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**To:** Planning Committee **Date:** September 10, 2018  
**From:** Wayne Craig **File:** CP 16-752923  
Director, Development  
**Re:** **Application by GBL Architects for an Official Community Plan (City Centre Area Plan) Amendment at 6551 No. 3 Road (CF Richmond Centre South)**

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### Staff Recommendation

1. That Official Community Plan Amendment Bylaw 9892, including:

- a) In Schedule 1 of Official Community Plan Bylaw 9000, to redesignate a portion of 6551 No. 3 Road from “Downtown Mixed Use” to “Park” in Attachment 1; and
- b) In Schedule 2.10 (City Centre Area Plan) of Official Community Plan 7100, to:
  - i. amend the existing land use designation in the Generalized Land Use Map (2031), Specific Land Use Plan: Brighthouse Village (2031), and reference maps throughout the Plan to change the locations of roads, park, pedestrian-oriented retail precincts, pedestrian linkages, greenways, bike routes, and related features specific to 6551 No. 3 Road;
  - ii. add a new policy encouraging multi-modal mobility hubs in the City Centre;
  - iii. add new Development Permit Guidelines specific to 6551 No. 3 Road; and
  - iv. make related minor map, text, page numbering, and table of contents amendments to the City Centre Area Plan;

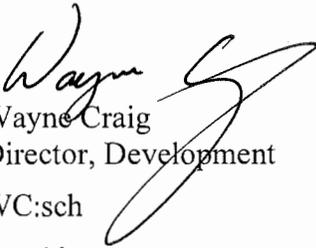
be introduced and given first reading.

2. That Bylaw 9892, having been considered in conjunction with:

- the City’s Financial Plan and Capital Program;
- the Greater Vancouver Regional District Solid Waste and Liquid Waste Management Plans;

is hereby found to be consistent with said program and plans, in accordance with Section 882(3)(a) of the *Local Government Act*.

- 3. That Bylaw 9892, having been considered in accordance with OCP Bylaw Preparation Consultation Policy 5043, is hereby found not to require further consultation.

  
Wayne Craig  
Director, Development  
WC:sch  
Att. 11

REPORT CONCURRENCE		
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER
Policy Planning	<input checked="" type="checkbox"/>	
Affordable Housing	<input checked="" type="checkbox"/>	
Engineering	<input checked="" type="checkbox"/>	
Parks	<input checked="" type="checkbox"/>	
Transportation	<input checked="" type="checkbox"/>	

## Staff Report

### Origin

GBL Architects has applied to the City of Richmond to amend the Official Community Plan (OCP), Schedule 2.10 (City Centre Area Plan) at 6551 No. 3 Road to permit a high-rise, mixed use project on roughly 50% of 6551 No. 3 Road at the south end of the CF Richmond Centre shopping centre. (Attachments 1 & 2)

The CF Richmond Centre shopping centre occupies three lots: the subject site at 6551 No. 3 Road; 6060 Minoru Boulevard (under separate ownership, but the same operator); and 6253 No. 3 Road (a City-owned lot under long-term lease to the shopping centre operator). The subject development is limited to roughly 60% of 6551 No. 3 Road. (Attachment 3) Neither 6253 No. 3 Road (City lot) nor 6088 Minoru Boulevard is proposed for redevelopment.

Key features of the subject development proposal include the:

- Demolition of 26,905 m<sup>2</sup> (289,601 ft<sup>2</sup>) of the existing mall, including 24,487 m<sup>2</sup> (263,571 ft<sup>2</sup>) of retail shops (e.g., former Sears), together with the demolition of the existing multi-storey parkade at the lot's south end and adjacent surface parking; and
- Construction of a high-rise, urban neighbourhood including approximately 2,000 dwellings, new public streets and outdoor spaces, parking for 4,000 cars (including two levels of underground parking), and 38,937 m<sup>2</sup> (419,114 ft<sup>2</sup>) of new commercial space, comprising 35,197 m<sup>2</sup> (378,861 ft<sup>2</sup>) of retail shops, which represents a net commercial increase of 12,032 m<sup>2</sup> (129,513 ft<sup>2</sup>).

To facilitate the subject development, the applicant proposes to amend Schedule 2.10 (City Centre Area Plan) of Official Community Plan 7100 to permit changes to various mobility features (e.g., roads, bike routes, and connectivity enhancements), public open space features (e.g., new plaza), and form and character features.

Engineering, road, public open space, District Energy Utility (DEU), affordable housing, public art, and related community amenities and City infrastructure required with respect to the subject development are proposed to be secured by legal agreements prior to OCP Amendment bylaw adoption and delivered on a phase-by-phase basis through the City's standard Servicing Agreement, Development Permit, and Building Permit processes.

### Findings of Fact

A Development Application Data Sheet providing details about the development proposal is attached. (Attachment 4)

### No Rezoning Required

In the late 1980s, the City rezoned CF Richmond Centre and nearby properties to "Downtown Commercial (C7)", later renamed "Downtown Commercial (CDT1)", to encourage densification of Richmond's downtown core. The CDT1 zone permits high-rise, mixed use development to a maximum height of 45 m (148 ft.) and 3.0 floor area ratio (FAR), together with bonus density for the provision of affordable housing. The subject development complies with the CDT1 zone's

maximum permitted height and the combined total floor area of proposed and existing mixed use development on 6551 No. 3 Road equals approximately 70% of the zone’s maximum permitted density on the lot. (Under the CDT1, the owner would be permitted to undertake additional development in the future to utilize the remaining 30% of the zone’s permitted floor area. In addition, the City Centre Area Plan designates the subject site for a maximum density of 4.0 floor area ratio, so the owner may give future consideration to rezoning.)

	Height (Max)	FAR (Max)	Buildable Floor Area
CDT1 Zone	45 m (148 ft.)	3.15 FAR*	339,106 m <sup>2</sup> (3.65 million ft <sup>2</sup> ) max. permitted
Proposed	45 m (148 ft.)	2.1 FAR**	232,258 m <sup>2</sup> (2.5 million ft <sup>2</sup> ) = +/-70% of max. permitted

\* Includes 0.15 FAR Affordable Housing bonus (5% Affordable Housing) applicable to applications, like the subject application, that were received prior to July 24, 2017 and considered by Council prior to July 24, 2018. (The subject application was first considered by Council on April 9, 2018.)

\*\* The subject development proposal includes 5% Affordable Housing.

Developments that comply with existing zoning typically proceed directly to Development Permit (DP) review. When that occurs, the City’s ability to secure community amenities is limited because Council does not have the discretionary power of a rezoning application. However, as the subject developer has made application to amend Schedule 2.10 (City Centre Area Plan) of Official Community Plan 7100, staff have worked with the developer to address community objectives.

Subject Site Existing Housing Profile

There are no dwelling units on 6551 No. 3 Road or the City-owned lot at 6253 No. 3 Road. The shopping centre’s north lot (6088 Minoru Boulevard) includes the “Horizons”, a twin-tower, 16-storey, 29,000 m<sup>2</sup> (312,000 ft<sup>2</sup>), residential development constructed in the late 1990s and comprising 248 dwellings. No changes are proposed to this existing residential use.

**Surrounding Development**

6551 No. 3 Road is a roughly 11 ha (28 ac) lot, located in the middle of the City Centre’s Brighthouse Village area, and occupied by the south part of CF Richmond Centre, a low-rise, low density, automobile-oriented shopping centre and associated parking. Existing development surrounding 6551 No. 3 Road includes the following:

To the North: CF Richmond Centre’s north portion, including “Horizons” residential towers;

To the South: Richmond City Hall and annex;

To the West: Minoru Boulevard, beyond which is a mix of low- and high-rise residential buildings, the Richmond Cultural Centre, and Minoru Park; and

To the East: No. 3 Road, beyond which is a mix of older, low-rise, auto-oriented commercial buildings, existing mixed use and residential towers, the Canada Line’s terminus station (Richmond-Brighthouse) and proposed bus mall, and several recent development applications, including:

- 6390 No. 3 Road (RZ 17-773703 / DP 18-822743 / Keltic) comprising three residential towers, one office tower, an Early Childhood Development Hub, and retail uses at grade (pending rezoning adoption);
- 6840 and 6860 No. 3 Road and 8061 Anderson Road (RZ 14-678448 / DP 15-708092 / iFortune) comprising a mid-rise residential building, one office tower, and retail uses at grade (pending rezoning adoption); and
- 6560, 6600, 6640 & 6700 No 3 Road (RZ 15-694855 / DP 16-754761 / Bene Richmond) comprising a mixed residential-office tower and retail uses at grade (pending rezoning adoption).

## **Related Policies & Studies**

### **Official Community Plan/City Centre Area Plan**

#### **City Centre Area Plan (CCAP)**

The subject site is located in the middle of Brighthouse Village (Attachment 5). The CCAP identifies this area as the traditional heart and civic focus of Richmond's downtown and supports its revitalization with a high density, high-rise, mix of commercial, residential, and civic uses centred on No. 3 Road and the Brighthouse Station transit exchange. More specifically:

- 1) The subject site and properties to its north, south (e.g., City Hall), and east (beyond No. 3 Road) generally share the same high density, high-rise, mixed use designations, including:
  - "Urban Core T6 (45 m)", which permits mixed use or commercial development to a maximum of 3.0 FAR and 45 m (148 ft.) in height;
  - "Village Centre Bonus", which permits additional commercial floor area to a maximum of 1.0 FAR; and
  - "Pedestrian-Oriented Retail Precinct – High-Street & Linkages", which encourages a high concentration of pedestrian-oriented retail, restaurant, and complementary activities at grade along No. 3 Road and other public streets and open spaces; and
- 2) West of the subject site, near Minoru Park, the CCAP encourages medium and high density, mid- and high-rise residential uses, generally designated as:
  - "Urban Centre T5 (25 m)", which permits residential and other uses to a maximum of 2.0 FAR and 25 m (82 ft.) in height; and
  - "Institution", which permits bonus density and increased height for developments that provide community amenity-type uses (e.g., Kiwanis Senior Citizens Housing Society); and
- 3) Mobility improvements are encouraged on and around 6551 No. 3 Road, including:
  - The extension of Park Road westward from No. 3 Road to Minoru Boulevard;
  - A new bike lane along No. 3 Road; and
  - Greenway improvements along No. 3 Road and Minoru Boulevard.

### Floodplain Management Implementation Strategy

The proposed redevelopment must meet the requirements of the Richmond Flood Plain Designation and Protection Bylaw 8204. Registration of a flood indemnity covenant on Title is required prior to final adoption of the rezoning bylaw.

### **Public Consultation**

It is Council policy (OCP Bylaw Preparation Consultation Policy 5043) that staff consider consultation with persons, organizations, and authorities that may be affected by the enactment, repeal, or amendment of the Official Community Plan bylaw where the other parties' land use, programming, servicing, transportation, and/or environmental interests may be impacted.

### Community Consultation

- 1) Council-Supported Developer-Led Consultation Process: On April 9, 2018, Council endorsed a developer-led community consultation process regarding the CF Richmond Centre South Development Plan and proposed changes to the CCAP. The process included:
  - **Public Display**: A public display was set up in the shopping centre's galleria from May 22 to June 3, 2018. The Community Consultation – Public Display Boards are attached. (Attachment 6)
  - **Open Houses**: Developer and City representatives attended the public display to answer questions on May 27 (1 – 4 p.m.) and May 31 (5 – 8 p.m.).
  - **Online**: The public display boards were available to view on the City's website and at LetsTalkRichmond.ca from May 22 to June 3, 2018.
  - **Feedback Forms**: Interested parties were able to complete a feedback form by:
    - a) Logging onto LetsTalkRichmond.ca; or
    - b) Completing a paper form (available at the open houses, or on request).
  - **Advertising**: Advertisements included:
    - a) Print ads in the Richmond News and Richmond Sentinel;
    - b) City social media postings on Facebook and Twitter; and
    - c) News releases sent to local media and posted on the City's website.
  - **Direct Mail-Outs**: Direct mail-outs (3,000 letters) were sent to tenants and owners of properties within 100 m (328 ft.) of the subject site to notify them of the public display, open houses, and opportunity to provide feedback.
- 2) Open House Attendance: Each of the two public open houses was attended by approximately 300 people (i.e. 600 in total).
- 3) Community Feedback: The feedback form included ten questions regarding the developer's proposed CCAP amendments and related voluntary developer contributions. The questions were included in the Public Display Boards, together with supportive diagrams and photographs, in the form of "Objective #1" to "Objective #10" (on the last ten pages of Attachment 6).

The City received a total of 164 feedback forms through LetsTalkRichmond.ca, by mail, and in person. Respondents primarily identified themselves as Richmond residents and CF Richmond Centre shoppers. A summary of the feedback form results is attached. (Attachment 7) In brief, the feedback indicated that:

- 65% liked the proposed street network;
- 81% liked the proposed improvements to Canada Line access, including public access through the mall's galleria outside regular business hours;
- 75% liked the proposed streetscape improvements for pedestrians and cyclists;
- 60% liked the proposed underground parking and features aimed at enhancing access by shoppers and the general public;
- 68% like the proposed outdoor retail precinct;
- 71% liked the proposed public plaza and other public open space features;
- 51% liked the proposed form of development;
- 64% liked the proposed affordable housing (i.e. 5% of total residential floor area in the form of 150 low-end-of-market-rental (LEMR) units);
- 66% liked the proposed housing mix (i.e. 50% 2- and 3-bedroom, family-friendly units and 25% Basic Universal Housing (BUH) units for people with mobility challenges); and
- 66% liked the proposed use of a low-carbon heating/cooling system to help reduce greenhouse gas.

Respondents who did not like the proposed changes at CF Richmond Centre primarily expressed concern regarding:

- **Growth:** Too much growth in the Richmond's downtown is changing the area's character and placing pressure on existing infrastructure, transit, schools, and other services.  
  
*Staff comments: Richmond, like most of Metro Vancouver, is growing. Since 1995, the City's OCP has aimed to direct 50% of Richmond's growth to the City Centre to support the establishment of a high-amenity urban core, protect farmland, and reduce development pressure on stable residential neighbourhoods outside the downtown. This OCP direction has been adopted in consultation with the Richmond School District, Vancouver Coastal Health, TransLink, and other stakeholders. In addition, through the City's capital plan and developer-funded contributions, the City seeks to ensure that the implementation of amenity and infrastructure improvements is coordinated with growth and minimizes taxpayer impacts. (Note that the subject development does not propose any increase in permitted density or change in permitted land uses.)*
- **Shopping:** Shopping centre redevelopment could undermine the downtown as Richmond's central shopping district, displace small businesses, force

people to shop outdoors (without weather protection), and result in expensive and/or empty street-facing storefronts.

**Staff comments:** *The subject developer is the owner of 6551 No. 3 Road and is committed to the long-term commercial viability of CF Richmond Centre and Richmond's downtown. The proposed development will increase the shopping centre's existing commercial floor area by 12,032 m<sup>2</sup> (129,513 ft<sup>2</sup>), most of which will be located in street-fronting retail space along No. 3 Road and the extension of Park Road. This approach will enable the developer to maintain the existing indoor mall, while better connecting it with the Canada Line, providing a broader range of shopping options (which may include grocery and other local-serving retailers), and contributing towards a more walkable downtown.*

- **Transportation:** Traffic congestion. Not enough parking. Overcrowding on the Canada Line. Impracticality of cycling in our climate.

**Staff comments:** *The CCAP supports the establishment of a well-connected downtown community that provides for an expanding range of sustainable travel options with an emphasis on walking, cycling, and transit. This is consistent with TransLink's 10-Year Vision, which includes, among other things, the acquisition of 24 new Canada Line cars and increased Canada Line frequency during rush hours, evenings, and weekends. The proposed CF Richmond Centre development complies with City parking bylaws and includes features that are supportive of the City's mobility objectives including:*

- a) *Secure public access through the mall's galleria outside regular business hours to be consistent with the Canada Line's operating hours to improve access to the Canada Line and future bus mall;*
- b) *Smaller blocks and a more connected and attractive network of pedestrian sidewalks and off-street bike paths;*
- c) *Wider sidewalks and crosswalk upgrades, including enhanced pedestrian access between the subject site and the Canada Line station; and*
- d) *Two multi-modal mobility hubs (one in each phase of development) that will be designed and operated to seamlessly integrate multiple travel modes, supportive infrastructure, and placemaking strategies with the aim of creating two pedestrian-oriented centres that will help maximize first-to-last kilometre connectivity. Each mobility hub will be comprised of an integrated suite of pedestrian-friendly, transportation and related features such as bike- and car-share facilities, taxi and ride-hailing services, secure bike storage for the public and repair services, electric vehicle charging stations, and weather protected public transit stops all conveniently located near shops, services, and public amenities. (Legal agreements, securing the developer's commitment to the construction and operation of the mobility hubs, at the developer's*

*sole cost, will be registered on title prior to OCP amendment adoption.)*

- **Housing:** The amount of affordable housing (too much or too little), whether the proposed units will really be affordable or family-friendly, and the amount of accessible Basic Universal Housing units were questioned.

**Staff comments:** *Access to livable, appealing, and varied housing options that meet the needs of the City Centre's changing downtown population is a core value of the CCAP. In recognition of this, while the City's ability to require developer-funded community amenities from pre-zoned properties (like the subject site) is limited, staff have worked with the developer to achieve key City housing objectives, including the developer's construction (at the developer's sole cost) of:*

  - a) *150 low-end-of-market-rental (LEMR) units secured with a Housing Agreement registered on title prior to OCP bylaw adoption;*
  - b) *Family-friendly residential buildings designed to include 50% 2- and 3-bedroom units (including 50% of the LEMR units), large outdoor amenity spaces equipped with children's playgrounds (on the podium rooftops), and various indoor family-friendly amenities (e.g., party rooms, swimming pools, multi-purpose recreation rooms); and*
  - c) *Accessible residential buildings containing universally accessible lobbies, circulation, and indoor/outdoor amenity spaces throughout, together with accessible unit features (secured with legal agreements registered on title prior to OCP bylaw adoption) including:*
    - i. *25% of total units will be constructed to Basic Universal Housing (BUH) standards suitable for people with mobility challenges (including 100% of LEMR units); and*
    - ii. *100% of units will include aging-in-place features (e.g., lever handles and blocking in walls for grab bars).*
  
- **Built Form:** Too many high-rises in the City Centre. Soil conditions, especially with respect to underground parking.

**Staff comments:** *The developer's proposal is consistent with the CCAP, which encourages a variety of building types and housing options across the downtown ranging from high-rise, high density development in locations, like the subject site, that are within 400 m (1,312 ft.) of a Canada Line station and low-rise buildings in less accessible locations. In addition, the developer's proposal is consistent with CCAP policies that encourage developers of high density developments to include features (such as underground parking) that help to free up the ground plane for active public use. The design and construction of all buildings and underground parking structures in Richmond must comply with provincial and municipal*

*legislation (e.g., BC Building Code) to ensure that, among other things, they appropriately address local soil conditions. Based on engineering work undertaken by the developer's consultants, the developer has confirmed that construction standards and requirements related to local soil conditions and the project's proposed underground parking will be fully satisfied.*

- Construction: Noise, dirt, and other construction impacts on nearby residents.

**Staff comments:** *The developer will be required to submit a Construction Traffic Management Plan for City approval prior to Building Permit issuance. City bylaws limit the hours when noisy construction activities may be undertaken (i.e. 7 a.m. to 8 p.m. from Monday to Friday and shorter hours on Saturdays) and require that public streets and sidewalks are kept clean and accessible.*

- 4) Correspondence: At the time of writing this report five emails/letters have been received by the City regarding the subject OCP amendment application. (Attachment 8) In brief:
- Item #1: Supports the proposed public route through the mall's galleria to outside regular business hours, but expresses concern that the area is already too dense and unaffordable.
  - Item #2: Expresses concern that the area currently lacks a supermarket, may not have adequate elementary school capacity, and has only 2 electric vehicle charging stations.
  - Item #3 and #4: Seek information regarding the proposed energy plant, potential airspace parcel subdivision, and transit planning. (Staff responses are included in Attachment 6.)
  - Item #5: Supports the proposed development and suggests that the developer gives consideration to including a multi-purpose indoor stadium for sports and cultural events.

**Staff comments:** *For the most part, the concerns expressed in the correspondence mirrors those conveyed through the feedback forms and have been addressed above. Regarding the inquiry into a new sports and cultural events venue, it is City policy to support the continued use and enhancement of the Richmond Cultural Centre, Minoru Park, the Richmond Olympic Oval, and other City facilities for this purpose.*

#### Developer Consultation with Existing Shopping Centre Tenants

The shopping centre owner shared information about the proposed development and sought feedback from current retail tenants on several occasions, including:

- 1) Memo announcing the construction of the project's marketing Centre on January 29, 2018;
- 2) Town Hall style meeting (40-50 attendees) on February 20, 2018;
- 3) Drop-in session at the public display in the mall (4 attendees) on May 30, 2018; and
- 4) Various one-on-one meetings with tenants (focussing on those near the development).

Tenant feedback has been positive and there has been interest from some retailers in relocating to the new development. Concerns and questions have generally fallen into two categories:

- 1) Parking availability during construction.

- The development will be phased to ensure that adequate parking and vehicle access from both No. 3 Road and Minoru Boulevard are maintained throughout construction.
- 2) Relocation strategies for long-term retailers within the construction/demolition area.
- The former Sears building will be used to temporarily house retailers during Phase 1 until they can be relocated to units in the new development.

The developer is committed to working with retailers to ensure that the mall is pleasant and attractive for customers and well managed for tenants throughout the construction process.

Advisory Design Panel

The CF Richmond Centre South Development Plan was presented to the Advisory Design Panel on March 7, 2018 (Attachment 9). The Panel voted unanimously in support of the application and commended the applicant on the Plan’s features (e.g., affordable, family-friendly, and accessible housing, smaller blocks defined by new streets and pedestrian linkages, underground parking, and a more vibrant public realm). ADP recommended that, at Development Permit stage, the applicant undertakes design development with respect to detailed public realm design, sun/shade impacts, and architectural expression.

Richmond School District

According to OCP Bylaw Preparation Consultation Policy 5043, adopted by Council and agreed to by the School District, OCP amendment applications must be referred to the School District if they have the potential to generate 50 or more additional school-aged children (i.e. equivalent to 295 or more additional multiple-family housing units) over and above the existing OCP. As the subject application does not propose any increase in permitted residential units, it is not required to be referred to the School District. The subject OCP amendment application was presented at the Council/School Board Liaison Committee meeting on April 25, 2018. City staff will continue to keep School Board staff apprised of the development of the property.

TransLink

No referral is necessary because the subject OCP amendment application does not include streets identified as part of TransLink’s Major Road Network (MRN) or involve significant road network changes. The subject OCP amendment application and related transportation impact study prepared by the applicant will be provided to TransLink as a courtesy.

Other Stakeholders

Staff have reviewed the proposed OCP amendments with respect to the *Local Government Act* and the requirements of the City’s OCP Bylaw Preparation Consultation Policy 5043 and recommend that this report does not require referral to any other external stakeholders, as indicated in the table below.

Stakeholder	Referral Comment (No Referral necessary)
BC Land Reserve Co.	No referral necessary because the Land Reserve is not impacted.
The Board of Metro Vancouver	No referral necessary because the Regional District is not impacted.

Stakeholder	Referral Comment (No Referral necessary)
The Councils of adjacent Municipalities	No referral necessary because adjacent Municipalities are not impacted.
First Nations (e.g., Sto:lo, Tsawwassen, Musqueam)	No referral necessary because First Nations are not impacted.
Port Authorities (Vancouver Port Authority and Steveston Harbour Authority)	No referral is necessary because the Port Authorities are not impacted.
Vancouver International Airport Authority (VIAA) (Federal Government Agency)	No referral is necessary because VIAA is not impacted.
Richmond Coastal Health Authority	No referral is necessary because the Richmond Coastal Health Authority is not impacted.
All relevant Federal and Provincial Government Agencies	No referral is necessary because no Federal or Provincial Government Agencies are impacted.

### Additional Comments

OCP amendment application signage has been installed on the subject site.

Should the Planning Committee endorse this application and Council grant first reading to the OCP amendment bylaw, the bylaw will be forwarded to a Public Hearing, where any area resident or interested party will have an opportunity to comment.

Public notification for the Public Hearing will be provided as per the *Local Government Act*.

### **Analysis**

The origin of the subject City Centre Area Plan (CCAP) amendment application is the developer's proposal to vary street and development features set out in the Plan. Through the CCAP amendment application review process, staff have worked with the developer towards satisfying City and Area Plan objectives through various proposed developer contributions and development features. The CF Richmond Centre development plan's proposed revisions to the CCAP are generally illustrated in the Community Consultation – Public Display Boards. (Attachment 6) The proposed Area Plan amendments, including community feedback highlights the developer's response, and the proposed OCP amendment implementation approach are summarized below.

### Proposed CCAP Amendments

#### 1) Mobility Network:

- **Existing CCAP:** The Plan currently requires the extension of Park Road from No. 3 Road to Minoru Boulevard (at Minoru Gate) and the extension of on-street bike lanes along No. 3 Road and Minoru Boulevard.
- **Proposed CCAP Amendment:** The subject development proposes to:
  - a) Satisfy the Plan's existing requirements with respect to the extension of Park Road to Minoru Gate (in the form of a statutory right-of-way secured for public access, constructed at the developer's sole cost to the City's satisfaction, and owned/maintained by the developer); and

- b) Provide additional community benefits, including:
- i. A new City-owned street and multi-use pathway (secured as a road dedication), together with special landscape features, along the south edge of the subject property (adjacent to the City Hall site);
  - ii. Smaller, more pedestrian-friendly blocks, which will be achieved by extending Park Road beyond Minoru Gate to Murdoch Avenue and adding a new north-south connection between Park Road and the new City-owned street (in the form of additional “private road” owned/maintained by the developer and secured for public access with a statutory right-of-way);
  - iii. Enhanced street design standards, including wider sidewalks and special landscape features;
  - iv. Off-street bike paths along Minoru Boulevard and No. 3 Road, shared off-street bike access via the new City-owned street’s broad multi-use pathway, and future off-street bike paths along the CCAP’s designated Cook-Murdoch connector;
  - v. Improved access to/from Brighthouse Station and the future bus mall via:
    - A secured public route across the subject site between Minoru Boulevard and No. 3 Road, including access through CF Richmond Centre’s galleria outside normal shopping mall business hours (during transit operating hours); and
    - Sidewalk widening, upgraded crosswalks, and related improvements along the entire No. 3 Road frontage of CF Richmond Centre (including the subject site and the portion of the mall located north of 6551 No. 3 Road);
  - vi. End-of-trip cycling facilities for commercial tenants and employees; and
  - vii. Two multi-modal mobility hubs incorporating pedestrian-friendly, transportation-related features (e.g., bike- and car-share, taxi and ride-hailing pick-up/drop-offs, secure public bike storage, electric vehicle charging stations, transit stops) co-located with shops, services, and public amenities (Attachment 11, Schedule J “Mobility Hub Vision”).
- **Community Feedback Highlights:** Support was expressed for the proposed street network (65%), cross-mall access outside regular business hours (81%), streetscape improvements (75%), and parking features (60%).
  - **Developer Response:** Following the community consultation process, the developer undertook design development regarding the proposed mobility hubs (Attachment 11, Schedule J) to further enhance pedestrian bicycle, transit, and vehicle connectivity, features, and operations. The developer also proposes to provide transit passes for residents for one year (i.e. 25% of market units and 100% of affordable housing units) and retail employees/customers (\$100,000 value). The developer’s commitment to provide the mobility hubs, transit passes, and other mobility features will be secured by legal agreements registered on title prior to OCP amendment adoption. Additional design development will be undertaken through CF Richmond Centre’s Development Permit processes.

