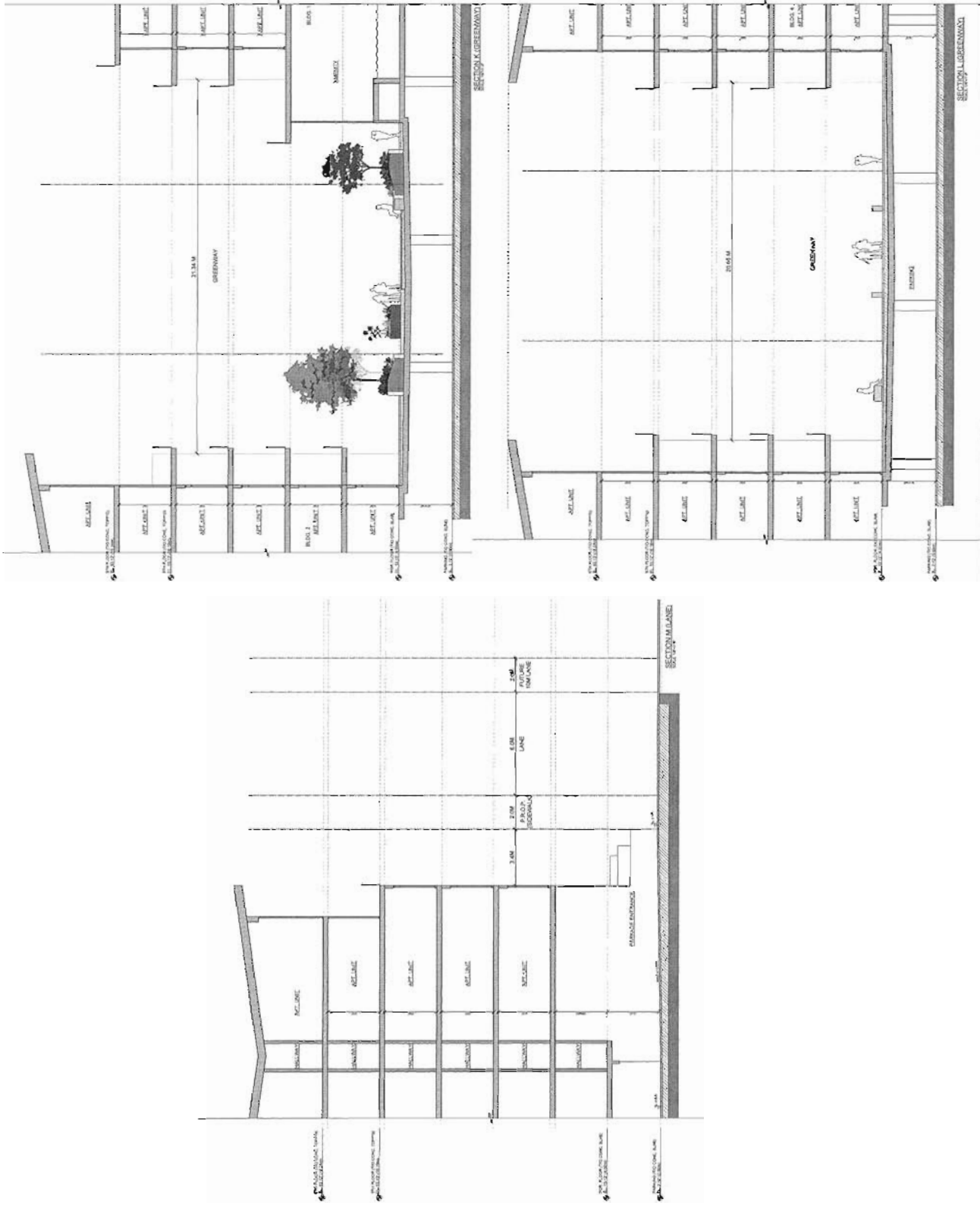
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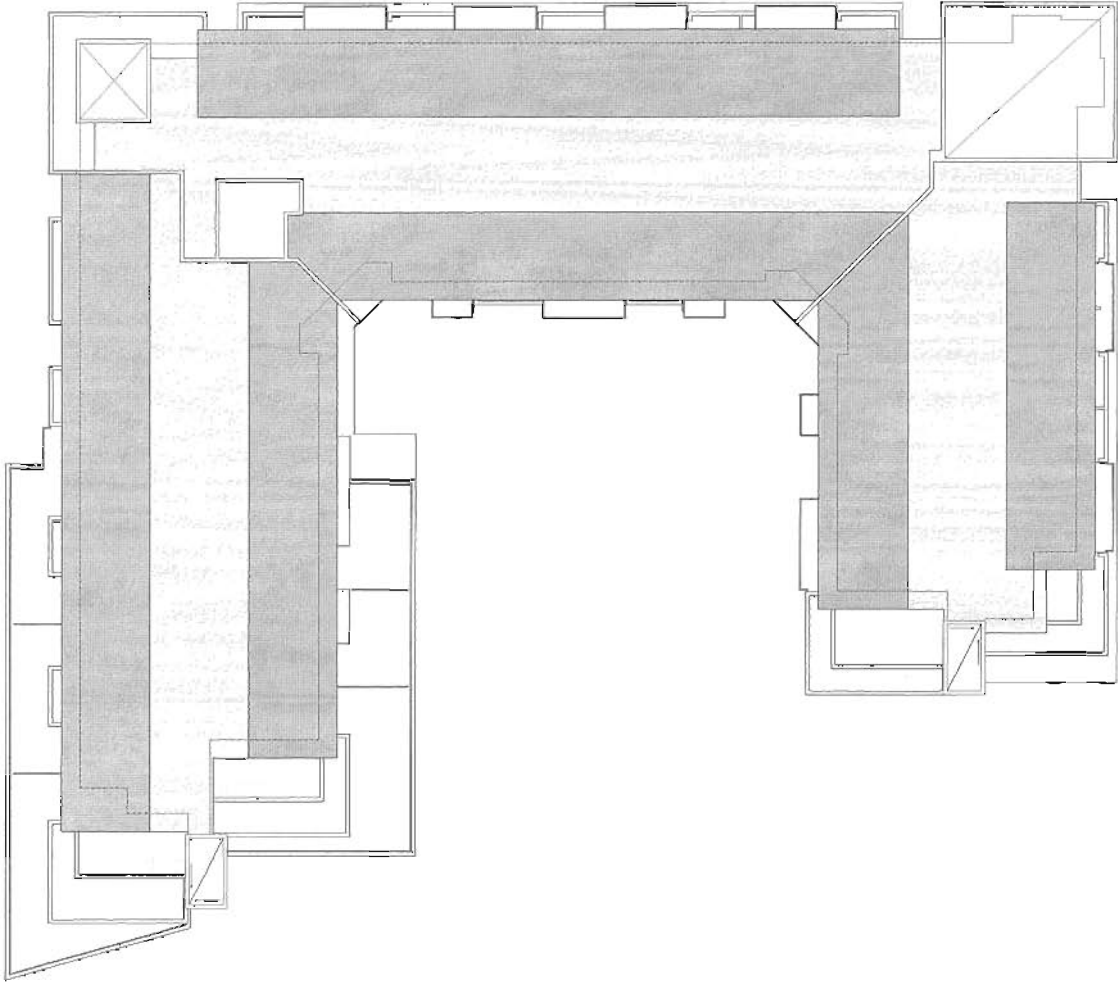
Yamamoto
Architecture Inc.

Plan 10 Sept 4 2012
DP 12-615424

SCALE	DATE	REVISION
1:100	10/10/12	ISSUED FOR PERMIT
1:100	10/10/12	ISSUED FOR PERMIT
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Plan 19 Sept 4 2012
 DP 12-615424

NO.	DATE	REVISION
1	NOV 14 2012	ISSUED FOR PERMITTING
2	MAY 11 2012	ISSUED FOR PERMITTING
3	FEB 8 2012	ISSUED FOR PERMITTING
4	JAN 11 2012	ISSUED FOR PERMITTING
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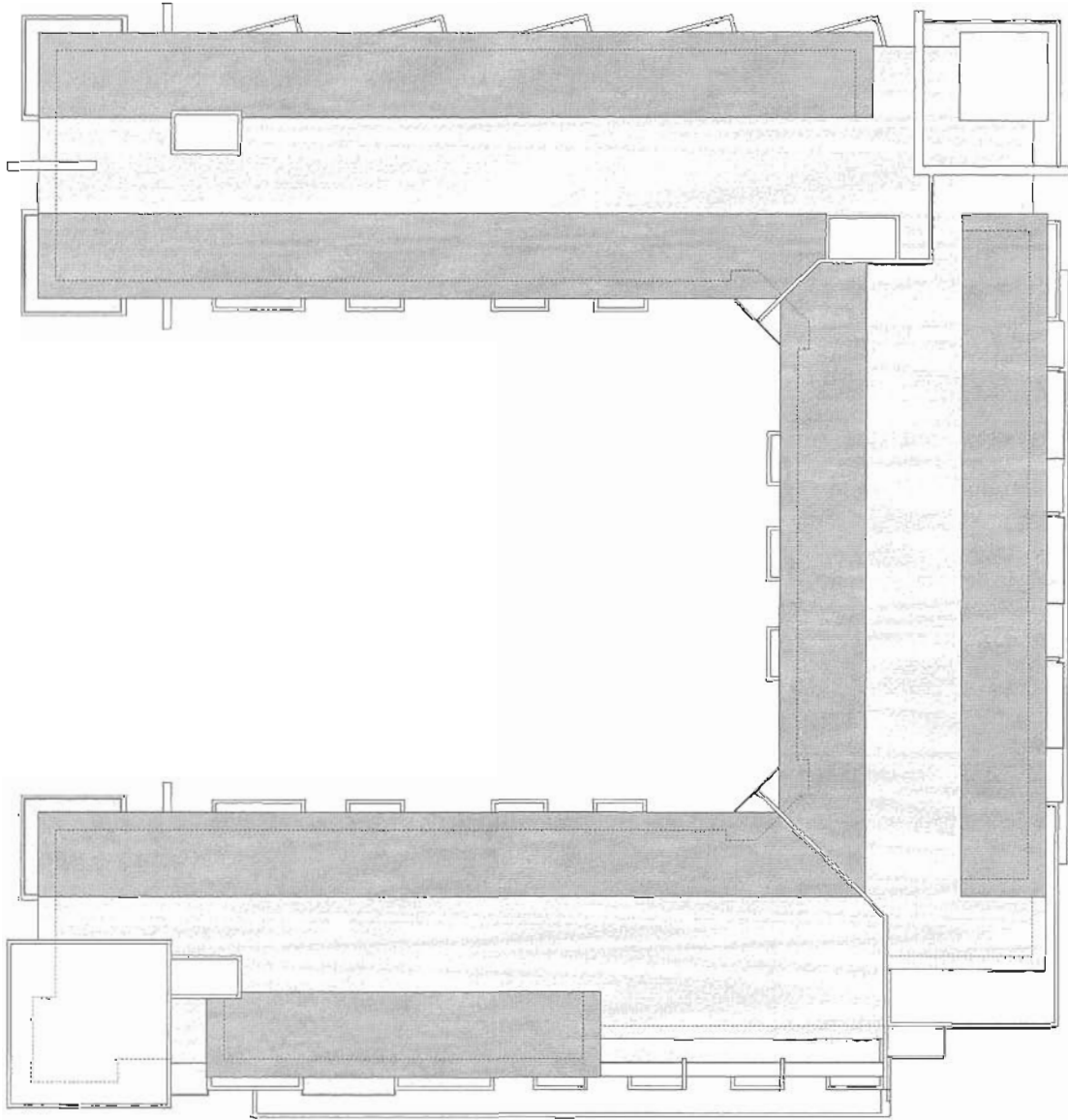
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 7701 17TH AVENUE, SUITE 100
 ROCKWOOD, IL 60087

Yamamoto
 Architecture Inc.

7701 17TH AVENUE, SUITE 100
 ROCKWOOD, IL 60087
 PROJECT: APARTMENT DEVELOPMENT
 SHEET NO: A1-1.7
 DATE: SEP 4 2012
 DRAWN BY: JAM

SCALE	1/8" = 1'-0"
DATE	SEP 4 2012
BY	JAM
CHECKED	JAM
APPROVED	JAM
PROJECT NO.	1100



Plan 26 Sept 4 2012
 DP 12-615424

DATE	2012.09.04
PROJECT	12-615424
CLIENT	YAMAMOTO ARCHITECTURE INC.
DESIGNER	YAMAMOTO ARCHITECTURE INC.
ARCHITECT	YAMAMOTO ARCHITECTURE INC.
ENGINEER	YAMAMOTO ARCHITECTURE INC.
PLANNING	YAMAMOTO ARCHITECTURE INC.
CONSTRUCTION	YAMAMOTO ARCHITECTURE INC.

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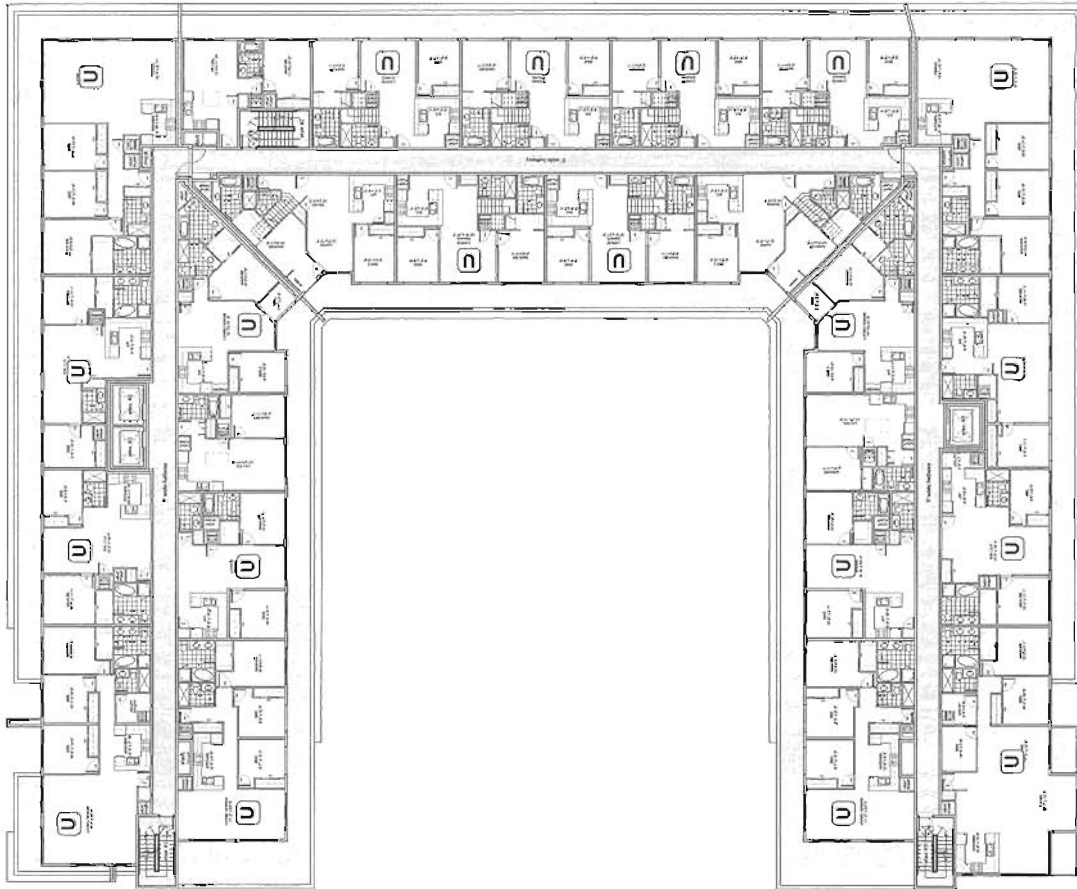
Yamamoto
 Architecture Inc.

100% OFFICE BUILDING
 100% OFFICE BUILDING
 100% OFFICE BUILDING

DATE	2012.09.04
PROJECT	12-615424
CLIENT	YAMAMOTO ARCHITECTURE INC.
DESIGNER	YAMAMOTO ARCHITECTURE INC.
ARCHITECT	YAMAMOTO ARCHITECTURE INC.
ENGINEER	YAMAMOTO ARCHITECTURE INC.
PLANNING	YAMAMOTO ARCHITECTURE INC.
CONSTRUCTION	YAMAMOTO ARCHITECTURE INC.

BUILDING 3 - 5TH FLOOR

UNIT COUNT	
1st	UNIT COUNTS
2nd	UNIT COUNTS
3rd	UNIT COUNTS
4th	UNIT COUNTS
5th	UNIT COUNTS
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47th	UNIT COUNTS
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51st	UNIT COUNTS
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UNIT COUNT	
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 7741 7TH ALPINE DRIVE
 IRVING, TEXAS 76039

Yamamoto
 Architecture Inc.

DATE: 09/04/12
 BY: J. YAMAMOTO
 DRAWING TITLE:
 FIFTH FLOOR PLAN
 BUILDING 3

UNIT COUNT	
1st	UNIT COUNTS
2nd	UNIT COUNTS
3rd	UNIT COUNTS
4th	UNIT COUNTS
5th	UNIT COUNTS
6th	UNIT COUNTS
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100th	UNIT COUNTS

Plan 30 Sept 4 2012
 DP 12-615424



DATE	TIME	DATE TIME	
08-17	11:19	AUG 17 2011	A1-3.7
SITE			00
CHECKED		JRILL ME	YSLF

1348	UNIT COUNTS	1	(ACCESSIBLE)	11
1349	UNIT COUNTS	25	(ACCESSIBLE)	294
1350	UNIT COUNTS	0	(ACCESSIBLE)	48
TOTAL	UNIT COUNTS	31		453

WILSON AREA

SHOWS FLOOR AREA	26,487 SQ FT
SHOWS FLOOR AREA	64,132 SQ FT
REDUCTIONS	
PROPOSED FL. AREA	27,344.5 SQ FT

WILSON AREA

1348	UNIT COUNTS	131	(ACCESSIBLE)	131
1349	UNIT COUNTS	25	(ACCESSIBLE)	25
1350	UNIT COUNTS	0	(ACCESSIBLE)	0
TOTAL	UNIT COUNTS	31	(ACCESSIBLE)	31

WILSON AREA

1351	SHOWS FLOOR AREA	26,487.5 SQ FT
1352	SHOWS FLOOR AREA	6,813.50 SQ FT
1353	PROPOSED FL. AREA	27,344.5 SQ FT

WILSON AREA

NO.	DATE	REVISIONS
	FEB 8, 2013	REWORK FOR IN-ARMED COMPLIANCE RECORD
	MAY 11, 2013	REWORK FOR COMPLIANCE RECORD
	AUG. 29, 2013	REWORK FOR COMPLIANCE RECORD

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help and the user's own will reflect the self-improvement of the user.

PROJECT

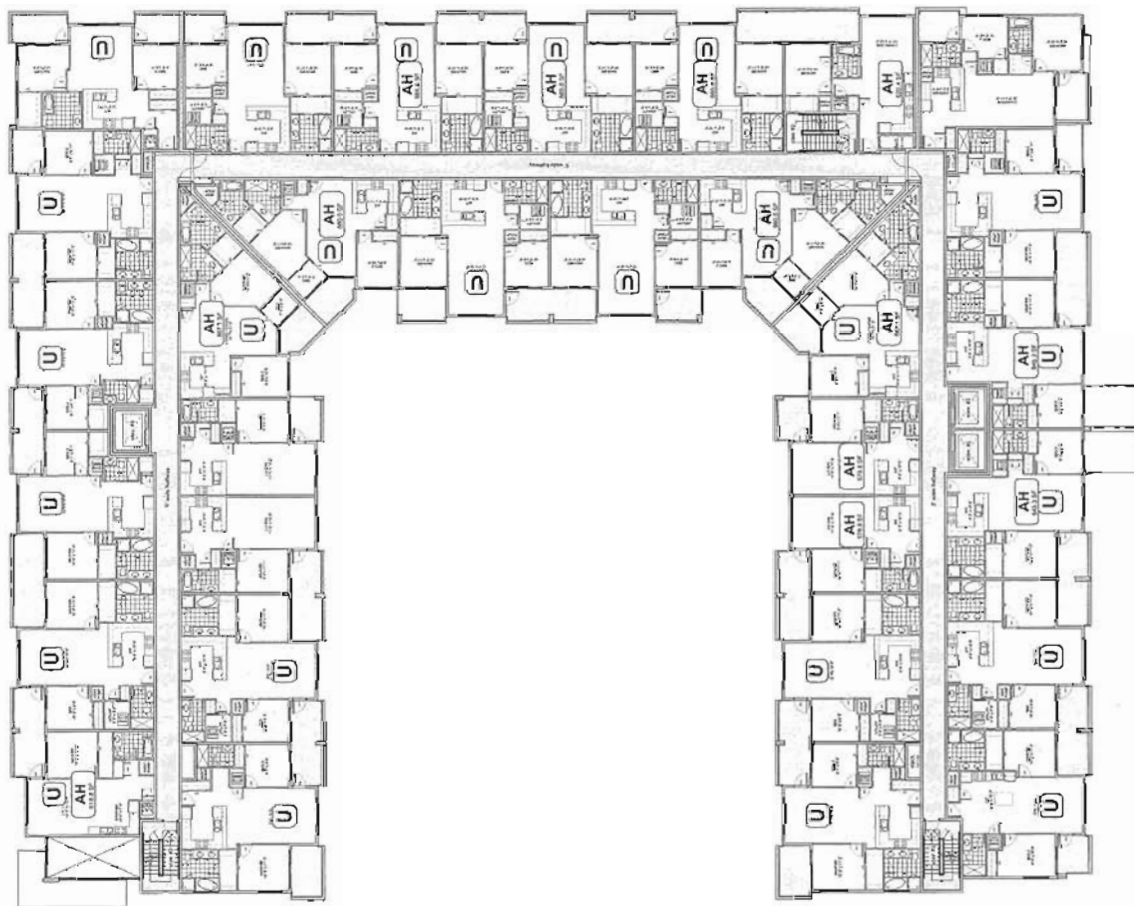
ADAPTATION DEVELOPMENT

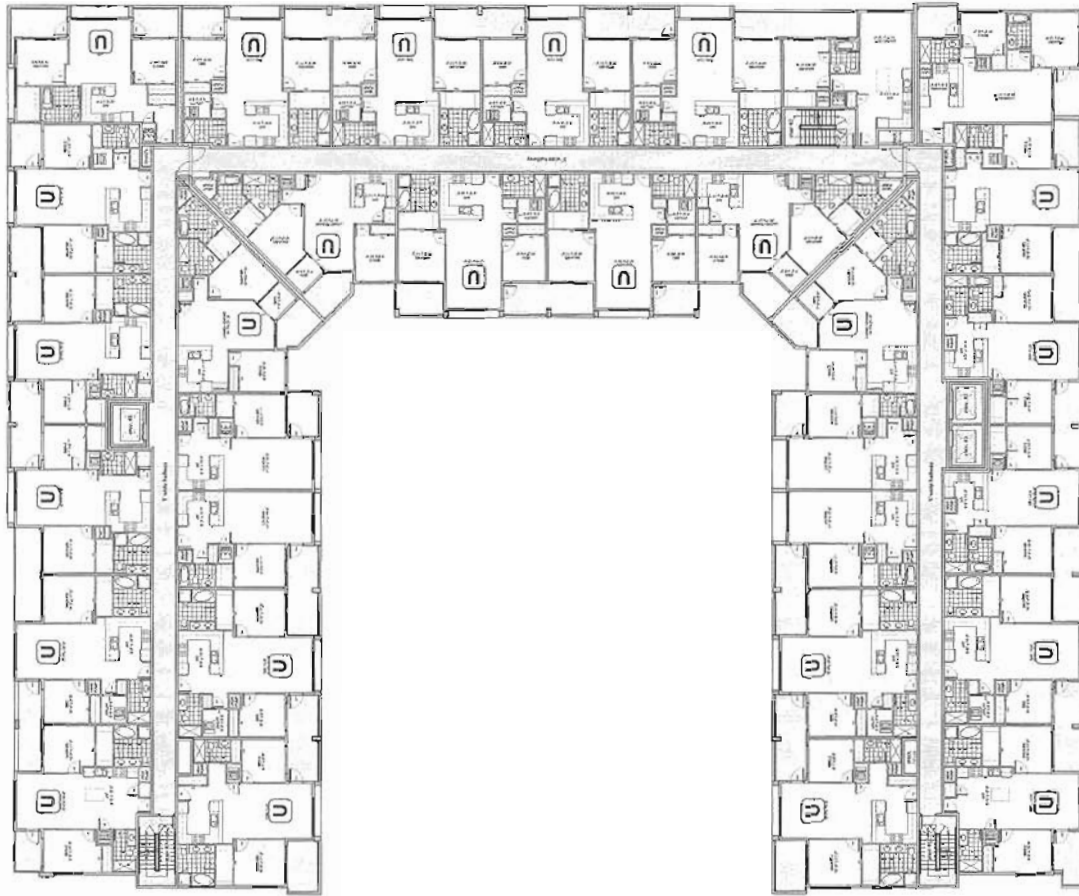
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7731, 7771 ALDERBROOK WAY
MONTCLAIR, NJ 07042

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Architecture Inc.

2025 RELEASE UNDER E.O. 14176
 TITLE PAGE
 SECOND FLOOR PLAN
 IN THE PENDING

DATE	1-18-88	INSTR NO	
DATE	NOV 21 1987		A14.2
NAME			RD
CHECKED		INSTR NO	1184

Plan 34 Sept 4 2012
DP 12-615424



BUILDING 4 - 4TH FLOOR - UNIT AREA		
100	UNIT COUNTS	INACCESSIBLE
200	UNIT COUNTS	INACCESSIBLE
300	UNIT COUNTS	INACCESSIBLE
TOTAL	IN	PRE
FLOOR AREA		
SPOKE FLOOR AREA		26,736.4 SQFT
TRUCKS		843.1 SQFT
PROPERTY AREA		37,732.2 SQFT
APPROXIMATE UNITS		

FLOOR AREA	
GROSS FLOOR AREA	26,375.4 SQFT
FUNCTIONS	842.8 SQFT
PROPOSED PL. AREA	37,192.3 SQFT

ADDITIONAL UNITS	
COUNT	0

AFFORDABLE UNITS
Controlled

NO.	DATE	REVISIONS
	FEB 8, 2012	DESIGNED FOR PLANNING COMMITTEE REVIEW
	MAY 1, 2012	DESIGNED FOR PLANNING COMMITTEE REVIEW
	AUG 29, 2012	DESIGNED FOR PLANNING COMMITTEE REVIEW

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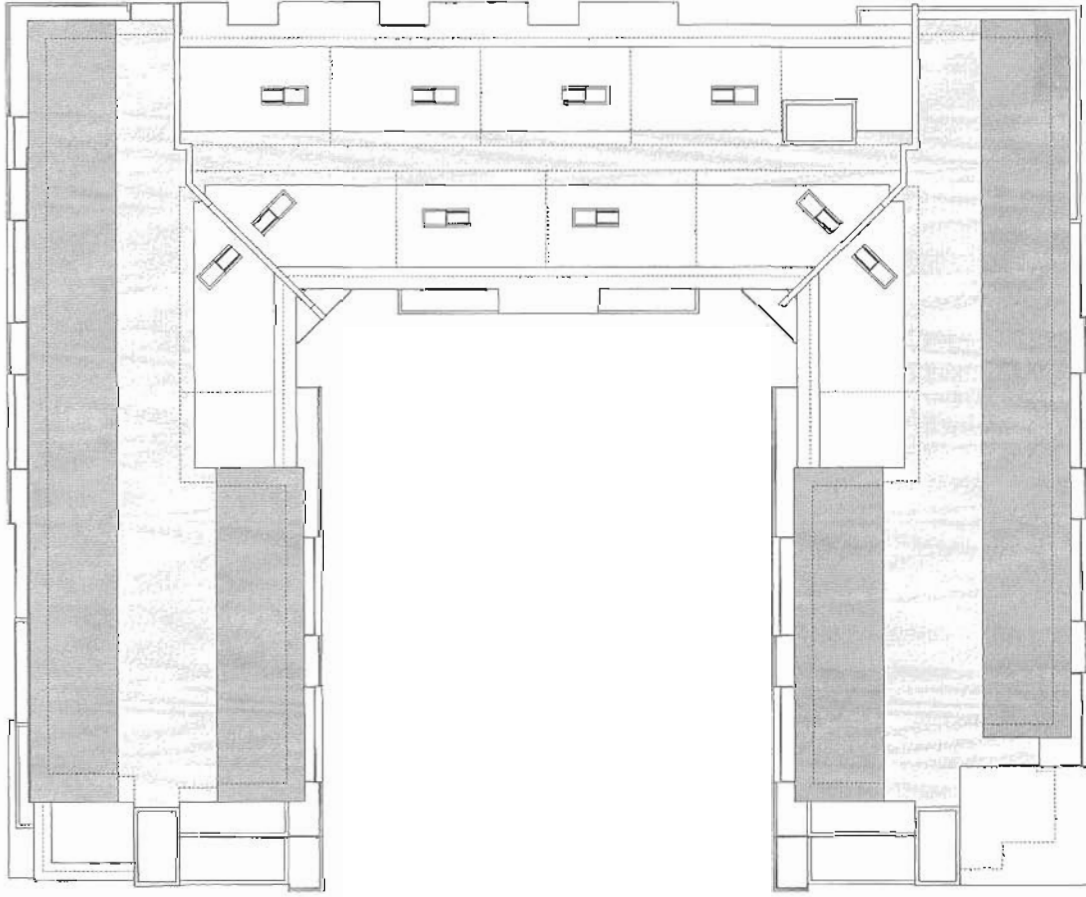
2008 and 2009, Inc., N.Y.
9041 41st St. 10464-7917 Tel: 718-776-1227

Colloquial Title

**FOURTH FLOOR PLAN
BUILDING 4**

Plan 36 Sept 4 2012
DP 12-615424

SCALE	1:500	SHEET NO.	A1-4.4
DATE	MAY 29, 2012	PROJECT NO.	00
DRAWN			



Plan 39 Sept 4 2012
 DP 12-615424

DATE	10/23/12
BY	YAMAMOTO
PROJECT	APARTMENT DEVELOPMENT
LOCATION	1001 37TH ALEXANDRIA DRIVE NORFOLK, VA
OWNER	YAMAMOTO ARCHITECTURE INC.
SCALE	AS SHOWN
NOTES	SEE SHEET 1001 FOR DETAILS

YAMAMOTO ARCHITECTURE INC.
 1001 37TH ALEXANDRIA DRIVE
 NORFOLK, VA 23502
 (757) 644-1111
 WWW.YAMAMOTO-VA.COM

RIVA
 1001 37TH ALEXANDRIA DRIVE
 NORFOLK, VA

Yamamoto
 Architecture Inc.

1001 37TH ALEXANDRIA DRIVE
 NORFOLK, VA 23502
 (757) 644-1111
 WWW.YAMAMOTO-VA.COM

1001 37TH ALEXANDRIA DRIVE
 NORFOLK, VA 23502
 (757) 644-1111
 WWW.YAMAMOTO-VA.COM

DATE	10/23/12
BY	YAMAMOTO
PROJECT	APARTMENT DEVELOPMENT
LOCATION	1001 37TH ALEXANDRIA DRIVE NORFOLK, VA
OWNER	YAMAMOTO ARCHITECTURE INC.
SCALE	AS SHOWN
NOTES	SEE SHEET 1001 FOR DETAILS



NOTES



SITE LOCATION

- DRAWING LEGEND**
- L1.1 LANDSCAPE SITE PLAN
 - L1.2 LANDSCAPE DETAIL PLAN - BUILDING 4
 - L1.3 LANDSCAPE DETAIL PLAN - BUILDING 1
 - L1.4 LANDSCAPE DETAIL PLAN - BUILDING 2
 - L1.5 LANDSCAPE DETAIL PLAN - BUILDING 3
 - L1.6 EAST / WEST COMMUNITY GARDEN GREENWAY
 - L1.7 FUTURE LANE CONDITION
 - L1.8 FUTURE LANE CONDITION - GREEN OPEN SPACE
 - L1.9 ON-SITE PERMEABILITY CALCULATIONS
 - L1.10 CENTRAL PLAZA DETAIL PLAN
 - L2.1 PLANT MATERIALS
 - L3.1 LANDSCAPE SECTIONS
 - L3.2 LANDSCAPE SECTIONS
 - L3.3 LANDSCAPE SECTIONS AND DETAILS
 - L4.1 PRECEDENT IMAGES
 - L5.1 PLAY AREA ELEMENTS
 - L5.2 LANDSCAPE DETAILS



SEE TERRACING AND WALLS AT WEST AND EAST SIDES OF THE BUILDINGS FOR ARCHITECTURAL DRAWINGS

BUILDINGS 1, 2, 3, 4
BUILDINGS 1, 2, 3, 4
BUILDINGS 1, 2, 3, 4
BUILDINGS 1, 2, 3, 4
OVERALL AREA

NO.	DESCRIPTION	DATE	BY	CHKD.
1	LANDSCAPE SITE PLAN	09/04/2012	YAMAMOTO	YAMAMOTO
2	LANDSCAPE DETAIL PLAN - BUILDING 4	09/04/2012	YAMAMOTO	YAMAMOTO
3	LANDSCAPE DETAIL PLAN - BUILDING 1	09/04/2012	YAMAMOTO	YAMAMOTO
4	LANDSCAPE DETAIL PLAN - BUILDING 2	09/04/2012	YAMAMOTO	YAMAMOTO
5	LANDSCAPE DETAIL PLAN - BUILDING 3	09/04/2012	YAMAMOTO	YAMAMOTO
6	EAST / WEST COMMUNITY GARDEN GREENWAY	09/04/2012	YAMAMOTO	YAMAMOTO
7	FUTURE LANE CONDITION	09/04/2012	YAMAMOTO	YAMAMOTO
8	FUTURE LANE CONDITION - GREEN OPEN SPACE	09/04/2012	YAMAMOTO	YAMAMOTO
9	ON-SITE PERMEABILITY CALCULATIONS	09/04/2012	YAMAMOTO	YAMAMOTO
10	CENTRAL PLAZA DETAIL PLAN	09/04/2012	YAMAMOTO	YAMAMOTO
11	PLANT MATERIALS	09/04/2012	YAMAMOTO	YAMAMOTO
12	LANDSCAPE SECTIONS	09/04/2012	YAMAMOTO	YAMAMOTO
13	LANDSCAPE SECTIONS	09/04/2012	YAMAMOTO	YAMAMOTO
14	LANDSCAPE SECTIONS AND DETAILS	09/04/2012	YAMAMOTO	YAMAMOTO
15	PRECEDENT IMAGES	09/04/2012	YAMAMOTO	YAMAMOTO
16	PLAY AREA ELEMENTS	09/04/2012	YAMAMOTO	YAMAMOTO
17	LANDSCAPE DETAILS	09/04/2012	YAMAMOTO	YAMAMOTO



SHARP & DIAMOND
LANDSCAPE ARCHITECTS

Yamamoto
Architecture Inc.

1000-427 704-947-7917
1000-427 704-947-7917

RIVA

onni
COMMERCIAL DEVELOPMENT

LANDSCAPE ARCHITECTS

LANDSCAPE ARCHITECTS

LANDSCAPE ARCHITECTS

LANDSCAPE ARCHITECTS

LANDSCAPE ARCHITECTS

Plan 40 Sept 4 2012
DP 12-615424

L1.1

LANDSCAPE SITE PLAN



[illegible]

SHARP & DIAMOND
LONDON & AUSTRIA LTD

**Yamamoto
Architecture Inc.**

3306 north street, nashua, n.h.
 03041 tel: 603-731-1121 fax: 603-731-1277

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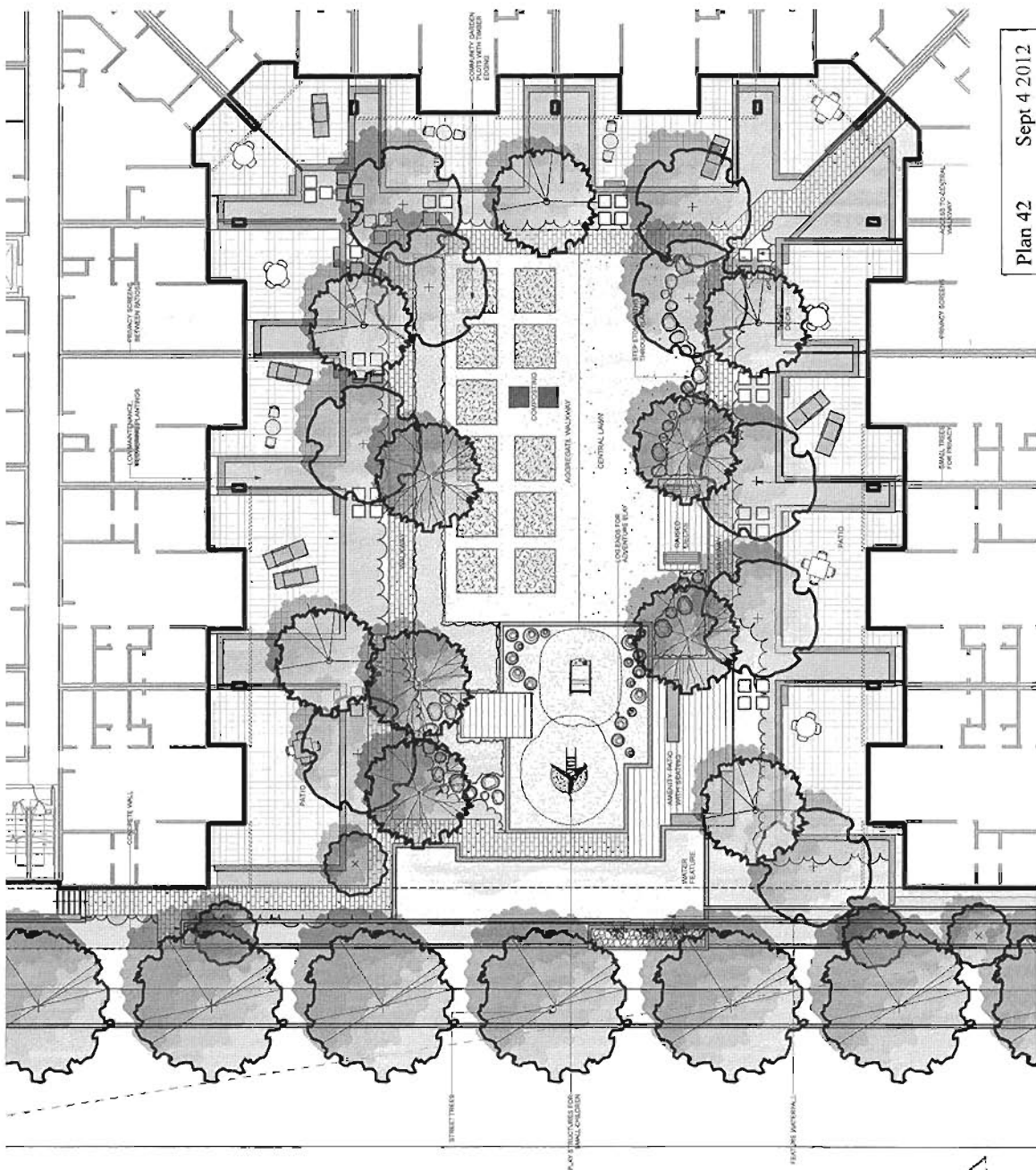
Omni

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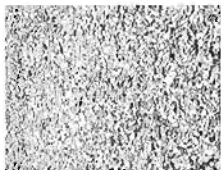
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BUILDING 4
COURTYARD
DETAIL PLAN

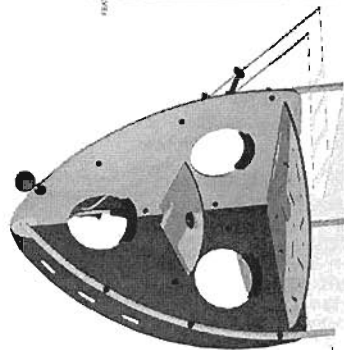
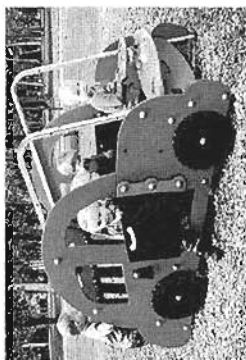
L1.3



Plan 42 Sept 4 2012
DP 12-615424



WOOD FIBRE FALL SURFACING



KOMPAN CLIMBY STRUCTURE

[illegible]

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 20000 p.m. of road, Washington, D.C.

RIVA

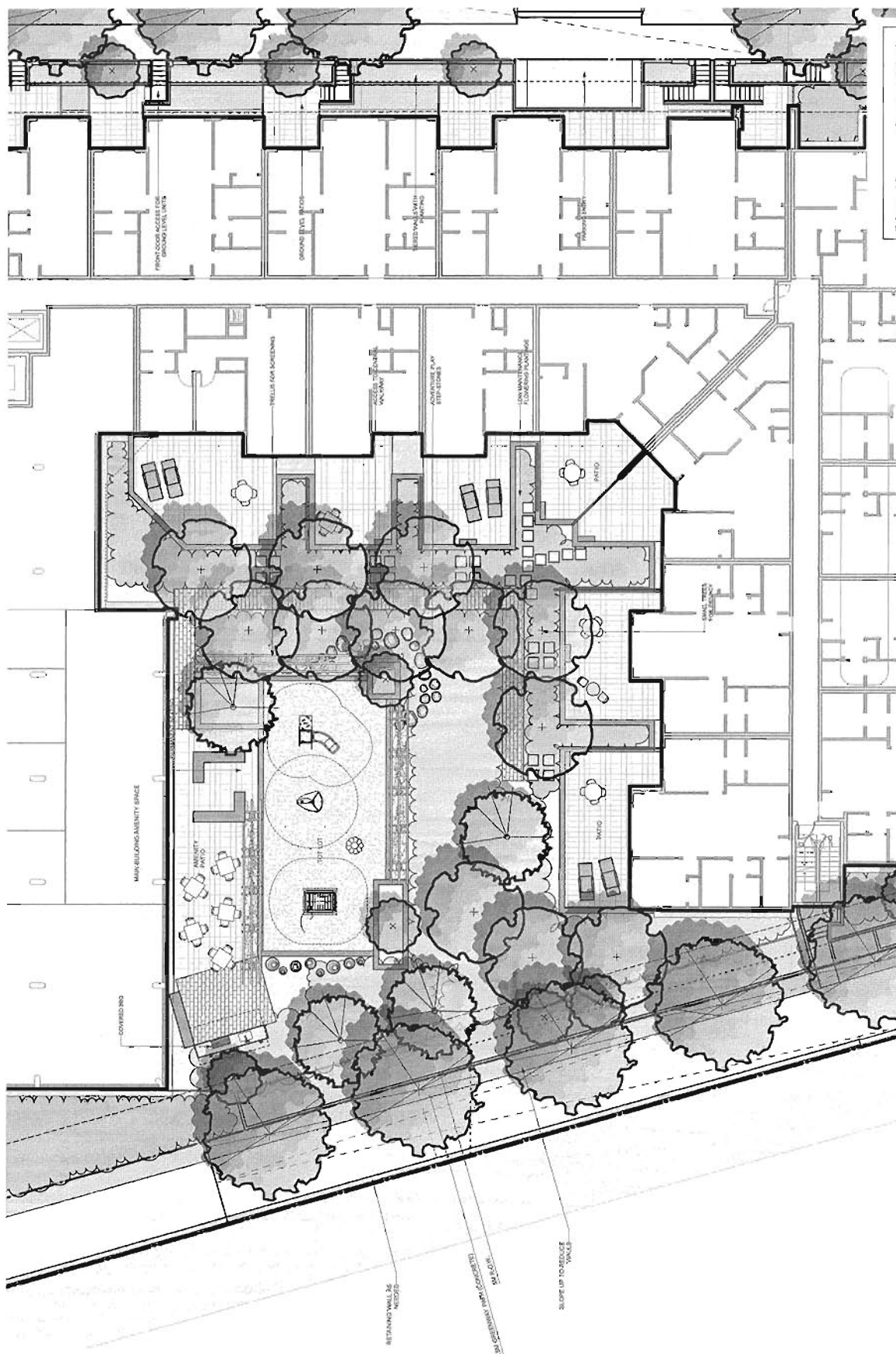


phases (B/C) (Schneider and Smith 1970). However, the two well-known examples (B/C) (Schneider and Smith 1970) are not the only ones. In fact, there are many other examples (B/C) (Schneider and Smith 1970) that are not well known.

