



City of
Richmond

Report to Development Permit Panel

To: Development Permit Panel

Date: April 20, 2023

From: Wayne Craig
Director, Development

File: DP 21-943565

Re: **Application by Orion Construction for a Development Permit at
9800 Van Horne Way**

Staff Recommendation

That a Development Permit be issued which would permit the construction of two multi-tenant light industrial buildings at 9800 Van Horne Way on a site zoned "Light Industrial (IL)".

Wayne Craig
Director, Development
(604-247-4625)

WC:bb
Att. 4

Staff Report

Origin

Orion Construction, on behalf of the owner of the subject property, has applied to the City of Richmond for permission to develop two industrial buildings on the subject lot at 9800 Van Horne Way. The owner intends to sell the buildings as strata titled units after completion of the project. The development site is currently zoned “Light Industrial (IL)” and contains one industrial building as well as accessory storage uses.

A Servicing Agreement is required prior to Development Permit issuance, and includes, but is not limited to, the following improvements:

- decommissioning and replacement of the existing water main located along the Van Horne Way frontage;
- decommissioning and replacement of the existing sanitary main along the Van Horne Way frontage;
- decommissioning and replacement of the storm sewer main located along the Van Horne Way frontage; and,
- frontage improvements to both River Drive and Van Horne Way including the undergrounding of utilities, street lighting as well as widening of the existing boulevard from 1.0m to 1.5m along Van Horne Way.

Development Information

Please refer to attached Development Application Data Sheet (Attachment 1) for a comparison of the proposed development data with the relevant Bylaw requirements.

Background

The subject site and the surrounding area are located in a predominantly industrial neighbourhood within the City Centre Area Plan. Development surrounding the subject site is as follows:

- To the North: Across River Drive, a “Light-Industrial (IL)” zoned property currently operated by South Coast BC Transportation Authority.
- To the South: The CP railway right-of-way (which is identified in the CCAP as a future City road), and further south, an industrial building offering mixed-employment uses on a site zoned ““Light-Industrial (IL)””.
- To the East: A “Light-Industrial (IL)” zoned property containing two warehouse buildings used for storing trailers. The property is subject to an active rezoning application currently at third reading (ZT 16-753545) to allow non-accessory vehicle parking. The property further east is owned and operated by BC Hydro and contains service utility infrastructure.
- To the West: Across Van Horne Way, two “Light-Industrial (IL)” zoned properties used by South Coast BC Transportation Authority.

Related Policies and Studies

Aircraft Noise Sensitive Development Policy

The subject site is located within Aircraft Noise Areas 1A and 2, which would restrict all new noise sensitive uses. The proposed industrial use is not restricted, and noise mitigation is not required. Prior to issuance of the Development Permit, the applicant is required to register an aircraft noise covenant on Title to address public awareness of the potential impact of aircraft noise on the site.

Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of Richmond Flood Plan Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on Title is required prior to issuance of the Development Permit.

Site Disclosure Statement

The applicant must provide confirmation of compliance and approval from the Ministry of Environment with regard to site investigation for any contaminants due to previous activities on the property prior to the issuance of the Development Permit.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan, City Centre Area Plan and is generally in compliance with the “Light Industrial (IL)” zoning regulations.

Advisory Design Panel Comments

On February 8, 2023, the Advisory Design Panel supported the subject Development Permit application moving forward to Development Permit Panel, subject to consideration of the panel’s comments. The applicant has incorporated a number of design changes in response to the Panel’s comments, including:

- Commitment to install five (5) Electric Vehicle (EV) charging stations in the parking areas. The charging stations are distributed throughout the parking areas with two (2) stations west of Building A, one (1) to the south of Building A adjacent to an outdoor amenity area, and two (2) west of Building B along the central drive aisle between the two buildings.
- Relocating the garbage enclosure to an area south of Building A and away from the edge of the future road south of the site.
- Providing more surface texture and line painting in the parking area to demarcate pedestrian pathways and enhancing pedestrian safety.
- Providing a centrally-located outdoor amenity area south of Building A.
- Providing an additional outdoor landscaped and amenity area immediately east of Building B.

A copy of the relevant excerpt from the Advisory Design Panel Minutes from February 8, 2023 is attached for reference (Attachment 2). The design response from the applicant has been included immediately following the specific Design Panel comments and is identified in '**bold italics**'.

Analysis

Conditions of Adjacency

- The buildings are designed to have a strong visible presence along both River Drive and Van Horne Way.
- Building A, which is the smaller building of the two, is proposed to be closer to the western portion of the site, and would have a strong visible presence along Van Horne Way.
- Both Buildings A and B would have a strong visible presence from trains entering and exiting the City via the Canada Line.
- The buildings are proposed to have similar height to the majority of the industrial buildings in the immediate area.
- The edges of the site would be landscaped through planting of shrubs, flowerbeds and trees in order to provide buffering from the streets as well from the lot south of the property, which is currently hosting the CP railway right-of-way and is earmarked for redevelopment as a future City road.

Urban Design and Site Planning

- The proposed development includes two new two-storey light industrial buildings that are oriented north-south.
- Vehicular access to the site is provided via one driveway off River Drive and two (2) off Van Horne Way. The two buildings are separated by a central drive aisle which contains loading areas as well as the majority of the on-site parking spaces. The proposed access locations and building arrangement has been designed to accommodate on-site truck turning movements.
- Building A will have three (3) painted and textured pedestrian access-ways from the multi-use pathway along Van Horne Way and connect to Building A. Paths of travel for pedestrians to the rest of the site are proposed to be clearly demarcated in order to provide additional pedestrian safety within the site.
- The CCAP identifies a future City road located along the southern portion of the property. In order to accommodate the construction of the future City road, the developer has agreed to provide a minimum 6.94 m wide Statutory Right-of-Way (SRW) along the properties southern frontage. This SRW is consistent with the SRW secured along the southern portion of the adjacent property to the east at 9920 River Road through the associated Zoning Text Amendment process for that property (ZT 16-753545).
- A garbage and recycling enclosure is proposed on the southeast corner of Building A. The proposed enclosure is to be on a concrete slab with steel swing doors finished to match the design character of the buildings.
- Sixty-six (66) Class 1 bicycle spaces are provided within the buildings in individual units. An additional 62 Class 2 bicycle spaces are provided via several bicycle racks grouped into 18 locations near building entrances and outdoor amenity areas.

Architectural Form and Character

- Two industrial buildings with similar design features and appearance are proposed.
- The buildings have a modern industrial appearance, with extensive glazing on all four elevations with a flat roof.
- Full-height vertical elements with accenting materials such as aluminum and metal cladding, break up the horizontal massing along all elevations for both buildings.
- Both buildings will incorporate built-in canopies along their west elevations in front of individual units' entrances in order to provide visually-pleasing façades and weather protection for employees and visitors.
- All elevations will include a variety of building material and colours for accenting as well as cladding to further signify the modern industrial character of the site and in order to integrate the buildings' form and character with the immediate neighbourhood's. Proposed building materials would include concrete, aluminum, steel, and clear and spandrel glazing.

Tree Removal, Replacement and Retention

- There are a total of 21 bylaw sized trees on the subject site. A Tree Management Plan is provided in Attachment 3.
- The applicant proposes to remove fourteen (14) trees. Two (2) trees proposed to be removed, which are located along the south property line, are in poor condition. Nine (9) trees proposed to be removed are adjacent to the existing on-site building and their retention would be impacted by the demolition of the existing building and associated construction works to accommodate any on-site development in the future. Three (3) trees, which are in fair to good condition, have been identified for removal due to conflict with the proposed Building B envelope.
- The City's 2:1 replacement ratio would require a total of 28 bylaw sized trees to be planted in lieu of the proposed trees' removal. The applicant is proposing to plant 40 bylaw sized trees on-site, which would exceed the minimum requirement of 28 replacement trees. The proposed replacement trees would include a mix of coniferous and deciduous species. Deciduous trees are proposed in close proximity to the parking areas to allow for clear sight lines, while eight (8) coniferous trees are to be planted along Van Horne Way frontage, clustered with other plantings.
- The applicant proposes to retain seven (7) trees (Tag #936, 937, 949, 5059, A, B, C) along the south property line. Prior to Development permit issuance, the applicant shall submit to the City a copy of a signed contract with a Certified Arborist to confirm the supervision of all works conducted within or in close proximity to tree protection zones. The contract must include the scope of work required, the number of proposed monitoring inspections at specified stages of construction, any special measures required to ensure tree protection, and a provision for the arborist to submit a post-construction impact assessment to the City for review.
- Prior to demolition of the existing building on the subject site, installation of tree protection fencing around all trees would be required. Tree protection fencing must be installed to City standard in accordance with the City's Tree Protection Information Bulletin Tree-03 prior to any works being conducted on-site, and remain in place until construction and landscaping on-site are completed.

The applicant is required to provide security deposit of \$70,000.00 for the protection and retention of the aforementioned seven (7) on-site trees.

Landscape Design and Open Space Design

- The focal pedestrian access to the site would be through the northwest at the corner at the intersection of Van Horne Way and River Drive. The entry point would include an outdoor amenity space designed to function as a plaza, comprising of seating as well as a landscaped area with a mix of shrubs, plants and flowers. The plaza area would also include a public art installation, which is required as part of the City of Richmond's Public Art policy for development proposals of this scale (See Public Art section below for more detail).
- The landscaping buffer of 3 meters fronting Van Horne Way will include a mix of deciduous trees as well as native plants and shrubs in addition to the eight (8) coniferous trees noted earlier, providing a diverse and seasonally varied landscaped area.
- Landscaping buffer of 3 meters fronting River Drive, north of Buildings A and B, will include a mix of deciduous trees and native plants and shrubs that are layered to provide seasonal variation and texture.
- An enlarged landscaped and outdoor amenity area is provided immediately east of Building B fronting River Drive. The landscaped area would include shrubs and plants around a central seating space which includes picnic tables and benches. The area would be accessible from both River Drive and via Building B.
- There are twelve (12) smaller landscape amenity areas in front of several individual units in both buildings. These spaces would contain benches and bicycle racks. Four of the areas in front of Building A would be landscaped with perennial shrubs and plants and each space would contain one (1) deciduous tree.
- An enlarged landscaped outdoor area is provided south of Building A. This space would contain a mix of trees and shrubs and would include several benches and bicycle racks.
- A landscaped outdoor area is provided on the southeast corner of the site bordering the edge of the future road planned for the CP railway right-of-way. This area would be landscaped with several deciduous trees as well as shrubs and plants, and would include benches and a picnic table for use by future employees and visitors.
- The applicant is required to submit an acceptable security for landscaping in the amount of \$126,108.30 plus applicable taxes, inclusive of a 10 per cent contingency cost, prior to the issuance of the Development Permit.
- Irrigation is to be provided for all soft landscaped areas with the utilization of a high efficiency irrigation system.

Sustainability

- The buildings are designed to achieve a minimum 10% saving on energy cost budget from the BC Building Code baseline standard (ASHRAE 90.1), which would be confirmed through an energy model, and outdoor lighting will be high efficiency LED to aid building energy efficiency (Attachment 4).
- Both buildings have been designed to allow for the installation of future solar photovoltaic roof mounted panels by individual tenants in the future, should the future tenant choose to do so.

- The proposed bicycle parking exceeds the requirements in the Zoning Bylaw and electrical outlets would be installed near several bicycle racks along buildings' walls to promote e-bike usage.
- All units would have the potential for Electric Vehicle charging stations through Electric Vehicle rough-in installations. This would enable the installation of charging stations for approximately 88 parking stalls in the future, which would be 46 per cent of the total number of on-site parking spaces proposed. In addition, the applicant has agreed to install five (5) Electric Vehicle (EV) charging stations through the parking areas. The charging stations would provide active charging to ten (10) vehicles.
- The landscaping plan includes drought-resistant species with minimal water usage requirements.

Crime Prevention Through Environmental Design

- The site has three (3) access points for vehicles and five (5) access points for pedestrians. Pedestrian pathways have clear sightlines from individual units in both buildings.
- Outdoor lighting will consist of wall-mounted LED lighting around both buildings and will also include surface-based lighting in front of units and loading areas. Lighting fixtures would be installed so as to ensure that lighting is directed downward thus limiting light spillover onto the adjacent sites.
- Outdoor amenity areas are to be built in areas with high visibility from various units in both buildings.
- The outdoor amenity area east of Building B would be visible from River Drive and would be accessible via Building B as well as from River Drive. The area would be well-lit with surface and wall-mounted LED lighting and landscaping would be done in such manner as to promote high visibility into and within the area.

Public Art

- As per the City's Public Art policy the developer is providing a voluntary contribution to Public Art in the City. The applicant has committed to provide a public art installation on-site at the northwest entry plaza area. The public art installation will be in the form of a multi-coloured surface mural throughout the entry plaza area and was endorsed by the City of Richmond's Artist Selection Panel on December 13, 2022. A legal agreement and letter of credit securing commitment to the installation and maintenance of the art installation will be registered between the owner and the City prior to the issuance of the Development Permit.

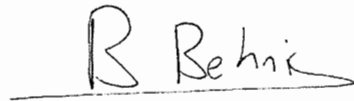
Site Servicing

- The applicant is required to enter into a Servicing Agreement prior to Building Permit issuance for the design and construction of the required site servicing and off-site work including but not limited to:
 - Road frontage improvements along Van Horne Way and River Drive
 - Drainage upgrades,
 - Sanitary system analysis, and
 - Watermain upgrade.

Conclusions

As the proposed development is consistent with the applicable policies and Development Permit Guidelines, staff recommend that the Development Permit be endorsed, and issuance by Council be recommended.

The list of Development Permit Considerations is included in Attachment 5, which has been agreed to by the owner (signed concurrence on file).

A handwritten signature in black ink that reads "B Behnia". The signature is written in a cursive style and is positioned above a horizontal line.

Babak Behnia
Planner 2
(604-204-8639)

BB:cas

- Att. 1: Development Application Data Sheet
2: Excerpt from the Meeting minutes of the Advisory Design Panel (February 8, 2023)
3: Tree Management Plan
4: Sustainability Strategy
5: Development Permit Considerations



DP 21-943565

Attachment 1

Address: 9800 Van Horne Way

Applicant: Orion Construction

Owner: 1321791 BC LTD

Planning Area(s): City Centre

	Existing	Proposed
Site Area:	36 379.78 m ²	No change
Land Uses:	Industrial	No change
OCP Designation:	Industrial	No change
Zoning:	Light Industrial (IL)	No change
Number of Units:	1	44 industrial units in two (2) buildings

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	1.2	0.62	None permitted
Gross Floor Area	43,603.2 m ²	22,609 m ²	None
Lot Coverage for Buildings:	Max. 80%	48.6%	None
Setback – Front Yard:	Min. 3 m (north)	3 m	None
Setback – Side Yard:	Min. 0 m (east)	1.2 m	None
Setback – Exterior Side Yard:	Min. 3 m (west)	13 m	None
Setback – Rear Yard:	Min. 0 m	Building A: 15.7 m Building B: 15.8 m	None
Height (m):	Max. 16 m	Building A: 9.9 m Building B: 10.3 m	None
Off-street Parking Spaces – Regular/Commercial:	146	190	None
Off-street Parking Spaces – Accessible:	3	5	None
Amenity Space – Outdoor:	N/A	706 m ²	None

Excerpt from the Meeting minutes of the Advisory Design Panel (February 8, 2023)

Panel Discussion

Comments from Panel members were as follows:

- understand the constraints of the long and narrow site from a design perspective;
- consider incorporating different levels of electric vehicle (EV) charging stations either spread out or concentrated in one area of the subject site considering the size of the proposed industrial development and the potential needs of tenants;

There will be several level 2 EV charging stations installed at the development. They have been dispersed appropriately. Level 2 chargers are a widely accepted standard and is an appropriate level of charging for a workplace.

- ensure that the proposed outdoor storage space adjacent to the property owned by BC Hydro does not become unsightly (e.g. a depository for garbage and junk) and follow the City's guidelines for outdoor storage spaces; consider installing a fence for the outdoor storage space; also consider incorporating an amenity space in the outdoor storage space;

The outdoor storage area has been placed further from the road providing room for an amenity space and landscaped area. A 6' high fence is also provided around this storage yard, which will support the City or Richmond and ADP comment to screen this area. Privacy slates have been specified to screen the outdoor storage. All design and materials decisions pertaining to the SRW, including the provision of amenity spaces and fencing, are subject to the approval of BC Hydro. The proposed landscaped area, amenity space, and storage yard are subject change.

- consider relocating the proposed garbage and recycling enclosure to the north and adjacent to Building A as the future road could become a busy road in the future and would be highly visible to the public; also consider other interim uses for the road-right-of-way, e.g. use the space for community gardens as there is demand for this type of outdoor amenity space;

The garbage enclosure has been moved adjacent to Building A per the ADP's recommendation.

- consider incorporating a more focused, dedicated and substantial landscaped area in the subject site at the front of the subject site to create a more visually appealing entrance to the site; consider shifting Building B to the south to provide space for this landscaped area without reducing the buildable area in the project;

The landscape area north of the buildings includes different sizes and colors of plant species to provide a visually appealing buffer along River Drive. Attractively landscaped amenity areas have been provided at the north-east of Building B and at the north-west of Building A to increase the landscape area near the roadway.

- consider installing tastefully designed line painting on the internal drive aisles of the subject site to demarcate pedestrian and vehicular circulation routes, e.g. “pedestrians only”, “cyclists only”, “no parking” pavement signs to enhance the safety of pedestrians and cyclists safety on the site;

For the street connections, from the multiuse path to the front of Building A on the west of the property, the drive aisle is proposed to be saw-cut with a colour contrasting finish. This will provide an obvious visual and tactile route of travel. For the pedestrian paths of travel between the buildings, line painting is proposed to demarcate the anticipated locations of pedestrian travel to pedestrians and vehicles.

- appreciate the legal agreement registered on Title that includes safeguarding the long-term durability/maintenance of the proposed public art in the project; the public art artist/designer is encouraged to use sustainable materials for long-term use or materials that can be replaced economically to ensure the long-term maintenance of the proposed public art;

Durability and sustainability have been considered to date. The project team will continue to work with the artist and art consultant to ensure sustainability and economical replacement are key consideration during material selection.

- appreciate the applicant’s focus on public art considering the industrial use of the site; the proposed public art would provide an interesting visual experience from Canada Line;
- review the location of the seating area in the corner plaza at the northwest corner and consider the safety of users from trucks turning at the corner; consider increasing the size of the corner plaza considering the size of the proposed industrial development, e.g. convert two adjacent parking stalls into a landscaped area to become part of the plaza;

The north corner amenity plaza is within the site and the City of Richmond’s engineering staff have considered the turning radius in the design of the road and multi-use pathway adjacent to our site at this corner.

- support the Panel comment to relocate the garbage and recycling enclosure elsewhere on-site and utilize the future road ROW for community gardens in the interim; also support the Panel comment to relocate the garbage and recycling enclosure to the space adjacent to Unit 126 in Building A;
- does not agree with the applicant's approach to provide more parking stalls as a marketing strategy; consider providing just the required minimum number of parking stalls and turn the extra parking stalls into an amenity space; the project could attract more tenants by providing more amenity spaces;

The project team are confident that the design has managed to strike a balance between providing attractive amenity spaces dispersed across the property and providing parking for employees and visitors. There are 44 units planned for the site. The users of these units have not been identified. Providing the site with ample parking ensures consumer facing businesses have adequate parking and will ensure there are no conflicts between users for spaces.

- proposed location for garbage and recycling enclosure is an ideal location for an outdoor amenity space as it is centrally located; consider relocating the garbage and recycling enclosure to a more hidden area in the subject site;

The garbage enclosure has been moved adjacent to Building A per the ADP's recommendation.

- agree with the Panel comment that the proposed corner plaza at the northwest corner is too small considering the size of the project; concerned about the usability of this amenity space as its location does not provide privacy to users;

The plaza is designed to be open and welcoming, with multiple lines of sight. Excessive privacy measures for amenity spaces have the potential to create safety concerns outside of peak hours.

The plaza is a prominent site entrance with adequate seating and landscaping, balancing its need to act as a transitory pedestrian space and amenity area. The size and configuration suits both of these competing priorities. As well, increasing the plaza size would inhibit entry to Unit 101 in Building A. Adding the proposed amenity space to the south of Building A should help relieve concerns with the size of the plaza.

- support Panel comments regarding too many extra parking stalls on the site; consider the possibility that some workers would be using public transit and could access the subject site at the southwest corner from Canada Line Bridgeport Station that would result in some pedestrian movement along the south side of Building A; look at ways to mitigate conflict between pedestrian and vehicular traffic on the site;

A street connection at the south side of Building A has been provided to allow for users to enter the site and safely cross the drive aisle to get to the building sidewalk.

- consider installing an outdoor amenity space on the south side of Building A as it is centrally located; also consider multiple smaller landscaped areas in selected areas on the site with opportunities for planting of trees to provide shade for users during summer especially in landscaped areas located on the west side of the two buildings;

The amenity area on the South of Building A has also been expanded to provide a second large amenity area that is accessible to both Buildings A and B.

- consider relocating the proposed garden and recycling enclosure elsewhere on-site; however, should the proposed location be retained, consider providing adequate landscaping around the enclosure;
- appreciate the applicant's improved sustainability strategy and comments regarding the use of bioswales and on-site water storage systems;
- review the design of the high SRI building rooftops, e.g. review the size of the ballasts and how it would be visually experienced from Canada Line;

The proposed roofing system is a 12lb per sq.ft. ballast comprised of smooth river rock. This roof system's appearance is not drastically affected by weathering like other materials. The smooth river rock is and will remain an attractive ballast material.

During our review of the Canada Line's adjacency, we determined that the train's height is similar to that of the building height, which will minimize sightlines to the roof. The roofing materials, combined with the height of the building and the speed that the SkyTrain will be traveling at make us confident the roof system's visual experience from the SkyTrain will be inoffensive and in conformance with other industrial developments along the Metro Vancouver's SkyTrain routes.

- support the Panel comment regarding the interim use of the ROW for the future road for outdoor amenity spaces such as community gardens;

The various amenity areas provided across the site satisfy the need for attractive outdoor amenity spaces. A community garden space presents a potential conflict between the tenants and customers of the industrial site and the users of the gardens. As well, this project's location away from significant residential development make demand for a garden minimal.

- future road ROW along the south side of the subject site could provide a linear park that provides connection to the site in the interim especially if there is still no plan to develop the future road; the proposed garbage and recycling enclosure on the south side needs to be relocated elsewhere on-site;

The development's extensive amenity and landscaping area eliminate the need for a linear park along the ROW.

- support the Panel comment to distribute the amenity spaces on the site so there would be fewer but more substantial smaller nodes throughout the site;

- irrigation for the public realm should be provided longer than what was previously required due to climate change; consider introducing water management strategies, e.g. water surge to allow plants to access water through capillary action;

The irrigation strategy proposed will be a high efficiency irrigation system. The plant selection includes native and adaptive plants material to response to water management strategy. Irrigation is noted for boulevard area as well as on site.

- the proposed development is better than a lot of industrial complexes in the City;
- the project is unique for a light industrial development;
- consider enhancing the project's sustainability strategy and integrating further landscaping into the proposed development;

The majority of the plant material is native and drought tolerant species in response to sustainability strategy. The plant locations have been studied to ensure the light requirements for particular plant species have been respected, providing a sustainable environment for plants.

The project's sustainability strategy is strong for an industrial development. It considers elements of building design and siting, energy usage, material choices, transportation sustainability, landscaping and natural considerations, and waste control and reduction measures. The sustainability strategy has incorporated applicable City of Richmond policies.

- support the Panel comment regarding the choice of materials to be used for the proposed public art to ensure long-term maintenance and durability; and
- proposed landscaping treatment in front of Building A is more successful than in front of Building B; investigate further landscaping opportunities in front of Building B, e.g. installing more tree canopies, considering that the number of proposed parking stalls is more than what is required.

Plant material has been carefully chosen to provide four season interest and softening the development, on both the streetscape and in nodes of planting at building B entrances. The area in front of Building B hosts parking and large truck bays, presenting a conflict with exorbitant landscaping.

In addition to the above comments from the Panel, the following written comments were submitted by Panel member Pam Andrews and were read into the record by Viren Kallianpur:

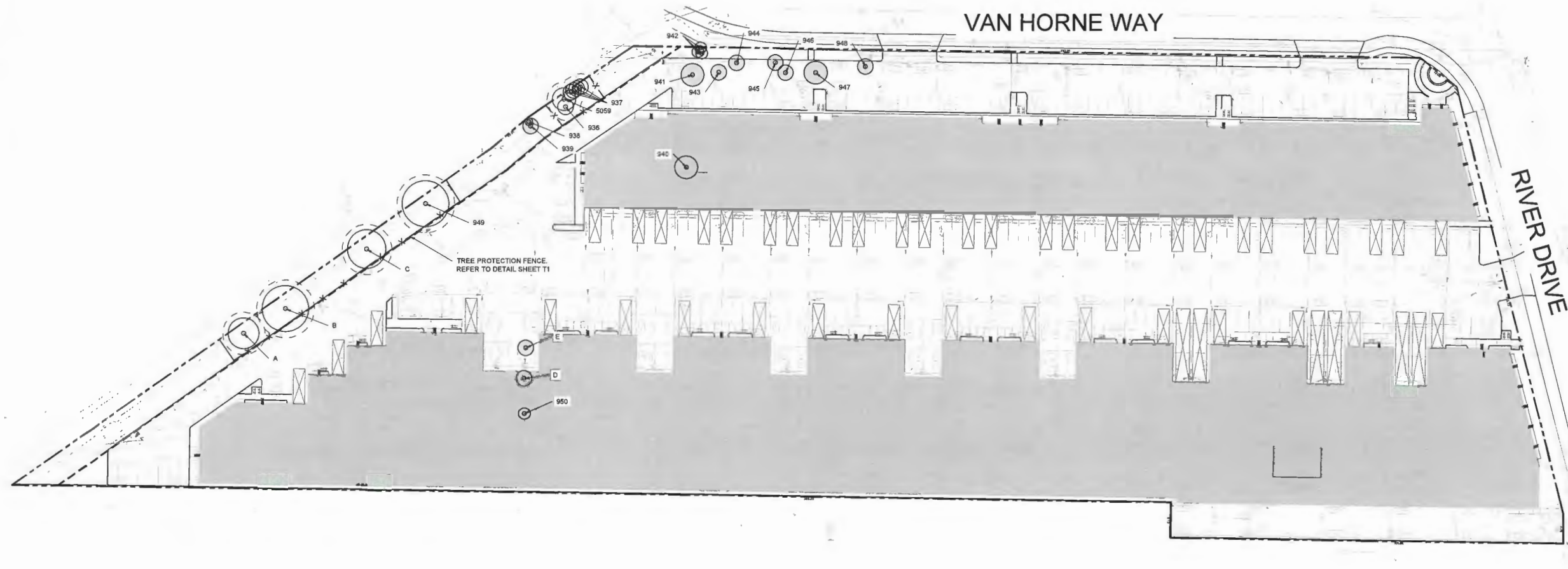
- the two multi-tenant light industrial buildings and parking look good.

Panel Decision

It was moved and seconded

That DP 21-943565 be supported to move forward to the Development Permit Panel subject to the applicant giving consideration to the comments of the Advisory Design Panel.

CARRIED



- LEGEND**
- EXISTING TREE PROPOSED FOR REMOVAL
 - EXISTING TREE TO BE RETAINED
 - TREE PROTECTION FENCING
 - TREE TAGNO
 - TREE PROTECTION ZONE: 1.5m OFFSET FROM CANOPY

04/10/21 ISSUED FOR PERMIT
27/09/21 ISSUED FOR REVIEW

Assessment Done Sept. 24, 2021
By Reed Moss I.S.A. #PN-6960A

TABLE 2. RETAINED TREE INVENTORY

The tree health and structural condition ratings are a relative and not based on factors that could vary site to site or a comparison of other conditions. Severe canopy dieback, significant decay, damaged trunk, significant disease or parasite, plant presence, Poor Condition - Moderate canopy dieback and/or loss, stem defects, multiple stems, moderate foliage damage from stress, root collar damage from disease or insects, Root Condition - Healthy response, crown, no or minor visible damage.

TAG/TREE	BOTANICAL NAME	COMMON NAME	DBH (cm)	CANOPY (m)	TRC (cm)	CONDITION	COMMENTS	RECOMMENDATION
936	Thuja plicata	Western Red Cedar	25.75	2	3.5	Good		Retain and Protect
937	Thuja plicata	Western Red Cedar	38.25, 38	2	3.5	Good	Address growth as a hedge	Retain and Protect
940	Populus trichocarpa	Black Cottonwood	47	8	7.5	Good		Retain and Protect
939	Thuja plicata	Western Red Cedar	32	2	4.5	Good		Retain and Protect
A	Populus trichocarpa	Black Cottonwood	54	4	5.5	Good		Retain and Protect
B	Populus trichocarpa	Black Cottonwood	79	6	7.5	Good		Retain and Protect
C	Populus trichocarpa	Black Cottonwood	65	5	6.5	Good		Retain and Protect

PROTECTING AND MANAGING TREES DURING CONSTRUCTION

- Upon receiving the necessary approvals and prior to the commencement of tree removals, all areas designated for preservation must be flagged in the field. All designated preservation areas must be left standing and undamaged during the removals work. All tree removals must be felled into the development area. The removals are to be completed outside of the migratory bird nesting season which is generally between mid-March to Mid-April. A Qualified Environmental Professional must provide a report indicating the presence or absence of bird nests for any permit submissions for tree removals during this time of the year.
- Upon completion of the tree removals, all felled trees are to be removed from the site, and all brush chipped and removed, unless otherwise directed by the Project Manager. No lumber or brush from the clearing is to be stored on the site, all brush, roots and wood debris must be shredded into piles.
- Tree Protection Fencing will be installed along retained tree areas adjacent to areas where construction will encroach into the adjacent tree edges. Refer to detail provided by Arborist and/or Municipality.
- Areas within the drip line of the trees designated for preservation are not to be used for any type of storage (e.g. storage of debris, construction material, surplus soils, and construction equipment). No trenching or tunneling for underground services shall be located within the tree protection zone or drip line of trees designated for preservation within or adjacent to construction zone.
- Trees shall not have any rigging cables or hardware of any sort attached or wrapped around them, nor shall any contaminants be dumped within the protective areas. Further, no contaminants shall be dumped or flushed where they may come into contact with the feeder roots of the trees.
- In the event that it is necessary to remove additional limbs or portions of trees, after construction has commenced, to accommodate construction, the Consulting Arborist or project administrator is to be informed and the removal is to be executed carefully and in full accordance with arboricultural techniques, by a certified and licensed Arborist.
- During excavation operations in which roots are affected, the Contractor is to prune all exposed roots cleanly. Pruned root ends shall point obliquely downwards. The exposed roots should not be allowed to dry out. The Contractor shall discuss watering of the roots with the Owner and Contract Administrator prior to pruning to ensure that optimum soil moisture is maintained during construction and backfilling operations. Backfilling must be completed with clean, uncontaminated native topsoil.