## 2) Public Open Space Network:

- **Existing CCAP:** The Plan currently requires greenway improvements (e.g., street tree planting, decorative paving, pedestrian lighting and furnishings) along the subject site’s No. 3 Road and Minoru Boulevard frontages.

- **Proposed CCAP Amendment:** The subject development proposes to:
  - a) Satisfy the Plan's existing requirements with respect to greenway improvements along the site's No. 3 Road and Minoru Boulevard frontages; and
  - b) Provide additional community benefits in the form of:
    - i. A central plaza (to be designated as park in the OCP and CCAP) secured for public use, approximately 0.2 ha (0.5 ac) in size (i.e. roughly twice the size of Lang Park); and
    - ii. Improved pedestrian and cycling linkages with Minoru Park and the Richmond Cultural Centre.
- **Community Feedback Highlights:** Support was expressed for the proposed public plaza and other public open space features (71%).
- **Developer Response:** Following the community consultation process, the developer undertook design development with respect to the private building frontages along the new City street (adjacent to City Hall) to increase the proposed size of the development's fronting gardens and enhance the ability to plant large trees on the subject site. The developer's commitment to provide the public plaza and other public open space features will be secured by legal agreement registered on title prior to OCP amendment adoption. Additional design development will be undertaken through the Development Permit process.

### 3) Form and Character:

- **Existing CCAP:** As the subject site is located within 400 m (1,312 ft.) of a Canada Line station, the Plan encourages high-rise, high density, mixed use, transit-oriented development, generally in the form of towers up to 45 m (148 ft.) in height, mid-rise streetwall buildings with landscaped roof decks (for use as residential outdoor amenity space), and a combination of ground floor, pedestrian-oriented retail and public amenities such as greenways (i.e. low-rise, lower density buildings are discouraged in key downtown transit nodes). Parking is encouraged to be screened from view (e.g., located underground or behind residential or commercial uses). The overall form of development is encouraged to contribute to a livable urban environment and a visually distinct and appealing urban village.
- **Proposed CCAP Amendment:** The subject development proposes to meet or exceed the Plan's existing requirements by:
  - a) Locating most of the development's required parking in two underground levels so as to screen it from public view and free up the ground plane for public open space, retail, restaurant, residential, and other non-parking uses;
  - b) Extending pedestrian-oriented commercial uses along No. 3 Road and the extension of Park Road, with links to the east and west ends of the mall's existing galleria, to provide for a connected outdoor/indoor pedestrian shopping (loop) precinct;
  - c) Designing the proposed public street and multi-use path along the subject site's south edge as a "civic promenade" framed by City Hall on its south and complementary architectural and landscape features on its north;
  - d) Providing opportunities for slim slab-type tower forms with large floorplates, wide tower spacing, and large podium-level landscaped outdoor amenity spaces; and

- e) Supporting the development of a distinctive public realm characterized by high quality, pedestrian-oriented retail and residential frontages and enhanced public spaces.
- **Community Feedback Highlights:** Support was expressed for the proposed outdoor shopping precinct (68%) and form of development (51%). Concerns expressed regarding the form of development were focussed on the number of towers and the potential impact of soil conditions on underground parking.
  - **Developer Response:** Following the community consultation process, the developer undertook design development with respect to the proposed:
    - a) Tower massing with the aim of refining measures intended to minimize the project's total number of towers, improve the development's ability to meet the needs of families with children, and enhance views from neighbouring existing residential towers. Features of the proposed development include:
      - i. Increasing the CCAP's maximum recommended mid-rise building height from 25 m (82 ft.) (8 storeys) to 30.5 m (100 ft.) (9 storeys) and increasing the CCAP's maximum recommended tower floorplate size on the subject site from 650 m<sup>2</sup> (7,000 ft<sup>2</sup>) to 1,200 m<sup>2</sup> (13,000 ft<sup>2</sup>), which together, as generally illustrated in Attachment 10:
        - Effectively cuts in half the number of towers that would otherwise be built under the existing CCAP; and
        - Increases the achievable tower spacing along all City street frontages (i.e. No. 3 Road, Minoru Boulevard, and the new City street adjacent to City Hall) to 35 m (115 ft.), as compared with the CCAP minimum recommended spacing (i.e. 24 m / 79 ft.) or the spacing that would potentially result if smaller, CCAP-size towers were constructed on the site (i.e. spacing of 20 m / 66 ft. or less, as shown in Attachment 10);
      - ii. Reducing tower width (i.e. narrow dimension of the slab) to 20 m (66 ft.) as compared with the width of Richmond's typical point towers, which are generally 35 m (115 ft.) or wider;
      - iii. Varying tower heights, shapes, and orientation to provide visual interest; and
      - iv. Creating large podium-level outdoor residential amenity spaces (co-located with indoor amenities) that are able to accommodate a variety of children's play opportunities suitable for a range of age groups and needs;
    - b) Increasing landscaping and terracing along the development's Minoru Boulevard frontage to enhance its interface with nearby residential buildings and Minoru Park; and
    - c) Providing greater façade articulation and incorporating special streetscape features along the north portion of Park Road (e.g., murals, public art, catenary lighting suspended over the roadway) to enhance the exposed portions of the existing mall, screen above-grade parking, and provide for a lively shopping environment.

Staff are supportive of the developer's proposed built form strategy because it effectively responds to the site's special constraints while respecting key urban design objectives of the CCAP. More specifically, while the net site is unusually large (109,353 m<sup>2</sup> / 28 ac.), 37% of the net site is occupied by the portion of the existing mall and surface parking that the owner wants to remain operational throughout and after construction and 18% is occupied by proposed publicly accessible streets and open spaces (which will be secured

with statutory right-of-ways prior to OCP Amendment adoption). As a result, the above-grade portion of the proposed development occupies just 45% of the net site. The developer's proposed underground parking, increased tower floorplate size, increased tower spacing, public and private open space amenities, and pedestrian-oriented streetscapes will help enable the lot's pre-zoned density to be accommodated on the site in a manner that is attractive, livable, and distinctive. The adoption of site-specific Development Permit (DP) Guidelines for the subject site is recommended to guide the developer's 2-phase development and define the boundaries of this distinct mixed use, urban precinct. (Proposed site-specific DP Guidelines are included in the attached OCP Amendment bylaw.)

Additional design development will be undertaken through CF Richmond Centre's Development Permit processes.

#### 4) Housing:

- **Existing CCAP:** The Plan encourages affordable housing, a diversity of unit types, and accessible housing options; however, as the subject site is pre-zoned site, the developer would not be obligated to provide for these features through a stand-alone Development Permit process.
- **Proposed CCAP Amendment:** The subject development proposes to provide:
  - a) 5% affordable, low-end-of-market rental (LEMR) housing in the form of two purpose-built rental buildings (one per phase) comprising a total of 150 units (secured in perpetuity with a Housing Agreement prior to adoption of the OCP amendment bylaw);
  - b) 50% family-friendly, 2- and 3-bedroom (market and LEMR) units; and
  - c) 25% Basic Universal Housing (BUH) units (including 100% of LEMR units), together with aging-in-place features (e.g., lever handles and blocking in walls for grab bars) in all units.

The developer's proposed 5% affordable housing contribution is consistent with the CDT1 zone, which permits a density bonus (0.15 FAR) for applications containing 5% LEMR units (secured with a Housing Agreement registered on title) that were submitted to the City prior to July 24, 2017 and presented for consideration by Council prior to July 24, 2018. The subject OCP amendment application was submitted on November 25, 2016 and first presented to Council on April 9, 2018.

The developer's proposed affordable housing contribution will be taken into account with respect to the Zoning Bylaw's permitted parking reduction applicable to pre-zoned CDT1 sites (i.e. from 1.5 spaces/unit to 1.0 space/unit). This is consistent with the Affordable Housing Strategy, which supports parking reductions in transit-oriented locations where it will help to facilitate increased affordable housing developer contributions.

- **Community Feedback Highlights:** Support was expressed for the proposed affordable housing units (64%) and family-friendly housing mix and accessibility features (66%).
- **Developer Response:** Following the community consultation process, the developer has engaged a non-profit affordable housing provider to be involved in the design and management of the LEMR units proposed for the development's first phase.

The developer's commitment to provide the proposed affordable housing (i.e. 150 units), family-friendly unit mix (i.e. 50% 2- and 3-bedroom market and LEMR units), and BUH and aging-in-place features (including 100% of LEMR units built to BUH standards) will be secured by legal agreements registered on title prior to OCP amendment adoption.

Additional design development will be undertaken through the Development Permit process.

5) District Energy Utility (DEU) Network:

- **Existing CCAP:** The Plan aims to support the development of a cleaner, greener, and healthier downtown and reductions in greenhouse gas (GHG) emissions, but does not set specific directions or targets for the City Centre.
- **Proposed CCAP Amendment:** To implement a low-carbon (i.e. low/zero GHG emissions) system to heat/cool the subject development with consideration being given to a City District Energy Utility (DEU).
- **Community Feedback Highlights:** Support was expressed for a low-carbon system that would help reduce GHG (66%).

**Developer Response:** Following the community consultation process, staff and the developer worked cooperatively to ensure that the subject development will contribute towards City objectives for the implementation of a low carbon system capable of meeting the heating/cooling needs of the subject development and providing for future network connections to Richmond's emerging City Centre DEU system. More specifically, the developer has proposed and staff have agreed that:

- a) As required under City policy for new City Centre development, the development (excluding commercial portions of the enclosed mall) will be designed and constructed, at the developer's sole cost, with the ability to connect to and be serviced by a City DEU system; and
- b) In addition, if prior to Development Permit issuance Council adopts a DEU service area bylaw that provides for the owner's construction of an energy generation plant on the subject site, the developer shall, at the developer's sole cost:
  - i. Design and construct a low carbon energy plant(s) capable of supplying at least 70% of the development's annual space heating, space cooling, and domestic hot water needs (excluding commercial portions of the enclosed mall) from a renewable (non-carbon) energy source;
  - ii. Transfer ownership of the low carbon energy plant(s), distribution system, and all ancillary components to the City or its DEU service provider prior to building occupancy; and
  - iii. Work with the City to explore opportunities for heat recovery from the enclosed mall and its integration with a City DEU system.

The developer's commitment to design, construct, and transfer a low carbon energy plant to the City's DEU service provider is consistent with that achieved through recent City Centre rezoning applications.

A separate staff report recommending a DEU service area bylaw with respect to the subject site will be presented for Council consideration.

The developer's commitment to implement the proposed low-carbon energy system will be secured by legal agreements registered on title prior to OCP amendment adoption.

Additional design development (e.g., energy modelling report, low carbon energy system detail design and configuration) will be undertaken through CF Richmond Centre's Development Permit and Building Permit processes.

### CCAP Amendment Implementation Approach

Implementation of the subject development as proposed would require a combination of site-specific amendments to the City Centre Area Plan (i.e. OCP Amendment Bylaw) and various related voluntary developer commitments towards specific development features and amenities secured through legal agreements registered on title to the property, as set out in the attached OCP Amendment Considerations (Attachment 11).

- 1) OCP Amendment Bylaw addresses items specific to the City Centre Area Plan, generally including amendments to:
  - Maps throughout the Plan, including changing in the locations of road, park, pedestrian-oriented retail precincts, pedestrian linkages and greenways, cycling linkages, and related features on and around the subject site;
  - The mobility section to add a mobility hub policy and related information;
  - The arts and culture section to revise the location of pedestrian-oriented retail street in response to the new street pattern;
  - The parks and open space section to add the central plaza and multi-modal route along the new City street (adjacent to City Hall);
  - The public realm and public life section to increase permitted tower floorplate sizes from 650 m<sup>2</sup> (7,000 ft<sup>2</sup>) to 1,200 m<sup>2</sup> (13,000 ft<sup>2</sup>) and revise recommended frontage conditions in response to the new street pattern; and
  - The Development Permit (DP) Guidelines section to add DP Guidelines specific to the subject site.
- 2) OCP Considerations address items to be secured via legal agreements for implementation by the developer, at the developer's sole cost. Development and the developer's delivery of community amenities and voluntary contributions will be undertaken in two phases, beginning with the site's west half (fronting Minoru Boulevard) and followed by the site's east half (fronting No. 3 Road). This includes:
  - Subdivision to create three fee-simple lots (i.e. Phase 1/west lot, Phase 2/east lot, and a remainder/north lot), as determined to the City's satisfaction, together with road widening (dedication) along the site's No. 3 Road and Minoru Boulevard frontages;
  - Statutory right-of-ways and related legal agreements securing:
    - a) A new publicly-accessible, privately-owned/maintained central plaza;
    - b) A new publicly-accessible, privately-owned/maintained street network (e.g., extensions of Park Road, Cook Road, Murdoch Avenue, and Minoru Gate); and
    - c) A future City-owned street and multi-use pathway along the site's south side (adjacent to City Hall), which right-of-way area will be:

- i. On an interim basis, occupied by the shopping centre's existing multi-storey parkade; and
  - ii. Prior to Building Permit, on a phase-by-phase basis (i.e. two phases), dedicated as City street (and the developer will enter into Servicing Agreements for the street's design and construction at the developer's sole cost).
- Transit access improvements, including:
    - a) Public access through the mall's galleria outside business hours to be consistent with Canada Line operating hours (to be secured with legal agreements); and
    - b) Upgrades to pedestrian crossings and sidewalks in proximity to the Richmond-Brighouse Station, including in Phase 1, widening of the shopping centre's No. 3 Road sidewalk to 3 m (10 ft.) from the pedestrian crossing at the transit station to the mall's north property line (i.e. north of Shopper's Drugmart);
  - Transportation demand management (TDM) measures including, among other things, two multi-modal mobility hubs and transit passes for residents and commercial tenants/employees;
  - Servicing Agreement requirements for the design and construction of new and upgraded streets, intersections, engineering infrastructure, public open space, greenways and bike paths, and related improvements (to be secured with Letters of Credit and, as applicable, statutory right-of-ways);
  - Electric vehicle and bike charging infrastructure and secure bike storage for the use of residents and, at the mobility hubs, for the general public;
  - Restrictions regarding driveway access along City-owned streets and tandem parking (i.e. permitted for market residential use and commercial valet parking only);
  - Affordable housing comprising at least 5% of total residential floor area (e.g., 150 units constructed to Basic Universal Housing standards, including 50% family-friendly 2- and 3-bedroom units);
  - 50% family-friendly 2- and 3-bedroom units;
  - 25% Basic Universal Housing units;
  - DEU-ready buildings and, upon Council's adoption of a DEU service area bylaw, the developer's commitment to the design, construction, and transfer of a low carbon energy plant to the City's DEU service provider;
  - Public art voluntary developer contribution (i.e. phase-by-phase, based on the City policy in effect at the time of Development Permit issuance);
  - Various construction traffic management requirements, including connectivity across the subject site for vehicles and pedestrians throughout demolition and construction;
  - Compliance with standard Richmond development requirements (e.g., covenants with respect to flood construction, aircraft noise, the Canada Line, view blockage, and other potential development impacts);
  - Tree retention, removal, and replacement in compliance with City bylaws;

- Submission and processing of a Development Permit application for the development's first phase to the satisfaction of the Director of Development prior to OCP Amendment bylaw adoption; and
- Various Development Permit, Building Permit, and occupancy holds and a development staging legal agreement (in the form of covenants and a no-separate-sales agreement registered on title to the lots) to ensure that community amenities and infrastructure improvements are delivered concurrently with the developer's proposed commercial and market residential uses.

### Proposed Development Schedule

The developer anticipates the subject 2-phase development to proceed generally as follows:

2018	Sales centre construction and demolition of the former automotive shop (near the former Sears building)
2019	Opening of the sales centre and the start of Phase 1, including demolition (i.e. west half of the existing multi-storey parkade and a portion of the mall), excavation, and site preparation followed near year-end by the start of construction
2021-2022	Building-by-building completion and occupancy of Phase 1, beginning with the proposed underground parking and new retail space connected to the existing mall
2022-2025	Phase 2 demolition (including the former Sears building, the east half of the multi-storey parkade, and a portion of the mall), excavation, site preparation, construction, and occupancy

### **Financial Impact or Economic Impact**

As a result of the proposed development, the City will take ownership of developer contributed assets such as road works, waterworks, storm sewers, sanitary sewers, street lights, street trees and traffic signals. The anticipated operating budget impact for the ongoing maintenance of these assets is \$33,000.00. This will be considered as part of the 2020 Operating budget.

### **Conclusion**

GBL Architects has applied to the City of Richmond to amend the Official Community Plan (OCP), Schedule 2.10 (City Centre Area Plan), at 6551 No. 3 Road to permit a 2-phase redevelopment of the south end of the CF Richmond Centre shopping centre with approximately 2,000 dwellings, new public streets and outdoor spaces, two levels of underground parking, and 38,937 m<sup>2</sup> (419,114 ft<sup>2</sup>) of new commercial space, the latter of which represents a net commercial increase of 12,032 m<sup>2</sup> (129,513 ft<sup>2</sup>). Rezoning is not required. However, the Richmond Centre South Development Plan includes new streets, public spaces, and building features that are not identified in the City Centre Area Plan (CCAP). To rectify this situation, the developer was required to make an application to amend the CCAP. Through the CCAP amendment review process, developer-led community consultation was undertaken (overseen by staff) and staff have worked with the developer to address community objectives, including ones that may not be readily achievable through a stand-alone Development Permit application process. An analysis of the subject development and proposed OCP amendments shows them to

be consistent with Richmond's objectives for development, livability, sustainability, and urban design in the downtown. On this basis, it is recommended that Official Community Plan Bylaw 7100, Amendment Bylaw No. 9892, be introduced and given first reading.

*Suzanne Carter-Huffman*

Suzanne Carter-Huffman  
Senior Planner/Urban Design

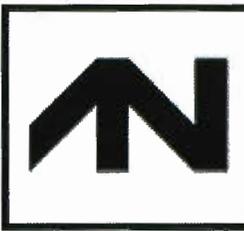
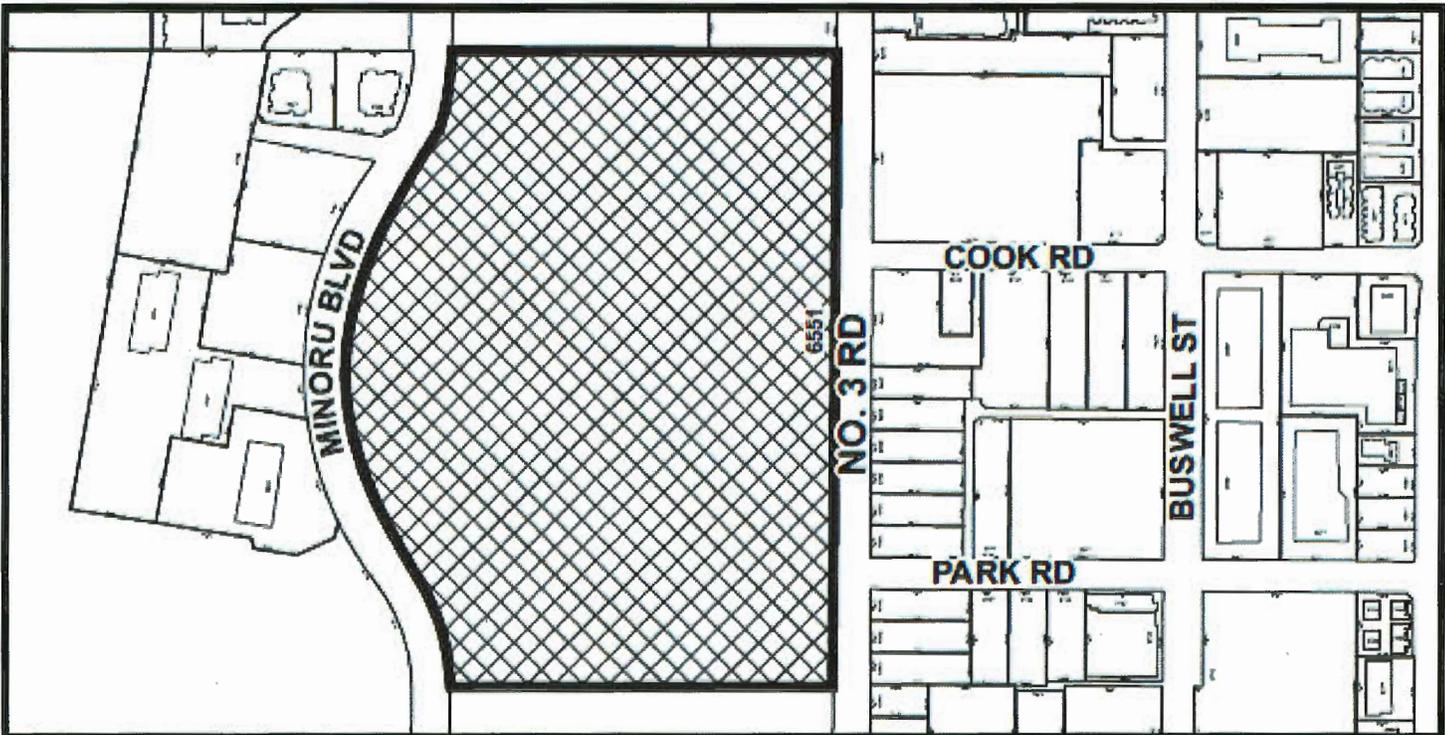
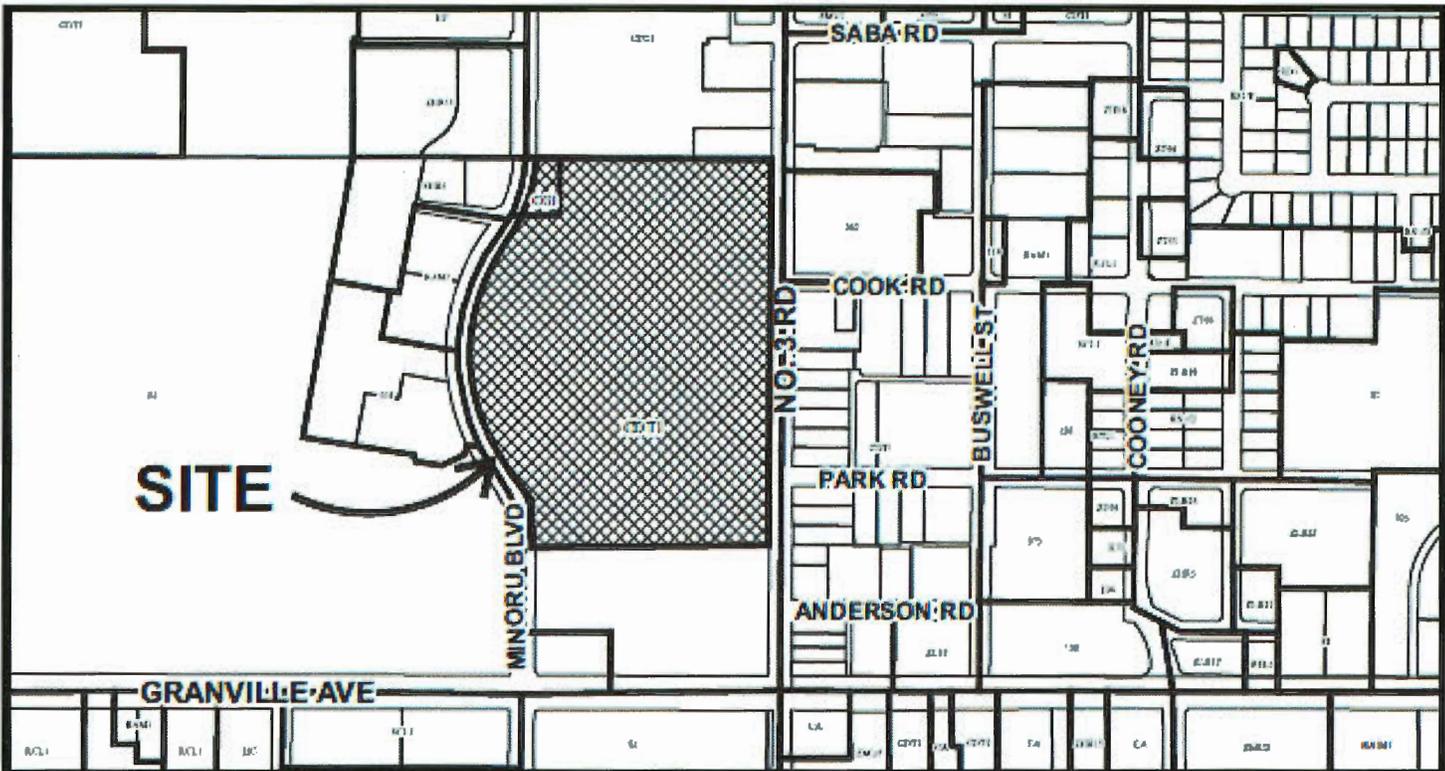
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Attachments:

1. Location Map
2. Aerial Photograph
3. Site Location & Proposed Phasing Boundaries
4. Development Application Data Sheet
5. City Centre Area Plan – Specific Land Use Map: Brighthouse Village (2031)
6. Community Consultation – Public Display Boards
7. Community Consultation – Feedback Form Summary
8. Correspondence (5 items)
9. Excerpt of the Advisory Design Panel (ADP) Meeting Minutes Held on March 7, 2018
10. Built Form Comparison
11. OCP Amendment Considerations

ATTACHMENT 1

Location Map



CP 16-752923

Original Date: 03/19/18

Revision Date:

Note: Dimensions are in METRES



CP 16-752923

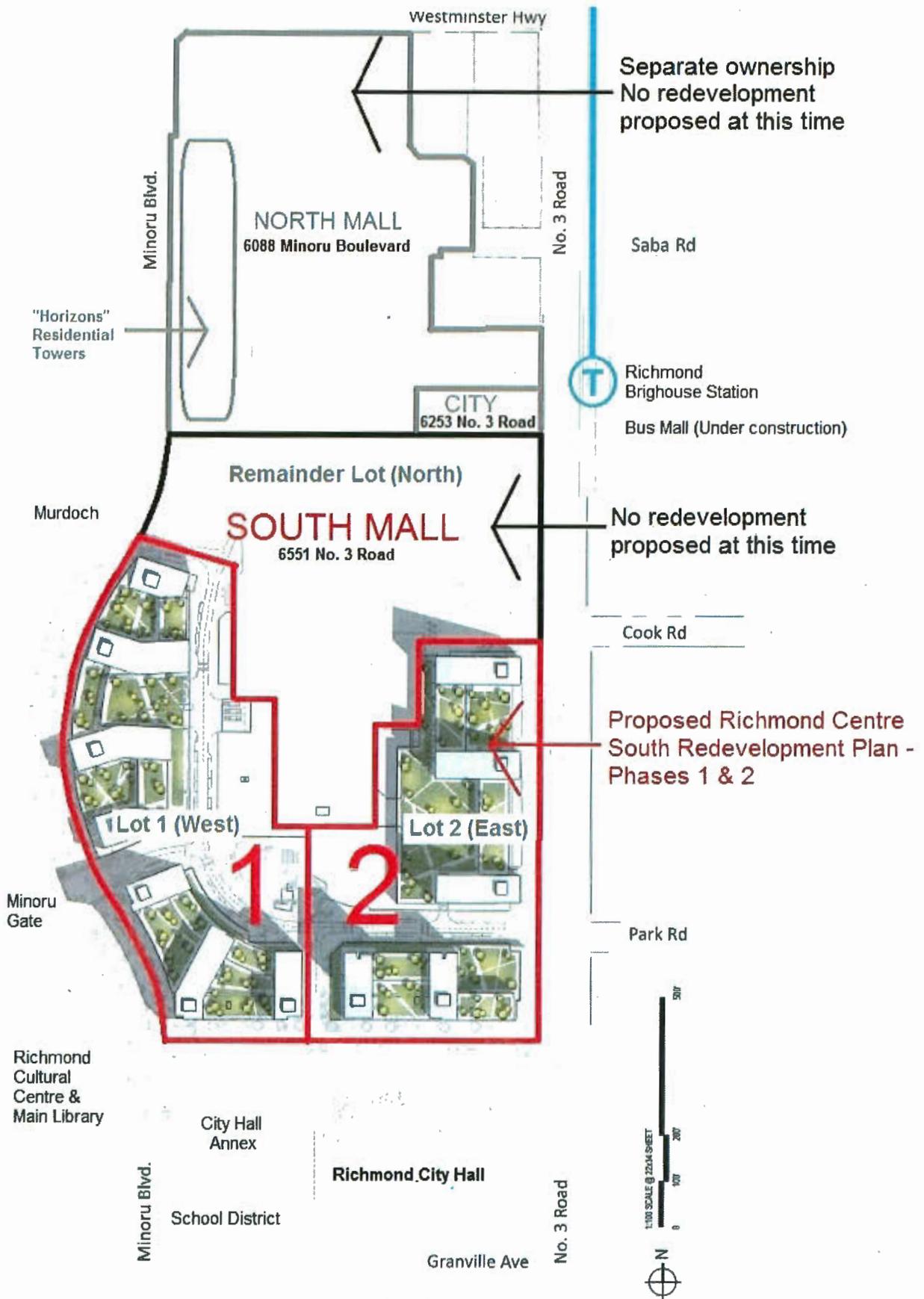
Original Date: 03/19/18

Revision Date:

Note: Dimensions are in METRES

**ATTACHMENT 3**

Site Location & Proposed Phasing Boundaries





**CP 16-752923**

Address: 6551 No. 3 Road (Richmond Centre / South Mall)  
 Applicant: GBL Architects Owner: RC (South) Inc. & 7904185 Canada Inc.  
 Planning Area(s): City Centre (Brighthouse Village)

Existing		Proposed
Site Area	<ul style="list-style-type: none"> <li>112,283.2 m<sup>2</sup> (1,208,605.8 ft<sup>2</sup> / 27.7 ac)</li> </ul>	<ul style="list-style-type: none"> <li>Road dedication: 2,930.4 m<sup>2</sup> (31,542.6 ft<sup>2</sup> / 0.7 ac)               <ul style="list-style-type: none"> <li>Minoru Blvd widening: 1,315.7 m<sup>2</sup> (14,162.1 ft<sup>2</sup>)</li> <li>No 3 Rd widening: 1,614.7 m<sup>2</sup> (17,380.5 ft<sup>2</sup>)</li> </ul> </li> <li>Net site: 109,352.7 m<sup>2</sup> (1,177,062.7 ft<sup>2</sup> / 27.0 ac)               <ul style="list-style-type: none"> <li>Proposed development site: 66,932.1 m<sup>2</sup> (16.5 ac)                   <ul style="list-style-type: none"> <li>Lot 1 (West): 36,497.7 m<sup>2</sup> (392,858.0 ft<sup>2</sup>)</li> <li>Lot 2 (East): 30,434.4 m<sup>2</sup> (327,593.2 ft<sup>2</sup>)</li> </ul> </li> <li>Remainder Lot (North): 42,420.6 m<sup>2</sup> (456,611.5 ft<sup>2</sup>)</li> </ul> </li> </ul>
Land Uses	<ul style="list-style-type: none"> <li>Auto-oriented commercial</li> </ul>	<ul style="list-style-type: none"> <li>High-rise, mixed use</li> </ul>
OCP Designation	<ul style="list-style-type: none"> <li>Downtown Mixed Use</li> </ul>	<ul style="list-style-type: none"> <li>No change</li> </ul>
CCAP Designation	<ul style="list-style-type: none"> <li>Urban Centre T6 (45 m)</li> <li>Village Centre (commercial) Bonus</li> <li>Pedestrian-Oriented Retail Precinct – “High Street” &amp; “Secondary”</li> <li>Proposed Streets</li> <li>Pedestrian Linkages</li> </ul>	<ul style="list-style-type: none"> <li>As existing, EXCEPT:               <ul style="list-style-type: none"> <li>Revised street network to create smaller blocks</li> <li>Revised pedestrian network</li> <li>Expanded “High Street” designation</li> <li>New “Park”</li> <li>Related DP Guideline changes</li> </ul> </li> </ul>
Aircraft Noise Sensitive Development	<ul style="list-style-type: none"> <li>Aircraft Noise Notification Area “Area 4” – All uses may be considered. (Covenant, acoustic report &amp; noise mitigation as required)</li> </ul>	<ul style="list-style-type: none"> <li>As required</li> </ul>
Zoning	<ul style="list-style-type: none"> <li>Downtown Commercial (CDT1)</li> <li>Gas &amp; Service Stations (CG1)</li> </ul>	<ul style="list-style-type: none"> <li>No change (Rezoning is NOT proposed)</li> </ul>
Number of Dwellings	<ul style="list-style-type: none"> <li>Nil</li> </ul>	<ul style="list-style-type: none"> <li>+/-2,000 units, including:               <ul style="list-style-type: none"> <li>Market housing: +/-1,850</li> <li>Affordable (LEMR) housing: +/-150, based on 5% of total residential floor area on Lot 1 (West) &amp; Lot 2 (East)</li> </ul> </li> </ul>
Dwelling Unit Types	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>50% Bachelor &amp; 1-BR (+/-1,000 units)</li> <li>50% 2-BR &amp; 3-BR (+/-1,000 units)</li> </ul>
Accessible Dwellings	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>25% Basic Universal Housing units (+/-500 units), including 100% of affordable (LEMR) units</li> <li>100% of units shall include aging-in-place features (e.g., handrails, lever handles &amp; blocking in walls for future grab bar installation)</li> </ul>

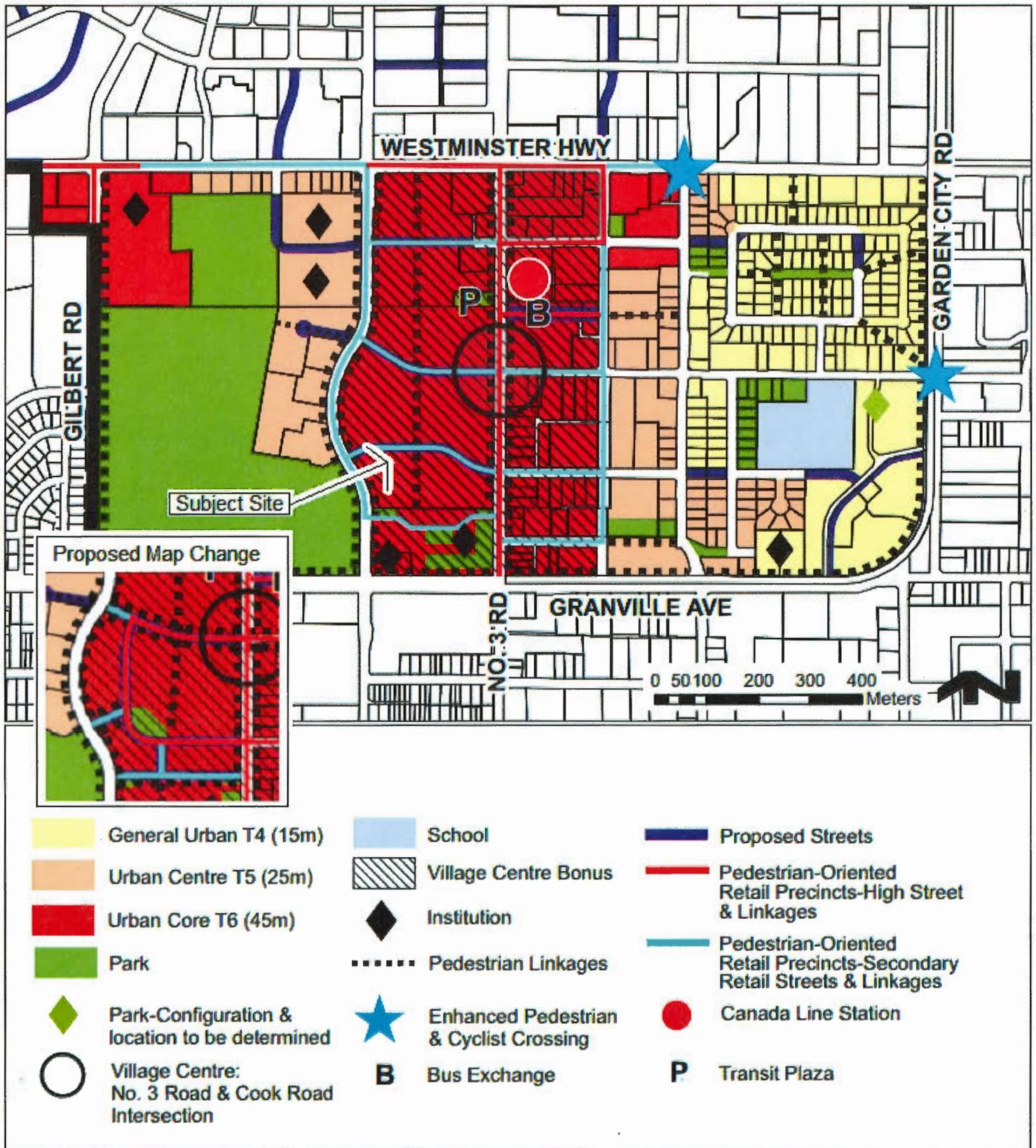
Existing CDT1 Zone Requirement	Proposed (No Rezoning Required)	Variance
Floor Area Ratio (Max.) <ul style="list-style-type: none"> <li>3.15 FAR, including a 0.15 FAR Affordable Housing bonus (as per City policy for applications considered prior to July 24, 2018)</li> </ul>	<ul style="list-style-type: none"> <li>+/-2.1 FAR, including new construction &amp; remaining portion of the mall building on the subject site</li> </ul>	None permitted
Buildable Floor Area (Max.)* <ul style="list-style-type: none"> <li>Based on site area net of road dedications: 339,106 m<sup>2</sup> (3.65 million ft<sup>2</sup>), including:               <ul style="list-style-type: none"> <li>Lot 1 (West): 114,968 m<sup>2</sup> (1.24 million ft<sup>2</sup>)</li> <li>Lot 2 (East): 95,868 m<sup>2</sup> (1.03 million ft<sup>2</sup>)</li> <li>Remainder Lot (North), excluding CG1 zoned site: 128,270 m<sup>2</sup> (1.38 million ft<sup>2</sup>)</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>232,258 m<sup>2</sup> (2.5 million ft<sup>2</sup>), including:               <ul style="list-style-type: none"> <li>Lot 1 (West): 105,259 m<sup>2</sup> (1,133,000 ft<sup>2</sup>)</li> <li>Lot 2 (East): 93,023 m<sup>2</sup> (1,001,290 ft<sup>2</sup>)</li> <li>Remainder Lot (North): 32,168 m<sup>2</sup> (346,257 ft<sup>2</sup>)</li> </ul> </li> </ul>	None permitted

Existing CDT1 Zone Requirement		Proposed (No Rezoning Required)	Variance
Height (Max.)	<ul style="list-style-type: none"> <li>45.0 m (148 ft.) to finished grade</li> </ul>	<ul style="list-style-type: none"> <li>45.0 m (148 ft.) max. to finished grade</li> </ul>	None
Lot Coverage (Max.)	<ul style="list-style-type: none"> <li>90% for buildings and landscaped roofs over parking spaces</li> </ul>	<ul style="list-style-type: none"> <li>90% for buildings and landscaped roofs over parking spaces</li> </ul>	None
Lot Size (Min.)	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>Lot 1 (West): 36,497.7 m<sup>2</sup> (392,858.0 ft<sup>2</sup>)</li> <li>Lot 2 (East): 30,434.4 m<sup>2</sup> (327,593.2 ft<sup>2</sup>)</li> <li>Remainder Lot (North): 42,420.6 m<sup>2</sup> (456,611.5 ft<sup>2</sup>)</li> </ul>	None
Setbacks (Min.)	<ul style="list-style-type: none"> <li>City Street: Min. 6 m (20 ft.), but may be reduced to 3 m (10 ft.) with a proper sidewalk interface</li> <li>Private Street (SRW): N/A</li> <li>Publicly-Accessible Open Space (SRW): 1.5 m (5 ft.)</li> <li>Interior Property Line: Nil</li> </ul>	<ul style="list-style-type: none"> <li>City Street: 3 m (10 ft.) or greater, except:                             <ul style="list-style-type: none"> <li>Underground parking: Nil</li> <li>New City street: 0.5 m (1.6 ft.)</li> </ul> </li> <li>Private Street: Nil to SRW</li> <li>Publicly-Accessible Open Space: 1.5 m (5 ft.) to SRW</li> <li>Interior Property Line: Nil</li> </ul>	Reduce from 3 m (10 ft.) to: Nil @ underground parking & 0.5 m (1.6 ft.) @ new City street
Off-Street Parking – Rates (Min.)	<ul style="list-style-type: none"> <li>Based on the provision of Affordable Housing &amp; Transportation Demand Management (TDM) Measures:                             <ul style="list-style-type: none"> <li>Market Housing: 0.9/unit</li> <li>Affordable Housing: 0.81/unit</li> <li>Commercial Uses: 3.375/100 m<sup>2</sup> GLA</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Market Housing: 0.9/unit</li> <li>Affordable Housing: 0.81/unit</li> <li>Commercial Uses: 3.375/100 m<sup>2</sup> GLA</li> </ul>	None
Off-Street Parking – Number of Spaces (Min.)	<ul style="list-style-type: none"> <li>3,896 spaces, including:                             <ul style="list-style-type: none"> <li>Market Housing: 1,665</li> <li>Affordable Housing: 122</li> <li>Commercial Uses: 2,109</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>4,000 spaces, including:                             <ul style="list-style-type: none"> <li>Market Housing: 1,769</li> <li>Affordable Housing: 122</li> <li>Commercial Uses: 2,109</li> </ul> </li> </ul>	None
Tandem Parking Spaces	<ul style="list-style-type: none"> <li>Market Housing: Maximum of 50% of required spaces</li> <li>Affordable Housing: Nil</li> <li>Commercial Uses: Limited to valet parking, as per legal agreement on title</li> </ul>	<ul style="list-style-type: none"> <li>Market Housing: Less than 50% of required spaces</li> <li>Affordable Housing: Nil</li> <li>Commercial Uses: Limited to valet parking, as per legal agreement on title</li> </ul>	None
Amenity Space – Indoor (Min.)	<ul style="list-style-type: none"> <li>Rate: 2 m<sup>2</sup> (22 ft<sup>2</sup>) / unit</li> <li>Rate x 2,000 units = 4,000 m<sup>2</sup> (43,056 ft<sup>2</sup>)</li> </ul>	<ul style="list-style-type: none"> <li>4,000 m<sup>2</sup> (43,056 ft<sup>2</sup>)</li> </ul>	None
Amenity Space – Outdoor (Min.)	<ul style="list-style-type: none"> <li>Rate: 6 m<sup>2</sup> (65 ft<sup>2</sup>) / unit</li> <li>Rate x 2,000 units = 12,000 m<sup>2</sup> (3 acres)</li> </ul>	<ul style="list-style-type: none"> <li>12,000 m<sup>2</sup> (3 acres)</li> </ul>	None
CCAP Additional Landscaped Space (Min.)	<ul style="list-style-type: none"> <li>10% of net site</li> <li>6,693 m<sup>2</sup> (1.7 acres), including:                             <ul style="list-style-type: none"> <li>Lot 1 (West): 3,650 m<sup>2</sup> (0.9 acres)</li> <li>Lot 2 (East): 3,043 m<sup>2</sup> (0.8 acres)</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>6,693 m<sup>2</sup> (1.7 acres), including:                             <ul style="list-style-type: none"> <li>Lot 1 (West): 3,650 m<sup>2</sup> (0.9 acres)</li> <li>Lot 2 (East): 3,043 m<sup>2</sup> (0.8 acres)</li> </ul> </li> </ul>	None

Other: Tree replacement compensation required for loss of significant trees.

\* Preliminary estimate, not including enclosed parking. The exact building size will be determined through zoning bylaw compliance reviews at Development Permit and Building Permit stages.

**Specific Land Use Map: Brighthouse Village (2031)**





## Proposed CF Richmond Centre South Development Plan

Here's your opportunity to share your input

You are invited to share your input on the proposed redevelopment of the south portion of the CF Richmond Centre shopping centre located at 6551 No. 3 Road.

The property owner has applied to the City to construct a two-phase redevelopment of the south end of the existing mall including an outdoor retail precinct, approximately 2,000 dwellings, and new streets and public spaces.

### Public Display & Open Houses

Residents and interested parties are invited to visit the public display at CF Richmond Centre, at the No. 3 Road entrance to the Galleria, from Tuesday, May 22 to Sunday, June 3, 2018.

The developer and City staff will be at the display to answer questions at two **Open House** events:

**Sunday, May 27, 2018**  
1 p.m. to 4 p.m.

**Thursday, May 31, 2018**  
5 p.m. to 8 p.m.

### Have your say!

#### LetsTalkRichmond: 3 easy steps



- Step 1: Go to LetsTalkRichmond.ca
- Step 2: Click on 'CF Richmond Centre South Development Plan'
- Step 3: Tap on 'TAKE SURVEY'

#### Not registered with LetsTalkRichmond?

- Tap the register button
- Enter your name, email & postal code
- Enjoy the display while you wait for a confirmation email
- Respond to the email & you are ready to go!

#### No cell phone? No problem!

- Fill in the survey on LetsTalkRichmond.ca at home
- At the Open House events, fill in a paper survey

*Be sure to submit your survey no later than 11:59 p.m. on Sunday, June 3, 2018.*



### For more information

Visit: [www.letstalkrichmond.ca/richmond-centre-south-development-plan](http://www.letstalkrichmond.ca/richmond-centre-south-development-plan)  
Contact: Suzanne Carter-Huffman, Senior Planner/Urban Design  
Phone: 604-276-4228  
Email: [communityplanning@richmond.ca](mailto:communityplanning@richmond.ca)

# INTRODUCTION OVERVIEW & DESCRIPTION

## CF RICHMOND CENTRE SOUTH

CF Richmond Centre is proposing a 2-phase redevelopment of a horseshoe-shaped portion of the south end of the existing mall, "CF Richmond Centre South."

The proposed Development Plan includes the replacement of the former Sears building, nearby shops, the existing multi-storey parkade, and adjacent surface parking with a high-rise, urban neighbourhood comprising:

- approximately 2,000 dwellings;
- new public streets and outdoor spaces;
- two levels of underground parking;
- 40,900 m<sup>2</sup> (440,000 ft<sup>2</sup>) of new retail space, which represents a net retail increase of approximately 9,290 m<sup>2</sup> (100,000 ft<sup>2</sup>).

The proposed high-rise, mixed use buildings are permitted under the mall's existing zoning and Richmond's City Centre Area Plan (CCAP), the long-range development plan for Richmond's downtown (approved in 2009).



### What does FAR mean?

FAR (floor area ratio) is a measure of building density. For example, 2.1 FAR means that a building's floor area equals 2.1 times the size of the property on which it is located.

As the property is pre-zoned to permit high-rise, high density development, no rezoning is required and the City's ability to secure development features, such as affordable housing, is compromised. Nevertheless, the developer has submitted an application to amend the City Centre Area Plan (CCAP) to permit changes to various circulation, public realm, and building design requirements of the Plan. Through this CCAP amendment review process, City staff are working with the developer to address community objectives and secure amenities, including ones not generally achievable through the development application processes applicable to pre-zoned sites.

## PROPOSED FOCUS OF CCAP CHANGES

### Circulation

- Streets
- Bike paths
- Transit access
- Innovative parking

### Public Realm

- Retail precinct
- Central plaza
- Greenways

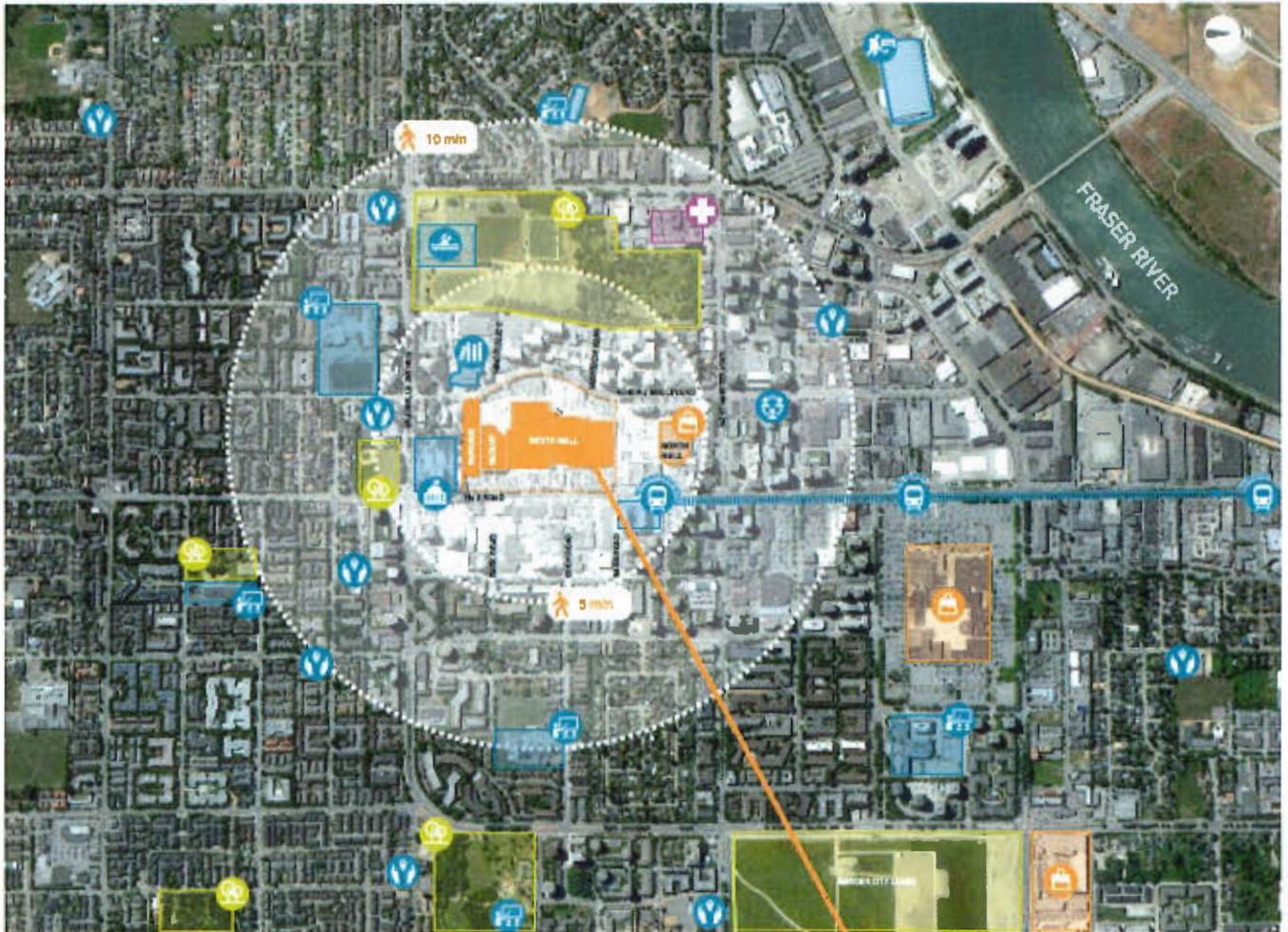
### Buildings

- Affordable housing
- Family housing
- Accessible housing
- Innovative design
- Reduced greenhouse gas



# INTRODUCTION

## CURRENT CONTEXT PLAN



- |  |                  |
|--|------------------|
| The Oval   | School           |
| Richmond Library & Cultural Centre                 | Place of Worship |
| Richmond City Hall                                 | Shopping Centre  |
| City Centre Community Centre                       | Park             |
| Canada Line Station                                | Hospital         |
| Minoru Centre for Active Living Under construction |                  |



Existing South Mall & Development Area



## RE-CONNECTING TO OUR CITY KEY COMMUNITY INITIATIVES

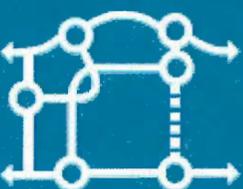
### RE-CONNECTING TO OUR CITY

**CF Richmond Centre** has been a key part of Richmond’s core for decades. As it evolves, the Centre is poised to bring new life to Richmond’s downtown and to reconnect key elements of the city. With its retail expansion, its mix of uses, and its public space additions, the CF Richmond Centre South Redevelopment aims to connect our streets, connect to transit, and connect to our open space network.

**Most importantly, it will connect our people . . . inviting everyone to come together and experience the new heart of Richmond.**

### KEY COMMUNITY INITIATIVES

The vision for the redevelopment plan of CF Richmond Centre South focuses on **six key community initiatives**.

<p><b>1</b> EVOLVE WITH THE GROWING RICHMOND COMMUNITY</p> 	<p><b>2</b> CONNECT THE CITY CENTRE GRID</p> 	<p><b>3</b> EXPAND RICHMOND’S OPEN SPACE AMENITIES</p> 
<p><b>4</b> REVITALIZE A CITY CENTRE RETAIL DESTINATION</p> 	<p><b>5</b> INTEGRATE A VARIETY OF HOUSING OPTIONS</p> 	<p><b>6</b> CONTINUE TO SERVE THE COMMUNITY DURING CONSTRUCTION</p> 



# 1 EVOLVE WITH THE GROWING RICHMOND COMMUNITY



**1964** RICHMOND POPULATION APPROXIMATELY 50,000  
Richmond Square opens to public



**1966** RICHMOND POPULATION APPROXIMATELY 50,000  
Hudson's Bay plans a location for site north of Richmond Square



**1973** RICHMOND POPULATION APPROXIMATELY 75,000  
View across Richmond Square from the south west



**1974** RICHMOND POPULATION APPROXIMATELY 75,000  
Photos taken from Minaru Blvd. Park Towers - Richmond's first high-rise buildings. The Twin Theatres and Municipal Hall are shown in the image farthest right.