Question	Score
1. Which of the following is a function of the skeletal system?	100
2. Which of the following is a function of the skeletal system?	100
3. Which of the following is a function of the skeletal system?	100
4. Which of the following is a function of the skeletal system?	100
5. Which of the following is a function of the skeletal system?	100

**BUILDING 1
COURTYARD
DETAIL PLAN**

L1.4



Plan 43 Sept 4 2012
DP 12-615424



Sl. No.	Category	Reference
1	General	1. General
2	General	2. General
3	General	3. General
4	General	4. General
5	General	5. General
6	General	6. General
7	General	7. General
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34	General	34. General
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95	General	95. General
96	General	96. General
97	General	97. General
98	General	98. General
99	General	99. General
100	General	100. General



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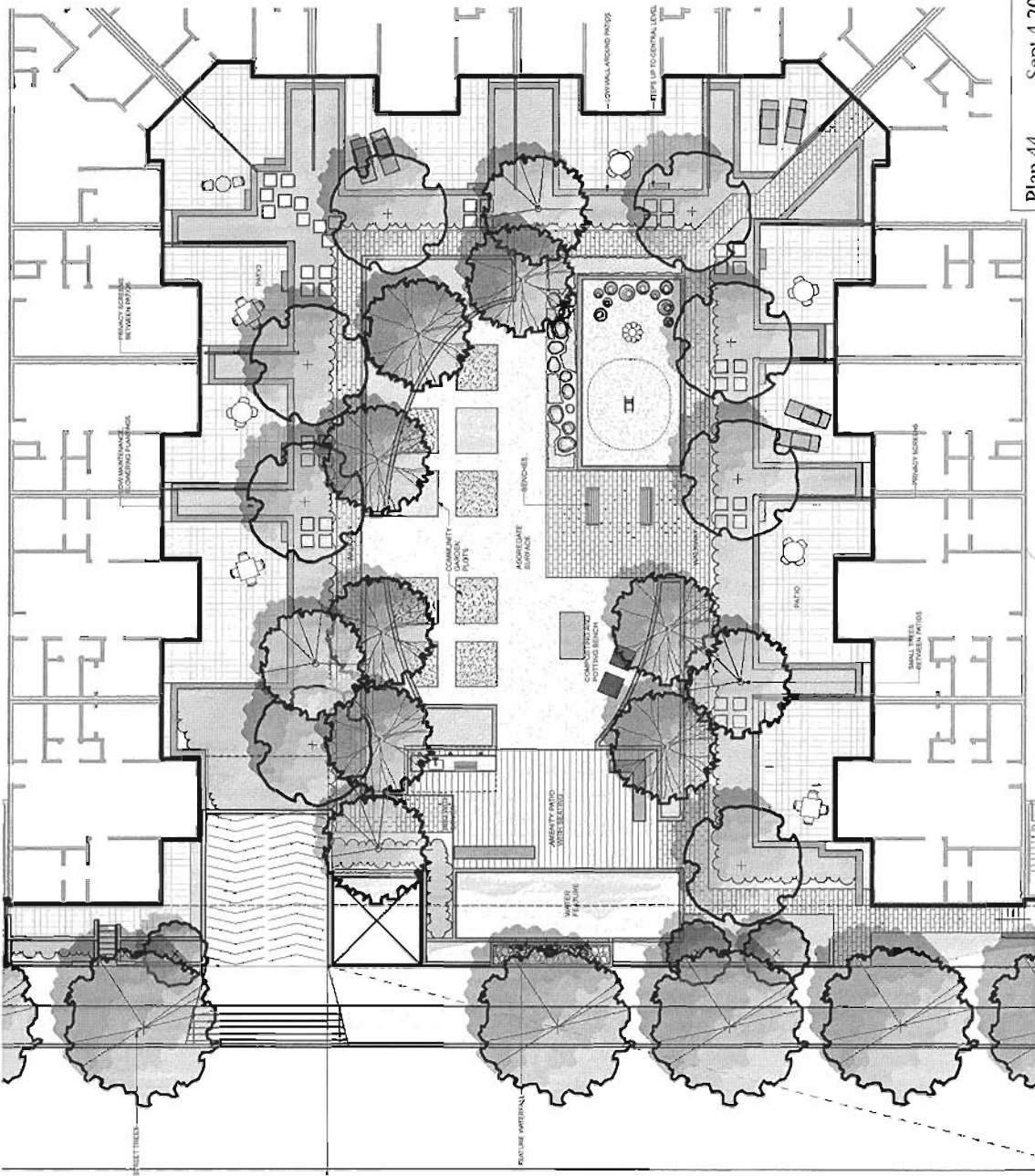
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INTEGRITY

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Sample 2	1.00	1.00
Sample 3	1.00	1.00
Sample 4	1.00	1.00
Sample 5	1.00	1.00
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Sample 99	1.00	1.00
Sample 100	1.00	1.00

BUILDING 3
COURTYARD
DETAIL PLAN

L1.5

Plan 44 Sept 4 2012
DP 12-615424

COURTYARD GARDENING PRECEDENTS



Activity	Category	Frequency	Duration	Location	Notes
1. Review of the previous week's work	Review	1 time	10 min	Classroom	
2. Introduction of the new topic	Introduction	1 time	10 min	Classroom	
3. Presentation of the new topic	Presentation	1 time	10 min	Classroom	
4. Practice of the new topic	Practice	1 time	10 min	Classroom	
5. Summary of the new topic	Summary	1 time	10 min	Classroom	
6. Homework assignment	Homework	1 time	10 min	Classroom	
7. Review of the homework assignment	Review	1 time	10 min	Classroom	
8. Introduction of the new topic	Introduction	1 time	10 min	Classroom	
9. Presentation of the new topic	Presentation	1 time	10 min	Classroom	
10. Practice of the new topic	Practice	1 time	10 min	Classroom	
11. Summary of the new topic	Summary	1 time	10 min	Classroom	
12. Homework assignment	Homework	1 time	10 min	Classroom	
13. Review of the homework assignment	Review	1 time	10 min	Classroom	
14. Introduction of the new topic	Introduction	1 time	10 min	Classroom	
15. Presentation of the new topic	Presentation	1 time	10 min	Classroom	
16. Practice of the new topic	Practice	1 time	10 min	Classroom	
17. Summary of the new topic	Summary	1 time	10 min	Classroom	
18. Homework assignment	Homework	1 time	10 min	Classroom	
19. Review of the homework assignment	Review	1 time	10 min	Classroom	
20. Introduction of the new topic	Introduction	1 time	10 min	Classroom	
21. Presentation of the new topic	Presentation	1 time	10 min	Classroom	
22. Practice of the new topic	Practice	1 time	10 min	Classroom	
23. Summary of the new topic	Summary	1 time	10 min	Classroom	
24. Homework assignment	Homework	1 time	10 min	Classroom	
25. Review of the homework assignment	Review	1 time	10 min	Classroom	

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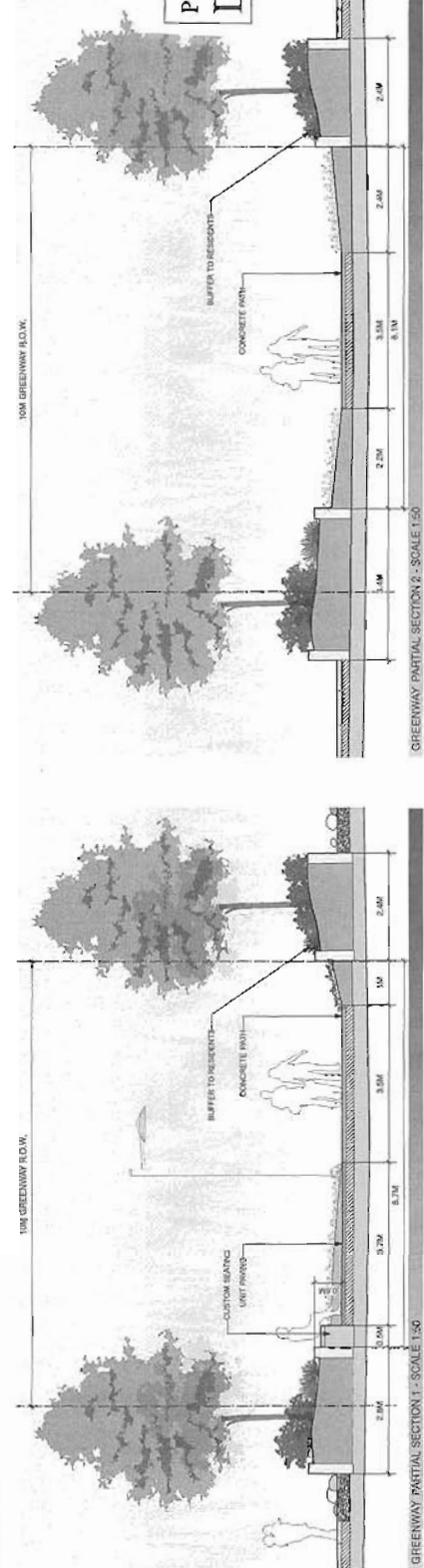
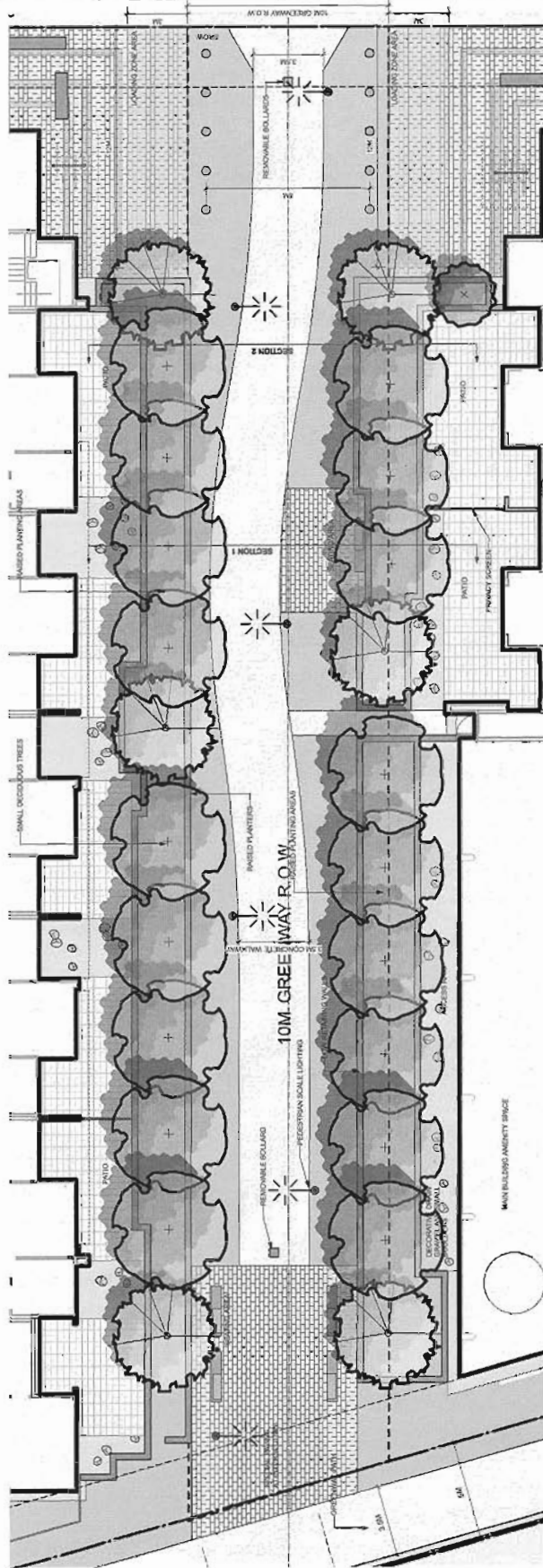
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Plan 45 Sept 4 2012
DP 12-615424

EAST / WEST
COMMUNITY GARDEN
GREENWAY

L1.6



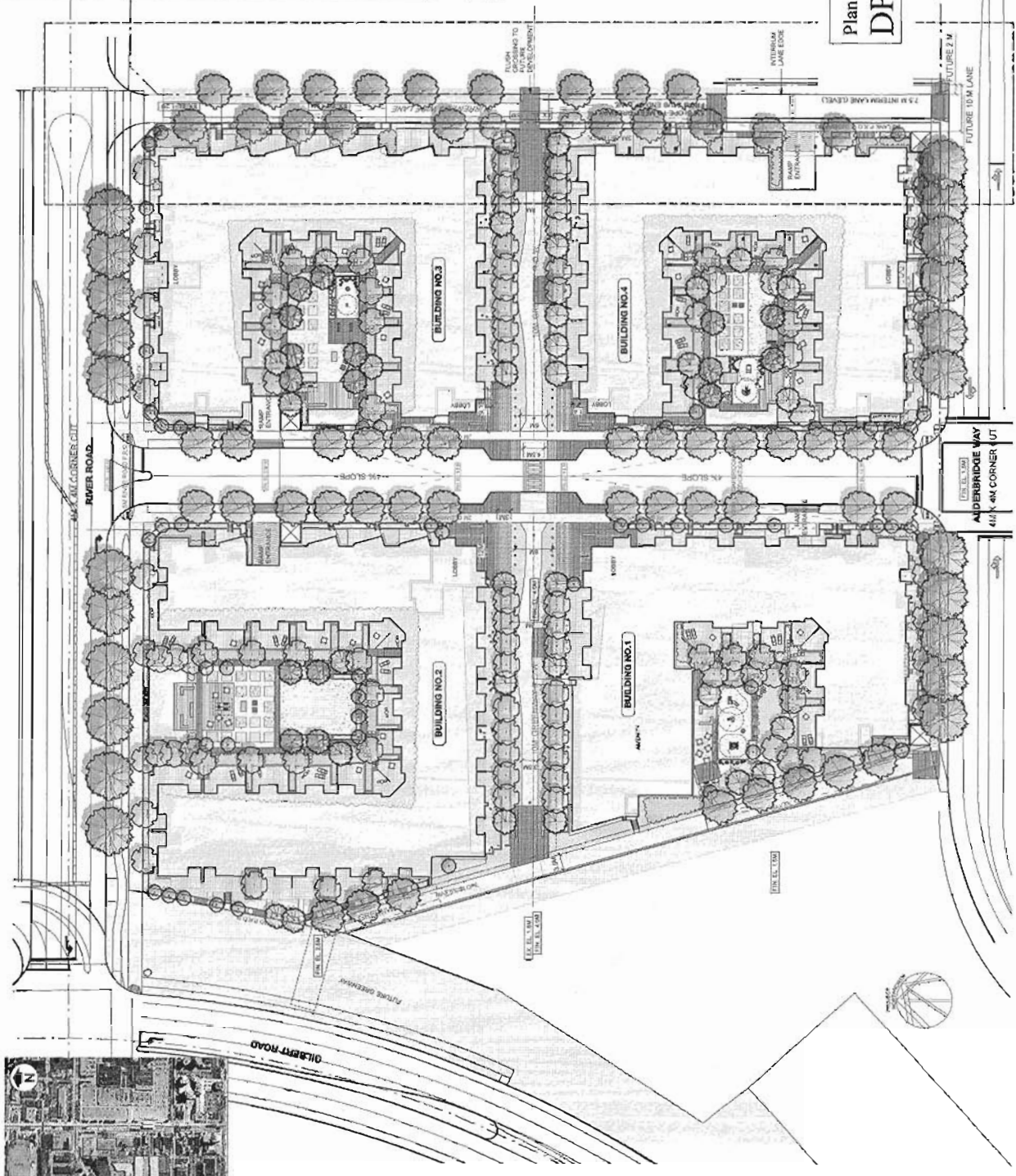
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2008 年 12 月 10 日 星期四 14:00:00



Plan 46 Sept 4 2012
DP 12-615424

L1.7



[illegible]

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 CONSULTING ARCHITECTS INC.

100 North First Street
 New York, NY 10002
 Tel: (212) 691-1000
 Fax: (212) 691-1001
sd@sharpdiamond.com

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DP 12-615424

ON-SITE PRIVATE AND
SHARED OPEN SPACE
AND PROPOSED PUBLIC
ART PLAN

L1.8



ON-SITE PRIVATE AND SHARED OPEN SPACE AND PROPOSED PUBLIC ART PLAN

STATISTICS / LEGEND

- SHARED OPEN SPACE**
- TOTAL OUTDOOR SHARED AMENITY AREA: 5,176 SQ.M. (55,714 SQ FT)
 - REQUIREMENTS AT 5 SQ.M. PER UNIT
660 TOTAL UNITS = 3,300 SQ.M. SHARED AMENITY SPACE
 - INDOOR SHARED AMENITY AREA:
BLDG 1 (SHARED BY BLDG 2): 1,632 SQ.M.
BLDG 3: 21 SQ.M.
BLDG 4: 21 SQ.M.
 - TOTAL INDOOR SHARED AMENITY AREA: 1,674 SQ.M. (20,171 SQ FT)
- PRIVATE OPEN SPACE**
- MINIMUM PRIVATE OPEN SPACE AT GROUND LEVEL = 24 SQ.M. AREA
 - MEDIAN PRIVATE OPEN SPACE AREA AT GROUND LEVEL = 48.5 SQ. M. AREA
 - REQUIREMENTS AT 20 SQ. M. PER UNIT MINIMUM PRIVATE OPEN SPACE AREA FOR URBAN CENTRE (T5) (GROUND LEVEL) AND 6 SQ. M. BALCONY SIZE
 - THE NUMBER OF BALCONIES PER BUILDING
BLDG 1: 161, BLDG 2: 221, BLDG 3: 195, AND BLDG 4: 212
TOTAL BALCONIES: 789
 - TOTAL BALCONY SPACE: 4,734 SQ. M. (50,955 SQ FT)

PUBLIC ART

- POTENTIAL ART LOCATIONS
- POTENTIAL LANDMARK ART LOCATION

NOTE:
ALL PROPOSED PUBLIC ART LOCATIONS SUBJECT TO PUBLIC ART
COMMITTEE AND STAFF REVIEW FOLLOWING PUBLIC ART POLICY

[illegible]

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Landscaping & Horticulture Co.

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Fax: 800-794-1377

RIVA

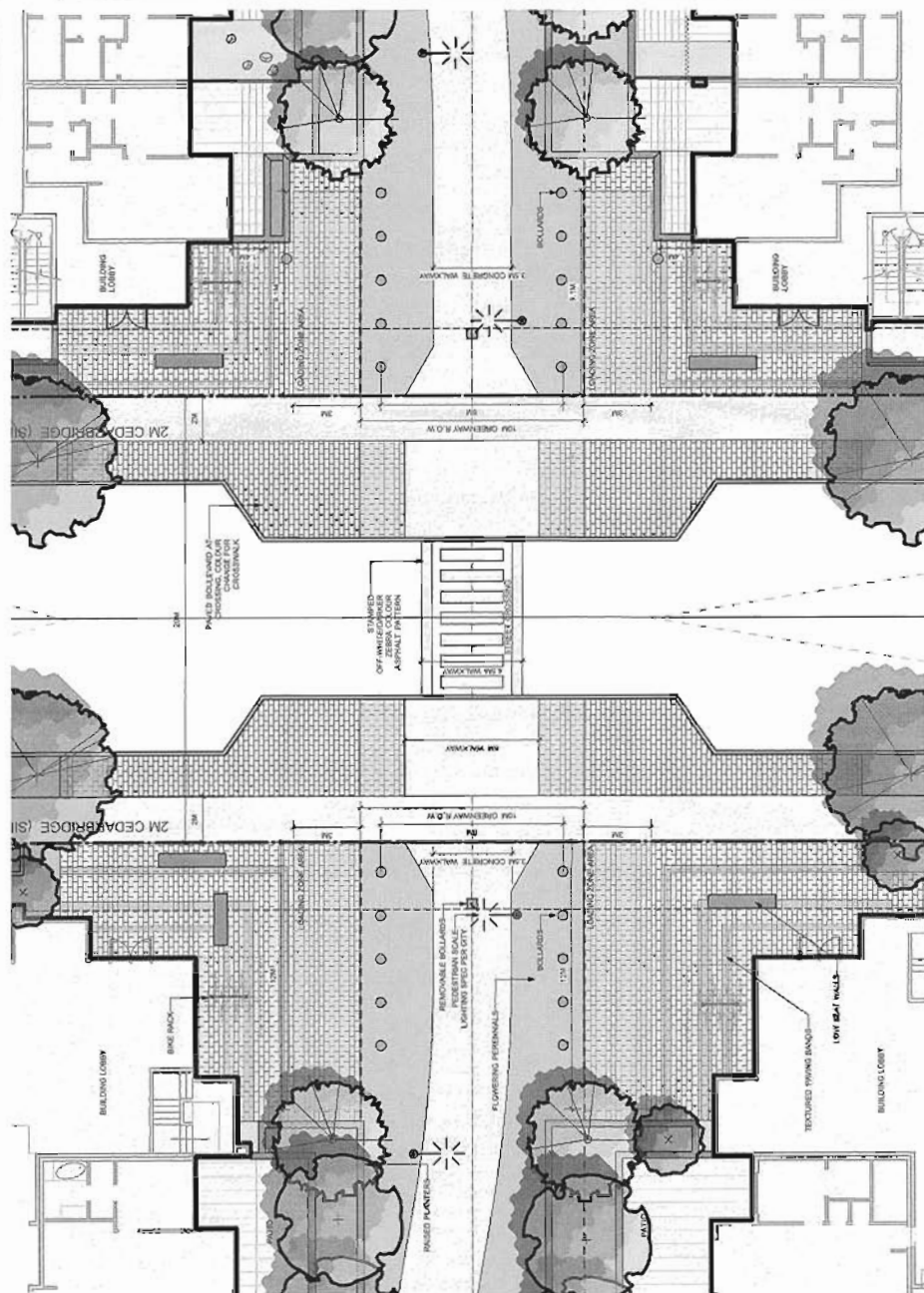
in

group of companies

Category	Unit	Value
Category 1	Unit 1	Value 1
Category 2	Unit 2	Value 2
Category 3	Unit 3	Value 3
Category 4	Unit 4	Value 4
Category 5	Unit 5	Value 5
Category 6	Unit 6	Value 6
Category 7	Unit 7	Value 7
Category 8	Unit 8	Value 8
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Category 95	Unit 95	Value 95
Category 96	Unit 96	Value 96
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Category 98	Unit 98	Value 98
Category 99	Unit 99	Value 99
Category 100	Unit 100	Value 100

CENTRAL
CROSSING

L1.10

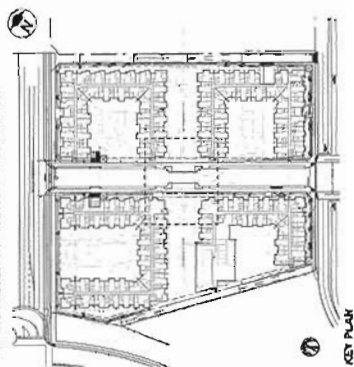


NOTE: CROSSWALK DESIGN DETAIL MAY BE ADJUSTED UNDER THE CROSSING AGREEMENT UPON FURTHER REVIEW OF THE CITY'S PARKS AND TRANSPORTATION DEPARTMENTS.

CENTRAL CROSSING DETAIL PLAN - SCALE 1:100

Plan 48 Sept 4 2012
DP 12-615424

PAVING PRECEDENT - PATTERNS AND COLOURS



KEY PLAY

[illegible][illegible]

TREES

Chinese Witchhazel
Hamamelis virginica

Red Oak
Quercus rubra

Japanese Maple
Acer japonicum

Coconut Bark Maple
Acer glabrum

Red Oak Dogwood
Cornus stolonifera

Colonaster
Colonaster dimidiatus

White Rhododendron
Rhododendron 'Snow Lady'

Pink Yak Rhododendron
Rhododendron yakushimanum 'Pink Saddle'

Alpine Currant
Ribes alpinum

SHRUBS

Arctic Aspen
Populus tremula

Arctic Birch
Betula nana

Arctic Willow
Salix arctica

Arctic Juniper
Juniperus communis

Arctic Larch
Larix laricina

Arctic Spruce
Picea mariana

Arctic Fir
Abies balsamea

Arctic Pine
Pinus strobus

Arctic Cedar
Thuja occidentalis

Arctic Cypress
Cupressus nana

Arctic Yew
Taxus canadensis

Arctic Hemlock
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Arctic Hemlock
Podocarpus nana

	Yarrow <i>Achillea millefolium</i>
	Sand Dune Sedge <i>Carex panicea</i>
	Berkeley Sedge <i>Carex lasiocarpa</i>
	Bleeding Heart <i>Lamorna temocosa</i>
	Golden Japanese Forest Grass <i>Habenochloa microstachya</i>
	Hosta <i>Hosta</i> sp.
	Silver Spider Grass <i>McCrumbia arundinacea</i>
	Narcissus Bulb <i>Narcissus</i>
	Black-eyed Susan <i>Rudbeckia hirta</i>
	Mexican Feather Grass <i>Stipa lessertiana</i>

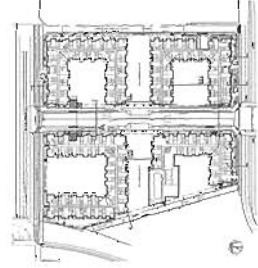
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Newark, NJ
Circle 106

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Journal of Internal Medicine 265: 1–12



LANDSCAPE SECTIONS



KEY PLAN

NOTES:

1. FINISH ELEVATION	2. FINISH ELEVATION
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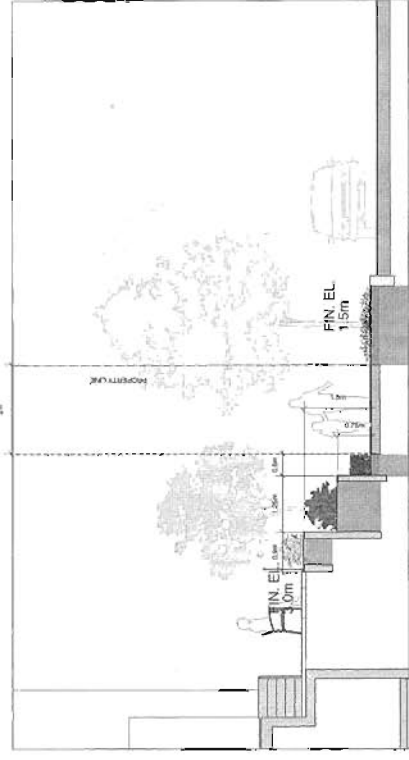
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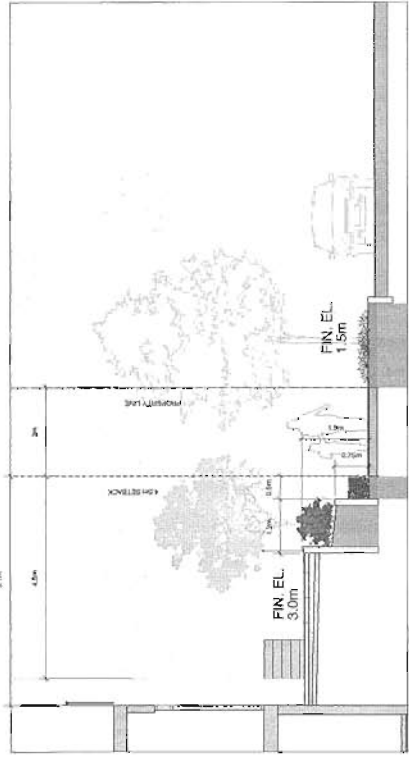
Plan 51 Sept 4 2012
DP 12-615424

LANDSCAPE SECTIONS

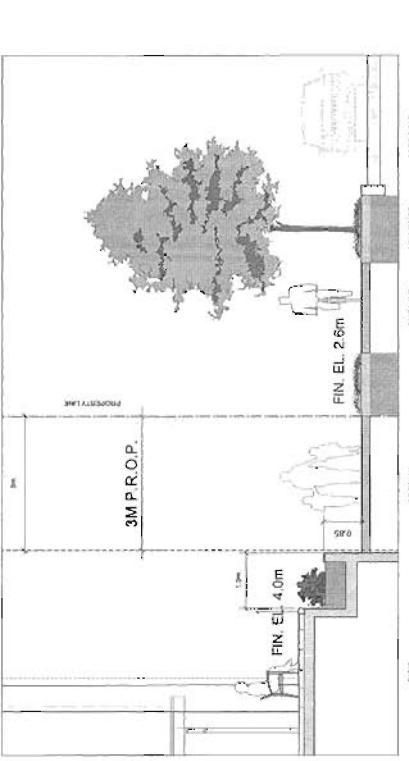
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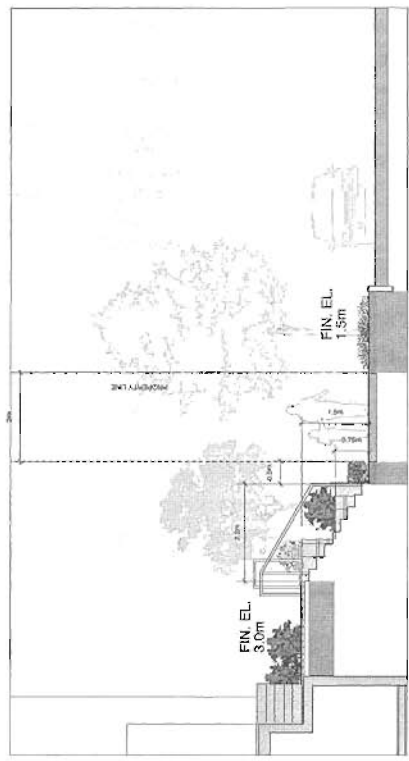
SECTION C



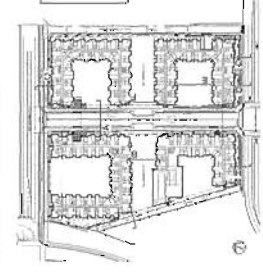
SECTION D



SECTION E



SECTION F



KEY PLAN

NOTES:

1. SEE SHEET 52.01 FOR GENERAL NOTES.
2. SEE SHEET 52.02 FOR GENERAL NOTES.
3. SEE SHEET 52.03 FOR GENERAL NOTES.
4. SEE SHEET 52.04 FOR GENERAL NOTES.
5. SEE SHEET 52.05 FOR GENERAL NOTES.
6. SEE SHEET 52.06 FOR GENERAL NOTES.
7. SEE SHEET 52.07 FOR GENERAL NOTES.
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19. SEE SHEET 52.19 FOR GENERAL NOTES.
20. SEE SHEET 52.20 FOR GENERAL NOTES.



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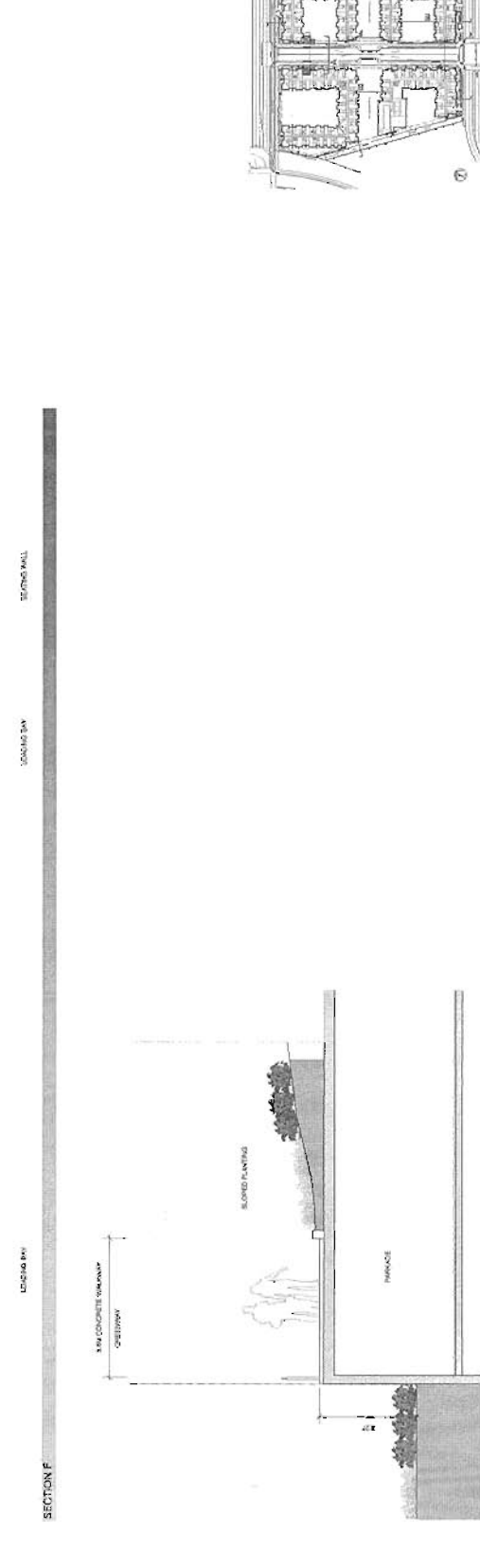
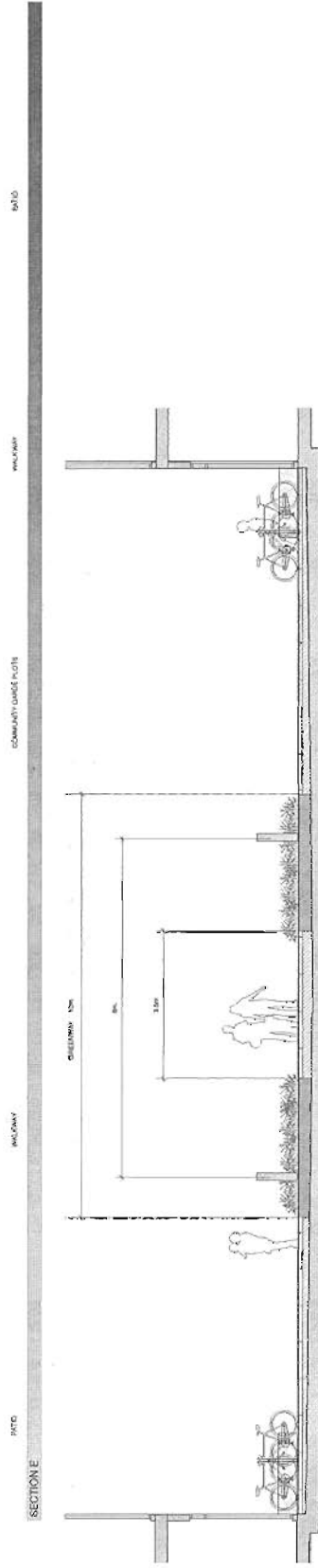
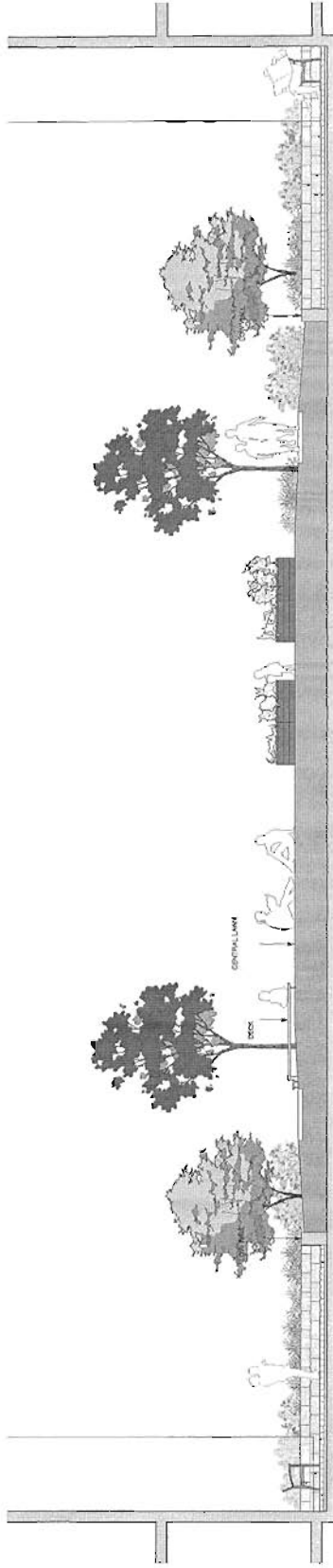
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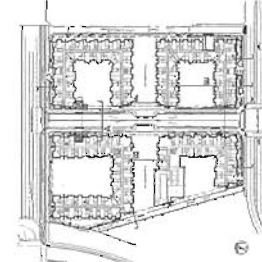
Plan 52 Sept 4 2012
DP 12-615424

LANDSCAPE SECTIONS

L3.3



KEY PLAN



MAPLE TREES

MAIN LOBBY ENTRANCE

STAIRS TO 2ND ENTRANCE

STAIRS TO 3RD FLOOR

ELEVATION - RIVER ROAD - SCALE 1/10

Age	Gender	Height (cm)	Weight (kg)	Body mass index (BMI)	Estimated bone density (g/cm ³)	Estimated bone area (cm ²)	Estimated bone volume (cm ³)
12	Male	150	40	17.8	1.00	100	100
13	Male	155	45	18.7	1.05	105	105
14	Male	160	50	19.6	1.10	110	110
15	Male	165	55	20.5	1.15	115	115
16	Male	170	60	21.4	1.20	120	120
17	Male	175	65	22.3	1.25	125	125
18	Male	180	70	23.2	1.30	130	130
19	Male	185	75	24.1	1.35	135	135
20	Male	190	80	25.0	1.40	140	140
21	Male	195	85	25.9	1.45	145	145
22	Male	200	90	26.8	1.50	150	150
23	Male	205	95	27.7	1.55	155	155
24	Male	210	100	28.6	1.60	160	160
25	Male	215	105	29.5	1.65	165	165
26	Male	220	110	30.4	1.70	170	170
27	Male	225	115	31.3	1.75	175	175
28	Male	230	120	32.2	1.80	180	180
29	Male	235	125	33.1	1.85	185	185
30	Male	240	130	34.0	1.90	190	190
31	Male	245	135	34.9	1.95	195	195
32	Male	250	140	35.8	2.00	200	200
33	Male	255	145	36.7	2.05	205	205
34	Male	260	150	37.6	2.10	210	210
35	Male	265	155	38.5	2.15	215	215
36	Male	270	160	39.4	2.20	220	220
37	Male	275	165	40.3	2.25	225	225
38	Male	280	170	41.2	2.30	230	230
39	Male	285	175	42.1	2.35	235	235
40	Male	290	180	43.0	2.40	240	240
41	Male	295	185	43.9	2.45	245	245
42	Male	300	190	44.8	2.50	250	250
43	Male	305	195	45.7	2.55	255	255
44	Male	310	200	46.6	2.60	260	260
45	Male	315	205	47.5	2.65	265	265
46	Male	320	210	48.4	2.70	270	270
47	Male	325	215	49.3	2.75	275	275
48	Male	330	220	50.2	2.80	280	280
49	Male	335	225	51.1	2.85	285	285
50	Male	340	230	52.0	2.90	290	290
51	Male	345	235	52.9	2.95	295	295
52	Male	350	240	53.8	3.00	300	300
53	Male	355	245	54.7	3.05	305	305
54	Male	360	250	55.6	3.10	310	310
55	Male	365	255	56.5	3.15	315	315
56	Male	370	260	57.4	3.20	320	320
57	Male	375	265	58.3	3.25	325	325
58	Male	380	270	59.2	3.30	330	330
59	Male	385	275	60.1	3.35	335	335
60	Male	390	280	61.0	3.40	340	340
61	Male	395	285	61.9	3.45	345	345
62	Male	400	290	62.8	3.50	350	350
63							



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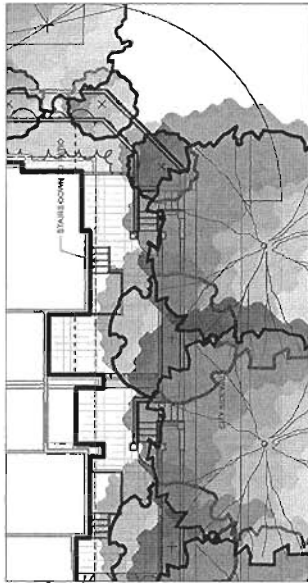


Journal of Interpersonal Violence

Plan 53 Sept 4 2012
DP 12-615424

LANDSCAPE ELEVATIONS
AND DETAILS

L3.4



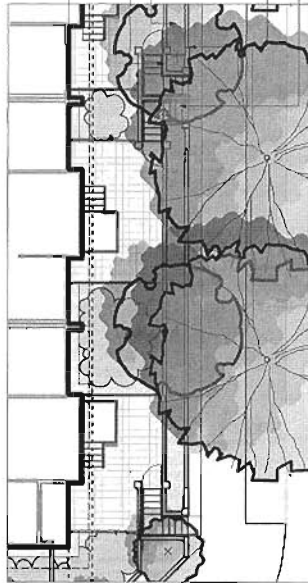
CEDAR

BUILDING HEIGHT: 6'

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ADJACENT HANDLES ON VESTIB

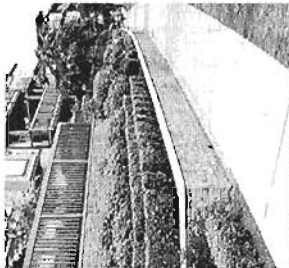
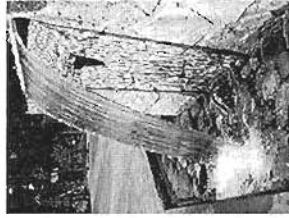
ELEVATION - ALDERBRIDGE WAY - SCALE 1:100



SLATES AND GABLE ENDS
 REDWOOD FOR FINANCY
 CONCRETE REAR WALL
 ROOF IN SIX THICK WALLS
 CONCRETE EXTERIOR WITH
 DRESS NUMBER PLATE
 SLATES ON BRICK WALLS
 SLATES WITH VERTICAL AND
 HORIZONTAL JOINTS
 BRICK WALL - COLOURS
 MATCHED TO THE BRICK
 CONCRETE STAIRS

DETAIL ELEVATION - TYPICAL STREET LEVEL UNIT ENTRY - SCALE 1:25

NOTES



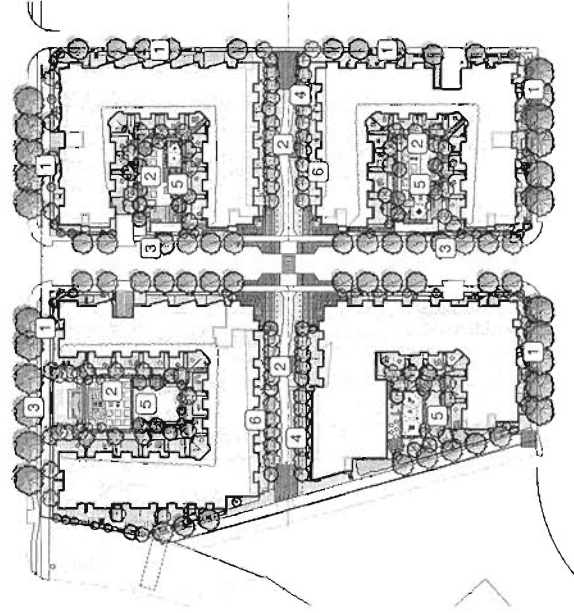
3 WATERFALL FEATURES

2 GARDEN PLOTS

1 TIERED WALLS WITH PLANTING



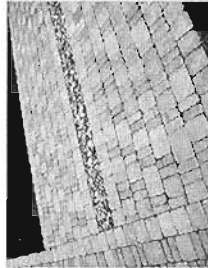
4 RAISED PLANTING BEDS



5 COURTYARD



6 ROCK SWALE ALONG PATIOS



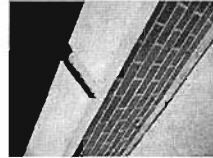
PAVING PATTERNS



DRAIN EDGE



CONCRETE WALLS



WALLS WITH BRICK

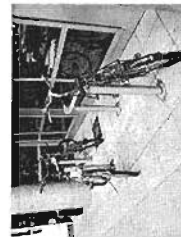
MATERIALS



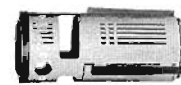
BENCHES ON WALLS



STANDARD BENCHES



CUSTOM BIKE RACKS



STANDARD RECEPTACLE

SITE FURNISHINGS

SITE PRECEDENT IMAGES

PRECEDENT IMAGES

L4.1

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www.sharpdiamond.com

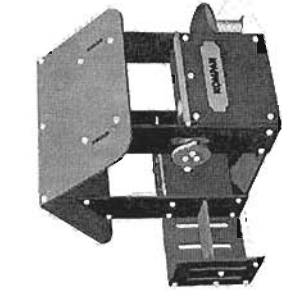
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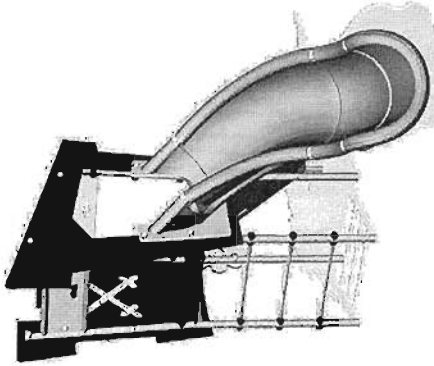
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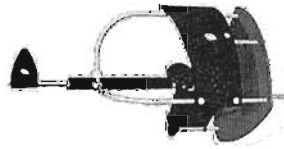
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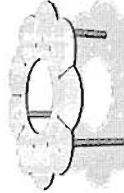
KOMPAN HOUSE M7000P



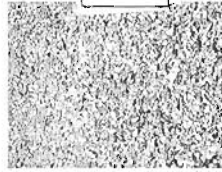
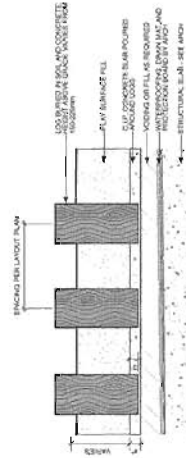
KOMPAN SMALL TOWER MSC413P



