



To: Development Permit Panel

Date: August 6, 2019

From: Wayne Craig
Director of Development

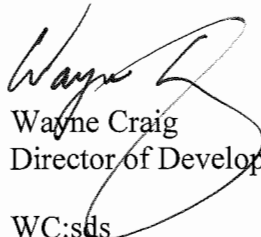
File: DP 18-829241

Re: Application by 1076694 BC Ltd. for a Development Permit at 10451, 10471 & 10491 No. 2 Road

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of 12 townhouse units at 10451, 10471 & 10491 No. 2 Road on a site zoned "Low Density Townhouses (RTL4)"; and
2. Vary the provisions of Richmond Zoning Bylaw 8500 to:
 - a) Reduce the minimum front yard setback from 6.0 m to 4.5 m; and
 - b) Allow one small car parking stall in each of the side-by-side garages (12 small car stalls in total).



Wayne Craig
Director of Development

WC:sds
Att. 4

Staff Report

Origin

1076694 BC Ltd. has applied to the City of Richmond for permission to develop 12 townhouse units at 10451, 10471 & 10491 No. 2 Road. The subject site is being rezoned from “Single Detached (RS1/E)” zone to “Low Density Townhouses (RTL4)” zone under Bylaw 9872 (RZ 17-778834), which received third reading following the Public Hearing on June 18, 2018. The subject site previously contained three single-family dwellings, which have been demolished.

A Servicing Agreement (SA 18-824442) is also associated with the proposal, which is a condition of Rezoning approval and includes, but is not limited to, the following improvements:

- new concrete sidewalk and grassed and treed boulevard along No. 2 Road frontage; and
- a new concrete bus pad at the existing southbound bus stop on No. 2 Road.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

Development surrounding the subject site is as follows:

To the North: Single-family dwelling on a lot zoned “Single Detached (RS1/E)” fronting No. 2 Road.

To the East: Across No. 2 Road, 133-unit townhouse development on a lot zoned “Town Housing (ZT72) – London/Steveston (No. 2 Road)”, a 9 m (30 ft.) wide public greenway connecting No. 2 Road to Steveston-London Park/School, and single-family dwellings on lots zoned “Single Detached (RS1/E)” fronting Dylan Place.

To the South: Single-family dwellings on lots zoned “Single Detached (RS1/E)” fronting No. 2 Road.

To the West: Single-family dwellings on lots zoned “Single Detached (RS1/B)” and a duplex zoned “Two-Unit Dwellings (RD1)” fronting Kozier Drive.

Rezoning and Public Hearing Results

The Public Hearing for the rezoning of this site was held on June 18, 2018. At the Public Hearing, the following concerns were expressed by the property owner of the neighbouring property at 10526 Kozier Drive:

- the existing retaining wall in place between the neighbouring property and 10491 No. 2 Road is in poor condition;
- construction at 10491 No. 2 Road by previous owners caused damage to the property owner home’s foundation;

- installation of perimeter fencing during demolition and construction; and
- unsightly premises with regard to tall grass on the subject site's lawn.

Staff worked with the applicant to address these issues in the following ways:

- the developer met with the property owner and agreed to replace the existing retaining wall through the development process at the Developer's cost. The City's Engineering department reviewed the proposed new retaining wall (shown on the plans) and has no concerns;
- the developer has provided a construction impact and management plan to address concerns of further foundation damage, including a pre and post construction assessment, and involvement of a geotechnical consultant, which was shared with the neighbours;
- the developer has noted perimeter fencing will be maintained on-site during all phases of construction; and
- the City's Community Bylaws Department initiated an unsightly premises investigation based on the neighbour's concerns, which the developer addressed by completing yard maintenance, contracted on a bi-weekly basis, to maintain the premises until demolition.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan (OCP) and is generally in compliance with the "Low Density Townhouse (RTL4)" zone except for the zoning variances noted below.

Zoning Compliance/Variances (staff comments in **bold**)

The applicant requests to vary the provisions of Richmond Zoning Bylaw 8500 to:

1) Reduce the minimum front yard setback from 6.0 m to 4.5 m;

- ***The proposed variance is consistent with the Arterial Road Guidelines for Townhouses in the OCP and allows for a 6.0 m rear yard setback to both the ground and second floors of the rear units adjacent to single-family development. This provides an improved rear yard interface with the existing single-family dwellings to the west and enhances solar access to the rear yards.***
- ***The proposed front yard setback ranges from 4.5 m to 6.0 m in order to reflect the scale and articulation found in single-family areas and avoid long, continuous and unarticulated building frontages along the street. Approximately 25% of the front building face is setback 4.5 m from the front property line, with the remaining building face proposed to be setback 4.9 m, 5.2 m and 6.0 m. The varied front yard setback also provides an adequate transition to the existing single-family dwellings to the north and south.***
- ***The location of the existing curb along No. 2 Road will not change as there is no road widening required. New grassed/treed boulevard and concrete sidewalk will be provided between the existing curb and the front property line. The shortest distance***

between the proposed building face and the back of curb on No. 2 Road would be approximately 7.4 m.

- *The proposed location of the buildings along No. 2 Road frontage accommodates the proposed shared outdoor amenity space, which is located in the northeast corner in order to maximize tree protection opportunities.*
 - *The variance was identified at rezoning and no concerns were raised.*
- 2) Allow one small car parking stall in each of the side-by-side garages (12 small car stalls in total).
- *Staff are supportive of the proposed variance as it enables the required resident vehicle parking spaces to be provided within the garages of each unit, in a side-by-side arrangement. The Zoning Bylaw allows a maximum 50% of the spaces provided to be small vehicle parking stalls for on-site parking areas which contain 31 or more spaces.*
 - *The variance was identified at rezoning and no concerns were raised.*

Advisory Design Panel Comments

The Advisory Design Panel (ADP) reviewed the proposal on January 8, 2019. A copy of the relevant excerpt from the Advisory Design Panel Minutes from January 8, 2019 is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '***bold italics***'.

Analysis

Conditions of Adjacency

- As per the Arterial Road Land Use Policy, the building height of the rear units is 9 m (two storeys) in response to the single-family rear yard adjacency to the west. In addition, a 6.0 m rear yard setback has been provided to both the ground and second floors of the rear units to provide an improved interface and increased sun exposure. Duplex units in three townhouse clusters are proposed along this interface, in order to break up the massing.
- The subject site is also bordered by single-family development to the north and south, which is identified for townhouse development in the Arterial Road Land Use Policy. The location of the shared outdoor amenity area in the northeast corner of the site, along with the retention of a significant hedge along the north property line, provides a buffer between the proposed townhouse units and the adjacent single-family development to the north. The three-storey townhouse units along No. 2 Road are reduced to two storeys within 7.5 m of the side property lines to provide an adequate transition to the north and south.
- The applicant has provided a shadow study to demonstrate how the proposed height and setbacks are articulated to address potential shadowing over adjacent properties to the north, south and west.
- A statutory right-of-way (SRW) allowing access to/from the adjacent future development sites through the subject site (over the entry driveway and internal drive aisle) has been secured at rezoning.
- Retaining walls will be provided along the west property line in order to provide usable yard space for the proposed units (ranging from 0.29 m to 0.87 m). A lower 1.2 m high timber

fence will be provided on top of the retaining wall to reduce the apparent height of the grade change, but also maintain privacy (cross-sections are provided in Plan #3.G).

- Perimeter drainage will be required as part of the Building Permit to ensure storm water is managed and addressed through the development and will not impact neighbouring properties.

Urban Design and Site Planning

- Vehicular access to the proposed development is to be from No. 2 Road through a new driveway in the middle of the site, with future connections to the neighbouring properties to the north and south, secured by a SRW.
- The layout of the townhouse buildings is oriented around a north-south internal maneuvering drive-aisle, providing access to the unit garages.
- Units along No. 2 Road will have direct pedestrian access from the street and the units along the rear will have access from the internal drive aisle.
- All townhouse units will have two vehicle parking spaces in side-by-side arrangement. A variance is requested for one small vehicle parking space in each garage, for a total of 12 small spaces. No tandem parking spaces are proposed.
- A total of three visitor parking spaces will be provided, including an accessible parking space. The number of parking spaces proposed is in compliance with the minimum Zoning Bylaw requirement.
- Both internal and external bicycle parking spaces have been incorporated into the proposal and are in compliance with Zoning Bylaw requirements.
- The shared outdoor amenity area is located in the northeast corner of the site, along No. 2 Road, in order to maximize tree retention opportunities. This location also offers casual surveillance from the street and adjacent units. The size of the outdoor amenity area is consistent with the OCP requirement (6.0 m² per unit).
- Cash-in-lieu of indoor amenity space was secured through rezoning (\$12,000) consistent with OCP policy.
- The proposal will utilize door-to-door pickup and has accommodated space for garbage and recycling containers within the individual garages and in front of the units without obstructing the drive-aisle. The City's Environmental Programs department reviewed the arrangement and has no concerns. The mail kiosk has been incorporated into the design of the buildings to minimize visual impact.

Architectural Form and Character

- The buildings were designed as a modern interpretation of the French Provincial style.
- Hipped roofs with steep pitches and bell roof detailing, balanced symmetrical proportions, tall second storey windows and stucco finish are distinctive characteristics of this architectural style.
- The street fronting units will have individual canopies. A pedestrian scale is generally achieved along the public streets and internal drive-aisle through the inclusion of variation in building form, projections, recesses, materials/colours and landscape features.
- Neutral tones and materials provide a contrast to the dark roof. Proposed building materials include predominantly stucco and stucco detailing and moulding.
- Juliette balconies, ornate trims, round dormers and oval windows are also provided to enhance the character.

Landscape Design and Open Space Design

- Tree preservation was reviewed at rezoning stage where five on-site trees were identified for removal. Based on a 2:1 tree replacement ratio stated in the OCP, 10 replacement trees on-site are required. The applicant is proposing to plant 16 replacement trees as per the size requirements based on the City's Tree Protection Bylaw.
- Four on-site trees (tag# 25, 26, 28, 30) located on the subject property are proposed to be protected. Two trees (tag# 25 & 26) are located in the shared outdoor amenity area in the northeast corner and two trees (tag# 28 & 30) are located in southwest corner in the rear yard. The shared outdoor amenity area was located in the northeast corner to maximize tree retention opportunities. A contract with a Certified Arborist and a tree survival security of \$35,000 were secured through rezoning for the retention of these trees.
- The shared outdoor amenity area is designed at multiple grades to ensure the retention of the significant trees, while still being accessible. A circular wood bench is proposed around one of the significant trees to be retained, additional seating in the upper area and significant planting are provided to activate the space and provide opportunities for social interaction. The two areas are linked by an accessible ramp along the north side of the area and stepping stones down into the tree protection area.
- The proposal provides a pedestrian-oriented streetscape fronting No. 2 Road with a landscaped edge treatment including a new tree in each front yard, lawn and patio areas, low transparent fencing, and a gate to each street fronting unit.
- All units along the rear will have a private yard with a patio and a lawn area. Various hedges, shrubs and ground covers have been selected to ensure the landscape treatment remains interesting throughout the year.
- Pockets of landscaping will be provided along the drive-aisle between entry doors/garages to soften the internal streetscape.
- An irrigation system will be provided for the proposed landscaping on-site.
- A children's play area is proposed in the shared outdoor amenity area, and play equipment has been chosen to provide different play opportunities (i.e. climbing, social, imagination, balance, motor skills) that can be used by different age groups and for multiple purposes. Seating is provided nearby for caregivers.
- The drive-aisle will be treated with pavers along with a concrete pathway for pedestrians. The surface parking spaces, patios and entry pathways will be treated with permeable pavers for better water infiltration and variety in paving surfaces. Coloured concrete circles are provided to indicate the transition from the drive-aisle to the shared outdoor amenity area.
- In order to ensure the proposed landscaping works are completed, the applicant is required to provide a landscape security of \$129,494.75 prior to issuance of the Development Permit.

Crime Prevention Through Environmental Design

- Windows are located to increase the visual presence and surveillance along No. 2 Road, the amenity area, as well as the internal drive aisle. All entrances are visible and overlooked by pedestrians or by neighbour's windows.
- Site lighting and clear site lines provide unobstructed views of surrounding area.
- Low permeable fencing and low landscaping are provided to maximize clear unobstructed views and casual surveillance opportunities.
- Landscaping and fencing is provided along the street edge to distinguish the public realm and private space.

- Good neighbour fencing is provided throughout the site and is 1.2 m high to encourage interaction between neighbours and foster a sense of community. The proposed fencing was discussed with the existing neighbours and had no concerns.
- Security lighting will be installed including wall mounted lighting for the drive-aisle and lighting at entries to provide adequate outdoor security illumination.

Sustainability

- As the application was submitted prior to the introduction of the Step Code requirement, the applicant has provided a report produced by a Certified Energy Advisor to confirm that the proposed townhouse units will be designed to achieve an EnerGuide rating of 82. The report prepared by the Energy Advisor is on file and will be utilized through the Building Permit review process to ensure these measures are incorporated in the Building Permit drawings.
- An air source heat pump system will be used for this development. The applicant has provided an acoustical report from an acoustical consultant indicating the proposed units, with consideration of the proposed location, will comply with the City's Noise Bylaw.
- The architect advised that the following design/features are incorporated into the proposal:
 - Energy star windows with low E-glazing to alleviate heating and cooling energy consumption;
 - Climate-based automatic irrigation controls to minimize use of water on site;
 - The use of low-flow water fixtures, low-energy lighting and high energy efficiency appliances;
 - Durable and long-lasting materials that can reduce building maintenance;
 - Permeable ground cover and planting to absorb rainwater runoff and reduce load on municipal sewers; and
 - Interior paint with low VOC content.

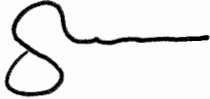
Accessible Housing

- The proposed development includes one convertible unit that is designed with the potential to be easily renovated to accommodate a future resident in a wheelchair. The potential conversion of these units will require installation of a chair lift in the staircase of Unit 11E.
- All of the proposed units incorporate aging in place features to accommodate mobility constraints associated with aging. These features include:
 - stairwell hand rails;
 - lever-type handles for plumbing fixtures and door handles; and
 - solid blocking in washroom walls to facilitate future grab bar installation beside toilets, bathtubs and showers.

Conclusions

As the proposed development would meet applicable policies and Development Permit Guidelines, staff recommend that the Development Permit be endorsed, and issuance by Council be recommended.

The list of Development Permit Considerations is included in Attachment 3, which has been agreed to by the applicant (signed concurrence on file).

A handwritten signature in black ink, consisting of a stylized 'S' followed by a horizontal line.

Steven De Sousa
Planner 1

SDS:cas

Attachment 1: Development Application Data Sheet

Attachment 2: Excerpt from the Meeting Minutes of the ADP (January 8, 2019)

Attachment 3: Development Permit Considerations



DP 18-829241

Attachment 1

Address: 10451, 10471 & 10491 No. 2 Road

Applicant: 1076694 BC Ltd.

Owner: 1076694 BC Ltd.

Planning Area(s): Steveston

Floor Area Gross: 2,376.5 m² (25,580 ft²) Floor Area Net: 1,701.3 m² (18,312 ft²)

	Existing	Proposed
Site Area:	2,833.7 m ² (30,502 ft ²)	No change
Land Uses:	Single-family residential	Multiple-family residential
OCP Designation:	Neighbourhood Residential (NRES)	No change
Zoning:	Single Detached (RS1/E)	Low Density Townhouses (RTL4)
Number of Units:	3	12

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Max. 0.6	0.6	None permitted
Lot Coverage – Buildings:	Max. 40%	40%	None
Lot Coverage – Non-porous:	Max. 65%	65%	None
Lot Coverage – Live Landscaping:	Min. 25%	26%	None
Setback – Front Yard:	Min. 6.0 m	4.5 m	Variance requested
Setback – Side Yard:	Min. 3.0 m	3.0 m	None
Setback – Rear Yard:	Min. 3.0 m	6.0 m	None
Height:	Max. 12.0 m (3 storeys)	Front buildings: 12.0 m (3 storeys) Rear buildings: 9.0 m (2 storeys)	None
Lot Width:	Min. 50.0 m	65.3 m	None
Lot Depth:	Min. 35.0 m	42.3 m	None
Lot Size:	N/A	2,833.7 m ²	None
Off-street Parking Spaces – Regular (R) / Visitor (V):	2 (R) and 0.2 (V) per unit	2 (R) and 0.2 (V)	None

Off-street Parking Spaces – Accessible:	Min 2% when three or more visitor stalls required = 1	1	None
Total off-street Spaces:	24 (R) and 3 (V)	24 (R) and 3 (V)	None
Tandem Parking Spaces:	Permitted – Max 50% of required spaces	0	None
Small Car Parking Spaces:	None when fewer than 31 spaces are provided on-site	12	Variance requested
Bicycle Parking Spaces – Class 1:	1.25 per dwelling unit	1.3 per dwelling unit	None
Bicycle Parking Spaces – Class 2:	0.2 per dwelling unit	0.2 per dwelling unit	None
Bicycle Parking Spaces – Total:	15 Class 1 and 3 Class 2	16 Class 1 and 3 Class 2	None
Amenity Space – Indoor:	Min. 50 m ² or cash-in-lieu	Cash-in-lieu	None
Amenity Space – Outdoor:	Min. 6 m ² per unit = 72 m ²	77 m ²	None

Excerpt from the Minutes from
The Design Panel Meeting

Tuesday, January 8, 2019 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall

DP 18-829241 – 12-UNIT TOWNHOUSE DEVELOPMENT

ARCHITECT: Fougere Architecture Inc.
LANDSCAPE ARCHITECT: Van der Zalm and Associates
PROPERTY LOCATION: 10451, 10471, and 10491 No. 2 Road

Applicant's Presentation

Wayne Fougere, Fougere Architecture Ltd., and Stephen Heller, Van der Zalm and Associates, presented the project and answered queries from the Panel on behalf of the applicant.

Panel Discussion

Comments from Panel members were as follows:

- investigate opportunities for introducing a gap at the centre of the rear buildings and having two-storey buildings flank the gap to extend the visibility at the entry driveway through the site;

The OCP does not support 3-plexes along the rear yard. Introducing a gap would only be possible at the expense of a unit which is not acceptable.

Also, one of the rear neighbours previously expressed concern about being exposed to No. 2 Road during construction (when buildings are demolished), we therefore believe our neighbours appreciate a buffer between them and the Arterial Road.

- consider increasing the gap between the two-storey rear buildings;
We provide 2.8 m in between buildings (at the tightest part) and increase the gap to 3.4m in between the entries columns. Furthermore the setback entries visually open up the space between buildings. We believe the gap is adequate and in line with what is typically provided in townhouse developments.

- look at the upper storeys on the three-storey buildings which step back on No. 2 Road but overhang the internal driveway at the back; review the scale to avoid becoming overbearing;

If we were to reduce the overhang on the internal drive, we would need to increase the 3rd floor massing on No. 2 Road. We believe it's better to maintain the dormer look on the front as the overhang at the drive aisle is minimal. 3rd storey is already reduced in size- it is less than 1/3 of the total building width (total width: 21.9 m; width of 3rd storey: 6.3 m).

- consider introducing a chimney element on the roof of the centre building at the rear to help focus on this character expression in the front buildings;

The maximum height limit of 9.0m prevents adding chimneys to the rear buildings.

- investigate opportunities for increasing the separation between the rear duplex buildings (especially Building 4) and the three-storey buildings fronting No. 2 Road through articulation of the rear duplex buildings along the internal drive aisle;

The rear buildings are articulated along the internal drive aisle by stepping back at the entries. Any more articulation (setbacks) would result in a decrease of habitable area and an increase of drive aisle area which is, in our opinion, not a good trade off.

- appreciate the limited materials and colour palette, helps pull the project concept together;

Noted.

- consider slightly varying the colours of the individual unit doors to provide more identity to the residential units;

The previous comment noted that the limited colour palette is beneficial to the design. We agree with this sentiment and don't think that adding different colours to doors would enhance the look.

- the project is straightforward;

Noted.

- understand that the City's Arterial Road Guidelines for Townhouses provide guidance regarding arrangement and scale of buildings, in particular the rear units of arterial road developments; however, would appreciate if an exception could be made for the subject development to improve its proposed site lay-out;

It's our opinion that the site layout is conforming to regulations and acceptable to our neighbours and the City.

- understand the site constraints and appreciate the applicant's intent to save the existing trees, however the proposed site for the outdoor amenity area is not appropriate as it is (i) disconnected from other buildings in the development due to its corner location, (ii) located in the part of the property that receives the least amount of sun exposure, and (iii) limited in terms of accessibility and size of the flat area;

i) Due to the small size of the development the maximum distance from any unit to the open space is very limited. Also, the location at the end of the internal road could be seen as a bonus since it removes the play area from any vehicular traffic.

ii) The current location was agreed upon by both the City and the developer in order to save the trees. In the Arterial Road Design Guidelines the only other acceptable location would be in between the rear buildings. This location would have the same issues in terms of sun exposure.

iii) See response to the following comments for a description of the improvements to the amenity area.

- Consider utilizing the space south of the existing trees area in the outdoor amenity area for access to minimize shadowing from Building 1 (Unit 3D) and to enhance the accessibility of the outdoor amenity area from the internal drive aisle;

Relocating the access point wouldn't have an impact on the shadowing. The access at the end of the road is more practical as the road slopes down, reducing the height difference between road and existing grade. An access south of the trees would require a very long ramp, which would conflict with the tree protection area.

- consider eliminating the proposed play equipment in the outdoor amenity area and take advantage of the topography of the area to design the whole amenity area as a play space;

Previous play house has been eliminated as requested. A new playhouse has been added as requested. Topography proposes terraces, as with the previous concept. Refer to Section 3, sheet 3F. Amenity area re-designed as play space with seating.

- consider a more accessible surface on the outdoor amenity area; also consider eliminating the concrete stepping stones on the outdoor amenity area as they appear discouraging and not "visually accessible" from an accessibility point of view;

Concrete stepping stones have been removed. Upper play area is accessible. The transition from the lane to the mulch area will be a disappearing concrete edge. Wheelchairs could still roll into the lower area and move around with assistance.

- the outdoor amenity space is limited and the play area appears stark due to the provision of just a single play element; the applicant could introduce something more creative;

Formal play equipment now accompanied by natural play elements.

- review the design of the outdoor amenity area to make it more visually appealing and stimulating for children;

The revised circular design is more visually appealing than the previous design.

- concerned about the proposed location of the exterior air handling units between the rear buildings due to potential noise issues; consider relocating the outdoor units to the rear yards;

A review by our Acoustical Engineer determined that the noise level emitted by the HVAC units is within the permitted sound level of the Noise Regulation By-law 8856.

- appreciate the proposed sustainability features of the project; the project is on the right track from a sustainability point of view;

Noted.

- the checklist for achieving EnerGuide 82 rating was not included in the package provided to the Panel;

Noted. It was provided at DP Resubmission on November 27, 2018.

- notice the 4/12 slope for the roof of the rear units; the applicant is reminded of the requirement to provide adequate ventilation for the attic space;

Noted.

- consider relocating the outdoor amenity area to the south end of the site;

The current location was agreed upon by both the City and the developer in order to save the trees.

- consider eliminating the proposed play equipment and redesigning the play area using natural materials, e.g. logs and boulders, to provide more interest and play opportunities for children; would also be more economical to build;

The play area has been re-designed to include natural materials. The formal play structure remains.

- consider installing a deck adjacent to the retained trees on the rear yards of rear units to increase the size of the rear yards;

The wood deck in the rear yards will not be reinstated due to the neighbour's concerns.

- concerned on the cross access easement; buyers should be informed on this arrangement;

While cross access easement is a common occurrence on No. 2 Road projects, future buyers will be informed about this.

- appreciate the project; the applicant has done well considering the site constraints;

Noted.

- the rear units appear inferior to the front units in terms of the level of development, materiality and detailing; the steep roof slopes for the front units are successful; however, the proposed roof forms for the rear units are not.

The form and slope of these roofs were designed in order to minimize the impact on the neighbours to the back. Other than that, the elevations were designed with the same materials and the same amount of detail as the front units (Juliette balconies, ornate trims, dormers).

- difficult to tell that the buildings consist of 12 individual units due to their design, e.g. window placements, access points, doorways, and detailing; design development is needed to provide more identity to individual units without being too prescriptive;

The clearly visible front doors and address sign which will be placed on the fence column should make it clear that there are multiple units within one building.

- not clear whether the large windows at the west elevation of rear buildings are for individual units or shared by two units; review the size of these windows as they are more appropriate for large houses;

The large windows are located in the living room which has a 5m room height. It is true that these types of large openings are commonly only found in single family houses. However, it was our and our client's intent to create townhomes that feel larger and more luxurious than the typical townhouse and providing these types of features is part of this design target.

- appreciate the limited materials and colour palette; however, concerned about the long-term maintenance of Exterior Insulation and Finish Systems (EIFS) materials considering the City's climate;

EIFS is only used for details like the quoins or window frames. The majority is regular stucco.

- not clear about the types of materials for gates, fencing and pilasters; note that the rear units use inexpensive wood; review whether the proposed materials tie together;

Refer to detail drawings for complete materials palette. Higher quality aluminum fencing wraps the corners of the development and extends to the middle of Building 5 on the South and terminates at the existing retaining wall on the North. The south condition is typical for a side yard residential setting. Fencing along the North PL may be revisited if/when the neighboring property is redeveloped and the proposed lane is extended.

- the whole internal drive aisle could be designed both as a “people” space and for vehicular circulation; could be used as a play space for children and social/gathering area for residents; some soft landscape suggested;

the design of the internal road was inspired by the woonerf principle (one common space rather than dedicated vehicular and pedestrian lanes) and intended exactly as described above- as multifunctional space, not only as vehicular circulation.

The north laneway ending was revised to create a playful environment that could be used by tenants as a social gathering space.

- appreciate the applicant contributing to the City’s Public Art Fund; the applicant is encouraged to incorporate public art in the project as it will enhance the public realm and benefit the development, residents and visitors;

Due to the small size of this project and limited space for an art installation, we feel a contribution to the Art Fund will be more useful.

- support the proposed paving treatment for the internal drive aisle and the defined pedestrian pathway as they will enhance the pedestrian experience throughout the development;

Noted.

- consider slightly varying the heights of canopies above the unit doors to provide variation between the entrances to individual units;

We tried some variations on the design of the entry roofs. Unfortunately these changes were not very successful as it only added complication to the roof construction without enhancing the design.