**1989** RICHMOND POPULATION 115,000  
Hudson's Bay and Richmond Square combine into Richmond Centre



**1996** RICHMOND POPULATION 148,000  
Construction of Horizons Towers brings residential units to Centre



**2009** RICHMOND POPULATION 140,000  
Brighthouse Station on new Canada Line opens in advance of 2010 Olympic Games



**2012** RICHMOND POPULATION 190,000  
Dining Terrace opens

**TODAY** RICHMOND POPULATION 219,000

- 180+** Retail stores
- 12 million** Annual visitors
- 8** Average visits per month
- 1.5** Average hours spent per visit
- 50%** Visitors arriving from Canada Line
- 25%** Visitors living in Richmond
- 17%** Visitors who are tourists

Statistics are for entire CF Richmond Centre Mall (North & South)



# 2 CONNECT THE CITY CENTRE GRID



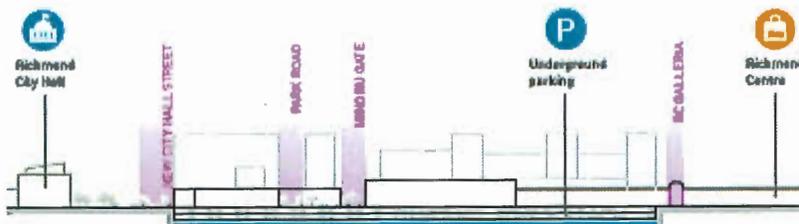
1 New City Hall Street



2 Off Street Bike Lanes



3 The Park Road extension connects the West and East side of the Development



## CONNECT THE CITY CENTRE GRID

Relocating the parking below ground allows for expanded development and new connections to be established between No. 3 Road to Minoru through a new street next to City Hall and an extension of Park Road. A new road will also connect Park Road to the new City Hall Street, while Minoru Gate and Murdoch Road will tie into the new street grid. New off street bike paths will be created on No. 3 Road, Minoru Boulevard, and the New City Hall Street. The existing mall Galleria will stay open during transit hours to create more permeability from Brighthouse Station.

- Richmond Brighthouse Station
- Richmond City Hall
- Richmond Library & Cultural Centre
- Minoru Park
- Park Plaza
- Existing Streets
- New Public Streets
- On Street Bike Lane
- Off Street Bike Lane



# 3 EXPAND RICHMOND'S OPEN SPACE AMENITIES



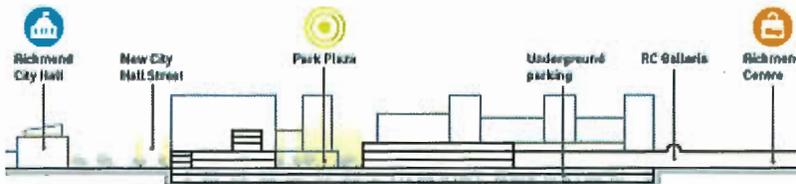
**1** Transit Station Crosswalk



**2** No. 3 Road Plaza



**3** Park Plaza



## EXPAND RICHMOND'S OUTDOOR SPACE

A network of open spaces of varying scales provide a variety of places to relax, socialize, and be entertained throughout the public realm, strengthening the connectivity of the City Centre.



- 1** Transit Station Crosswalk  
Weather-protected connection from No. 3 Road to Galleries
- 2** No. 3 Road Plaza  
Provides a green respite with outdoor seating along No. 3
- 3** Park Plaza  
Premier gathering place & center of activity
- 4** Public Art  
Public art will be located in key locations to engage visitors
- 5** Additional Public Art  
Artworks and artist infrastructure will enhance and animate the public realm
- 6** Residential Entry Court  
Rest areas along the townhouses and lobby frontage that delineate residential entries



# 3 EXPAND RICHMOND'S OPEN SPACE AMENITIES



## PUBLIC AMENITIES

CF Richmond Centre will provide a wide array of public amenities, enriching the retail and residential experience of the place. It includes landmark public art, enhanced sidewalks, crosswalks, and outdoor seating. A new plaza will become the heart of the City Centre, offering outdoor dining, events, and moments of relaxation.



# 4 REVITALIZE A CITY CENTRE RETAIL DESTINATION



1 Over 100,000 ft<sup>2</sup> of new retail



2 New retail high street on Park Road



3 Strong retail presence on No. 3 Road



## CITY CENTRE RETAIL DESTINATION

An expanded Commercial Centre creates a more connected, walkable, and attractive indoor/outdoor shopping experience characterized by pedestrian-scaled streets lined with shops, small plazas, continuous weather protection, street furnishings, public art, and special architectural and landscape features. The parkades are located for convenient access and can be entered from multiple locations to allow for better flow.

- 1 Over 100,000 ft<sup>2</sup> of new retail
  - Food & Beverage
  - Entertainment
  - Fashion & Services
- 2 New retail high street on Park Road
- 3 Strong retail presence on No. 3 Road
- 4 New mall entry on Park Plaza

- New Outdoor Shopping Precinct
- Richmond Centre Mall



# 4 REVITALIZE A CITY CENTRE RETAIL DESTINATION



## OUTDOOR SHOPPING PRECINCT

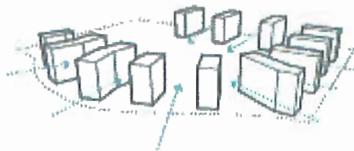
The new additions along No. 3 Road & the new Park Road extension will transform CF Richmond Centre into a vibrant outdoor shopping precinct. To enliven the sidewalk activity, the shops and restaurants will spill their spaces to sidewalk seating, entries, and displays. The amenities of weather coverage, landscaping, lighting, and sidewalk furnishings will also add to the pedestrian shopping experience of those passing through on their commutes.

## NEW RETAIL

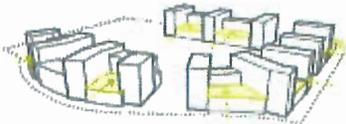
The retail expansion looks to integrate new restaurants, entertainment, fashion & service retail into the existing mall experience. The variety of retail choices will help support a true Live, Shop, Play environment for CF Richmond Centre's visitors & inhabitants.



# 5 INTEGRATE A VARIETY OF HOUSING OPTIONS



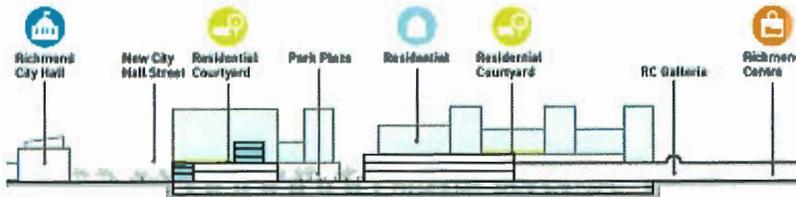
**1** Slender tower design maximizes daylight into Park Plaza and Retail streets.



**2** Amenity roof gardens integrate private patios & semi-public outdoor spaces.



**3** Over 2,000 new homes



## HOUSING VARIETY

The redevelopment of CF Richmond Centre South will bring over 2,000 new homes spread across 12 towers. The towers take on a courtyard form that is unique to the City Centre. This slender form allows more sunlight into Richmond Centre and creates large amenity roof gardens. The new homes will meet a diverse range of demands for housing in the City Centre, including affordable rental, family homes, and accessible housing for people in wheelchairs and with mobility challenges.

**1** Courtyard building form contributes to a varied streetscape and public realm.

**2** Amenity roof gardens enhance livability and complement public outdoor spaces

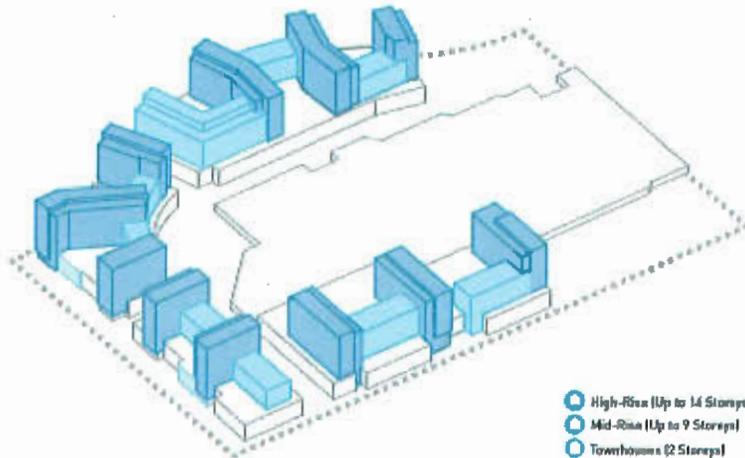
- Over 150,000 sq ft of roof gardens

**3** Over 2,000 new homes

- Approximately 150 homes to be secured as affordable rental
- 55% of homes designed for families two & three bedrooms & townhomes
- 25% of homes designed to Basic Universal Housing standards for enhanced accessibility



# 5 INTEGRATE A VARIETY OF HOUSING OPTIONS



## HOUSING FORM

Townhouses wrap the buildings at street level along Minoru Blvd and New City Hall street.

Mid-rise buildings line the new retail street, Minoru Blvd, and No. 3 Road to help define these important routes and soften the transition to high-rise forms.

High-rise towers are thin and elongated to visually connect with the mid-rises and create courtyard building forms. The slender courtyard building form creates a variety of architectural expressions while increasing tower separation and daylight. Large balconies wrap the exterior of the towers and provide residents with a true sense of indoor/outdoor living.



## PRIVATE AMENITIES

Residents will be able to enjoy over 42,000 ft<sup>2</sup> of indoor amenities and 130,000 ft<sup>2</sup> of outdoor facilities distributed throughout the proposed residential buildings.

Facilities include fitness areas, outdoor lounges, guest suites, community gardens, outdoor barbecues and more.



# 6 CONTINUE TO SERVE THE COMMUNITY DURING CONSTRUCTION



## PHASE 1 CONSTRUCTION

The CF Richmond Centre South Development Plan will take a number of years to complete, starting with the Minoru Boulevard side of the mall (Phase 1) and ending with the No. 3 Road side of the mall (Phase 2).

**CONSTRUCTION OF PHASE 1**  
BEGINS SPRING 2019

**PRESENTATION CENTRE**  
OPENS FALL 2018



## PHASE 2 CONSTRUCTION

**NEW PARKING ENTRY**  
LATE 2022

**PHASE 1 SHOPS & STREETS OPEN**  
LATE 2022

**NEW PLAZA & MALL ENTRY**  
LATE 2022

**CONSTRUCTION OF PHASE 2**  
BEGINS LATE 2022

**PRESENTATION CENTER DEMOLITION AND PARKING RECONFIGURATION**  
LATE 2023



## DEVELOPMENT COMPLETION

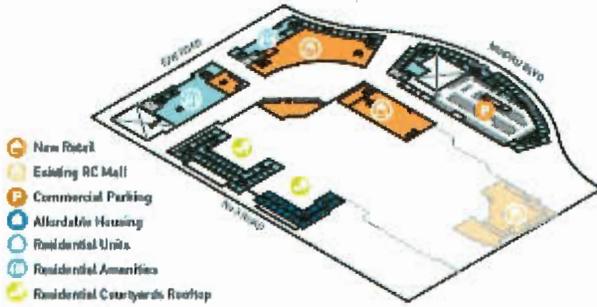
LATE 2026

Public pedestrian and vehicle access to the mall's shops and services will be maintained throughout both phases of the redevelopment process. Noise, dirt, worker parking, and other things related to the mall's demolition and construction must comply with City Bylaws.

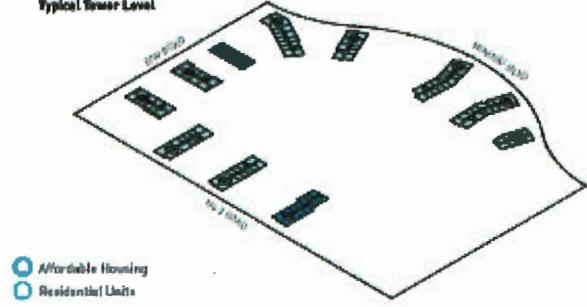


# REDEVELOPMENT OVERVIEW

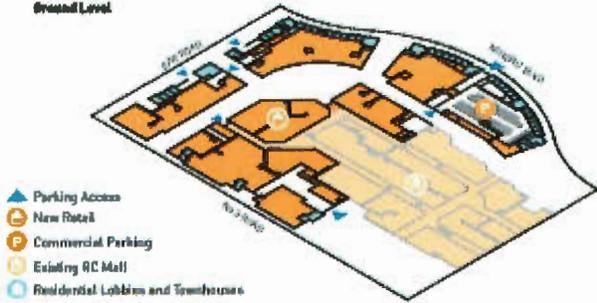
**LEVEL 2**



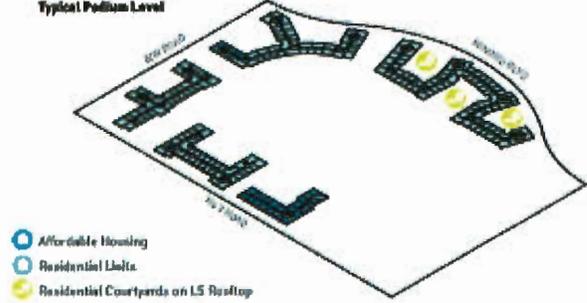
**LEVEL 12**  
Typical Tower Level



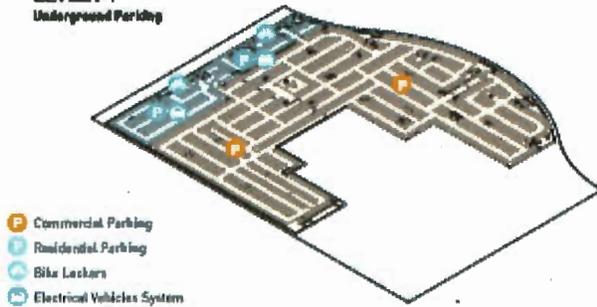
**Level 1**  
Ground Level



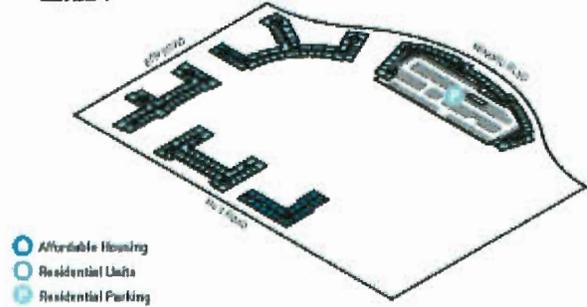
**LEVEL 6**  
Typical Podium Level



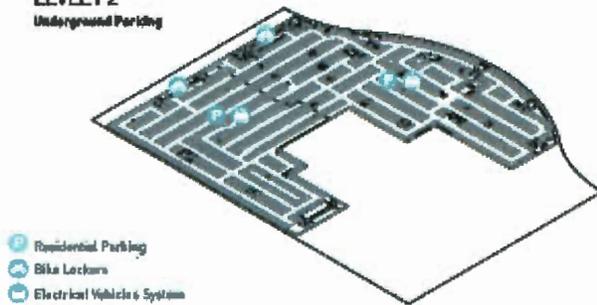
**LEVEL P1**  
Underground Parking



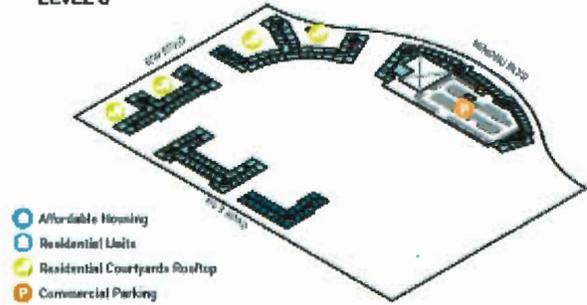
**LEVEL 4**



**LEVEL P2**  
Underground Parking



**LEVEL 3**



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

Over the next 100 years, Richmond's City Centre population is expected to triple to 120,000 and its jobs may more than double to 80,000. To accommodate this growth, Richmond's City Centre Area Plan (CCAP), adopted in 2009, proposes that the downtown develops as a connected network of urban villages focused on the Canada Line's 4 existing stations, the future Capstan Canada Line station, and the Richmond Olympic Oval.

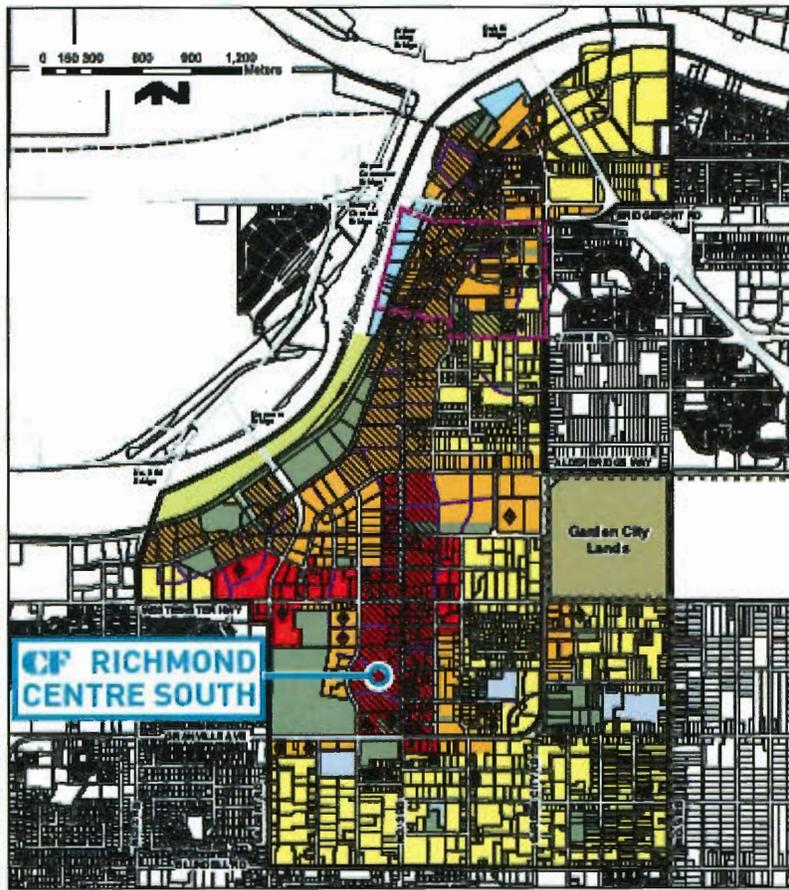
The CF Richmond Centre South Development Plan is consistent with the CCAP's goals, but proposes changes to how those goals will be achieved (e.g., new street locations).

Through this CCAP amendment process, City staff are working with the developer to address **10 key community objectives** and secure amenities, including ones not generally achievable through the development application processes applicable to pre-zoned sites.

## What does FAR mean?

FAR (floor area ratio) is a measure of building density. For example, 2.1 FAR means that a building's floor area equals 2.1 times the size of the property on which it is located.

## City Centre Area Plan (CCAP) Generalized Land Use Map



### Typical Characteristics:

- High Density/High-Rise**  
 Density: 3 - 4 FAR max  
 Height: 45m max  
 Use: Mixed
- Medium Density/Mid-Rise**  
 Density: 2 - 2.5 FAR max  
 Height: 25m - 35m max  
 Use: Residential
- Low Density/Low-Rise**  
 Density: 1.2 FAR max  
 Height: 15m max  
 Use: Residential
- Park**
- New Streets**

Residential is not permitted in some locations due to precinct rules



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #1: A More Connected Street Network

### TODAY

The CCAP aims to reduce the barrier posed by the existing mall by requiring the developer extend Park Road west from No. 3 Road to Minoru Boulevard.

### PROPOSED CHANGE

In addition to extending Park Road to Minoru Boulevard, the CF Richmond Centre South Development Plan proposes to create a more walkable neighbourhood comprised of smaller city blocks by establishing a connected network of local streets and off-street bike paths.



On-street bike lanes



New off-street bike lanes



No. 3 Road Sidewalk



New sidewalks & pedestrian seating areas



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

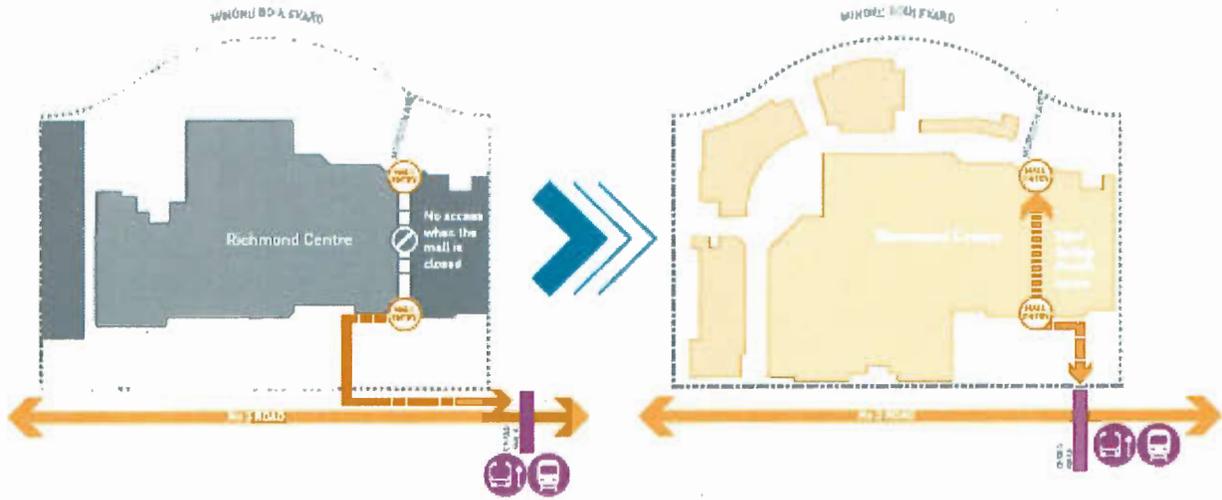
## Objective #2: Better Transit Access

### TODAY

Pedestrian access to/from Brighthouse Station can be inconvenient when the mall is closed and the No. 3 Road crosswalk near the station is congested.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes to improve public access to/from buses and the Canada Line by keeping the mall's Galleria open during transit hours, installing rain protection between the Galleria and No. 3 Road, upgrading the No. 3 Road crosswalk, and widening the No. 3 Road sidewalk along the entire frontage of the mall.



No. 3 Road Crosswalk



Weather protection from No. 3 Road to RC Galleria open during transit hours



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #3: Friendlier Streets for Pedestrians & Cyclists

### TODAY

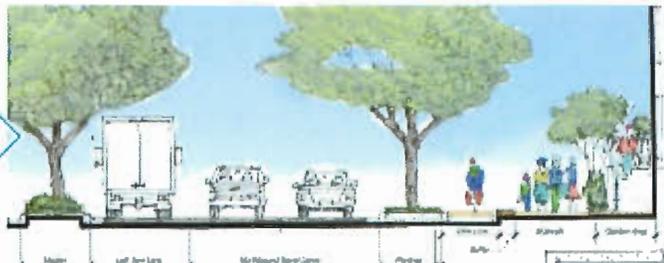
In the City Centre, sidewalks and boulevards occupy no more than 40% of a typical local street and even less of a major street, like No. 3 Road or Minoru Boulevard.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan aims to create more pedestrian- and bike-friendly streets by providing wider sidewalks, off-street bike paths, special landscape features, lighting, and seating.



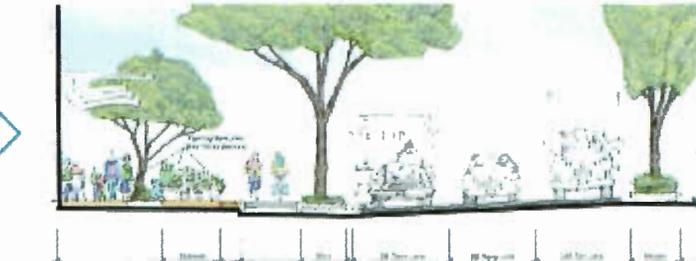
Minoru Blvd - Existing condition



MINORU GREENWAY: Minoru Blvd - Future



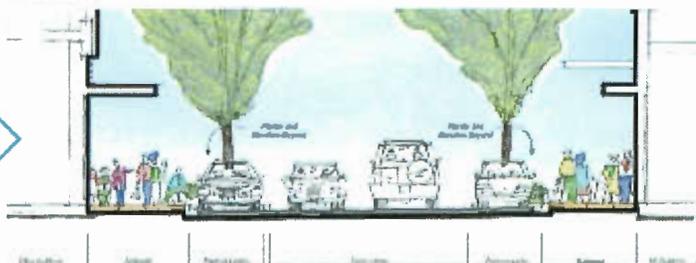
No 3 Road - Existing condition



COMMERCIAL HIGH STREET: No 3 Road - Future



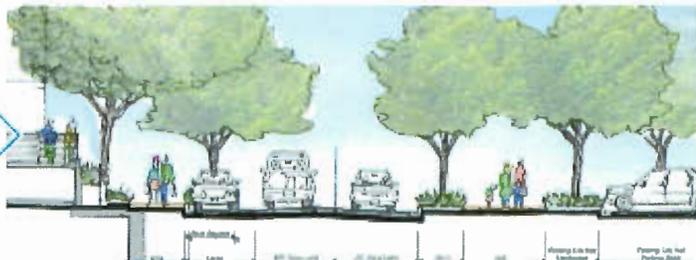
Park Road - Existing condition



OUTDOOR SHOPPING PRECINCT: Park Road - Future



City Hall Street - Existing condition



CIVIC GREENWAY: New City Hall Street - Future



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #4: A More Connected Parking Strategy

### TODAY

Unattractive parking lots ring the mall and are a barrier to pedestrians and cyclists, unpleasant in bad weather, and, at times, inconvenient.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes to improve on the current situation with a 2-level underground parking structure with direct vehicle access to No. 3 Road and Minoru Boulevard and "mobility hubs" designed to provide easy access for shoppers and the general public, linking the existing mall and new retail with parking, electric vehicle (EV) charging stations, secure bike storage, and car- and bike-share facilities.



P Parking  
A Richmond Library & Cultural Centre  
B Richmond Brighouse Station  
C Richmond City Hall



P Main Underground Parking Entry  
A Richmond Library & Cultural Centre  
B Richmond Brighouse Station  
M Mobility Hub  
C Richmond City Hall



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #5: A New Outdoor Shopping Precinct

### TODAY

The mall is inwardly focused and contributes little to the amenity or vitality of the downtown's public realm.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes to create a more connected, walkable, and attractive indoor/outdoor shopping precinct characterized by pedestrian-scaled streets lined with shops, small plazas, continuous weather protection, street furnishings, public art, and special architectural and landscape features.



Interior Mall



Future Outdoor Shopping Precinct



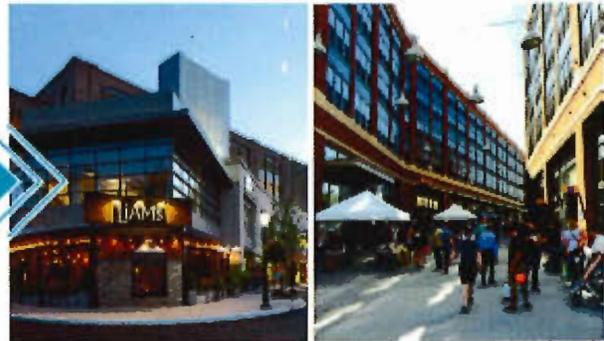
Existing Park Road



Future Park Road



Existing Surface Parking



Future Outdoor Shopping Precinct



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #6: New Outdoor Public Spaces

### TODAY

The mall provides no outdoor public space.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes to enhance the proposed outdoor shopping precinct with special landscape treatments along No. 3 Road and a central public plaza (roughly 0.5 acres in size or twice Lang Park), for relaxation, public gathering, and seasonal events and activities.



