




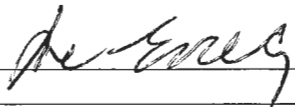

To: Planning Committee **Date:** August 16, 2017
From: Wayne Craig **File:** 08-4430-03-11/2017-Vol 01
Director, Development
Re: **Establishment of Underlying Zoning for Properties Developed Under Land Use Contracts 039, 040, 064, 079 and 126 in the north portion of the City Centre**

Staff Recommendation

1. That Richmond Zoning Bylaw 8500, Amendment Bylaw 9744, to establish underlying zoning for the property developed under Land Use Contract 039, be introduced and given first reading;
2. That Richmond Zoning Bylaw 8500, Amendment Bylaw 9745, to establish underlying zoning for the property developed under Land Use Contract 040, be introduced and given first reading;
3. That Richmond Zoning Bylaw 8500, Amendment Bylaw 9746, to establish underlying zoning for the property developed under Land Use Contract 064, be introduced and given first reading;
4. That Richmond Zoning Bylaw 8500, Amendment Bylaw 9747, to establish underlying zoning for the property developed under Land Use Contract 079, be introduced and given first reading; and
5. That Richmond Zoning Bylaw 8500, Amendment Bylaw 9748, to establish underlying zoning for five properties developed under Land Use Contract 126, be introduced and given first reading.


Wayne Craig
Director, Development
(604-247-4625)

CL/JH:blg
Att. 4

REPORT CONCURRENCE		
ROUTED TO: Law	CONCURRENCE <input checked="" type="checkbox"/>	CONCURRENCE OF GENERAL MANAGER 
REVIEWED BY STAFF REPORT / AGENDA REVIEW SUBCOMMITTEE	INITIALS: CJ	APPROVED BY CAO (DEPUTY). 

Staff Report

Origin

In 2014, the Provincial Government amended the *Local Government Act* to require municipalities to adopt underlying zoning bylaws for all LUC properties by June 30, 2022, and to provide for the termination of all LUCs on June 30, 2024. The amending legislation also established an optional process to enable municipalities, by bylaw, to undertake early termination of LUCs and provided expanded authority to Boards of Variance to hear appeals and grant time extensions to existing property owners for reasons of hardship.

On November 24, 2015, Richmond City Council adopted a set of bylaws that established underlying zoning for 93 separate LUCs that included single-family properties, as well as adopted bylaws to terminate these LUCs effective one year from the date of adoption (i.e., November 24, 2016). Because the remaining 46 LUCs on properties that include multi-family, commercial, industrial, and agricultural land uses are not subject to the same redevelopment pressures as that of the LUCs that included single-family properties, the remaining LUCs were to be dealt with separately at a later date.

There are currently 46 remaining LUCs still in effect on a total of 95 properties in the city (including 3,078 units) containing multi-family, commercial, industrial, and agricultural uses, which were not subject to the underlying zoning bylaws and early termination bylaws adopted on November 24, 2015 (Attachment 1).

Consistent with the *Local Government Act*, City Council will have to consider bylaws to establish underlying zoning for the properties developed under the remaining LUCs. This involves the standard bylaw reading and adoption process, and includes holding a Public Hearing for all bylaws.

This report brings forward underlying zoning bylaws for five out of the 46 remaining LUCs (LUCs 039, 040, 064, 079 and 126). The proposed bylaws are applicable to nine commercial/ industrial properties in the north portion of City Centre at 8520 Cambie Road, 4940 and 3791 No. 3 Road, 8191 Alderbridge Way, 8260, 8280, 8300, 8380 Bridgeport Road, and a portion of 8211 Sea Island Way (Attachment 2). Subsequent underlying zoning bylaws for the remaining 41 LUCs will be brought forward separately for consideration on the basis of their geographic area, as illustrated in Attachment 3 (i.e., the south portion of City Centre, Seafair/Blundell, Broadmoor, Steveston, and East Richmond).

Unlike the approach used for the 93 LUCs that included single-family properties, no early termination bylaws are proposed to be brought forward for the remaining LUCs. Essentially, the existing remaining LUCs will remain effective and continue to govern the use and development of the affected properties until their termination date of June 30, 2024, at which time the underlying zoning will take effect.

This report supports Council's 2014-2018 Term Goal # 3 – A Well-Planned Community:

Adhere to effective planning and growth management practices to maintain and enhance the livability, sustainability and desirability of our City and its neighbourhoods, and to ensure the results match the intentions of our policies and bylaws.

3.1 Growth and development that reflects the Official Community Plan (OCP) and related policies and bylaws

This report and the proposed bylaws are also consistent with policies from the 2041 Official Community Plan (OCP), which support exploring alternatives to Land Use Contracts to achieve better land use management over time.

Findings of Fact

A Land Use Contract (LUC) is a contract between a property owner (typically a developer) and a municipality addressing the use and development rights of a property. The LUC regulations are similar to zoning, with the exception that the LUC is registered on the Title of the property and, until recently, agreement from both the property owner and municipality was required to amend or discharge the contract.

The provincial legislation enabling LUCs was in effect for a short period of time between 1973 and 1979 and allowed the ability to create tailor-made development contracts for specific sites. LUCs were also used to control the form and character of buildings and landscaping of sites and, in some cases, included detailed servicing requirements. Typically, the same LUC was registered by a developer against all the properties in a particular subdivision, thereby creating consistent use and development rights for those properties. Unless discharged, LUCs registered during such period remain in place today governing the use and development rights of the affected properties.

LUCs typically include limited development restrictions compared to today's standards. Any reference to a zoning bylaw within a LUC is specific to the zoning bylaw in place at the date of contract execution. Since LUCs are registered on Title and can only be amended or discharged with the property owner's consent, the result is that LUCs have not evolved over time as land use considerations have changed. Properties under the current Richmond Zoning Bylaw 8500 have had multiple amendments over time to address various land issues such as building interface, landscaping, sustainability and overall building form.

Analysis

Staff propose a set of bylaws that introduce underlying zoning for the nine properties developed under LUCs 039, 040, 064, 079 and 126 in the north portion of the City Centre, which are identified in the table below:

LUC #	No. of Properties	Address	No. of Units (Strata & Non-Strata)
039	1	8520 Cambie Road	N/A
040	1	4940 No. 3 Road	48
064	1	3791 No. 3 Road	N/A
079	1	8191 Alderbridge Way	N/A
126	5	8260 Bridgeport Road	N/A
		8280 Bridgeport Road	N/A
		8300 Bridgeport Road	N/A
		8380 Bridgeport Road	N/A
		8211 Sea Island Way	3

There are an additional six properties at 8320, 8340, 8360, 8440 Bridgeport Road, and 8311, 8351 Sea Island Way that were also developed under LUC 126, which are not included in the proposed underlying zoning bylaws as they are currently the subject of a separate LUC discharge and rezoning application which was considered at the July 17, 2017 Public Hearing and the associated zoning amendment bylaw granted third reading (RZ 13-628557). If the rezoning bylaw is not adopted, City staff will bring forward a separate report and zoning amendment bylaw to establish underlying zoning for these properties.

Attachment 4 contains a series of summary tables that provide a comparison of the regulations under each of the five LUCs with those of the proposed underlying zone, and includes a map of each LUC. The summary tables in Attachment 4 are for reference purposes only and should not be interpreted as the actual LUC.

In developing the underlying zoning for the nine subject properties, staff considered the specific provisions in each individual LUC, as well as the existing zoning of adjacent properties within the immediate surrounding area. With the exception of one site under LUC 126, staff were not able to use existing commercial and industrial zones in Richmond Zoning Bylaw 8500 to develop the underlying zoning bylaws for the properties due to the very specific provisions contained in these particular LUCs.

The property at 8211 Sea Island Way is under two zones: the “Auto-Oriented Commercial (CA)” zone, and LUC 126. Since the current use of the land that is under LUC 126 is that of a vehicle lane only, staff propose the CA zone as the underlying zoning for that portion of the property to bring the entire property under the CA zone.

For the remaining properties under LUC 039, 040, 064, 079, and 126, staff propose five new site-specific zones. The proposed site-specific zones combine both the specific provisions from each LUC as well as certain provisions contained within Richmond Zoning Bylaw 8500 for aspects not anticipated by the LUC. This enables the underlying zoning bylaws to mirror what is contained in the LUCs without granting additional permitted land uses while allowing some flexibility after LUCs expire on June 30, 2024 for landowners to make minor changes to their properties that would be in character with what is permitted on lots within the surrounding neighbourhood.

The five new site-specific zones are described below.

- *Commercial Storage (ZI13) – Cambie Road (City Centre)*, (Bylaw No. 9744), is proposed for the portion of the property developed under LUC 039 at the north end of 8520 Cambie Road, which currently contains a one-storey public storage facility.
- *Auto-Oriented Commercial (ZC40) – No. 3 Road (City Centre)*, (Bylaw No. 9745), is proposed for the property developed under LUC 040 at 4940 No. 3 Road, which currently contains a three-storey building with commercial retail uses at grade and office uses in the upper floors.
- *Vehicle Sales Commercial (ZC41) – No. 3 Road (City Centre)*, (Bylaw No. 9746), is proposed for the property developed under LUC 064 at 3791 No. 3 Road, which currently contains a one-storey building that is part of the vehicle sales operation on the neighbouring site at 3771 No. 3 Road.
- *Restaurant Commercial (ZC42) – Alderbridge Way (City Centre)*, (Bylaw No. 9747), is proposed for the property developed under LUC 079 at 8191 Alderbridge Way, which currently contains a two-storey McDonald's restaurant and drive-through.
- *Commercial (ZC43) – Bridgeport Road (City Centre)*, (Bylaw No. 9748), is proposed for four of the properties developed under LUC 126 at 8260, 8280, 8300, 8380 Bridgeport Road, which currently contain vehicle parking and storage, and a restaurant.

Where there are inconsistencies between the provisions of the proposed underlying zones and what actually exists on the subject properties, any use and development of the land that was lawful under the LUC will be protected in accordance with the provisions for non-conforming uses and buildings under the *Local Government Act* after the LUCs expire on June 30, 2024.

Ministry of Transportation and Infrastructure (MOTI) Approval

As seven of the subject properties under LUC 039, 064, and 126 are located within 800m of an intersection of a Provincial Limited Access Highway and a City road, three of the proposed underlying zoning bylaws (Bylaws 9744, 9746, and 9748) have been referred to MOTI for preliminary approval. Final approval from MOTI is required prior to final adoption of the underlying zoning bylaws.

Public Consultation and Public Hearing

Since the existing remaining LUCs will remain effective and will continue to govern the use and development of the affected properties until their termination date of June 30, 2024, at which time the proposed underlying zoning will be in place, it is anticipated that the proposed approach will not generate a significant amount of public interest. Therefore the standard bylaw adoption and public consultation processes are proposed. This approach is also proposed for the underlying zoning bylaws that are subsequently to be brought forward for the remaining 41 LUCs on the basis of their geographic area.

The standard bylaw adoption and public consultation process involves the underlying zoning bylaws being considered at a Planning Committee meeting, bylaw readings by City Council, the publication of the statutory Public Hearing Notice and newspaper ads, and includes the holding of a regular Public Hearing in the Council Chambers. This approach does not require additional financial or human resources beyond that of the standard rezoning and public hearing processes.