- garbage and recycling bins should not be permanently located along the internal drive aisle; and

Garbage and recycling will be stored in each unit’s garage. Locations shown on drive aisle are only temporary locations on pick-up days.

- consider making the whole outdoor amenity area one level and using natural materials to enhance its usability and provide more play opportunities for children.

This conflicts with a previous comment to 'take advantage of the topography'. We have not made the amenity area one level, as this makes the eastern portion of the amenity area ~ 3 feet lower than the sidewalk and the neighbouring site. Natural play elements have been added.

Panel Decision

It was moved and seconded

That DP 18-829241 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Advisory Design Panel.

CARRIED



Development Permit Considerations

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 10451, 10471 & 10491 No. 2 Road

File No.: DP 18-829241

Prior to forwarding the application to Council for approval, the developer is required to complete the following:

1. Final Adoption of Zoning Amendment Bylaw 9872.
2. Receipt of a Letter-of-Credit for landscaping in the amount of \$129,494.75. The letter-of-credit will not be returned until the Letter of Assurance, confirming the landscaping is installed as per the Development Permit, prepared by the Landscape Architect, is reviewed by staff.

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin TREE-03, and must remain in place until construction and landscaping on-site is completed.
2. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.
3. Incorporation of energy efficiency, CPTED, sustainability and accessibility measures in Building Permit (BP) plans as determined via the Rezoning and/or Development Permit processes.
4. If applicable, payment of latecomer agreement charges, plus applicable interest associated with eligible latecomer works.
5. If applicable, obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.

All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.

The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.

- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

Initial: _____

- Applicants for all City Permits are required to comply at all times with the conditions of the Provincial *Wildlife Act* and Federal *Migratory Birds Convention Act*, which contain prohibitions on the removal or disturbance of both birds and their nests. Issuance of Municipal permits does not give an individual authority to contravene these legislations. The City of Richmond recommends that where significant trees or vegetation exists on site, the services of a Qualified Environmental Professional (QEP) be secured to perform a survey and ensure that development activities are in compliance with all relevant legislation.

[Signed copy on file]

Signed

Date



No. DP 18-829241

To the Holder: 1076694 BC Ltd.
Property Address: 10451, 10471 & 10491 No. 2 Road
Address: c/o Allen Zu
155 – 3757 Jacombs Road
Richmond, BC V6V 2R3

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning Bylaw 8500" is hereby varied to:
 - a) Reduce the minimum front yard setback from 6.0 m to 4.5 m; and
 - b) Allow one small car parking stall in each of the side-by-side garages (12 small car stalls in total).
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 to #3 attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$129,494.75 to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

**Development Permit
No. DP 18-829241**

To the Holder: 1076694 BC Ltd.
Property Address: 10451, 10471 & 10491 No. 2 Road
Address: c/o Allen Zu
155 – 3757 Jacombs Road
Richmond, BC V6V 2R3

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.
This Permit is not a Building Permit.

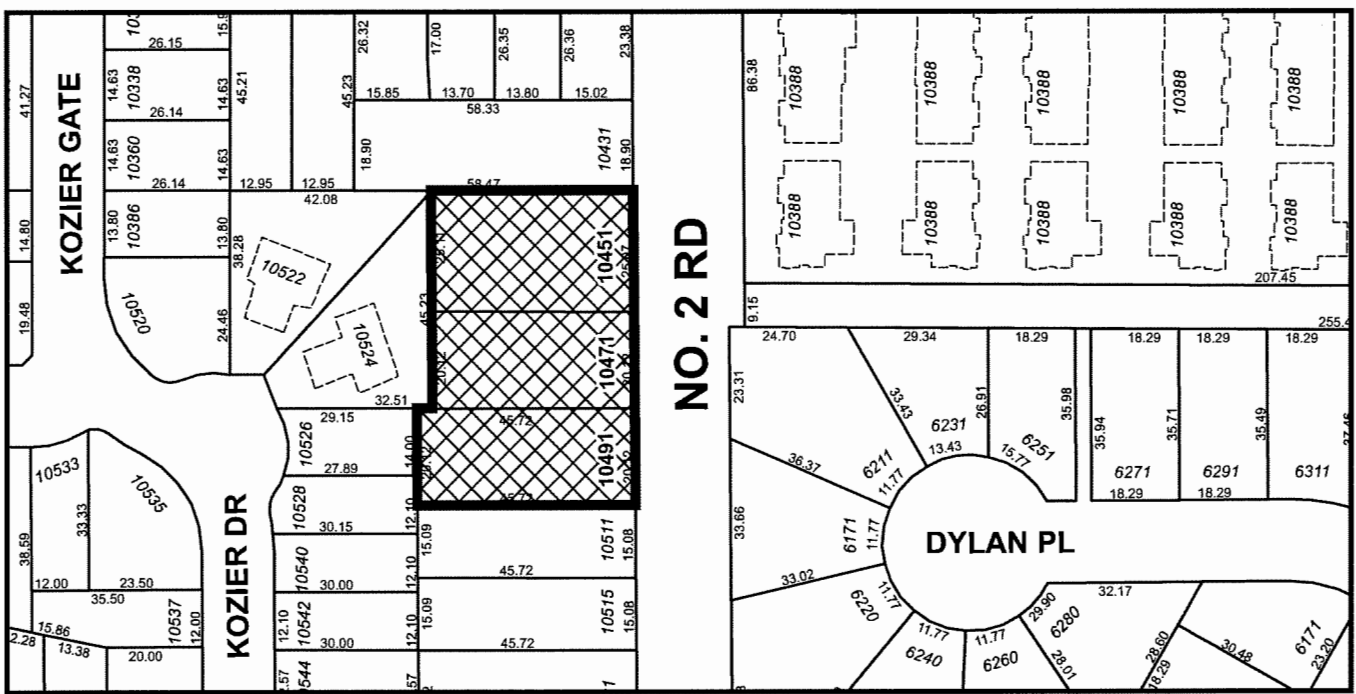
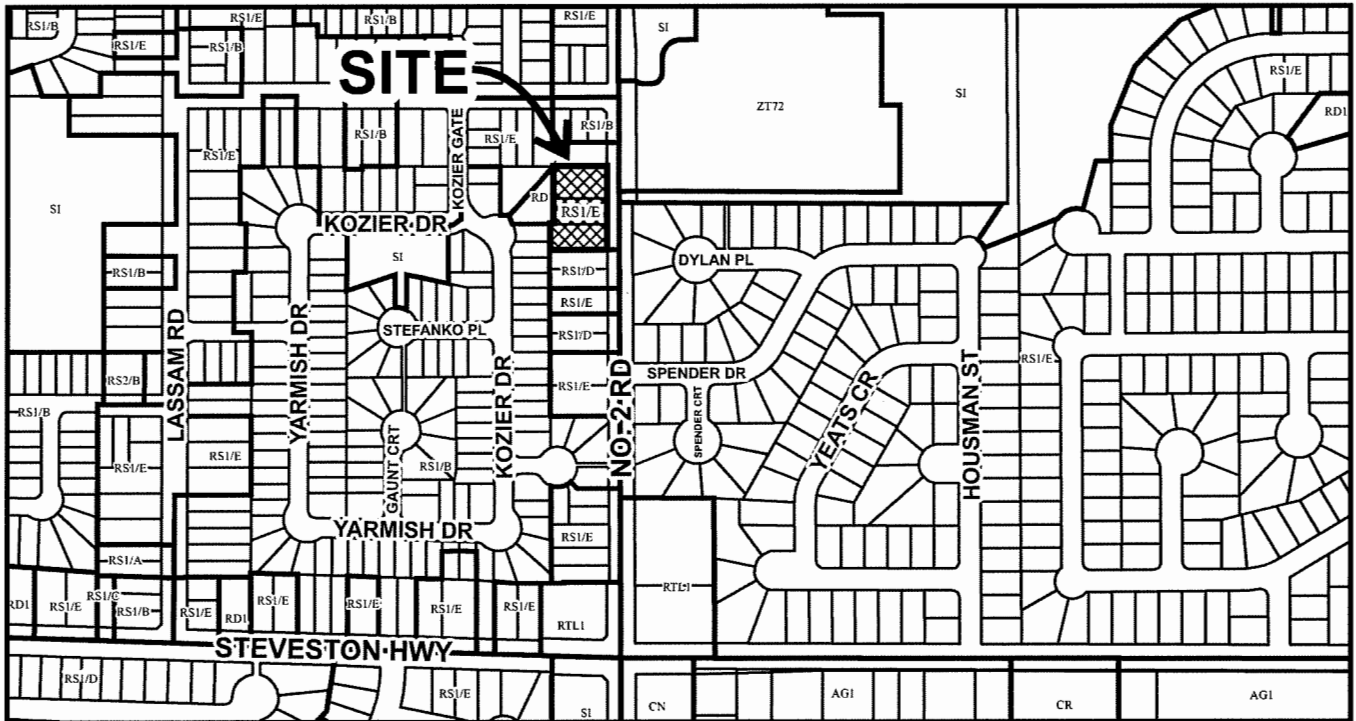
AUTHORIZING RESOLUTION NO. ISSUED BY THE COUNCIL THE
DAY OF , .

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond

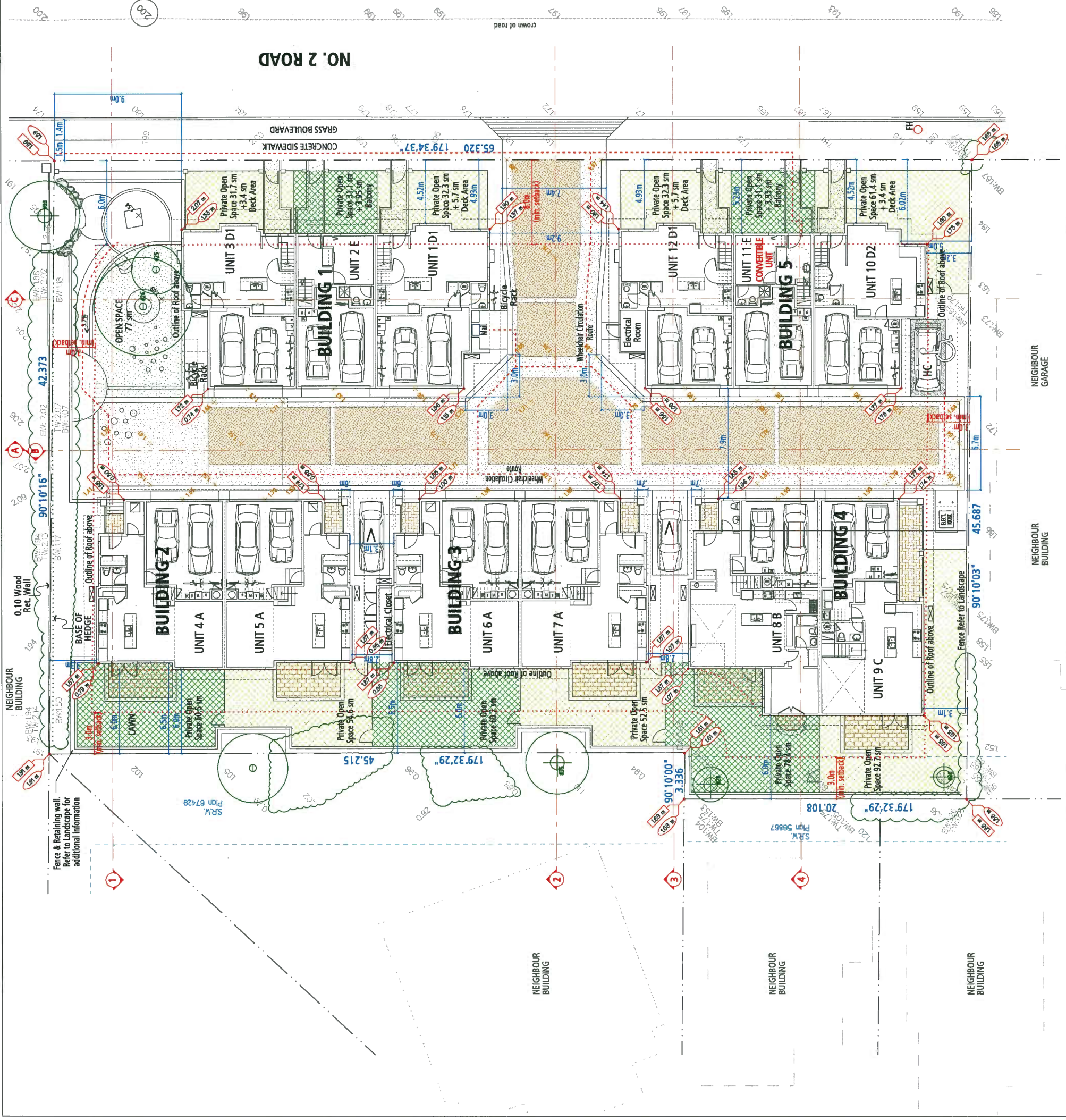


DP 18-829241 SCHEDULE "A"

Original Date: 08/21/18

Revision Date:

Note: Dimensions are in METRES



ZONING VARIANCE
 12 Small Cars
 4.5m Front Yard Setback

- 0.00 m PROPOSED SITE GRADE
- 0.00 m EXISTING SITE GRADE
- PEDESTRIAN WALKWAY
- OPEN SPACE (PUBLIC)
- OPEN SPACE (PRIVATE)
- HEAT PUMP (UNIT 2 & 11 ON DECKS)
- WHEELCHAIR CIRCULATION

10451-10491 NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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SITE PLAN FIRST FLOOR



DP 18-829241
JULY 29, 2019

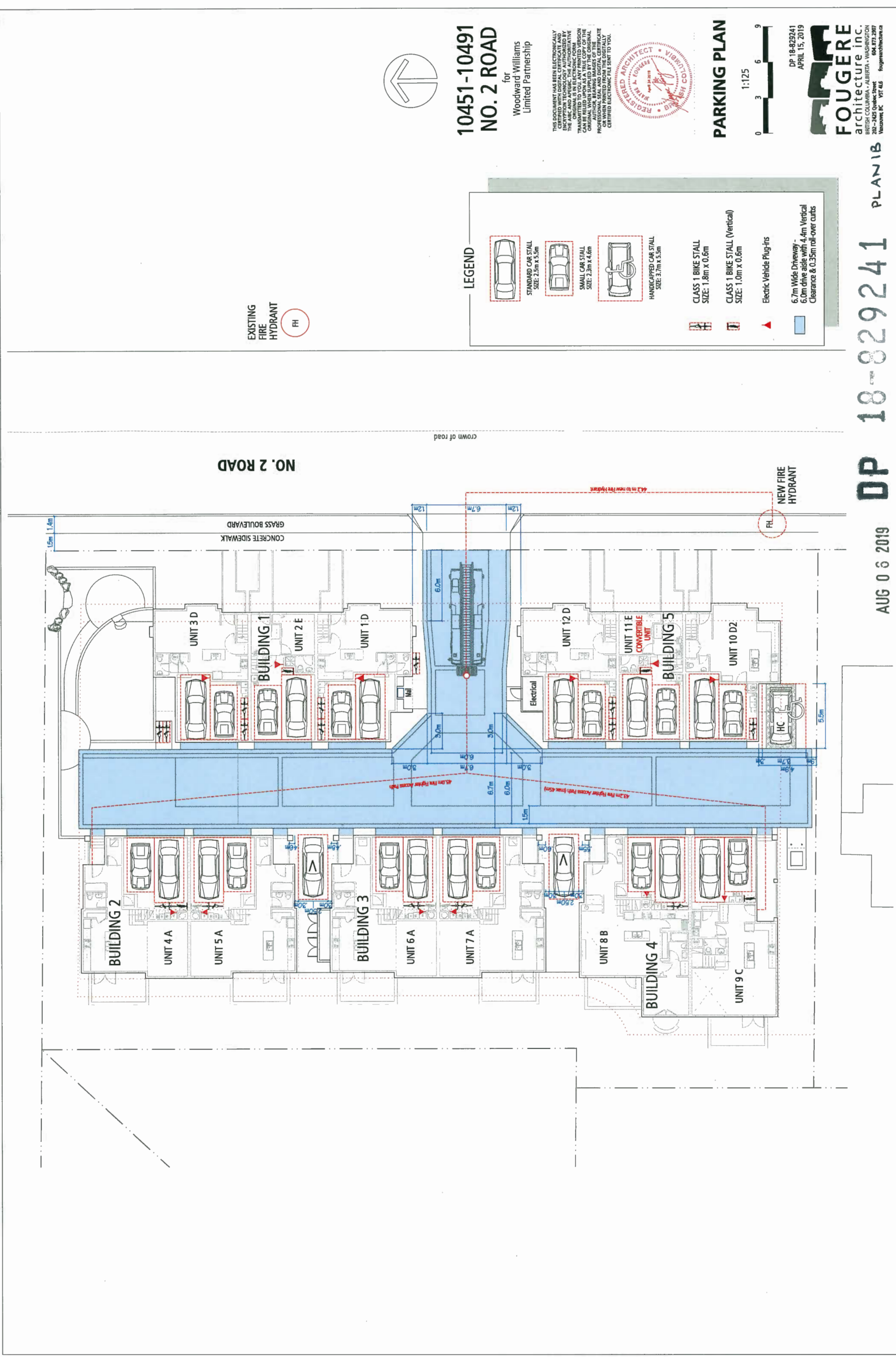


DP 18-829241

PLAN 1A

AUG 06 2019

LEGAL ADDRESS:
 LOT 67 EXCEPT: FIRSTLY: PART SUBDIVIDED BY PLAN 38549
 SECONDLY: PART SUBDIVIDED BY PLAN 61539
 PLAN 8141
 LOT 103 EXCEPT: PART SUBDIVIDED BY PLAN 61539
 PLAN 38549
 LOT 769: PLAN 56002
 ALL OF SECTION 36 BLOCK 4 NORTH RANGE 7 WEST
 NEW WESTMINSTER DISTRICT PLAN
 AVERAGE GRADE: 1.83 m



NO. 2 ROAD

CONCRETE SIDEWALK
GRASS BOULEVARD

crown of road

EXISTING
FIRE
HYDRANT
FH

NEW FIRE
HYDRANT
FH

10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership



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PARKING PLAN



DP 18-829241
APRIL 15, 2019

FOUGERE
architecture inc.
BRITISH COLUMBIA, ALBERTA, WASHINGTON
200 - 2425 Quebec Street, Suite 400
Vancouver, BC V6T 4L5
fougerearchitect.ca

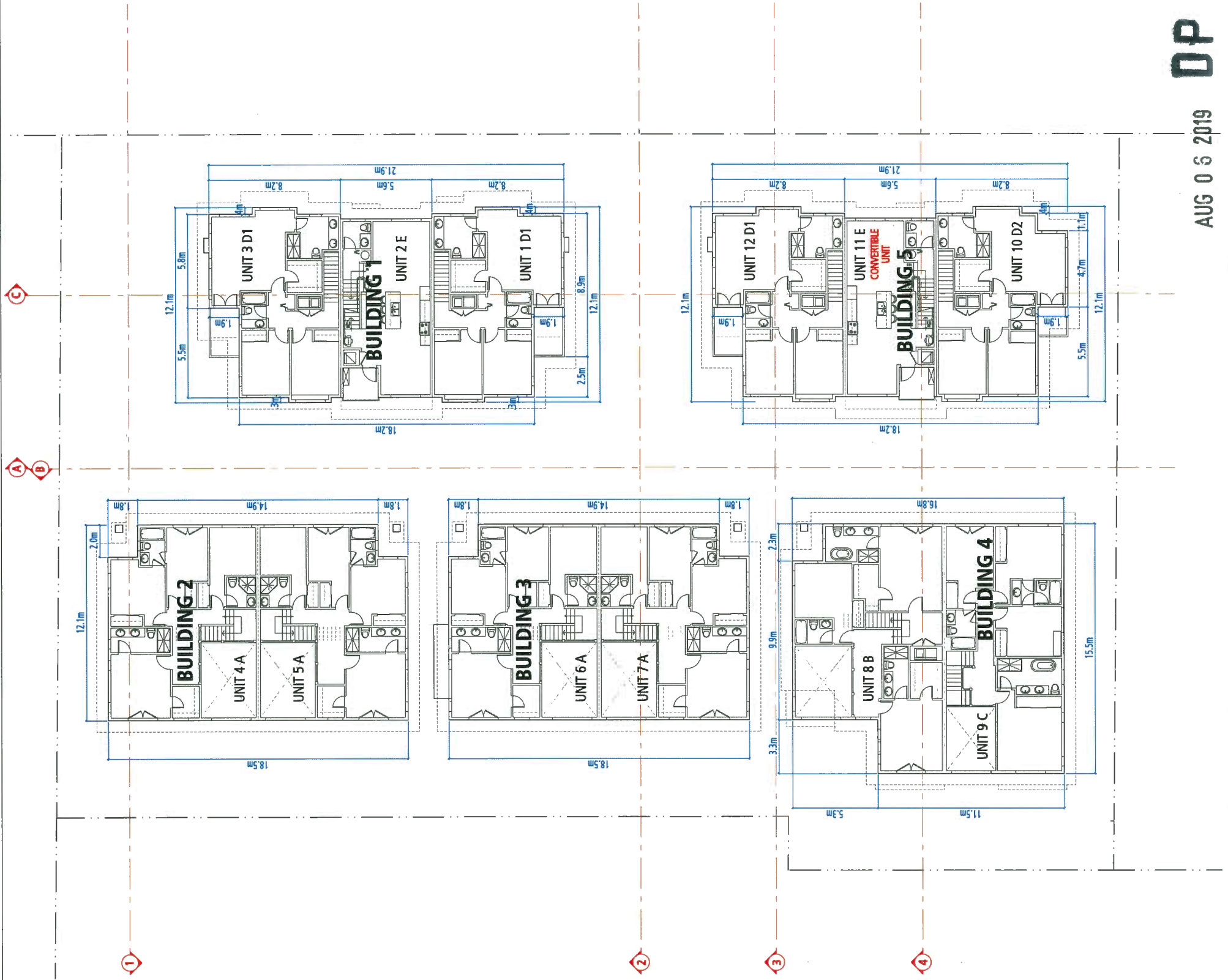
LEGEND

- STANDARD CAR STALL
SIZE: 2.5m x 5.5m
- SMALL CAR STALL
SIZE: 2.3m x 4.6m
- HANDICAPPED CAR STALL
SIZE: 3.7m x 5.5m
- CLASS 1 BIKE STALL
SIZE: 1.8m x 0.6m
- CLASS 1 BIKE STALL (Vertical)
SIZE: 1.0m x 0.6m
- Electric Vehicle Plug-ins
- 6.7m Wide Driveway -
6.0m drive aisle with 4.4m Vertical
Clearance & 0.35m roll-over curbs

DP 18-829241

AUG 06 2019

PLAN 1B



10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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SITE PLAN
SECOND FLOOR

1:125



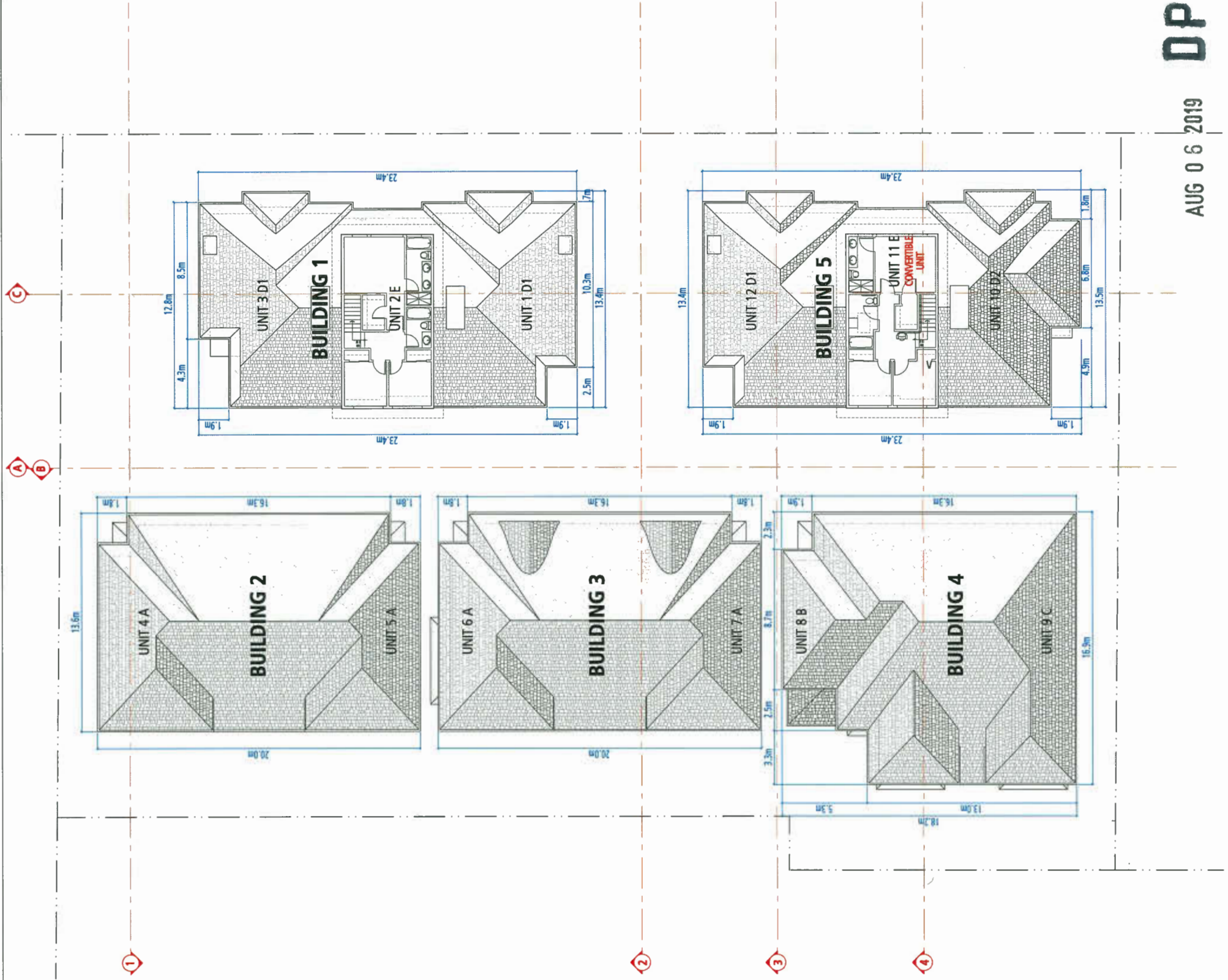
DP 18-829241
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DP 18-829241