CONSTRUCTION MITIGATION AND MANAGEMENT

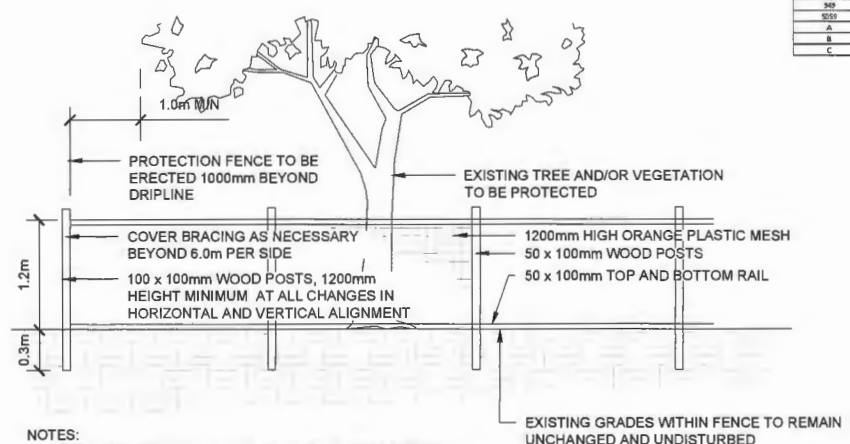
POTENTIAL CONSTRUCTION IMPACTS TO TREES
Trees are living organisms that react to changes in their environment. Trees can be damaged during construction without showing signs of damage until some years later. Many of the problems relate to the roots and result in slow death of the tree through its inability to absorb sufficient water and nutrients.

SOIL COMPACTION
The leading cause of construction damage to trees is compaction of the soil around the roots. Equipment entering into a preservation zone compresses the air pockets around the roots inhibiting the tree from absorbing nutrients and water. This damage ultimately reduces the health of the tree. Accordingly, during the removal stage, equipment use within the preservation zones should be restricted to ensure that the tree's roots are not disturbed, thereby assisting to maintain their continued health.

MECHANICAL DAMAGE
Equipment can also physically damage the trees through striking the trunk, limbs and/or roots. Felled trees can also cause damage during the tree removal stage of development. Some damage is unavoidable due to the density of the trees, however, through the use of proper equipment and best management practices the damage can be minimized. The contractor should be held responsible for all avoidable damage to the trees during all stages of development.

ROOT DAMAGE
Tree preservation is dependent not only on protecting the root zone from compaction and damage, but it is also critical to ensure that the structural roots within the root plate are not disturbed. Impacts to this area may result in a failure of the trees.
During construction, there are situations where work must be completed within a protection zone that will have an impact to the roots of the preserved trees. The most common root injury is cutting typically caused by trenching for utilities. The feeder roots of a tree are typically situated within the upper 30 cm of the soil profile. Any trenching or excavation within this zone will damage roots. Roots play a critical role in anchoring a tree. If major support roots are severed the tree is more susceptible to wind throw damage and could pose a hazard to adjacent structures and people.

SMOTHERING ROOTS WITHIN THE TREE PROTECTION ZONE
Tree roots require space and air to absorb water and minerals. When grades are increased the feeder roots are no longer able to effectively function. Grade changes of only ten centimeters can kill the fine roots of some trees.



- NOTES:**
- ATTACHMENT OF FENCE TO TREES WILL NOT BE PERMITTED.
 - ANY EXPOSED ROOTS ARE TO BE HAND PRUNED USING PROPER ARBORICULTURAL PRACTICES.
 - UNDER NO CIRCUMSTANCES SHALL ANY CONSTRUCTION MATERIALS, EQUIPMENT OR VEHICLES BE PLACED WITHIN THE TREE PROTECTION ZONE.
 - ALL TREE PROTECTION TO BE ERRECTED PRIOR TO ANY CONSTRUCTION ACTIVITY AND IS TO REMAIN IN PLACE UNTIL ALL CONSTRUCTION HAS BEEN COMPLETED. OBTAIN WRITTEN APPROVAL FROM CONTRACT ADMINISTRATOR PRIOR TO REMOVAL OF FENCING.

Tree Protection Zone No Entry

No Dumping No Fill No Digging or Excavating No Lumber or Supply Storage

Fencing to be installed prior to demolition and remain in place until construction is complete. A fine of \$1,000.00 per tree will be issued for relocating or removing tree protection fencing.

SIGNAGE TO BE ATTACHED TO AT LEAST 2 SIDES OF FENCING. SIGNAGE TO BE MINIMUM 11"X14"

DISCLAIMER
The assessment of the trees presented within this report has been prepared using accepted arboricultural techniques. These include a visual examination of the above-ground parts of each tree for structural defects, scars, external indications of decay, evidence of insect presence, the general condition of the trees and the surrounding site, as well as the proximity of property and people. None of the trees examined were dissected, cored, probed, or climbed, and detailed root crown examinations involving excavation were not undertaken.
Notwithstanding the recommendations and conclusions resulting from the assessment, it must be realized that trees are living organisms and their health and vigour is constantly changing. They are not immune to changes in site conditions or seasonal variations in the weather.
While reasonable efforts have been made to ensure that the trees recommended for retention are healthy, no guarantees are offered or implied, that these trees or any part of them will remain standing. It is both professionally and practically impossible to predict with absolute certainty the behavior of any single tree or group of trees in all circumstances. Inevitably a standing tree will always pose some risk. Most trees have the potential for failure provided with the necessary combinations of stresses and elements. This risk can only be eliminated if the tree is removed.
Although every effort has been made to ensure that this assessment is reasonably accurate the trees should be re-assessed periodically. The assessment presented here is valid at the time of inspection.

SUMMARY: RICHMOND
21 TREES ASSESSED
21 ON SITE TREES
14 ON SITE TREES PROPOSED FOR REMOVAL
7 ON SITE TREES PROPOSED FOR RETENTION

CEDAR COAST VAN HORNE

9800 VAN HORNE WAY,
RICHMOND, BC

OVERALL TREE MANAGEMENT PLAN

SCALE: NTS
DATE: RM
PROJECT: JT
PROJECT NO: 210669-L

T1

Sustainability Strategy

As one of the largest industrial builders in the Lower Mainland, Orion Construction and our team of consultants are familiar with the City of Richmond's goal of sustainable development. We strive to incorporate as many sustainable initiatives in our projects as possible within the limitations of applicability and feasibility. Orion Construction has created our own approach to sustainability which is consistent with and complements the City's own goals. As project designs are finalized in the coming months, the project team will pay close attention to City of Richmond feedback and looks forward to working with staff on creating an exemplary industrial development.

1. Building design features include:

- The buildings' simple massing design and unit configurations not only allow for efficient commercial uses but also enhances thermal performance and building system performance*
- Limited window-to-wall ratio (WMR) to reduce solar heat gain*
- Enhanced thermal performance of the building envelope by utilizing variation in colour, materials, and pattern in lieu of complex massing and architectural features*
- Construct exterior envelope walls as fully insulated sandwich panel design to increase energy efficiency. Anticipated r-value of 11.69
- Incorporate extensive Low-E glazing in office areas to maximise building efficiency
- Maximise solar reflectivity on walls through the use of a high albedo paint, accounting for up to 70 percent of vertical building surfaces on a typical project
- Selecting materials that have long-life and low maintenance properties to extend the lifespan of the building and reduce future obsolescence of building elements
- The roofing system will utilise a 12lb per square foot ballast. This assembly has been found to provide a solution exceeding the performance of a high-SRI roof (such as TPO) for solar reflectance

2. Energy sustainability features include:

- Both buildings will be designed as solar ready to allow for future installations by tenants. This includes ensuring the building structure can withstand anticipated panel array loads
- Use of water efficient plumbing fixtures*
- Demonstrate through an energy model a minimum 10 percent saving on energy cost budget from ASHRAE 90.1 baseline
- Design all outdoor lighting as high efficiency LED to maximise building efficiency
- Base building electrical will support tenant installation of high efficiency LED interior lighting and mechanical systems
- Building commissioning activities will ensure base building mechanical and electrical systems are functioning efficiently

3. Transport sustainability features:

- Efficient parking and bay design reduces the amount of vehicle idling and driving on site
- Provide 100% of units with an Electric Vehicle rough-in. This translates to approximately 88 stalls being EV ready, which is nearly 80% of the required parking spaces. This exceeds the City's requirements by 8:1 and future proofs the site against growing EV use. The rough-ins may support the charging of light-duty vehicles
- Meet or exceed bicycle parking requirements and provide bike parking throughout. The project creates visible connections to the surrounding cycling network to promote commuting and visits by bicycle
- Include exterior electrical outlets near select bike parking locations to promote charging of e-bikes (locations TBD as building designs are finalized)

- The development is located within walking distance to bus stops and the Bridgeport SkyTrain and contains a pedestrian-only entrance beautified by public art, thus promoting commuting and visits by transit and by foot

4. Landscaping and natural considerations include:

- Design for reduced water usage through selecting drought resistant species and minimizing use of sod*
- Select endemic flora and ensure alien or invasive species are not installed
- Maximised canopy cover in car parking areas through selected tree species
- Significant amenity areas throughout the project, promoting activation of spaces and eliminating potential dead-zones
- Limiting the removal of trees and planting new trees will improve Richmond's urban tree canopy

5. Waste control and reduction features include:

- Use local and recycled building materials, including concrete and steel for foundations, slabs and exterior walls*
- Provide facilities to encourage three stream waste separation and recycling*
- Garbage and waste materials will be stored in weatherproof and animal resistant containers. These containers will be visually screened from adjacent sites and current public thoroughfares*
- Create a waste disposal and recycling services plan to guide demolition operations. This plan will promote diversion of waste from landfills and include measures for recycling and reuse of demolished materials
- When necessary, dispose of any contaminated soils in a fully compliant and environmentally respectful manner

*** Policies from the City's DP Guidelines for Green Buildings and Sustainable Infrastructure**



Development Permit Considerations

Development Applications Department
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 9800 Van Horne Way

File No.: DP 21-943565

Prior to approval of the Development Permit, the developer is required to complete the following:

1. Receipt of a Letter of Credit for landscaping in the amount of \$126,108.30 plus taxes, inclusive of a 10% contingency cost (as per the landscape cost estimate provided by KD Planning & Design Ltd., on June 14, 2022).
2. Submission of a Contract entered into between the applicant and a Certified Arborist for supervision of any on-site works conducted within the tree protection zone of the trees to be retained. The Contract should include the scope of work to be undertaken, including: the proposed number of site monitoring inspections, and a provision for the Arborist to submit a post-construction assessment report to the City for review.
3. Submission of a Tree Survival Security to the City in the amount of \$70,000.00 for seven (7) trees to be retained (Trees Tag #936, 937, 949, 5059, A, B, C). A legal agreement is to accompany the Tree Survival Security to set the terms for its use and release.
4. Installation of appropriate tree protection fencing around all trees to be retained as part of the development prior to any construction activities, including building demolition, occurring on-site.
5. City acceptance of the developer's offer to make a voluntary contribution towards the City's Public Art Fund, the terms of which shall include the following:
 - a) The value of the developer's voluntary public art contribution shall be based on the Council-approved rates for residential and non-residential uses and the maximum buildable floor area permitted under the subject site's proposed zoning, excluding floor area associated with affordable housing and market rental, as indicated in the table below.

Building Type	Rate	Proposed Floor Area (after exemptions)	Minimum Voluntary Cash Contribution
Industrial	\$0.29 (2023 rate)	243,783 SF	\$70,697.07

- b) In the event that the contribution is not provided within one year of the application receiving endorsement from the Development Permit Panel (i.e. the Development Permit Panel meeting date), the contribution rate (as indicated in the table in item a) above) shall be increased annually thereafter based on the Statistics Canada Consumer Price Index (All Items) – Vancouver yearly quarter-to-quarter change, where the change is positive.
- c) Prior to Development Permit approval by Council, the developer shall submit a Public Art Plan, for the subject site, to the satisfaction of the Director, Arts Culture and Heritage Services. The Public Art Plan shall be:
 - (1) Prepared by an appropriate professional.
 - (2) Based on a contribution value of at least the total amount indicated in the table in item a) above.
 - (3) Consistent with applicable City policies and objectives (for example, the Richmond Public Art Program, City Centre Public Art Plan, Capstan Public Art Plan and other relevant supplementary public art and heritage planning that may be undertaken by the City), as determined to the satisfaction of the Director of Development and Director, Arts, Culture, and Heritage Services.
 - (4) Presented for review(s) by the Public Art Advisory Committee and endorsement by Council, as required by the Director, Arts, Culture, and Heritage Services.

NOTE: For contributions with a cumulative budget over \$250,000, the Public Art Plan is presented for Council approval prior to Building Permit issuance.

Initial: _____

- (5) Implemented by the developer, as required by legal agreement(s) registered on Title to prior to Development Permit approval.
- d) Prior to Development Permit approval, the developer shall enter into a legal agreement for the provision of on-site Public Art, the terms of which shall include:
- (1) "No building" shall be permitted on the subject site, restricting Building Permit issuance for any building on the lot, in whole or in part (excluding parking), until the developer, to the City's satisfaction:
 - (a) Enters into additional legal agreement(s), if any, required to facilitate the implementation of the City approved Public Art Plan, which may require that, prior to entering into any such additional agreement(s), a Detailed Public Art Plan is submitted by the developer for the subject site and/or an artist is engaged, to the satisfaction of the City (as generally set out in the legal agreement entered into and the Public Art Plan submitted prior to Development Permit approval).
 - (b) Submits a Letter of Credit or cash security or cash contribution (as determined in the sole discretion of the City) to secure the developer's implementation of the Public Art Plan, the collective value of which shall be at least \$70,697.07, including \$3,534.9 as a cash contribution equal to 5% of the total amount indicated in the table in item a) above (\$70,697.07) and a Public Art security Letter of Credit in the amount of (\$70,697.07) being the total amount identified in item a) above.
 - (2) "No occupancy" shall be permitted on the subject site, restricting final Building Permit* inspection granting occupancy of the building (exclusive of parking), in whole or in part, on the subject site until:
 - (a) The developer, at the developer's sole cost and expense, commissions one or more artists to conceive, create, manufacture, design, and oversee or provide input about the manufacturing of the public artwork, and causes the public artwork to be installed on City owned site, if expressly permitted by the City in writing and pre-approved by Council, or within a statutory right-of-way on the subject site (which right-of-way shall be to the satisfaction of the City and secured by one or more legal agreements for rights of public passage, public art, and related purposes including maintenance, in accordance with the City-approved Public Art Plan and, as applicable, Detailed Public Art Plan).
 - (b) The developer, at the developer's sole cost and expense and within 30 days after the date on which the applicable public art is installed in accordance with the City-approved Public Art Plan, executes and delivers to the City a transfer of all of the developer's rights, title, and interest in the public artwork to the City if on City property or to the subsequent strata corporation if on the subject site (including transfer of joint world-wide copyright) or as otherwise determined to be satisfactory by the City Solicitor and Director, Arts, Culture, and Heritage Services.
 NOTE: It is the understanding of the City that the artist's title and interest in the public artwork will be transferred to the developer upon acceptance of the artwork based on an agreement solely between the developer and the artist and that these interests will in turn be transferred to the City, subject to approval by Council to accept the transfer of ownership of the artwork.
 - (c) The developer, at the developer's sole cost and expense, has submitted a final report to the City, to be submitted promptly after completion of the installation of the public art in accordance with the City-approved Public Art Plan, which report, to the satisfaction of the Director of Development and Director, Arts, Culture, and Heritage Services, includes:
 - (i) Information regarding the siting of the public art, a brief biography of the artist(s), a statement from the artist(s) on the public art, and other such details as the Director of Development and Director, Arts, Culture, and Heritage Services may require;
 - (ii) A statutory declaration, satisfactory to the City Solicitor, confirming that the developer's financial obligation(s) to the artist(s) have been fully satisfied;
 - (iii) The maintenance plan for the public art prepared by the artist(s);
 - (iv) Digital records (e.g., photographic images) of the public art, to the satisfaction of the Director of Development and Director, Arts, Culture, and Heritage Services.
- e) As an alternative to the provision of public art on-site, the developer may offer to make a voluntary cash contribution in lieu; provided that the value of such voluntary public art contribution shall be at least the total amount indicated in the table in item a) above. In this case, the requirements of c) through e) above will not apply.

Initial: _____

6. Registration of an Aircraft Noise covenant on Title (Areas 1A and 2).
7. Discharge previous Flood Covenant BE227221 from title.
8. Registration of a Flood Indemnity Covenant on Title (2.9 m GSC).
9. Registration of a legal agreement to secure the building energy performance target achieving a minimum 10% saving on energy cost budget from ASHRAE 90.1 baseline standards and requiring outdoor lighting installation of high efficiency LED to aid building energy efficiency. The legal agreement would also include requirements that both buildings would be designed to allow for the installation of future solar photovoltaic roof mounted panels by individual tenants in the future.
10. Registration of an Electric Vehicle (EV) Charging Infrastructure covenant on title, securing the owner's commitment to voluntarily provide, install, and maintain five (5) EV charging equipment (including charging stations) for a total of ten (10) vehicles to be used by tenants and others, on-site. More specifically, the five (5) required charging stations must be designed to the specifications of Level 2 EV standards and ability to provide charging to ten (10) vehicles simultaneously. The covenant shall also require that, in addition to the ten (10) stalls noted above, a minimum of 88 parking stalls (being a min. of 1 stall per unit) shall feature conduits and pre-ducting in order to support future EV charging installation.
11. Confirmation of compliance and approval from the Ministry of Environment with regard to site investigation for any contaminants due to previous activities on the property prior to the issuance of the Development Permit.
12. Registration of a legal agreement on Title identifying that the property is located in an industrial area and that land use of the property must be consistent with the Zoning Bylaw, and requiring that the owner provide an acknowledgement of the same in all purchase and sale agreements.
13. Granting a Statutory Right-of-Way of 6.94 m width along the south portion of the site in order to secure the area for the future road connecting Van Horne Way to No. 4 Road.

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Enter into a Servicing Agreement for the design and construction of site servicing and off-site works and frontage improvements. A Letter of Credit or cash security for the value of the Service Agreement works, as determined by the City, will be required as part of entering into the Servicing Agreement. Works include, but may not be limited to:

Water Works:

- a) Using the OCP Model, there is 756.0 L/s of water available at a 20 psi residual at the River Dr frontage. Based on your proposed development, your site requires a minimum fire flow of 250 L/s.
- b) The developer is required to:
 - i) Subject to funding approval, the City will pay for the replacement of the existing water main located along the Van Horne frontage. The existing water main is to be replaced by the developer with approximately 250m of new water main located on the roadway.
- c) At Developer's cost, the Developer is required to:
 - i) Install a new 100mm water service connection extending from the newly proposed new water main replacement, complete with water meter and water meter box as per standard city specifications.
 - ii) Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage building designs.
 - iii) Provide a right-of-way for the water meter. Minimum right-of-way dimensions to be the size of the meter box (from the City of Richmond supplementary specifications) + any appurtenances (for example, the bypass on W2o-SD) + 0.5 m on all sides. Exact right-of-way dimensions to be finalized during the building permit process (or via the servicing agreement process, if one is required).

Initial: _____

d) At Developer's cost, the City will:

- i) Complete all tie-ins for the proposed works to existing City infrastructure.

Storm Sewer Works:

a) At Developer's cost, the Developer is required to:

- i) Decommission the existing storm sewer main along the Van Horne Way frontage and replace it with a new 600mm minimum storm sewer main or OCP size as per the City's specifications. The new storm sewer shall be installed within the roadway.
- ii) Install a new 150mm storm service connection extending from the newly proposed storm main replacement, complete with IC.

b) At Developer's cost, the City will:

- i) Complete all tie-ins for the proposed works to existing City infrastructure.

Sanitary Sewer Works:

a) The Developer is required to:

- i) Subject to funding approval, the City will pay for the replacement of the existing sanitary main located along the Van Horne frontage. Sanitary sewer main is to be replaced by the developer with approximately 220m of new sanitary sewer main located on the roadway. The new sanitary main diameter will be confirmed through the servicing agreement design. The proposed sanitary main replacement is required to extend from the south west face of the existing manhole located at the North West corner of the lot until where it will tie into a manhole located along the south PL of 9800 Van Horne Way. The new sanitary main will require a junction and the installation of an additional manhole to accommodate the realignment of the main unto the roadway.
- ii) Subject to funding approval, the City will pay for the installation of a new manhole located along the south PL of 9800 Van Horne Way where the newly proposed sanitary main will tie into the existing sanitary main.

b) At Developer's cost, the Developer is required to:

- i) Not start onsite excavation or foundation construction until completion of rear-yard sanitary works by City crews.
- ii) Install a new 150mm sanitary service connection extending from the newly proposed sanitary main replacement, complete with IC

c) At Developer's cost, the City will:

- i) Complete all tie-ins for the proposed works to existing City infrastructure.

Street Lighting:

a) At Developer's cost, the Developer is required to:

- i) Review street lighting levels along all road and lane frontages, and upgrade as required.

Frontage Improvements and General Items:

a) At Developer's cost, the Developer is required to:

- i) Complete other frontage improvements as per Transportation requirements.

Initial: _____

- ii) Underground utility upgrade works that are required beyond the development frontages (subject to the model run results) shall be eligible for a latecomer agreement.
- iii) Van Horne frontage: Widen existing grass boulevard (varying width +/- 1.0m) to 1.5 m and provide street trees. Existing 3.0 m wide multi-use pathway will need to be shifted into the site to maintain the existing width.
- iv) River Drive frontage: install a new curb and gutter along the existing road edge and enhance the existing boulevard to include trees/grass, while maintaining existing multi-use pathway.
- v) Coordinate with BC Hydro, Telus and other private communication service providers:
 - (1) To pre-duct for future hydro, telephone and cable utilities along all road frontages.
 - (2) Before relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
 - (3) To underground overhead service lines.
- vi) Locate/relocate all above ground utility cabinets and kiosks required to service the proposed development and proposed undergrounding works, and all above ground utility cabinets and kiosks located along the development's frontages, within the developments site (see list below for examples). A functional plan showing conceptual locations for such infrastructure shall be included in the development design review process. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements (e.g., statutory right-of-way dimensions) and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of statutory right-of-ways that shall be shown on the architectural plans/functional plan, the servicing agreement drawings, and registered prior to SA design approval:
 - BC Hydro PMT – 4.0 x 5.0 m
 - BC Hydro LPT – 3.5 x 3.5 m
 - Street light kiosk – 1.5 x 1.5 m
 - Traffic signal kiosk – 2.0 x 1.5 m
 - Traffic signal UPS – 1.0 x 1.0 m
 - Shaw cable kiosk – 1.0 x 1.0 m
 - Telus FDH cabinet – 1.1 x 1.0 m
- vii) Provide, prior to start of site preparation works or within the first servicing agreement submission, whichever comes first, a preload plan and geotechnical assessment of preload, dewatering, and soil preparation impacts on the existing utilities fronting the development site and provide mitigation recommendations.
- viii) If required, submit a proposed strategy at the building permit stage for managing excavation de-watering. Note that the City's preference is to manage groundwater onsite or by removing and disposing at an appropriate facility. If this is not feasible due to volume of de-watering, the Developer will be required to apply to Metro Vancouver for a permit to discharge into the sanitary sewer system. If the sanitary sewer does not have adequate capacity to receive the volume of groundwater, the Developer will be required to enter into a de-watering agreement with the City wherein the developer will be required to treat the groundwater before discharging it to the City's storm sewer system.

Initial: _____

- ix) Provide a video inspection report of the existing utilities along the road frontages prior to start of site preparation works or within the first servicing agreement submission, whichever comes first. A follow-up video inspection, complete with a civil engineer's signed and sealed recommendation letter, is required after site preparation works are complete (i.e. pre-load removal, completion of dewatering, etc.) to assess the condition of the existing utilities and provide recommendations to retain, replace, or repair. Any utilities damaged by the pre-load, de-watering, or other ground preparation shall be replaced or repaired at the Developer's cost.
 - x) Conduct pre- and post-preload elevation surveys of all surrounding roads, utilities, and structures. Any damage, nuisance, or other impact to be repaired at the developer's cost. The post-preload elevation survey shall be incorporated within the servicing agreement design.
 - xi) Monitor the settlement at the adjacent utilities and structures during pre-loading, dewatering, and soil preparation works per a geotechnical engineer's recommendations, and report the settlement amounts to the City for approval.
 - i) Not encroach into City rights-of-ways with any proposed trees, retaining walls, or other non-removable structures. Retaining walls proposed to encroach into rights-of-ways must be reviewed by the City's Engineering Department.
 - ii) Coordinate the servicing agreement design for this development with the servicing agreement(s) for the adjacent development(s), both existing and in-stream. The developer's civil engineer shall submit a signed and sealed letter with each servicing agreement submission confirming that they have coordinated with civil engineer(s) of the adjacent project(s) and that the servicing agreement designs are consistent. The City will not accept the 1st submission if it is not coordinated with the adjacent developments. The coordination letter should cover, but not be limited to, the following:
 - (a) Corridors for City utilities (existing and proposed water, storm sewer, sanitary and DEU) and private utilities.
 - (b) Pipe sizes, material and slopes.
 - (c) Location of manholes and fire hydrants.
 - (d) Road grades, high points and low points.
 - (e) Alignment of ultimate and interim curbs.
 - (f) Proposed street lights design.
 - iii) Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.
2. At the Owner's cost via City Work Order*, design and construct/install utility connections and frontage improvements, works include, but not be limited to water works and storm sewer works.
 3. The owner is required to review street lighting levels along all road frontages and upgrade as required.
 4. Submission of a Construction Parking and Traffic Management Plan to the Transportation Department. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570.

Initial: _____

5. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Department at 604-276-4285.
6. Additional width along Van Horne Way for frontage improvement to be secured through an SRW.