Prior to each Public Hearing at which underlying zoning bylaws are to be considered, a press release will be issued to publicize Council's decision to establish underlying zoning bylaws for the affected properties and to direct further inquiries to the City's LUC webpage, and to the general LUC inquiry email address and phone number. Staff will also send a letter to each of the affected property owners, which will contain information that is specific to the proposed underlying zoning for their respective property.

Following each Public Hearing, Council may consider adoption of those underlying zoning bylaws that do not require MOTI approval. For those bylaws that do require MOTI approval Council may be consider bylaw adoption at a subsequent Council meeting after MOTI approval has been granted.

Following adoption of the underlying zoning bylaws, the existing LUCs on the affected properties will remain effective until June 30, 2024, after which time the underlying zoning bylaws will be in place to govern the use and development of the properties.

Financial Impact

As mentioned in the previous section, the consideration of the proposed Bylaws 9744 through 9748 by the Planning Committee, City Council, and at a regular Public Hearing in the Council Chambers will not require additional financial or human resources beyond that of the standard rezoning and public hearing processes.

Conclusion

Consistent with the *Local Government Act*, City Council will have to consider bylaws to establish underlying zoning for the properties developed under the remaining 46 LUCs in the city prior to June 30, 2022.

Staff propose to bring forward the underlying zoning bylaws for the remaining LUCs as separate items on the basis of their geographic area for consideration by Planning Committee, City Council, and at regular Public Hearings in the Council Chambers.

This report brings forward five underlying zoning bylaws for nine commercial/industrial properties developed under Land Use Contracts 039, 040, 064, 079 and 126 in the north portion of the City Centre area (i.e., 8520 Cambie Rd, 4940 and 3791 No. 3 Road, 8191 Alderbridge Way, 8260, 8280, 8300, 83380 Bridgeport Road, and 8211 Sea Island Way).

Staff recommends that Richmond Zoning Bylaw 8500, Amendment Bylaws 9744, 9745, 9746, 9747, and 9748, be introduced and given first reading.



Cynthia Lussier
Planner 1
(604-276-4108)



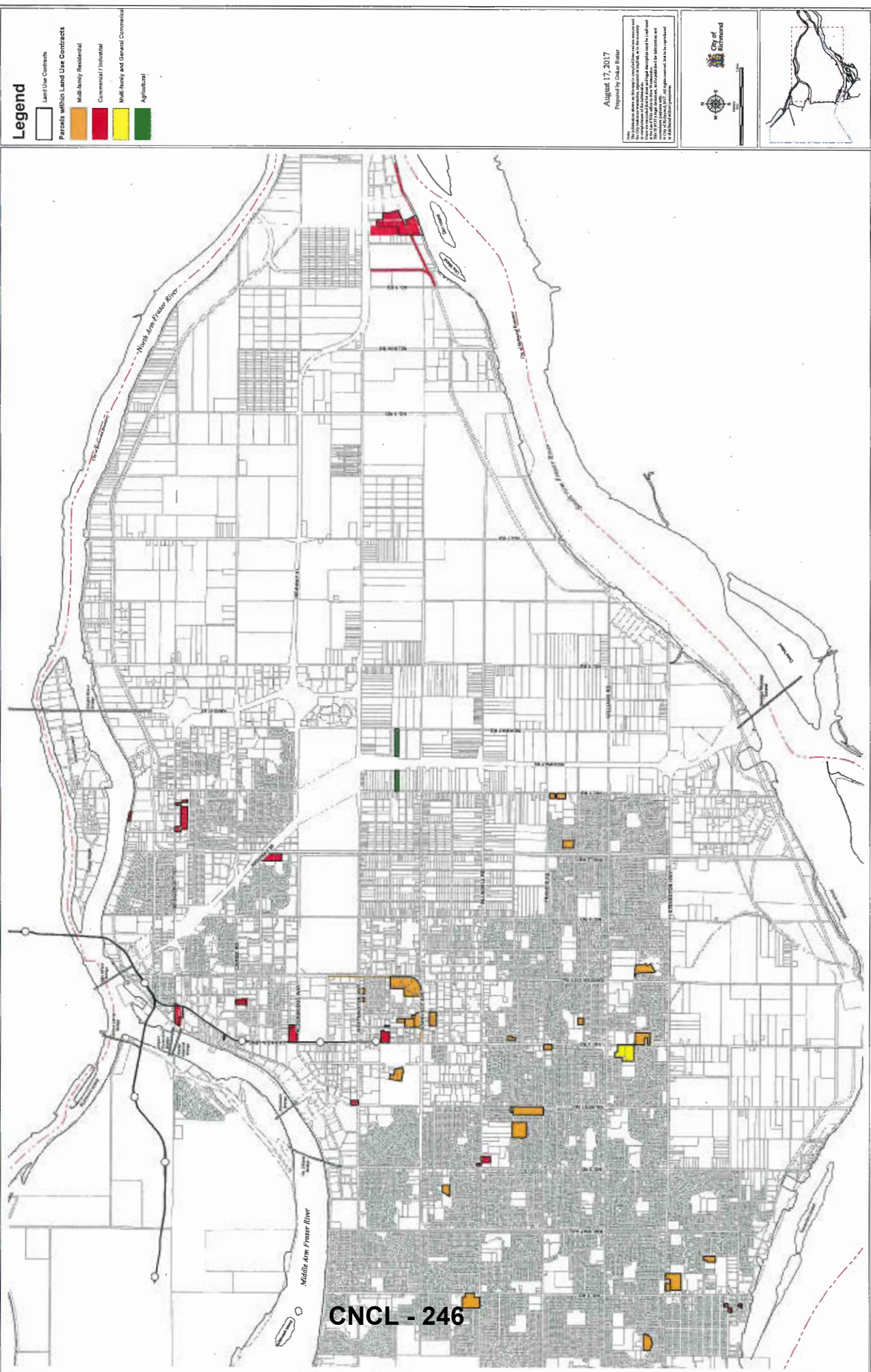
John Hopkins, MCIP, RPP
Senior Planner
(604-276-4279)

CL/JH:blg

Attachments:

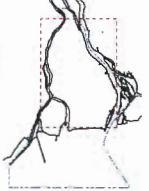
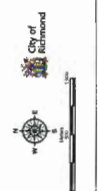
- Attachment 1: Map of Remaining Land Use Contracts
- Attachment 2: Land Use Contracts in City Centre (North)
- Attachment 3: Land Use Contracts by Geographic Area
- Attachment 4: Land Use Contract Summary and Comparison Tables

Remaining Land Use Contracts



August 17, 2017
 Prepared by: Oskar Baller

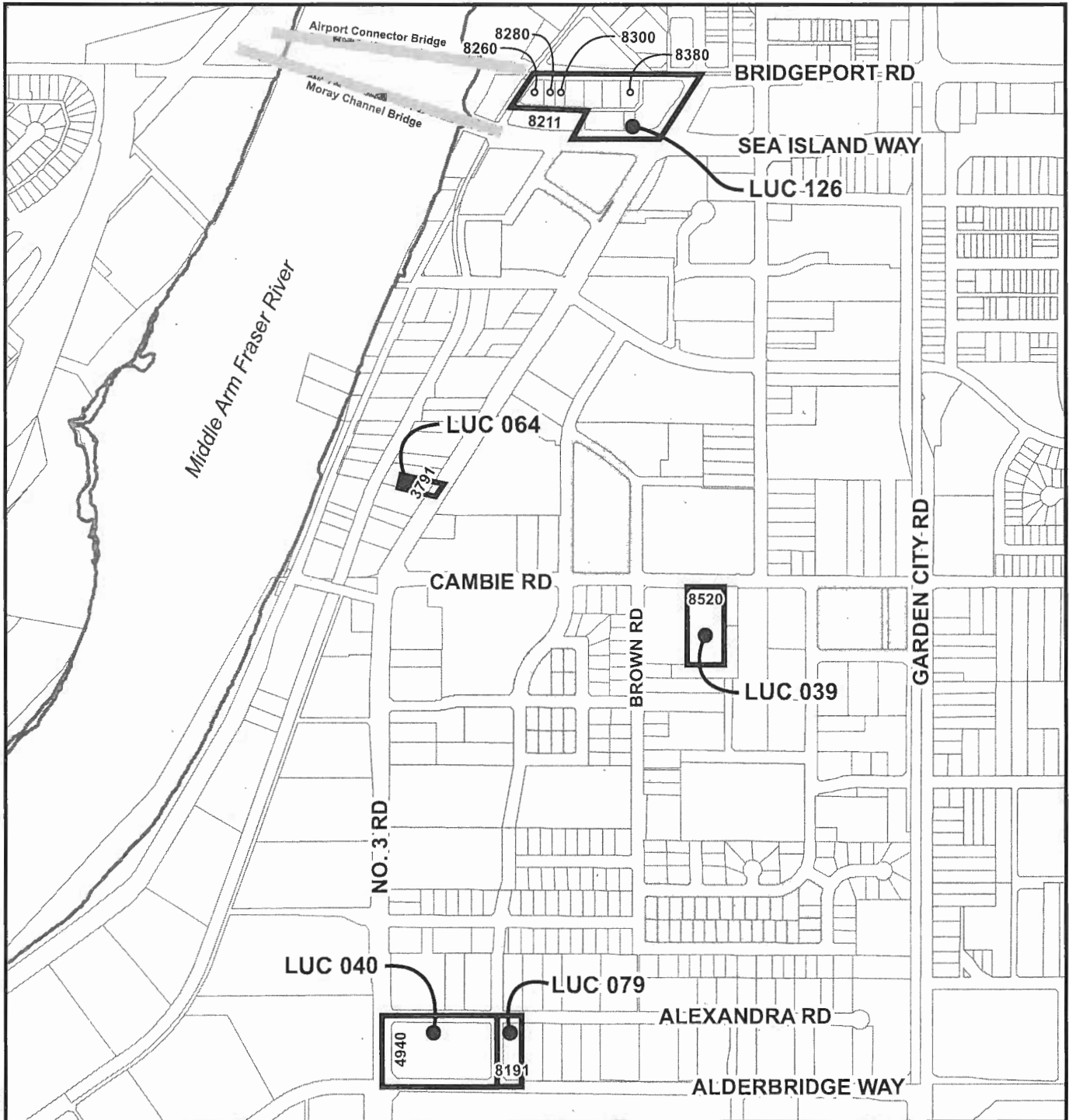
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CNCL - 246



City of Richmond



Land Use Contracts (LUC) in City Centre (North)