AUG 06 2019

PLAN 1C



10451-10491
NO. 2 ROAD

for
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Limited Partnership

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SITE PLAN
THIRD FLOOR



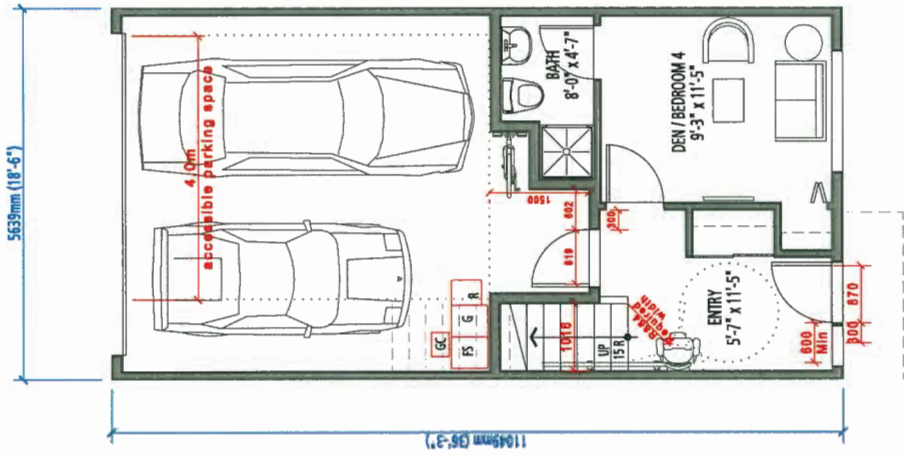
DP 18-829241
APRIL 15, 2019



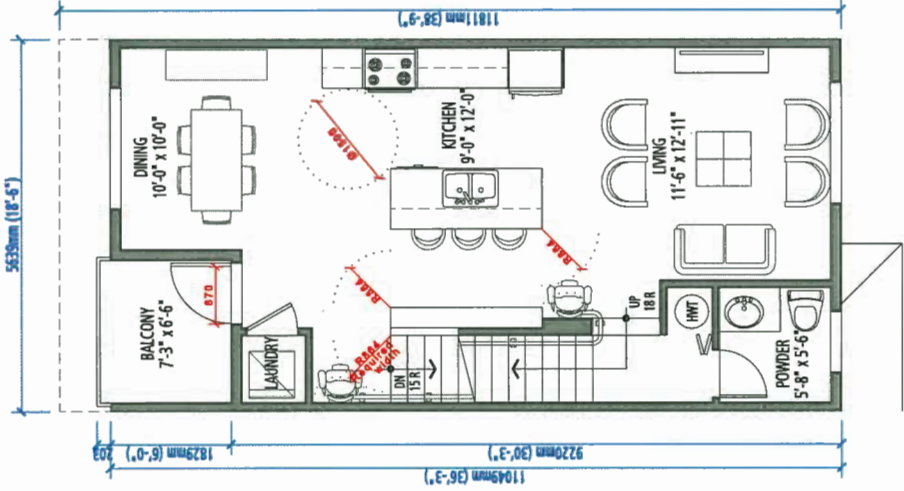
DP 18-829241 PLAN ID

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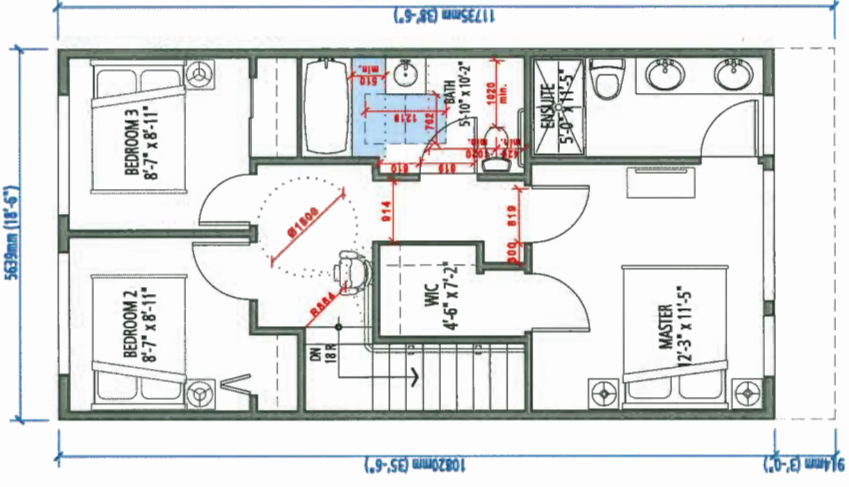
CONVERTIBLE UNIT



GROUND FLOOR



MAIN FLOOR



UPPER FLOOR

Summary of the Convertible Unit Features Checklist as Submitted

- Entry doors min. 863mm clear opening (3'-0" swinging door spec.)
- Entry door clear exterior floor space min. 1220 mm depth by door width plus 600 mm on latch side.
- Interior doors to entry & main living areas, min. 800 mm clear opening. (2'8" sliding or 2'10" swinging door spec.) with flush thresholds max. 13mm height.
- Stair lift as per manufacturer spec (Bruno, SRE-2010-Electra-Ride)
- Hallways minimum 900 mm width
- Door from garage to living area 819 mm clear opening (2'-10" swinging door)
- 1 accessible parking space with min. 4 m garage width.
- Min. clear opening 860 mm clear opening to Patios and Balconies.

- Toilet clear floor space min. 1020 mm at side and in front
- Interior Doors to main living areas, 1 bedroom and 1 bedroom min 800 mm clear opening with flush thresholds max. 13 mm height.
- Wall blocking for future installation of grab-bars (toilet, tub and shower)
- Clear area needed under future work space. Plumbing and gas pipes (in-wall and in-floor) located clear of under counter area of future work space (stove, sink & min. 810 mm wide counter)
- 1500 mm turning diameter or turning path diagram
- One window that can be opened with a single hand in the living room
- One window that can be opened with a single hand in one bedroom

10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

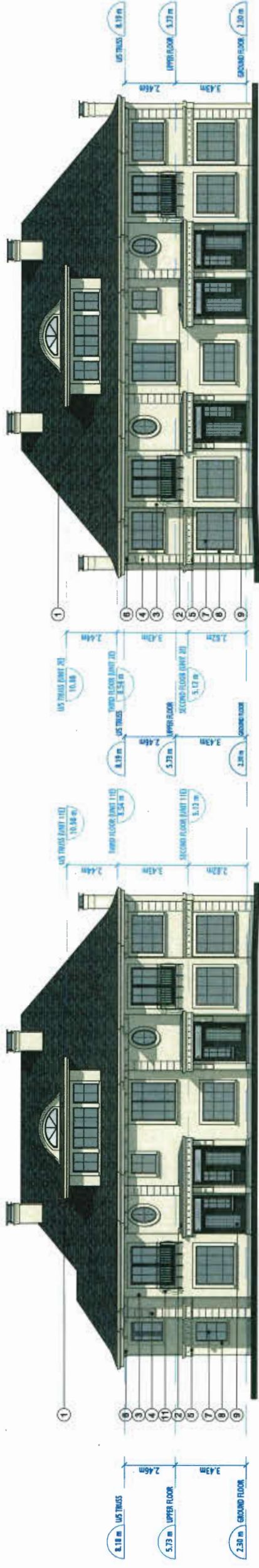


ACCESSIBILITY

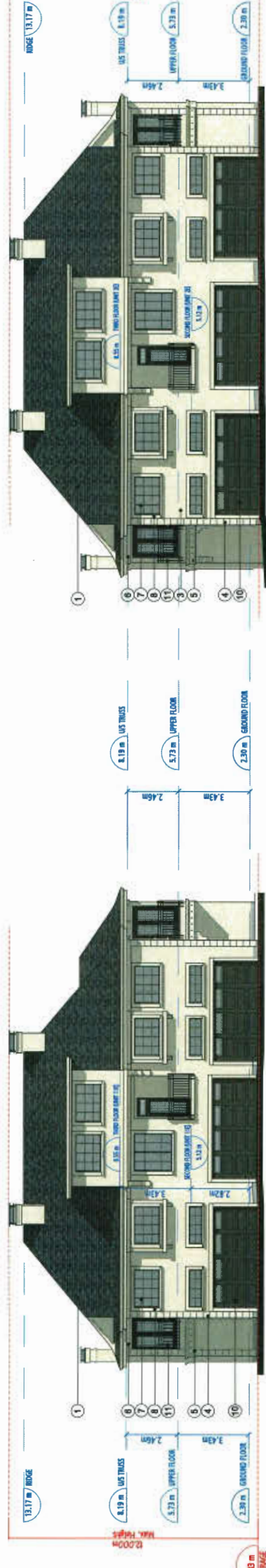


DP 18-829241
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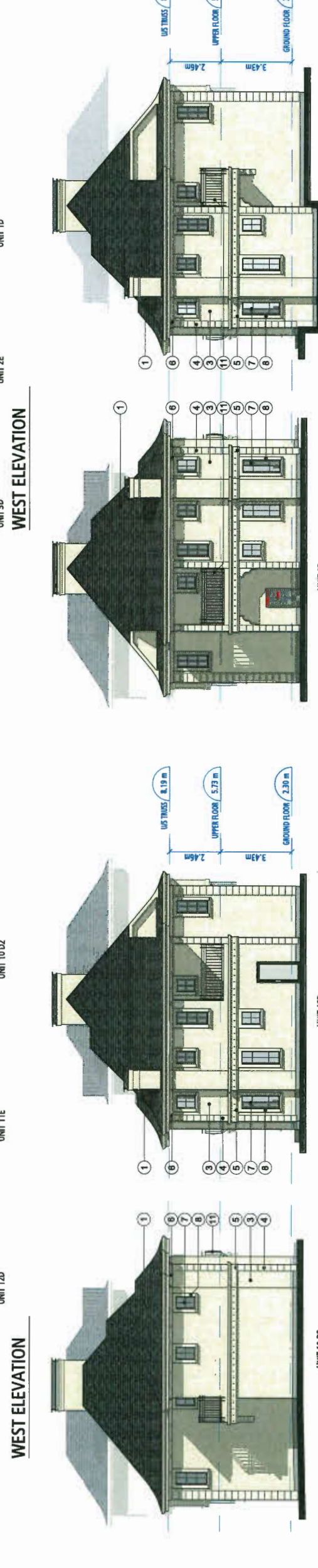




EAST ELEVATION
 UNIT 1D
 UNIT 2E
 UNIT 3D



EAST ELEVATION
 UNIT 1D
 UNIT 2E
 UNIT 3D



EAST ELEVATION
 UNIT 1D
 UNIT 2E
 UNIT 3D

WEST ELEVATION
 UNIT 1D
 UNIT 2E
 UNIT 3D



SOUTH ELEVATION
 UNIT 1D
 UNIT 2E
 UNIT 3D

NORTH ELEVATION
 UNIT 1D
 UNIT 2E
 UNIT 3D

10451-10491
NO. 2 ROAD
 for
 Woodward Williams
 Limited Partnership

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BUILDING 1, 5
ELEVATIONS
 SCALE: 1:100
 0 2 4 6m

DP 18-829241
 APRIL 15, 2019

FOUGERE
 architecture inc.
 BRITISH COLUMBIA - ALBERTA - VANCOUVER
 301-2425 Oakridge Street - VANCOUVER BC V6T 4L5
 604.673.2807
 fougerearchitects.ca

PLAN 2A

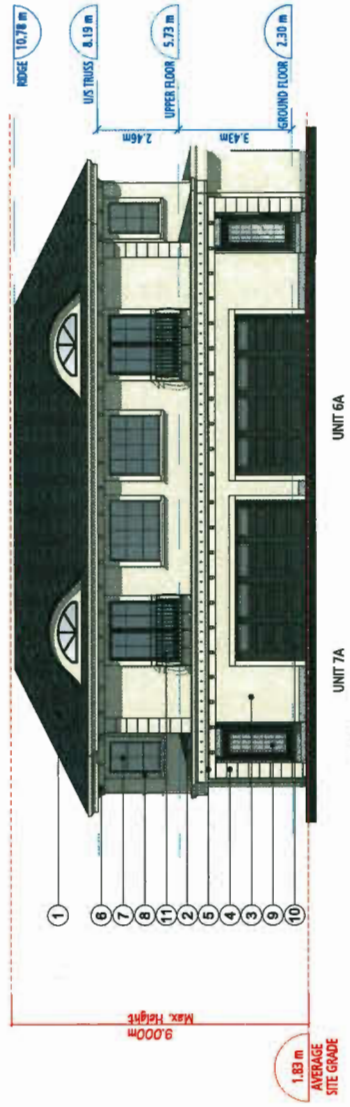
EXTERIOR FINISHES SCHEDULE

SYMBOL NUMBER	MATERIAL	COLOUR
1	ASPHALT SHINGLES	IKO Cambridge - Dual Black
2	METAL ROOF	Sherwin Williams 7069 Iron Ore
3	STUCCO	Benjamin Moore OC36 Niveous
4	EFS STUCCO DETAILS	Benjamin Moore OC40 Albescent
5	EFS STUCCO MOULDING	Benjamin Moore OC40 Albescent
6	EFS STUCCO MULDING	Benjamin Moore OC40 Albescent
7	VINYL WINDOWS	Black
8	EFS STUCCO WINDOW TRIMS	Benjamin Moore OC40 Albescent
9	EXTERIOR ENTRY DOOR	Sherwin Williams 6990 Cwiler
10	GARAGE DOOR	Sherwin Williams 6990 Cwiler
11	METAL RAILING	Black

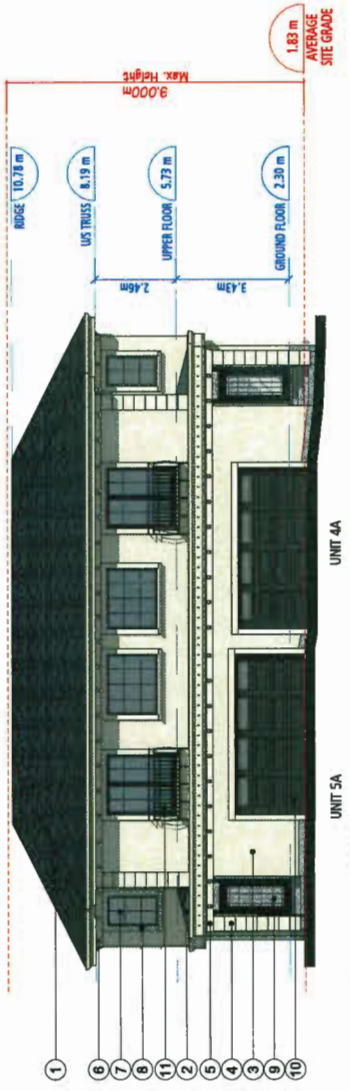
BUILDING 1

BUILDING 5

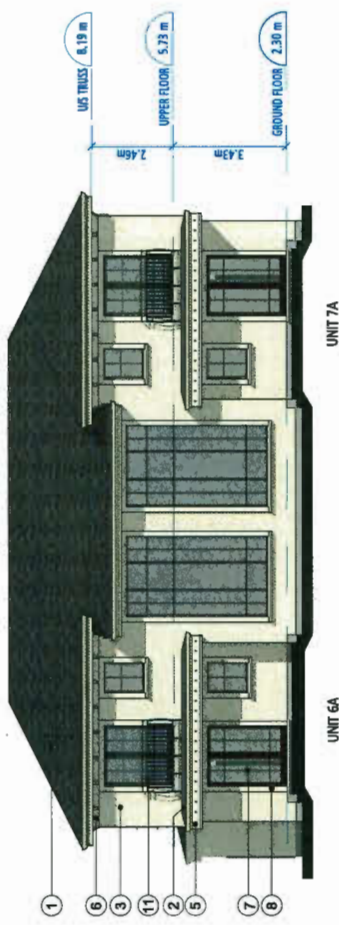
DP 18-829241
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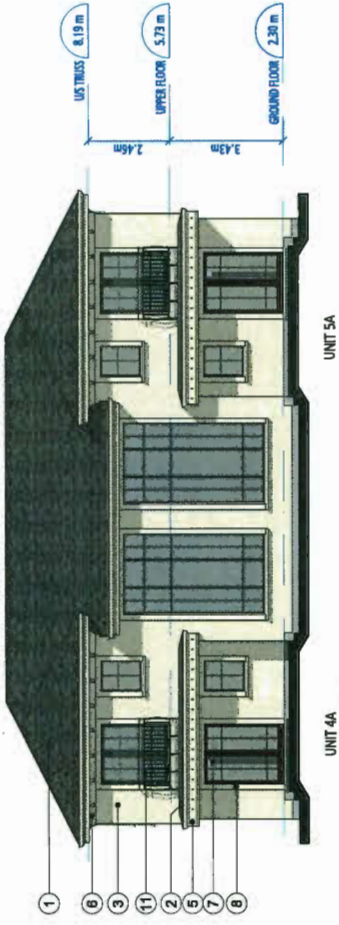
EAST ELEVATION



EAST ELEVATION



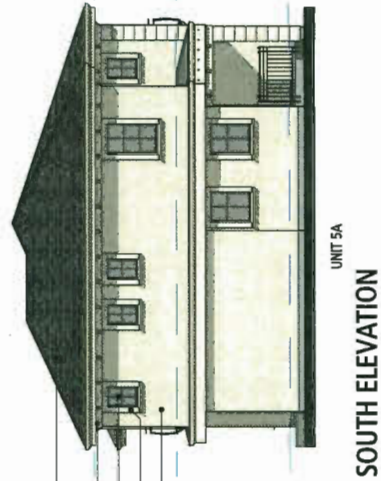
WEST ELEVATION



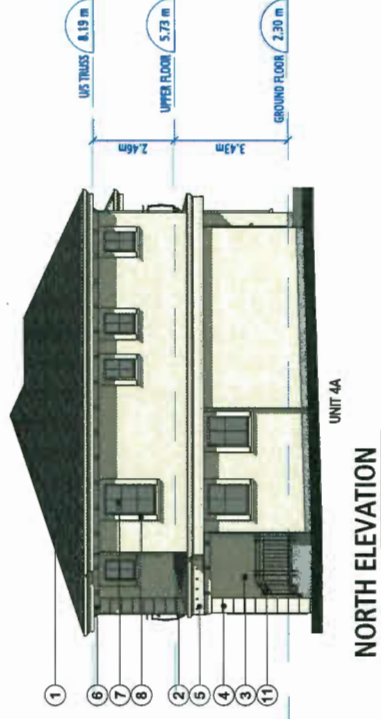
WEST ELEVATION



SOUTH ELEVATION



SOUTH ELEVATION



NORTH ELEVATION

BUILDING 3

BUILDING 2

EXTERIOR FINISHES SCHEDULE

SYMBOL NUMBER	MATERIAL	COLOUR
1	ASPHALT SHINGLES	IKO Cambridge - Dual Black
2	METAL ROOF	Sherwin Williams 7069 Iron Ore
3	STUCCO	Benjamin Moore DC36 Niwonus
4	EIFS STUCCO DETAILS	Benjamin Moore DC40 Albescent
5	EIFS STUCCO MOULDING	Benjamin Moore DC40 Albescent
6	EIFS STUCCO MOULDING	Benjamin Moore DC40 Albescent
7	VINYL WINDOWS	Black
8	EIFS STUCCO WINDOW TRIMS	Benjamin Moore DC40 Albescent
9	EXTERIOR ENTRY DOOR	Sherwin Williams 6990 Cavalr
10	GARAGE DOOR	Sherwin Williams 6990 Cavalr
11	METAL RAILING	Black

SCALE: 1:100



DP 18-829241
APRIL 15, 2019



10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

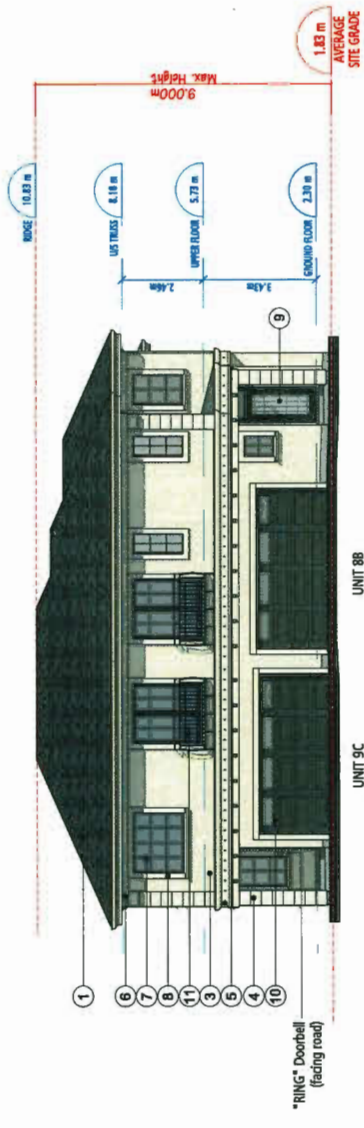
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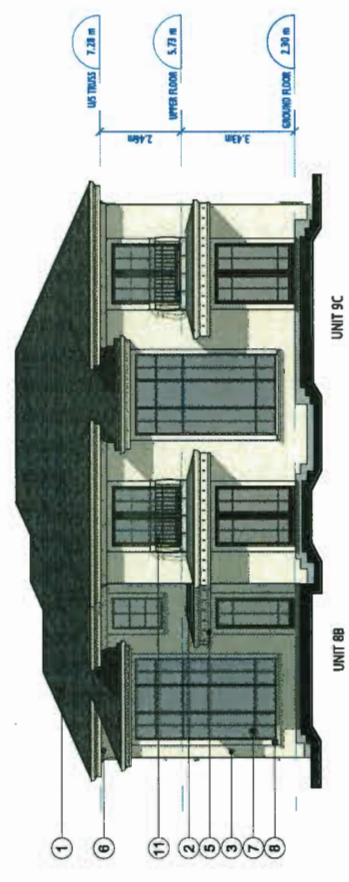
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DP 18-829241

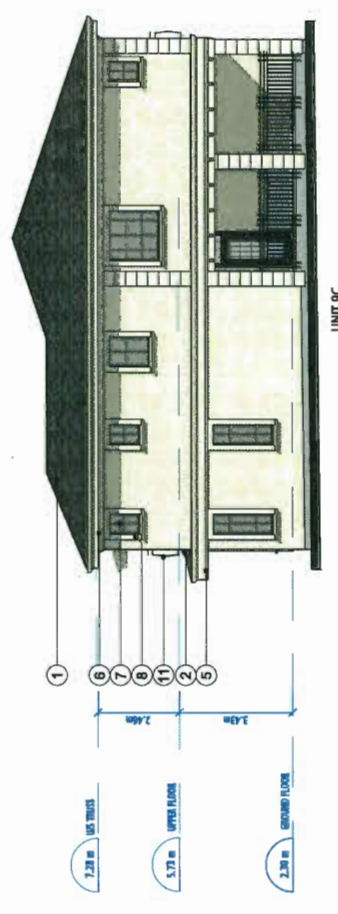
PLAN 25



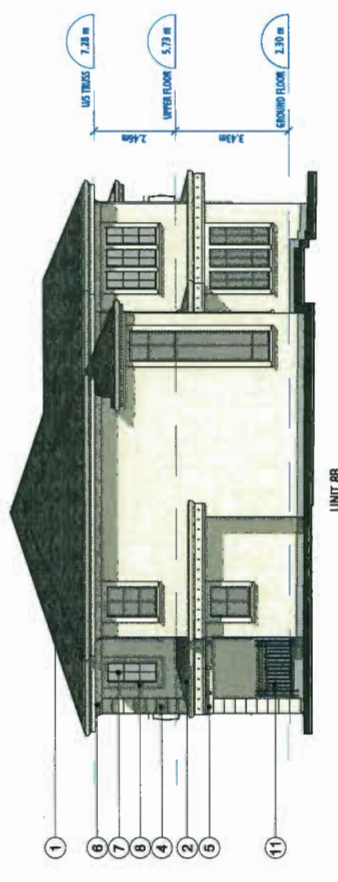
EAST ELEVATION



WEST ELEVATION



SOUTH ELEVATION



NORTH ELEVATION

10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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**BUILDING 4
ELEVATIONS**

SCALE: 1:100



DP 18-829241
JULY 29, 2019



EXTERIOR FINISHES SCHEDULE

SYMBOL NUMBER	MATERIAL	COLOUR
1	ASPHALT SHINGLES	IKO Cambridge - Dual Black
2	METAL ROOF	Sherwin Williams 7069 Iron Ore
3	STUCCO	Benjamin Moore DC36 Niveous
4	EPS STUCCO DETAILS	Benjamin Moore DC40 Alabescent
5	EPS STUCCO MOULDING	Benjamin Moore DC40 Alabescent
6	EPS STUCCO MOULDING	Benjamin Moore DC40 Alabescent
7	VINYL WINDOWS	Black
8	EPS STUCCO WINDOW TRIMS	Benjamin Moore DC40 Alabescent
9	EXTERIOR ENTRY DOOR	Sherwin Williams 6990 Cavalr
10	GARAGE DOOR	Sherwin Williams 6990 Cavalr
11	METAL RAILING	Black