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #7: A New Architectural Character

### TODAY

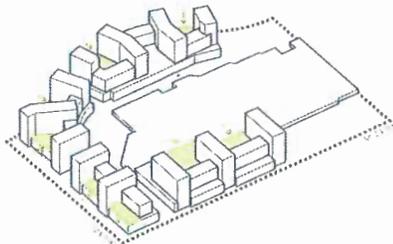
Much of the high-rise area surrounding the existing mall can be characterized as one- and two-tower residential and mixed-use developments with varied, individual identities.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes a cohesive neighbourhood identity characterized by a series of slim towers framing rooftop courtyards that fan out along the edges of the mall property like spokes on a wheel to frame the proposed public plaza and shopping precinct, provide for attractive commercial and residential streetscapes, allow sunlight and views through to public and private spaces, and create sunny rooftop courtyards for residents.

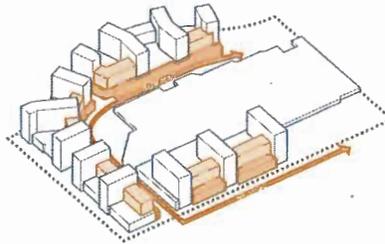


#### DAYLIGHT & OPEN SPACE



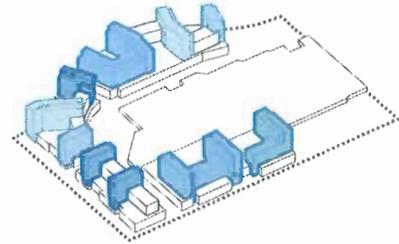
Towers optimize solar orientation & form large courtyards

#### RETAIL INTEGRATION



Strong street walls line retail streets & integrate a mix of uses

#### MASSING VARIETY



Long slender tower forms step around the skyline and form a unique variety of spaces



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #8: New Affordable Housing

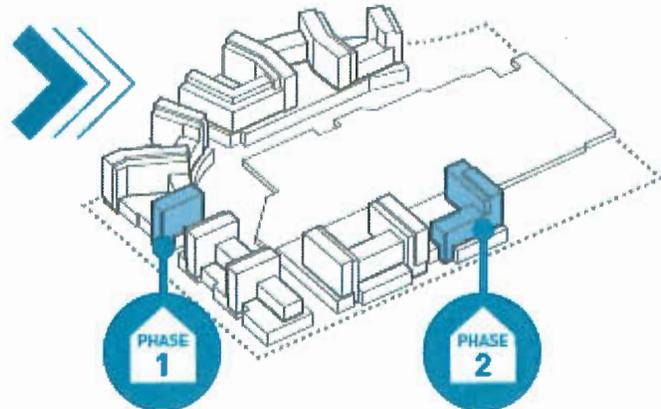
### TODAY

The mall and other pre-zoned sites that do not require a change to their existing zoning are not obligated to provide affordable housing.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes approximately 150 dwellings for low-income, workforce households (e.g., retail sales employees, teachers, nurses, etc.) in 2 purpose-built rental buildings suitable for operation by non-profit housing providers.

RICHMOND HOUSEHOLDS ON SOCIAL HOUSING WAIT-LISTS, BY NEED (2017)



## EXAMPLES OF AFFORDABLE HOUSING DESIGN QUALITY & SCALE



Jubilee House (Yaletown, Vancouver)  
162 units



McLaren House (Downtown, Vancouver)  
110 units



First Place (Mt Pleasant, Vancouver)  
129 units



# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #9: Housing for a Diverse Downtown Community

### TODAY

There is a growing need for new housing near transit, schools, and services that is designed to meet the needs of families with children, seniors, and people with disabilities.

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes that roughly 50% of dwellings will be family-friendly, 2- or 3-bedroom units and at least 25% of dwellings will meet Richmond's Basic Universal Housing standards (making them suitable for people with wheelchairs and mobility challenges).



**50%** FAMILY FRIENDLY DWELLINGS



**25%** BUH UNITS



- Richmond City Hall
- School
- Place of Worship
- Park
- Canada Line Station
- Library & Cultural Centre
- The Oval
- Hospital
- Minoru Centre for Active Living Under construction
- City Centre Community Centre
- Shopping Centre

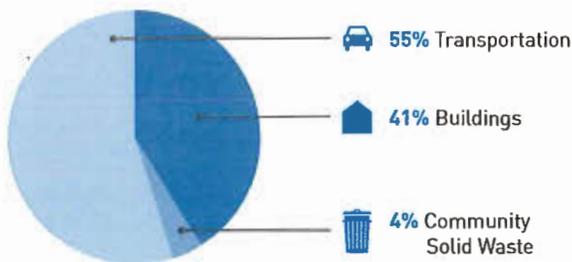


# CITY CENTRE AREA PLAN: PROPOSED CHANGES

## Objective #10: Reduced Greenhouse Gas (GHG)

### TODAY

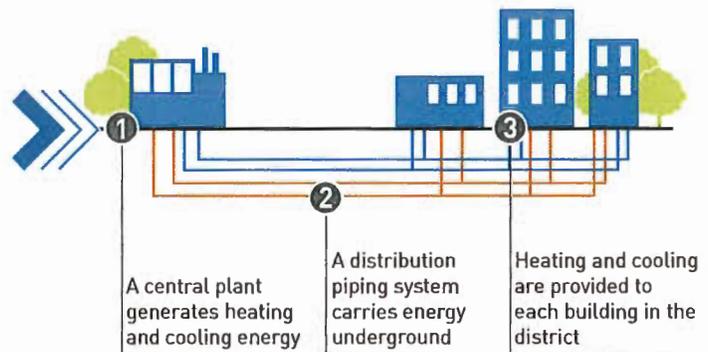
Richmond's Official Community Plan (OCP) aims to support the development of a cleaner, greener, and healthier community by reducing greenhouse gas (GHG) emissions, but does not set specific directions or targets for the City Centre.



Richmond Community GHG Emission by Sector (2010).

### PROPOSED CHANGE

The CF Richmond Centre South Development Plan proposes to adopt specific strategies supportive of the City's GHG reduction objectives, which may include the fast-tracking of the City's District Energy (DEU) plans by constructing a central energy plant on the mall property to heat/cool the proposed development and connect to a future City system.



Example of possible rooftop DEU installation on the mall



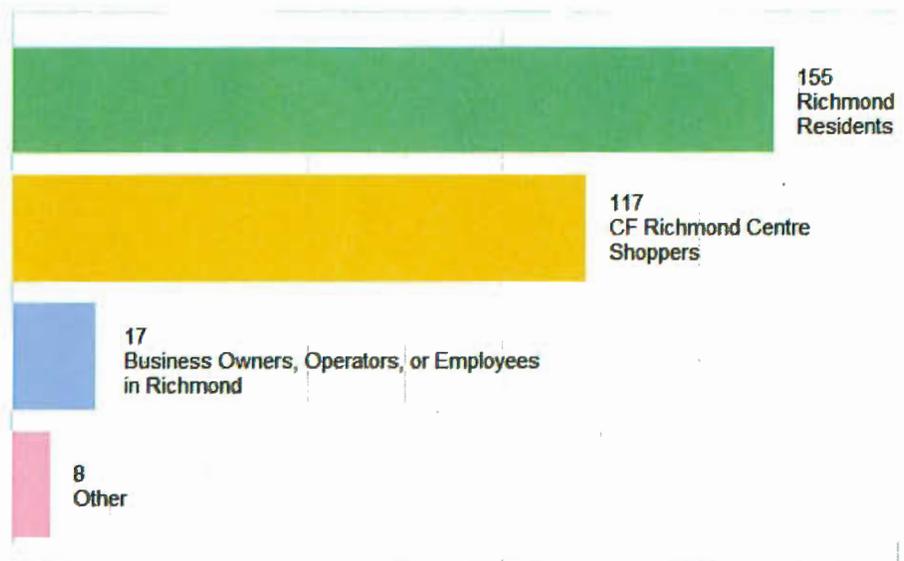
Rooftop will be screened from view from nearby towers



Proposed CF Richmond Centre South Development Plan  
**COMMUNITY CONSULTATION – FEEDBACK FORM SUMMARY**  
 Tuesday, May 22<sup>nd</sup> to Sunday, June 3<sup>rd</sup>, 2018

164 feedback forms were submitted to the City via LetsTalkRichmond.ca, mail, and in person.

Respondents primarily identified themselves as Richmond residents and/or CF Richmond Centre shoppers.

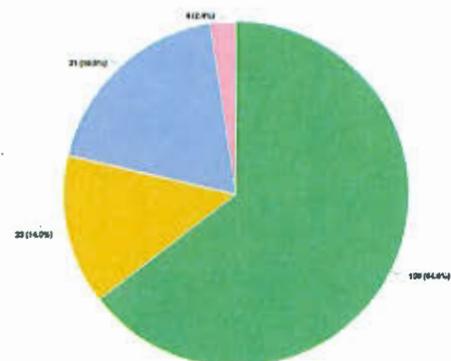


**Objective #1: A More Connected Street Network**

- **Today:** The CCAP aims to reduce the barrier posed by the existing mall by requiring the developer to extend Park Road west from No. 3 Road to Minoru Boulevard.
- **Proposed Change:** In addition to extending Park Road to Minoru Boulevard, the CF Richmond Centre South Development Plan proposes to create a more walkable neighbourhood comprised of smaller city blocks by establishing a connected network of local streets and off-street bike paths.

**#1 Public Response Overview:**

- a) Number of Responses: 164
- b) How did respondents feel about the proposed change:
  - ❖ **65% liked the change (106 responses)**
  - ❖ **14% were neutral (23 responses)**
  - ❖ **19% did not like the change (31 responses)**
  - ❖ **2% did not know (4 responses)**
- c) Respondents who did **NOT** like the proposed change expressed concern regarding:
  - Increased traffic congestion & delays
  - New roads not needed
  - Not enough parking
  - Skeptical that people will walk & bike
  - Need a transit priority lane on No. 3 Road
  - Growth will further strain infrastructure, transit & hospital
  - City Centre is overcrowded & has too many high-rises
  - City is losing doctors & small businesses
  - Soil conditions

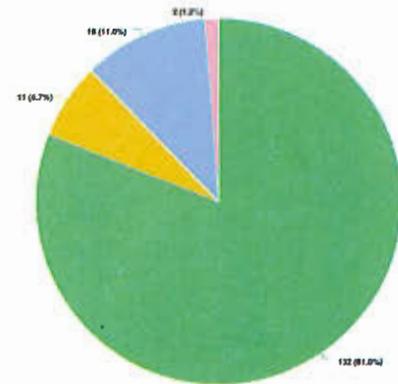


## Objective #2: Better Transit Access

- **Today:** Pedestrian access to/from Brighthouse Station can be inconvenient when the mall is closed and the No. 3 Road crosswalk near the station is congested.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes to improve public access to/from buses and the Canada Line by keeping the mall's Galleria open during transit hours, installing rain protection between the Galleria and No. 3 Road, upgrading the No. 3 Road crosswalk, and widening the No. 3 Road sidewalk along the entire frontage of the mall.

### #2 Public Response Overview:

- Number of Responses: 163
- How did respondents feel about the proposed change:
  - ❖ **81% liked the change (132 responses)**
  - ❖ **7% were neutral (11 responses)**
  - ❖ **11% did not like the change (18 responses)**
  - ❖ **1% did not know (2 responses)**
- Respondents who did **NOT** like the proposed change expressed concern regarding:
  - Canada Line is too busy
  - Wider sidewalks are needed
  - Bus mall & other transit improvements are needed
  - Will add to traffic congestion on No. 3 Road
  - Need cars & trucks (not transit) to do business
  - May increase panhandling

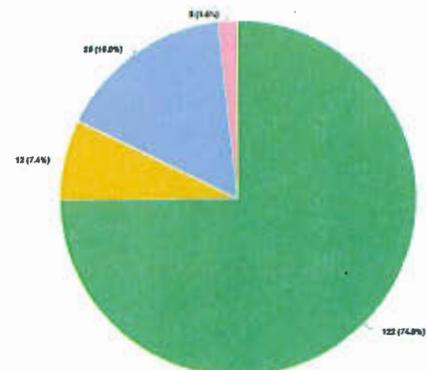


## Objective #3: Friendlier Streets for Pedestrians & Cyclists

- **Today:** In the City Centre, sidewalks and boulevards occupy no more than 40% of a typical local street and even less of a major street, like No. 3 Road or Minoru Boulevard.
- **Proposed Change:** The CF Richmond Centre South Development Plan aims to create more pedestrian- and bike-friendly streets by providing wider sidewalks, off-street bike paths, special landscape features, lighting, and seating.

### #3 Public Response Overview:

- Number of Responses: 163
- How did respondents feel about the proposed change:
  - ❖ **75% liked the change (122 responses)**
  - ❖ **7% were neutral (12 responses)**
  - ❖ **16% did not like the change (26 responses)**
  - ❖ **2% did not know (3 responses)**
- Respondents who did **NOT** like the proposed change expressed concern regarding:
  - Skeptical that people will ride bikes
  - Skeptical that there is enough space for multiple modes
  - More bike lanes/paths are not needed
  - Widen sidewalks for shared pedestrian/bike use
  - Features will only benefit the development's residents

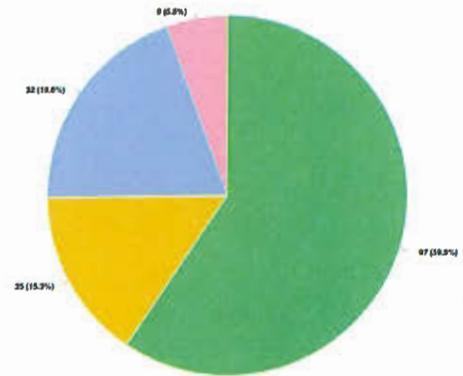


## Objective #4: A More Connected Parking Strategy

- **Today:** Unattractive parking lots ring the mall and are a barrier to pedestrians and cyclists, unpleasant in bad weather, and, at times, inconvenient.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes to improve on the current situation with a 2-level underground parking structure with direct vehicle access to No. 3 Road and Minoru Boulevard and "mobility hubs" designed to provide easy access for shoppers and the general public, linking the existing mall and new retail with parking, electric vehicle (EV) charging stations, secure bike storage, and car- and bike-share facilities.

### #4 Public Response Overview:

- Number of Responses: 163
- How did respondents feel about the proposed change:
  - ❖ **60% liked the change (97 responses)**
  - ❖ **15% were neutral (25 responses)**
  - ❖ **20% did not like the change (32 responses)**
  - ❖ **6% did not know (9 responses)**
- Respondents who did NOT like the proposed change expressed concern regarding:
  - City Centre is overcrowded
  - Traffic congestion
  - Difficulty finding parking
  - Bikes are for California
  - Electric vehicles are for the rich

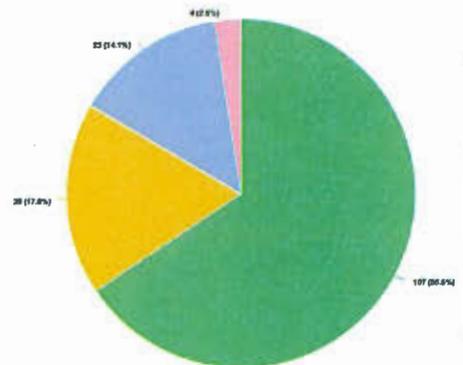


## Objective #5: A New Outdoor Shopping Precinct

- **Today:** The mall is inwardly focused and contributes little to the amenity or vitality of the downtown's public realm.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes to create a more connected, walkable, and attractive indoor/outdoor shopping precinct characterized by pedestrian-scaled streets lined with shops, small plazas, continuous weather protection, street furnishings, public art, and special architectural and landscape features.

### #5 Public Response Overview:

- Number of Responses: 163
- How did respondents feel about the proposed change:
  - ❖ **66% liked the change (107 responses)**
  - ❖ **18% were neutral (29 responses)**
  - ❖ **14% did not like the change (23 responses)**
  - ❖ **2% did not know (4 responses)**
- Respondents who did NOT like the proposed change expressed concern regarding:
  - Prefer a weather protected indoor mall
  - Mall fitness groups (seniors) will be displaced
  - Potential empty street-fronting storefronts ("slum")
  - Richmond Centre is unaffordable for normal tenants
  - Should be a shopping centre, not a gathering place

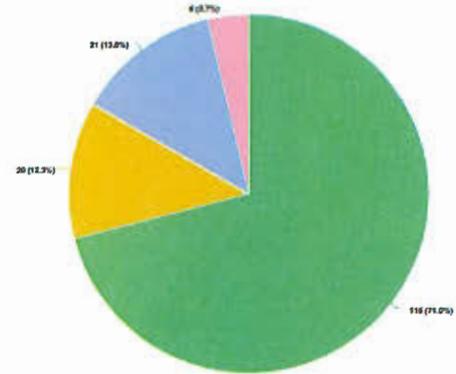


## Objective #6: New Outdoor Public Spaces

- **Today:** The mall provides no outdoor public space.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes to enhance the proposed outdoor shopping precinct with special landscape treatments along No. 3 Road and a central public plaza (roughly 0.5 acres in size or twice Lang Park) for relaxation, public gathering, and seasonal events and activities.

### #6 Public Response Overview:

- Number of Responses: 162
- How did respondents feel about the proposed change:
  - ❖ **71% liked the change (115 responses)**
  - ❖ **12% were neutral (20 responses)**
  - ❖ **13% did not like the change (21 responses)**
  - ❖ **4% did not know (6 responses)**
- Respondents who did NOT like the proposed change expressed concern regarding:
  - Skeptical that public space will be provided
  - Plaza is good, but would prefer a large park
  - Plaza will be noisy
  - Plaza is not needed (People should use Minoru Park)
  - Costly for taxpayers

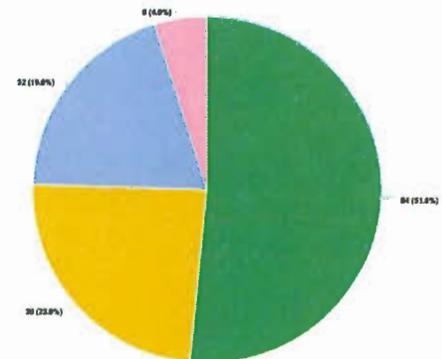


## Objective #7: A New Architectural Character

- **Today:** Much of the high-rise area surrounding the existing mall can be characterized as one- and two-tower residential and mixed-use developments with varied, individual identities.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes a cohesive neighbourhood identity characterized by a series of slim towers framing rooftop courtyards that fan out along the edges of the mall property like spokes on a wheel to frame the proposed public plaza and shopping precinct, provide for attractive commercial and residential streetscapes, allow sunlight and views through to public and private spaces, and create sunny rooftop courtyards for residents.

### #7 Public Response Overview:

- Number of Responses: 163
- How did respondents feel about the proposed change:
  - ❖ **51% liked the change (84 responses)**
  - ❖ **24% were neutral (39 responses)**
  - ❖ **20% did not like the change (32 responses)**
  - ❖ **5% did not know (8 responses)**
- Respondents who did NOT like the proposed change expressed concern regarding:
  - Do not like high-rises
  - Too many high-rises in City Centre
  - Tall buildings will block views & sunlight
  - Existing units are vacant / New units not needed
  - Form is unattractive
  - Rationale needed for large tower floorplates
  - Buildings do not mean "neighbourhood"
  - Allow public access to podium-level outdoor spaces
  - Need for master plan for entire mall

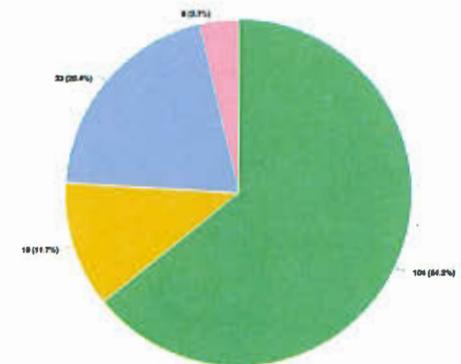


## Objective #8: New Affordable Housing

- **Today:** The mall and other pre-zoned sites that do not require a change to their existing zoning are not obligated to provide affordable housing.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes approximately 150 dwellings for low-income, workforce households (e.g., retail sales employees, teachers, nurses, etc.) in 2 purpose-built rental buildings suitable for operation by non-profit housing providers.

### #8 Public Response Overview:

- Number of Responses: 162
- How did respondents feel about the proposed change:
  - ❖ **64% liked the change (104 responses)**
  - ❖ **12% were neutral (19 responses)**
  - ❖ **20% did not like the change (33 responses)**
  - ❖ **4% did not know (6 responses)**
- Respondents who did NOT like the proposed change expressed concern regarding:
  - Skeptical that units will be affordable
  - More affordable housing is needed
  - Affordable housing is not needed
  - Something like Storeys should be included
  - Affordable units should be dispersed
  - Teachers & nurses are not low income earners
  - Too dense
  - Shopping centre will not be able to expand

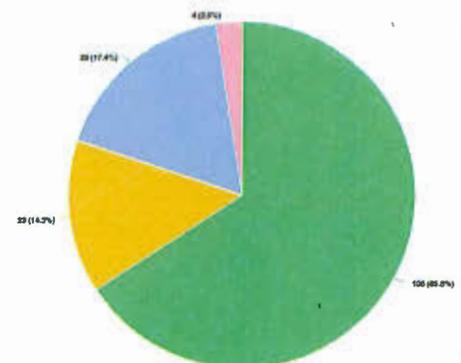


## Objective #9: Housing for a Diverse Downtown Community

- **Today:** There is a growing need for new housing near transit, schools, and services that is designed to meet the needs of families with children, seniors, and people with disabilities.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes that roughly 50% of dwellings will be family-friendly, 2- or 3-bedroom units and at least 25% of dwellings will meet Richmond's Basic Universal Housing standards (making them suitable for people with wheelchairs and mobility challenges).

### #9 Public Response Overview:

- Number of Responses: 161
- How did respondents feel about the proposed change:
  - ❖ **66% liked the change (109 responses)**
  - ❖ **14% were neutral (23 responses)**
  - ❖ **17% did not like the change (28 responses)**
  - ❖ **3% did not know (4 responses)**
- Respondents who did NOT like the proposed change expressed concern regarding:
  - Skeptical that units can be family-friendly
  - Basic Universal Housing standards throughout
  - Too much development / Too much traffic
  - Too expensive
  - School & child care capacity
  - Housing is replacing Richmond's shopping precinct

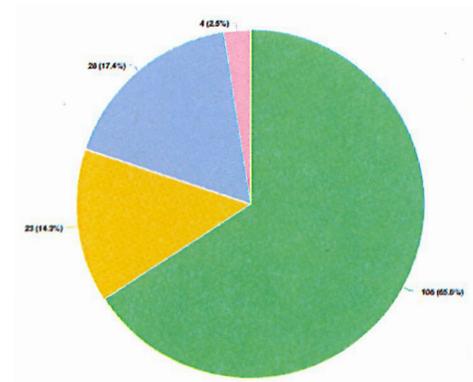


## Objective #10: Reduced Greenhouse Gas (GHG)

- **Today:** Richmond's Official Community Plan (OCP) aims to support the development of a cleaner, greener, and healthier community by reducing greenhouse gas (GHG) emissions, but does not set specific directions or targets for the City Centre.
- **Proposed Change:** The CF Richmond Centre South Development Plan proposes to adopt specific strategies supportive of the City's GHG reduction objectives, which may include the fast-tracking of the City's District Energy Utility (DEU) plans by constructing a central energy plant on the mall property to heat/cool the proposed development and connect to a future City system.

### #10 Public Response Overview:

- a) Number of Responses: 163
- b) How did respondents feel about the proposed change:
  - ❖ **66% liked the change (108 responses)**
  - ❖ **20% were neutral (33 responses)**
  - ❖ **8% did not like the change (13 responses)**
  - ❖ **6% did not know (9 responses)**
- c) Respondents who did NOT like the proposed change expressed concern regarding:
  - Skeptical that GHG levels will be reduced
  - Why not geo-thermal?
  - What will be the DEU energy source?
  - Higher building standards encouraged (e.g., LEED-ND)
  - Install green roofs on the mall
  - Increased noise pollution
  - Costly for taxpayers



## Additional Comments

- A. Growth/Change Impacts
  - Richmond Centre is great now, but the proposed development will ruin / over-populate it.
  - I don't like how Richmond is changing. It is too busy already.
  - More density doesn't mean a better place to live.
- B. Shopping Impacts
  - Redevelopment of Richmond Centre and Lansdowne is threatening Richmond's centralized shopping.
- C. Transportation Impacts
  - Too much congestion.
  - Not enough parking.
- D. Built Form Impacts
  - I expect bland streetscapes and over-priced and empty shops.
  - Don't allow towers along No. 3 Road. Limit No. 3 Road to 4-6 storeys.
  - Need more green space for families and children, not towers.
- E. Construction Impacts
  - As a nearby resident, I object to the noise, dirt, and other construction impacts the development will bring.

Item #1

**From:** Bill Sorenson [mailto:billmel36@gmail.com]  
**Sent:** Friday, 13 April 2018 21:33  
**To:** MayorandCouncillors  
**Subject:** Richmond City Centre South Redevelopment Plan

I read in the Richmond News, April 12, 2018 edition, that what was the Sears Building and mall parkade will be demolished as well as the southern-most parking lots. In their place will be a dozen new towers and about 2,000 new dwellings adding to the almost intolerable density that has been, and continues to fostered by the City. To suggest these changes are not subject to usual City demands, because the enabling zoning was put in place in the 1980's is beyond belief! The owners of this site were allowed to build two large residential towers in the vicinity of The Bay some years ago, which had to equate to a significant financial windfall at that time, but nothing compared to what is now going forward.

In January 2011 Richmond Centre applied for a Development Permit to undertake renovations to add second floor space to house their food court, and free up valuable ground floor space to be converted into retail stores. This had to represent yet another significant windfall, given the lease rates being charged to their store operators. The only good news I can see in all of this, is a commitment to keep the malls galleria open to the public during transit hours to allow residents to flow easily between Minoru Blvd. and No. 3 Road. I would hope the City will get a covenant registered to cover this, so it can't be changed in the future without the City's approval.

The average Richmond resident could not be faulted for thinking someone is getting a kick back or compensation in some form for letting this proceed unchallenged.

We've already lost the liveability of Richmond, our children can't afford to live here and there is a crisis whereby employees, clerks, waiters and waitresses can't afford to live here, and are seeking work elsewhere. When is it all going to stop? When will someone step forward to inject some common sense into the situation?

Respectfully,

Bill Sorenson  
604-278-9770

Item #2

**From:** Nadine Iwata [mailto:nadineiwata@gmail.com]  
**Sent:** Saturday, 7 July 2018 21:21  
**To:** MayorandCouncillors  
**Subject:** CF Richmond Centre South Development Proposal

Please consider:

There is no major supermarket on this end of town. (At #3 Rd and Granville) There is no elementary school that will have to accommodate all those new condos.

There are only TWO electric car plug ins for this area of town.

**From:** Elizabeth Purves [mailto:burroug@telus.net]  
**Sent:** Monday, 28 May 2018 3:08 PM  
**To:** Community Planning  
**Subject:** CF Richmond Centre South Development Plan

Dear Madam,

2 aspects of the above Development Plan are lacking answers:

1. Is the project being built on 'airspace'?
2. Where is the location of the proposed 'geothermal' plant?

Your response will be much appreciated!