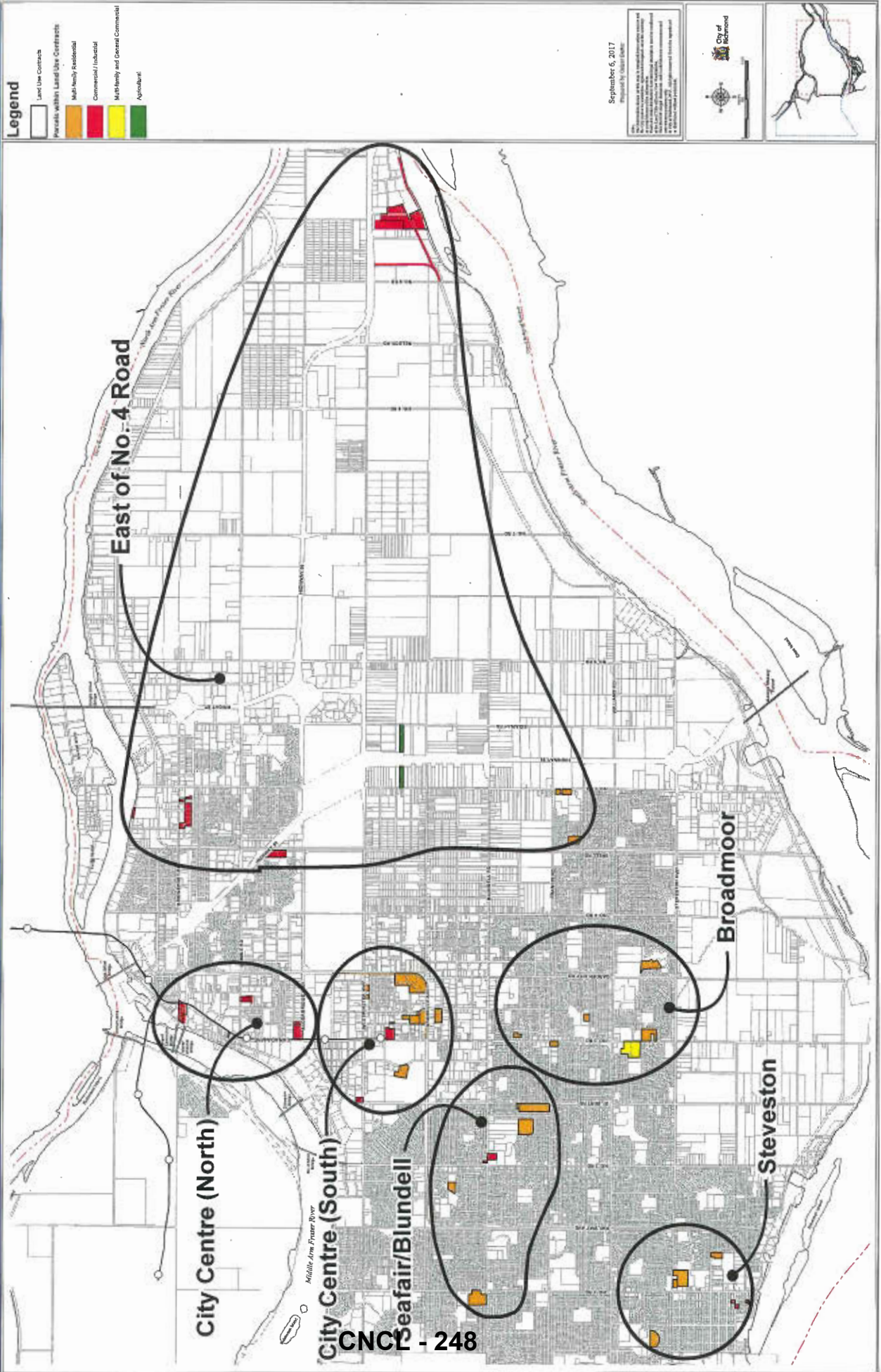
GNCL - 247

Original Date: 08/24/17

Revision Date: 00/00/00

Note: Dimensions are in METRES

Remaining LUCs by Geographic Area



Attachment 4

Land Use Contract Summary & Comparison Tables

LUC 039

LUC 040

LUC 064

LUC 079

LUC 126

LAND USE CONTRACT SUMMARY & COMPARISON TABLES

Land Use Contract 039

Permitted Uses: Commercial storage and a secondary residential security/operator unit

Number of properties: 1

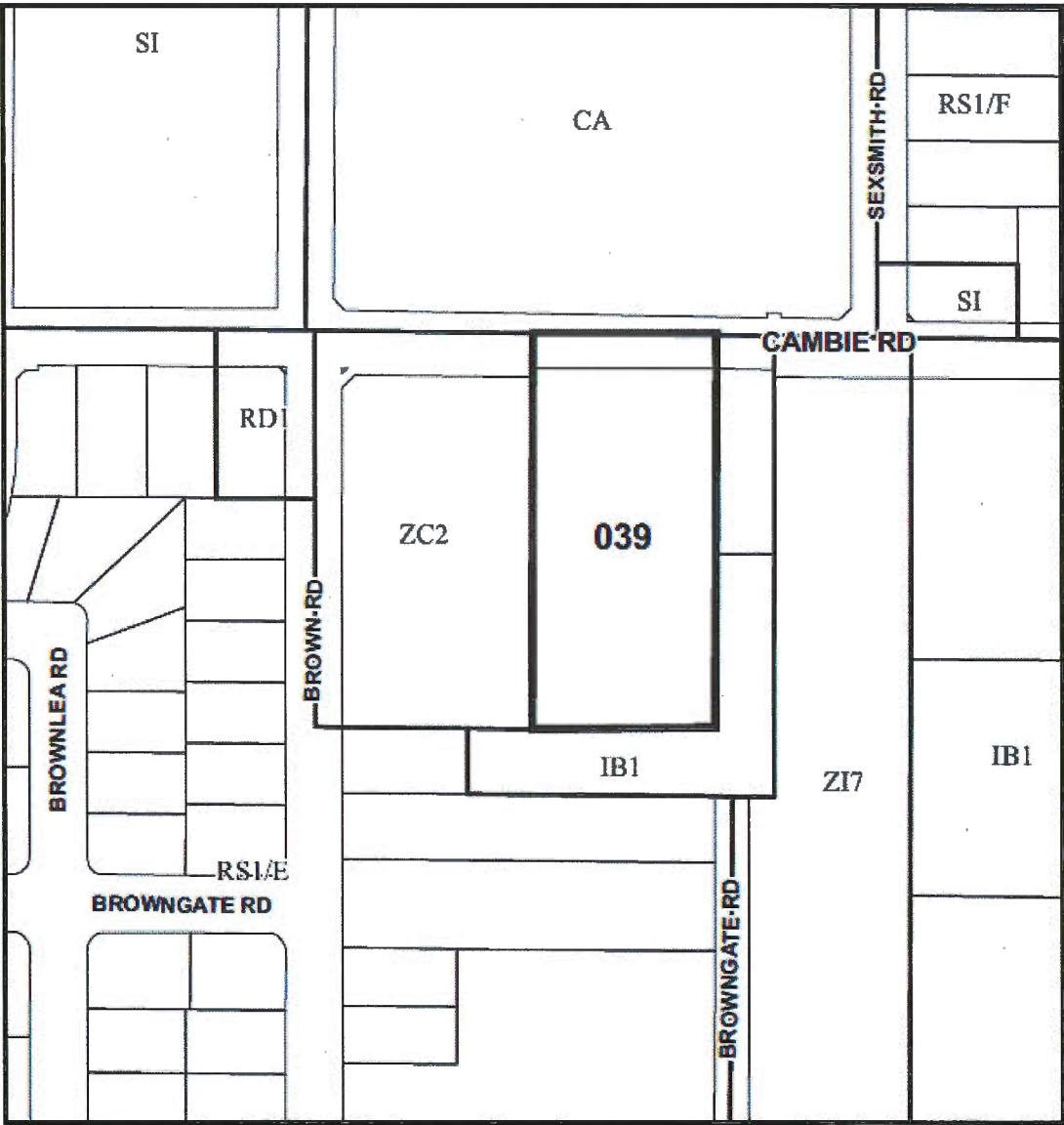
Proposed Zone: Commercial Storage (ZI13) – Cambie Road (City Centre)

The table below is intended to provide a general comparison between the land use contract regulations and the proposed new zone. The table may not include site specific amendments or court orders made since registration of the land use contract.

	LUC 039	ZI13
FAR (max)	N/A	<ul style="list-style-type: none"> The maximum permitted floor area is 3,800 m²; 0.48 FAR
Lot Coverage (max)	As per drawings (approx. 48%)	48%
Front Yard Setback (min)	As per drawings	7.0 m
Interior Side Yard Setback (min)	As per drawings	6.0 m
Rear Yard Setback (min)	As per drawings	7.0 m
Building Height (max)	As per drawings (1 storey)	<ul style="list-style-type: none"> 1 storey; 5.0 m.

Disclaimer: This summary is provided for general public information only and does not form a representation by the City. Any person making a land use, building construction or financial decision should obtain independent advice regarding all applicable regulations.

LAND USE CONTRACT SUMMARY & COMPARISON TABLES



Land Use Contract 039

LAND USE CONTRACT SUMMARY & COMPARISON TABLES

Land Use Contract 040

Permitted Uses:

- contractor service
- education commercial
- entertainment, spectator
- government service
- health service, minor
- hotel
- manufacturing, custom indoor
- office
- private club
- recreation, indoor
- restaurant
- retail, convenience
- retail, general
- service, business support
- service, financial
- service, household repair
- service, personal
- transportation depot
- veterinary service

Number of properties: 1

Proposed Zones: Auto-Oriented Commercial (ZC40) – No. 3 Road (City Centre)