BUILDING 4

AUG 06 2019 DP 18-829241

10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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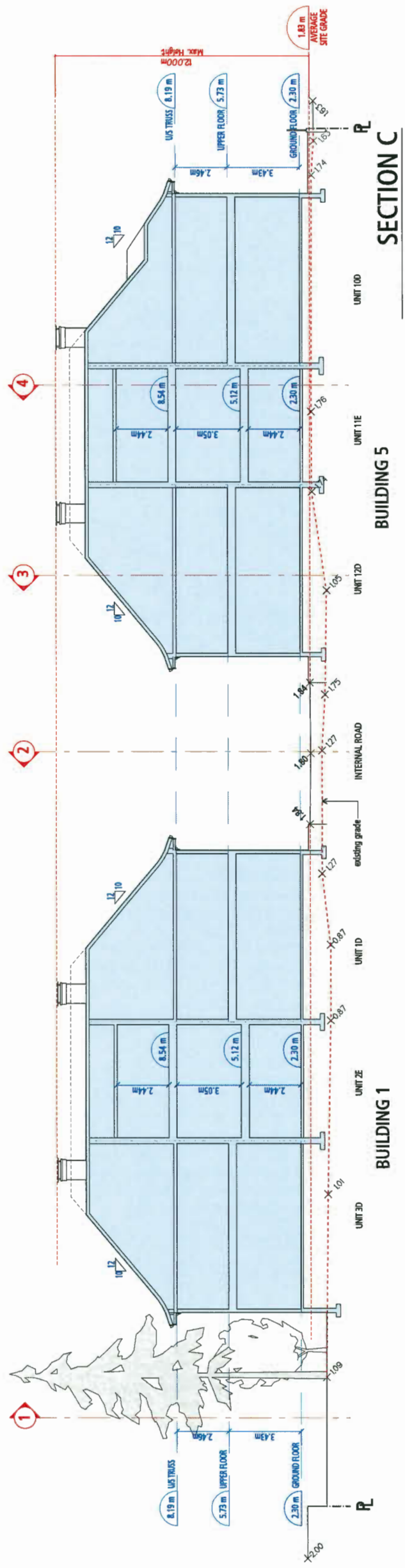
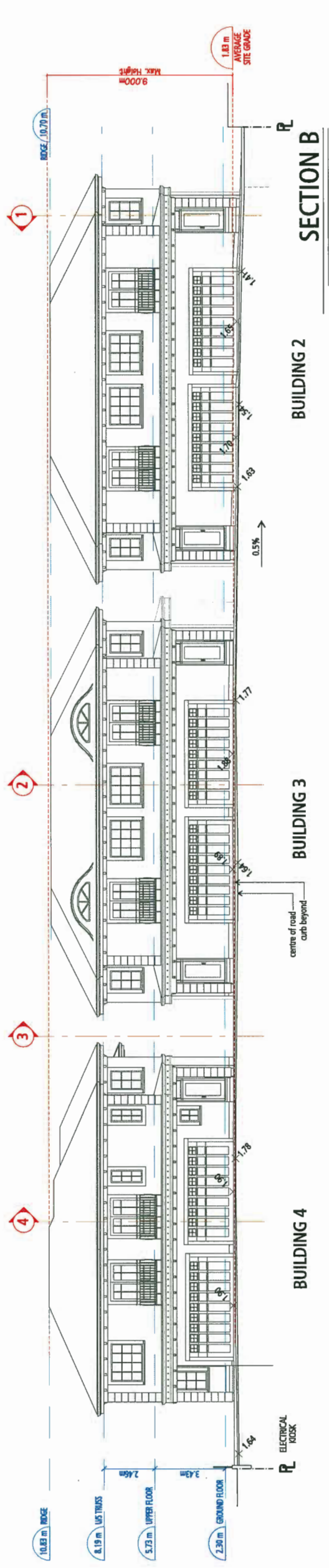
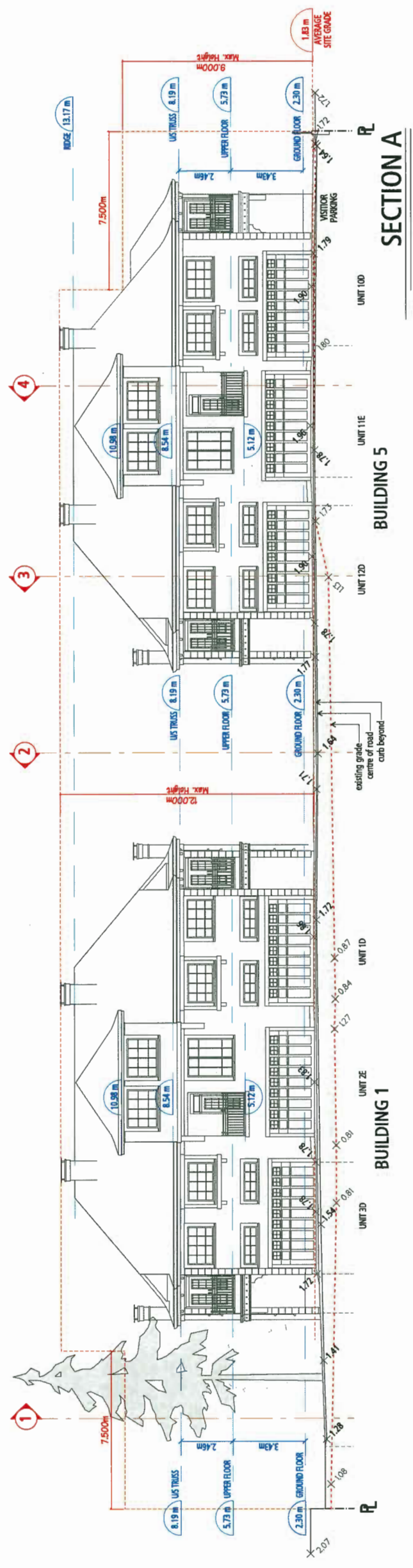
SCALE: 1:100



DP 18-829241
APRIL 15, 2019



PLAN 20



18-829241

DP

AUG 06 2019

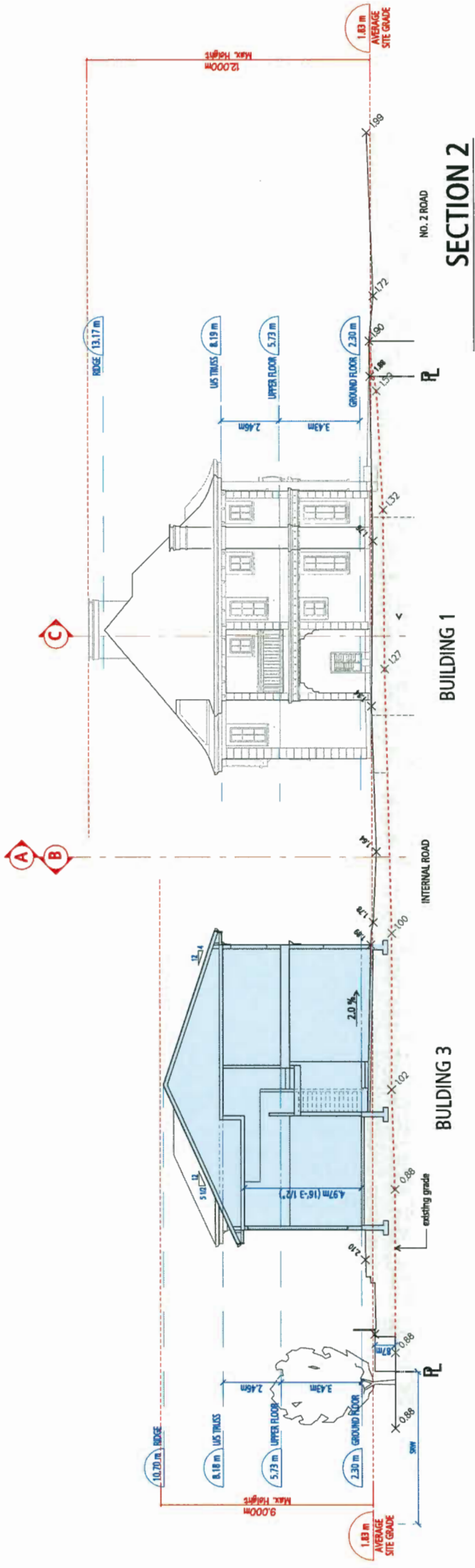
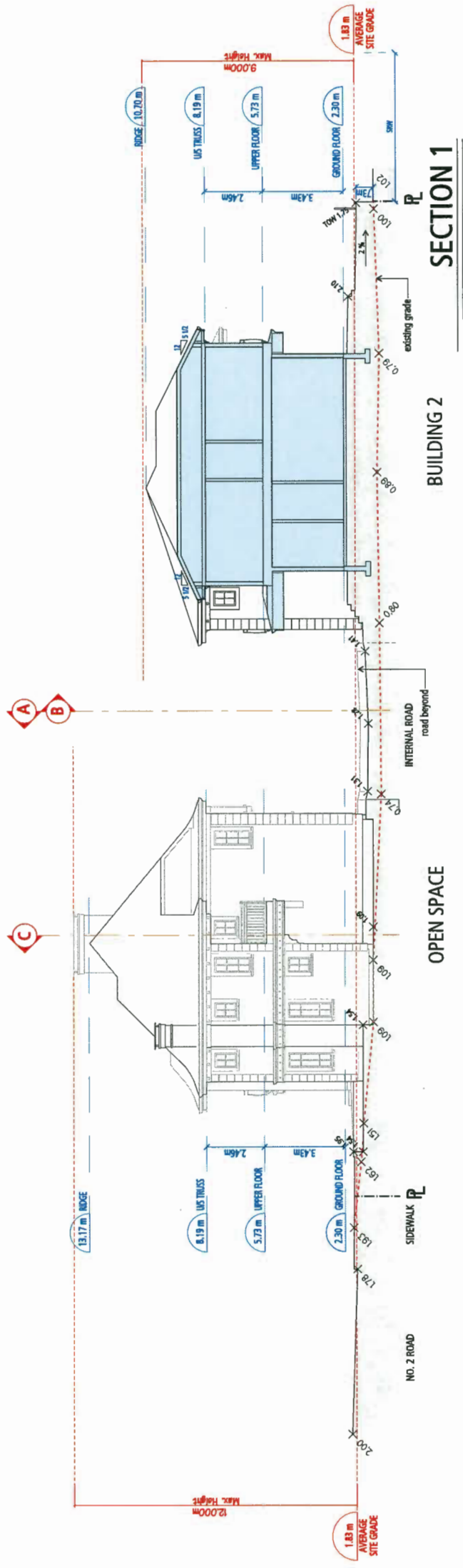
BUILDING 1

BUILDING 2

BUILDING 3

BUILDING 4

BUILDING 5



10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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SECTIONS

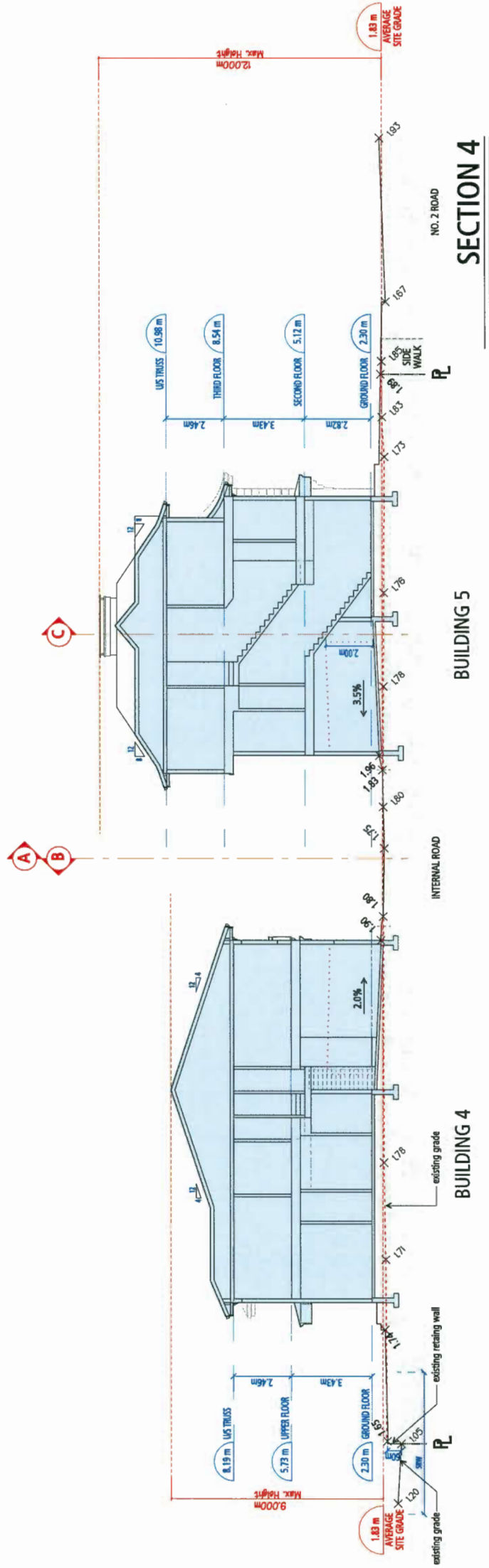
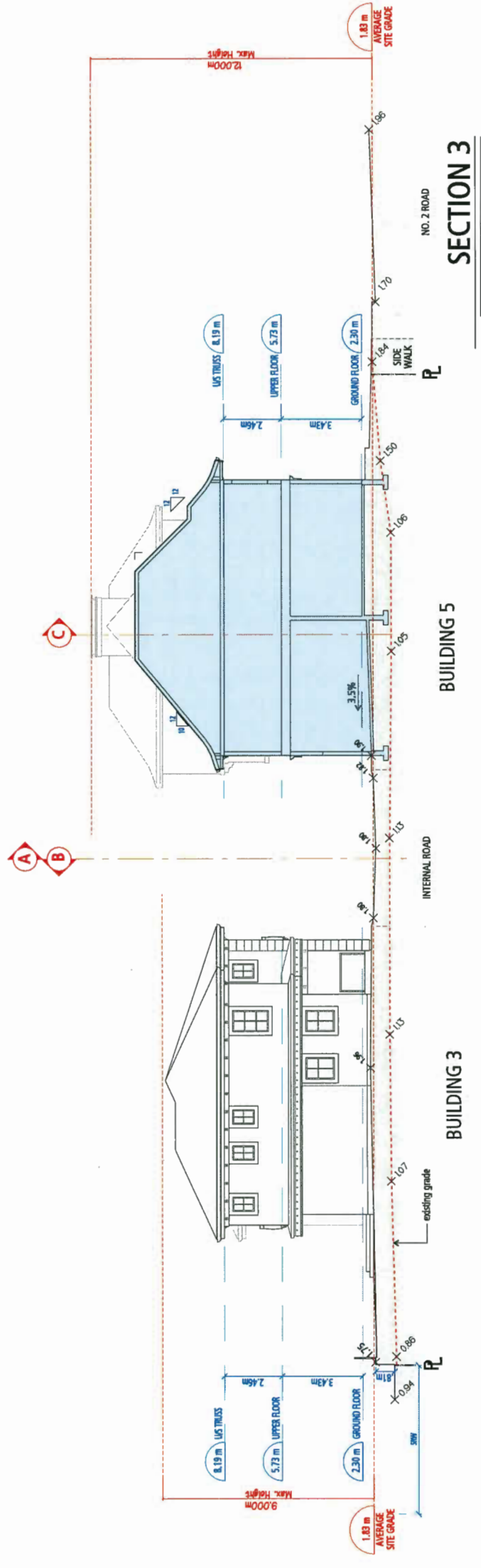


DP 18-829241
AUGUST 1, 2019



AUG 06 2019 DP 18-829241

PLAN 2E



10451-10491
NO. 2 ROAD
for
Woodward Williams
Limited Partnership

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SECTIONS

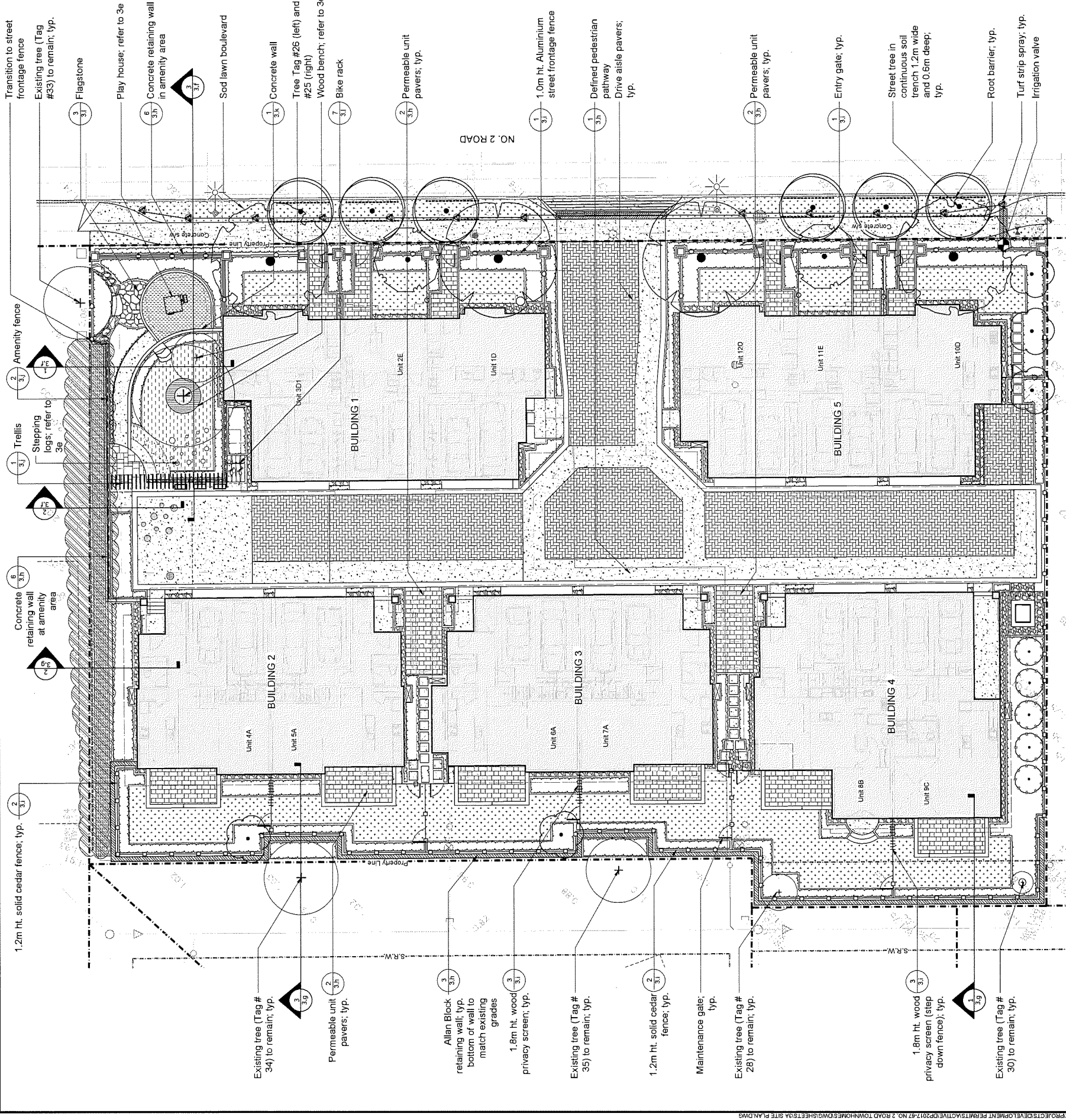


DP 18-829241
AUGUST 1, 2019



AUG 06 2019 DP 18-829241

PLAN 2F



1:150 0 1.5m 3 4.5 6 7.5 9 10.5 12 13.5 15 16.5 18 19.5 21 22.5

HARDSCAPE MATERIALS

KEY	REF.	DESCRIPTION
	1 3.h	CONCRETE PATHWAY
	2 3.h	PERMEABLE PAVERS: Allan Block Manufacturer: Epoxcrete Colour: Charcoal
	3 3.h	CONCRETE RETAINING WALL
	3 3.h	ALLAN BLOCK RETAINING WALL
	3 3.k	BASALT RETAINING WALL

FENCING

KEY	REF.	DESCRIPTION
	1 3.i	1.0M HT. STREET FRONTAGE FENCE, ALUMINIUM PICKET
	2 3.i	1.2M SOLID CEDAR FENCE
	3 3.i	1.8M HT. WOOD PRIVACY SCREEN (STEP DOWN FENCE)
	2 3.j	AMENITY FENCE

SOFTSCAPE MATERIALS

KEY	REF.	DESCRIPTION
	+	EXISTING TREE
	3-4 3.j	PROPOSED TREE
		SOD See Critical Landscaping Notes for Specifications
	5-6 3.j	SHRUB PLANTING
	1 3.k	FIBER MULCH @ LOWER PLAY AREA 100mm depth
	5-6 3.k	FIBER MULCH @ UPPER PLAY AREA 300mm depth

SITE FURNISHINGS/LIGHTING

KEY	REF.	DESCRIPTION
	7 3.l	BIKE RACK Model # N887000 Manufacturer: Magis Site Furnishings Mount: Direct Bolt



DP2017-67
SIT PLAN
Drawing Title: DP2017-67



3A
Drawing #:
VZ Project #

REVISIONS TABLE FOR DRAWINGS

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No.	By	Description	Date
11	LJ	Re-issued for DP	June 7, 2019
10	MES	Re-issued for DP	Apr. 23, 2019
9	SH	Re-issued for DP	Mar. 28, 2019
8	MES	Re-issued for DP	Feb. 19, 2019
7	MES	Issued for ADP	Dec. 20, 2018
6	MES	Re-issued for DP	Nov. 23, 2018
5	MES	Client Review	Oct. 24, 2018
4	MES	Issued for DP	July 13, 2018
3	MES	Issued for Reopening	Jan. 11, 2018
2	MES	Client Review	Dec. 12, 2017
1	MES	Client Review	Nov. 24, 2017

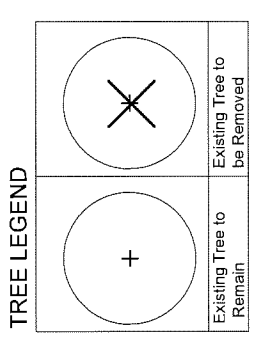
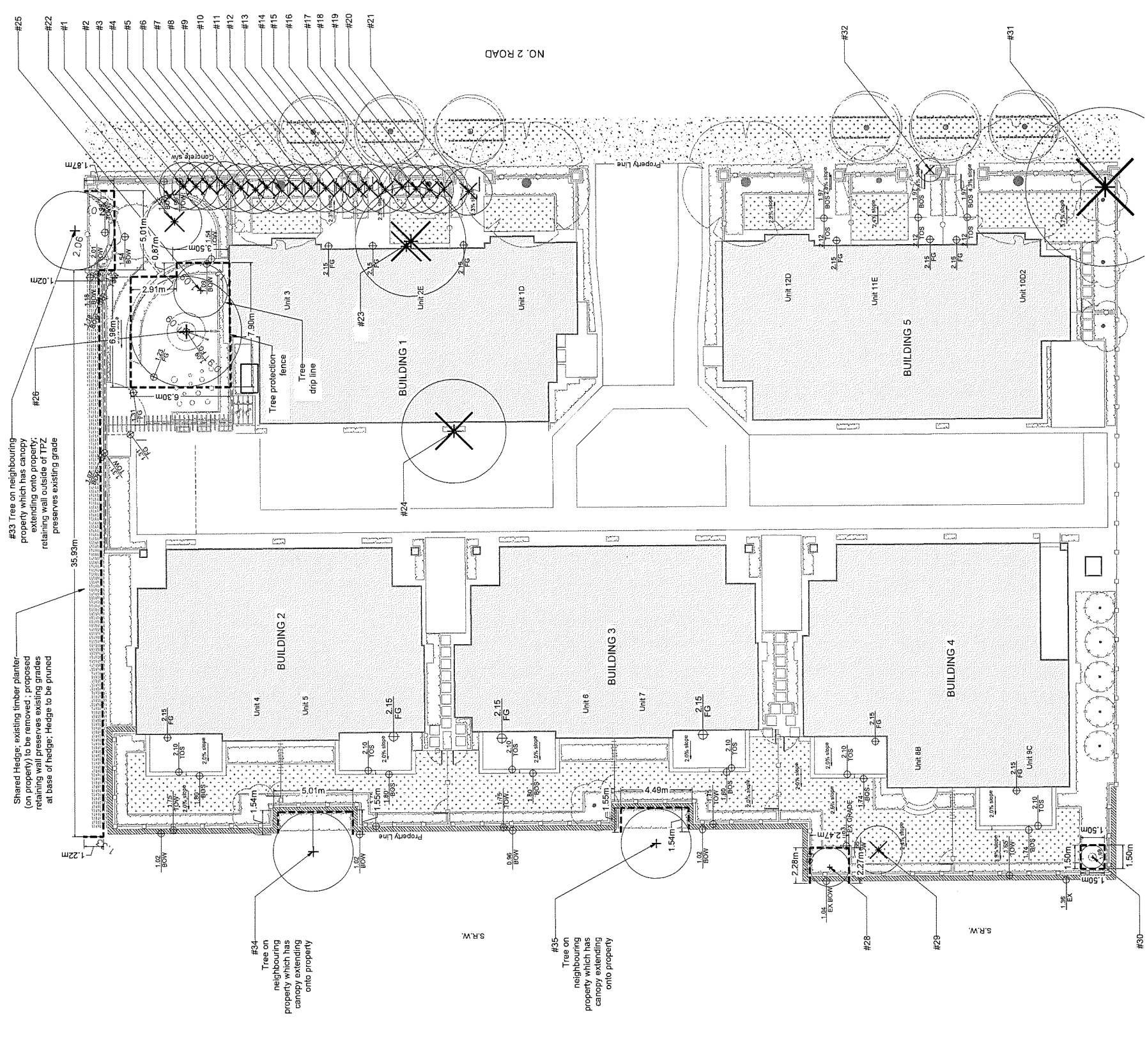
REVISIONS TABLE FOR SHEET

No.	By	Description	Date

Project:
Proposed Multi-Family Residential
for Woodward Williams Limited
Partnership
Location:
10451 - 10491 No. 2 Road
Richmond, BC

Drawn: MJS
Checked: SH
Approved: MVDZ
Scale: 1:150
Original Sheet Size: 24"x36"

CONTRACTOR SHALL CHECK ALL DIMENSIONS AND LOCATIONS OF ALL ELEMENTS TO THE CONSTRUCTION BEFORE PROCEEDING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE EXCLUSIVE PROPERTY OF THE OWNER AND THE CONTRACTOR SHALL NOT BE RESPONSIBLE FOR THE WORK AND DIMENSIONS OF ANY OTHER DRAWINGS THAT MAY BE USED FOR THE CONSTRUCTION.



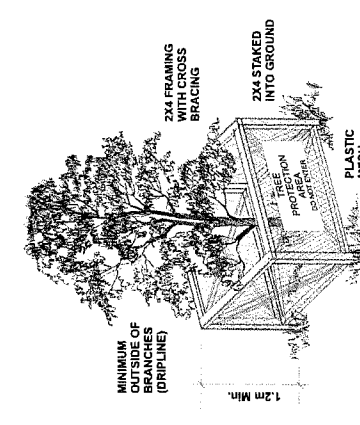
KEY	REF.	DESCRIPTION
---	2	TREE PROTECTION FENCE
	3b	TREE PROTECTION FENCE

Tree Summary

of significant on-site trees removed: 5
 # of replacement trees required (2:1): 10
 # of replacement trees proposed: 16

NOTE: Tree tag #1-22 is a cedar hedge and compensation is not required.