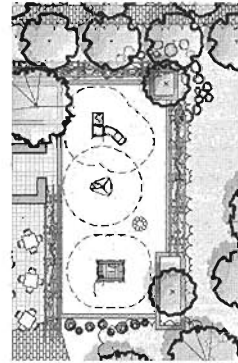
KOMPAN EXPLORER ELE4000B



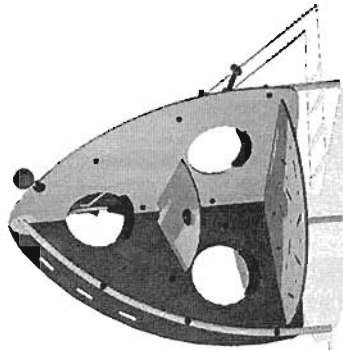
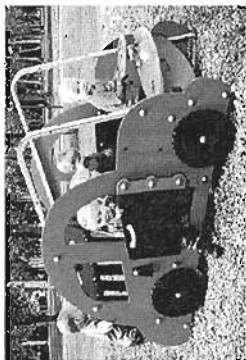
KOMPAN DAISY DESK WITH HOLE 20001P



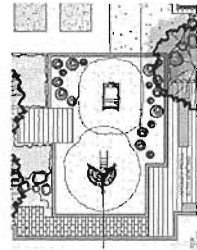
WOOD FIBRE FALL SURFACING



BUILDING 1 PLAY AREA TOYS

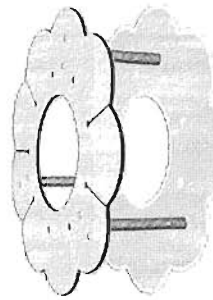
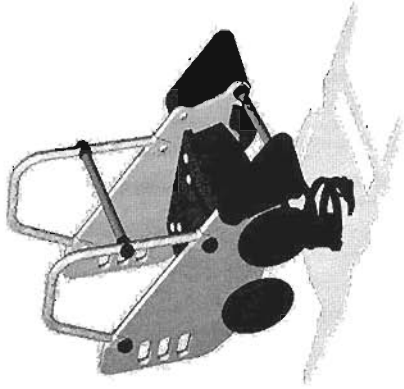


KOMPAN CLIMBY STRUCTURE

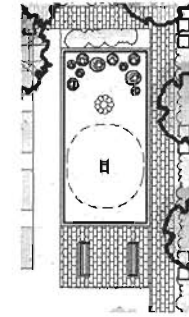


BUILDING 1 PLAY AREA TOYS

KOMPAN FRONTLOADER MT11P



KOMPAN DAISY DESK WITH HOLE 20031P



BUILDING 1 PLAY AREA TOYS

1	SHARP & DIAMOND	LANDSCAPE ARCHITECTS
2	SHARP & DIAMOND	LANDSCAPE ARCHITECTS
3	SHARP & DIAMOND	LANDSCAPE ARCHITECTS
4	SHARP & DIAMOND	LANDSCAPE ARCHITECTS
5	SHARP & DIAMOND	LANDSCAPE ARCHITECTS
6	SHARP & DIAMOND	LANDSCAPE ARCHITECTS
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8	SHARP & DIAMOND	LANDSCAPE ARCHITECTS
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LANDSCAPE PLAY
AREA ELEMENTS

L5.1

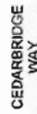
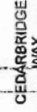
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Plan 57 Sept 4 2012
DP 12-615424

---BUILDING NO. 1---
SOUTH / EAST ELEVATIONS

SCALE	1" = 10'	DATE	A2-1.1
DATE	AUG. 18, 2007		
BY			

[illegible]

[illegible][illegible]

2390 York Road, Oak, Ind.
504.331.5611 fax 504.331.5127 fax 504.331.5127

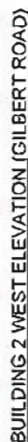
---BUILDING NO. 1---
COURTYARD ELEVATIONS

BUILDING 1 COURT YARD NORTH ELEVATION

Plan 59 Sept 4 2012
DP 12-615424



- [illegible]

[illegible]Plan 61 Sept 4 2012
DP 12-615424

DATE	10/1/94	DEPT NO.	A2-2.1
TIME	1400		
LOCATION	1400		
REMARKS			

WEST / SOUTH EVALUATION

THE UNIVERSITY OF CHICAGO

1997, 1998, 1999, 2000, 2001, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, 2010, 2011, 2012, 2013, 2014, 2015, 2016, 2017, 2018, 2019, 2020, 2021, 2022, 2023, 2024, 2025, 2026, 2027, 2028, 2029, 2030, 2031, 2032, 2033, 2034, 2035, 2036, 2037, 2038, 2039, 2040, 2041, 2042, 2043, 2044, 2045, 2046, 2047, 2048, 2049, 2050, 2051, 2052, 2053, 2054, 2055, 2056, 2057, 2058, 2059, 2060, 2061, 2062, 2063, 2064, 2065, 2066, 2067, 2068, 2069, 2070, 2071, 2072, 2073, 2074, 2075, 2076, 2077, 2078, 2079, 2080, 2081, 2082, 2083, 2084, 2085, 2086, 2087, 2088, 2089, 2090, 2091, 2092, 2093, 2094, 2095, 2096, 2097, 2098, 2099, 2100, 2101, 2102, 2103, 2104, 2105, 2106, 2107, 2108, 2109, 2110, 2111, 2112, 2113, 2114, 2115, 2116, 2117, 2118, 2119, 2120, 2121, 2122, 2123, 2124, 2125, 2126, 2127, 2128, 2129, 2130, 2131, 2132, 2133, 2134, 2135, 2136, 2137, 2138, 2139, 2140, 2141, 2142, 2143, 2144, 2145, 2146, 2147, 2148, 2149, 2150, 2151, 2152, 2153, 2154, 2155, 2156, 2157, 2158, 2159, 2160, 2161, 2162, 2163, 2164, 2165, 2166, 2167, 2168, 2169, 2170, 2171, 2172, 2173, 2174, 2175, 2176, 2177, 2178, 2179, 2180, 2181, 2182, 2183, 2184, 2185, 2186, 2187, 2188, 2189, 2190, 2191, 2192, 2193, 2194, 2195, 2196, 2197, 2198, 2199, 2200, 2201, 2202, 2203, 2204, 2205, 2206, 2207, 2208, 2209, 2210, 2211, 2212, 2213, 2214, 2215, 2216, 2217, 2218, 2219, 2220, 2221, 2222, 2223, 2224, 2225, 2226, 2227, 2228, 2229, 2230, 2231, 2232, 2233, 2234, 2235, 2236, 2237, 2238, 2239, 2240, 2241, 2242, 2243, 2244, 2245, 2246, 2247, 2248, 2249, 2250, 2251, 2252, 2253, 2254, 2255, 2256, 2257, 2258, 2259, 2260, 2261, 2262, 2263, 2264, 2265, 2266, 2267, 2268, 2269, 2270, 2271, 2272, 2273, 2274, 2275, 2276, 2277, 2278, 2279, 2280, 2281, 2282, 2283, 2284, 2285, 2286, 2287, 2288, 2289, 2290, 2291, 2292, 2293, 2294, 2295, 2296, 2297, 2298, 2299, 2300, 2301, 2302, 2303, 2304, 2305, 2306, 2307, 2308, 2309, 2310, 2311, 2312, 2313, 2314, 2315, 2316, 2317, 2318, 2319, 2320, 2321, 2322, 2323, 2324, 2325, 2326, 2327, 2328, 2329, 2330, 2331, 2332, 2333, 2334, 2335, 2336, 2337, 2338, 2339, 2340, 2341, 2342, 2343, 2344, 2345, 2346, 2347, 2348, 2349, 2350, 2351, 2352, 2353, 2354, 2355, 2356, 2357, 2358, 2359, 2360, 2361, 2362, 2363, 2364, 2365, 2366, 2367, 2368, 2369, 2370, 2371, 2372, 2373, 2374, 2375, 2376, 2377, 2378, 2379, 2380, 2381, 2382, 2383, 2384, 2385, 2386, 2387, 2388, 2389, 2390, 2391, 2392, 2393, 2394, 2395, 2396, 2397, 2398, 2399, 2400, 2401, 2402, 2403, 2404, 2405, 2406, 2407, 2408, 2409, 2410, 2411, 2412, 2413, 2414, 2415, 2416, 2417, 2418, 2419, 2420, 2421, 2422, 2423, 2424, 2425, 2426, 2427, 2428, 2429, 2430, 2431, 2432, 2433, 2434, 2435, 2436, 2437, 2438, 2439, 2440, 2441, 2442, 2443, 2444, 2445, 2446, 2447, 2448, 2449, 2450, 2451, 2452, 2453, 2454, 2455, 2456, 2457, 2458, 2459, 2460, 2461, 2462, 2463, 2464, 2465, 2466, 2467, 2468, 2469, 2470, 2471, 2472, 2473, 2474, 2475, 2476, 2477, 2478, 2479, 2480, 2481, 2482, 2483, 2484, 2485, 2486, 2487, 2488, 2489, 2490, 2491, 2492, 2493, 2494, 2495, 2496, 2497, 2498, 2499, 2500, 2501, 2502, 2503, 2504, 2505, 2506, 2507, 2508, 2509, 2510, 2511, 2512, 2513, 2514, 2515, 2516, 2517, 2518, 2519, 2520, 2521, 2522, 2523, 2524, 2525, 2526, 2527, 2528, 2529, 2530, 2531, 2532, 2533, 2534, 2535, 2536, 2537, 2538, 2539, 2540, 2541, 2542, 2543, 2544, 2545, 2546, 2547, 2548, 2549, 2550, 2551, 2552, 2553, 2554, 2555, 2556, 2557, 2558, 2559, 2560, 2561, 2562, 2563, 2564, 2565, 2566, 2567, 2568, 2569, 2570, 2571, 2572, 2573, 2574, 2575, 2576, 2577, 2578, 2579, 2580, 2581, 2582, 2583, 2584, 2585, 2586, 2587, 2588, 2589, 2590, 2591, 2592, 2593, 2594, 2595, 2596, 2597, 2598, 2599, 2600, 2601, 2602, 2603, 2604, 2605, 2606, 2607, 2608, 2609, 2610, 2611, 2612, 2613, 2614, 2615, 2616, 2617, 2618, 2619, 2620, 2621, 2622, 2623, 2624, 2625, 2626, 2627, 2628, 2629, 2630, 2631, 2632, 2633, 2634, 2635, 2636, 2637, 2638, 2639, 2640, 2641, 2642, 2643, 2644, 2645, 2646, 2647, 2648, 2649, 2650, 2651, 2652, 2653, 2654, 2655, 2656, 2657, 2658, 2659, 2660, 2661, 2662, 2663, 2664, 2665, 2666, 2667, 2668, 2669, 2670, 2671, 2672, 2673, 2674, 2675, 2676, 2677, 2678, 26

1998-1999 Annual Report

**Yamamoto
Architecture Inc.**

RIVA

APARTMENT DEVELOPMENT

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DATE	APPROVAL	REASON FOR REVISION
AUG 28, 2012		REVISION FOR COMPLIMENT PROJECT PANEL
MAY 11, 2012		REVISION FOR COMPLIMENT PROJECT PANEL
FEB 28, 2012		REVISION FOR PLANNING COMMISSION REPORT

[illegible]

Plan 62 Sept 4 2012
DP 12-615424

	AUG 28, 2012	ISSUED FOR DEVELOPMENT PERMIT PANEL
	MAY 11, 2012	ISSUED FOR DEVELOPMENT PERMIT
	FEB 8, 2012	ISSUED FOR PLANNING COMMITTEE REPORT
DATE	REVISED	

APARTMENT LEVEL OPINION

RIVA

**Yamamoto
Architecture Inc.**

1200-1100-0000, ext. 342
 1000-1100-0000, ext. 342
 1000-1100-0000, ext. 342

DATE	TIME	PREPARED BY	REVIEWED BY
10/10/2009	10:00	MD	MD
PATIENT NAME		PATIENT ID	
A2-2.2		1000	

BUILDING 2 - ELEVATION KEY NOTES:

1. PRECAST PANEL (SMALL) - PAINTED UP ON THE EAST SIDE
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GREENWAY

BUILDING 2 COURT YARD WEST ELEVATION



BUILDING 2 COURT YARD EAST ELEVATION

Plan 63 Sept 4 2012
DP 12-615424

SCALE: 1" = 10'	DATE: 09/04/12	PROJECT: APARTMENT DEVELOPMENT
DRAWN: J. L. LEE	CHECKED: J. L. LEE	PROJECT: APARTMENT DEVELOPMENT
DESIGNED: J. L. LEE	APPROVED: J. L. LEE	PROJECT: APARTMENT DEVELOPMENT
CLIENT: YAMAMOTO ARCHITECTURE INC.	LOCATION: 12-615424	PROJECT: APARTMENT DEVELOPMENT

BUILDING NO. 2
COURTYARD ELEVATIONS

RIVA
ARCHITECTURE INC.

Yamamoto
Architecture Inc.

DATE: 09/04/12	DESIGNED: J. L. LEE	PROJECT: APARTMENT DEVELOPMENT
DATE: 09/04/12	DESIGNED: J. L. LEE	PROJECT: APARTMENT DEVELOPMENT
DATE: 09/04/12	DESIGNED: J. L. LEE	PROJECT: APARTMENT DEVELOPMENT
DATE: 09/04/12	DESIGNED: J. L. LEE	PROJECT: APARTMENT DEVELOPMENT

[illegible]

Yam Arch
Plan 64 Sept 4 2012
DP 12-615424

Plan 64
Arch

Sept 4 2012

DP 12-615424

Building No. 2
Detail Elevation 1

A2-24

DATE: JAN 26, 2012

TOTAL

10' x 10'

10'

TOTAL

10' x 10'

10'

TOTAL

10' x 10'

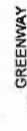
10'

	AUG 29 2012	FISHED ON	DEVELOPMENT PERMIT NAME
	MAY 28 2012	ISSUED FOR	DEVELOPMENT NAME
	DATE	REVISIONS	

BUILDING 3 - ELEVATION KEY NOTES:



BUILDING 3 SOUTH ELEVATION (10M GREENWAY)

BUILDING 3 EAST ELEVATION (LANE)Plan 66 Sept 4 2012
DP 12-615424

NAME	1st 1/2	500 PT. ACCL
DATE	AUG. 28, 2012	A2-3.2
TIME		
CHARTED		500 PT. ACCL

PROJECT

APARTMENT DEVELOPMENT

RIVA
FRT, 777 ALDREHODGE WAY

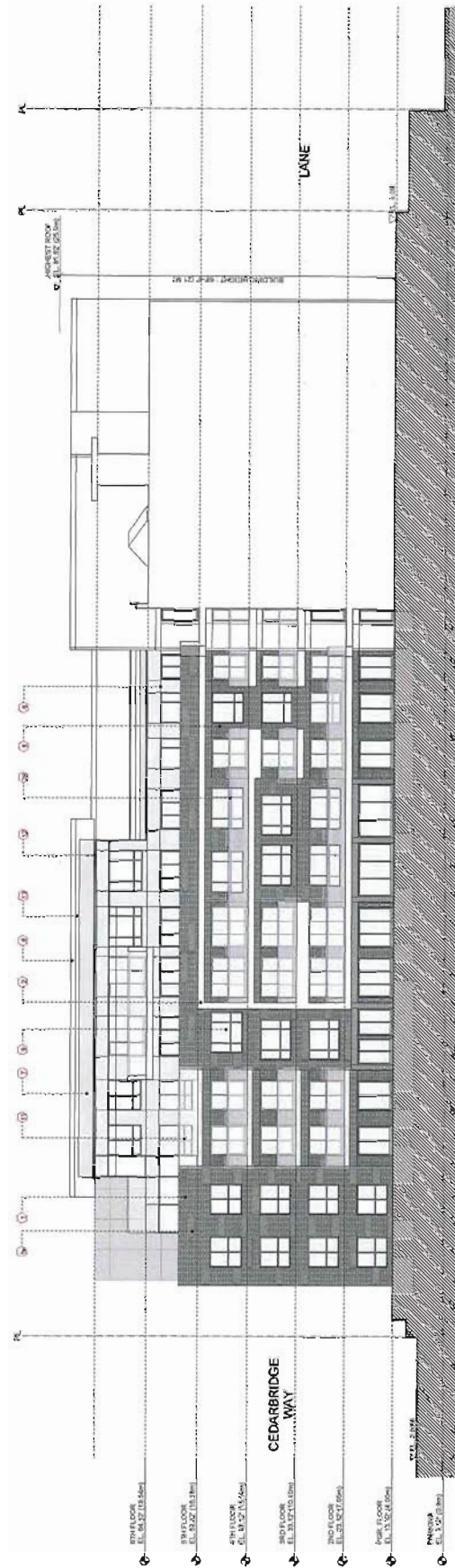
**Yamamoto
Architecture Inc.**

2007 4th Quad, rms. 3, 6.
 1998 A-11 Nat 104-771-1427 Nat 104-771-1427
 DEBORAH TITUS
 BUILDING NO. 3

Architectural elevation drawing of a building facade. The drawing shows a multi-story building with a central entrance and a large windowed section. The facade is composed of various materials and colors, as indicated by the legend. The building is situated on a street corner, with "CEDARBRIDGE WAY" and "LANE" labeled. The drawing includes a grid system with letters A through T and numbers 1 through 20. The legend lists materials and colors for different parts of the building, such as brick, stone, and metal.

Legend:

- 1. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 2. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 3. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 4. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 5. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 6. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 7. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 8. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
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- 18. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 19. BRICK-CLAY, PANEL, GRANULAR, PARTIAL
- 20. BRICK-CLAY, PANEL, GRANULAR, PARTIAL

[illegible]

BUILDING 1 COURT YARD NORTH ELEVATION

Plan 67 Sept 4 2012
DP 12-615424

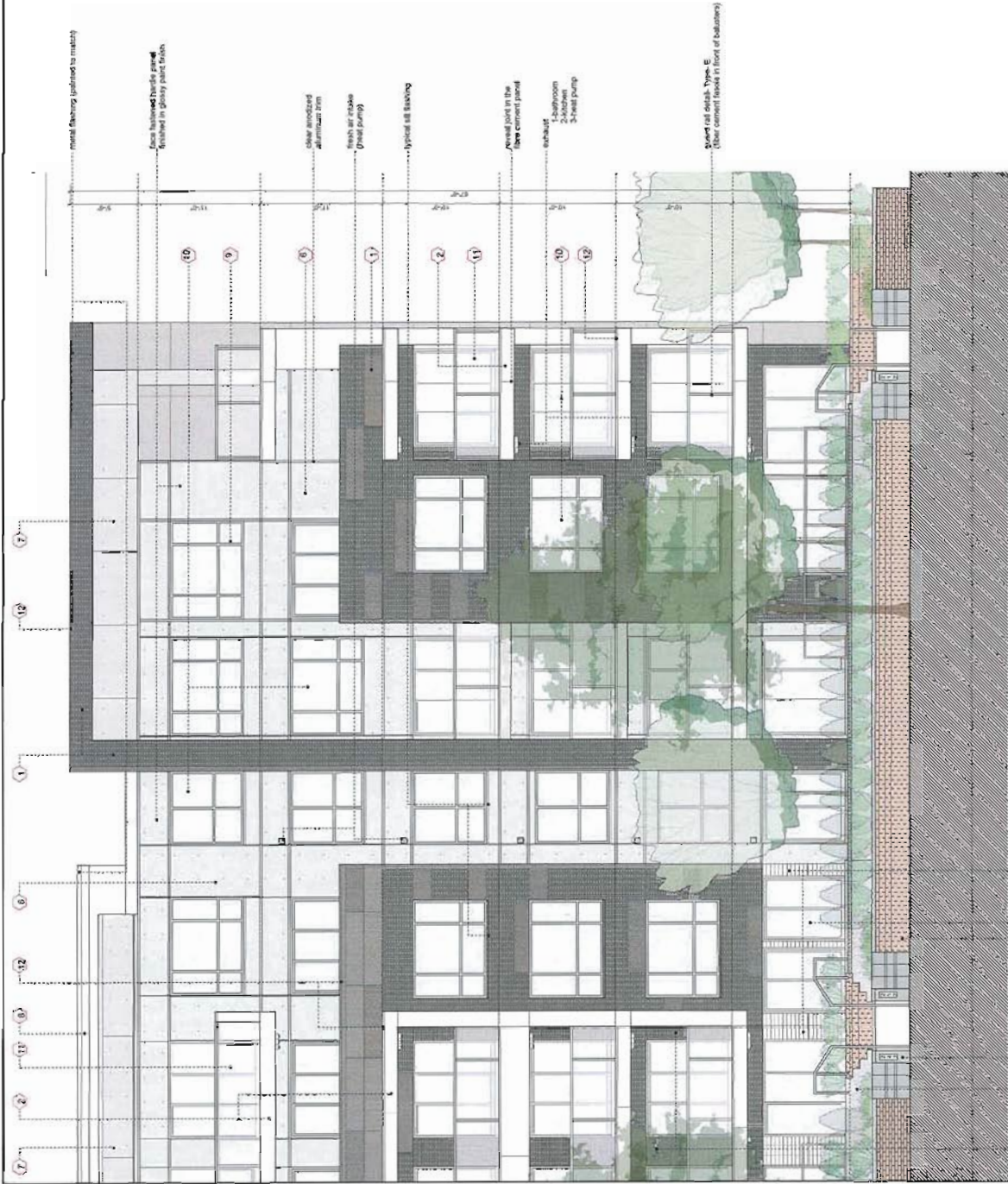
DATE	11-1-19	DATE REC	11-1-19
DATE	AUG 28 2019	DATE REC	
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CALLER		CALLER	
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RIVA
 1775 17TH ALENDRIDGE WAY
 ROCKFORD, ILL. 61105

APARTMENT DEVELOPMENT
 PROJECT

Yamamoto
 Architecture Inc.

- BUILDING 3 - ELEVATION KEY NOTES:**
- 1 FIBRE-CEMENT PANEL (SMOOTH) - PAINTED GP AX
 - 2 FIBRE-CEMENT PANEL (TEXTURED) - PAINTED GP CL 285A JACK IN THE PLANT
 - 3 FIBRE-CEMENT PANEL (GRANULAR) - PAINTED GP CL 101N DEEP FEATHER
 - 4 BRICK (STACKED) - INL WHITE
 - 5 -
 - 6 HARDBOARD PANEL BEING BRICKWORK PAINTED GPCL 285A (GABE AS 14)
 - 7 HARDBOARD PANEL IN ALUM TRIM - PAINTED GP CL 101N IN TRIM
 - 8 HARDBOARD PANEL SHOWN LINE (SMOOTH) - PAINTED GP CL 101N IN TRIM
 - 9 FIBRE-CEMENT PANEL (SMOOTH) - PAINTED GP CL 101N IN TRIM
 - 10 DOUBLE GLAZED PUPA WOODS ALUM FRAMED STOPS FRONT WINDOW
 - 11 ALUM FRAMED GUARD - WHITE W/ FROSTED TEMPERED GLASS
 - 12 PRE-FINISHED ALUM FLASHING CAP - WHITE
 - 13 HARDBOARD PANEL - OFF WHITE
 - 14 ALUMINUM SINGLES MOUNTED TO CANOPY
 - 15 LOWER BRICK WALL - COLORED ARCHITECTURAL BRICK ALUM BRICK SMOOTH
 - 16 LOWER CONCRETE WALL - LIGHT SAND BLASTED CONCRETE FINISH WITH VERTICAL AND HORIZONTAL REINFORCING
 - 17 PARADE GATE & EXIT DOOR - STEEL - PAINTED
 - 18 GROUND FLOOR ALUMINUM FRAMED PAIRING SILVER
 - 19 RAILING TYPE 10 - FIBRE-CEMENT FASCIA IN FRONT OF BALUSTERS - JAGGED GP CL 285A JACK IN THE PLANT
 - 20 ALUMINUM GP CL 285A JACK IN THE PLANT



BUILDING 3 DETAIL ELEVATION (AT ALDERBRIDGE WAY AND LANE)

Yb Art

Plan 68 Sept 4 2012

DP 12-615424

PROJECT NO. 12-615424

DATE: 09/04/12

BY: J. H. H. H.

SCALE: 1/8" = 1'-0"

PROJECT NO. 12-615424

DATE: 09/04/12

BY: J. H. H. H.

SCALE: 1/8" = 1'-0"

BUILDING 4 - ELEVATION KEY NOTES:



BUILDING 4 NORTH ELEVATION (10M GREENWAY)



BUILDING 4 WEST ELEVATION (CEDARBRIDGE WAY)

NO.	DATE	REVISION
	FEB 8, 2012	ISSUED FOR PLANNING COMMITTEE REPORT
	MAY 1, 2012	ISSUED FOR DEVELOPMENT PERMIT
	AUG. 19, 2012	ISSUED FOR DEVELOPMENT PERMIT APP

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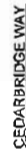
RIVA
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77H, 77H ALUMINUM

Yamamoto
Architecture Inc.

2385 and Street, N.W., D.C.
20037-4241 Tel: 202-775-1327 Fax: 202-775-1327

Plan 69 Sept 4 2012
DP 12-615424

SCALE	1" = 10'	A24.1	PROJECT NO.
DATE	AUG 28 2012		
DRAWN			
CHECKED			PROJECT TITLE

[illegible]

BUILDING 4 DETAIL ELEVATION (AT ALDERBRIDGE WAY)

Plan 72 Sept 4 2012
DP 12-615424

DRAWING TITLE	BUILDING NO. A DETAIL ELEVATION 4	SHEET NO.	AZ-44
DATE	SEP 28 2012		
CUSTOMER	TR		
PROJECT NO.			

STATE OF NEW YORK
COUNTY OF ALBANY

RIVA

APARTMENT DEVELOPMENT
7731, 7771 ALDERBRIDGE WAY
RICHMOND, B.C.



LEED SCORECARD:

InterGreen, Green Building Solutions, Inc.

LEED™ Registered at 42,307-1

Project Name: RIVA, 7731, 7771 Alderbridge Way

Project Number: 42,307-1

Project Address: 7731, 7771 Alderbridge Way

Project City: Richmond, BC

Project Country: Canada

Project Status: In Progress

Project Start Date: 2010-01-01

Project End Date: 2010-12-31

Project Manager: [Name]

Project Architect: [Name]

Project Engineer: [Name]

Project Designer: [Name]

Project Contractor: [Name]

Project Subcontractor: [Name]

Project Supplier: [Name]

Project Manufacturer: [Name]

Project Installer: [Name]

Project Maintainer: [Name]

Project Operator: [Name]

Project User: [Name]

Project Owner: [Name]

Project Sponsor: [Name]

Project Stakeholder: [Name]

Project Partner: [Name]

Project Client: [Name]

Project Investor: [Name]

Project Lender: [Name]

Project Insurer: [Name]

Project Regulator: [Name]

Project Auditor: [Name]

Project Consultant: [Name]

Project Advisor: [Name]

Project Specialist: [Name]

Project Expert: [Name]

Project Analyst: [Name]

Project Researcher: [Name]

Project Writer: [Name]

Project Editor: [Name]

Project Publisher: [Name]

Project Distributor: [Name]

Project Retailer: [Name]

STATISTICS:

UNIT TYPE	NO. OF UNITS	NO. OF SQ. FT.	NO. OF SQ. YD.
1-BEDROOM	10	1,100	127.7
2-BEDROOM	10	1,800	207.9
3-BEDROOM	10	2,500	292.7
4-BEDROOM	10	3,200	370.3
5-BEDROOM	10	3,900	451.5
6-BEDROOM	10	4,600	532.7
7-BEDROOM	10	5,300	613.9
8-BEDROOM	10	6,000	695.1
9-BEDROOM	10	6,700	776.3
10-BEDROOM	10	7,400	857.5
TOTAL	100	44,100	5,091.6

PARKING SPACE REQUIREMENTS:

UNIT TYPE	NO. OF UNITS	NO. OF PARKING SPACES
1-BEDROOM	10	10
2-BEDROOM	10	20
3-BEDROOM	10	30
4-BEDROOM	10	40
5-BEDROOM	10	50
6-BEDROOM	10	60
7-BEDROOM	10	70
8-BEDROOM	10	80
9-BEDROOM	10	90
10-BEDROOM	10	100
TOTAL	100	500

LOADING SPACE REQUIREMENTS:

UNIT TYPE	NO. OF UNITS	NO. OF LOADING SPACES
1-BEDROOM	10	10
2-BEDROOM	10	20
3-BEDROOM	10	30
4-BEDROOM	10	40
5-BEDROOM	10	50
6-BEDROOM	10	60
7-BEDROOM	10	70
8-BEDROOM	10	80
9-BEDROOM	10	90
10-BEDROOM	10	100
TOTAL	100	500

LOADING SPACE REQUIREMENTS:

UNIT TYPE	NO. OF UNITS	NO. OF LOADING SPACES
1-BEDROOM	10	10
2-BEDROOM	10	20
3-BEDROOM	10	30
4-BEDROOM	10	40
5-BEDROOM	10	50
6-BEDROOM	10	60
7-BEDROOM	10	70
8-BEDROOM	10	80
9-BEDROOM	10	90
10-BEDROOM	10	100
TOTAL	100	500

Project Name: RIVA, 7731, 7771 Alderbridge Way

Project Number: 42,307-1

Project Address: 7731, 7771 Alderbridge Way

Project City: Richmond, BC

Project Country: Canada

Project Status: In Progress

Project Start Date: 2010-01-01

Project End Date: 2010-12-31

Project Manager: [Name]

Project Architect: [Name]

Project Engineer: [Name]

Project Designer: [Name]

Project Contractor: [Name]

Project Subcontractor: [Name]

Project Supplier: [Name]

Project Manufacturer: [Name]

Project Installer: [Name]

Project Maintainer: [Name]

Project Operator: [Name]

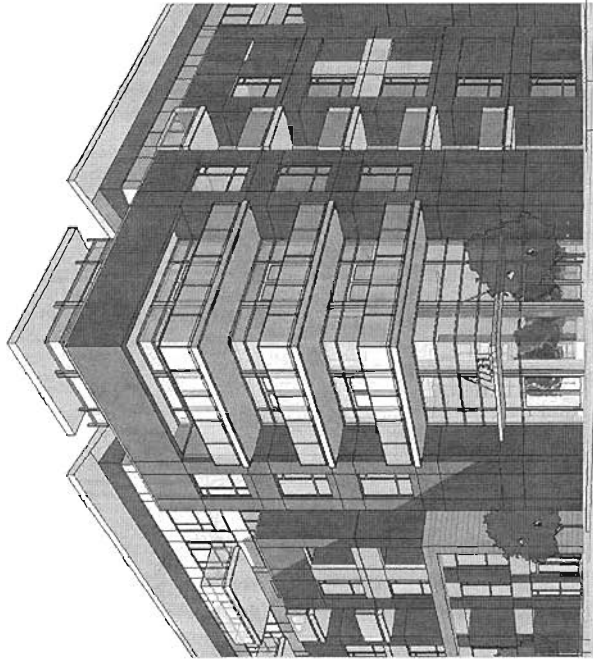
PROPOSED UNIT MIX / UNIVERSAL ACCESSIBLE UNITS (FLOOR AREA / AFFORDABLE UNITS)

UNIT TYPE	NO. OF UNITS	NO. OF SQ. FT.	NO. OF SQ. YD.
1-BEDROOM	10	1,100	127.7
2-BEDROOM	10	1,800	207.9
3-BEDROOM	10	2,500	292.7
4-BEDROOM	10	3,200	370.3
5-BEDROOM	10	3,900	451.5
6-BEDROOM	10	4,600	532.7
7-BEDROOM	10	5,300	613.9
8-BEDROOM	10	6,000	695.1
9-BEDROOM	10	6,700	776.3
10-BEDROOM	10	7,400	857.5
TOTAL	100	44,100	5,091.6

Reference Plan Sept 4 2012
DP 12-615424

Yamamoto Architecture Inc.

PANEL
AUG. 28, 2012



DATE: 09-04-2012	PROJECT: ADAPTATION DEVELOPMENT
DESIGNER: YAMAMOLO ARCHITECTURE INC.	CLIENT: RIVA
LOCATION: 1000 10TH AVENUE, SUITE 100, NEW YORK, NY 10018	PHASE: PRELIMINARY
SCALE: 1/8" = 1'-0"	STATUS: FOR REVIEW
BY: [Signature]	REVISIONS:
CHECKED BY: [Signature]	APPROVED BY: [Signature]

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RIVA
1000 10TH AVENUE, SUITE 100
NEW YORK, NY 10018

Yamamolo Architecture Inc.

DATE: 09-04-2012
BY: [Signature]
CHECKED BY: [Signature]
APPROVED BY: [Signature]

COVER SHEET

DIMENSIONS (INCH)	
1/4" = 1'-0"	1/4" = 1'-0"

TOTAL AREA	
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Reference Plan Sept 4 2012
DP 12-615424

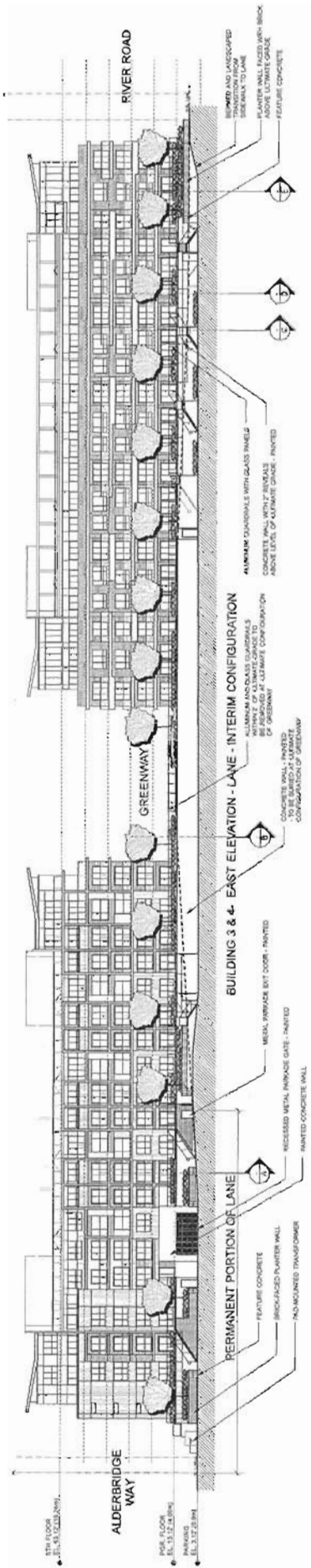
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Yamamoto
Architecture Inc.

PROJECT
ARCHITECT DEVELOPMENT

SERVICE TITLE
ELEVATIONS

DATE
2

Reference Plan Sept 4, 2011

DP 12-615424

SCALE
A0.4

REVISIONS

DATE
2011-09-04

BY
JAM

CHK
JAM

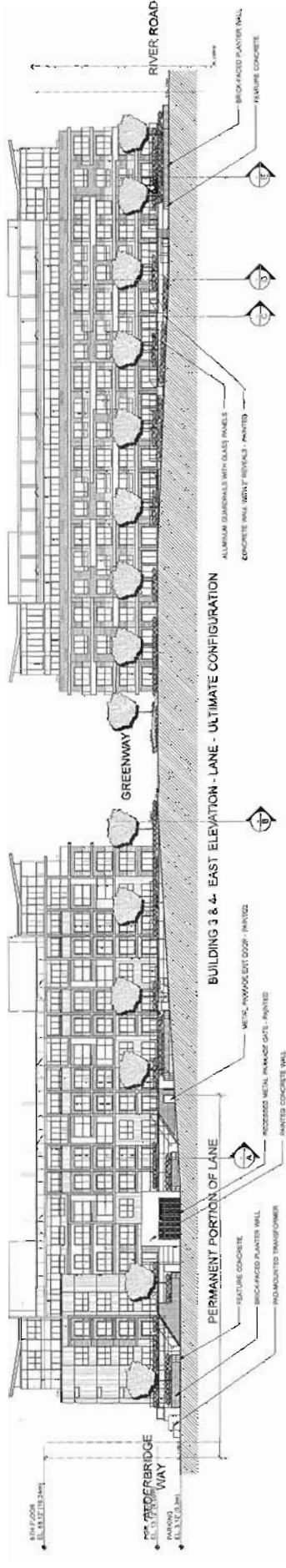
APP
JAM

DATE
2011-09-04

BY
JAM

CHK
JAM

APP
JAM



Yamamoto
Architecture

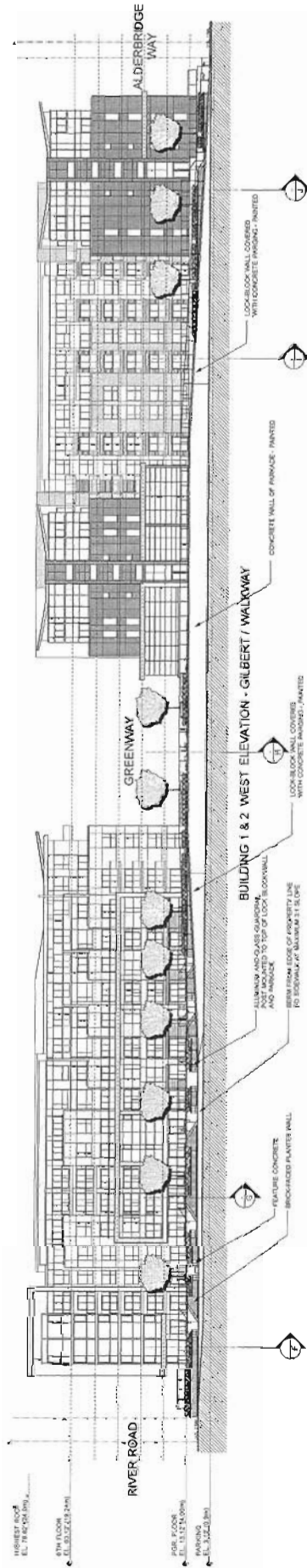
PROJECT
PHASE 1 DEVELOPMENT

DATE
10/11/2012

Reference Plan Sept 4 2012
DP 12-615424

ELEVATIONS
LANE - ULTIMATE CONFIGURATION
SHEET NO.
A0.5

NO.	DATE	BY	CHKD.
1	10/11/2012	YAMAMOTO	YAMAMOTO
2	10/11/2012	YAMAMOTO	YAMAMOTO
3	10/11/2012	YAMAMOTO	YAMAMOTO
4	10/11/2012	YAMAMOTO	YAMAMOTO
5	10/11/2012	YAMAMOTO	YAMAMOTO
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7	10/11/2012	YAMAMOTO	YAMAMOTO
8	10/11/2012	YAMAMOTO	YAMAMOTO
9	10/11/2012	YAMAMOTO	YAMAMOTO
10	10/11/2012	YAMAMOTO	YAMAMOTO



Yaniamolo
Architecture Inc.

PROJECT
APARTMENT DEVELOPMENT

DATE
ELEVATION

Reference Plan Sept 4 2012

DP 12-615424

ALERT

SW 11-12

A0.6

100% 2012

NO.	DATE	REVISIONS
1	08/13/2012	ISSUED FOR PERMIT
2	09/04/2012	REVISED FOR PERMIT
3	09/11/2012	REVISED FOR PERMIT
4	09/11/2012	REVISED FOR PERMIT
5	09/11/2012	REVISED FOR PERMIT
6	09/11/2012	REVISED FOR PERMIT
7	09/11/2012	REVISED FOR PERMIT
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10	09/11/2012	REVISED FOR PERMIT



City of Richmond

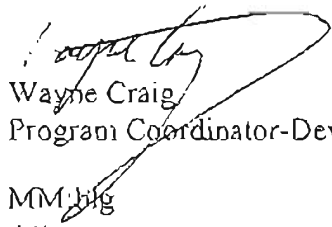
Report to Development Permit Panel Planning and Development Department

To: Development Permit Panel **Date:** September 9, 2012
From: Wayne Craig **File:** DP 12-615424
Program Coordinator-Development
Re: Application by Onni Contracting Ltd. for a Development Permit at
7731 and 7771 Alderbridge Way

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of a 659-unit project in four (4), six-storey wood frame buildings over two (2) concrete parking structures located at 7731 and 7771 Alderbridge Way; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - (a) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures on the proposed Lots 1 and 2 from 3.0 m to 0.0 m respectively along Cedarbridge Way and Gilbert Road;
 - (b) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the west property line of the proposed Lot 1 and the east property line of the proposed Lot 2;
 - (c) Reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15 spaces/dwelling unit for the development as a whole; and
 - (d) Relax the requirement for the provision of on-site loading spaces for two (2) WB-17 loading spaces.


Wayne Craig
Program Coordinator-Development

MM:hlg
Att.

Staff Report

Origin

Onni Contracting Ltd. has applied to the City of Richmond for permission to develop a 659-unit project in four (4), six-storey wood frame buildings over two (2) concrete parking structures. Two (2) buildings will be located on top of one (1) large single storey parkade on proposed Lot 1 (West) and Lot 2 (East) on each side of a newly dedicated section of Cedarbridge Way.

The site is currently under application for rezoning from “Industrial Retail (IR1)” to “High Density Low Rise Apartments (RAH2)” (RZ 11-585209) to facilitate this Development Permit application. The applicable Zoning Amendment Bylaw No. 8884 has received third reading with the applicant currently completing the Rezoning Considerations.

As part of the Rezoning Considerations for the development, a Servicing Agreement is being registered on title that will secure the servicing and road works to be completed. No-build covenants are also required to ensure that the necessary servicing works and streets are constructed at the necessary times prior to issuance of building and occupancy permits for the four (4) proposed buildings.

Development Information

Of the 659 units proposed, Buildings 1 and 2, located on top of a parkade to the west of Cedarbridge Way, contain 340 units. Buildings 3 and 4, located on second parkade on the east of Cedarbridge Way, contain 319 units. Lot 1 is proposed to be developed first under a two (2) phase strata for Buildings 1 and 2. Subsequently, Lot 2 is proposed to be developed under a separate two (2) phase strata for Buildings 3 and 4. Please refer to the attached Development Application Data Sheet (**Attachment 1**) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

The existing two (2) lots within the subject site of 2.87 ha. (7.09 acres) were created in 1969 as part of the Brighthouse Industrial Estate subdivision along Alderbridge Way. Of note, the western lot was the site of the long-standing Stacey's Furniture World and the eastern lot now includes a Tim Horton's amongst numerous other smaller commercial and light industrial tenants.

The existing and proposed development surrounding the subject consolidated site is described as follows:

To the North, lies the former CPR line property which is now owned by the City and will form part of New River Road. Further to the north, one (1) large light industrial building is located on a site zoned as “Industrial Business (IB1).” This site is designated within the CCAP as part of a large future Riverfront Park.

To the South, lies Alderbridge Way with the former Grimm's sausage factory site on the south side of the street. This site is now zoned "Industrial Retail (IR1)" and is the subject of a current rezoning application to rezone the site to a "Residential Limited Commercial (RCL)" zone allow for a higher density, mixed-use development.

To the East, two (2) light industrial / retail buildings are located on site to the east of an adjacent lane and on a site zoned "Industrial Retail (IR1)".

To the West, the Gilbert Road approach to the Dinsmore Bridge forms the north-west boundary of the subject site. The remainder of the site is bounded by the former "V-Tech" building site which is zoned "Industrial Retail (IR1)."

Rezoning and Public Hearing Results

Public Hearings for the rezoning of this site were held on May 22, 2012 and June 18, 2012. At the Public Hearings, the following concerns about rezoning the property were expressed.

There were concerns raised by Tim Horton's Canada (TDL Group Corp.) and local franchise owner, Mr. Mike Raspberry, regarding the applicant's consultation with them on the existing lease for the Tim Horton's located in part of the large existing building at 7771 Alderbridge Way. As result of this concern, the following resolution PH/12 5-6 was made at the May 22, 2012 Public Hearing:

That, in relation to this rezoning, as a further condition of fourth reading of the Bylaw, that any leases registered on title, including the lease in favour of Tim Hortons Canada, would be discharged.

There was also a concern raised by raised in letters from by Ebco Industries Ltd. regarding view corridors mentioned in the Rezoning Staff Report on the project. Staff explained that this report text concerned the context site and potential views towards the north and was not granting any view corridors over the Ebco property. A letter was also sent to Ebco Industries in follow-up.

Following the May 22, 2012 Public Hearing, new information was brought forward by the applicant to staff and Council leading to the above resolution PH/12 5-6 being rescinded and with a second Public Hearing being held and Third Reading being given on June 18, 2012. At this second hearing, similar concerns were raised by TDL Group Corp. and Mike Raspberry and Ebco Industries. A representative from the Jones New York store at 7771 Alderbridge Way also noted they had not been consulted by the applicant. While the tenant concerns are private property matters, staff and Council urged the applicant to communicate with its tenants concerning the future plans for buildings on the project site.

After the second Public Hearing, Onni sent a letter to all of its tenants updating them on the development process, advising that 7731 Alderbridge Way (proposed Buildings 1 and 2 on Lot 1) is scheduled to be re-developed first. The letter also stated that re-development of 7771 Alderbridge Way (proposed Buildings 3 and 4 on Lot 2) was contingent on the rate of market absorption of the first two buildings.

Staff Comments

The proposed development scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, the proposal complies with the intent of the applicable sections of the Official Community Plan (OCP) and the CCAP which designates the site as “Urban Centre (25 m)” and includes the Sub-Area B.2 Mixed-Use Mid-Rise Residential and Limited Commercial Development Permit guidelines.

The proposed application is in compliance with the “High Density Low Rise Apartments (RAH2)” zone of City of Richmond Zoning Bylaw except for the zoning variances noted below.