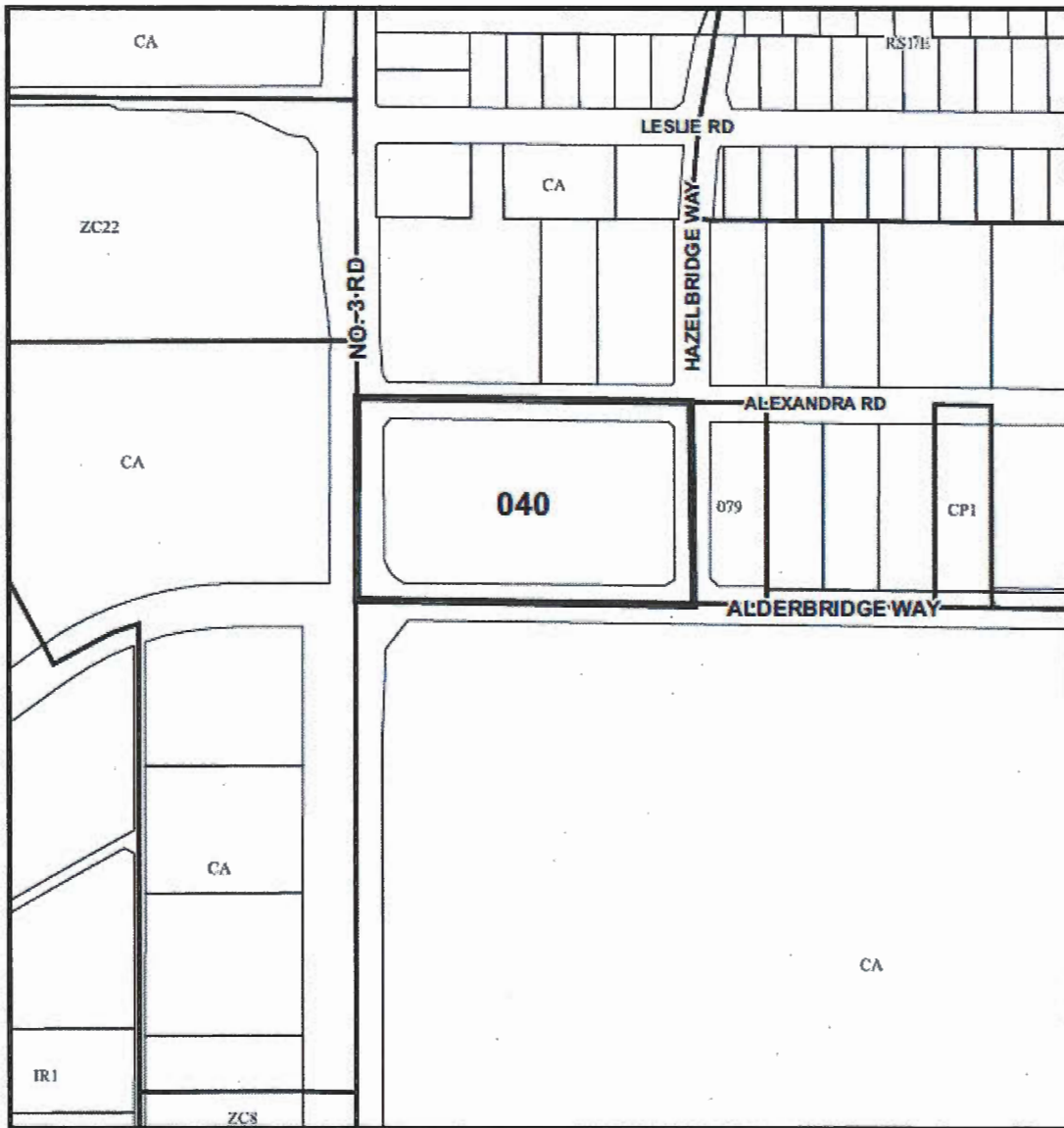
The table below is intended to provide a general comparison between the land use contract regulations and the proposed new zone. The table may not include site specific amendments or court orders made since registration of the land use contract.

	LUC 040	ZC40
FAR (max)	N/A Note: a maximum gross floor area of 4,190 m ² was specified for the first storey, and a maximum gross floor area of 4,576 m ² was specified for the 2 nd and 3 rd storeys combined.	<ul style="list-style-type: none"> • The maximum permitted floor area is 4,190 m² on the first storey, and 4,576 m² for the 2nd and 3rd storeys combined; • 0.60 FAR.
Lot Coverage (max)	As per drawings (approx. 30%)	30%
Front Yard Setback (min)	As per drawings	19.0 m to No. 3 Road
Side Yard Setback (min)	As per drawings	19.0 m to Alderbridge Way; 3.0 m to Alexandra Road.
Rear Yard Setback (min)	As per drawings	17.0 m to Hazelbridge Way
Height (max)	3 storeys (approx. 17.0 m)	17.0 m, but containing no more than 3 storeys.

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LAND USE CONTRACT SUMMARY & COMPARISON TABLES



Land Use Contract 040

LAND USE CONTRACT SUMMARY & COMPARISON TABLES

Land Use Contract 064

Permitted Uses: Vehicle sale/rental

Number of properties: 1

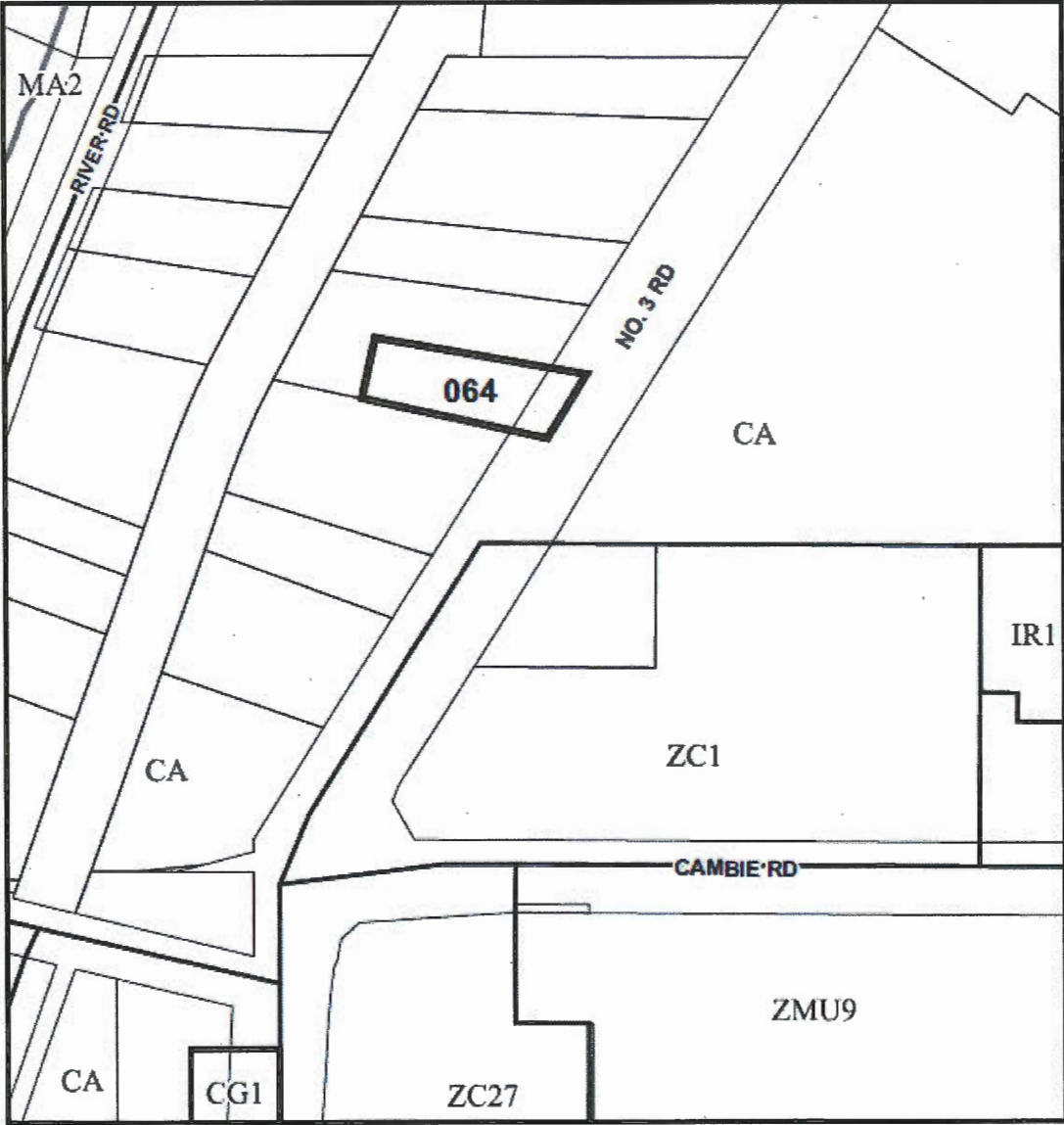
Proposed Zone: Vehicle Sales Commercial (ZC41) – No. 3 Road (City Centre)

The table below is intended to provide a general comparison between the land use contract regulations and the proposed new zone. The table may not include site specific amendments or court orders made since registration of the land use contract.

	LUC 064	ZC41
FAR (max)	N/A	2.3 FAR
Lot Coverage (max)	N/A	N/A
Front Yard Setback (min)	N/A	7.6 m
Side Yard Setback (min)	<ul style="list-style-type: none"> • 3.0 m to one side lot line where there is no rear lane adjacent to the lot. • 3.0 m next to containing 1 storey buildings; • 7.6 m next to neighbouring lots zoned for low density residential and agricultural uses containing buildings greater than 1 storey; 	<ul style="list-style-type: none"> • 3.0 m to one interior side lot line; • Notwithstanding the above, the minimum side yard on a lot that is adjacent to single detached housing, agriculture, or two-unit dwelling zones, shall be: <ul style="list-style-type: none"> - 3.0 m for a 1 storey building; and - 7.5 m for a building containing more than 1 storey.
Rear Yard Setback (min)	N/A	N/A
Building Height (max)	10.7 m, but containing no more than 3 storeys	10.7 m, but containing no more than 3 storeys

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LAND USE CONTRACT SUMMARY & COMPARISON TABLES



Land Use Contract 064

LAND USE CONTRACT SUMMARY & COMPARISON TABLES

Land Use Contract 079

Permitted Uses: Restaurant

Number of properties: 1

Proposed Zone: Restaurant Commercial (ZC42) – Alderbridge Way (City Centre)

The table below is intended to provide a general comparison between the land use contract regulations and the proposed new zone. The table may not include site specific amendments or court orders made since registration of the land use contract.

	LUC 079	ZC42
FAR (max)	As per drawings	<ul style="list-style-type: none"> The maximum permitted floor area is 800 m²; 0.27 FAR
Lot Coverage (max)	As per drawings (approx. 25%)	25%
Front Yard Setback (min)	As per drawings	6.0 m to Alderbridge Way
Interior Side Yard Setback (min)	As per drawings	N/A
Exterior Side Yard Setback (min)	As per drawings	1.5 m to Hazelbridge Way
Rear Yard Setback (min)	As per drawings	50.0 m to Alexandra Road
Height (max)	As per drawings (2 storeys; approx. 9.0 m)	9.0 m, but containing no more than 2 storeys

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LAND USE CONTRACT SUMMARY & COMPARISON TABLES



Land Use Contract 079

LAND USE CONTRACT SUMMARY & COMPARISON TABLES

Land Use Contract 126

Permitted Uses:

- contractor service
- entertainment, spectator
- equipment, minor
- manufacturing, custom indoor
- office
- recreation, indoor
- restaurant
- retail, general
- service, business support
- service, household repair

Site-specific Permitted Uses:

The following uses are permitted only at 8280 and 8300 Bridgeport Road:

- commercial vehicle parking and storage *
- fleet service *
- parking, non-accessory *

The following uses are permitted only at 8300 Bridgeport Road:

- vehicle rental, convenience

(* In 1989, City Council supported an amendment to the LUC to include what is now interpreted as “commercial vehicle parking and storage”, “fleet service”, and “parking, non-accessory” as permitted uses at 8280 and 8300 Bridgeport Rd. The amendment was never registered on title of the lots.)

Number of properties: 5

Proposed Zones:

Commercial (ZC43) – Bridgeport Road (City Centre) for 4 properties on Bridgeport Road.

Auto-Oriented Commercial (CA) for a portion of 1 property on Sea Island Way.

The table below is intended to provide a general comparison between the land use contract regulations and the proposed new zone. The table may not include site specific amendments or court orders made since registration of the land use contract.