For Tree Table Information, refer to project Arbolist: Arbolicultural Assessment Report, Proposed Multi-Family Residential Development 10451-10491 #2 Road Richmond BC by Michael J Mills Consulting MJM File # 1725 Email: mills@dccnet.com



NOTE: WOOD FRAM CONSTRUCTION STAYS INTO THE GROUND IS REQUIRED. (METAL FRAMING DOES NOT MEET CITY STANDARDS)

TREE TRUNK DIAMETER	DISTANCE FROM TRUNK	TOTAL DIAMETER
20 cm	0.6 feet	3.9 feet
25 cm	0.8 feet	4.9 feet
30 cm	1.0 feet	5.9 feet
35 cm	1.2 feet	6.9 feet
40 cm	1.4 feet	7.9 feet
45 cm	1.6 feet	8.9 feet
50 cm	1.8 feet	9.9 feet
55 cm	2.0 feet	10.9 feet
60 cm	2.2 feet	11.9 feet
65 cm	2.4 feet	12.9 feet
70 cm	2.6 feet	13.9 feet
75 cm	2.8 feet	14.9 feet
80 cm	3.0 feet	15.9 feet
85 cm	3.2 feet	16.9 feet
90 cm	3.4 feet	17.9 feet
95 cm	3.6 feet	18.9 feet
100 cm	3.8 feet	19.9 feet

TREE TRUNK DIAMETER	MINIMUM PROTECTION REQUIRED AROUND TREE
20 cm	0.6 feet
25 cm	0.8 feet
30 cm	1.0 feet
35 cm	1.2 feet
40 cm	1.4 feet
45 cm	1.6 feet
50 cm	1.8 feet
55 cm	2.0 feet
60 cm	2.2 feet
65 cm	2.4 feet
70 cm	2.6 feet
75 cm	2.8 feet
80 cm	3.0 feet
85 cm	3.2 feet
90 cm	3.4 feet
95 cm	3.6 feet
100 cm	3.8 feet

2 TREE PROTECTION - CITY OF RICHMOND
 Scale: NTS



Project: Proposed Multi-Family Residential for Woodward Williams Limited Partnership
 Location: 10451 - 10491 No. 2 Road Richmond, BC

REVISIONS TABLE FOR SHEET

No.	By	Description	Date
1	MJS	Revise Tree Management Plan	May 2, 2018
2	MJS	Client Review	Nov 24, 2017
3	MJS	Client Review	Dec 12, 2017
4	MJS	Issued for Receiving	Jan 11, 2018
5	MJS	Client Review	July 13, 2018
6	MJS	Client Review	Nov 23, 2018
7	MJS	Issued for ADP	Dec 20, 2018
8	MJS	Revise for DP	Feb 19, 2019
9	SH	Re-issued for DP	Mar. 26, 2019
10	MJS	Re-issued for DP	Apr. 23, 2019
11	LJ	Re-issued for DP	June 7, 2019

1 TREE MANAGEMENT PLAN
 Scale: 1:150

TREE SCHEDULE

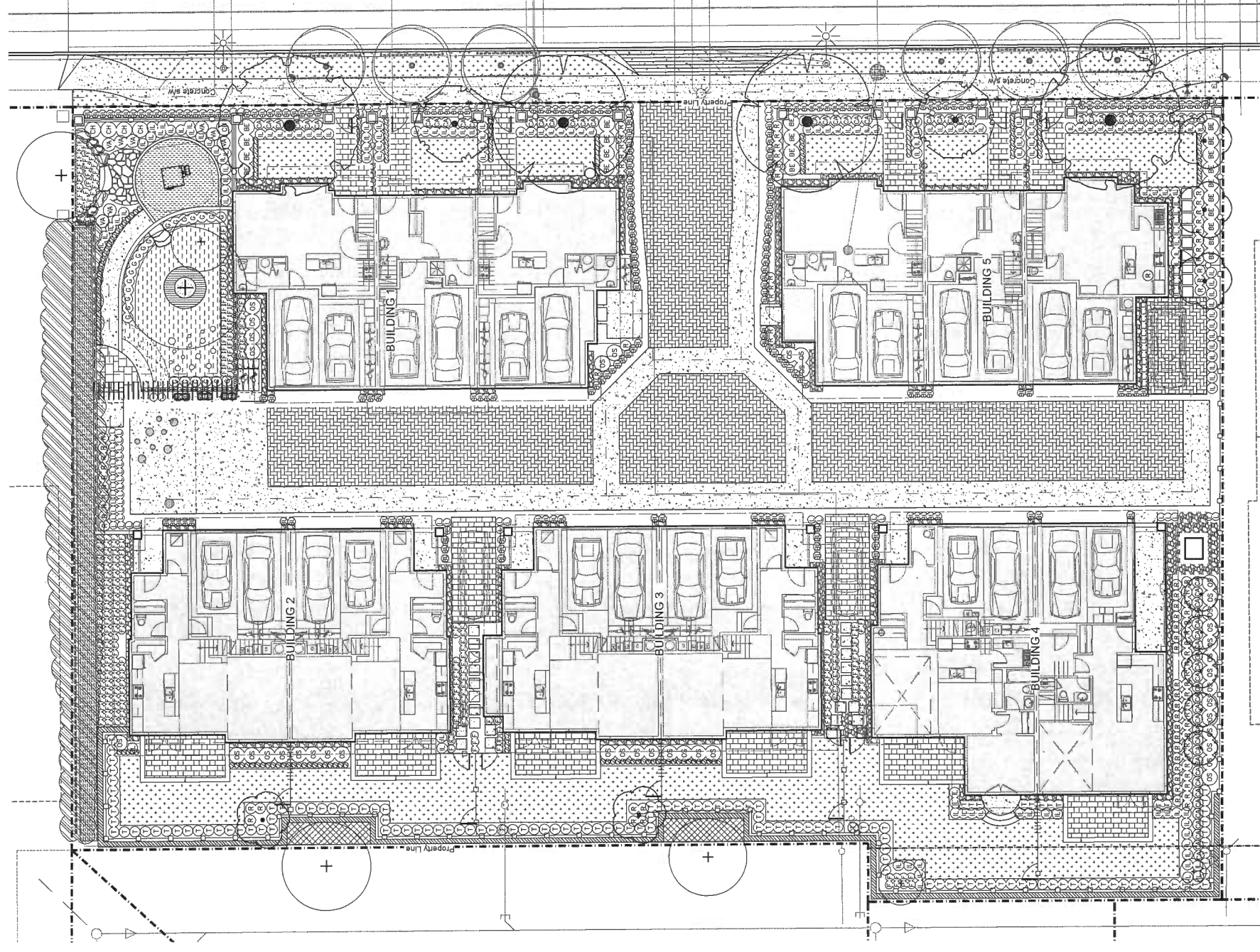
TREES	BOTANICAL / COMMON NAME	CONT.	CALL	SIZE	QTY
	Magnolia x 'Vulcan' / Magnolia Tree Form	B & B	6m	1.2m std.	5
	Pinus nigra 'Arnold Sentinel' / Arnold Sentinel / Austrian Black Pine	B & B	-	3m Ht.	5
	Stewartia pseudocamellia / Japanese Stewartia	B & B	6m cal	1.8 m std.	2
	Tilia tomentosa 'Sterling' / Sterling Silver Linden	B & B	12cm cal		2
	Tilia tomentosa 'Silver Lining' / Silver Linden	B & B	10cm cal		2
	STREET TREES				
	Cornuspa x lavallee / Hybrid Hawthorn	B & B	7cm cal	1.8 m std.	6

PLANT SCHEDULE ONSITE

SHRUBS	BOTANICAL NAME / COMMON NAME	CONT.	SPACING	QTY
BE	Berberis x glaucocarpa 'William Penn' / William Penn Barberry	#3	0.30m	22
BU	Buxus 'Green Velvet' / Boxwood	#2	0.45m	221
CA	Chorophytis x clandonensis 'Dark Knight' / Blue Mist Shrub	#3	0.60m	89
CH	Choisya ternata 'Aztec Pearl' / Mexican Orange	#3	0.60m	5
G	Gaultheria shallon / Salal	#3	0.60m	17
IL	Ilex crenata 'Soft Touch' / Soft Touch Japanese Holly	#3	0.60m	154
OS	Osmanthus x burkeoides / Burkwood Osmanthus	#3	0.30m	40
F	Forsythia x intermedia 'Western Sword Fern'	#1	0.45m	248
R	Rosa 'Medallion White' / Medallion White Rose	#2	0.60m	50
T	Taxus x media 'Hedder' / Heda Yew	#2	1.2m Ht.	142
VA	Vaccinium corymbosum / Blueberry bush	#2	0.75m	8
	ANNUALS/PERENNIALS			
Z	Iberis sempervirens / Candytuft	10cm pot	0.30m	175
L	Lavandula angustifolia 'Hidcole' / Hidcole Lavender	#1	0.45m	98
	GROUND COVERS			
	Hibiscus nervosa / Dahl Oregon Sage	#1	450mm	80

1. ALL PLANT MATERIAL SHALL MEET OR EXCEED STANDARDS REQUIRED BY SCLMA OR
2. TOPSOIL DEPTHS FOR PLANTING AS FOLLOWS
 A. GROUNDCOVERS: 300mm
 B. SHRUBS: 450mm
 C. TREES: 600mm
 D. SOIL: 150mm
3. 2" MINUS COMPOST MULCH TO BE INSTALLED IN ALL SHRUB PLANTING AREAS.
4. ALL PLANTING AREAS TO BE IRRIGATED.

NO. 2 ROAD



STREET FRONTAGE PLANTING



1:150 0 1.5m 3 4.5 6 7.5 9 10.5 12 13.5 15 16.5 18 19.5 21 22.5

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3	MIS	Issued for Reasoning	Jan 11, 2018
2	MIS	Client Review	Dec. 12, 2017
1	MIS	Client Review	Nov. 24, 2017

REVISIONS TABLE FOR SHEET

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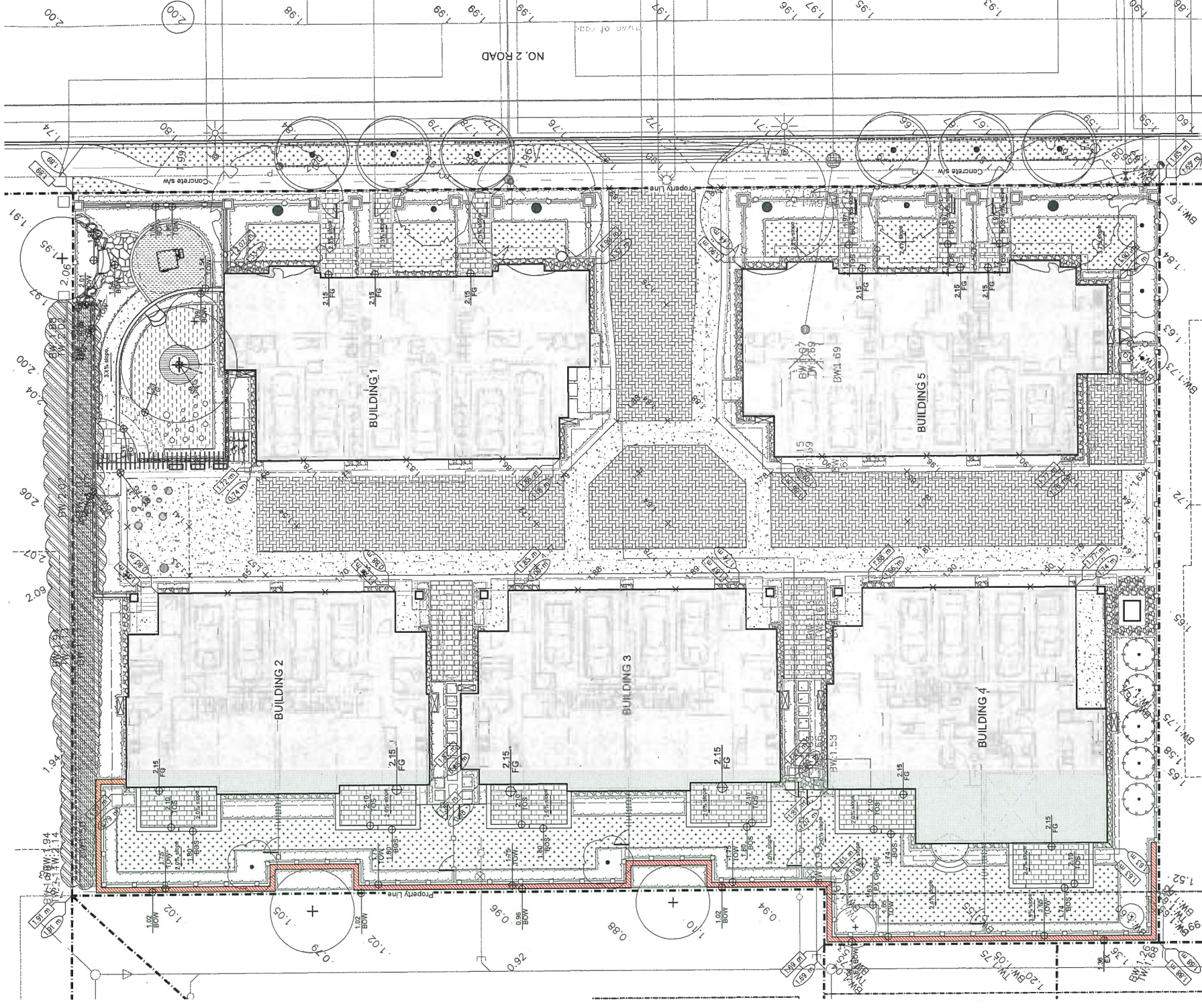
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 for Woodward Williams Limited
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 Location:
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 Richmond, BC

Drawn: MGS
 Checked: SH
 Approved: MVDJ
 Scale: 1:150
 Original Sheet Size: 24"x36"



GRADING AND DRAINAGE

KEY	DESCRIPTION
1.65' Ø BOS	PROPOSED ELEVATION
	PROPOSED SLOPE
	PROPOSED (RECTANGULAR) ARCHITECTURE GRADES BY ARCHITECTURE
	PROPOSED SLOPE BY ARCHITECTURE
1.72	EXISTING ELEVATION
	CONCRETE RETAINING WALL
	ALLAN BLOCK RETAINING WALL



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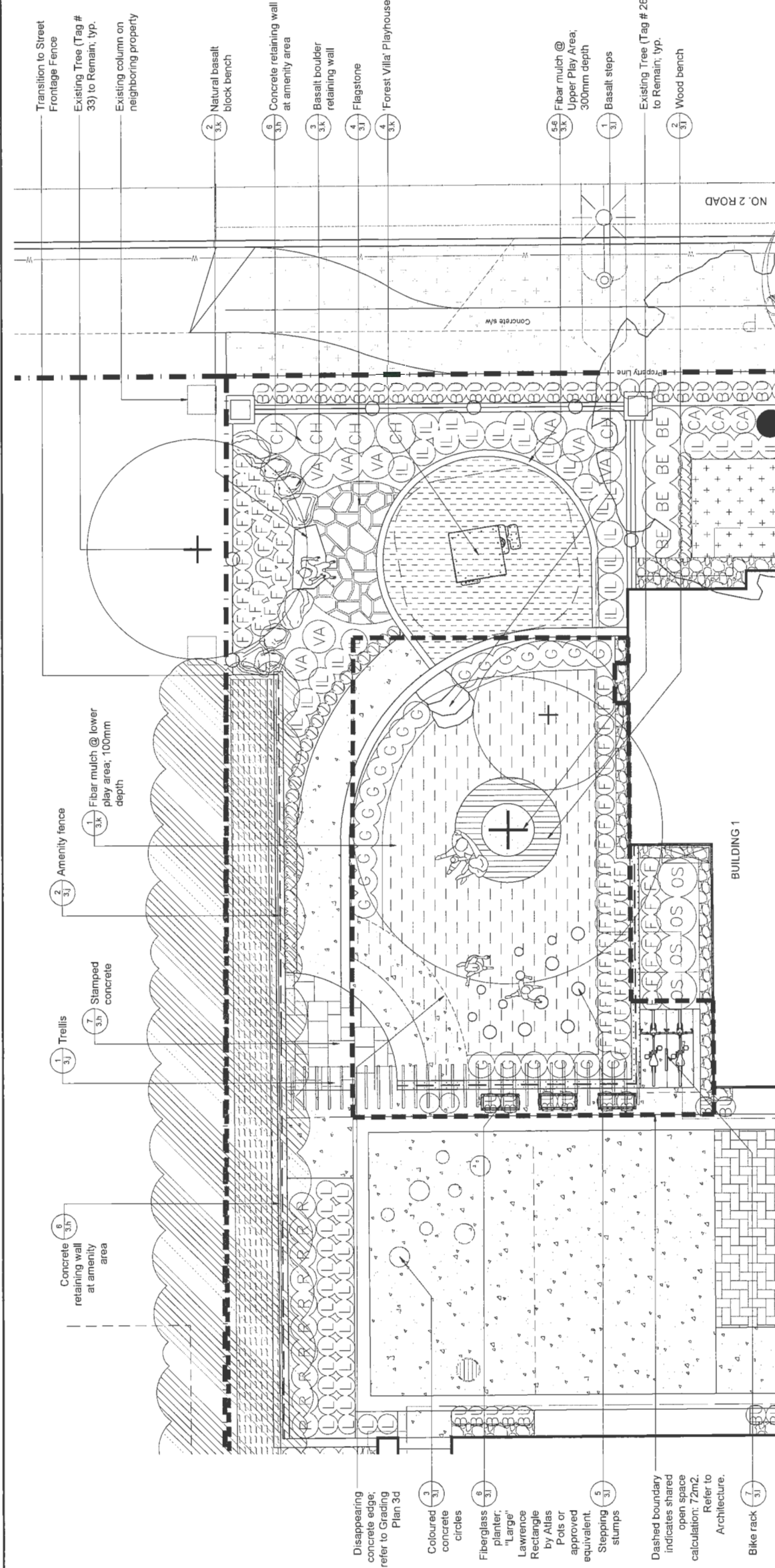
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Disappearing concrete edge; refer to Grading Plan 3d
 Coloured concrete circles
 Fiberglass planter "Large" Lawrence Rectangle by Atlas POTS or approved equivalent.
 Stepping stumps
 Dashed boundary indicates shared open space calculation: 72m². Refer to Architecture.
 Bike rack
1 AMENITY AREA
 Scale 1:50

Laneway Play


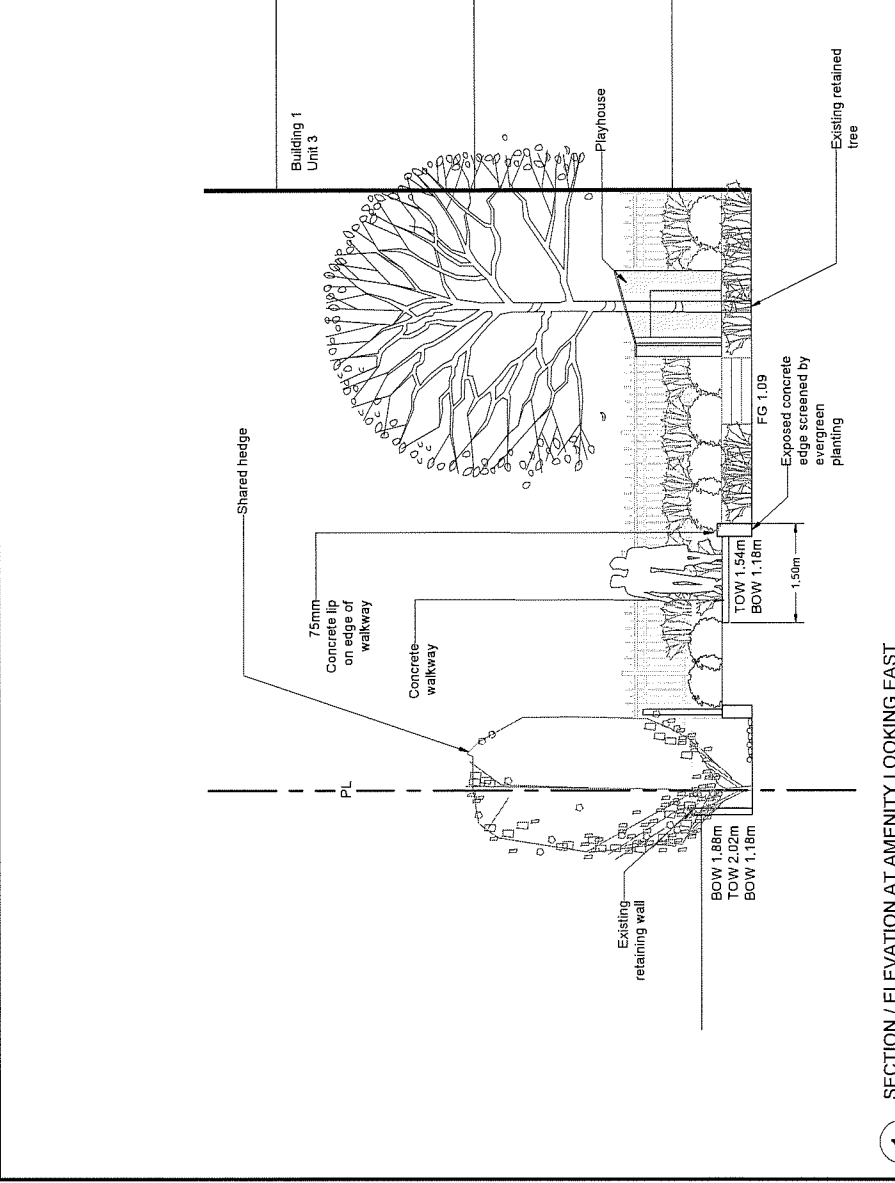
Formal Play Structure


Nature Play


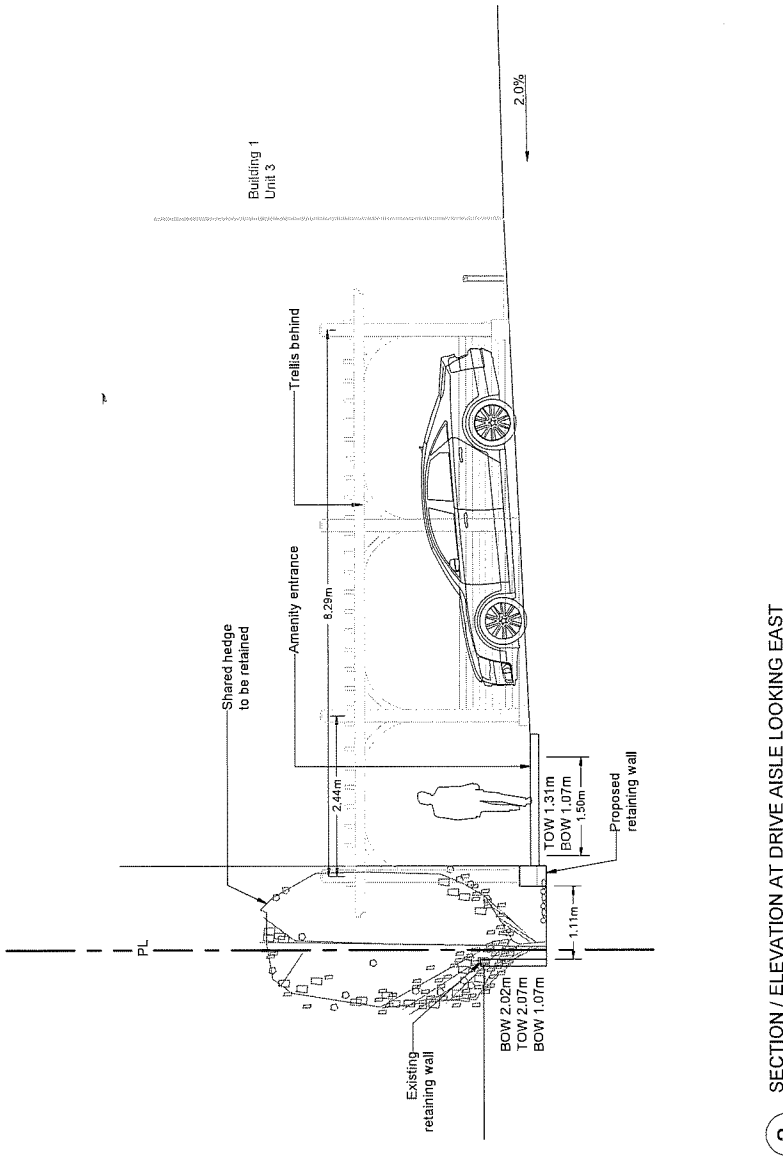


1:50
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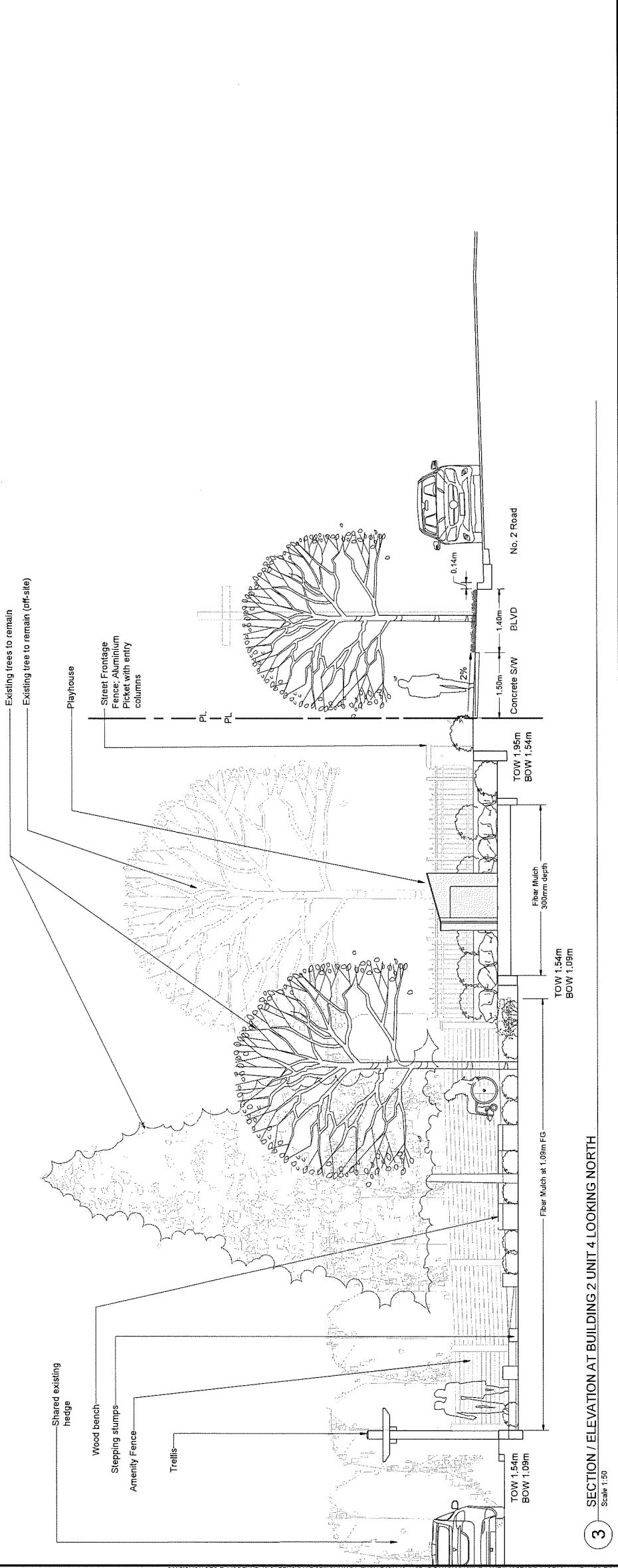
- Transition to Street Frontage Fence
- Existing Tree (Tag # 33) to Remain, typ.
- Existing column on neighboring property
- 1 Fiberglass planter "Large" Lawrence Rectangle by Atlas POTS or approved equivalent.
- 2 Natural basalt block bench
- 3 Concrete retaining wall at amenity area
- 4 Basalt boulder retaining wall
- 5 Flagstone
- 6 Forest Villa Playhouse
- 7 Fiberglass planter "Large" Lawrence Rectangle by Atlas POTS or approved equivalent.
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1 SECTION / ELEVATION AT AMENITY LOOKING EAST
Scale 1:50