Note:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.
All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.
The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.
- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

[Signed copy on file]

Signed

Date



No. DP 21-943565

To the Holder: 1321791 BC Ltd.
Property Address: 9800 Van Horne Way
Address: Orion Construction
Unit 104 - 3950 191 Street
Surrey, BC V6C 0A6

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. Subject to Section 692 of the Local Government Act, R.S.B.C; buildings, and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans #1 -29 attached hereto.
4. Sanitary systems, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
5. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$126,108.30 plus applicable taxes to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the development permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
6. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.
7. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

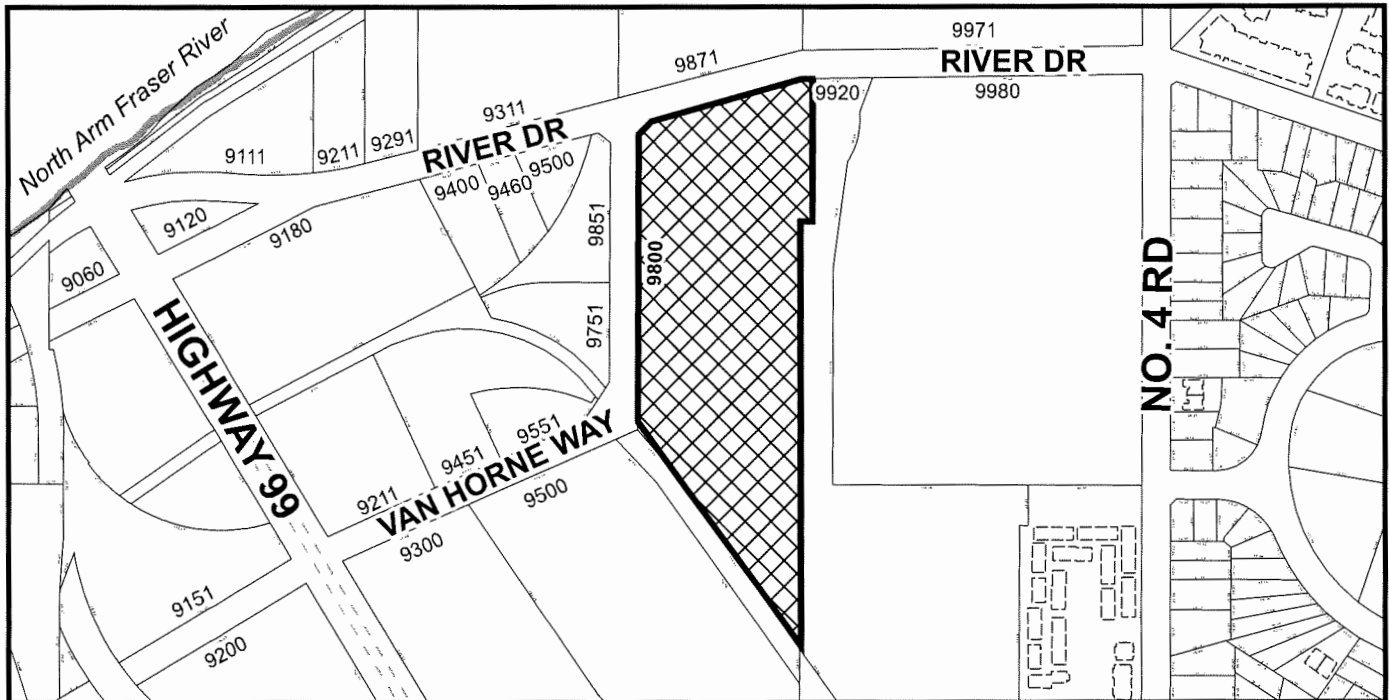
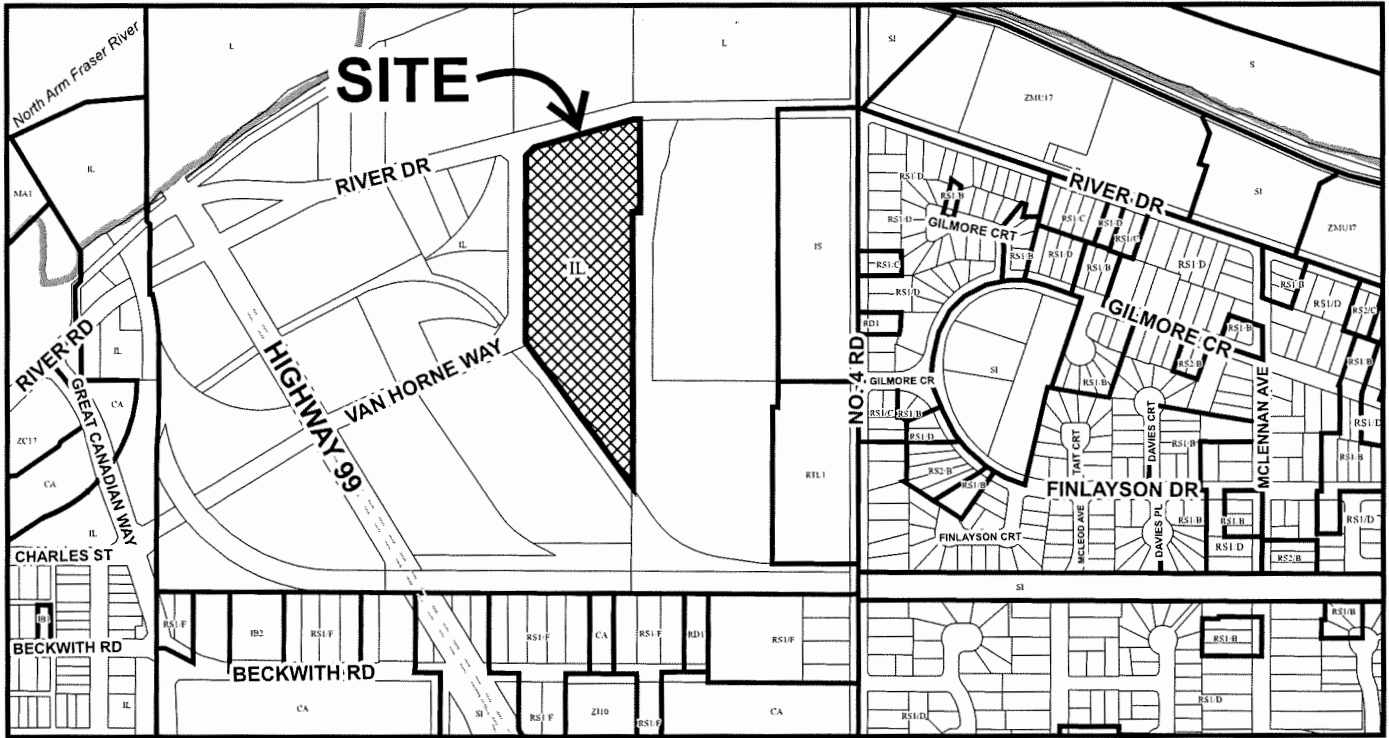
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR



City of Richmond



DP 21-943565
SCHEDULE "A"

Original Date: 11/09/21

Revision Date: 04/06/21

Note: Dimensions are in METRES

PROPOSED INDUSTRIAL BUILDINGS FOR:

CEDAR COAST VAN HORNE

9800 VAN HORNE WAY, RICHMOND, BC

LOT A SEC. 22 BLK 5N R.6 6N PL LMP1545 LOT A, BLOCK 5N, PLAN LMP1545, SECTION 22, RANGE 6N, NEW WESTMINSTER LAND DISTRICT, EXCEPT PLAN BCP220 19



1 BUILDING A FROM NORTH-WEST CORNER
SCALE: NTS



2 BUILDING B FROM RIVER DRIVE
SCALE: NTS



3 BUILDING A ENTRANCE
SCALE: NTS



4 BUILDING B TYPICAL UNIT ENTRANCE AND LOADING
SCALE: NTS

CONSULTANTS LIST:

<p>ARCHITECT CHIP BARRETT ARCHITECT</p> <p>#40-15055 20 AVE, SURREY BC, V4A 1S9 TEL: 604-536-1212 CONTACT: CHIP BARRETT</p>	<p>DEVELOPER ORION CONSTRUCTION</p> <p>104 - 3950 131 STREET, SURREY BC V3Z 0Y6 TEL: 604-362-2994 CONTACT: BRADEN SMITH</p>	<p>CIVIL ENGINEER CENTRAS ENGINEERING LTD.</p> <p>#316-2850 GLENDON DRIVE, SURREY BC V3S 5T3 TEL: 604-792-6421 CONTACT: STEVE O'CONNELL</p>	<p>GEOTECHNICAL ENGINEER GEOPACIFIC CONSULTANTS LTD.</p> <p>1114 PL 15TH AVENUE VANCOUVER, B.C., V6P 6P2 TEL: 604-694-0422 CONTACT: MATT AKENHEAD</p>	<p>LANDSCAPE ARCHITECT KD PLANNING & DESIGN LTD</p> <p>#400-34011 GLADYS AVENUE ABBOTSFORD, BC V2S 2E6 TEL: 604-853-8931 FAX: 604-853-1580 CONTACT: JESSICA THIESSEN</p>	<p>ARBORIST KD PLANNING & DESIGN LTD</p> <p>#400-34011 GLADYS AVENUE ABBOTSFORD, BC V2S 2E6 TEL: 604-853-8931 FAX: 604-853-1580 CONTACT: ALEX GROENEVOLD</p>
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ARCHITECTURAL DRAWING LIST

COVER SHEET	A-0.0
PERMISSALS	A-0.1
CONTEXT PLAN & PHOTOS	A-0.2
STREETScape ELEVATIONS	A-0.2a
TOPOGRAPHY PLAN & PRE RESPONSE PLAN	A-0.3
WASTE MANAGEMENT PLAN & DETAILS	A-0.4
SHADOW STUDY	A-0.5
ACCESSIBILITY & AVENUE AREA PLAN	A-0.6
OVERALL SITE PLAN	A-1.0
ENLARGED SOUTHERN SITE PLAN	A-1.1
ENLARGED NORTHERN SITE PLAN	A-1.2
ENLARGED SITE PLANS & SITE SECTIONS	A-1.3
SITE DETAILS	A-1.4
BUILDING A FLOOR PLANS	A-2.0
BUILDING A TYP. UNIT PLANS	A-2.1
BUILDING A ELEVATIONS	A-3.0
BUILDING B FLOOR PLANS	A-2.0
BUILDING B TYP. OFFICE PLANS	A-2.1
BUILDING B ELEVATIONS	A-3.0
BUILDING B ENLARGED ELEVATIONS	A-3.1



ARCHITECTURE PANEL INC.
ARCHITECTURE, LANDSCAPE ARCHITECTURE, URBAN DESIGN
206 - 1475 FORSTER STREET, WHITE ROCK, BC, V8B 5G4
604.735.1600 | RICHMOND@ARCHITECTUREPANEL.COM

IN ASSOCIATION WITH

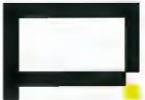


2025 ALLANWAY STREET, ANNEBORO, B.C., V2S 2J7
TEL: 604-457-9555, 5900 AVALON BLVD, WOODSVILLE, B.C.

THESE DRAWINGS AND THE DESIGN ARE AS AT ALL TIMES SUBJECT TO THE RISK OF CHANGES IN REQUIREMENTS OR CHANGES IN DESIGN AND ORIGINATOR SHALL BE RESPONSIBLE FOR THE DESIGN AND/OR CONSTRUCTION OF THE PROJECT. ALL CHANGES TO THE DESIGN SHALL BE MADE BY THE ARCHITECT/ENGINEER/DESIGNER AND/OR CONTRACTOR. THE ARCHITECT/ENGINEER/DESIGNER SHALL BE RESPONSIBLE FOR THE DESIGN AND/OR CONSTRUCTION OF THE PROJECT. THE ARCHITECT/ENGINEER/DESIGNER SHALL BE RESPONSIBLE FOR THE DESIGN AND/OR CONSTRUCTION OF THE PROJECT. THE ARCHITECT/ENGINEER/DESIGNER SHALL BE RESPONSIBLE FOR THE DESIGN AND/OR CONSTRUCTION OF THE PROJECT.



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4	MAR.13.23	UPDATED FOR ADP COMMENT
3	DEC.15.22	UPDATED FOR CITY COMMENT
2	JUL.19.22	RE-ISSUED FOR DIP
1	OCT.04.21	ISSUED FOR DIP
NO.	DATE	DESCRIPTION
PROJECT MANAGERS / CONTRACTORS		
ORION CONSTRUCTION		
ORION CONSTRUCTION 104 - 3950 131 STREET, SURREY BC, V3Z 0Y6 PHONE: (604) 362-2994		
PROJECT		
CEDAR COAST VAN HORNE		
ADDRESS: 9800 VAN HORNE WAY RICHMOND, BC		
DRAWING		
COVER SHEET		
SEAL	JOB NO. 21429	DRAWN VCF
	DESIGNED	
CHECKED	R.L.D.	
FLOT DATE	MAR.13.23	
PROJECT-DRAWING NUMBER	REV.	
A-0.0		4



ARCHITECTURE PANEL INC.
ARCHITECTURE LANDSCAPE ARCHITECTURE URBAN DESIGN
334 - 5455 107TH STREET, WHITE MOOD, BC, V1R 0C4
TEL: 604-273-3868 | INFO@ARCHITECTUREPANEL.COM

IN ASSOCIATION WITH
D.FORCE DESIGN INC.

33255 BURNHAMTHORPE ROAD, MARKHAM, ONT. M3L 2V8
TEL: 905-477-8777 | INFO@DFORCEDSIGN.COM

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CEDAR COAST

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4	MAR 13 23	UPDATED PER ACP COMMENT
3	DEC 15 22	UPDATED PER CITY COMMENT
2	JUN 16 22	RE-EVALUATED ORISP
1	OCT 20 21	ISSUED FOR CIP

PROJECT MANAGER / CONTRACTOR



ORION CONSTRUCTION
104 - 3950 191 STREET,
SURREY BC, V3Z 0Y8
PHONE: (604) 362-2994

PROJECT

**CEDAR COAST
VAN HORNE**

ADDRESS: 5132 VAN HORNE WAY, RICHMOND, BC
DRAWING

RENDERINGS

DATE	21-02-23	DRAWN	VJF
DESIGNED		CHECKED	R.D.
PROJECT DATE	MAR 13 23		

PROJECT - DRAWING NUMBER
A-0.1

4



1 **NORTHWEST VIEW OF SITE FROM SKYTRAIN**
SCALE: NTS



2 **BIRD'S EYE VIEW OF SOUTHWEST OF SITE**
SCALE: NTS



3 **BIRD'S EYE VIEW OF SOUTH OF SITE**
SCALE: NTS



4 **WEST VIEW OF SITE FROM SKYTRAIN**
SCALE: NTS

CONTEXT PLAN LEGEND

- RESIDENTIAL USE
- INDUSTRIAL USE
- COMMERCIAL USE
- CITY INFRASTRUCTURE
- SCHOOL
- GREEN SPACE
- WALKING TRAIL
- BIKING PATH
- EXISTING ROAD NETWORK
- FUTURE ROAD



2 VIEW OF SITE FROM OAK STREET BRIDGE
SCALE: NTS



1 CONTEXT PLAN
SCALE: NTS



4 NORTH-WEST CORNER OF SITE FROM STREET
SCALE: NTS



3 SOUTH-WEST CORNER OF SITE FROM STREET
SCALE: NTS



6 NORTH-EAST CORNER OF SITE FROM STREET
SCALE: NTS



5 INTERSECTION OF VAN HORNE WAY & RIVER DRIVE CONNECTION TO NORTH ARM BRIDGE BIKEWAY
SCALE: NTS



ARCHITECTURE PANEL INC.
ARCHITECTURE LANDSCAPE ARCHITECTURE URBAN DESIGN
306 VAN HORNE STREET, SUITE 100, BC V6R 4G3
604 363 5850 | RUCBA@ARCHITECTUREPANEL.COM

IN ASSOCIATION WITH
D.FORCE DESIGN INC.

2525A ALLENWAY STREET, ANIMAPOLIS, B.C. V2R 2J5
TELE: (604) 837-5855 EMAIL: DANNY@DFORCE.COM

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CEDAR COAST

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4	MAR.13.23	UPDATED PER ACP COMMENT
3	DEC.13.22	UPDATED PER CITY COMMENT
2	JUN.16.22	REDESIGNED FOR DP
1	OCT.24.21	ISSUED FOR DP

NO. DATE DESCRIPTION

PROJECT MANAGER / CONTRACTORS



ORION CONSTRUCTION
104 - 3650 191 STREET,
SURREY BC, V3Z 0Y6
PHONE: (604) 362-2894

PROJECT

**CEDAR COAST
VAN HORNE**

ADDRESS: 900 VAN HORNE WAY, RICHMOND, BC

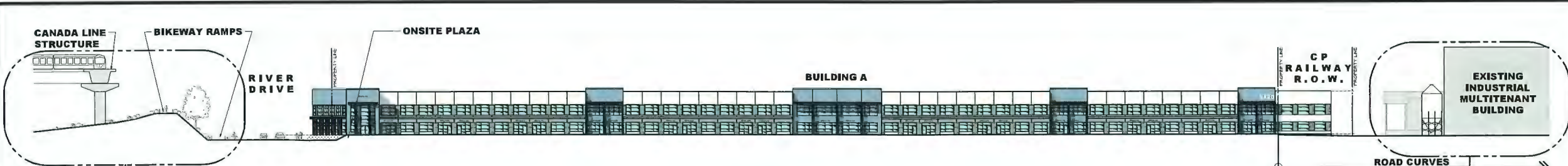
DRAWING

**CONTEXT
PLAN & PHOTOS**

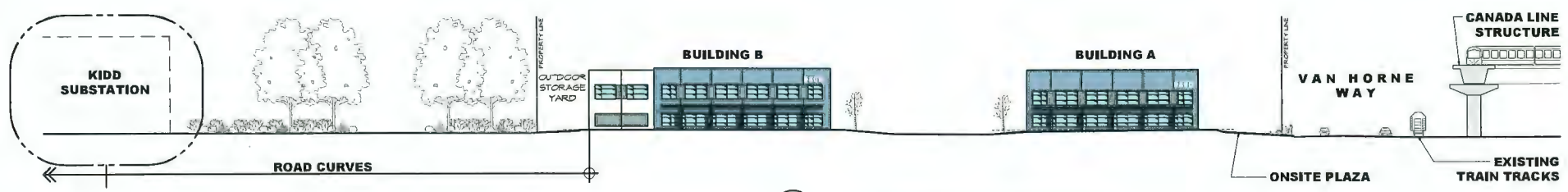
SEAL	JOB NO. 21-029	DRAWN DRVCF
	DESIGNED	R.D.
	CHECKED	R.D.
	PLOT DATE	MAR.13.23

PROJECT - DRAWING NUMBER
A-0.2

REV.
4



1 **STREETSCAPE PARALLEL TO VAN HORNE WAY**
SCALE: NTS



2 **STREETSCAPE PARALLEL TO RIVER DRIVE**
SCALE: NTS

NOTE: ALL OFFSITE STRUCTURES ARE NOTIONAL ONLY. REFER TO PHOTOGRAPHS FOR ADDITIONAL DETAILS



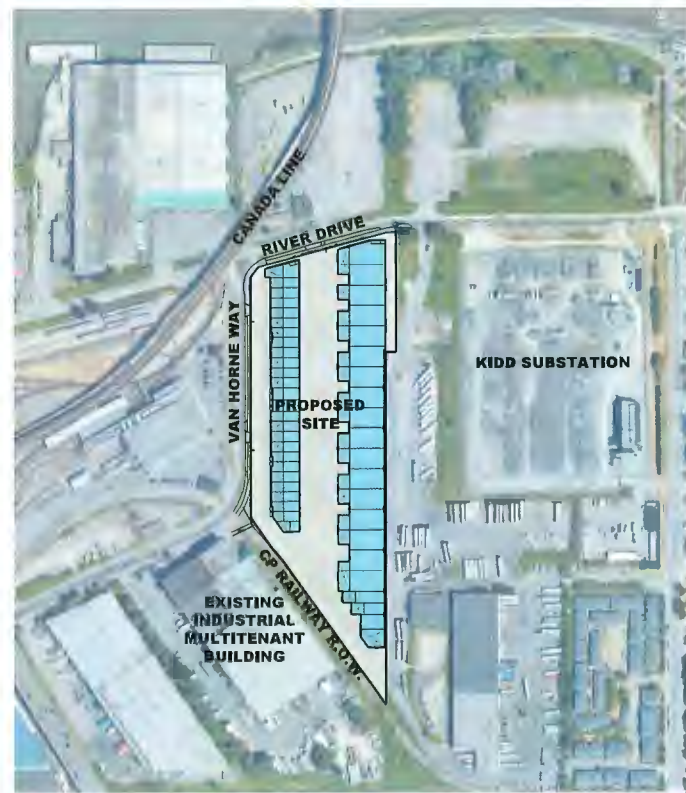
3 **VIEW OF NEIGHBOUR TO THE SOUTH**
SCALE: NTS



4 **VIEW OF NEIGHBOUR TO THE EAST**
SCALE: NTS



5 **VIEW OF INTERSECTION AT RIVER DR & VAN HORNE WAY**
SCALE: NTS



6 **CONTEXT PLAN**
SCALE: NTS

ARCHITECTURE PANEL INC.
ARCHITECTURE, LANDSCAPE ARCHITECTURE, URBAN DESIGN
228 - 1493 FOSTER STREET, WHITE ROCK, BC V8B 0C4
604.383.5001 | A-0.2a@ARCHITECTUREPANEL.COM

IN ASSOCIATION WITH
D.FORCE DESIGN INC.
ARCHITECTURE, INTERIOR ARCHITECTURE, PERFORMING ARTS, MUSIC, THEATRE, FILM, VIDEO, DIGITAL MEDIA, GRAPHIC DESIGN, VISUAL COMMUNICATIONS, USER EXPERIENCE DESIGN

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CEDAR COAST

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2 1986.11.23 UPDATED PER AEP SCHEMATIC
1 2012.01.11 ISSUED FOR AEP PRELIMINARY
01 DATE DESCRIPTION
PROJECT MANAGERS / CONTRACTORS
ORION CONSTRUCTION
ORION CONSTRUCTION
104 - 3950 191 STREET,
SURREY BC, V2Z 0Y6
PHONE: (604) 382-2594

PROJECT
CEDAR COAST VAN HORNE

ADDRESS: 104 VAN HORNE WAY, WHITE ROCK, BC
DRAWING

STREETSCAPE ELEVATIONS

SCALE	JOB NO.	DRAWN BY
	21-029	WCP
	DESIGNED	
	CHECKED	RLS
	PLLOT DATE	MAR, 13, 23

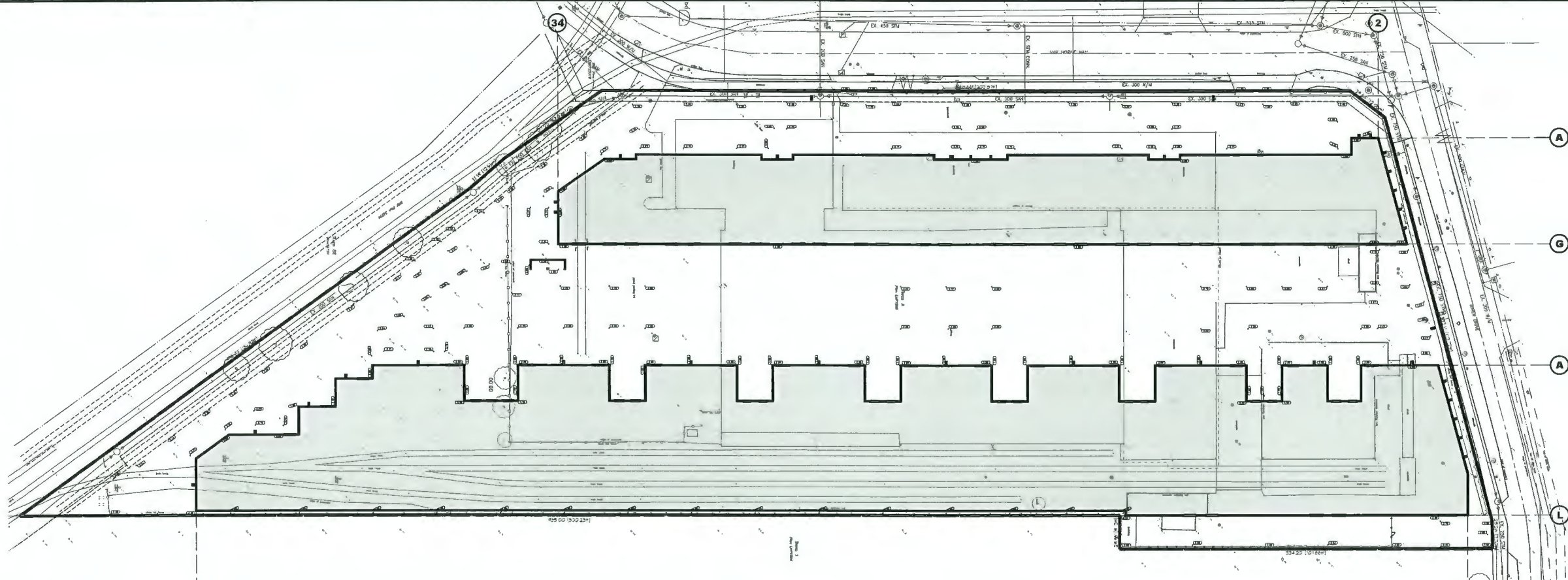
PROJECT - DRAWING NUMBER
A-0.2a 2

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NO.	DATE	DESCRIPTION
1	OCT.24.21	ISSUED FOR CP
2	JUN.10.22	REISSUED FOR CP
3	AUG.11.22	FIRE RESPONSE COMMENTS
4	AUG.11.22	UPDATED PER CITY COMMENT
5	MAR.13.23	UPDATED PER ARCH COMMENT

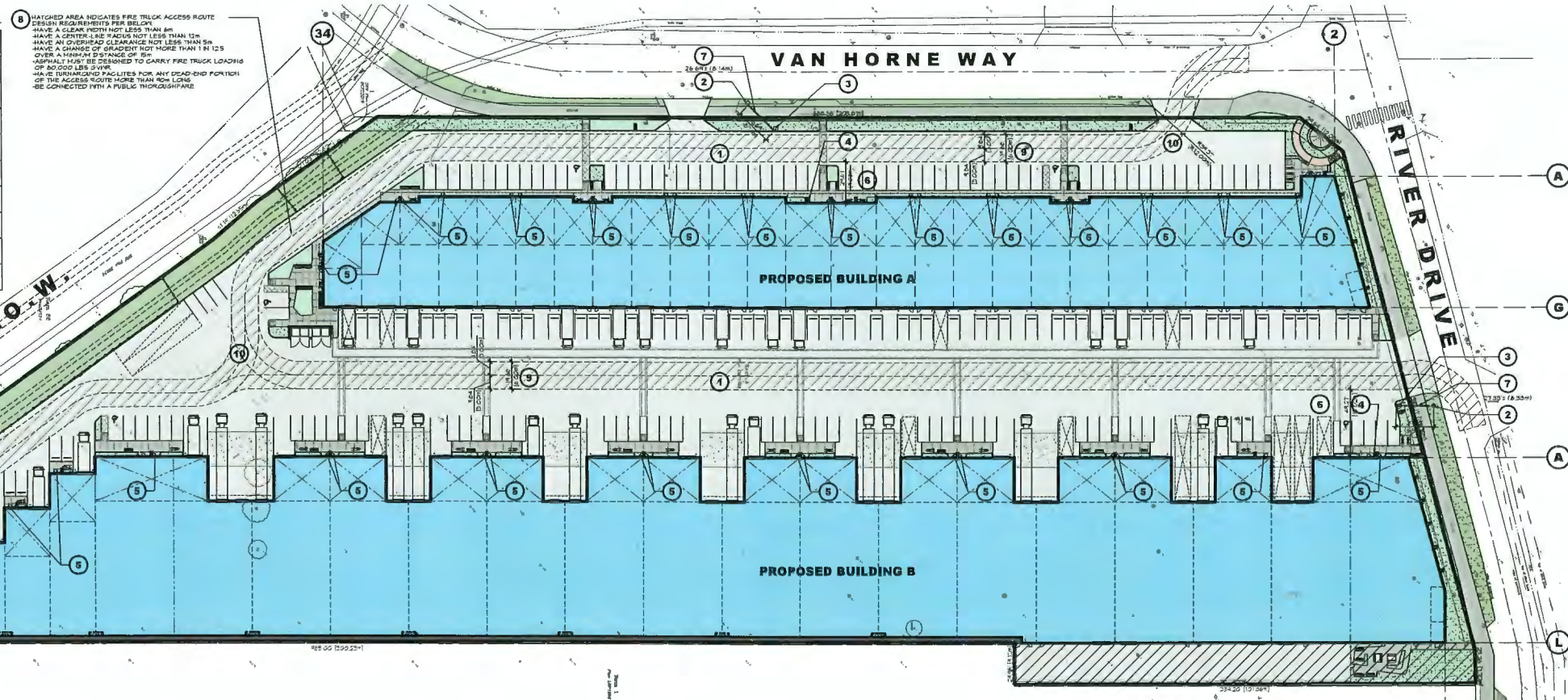
PROJECT MANAGERS / CONTRACTORS	
ORION CONSTRUCTION	
ORION CONSTRUCTION 104 - 3550 191 STREET, SURREY BC, V3Z 0Y8 PHONE: (604) 362-2994	

CEDAR COAST VAN HORNE		
ADDRESS: 1800 VAN HORNE WAY, RICHMOND, BC		
DRAWING: TOPOGRAPHY PLAN & FIRE RESPONSE PLAN		
SEAL	JOB NO. 21403	DRAWN BRV/CF
	DESIGNED	R.D.
	CHECKED	R.D.
	PLOT DATE	MAR.13.23
PROJECT-DRAWING NUMBER A-0.3	REV.	5



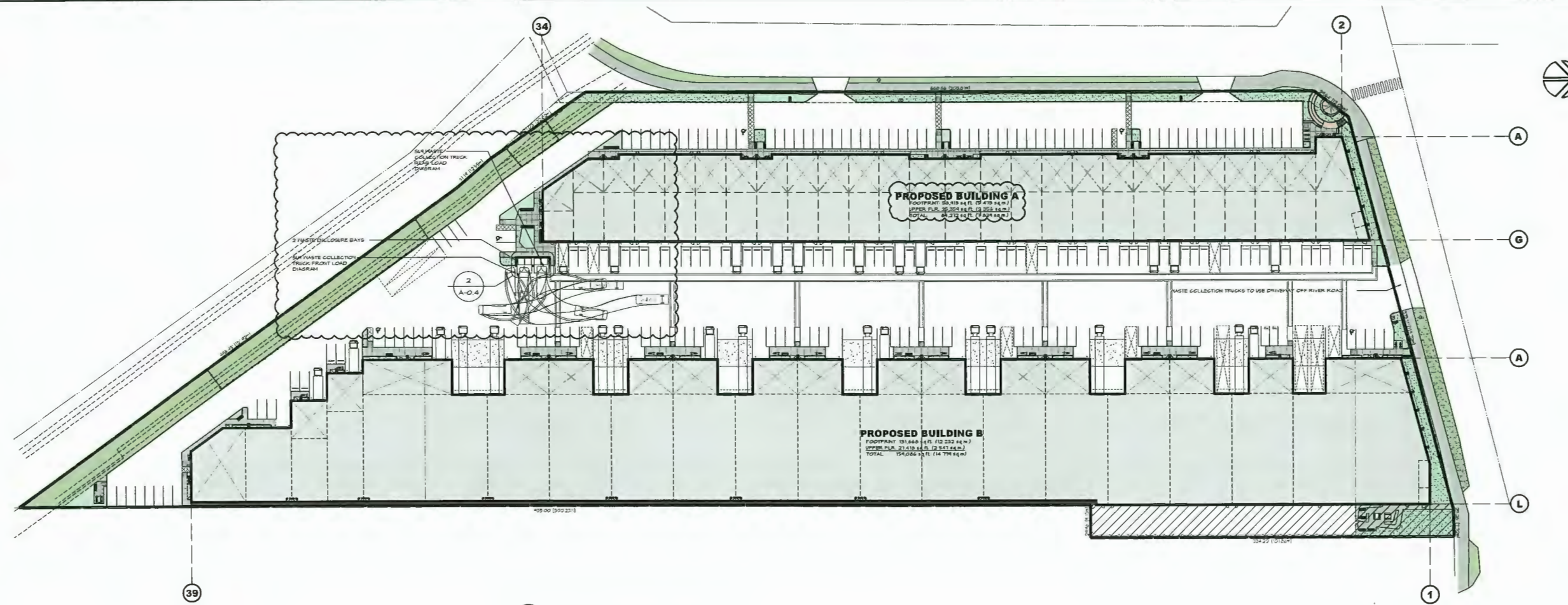
1 TOPOGRAPHY PLAN
 SCALE: 1" = 40'-0"
 LEGEND
 EXISTING GRADE: X.858
 PROPOSED GRADE: (11.10)
 SELECT GRADES SHOWN. REFER TO CIVIL PLANS AND TOPOGRAPHY PLAN FOR GRADES.