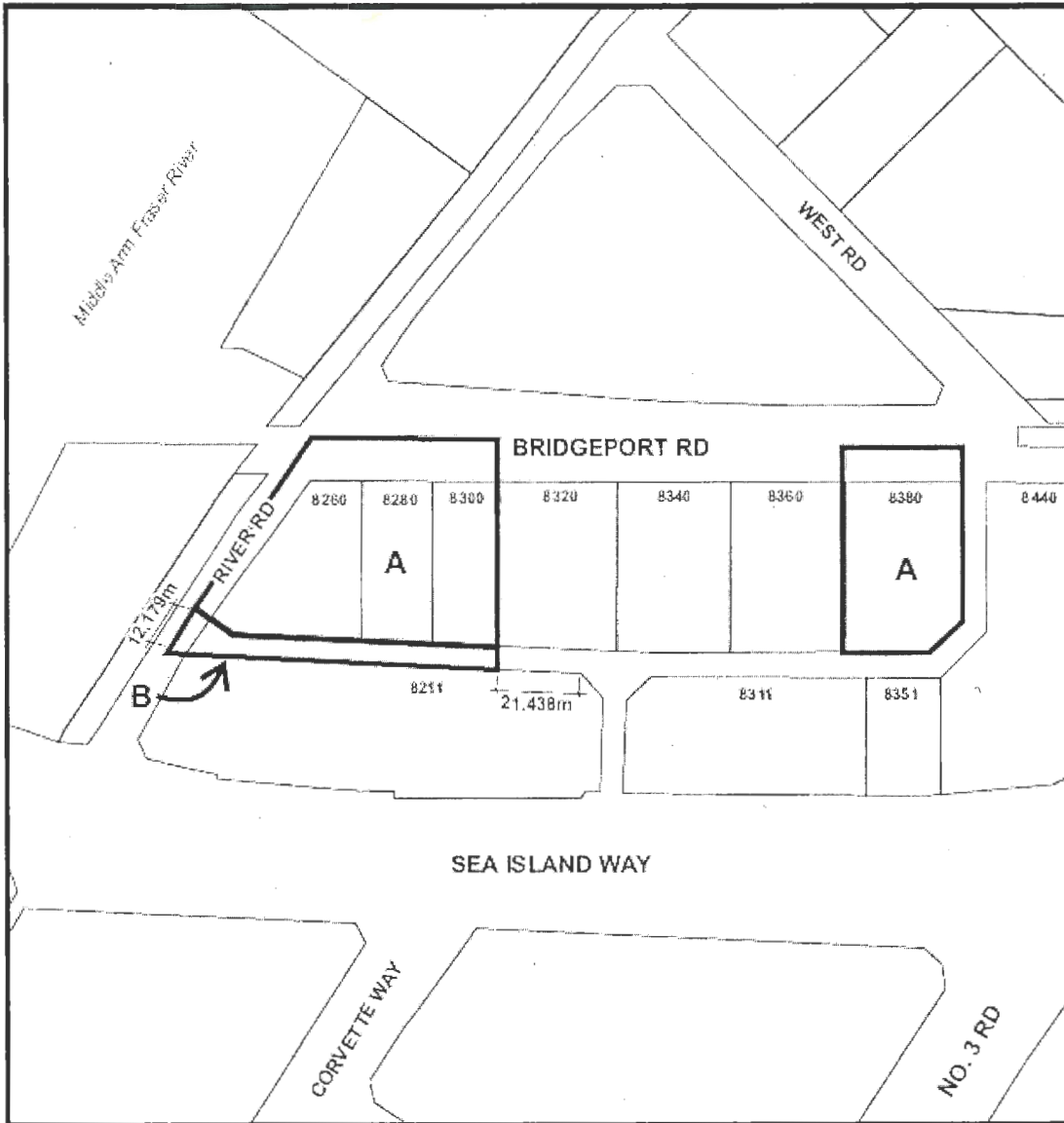
	LUC 126	ZC43	CA
FAR (max)	N/A	0.35	0.50
Lot Coverage (max)	N/A	35%	50%
Front Yard Setback (min)	As per drawings (7.5 m)	7.5 m	3.0 m
Interior Side Yard Setback (min)	As per drawings	There is no minimum interior side yard, except that the minimum east side yard for 8380 Bridgeport Road is 3.0 m.	3.0 m
Exterior Side Setback (min)	As per drawings (7.5 m)	7.5 m	3.0 m

LAND USE CONTRACT SUMMARY & COMPARISON TABLES

	LUC 126	ZC43	CA
Rear Setback (min)	As per drawings (3.0 m)	3.0 m	3.0 m
Building Height (max)	11.0 m, but containing no more than 3 storeys	11.0 m but containing no more than 3 storeys	12.0 m

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LAND USE CONTRACT SUMMARY & COMPARISON TABLES

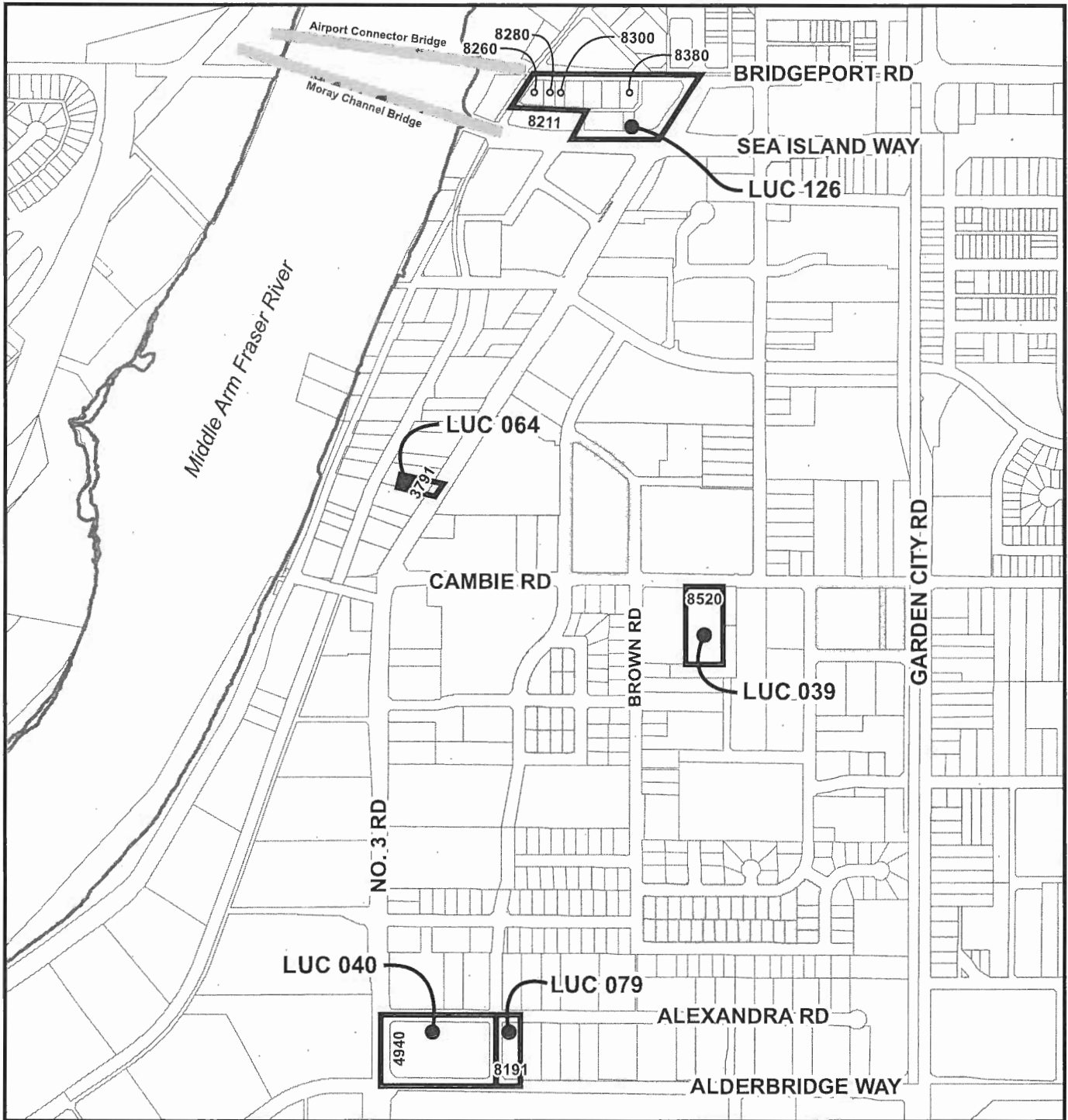


Land Use Contract 126

CNCL - 260



City of Richmond



Land Use Contracts (LUC) in City Centre (North)

Original Date: 08/24/17

Revision Date: 00/00/00

Note: Dimensions are in METRES

GNCL - 261



**Richmond Zoning Bylaw 8500, Amendment Bylaw 9744
to Establish Zoning for the Property Developed
under Land Use Contract 039**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by inserting the following into Section 23 (Site Specific Industrial Zones), in numerical order:

“23.13 Commercial Storage (Z113) – Cambie Road (City Centre)

23.13.1 Purpose

The **zone** provides for **commercial storage** and a secondary **residential security/operator unit**. This **zone** is for the property developed under Land Use Contract 039.

23.13.2 Permitted Uses

- **commercial storage**

23.13.3 Secondary Uses

- **residential security/operator unit**

23.13.4 Permitted Density

1. The maximum number of **commercial storage buildings** is three.
2. The maximum number of **residential security/operator units** is one.
3. The maximum **floor area** permitted is 3,800 m².
4. The maximum **floor area ratio** is 0.48.

23.13.5 Permitted Lot Coverage

1. The maximum **lot coverage** is 48% for **buildings**.

23.13.6 Yards & Setbacks

1. For a **building** containing **commercial storage**:
 - a) the minimum **front yard** and **rear yard** is 7.0 m.
 - b) the minimum **interior side yard** is 6.0 m.

2. For a **building** containing a **residential security/operator unit**:

- a) the minimum **front yard** is 10.0 m.
- b) the minimum **interior side yard** is 3.0 m.
- c) the minimum **rear yard** is 95.0 m.

23.13.7 Permitted Heights

- 1. The maximum **height** for **buildings** is 5.0 m, but containing no more than 1 **storey**.
- 2. The maximum **height** for **accessory structures** is 9.0 m.

23.13.8 Subdivision Provisions/Minimum Lot Size

- 1. The minimum **lot area** is 8,100 m².
- 2. The minimum **lot width** is 64.0 m.
- 3. The minimum **lot depth** is 125.0 m

23.13.9 Landscaping & Screening

- 1. **Landscaping** and **screening** shall be provided in accordance with the provisions of Section 6.0.

23.13.10 On-Site Parking and Loading

- 1. On-site **vehicle** and bicycle parking and loading shall be provided according to the standards set out in Section 7.0.

23.13.11 Other Regulations

- 1. In addition to the regulations listed above, the General Development Regulations of Section 4.0 and the Specific Use Regulations of Section 5.0 apply.”

- 2. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, as amended, is further amended by designating that portion outlined in bold and shown on “Schedule A attached to and forming part of Bylaw 9744” as **“COMMERCIAL STORAGE (ZI13) – CAMBIE ROAD (CITY CENTRE)”**.