2 SECTION / ELEVATION AT DRIVE AISLE LOOKING EAST
Scale 1:50



3 SECTION / ELEVATION AT BUILDING 2 UNIT 4 LOOKING NORTH
Scale 1:50



LANDSCAPE SECTIONS
Drawing Title:

DP2017-67
VDZ Project #

3F
Drawing #

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11	LJ	Re-issued for DP	June 7, 2019
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6	MES	Re-issued for DP	Nov 23, 2018
5	MES	Client Review	Oct 24, 2018
4	MES	Issued for DP	July 13, 2018
3	MES	Issued for Re-zoning	Jan 11, 2018
2	MES	Client Review	Dec 12, 2017
1	MES	Client Review	Nov 24, 2017
No.	By	Description	Date

REVISIONS TABLE FOR DRAWINGS
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for Woodward Williams Limited
Partnership
Location:
10451 - 10491 No. 2 Road
Richmond, BC

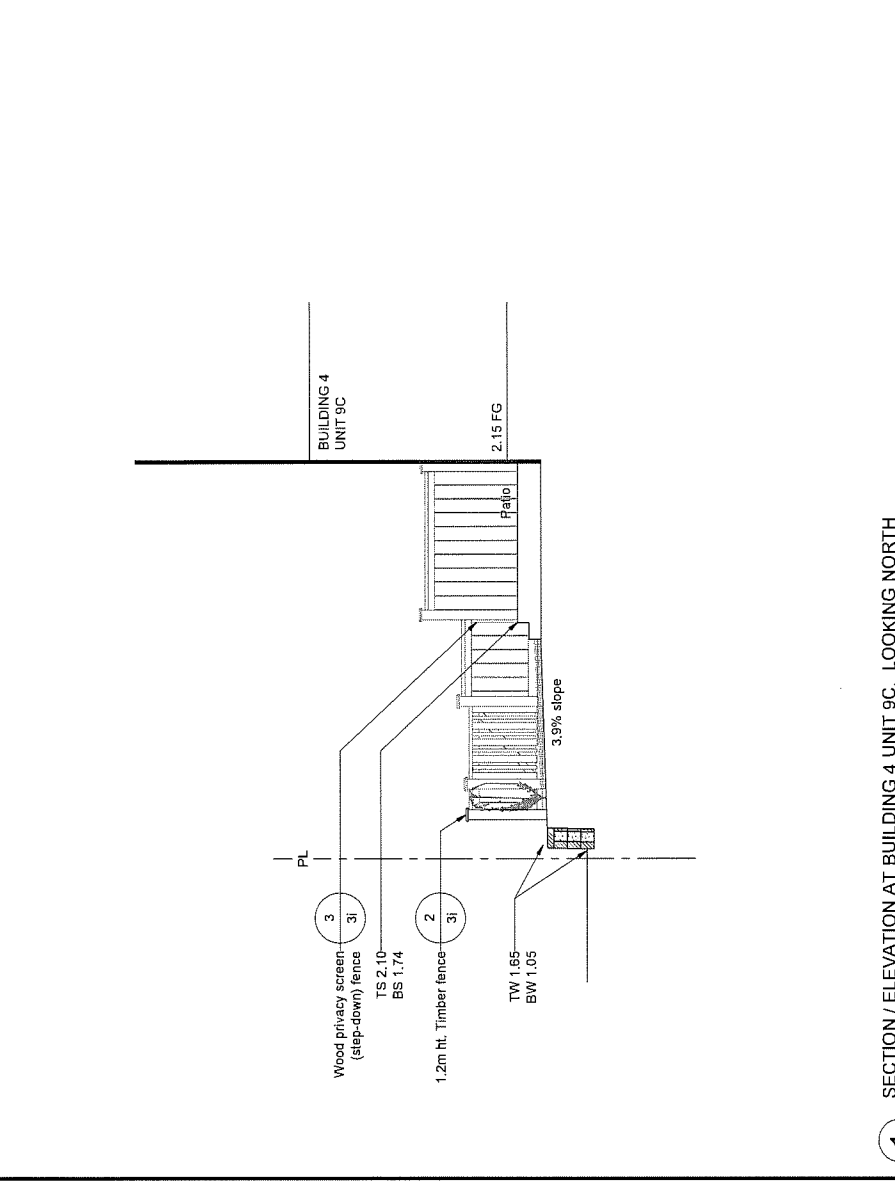
Drawn: MJS
MJS
Checked: SH
SH
Approved: MVDZ
MVDZ
Original Sheet Size: 24"x36"
Scale: 1:50

Stamp: [Signature]

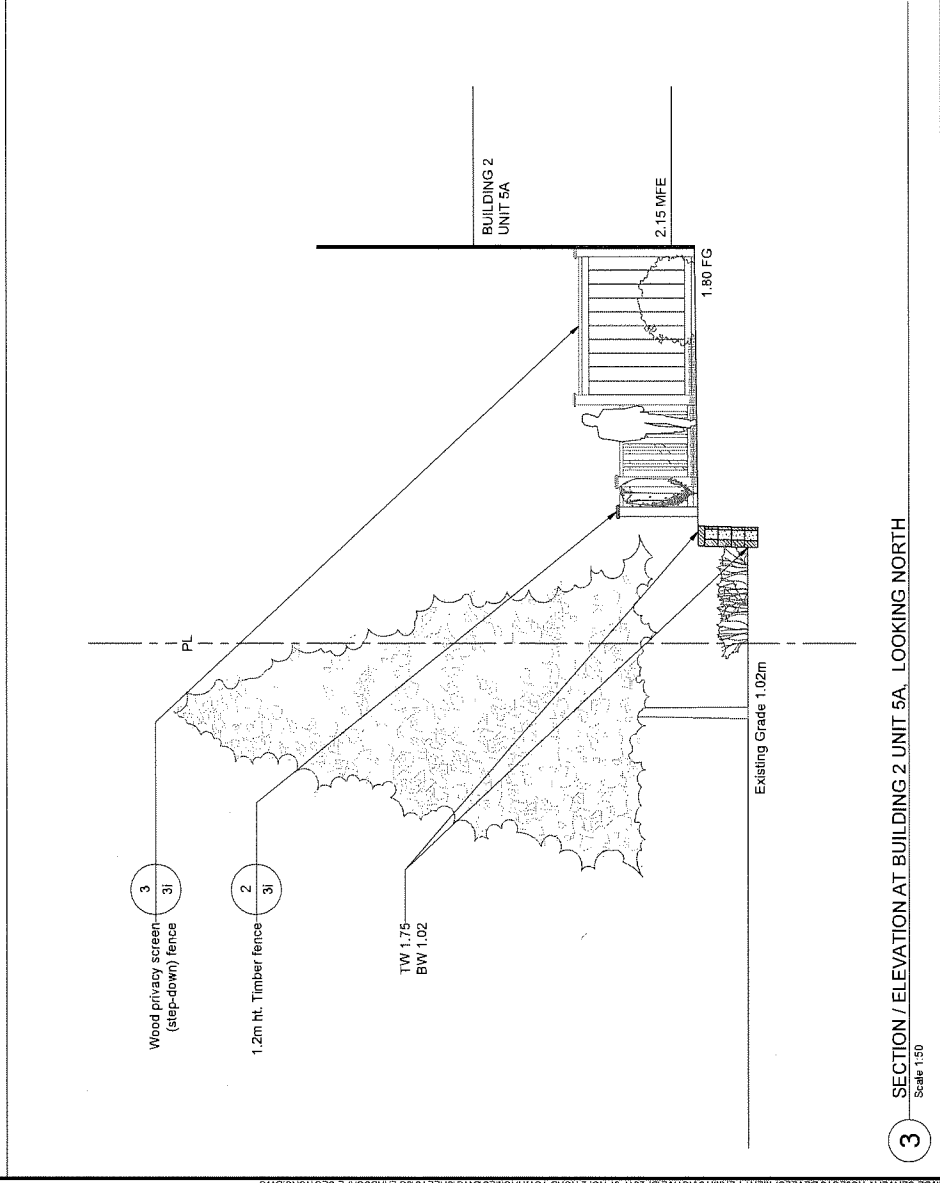
CONTRACTOR SHALL CHECK ALL DIMENSIONS TO THE CONSULTANT BEFORE PROCEEDING TO THE FIELD. THE CONSULTANT SHALL BE RESPONSIBLE FOR THE EXISTENCE AND LOCATION OF ALL UTILITIES AND SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS. DIMENSIONS MUST BE FIELD FOR TRACK/CONSTRUCTION.

AUG 06 2019 DP 18-829241

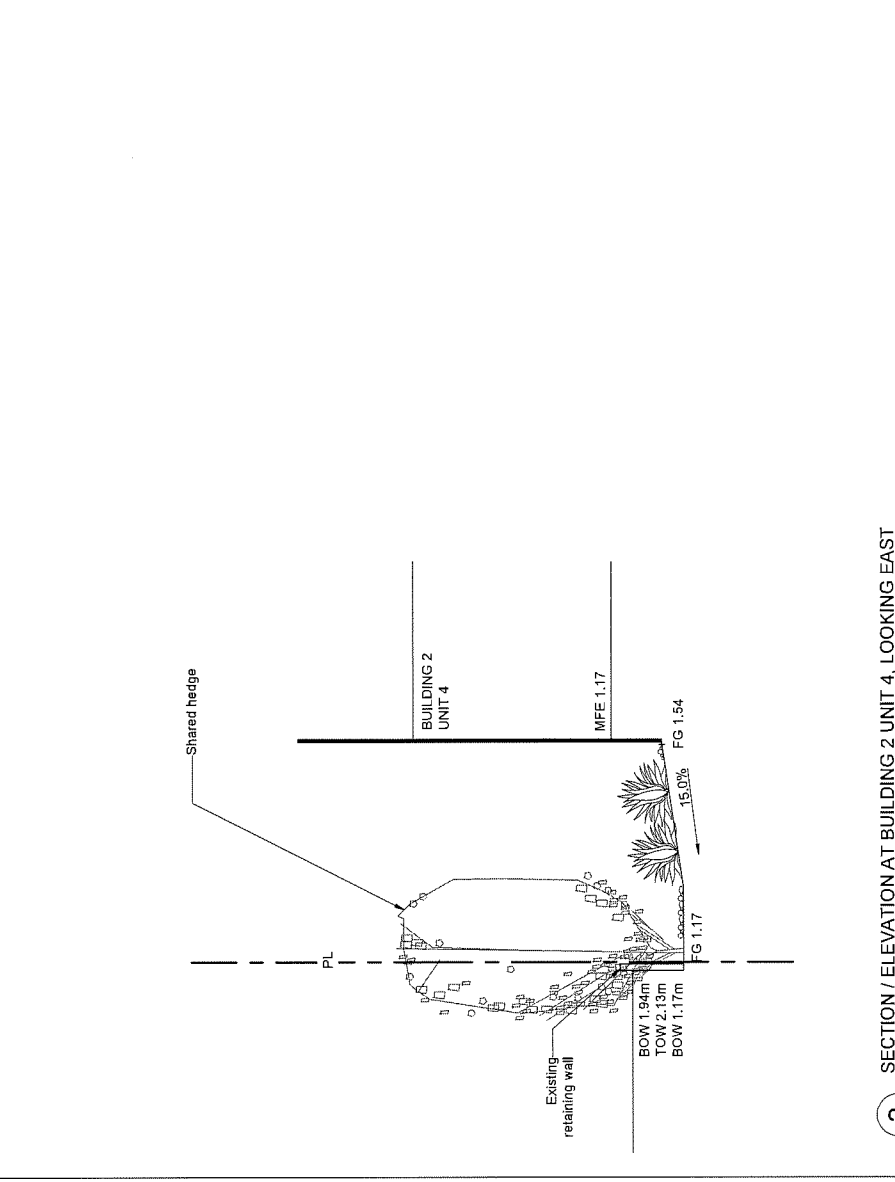
PLAN 3F



1 SECTION / ELEVATION AT BUILDING 4 UNIT 9C, LOOKING NORTH
Scale: 1:50



3 SECTION / ELEVATION AT BUILDING 2 UNIT 5A, LOOKING NORTH
Scale: 1:50



2 SECTION / ELEVATION AT BUILDING 2 UNIT 4, LOOKING EAST
Scale: 1:50



LANDSCAPE SECTIONS
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DP2017-67
VDZ Project #

3G
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4	MSS	Issued for DP	July 13, 2018
3	MSS	Issued for Reasoning	Jan. 11, 2018
2	MSS	Client Review	Dec. 12, 2017
1	MSS	Client Review	Nov. 24, 2017

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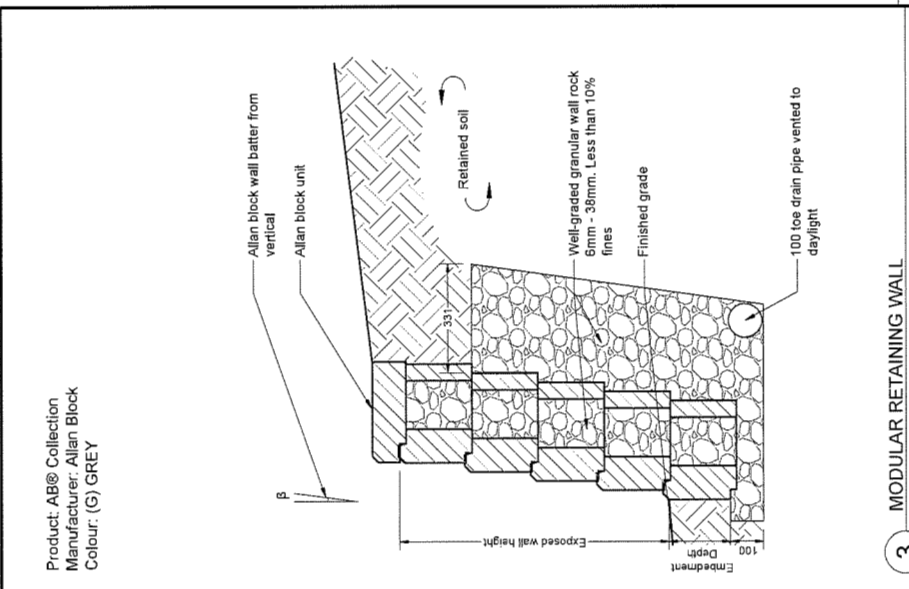
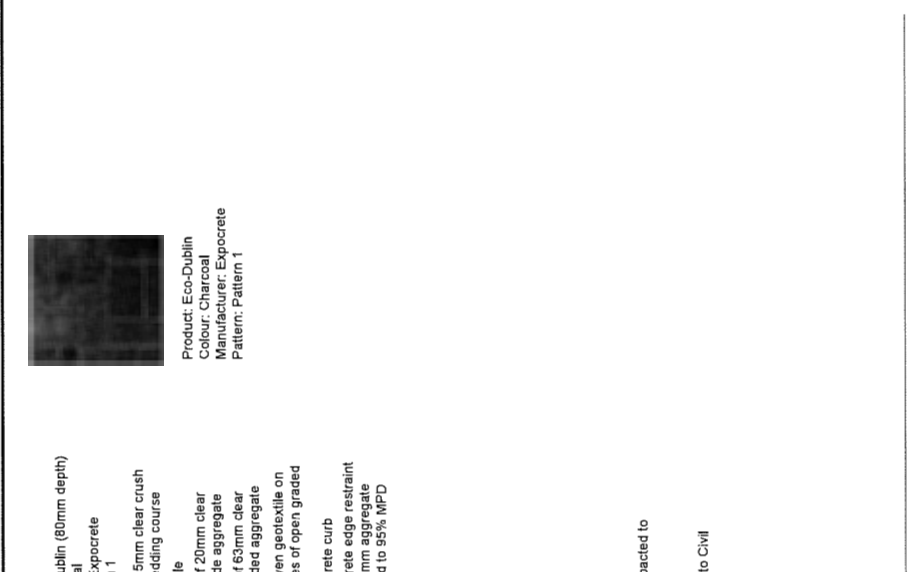
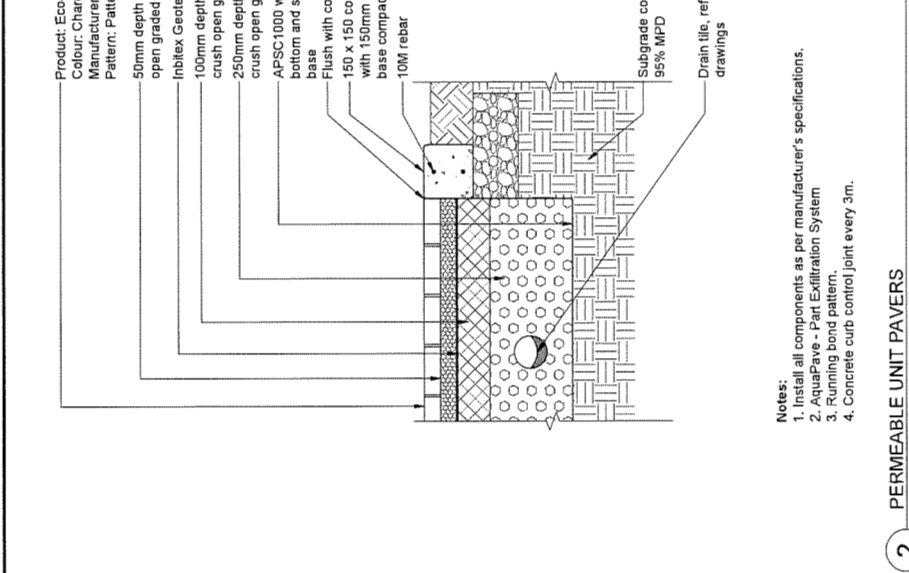
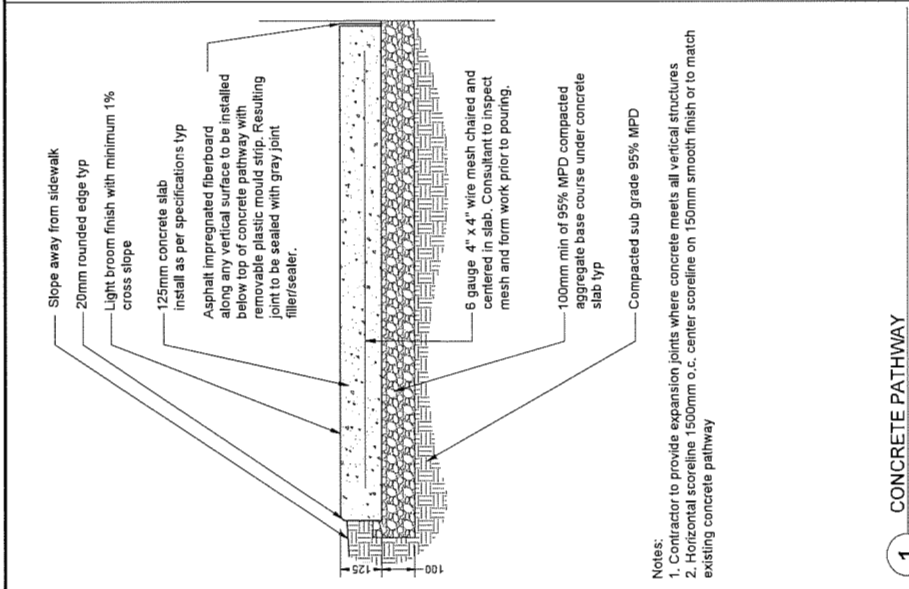
Project:
Proposed Multi-Family Residential
for Woodward Williams Limited
Partnership
Location:
10451 - 10491 No. 2 Road
Richmond, BC

Drawn: MSS
MSS
Checked: SH
Approved: MYDZ
Scale: 1:50
Original Sheet Size: 24"x36"

Stamp:

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18-829241 DP AUG 06 2019 PLAN 3G



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www.vdz.ca

REVISIONS TABLE FOR DRAWINGS

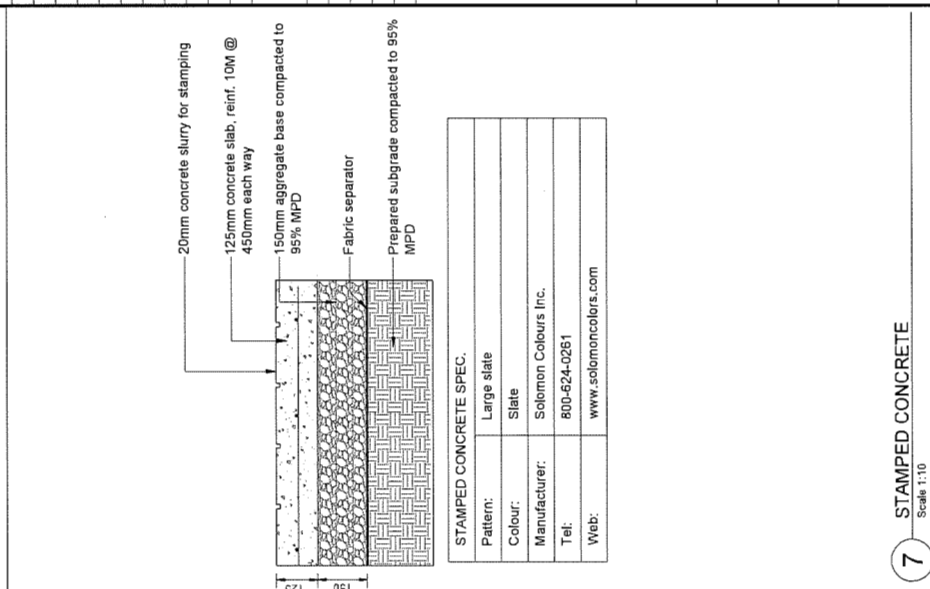
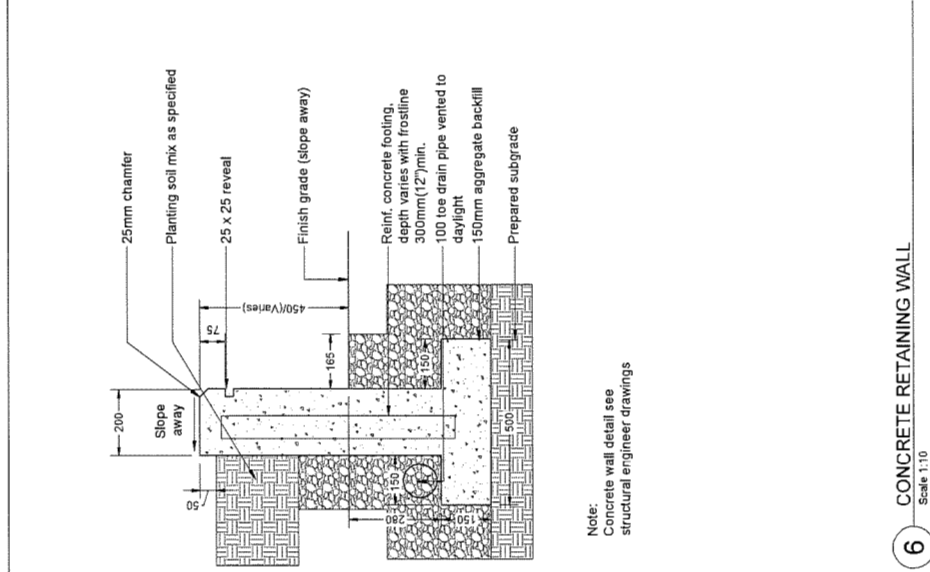
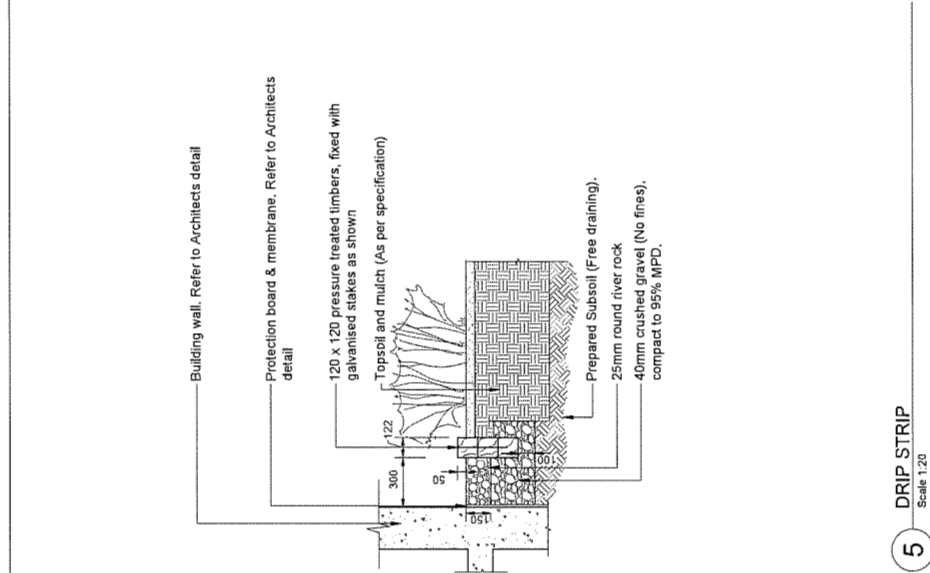
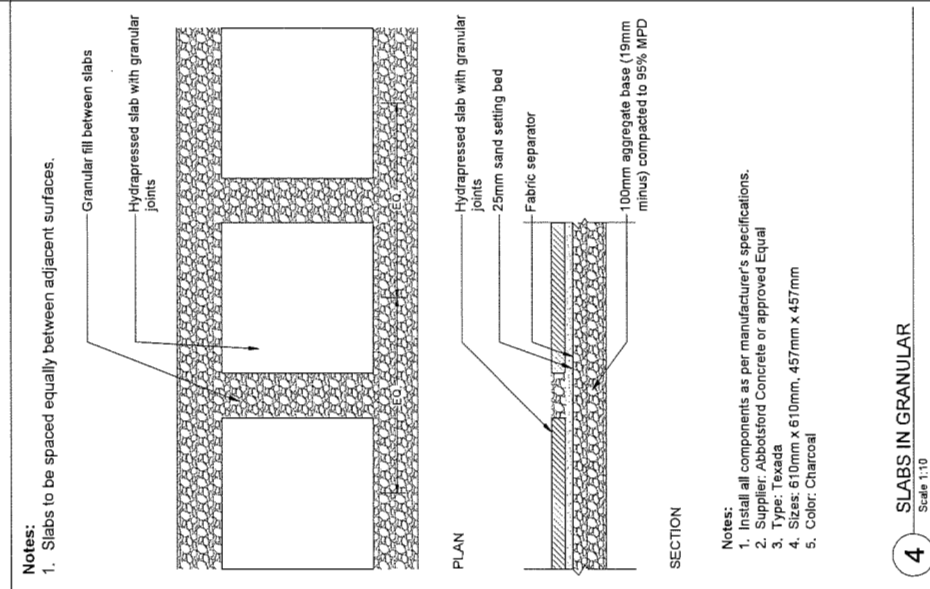
No.	By	Description	Date
11	LU	Re-issued for OP	June 7, 2019
10	MDS	Re-issued for OP	Apr. 23, 2019
9	SH	Re-issued for OP	Mar. 26, 2019
8	MDS	Re-issued for OP	Feb. 19, 2019
7	MDS	Issued for ADP	Dec. 20, 2018
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5	MDS	Client Review	Oct. 24, 2018
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3	MDS	Issued for Receiving	Jan. 11, 2018
2	MDS	Client Review	Dec. 12, 2017
1	MDS	Client Review	Nov. 24, 2017