- KEYED FIRE RESPONSE PLAN NOTES**
- 1 A CLEARLY MARKED OR HIGHLIGHTED ACCESS ROUTE(S)
 - 2 THE LOCATION OF HIGH AND/or OTHER FIRE HAZARDS
 - 3 THE LOCATION OF THE FIRE DEPARTMENT (IF KNOWN)
 - 4 THE LOCATION OF THE FIRE ALARM INDICATOR PANEL(S)
 - 5 THE LOCATION OF THE PRIMARY ENTRANCE TO THE BUILDING AND ANY PRIMARY ENTRANCES TO UNITS NOT GOING DIRECT TO THE BUILDING THROUGH AN INTERNAL CORRIDOR
 - 6 THE MINIMUM DISTANCE FROM THE PRINCIPAL ENTRANCE TO THE CLOSEST PORTION OF THE ACCESS ROUTE. (SIBC 3.3.3.3)
 - 7 THE MINIMUM UNOBSTRUCTED WIDTH OF THE ACCESS ROUTE(S) AT THE FIRE DEPARTMENT CONNECTION(S). (SIBC 3.3.3.3)
 - 8 THE ACCESS ROUTE THAT MUST BE DESIGNED TO SUPPORT A MINIMUM HEIGHT OF 90 000 LBS. (SIBC 3.3.3.3)
 - 9 THE MINIMUM WIDTH OF THE ACCESS ROUTE. (SIBC 3.3.3.3)
 - 10 THE MINIMUM CLEARANCE RADIUS OF THE ACCESS ROUTE. (SIBC 3.3.3.3)



2 FIRE RESPONSE PLAN
 SCALE: 1" = 40'-0"

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1 WASTE MANAGEMENT PLAN
 SCALE: 1" = 40'-0"

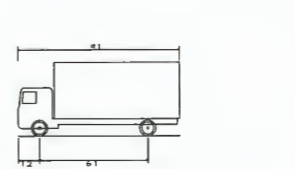


CITY OF RICHMOND RECOMMENDATIONS:

Floor Area (sq ft)	Recycling Materials & Waste						
	Mixed Container	Recyclable Beverage Containers	Mixed Paper (Including newspapers)	Glass	Food Scraps and Yard Trimmings	Cardboard Bin	Cubic Yard Bin (Est)
1-700	1	1	1	1	1	1	1
701-1400	1	1	2	1	1	1	2
1401-2100	1	1	3	1	1	1	3
2101-2800	1	1	4	1	1	1	4
2801-3500	1	1	5	1	1	1	5
3501-4200	1	1	6	1	1	1	6
4201-4900	1	1	7	1	1	1	7
4901-5600	1	1	8	1	1	1	8
5601-6300	1	1	9	1	1	1	9
6301-7000	1	1	10	1	1	1	10
7001-7700	1	1	11	1	1	1	11
7701-8400	1	1	12	1	1	1	12
8401-9100	1	1	13	1	1	1	13
9101-9800	1	1	14	1	1	1	14
9801-10500	1	1	15	1	1	1	15
10501-11200	1	1	16	1	1	1	16
11201-11900	1	1	17	1	1	1	17
11901-12600	1	1	18	1	1	1	18
12601-13300	1	1	19	1	1	1	19
13301-14000	1	1	20	1	1	1	20
14001-14700	1	1	21	1	1	1	21
14701-15400	1	1	22	1	1	1	22
15401-16100	1	1	23	1	1	1	23
16101-16800	1	1	24	1	1	1	24
16801-17500	1	1	25	1	1	1	25
17501-18200	1	1	26	1	1	1	26
18201-18900	1	1	27	1	1	1	27
18901-19600	1	1	28	1	1	1	28
19601-20300	1	1	29	1	1	1	29
20301-21000	1	1	30	1	1	1	30

WASTE COLLECTION CALCULATIONS

BIN RECOMMENDATIONS PROVIDED BY MAPLE LEAF DISPOSAL LTD.
 TOTAL WASTE PRODUCED DEPENDANT ON END USERS. THESE ARE ESTIMATES ONLY.
 RECOMMENDATIONS FOR 2 INDUSTRIAL BUILDINGS WITH ONCE-A-WEEK PICK UP CYCLE:
 1 X ORGANIC CART (240 LITRES/64 GAL)
 1 X FRONT END CONTAINER FOR GENERAL WASTE (NON-RECYCLABLE)
 1 X FRONT END CONTAINER FOR CARDBOARD
 CARTS FOR RECYCLING - TO BE DETERMINED AT TIME OF CONTRACT

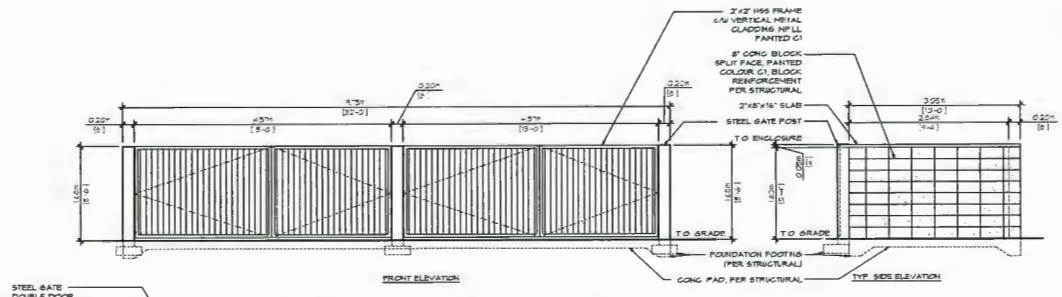


SU4 - Single Unit or Bus
 Overall Length 9.100m
 Overall Width 2.600m
 Overall Body Height 4.150m
 Min Body Ground Clearance 0.445m
 Track Width 2.600m
 Lock-to-lock time 4.00s
 Curb to Curb Turning Radius 12.800m

4 SU9 PROFILE
 SCALE: NTS



3 WASTE COLLECTION BIN EXAMPLE IMAGES
 SCALE: 1/4" = 1'-0"



DEVELOPER/STRATA COMPANY TO ENSURE CONTRACT WITH LOCAL WASTE HAULER (SUCH AS MAPLE LEAF DISPOSAL LTD.) IN PLACE TO ENSURE REGULAR AND ONGOING WASTE REMOVAL OF THE SITE.

ALL WASTE BINS TO BE PROVIDED BASED ON THE RECOMMENDATIONS OF WASTE HAULER UNDER CONTRACT, AND IN CONFORMANCE WITH CITY OF RICHMOND STANDARDS, BASED ON TENANT REQUIREMENTS.

NO.	DATE	DESCRIPTION
1	OCT.04.21	ISSUED FOR DP
2	MAY.17.22	ISSUED FOR CLIENT USE
3	JUN.10.22	ISSUED FOR DP
4	AUG.16.22	UPDATED PER CITY COMMENTS
5	DEC.11.22	UPDATED PER CITY COMMENT
6	MAR.11.23	UPDATED PER ACP COMMENT

ORION CONSTRUCTION
 ORION CONSTRUCTION
 104-3950 191 STREET,
 SURREY BC, V3Z 0Y6
 PHONE: (604) 362-2994

CEDAR COAST VAN HORNE

ADDRESS: 380 VAN HORNE WAY, RICHMOND, BC

WASTE MANAGEMENT PLAN & DETAILS

SEAL:	JOB NO.:	DRAWN BY:
	21-029	DP/CP
	DESIGNED	
	CHECKED	RLD
	PLOT DATE	MAR.13.23
PROJECT - DRAWING NUMBER		REV.
A-0.4		6

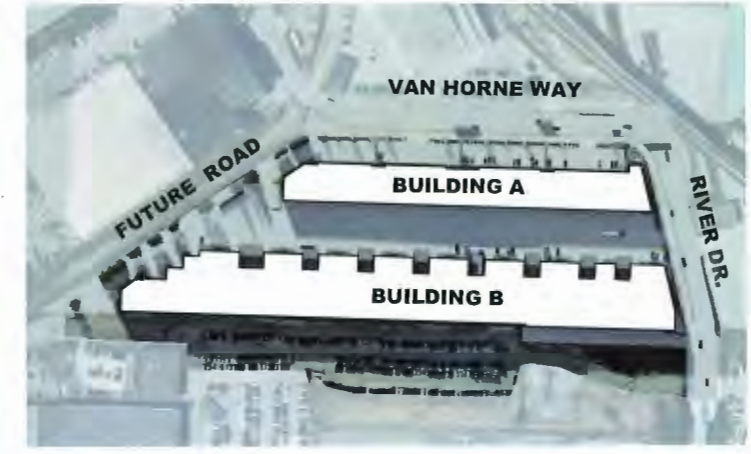
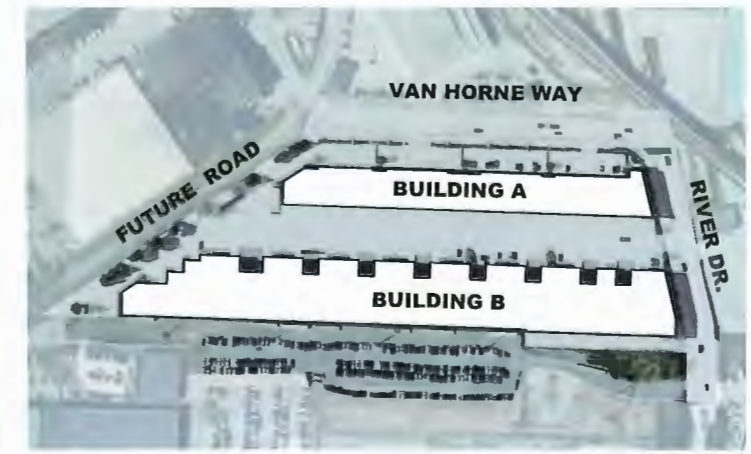
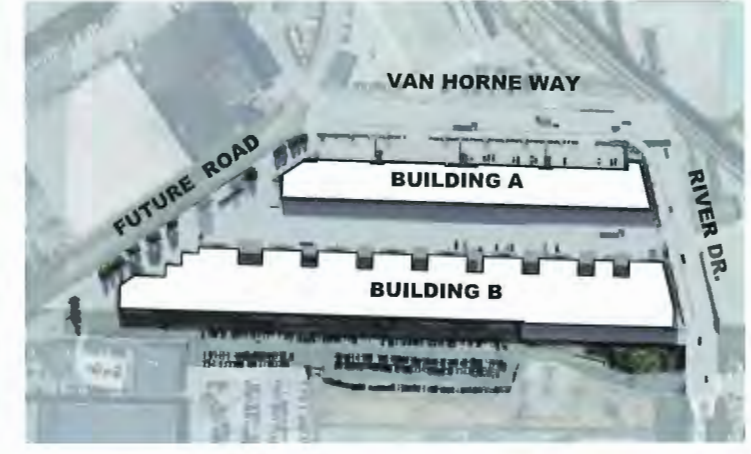
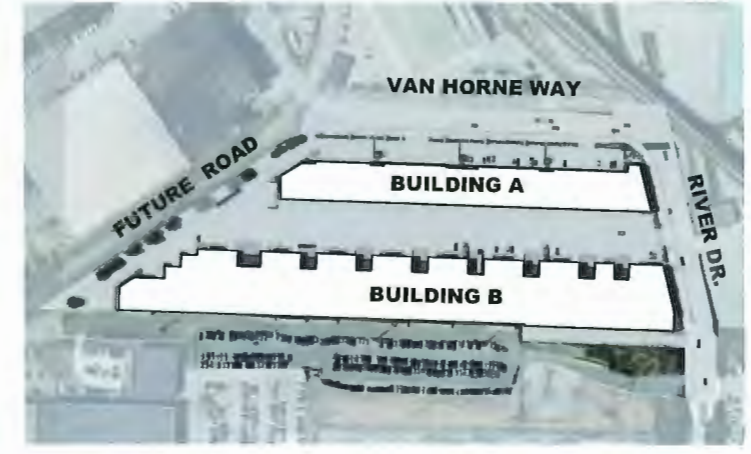
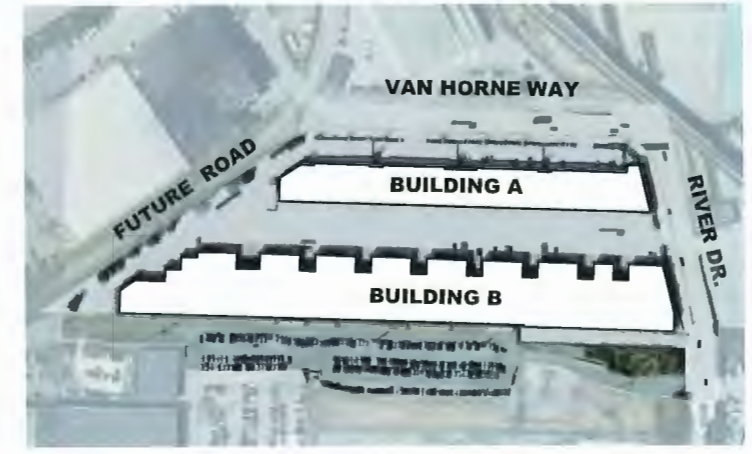
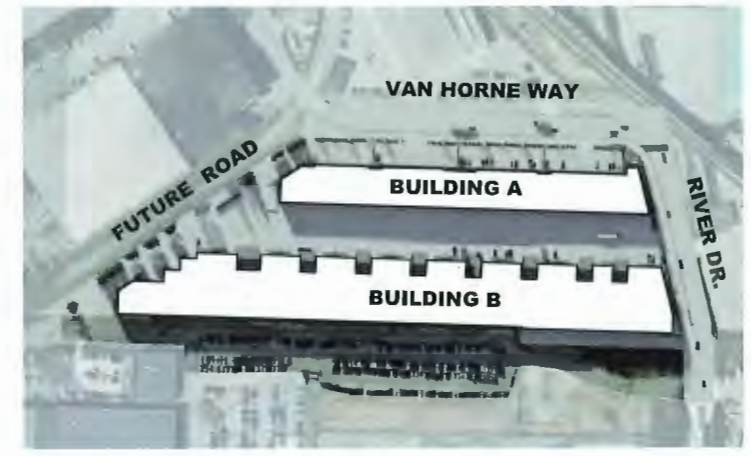
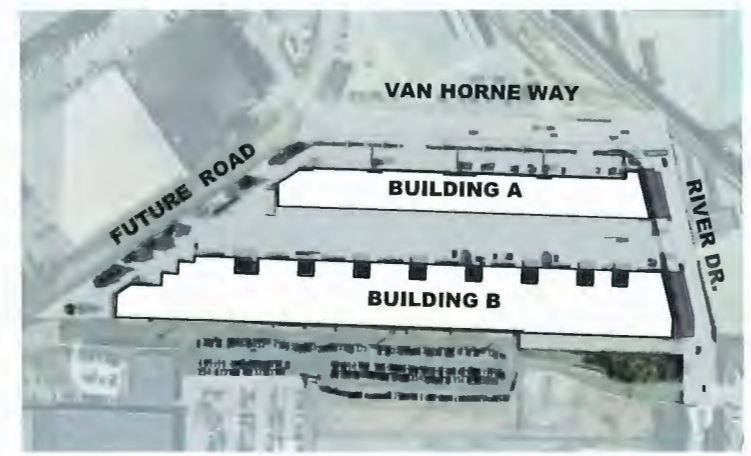
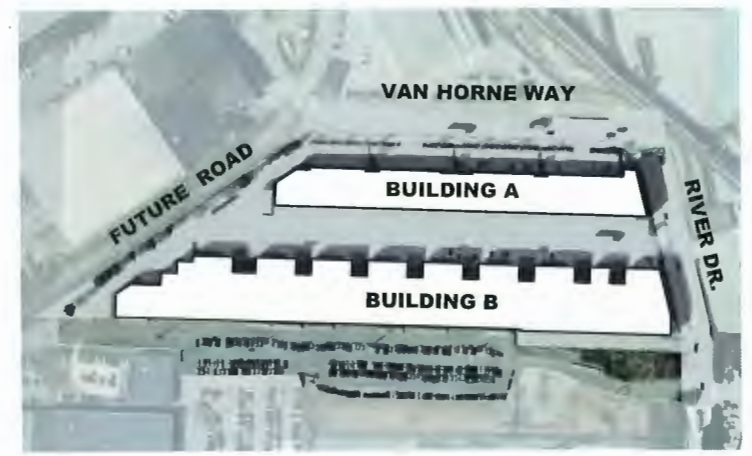


ARCHITECTURE PANEL INC.
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 204 - 1043 FORT STREET, WHITE ROCK, BC, V4B 3C4
 604 363 3600 | INFO@ARCHITECTUREPANEL.COM

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 10:00 am


 12:00 noon


 4:00 pm

MARCH 21

JUNE 21

SEPTEMBER 21

 CEDAR COAST

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3	MARK 1121	APPROVED PER ACP COMMENT
2	08/11/22	APPROVED PER CITY COMMENT
1	2/16/22	RECEIVED FOR OIP
NO.	DATE	DESCRIPTION

PROJECT MANAGER / COORDINATOR:


ORION CONSTRUCTION
 ORION CONSTRUCTION
 104 - 3950 191 STREET,
 SURREY BC, V3Z 0Y6
 PHONE: (604) 362-2994

PROJECT:
CEDAR COAST VAN HORNE

ADDRESS: 980 VAN HORNE WAY, RICHMOND BC
 ORP49840

SHADOW STUDY

REAL	JOB NO.	DRAWN
	24-229	BRUNN
CHECKED		R.D.
DATE	DATE	DATE
	MARK 1121	

PROJECT - DRAWING NUMBER: **A-0.5** REV: **3**

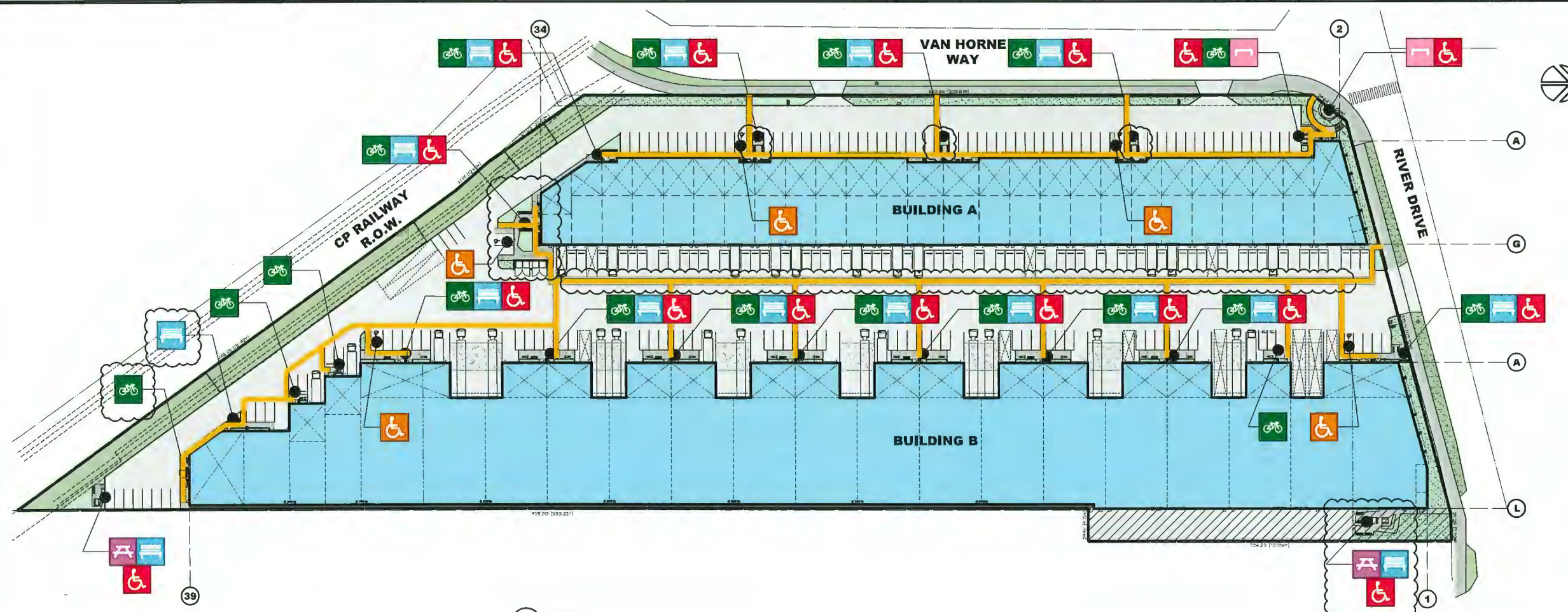
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3	1 MAR 13 23	UPDATED PER ACP COMMENT
2	1 JAN 26 23	ISSUED FOR ACP MEETING
1	1 DEC 15 22	UPDATED PER CITY COMMENT
NO.	DATE	DESCRIPTION

PROJECT MANAGERS / CONTRACTORS
ORION CONSTRUCTION
 ORION CONSTRUCTION
 104 - 3650 191 STREET,
 SURREY BC, V3Z 8Y6
 PHONE: (604) 362-2994

PROJECT
CEDAR COAST VAN HORNE
 ADDRESS: 9900 VAN HORNE WAY, RICHMOND, BC

ACCESSIBILITY & AMENITY AREA PLAN

SEAL	JOB NO. 21-023	DRAWN DR/CF
	DESIGNED	
	CHECKED R.D.	
	PLOT DATE MAR 13 23	
PROJECT - DRAWING NUMBER		REV. 3
A-0.6		



1 ACCESSIBILITY & AMENITY AREA PLAN
 SCALE: 1" = 40'-0"



AMENITY PLANNING DESIGN RATIONAL

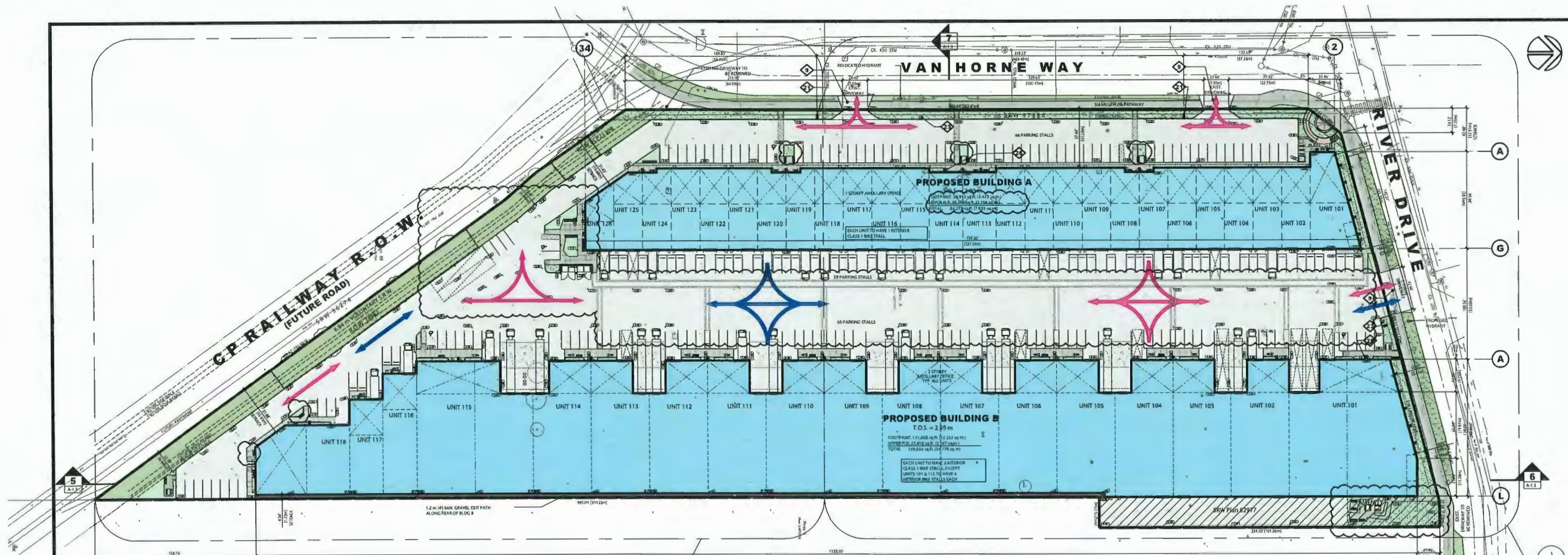
- KEY DESIGN CONSIDERATIONS:
- DECENTRALIZED
 - ALLOWS FOR ACCESS FROM A VARIETY OF UNITS AT A TIME AND REDUCES WALKING DISTANCE FROM UNITS
 - VARYING AMENITY PROVISIONS
 - MEETS THE VARYING DENSITY NEEDS OF THE DIFFERENT BUILDING STYLES BY PROVIDING MORE DENSE AMENITIES AT BUILDING A WHICH WILL HAVE A HIGHER USER RATE THAN BUILDING B
 - MULTIPLE STREET CONNECTIONS
 - ACCESS POINTS TO THE SITE HAVE BEEN PROVIDED ALONG BOTH ROADWAYS AND PROVISIONS HAVE BEEN MADE FOR THE FUTURE ROAD TO THE SOUTH TO ALLOW FOR MULTIPLE CONNECTIONS TO THE SITE FOR PEDESTRIANS AND CYCLISTS
 - SOUTHERN EXPOSURE
 - EFFORTS HAVE BEEN MADE TO TAKE ADVANTAGE OF THE SOUTHERN EXPOSURE TO SUNLIGHT FOR MANY OF THE AMENITY AREAS

ACCESSIBILITY RATIONAL

- FULLY COMPLIANT PER BCGC 2018 SECTION 3.8.4 BUILDING ACCESS HANDBOOK
- ALL UNITS ARE ACCESSIBLE
 - ACCESSIBLE MAIN FLOOR (ALL AMENITIES PROVIDED ON MAIN FLOOR) - 3.8.2.1.1(F)
 - 50% OF PEDESTRIAN ENTRANCES ARE ACCESSIBLE - 3.8.2.2.1
 - ACCESSIBLE PATH OF TRAVEL
 - ACCESSIBLE PATH OF TRAVEL PROVIDED FROM ACCESSIBLE PARKING STALLS TO UNIT ENTRANCES - 3.8.2.5
 - 1500mm WIDE PATH PROVIDED TO ALL UNIT ENTRANCES - 3.8.3.3
 - ACCESSIBLE WASHROOM
 - ALL UNITS PROVIDED WITH ONE ACCESSIBLE UNIVERSAL WASHROOM - 3.8.2.8
 - ALL ACCESSIBLE UNIVERSAL WASHROOMS TO CONFORM TO THIS SECTION - 3.8.3.12
 - SIGNS & INDICATORS
 - ALL UNITS PROVIDED WITH SIGNAGE FOR ACCESSIBLE WASHROOMS & PARKING STALLS - 3.8.2.10.1
 - ALL STAIRS & RAMPS PROVIDED WITH TACTILE SURFACE INDICATORS - 3.8.2.10.3
 - ACCESSIBLE PARKING STALLS
 - ACCESSIBLE STALLS PROVIDED PER THE CITY OF RICHMOND ZONING BYLAW 7.5.11 & 7.5.14

SYMBOL LEGEND

- CLASS 2 EXTERIOR BIKE PARKING
- PICNIC TABLE
- BENCH
- CONCRETE BENCH SEAT
- ACCESSIBLE PARKING STALL
- ACCESSIBLE ACCESS TO AMENITIES
- ACCESSIBLE PATH OF TRAVEL