3. This Bylaw may be cited as “**Richmond Zoning Bylaw 8500, Amendment Bylaw 9744**”.

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

MINISTRY OF TRANSPORTATION AND
INFRASTRUCTURE APPROVAL

ADOPTED



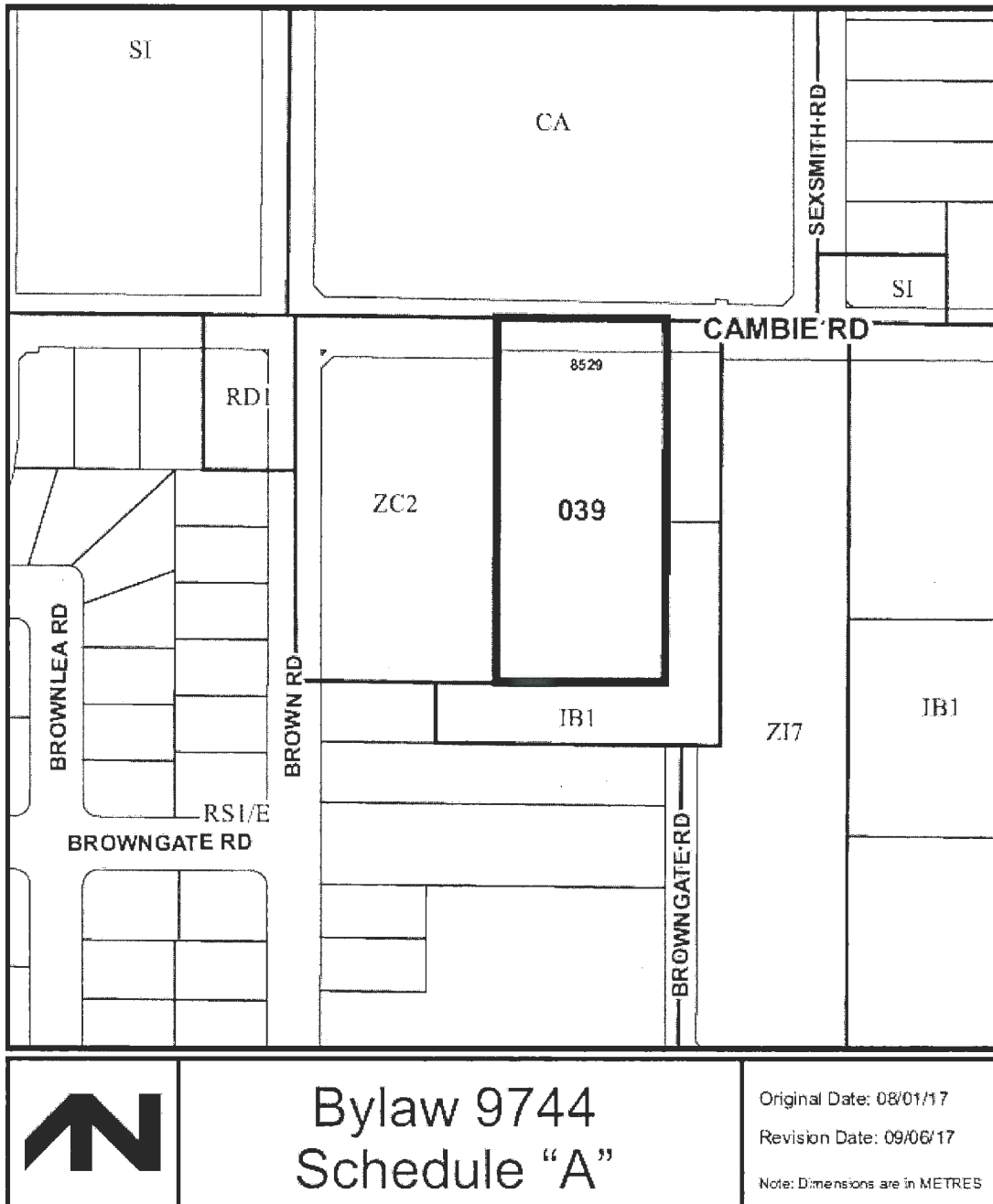
MAYOR

CORPORATE OFFICER

Schedule A attached to and forming part of Bylaw 9744



City of
Richmond





**Richmond Zoning Bylaw 8500, Amendment Bylaw 9745
to Establish Zoning for the Property Developed under
Land Use Contract 040**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by inserting the following into Section 22 (Site Specific Commercial Zones), in numerical order:

“22.40 Auto-Oriented Commercial (ZC40) – No. 3 Road (City Centre)”

22.40.1 Purpose

The **zone** provides for a mix of commercial and related **uses** oriented to vehicular access. This **zone** is for the property developed under Land Use Contact 040.

22.40.2 Permitted Uses

- contractor service
- education commercial
- entertainment, spectator
- government service
- health service, minor
- hotel
- manufacturing, custom indoor
- office
- private club
- recreation, indoor
- restaurant
- retail, convenience
- retail, general
- service, business support
- service, financial
- service, household repair
- service, personal
- transportation depot
- veterinary service

22.40.3 Secondary Uses

- n/a

22.40.4 Permitted Density

1. The maximum **floor area** permitted on the **lot** is 8,766 m².
2. The maximum **floor area** permitted on the first **storey** is 4,190 m².
3. The maximum **floor area** permitted on the second and third **storeys** combined is 4,576 m².
4. The maximum **floor area ratio** is 0.60.

22.40.5 Permitted Lot Coverage

1. The maximum **lot coverage** is 30% for **buildings**.

22.40.6 Yards & Setbacks

1. The minimum **setback** to the **lot line abutting** No. 3 Road and Alderbridge Way is 19.0 m.
2. The minimum **setback** to the **lot line abutting** Alexandra Road is 3.0 m.
3. The minimum **setback** to the **lot line abutting** Hazelbridge Way is 17.0 m.

22.40.7 Permitted Heights

1. The maximum **height** for **buildings** is 17.0 m, but containing no more than 3 **storeys**.

22.40.8 Subdivision Provisions/Minimum Lot Size

1. The minimum **lot area** is 14,000 m².
2. The minimum **lot width** is 70.0 m.
3. The minimum **lot depth** is 145.0 m.

22.40.9 Landscaping & Screening

1. **Landscaping** and **screening** shall be provided in accordance with the provisions of Section 6.0.

22.40.10 On-Site Parking and Loading

1. On-site **vehicle** and bicycle parking and loading shall be provided according to the standards set out in Section 7.0, except that the basic on-site parking requirement shall be a minimum of 210 **vehicle parking spaces** and a minimum of 5 on-site **loading spaces**.

22.40.11 Other Regulations

1. The following **permitted uses** shall be located on the first **storey** only:
 - a) **contractor service**
 - b) **education commercial**
 - c) **entertainment, spectator**
 - d) **government service**
 - e) **health service, minor**
 - f) **hotel**
 - g) **manufacturing, custom indoor**
 - h) **office**
 - i) **private club**
 - j) **recreation, indoor**
 - k) **restaurant**
 - l) **retail, convenience**
 - m) **retail, general**
 - n) **service, business support**
 - o) **service, financial**
 - p) **service, household repair**
 - q) **service, personal**
 - r) **transportation depot**
 - s) **veterinary service**
 2. The following **permitted use** shall be located on the second and third **storeys** only:
 - a) **office**
 3. In addition to the regulations listed above, the General Development Regulations of Section 4.0 and the Specific Use Regulations of Section 5.0 apply. ”
-
2. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, as amended, is further amended by designating that portion outlined in bold and shown on “Schedule A attached to and forming part of Bylaw 9745” as “**AUTO-ORIENTED COMMERCIAL (ZC40) – NO. 3 ROAD (CITY CENTRE)**”.

3. This Bylaw may be cited as “**Richmond Zoning Bylaw 8500, Amendment Bylaw 9745**”.

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

ADOPTED

CITY OF RICHMOND
APPROVED by 
APPROVED by Director or Solicitor 

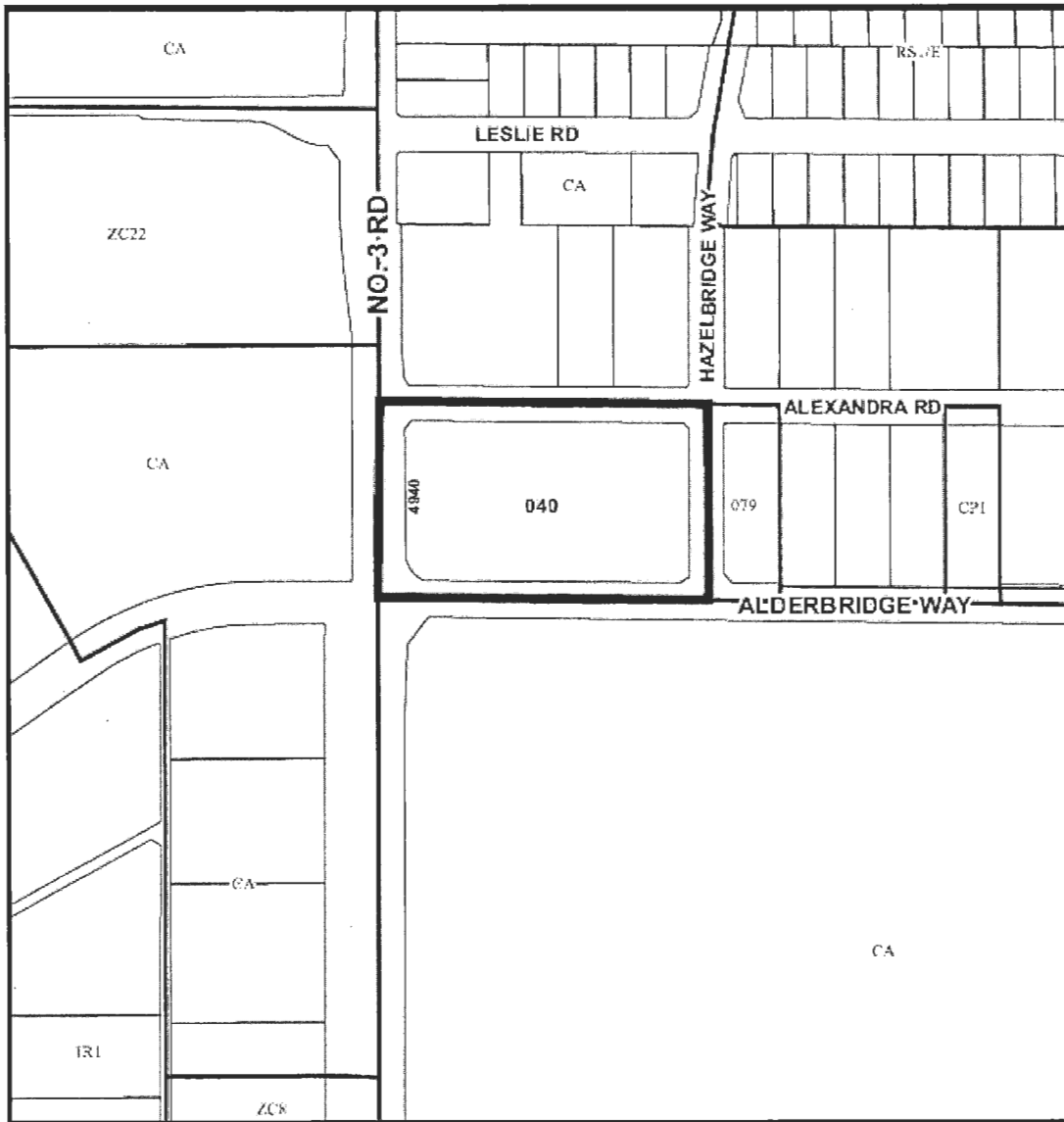
MAYOR

CORPORATE OFFICER

Schedule A attached to and forming part of Bylaw 9745



City of
Richmond



Bylaw 9745
Schedule "A"