Elizabeth Purves

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## City Response

**From:** "Carter-Huffman,Suzanne" <SCarter@richmond.ca>  
**To:** "Elizabeth Purves" <burroug@telus.net>  
**Sent:** Monday, May 28, 2018 5:39:12 PM  
**Subject:** RE: CF Richmond Centre South Development Plan

Hello.

Thanks for your interest in the Richmond Centre redevelopment. In answer to your questions:

- 1) Cadillac Fairview (CF) proposes to utilize an "air space subdivision" to create multiple legal parcels within the existing south mall property, including:
  - Several air space parcels containing the development's proposed market residential buildings (which buildings will be divided multiple strata units);
  - Two air space parcels containing the development's proposed affordable housing buildings, which will be owned by Cadillac Fairview; and
  - The remainder of the site, which will include the remaining portion of the existing south mall, together with the development's proposed new shops and parking, all of which will be owned by Cadillac Fairview.
- 2) A central energy plant is proposed for the roof of the mall, generally in the vicinity of Sportchek. The central energy plant is proposed to be part of a District Energy System operated by the Lulu Island Energy Company (LIEC). The Lulu Island Energy Company is a wholly-owned City of Richmond corporation (established to operate district energy utility systems in Richmond) that is fully supported by user fees, and has no impact on Richmond property taxes. Current service areas include the Alexandra area (geothermal) and Oval Village (currently natural gas with plans to change over to sewer heat recovery). Additional City Centre areas (including the CF Richmond Centre site) will be added to the system as development occurs. For more information about District Energy or LIEC, please contact Alen Postolka, Manager – District Energy (apostolka@luluslandenergy.ca or 604-276-4283).

If you have any other questions, please let me know.

**Suzanne Carter-Huffman | Senior Planner/Urban Design | Planning & Development**  
City of Richmond | 6911 No. 3 Road, Richmond, BC, V6Y 2C1 | www.richmond.ca

**General Comments, Compliments and Questions****Category:** Question**Comment/Compliment/Question:**

Right now in Richmond Centre there is a show on the plan for the future development. There will be more than ten high-rise buildings to be built within the centre area. And right now the sky trans are getting full all the time. I wonder whether the city has any plan for such a growth of the population in Richmond?

**Personal Information:**

Ray Wong

778-384-1233

RWong218@hotmail.com

**Tech Information:**

Submitted By: 172.29.0.6

Submitted On: Jun 04, 2018 06:26 PM

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**City Response**

Operation of the Canada Line is the responsibility of TransLink (regional transportation authority) not the City of Richmond. TransLink's 10-Year Vision for transportation upgrades across the region (<https://tenyearvision.translink.ca/>) identifies a number of Canada Line and bus improvements including:

**Phase 1 (2017-2019)**

- increase of passenger capacity on the Canada Line during peak periods by 11% in January 2017 via increased frequency of trains
- acquisition of 24 new Canada Line cars to allow further frequency of service improvements
- upgrade of selected Canada Line stations to enhance passenger access/egress

**Phase 2 (2020-2021)**

- new B-Line service between Richmond-Brighthouse Station and Metrotown Station
- increased service on Canada Line during rush hours, evenings and weekends

Should you wish to contact TransLink directly, you can use the online feedback form at <https://feedback.translink.ca/>.

## CANADIAN STYLE BASKETBALL

205-7388 Gollner Ave., Richmond, BC, V6Y 0H4, Tel. No. 604-241-1271, Email: [jypestano181@gmail.com](mailto:jypestano181@gmail.com)

May 27, 2018

**Ms. Suzanne Carter-Huffman**  
Senior Planner / Urban Design  
Planning and Development Division  
City of Richmond  
6911 No. 3 Road  
Richmond, BC V6Y 2C1

**Subject: Public Consultation for the Proposed CF Richmond Centre South Development Plan**

Dear Ms. Huffman,

Thank you very much for this opportunity to present my thoughts and ideas for the "Proposed CF Richmond Centre South Development Plan" as follows:

- I agree with the proposed Development Plan that includes the removal and replacement of the former Sears building, nearby shops, the existing multi-storey parkade, and adjacent surface parking with high-rise, urban neighborhood comprising approximately 2,000 dwellings, new public streets and outdoor spaces, two levels of underground parking, and 40,900 sq. m. (440,000 sq. ft.) of new retail space, the latter of which represents a net retail increase of approximately 9,290 sq. m. (100,00 sq. ft.).
- In addition may I suggest and include construction of multipurpose indoor stadium and arena for concerts, cultural shows, and indoor sports like basketball, ice hockey, volleyball, boxing, wrestling, martial arts and gymnastics tournaments with retail facilities for fast food restaurants, cafes, boutiques and gift shops concessionaires to serve spectators and at the same attract more shoppers to Richmond Centre.

Thank you and more power to you and your staff. Best regards,

Yours truly,

  
Jaime Y. Pestano

1. **CP 16-752923 - OCP AMENDMENT TO PERMIT MIXED USE REDEVELOPMENT OF THE SOUTH PART OF RICHMOND CENTRE SHOPPING CENTRE**

ARCHITECT: GBL Architects

PROPERTY LOCATION: 6551 No. 3 Road

**Applicant's Presentation**

Joey Stevens, GBL Architects, David Chamness, Callison RTKL, and Kris Snider, Hewitt Landscape, presented the project and answered queries from the Panel.

**Panel Discussion**

*Comments from Panel members were as follows:*

- appreciate the applicant's intention to incorporate public art into the project; applicant needs to pay attention to the future location of public art and how it facilitates the pedestrian aspect of the project, e.g. wayfinding and differentiation between public versus private realms;
- No. 3 Road is the main public road in Richmond; consider locating public art at the Park Road entrance along No. 3 Road or widening up the area of the pedestrian space to emphasize the publicness of this important corner;
- appreciate the different textures of paving on each block in the proposed development;
- the project will improve the current street network connection; however, controlling the speed of vehicles in the proposed internal streets is a concern; consider installing clearly marked crosswalks in busy areas to enhance pedestrian safety;
- opening of the mall Galleria during transit hours will significantly improve public access to transit;
- applicant should address and not underestimate wayfinding concerns in the underground parkade as it is more challenging to navigate in the parkade than on the ground;
- not supportive of the outdoor sidewalks for the proposed outdoor shopping precinct as it may not provide adequate weather protection for pedestrians during the rainy season;
- appreciate the provision for a public plaza; however, it may not be adequate to serve the needs of the proposed development;
- east-west orientation of some proposed buildings will not provide protection from the cold west winds for pedestrians walking in the vicinity of these buildings;
- appreciate the proposed location of affordable housing units;

- proposed diverse mix of dwellings is well thought out and meets the needs of families with children, seniors and people with mobility challenges;
- the applicant is encouraged to look into the thermo-energy demand of the proposed building forms in order to meet the energy-efficiency requirements of the BC Energy Step Code (as Step Code requirements may make it necessary to reconsider the proposed residential built form concept);
- significant size of the proposed development requires a District Energy Utility (DEU) plant; however, the applicant is advised that advance planning is needed in terms of the plant's location, serviceability, gas connections, location of cooling towers, and other important considerations;
- review proposed floor to floor height of the underground parking levels as it appears too low to accommodate necessary services for the buildings; also ensure adequate provision for space for service corridor considering that a DEU system is proposed for the project;
- required service connections for the size of the project would be massive; two service connections will not be adequate; water stagnation may also pose a challenge due to the magnitude of required services for the proposed towers;
- appreciate the comprehensive package provided by the applicant; however, a sustainability section could have been included in the package considering the size of the project;
- commend the applicant for the package provided to the Panel;
- proposed project has many positives, e.g., replacing the expansive surface parking lots with high-rise towers and amenity roof gardens;
- building lay-out is good in terms of solar aspect; outdoor amenity spaces are well done and usable to residents;
- larger scale plans would be useful for the public presentation of the project; families would be interested to see the project's site context in terms of its location relative to transit, schools, parks and other community amenities;
- proposed towers on the subject site will overlook the north portion of Richmond Centre; consider introducing green treatment to the existing roof;
- appreciate the permeability of the connected street network; hope that the richness of the design and materials of the proposed development will not be lost through the detailing; appreciate the open mall strategy; hope that the applicant will devote necessary resources for public spaces and public interface;
- appreciate the applicant's presentation of the project which is located in an important and central part of Richmond;
- a larger context plan would be helpful for the project's public presentation; statistical data included in the applicant's submission regarding visitors coming to Richmond Centre Mall are useful for designing the project;

- Minoru Park is a major regional destination for people coming from Brighthouse Canada Line station through Richmond Centre; the applicant is advised to acknowledge more the Park destination and give more attention to wayfinding from the northeast surface parking lot to Minoru Park through the Galleria;
- hope that the City's Parks Department will respond to the proposed development through programming Minoru Park in order to serve the broader needs of visitors/users in addition to current active sports uses;
- appreciate the proposed weather protected connection from No. 3 Road to the Galleria; ensure that the canopies along the building face are generous and consider making the weather-protected walkway through the parking lot more ample, e.g., widening it if possible to five meters to provide a more public feel to it;
- appreciate the provision for bicycle parking in the project as there is huge demand for it; will complement bicycle parking at Brighthouse Canada Line station; also appreciate the proposed off-street bicycle paths along No. 3 Road and Minoru Boulevard;
- appreciate the proposed on-site at grade planting and proposed structures to support large trees;
- the applicant is encouraged to install as much as possible a continuous row of street trees along the internal streets especially at the Park Plaza area;
- notice that there are no sight lines to the proposed Park Plaza from public streets, e.g. from the new City Hall street, Minoru Gate and No. 3 Road; applicant is advised not to oversell the Park Plaza as a public space if it is intended to be a commercial space rather than a public/civic space;
- shadow diagrams could have been helpful in determining the extent of park area that will be in shade; concerned that the southwest edge of the park will be in shade for a significant period; applicant could consider locating the gathering space on the northeast side of the plaza where there would be more sun exposure;
- notice that the proposed affordable housing units are segregated in individual buildings/blocks; consider distributing the affordable housing units in different places throughout the residential component of the project to make them less conspicuous;
- agree with comment from the Panel for the applicant to introduce roof planting on the north portion of Richmond Centre; applicant may also consider the alternative of hiring a graphic designer to introduce design/colour on the roof to make it more visually appealing for residents of adjacent high-rise towers on the south side;
- appreciate the applicant having a public art consultant on board for the project; a public art plan is more critical at this stage of the project rather than identifying public art location as all other public art decisions will flow from the public art plan;

- suggest that the applicant clarify the presentation board for public consultation Question 1 (i.e., More Connected Street Network) and break out vehicular, bicycle and pedestrian movements along the proposed network of internal streets;
- consider asking neutral as opposed to leading questions for public consultation;
- for public consultation Question 3 (i.e., Friendlier Streets for Pedestrian and Cyclists), the applicant needs to correct the image and section drawing for new City Hall Street as the photograph is looking east while the section drawing is looking west;
- commend the applicant's presentation of the project which will transform an existing development with vast expanse of surface parking to a pedestrian-friendly community;
- proposed street connections for vehicular and pedestrian circulation are logical from an urban design point of view;
- scale of the main and connecting streets are pedestrian-friendly;
- appreciate the proposed Park Plaza; support the proposal to externalize the shopping experience which is becoming the norm in North America;
- selection of retailers in terms of type and scale is crucial for the proposed development; activating the second floor is important for animating the whole street;
- the northeast corner of the subject development is not well resolved; has the potential to become a gateway into the site from Brighthouse Canada Line station; consider creating a mini plaza to focus attention to this corner and connect to the Galleria; also consider creating a mini plaza at the northwest corner of the site and connect the two mini-plazas through the Galleria to create a loop rather than a destination to the main plaza;
- incorporate images of precedents for the proposed Park Plaza in the presentation board for public display/consultation to help the public visualize the design of the future plaza and its public amenities; also incorporate the connection of the two mini-plazas with the main plaza (i.e, showing a loop) and their connection to transit and other public amenities;
- the proposed Park Plaza lacks visual connection from external public streets; consider shifting the location of the plaza to provide visual connection to the corner of the plaza from City Hall through the north-south connector road (connecting the new City Hall Street to Park Road extension) to encourage more pedestrian traffic from City Hall to the plaza and making it more of a public than a mainly commercial space;
- support the proposed underground parking considering the challenges associated with such proposal in Richmond; the approach is in the right direction towards Richmond becoming a more sustainable city;
- commend the design team and the developer for a significant and well thought out project;

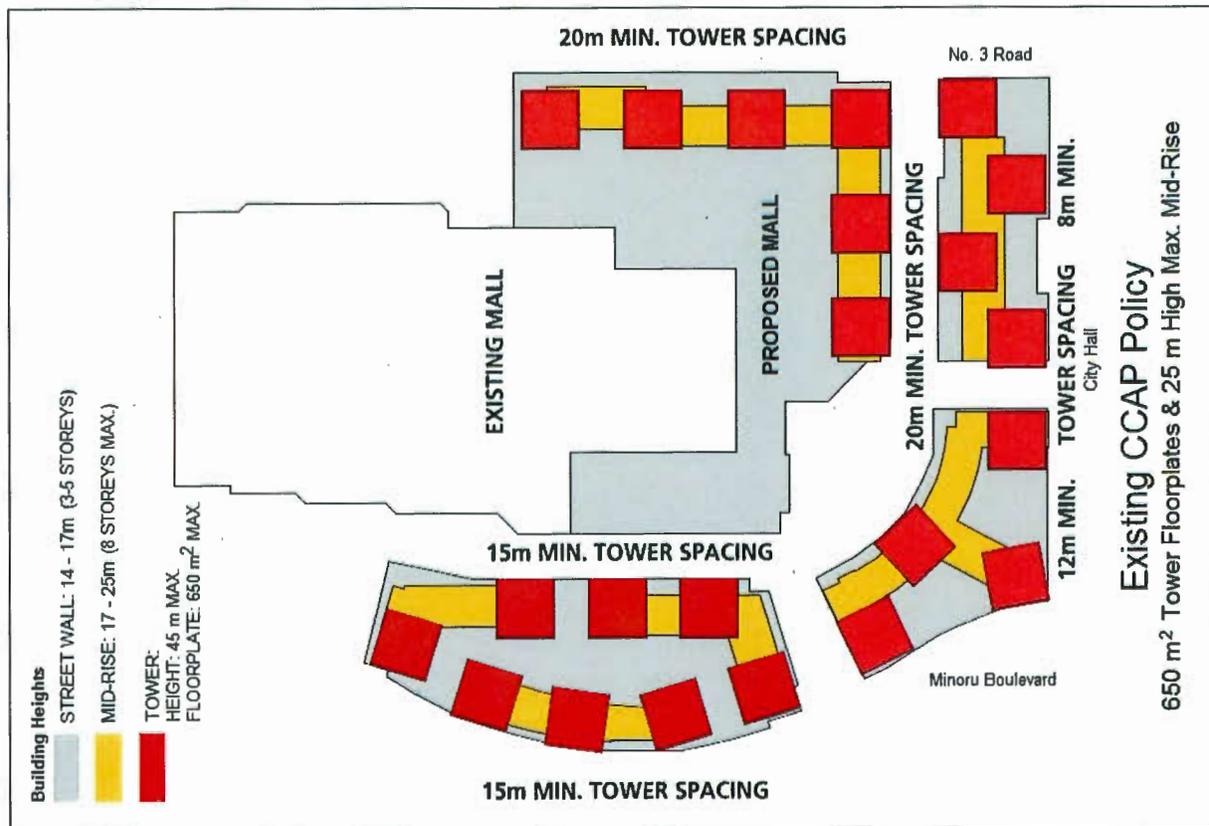
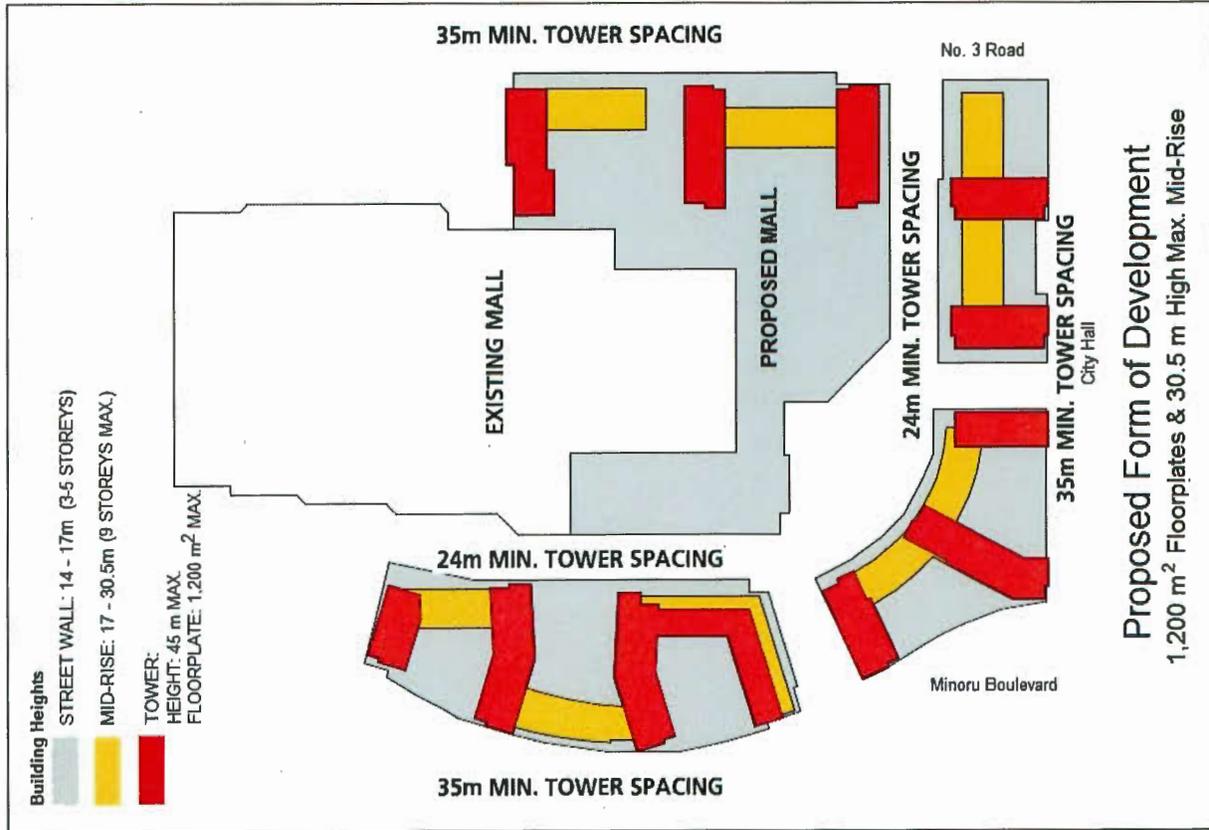
- appreciate the provision for affordable housing in the proposed development; also appreciate the applicant working within the existing City Centre Area Plan (CCAP) guidelines in terms of density and height of towers;
- support Panel comments for the applicant to address the overlook from the proposed high-rise towers onto the north portion of Richmond Centre; consider introducing appropriate architectural and landscaping treatments to the roof of the existing north portion of Richmond Centre;
- appreciate the applicant addressing the pedestrian movement to transit through the Galleria;
- package provided by the applicant lacks details regarding the public realm; significant amount of work and details still needs to be done (e.g., in terms of public realm details, loading, and architectural design) which the Panel would look forward to see when the applicant comes back to the Panel;
- consider larger and more detailed plans for public presentation/consultation for the project and also for future presentation to the Panel;
- recommend a small portion of parking should be used for park-and-ride;
- applicant is advised to give attention to the interface between City Hall and the proposed development; review the proposed location of the loading area and other things happening at the southern edge of the development;
- suggest that the applicant provide more presentation boards and details for the public consultation; agree with Panel comment that vehicular, bicycle and pedestrian circulation on the site should be demonstrated more graphically; applicant is also advised to provide more presentation boards for the public realm; also integrate architectural and landscaping precedents; and
- applicant and City staff are advised to consider installing an iconic art piece at the northeast corner of the site similar to the one at Brentwood Town Centre considering the huge number of people coming into the site from Brighthouse Canada Line station.

#### **Panel Decision**

It was moved and seconded

*That CP 16-752923 be supported to move forward to the Planning Committee subject to the applicant giving consideration to the comments of the Panel.*

**CARRIED**





## OCP Amendment Considerations

Development Applications Department  
6911 No. 3 Road, Richmond, BC V6Y 2C1

**Address:** 6551 No 3 Road

**File No.:** CP 16-752923

**Prior to final adoption of Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 9892, the developer is required to satisfy the following requirements:**

1. Site Contamination:

- 1.1. Development Approval Requirements: Submission to the City of a contaminated sites legal instrument from the Ministry of Environment and Climate Change Strategy (e.g. Certificate of Compliance (COC) or Final Site Determination (FSD)) showing no contamination within the subject site or an alternative notification from the Ministry confirming that the City may approve the owner's OCP amendment, development, subdivision, and demolition applications.
- 1.2. Road Dedication Requirements: Submission to the City of a contaminated sites legal instrument from the Ministry of Environment and Climate Change Strategy (e.g., COC or FSD) showing no contamination within the portion of the lands required to be dedicated to the City for road or an alternative form of assurance satisfactory to the City, in the City's sole discretion; which alternative assurance shall include, but may not be limited to, registration of a legal agreement on title to the lands requiring that:
  - 1.2.1. Prior to Building Permit\* issuance for the first building to be constructed on the lands (i.e. excluding existing buildings), in whole or in part, the owner shall submit:
    - a) Evidence that the environmental condition of the required road dedication is satisfactory, as determined at the sole discretion of the City; and
    - b) A contaminated sites legal instrument from the Ministry of Environment and Climate Change Strategy (e.g., COC or FSD) with respect to the required road dedication; and
  - 1.2.2. The owner will release and indemnify the City from and against any and all claims or actions that may arise in connection with any environmental contamination upon the lands, in whole or in part, including the required road dedication.

2. Subdivision: Registration of a Subdivision Plan to the satisfaction of the City.

*Prior to the registration of a Subdivision Plan, the following conditions shall be satisfied:*

- 2.1. Road Dedication: Dedication of 2,930.45 m<sup>2</sup> (31,542.6 ft<sup>2</sup>) for road and related purposes, as per the Preliminary Subdivision Plan (**Schedule A**), including:
  - 2.1.1. **Minoru Boulevard Widening:** "Road A", comprising 1,315.7 m<sup>2</sup> (14,162.1 ft<sup>2</sup>) in the form of a 3.2 m (10.5 ft.) wide strip of land along the subject site's entire Minoru Boulevard frontage, excluding the portion north of the Murdoch Avenue intersection, together with an additional 5.0 m (16.4 ft.) wide strip of land and 4.0 m by 4.0 m (13.1 ft. by 13.1 ft.) corner cuts at the Murdoch Avenue intersection; and
  - 2.1.2. **No. 3 Road Widening:** "Road D", comprising 1,614.7 m<sup>2</sup> (17,380.5 ft<sup>2</sup>) in the form of a 3.55 m (11.7 ft.) wide strip of land along the subject site's entire No. 3 Road frontage, together with an additional 5.0 m (16.4 ft.) wide strip of land and 4.0 m by 4.0 m (13.1 ft. by 13.1 ft.) corner cuts at the Cook Road intersection.

NOTE: The required Minoru Boulevard and No. 3 Road dedications shall not be used for density calculation purposes and are not eligible for Development Cost Charge (road acquisition) credits.

- 2.2. Lot Subdivision: The creation of three (3) lots for development purposes, as per the Preliminary Subdivision Plan (**Schedule A**), including:
- 2.2.1. **Lot 1 (West)**: 36,497.7 m<sup>2</sup> (392,858.0 ft<sup>2</sup>), including future “Road B”;
- 2.2.2. **Lot 2 (East)**: 30,434.4 m<sup>2</sup> (327,593.2 ft<sup>2</sup>), including future “Road C”; and
- 2.2.3. **Remainder Lot (North)**: 42,420.6 m<sup>2</sup> (456,611.5 ft<sup>2</sup>).
- 2.3. Coordination with Existing Uses & Structures:
- 2.3.1. **General Requirements**: Completion of requirements necessary to facilitate the owner’s proposed subdivision, as determined to the satisfaction of City of Richmond Building Approvals Division, which may include, but may not be limited to, registration of a restrictive covenant(s), registration of a blanket Statutory Right-of-Way(s), and/or submission of a Building Demolition Bond(s).
- 2.3.2. **Cross-Access**: Delivery of a registered cross-access easement(s) and/or other legal agreement(s), as determined to the satisfaction of the Director of Development, Director of Transportation, and the City Solicitor, over the internal drive-aisles, pedestrian circulation, utilities, and related linkages between Lot 1 (West), Lot 2 (East), and Remainder Lot (North), as applicable.
- 2.4. Future City Street: Measures to secure the lot-by-lot dedication of the Future City Street across Lot 1 (West) and Lot 2 (East) and related improvements, to the satisfaction of the City. The City agrees that the owner’s dedication of the Future City Street may occur after adoption of the subject OCP Amendment to facilitate the interim retention of the owner’s existing multi-storey parking structure and its lot-by-lot (phase-by-phase) demolition as part of necessary enabling works (i.e. clearing, excavating, and related site preparation) for the development of Lot 1 (West) and Lot 2 (East) respectively. Measures required to facilitate the proposed process shall include the following items, as determined to the satisfaction of the City.
- 2.4.1. **Demolition Covenant**: Registration of a restrictive covenant and blanket Statutory Right-of-Way (SRW) over Lot 1 (West) and Lot 2 (East) to ensure that the lot-by-lot demolition of the owner’s existing multi-storey parking structure is completed, at the sole cost of the owner, prior to the lot-by-lot issuance of any Building Permit\* for Lot 1 (West) or Lot 2 (East), in whole or in part, that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion. For clarity, demolition of the portion of the existing parking structure on:
- a) Lot 1 (West) shall occur prior to Building Permit\* issuance for Lot 1 (West); and
  - b) Lot 2 (East) shall occur prior to Building Permit\* issuance for Lot 2 (East).
- If the owner does not demolish the existing parking structure according to the provisions of the agreement, the covenant and SRW shall allow the City to enter the property and demolish the structure.
- 2.4.2. **Demolition Bond**: Provision of a Building Demolition Bond for the owner’s existing multi-storey parking structure located on Lot 1 (West) and Lot 2 (East), the value of which Building Demolition Bond shall be 105% of the estimated cost or as otherwise determined to the satisfaction of the City of Richmond Building Approvals Division.
- 2.4.3. **Public Rights of Passage**: Registration of a Statutory Right-of-Way (SRW) to provide for the establishment of the Future City Street between No. 3 Road and Minoru Boulevard, along the south side of Lot 1 (West) and Lot 2 (East), as per the Preliminary Statutory Right-of-Way Plan (**Schedule B**), together with an option for the City to dedicate the SRW area on a lot-by-lot basis (at a nominal cost to the City) following the demolition of the owner’s existing multi-storey parking structure on the subject site.