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KEYED SITE PLAN NOTES

- 1 INTERNATIONAL SYMBOL OF ACCESS PAINTED ON ASPHALT PER DETAIL 3/A-1.4
- 2 BUILDING HOISTED H.C. SIGN PER DETAIL 2/A-1.4
- 3 5'X7' REFRIGERATOR LIFT OFF FROM MINIMUM 5' CLEAR AT TOP OF LIFTDOWN. SEE DETAIL 1/A-1.4
- 4 CONCRETE SIDEWALK 6" ± ABOVE ADJACENT ASPHALT DRIVE BROOK FRESH
- 5 6"X EXTRUDED CONCRETE CURB TYPICAL AT EDGE OF LANDSCAPING. PER DETAIL 4/A-1.4
- 6 LANDSCAPING PER LANDSCAPE ARCHITECT'S DRAWINGS
- 7 WHEELSTOP PER DETAIL 7/A-1.4
- 8 4" PAINT STRIKE TYPICAL
- 9 ASPHALT CROSSING TO MUNICIPAL SPECS
- 10 6"X 1/4" THICK CONCRETE FLEED STEEL ROLLBAR PAINTED "SMOOTH YELLOW" OR EQUIVALENT AT EACH SIDE OF GARAGE LEVEL. TYPICAL AT ALL GARAGE DOORS.
- 11 RETAINING WALL, SHIP LAMINATE (1/4" TYPE GUARDRAIL). GUARDRAIL REQUIRED UNTIL THE GRADE DIFFERENCE IS LESS THAN 2'-0" (600mm)
- 12 LINE OF CANOPY ABOVE. SEE PLANS AND DETAILS FOR MORE INFORMATION
- 13 EXTERIOR GARAGE ENCLOSURE. SEE WALL MANAGEMENT PLAN AND DETAILS A-2.3
- 14 CONCRETE GULLY PAD
- 15 6" X 1/4" EXTERIOR BICYCLE PARKING (2 SPOTS PER BIKE BAY). PER DETAIL 8/A-1.4
- 16 6" X 1/4" EXTERIOR BICYCLE PARKING (2 SPOTS PER BIKE BAY). PER DETAIL 8/A-1.4
- 17 BRUSH PER LANDSCAPE DRAWINGS
- 18 ELECTRICAL & MECHANICAL ROOMS / EQUIPMENT
- 19 GAS METERS FOR MECHANICAL DRAWINGS WITH FLICE AND PROTECTION AS REQUIRED BY BC, WISCONSIN AND MUNICIPALITY. SEE DETAIL 1/A-1.4
- 20 UNIT SUBSTATION ROOM. SEE ELECTRICAL DRAWING-GS
- 21 FREE STANDING SIGNAGE. SEE DETAIL 6/A-1.4
- 22 TYPICAL ACCESSIBLE ENTRANCE
- 23 FIRE DEPARTMENT CONNECTION. SEE MECHANICAL DRAWINGS
- 24 TRAVEL DISTANCE HYDRANT TO FOC (147' ASH) MAX CONFORM C-9 (BFD)
- 25 MATCHED AREA INDICATES FIRE TRUCK ACCESS ROUTE DESIGN REQUIREMENTS FOR BELOW:
 - HAVE A CLEAR WIDTH NOT LESS THAN 6m
 - HAVE A CLEAR HEIGHT NOT LESS THAN 12m
 - HAVE AN OVERHEAD CLEARANCE NOT LESS THAN 5m
 - HAVE A CHANGE OF GRADE NOT MORE THAN 1:10
 - OVER 8' BARRIERS (10' MIN) OF 100mm ASPHALT MUST BE DESIGNED TO CARRY 100 TONS LOADS OF 100 TONS
 - HAVE TURNING FACILITIES FOR ANY DEAD END PORTION OF THE ACCESS ROUTE MORE THAN 10m LONG
 - BE CONNECTED WITH A PUBLIC THROUGHFARE
- 26 IAC LISTED EXTERIOR ANTI-RADIATION PANEL. SEE ELECTRICAL DRAWINGS
- 27 EV CHARGING JUNCTION BOX & ROUGH-IN. SEE ELECTRICAL DRAWINGS FOR DETAILS
- 28 SQUARE-CLIP PATTERN SAWCUT. C.W. CONTRASTING COLOUR FINISH. SEE LANDSCAPE DRAWINGS FOR DETAILS

LEGEND

- EXISTING GRADE: 8.855
- PROPOSED GRADE: 11.10
- SELECT GRADES SHOWN REFER TO CIVIL PLANS AND TOPOGRAPHY PLAN FOR GRADE

ARROW LEGEND

- CAR TRAFFIC FLOW: (Red arrow pointing right)
- LOADING TRAFFIC FLOW: (Blue arrow pointing left)

UNIT AREAS

Building	Unit	Area (sq. ft.)	Area (sq. m.)
Building A	101	1,100	101.7
	102	1,100	101.7
	103	1,100	101.7
	104	1,100	101.7
	105	1,100	101.7
	106	1,100	101.7
	107	1,100	101.7
	108	1,100	101.7
	109	1,100	101.7
	110	1,100	101.7
	111	1,100	101.7
	112	1,100	101.7
	113	1,100	101.7
	114	1,100	101.7
	115	1,100	101.7
	116	1,100	101.7
	Building B	117	1,100
118		1,100	101.7

OVERALL SITE DATA

CIVIC ADDRESS: 8500 VAN HORNE WAY, RICHMOND, BC
 LEGAL DESCRIPTION: LOT A SEC 22 BLD ON PG 6W PL LAPS 565 LOT A, BLOCK 88, PLAN LAPS 565, SECTION 22, RANGE 6W, T10W WESTMINSTER LAND DISTRICT, EXCEPT PLAN B022019
 EXISTING ZONING: I-1
 CADDS LOT AREA: 36 379.78 m² (391,282.75 sq. ft. / 8,999 AC.)
 PROPOSED BUILDING AREA (FOOTPRINT):
 BUILDING A: 5 473 m² (58,813 sq. ft.)
 BUILDING B: 12 232 m² (131,668 sq. ft.)
 TOTAL: 17 705 m² (190,481 sq. ft.)
 SITE COVERAGE: 60% MAX.
 F.A.R.: 1.2 MAX.
 190,481 ± 15,777 / 391,282.75 sq. ft. = 64%

PARKING CALCULATIONS

Building Area	sq. ft.	sq. ft.	sq. ft.
Footprint:	58,013	131,668	190,581
Upper floor area:	25,359	27,418	52,777
Total Area:	84,272	159,086	243,358
Industrial:	33,554	131,668	165,222
Auxiliary Office (1st Floor):	25,359	-	25,359
2nd floor storage:	25,359	27,418	52,777
Parking Stalls Required:			
General Industrial - 1 stall per:	1,435	23.4	91.7
Auxiliary Office - 1 stalls per:	1,435	35.3	58
Total:	59	111	170.0
Total after 15% City Centre reduction:	51	95	146.0
Stalls provided:	125	65	190

Parking Dimensions

Stall Type	Width (m)	Depth (m)
Regular Stall (90°)	2.45	5.5
Small Car Stall	2.4	5
Accessible Stall	2.5	5.5
Accessible Stall (van)	3.4	5.5
Accessible Aisle	1.5 min.	5.5
Dead End Stall (90°)	3.25	5.5
Medium Loading Stall	3	9.1
Large Loading Stall	3.5	19
Class 1 Bike Space	0.6 min.	1.8 min.
Class 2 Bike Space	0.6 min.	1.8 min.
Minimum Drive Aisle*	7.5	
Minimum Sidewalk*	1.5	
Minimum Crosswalk*	1.5	

*May vary, plans take precedent

SITE DATA - BUILDING A

STREETS: MILL (PROPOSED)
 FRONT DIMENSION: 241 (9.84') 3.68 m (119.32')
 SIDE (EAST): 646 (25.4') 18.81 m (61.71')
 SIDE (WEST): 141 (9.84') 12.93 m (42.40')
 REAR DIMENSION: 1548 (57.41') 45.95 m (150.72')
 HEIGHT: 16m (52.47') MAX. 9.95m (32.64')
 ACCESSIBLE STALLS REQUIRED (2%): 133 @ 0.82 = 2.87 STALLS
 ACCESSIBLE STALLS PROVIDED: 3 STALLS
 (3 VAN ACCESSIBLE - 50% OF REQ'D)
 BIKE PARKING REQUIRED:
 CLASS 1: 1 PER 370.27 sq. m. (12,964.53 sq. ft.) 84,272 sq. ft. / 12,964.53 sq. ft. = 6.5
 CLASS 2: 1 PER 370.27 sq. m. (12,964.53 sq. ft.) 84,272 sq. ft. / 12,964.53 sq. ft. = 6.5
 24 HORIZONTAL INTERIOR STALLS PROVIDED
 22 HORIZONTAL EXTERIOR STALLS PROVIDED
 LOADING SPACES REQUIRED:
 MEDIUM: 1 SPACE PLUS 1 SPACE FOR EACH ADDITIONAL 5,000.00 OVER 1,800.00 M² 3 SPACES REQUIRED
 LARGE: 1 SPACE FOR EACH 5,000.00 M² OVER 1,800.00 M² 2 SPACES REQUIRED
 2 SPACES PROVIDED

SITE DATA - BUILDING B

STREETS: MILL (PROPOSED)
 FRONT DIMENSION: 241 (9.84') 3.68 m (119.32')
 SIDE (EAST): 646 (25.4') 18.81 m (61.71')
 SIDE (WEST): 141 (9.84') 12.93 m (42.40')
 REAR DIMENSION: 1548 (57.41') 45.95 m (150.72')
 HEIGHT: 16m (52.47') MAX. 9.95m (32.64')
 SMALL CAR STALLS PROVIDED: 2 STALLS
 ACCESSIBLE STALLS REQUIRED (2%): 49 @ 0.82 = 1.3 STALLS
 ACCESSIBLE STALLS PROVIDED: 2 STALLS
 (1 VAN ACCESSIBLE - 50% OF REQ'D)
 BIKE PARKING REQUIRED:
 CLASS 1: 1 PER 370.27 sq. m. (12,964.53 sq. ft.) 159,086 sq. ft. / 12,964.53 sq. ft. = 12.3
 CLASS 2: 1 PER 370.27 sq. m. (12,964.53 sq. ft.) 159,086 sq. ft. / 12,964.53 sq. ft. = 12.3
 40 HORIZONTAL INTERIOR STALLS PROVIDED
 40 HORIZONTAL EXTERIOR STALLS PROVIDED
 LOADING SPACES REQUIRED:
 MEDIUM: 1 SPACE PLUS 1 SPACE FOR EACH ADDITIONAL 5,000.00 OVER 1,800.00 M² 4 SPACES REQUIRED
 LARGE: 1 SPACE FOR EACH 5,000.00 M² OVER 1,800.00 M² 4 SPACES PROVIDED
 4 SPACES PROVIDED

CEDAR COAST

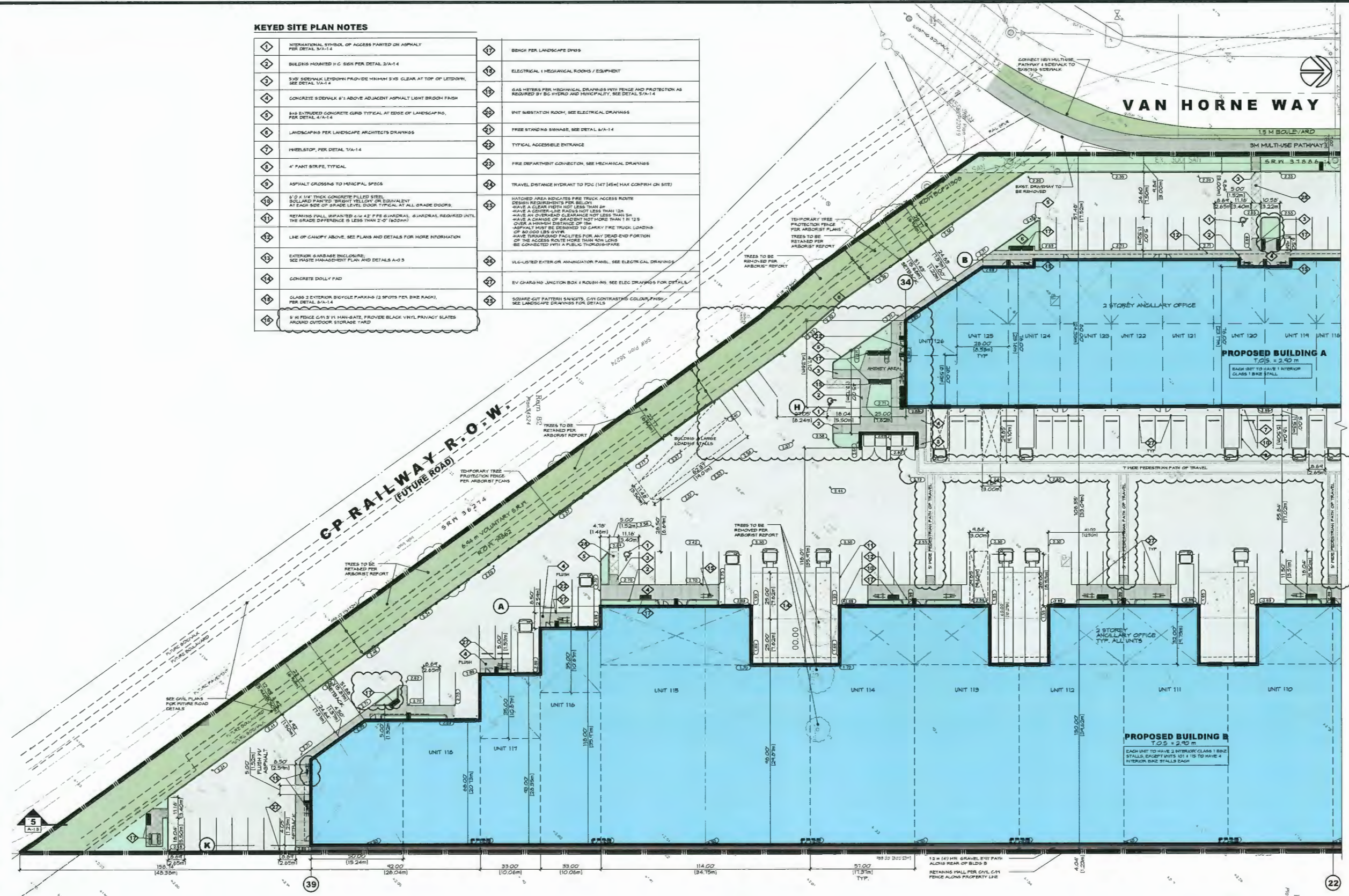
NO.	DATE	DESCRIPTION
1	APR. 14, 21	CLIENT REVIEW
2	APR. 30, 21	CLIENT REVIEW
3	MAY. 7, 21	CLIENT REVIEW
4	JUN. 29, 21	CLIENT REVIEW
5	JUL. 5, 21	CLIENT REVIEW
6	AUG. 10, 21	CONSULTANT USE
7	SEP. 21, 21	CONSULTANT USE
8	OCT. 24, 21	ISSUED FOR CP
9	MAY. 17, 22	CLIENT REVIEW
10	MAY. 31, 22	CONSULTANT COORDINATION
11	JULY 04, 22	CONSULTANT COORDINATION
12	JULY 10, 22	RE-ISSUED FOR DP
13	OCT. 10, 22	ISSUED FOR CITY COMMENTS
14	MAR. 06, 23	CONSULTANT USE
15	MAR. 13, 23	ISSUED PER ADP COMMENT

PROJECT MANAGERS / CONTRACTORS
ORION CONSTRUCTION
 ORION CONSTRUCTION
 104 - 3950 191 STREET,
 SURREY BC, V3Z 0Y6
 PHONE: (604) 362-2994

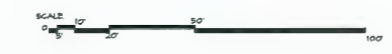
PROJECT: **CEDAR COAST VAN HORNE**
 ADDRESS: 8500 VAN HORNE WAY, RICHMOND BC
 DRAWING: **OVERALL SITE PLAN**
 SEAL: [Signature] JOB NO: 21-028 DRAWN: DP/VP
 DESIGNED: [Signature] CHECKED: R.D.
 PLOT DATE: MAR. 13, 23
 PROJECT - DRAWING NUMBER: **A-1.0** REV: **15**

KEYED SITE PLAN NOTES

INTERNATIONAL SYMBOL OF ACCESS PARTED ON ASPHALT PER DETAIL 5/A-1.4	BENCH PER LANDSCAPE DRAWING
BUILDING MOUNTED H.C. SIGN PER DETAIL 3/A-1.4	ELECTRICAL / MECHANICAL ROOMS / EQUIPMENT
5'x3' SIDEWALK LETDOWN PROVIDE MINIMUM 5'x3' CLEAR AT TOP OF LETDOWN, SEE DETAIL 5/A-1.4	GAS METERS PER MECHANICAL DRAWINGS WITH FENCE AND PROTECTION AS REQUIRED BY BC HYDRO AND MUNICIPALITY, SEE DETAIL 5/A-1.4
CONCRETE SIDEWALK 6" ABOVE ADJACENT ASPHALT LIGHT BROOD FINISH	UNIT SUBSTATION ROOM, SEE ELECTRICAL DRAWINGS
8x8 EXTRUDED CONCRETE GRID TYPICAL AT EDGES OF LANDSCAPING, PER DETAIL 6/A-1.4	UNIT STANDING SIGNAGE, SEE DETAIL 5/A-1.4
LANDSCAPING PER LANDSCAPE ARCHITECT'S DRAWINGS	TYPICAL ACCESSIBLE ENTRANCE
WHEELSTOP, PER DETAIL 1/A-1.4	FIRE DEPARTMENT CONNECTION, SEE MECHANICAL DRAWINGS
4" PAINT STRIPE, TYPICAL	TRAVEL DISTANCE HYDRANT TO FDIC (147' MAX) CONFORMANCE ON SITE
ASPHALT CROSSING TO MUNICIPAL STREET	HATCHED AREA INDICATES FIRE TRUCK ACCESS ROUTE DESIGN REQUIREMENTS PER BC2017: HAVE A CLEAR WIDTH NOT LESS THAN 8' HAVE AN OVERHEAD CLEARANCE NOT LESS THAN 10' HAVE A CENTERLINE MARKING NOT LESS THAN 10' OVER A MINIMUM DISTANCE OF 10' ASPHALT MUST BE DESIGNATED TO CARRY FIRE TRUCK LOADINGS OF 80,000 LBS GVW HAVE TURNAROUND FACILITIES FOR ANY DEAD-END PORTION OF THE ACCESS ROUTE MORE THAN 10M LONG BE CONNECTED WITH A PUBLIC THROUGHWAY
8" x 2" 1/4" THICK CONCRETE FILLED STEEL BELLOWS PAVED "BRIGHT" YELLOW OR EQUIVALENT AT EACH SIDE OF GRADE LEVEL DOOR TYPICAL AT ALL GRADE DOORS	UNLISTED EXTERIOR ANNUNCIATOR PANEL, SEE ELECTRICAL DRAWINGS
RETAINING WALL IMPANATED 2x4 P.F.F. GUARDRAIL, GUARDRAIL REQUIRED UNTIL THE GRADE DIFFERENCE IS LESS THAN 2'-0" (600MM)	EV CHARGING STATION BOX 4 ROWS MAX, SEE ELEC DRAWINGS FOR DETAILS
LINE OF CANOPY ABOVE, SEE PLANS AND DETAILS FOR MORE INFORMATION	SQUARE-GUT PATTERN SANITARY, GUY CONTRASTING COLOR, FINISH, SEE LANDSCAPE DRAWINGS FOR DETAILS
EXTERIOR SHARPS ARE INCORPORATED, SEE WASTE MANAGEMENT PLAN AND DETAILS A-0.3	
CONCRETE DOLLY PAD	
CLASS 2 EXTERIOR BICYCLE PARKING (2 SPOTS PER BIKE RACK), PER DETAIL 6/A-1.4	
6" HI FENCE 6x4 5' HI HANDBATE, PROVIDE BLACK VINYL PRIVACY SLATES AROUND OUTDOOR STORAGE YARD	



1 ENLARGED SOUTHERN SITE PLAN
SCALE: T = 20'-0"



Parking Dimensions

Stall Type	Width (m)	Depth (m)
Regular Stall (90°)	2.65	5.5
Small Car Stall	2.4	5
Accessible Stall	2.9	5.5
Accessible Stall (Van)	3.4	5.5
Accessible A/s	1.5 min.	
Dead-End Stall (90°)	3.26	5.5
Medium Loading Stall	3	9.1
Large Loading Stall	3.5	19
Class 1 Bk Space	0.6 min.	1.8 min.
Class 2 Bk Space	0.6 min.	1.8 min.
Minimum Drive Aisle	7.5	
Minimum Stewage	1.5	
Minimum Crossover	1.5	
*May vary, if min. like present		

LEGEND

EXISTING GRADE	X 0.50
PROPOSED GRADE	(11.10)
SELECT GRADES SHOWN REFER TO CIVIL PLANS AND TOPOGRAPHY PLAN FOR GRADES	

ARCHITECTURE PANEL INC.
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CEDAR COAST

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7	MAR.13.23	UPDATED PER ACP COMMENT
6	FEB.03.23	UPDATED PER CITY COMMENTS
5	DEC.14.22	UPDATED PER CITY COMMENTS
4	JUN.19.22	ISSUED FOR DP
3	MAY.12.22	CONSULTANT COORDINATION
2	MAY.12.22	CLIENT REVIEW
1	OCT.04.21	ISSUED FOR DP
NO.	DATE	DESCRIPTION

ORION CONSTRUCTION

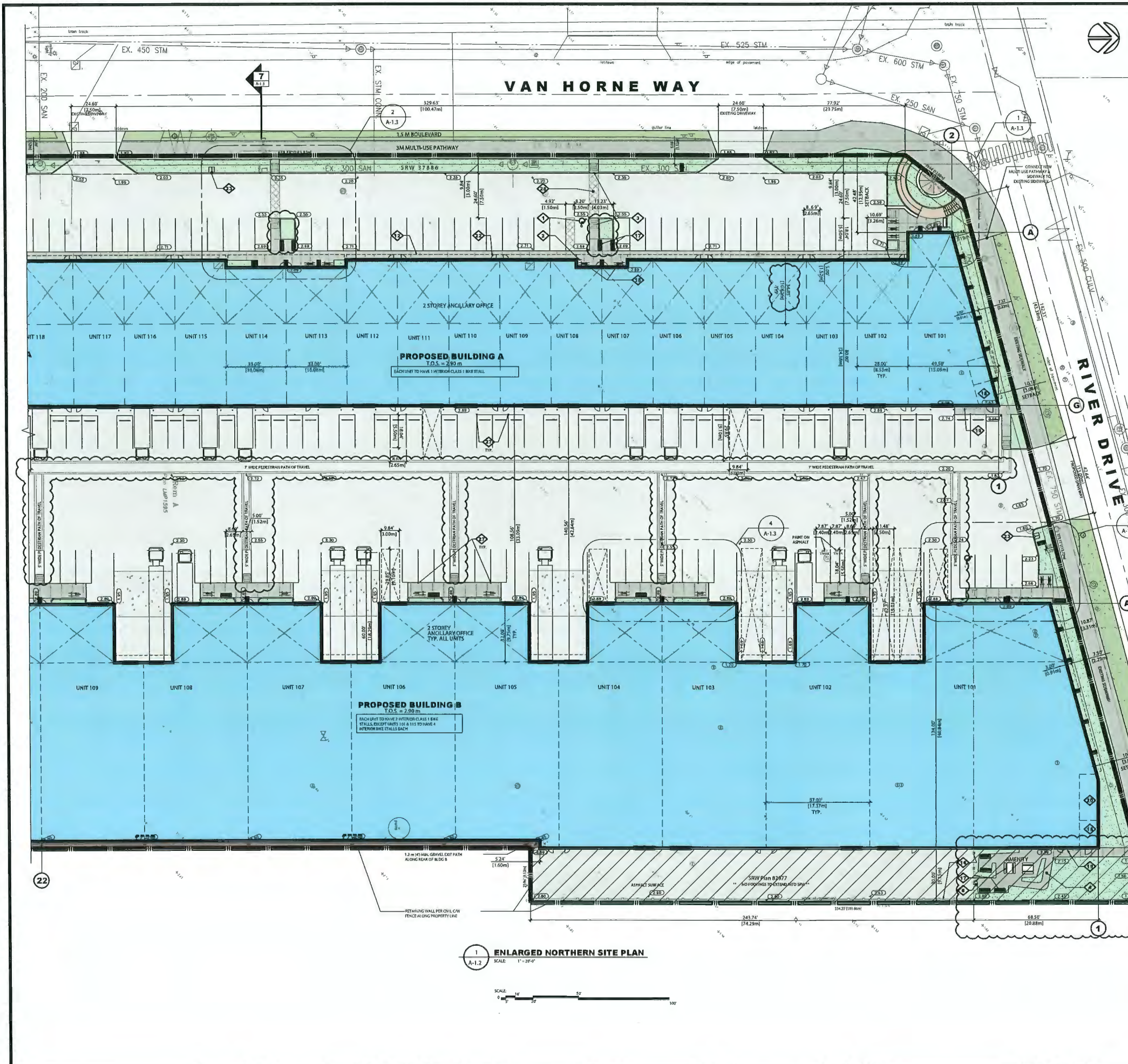
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PHONE: (604) 362-2994

CEDAR COAST VAN HORNE

ADDRESS: 800 VAN HORNE WAY, RICHMOND, BC

ENLARGED SOUTHERN SITE PLAN

REAL	JOB NO. 21-028	DRAWN DRVCF
	DESIGNED	
	CHECKED	REL
	PLUT DATE	MAR.13.23
PROJECT - DRAWING NUMBER		REV. 7



- ### KEYED SITE PLAN NOTES
- 1. INTERNATIONAL SYMBOL OF ACCESS PAINTED ON ASPHALT PER DETAIL A-1.4
 - 2. BUILDING MOUNTED H.C. SIGN PER DETAIL A-1.4
 - 3. 3'x3' SIDEWALK LITDOWN PROVIDE MINIMUM 3'x3' CLEAR AT TOP OF LITDOWN. SEE DETAIL A-1.4
 - 4. CONCRETE SIDEWALK 4" C. ABOVE ADJACENT ASPHALT LIGHT BROWN FINISH
 - 5. 6"x6" EXTENDED CONCRETE CURB TYPICAL AT EDGE OF LANDSCAPING. PER DETAIL A-1.4
 - 6. LANDSCAPING PER LANDSCAPE ARCHITECT DRAWINGS
 - 7. WHEELSTOP, PER DETAIL A-1.4
 - 8. 4" PAINT STRIPS, TYPICAL
 - 9. ASPHALT CROSSING TO MUNICIPAL SPEEDS
 - 10. 6" x 1/4" THICK CONCRETE FIELDED STEEL BOLL AND PAINTED "RIGHT-OF-WAY" OR EQUIVALENT AT EACH SIDE OF GRADE LEVEL. BOLL TYPICAL AT ALL GRADE CHANGES.
 - 11. RETAINING WALL, UNPAINTED 4" x 8" QUADRANT, QUADRANT REQUIRED UNTIL THE GRADE DIFFERENCE IS LESS THAN 2" (50mm)
 - 12. LINE OF CANOPY ABOVE, SEE PLANS AND DETAILS FOR MORE INFORMATION
 - 13. EXTERIOR GARAGE ENCLOSURE. SEE WASTE MANAGEMENT PLAN AND DETAILS A-0.3
 - 14. CONCRETE DOLLY PAD
 - 15. CLASS 2 EXTERIOR BICYCLE PARKING (2 SPOTS PER BAY). PER DETAIL A-1.4
 - 16. 6" W/ 1/4" J.W. MESH-GATE, PROVIDE BLACK VINYL PRIVACY SLATS AROUND OUTDOOR STORAGE YARD
 - 17. MESH PER LANDSCAPE DWGS
 - 18. ELECTRICAL & MECHANICAL ROOMS - EQUIPMENT
 - 19. GAS METERS PER MECHANICAL DRAWINGS WITH FENCE AND PROTECTION AS REQUIRED BY BC BYLAWS AND MUNICIPALITY. SEE DETAIL A-1.4
 - 20. UNIT SUBSTATION ROOM, SEE ELECTRICAL DRAWINGS
 - 21. FREE STANDING SIGNAGE, SEE DETAIL A-1.4
 - 22. TYPICAL ACCESSIBLE ENTRANCE
 - 23. FIRE DEPARTMENT CONNECTION, SEE MECHANICAL DRAWINGS
 - 24. TRAVEL DISTANCE IN ACCORDANCE TO FC (147) (4th) MAX. CONFIRM ON SITE
 - 25. HATCHED AREA INDICATES FIRE TRUCK ACCESS ROUTE. DESIGN REQUIREMENT PER BELOW: HAVE A CLEAR WIDTH NOT LESS THAN 12m. HAVE A CENTERLINE AND NOT LESS THAN 12m. HAVE AN OVERHEAD CLEARANCE OF 5.5m TO 6m. HAVE A 10% MAX. SLOPE TO CLEARANCE. TRUCK LOADING OVER A DRIVEWAY OF SPACES OF 15m. TRUCKS MUST BE DESIGNED TO CLEAR THE TRUCK LOADING OF 80 BUSES COVER. HAVE TURNING RADIUS FACILITIES FOR ANY DEAD-END PORTION OF THE ACCESS ROUTE MORE THAN 10m LONG. BE CONNECTED WITH A PUBLIC INFRASTRUCTURE.
 - 26. I.E.C LISTED EXTERIOR AIR-CONDITIONER, SEE ELECTRICAL DRAWINGS
 - 27. BY CHARGING JUNCTION BOX & ROUGH-WIRE, SEE ELEC DRAWINGS FOR DETAILS
 - 28. SQUARE-CUT PATTERN SAMPLERS, CIVIL CONTRASTS COLOR FINISH. SEE LANDSCAPE DRAWINGS FOR DETAILS

LEGEND

EXISTING GRADE 78.51

PROPOSED GRADE 78.50

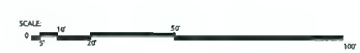
SELECT GRADES SHOWN REFER TO CIVIL PLANS AND TOPOGRAPHY PLANS FOR GRADES

Parking Dimensions

Stall Type	Width (m)	Depth (m)
Regular Stall (90°)	2.65	5.5
Small Car Stall	2.4	5
Accessible Stall	3.5	5.5
Accessible Stall (Van)	3.4	5.5
Accessible Aisle	1.5 min.	
Dead-End Stall (90°)	3.26	5.5
Medium Loading Stall	3	9.1
Large Loading Stall	3.5	19
Class 1 Bike Space	0.6 min.	1.8 min.
Class 2 Bike Space	0.6 min.	1.8 min.