Original Date: 08/01/17

Revision Date: 09/06/17

Note: Dimensions are in METRES



**Richmond Zoning Bylaw 8500, Amendment Bylaw 9746
to Establish Zoning for the Property Developed
under Land Use Contract 064**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by inserting the following into Section 22 (Site Specific Commercial Zones), in numerical order:

“ 22.41 Vehicle Sales Commercial (ZC41) – No. 3 Road (City Centre)

- 22.41.1 Purpose**
- The **zone** provides for **vehicle sale/rental**. This **zone** is for the property developed under Land Use Contract 064.
- 22.41.2 Permitted Uses**
- **Vehicle sale/rental**
- 22.41.3 Secondary Uses**
- **n/a**
- 22.41.4 Permitted Density**
1. The maximum **floor area ratio** is 2.3.
- 22.41.5 Permitted Lot Coverage**
1. There is no maximum **lot coverage** for **buildings**.
- 22.41.6 Yards & Setbacks**
1. The minimum **front yard** is 7.6 m.
 2. The minimum **setback** to one **interior side lot line** is 3.0 m.
 3. There is no minimum **rear yard**.
 4. Notwithstanding Section 22.41.6.2, the minimum **interior side yard** on a lot that is adjacent to **single detached housing, agriculture, and two-unit housing zones** shall be:
 - a) 3.0 m for a 1 **storey building**;
 - b) 7.5 m for a **building** containing more than 1 **storey**.

22.41.7 Permitted Heights

- 1. The maximum **height** for **buildings** is 10.7 m, but containing no more than **3 storeys**.
- 2. The maximum **height** for **accessory structures** is 9.0 m.

22.41.8 Subdivision Provisions/Minimum Lot Size

- 1. The minimum **lot width** is 15.2 m.
- 2. There is no minimum **lot depth** requirement.
- 3. The minimum **lot area** is 1,000 m².

22.41.9 Landscaping & Screening

- 1. **Landscaping** and **screening** shall be provided in accordance with the provisions of Section 6.0.

22.41.10 On-Site Parking and Loading

- 1. On-site **vehicle** and bicycle parking and loading shall be provided according to the standards set out in Section 7.0.

22.41.11 Other Regulations



- 1. In addition to the regulations listed above, the General Development Regulations of Section 4.0 and the Specific Use Regulations of Section 5.0 apply. ”
- 2. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, as amended, is further amended by designating that portion outlined in bold and shown on “Schedule A attached to and forming part of Bylaw 9746” as “**Vehicle Sales Commercial (ZC41) – No. 3 Road (City Centre)**”.
- 3. This Bylaw may be cited as “**Richmond Zoning Bylaw 8500, Amendment Bylaw 9746**”.

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

_____	CITY OF RICHMOND
_____	APPROVED by
_____	
_____	APPROVED by Director or Solicitor
_____	

MINISTRY OF TRANSPORTATION AND
INFRASTRUCTURE APPROVAL

ADOPTED

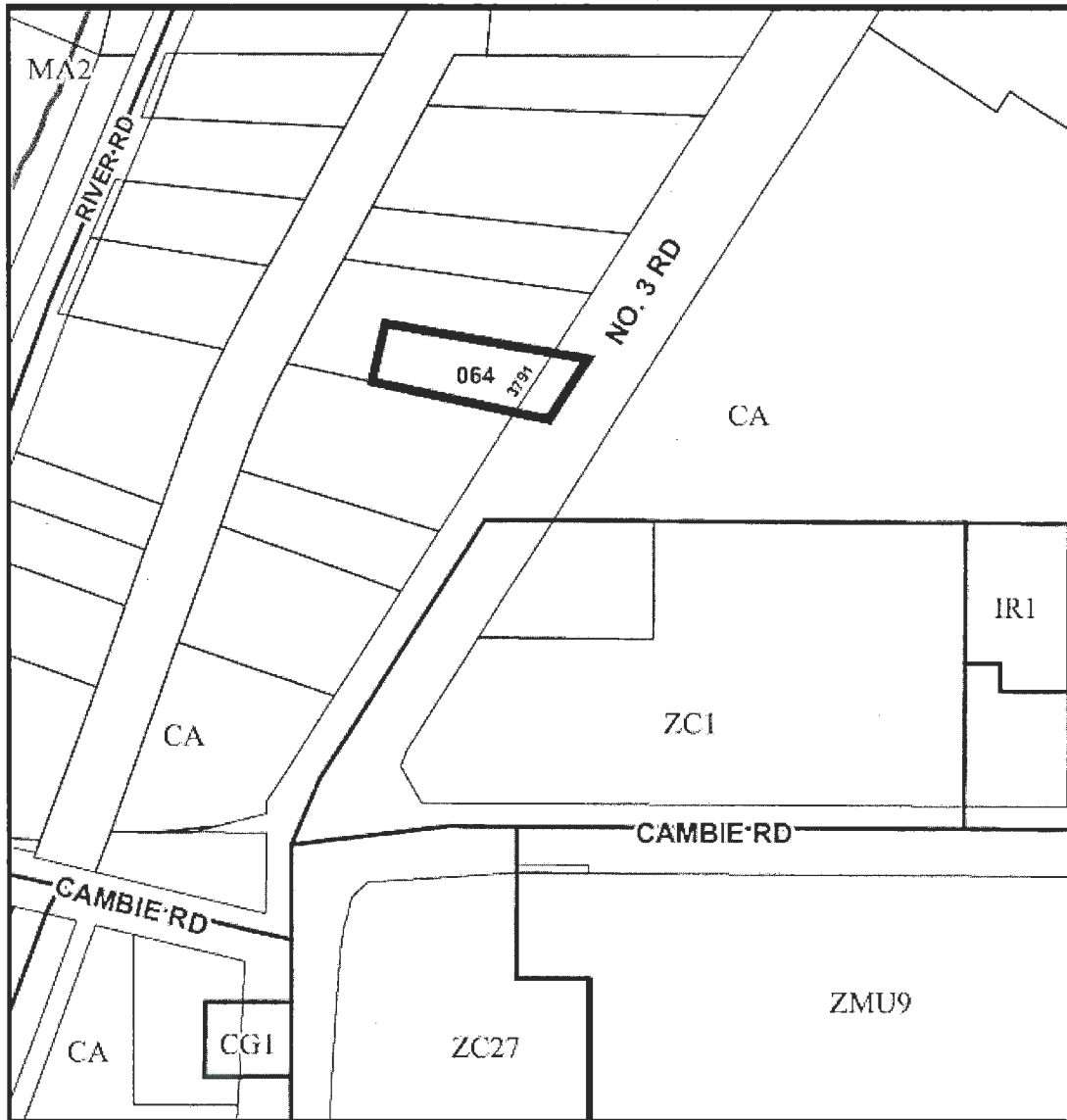
MAYOR

CORPORATE OFFICER

Schedule A attached to and forming part of Bylaw 9746.



City of
Richmond



	<p>Bylaw 9746 Schedule "A"</p>	<p>Original Date: 08/01/17 Revision Date: 09/06/17 Note: Dimensions are in METRES</p>
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**Richmond Zoning Bylaw 8500, Amendment Bylaw 9747
to Establish Zoning for the Property Developed
under Land Use Contract 079**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by inserting the following into Section 22 (Site Specific Commercial Zones), in numerical order:

22.42 Restaurant Commercial (ZC42) – Alderbridge Way (City Centre)

22.42.1 Purpose

The **zone** provides for a **restaurant** and a secondary **drive-through restaurant**. This **zone** is for the property developed under Land Use Contract 079.

22.42.2 Permitted Uses

- **restaurant**

22.42.3 Secondary Uses

- **restaurant, drive-through**

22.42.4 Permitted Density

1. The maximum **floor area** permitted is 800 m².
2. The maximum **floor area ratio** is 0.27.

22.42.5 Permitted Lot Coverage

1. The maximum **lot coverage** is 25% for **buildings**.

22.42.6 Yards & Setbacks

1. The minimum **setback** to the **lot line abutting** Alderbrige Way is 6.0 m.
2. The minimum **setback** to the **lot line abutting** Hazelbridge Way is 1.5 m.
3. The minimum **setback** to the **lot line abutting** Alexandra Road is 50.0 m.
2. There is no minimum **interior side yard** requirement.

22.42.7 Permitted Heights

1. The maximum **height** for **buildings** is 9.0 m. but containing no more than 2 **storeys**.
2. The maximum **height** for **accessory structures** is 9.0 m

22.42.8 Subdivision Provisions/Minimum Lot Size

1. The minimum **lot area** is 2,800 m².
2. The minimum **lot width** is 26.0 m.
3. The minimum **lot depth** is 86.0 m.

22.42.9 Landscaping & Screening

1. **Landscaping** and **screening** shall be provided in accordance with the provisions of Section 6.0.

22.42.10 On-Site Parking and Loading

1. On-site **vehicle** and bicycle parking and loading shall be provided according to the standards set out in Section 7.0, except that the basic on-site parking requirement shall be 42 **vehicle parking spaces** and one on-site **loading space**.

22.42.11 Other Regulations

1. The customer **floor area** of a **restaurant** shall be limited to a total of 222 m².
2. In addition to the regulations listed above, the General Development Regulations of Section 4.0 and the Specific Use Regulations of Section 5.0 apply. ”
2. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, as amended, is further amended by designating that portion outlined in bold and shown on “Schedule A attached to and forming part of Bylaw 9747” as **“RESTAURANT COMMERCIAL (ZC42) – ALDERBRIDGE WAY (CITY CENTRE)”**.
3. This Bylaw may be cited as **“Richmond Zoning Bylaw 8500, Amendment Bylaw 9747”**.