The SRW shall, as determined to the satisfaction of the City:

- a) Be at least 3,487.6 m<sup>2</sup> (37,540.2 ft<sup>2</sup>) in size, in the form of a 14.7 m (48.2 ft.) wide strip of land along the entire south edge of Lot 1 (West) and Lot 2 (East), together with 4.0 m by 4.0 m (13.1 ft. by 13.1 ft.) corner cuts at No. 3 Road and Minoru Boulevard, and shall include, as per the Preliminary Subdivision Plan (**Schedule A**) and Preliminary Statutory Right-of-Way Plan (**Schedule B**):
  - i) Lot 1 (West): "Road B", comprising an area of 1,518.7 m<sup>2</sup> (16,347.2 ft<sup>2</sup>); and
  - ii) Lot 2 (East): "Road C", comprising an area of 1,968.9 m<sup>2</sup> (21,193.0 ft<sup>2</sup>);
- b) Provide for unrestricted, 24-hour-a-day, public access including, but not limited to, pedestrians (universally accessible), bicycles, emergency and service vehicles, and general purpose traffic, together with related uses, features, City and private utilities, and City bylaw enforcement, as typically required in respect to the design, construction, and operation of a public road, except as otherwise permitted through a City-approved Construction Traffic Management Plan;
- c) Prohibit building encroachments above, at, or below the finished grade of the SRW area;
- d) Require the owner to be solely responsible for maintenance of the SRW area;
- e) Require the owner to be solely responsible for design and construction of the SRW, as determined via the City's standard permitting\* and Servicing Agreement (SA)\* processes; and
- f) Restrict the City's ability to exercise its right to unrestricted public access until, on a lot-by-lot basis, demolition of the owner's existing multi-storey parking structure on the subject site is complete.

2.4.4. **No Development Covenant:** Registration of a restrictive covenant(s) on title to Lot 1 (West) and Lot 2 (East) securing that "no development" will be permitted and restricting Development Permit\* issuance, on a lot-by-lot basis, in whole or in part, for any Development Permit\* that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, until the following is complete to the satisfaction of the City:

- a) For Lot 1 (West), the Development Permit\* includes the "Road B" SRW area, complies with the SRW agreement, and, as applicable, satisfies requirements with respect to the developer's future dedication, design, and construction of the SRW area as City road; and
- b) For Lot 2 (East), the Development Permit\* includes the "Road C" SRW area, complies with the SRW agreement, and, as applicable, satisfies requirements with respect to the developer's future dedication, design, and construction of the SRW area as City road.

*NOTE: For clarity, site area for density calculation purposes for a Development Permit\* for:*

- Lot 1 (West) shall include "Road B"; and
- Lot 2 (East) shall include "Road C".

2.4.5. **No Build Covenant:** Registration of a restrictive covenant(s) on title to Lot 1 (West) and Lot 2 (East) securing that "no building" will be permitted and restricting Building Permit\* issuance, on a lot-by-lot basis, in whole or in part, for any Building Permit\* that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, until the following is complete to the satisfaction of the City:

- a) The developer must:
  - i) For Lot 1 (West), dedicate the "Road B" portion of the Future City Street; and
  - ii) For Lot 2 (East), dedicate the "Road C" portion of the Future City Street;

*NOTE: The dedication of "Road B" and "Road C" shall not be eligible for Development Cost Charge credits for road acquisition or construction purposes.*

- b) The developer must enter into a Servicing Agreement (SA)\* for the design and construction, at the developer's sole cost, of the Future City Street along the frontage of the applicable lot, including all related transportation, engineering, and parks works;
  - c) Prior to Building Permit\* issuance, all works identified via the SA\* with respect to the applicable lot must be secured via a Letter(s) of Credit, to the satisfaction of the Director of Development, Director of Engineering, Director of Transportation, and Director, Parks Services.
- 2.4.6. **No Occupancy Covenant:** All SA\* works identified by the City with respect to the Future City Street shall be completed prior to final Building Permit\* inspection granting occupancy for the first building, in whole or in part, on the applicable lot (excluding parking and commercial uses that can be accessed directly from the inside of the existing shopping centre) or as otherwise determined at the sole discretion of the City (i.e. via the Development Permit\*, Building Permit\*, and/or SA\* processes) and specifically provided for via "no build" covenant(s) and/or other legal agreement(s) registered on title.
- 2.4.7. **East-West Connectivity During Construction:** Registration of a restrictive covenant and blanket Statutory Right-of-Way (SRW) over Lot 1 (West) and Lot 2 (East) to ensure that a publicly-accessible route for vehicles and pedestrians is provided and maintained, at the developer's sole cost, providing continuous public access (with limited temporary interruptions) between Minoru Boulevard and No. 3 Road to the south of the retail portion of the existing CF Richmond Centre mall throughout pre-construction, construction, and post-construction stages, as determined to the City's satisfaction.
- a) The required east-west vehicle and pedestrian connectivity shall provide for two (2) vehicle travel lanes, designed and operated to provide for simultaneous two-way traffic movements in a form consistent with City standards, together with a designated, safe, universally-accessible path for pedestrians with a minimum clear width of at least 1.5 m (4.9 ft.). (Note that the vehicle and/or pedestrian route may vary over the course of their operation to accommodate various construction-related activities, provided that such changes do not compromise required connectivity and are pre-approved by the City.)
  - b) "No building" will be permitted, restricting Building Permit\* issuance, on a lot-by-lot basis, in whole or in part, until the developer submits a Construction Traffic Management Plan that provides for the required east-west vehicle and pedestrian connectivity, to the City's satisfaction. The Plan shall include, among other things, strategies for maintaining safe, continuous operation of the required access throughout all stages of construction, except in the case of emergencies, temporary interruptions pre-approved by the City through the Plan, or, in the case of other temporary interruptions, with the written pre-approval of the City.
- 2.4.8. **Discharge:** Discharge of the agreement(s) may occur on a lot-by-lot basis upon the lot-by-lot completion of the Future City Street and Private (SRW) Streets, as determined to the City's satisfaction.
- 2.5. **Statutory Right-of-Way (SRW) – Public Rights of Passage:** Registration of Statutory Right-of-Ways (SRW), as per the Preliminary Statutory Right-of-Way Plan (**Schedule B**), to facilitate public access and open space uses, together with related landscaping and infrastructure (which may include, but may not be limited to, vehicle travel lanes, parking, bike facilities, street furnishings, street lighting, decorative paving, trees and plant material, public art, special mobility features, recreation amenities, innovative storm water management measures, and City utilities), to the satisfaction of the City. The specific location, configuration, design, and related terms of the SRWs shall be confirmed via the development's Development Permit\*, Servicing Agreement\*, and/or other City approval processes, to the satisfaction of the City, taking into account the following items.

Any works essential for public access within the required SRW areas are to be included in the Servicing Agreement\*. The SRW agreement must clearly describe responsibilities with respect to maintenance and

liability. Moreover, the design of the SRW areas must be prepared in accordance with good engineering practice with the objective of optimizing public safety. After completion of the SRW works, the owner is required to provide a certificate of inspection for the works or equivalent, prepared and sealed by the owner's engineer, architect, and/or landscape architect, as determined to the City's satisfaction, in a form and content acceptable to the City, certifying that the works have been constructed and completed in accordance with the accepted design.

Prior to OCP Amendment application adoption, the agreements shall be registered as blanket SRWs (accompanied by sketch plans) and shall include provisions for replacement agreements at Development Permit\*, Building Permit\*, and/or occupancy, as determined to the satisfaction of the City, at the owner's cost, for the purpose of accurately reflecting the City-approved permits and replacing the sketch plans with survey plans (which may be volumetric).

**2.5.1. General SRW Requirements:**

- a) The right-of-ways shall provide for:
  - i) 24 hour-a-day, universally accessible, public access in the form of vehicle route(s), paved walkway(s), off-street bike path(s), and/or related landscape features, which may include, but may not be limited to, lighting, furnishings, street trees and planting, decorative paving, and storm water management measures, to the satisfaction of the City;
  - ii) Public art;
  - iii) Public access to fronting commercial, residential, public open space, and other on-site uses;
  - iv) Emergency and service vehicle access, City bylaw enforcement, and any related or similar City-authorized activities;
  - v) City utilities, such as streetlights, traffic control infrastructure (e.g., signals, detector loops, equipment kiosks), and related and/or similar features;
  - vi) The owner-developer's ability to close a portion of the SRW area to public access to facilitate maintenance, repairs, or construction to the SRW area or the fronting uses, provided that adequate public access is maintained and the duration of the closure is limited, as determined through the applicable Development Permit\* process and specified in the SRW agreement(s) or approved by the City in writing in advance of any such closure;
  - vii) The owner-developer's ability to close a portion of the SRW area to public access for the purpose of hosting special events, provided that adequate public access is maintained and the duration of the closure is limited, as determined through the applicable Development Permit\* process and specified in the SRW agreement(s) or approved by the City in writing in advance of any such closure;
  - viii) Design and construction of the SRW area, via a Servicing Agreement\* (undertaken in coordination with a Development Permit\*), at the sole cost and responsibility of the developer, as determined to the satisfaction of the City;
  - ix) Maintenance of the SRW area at the sole cost of the owner-developer, except for City utilities and any other City property to be maintained by the City following the expiry of the Servicing Agreement\* maintenance period;
  - x) Existing site features (e.g., parking, driveways, signage, utilities, furnishings) where such features are not required to be removed or altered through an approved Development Permit\*, Servicing Agreement\*, and/or other City approval process; and
  - xi) Encroachments, provided that such features do not conflict with the design, construction, operation, or intended quality or public amenity of the right-of-way area (e.g., tree planting, accessible grades, underground utilities) or, as applicable, potential future road dedication, as determined to the satisfaction of the City, and the encroachments are included in a Development Permit\*, Servicing Agreement\*,

and/or other permit approved by the City and specified in the applicable SRW agreement(s), including:

- Permanent encroachments in the form of:
  - Parking concealed below the finished grade of the SRW area;
  - Driveway crossings;
  - Weather protection, architectural appurtenances, and building projections, typically located at least 2.5 m (8.2 ft.) clear above the finished grade of the SRW area; and
  - Signage; and
- Temporary encroachments in the form of:
  - Outdoor restaurants (e.g., food trucks, coffee kiosks, café seating);
  - Commercial uses (e.g., pop-up shops, sidewalk sales); and
  - Special event and recreation features (e.g., amusement rides, tents and shelters, event signage); and
  - Movable furnishings, planters, displays, railings, partitions, and similar features.

*NOTE: Outdoor space(s) designated for the exclusive year-round use of restaurant and/or commercial use(s) shall not be considered to be a "temporary encroachment(s)" and will not be permitted within the SRW area.*

- b) "No development" shall be permitted, on a lot-by-lot basis, on Lot 1 (West), Lot 2 (East), or Remainder Lot (North) that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, restricting Development Permit\* issuance for any such building on the lot, in whole or in part, unless the Development Permit\* and Servicing Agreement\* include the design of the SRW area, to the City's satisfaction.
- c) No Building Permit\* shall be issued, on a lot-by-lot basis, for a building on Lot 1 (West), Lot 2 (East), or Remainder Lot (North) that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, in whole or in part (excluding parking intended as an ancillary use to non-parking uses), unless the permit includes the design of the SRW area, to the City's satisfaction.
- d) "No occupancy" shall be permitted, on a lot-by-lot basis, for a building on Lot 1 (West), Lot 2 (East), or Remainder Lot (North) that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, restricting final Building Permit\* inspection granting occupancy for any such building on the lot, in whole or in part except:
- i) For Lot 1 (West), parking and commercial uses that can be directly accessed from the inside of the existing shopping centre; and
  - ii) For Lot 2 (East), parking,
- until the SRW area is completed to the satisfaction of the City, the owner has provided a certificate of inspection for the works or equivalent, prepared and sealed by the owner's engineer, architect, and/or landscape architect, as determined to the City's satisfaction, in a form and content acceptable to the City, certifying that the works have been constructed and completed in accordance with the accepted design, and ~~has received, as applicable,~~ if required by the City, a Certificate of Completion and/or final Building Permit\* inspection granting occupancy have been issued.

#### 2.5.2. **Private Streets:**

- a) **Park Road, Minoru Gate & New North-South Street:** At least 10,038 m<sup>2</sup> (108,047 ft<sup>2</sup>), in the form of an irregular, linear strip with a minimum width of 18.0 m (59.1 ft.), for the purpose of seamlessly extending the City road network between Minoru Boulevard and No. 3 Road to facilitate unrestricted public access (as if this was a City road), together with related

landscaping, street furnishings and lighting, City utilities, and other features as determined to the satisfaction of the City. Permitted encroachments shall be confirmed, on a lot-by-lot basis, through the Development Permit\* and Servicing Agreement\* approval processes for Lot 1 (West) and Lot 2 (East).

- b) **Cook Road:** At least 1,395 m<sup>2</sup> (15,016 ft<sup>2</sup>), in the form of a linear strip with a minimum width of 31.0 m (101.7 ft.), for the purpose of seamlessly extending Cook Road west of No. 3 Road to facilitate unrestricted public access (as if this was a City road), together with related landscaping, street furnishings and lighting, City utilities, and other features as determined to the satisfaction of the City. Permitted encroachments shall be limited to weather protection, architectural appurtenances, building projections, and temporary encroachments within the sidewalk portion of the SRW area, the specifics of which shall be confirmed through the Development Permit\* and Servicing Agreement\* approval processes for Lot 2 (East).

*NOTE: Cook Road shall be designed and constructed to City standards, to the satisfaction of the City, to facilitate its future potential dedication as a City road. In addition, prior to OCP amendment bylaw adoption, a covenant shall be registered on title to the Remainder Lot (North) securing the owner's commitment to dedicate the Cook Road SRW area prior to any future subdivision of the lot and/or issuance of a Development Permit\* for the lot, in whole or in part, that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion.*

- c) **Murdoch Road:** At least 1,422 m<sup>2</sup> (15,308 ft<sup>2</sup>), in the form of a linear strip with a minimum width of 25.0 m (82.0 ft.), for the purpose of seamlessly extending Murdoch Avenue east of Minoru Boulevard to facilitate unrestricted public access (as if this was a City road), together with related landscaping, street furnishings and lighting, City utilities, and other features as determined to the satisfaction of the City. Permitted encroachments shall be limited to parking below finished grade, weather protection, architectural appurtenances, building projections, and temporary encroachments within and a 2.5 m (8.2 ft.) wide strip along the north and south sides of the SRW area, the specifics of which shall be confirmed through the Development Permit\* and Servicing Agreement\* approval processes for Lot 1 (West).

*NOTE: The central 20.0 m (65.6 ft.) wide portion of Murdoch Avenue (that is unencumbered by permitted encroachments) shall be designed and constructed to City standards, to the satisfaction of the City, to facilitate its future potential dedication as a City road. In addition, prior to OCP amendment bylaw adoption, a covenant shall be registered on title to the Remainder Lot (North) securing the owner's commitment to dedicate the central 20.0 m (65.6 ft.) wide portion of the Murdoch Avenue SRW area prior to any future subdivision of the lot and/or issuance of a Development Permit\* for the lot, in whole or in part, that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion.*

### 2.5.3. **Sidewalk Widening:**

- a) **Minoru Boulevard:** At least 804 m<sup>2</sup> (8,654 ft<sup>2</sup>), in the form of a linear strip with a minimum width of 2.5 m (8.2 ft.), for the purpose of a City sidewalk, together with related landscaping, street furnishings and lighting, City utilities, and other features as determined to the satisfaction of the City. Permitted encroachments shall be limited to parking below finished grade, weather protection, architectural appurtenances, and building projections, the specifics of which shall be confirmed through the Development Permit\* and Servicing Agreement\* approval processes for Lot 1 (West).
- b) **Future City Street:** At least 102 m<sup>2</sup> (1,094 ft<sup>2</sup>), in the form of a linear strip with a minimum width of 0.5 m (1.6 ft.), for the purpose of a City sidewalk, together with related landscaping, street furnishings and lighting, City utilities, and other features as determined to the satisfaction of the City. Permitted encroachments shall be limited to parking below finished grade, weather protection, architectural appurtenances, and building projections, the specifics of which shall be

confirmed, on a lot-by-lot basis, through the Development Permit\* and Servicing Agreement\* approval processes for Lot 1 (West) and Lot 2 (East).

2.5.4. **Park Road Plaza:** At least 1,996.0 m<sup>2</sup> (0.5 acres), in the form of an irregular area fronting Park Road along approximately 50% of its perimeter, for the purpose of public open space uses, together with related landscaping, street furnishings and lighting, City utilities, mobility hub, and other features as determined to the satisfaction of the City. Permitted encroachments shall be confirmed, on a lot-by-lot basis, through the Development Permit\* and Servicing Agreement\* approval processes for Lot 1 (West) and Lot 2 (East).

2.6. **Statutory Right-of-Way (SRW) – Canada Line Connectivity Improvements:** Registration of a statutory right-of-way(s) on Remainder Lot (North), 6253 No. 3 Road, and 6060 Minoru Boulevard, together with restrictive covenants on Lot 1 (West), Lot 2 (East), and Remainder Lot (North) and/or other legal agreement(s) or measures, as determined to the satisfaction of the Director of Development, Director of Transportation, and the City Solicitor, for the purpose of securing the owner's commitment to improving public access across the owner's property to improve public pedestrian access to/from the Canada Line and proposed bus mall along No. 3 Road.

2.6.1. **No. 3 Road Sidewalk Widening:** A linear strip with a minimum width of 3.55 m (11.7 ft.) along the entire No. 3 Road frontage of 6253 No. 3 Road and 6060 Minoru Boulevard for the purpose of a City sidewalk, together with related landscaping, street furnishings and lighting, City utilities, and other features as determined to the satisfaction of the City. The SRW area shall provide for public access and related activities and uses generally as per a City street (as generally set out in the "General SRW Requirements" in the previous section).

Prior to OCP Amendment application adoption, registration of this SRW agreement(s) shall include a survey plan(s).

- a) Permitted encroachments shall be confirmed through the Servicing Agreement\* and related permit\* approval processes required with respect to the development of Lot 1 (West) and may include, but may not be limited to, signage.
- b) Implementation of the required public access shall be completed via the City's standard Servicing Agreement\* process, to the satisfaction of the City, prior to final Building Permit inspection granting occupancy of the first building on Lot 1 (West) (excluding parking and commercial uses that can be directly accessed from the inside of the existing shopping centre).

*NOTE: The SRW agreement shall have no financial or other impacts on the City with respect to the terms of the existing lease over the City-owned lot at 6253 No. 3 Road.*

2.6.2. **Cross-Mall Public Pedestrian Access:** A continuous route across the Remainder Lot (North), providing convenient, universal, public pedestrian access, during transit operating hours within 400 m (1,312.3 ft.) of the subject site, between the Murdoch Avenue SRW area and No. 3 Road (at the signalized pedestrian crossing at the bus mall), which route shall include passage through the owner's existing retail building and across the outdoor spaces surrounding the existing retail building (e.g., surface parking lots and walkways) via a generally weather protected route, as determined to the satisfaction of the City.

Prior to OCP Amendment application adoption, this agreement shall be registered as blanket SRW accompanied by a sketch plan.

- a) Maintenance of the SRW area shall be at the sole cost of the owner-developer.
- b) Encroachments shall be permitted, provided that they do not conflict with public access, as determined to the mutual satisfaction of the City and the owner as set out in the SRW.
- c) Implementation of the required public access shall be completed in two stages:
  - i) **Interim Connection:** Prior to final Building Permit inspection granting occupancy of the first building on Lot 1 (West), the required public access shall be complete, EXCEPT that the outdoor portion between the existing retail building and No. 3

Road shall be permitted to be in an interim form to coordinate with the owner's temporary sales centre; which interim form shall be confirmed, to the City's satisfaction, through the Lot 1 (West)/Phase 1 Development Permit\* and related Servicing Agreement\*; and

- ii) Ultimate Connection: Prior to final Building Permit inspection granting occupancy of the first building on Lot 2 (East), the required public access shall be completed in its final form, which shall be confirmed to the City's satisfaction through the Lot 2 (East)/Phase 2 Development Permit\* and related Servicing Agreement\*.

- 2.7. Statutory Right-of-Way (SRW) – City Utilities: Registration of right-of-ways for the purpose of securing City utilities, together with the City's ability to access, install, replace, alter, remove, operate, and maintain such utilities and related features, all as determined to the satisfaction of the City.

Prior to OCP Amendment application adoption, the agreements may be registered as blanket SRWs (which may be accompanied by sketch plans) and shall include provisions for replacement agreements at Development Permit\*, Building Permit\*, and/or occupancy, as determined to the satisfaction of the City, at the owner's cost, for the purpose of accurately reflecting the City-approved permits and attaching survey plans.

- 2.7.1. ***Parkade Driveway Traffic Signal Infrastructure (Minoru Boulevard)***: Traffic signal infrastructure (e.g., signal poles, lights, detector loops, and traffic signal kiosks) and related features on Lot 1 (West) in the vicinity of the developer's proposed Minoru Boulevard parkade driveway;
- 2.7.2. ***Existing Sanitary Sewer (Minoru Boulevard)***: The existing City sanitary sewer serving the existing CF Richmond Centre mall, in the form of a 6.0 m (19.7 ft.) wide strip of land generally extending the length of the existing sewer line, which right-of-way shall be discharged (at the developer's sole cost) upon the developer's removal of the existing sewer and the installation of new (replacement) City services in an alternative location, together with the registration of right-of-ways and/or other legal agreements, as required to accommodate the subject development and existing mall; and
- 2.7.3. ***Additional City Utilities (No. 3 Road)***: An additional utility SRW on Lot 2 (East) and Remainder Lot (North) to facilitate the developer's installation, at the developer's sole cost, of a new City sanitary sewer along approximately 330 m (1,083 ft.) of the lots' No. 3 Road frontages, as determined to the satisfaction of the City. The SRW area may include the required sanitary sewer, other City utilities, and/or related features, as determined to the City's sole satisfaction, to provide for the developer's installation of the required sanitary sewer. For clarity, as determined to the City's satisfaction the SRW agreement shall include, among other things:
  - a) ***No Development Covenant***: Registration of a restrictive covenant(s) on title to Lot 2 (East) and Remainder Lot (North) securing that "no development" will be permitted and restricting Development Permit\* issuance, in whole or in part, for any Development Permit\* that includes any residential use, increase in gross leasable floor area on the lot, and/or structure (including underground parking) along the No. 3 Road frontage of one or both lots, as determined in the City's discretion, until the blanket SRW is replaced with a survey plan (registered on both lots), to the satisfaction of the Director of Engineering; and
  - b) ***No Build Covenant***: Registration of a restrictive covenant(s) on title to Lot 2 (East) and Remainder Lot (North) securing that "no building" will be permitted and restricting Building Permit\* issuance, in whole or in part, for any Building Permit\* that includes any residential use, increase in gross leasable floor area on the lot, and/or structure (including underground parking) along the No. 3 Road frontage of one or both lots, as determined in the City's discretion, until the developer enters into a Servicing Agreement\* for the design and construction of the City utilities (on both lots), to the satisfaction of the City Director of Engineering.

- 2.8. Driveway Crossings: Registration of a restrictive covenant(s) and/or alternative legal agreement(s) on title to limit vehicle access to/from the subject site along City-owned streets. Requirements shall be confirmed to the satisfaction of the City, on a lot-by-lot basis, prior to Development Permit\* and Servicing Agreement\* issuance.
- 2.8.1. *Lot 1 (West)*: Six (6) driveway crossings, including along:
- a) Minoru Boulevard: Three (3) permanent crossings, including those at the Murdoch Avenue SRW, Minoru Gate SRW, and one on-site parking access between Murdoch Avenue and Minoru Gate, and one (1) interim crossing at the Future City Street SRW; and
  - b) Future City Street: Two (2) crossings, including the North-South Street SRW and one on-site parking access;
- 2.8.2. *Lot 2 (East)*: Three (3) driveway crossings, including along:
- a) No. 3 Road: One (1) permanent crossing at the Park Road SRW and one (1) interim crossing at the Future City Street SRW; and
  - b) Future City Street: One (1) on-site parking access; and
- 2.8.3. *Remainder Lot (North)*: Four (4) driveway crossings, including:
- a) Minoru Boulevard: Two (2) crossings, including one (1) at the Murdoch Avenue SRW and one (1) on-site parking access; and
  - b) No. 3 Road: Two (2) crossings, including one (1) at the Cook Road SRW and one (1) on-site parking access.
- 2.9. No Separate Sale: Registration of legal agreements on title on Lot 1 (West), Lot 2 (East), and the Remainder Lot (North), as per the Preliminary Subdivision Plan (**Schedule A**), requiring that the lots may not be sold or otherwise transferred separately without prior approval of the City, to ensure that legal agreements and business terms related to financial, legal, development, and other obligations assigned to each lot as a result of the subject OCP Amendment application are transferred and secured to the satisfaction of the Director of Development and the City Solicitor. The City acknowledges that (i) a limited partnership for each lot will be created to facilitate the funding/financing of the development; (ii) following the initial subdivision, each lot will be transferred to a related limited partnership; (iii) following the registration of an airspace subdivision for the applicable lot, the remainder will be transferred back to the owners of the enclosed shopping centre; and (iv) one or more nominees may be used as registered owners in connection with the aforementioned transfers. The City approves in advance the noted transfers and the developer will cause each new owner to assume the legal agreements and obligations in respect of the applicable lot(s).
3. Affordable Housing: The City's acceptance of the developer's offer to voluntarily contribute affordable housing, in the form of low-end market rental (LEMR) units, constructed to a turnkey level of finish on Lot 1 (West) and Lot 2 (East) at the sole cost of the developer, the terms of which voluntary contribution shall include, but will not be limited to, the registration of the City's standard Housing Agreement and Covenant on title to each lot to secure the affordable housing units. The form of the Housing Agreements and Covenants shall be agreed to by the developer and the City prior to final adoption of the subject OCP Amendment application; after which time, only the Housing Covenants may be amended or replaced and any such changes will only be permitted for the purpose of accurately reflecting the specifics of the Development Permit\* for Lot 1 (West) and Lot 2 (East) and other non-materials changes resulting thereof and made necessary by the Lot 1 (West) and Lot 2 (East) Development Permit\* approval requirements, as determined to the satisfaction of the Director of Development and Manager of Community Social Development. The terms of the Housing Agreements and Covenants shall indicate that they apply in perpetuity and provide for, but will not be limited to, the requirements set out in **Schedule C**.
4. District Energy Utility (DEU): Registration of a restrictive covenant and statutory right of way and/or alternative legal agreement(s) on title to Lot 1 (West) and Lot 2 (East), to the satisfaction of the City, securing the owner's commitment to connect to District Energy Utility (DEU) and granting the statutory right of way(s) necessary for

supplying the DEU services to the building(s), which covenant and statutory right of way and/or legal agreement(s) will include, at minimum, the terms and conditions set out in **Schedule D**.