Minimum Drive Aisle* 7.5
Minimum Side Walk* 1.5
Minimum Crosswalk* 1.5
*May vary, plans take precedent

1 ENLARGED NORTHERN SITE PLAN
SCALE: 1" = 20'-0"



ARCHITECTURE PANEL INC.
ARCHITECTURE LANDSCAPE ARCHITECTURE URBAN DESIGN
204 - 1880 RUTHER STREET, WHITE ROCK, BC V4B 4C3
604 343 8600 | RUCKER@ARCHITECTUREPANEL.COM

IN ASSOCIATION WITH

dForce DESIGN INC.
2221 - 4 Street Street, Abbotsford, B.C. V2S 1R9
TEL: (604) 877-5855 email: darryl@dforce.ca

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CEDAR COAST

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7	MAR.13.23	UPDATED PER ADP COMMENT
6	MAR.03.23	UPDATED PER CITY COMMENTS
5	DEC.15.22	UPDATED PER CITY COMMENTS
4	JUN.16.22	REVISED FOR DP
3	MAR.31.22	CONVULSANT COORDINATION
2	MAY.17.22	CLIENT REVIEW
1	OCT.04.21	ISSUED FOR DP
NO.	DATE	DESCRIPTION

PROJECT MANAGERS / CONTRACTORS

ORION CONSTRUCTION

ORION CONSTRUCTION
104 - 2950 191 STREET,
SURREY BC, V3Z 0Y6
PHONE: (604) 362-2994

PROJECT: **CEDAR COAST VAN HORNE**

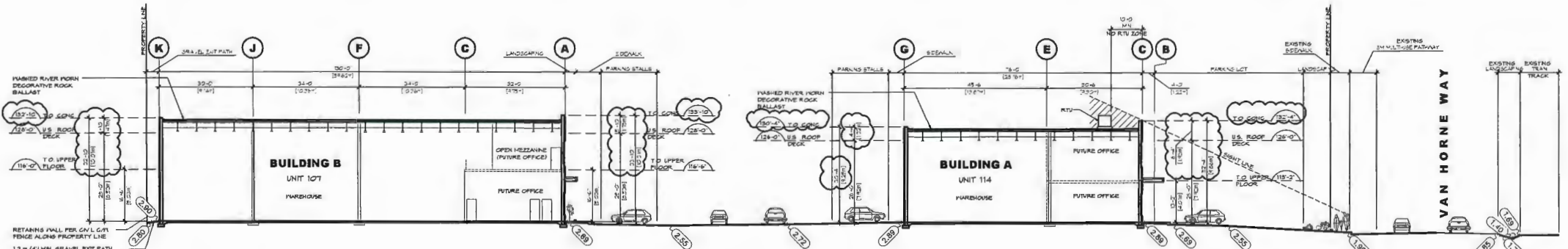
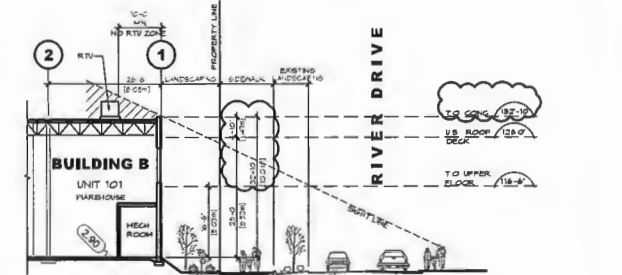
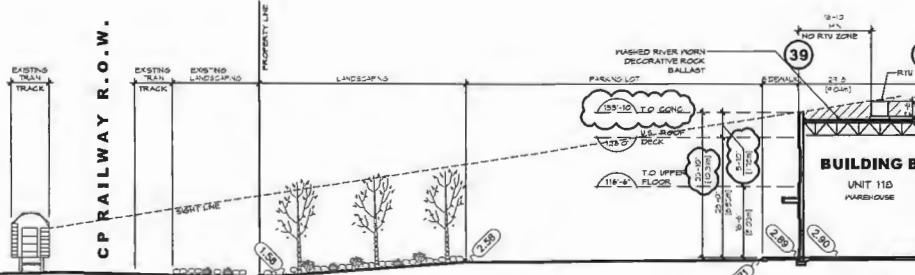
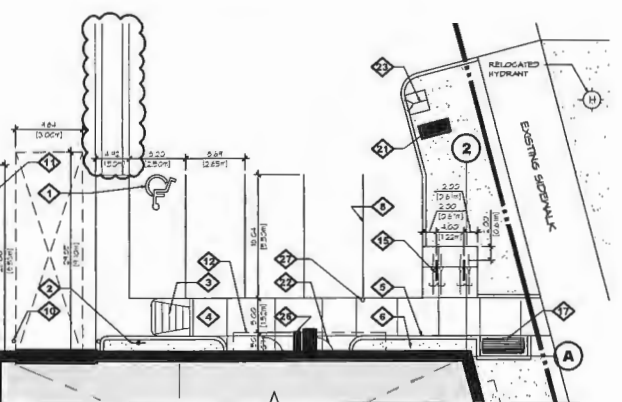
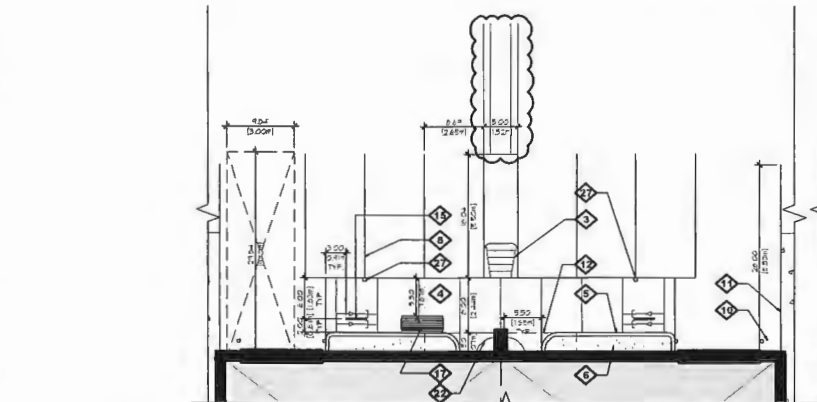
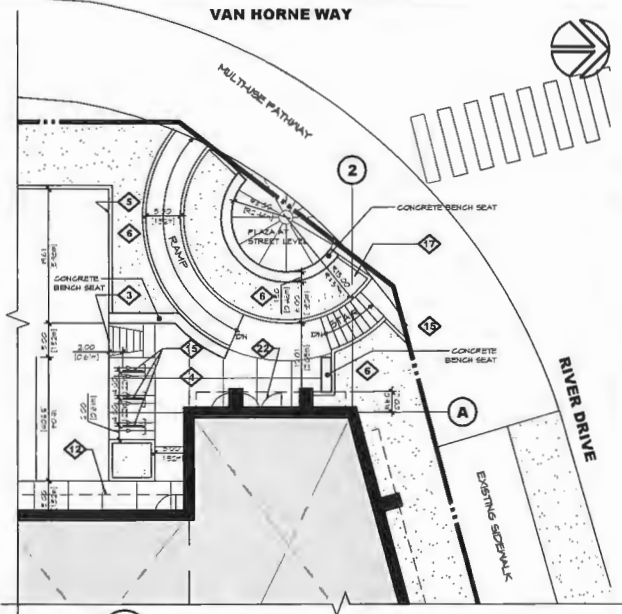
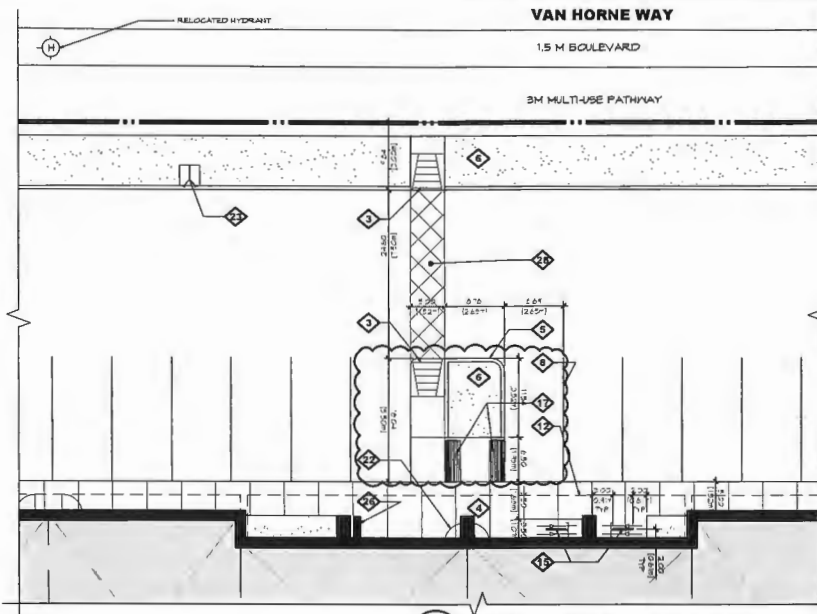
ADDRESS: 3800 VANI HORNE WAY, RICHMOND, BC

DRAWING: **ENLARGED NORTHERN SITE PLAN**

SEAL	JOB NO.	DRAWN
	21-023	DF/CF
CHECKED		
PLOT DATE		
PROJECT - DRAWING NUMBER		REV
A-1.2		7

KEYED SITE PLAN NOTES

1	INTERNATIONAL SYMBOL OF ACCESS PAINTED ON ASPHALT PER DETAIL S/A-1.4
2	BUILDING MOUNTED H.C. SIGN PER DETAIL S/A-1.4
3	5'-0" SIDEWALK LETDOWN PROVIDE MINIMUM 5'-0" CLEAR AT TOP OF LETDOWN. SEE DETAIL S/A-1.1
4	CONCRETE SIDEWALK #1 ABOVE ADJACENT ASPHALT LIGHT BROWN FINISH
5	8" x 8" EXPOSED CONCRETE CURB TYPICAL AT EDGE OF LANDSCAPING. PER DETAIL S/A-1.4
6	LANDSCAPING PER LANDSCAPE ARCHITECTS DRAWINGS
7	WHEELSTOP, PER DETAIL S/A-1.4
8	4" PAINT STRIPE, TYPICAL
9	ASPHALT CROSSING TO HIGHWAY SPECS
10	6" x 8" x 1/4" THICK CONCRETE FILLED STEEL BOLDED PAINTED "BRIGHT YELLOW" OR EQUIVALENT AT EACH SIDE OF GRADE LEVEL DOOR TYPICAL AT ALL GRADE DOORS
11	RETAINING WALL, UNPAINTED C/S #2 PIPE GUARDRAIL, GUARDRAIL REQUIRED UNTIL THE GRADE DIFFERENCE IS LESS THAN 2'-0" (WOODING)
12	LINE OF CANOPY ABOVE, SEE PLANS AND DETAILS FOR MORE INFORMATION
13	EXTERIOR AIRBASE ENCLOSURE, SEE WASTE MANAGEMENT PLAN AND DETAILS A-0.3
14	CONCRETE DOLLY PAD
15	CLASS 2 EXTERIOR BICYCLE PARKING (2 SPOTS PER BIKE RACK) PER DETAIL S/A-1.4
16	4' HI FENCE C/S 2" PL. MAN-GATE, PROVIDE BLACK VINYL PRIVACY SLATES AROUND OUTDOOR STORAGE YARD
17	BENCH PER LANDSCAPE DWGS
18	ELECTRICAL & MECHANICAL ROOMS / EQUIPMENT
19	6-8 HYPERS PER MECHANICAL DRAWINGS WITH PRICE AND PROTECTION AS REQUIRED BY BC HYDRO AND MUNICIPALITY, SEE DETAIL S/A-1.4
20	UNIT SUBSTATION ROOM, SEE ELECTRICAL DRAWINGS
21	FREE STANDING SIGNAGE, SEE DETAIL S/A-1.4
22	TYPICAL ACCESSIBLE ENTRANCE
23	FIRE DEPARTMENT CONNECTION, SEE MECHANICAL DRAWINGS
24	TRAVEL DISTANCE HYDRANT TO FDC (MAX 145) (MAX CONFINED ON SITE)
25	WATCHED AREA INDICATES FREE TRUCK ACCESS ROUTE DESIGN & EQUIPMENTS FOR BELOW: - HAVE A CLEAR WIDTH NOT LESS THAN 8'-6" - HAVE A CENTER-LINE RADIUS NOT LESS THAN 12M - HAVE AN OVERHEAD CLEARANCE NOT LESS THAN 5M - HAVE A GRADIENT NOT MORE THAN 1 IN 12.5 - COVER A MINIMUM DISTANCE OF 75M - ASPHALT MUST BE DESIGNED TO CARRY FIRE TRUCK LOADS OF 80 000 LBS W/WE - HAVE THROUGHOUT FACILITIES FOR ANY DEAD-END PORTION OF THE ACCESS ROUTE MORE THAN 100' LONG - BE CONNECTED WITH A PUBLIC THROUGHWAY
26	UL-C LISTED EXTERIOR AMMUNITION PANEL, SEE ELECTRICAL DRAWINGS
27	EV CHARGING JUNCTION BOX & ROUGH-IN, SEE ELEC DRAWINGS FOR DETAILS
28	SQUARE-GUT PATTERNS, SPOTS, CUT CONTRASTING COLOUR FINISH, SEE LANDSCAPE DRAWINGS FOR DETAILS



ARCHITECTURE PANEL INC.
ARCHITECTURE, LANDSCAPE ARCHITECTURE, INTERIOR DESIGN
210 - 1445 FORBES STREET, WHITE ROCK, BC, V8B 0G4
(604) 885-3821 | RICH@ARCHITECTUREPANEL.COM

D. FORCE DESIGN INC.
DESIGN & CONSTRUCTION MANAGEMENT, PROJECT MANAGEMENT, ARCHITECTURAL CONSULTING
1000 - 1445 FORBES STREET, WHITE ROCK, BC, V8B 0G4
(604) 885-3821 | RICH@ARCHITECTUREPANEL.COM

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CEDAR COAST

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ORION CONSTRUCTION

ORION CONSTRUCTION
104 - 3050 191 STREET,
SURREY BC, V3Z 0Y6
PHONE: (604) 362-2994

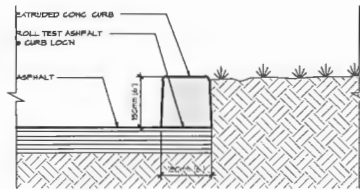
CEDAR COAST VAN HORNE

ADDRESS: 3800 VAN HORNE WAY, RICHMOND, BC

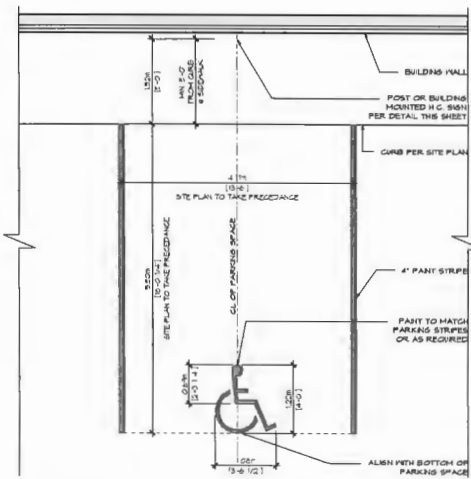
ENLARGED SITE PLANS & SITE SECTIONS

SEAL	JOB NO. 21429	DRAWN OFFICE
DESIGNED		
CHECKED	R.D.	
PLOT DATE	MAR, 13, 23	

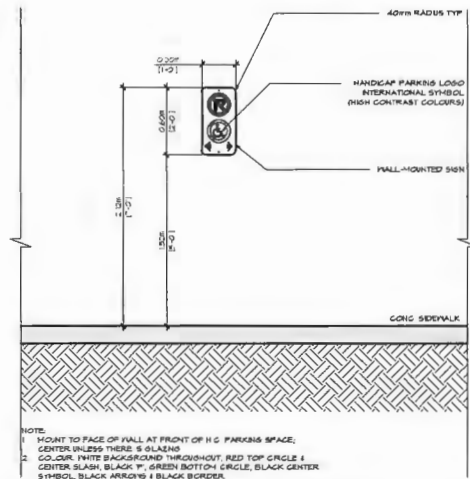
PROJECT - DRAWING NUMBER: **A-1.3** REV: **7**



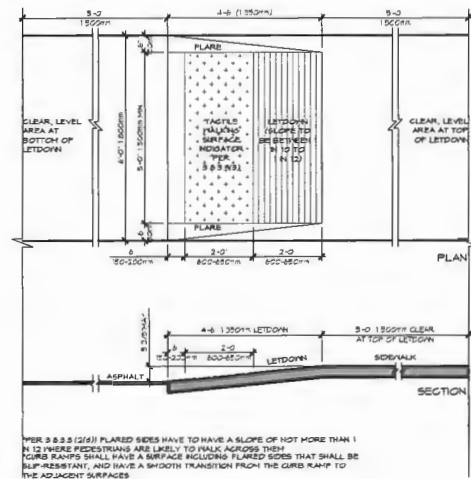
4 CURB DETAIL
A-1.4 SCALE: 1/4" = 1'-0"



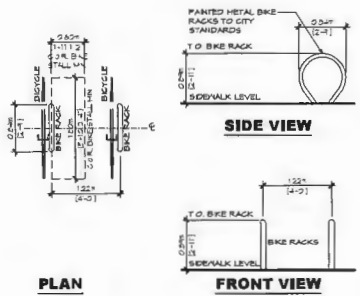
3 ACCESSIBLE PARKING STALL DETAIL
A-1.4 SCALE: 1/4" = 1'-0"



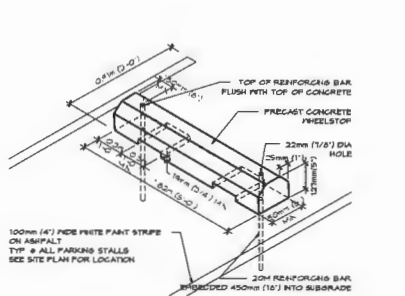
2 ACCESSIBLE PARKING WALL-MOUNT SIGN DETAIL
A-1.4 SCALE: 1/2" = 1'-0"



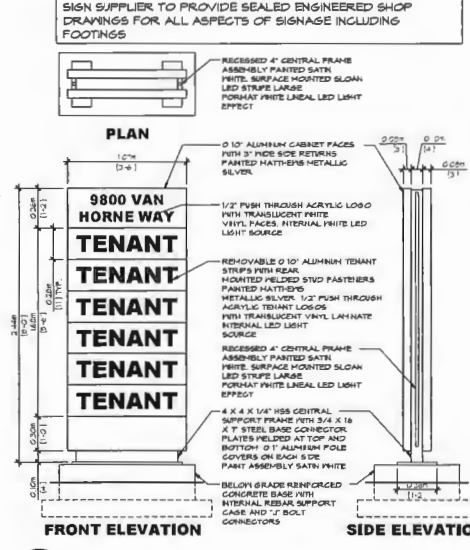
1 LETDOWN DETAIL
A-1.4 SCALE: 1/2" = 1'-0"



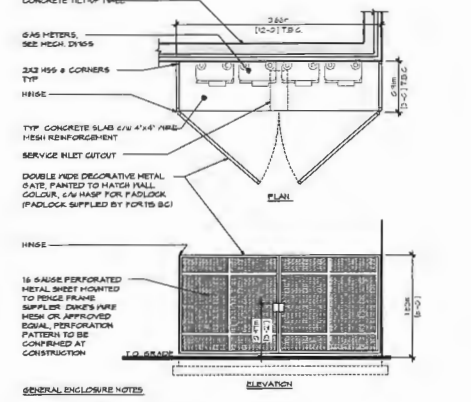
8 BIKE RACK DETAIL
A-1.4 SCALE: 1/4" = 1'-0"



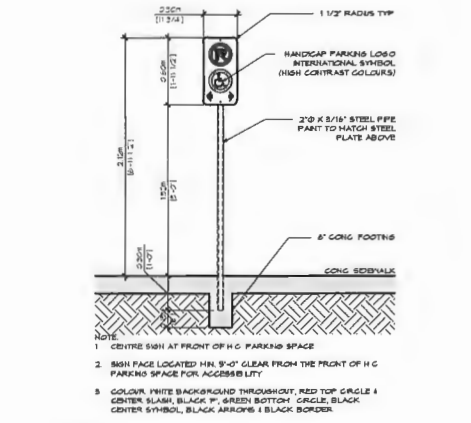
7 WHEEL STOP DETAIL
A-1.4 SCALE: NTS



6 FREE STANDING SIGN
A-1.4 SCALE: 1/2" = 1'-0"



5 GAS METER DETAIL
A-1.4 SCALE: 1/4" = 1'-0"



9 ACCESSIBLE PARKING SIGN POST DETAIL
A-1.4 SCALE: 1/2" = 1'-0"

ARCHITECTURE PANEL INC.
ARCHITECTURE LANDSCAPE ARCHITECTURE URBAN DESIGN
306-349 HORTON STREET, WHITE ROCK, BC, V4B 3G6
604-883-5100 FAX: 604-883-5101
WWW.ARCHITECTUREPANEL.COM

IN ASSOCIATION WITH
D.FORCE DESIGN INC.
2678 ALANAVE STREET, ABERTFORD, B.C. T9B 0J8
781-16241 807-9545 EMAIL: DFF@DFORCE.COM

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CEDAR COAST

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ORION CONSTRUCTION

ORION CONSTRUCTION
104 - 3950 191 STREET,
SURREY BC, V2Z 0Y8
PHONE: (604) 362-2894

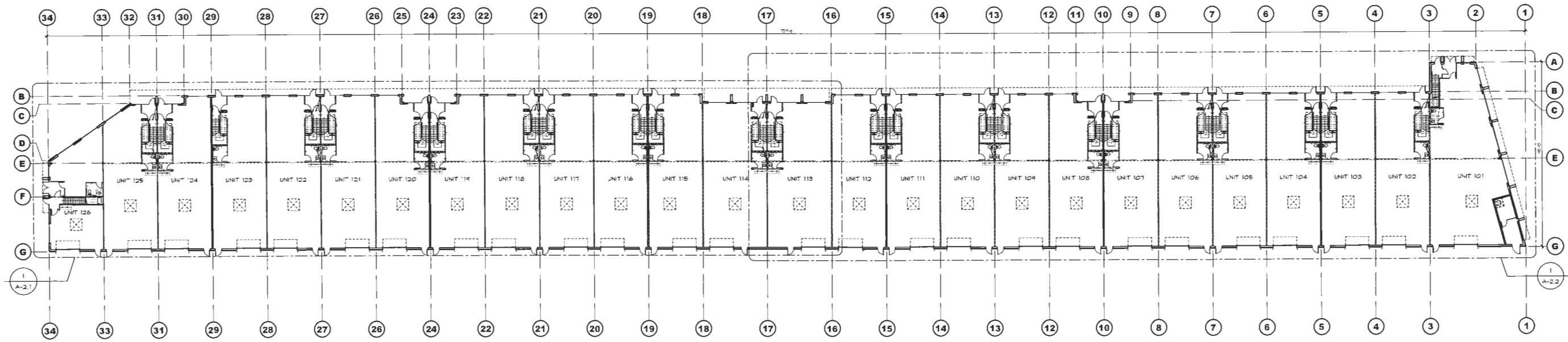
PROJECT
CEDAR COAST VAN HORNE

ADDRESS: 5300 VAN HORNE WAY, RICHMOND, BC
DRAWING

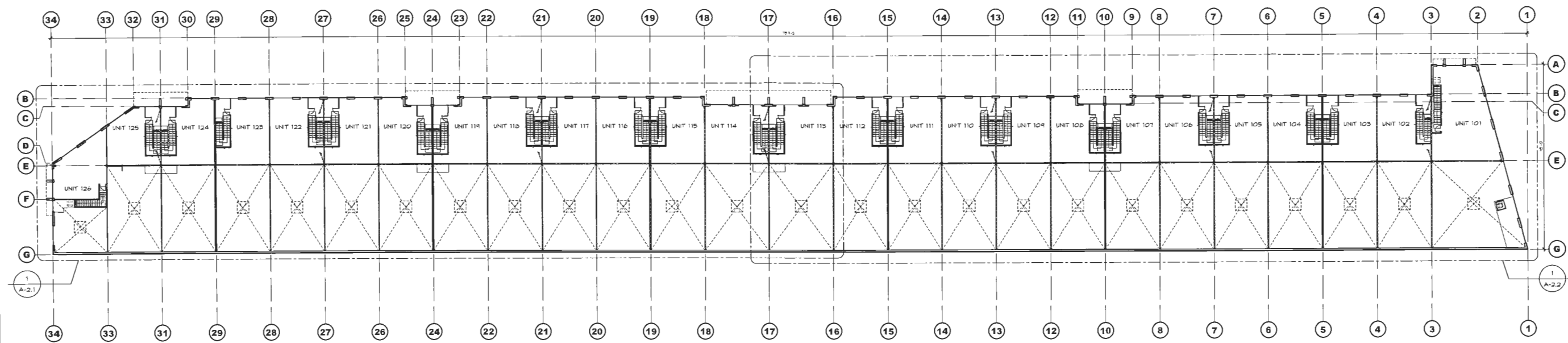
SITE DETAILS

DESIGNED	21-029	DFVCF
CHECKED		R.D.
PLOT DATE		MAR, 13, 23

PROJECT - DRAWING NUMBER: **A-1.4** REV: **4**



1
A-2.0
OVERALL
GROUND FLOOR PLAN
SCALE: 1"=30'-0"



2
A-2.0
OVERALL
SECOND FLOOR PLAN
SCALE: 1"=30'-0"

NOT TO SCALE
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IN ASSOCIATION WITH
D.FORGE DESIGN INC.
ARCHITECTURE PAEL INC
2625A ALLIANCE STREET, ABBOTSFORD, B.C., V2S 3J9
TEL: (604) 807-8655 EMAIL: DANCY@DFORGE.CA

ORION CONSTRUCTION
UNIT 105 19923 80A AVE
LANGLEY, BC V2Y 0E2
PHONE: (604) 362-2994

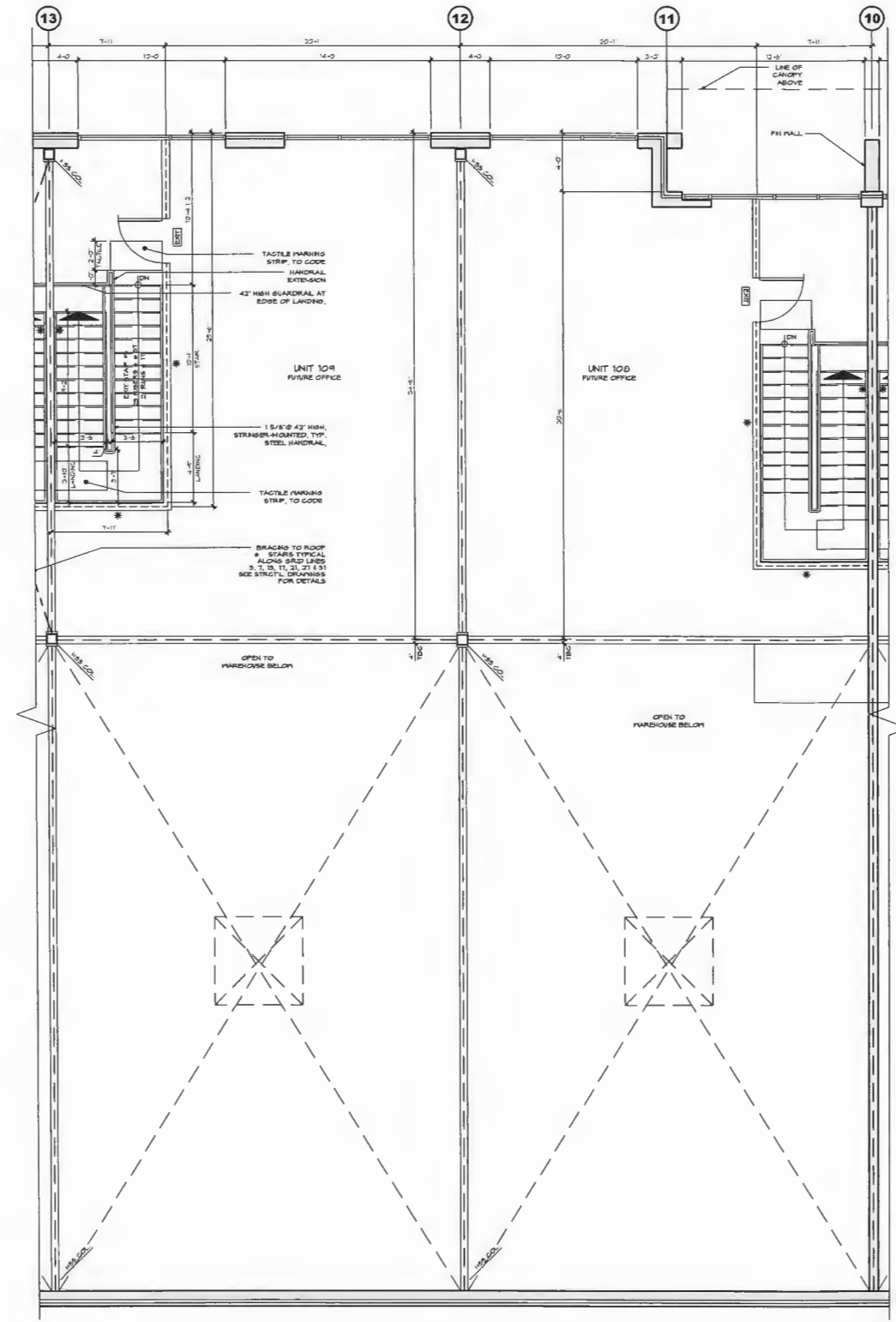
CEDAR COAST
CEDAR COAST
UNIT 3300 - 1021 West Hastings St.
VANCOUVER, BC V6E 0C3