Zoning Compliance/Variations (staff comments in bold)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

- 1) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures on the proposed Lots 1 and 2 from 3.0 m to 0.0 m respectively along Cedarbridge Way and Gilbert Road.

(Staff supports the proposed variance for the parkade setbacks on Lots 1 and 2 to the new Cedarbridge Way as the sections of parkade walls located at 0.0 m. are largely located below the Cedarbridge Way street grade where the east-west greenway crosses the street. Short sections of these parkade walls rise up to approximately 0.75 m (2.5 ft.) to meet adjacent terraced planter (see Architectural plan A0.7 and Landscape Plan L3.1).

Similar adjacent terraced planter walls are located between Building 2 and the Gilbert Road allowance property line on the proposed Lot 1 (see Architectural plans A3.04 and A3.06). There will be approximately 20 m (66 ft.) of slightly lower open space between the project property line and the future curb line within the Gilbert Road allowance. As discussed below in this report, the City will be constructing a plaza, pedestrian and cycling paths and locating a major public art work adjacent to this property line within the Gilbert Road allowance. This area will be raised to the level of Gilbert and River Roads which at a similar level to the base of the project's landscape walls which are located in front of the parkade.

On both Cedarbridge Way and Gilbert Road, lowered parkade ceilings over bike parking and the front vehicle parking have been used to assist in accommodating these planters and hiding the parkade structures.)

- 2) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the west property line of the proposed Lot 1 and the east property line of the proposed Lot 2;

(Staff supports the proposed variance for the parkade wall and interim retaining wall under Buildings 1 and 2 on the west side of the proposed Lot 1. The proposed retaining wall and parkade wall range from 0.0 m to approximately 2.5 m (8.2 ft.) in height above grade see Architectural plans A3.04 and A3.06).

This condition is largely the result of gradually raising the grade along the North-South greenway along the west side of the site up to the level of the intersecting East-West greenway at the centre of the proposed development site. Currently, there is an approximately 6.0 m (20 ft.) high concrete building located on the subject site with no setback to west property line. This building will be removed when the subject development commences. It is expected that the future development of the site to the west will butt into the parkade and meet the grade of the North-South greenway on the subject site as the greenway will also be provided on the adjacent property.)

(The second variance is for the parkade wall under Buildings 3 and 4 on the east side of the proposed Lot 2 adjacent to a 6.0m (20.0 ft.) lane. The wall ranges between approximately 2.0 m (8.2 ft.) to 3.0 m (9.8 ft.) in height above grade of the adjacent east lane. This condition is an interim condition as a future pedestrian path will be constructed within the east lane at a higher grade rising up to the level of the intersecting East-West greenway. An ultimate grading plan is included within the Development Permit plans that address this future raised pathway with planter walls not exceeding 1.2 m (4.0 ft.) against the final pedestrian pathway elevation (see Architectural plans A3.04 and A3.05). Staff have also discussed this proposed change of grade with the owners of the future development site to the east who will be required to construct the pathway over the current lane alignment as discussed above.)

- 3) Reduce the required visitor parking from 0.20 spaces /dwelling unit to 0.15 spaces /dwelling unit for the development as a whole.

(Staff supports this visitor parking reduction beyond the maximum 10% TDM reduction of 10% to 0.18 spaces/dwelling unit set out under the zoning bylaw. Of note, the TDM Rezoning Considerations allowed for an overall 7.5% reduction in total parking requirements (resident and visitor parking combined) which would allow for flexibility in allocating the 7.5% parking reduction between the resident and visitor parking. As the visitor parking is proposed to be reduced by 25% under the Development Permit (more than the 10% TDM maximum), a formal variance is required. The applicant has agreed to interconnect the visitor parkade intercoms and include parkade entrance signage allowing for visitor parking sharing for Buildings 1 / 2 and Buildings 3 / 4 so each parkade provides the 0.15 visitor spaces for the two buildings located above. This parking reduction was approved by City Transportation and results in having 51 visitor parking spaces located within the parkade on the proposed Lot 1 and 49 visitor spaces located within the parkade on the proposed Lot 2. The TDM package is discussed below in this report.)

- 4) To relax the requirement to provide two (2) large WB-17 loading spaces for the site.

(Staff supports the proposed variance as there will be four (4) medium SU9 loading spaces within the development site with one (1) space being located next to each building's lobby entrance off of Cedarbridge Way. While most moving and delivery trucks will be accommodated by the SU9 spaces on-site, City Transportation has also provided that they will offer temporary on-street loading permits near the building lobbies upon-request on occasion when very large semi-trailers need to load.

Advisory Design Panel Comments

The Advisory Design Panel reviewed the application on July 18, 2012. A copy of the relevant excerpt from the Advisory Design Panel Minutes is attached for reference (**Attachment 2**). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '***bold italics***'.

Analysis

Conditions of Adjacency

The developer will construct an entirely new River Road from Gilbert Road to the east side of the subject development site where the former CPR line is now located. This section will include two (2) eastbound and two (2) westbound lanes along with an adjacent bi-direction separate bike path and sidewalk immediately to the north of the development.

On the north side of the new River Road, the site containing existing light industrial buildings is designated for the new Riverfront Park in the CCAP. Upon creation of this park, the development will be afforded views of the park, and better views of the Middle Arm of the Fraser River and North Shore mountains in the distance.

There will be widening of the Alderbridge Way to the south, with new sidewalks and treed boulevards being constructed as part of the subject development.

To the west, the existing "V-Tech" building will remain until sometime in the future when the site is redeveloped. As a Greenway, connecting Alderbridge Way with Gilbert Road, is designated in the CCAP along the west property line of the site, the west-half of this greenway of at least 5.0 m (16.5 ft.) will be landscaped and included within a Statutory Right of Way (SROW) to complement the half width of the north-south greenway now being provided on the subject development. To the northwest, the applicant is required to construct the full curb to curb widening of Gilbert Road northward of approximately where this greenway intersects Gilbert Road.

To provide for an additional pedestrian pathway linking Alderbridge Way with New River Road within the wide block between Cedarbridge Way and Minoru Boulevard, Planning, Transportation, Parks and Engineering staff have agreed to close the northerly three-quarters of the lane immediately to the east of the site. With the current application, there will be reconstruction of the southern part of the current lane to the east along with registration of SROW for a sidewalk. The remaining northerly two-thirds of the lane will remain in its current state for the time being. At the time of redevelopment of the current warehouse building immediately to the east, this remaining section will be closed and re-grade and turned into another pedestrian link which will intersect the major East-West greenway crossing the subject and adjacent site. Lastly, Cedarbridge Way will be constructed as part of this application through the development site from Alderbridge Way to New River Road.

Urban Design and Site Planning

The proposed development is composed of a family of four (4) buildings centered on the extension of Cedarbridge Way from Alderbridge Way to the new River Road extension.

Buildings' Orientation: The four (4) buildings have a similar U-shaped building form three (3) sides around the garden courtyards for each building that rises between four (4) to six (6) storeys

above street grade. Differentiation amongst the buildings has been achieved by mainly varying the orientation of the “U” shaped buildings and courtyard openings while differentiating the materials and small-scale facade articulation between Buildings 1 and 4 facing Alderbridge Way and Buildings 2 and 3 facing the New River Road.

View Corridors: View corridors are particularly important due to the proposed riverfront park being developed immediately to the north, and the distant mountain views to the north over the existing warehouses which occupy the site of the future Riverfront Park. The spacing between the buildings across Cedarbridge Way allows for good view corridors north-south and sunlight penetration.

East-West Greenway: The CCAP’s envisioned east-west greenway connects the Oval Village local commercial and major recreational destinations to the Aberdeen Village Commercial and Arts District. The applicant has addressed these components to the satisfaction of Planning, Transportation and Parks Department staff.

The separation between the buildings averages approximately 22 m (72.5 ft.) along the greenway, leaving sufficient area for ground floor patios and common strata property on each side. The greenway will include a 3.5 m (11.5 ft.) wide hard-surfaced public path that extends from the east to the west boundaries of the development, crossing Cedarbridge Way in the centre of the development site. The greenway also includes landscaping on both sides of the hard-surface pathway.

North-South Greenway: This greenway, extending from north to south on the west side of the proposed west Lot 1, will include a 3.5 m (11.5 ft.) hard-surfaced public path which intersects the west end of the east-west greenway. The greenway also includes landscaping on the east of the hard surface path within the 5.0m (16.5 ft.) SROW for east half of the greenway being constructed for the subject development. The future development of the “V-Tech” site to the west will be required to provide the remaining landscaping within a further a 5.0m (16.5 ft.) SROW, thus completing the west side of the north-south greenway.

Gilbert Road Boulevard and Greenway: The development of the greenway on the east side of the very wide unused Gilbert Road allowance, a prominent gateway to the City Centre, remains to be finalized. Given that there will be approximately 20 m (66 ft.) of open space between the project property line and the future Gilbert Road curb in this high visibility area, a plaza, pedestrian and cycling paths, lighting, significant tree planting and a major \$350,000 Landmark Public Art piece is included under the project’s Public Art Plan.

Parking and Transportation Demand Management (TDM): The proposed project includes a total of 871 parking spaces with 462 spaces (including 51 visitor spaces) in the parkade on Lot 1 for Buildings 1 and 2, and 409 spaces (including 49 visitor spaces) within the parkade on Lot 2 for Buildings 3 and 4 (See Architectural plans for full parking statistics). While there are 192 proposed tandem parking spaces, there will be a covenant register on title ensuring that each pair of tandem parking spaces is assigned to the same dwelling unit.

While there is one (1) parkade under Buildings 1 / 2 on Lot 1 and one (1) parkade under Buildings 3 / 4 on Lot 2, each building has its own parkade entrance and adjacent partitioned visitor parking area. Buildings 1 and 2 on Lot 2 have parkade entrances from the west side of Cedarbridge Way. The parkade for Building 3 is located on the east side of Cedarbridge Way while the entrance to the Building 4 parkade is from the east lane near Cedarbridge Way.

The parking includes a combined visitor and resident parking reduction of 7.5% below the parking requirements set out in Zoning Bylaw 8500 subject to the provision of TDM measures included in the Rezoning Considerations (see attached Development Reference Plans). This combined parking TDM reduction includes visitor parking being reduced by 25% from 0.20 to 0.15 spaces/unit on each of Lot 1 (Buildings 1 & 2) and Lot 2 (Buildings 3 & 4) as discussed in the variance section above.

In lieu of this reduction, the City accepts the Developer's offer to voluntarily:

- Contribute \$100,000 to the City for the construction of a 3.0 m (9.8 ft.) bike/pedestrian pathway along the east side of Gilbert Road from the southern end of the applicant's required frontage improvements to Lansdowne Road. (Not eligible for DCC credits.)
- Contribute \$25,000 to the City for a City Centre-type bus shelter. (Not eligible for DCC credits.)
- Enter into an agreement with the City to ensure that the electrical vehicle and bicycle plug-ins be provided as a condition of issuance of the City building and occupancy permits for each building. This will include provision of 20% of the total resident parking spaces in each parkade with electric service for vehicle plug-ins with conduits, circuits breakers and wiring (actual outlets to be provided later by strata owners). Provision of one (1) standard 120 volt electric plug-in for every forty (40) resident bicycle parking spaces.

It should be noted that there will be also on-street parking provided on Cedarbridge Way throughout the day and off-peak on-street parking on Alderbridge Way and River Road over the short to intermediate term.

Bicycle Parking: The proposed project includes a total of 860 resident bicycle parking spaces with 424 resident spaces in the parkade and 68 surface visitor spaces for Buildings 1 and 2; and 436 resident spaces within the parkade and 64 surface visitor spaces for Buildings 3 and 4. The resident and visitor bicycle parking provided meets the minimum requirements of Zoning Bylaw 8500.

Loading Space Requirements: Section 7.13 of Zoning Bylaw 8500 requires that one medium-sized (1) SU9 off-street loading space be provided for each building and one large (1) off-street WB 17 (17 m trucks) loading space be provided for every two (2) buildings. The applicant has accommodated the four (4) required SU9 loading spaces on either side of the East-West greenway junction with Cedarbridge Way. Given the low frequency of use of such large WB17 trucks in a purely residential project, staff supports a variance to relax this requirement as discussed above.

Recycling and Garbage: Each parkade services a pair of buildings and includes one (1) large recycling / garbage room and garbage compactor meeting the size and access requirements of the City's Sanitation Division. However, given the long distance between these rooms and loading bays where recycling trucks will pick-up and given there are over 300 units serviced by each room, the Sanitation Division is requesting that the applicant provide suitable golf carts for recycling movement for each building. The applicant is required to post a security with the City for the provision of these golf carts prior to Development Permit issuance.

Architectural Form and Character

Key Street Walls: This development expresses itself with continuous street walls on Alderbridge Way and New River Road which are two (2) of the major curvilinear streets in the City Centre. The development's units face streets with stairs and entrance doors with design techniques to deliver a townhouse character. As well, the use of stepped patio and landscaped terraces reduce the appearance of the grade difference.

Street Frontage Elevations: The current Alderbridge Way elevation is lower at 1.5 m (4.9 ft.) compared to the New River Road which is located at 2.6 m (8.6 ft.) geodectic elevation. This elevation difference results in a 2.5 m (8.2 ft.) grade difference between Alderbridge Way and the first floor of the adjoining units. To address this grade difference, the patios have been located 1.5 m (4.9 ft.) above street level instead of at the unit ground floor level of 2.5 m (8.2 ft.) above the street. To achieve this grade transition, the stairs have been broken into two (2) sections. Also, cladding materials have been extended over the upper part of the parkade that lies between the unit's floors and the patios, further reducing the appearance of the grade different between street and unit levels. Terraced planters have been located between the unit patios and street level.

The grade difference of approximately 1.4 m (4.6 ft.) on New River Road and Gilbert Road presents far less of a challenge and has been well addressed through the use of terraced planters located between the unit patios and street level. Lastly, Cedarbridge Way begins at the lower Alderbridge Way grade rising to being level with the ground floor of the buildings and the East-West greenway and then falling again to the River Road grade.

Individual tower-like elements have been included at corners of all four (4) buildings to anchor the buildings at the intersections of Cedarbridge Way with Alderbridge Way and the east-west greenway. Also, a prominent tower element has been included at the north-west corner of Building 2 given the prominent view of this building from the adjacent Gilbert and River Roads.

Building Height and Rooflines: Each of the four (4) buildings rises to six (6) storeys in height. The buildings include terraces downward to as low as four (4) storeys to provide for a variety of building form and larger decks for some of the units on the top two (2) floors of each building. The use of inverse gable or butterfly roofs and higher ceilings for the sixth floor in each building provides variety to building masses while also maintaining continuity within the family of buildings in the proposed development.

Building Materials and Articulation: While the buildings are similar, the use of varied architectural building frames, balcony forms, materials, and wall articulation have been utilized to provide for further differentiation between the buildings. Facing Alderbridge Way, Building 1 has a fibre-cement cladding materials with darker brown and grey colours while Building 4 includes both fibre-cement metal panelling with generally brighter colours, including orange.

Facing River Road, Buildings 2 and 3 are designed with more of a mid-20th modernist style with bolder frames, more simple articulation and use of lighter coloured fibre panelling that appears to read as stone in Building 2 and read as concrete in Building 3.

In summary, the massing, articulation, and materials give these wood-frame buildings the appearance of concrete buildings with varied characters.

On-Site Landscape and Amenity Space Design

On-Site Outdoor Amenity: As noted above, the “U” shape buildings provide for large semi-private courtyards while maintaining highly visible smaller water features which provide variety to gardens and create attractive transitions from the semi-private courtyards to adjacent public streets. The typical width of the courtyards from building face to building face is approximately 35 m (115 ft.) which provides privacy and ample room for on-site outdoor amenities and patios for each ground floor unit.

The applicant has responded to staff’s concern about having enlarged play areas included within the courtyards of Buildings 1, 2 and 4 on either side of Cedarbridge Way. Multi-purpose amenity/BBQ areas are provided for the Buildings 1 and 2 courtyards while community garden plots are provided in the Building 2, 3 and 4 courtyards.

The OCP includes on-site open space guidelines for active uses including socializing, children’s play and related use. The additional CCAP guidelines provide for on-site walkways, planting, garden plots, etc. The development includes approximately 5,176 m² (55,714 ft.²) of such on-site socializing areas.

Common Indoor Amenity: A shared indoor amenity space of approximately 951 m² (10,235 ft.²) within Building 1, which includes an indoor swimming pool, will open up on a large common courtyard patio. There will also be small meeting rooms of approximately 21m² (230 ft.²) in each of Buildings 3 and 4. There will be registration of a reciprocal access easement on the proposed Lots 1 and 2 to ensure that the large proposed indoor recreation space in Building 1 is accessible to all building residents and is constructed in the first phase.

Private Outdoor Amenity: As provided under the OCP DPA guidelines on the minimum private amenity space requirements, the architect has confirmed that each ground level unit has been provided with at least 24 m² (258 ft.²) of patio space and each upper floor unit has been provided with at least 6 m² (65 ft.²) of deck space.

Trees: Of note, while there are no trees on the subject site, staff have requested and reviewed an arborist’s report confirming that the proposed buildings and north-south greenway with a retaining wall (discussed earlier in the report) will not adversely affect several significant trees on the adjacent property to the west.

Summary: In summary, staff feels that the applicant has gone a long way to developing a wood-frame project that has the modern, urban character desired for the City Centre and which responds well to the CCAP’s DPA design guidelines. Particularly, the applicant has responded well to staff and the ADP recommendations to apply high quality, durable materials and undertake modifications to the detailed design of the buildings.

Crime Prevention Through Environmental Design

Casual surveillance of the public streets is provided by the ground-orientated units with individual patios between 1.2 m (4.0 ft.) and 1.5 m (5.0 ft.) above the public sidewalks and front doors facing the streets. The average 22.0 m (72.5 ft.) spacing between buildings along the east-west greenway provides for sufficient building separation, but also provides for surveillance from the units on the first four (4) floors of the adjacent buildings which are placed at an average setback of 6.0 m (20 ft.) from each side of the 10 m (33 ft.) greenway right-of-way.

Proposed street lighting and on-site lighting fixtures provide for safe pedestrian circulation from streets and along both of the east-west and north-south greenways. The City Centre's modern "Type 8" lamp standards with "Hestia" style light fixtures are provided on both the greenways and streets. These fixtures are outfitted to provide for white light and in such a way to prevent ambient light pollution that conflicts with neighbouring apartment units.

Special attention has been given to open-up the greenway intersections where they intersect with public streets. In particular, the intersection of the east-west greenway with Cedarbridge Way has been opened up to create mini-plazas on each side of Cedarbridge Way by shifting planters and loading spaces away from the greenway. These mini plazas include the four (4) SU9 loading which can read and appear as part of the mini plazas when not occupied by loading trucks.

Public streets and greenways and the semi-public spaces (building courtyards) have been well defined and highly visible from the private patios and decks. Symbolic barriers have been incorporated through changes in vegetation, grade and landscape features. (e.g. low walls, fences and trellises).

Accessibility

The proposed 528 Basic Universal Accessible Housing units are each designated throughout the buildings plans on the attached Development Permit architectural plans which confirm the units meeting all of the accessibility requirements under Section 4.16 of Zoning Bylaw 8500. Compliance with the design requirements will be re-confirmed through the Building Permit process.

Affordable Housing

Following the City's Affordable Housing Policy, the applicant will be providing 40 affordable housing (low-end market rental) to the satisfaction of the City. The affordable housing units will have a total combined habitable area of at least 5% of the residential floor area ratio (FAR) permitted (not including hallways and common areas) with a minimum 30,931 ft.² combined habitable area to be provided in the assigned buildings planned to be developed in Buildings 1, 3 and 4 as follows:

Location	1 Bedroom	2 Bedroom	Total
Building 1	4	5	9
Building 3	0	8	8
Building 4	8	15	23
Overall Total	12	28	40

The affordable housing units are located on in Buildings 1, 3 and 4 and are designated on the attached Development Permit Architectural Plans which is included under Affordable Housing Bylaw 8936 considered by Planning Committee on September 5, 2012.

Noise Related Aspects of the Proposal

A restrictive covenant is being registered requiring that prior to the Development Permit being issued, the applicant is required to provide reports by a registered professional qualified in acoustics identifying the measures needed to satisfy the "Noise Management" standards set out in the OCP. As the site is temporarily situated in a transitional industrial area and may be subject to noises not typical of other residential neighbourhoods, a covenant was registered on title during the rezoning phase, on this regard.

Public Art

The City has accepted the applicant's offer to voluntarily provide \$440,411 to Richmond's Public Art Program. This amount includes a cash contribution of \$139,700 to be provided to the City's Public Art Reserve Fund for a Landmark Art piece and providing a security to the City for \$300,711 for other Public Art with locations generally shown on sheet L.1.8 of the landscape plans. The applicant has submitted a detailed Public Art Plan which will be reviewed and need to be accepted by Planning and Public Art staff.

District Energy Utility (DEU)

The applicant has agreed to commit to connecting to the proposed City Centre DEU. The DEU terms will be finalized as a Rezoning Consideration include:

- Design and construction of the development's buildings to facilitate hook-up to a DEU system (e.g., hydronic water-based heating system); and
- Entering into a Service Provision Agreement(s) and statutory right-of-way(s) and/or alternative legal agreements, to the satisfaction of the City.

Sustainability

The applicant has committed to meet the Canadian Green Building Council LEED Silver 2009 criteria. In this regard, Stantec, providing LEED consulting services for the project, has provided the LEED Scorecard confirming how the project meets LEED Silver criteria. A letter of assurance will be required prior to issuance of an occupancy permit for each building confirming meeting the LEED Silver score. The proposed project includes at least 50 points with up to a possible 54 points within the LEED Silver category of 50 to 59 points.

The CCAP requires two LEED criteria to be met, including *Heat Island Effect: Roof Credit* and *Storm Water Management Credit* which have both been met. Other sustainability measures, contained within the development include:

- Permeable pavers have been applied in pedestrian pathways areas to allow for storm water infiltration, and less storm runoff;
- Water efficient landscaping with planting is drought tolerant and many are native species, reducing water use by 50%;

- Domestic water use reduction, including use of low flow fixtures will be specified to conserve water;
- Optimize energy performance with a 25% to 56% energy cost savings;
- Construction waste management, diverting 50% to 75% of waste to recycling; and
- Use of low-emitting interior finishing materials such as adhesives, sealants, paints and carpets.

Conclusions

Issues identified during the Development Permit Application design review process have been considered and addressed, including staff and Advisory Design Panel comments regarding adjacencies and landscape design. In summary, the development:

- Forms a distinctive, modern, wood-frame, high-quality, high-density yet low-rise part of the Lansdowne Village neighbourhood;
- Completes important sections of the major road network in the CCAP including New River Road east of Gilbert Road and the extension of Cedarbridge Way to New River Road; and
- Provides the first significant section of the major east-west and north-south greenways that will connect Lansdowne Village to the rest of the City Centre.

Therefore, staff recommends support for this Development Permit Application.



Mark McMullen
Senior Coordinator-Major Projects
(604-276-4173)

MM:blg

Attachment 1: Development Application Data Sheet

Attachment 2: Excerpt of Minutes from July 18, 2012 Meeting of the Advisory Design Panel

The following are to be met prior to forwarding this application to Council for approval

- Receipt of a Letter-of-Credit for landscaping in the amount of \$826,437 based on an estimate provided by the landscape architect.
- Receipt of security for one (1) recycling golf cart for each of the two (2) parkade recycling/garbage rooms in an amount to secure purchase of such vehicles to the satisfaction of the City.

- The applicant is required to retain a registered professional who is qualified in the design and installation of an air condition, or an alternative cooling system that meets the ASHRAE 55-2004 standard, or subsequent updates as they may occur. Further, the retained registered professional is to certify that any required noise insulation measures have been installed according to the report's recommendations before the building may obtain an Occupancy Permit. Maximum noise levels (decibels) within the dwelling units must be as follows:

Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

Prior to future Building Permit issuance, the developer is required to complete the following:

- Address possible changes to the width, material and elevation of the greenway crosswalk at Cedarbridge Way under the Servicing Agreement to the satisfaction of the City's Planning, Transportation and Parks Departments.
- Incorporation of the above-noted acoustical report's recommendations into the BP drawings.
- Confirmation of accessibility measures under Section 4.16 of Zoning Bylaw 8500 being incorporated into BP drawings for the designated 528 Universal Accessible Units.
- Obtain a Building Permit for any construction hoarding associated with the proposed development. If construction hoarding is required to temporarily occupy a street, or any part thereof, or occupy the air space above a street or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. *For further information on the Building Permit, please contact Building Approvals Division at 604-276-4285.*
- Submission of a construction traffic and parking management plan to the satisfaction of the City's Transportation Division (<http://www.richmond.ca/services/tp/special.htm>).

Attachment 1



City of Richmond
6911 No. 3 Road
Richmond, BC, V6Y 2C1
www.richmond.ca

Development Application Data Sheet

Development Applications Division

RZ 11-585209

Address: 7731 & 7771 Alderbridge Way

Applicant/Owner: Onni Contracting Ltd

Owner: Onni 7731 Alderbridge Way Holding Corp. & 7771 Alderbridge Way Holding Corp.

Planning Area(s): City Centre Area (Lansdowne Village)

Floor Area: No change is proposed in maximum permitted floor area or density

	Bylaw Requirement	Proposed Development	Variance
Lot Size (Min.)	<ul style="list-style-type: none"> 2400 m² (25,833 ft²) 	<ul style="list-style-type: none"> Lot 1: 13,288.40 m² (143,040 ft²) Lot 2: 11,885.75 m² (127,937 ft²) 	<ul style="list-style-type: none"> None
Lot Coverage (Max.)	<ul style="list-style-type: none"> 60% for buildings 80% for building and non porous surfaces 	<ul style="list-style-type: none"> Lot 1: building footprint: 45% non-porous surfaces: 69.5% Lot 2: building footprint: 45% non-porous surface: 70.3% 	<ul style="list-style-type: none"> None
FAR	<ul style="list-style-type: none"> 1.2, up to 2.0 FAR of the total site with provision of 5% of total floor area for affordable housing units. 	<ul style="list-style-type: none"> 2.0 FAR of the total site with 20 m Cedarbridge dedication and 5% affordable housing as per the RAH2 zone and Zoning Amendment Bylaw No. 8884 text not deducted. 	<ul style="list-style-type: none"> None
Habitable Floor Elevation (Min.)	<ul style="list-style-type: none"> Residential: 2.9 m geodetic 	<ul style="list-style-type: none"> Residential: 4.064 m geodetic Local exception permitted for one (1) lobby per building. 	<ul style="list-style-type: none"> None
Height (Max.)	<ul style="list-style-type: none"> 25 m, but with specific areas allowing up to 35 m as outlined in CCAP. 	<ul style="list-style-type: none"> Varies, but less than 25 m above finished grade in all cases. 	<ul style="list-style-type: none"> None

	Bylaw Requirement	Proposed Development	Variance
Setbacks (Min.)	a) 3.0m @ Alderbridge P/L b) 1.5m @ East Lane P/L c) 3.0m @ River Road P/L d) 3.0m @ Cedarbridge P/L e) 3.0m @ Gilbert P/L f) 1.5m @ West Side P/L	a) 4.8 m for Building 1 and 5.3 m Building 4 @ Alderbridge from PROP b) 0.0m @ East Lane P/L for parkade c) 3.0m @ River Road P/L d) 0.0m @ Cedarbridge Way P/L for parkade e) 0.0m @ Gilbert Road P/L f) 0.0m @ West Side P/L for parkade Based on setback to PROP/SROW on Alderbridge Way; setbacks from the actual property lines are greater.	<ul style="list-style-type: none"> DVP for interior side west P/L on Lot 1 and east P/L on Lot 2 on from 1.5m to 0.0 m; exterior side for Gilbert Road for Lot 1 and exterior side for Cedarbridge Way for Lots 1 and 2
Off-Street Parking	<u>Lot 1: Parkade (Bldgs1/2): 475</u> Min Residents/affordable: 407 (max small car: 50%) Min Visitors: 0.2/unit: 68 (With maximum 7.5% TDM overall reduction) <u>Lot 2 Parkade: (Bldgs 3/4): 438</u> Min Residents/affordable: 374 (max small car 50%:) Min Visitors: 0.2/unit: 64 (Before maximum 7.5% TDM overall reduction and 25% visitor reduction to 0.15 spaces/unit)	<u>Lot 1: Parkade (Bldgs1/2): 462</u> Residents/Affordable: 411 (small car: 49%) Visitors: 51 <u>Lot 2: Parkade (Bldgs3/4): 409</u> Residents/Affordable: 360 (small car: 50%) Visitors: 49 (With maximum 7.5% TDM overall reduction and 25% visitor reduction to 0.15 spaces/unit)	DVP to reduce visitor parking to 0.15 spaces/dwelling unit.
Bicycle Parking	<u>Lot 1: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 425 Visitor (0.2/unit): 68 <u>Lot 2: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 425 Visitor (0.2/unit): 68 	<u>Lot 1: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 399 Visitor (0.2/unit): 68 <u>Lot 2: Parkade (Bldgs1/2)</u> <ul style="list-style-type: none"> Resident (1.25/unit): 436 Visitor (0.2/unit): 64 	<ul style="list-style-type: none"> None
Loading	<ul style="list-style-type: none"> 2 medium; 2 large with one being provided for each building with sizes as per Section 7.10.2. To be on-site. 	<ul style="list-style-type: none"> Required 4 SU9 loading spaces with 1 provided for each of the four buildings. 	<ul style="list-style-type: none"> DVP to relax the requirement for 2 WB 17 spaces required.

Attachment 2

Excerpt from the Minutes from The Design Panel Meeting

Wednesday, July 18, 2012 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall

Panel Discussion

Comments from the Panel were as follows:

- like the roof design, i.e. the butterfly roofs with four different forms and facing different directions; appealing feel for the neighbouring towers and from River Road;

Butterfly roofs are a distinguishing characteristic of the project and serve to unify all four buildings.

- four forms are similar in height and mass and a lot of detail has been introduced to differentiate the four forms; differences are not just in colour and materials but also in forms and details; four forms have subtle differences; applicant has addressed a previous comment of the Panel to differentiate the building in terms of colour and texture;

We have worked with the Staff and the preliminary ADP comments to further differentiate the four buildings in terms of massing and material treatment.

- concern on heat gain on metal panel system and the use of dark colours in Building 4; however, appreciate the right balance in the use of bright colours in small and different areas in comparison to the two field colours;

Metal panels will be properly detailed to ensure heat gain is minimized and ventilation is provided behind panels.

- model presented by the applicant should have been a coloured model; use of materials should have been shown in the model to show the significant and minute details as it would be difficult to see them in the elevations and renderings;

3D views were supplied to suggest materiality of each of the buildings.

- there are a lot of community gardens in the project; like its use in the project but not in the smaller courtyards, i.e. in Buildings 2, 3 and 4; garden plots will diminish the usable space for residents due to their limited use during certain seasons of the year, e.g. during winter when they are mostly dormant and visually unappealing; consider scaling back the community gardens in the courtyards;

Community gardens have been slightly scaled back to make room for other amenities in two of the courtyards. Community gardens can be made to stay more attractive through strata regulations and the use of a management company.

- concern on the central intersection between the roadway and greenway due to truck loading; consider having more intricate pattern for the pavers, e.g. by using two different materials and textures;

Landscape paving patterns have been updated to provide more clear distinction between the different areas. Planting has also been used to establish separation.

- overall, support the project;

Noted

- hope to see aging in place features in the project; consider sliding door access to ensuite washrooms;

Aging in place features will be incorporated at BP stage including; blocking in bathrooms for grab bars, lever handles, pocket doors for washrooms.

- interesting model for the neighbourhood; building is interstitial type; inverted roof is interesting;

We feel that this project provides an alternate form and variety of housing typology to the tower and townhouse developments occurring in the area.

- agree with comment regarding the preponderance of plot gardens in the courtyards;

As mentioned above, community gardens have been reduced and other amenities expanded.

- no major concern with the courtyards; they are distinct from each other and have a lot of programmatic elements that make them useful within a small space; nice hierarchy of spaces in the courtyards;

Thank you

- there is opportunity to create a public realm in the center court that is significant and pedestrian-focused; however, location of loading presents a challenge to this opportunity; need to work with Transportation to figure out how to integrate loading at the center court in a more sensitive way; consider more detail in the crossroads to privilege the walking traffic, e.g. add bollards and designate loading on the street as loading at the center court is a suburban model for loading and not an urban model;

The centre court has been refined to help create good public spaces and incorporate loading. Loading bays have been located closer to the buildings to allow for a wider centre court. Bollards have been added to define the loading spaces.

- reconsider building garden plots as the greenway ROW feels domestic and tends to privatize the place; 4 x 8 ft. timber grids don't enhance visual appeal; consider other ways to achieve urban agriculture that are more civic, e.g. street trees bearing fruit, grape/kiwi vine on a trellis, etc.; need not depend on garden plots to animate the space; greenway should become a really critical walkway link in the emerging neighbourhood;

Greenway garden plots have been removed from the project.

- applicant has not provided information/vision regarding how the western terminus of the greenway might integrate with the properties to the west; similar suggestions for east lane integration; information was requested by the Panel during the rezoning stage; applicant is encouraged to suggest some ideas;

The West end of the greenway is designed to end at the intersection of the North-South greenway per City direction. The East end will terminate at the City East lane as a temporary solution. In the future the East lane will be converted to a greenway by others, which will include raising the elevation of the future greenway to meet the grade of the East-West greenway on the Onni development. This will allow the East West greenway to be extended eastward across neighbouring sites to the east as planned for in the CCAP.

- sections on Alderbridge Way are fairly well resolved; applicant has adequately handled a difficult scenario, i.e. a 2-meter grade change;

We have worked closely with staff to create a pedestrian friendly sidewalk along Alderbridge. This has been achieved by; setting back buildings 1 and 4 from Alderbridge further than the setback allows, providing stepped planters along the front of the parkade, and dropping the front patios of the Alderbridge fronting buildings down from 4.0m to 3.0m

- the project suffers from the model presented to the Panel; the model does not resemble the quality of its design; model should show colours and textures;

3D views were supplied to suggest materiality of each of the buildings. In future, a coloured model will be provided.

- each building works well as standalone but not together as one development; buildings are object buildings, i.e. unique and specialized buildings which stand out differently from each other;

Buildings are differentiated to address site specific conditions, including prominent corners at the northwest, and at the new intersections of Cedarbridge with Alderbridge and River Road. Per the preliminary ADP and previous direction from Staff, we have endeavoured to create more differentiation between the building material and massing. However, there are unifying elements that work to tie all four buildings together as one "precinct", including; butterfly roofs, corner tower element massing at the center of the site, cedarbridge and greenway lobby treatments, material treatment and massing of upper floors, and consistent landscape features.

- buildings are too similar, both in the use of materials and articulation; too much "sameness" in the project; elements are the same but articulated a little bit differently; not enough difference in mass, colour and texture;

See opposing comment above. We have introduced more differentiation between materiality and massing of each building. Since the formal ADP, we have created more distinct volumes within each of the four buildings. This serves to break down the lengths of the buildings into smaller volumes, and create more diverse compositions within each of the buildings.

- the use of the word "monolithic" is not appropriate to describe the buildings which are actually pedestrian friendly and have a residential character;

The word "monolithic" was incorrectly used to describe the heavier base of the buildings relative to the cleaner, lighter top floors.

- concrete look of wood-frame buildings would be difficult to achieve; flashing details would be prevalent; rooftop cantilevers don't work; eyebrow detail will require parapet and thus a deeper profile; waterproofing does not have termination;

Details were developed and presented at ADP that illustrate how we are achieving the appearance of the buildings. Flashing details will be minimized and integrated within the pattern of material panels. Eyebrow details are shown with flashing in places and parapets in other locations where the deeper profile is aesthetically warranted.

- massing of the building needs to be broken down; massing needs relief at certain points on the site; there is need for certain elements to become expressed; The Gardens by Townline is a precedent;

The buildings have been further broken down into distinct volumes. This serves to break down the lengths of the buildings into smaller elements, and create more diverse compositions within each of the buildings. Secondary balconies have been removed in locations to emphasize breaks in the massing similar to what was done at The Gardens project. Rooflines have been adjusted to reinforce corner tower elements.

- courtyards are sterile and enhance the monolithic aspect of the project; water features are contained, i.e. they don't flow from one courtyard to the other; given the monolithic nature of the project, it would have helped if the landscape would have been more grainy to soften the edges in and around the buildings;

Landscape designs have been adjusted to 'loosen up' and relax some of the formal patterns to help address this. Water feature designs don't link between courtyards, but have been designed to connect with people at public and private levels.

- project needs an extra push;

Since formal ADP and per previous responses above, we have further developed the buildings. The tower element on the northwest corner of building 2 has been improved with additional height and more expansive glazing. All four buildings have been improved with more breaks within the building length and differentiated massing elements.

- great presentation materials; comments by the Panel during the preliminary stage have been successfully addressed by the applicant;

Noted

- wood frame buildings do not appear as such as they have a concrete look; hope that this look will survive through the entire development application process; lightening up of the upper levels of the buildings, i.e. 4th, 5th and 6th levels adds to the design of the buildings; hope that this detail will be maintained; and

Though these are wood frame buildings, we will endeavour to carry out the look of the buildings as presented. Detail of the simplified upper levels will be maintained.

- applicant has done a good job in addressing the elevation changes at the street level in terms of landscaping; e.g. through stepping up.

Noted

At this point, Sara Badyal, Planner and Staff Liaison to the Panel, read the following comments of Hal Owens who left the meeting earlier:

- in terms of site context, sensitivity and connectivity to surrounding land uses are generally good; slope up at internal street is a bit steep still with high wall along pedestrian sidewalk; needs some detail at wall or planting;

Internal street is sloping at less than 5% as supported by Transportation. Stepped planters are proposed along the sidewalk that will reduce in height towards the intersection of Cedarbridge and the east-west Greenway.

- site layout is generally okay but not clear how is through access by cars to be discouraged;

Through access by cars is not discouraged. The central crossing of the Greenway and Cedarbridge is proposed to be paved to mark the crossing and link all four buildings with the greenway to the extent permitted by City transportation.

- building character, massing, roof form, facade articulation are generally okay;

Noted

- generally, the applicant has addressed the concerns from the previous presentation; however, prominent corner element is well integrated but still lacks an iconic character that announces this project;

The tower element on the northwest corner of building 2 has been improved with additional height and more expansive glazing. The additional glazing helps to further differentiate the corner element from the rest of the building.

- character of the building is essentially the same, i.e. there is little differentiation other than minor material/colour detail changes;

Additional work has been done to further differentiate the massing within each building, and between the buildings. Stacks of secondary balconies have been removed in key locations to break down the large buildings into smaller volumes. Material treatment of lower levels of each building is quite distinct, upper levels are treated similarly in order to create unity to the project.

- exterior finishes and colours are fairly good but not as robust as what is on the adjacent parcels in the Oval district; may cause this project to look like a “poor neighbour”;

Bright colours have been avoided except as accents. We feel that the more subtle palette will age better, and not be prone to ‘date-ing’ the project.

- landscape design has generally addressed most of the concerns from previous presentation;

Noted

- applicant should show where and how public art is being considered;

Omni has contributed funds at the request of the City for a landmark art piece to be located on City land at the corner of Gilbert and New River Road.

On the development site a number of potential locations for public art have been identified in the Detailed Public Art Plan prepared by ‘Id Public Art Consulting’ who are the public art consultants for the project. Areas that are hubs for pedestrian movement have been identified as potential artworks locations, such as areas along the east-west greenway, north south pedestrian link and around the lobby entrances that front onto the east-west greenway.

- consider enlarging the water features and tie into river garden or canal theme;

This option was previously explored, it was not supported by City staff as there was a need for more usable outdoor space. The elevation changes in the project would make the river or canal theme hard to see and understand at a pedestrian level. Instead, we have tried to make water accessible at a number of different levels.

- consider reducing small bits of colour and instead use a larger or bolder mass or element on each building to differentiate them.

Smaller colour elements on building 4 have been complemented with larger areas of colour at the Greenway lobby and prominent building corners. Mass of all buildings has been broken down into smaller elements to further differentiate each building and to emphasize tower and corner elements.



No. DP 12-615424

To the Holder: ONNI CONTRACTING LTD
Property Address 7731 AND 7771 ALDERBRIDGE WAY
Address: C/O MR. ERIC HUGHES, ONNI CONTRACTING LTD,
550 ROBSON STREET – SUITE 300
VANCOUVER, BC V6B 2B7

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - (a) Reduce the required exterior side yard setbacks for portions of partially below-grade parking structures on the proposed Lots 1 and 2 from 3.0 m to 0.0 m respectively along Cedarbridge Way and Gilbert Road,
 - (b) Reduce the required interior side yard setback for limited portions of partially-below grade parking structures from 1.5 m to 0.0 m along the west property line of the proposed Lot 1 and the east property line of the proposed Lot 2;
 - (c) Reduce the required visitor parking from 0.20 spaces/dwelling unit to 0.15spaces/dwelling unit for the development as a whole; and
 - (d) Relax the requirement for the provision of on-site loading spaces for two (2) WB-17 loading spaces.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Architectural and Landscape Architecture Plans labelled Plans 1 to 74 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.