REVISIONS TABLE FOR SHEET

No.	By	Description	Date

Project:
Proposed Multi-Family Residential
for Woodward Williams Limited
Partnership
Location:
10451 - 10491 No. 2 Road
Richmond, BC

VDZ Project #: DP2017-67
Drawing Title: LANDSCAPE DETAILS
Drawing #: 3H



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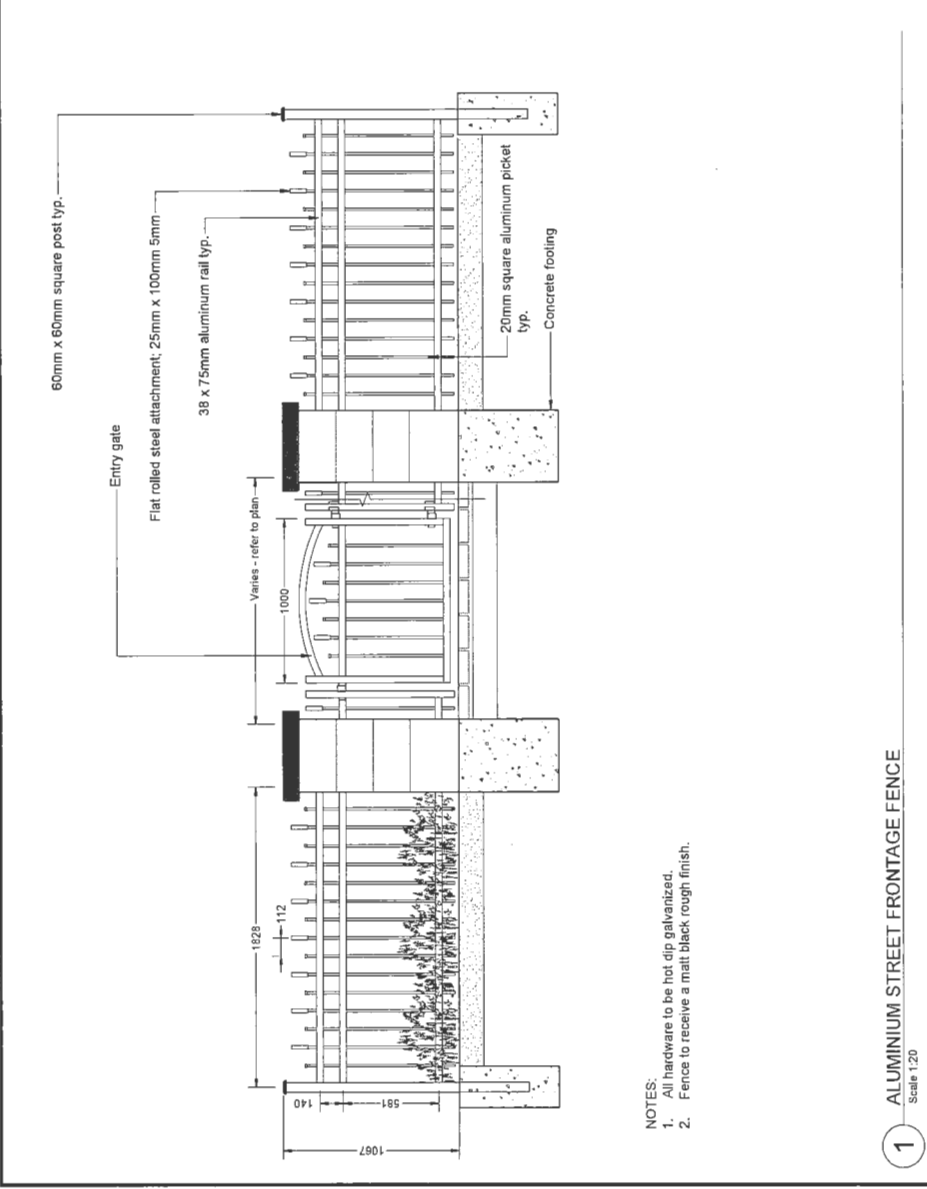
No.	By	Description	Date
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1	MDS	Client Review	Nov. 24, 2017

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No.	By	Description	Date

Project:
Proposed Multi-Family Residential
for Woodward Williams Limited
Partnership
Location:
10451 - 10491 No. 2 Road
Richmond, BC

VDZ Project #: DP2017-67
Drawing Title: LANDSCAPE DETAILS
Drawing #: 3H



NOTES:

1. All hardware to be hot dip galvanized.
2. Fence to receive a matt black rough finish.

1 ALUMINIUM STREET FRONTAGE FENCE

Scale 1:20

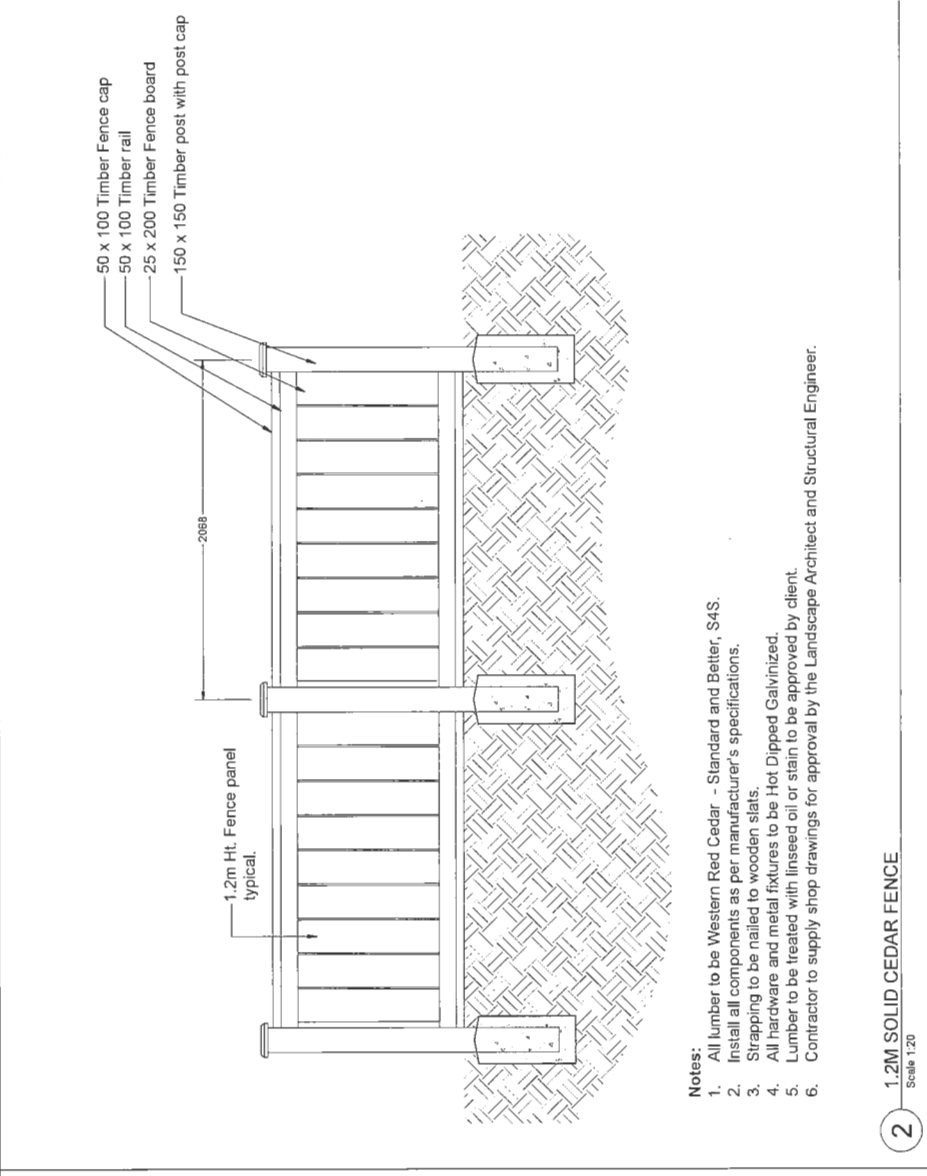


NOTES:

1. All lumber to be Western Red Cedar - Standard and Better, S4S.
2. Install all components as per manufacturer's specifications.
3. Strapping to be nailed to wooden slats.
4. All hardware and metal fixtures to be Hot Dipped Galvanized.
5. Lumber to be treated with linseed oil or stain to be approved by client.
6. Contractor to supply shop drawings for approval by the Landscape Architect and Structural Engineer.
7. 1838mm fence panel directly adjacent to unit between units where no privacy screen provided by Architecture.

3 WOOD PRIVACY SCREEN (STEP DOWN) FENCE

Scale 1:20

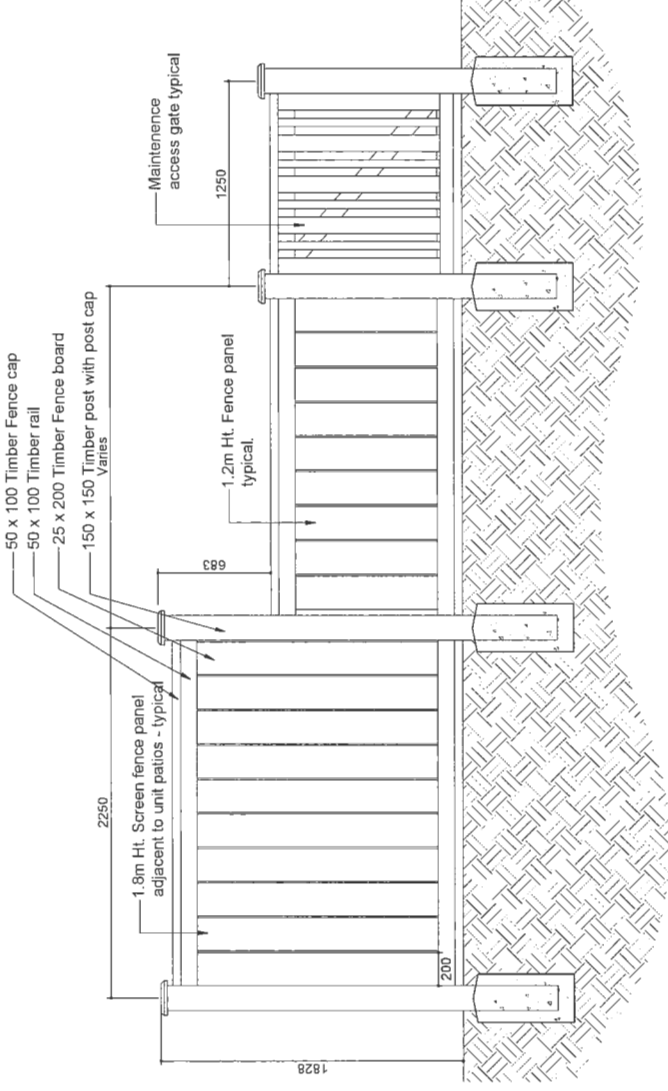


NOTES:

1. All lumber to be Western Red Cedar - Standard and Better, S4S.
2. Install all components as per manufacturer's specifications.
3. Strapping to be nailed to wooden slats.
4. All hardware and metal fixtures to be Hot Dipped Galvanized.
5. Lumber to be treated with linseed oil or stain to be approved by client.
6. Contractor to supply shop drawings for approval by the Landscape Architect and Structural Engineer.

2 1.2M SOLID CEDAR FENCE

Scale 1:20



NOTES:

1. All lumber to be Western Red Cedar - Standard and Better, S4S.
2. Install all components as per manufacturer's specifications.
3. Strapping to be nailed to wooden slats.
4. All hardware and metal fixtures to be Hot Dipped Galvanized.
5. Lumber to be treated with linseed oil or stain to be approved by client.
6. Contractor to supply shop drawings for approval by the Landscape Architect and Structural Engineer.
7. 1838mm fence panel directly adjacent to unit between units where no privacy screen provided by Architecture.

3 WOOD PRIVACY SCREEN (STEP DOWN) FENCE

Scale 1:20

VDZ+A
LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | DESIGN FACILITY
FORT LANGLEY STUDIO | WOODWARD RESIDENTIAL STUDIO
300-3333 Church St. | 100-5555 Kingsport
Fort Langley, BC | Vancouver, BC
V3M 2B8 | V5T 3J7 | 604-882-0824
www.vdz.ca

LANDSCAPE DETAILS

Drawing Title: **DP2017-67**

VDZ Project #: **31**

Drawing #: **31**

No.	By	Description	Date
11	LU	Re-issued for DP	June 7, 2019
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1	MJS	Client Review	Nov 24, 2017

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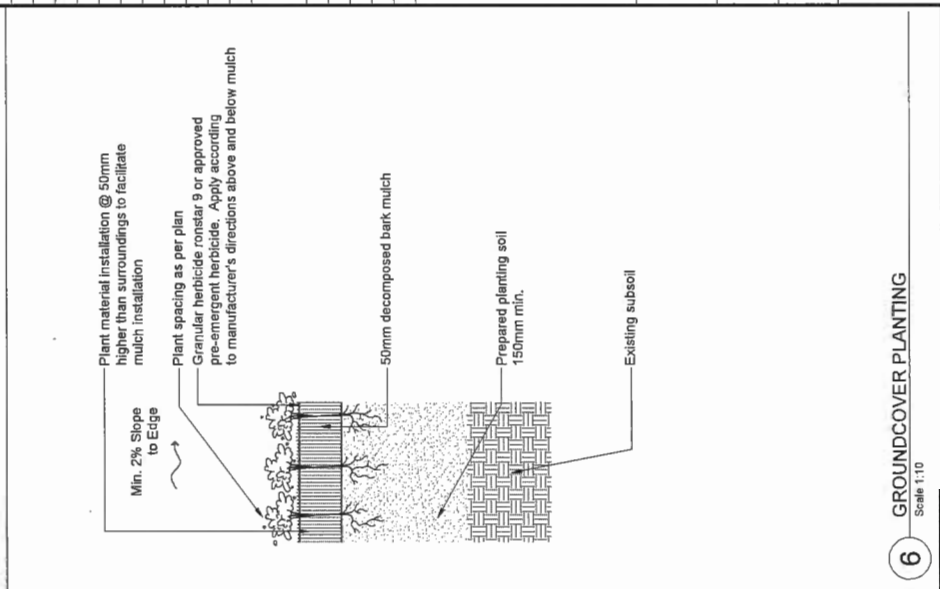
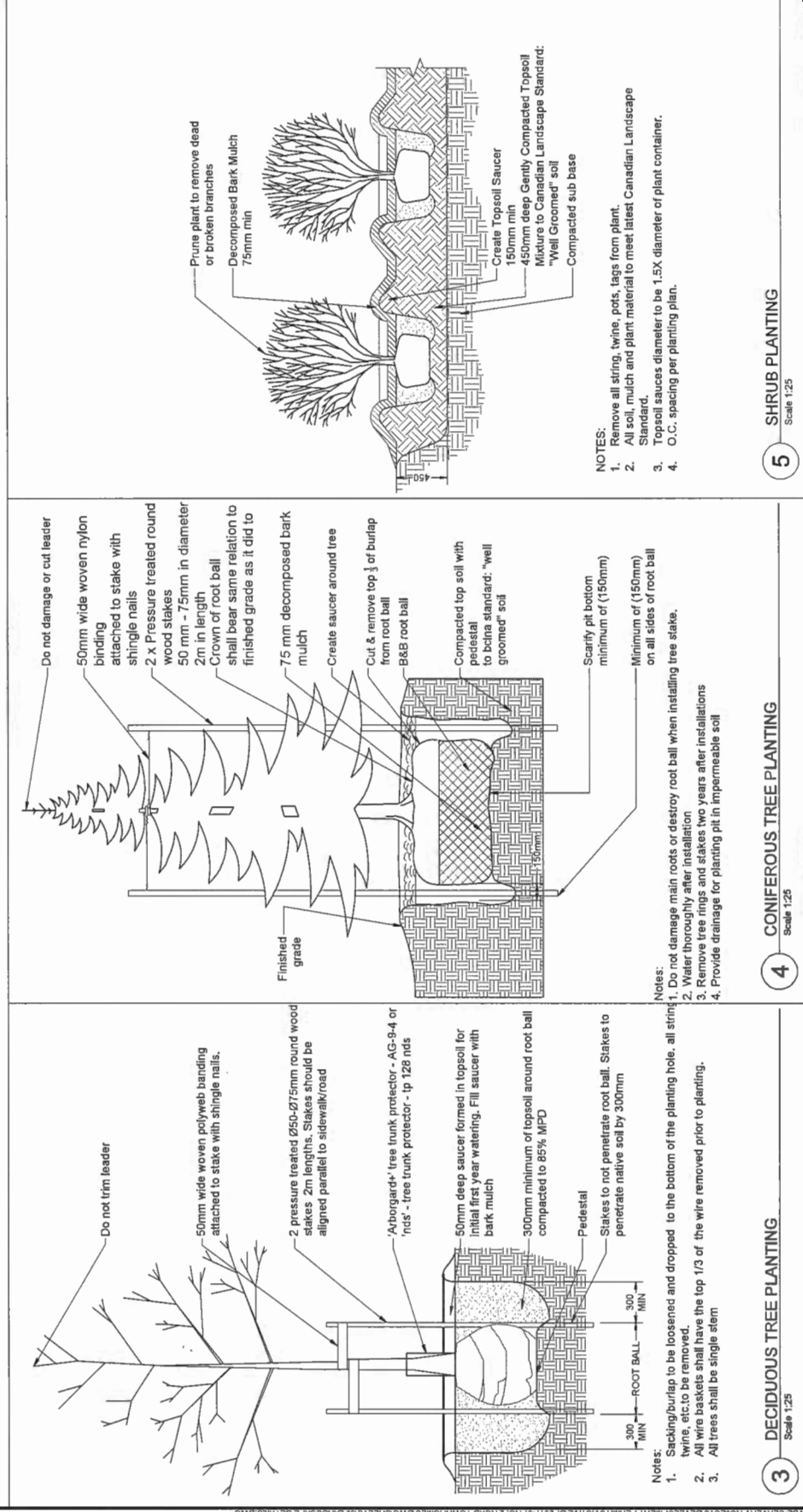
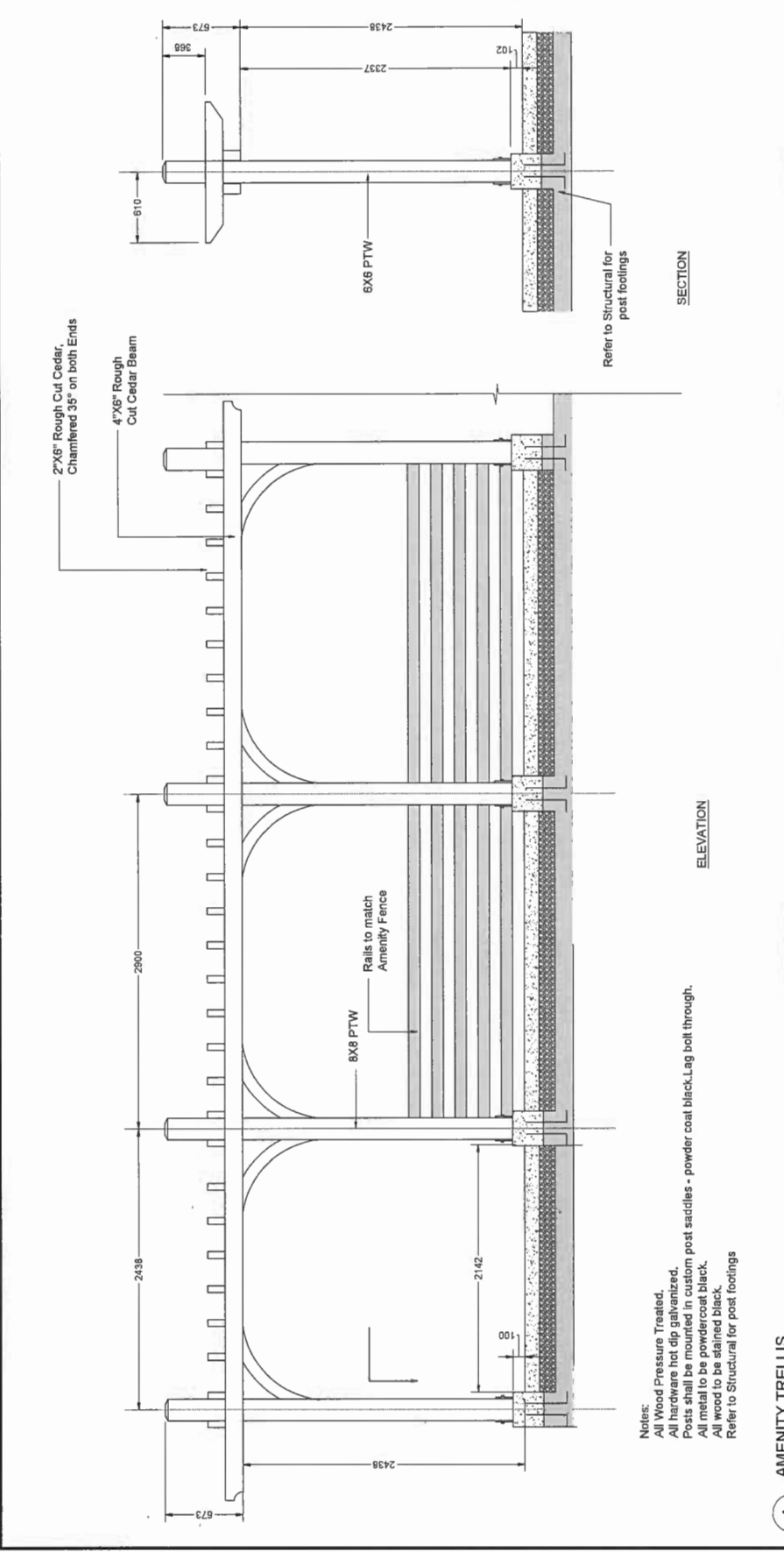
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REVISIONS TABLE FOR SHEET			