ISSUE NO.	DATE	REVISION	NO.	DATE	REVISION
1	SEP 17 21	CONSULTANT USE	13		
2	OCT 04 21	ISSUED FOR DP	14		
3	DEC 03 21	REVISION FOR I/P	15		
4	DEC 15 21	UPDATED PER CITY COMMENT	16		
5	MAR 09 22	CONSULTANT USE	17		
6	MAR 13 22	UPDATED PER REP COMMENT	18		
7			19		
8			20		
9			21		
10			22		
11			23		
12			24		

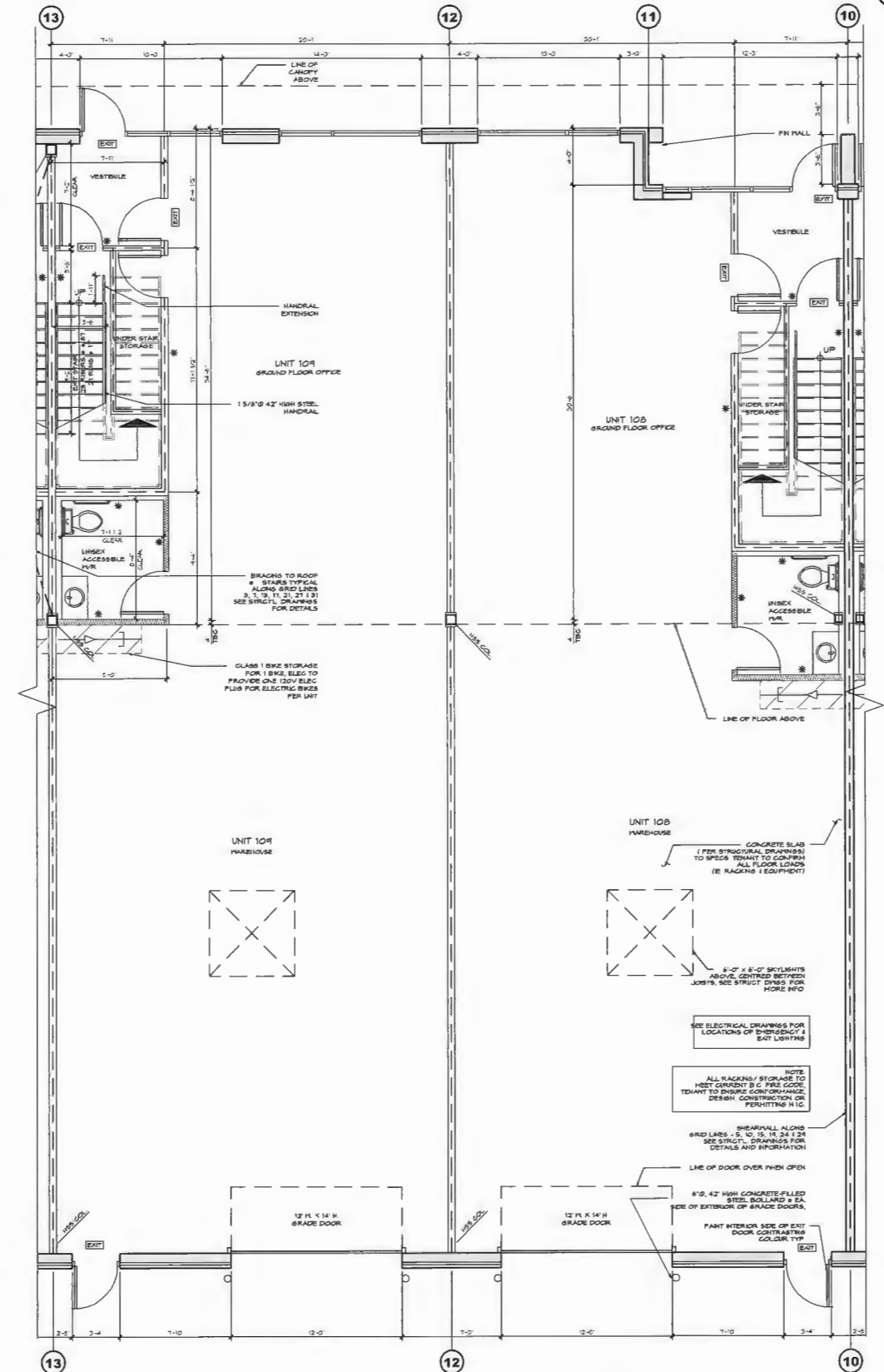
ISSUE NO.	DATE	REVISION	NO.	DATE	REVISION
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26			26		
27			27		
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29			29		
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PROJECT	PROPOSED BUILDING FOR	DATE PLOTTED	DRAWING NUMBER
CEDAR COAST	UNIT 3300 - 1021 West Hastings St.	MAR 13 23	A-2.0

CEDAR COAST VAN HORNE
ADDRESS: 5880 VAN HORNE WAY, RICHMOND, BC
DRAWING: **BUILDING A FLOOR PLANS** REV. 6
JOB NO. 21-029
DATE PLOTTED: MAR 13 23
DRAWING NUMBER: A-2.0



2 ENLARGED UPPER FLOOR PLAN
SCALE 1/4"=1'-0"



1 ENLARGED GROUND FLOOR PLAN
SCALE 1/4"=1'-0"

IN ASSOCIATION WITH
D.FORGE DESIGN INC.
ARCHITECTURE PAINT INC.
2633A ALLIANCE STREET, ABBOTSFORD, B.C., V2S 3J9
TEL: (604) 807-5855 EMAIL: DANCY@DFORGE.CA

ORION CONSTRUCTION
ORION CONSTRUCTION
UNIT 105 1923 80A AVE
LANGLEY, BC V2Y 0E2
PHONE: (604) 352-2994

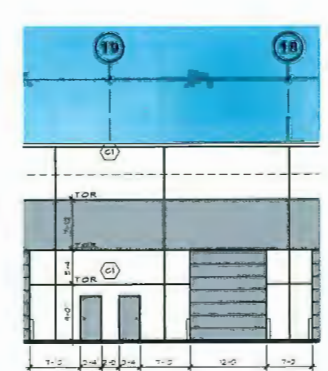
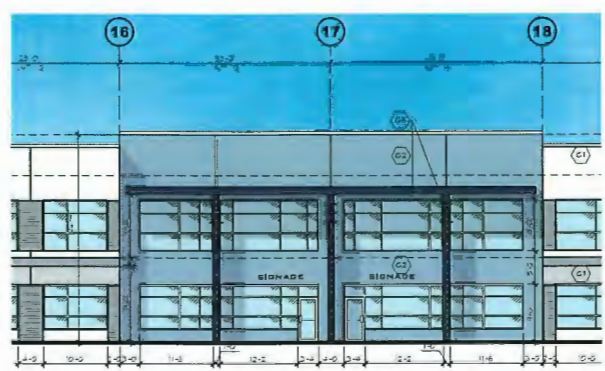
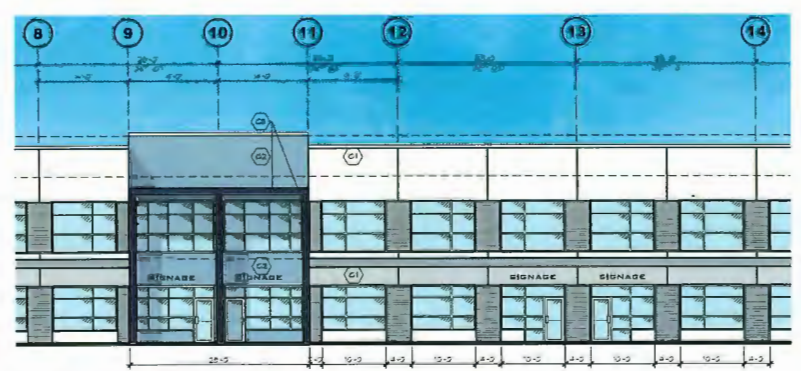
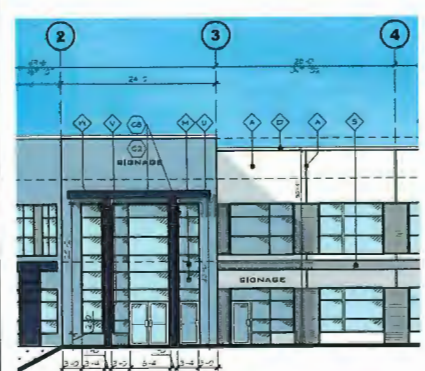
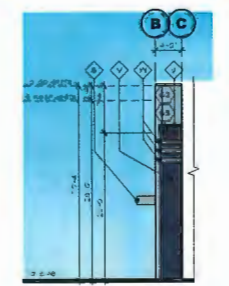
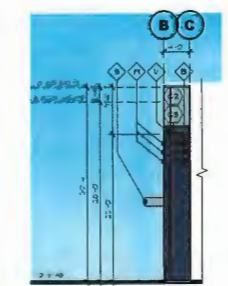
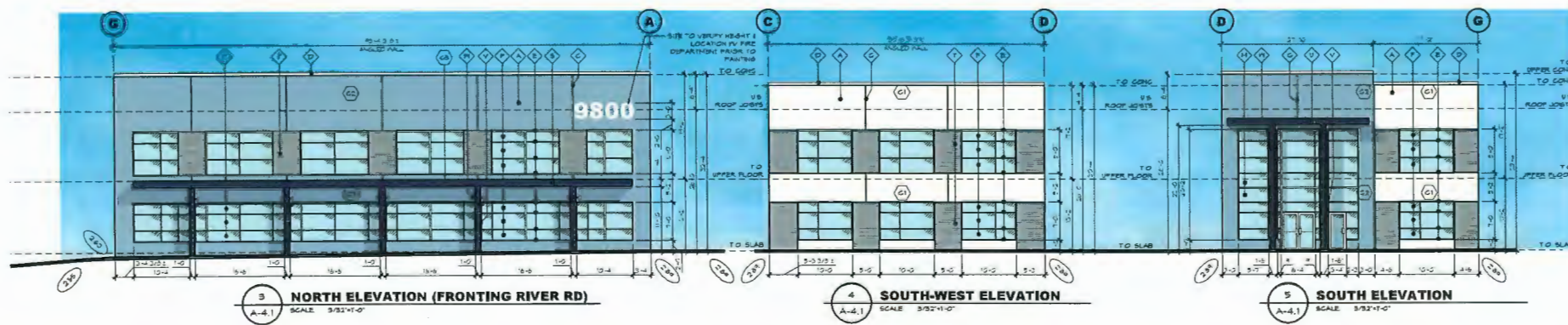
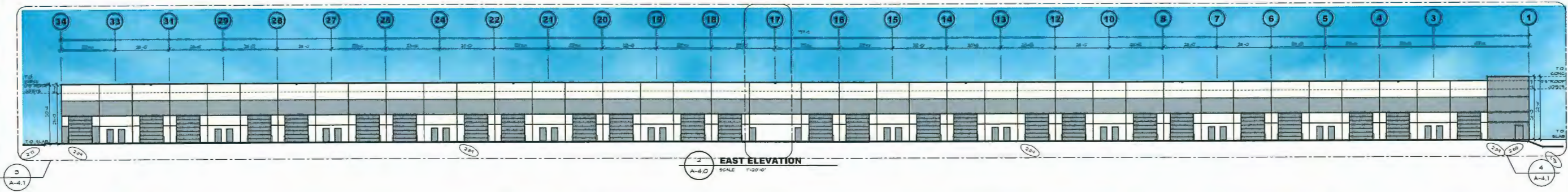
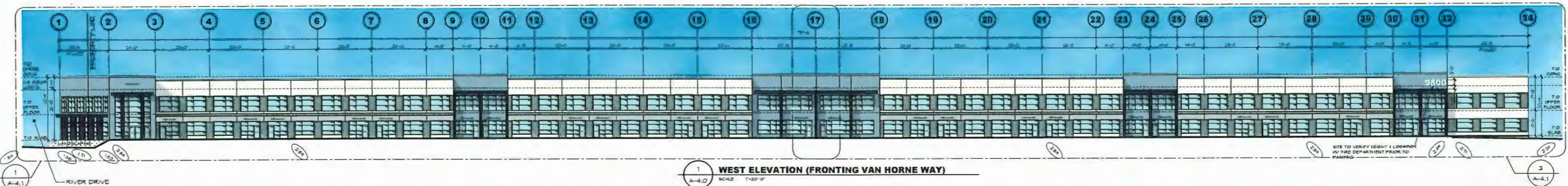
CEDAR COAST
CEDAR COAST
UNIT 3300 - 1021 West Hastings St.
VANCOUVER, BC V6E 0C3

ISSUE NO.	DATE	REVISION
1	SEP 17 21	ISSUED FOR CONSULTANT USE
2	OCT 14 21	ISSUED FOR OP
3	JAN 18 22	REVISIONS FOR OP
4	DEC 15 22	UPDATED PER CITY COMMENT
5	MAR 09 23	ISSUED FOR CONSULTANT USE
6	MAR 13 23	UPDATED PER ADP COMMENT
7		
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ISSUE NO.	DATE	REVISION
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PROJECT PROPOSED BUILDING FOR
CEDAR COAST VAN HORNE
ADDRESS: 8802 VAN HORNE WAY, RICHMOND, BC
DRAWING: **BUILDING A TYP. UNIT PLANS**

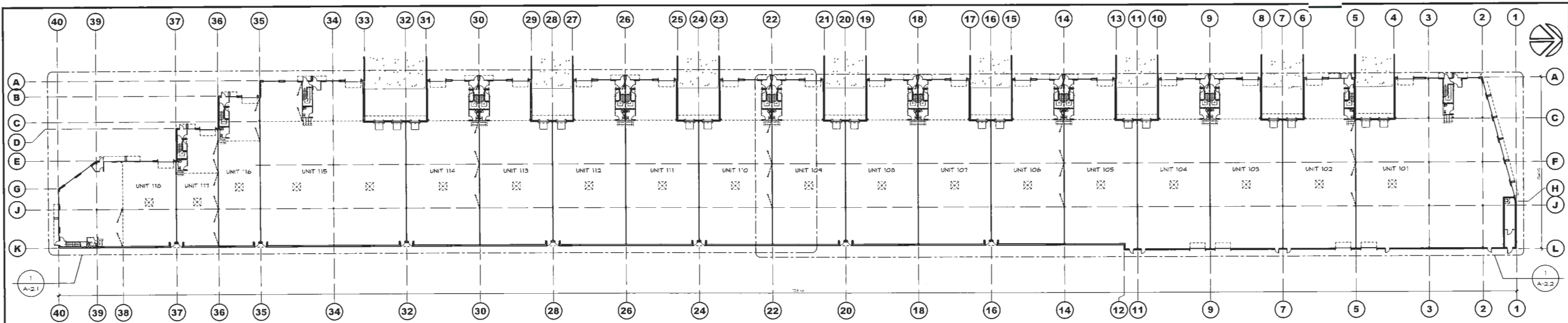
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CHECKED: R.D.
JOB NO.: 21-029
DATE PLOTTED: MAR 13 23
DRAWING NUMBER: 6
REV: A-2.1



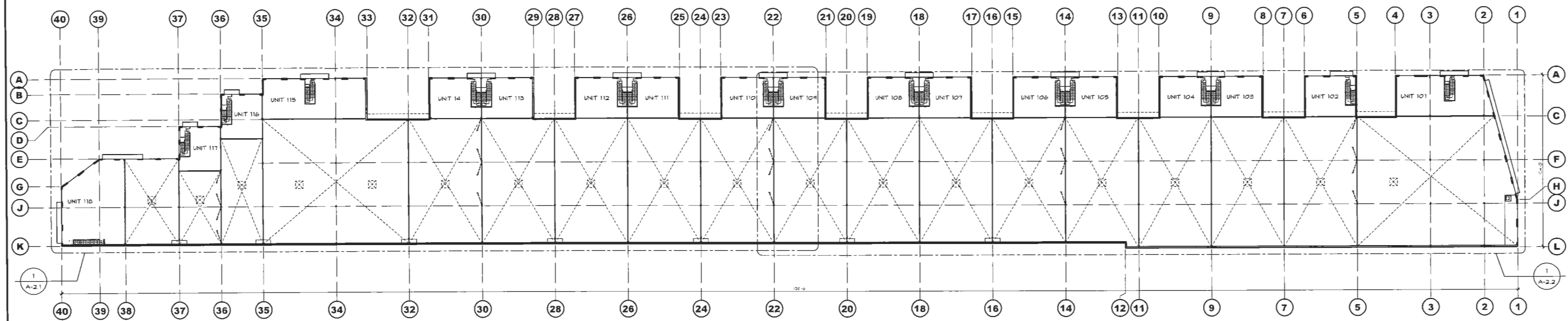
- MATERIAL LEGEND**
- ◊ CONCRETE SANDWICH TILT-UP WALL PANEL - PAINTED
 - ◊ REVEAL IN CONCRETE - PAINTED
 - ◊ PANEL JOINT
 - ◊ FIRE-FINISHED METAL CAP FLASHING
 - ◊ STOREFRONT FRAMES - CLEAR ANODIZED ALUMINUM
 - ◊ STOREFRONT GLASS - CLEAR
 - ◊ STEEL MAIN DOORS - PAINTED
 - ◊ STEEL SECTIONAL OVERHEAD DOORS - PAINTED
 - ◊ STEEL GUARDRAILS - PAINTED
 - ◊ CONCRETE RETAINING WALLS - NATURAL FINISH UNPAINTED TYP.
 - ◊ LIGHT FIXTURE - SEE ELECTRICAL DWG'S
 - ◊ SPANDREL GLAZING JOBSIGRE TO MATCH VISION GLASS
 - ◊ SCUPPER
 - ◊ METAL CLAD CANOPY
 - ◊ STEEL BOLLARD - PAINTED
 - ◊ CONCRETE CURB UNDER, PAINTED
 - ◊ STEEL CANOPY, PAINTED
 - ◊ HORIZONTAL METAL GLAZING
 - ◊ ENTRANCE CANOPY
 - ◊ FIN WALL
 - ◊ ANODIZED ALUMINUM FRAME ACCENT

- LEGEND:**
- ◊ PROPOSED GRADE
- PROJECT COLOURS**
- ALL COLOURS SHERWIN WILLIAMS
- ◊ (C) MAIN FIELD COLOUR 1
 - ◊ (C) FIELD ACCENT COLOUR 1
 - ◊ (C) FIELD ACCENT COLOUR 2
 - ◊ (C) HIGH REFLECTIVE WHITE (SW 7571)
 - ◊ (C) ALUMINUM (SW 6241)
 - ◊ (C) ANCHORS APRECH (SW 4196)
 - ◊ (C) POWDER COAT BLACK VELVET
 - ◊ (C) CLEAR ANODIZED ALUMINUM
 - ◊ (C) CLEAR LOW-E ASHRAE 10-1 (2016 COMPLIANT)
 - ◊ (C) SPANDREL BLACK-GRAY
 - ◊ (C) BLACK-GRAY
 - ◊ (C) SALVAMIN FINISH
 - ◊ (C) RESTFORM METALS REBBIT GRAY
- NOTE:**
 - NOT ALL MATERIALS ARE APPLICABLE
 - SPANDREL GLAZING
 - HORIZONTAL METAL GLAZING, 1/8" CORRUGATED METAL GLAZING, OR ALUMINUM FINISH - OR APPROVED EQUAL

<p>IN ASSOCIATION WITH</p> <p>D.FORCE DESIGN INC.</p> <p>2628A ALLIANCE STREET, ARBOTHFORD, B.C. V8B 3J9 TEL: (604) 807-9995 EMAIL: DANDY@DFORCE.CA</p>	<p>PROJECT MANAGERS/CONTRACTORS</p> <p>ORION CONSTRUCTION</p> <p>ORION CONSTRUCTION UNIT 105 19223 86A AVE LANGLEY, BC V2Y 0E2 PHONE: (604) 362-2994</p>	<p>DEVELOPER</p> <p>CEDAR COAST</p> <p>CEDAR COAST UNIT 3300 - 1021 West Hastings St, VANCOUVER, BC V6E 0G3</p>	<p>ISSUE NO. DATE REVISION</p> <p>1 SEP 17 21 CONSULTANT USE</p> <p>2 OCT 04 21 ISSUED FOR DP</p> <p>3 APR 09 23 RE-ARRANGED FOR DP</p> <p>4 DEC 15 23 UPDATED PER CITY COMMENT</p> <p>5 MAR 06 23 CONSULTANT USE</p> <p>6 UNK 13 23 UPDATED PER ASP COMMENT</p> <p>7</p> <p>8</p> <p>9</p> <p>10</p> <p>11</p> <p>12</p>	<p>NO. DATE REVISION</p> <p>13</p> <p>14</p> <p>15</p> <p>16</p> <p>17</p> <p>18</p> <p>19</p> <p>20</p> <p>21</p> <p>22</p> <p>23</p> <p>24</p> <p>25</p>	<p>DATE REVISION</p> <p>26</p> <p>27</p> <p>28</p> <p>29</p> <p>30</p> <p>31</p> <p>32</p> <p>33</p> <p>34</p> <p>35</p> <p>36</p>	<p>PROJECT PROPOSED BUILDING FOR</p> <p>CEDAR COAST VAN HORNE</p> <p>ADDRESS 8800 VAN HORNE WAY, RICHMOND, BC</p> <p>DRAWING BUILDING A ELEVATIONS REV. 6</p>	<p>DRAWN BY</p> <p>CHECKED BY</p> <p>JOB NO. 214201</p> <p>DATE PLOTTED MAR 13 23</p> <p>DRAWING NUMBER</p> <p>A-4.0</p>
			<p>BASED ON APPROVAL OF THE DESIGN AND 25% OF THE BUILDING PERMITS. THE SIGNATURE OF THE ARCHITECTURE PANEL INC. CAN ONLY BE USED WITHOUT THE ARCHITECTURE PANEL INC. DESIGNER'S SIGNATURE. THE ARCHITECTURE PANEL INC. IS NOT RESPONSIBLE FOR THE DESIGN OF ANY STRUCTURAL ELEMENTS OR THE DESIGN OF ANY MECHANICAL, ELECTRICAL, OR PLUMBING SYSTEMS. THE ARCHITECTURE PANEL INC. IS NOT RESPONSIBLE FOR THE DESIGN OF ANY MECHANICAL, ELECTRICAL, OR PLUMBING SYSTEMS. THE ARCHITECTURE PANEL INC. IS NOT RESPONSIBLE FOR THE DESIGN OF ANY MECHANICAL, ELECTRICAL, OR PLUMBING SYSTEMS.</p>	<p>ARCHITECTURE PANEL INC.</p> <p>3633 150th Street, White Rock, BC V4B 3G4 604-881-7000 INFO@ARCHITECTUREPANEL.COM</p>			



1
A-2.0
OVERALL GROUND FLOOR PLAN
SCALE: 1/8"=1'-0"



2
A-2.0
OVERALL MEZZANINE PLAN

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 2425A ALLIANCE STREET, ABBOTSFORD, B.C. V2B 3J9
 TEL: (604) 807-9655 EMAIL: DANDY@DFORCE.CA

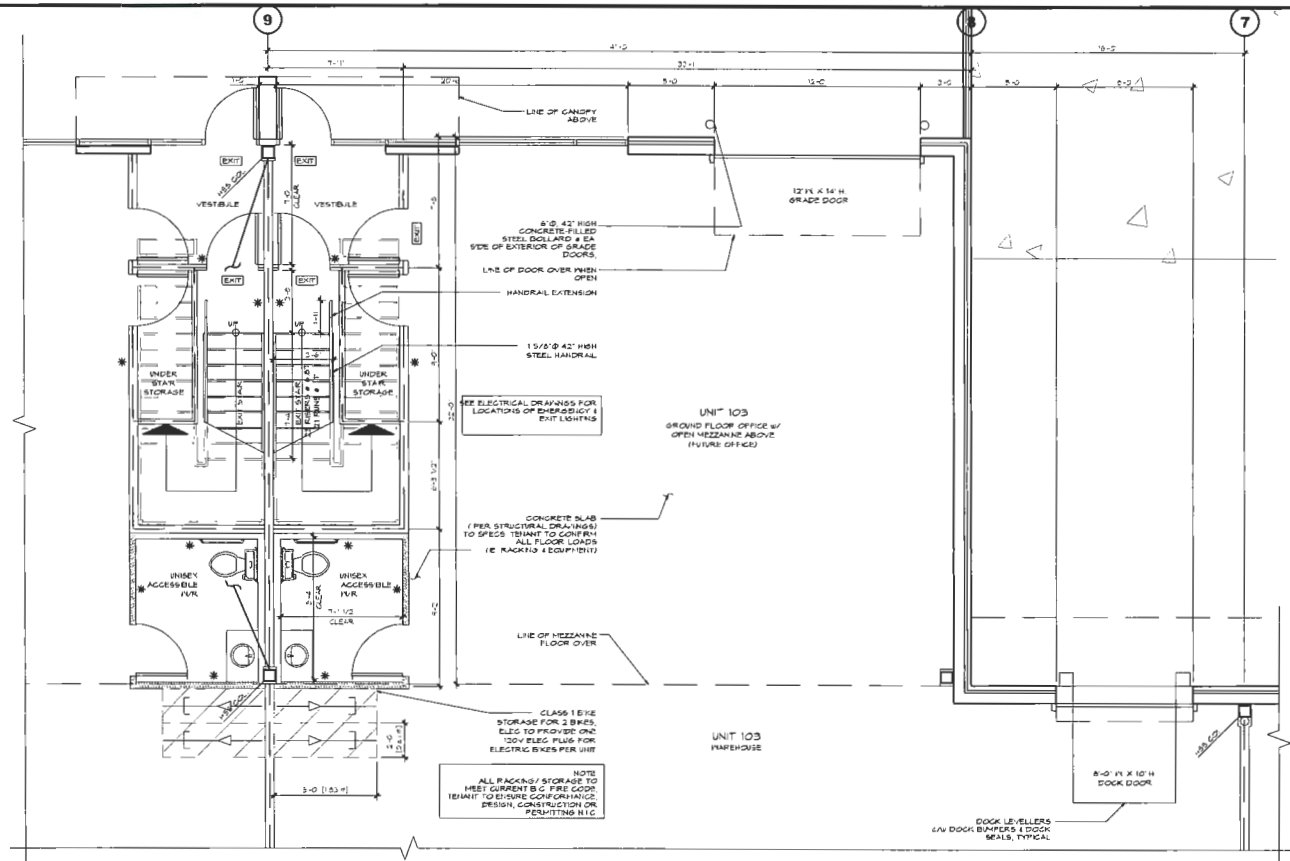
ORION CONSTRUCTION
 1015 CONSTRUCTION UNIT 105 19923 80A AVE
 LANGLEY, BC V2Y 0E2
 PHONE: (604) 352-2994

CEDAR COAST
 CEDAR COAST UNIT 3300 - 1021 West Hastings St.
 VANCOUVER, BC V6E 0C3