No. DP 12-615424

To the Holder: ONNI CONTRACTING LTD.
Property Address: 7731 AND 7771 ALDERBRIDGE WAY
Address: C/O MR. ERIC HUGHES, ONNI CONTRACTING LTD.
550 ROBSON STREET – SUITE 300
VANCOUVER, BC V6B 2B7

6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$826,437 ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF

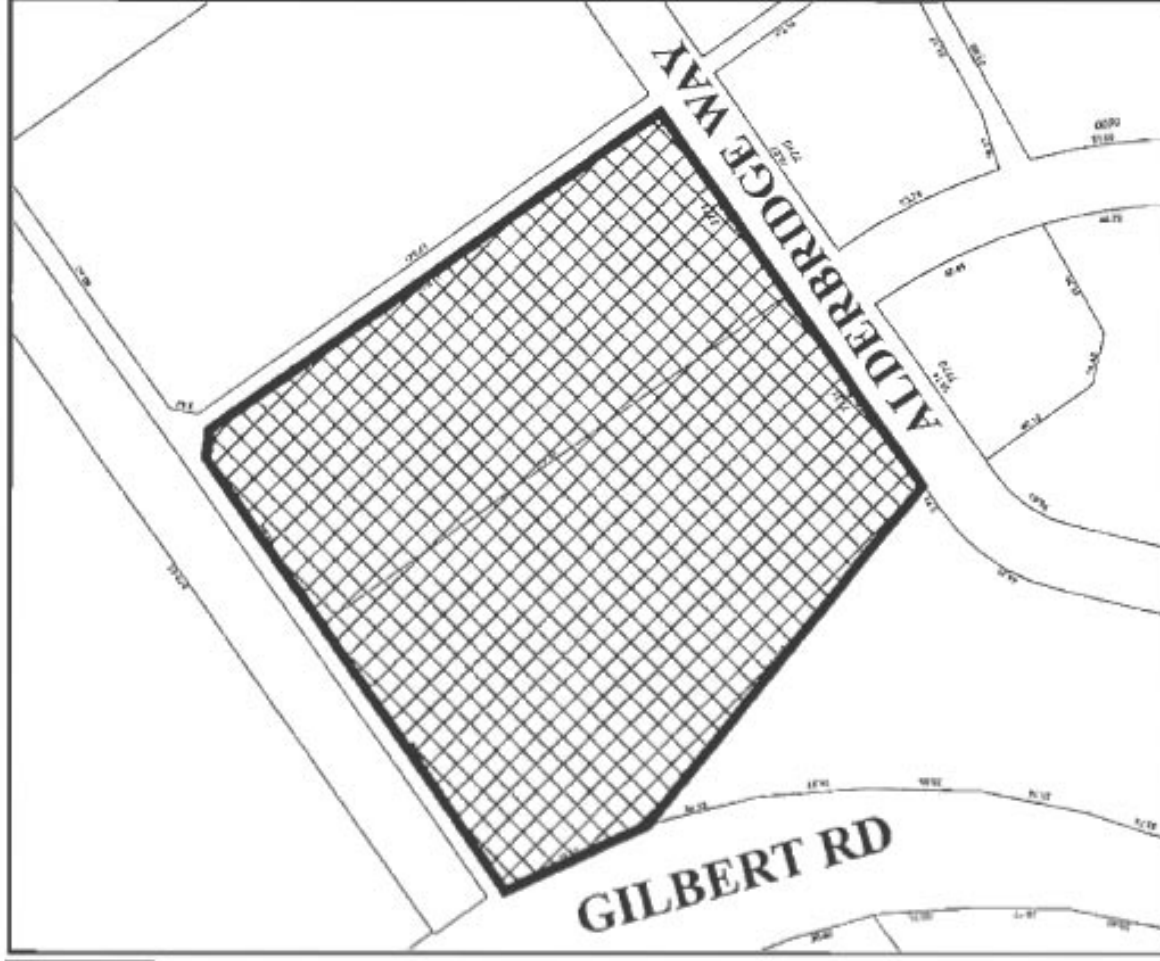
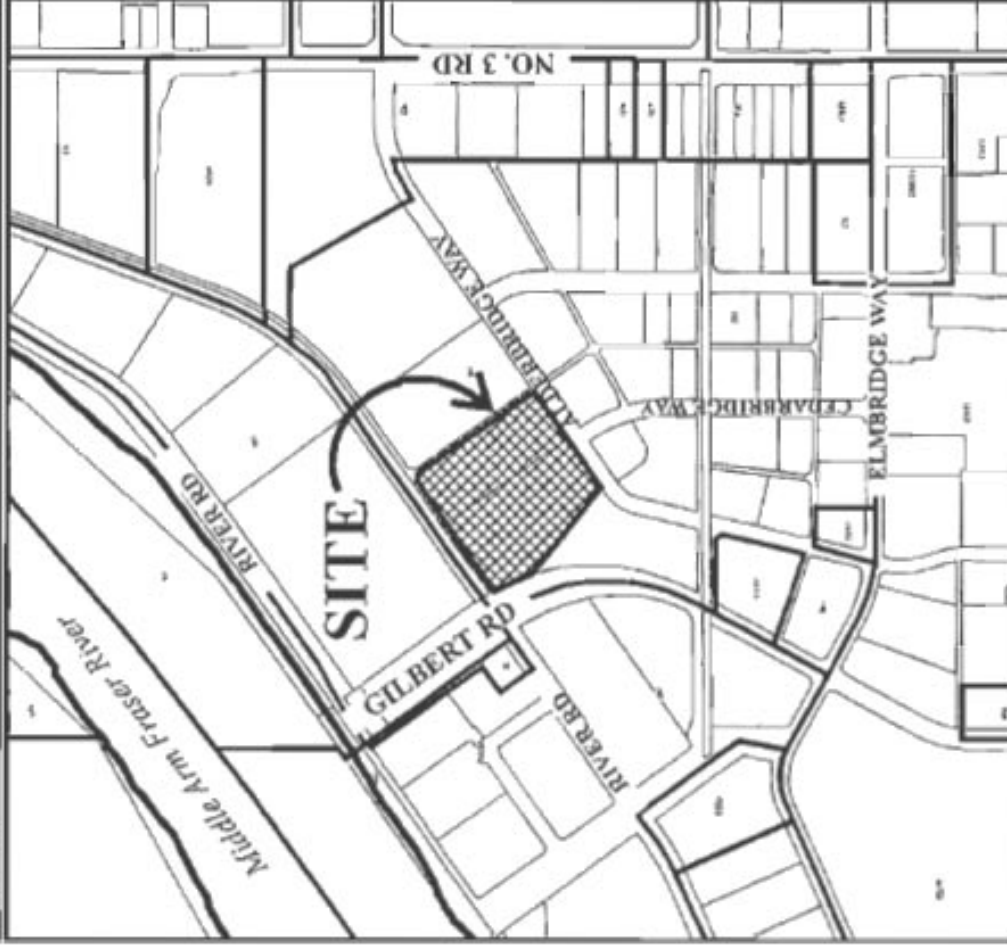
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF

MAYOR



City of Richmond

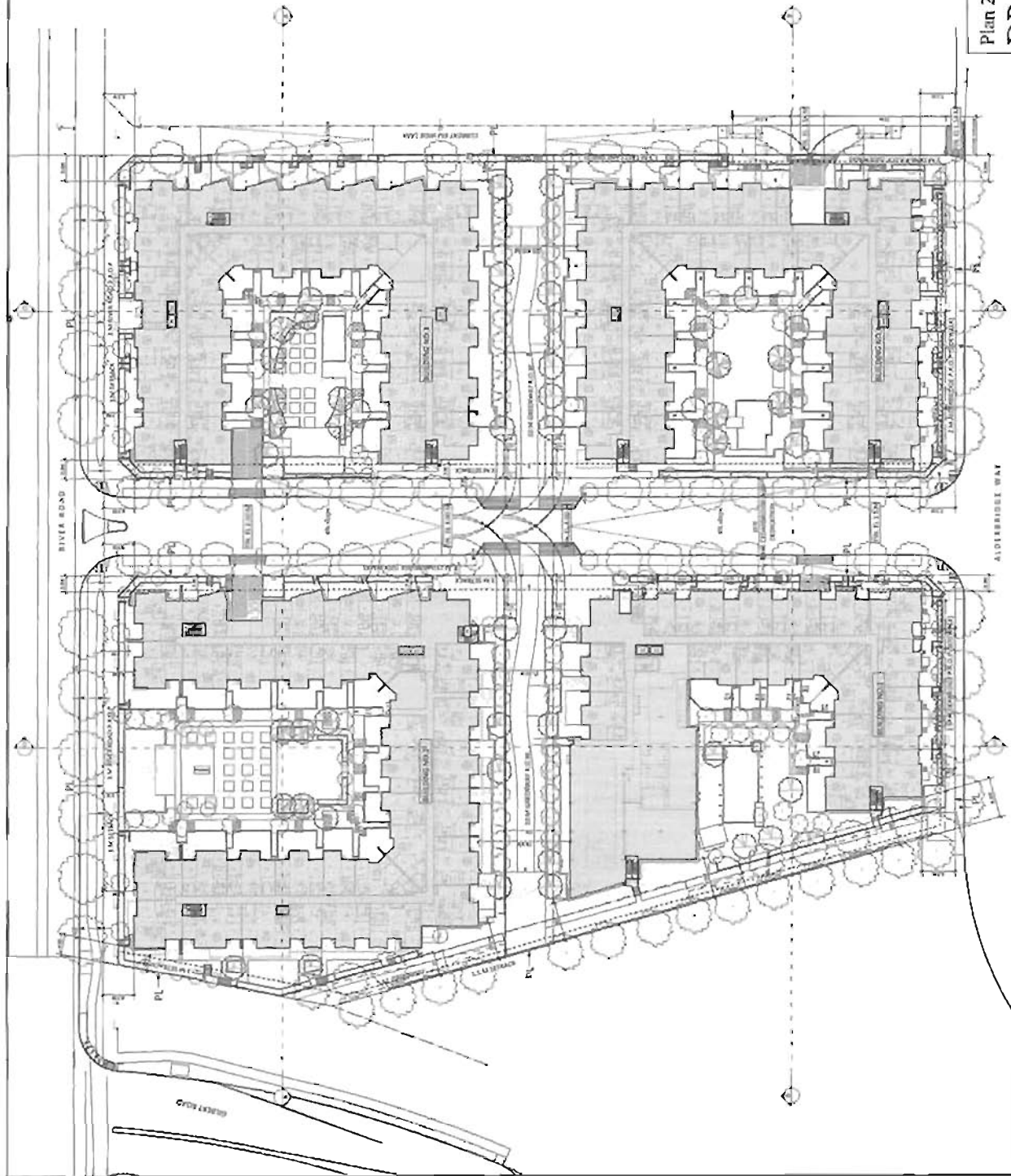


DP 12-615424 SCHEDULE "A"

Original Date: 07/11/J2

Revision Date:

Note: Dimensions are in METRES



LEGEND
 PL - PROPOSED LINE AFTER
 CONSTRUCTION OF ROAD
 INDICATED

Plan 2 Sept 4 2012
 DP 12-615424

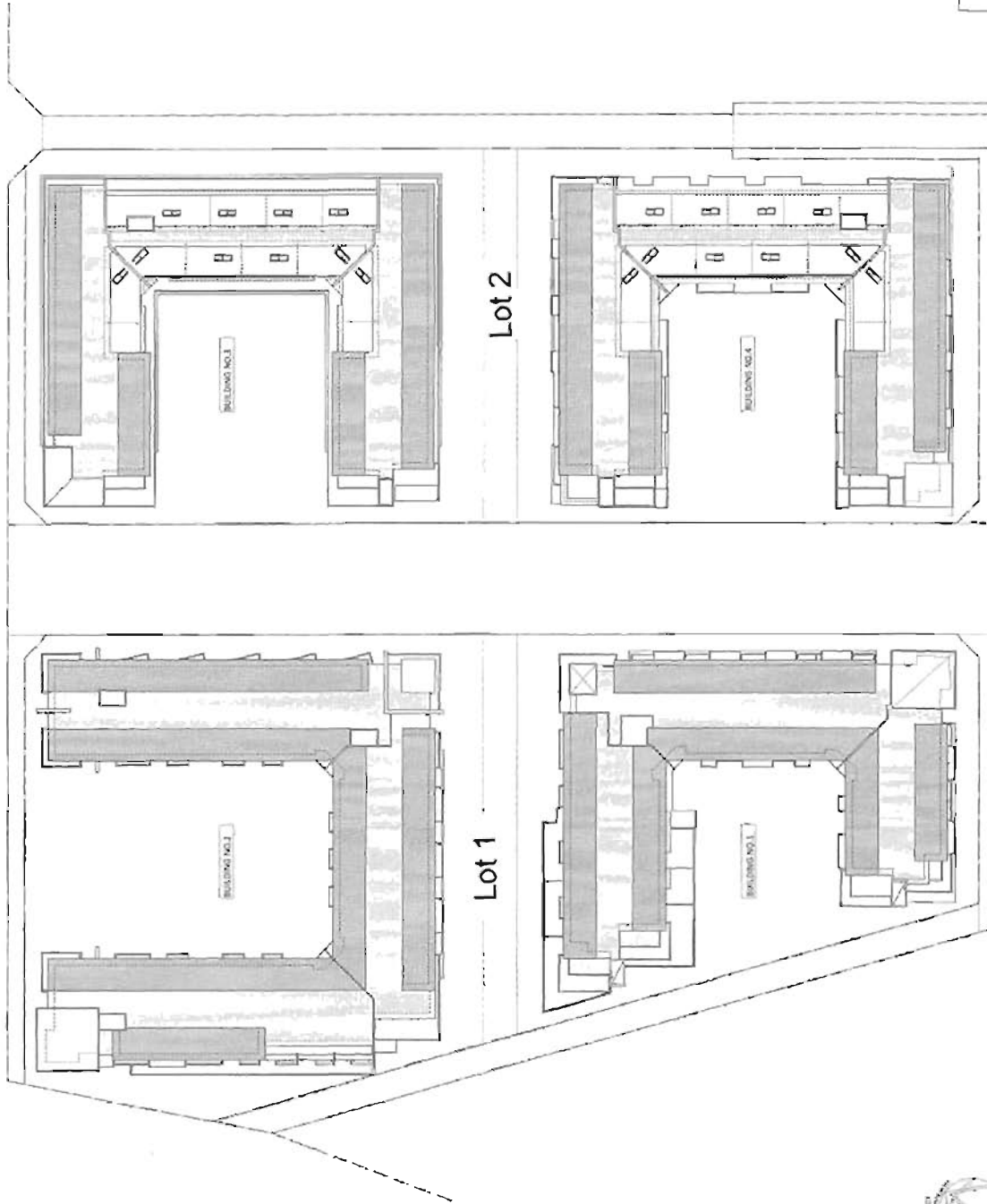
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8	09/04/2012	ISSUED FOR PERMIT
9	09/04/2012	ISSUED FOR PERMIT
10	09/04/2012	ISSUED FOR PERMIT

APPROVED DEVELOPMENT
 RIVA
 1000 RIVA DRIVE, SUITE 100
 WILLOWDALE, ONTARIO M2H 1B7
 TEL: (416) 490-1100
 FAX: (416) 490-1101
 WWW.RIVA.COM

RIVA
 1000 RIVA DRIVE, SUITE 100
 WILLOWDALE, ONTARIO M2H 1B7
 TEL: (416) 490-1100
 FAX: (416) 490-1101
 WWW.RIVA.COM

DESIGNED BY
 SITE PLAN

NO.	DATE	DESCRIPTION
1	09/04/2012	ISSUED FOR PERMIT
2	09/04/2012	ISSUED FOR PERMIT
3	09/04/2012	ISSUED FOR PERMIT
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7	09/04/2012	ISSUED FOR PERMIT
8	09/04/2012	ISSUED FOR PERMIT
9	09/04/2012	ISSUED FOR PERMIT
10	09/04/2012	ISSUED FOR PERMIT



Plan 1 Sept 4 2012
 DP 12-615424

NO. OF PLOTS	2
NO. OF BUILDINGS	2
NO. OF PARKING SPACES	20
NO. OF LANDSCAPING AREAS	2
NO. OF UTILITIES AREAS	2
NO. OF OTHER AREAS	2
NO. OF TOTAL AREAS	10
NO. OF TOTAL PLOTS	2
NO. OF TOTAL BUILDINGS	2
NO. OF TOTAL PARKING SPACES	20
NO. OF TOTAL LANDSCAPING AREAS	2
NO. OF TOTAL UTILITIES AREAS	2
NO. OF TOTAL OTHER AREAS	2
NO. OF TOTAL AREAS	10
NO. OF TOTAL PLOTS	2
NO. OF TOTAL BUILDINGS	2
NO. OF TOTAL PARKING SPACES	20
NO. OF TOTAL LANDSCAPING AREAS	2
NO. OF TOTAL UTILITIES AREAS	2
NO. OF TOTAL OTHER AREAS	2
NO. OF TOTAL AREAS	10

PROJECT
 APARTMENT DEVELOPMENT

RIVA
 1000 RIVA AVENUE
 BIRMINGHAM, AL 35203

Yamamoto
 Architecture Inc.

DATE: 09/04/12
 DRAWN BY: J. YAMAMOTO
 CHECKED BY: J. YAMAMOTO
 PROJECT NO. 12-615424

TOTAL PLOT
 100.00
 TOTAL BUILDING
 100.00
 TOTAL PARKING
 100.00
 TOTAL LANDSCAPING
 100.00
 TOTAL UTILITIES
 100.00
 TOTAL OTHER
 100.00
 TOTAL AREAS
 100.00
 TOTAL PLOTS
 2
 TOTAL BUILDINGS
 2
 TOTAL PARKING SPACES
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 TOTAL LANDSCAPING AREAS
 2
 TOTAL UTILITIES AREAS
 2
 TOTAL OTHER AREAS
 2
 TOTAL AREAS
 10



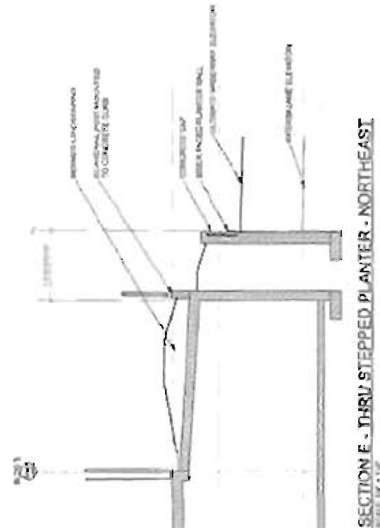
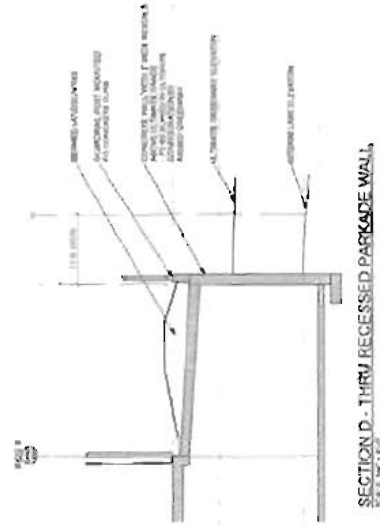
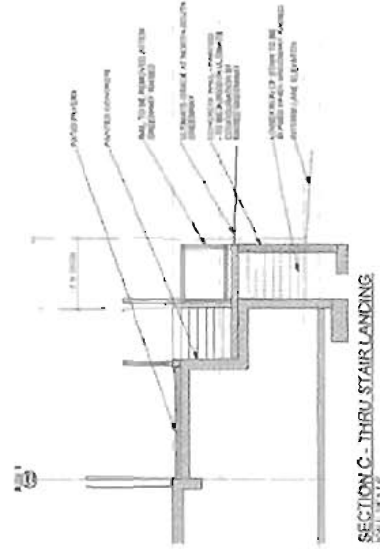
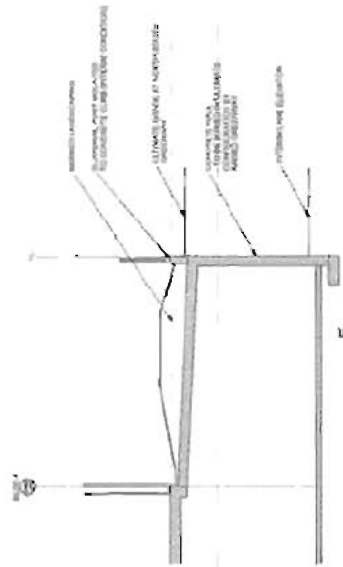
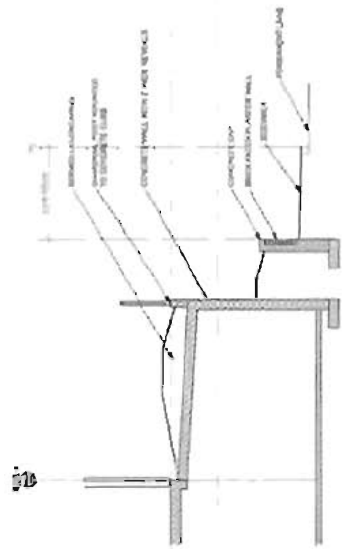
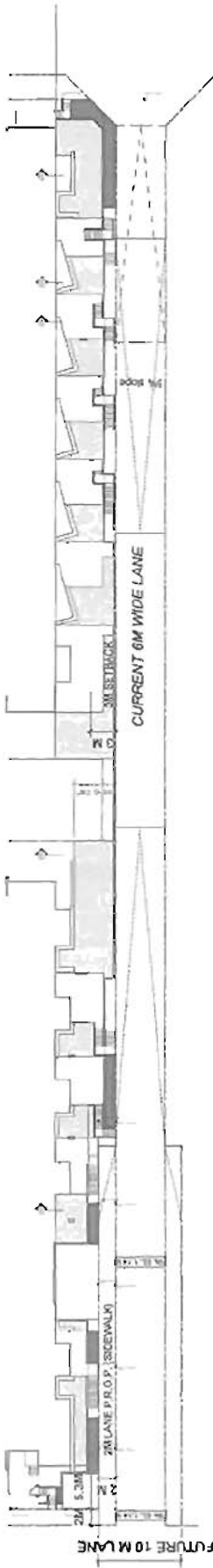
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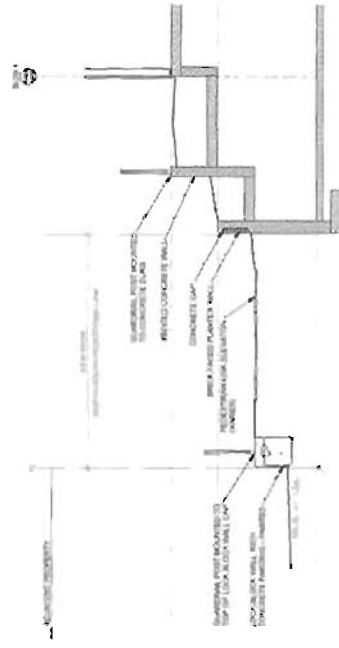
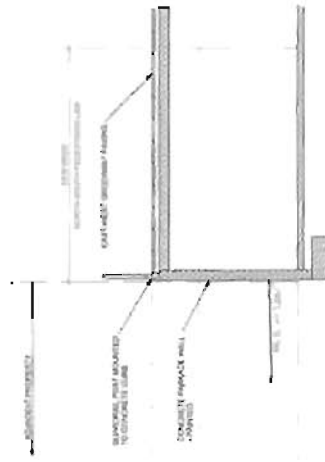
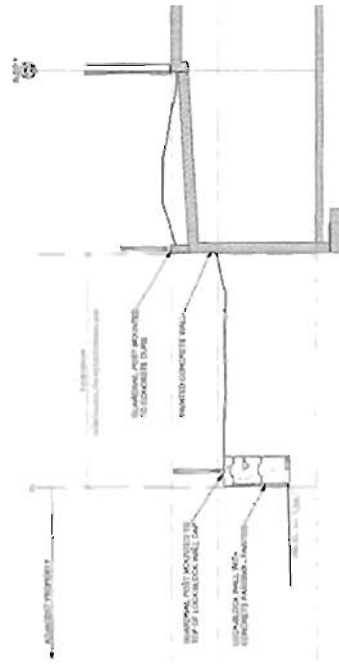
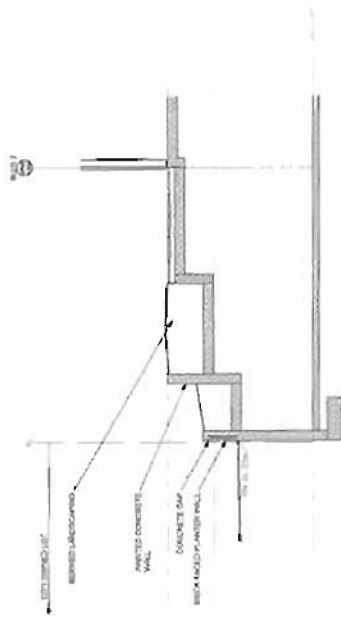
Yamamoto
Architect

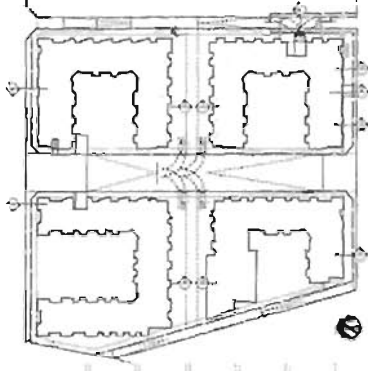
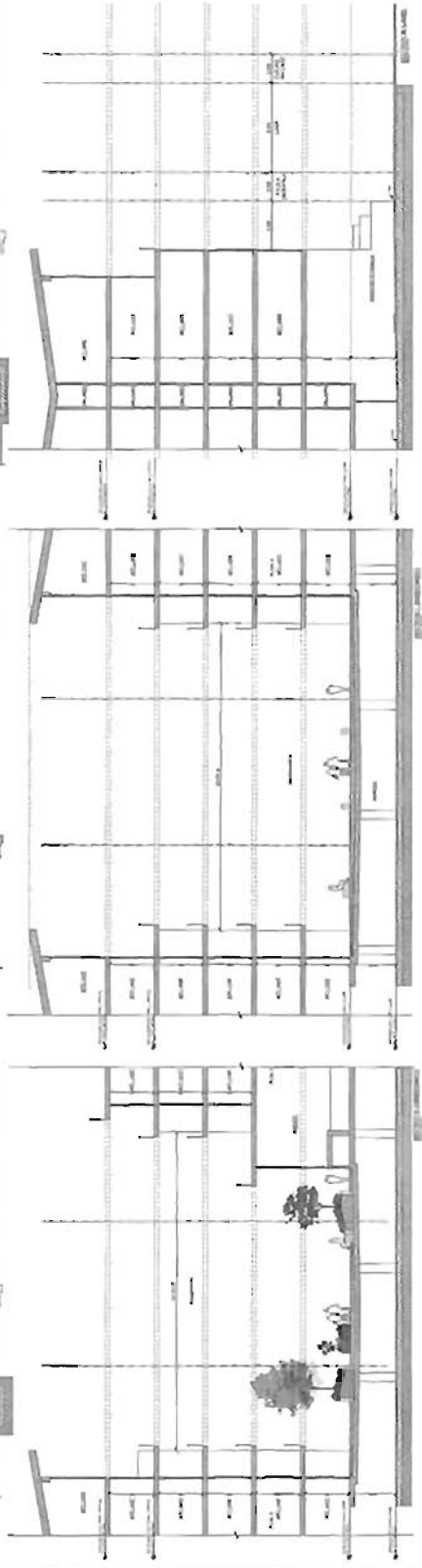
Plan 5 Sept 4 2012
DP 12-615424

PROJECT NO. 12-615424
SHEET NO. A3.04

NO.	DATE	BY	CHKD.	DESCRIPTION
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2	09/04/12	YAMAMOTO	YAMAMOTO	REVISION
3	09/04/12	YAMAMOTO	YAMAMOTO	REVISION
4	09/04/12	YAMAMOTO	YAMAMOTO	REVISION
5	09/04/12	YAMAMOTO	YAMAMOTO	REVISION







DATE	10/10/12
BY	YAMAMOTO
PROJECT	12-615424
NO.	001
SCALE	1/8" = 1'-0"

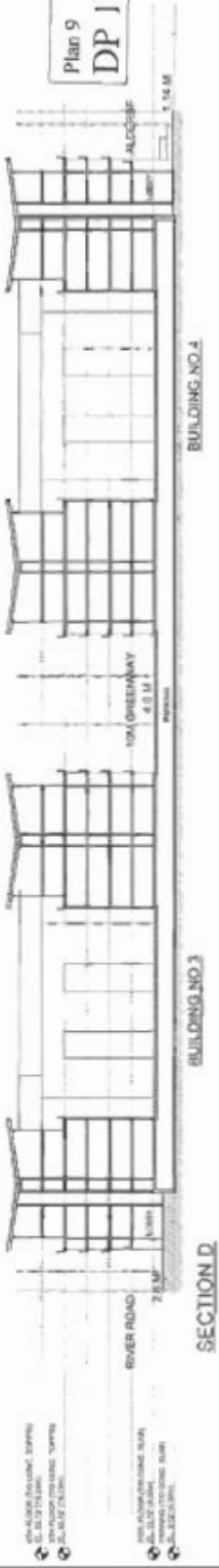
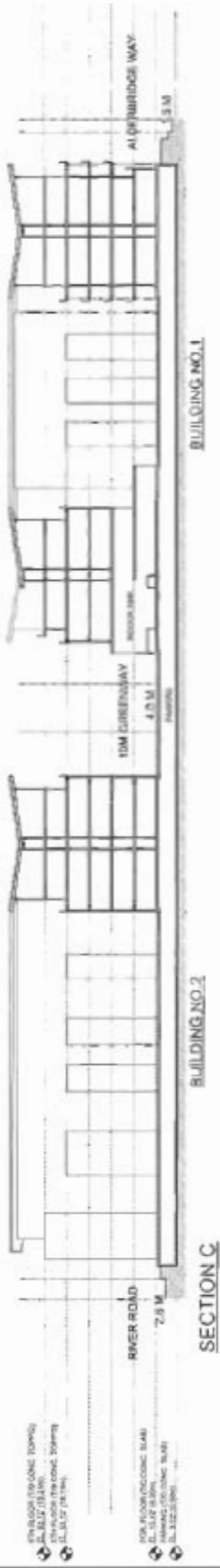
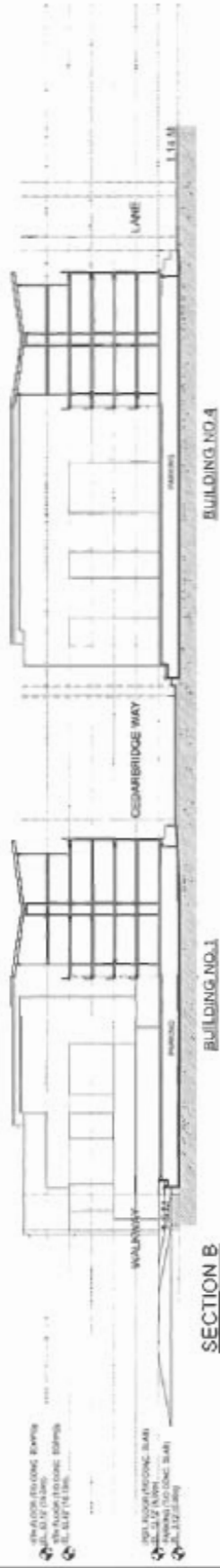
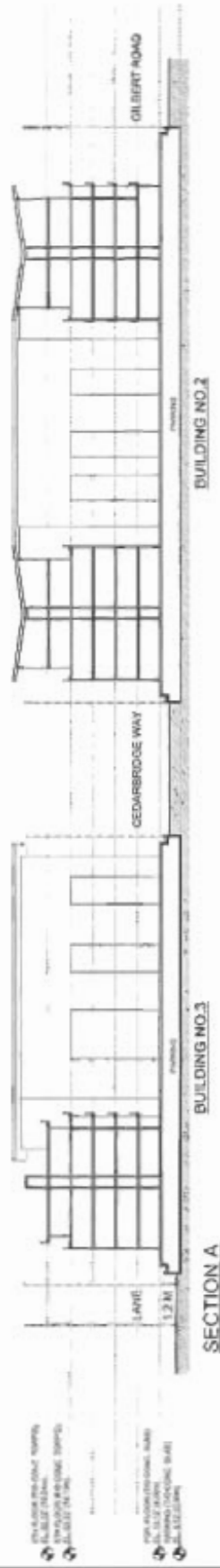
YAMAMOTO ARCHITECTURE INC.
1000 10TH AVENUE, SUITE 100
DENVER, CO 80202
TEL: 303.733.1000
WWW.YAMAMOTOARCHITECT.COM

RIVA

Yamamoto
Architecture Inc.

Plan 8 Sept 4 2012
DP 12-615424

DATE	10/10/12
BY	YAMAMOTO
PROJECT	12-615424
NO.	001
SCALE	1/8" = 1'-0"



NO.	DATE	DESCRIPTION
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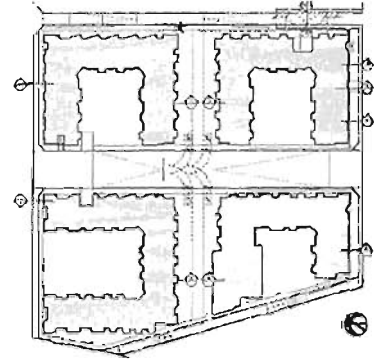
THIS PLAN IS THE PROPERTY OF YAMAMOTO ARCHITECTURE INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFIC TO THIS PLAN. IT IS NOT TO BE REPRODUCED OR COPIED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN PERMISSION OF YAMAMOTO ARCHITECTURE INC.

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Yamamoto
Architecture Inc.

Plan 9 Sept 4 2012
DP 12-615424

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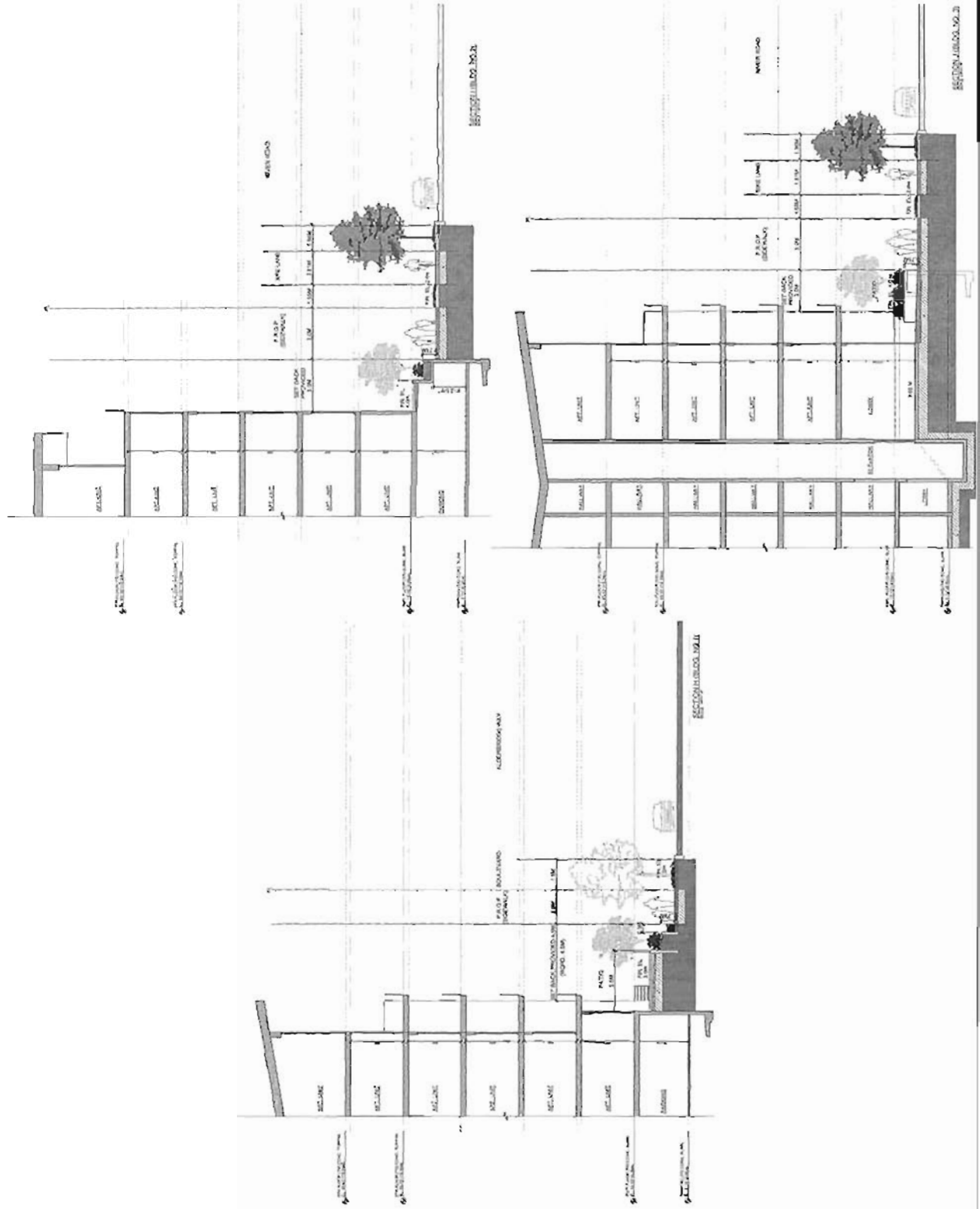
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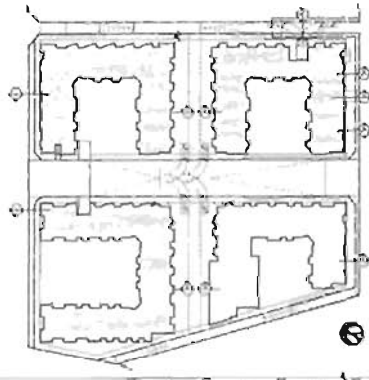
Received 20 April 2004
Accepted 20 April 2004
Published online 20 April 2004

Yamamoto
Architecture Inc.

Plan 10 Sept 4 2012
DP 12-615424

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BY	10:10
TO	10:10
FROM	10:10
REMARKS	10:10





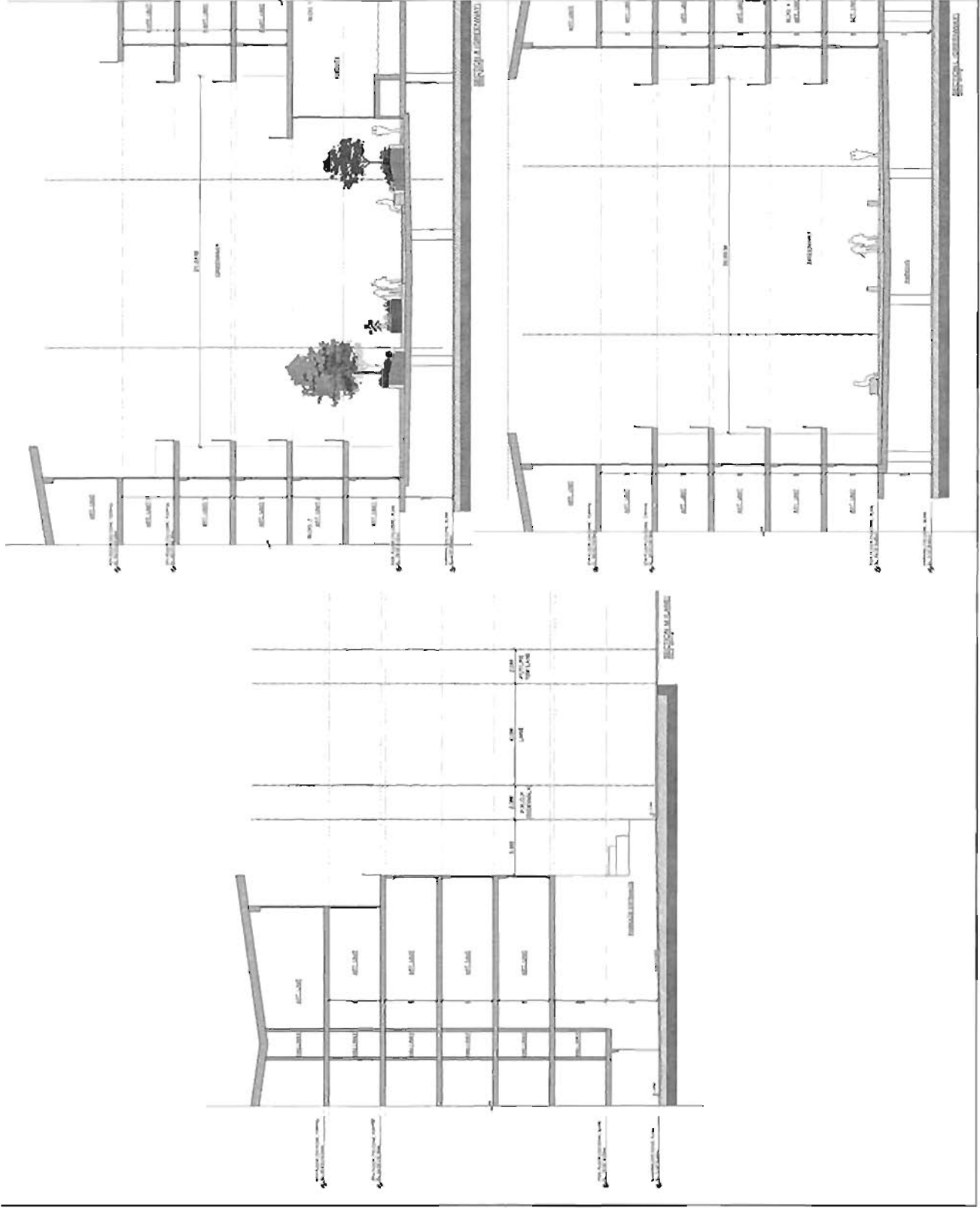
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Project No. 12-615424
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 12-615424-02
 12-615424-03
 12-615424-04
 12-615424-05
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 12-615424-07
 12-615424-08
 12-615424-09
 12-615424-10

Yamamoto
 Architecture Inc.

Plan 12 Sept 4 2012
 DP 12-615424

NO.	DATE	REVISION
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BUILDING 1 - GROUND FLOOR	
DATE	10/1/11
BY	YAMAMOTO
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DATE	10/1/11
BY	YAMAMOTO
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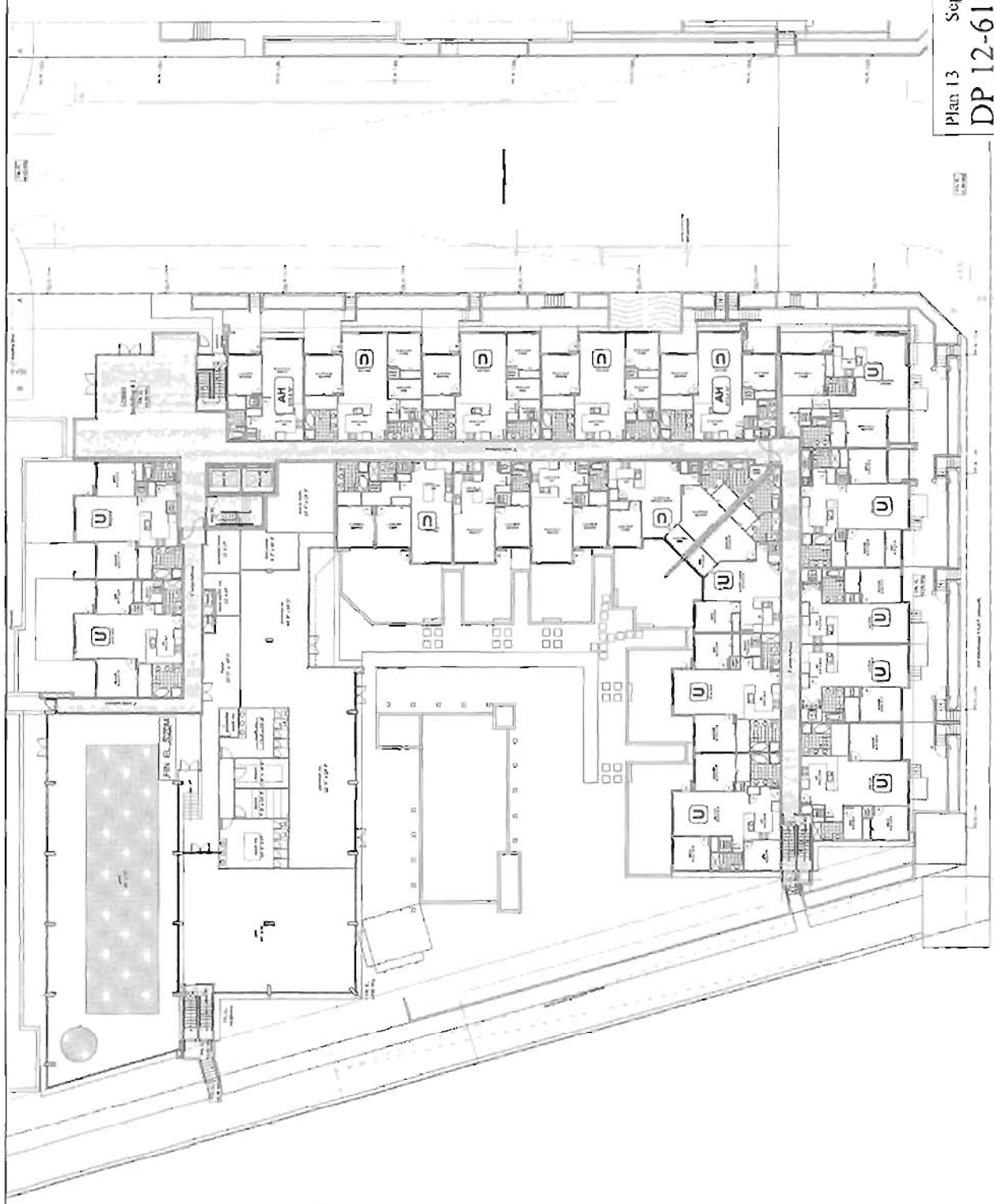
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 RIVA
 100% ASSOCIATED WITH
 NICHOLS, L.L.C.

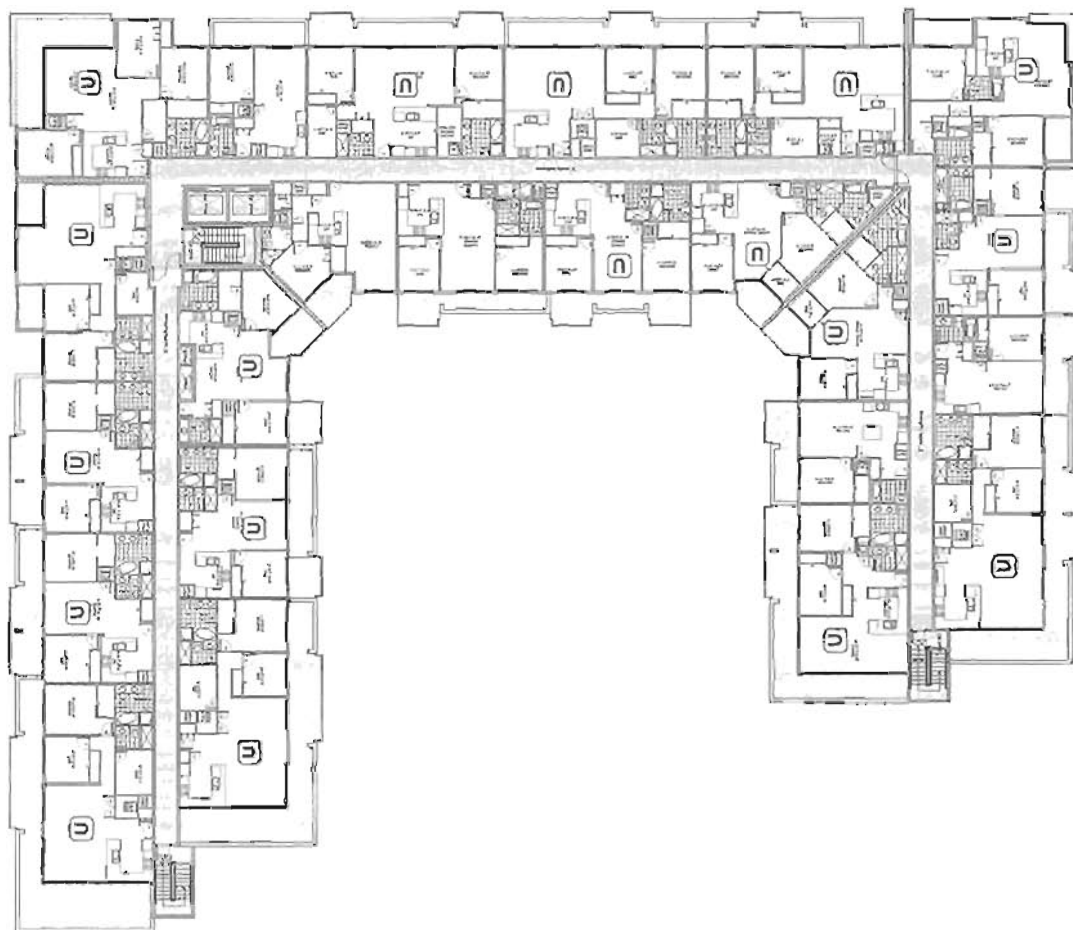
Yamamoto
 Architecture Inc.