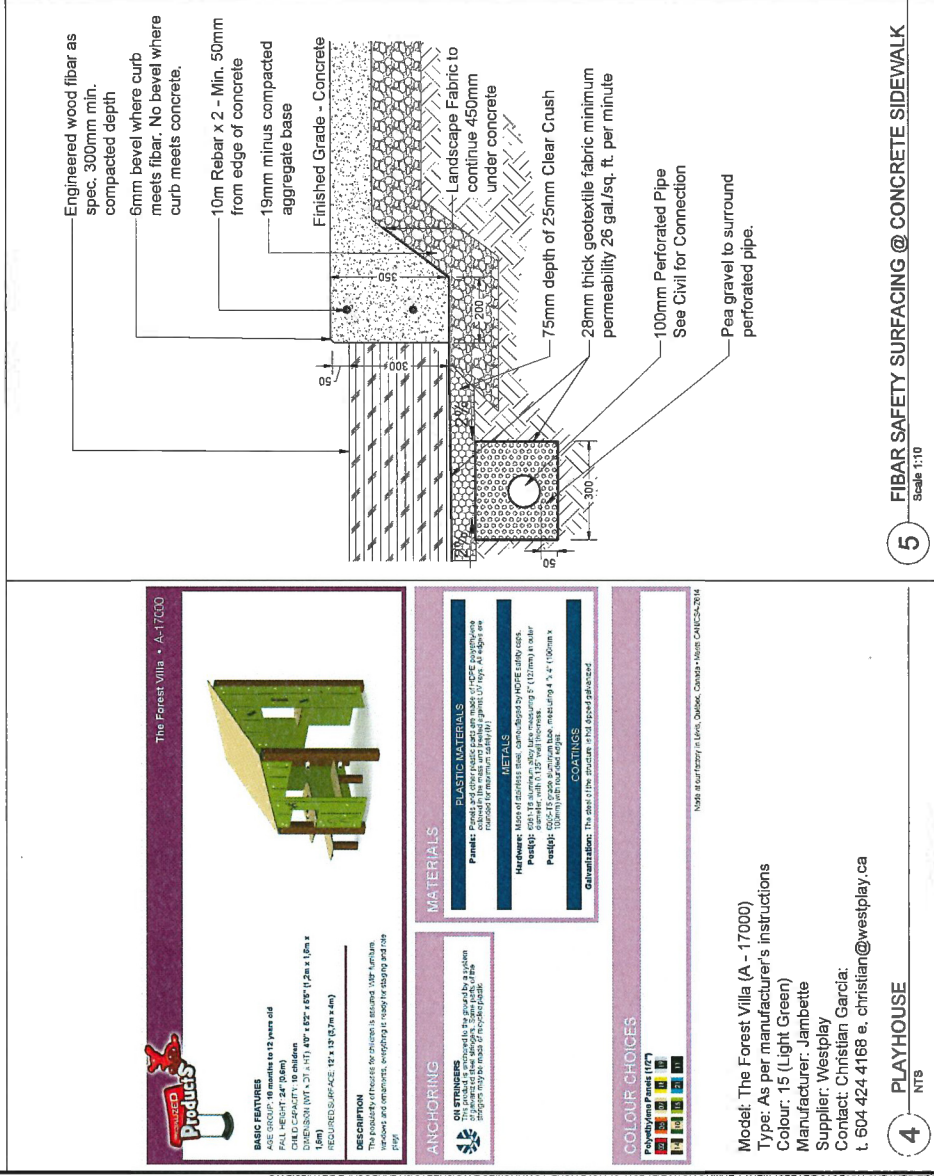
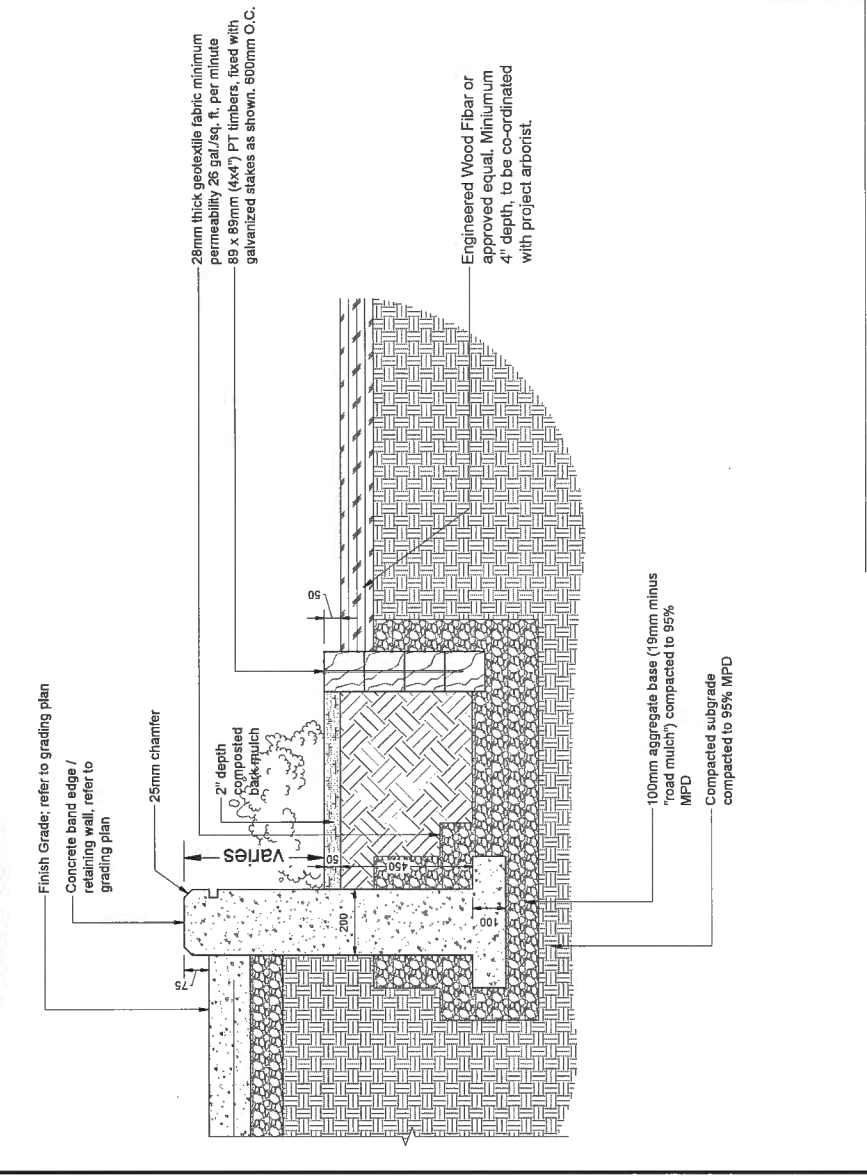
Project:
Proposed Multi-Family Residential
for Woodward Williams Limited
Partnership
Location:
10451 - 10491 No. 2 Road
Richmond, BC

Drawn: MJS
Checked: SH
Approved: MVDZ
Original Sheet Size: 24"x36"

Scale:
AS SHOWN

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Natural Basalt Block Benches
LANDSCAPE & STONE SUPPLY

Specifications:
Basalt Block Bench
Typical Depth: 20" - 30"
Typical Length: 48" - 74"
Typical Height: 19" - 24"
Typical Weight Range: 2200 - 3600 lbs

We can create this bench according to your specifications and requirements. We have found through trial & error that a machine sawn bench provides the most stability with the least amount of settling.

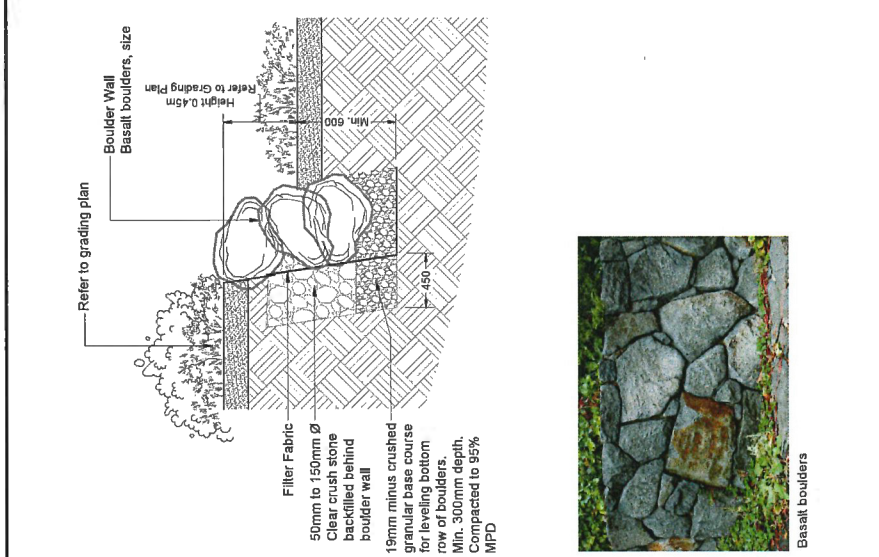
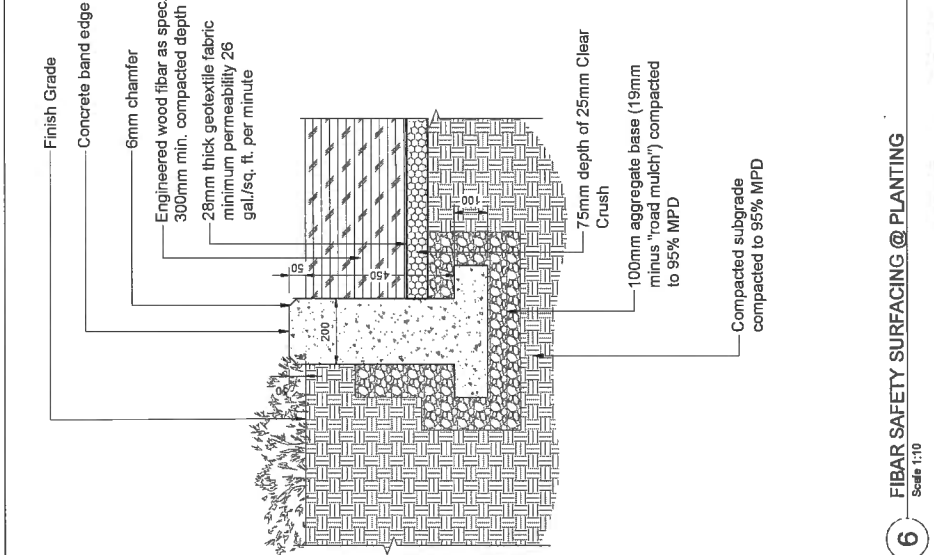
Characteristics:
Our natural stone benches are made from indigenous Basalt. Locally quarried in the Squamish Basin, British Columbia, Machine sawn top and bottom create these stunning site furnishings.

Description & Color:
Depending on the quarry, Basalt is typically mostly grey in color with brown, gold and tan variations. These benches are machine sawn on all sides with some moss and lichen present. The color of the sawn is typically grey in colour.

Packaging & Delivery:
Our Basalt Block Benches are typically packaged individually on pallets depending on weight.

NATURAL STONE ADDS UNFINISHED ELEGANCE
Basalt Block Bench

Distributed by:
NORTHWEST LANDSCAPE & STONE SUPPLY
www.landscapesupply.ca
3633 Byrne Road
Richmond, BC V6V 2K1
Tel: 604-486-8443
Fax: 604-486-8443
www@landscapesupply.com



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No.	By:	Description	Date
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1	MJS	Client Review	Nov 24, 2017

REVISIONS TABLE FOR SHEET

No.	By:	Description	Date

Project:
Proposed Multi-Family Residential for Woodward Williams Limited Partnership
Location: 10451 - 10491 No. 2 Road, Richmond, BC

Drawn: MJS
Checked: SH
Approved: MVDZ
Scale: AS SHOWN
Original Sheet Size: 24"x36"

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Basalt Risers and Treads

Specifications:
 Product Code: BRT (Bulk), BRTF (Palletized)
 Thickness: 8" - 9"
 Width: 24" - 36"
 Length: 24" - 36"
 Common Uses: Stair risers and stair treads

Note: Bulk or selected loads available.
 All measurements and coverage are approximate.

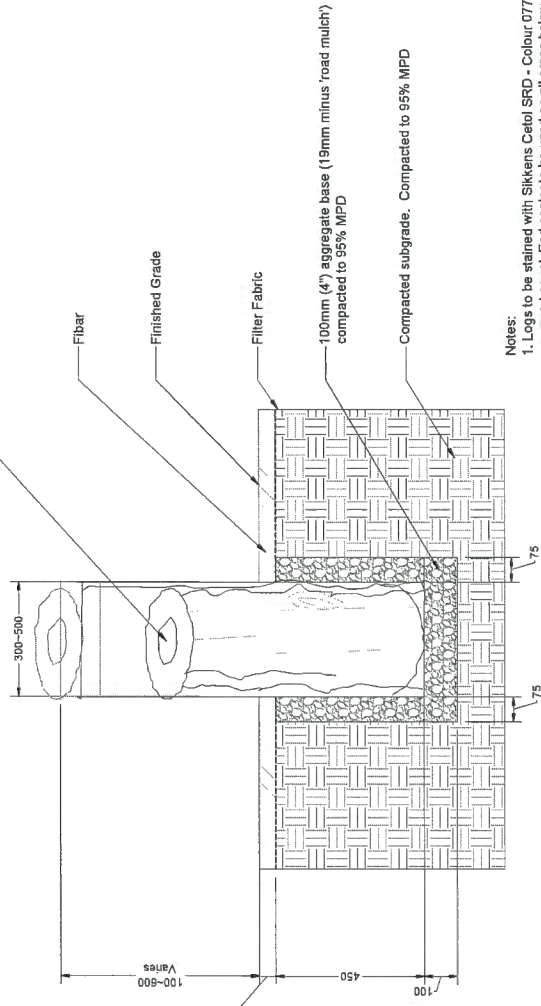
Characteristics:
 Basalt Risers and Treads are made from natural basalt slabs or can be used side by side to create stunning variations between situations.

Description & Color:
 Basalt Risers and Treads are made from natural basalt slabs or can be used side by side to create stunning variations.

Basalt Risers and Treads are packaged in wire baskets on pallets with a net weight of approximately 3000 pounds per pallet. Basalt Risers are 18" wide and 8" high. Basalt Treads are 24" wide and 8" high. Bulk orders are delivered on flatbed or dump trucks.

NORTHWEST LANDSCAPE & STONE SUPPLY
 6628 Byrne Road
 Burnaby, BC V5L 2J1
 Tel: 604.298.9443
 Fax: 604.298.9442
 Toll Free: 1.800.667.7788
 www.nwlandscape.com

1 BASALT STEPS
NTS



5 STEPPING LOGS
Scale 1:10

Notes:
 1. Logs to be stained with Sikken's Cetol SRD - Colour 077-Cedar or approved equal. End sealer to be used on all areas below grade. Apply as per manufacturer's specifications.
 2. Size of log uprights as per plan.
 3. All branches and trunk splits to be cut flush, routed and sealed. Bark to remain intact.

2 MAGLIN BENCH
NTS

MAGLIN
 10000 150th Street, Surrey, BC V3R 1G5
 Tel: 604.273.3333
 Fax: 604.273.3334
 Email: info@maglin.com

Model: Ogden OGM1900-SCR3-WFB-29
 Inner Radius: 29"
 Mount: Square Legs
 Material: IPE Wood

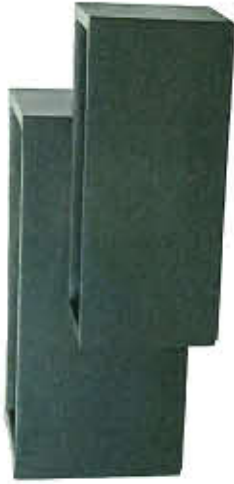
Contact: RHEZIE EMIA
 MAGLIN SITE FURNITURE
 T 800 716 5506 X 1187

3 COLOURED CONCRETE CIRCLES @ DRIVE AISLE
NTS



Acrylic concrete paint shapes
 Concrete paint to be Star spectrum acrylic based vibrantly coloured speciality coating (or approved substitute).
 Colours: Sky Blue, Cool Grey

6 LAWRENCE RECTANGLE PLANTER
NTS



Fiberglass planter
 Model: "Large" Lawrence Rectangle
 Dimensions: 45cm x 45cm x 100cm (18" wide x 18" tall x 40" long)
 Manufacture: Alias Pots or approved equivalent.
 Colour: Charcoal
 Quantity: 4

Contact: Alias Pots 604-285-4873
 1087 Churchill Crescent, North
 Vancouver, BC V7P 1P9 Canada

7 BIKE RACK
NTS

MAGLIN
 10000 150th Street, Surrey, BC V3R 1G5
 Tel: 604.273.3333
 Fax: 604.273.3334
 Email: info@maglin.com

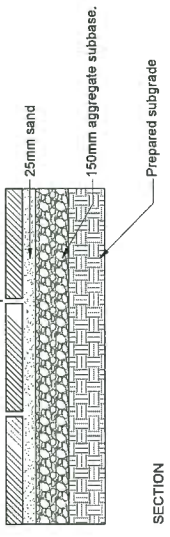
MBR100-DB

MATERIALS: The fix such as mentioned being U.S. steel bar and an aluminum pipe.
 FINISH: All steel components are protected with a Coat-100 paint.
 INSTALLATION: The fix such as delivered per attached.
 TO PEOPLE: Select MBR100-DB
 - Installation Code
 - Supplier Name (MAGLIN000-DB)

Dimensions:
 DIA: 116mm
 H: 55mm
 W: 116mm

Bike Rack, Direct Burial - MBR100-DB
 Colour: Powdercoat Black

4 FLAGSTONE
Scale 1:10



SECTION
 FLAGSTONE
 Stone: Trout Creek 2'-3", Pallo Flagstone
 Joints in Sand Set: 10-15mm
 Supplier: Northwest Landscape Supply

REVISIONS TABLE FOR DRAWINGS

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3	MMS	Issued for Reconciling	Jan 11, 2018
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REVISIONS TABLE FOR SHEET

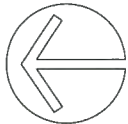
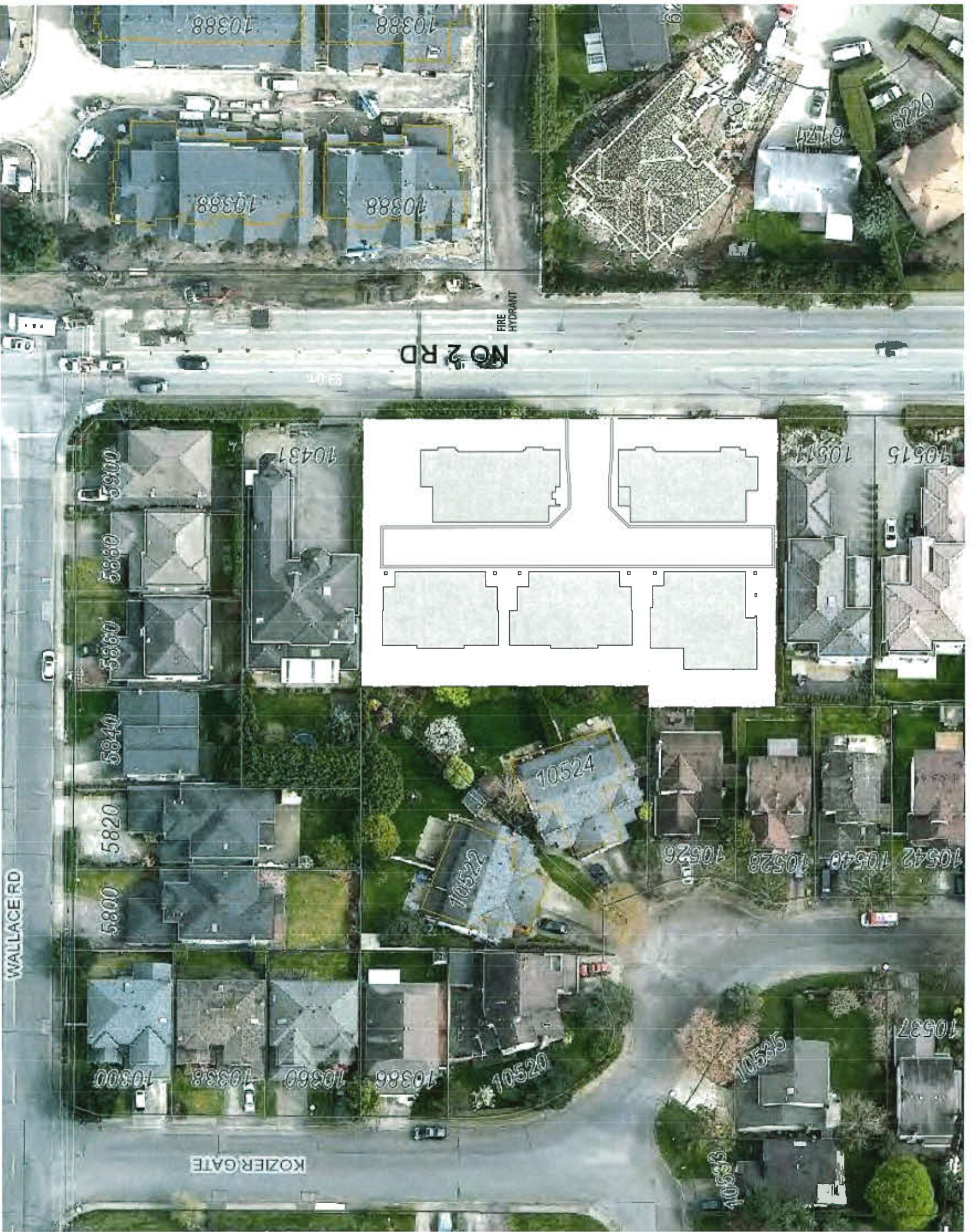
No.	By:	Description	Date

Project:
 Proposed Multi-Family Residential
 for Woodward Williams Limited
 Partnership
 Location:
 10451 - 10491 No. 2 Road
 Richmond, BC

Drawn: MMS
 Checked: SH
 Approved: MVDZ
 Scale: AS SHOWN
 Original Sheet Size: 24"x36"

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 102-9181 Church St. 102-555 Kingsway
 Fort Langley, BC Vancouver, BC
 1.781.208.1571 604.892.0024
 www.vdz.ca



**10451-10491
NO. 2 ROAD**

for
Woodward Williams
Limited Partnership

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CONTEXT PLAN



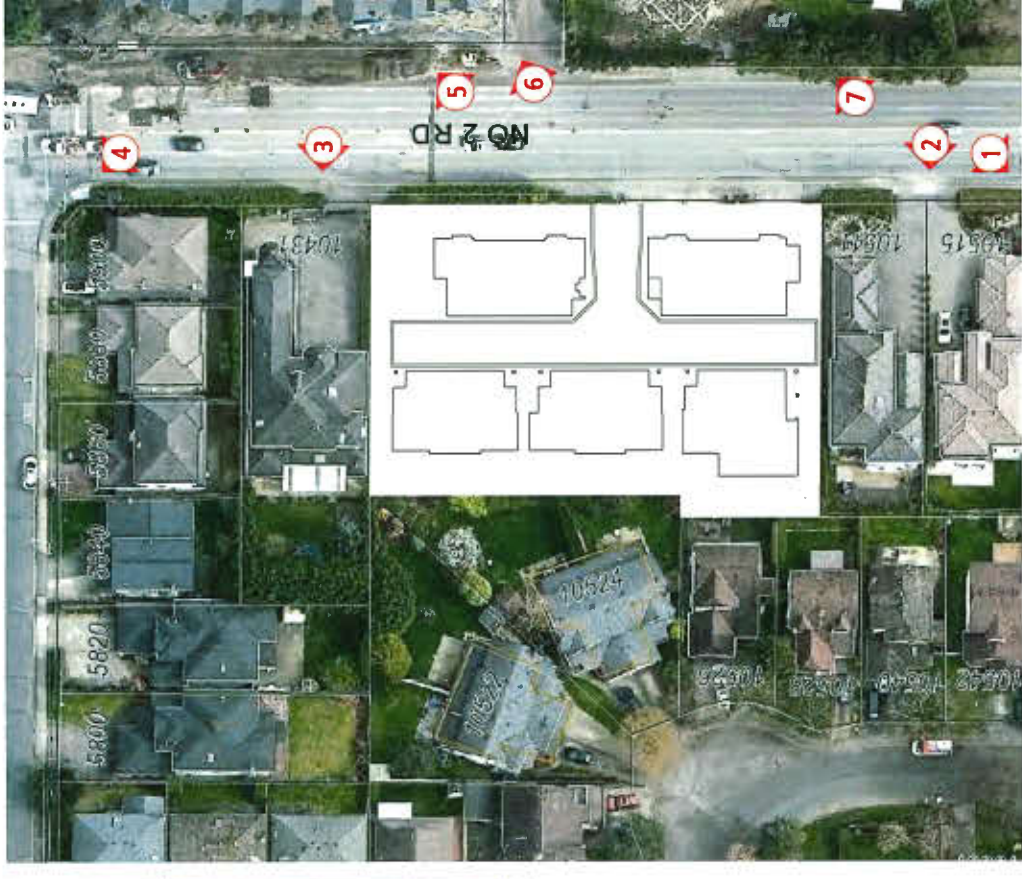
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APRIL 15, 2019



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AUG 06 2019



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CONTEXT PHOTOS NTS

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architecture inc.
BRITISH COLUMBIA • ALBERTA • WASHINGTON
200 - 2405 Quebec Street
Vancouver BC V5T 4L5
604.873.2907
fougerearchitecture.ca



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10515 No 2 Road

R

10511 No 2 Road

R

PROPOSED DEVELOPMENT

R

10431 No 2 Road

10451-10491
NO. 2 ROAD

for
Woodward Williams
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STREETSCAPE

SCALE: 1:150



DP 18-829241
APRIL 15, 2019

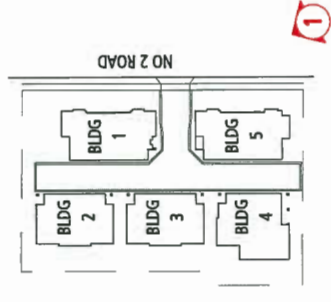


FOUGERE
architecture inc.
BRITISH COLUMBIA • ALBERTA • WASHINGTON
202-2425 Quebec Street • 604.871.2007
Vancouver, BC V6T 4L5 fougerearchitecture.ca

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VIEW 1



KEY PLAN

**10451-10491
NO. 2 ROAD**

for
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PERSPECTIVES

DP 18-829241
APRIL 15, 2019



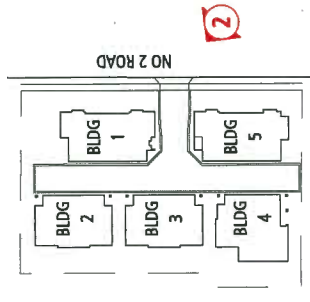
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PLAN

DP 18-829241

AUG 06 2019



VIEW 2



KEY PLAN

10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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PERSPECTIVES

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APRIL 15, 2019



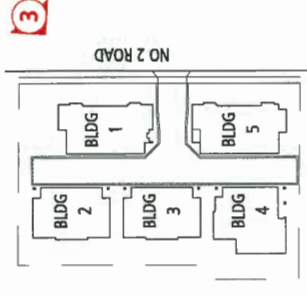
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VIEW 3



KEY PLAN

10451-10491
NO. 2 ROAD

for
Woodward Williams
Limited Partnership

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PERSPECTIVES

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