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

ADOPTED

CITY OF RICHMOND
APPROVED by 
APPROVED by Director or Solicitor 

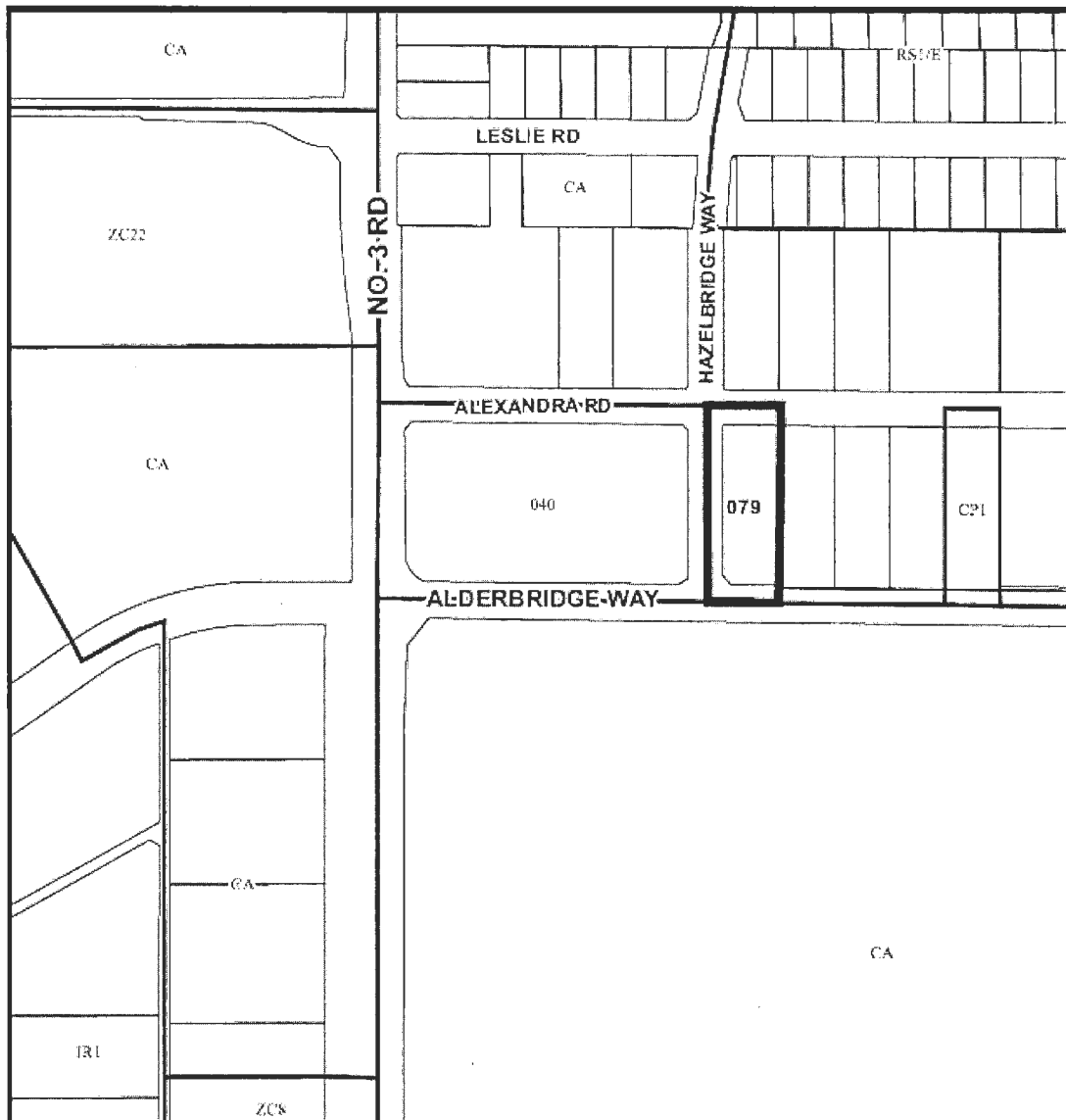
MAYOR

CORPORATE OFFICER

Schedule A attached to and forming part of Bylaw 9747



City of
Richmond



	<p>Bylaw 9747 Schedule "A"</p>	<p>Original Date: 08/01/17 Revision Date: 09/06/17 Note: Dimensions are in METRES</p>
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**Richmond Zoning Bylaw 8500, Amendment Bylaw 9748
to Establish Zoning for the Properties Developed
under Land Use Contract 126**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by inserting the following into Section 22 (Site Specific Commercial Zones), in numerical order:

“22.43 Commercial (ZC43) – Bridgeport Road (City Centre)

22.43.1 Purpose

The **zone** provides for commercial **uses**. This **zone** is for the properties developed under Land Use Contract 126.

22.43.2 Permitted Uses

- contractor service
- entertainment, spectator
- equipment, minor
- manufacturing, custom indoor
- office
- recreation, indoor
- restaurant
- retail, general
- service, business support
- service, household repair

22.43.3 A. Secondary Uses

- n/a

22.43.3 B. Additional Uses

- commercial vehicle parking and storage
- fleet service
- parking, non-accessory
- vehicle rental, convenience

22.43.4 Permitted Density

1. The maximum **floor area ratio** is 0.35, except that a **lot** with a **lot area** of less than 450 m² shall not be used as the site of a **building**.

22.43.5 Permitted Lot Coverage

1. The maximum **lot coverage** is 35% for **buildings**.

22.43.6 Yards & Setbacks

1. The minimum **front yard** is 7.5 m.
2. There is no minimum **interior side yard**, except that for the following listed site, the minimum eastern **interior side yard** is 3.0 m:
 - a) 8380 Bridgeport Road
P.I.D. 001-209-744
Lot 82 Section 28 Block 5 North Range 6 West New Westminster
District Plan 56425.
3. The minimum **exterior side yard** is 7.5 m
4. The minimum **rear yard** is 3.0 m.

22.43.7 Permitted Heights

1. The maximum **height** for **buildings** is 11.0 m, but containing no more than **3 storeys**.
2. The maximum **height** for **accessory structures** is 9.0 m.

22.43.8 Subdivision Provisions/Minimum Lot Size

1. The minimum **lot area** is 695 m².
2. The minimum **lot width** is 15.0 m
3. There is no minimum **lot depth** requirement.

22.43.9 Landscaping & Screening

1. **Landscaping** and **screening** shall be provided in accordance with the provisions of Section 6.0.

22.43.10 On-Site Parking and Loading

1. On-site **vehicle** and bicycle parking and loading shall be provided according to the standards set out in Section 7.0.

22.43.11 Other Regulations

1. In addition to the regulations listed above, the General Development Regulations of Section 4.0 and the Specific Use Regulations of Section 5.0 apply.

2. **Commercial vehicle parking and storage, fleet service, and parking, non-accessory** is only permitted on the following listed sites:

- a) 8280 Bridgeport Road
P.I.D. 004-274-059
Lot B Section 28 Block 5 North Range 6 West New Westminster District
Plan 71920
- b) 8300 Bridgeport Road
P.I.D. 024-947-954
Lot 1 Section 28 Block 5 North Range 6 West New Westminster District
Plan LMP48700

3. **Vehicle rental, convenience** is only permitted on the following listed sites:

- a) 8300 Bridgeport Road
P.I.D. 024-947-954
Lot 1 Section 28 Block 5 North Range 6 West New Westminster District
Plan LMP48700 ”

2. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, as amended, is further amended by designating that portion outlined in bold and shown as Area “A” on “Schedule A attached to and forming part of Bylaw 9748” as **“Commercial (ZC43) – Bridgeport Road (City Centre)”**.

3. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, as amended, is further amended by designating that portion outlined in bold and shown as Area “B” on “Schedule A attached to and forming part of Bylaw 9748” as **“Auto-Oriented Commercial (CA)”**.

4. This Bylaw may be cited as **“Richmond Zoning Bylaw 8500, Amendment Bylaw 9748”**.

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

MINISTRY OF TRANSPORTATION AND
INFRASTRUCTURE APPROVAL

ADOPTED



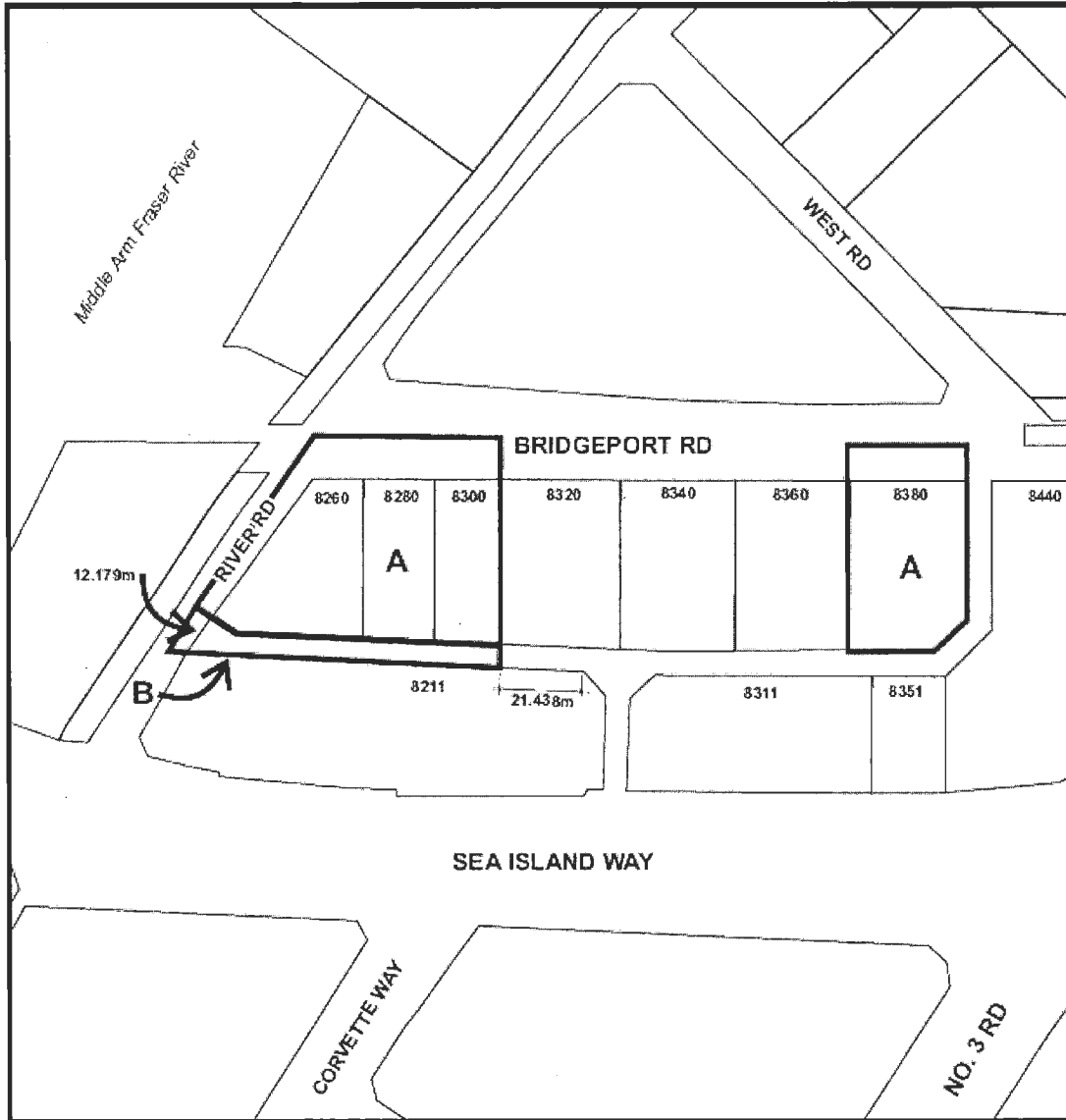
MAYOR

CORPORATE OFFICER

Schedule A attached to and forming part of Bylaw 9748



City of
Richmond



Bylaw 9748
SCHEDULE "A"

Original Date: 08/18/17

Revision Date: 09/06/17

Note: Dimensions are in METRES