100% ASSOCIATED WITH
 NICHOLS, L.L.C.
 100% ASSOCIATED WITH
 NICHOLS, L.L.C.

DATE	10/1/11
BY	YAMAMOTO
REVISION	
NO.	DESCRIPTION
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Plan 13 Sept 4 2012
 DP 12-615424



[illegible]

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	NOV 21 1972	WALSH TRAIL
	NOV 22 1972	WALSH TRAIL
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	NOV 29 1972	WALSH TRAIL
	NOV 30 1972	WALSH TRAIL

ADAPTANT DEVELOPMENT

RIVA
PPTV AS LUBANGSILOK NO-9

Yamamoto
Architecture Inc.

[illegible]

DATE	10/10/10	TIME	10:00
NAME	A1-1.5		
TEST	10/10/10		
TEST	10/10/10		
TEST	10/10/10		

Plan 17 Sept 4 2012
DP 12-615424



DATE	TIME	LOCATION	REMARKS
July 10, 1978	10:00 AM	W. 1st St. & 1st Ave.	1st Survey Point
July 11, 1978	10:00 AM	W. 1st St. & 1st Ave.	2nd Survey Point
July 12, 1978	10:00 AM	W. 1st St. & 1st Ave.	3rd Survey Point
July 13, 1978	10:00 AM	W. 1st St. & 1st Ave.	4th Survey Point
July 14, 1978	10:00 AM	W. 1st St. & 1st Ave.	5th Survey Point

ABSTRACT DEVELOPMENT

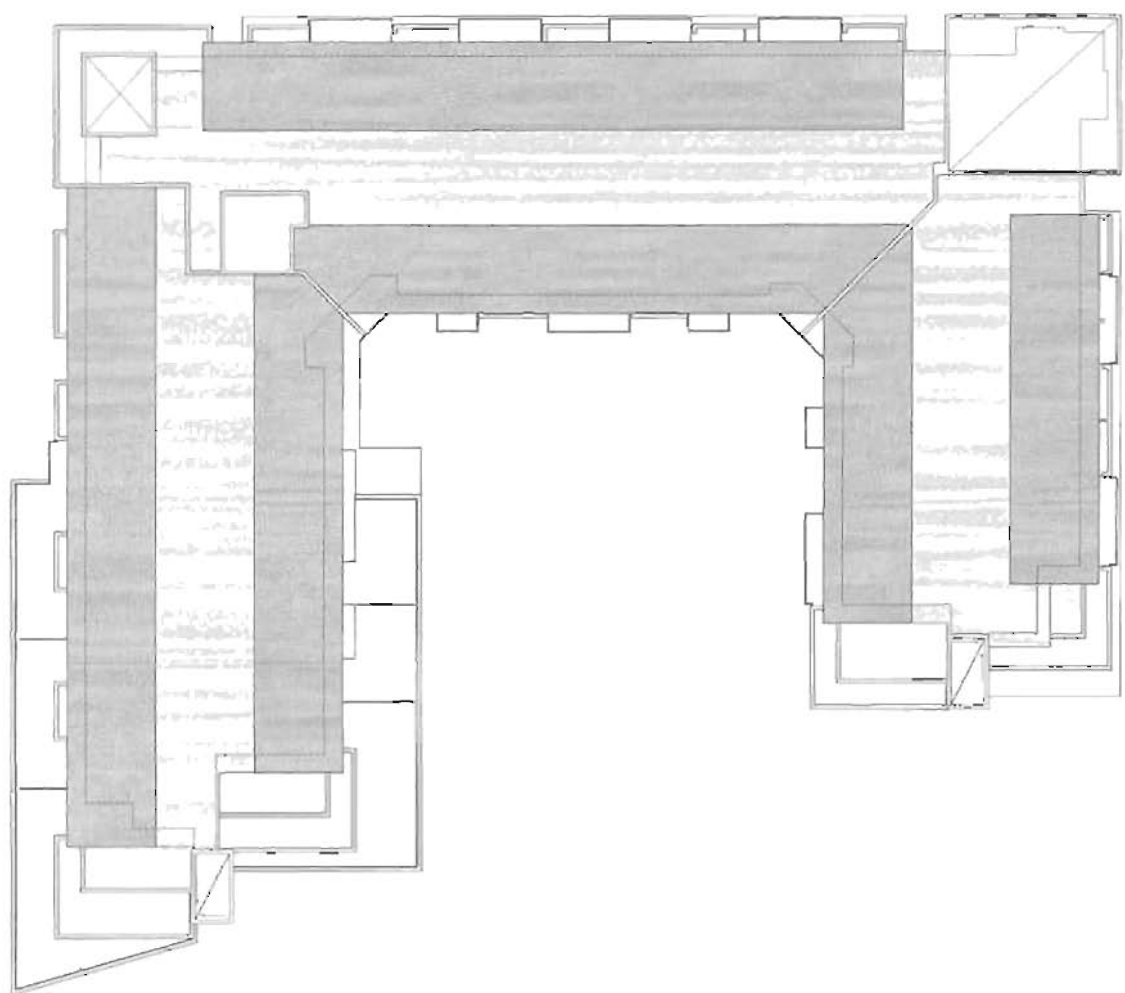
RIVA

Yamamoto
Architecture Inc.

SEVENTH FLOOR PLAN
1/10/2004

DATE	1/19	PROJECT NO.	A1-1.6	DATE OF	02
TIME		DATE OF			
LOCATION					
DESCRIPTION					

Plan 18 Sept 4 2012
DP 12-615424



Plan 19 Sept 4 2012
 DP 12-615424

NO. 10	NO. 11	NO. 12	NO. 13	NO. 14	NO. 15	NO. 16	NO. 17	NO. 18	NO. 19	NO. 20	NO. 21	NO. 22	NO. 23	NO. 24	NO. 25	NO. 26	NO. 27	NO. 28	NO. 29	NO. 30	NO. 31	NO. 32	NO. 33	NO. 34	NO. 35	NO. 36	NO. 37	NO. 38	NO. 39	NO. 40	NO. 41	NO. 42	NO. 43	NO. 44	NO. 45	NO. 46	NO. 47	NO. 48	NO. 49	NO. 50	NO. 51	NO. 52	NO. 53	NO. 54	NO. 55	NO. 56	NO. 57	NO. 58	NO. 59	NO. 60	NO. 61	NO. 62	NO. 63	NO. 64	NO. 65	NO. 66	NO. 67	NO. 68	NO. 69	NO. 70	NO. 71	NO. 72	NO. 73	NO. 74	NO. 75	NO. 76	NO. 77	NO. 78	NO. 79	NO. 80	NO. 81	NO. 82	NO. 83	NO. 84	NO. 85	NO. 86	NO. 87	NO. 88	NO. 89	NO. 90	NO. 91	NO. 92	NO. 93	NO. 94	NO. 95	NO. 96	NO. 97	NO. 98	NO. 99	NO. 100
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PROJECT
 AMATEUR DEVELOPMENT

RIVA
 ARCHITECTURE INC.

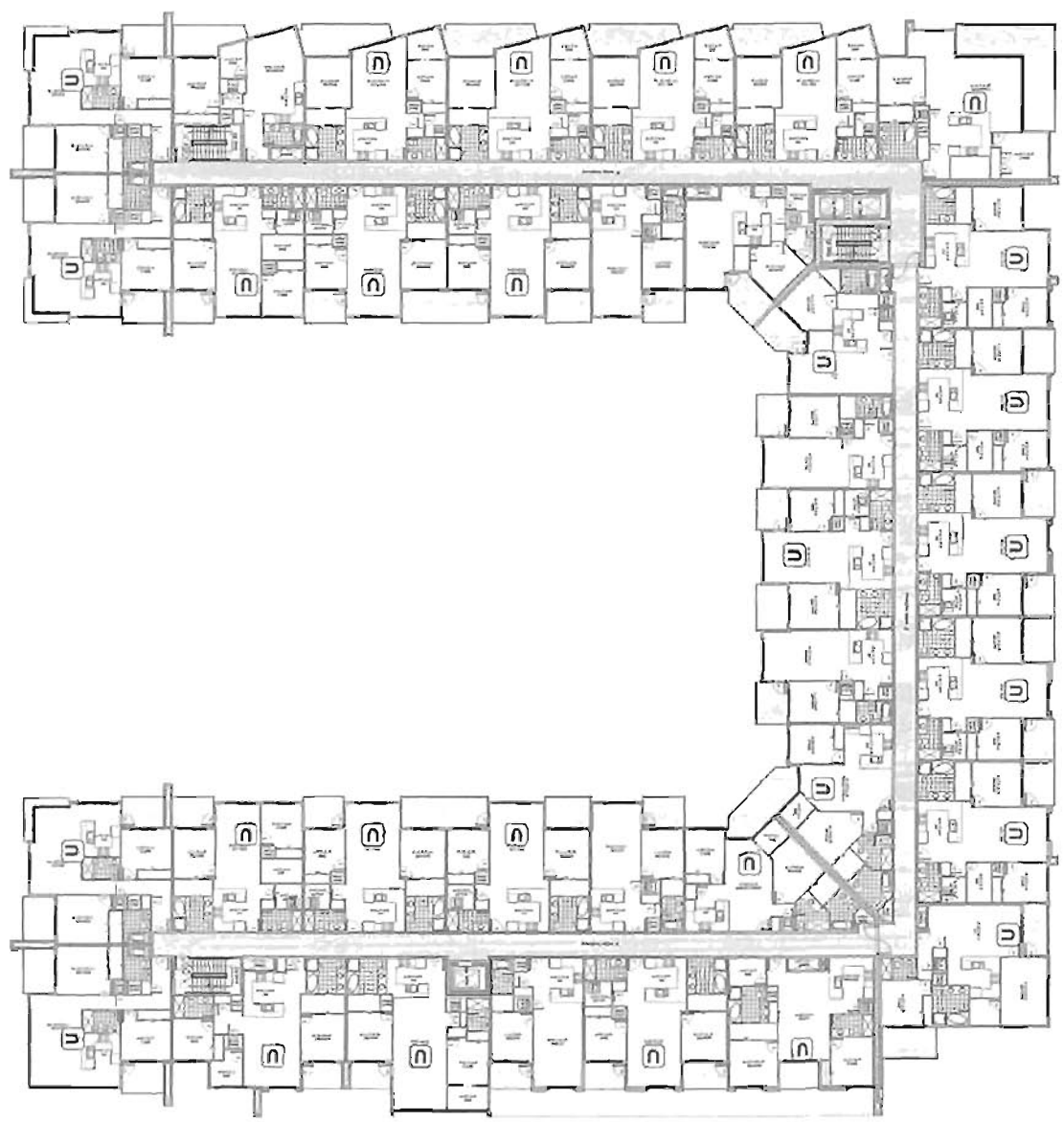
Yamamoto
 Architecture Inc.

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NO. 10	NO. 11	NO. 12	NO. 13	NO. 14	NO. 15	NO. 16	NO. 17	NO. 18	NO. 19	NO. 20	NO. 21	NO. 22	NO. 23	NO. 24	NO. 25	NO. 26	NO. 27	NO. 28	NO. 29	NO. 30	NO. 31	NO. 32	NO. 33	NO. 34	NO. 35	NO. 36	NO. 37	NO. 38	NO. 39	NO. 40	NO. 41	NO. 42	NO. 43	NO. 44	NO. 45	NO. 46	NO. 47	NO. 48	NO. 49	NO. 50	NO. 51	NO. 52	NO. 53	NO. 54	NO. 55	NO. 56	NO. 57	NO. 58	NO. 59	NO. 60	NO. 61	NO. 62	NO. 63	NO. 64	NO. 65	NO. 66	NO. 67	NO. 68	NO. 69	NO. 70	NO. 71	NO. 72	NO. 73	NO. 74	NO. 75	NO. 76	NO. 77	NO. 78	NO. 79	NO. 80	NO. 81	NO. 82	NO. 83	NO. 84	NO. 85	NO. 86	NO. 87	NO. 88	NO. 89	NO. 90	NO. 91	NO. 92	NO. 93	NO. 94	NO. 95	NO. 96	NO. 97	NO. 98	NO. 99	NO. 100
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BUILDING 1 - 1RD FLOOR

DATE	10/1/2011
BY	YAMAMOTO
PROJECT	1RD FLOOR
CLIENT	YAMAMOTO
ARCHITECT	YAMAMOTO
ENGINEER	YAMAMOTO
DESIGNER	YAMAMOTO
DATE	10/1/2011
BY	YAMAMOTO
PROJECT	1RD FLOOR
CLIENT	YAMAMOTO
ARCHITECT	YAMAMOTO
ENGINEER	YAMAMOTO
DESIGNER	YAMAMOTO



DATE	10/1/2011
BY	YAMAMOTO
PROJECT	1RD FLOOR
CLIENT	YAMAMOTO
ARCHITECT	YAMAMOTO
ENGINEER	YAMAMOTO
DESIGNER	YAMAMOTO

MAINTENANCE DEVELOPMENT

RIVA

Yamamoto Architecture Inc.

1RD FLOOR PLAN

Plan 22 Sept 4 2012
DP 12-615424

DATE	10/1/2011
BY	YAMAMOTO
PROJECT	1RD FLOOR
CLIENT	YAMAMOTO
ARCHITECT	YAMAMOTO
ENGINEER	YAMAMOTO
DESIGNER	YAMAMOTO



Subband 2 - 4TH FLOOR -

[illegible]

Q1	Q175	REVISION
Q18	Q187	REVISION
Q19	Q197	REVISION
Q20	Q207	REVISION
Q21	Q217	REVISION
Q22	Q227	REVISION
Q23	Q237	REVISION
Q24	Q247	REVISION
Q25	Q257	REVISION
Q26	Q267	REVISION
Q27	Q277	REVISION
Q28	Q287	REVISION
Q29	Q297	REVISION
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Q96	Q967	REVISION
Q97	Q977	REVISION
Q98	Q987	REVISION
Q99	Q997	REVISION
Q100	Q1007	REVISION

LEARNING OBJECTIVES

RIVA
PURA, PROFUMO DI PROFUMO

Yamamoto
Architecture Inc.

FOURTH FLOOR PLAZA
FLOORING ?

Plan 23 Sept 4 2012
DP 12-615424

Model	1100	Serial No.	A1-2.4
Date	10/10/2001	Page No.	10

[illegible]

Item	Qty	Unit
1. 100% Cotton T-shirt	100	100
2. 100% Cotton T-shirt	100	100
3. 100% Cotton T-shirt	100	100
4. 100% Cotton T-shirt	100	100
5. 100% Cotton T-shirt	100	100
6. 100% Cotton T-shirt	100	100
7. 100% Cotton T-shirt	100	100
8. 100% Cotton T-shirt	100	100
9. 100% Cotton T-shirt	100	100
10. 100% Cotton T-shirt	100	100

ADULTS: DEVELOPMENT

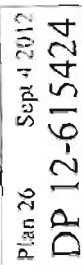
RIVA
 100% POLYESTER
 100% POLYESTER

FOR THE CITY OF NEW YORK, BY
JAMES H. HARRIS, JR., DEPUTY CLERK

BOOK	108	DATE	NOV 25 1979	PRICE	\$2.50
A1-2.5					



Plan 24 Sept 4 2012
DP 12-615424

[illegible]

[illegible]

Item	Unit	Qty
1. 100% Cotton T-shirt	100	100
2. 100% Cotton T-shirt	100	100
3. 100% Cotton T-shirt	100	100
4. 100% Cotton T-shirt	100	100
5. 100% Cotton T-shirt	100	100
6. 100% Cotton T-shirt	100	100
7. 100% Cotton T-shirt	100	100
8. 100% Cotton T-shirt	100	100
9. 100% Cotton T-shirt	100	100
10. 100% Cotton T-shirt	100	100

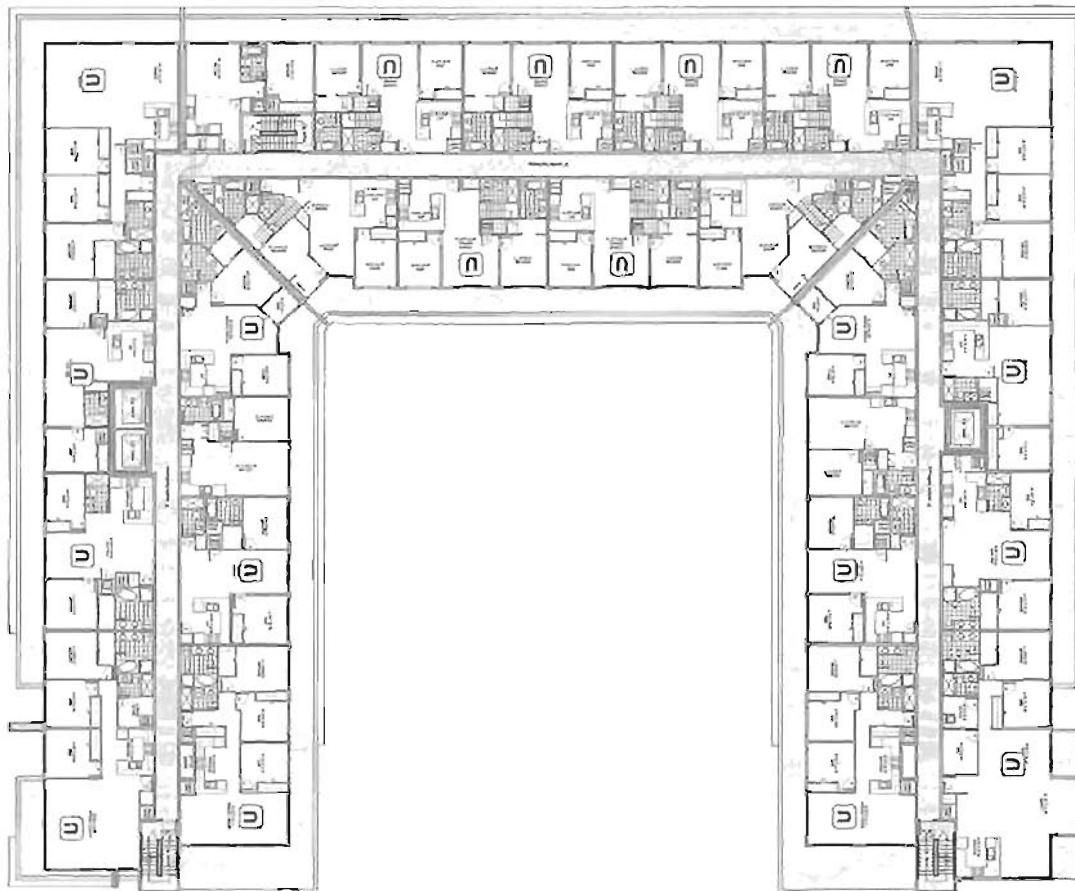
ALAN TAYLOR DEVELOPMENT

RIVA
PVA, PVP, Acetaminophen, etc.
Acetaminophen, B.C.

**Yamamoto
Architecture Inc.**

15TH FLOOR PLAN
 BUILDING 1
 15TH FLOOR PLAN
 BUILDING 1

DATE	TIME	SECTION	PRICE
7/11	7:00 PM	A1-3.5	00
LIBRARY			
UNIVERSITY			

Plan 30 Sept 4 2012
DP 12-615424

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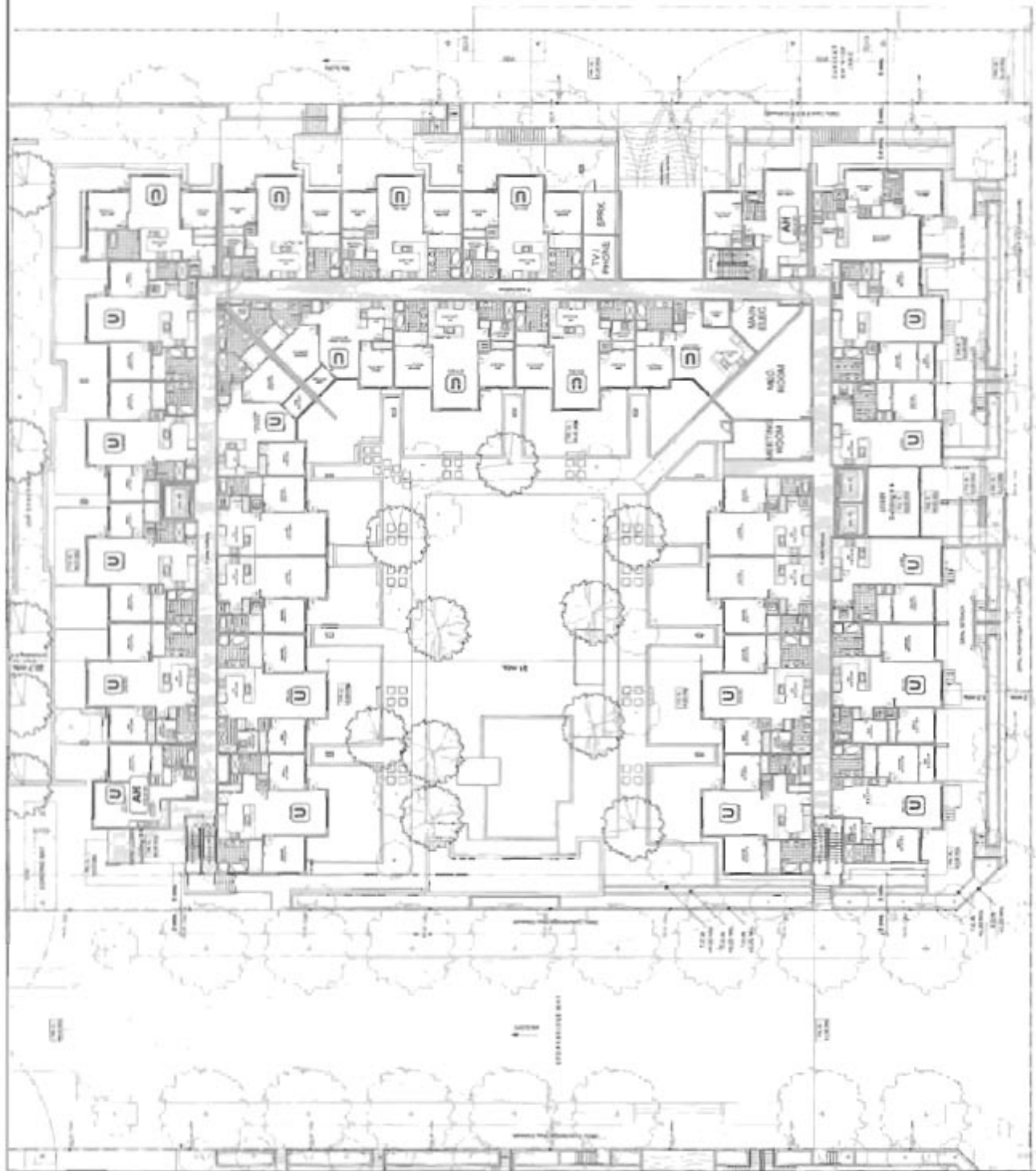
ARGUMENT DEVELOPMENT

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Yamamoto
Architecture Inc.

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DATE	TIME	NAME	NO.
			A1-3.7



Plan 33 Sept 4 2012
 DP 12-615424

BUILDING & CIRCULATION FLOOR			
CLASSROOMS	100	101	102
LABORATORY	103	104	105
MEETING ROOM	106	107	108
OFFICE	109	110	111
RECEPTION	112	113	114
STORAGE	115	116	117
UTILITY	118	119	120

MECHANICAL DEVELOPMENT			
MECHANICAL	121	122	123
ELECTRICAL	124	125	126
PLUMBING	127	128	129
HEATING	130	131	132
Cooling	133	134	135

RIVA
 RIVA ARCHITECTURE LLC
 1000 10th Ave. N.W.
 Seattle, WA 98107
 (206) 461-1000
 www.rivaarch.com

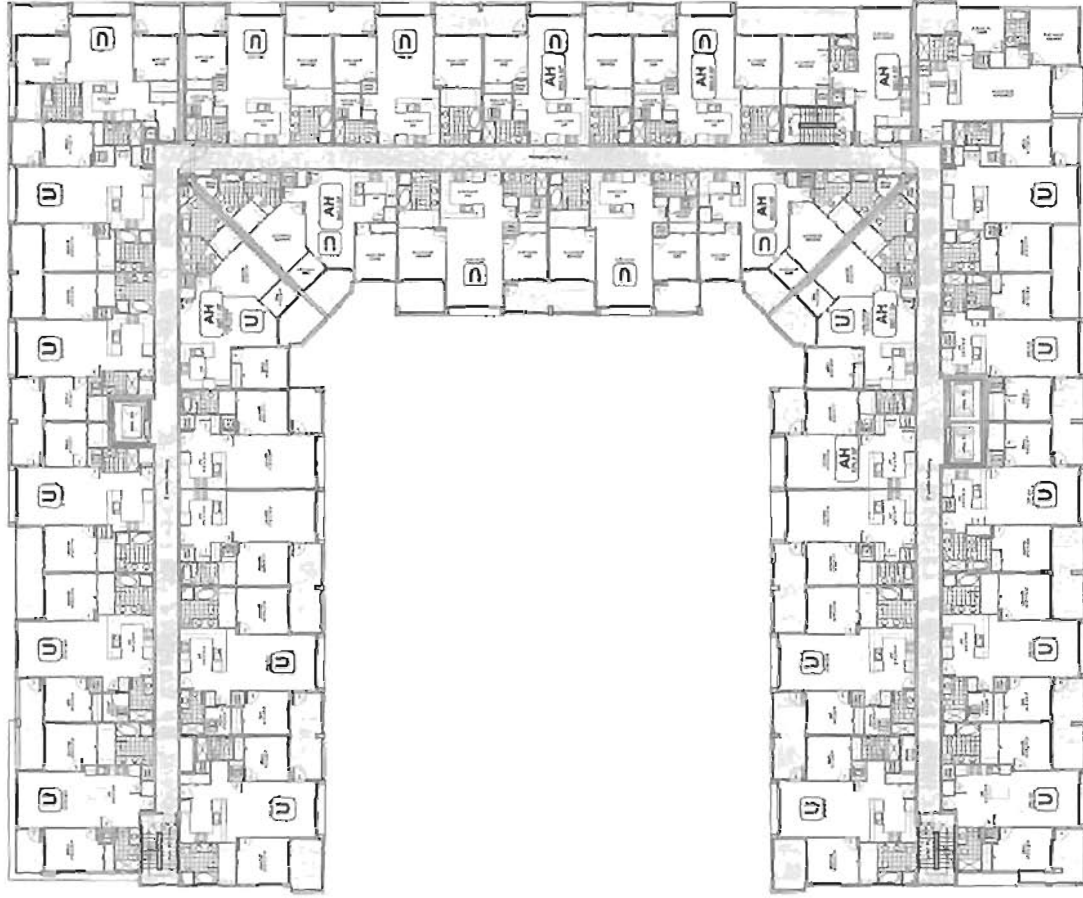
Yamamoto
 Architecture Inc.
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 (206) 461-1000
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1000 10th Ave. N.W.
 Seattle, WA 98107
 (206) 461-1000
 www.yamamotoarch.com

FIRST FLOOR PLAN			
DATE	09/04/12	BY	AL-4.1
SCALE	1/8" = 1'-0"	DATE	09/04/12
PROJECT	12-615424	SCALE	1/8" = 1'-0"

BUILDING 4 - 3RD FLOOR

NO.	DESCRIPTION	QUANTITY	UNIT
1	CEILING	1	SQ. FT.
2	FLOOR	1	SQ. FT.
3	WALL	1	SQ. FT.
4	DOOR	1	EA.
5	WINDOW	1	EA.
6	STAIR	1	SQ. FT.
7	ELEVATOR	1	SQ. FT.
8	MECHANICAL	1	SQ. FT.
9	ELECTRICAL	1	SQ. FT.
10	PLUMBING	1	SQ. FT.
11	PAINT	1	SQ. FT.
12	FINISH	1	SQ. FT.
13	LANDSCAPE	1	SQ. FT.
14	TOTAL	1	SQ. FT.



Plan 35 Sept 4 2012
DP 12-615424

NO.	DESCRIPTION	QUANTITY	UNIT
1	CEILING	1	SQ. FT.
2	FLOOR	1	SQ. FT.
3	WALL	1	SQ. FT.
4	DOOR	1	EA.
5	WINDOW	1	EA.
6	STAIR	1	SQ. FT.
7	ELEVATOR	1	SQ. FT.
8	MECHANICAL	1	SQ. FT.
9	ELECTRICAL	1	SQ. FT.
10	PLUMBING	1	SQ. FT.
11	PAINT	1	SQ. FT.
12	FINISH	1	SQ. FT.
13	LANDSCAPE	1	SQ. FT.
14	TOTAL	1	SQ. FT.

AMATEUR DEVELOPMENT

RIVA

Yamamoto Architecture Inc.

THIRD FLOOR PLAN

NO.	DESCRIPTION	QUANTITY	UNIT
1	CEILING	1	SQ. FT.
2	FLOOR	1	SQ. FT.
3	WALL	1	SQ. FT.
4	DOOR	1	EA.
5	WINDOW	1	EA.
6	STAIR	1	SQ. FT.
7	ELEVATOR	1	SQ. FT.
8	MECHANICAL	1	SQ. FT.
9	ELECTRICAL	1	SQ. FT.
10	PLUMBING	1	SQ. FT.
11	PAINT	1	SQ. FT.
12	FINISH	1	SQ. FT.
13	LANDSCAPE	1	SQ. FT.
14	TOTAL	1	SQ. FT.

NOTES



SITE LOCATION

- DRAWING LEGEND**
- L1.1 LANDSCAPE SITE PLAN
 - L1.2 LANDSCAPE DETAIL PLAN - BUILDING 4
 - L1.3 COURTYARD DETAIL PLAN - BUILDING 3
 - L1.4 COURT DETAIL PLAN - BUILDING 2
 - L1.5 COURT DETAIL PLAN - BUILDING 1
 - L1.6 COURT DETAIL PLAN - BUILDING 0
 - L1.7 FUTURE LAND CONDITION
 - L1.8 FUTURE LAND CONDITION - OPEN SPACE
 - L1.9 OPEN SPACE DETAIL PLAN
 - L1.10 CENTRAL PLAZA DETAIL PLAN
 - L1.11 PLANT MATERIALS
 - L1.12 LANDSCAPE SECTIONS
 - L1.13 LANDSCAPE SECTIONS
 - L1.14 LANDSCAPE ELEVATIONS AND DETAILS
 - L1.15 PRECINCT MARKERS
 - L1.16 PLAY AREA ELEMENTS
 - L1.17 LANDSCAPE DETAILS



NOTES:
 1. ALL PLANTING SHALL BE AT THE SITE AND NOT ALONG THE STREET.
 2. ALL PLANTING SHALL BE AT THE SITE AND NOT ALONG THE STREET.
 3. ALL PLANTING SHALL BE AT THE SITE AND NOT ALONG THE STREET.
 4. ALL PLANTING SHALL BE AT THE SITE AND NOT ALONG THE STREET.
 5. ALL PLANTING SHALL BE AT THE SITE AND NOT ALONG THE STREET.
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 9. ALL PLANTING SHALL BE AT THE SITE AND NOT ALONG THE STREET.
 10. ALL PLANTING SHALL BE AT THE SITE AND NOT ALONG THE STREET.

NO.	DESCRIPTION	QUANTITY	UNIT
1	PLANTING	100	PLANT
2	PLANTING	200	PLANT
3	PLANTING	300	PLANT
4	PLANTING	400	PLANT
5	PLANTING	500	PLANT
6	PLANTING	600	PLANT
7	PLANTING	700	PLANT
8	PLANTING	800	PLANT
9	PLANTING	900	PLANT
10	PLANTING	1000	PLANT

SHARP & DIAMOND
 LANDSCAPE ARCHITECTS P.C.
 1000 RIVER ROAD, SUITE 100
 RICHMOND, BC V6X 1A6
 TEL: 604.271.1111
 FAX: 604.271.1112
 WWW.SHARPANDDIAMOND.COM

Yamamoto
 Architecture Inc.

RIVA

onni

DATE: 09/04/2012
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 APPROVED BY: [Name]

LANDSCAPE SITE PLAN

Plan 40 Sept 4 2012
 DP 12-615424

L1.1

[illegible]

**Yamamoto
Architecture Inc.**

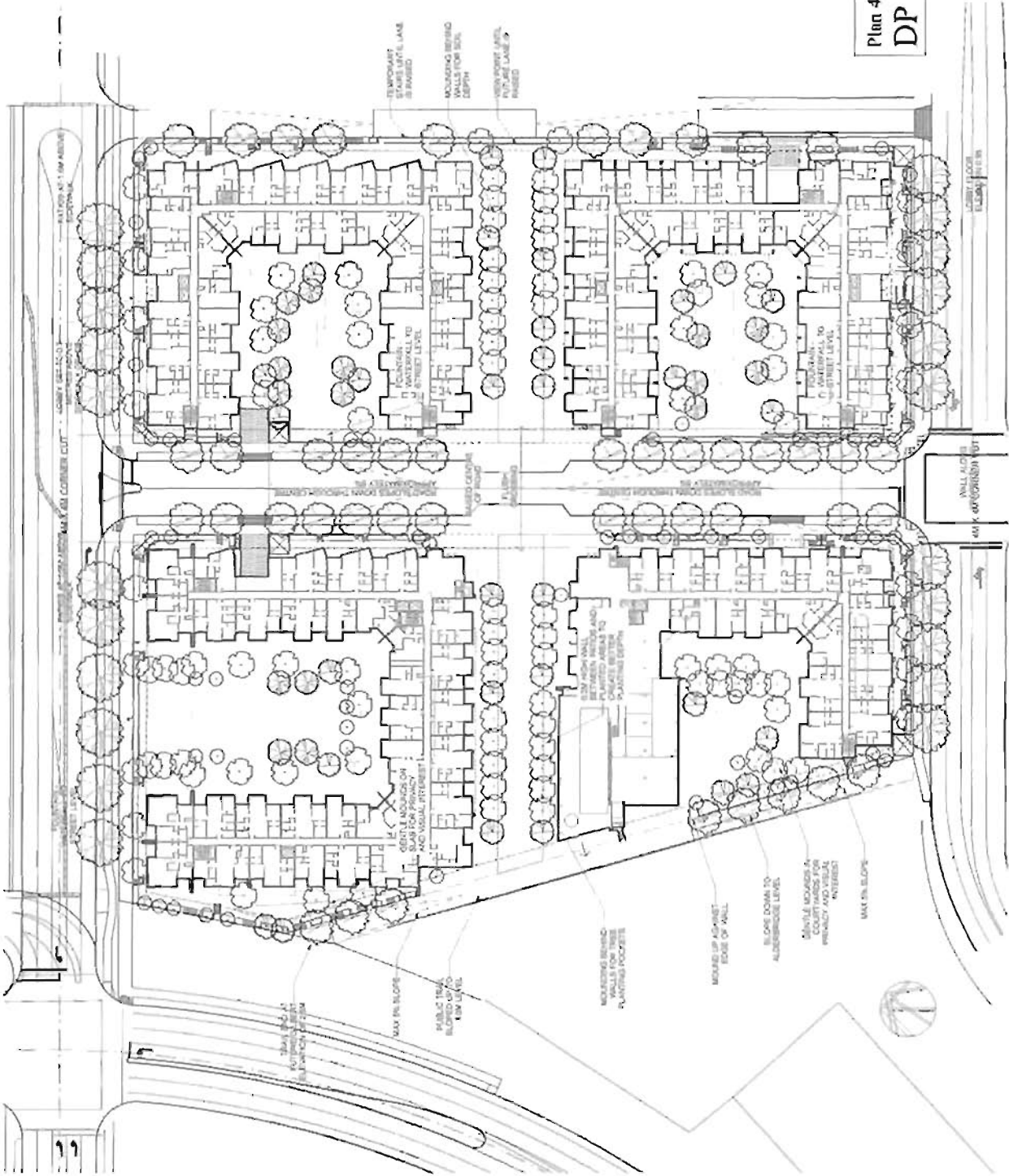
THESE ARE THE RESULTS OF THE RESEARCH BY THE NATIONAL ACADEMY OF SCIENCES, THE NATIONAL ACADEMY OF MEDICAL SCIENCES, AND THE NATIONAL ACADEMY OF ENGINEERING.

RIVA

[illegible]

Plan 41 Sept 4 2012
DP 12-615424

L1.2





Year	1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036	2037	2038	2039	2040	2041	2042	2043	2044	2045	2046	2047	2048	2049	2050	2051	2052	2053	2054	2055	2056	2057	2058	2059	2060	2061	2062	2063	2064	2065	2066	2067	2068	2069	2070	2071	2072	2073	2074	2075	2076	2077	2078	2079	2080	2081	2082	2083	2084	2085	2086	2087	2088	2089	2090	2091	2092	2093	2094	2095	2096	2097	2098	2099	2100
1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036	2037	2038	2039	2040	2041	2042	2043	2044	2045	2046	2047	2048	2049	2050	2051	2052	2053	2054	2055	2056	2057	2058	2059	2060	2061	2062	2063	2064	2065	2066	2067	2068	2069	2070	2071	2072	2073	2074	2075	2076	2077	2078	2079	2080	2081	2082	2083	2084	2085	2086	2087	2088	2089	2090	2091	2092	2093	2094	2095	2096	2097	2098	2099	2100	

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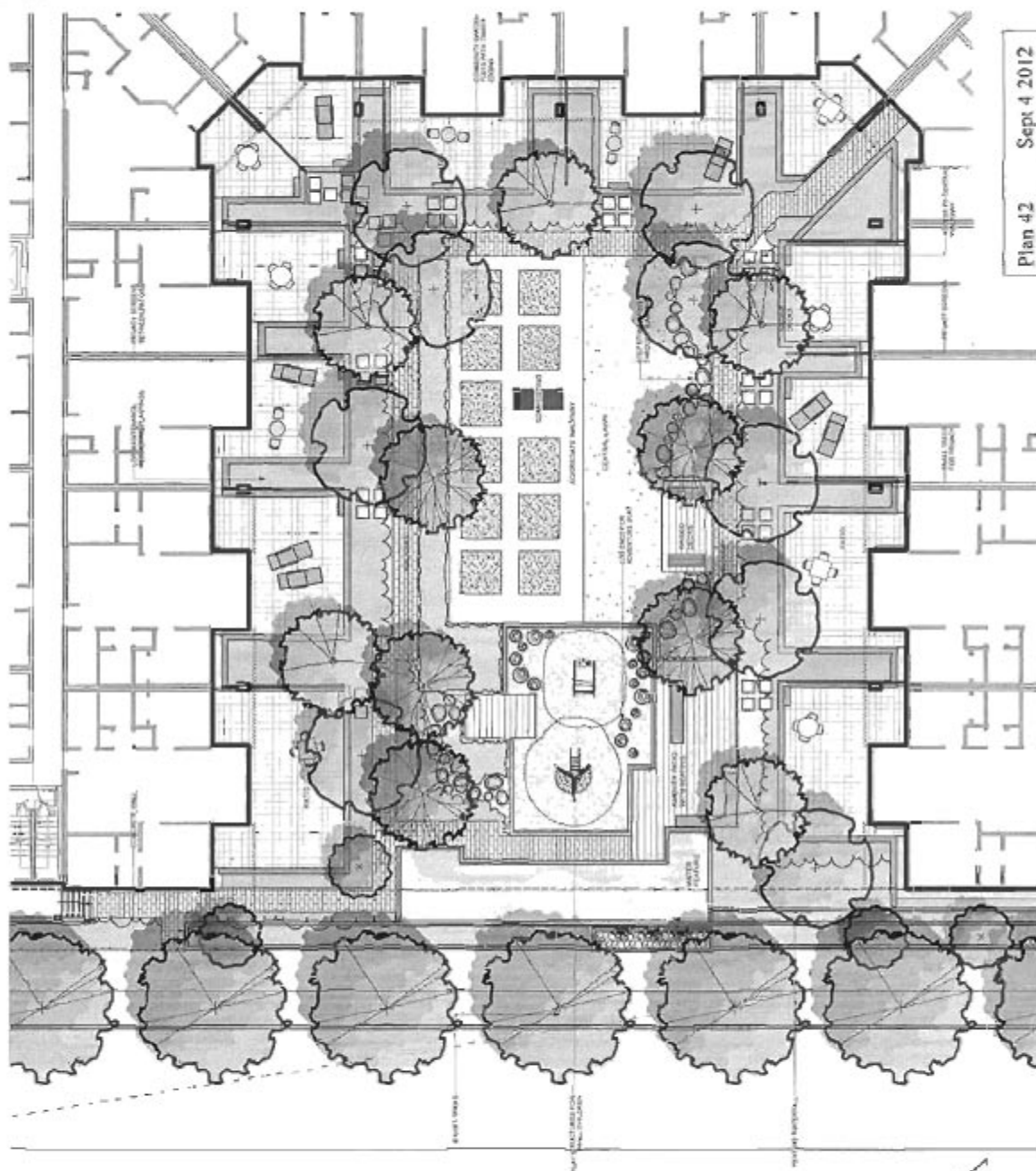
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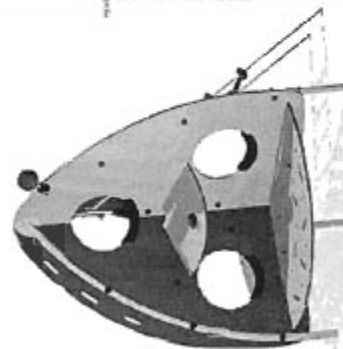
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1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036	2037	2038	2039	2040	2041	2042	2043	2044	2045	2046	2047	2048	2049	2050	2051	2052	2053	2054	2055	2056	2057	2058	2059	2060	2061	2062	2063	2064	2065	2066	2067	2068	2069	2070	2071	2072	2073	2074	2075	2076	2077	2078	2079	2080	2081	2082	2083	2084	2085	2086	2087	2088	2089	2090	2091	2092	2093	2094	2095	2096	2097	2098	2099	2100	

BUILDING 4
COURTYARD
DETAIL PLAN

L1.3



Plan 42 Sept 4 2012
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CONSUMER CHOICE



Question	Answer
1. What is the purpose of the study?	To investigate the effect of the new curriculum on the learning outcomes of the students.
2. What is the research design?	Quasi-experimental design.
3. What is the sample size?	100 students.
4. What is the data collection instrument?	Questionnaire.
5. What is the data analysis technique?	Descriptive statistics.
6. What is the conclusion of the study?	The new curriculum has a positive effect on the learning outcomes of the students.