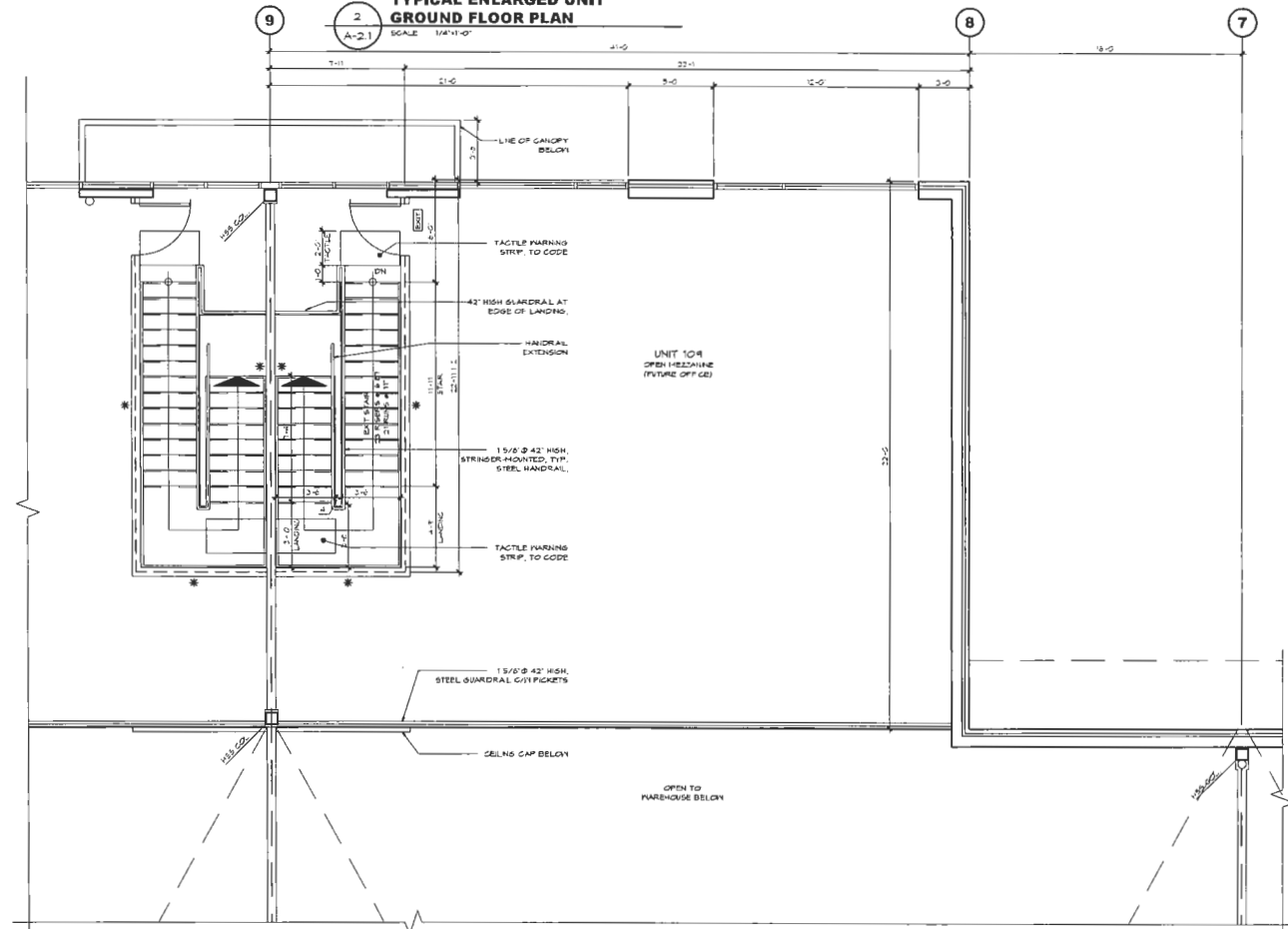
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2	OCT 04, 21	ISSUED FOR CP			
3	FEB 03, 22	RESUBMITTED FOR CP			
4	DEC 15, 22	UPDATED PER CITY COMMENT			
5	MAR 08, 23	ISSUED FOR USE			
6	MAR 13, 23	UPDATED PER JEP COMMENT			
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ISSUE NO.	DATE	REVISION	BY	DATE	REVISION
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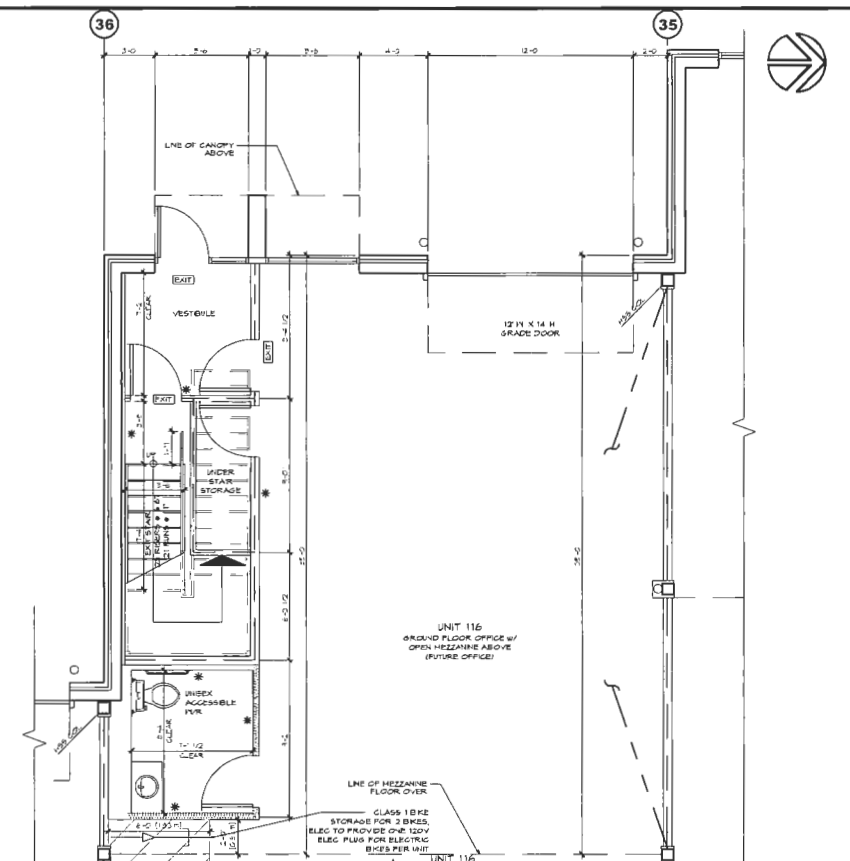
PROJECT PROPOSED BUILDING FOR
CEDAR COAST VAN HORNE
 ADDRESS: 980 VAN HORNE WAY, RICHMOND, BC
 DRAWING: **BUILDING B FLOOR PLANS** REV. 6
 DRAWN: **A-2.0**



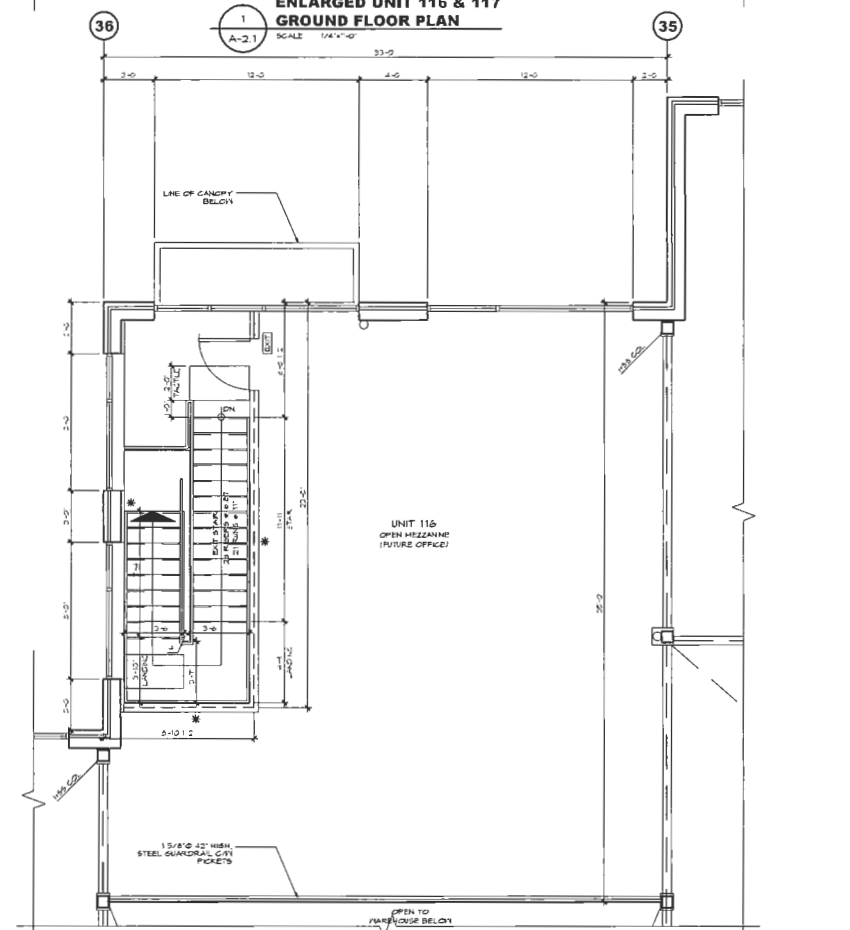
TYPICAL ENLARGED UNIT 2 GROUND FLOOR PLAN
SCALE 1/4" = 1'-0"



TYPICAL ENLARGED UNIT 4 UPPER FLOOR PLAN
SCALE 1/4" = 1'-0"



ENLARGED UNIT 116 & 117 GROUND FLOOR PLAN
SCALE 1/4" = 1'-0"



ENLARGED UNIT 116 & 117 UPPER FLOOR PLAN
SCALE 1/4" = 1'-0"

IN ASSOCIATION WITH
D.FORGE DESIGN INC.
2625A ALLIANCE STREET, ABBOTSFORD, B.C., V2S 3J9
TEL: (604) 607-6655 EMAIL: DARCY@DFORGE.CA

ARCHITECTURE PANEL INC.
8 ARCHITECTURE, LANGFORD ARCHITECTURE, STEVEN BERTON
225-1031 HUTTEN STREET, WHITE ROCK, B.C. V8B 6C4
484-1500 FAX: 1-800-888-7668

PROJECT MANAGERS/COORDINATORS
ORION CONSTRUCTION
ORION CONSTRUCTION
UNIT 105 19923 80A AVE
LANGLEY, BC V2Y 0E2
PHONE: (604) 352-2994

DEVELOPER
CEDAR COAST
CEDAR COAST
UNIT 3300 - 1021 West Hastings St.
VANCOUVER, BC V6E 0C3

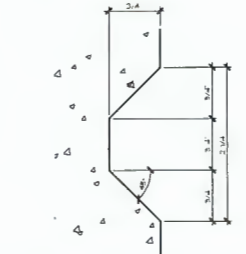
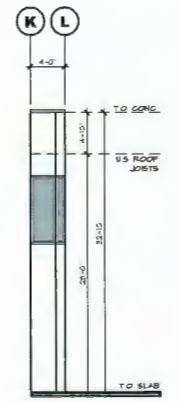
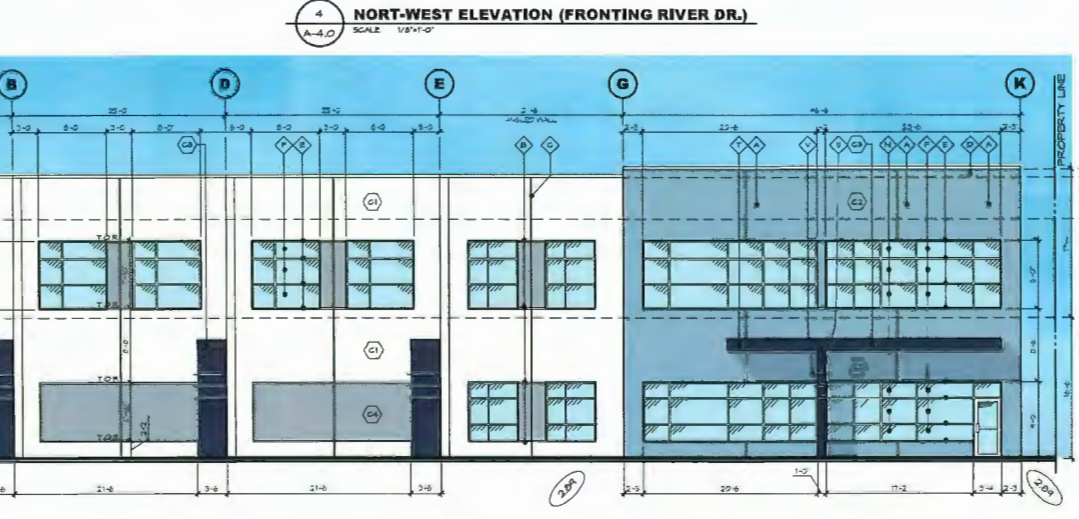
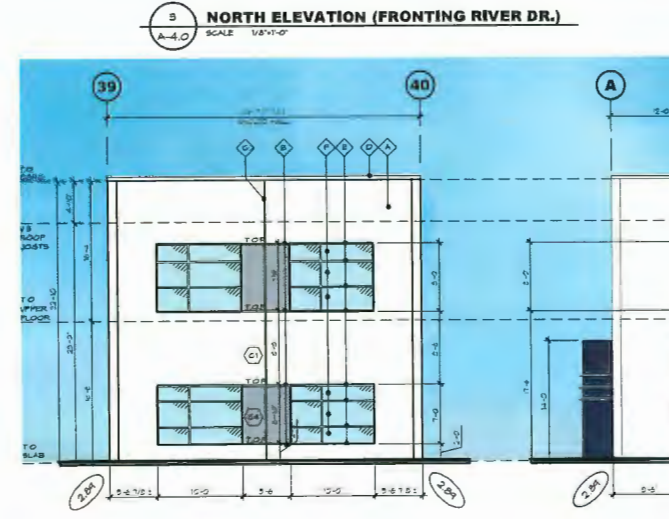
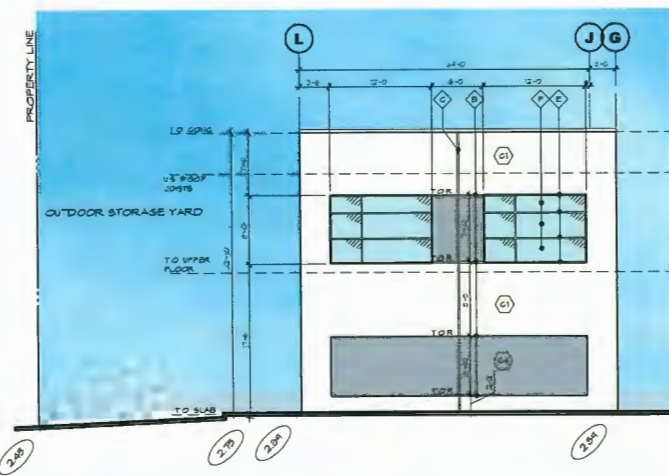
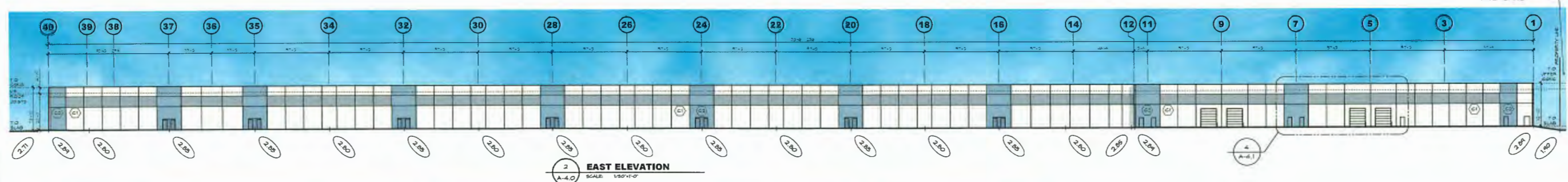
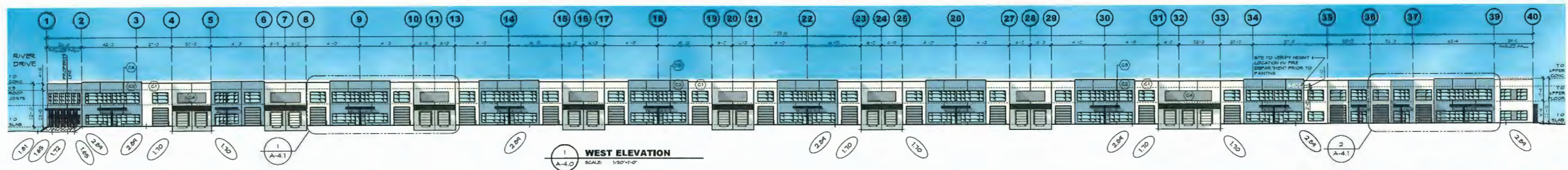
NO.	DATE	REVISION	NO.	DATE	REVISION
1	MAR.23	ISSUED FOR CONSULTANT USE	13		
2	MAR.13.23	UPDATED PER ACP COMMENT	14		
3			15		
4			16		
5			17		
6			18		
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9			21		
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11			23		
12			24		

PROJECT PROPOSED BUILDING FOR
CEDAR COAST VAN HORNE
ADDRESS: 3800 VAN HORNE WAY, RICHMOND, BC

DRAWING: **BUILDING B TYP. OFFICE PLANS**

REV. 1

DRAWN BY: [] CHECKED BY: []
JOB NO. 24-029
DATE PLOTTED: MAR.22.23
DRAWING NUMBER: **A-2.1**



- MATERIAL LEGEND**
- ◊ CONCRETE SMOOTH TILT-UP PANEL TYP - PAINTED
 - ◊ REVEAL IN CONCRETE - PAINTED
 - ◊ PANEL JOINT
 - ◊ PRE-FINISHED METAL CAP FLASHING
 - ◊ STOREFRONT FRAMES - CLEAR ANODIZED ALUMINUM
 - ◊ STOREFRONT GLASS - CLEAR
 - ◊ STEEL HANDDOORS - PAINTED
 - ◊ STEEL REGIONAL OVERHEAD DOORS - PAINTED
 - ◊ STEEL GUARDRAILS - PAINTED
 - ◊ CONCRETE RETAINING WALLS - NATURAL FINISH UNPAINTED TYP.
 - ◊ LIGHT FIXTURE - SEE ELECTRICAL DWS'S
 - ◊ SPANDREL GLAZING (OPSCURE, TO MATCH VISION GLASS)
 - ◊ SCUPPER
 - ◊ METAL GLAD CANOPY
 - ◊ STEEL BOLLARD - PAINTED
 - ◊ CONCRETE CURB UNDER, PAINTED
 - ◊ STEEL CANOPY, PAINTED
 - ◊ ENTRANCE CANOPY
 - ◊ FIN WALL
 - ◊ ANODIZED ALUMINUM FRAME ACCENT

- PROJECT COLOURS**
- ALL COLOURS SHEEN PELLUMS
- ◊ MAIN FIELD COLOUR 1 HIGH REFLECTIVE WHITE (S4 751)
 - ◊ FIELD ACCENT COLOUR 1 ALBATROSS (S4 8241)
 - ◊ FIELD ACCENT COLOUR 2 ANCHORS AWEIGH (S4 8174)
 - ◊ FIELD ACCENT COLOUR 3 COLOUR HATCH TO FESTPORH METALS RESIST GRAY
 - ◊ FIELD ACCENT COLOUR 5 FORDER GOAT BLACK VELVET
- EXTERIOR GUARDRAILS FORDER GOAT BLACK VELVET
 ALUMINUM STOREFRONT MULLIONS CLEAR ANODIZED ALUMINUM
 TYPICAL GLAZING CLEAR (LOW E, ASH-RUE RD 1 2010 COMPLIANT)
 SPANDREL BLACK-GRAY
 TYPICAL METAL CAP FLASHING FESTPORH METALS RESIST GRAY

- LEGEND:**
- ◊ PROPOSED GRADE

IN ASSOCIATION WITH

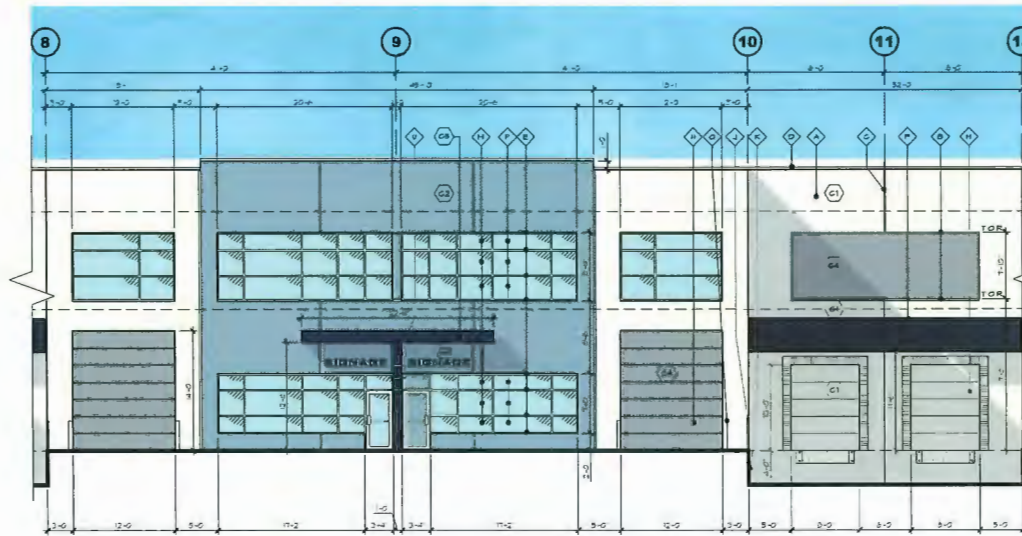
D.FORCE DESIGN INC.
 3625A ALLIANCE STREET, ABBOTSFORD, B.C., V2R 3J9
 TEL: (604) 607-9699 EMAIL: DARCY@DFORCE.CA

ORION CONSTRUCTION
 ORION CONSTRUCTION
 UNIT 105 19823 80A AVE
 LANGLEY, BC V2Y 0E2
 PHONE: (604) 352-2994

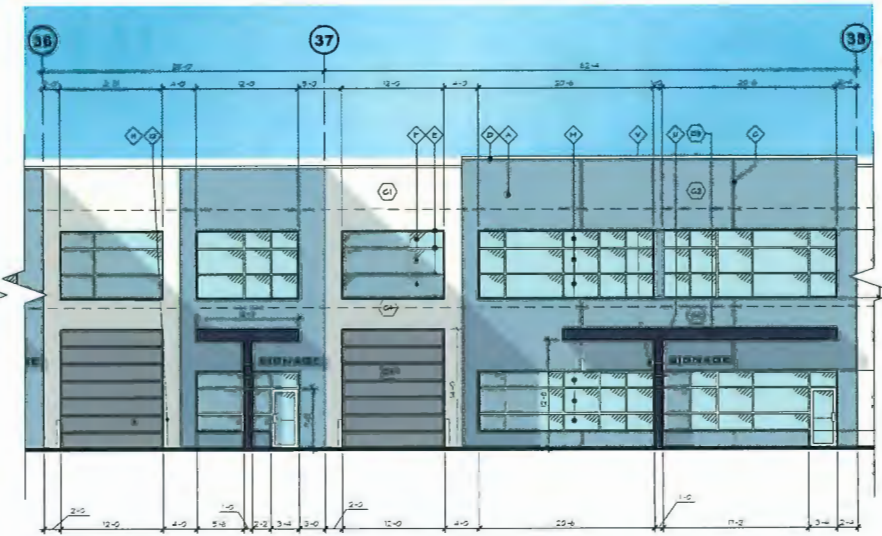
CEDAR COAST
 CEDAR COAST
 UNIT 3300 - 1021 West Hastings St,
 VANCOUVER, BC V5E 0C3

ISSUE NO.	DATE	REVISION	NO.	DATE	REVISION
1	SEP 17 21	CONSULTANT USE	13		
2	OCT 04 21	ISSUED FOR DP	14		
3	JAN 10 22	REISSUED FOR SP	15		
4	DEC 15 22	UPDATED PER CITY COMMENT	16		
5	MAR 05 23	ISSUED FOR SP	17		
6	MAR 15 23	UPDATED PER AEP COMMENT	18		
7			19		
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11			23		
12			24		

PROJECT: PROPOSED BUILDING FOR
CEDAR COAST VAN HORNE
 ADDRESS: 9850 VAN HORNE WAY, RICHMOND, BC
 DRAWING: BUILDING B ELEVATIONS
 REV. 6
 A-4.0



1 ENLARGED WEST ELEVATION
SCALE: 1/8"=1'-0"



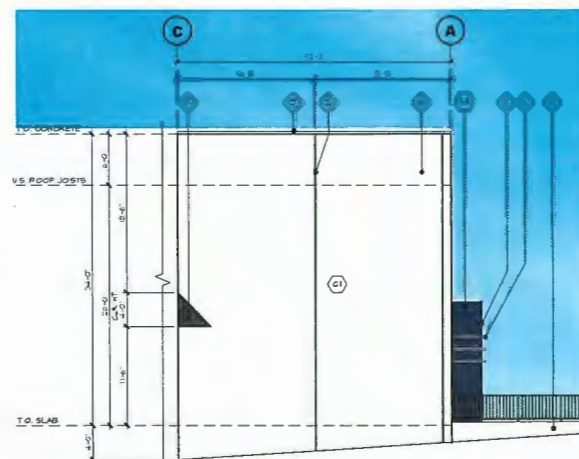
2 ENLARGED WEST ELEVATION
SCALE: 1/8"=1'-0"

- MATERIAL LEGEND**
- ◇ CONCRETE SANDRICH TILT-UP WALL PANEL, TYP. - PAINTED
 - ◇ REVEAL IN CONCRETE - PAINTED
 - ◇ PANEL JOINT
 - ◇ PRE-FINISHED METAL CAP FLASHING
 - ◇ STOREFRONT FRAMES - CLEAR ANODIZED ALUMINUM
 - ◇ STOREFRONT GLASS - CLEAR
 - ◇ STEEL HANDDOORS - PAINTED
 - ◇ STEEL SECTIONAL OVERHEAD DOORS - PAINTED
 - ◇ STEEL GUARDRAILS - PAINTED
 - ◇ CONCRETE RETAINING WALLS - NATURAL FINISH UNPAINTED TYP.
 - ◇ LIGHT FIXTURE - SEE ELECTRICAL DWD'S
 - ◇ SPANDREL GLAZING (OBSERVE TO MATCH VISION GLASS)
 - ◇ SOFFIT
 - ◇ METAL GLAD CANOPY
 - ◇ STEEL BOLLARD - PAINTED
 - ◇ CONCRETE CURB UNDER, PAINTED
 - ◇ STEEL CANOPY, PAINTED
 - ◇ ENTRANCE CANOPY
 - ◇ FIN WALL
 - ◇ ANODIZED ALUMINUM FRAME ACCENT

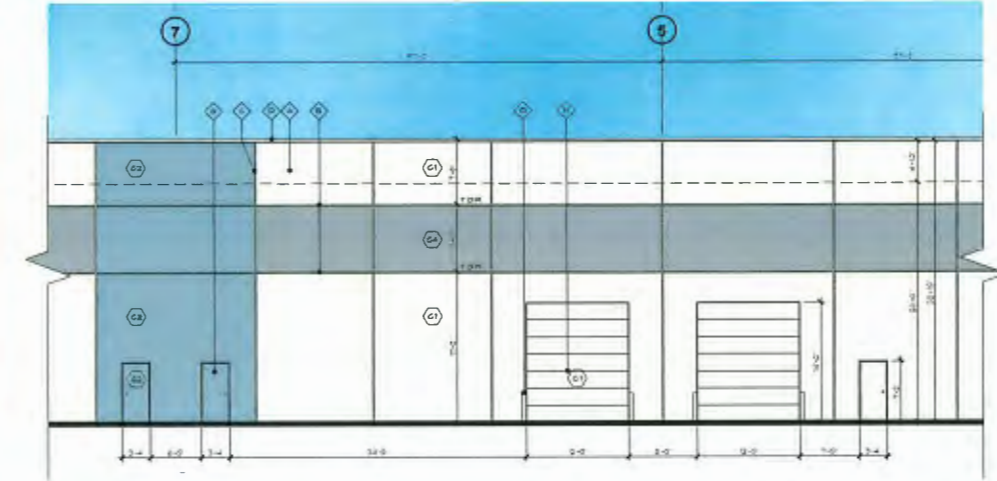
NOTE
- NOT ALL MATERIALS ARE APPLICABLE
◇ SPANDREL GLAZING

- PROJECT COLOURS**
- ALL COLOURS SHERWIN WILLIAMS
- ◇ MAIN FIELD COLOUR 1 HIGH REFLECTIVE WHITE (SW 7851)
 - ◇ FIELD ACCENT COLOUR 1 ALEXITAN (SW 6341)
 - ◇ FIELD ACCENT COLOUR 2 ANGLORS APESH (SW 4131)
 - ◇ FIELD ACCENT COLOUR 3 COLOUR MATCH TO PEDESTFORM METALS RESIDENT GRAY
 - ◇ FIELD ACCENT COLOUR 5 POWDER COAT BLACK VELVET
- EXTERIOR GUARDRAILS CLEAR ANODIZED ALUMINUM
ALUMINUM STOREFRONT MULLIONS CLEAR (ELOME, ABRASIVE TO 1 2010 COMPLIANT)
TYPICAL GLAZING CLEAR (ELOME, ABRASIVE TO 1 2010 COMPLIANT)
SPANDREL BLACK-GRAY
TYPICAL METAL CAP FLASHING PEDESTFORM METALS RESIDENT GRAY

LEGEND:
◇ PROPOSED GRADE



3 TYPICAL ELEVATION
@ GRID LINES 4, 7, 12, 16, 20, 24, 28, 32
@ GRID LINES (OPP) 3, 5, 9, 14, 18, 22, 26, 30
SCALE: 1/8"=1'-0"



4 ENLARGED EAST ELEVATION
SCALE: 1/8"=1'-0"

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IN ASSOCIATION WITH

D.FORCE DESIGN INC.

ARCHITECTURE PANEL INC.
2425A ALLIANCE STREET, ABBOTSFORD, B.C. V3B 3J9
TEL: (604) 807-5695 EMAIL: DARYL@DFORCE.COM

SEAL

PROJECT MANAGERS / CONTRACTORS

ORION CONSTRUCTION

ORION CONSTRUCTION
UNIT 105 19923 80A AVE
LANGLEY, BC V2Y 0E2
PHONE: (604) 362-2994

DEVELOPER

CEDAR COAST

CEDAR COAST
UNIT 3300 - 1021 West Hastings St.
VANCOUVER, BC V6E 0C3

ISSUE NO.	DATE	REVISION	NO.	DATE	REVISION
1	SEP 13 21	ISSUED FOR CONSULTANT USE	13		
2	DEC 24 21	ISSUED FOR GP	14		
3	JAN 10 22	RE-ISSUED FOR GP	15		
4	DEC 15 22	UPDATED PER CITY COMMENT	16		
5	MAR 09 23	CONSULTANT USE	17		
6	MAR 13 23	UPDATED PER ADP COMMENT	18		
7			19		
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ISSUE NO.	DATE	REVISION	NO.	DATE	REVISION
13			25		
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ISSUE NO.	DATE	REVISION	NO.	DATE	REVISION
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PROJECT PROPOSED BUILDING FOR

CEDAR COAST VAN HORNE

ADDRESS: 9800 VAN HORNE WAY, RICHMOND, BC

DRAWING: **BUILDING B ENLARGED ELEVATIONS**

REV: 6

SCALE: **A-4.1**

DRAWN	CHECKED
6	A-4.1