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asked with only a few minutes' notice to arrange a visit to the

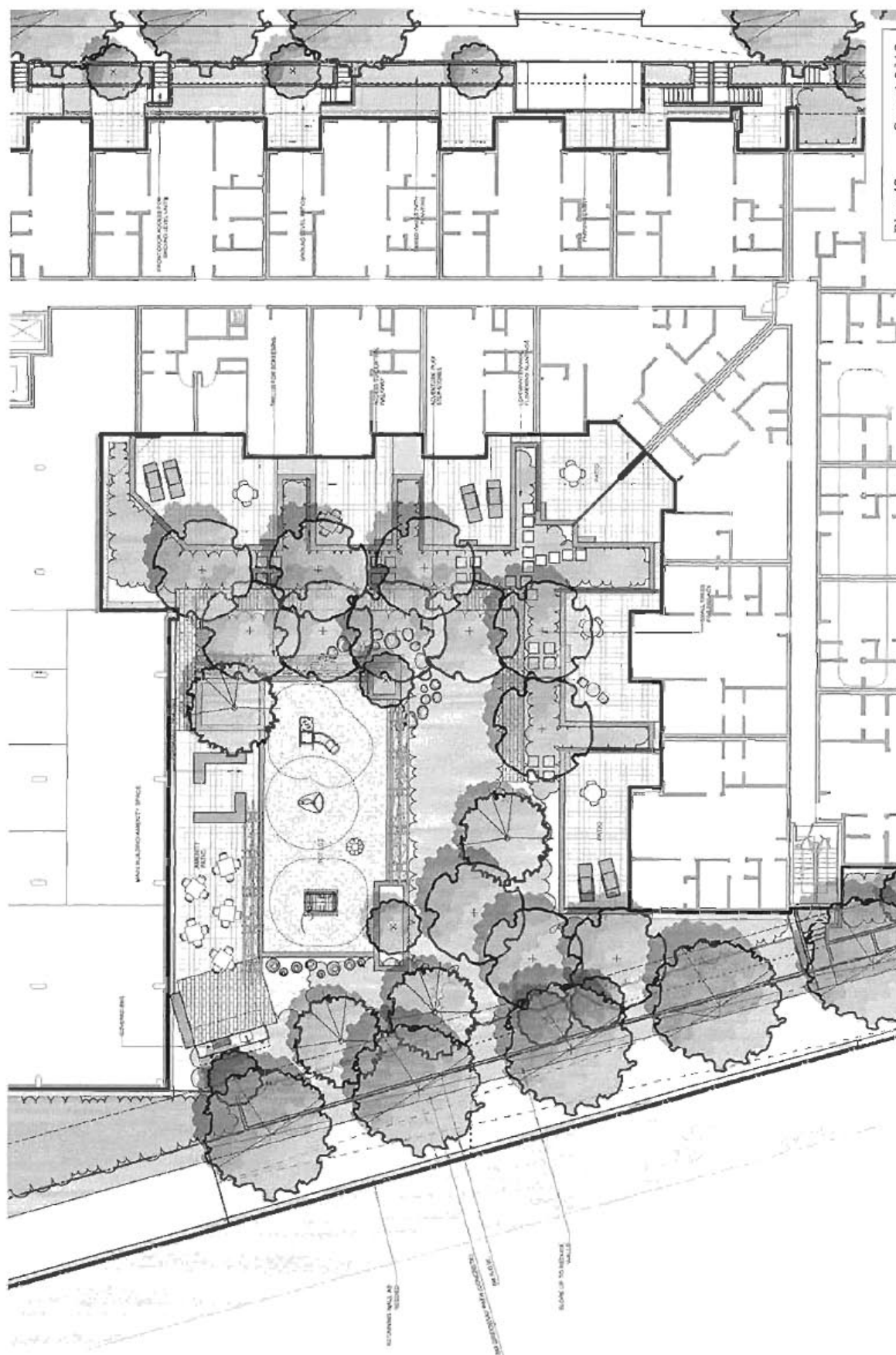
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SOLUBLE AND INSOLUBLE FRACTIONS	
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98.63	98.63
98.62	98.62

BUILDING 1
COURTYARD
DETAIL PLAN

L1.4

Plan 43 Sept 4 2012
DP 12-615424



1	PLAN	12-615424
2	SECTION	12-615424
3	ELEVATION	12-615424
4	DETAIL	12-615424
5	LANDSCAPE	12-615424
6	EXTERIOR	12-615424
7	INTERIOR	12-615424
8	MECHANICAL	12-615424
9	ELECTRICAL	12-615424
10	PLUMBING	12-615424
11	HAZARDOUS WASTE	12-615424
12	ASBESTOS	12-615424
13	LEAD	12-615424
14	PCB	12-615424
15	OTHER	12-615424



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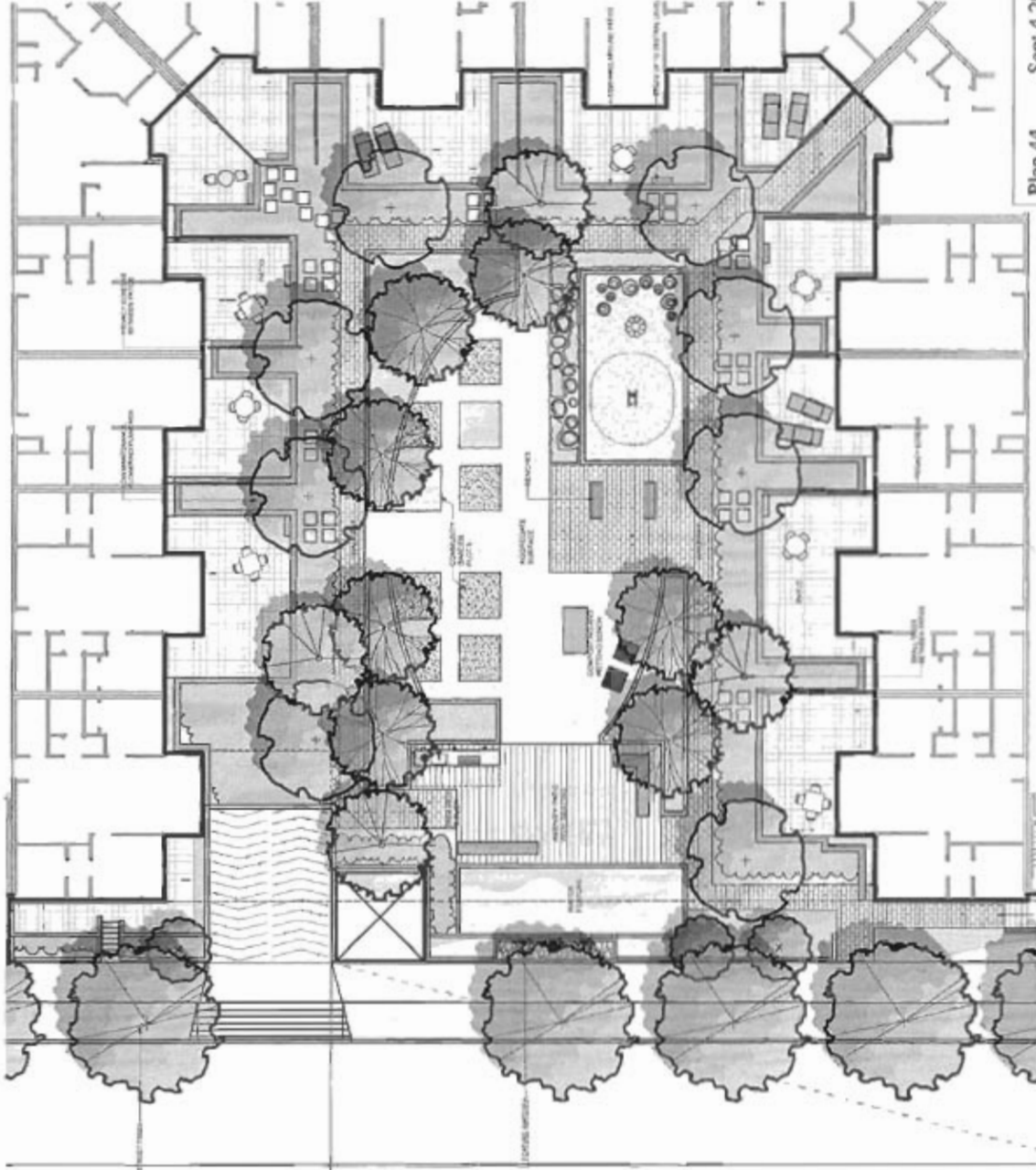
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Landscape Architecture

1	PLAN	12-615424
2	SECTION	12-615424
3	ELEVATION	12-615424
4	DETAIL	12-615424
5	LANDSCAPE	12-615424
6	EXTERIOR	12-615424
7	INTERIOR	12-615424
8	MECHANICAL	12-615424
9	ELECTRICAL	12-615424
10	PLUMBING	12-615424
11	HAZARDOUS WASTE	12-615424
12	ASBESTOS	12-615424
13	LEAD	12-615424
14	PCB	12-615424
15	OTHER	12-615424

BUILDING 3
COURTYARD
DETAIL PLAN

L1.5

Plan 44 Sept 4 2012
DP 12-615424



COURTYARD GARDENING PRESENTS



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
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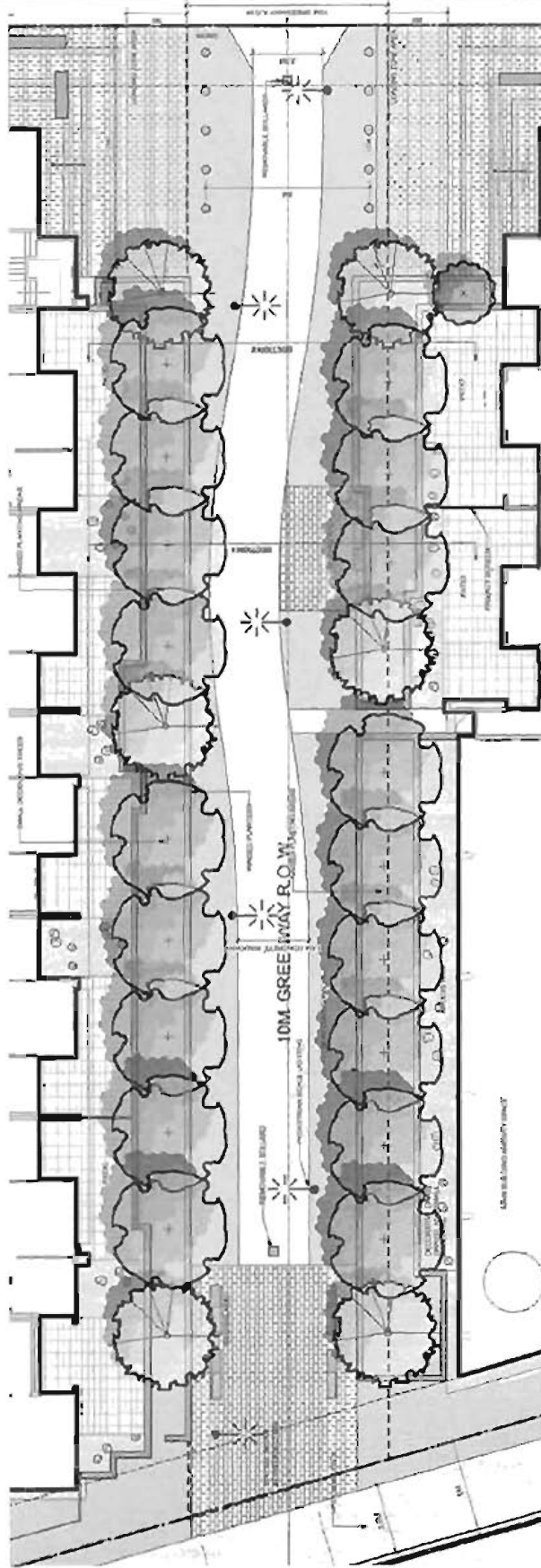
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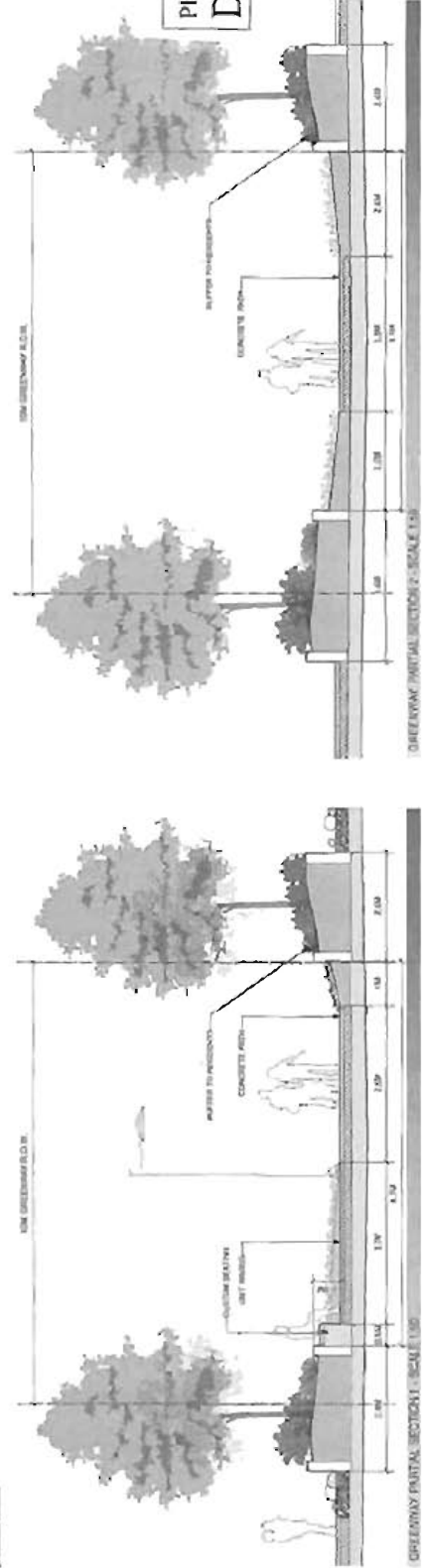
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**EAST / WEST
COMMUNITY GARDEN
GREENWAY**

21.8



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NOTES

1	GENERAL NOTES
2	EXISTING CONDITIONS
3	PROPOSED CONDITIONS
4	CONSTRUCTION METHODS
5	ENVIRONMENTAL NOTES
6	UTILITIES
7	LANDSCAPE
8	TRAVEL
9	STRUCTURE
10	MECHANICAL
11	ELECTRICAL
12	TELEPHONE
13	CABLE TELEVISION
14	WATER
15	SEWER
16	STORM DRAINAGE
17	IRRIGATION
18	LANDSCAPE ARCHITECTURE
19	ARCHITECTURE
20	ENGINEERING
21	PLANNING
22	ENVIRONMENTAL
23	TRAVEL
24	STRUCTURE
25	MECHANICAL
26	ELECTRICAL
27	TELEPHONE
28	CABLE TELEVISION
29	WATER
30	SEWER
31	STORM DRAINAGE
32	IRRIGATION
33	LANDSCAPE ARCHITECTURE
34	ARCHITECTURE
35	ENGINEERING
36	PLANNING
37	ENVIRONMENTAL
38	TRAVEL
39	STRUCTURE
40	MECHANICAL
41	ELECTRICAL
42	TELEPHONE
43	CABLE TELEVISION
44	WATER
45	SEWER
46	STORM DRAINAGE
47	IRRIGATION
48	LANDSCAPE ARCHITECTURE
49	ARCHITECTURE
50	ENGINEERING
51	PLANNING
52	ENVIRONMENTAL
53	TRAVEL
54	STRUCTURE
55	MECHANICAL
56	ELECTRICAL
57	TELEPHONE
58	CABLE TELEVISION
59	WATER
60	SEWER
61	STORM DRAINAGE
62	IRRIGATION
63	LANDSCAPE ARCHITECTURE
64	ARCHITECTURE
65	ENGINEERING
66	PLANNING
67	ENVIRONMENTAL
68	TRAVEL
69	STRUCTURE
70	MECHANICAL
71	ELECTRICAL
72	TELEPHONE
73	CABLE TELEVISION
74	WATER
75	SEWER
76	STORM DRAINAGE
77	IRRIGATION
78	LANDSCAPE ARCHITECTURE
79	ARCHITECTURE
80	ENGINEERING
81	PLANNING
82	ENVIRONMENTAL
83	TRAVEL
84	STRUCTURE
85	MECHANICAL
86	ELECTRICAL
87	TELEPHONE
88	CABLE TELEVISION
89	WATER
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91	STORM DRAINAGE
92	IRRIGATION
93	LANDSCAPE ARCHITECTURE
94	ARCHITECTURE
95	ENGINEERING
96	PLANNING
97	ENVIRONMENTAL
98	TRAVEL
99	STRUCTURE
100	MECHANICAL

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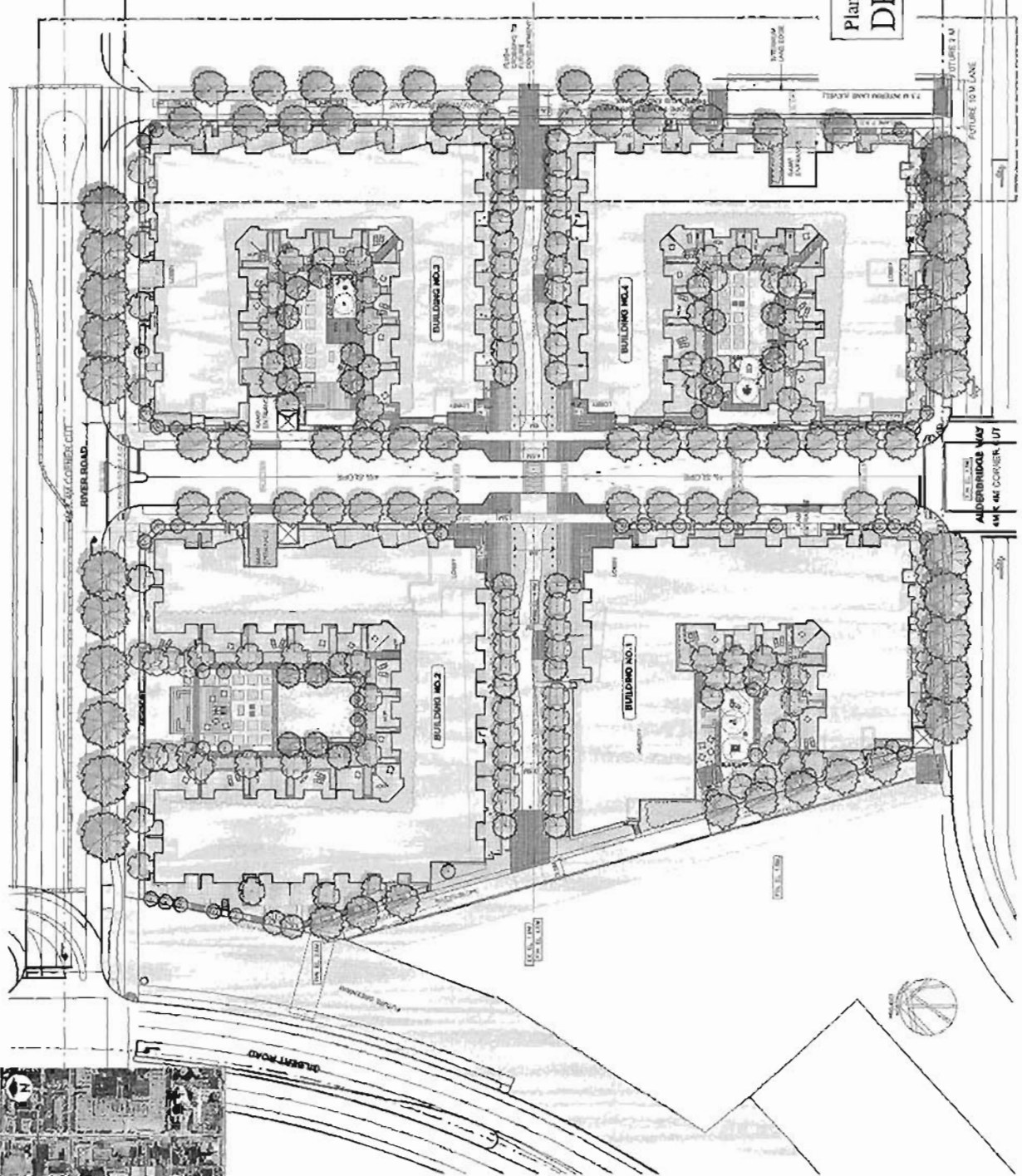
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FUTURE LANE GREENWAY

L1.7



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Plan 47 Sept 4 2012
DP 12-615424ON-SITE PRIVATE AND
SHARED OPEN SPACE
AND PROPOSED PUBLIC
ART PLAN

L1.8



ON-SITE PRIVATE AND SHARED OPEN SPACE
AND PROPOSED PUBLIC ART PLAN

STATISTICS / LEGEND

SHARED OPEN SPACE

- TOTAL OUTDOOR SHARED AMENITY AREA, 5,176 SQ.M. (55,714 SQ.FT.)
- REQUIREMENTS AT 6 SQ.M. PER UNIT
865 TOTAL UNITS = 5,190 SQ.M. SHARED AMENITY SPACE
- INDOOR SHARED AMENITY AREA
BLDG 1 (SHARED BY BLDG 2): 1,832 SQ.M.
BLDG 3: 21 SQ.M.
BLDG 4: 35 SQ.M.
BLDG 5: 50 SQ.M.
TOTAL INDOOR SHARED AMENITY AREA, 1,874 SQ.M. (20,171 SQ.FT.)

PRIVATE OPEN SPACE

- MINIMUM PRIVATE OPEN SPACE AT GROUND LEVEL = 24.50 M. AREA
MEDIAN PRIVATE OPEN SPACE AREA AT GROUND LEVEL = 43.50 M. AREA
REQUIREMENTS AT 30.50 M. PER UNIT MINIMUM PRIVATE
OPEN SPACE AREA FOR URBAN CENTRE (75) (GROUND LEVEL)
AND 6.50 M. BALCONY SIZE

• THE NUMBER OF BALCONIES PER BUILDING
BLDG 1: 181 BLDG 2: 221, BLDG 3: 194, AND BLDG 4: 212
TOTAL BALCONIES: 709
TOTAL BALCONY SPACE: 4,734 SQ. M. (50,940 SQ. FT.)

PUBLIC ART

- * POTENTIAL ART LOCATIONS
* POTENTIAL LANDMARK ART LOCATION

NOTE:
ALL PROPOSED PUBLIC ART LOCATIONS SUBJECT TO PUBLIC ART
COMMITTEE AND STAFF REVIEW FOLLOWING PUBLIC ART POLICY

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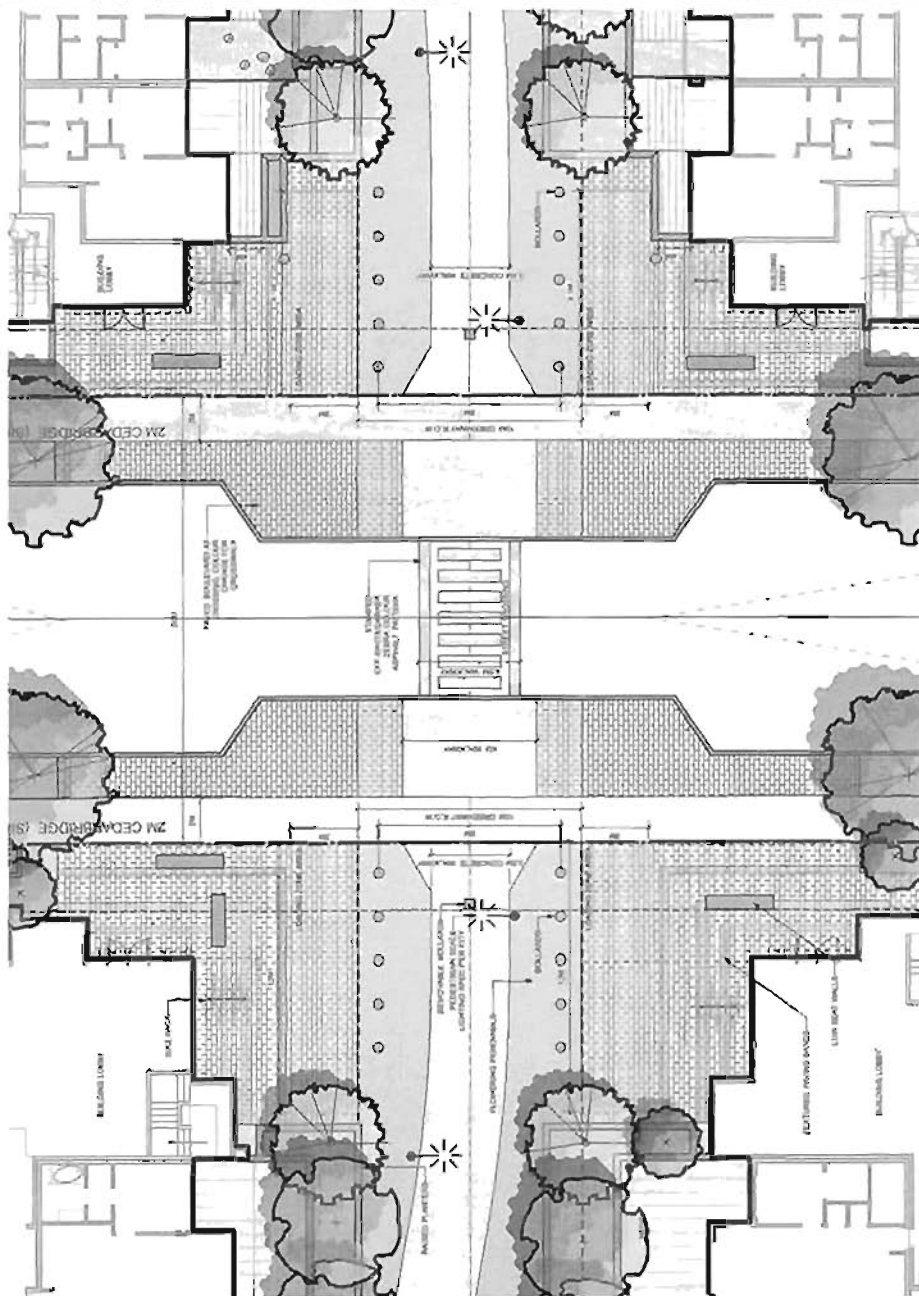
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CROSSING

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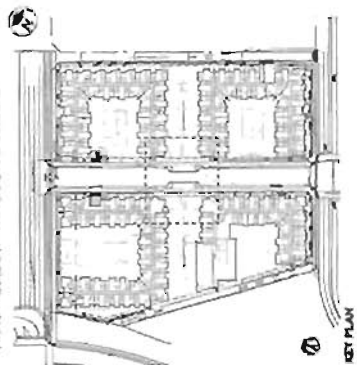


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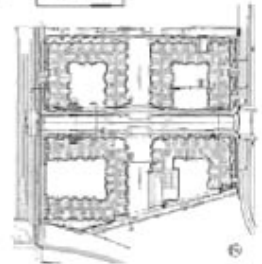
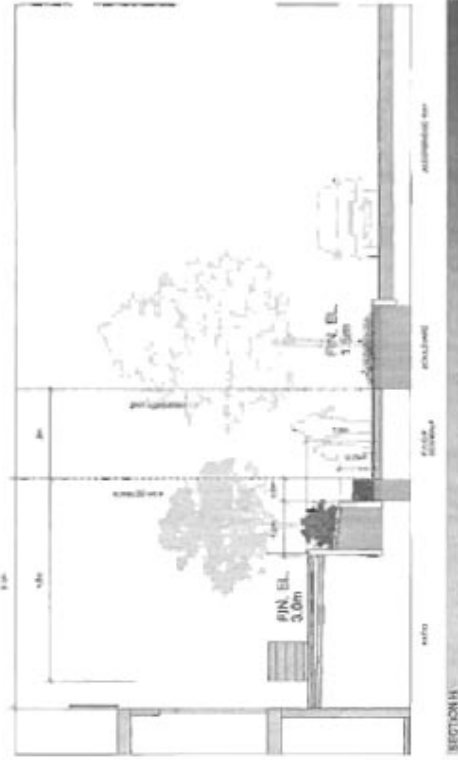
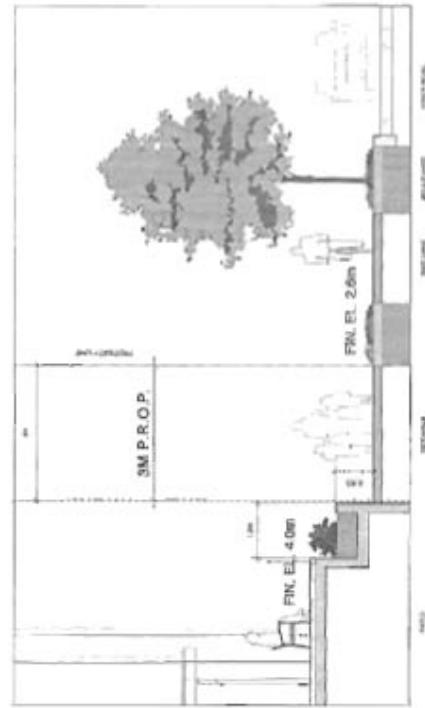
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PAVING PRECEDENT: PATTERNS AND COLORING



KEY PLAN



NOTES

1.	ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FOLLOWING:
2.	THE 2009 EDITIONS OF THE CALIFORNIA BUILDING CODE (CBC) AND THE CALIFORNIA ELECTRICAL CODE (CEC).
3.	THE 2009 EDITIONS OF THE CALIFORNIA MECHANICAL CODE (CMC) AND THE CALIFORNIA PLUMBING CODE (CPC).
4.	THE 2009 EDITIONS OF THE CALIFORNIA FIRE CODE (CFC) AND THE CALIFORNIA SAFETY CODE (CSC).
5.	THE 2009 EDITIONS OF THE CALIFORNIA ENVIRONMENTAL CODE (CEC) AND THE CALIFORNIA WASTE MANAGEMENT CODE (CWM).
6.	THE 2009 EDITIONS OF THE CALIFORNIA LAND USE CODE (CLUC) AND THE CALIFORNIA ZONING CODE (CZC).
7.	THE 2009 EDITIONS OF THE CALIFORNIA PLANNING AND ZONING ACT (CPZA) AND THE CALIFORNIA PLANNING AND ZONING ACT (CPZA).
8.	THE 2009 EDITIONS OF THE CALIFORNIA PLANNING AND ZONING ACT (CPZA) AND THE CALIFORNIA PLANNING AND ZONING ACT (CPZA).
9.	THE 2009 EDITIONS OF THE CALIFORNIA PLANNING AND ZONING ACT (CPZA) AND THE CALIFORNIA PLANNING AND ZONING ACT (CPZA).
10.	THE 2009 EDITIONS OF THE CALIFORNIA PLANNING AND ZONING ACT (CPZA) AND THE CALIFORNIA PLANNING AND ZONING ACT (CPZA).



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LANDSCAPE SECTIONS

L3.2

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After 12 days

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publ. Sept.-Nov., vol. 9, 1968

Abstract

KEY PLAN

DP 12-615424

LANDSCAPE SECTIONS

L3.3

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Architecture Inc.**

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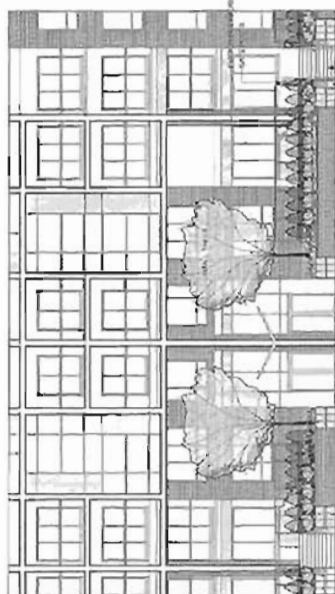
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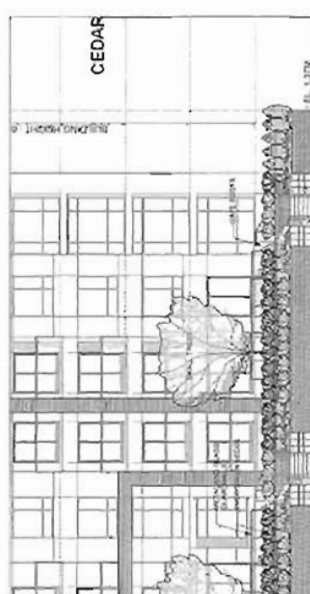
BUILDING 3 PATIO DETAIL PLAN - SCALE 1:100



ELEVATION - RIVER ROAD - SCALE 1:100



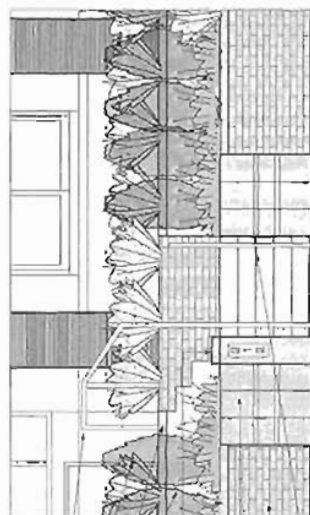
BUILDING 1 PATIO DETAIL PLAN - SCALE 1:100



ELEVATION - ADAIR ROAD - SCALE 1:100



BUILDING 4 PATIO DETAIL PLAN - SCALE 1:100



DETAIL ELEVATION - TYPICAL STREET LEVEL UNIT ENTRY - SCALE 1:25

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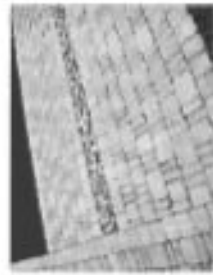
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Plan 53 Sept 4 2012
DP 12-615424

LANDSCAPE ELEVATIONS
AND DETAILS

L3.4



BRICK FACETING



BRICK FACETING



BRICK FACETING



BRICK FACETING

MATERIALS



BRICK FACETING



WOODEN BENCH



MODERN BENCH



MODERN BENCH

SITE FURNISHINGS



1. TYPICAL WALLS WITH PLANTING



2. TYPICAL WALLS WITH PLANTING



3. TYPICAL WALLS WITH PLANTING



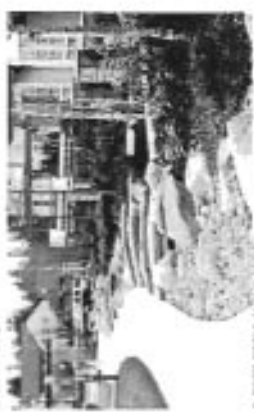
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5. TYPICAL WALLS WITH PLANTING

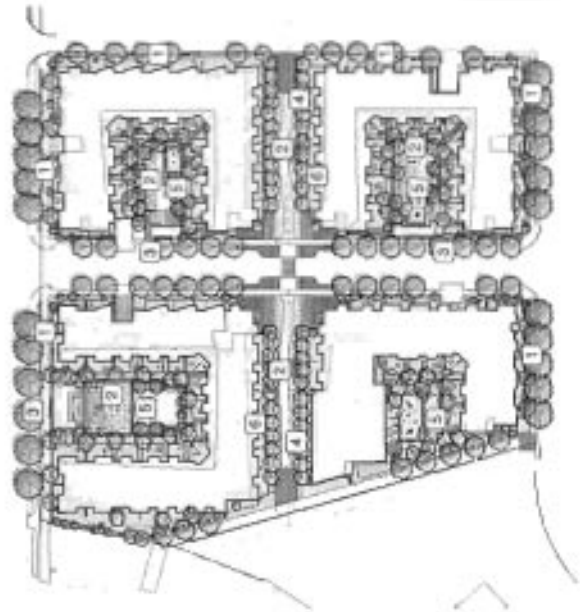


6. TYPICAL WALLS WITH PLANTING



7. TYPICAL WALLS WITH PLANTING

SITE PRECEDENT IMAGES



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12. TYPICAL WALLS WITH PLANTING	12. TYPICAL WALLS WITH PLANTING

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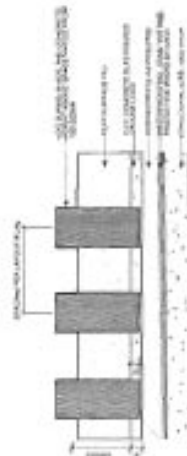
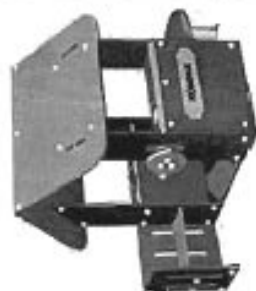
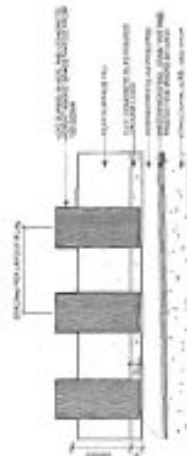
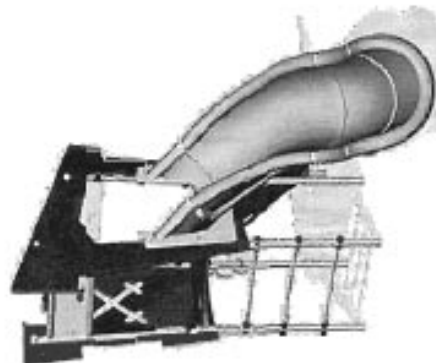
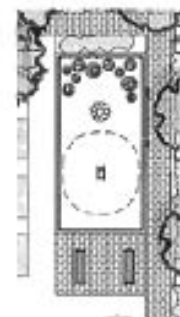
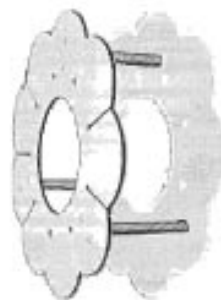
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Plan 54 Sept 4 2012
DP 12-615424

PRECEDENT IMAGES

L4.1



1. **NAME OF THE PARTY:** THE NATIONAL DEMOCRATIC PARTY

2. **NAME OF THE CANDIDATE:** MR. JAMES M. SMITH

3. **NAME OF THE DISTRICT:** 1ST DISTRICT

4. **NAME OF THE COUNTY:** CLATSOP COUNTY

5. **NAME OF THE STATE:** OREGON

6. **NAME OF THE CITY:** ASTORIA

7. **NAME OF THE TOWN:** ASTORIA

8. **NAME OF THE WARD:** 1ST WARD

9. **NAME OF THE POLLING PLACE:** ASTORIA HIGH SCHOOL

10. **NAME OF THE JUDICIAL DISTRICT:** 1ST JUDICIAL DISTRICT

11. **NAME OF THE COUNTY CLERK:** JOHN A. SMITH

12. **NAME OF THE COUNTY CLERK'S OFFICE:** CLATSOP COUNTY CLERK'S OFFICE

13. **NAME OF THE COUNTY CLERK'S ADDRESS:** 1000 1ST AVENUE, ASTORIA, OREGON

14. **NAME OF THE COUNTY CLERK'S PHONE NUMBER:** 503-325-1234

15. **NAME OF THE COUNTY CLERK'S FAX NUMBER:** 503-325-1234

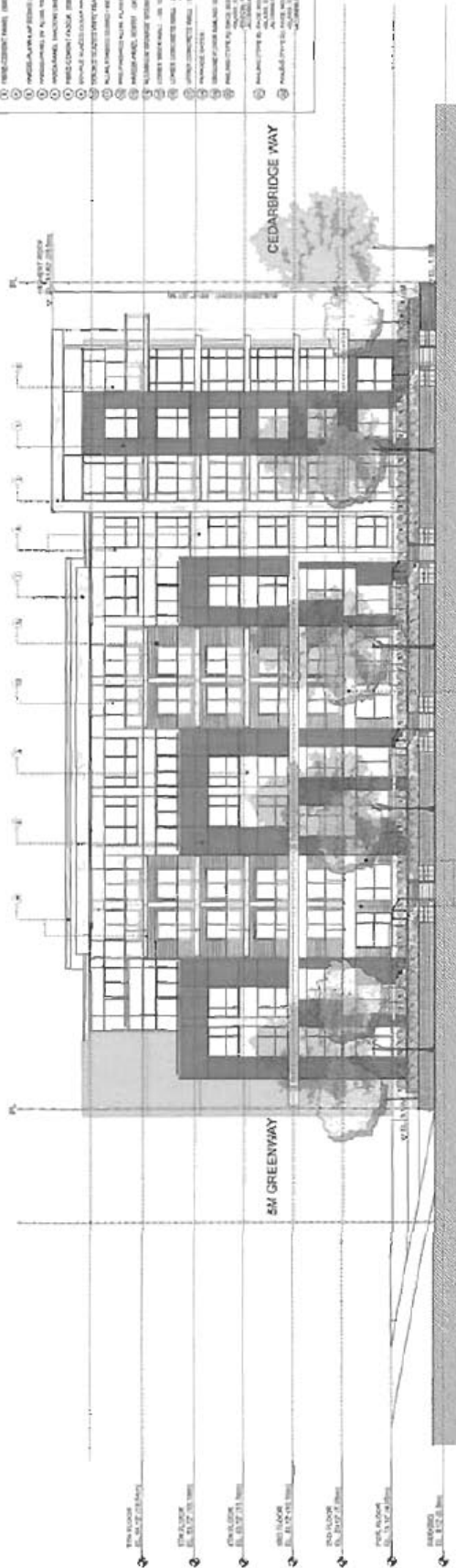
16. **NAME OF THE COUNTY CLERK'S E-MAIL ADDRESS:** JOHN.A.SMITH@CLATSOPCOUNTY.OREGON.GOV

17. **NAME OF THE COUNTY CLERK'S WEBSITE:** WWW.CLATSOPCOUNTY.OREGON.GOV

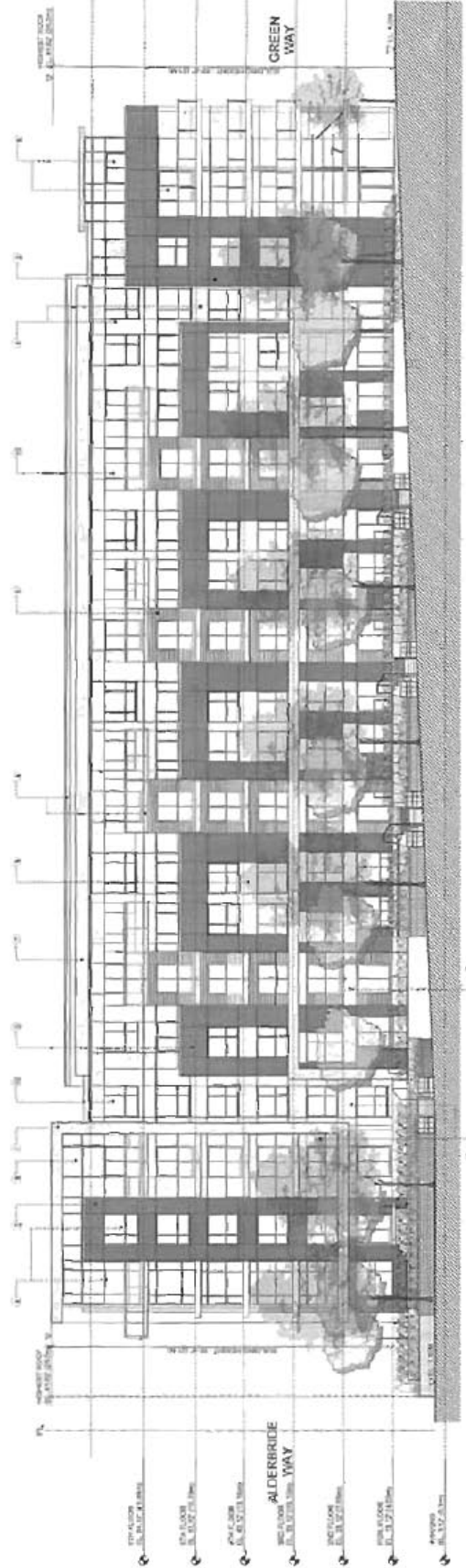
18. **NAME OF THE COUNTY CLERK'S SOCIAL MEDIA PAGE:** CLATSOPCOUNTY

19. **NAME OF THE COUNTY CLERK'S SOCIAL MEDIA PAGE:** CLATSOPCOUNTY

20. **NAME OF THE COUNTY CLERK'S SOCIAL MEDIA PAGE:** CLATSOPCOUNTY



BUILDING 1 SOUTH ELEVATION (ALDERBRIDGE WAY)



BUILDING 1 EAST ELEVATION (CEDARBRIDGE WAY)

Plan 57 Sept 4 2012

DP 12-615424

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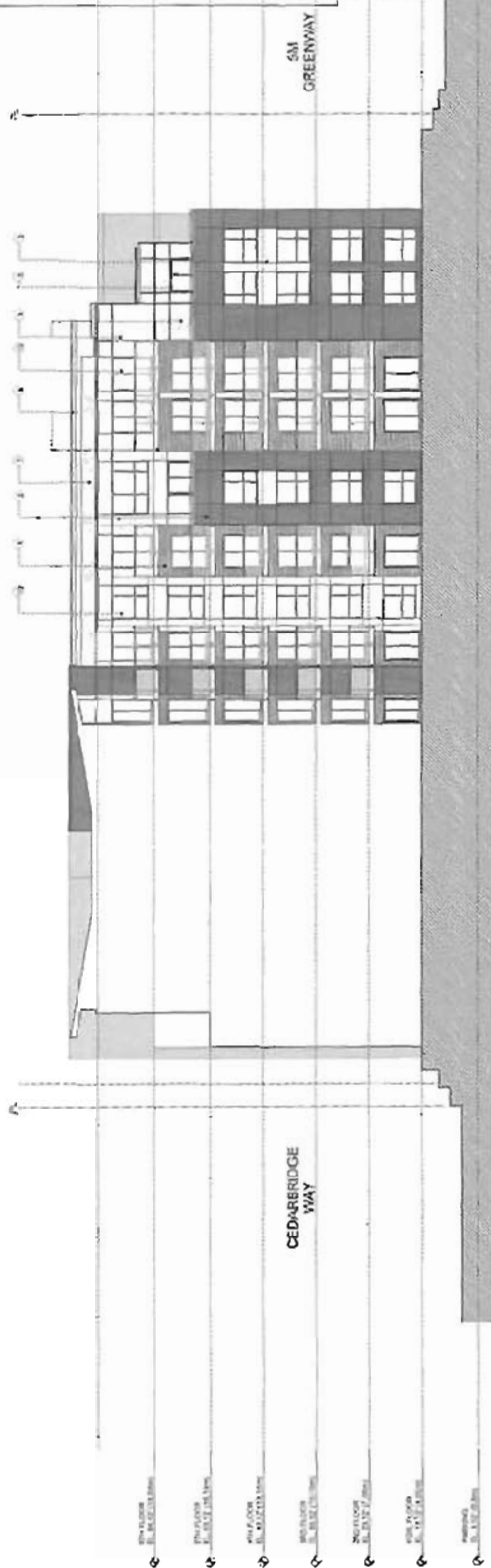
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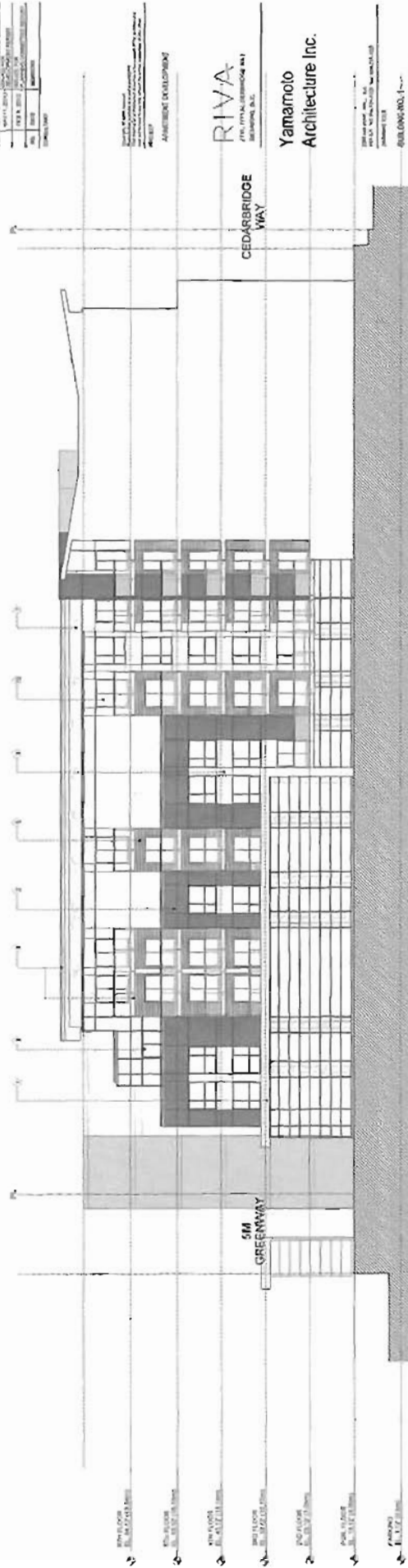
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BUILDING 1 COURT YARD SOUTH ELEVATION



BUILDING 1 COURT YARD NORTH ELEVATION

Plan S9 Sept 4 2012
DP 12-615424

DATE: 09/04/12
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BUILDING NO. 1
COURTYARD ELEVATIONS

RIVA
ARCHITECTURAL
DESIGN, INC.

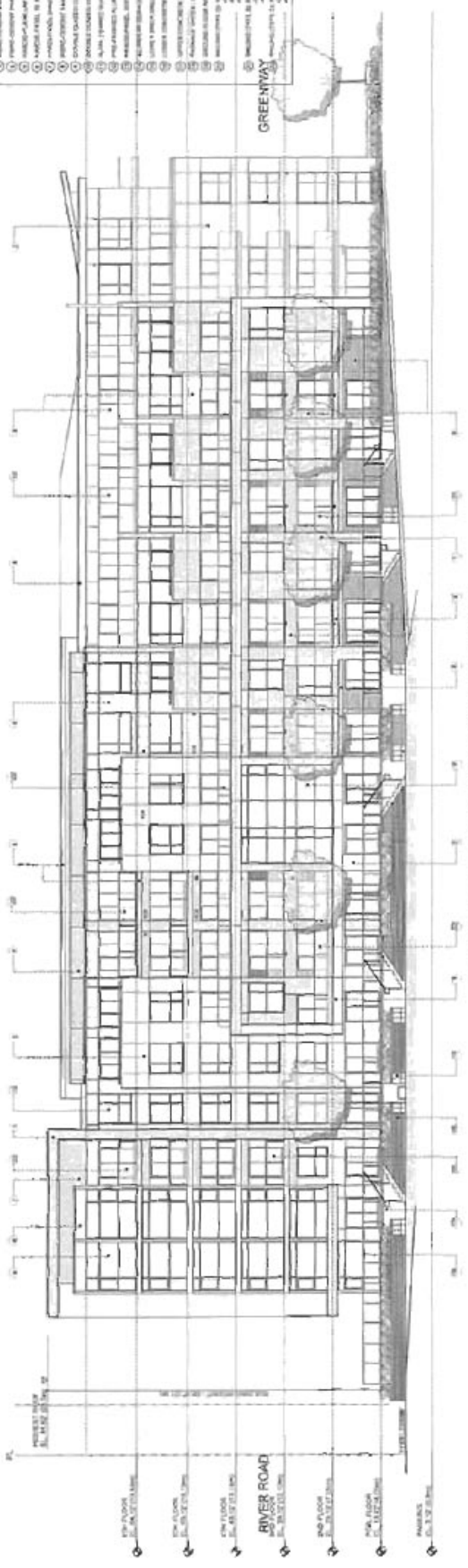
Yamamoto
Architecture Inc.

ARCHITECT DEVELOPMENT

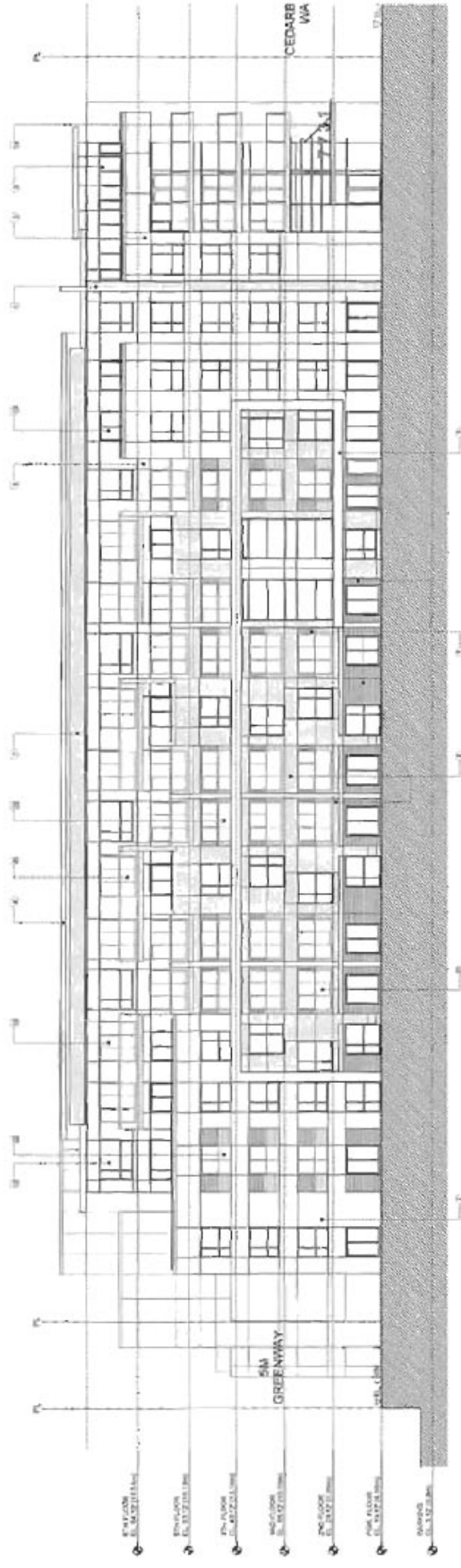
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BUILDING ELEVATION NOTES:

1. EXTERIOR FINISHES: BRICK, STONE, AND CONCRETE.
2. ROOF FINISHES: ASPHALT/FLY ASH, METAL, OR CLAY TILE.
3. FLOOR FINISHES: POLISHED CONCRETE, CARPET, OR HARDWOOD.
4. WALL FINISHES: PLASTER, GYPSONUM, OR STUCCO.
5. CEILING FINISHES: PLASTER, GYPSONUM, OR STUCCO.
6. DOOR FINISHES: STAINLESS STEEL, ALUMINUM, OR WOOD.
7. WINDOW FINISHES: ALUMINUM, WOOD, OR VINYL.
8. PAINT FINISHES: EXTERIOR - WHITE, INTERIOR - VARIOUS COLORS.
9. GLASS FINISHES: CLEAR, TINTED, OR LOW-E.
10. METAL FINISHES: GALVALUM, ALUMINUM, OR STAINLESS STEEL.
11. CONCRETE FINISHES: POLISHED, STAINED, OR PAINTED.
12. FINISHES TO BE DETERMINED BY THE ARCHITECT.
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BUILDING 2 WEST ELEVATION (GILBERT ROAD)



BUILDING 2 SOUTH ELEVATION (10M GREENWAY)

NO.	DATE	DESCRIPTION
1	10/1/11	ISSUED FOR PERMIT
2	10/1/11	ISSUED FOR PERMIT
3	10/1/11	ISSUED FOR PERMIT
4	10/1/11	ISSUED FOR PERMIT
5	10/1/11	ISSUED FOR PERMIT
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9	10/1/11	ISSUED FOR PERMIT
10	10/1/11	ISSUED FOR PERMIT

RIVA
ARCHITECTURE
1000 10TH AVENUE, SUITE 100
DENVER, CO 80202
TEL: 303.733.1111
WWW.RIVAARCHITECTURE.COM

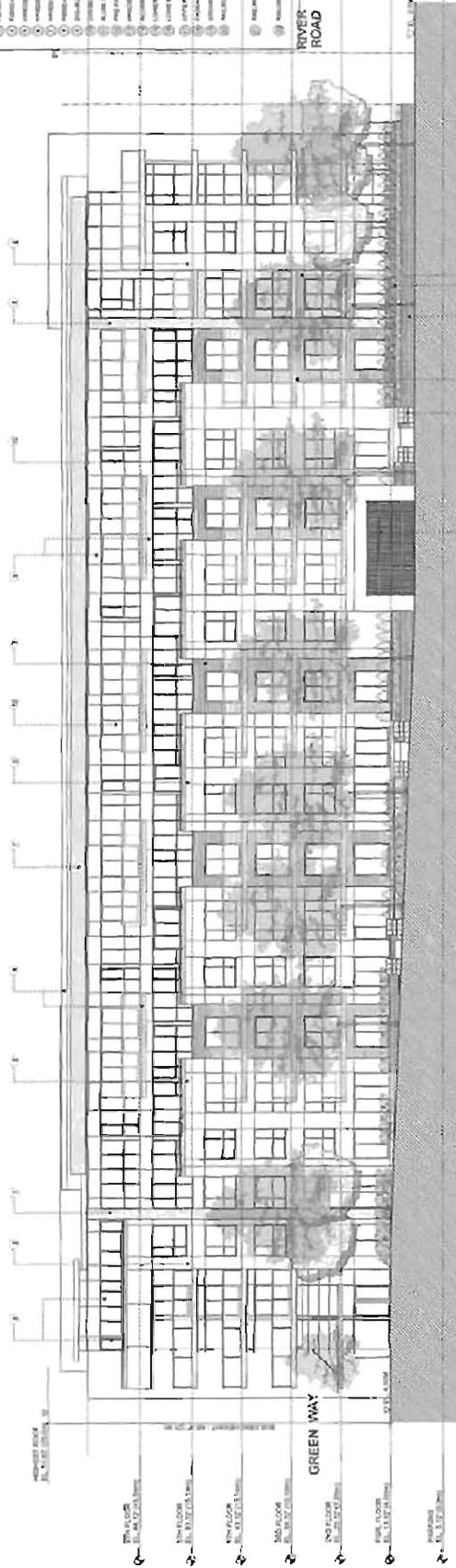
Yamamoto
Architecture Inc.
1000 10TH AVENUE, SUITE 100
DENVER, CO 80202
TEL: 303.733.1111
WWW.YAMAMOTOARCHITECTURE.COM

BUILDING NO. 2
WEST / SOUTH ELEVATIONS

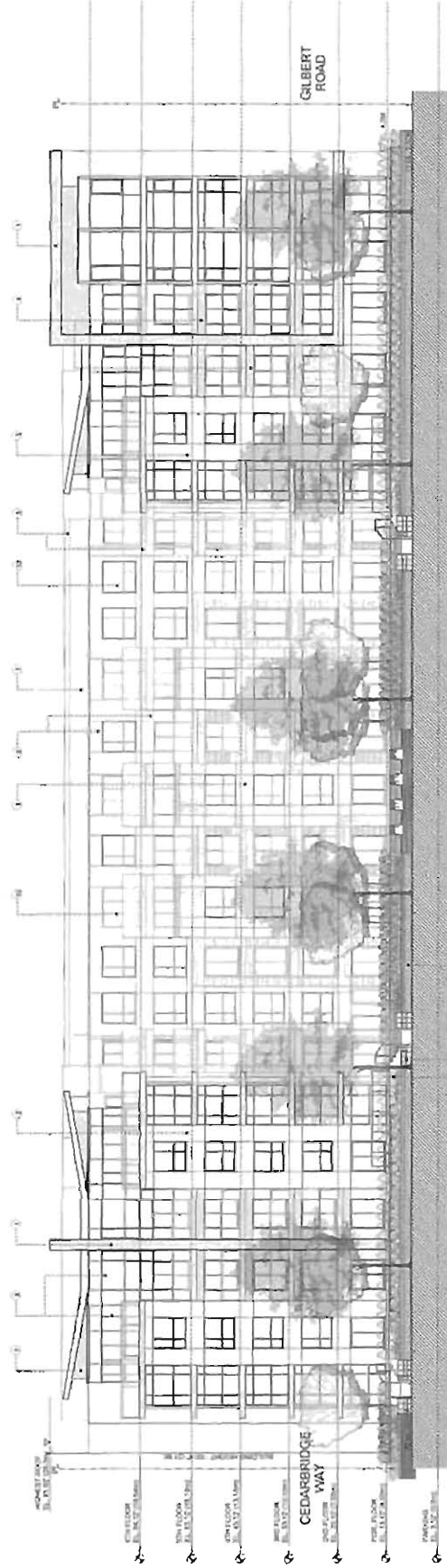
Plan 61 Sept 4 2012
DP 12-615424

NO.	DATE	DESCRIPTION
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9	10/1/11	ISSUED FOR PERMIT
10	10/1/11	ISSUED FOR PERMIT

- 1) **What is the primary purpose of the "Statement of Financial Position" (Balance Sheet)?**
The primary purpose of the Statement of Financial Position is to provide a snapshot of a company's financial health at a specific point in time, showing its assets, liabilities, and equity.
- 2) **What are the three main components of the Statement of Financial Position?**
The three main components are Assets, Liabilities, and Equity.
- 3) **How is the Statement of Financial Position related to the Statement of Income?**
The Statement of Financial Position is related to the Statement of Income because the net income (or loss) reported in the Statement of Income is added to (or subtracted from) the retained earnings account in the Equity section of the Statement of Financial Position.
- 4) **What is the difference between current assets and non-current assets?**
Current assets are assets that are expected to be converted into cash or consumed within one year or the operating cycle, whichever is longer. Non-current assets are assets that are not expected to be converted into cash or consumed within one year or the operating cycle.
- 5) **What is the difference between current liabilities and non-current liabilities?**
Current liabilities are liabilities that are expected to be settled within one year or the operating cycle, whichever is longer. Non-current liabilities are liabilities that are not expected to be settled within one year or the operating cycle.
- 6) **What is the difference between common stock and preferred stock?**
Common stock represents ownership in a company and typically has no fixed dividend. Preferred stock is a type of equity that typically has a fixed dividend and may have certain rights, such as the right to elect directors.
- 7) **What is the difference between a debit and a credit?**
A debit is an entry on the left side of an account, and a credit is an entry on the right side of an account. Debits and credits are used to record transactions in the accounting system.
- 8) **What is the difference between a debit and a credit in the context of the Statement of Financial Position?**
In the context of the Statement of Financial Position, a debit entry increases an asset account or decreases a liability or equity account. A credit entry decreases an asset account or increases a liability or equity account.
- 9) **What is the difference between a debit and a credit in the context of the Statement of Income?**
In the context of the Statement of Income, a debit entry increases an expense account or decreases a revenue account. A credit entry decreases an expense account or increases a revenue account.
- 10) **What is the difference between a debit and a credit in the context of the Statement of Cash Flows?**
In the context of the Statement of Cash Flows, a debit entry increases a cash account or decreases a liability or equity account. A credit entry decreases a cash account or increases a liability or equity account.



BUILDING 2 EAST ELEVATION (CEDARBRIDGE WAY)



BUILDING 2 NORTH ELEVATION (RIVER ROAD)

Plan 62 Sept 4 2012
DP 12-615424

DATE	TIME	DATE	TIME
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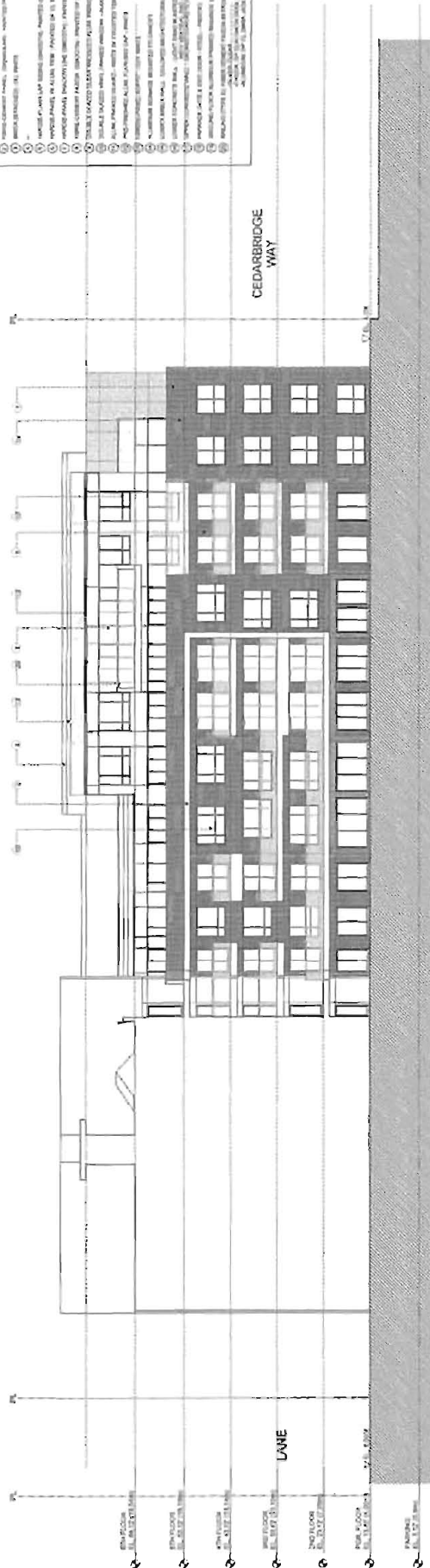
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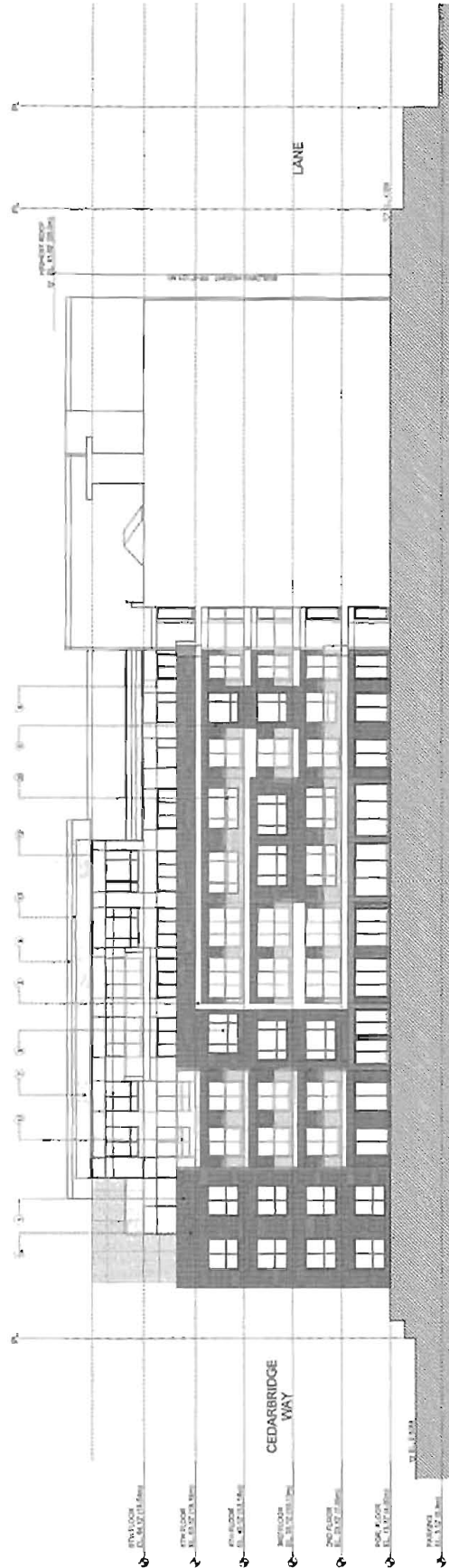
1000 Oakwood, Inc., Ltd.
4400 4th St. S.W. 98104-1000 Seattle, WA 98148

BUILDING 3 - ELEVATION KEY NOTES

- [illegible]



BUILDING 1 COURT YARD SOUTH ELEVATION



BUILDING 1 COURT YARD NORTH ELEVATION

Plan 67 Sept 4 2012
DP 12-615424

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THE CHINESE

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Plan 68 Sept 4 2012
DP 12-615424

BUILDING NO. 3		A2-3.4	
DETAIL ELEVATION 3			
SHEET NO.			
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CHECKED			
APPROVED			

BUILDING 3 DETAIL ELEVATION (AT ALDERBRIDGE WAY AND LANE)

[illegible]

BUILDING 4 SOUTH ELEVATION (ALDERBRIDGE WAY)

[illegible]

RECEIVED
AGENCY OF DEVELOPMENT

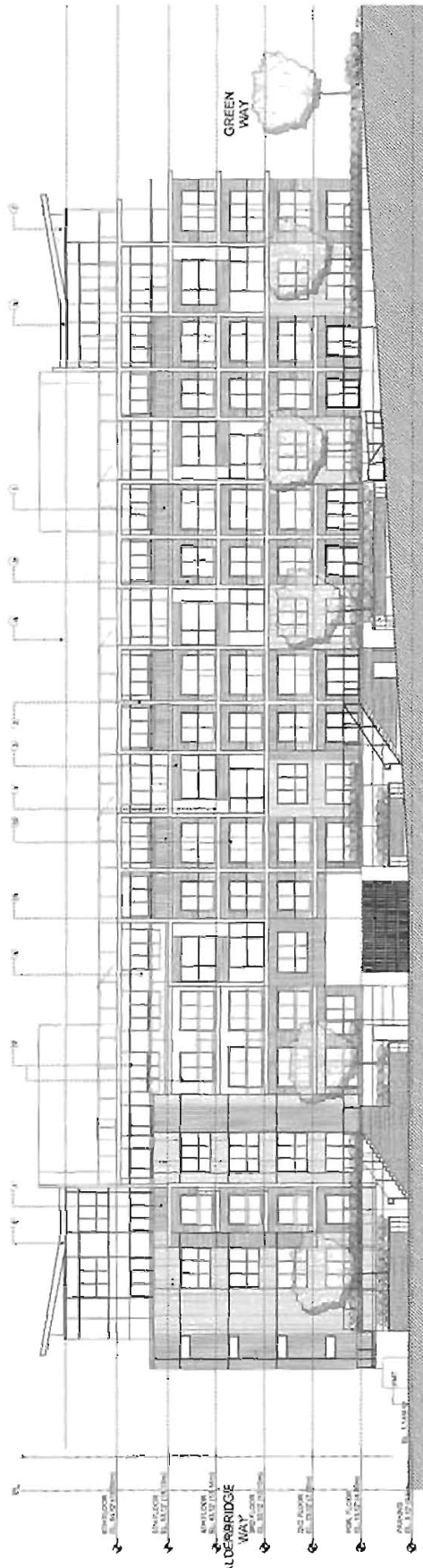
RIVA

Yamamoto
Architecture Inc.

ILLINOIS JOURNAL OF LAW AND MEDICAL PROFESSIONS
JULY 2015 VOL 19 / NO 4

		AZ-4.7			
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Plan 70 Sept 4 2012
DP 12-615424

BUILDING & EAST ELEVATION (LANE)

Architectural drawing of the Cedarbridge Way building facade. The drawing shows a multi-story building with a grid of windows. The facade is divided into sections by vertical and horizontal lines. The building is labeled "CEDARBRIDGE WAY" in the center. To the left, there are labels for "BYPASS ROOM" and "PARK FLOOR". To the right, there are labels for "PARK FLOOR" and "PARK FLOOR". The drawing includes a north arrow pointing towards the top right. The building is shown in a perspective view, with the ground level indicated by a dashed line. The drawing is a black and white line drawing.

PROPOSED SITE PLAN

LANE

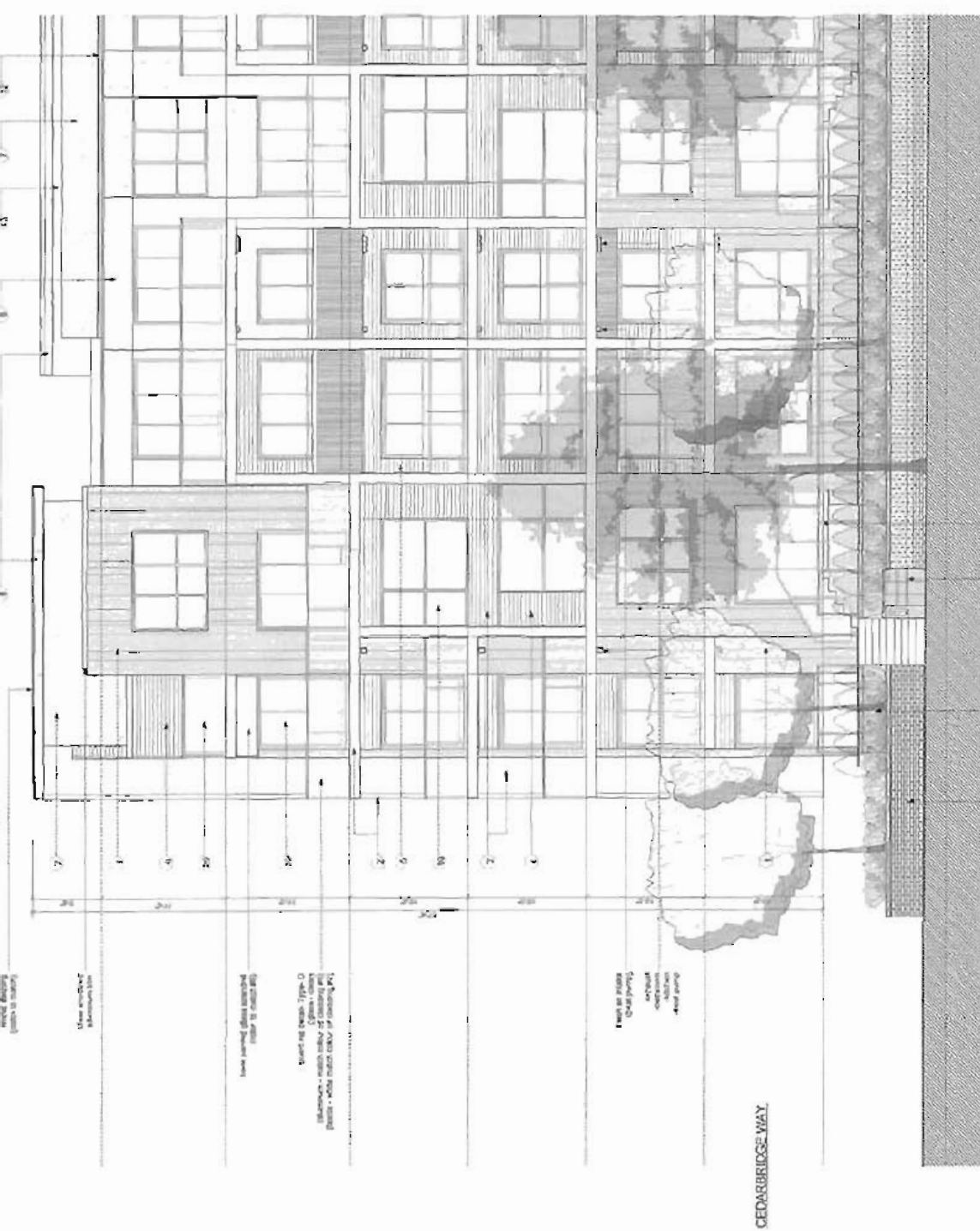
PARKING LOT

North Arrow

Plan 71 Sept 4 2012
DP 12-615424

BLANDFORD ELEVATION NOTES

1. EXTERIOR WALLS: CONCRETE BLOCK, FINISHED WITH STUCCO
2. EXTERIOR DOORS: 6'0" x 2'0" SLIP DOOR WITH GLASS
3. EXTERIOR WINDOWS: 6'0" x 4'0" SLIP WINDOW WITH GLASS
4. EXTERIOR ROOF: 12/12 PITCH, FINISHED WITH SHINGLES
5. EXTERIOR FLOORING: 2" x 6" T&G DECKING
6. EXTERIOR PAINT: WHITE, EXCEPT WHERE NOTED
7. EXTERIOR TRIM: WHITE, EXCEPT WHERE NOTED
8. EXTERIOR LIGHTS: 1' x 4' RECESSED CAN LIGHTS
9. EXTERIOR VENTILATION: 1' x 4' EXHAUST FANS
10. EXTERIOR STAIRS: 8" x 12" T&G STEPS, 3" x 6" STRINGERS
11. EXTERIOR RAILINGS: 4" x 4" POSTS, 1" x 4" BALUSTRADES
12. EXTERIOR SIGNAGE: 2' x 4" SIGN, 1" x 4" LETTERS
13. EXTERIOR LANDSCAPING: 2" x 4" MULCH, 1' x 1' PLANTERS
14. EXTERIOR FENCE: 4' TALL, 2" x 4" POSTS, 1" x 4" RAILS
15. EXTERIOR GATE: 6'0" x 8'0" SLIP GATE WITH GLASS
16. EXTERIOR DRIVEWAY: 12" x 18" CONCRETE SLABS
17. EXTERIOR PAVEMENT: 4" ASPHALT PAVEMENT
18. EXTERIOR CURB: 4" HIGH, 12" WIDE CONCRETE CURB
19. EXTERIOR SIDEWALK: 4" THICK, 12" WIDE CONCRETE SIDEWALK
20. EXTERIOR STAIRS: 8" x 12" T&G STEPS, 3" x 6" STRINGERS
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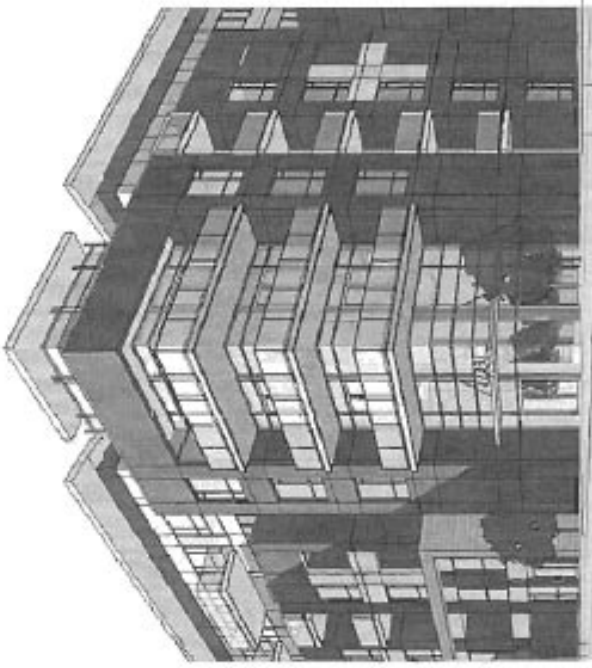


BLANDFORD ELEVATION
DETAIL ELEVATION 4

Project: Blandford Elevation
Date: Sept 4 2012
Drawing: A2-44

Scale: 1/8" = 1'-0"
North Arrow: [Symbol]

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DATE: 09/04/2012	PROJECT: 12-615424
DRAWN BY: J. YAMAMOTO	CHECKED BY: J. YAMAMOTO
SCALE: 1/8" = 1'-0"	PROJECT NO: 12-615424
NO. 001	DATE: 09/04/2012

REVISIONS

NO.	DATE	DESCRIPTION
1	09/04/2012	ISSUED FOR PERMITTING

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PROJECT NO. 12-615424
 SHEET NO. 001
 DATE: 09/04/2012
 DRAWN BY: J. YAMAMOTO
 CHECKED BY: J. YAMAMOTO

DATE: 09/04/2012	PROJECT: 12-615424
DRAWN BY: J. YAMAMOTO	CHECKED BY: J. YAMAMOTO
SCALE: 1/8" = 1'-0"	PROJECT NO: 12-615424
NO. 001	DATE: 09/04/2012

Reference Plan Sept 4 2012
DP 12-615424

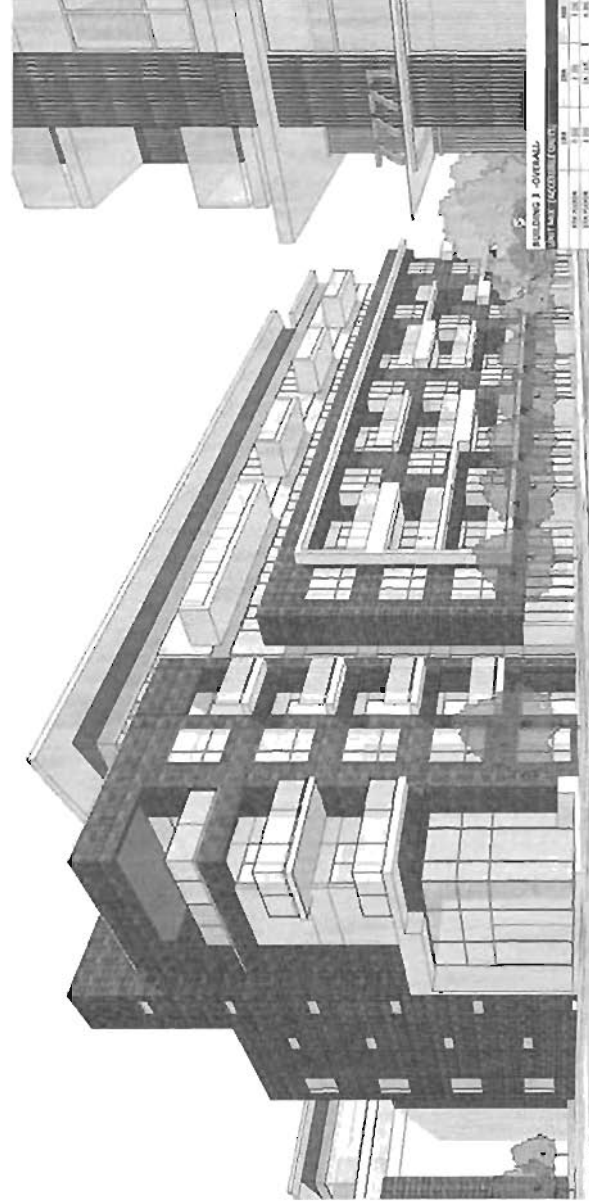
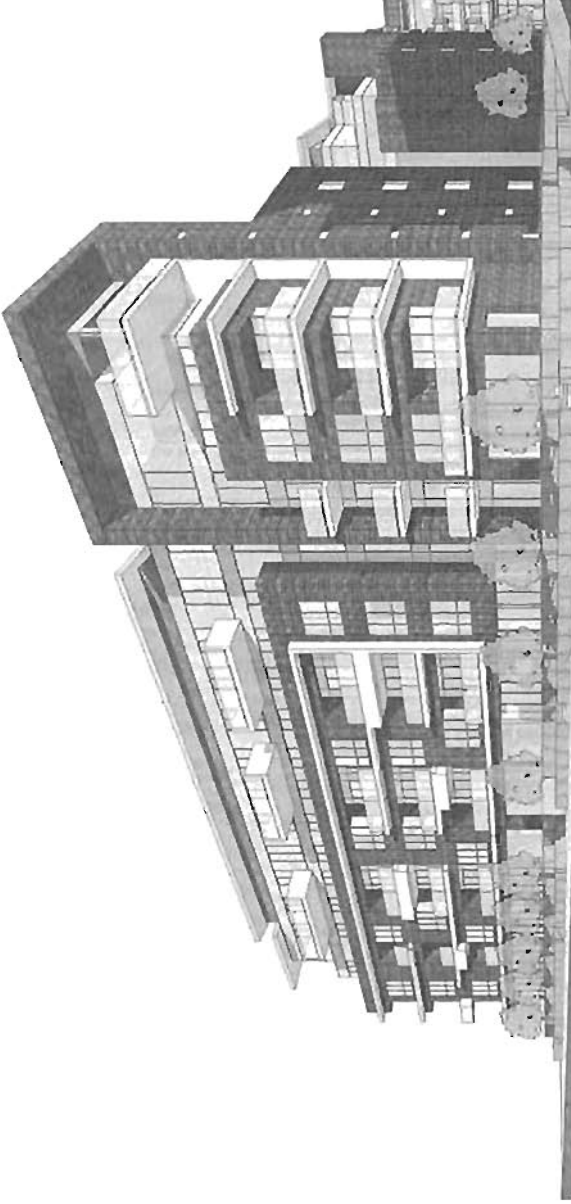
NO.	DATE	DESCRIPTION
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NO.	DATE	DESCRIPTION
1	09/04/2012	ISSUED FOR PERMITTING

NO.	DATE	DESCRIPTION
1	09/04/2012	ISSUED FOR PERMITTING

NO.	DATE	DESCRIPTION
1	09/04/2012	ISSUED FOR PERMITTING

NO.	DATE	DESCRIPTION
1	09/04/2012	ISSUED FOR PERMITTING



RENDERING 3 - OVERALL

UNIT MIX FACILITIES SUMMARY									
UNIT TYPE	NO. OF UNITS	SQ. FT.	NO. OF UNITS	SQ. FT.	NO. OF UNITS	SQ. FT.	NO. OF UNITS	SQ. FT.	NO. OF UNITS
1-BED	10	1,100	10	1,100	10	1,100	10	1,100	10
2-BED	20	2,200	20	2,200	20	2,200	20	2,200	20
3-BED	30	3,300	30	3,300	30	3,300	30	3,300	30
4-BED	40	4,400	40	4,400	40	4,400	40	4,400	40
5-BED	50	5,500	50	5,500	50	5,500	50	5,500	50
TOTAL	110	11,500	110	11,500	110	11,500	110	11,500	110

TOTAL AREA									
AREA TYPE	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)
1-BED	1,100	1,100	1,100	1,100	1,100	1,100	1,100	1,100	1,100
2-BED	2,200	2,200	2,200	2,200	2,200	2,200	2,200	2,200	2,200
3-BED	3,300	3,300	3,300	3,300	3,300	3,300	3,300	3,300	3,300
4-BED	4,400	4,400	4,400	4,400	4,400	4,400	4,400	4,400	4,400
5-BED	5,500	5,500	5,500	5,500	5,500	5,500	5,500	5,500	5,500
TOTAL	11,500	11,500	11,500	11,500	11,500	11,500	11,500	11,500	11,500

RENDERING 4 - OVERALL

Reference Plan Sept 4 2012
DP 12-615424

A1.2.0

DATE: 12/12/12

SCALE: 1/8" = 1'-0"

NO.	DATE	DESCRIPTION
1	12/12/12	ISSUED FOR PERMIT
2	12/12/12	ISSUED FOR PERMIT
3	12/12/12	ISSUED FOR PERMIT
4	12/12/12	ISSUED FOR PERMIT
5	12/12/12	ISSUED FOR PERMIT
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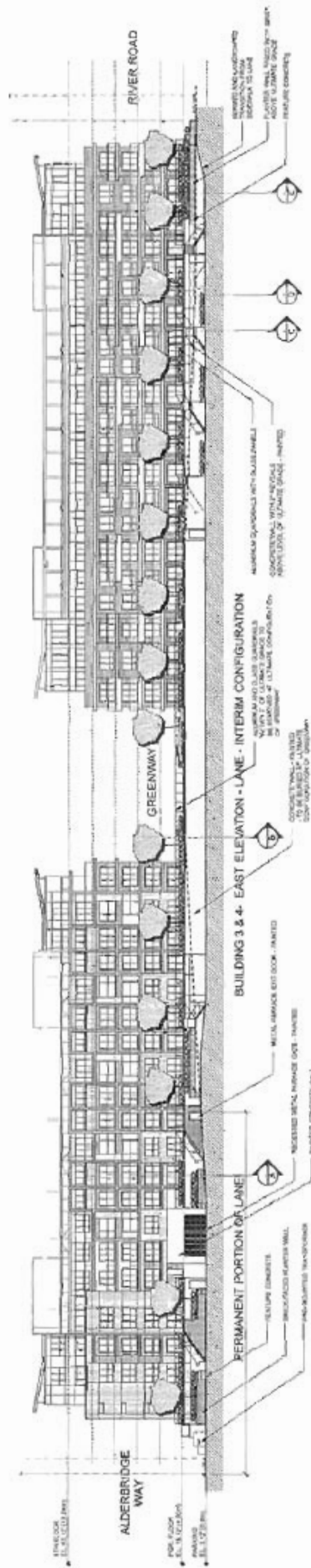
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COVER SHEET



Yamamoto
Architecture Inc.

PROJECT
ARCHITECTURAL DEVELOPMENT
ELEVATIONS
ELEVATION

Reference Plan Sept 4 2012
DP 12-615424

Sheet No.
A04

Scale
1/8" = 1'-0"

NO.	DATE	DESCRIPTION
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2	11/15/11	REVISED FOR COMMENTS
3	12/15/11	REVISED FOR COMMENTS
4	01/15/12	REVISED FOR COMMENTS
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6	03/15/12	REVISED FOR COMMENTS
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97	10/15/19	REVISED FOR COMMENTS
98	11/15/19	REVISED FOR COMMENTS
99	12/15/19	REVISED FOR COMMENTS
100	01/15/20	REVISED FOR COMMENTS

NOTES

1. ALL SURFACES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST EDITIONS OF THE MICHIGAN DEPARTMENT OF TRANSPORTATION (MDOT) STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES.
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RIVA

onni

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ON-SITE LANDSCAPE
PERMEABLE AREAS

L1.9

Reference Plan Sept 4 2012
DP 12-615424

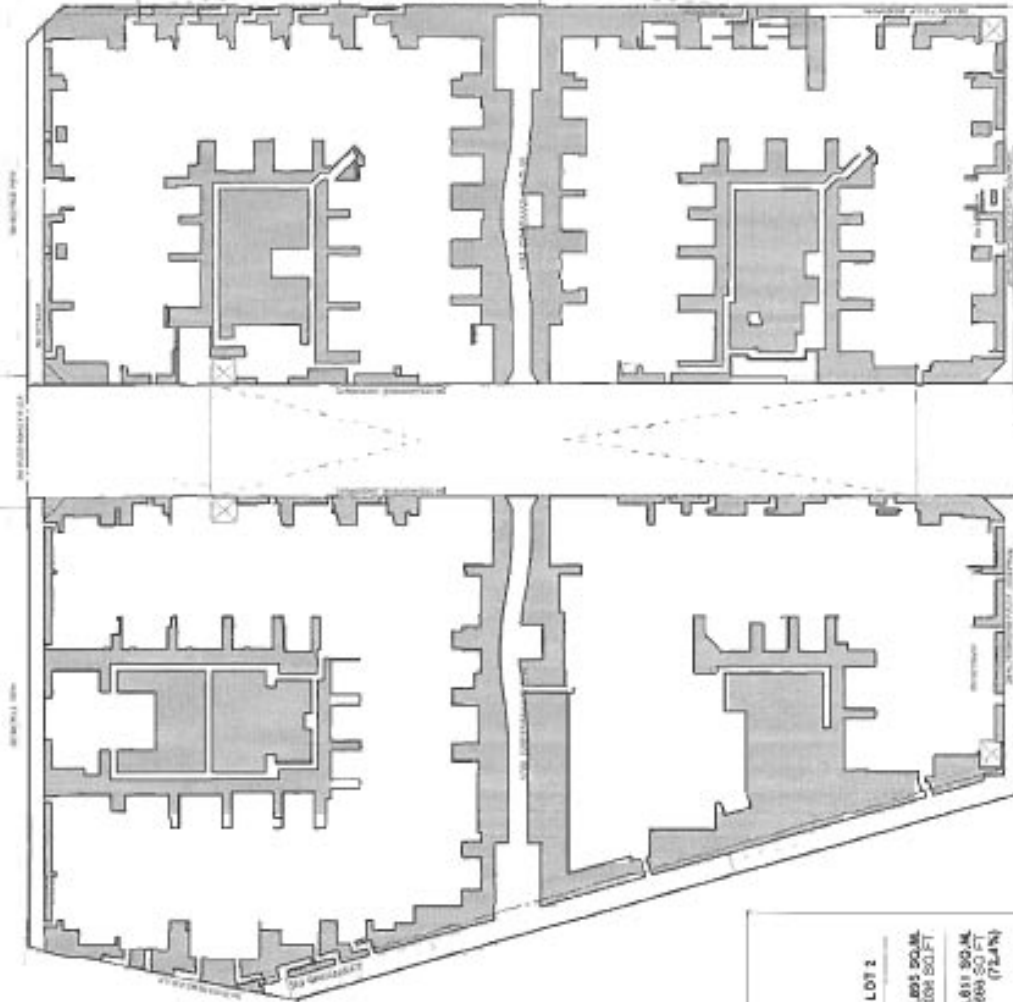
LOT 2

40' X 100' CORNER CUT

LOT 1



40' X 100' CORNER CUT



ON-SITE PERMEABILITY CALCULATIONS

STATISTICS / LEGEND

	LOT 1	LOT 2
TOTAL AREA	13,300 SQ. FT. 126,000 SQ. FT.	11,805 SQ. FT. 126,000 SQ. FT.
HARD SURFACE AREA (INCLUDES ALL PAVED SURFACES AND ROOF SURFACES)	8,787 SQ. FT. 65,000 SQ. FT. (73.4%)	8,611 SQ. FT. 65,000 SQ. FT. (73.4%)
SOFT SURFACE AREA (INCLUDES ALL SOFT LANDSCAPE AREAS AND PERMEABLE PAVED SURFACES)	4,513 SQ. FT. 61,000 SQ. FT. (26.6%)	3,194 SQ. FT. 61,000 SQ. FT. (26.6%)