5. No Development Omnibus: Registration of a restrictive covenant(s) and/or alternative legal agreement(s) on title to Lot 1 (West) and Lot 2 (East) securing that “no development” will be permitted and restricting Development Permit\* issuance, on a lot-by-lot basis, in whole or in part, for any Development Permit\* that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion, (together with various Building Permit\* and occupancy restrictions, as determined to the satisfaction of the City), until the following is complete to the satisfaction of the City:
  - 5.1. Development Staging: Development of Lot 1 (West) and Lot 2 (East) shall comprise a maximum of two (2) stages or phases (i.e. one per lot), the comprehensive design and development of which shall be approved through two (2) Development Permits\* (i.e. one for each lot), unless otherwise determined to the satisfaction of the Director of Development. Moreover:
    - 5.1.1. Development Permit\* issuance for the entirety of Lot 1 (West), shall:
      - a) Comprise a single Development Permit\*, generally as per the Lot 1 Development Permit (DP) Scope Diagram (**Schedule M**) (exclusive of Development Permits that do not include any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion);
      - b) Include, among other things, Canada Line Connectivity Improvements (e.g., No. 3 Road Sidewalk Widening, Cross-Mall Public Pedestrian Access “Interim Connection”, pedestrian crossing improvements at the No. 3 Road/Bus Mall intersection); and
      - c) Occur prior to Development Permit\* issuance for the first building on Lot 2 (East);
    - 5.1.2. Development Permit\* issuance for the entirety of Lot 2 (East), which shall:
      - a) Comprise a single Development Permit\* (exclusive of Development Permits that do not include any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion); and
      - b) Include, among other things, Canada Line Connectivity Improvements (e.g., Cross-Mall Public Pedestrian Access “Ultimate Connection”);
    - 5.1.3. Building Permit\* issuance for the entirety of Lot 1 (West) (exclusive of Building Permits that do not include any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion), which may include multiple Building Permits\*, shall occur prior to issuance of the first Building Permit\* for Lot 2 (East);
    - 5.1.4. Final Building Permit(s)\* inspection granting occupancy for the entirety of Lot 1 (West) (exclusive of Building Permits that do not include any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion) shall occur prior to final Building Permit\* inspection granting occupancy for the first building, in whole or in part, on Lot 2 (East); and
    - 5.1.5. Notwithstanding the above, the City will permit occupancy of the building on Lot 1 (West) and/or Lot 2 (East) to proceed in stages (e.g., tower-by-tower), provided that "no occupancy" shall be permitted of any stage except as expressly provided for with legal agreements registered on title and other measures (e.g., security), for the purpose of ensuring that the completion of affordable housing, publicly-accessible streets and open spaces, residential amenities, City utilities, public art, parking, end-of-trip facilities, mobility hubs, off-site transportation improvements, and other features are appropriately coordinated with the completion of the developer’s market residential and non-residential uses, as determined to the satisfaction of the Director of Development, Director of Transportation, Director, Parks Services, Director of Arts, Culture, and Heritage, Manager of Community Social Development, Manager of Environmental Sustainability, Director of Engineering, and City Solicitor.

- 5.2. Remainder Lot (North): “No development” shall be permitted on the Remainder Lot (North), restricting Development Permit\* issuance for any building on the lot, in whole or in part, that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion, unless, as determined to the sole satisfaction of the City:
- 5.2.1. The Development Permit\* and any related permit(s) include the design of any required SRW area on the lot, to the City’s satisfaction;
  - 5.2.2. The owner provides road dedications in compliance with the Murdoch Avenue and Cook Road SRW agreements, as determined to the City’s satisfaction; and
  - 5.2.3. The required “Canada Line Connectivity Improvements” are complete or as otherwise determined to the City’s satisfaction.
- 5.3. Servicing Agreement (SA)\* Requirements:
- 5.3.1. Prior to Building Permit\* issuance for the first building to be constructed on a lot (that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion), in whole or in part, the owner shall:
- a) For Lot 1 (West), enter into Servicing Agreement #1\* for the design and construction, at the developer’s sole cost, of full upgrades across the Lot 1 (West) street frontages, together with other engineering, transportation, and parks works, as determined to the satisfaction of the City, which shall include, but shall not be limited to:
    - i) Road widening along Minoru Boulevard, together with various intersection improvements;
    - ii) Construction of the portion of the Future City Street along the south side of Lot 1 (West);
    - iii) Interim improvements with respect to Murdoch Avenue on the Remainder Lot (North);
    - iv) Canada Line connectivity enhancements in the form of frontage improvements across the Remainder Lot (North), 6253 No. 3 Road, and 6060 Minoru Boulevard;
    - v) Construction of the portion of Park Road and related private (SRW) streets located on Lot 1 (West);
    - vi) Construction of the Park Road Plaza; and
    - vii) Various utility upgrades; and
  - b) For Lot 2 (East), enter into Servicing Agreement #2\* for the design and construction, at the developer’s sole cost, of full upgrades across the Lot 2 (East) street frontages, together with other engineering, transportation, and parks works, as determined to the satisfaction of the City, which shall include, but shall not be limited to:
    - i) Road widening along No. 3 Road, together with various intersection improvements;
    - ii) Construction of the portion of the Future City Street along the south side of Lot 2 (East);
    - iii) Construction of Cook Road on the Remainder Lot (North);
    - iv) No. 3 Road frontage improvements across Lot 2 (East) and the Remainder Lot (North);
    - v) Construction of the portion of Park Road and related private (SRW) streets located on Lot 2 (East); and
    - vi) Pump station improvements and various utility upgrades.
- 5.3.2. Except as expressly provided for and in compliance with the subject development’s approved “*Development Staging*”, related legal agreement(s), and security, to the satisfaction of the Director of Development, Director of Transportation, Director, Parks Services, and Director of Engineering:
- a) Prior to Building Permit\* issuance, all Servicing Agreement (SA)\* works must be secured via a Letter(s) of Credit;

- b) Except as expressly determined in the sole discretion of the City and secured with legal agreement(s) registered on title to the lot(s), all works shall be completed, on a stage-by-stage (phase-by-phase) basis, prior to final Building Permit\* inspection granting occupancy of the first building in the stage (phase) (excluding parking intended as an ancillary use to non-parking uses), in whole or in part; and
  - c) Development Cost Charge (DCC) credits may apply.
- 5.3.3. Servicing Agreement (SA)\* works will include, but may not be limited to, the following:
- a) Parks: The developer shall be responsible, at the developer sole cost, for the design and construction of the Park Road Plaza SRW area, based on a developer-prepared/City-approved functional program for the plaza (completed as part of the Lot 1 (West) Development Permit\* design review process prior to preparation of the Development Permit\* staff report), as determined to the satisfaction of the Director of Development, Director, Parks Services, Director of Arts, Culture, and Heritage Services, and Director of Engineering. For clarity, the Parks SA\* works shall only include the Park Road Plaza, EXCEPT if otherwise determined by the Director of Development through the Development Permit\* process for Lot 1 (West) or Lot 2 (East);
  - b) Engineering Servicing: Requirements as set out in **Schedule E** and **Schedule F**; and
  - c) Transportation: Requirements as set out in **Schedule G** and the Preliminary Functional Road Plan (**Schedule H**).
- 5.4. City Centre “Parking Zone 1” & TDM Strategy Requirements: Prior to Development Permit\* issuance for Lot 1 (West) and Lot 2 (East), on a lot-by-lot basis, legal agreements shall be registered on title to Lot 1 (West), Lot 2 (East), and Remainder Lot (North) securing the developer’s voluntary commitment to provide, at the developer’s sole cost, various transportation-related improvements and transportation demand management (TDM) measures for the purpose of satisfying Zoning Bylaw requirements for reducing the development’s required parking rates (i.e. from CDT1 rates to Parking Zone 1 rates) and permitting a further parking reduction of up to 10% for the provision of TDM measures, as determined to the satisfaction of the Director of Transportation..
- 5.4.1. Actual parking rates shall be confirmed prior to Development Permit\* issuance, on a lot-by-lot basis, to the satisfaction of the Director of Transportation.
- NOTE: Required parking may be provided collectively (i.e. the required need may be determined and satisfied across two or more lots) provided that the affected parking facilities are located not more than 150 m (492 ft.) from any building or use being served and use of the parking facilities is secured with legal agreements to the satisfaction of the City.
- 5.4.2. The development’s required transportation-related improvements and TDM measures shall include, but may not be limited to those items set out in **Schedule I** and the Mobility Hub Vision (**Schedule J**).
- 5.5. Additional Development Requirements: Prior to Development Permit\* issuance for Lot 1 (West) and Lot 2 (East), on a lot-by-lot basis, the developer shall satisfy the following items, as set out in **Schedule K**, to the satisfaction of the City:
- 5.5.1. *NAV Canada Building Heights*;
  - 5.5.2. *Family-Friendly Housing Unit Mix*;
  - 5.5.3. *Public Art*;
  - 5.5.4. *Electric Vehicle (EV) Charging Infrastructure for Vehicles & “Class 1” Bicycle Storage*; and
  - 5.5.5. *Tree Removal and Replacement*.

5.6. Standard City Legal Requirements: Prior to Development Permit\* issuance for Lot 1 (West) and Lot 2 (East), on a lot-by-lot basis, the developer shall satisfy the following items, as set out in **Schedule L**, to the satisfaction of the City:

5.6.1. *Flood Construction Covenants*;

5.6.2. *Aircraft Noise Covenants*;

5.6.3. *Canada Line Covenants*;

5.6.4. *View Blockage & Other Development Impacts Covenants*; and

5.6.5. *Tandem Parking Covenants*.

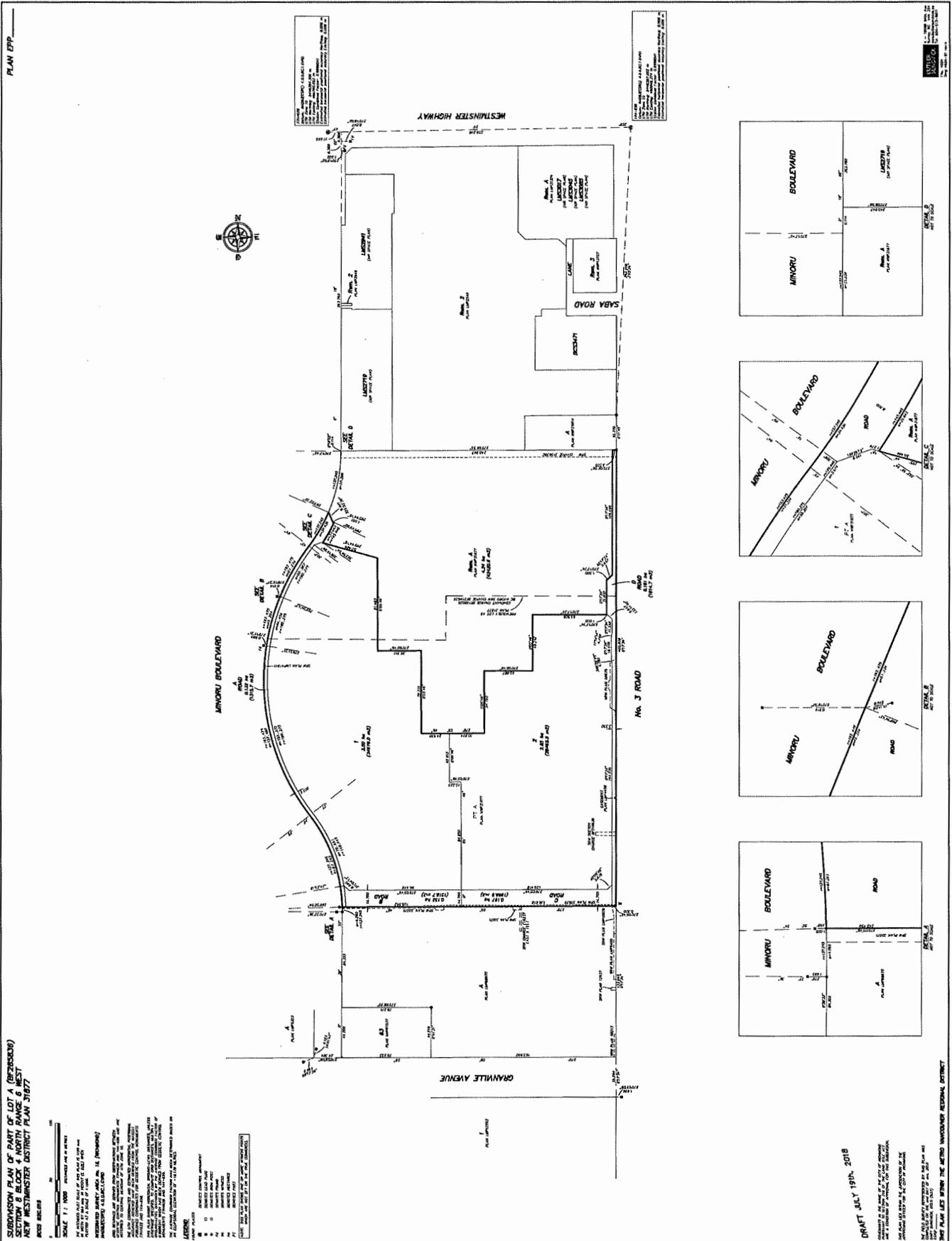
6. Development Permit\* Readiness for Lot 1 (Phase 1): The submission and processing of a Development Permit\* for Lot 1 (West), generally as per the Lot 1 Development Permit (DP) Scope Diagram (**Schedule M**), shall be completed to a level deemed acceptable by the Director of Development, which shall include, among other things, the non-redeveloping portion of the existing shopping centre (e.g., walls, roof, ground plane, landscape, and/or related features) where the City determines that its form and character will impact the character, quality, and/or livability of the redeveloping portion of the site due to, for example, its prominence along proposed private-owned, publicly-accessible streets.

**SIGNED COPY ON FILE**

\_\_\_\_\_  
Signed

\_\_\_\_\_  
Date

SCHEDULE A  
Preliminary Subdivision Plan



SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**SCALE** 1:1000

**DATE** 17 JULY 2018

**DRAWN BY** J. J. JONES

**CHECKED BY** J. J. JONES

**APPROVED BY** J. J. JONES

**DATE** 17 JULY 2018

**PROJECT** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**CLIENT** J. J. JONES

**PROJECT LOCATION** SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677

**PROJECT DESCRIPTION** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**PROJECT OBJECTIVES** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**PROJECT CONSTRAINTS** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**PROJECT RISKS** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

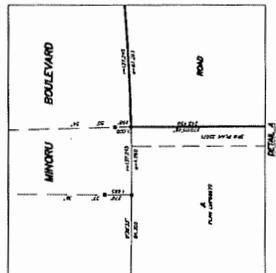
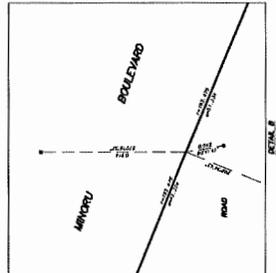
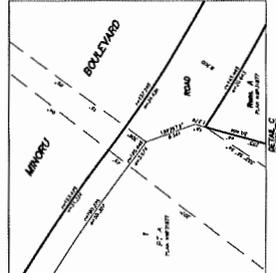
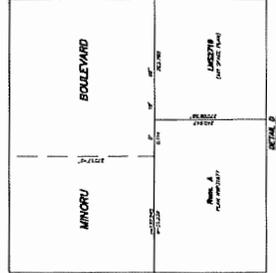
**PROJECT BENEFITS** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**PROJECT IMPACTS** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**PROJECT MONITORING** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

**PROJECT EVALUATION** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)

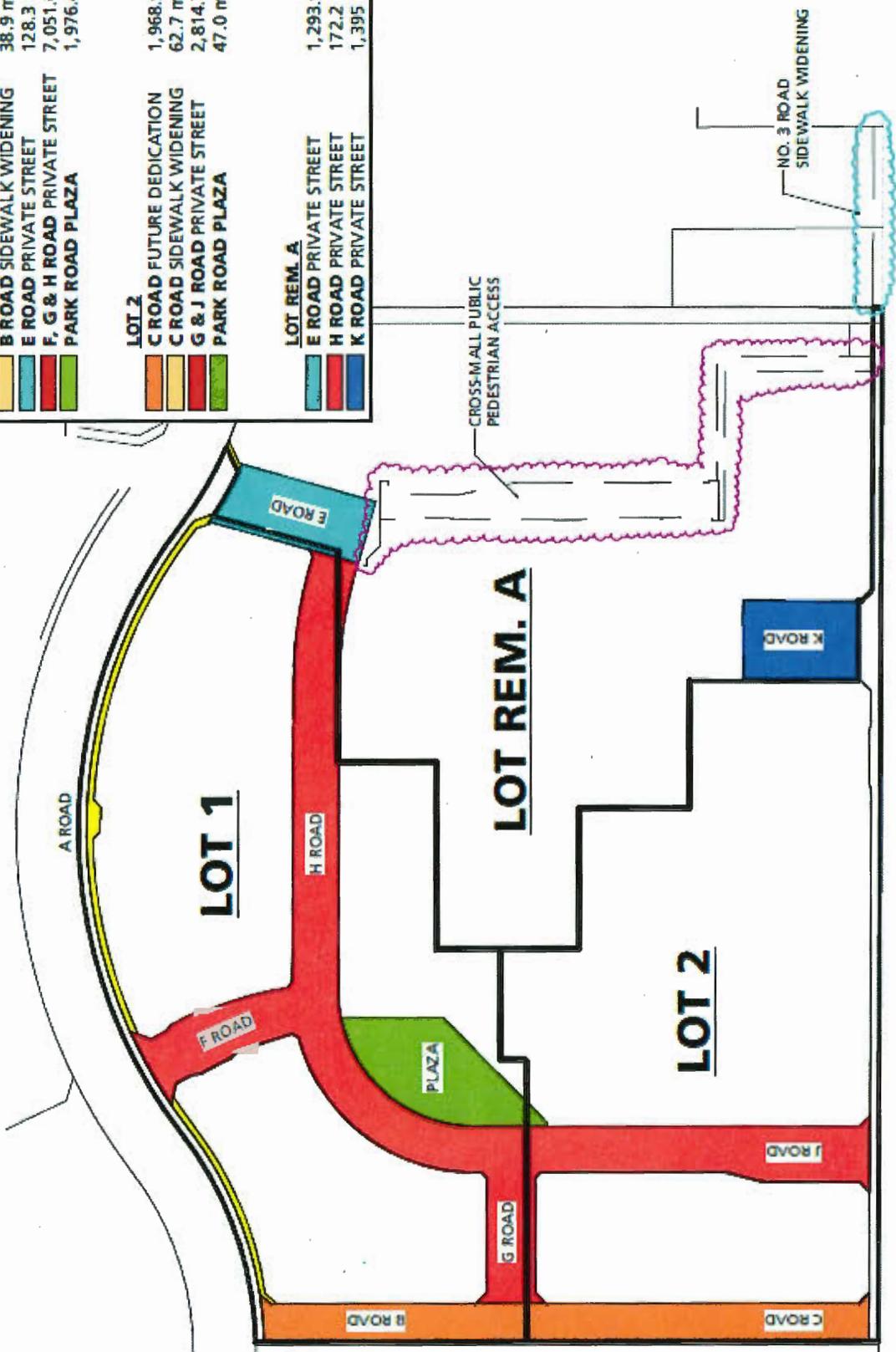
**PROJECT CLOSURE** SUBDIVISION PLAN OF PART OF LOT 1 (REFERRING TO SECTION 8 BLOCK 4 NORTH RANGE 6 WEST NEW WESTMINSTER DISTRICT PLAN 31677)



DRAFT JULY 19th, 2018

THIS PLAN LIES WITHIN THE METRO WINDSOR RESIDENTIAL DISTRICT

LOT 1	
A ROAD SIDEWALK WIDENING	804.0 m <sup>2</sup>
B ROAD FUTURE DEDICATION	1,518.7 m <sup>2</sup>
B ROAD SIDEWALK WIDENING	38.9 m <sup>2</sup>
E ROAD PRIVATE STREET	128.3 m <sup>2</sup>
F, G & H ROAD PRIVATE STREET	7,051.0 m <sup>2</sup>
PARK ROAD PLAZA	1,976.4 m <sup>2</sup>
LOT 2	
C ROAD FUTURE DEDICATION	1,968.9 m <sup>2</sup>
C ROAD SIDEWALK WIDENING	62.7 m <sup>2</sup>
G & J ROAD PRIVATE STREET	2,814.7 m <sup>2</sup>
PARK ROAD PLAZA	47.0 m <sup>2</sup>
LOT REM. A	
E ROAD PRIVATE STREET	1,293.9 m <sup>2</sup>
H ROAD PRIVATE STREET	172.2 m <sup>2</sup>
K ROAD PRIVATE STREET	1,395 m <sup>2</sup>



**Affordable Housing**  
Terms & Conditions

The City's acceptance of the developer's offer to voluntarily contribute affordable housing, in the form of low-end market rental (LEMR) units, constructed to a turnkey level of finish on Lot 1 (West) and Lot 2 (East) at the sole cost of the developer, the terms of which voluntary contribution shall include, but will not be limited to, the registration of the City's standard Housing Agreement and Covenant on title to each lot to secure the affordable housing units. The form of the Housing Agreements and Covenants shall be agreed to by the developer and the City prior to final adoption of the subject OCP Amendment application; after which time, only the Housing Covenants may be amended or replaced and any such changes will only be permitted for the purpose of accurately reflecting the specifics of the Development Permit\* for Lot 1 (West) and Lot 2 (East) and other non-materials changes resulting thereof and made necessary by the Lot 1 (West) and Lot 2 (East) Development Permit\* approval requirements, as determined to the satisfaction of the Director of Development and Manager of Community Social Development. The terms of the Housing Agreements and Covenants shall indicate that they apply in perpetuity and provide for, but will not be limited to, the following requirements.

*NOTE: In accordance with Richmond's Affordable Housing Strategy, effective July 24, 2017, the subject OCP amendment application shall be grandfathered under the City's built unit requirement of 5% of total residential building area on the basis that it was (i) submitted prior to July 24, 2017, and (ii) presented for consideration by Council prior to July 24, 2018 (i.e. April 9, 2018). For clarity, the developer's affordable contribution and the grandfathering of City's built unit (5%) requirement applies only to Lot 1 (West) and Lot 2 (East) and does not apply to any future development of Remainder Lot (North).*

1. **Stand-Alone Buildings & Non-Profit Operator:** The applicant has indicated to the City that it plans to pursue an agreement with a non-profit organization(s) to manage the development's required LEMR units on Lot 1 (West) and Lot 2 (East). To support this partnership, the City is willing to accept lot-by-lot clustering of the required units in the form of stand-alone buildings, together with the clustering of other building features intended for the exclusive use of the affordable housing tenants (e.g., parking, Class 1 bike storage, waste management features).
  - a) The affordable housing shall occupy two (2) stand-alone buildings, including:
    - i) One near the southeast corner of Lot 1 (West), fronting the Future City Street; and
    - ii) One near the northeast corner of Lot 2, fronting Cook Road.
  - b) Both stand-alone buildings shall be integrated with the development's underground parking structure, roof deck, and related features, but will be designed to function as independent buildings that do not share common circulation (e.g., lobbies, hallways, elevators, stairs) or indoor residential amenity spaces with the market-residential or commercial uses on Lot 1 (West) or Lot 2 (East).
  - c) The affordable housing shall be distributed such that a proportional share of the required habitable space for the affordable housing units will be located on each of Lot 1 (West) and Lot 2 (East).
2. **Minimum Required Floor Area:** The required minimum floor area of the affordable housing buildings, exclusive of parking, bike storage, and ancillary uses not intended for the exclusive use of the affordable housing occupants (e.g., visitor parking, waste management areas, any amenity spaces or other uses shared with the market residential dwelling occupants, landscaping) shall comprise the combined total area of the following, as determined to the satisfaction of the Director of Development and Manager of Community Social Services and set out in an approved Development Permit\*:
  - a) 5% of the subject development's total residential building area, calculated on a lot-by-lot basis, on Lot 1 (West) and Lot 2 (East), as specified in the Development Permit\* approved by the City for each lot, all of which area is to be allocated for the net floor area of the affordable housing dwelling units;
  - b) Circulation (e.g., lobbies, hallways, elevators, stairs) intended for the exclusive use of the affordable housing occupants;
  - c) Indoor amenity space within and around the affordable housing building, designed and secured for the exclusive use of the affordable housing occupants, the size of which space shall comply, on a lot-by-lot basis,

with standard City OCP and City Centre Area Plan (CCAP) policy as applicable to a “stand alone” building without access to amenities shared with another building; and

- d) All walls, mechanical, electrical, and similar spaces required to facilitate the owner’s provision of the proposed “stand alone” affordable housing building on each lot.

3. **Housing Requirements:** The developer shall, on a lot-by-lot basis, as generally indicated in the table below:

- a) Ensure that the types, sizes, rental rates, and occupant income restrictions for the affordable housing units are in accordance with the City’s Affordable Housing Strategy and guidelines for Low End Market Rental (LEMR) housing, unless otherwise agreed to by the Director of Development and Manager, Community Social Development; and
- b) Achieve the Project Targets for unit mix and Basic Universal Housing (BUH) standard compliance or as otherwise determined to the satisfaction of the Manager, Community Social Development through an approved Development Permit\*.

Unit Type	Minimum Unit Area	Maximum Monthly LEMR Unit Rent***	Total Maximum Household Income**	Project Targets	
				Unit Mix**	BUH Units*
Bachelor	400 ft2	\$811	\$34,650 or less	10%	100%
1-Bedroom	535 ft2	\$975	\$38,250 or less	30%	100%
2-Bedroom	741 ft2	\$1,218	\$46,800 or less	30%	100%
3-Bedroom	980 ft2	\$1,480	\$58,050 or less	30%	100%

\* BUH units means those units that are designed and constructed to satisfy the Zoning Bylaw’s Basic Universal Housing standards. (NOTE: The Zoning Bylaws permits a floor area exemption of 1.86 m<sup>2</sup> / 20 ft<sup>2</sup> per BUH unit.)

\*\* The unit mix will be confirmed to the satisfaction of the City, on a lot-by-lot basis, through the Development Permit\* processes for each lot. The recommended unit mix is indicated in the table; however, based on approved design, which may take into account non-profit housing operator input, the unit mix may be varied provided that at least 50% of total affordable housing units are some combination of “family friendly”, 2- and 3-bedroom units.

NOTE: The targeted unit mix is intended to apply to each lot on a stand-alone basis; however, the City, in its sole discretion, may apply the targeted unit mix to the comprehensive development of Lot 1(West) and Lot 2 (East) such that, for example, one lot may have a lesser percentage of family-friendly units and the other may have a higher percentage, provided that, as determined to the City’s satisfaction, through the Development Permit\* approval processes:

- A non-profit housing provider(s) is involved (e.g., memorandum of understanding);
- The Housing Covenant on each lot is revised to accurately reflect the specifics of the affordable housing units and ancillary spaces and uses, as per the approved Development Permit\* for each lot; and
- Additional legal agreement(s) are registered on title to the lot(s) to secure the developer’s commitment to the phased (lot-by-lot) implementation of City-approved unit mix across the comprehensive development of Lot 1(West) and Lot 2 (East).

\*\*\* Rate shall be adjusted periodically as provided for under adopted City policy.

- c) Occupants of the affordable housing units shall, on a lot-by-lot basis, to the satisfaction of the City (as determined prior to Development Permit\* approval), enjoy full and unlimited access to and use of all on-site indoor and outdoor amenity spaces provided with respect to the affordable housing building as per OCP, City Centre Area Plan, and Development Permit\* requirements, at no additional charge to the affordable housing tenants (i.e. no monthly rents or other fees shall apply for the casual, shared, or exclusive use of any amenities).
- d) On-site parking, “Class 1” bike storage, and related electric vehicle (EV) charging stations shall be provided, on a lot-by-lot basis, for the use of affordable housing occupants as per the OCP, Zoning Bylaw, and approved Development Permit\* at no additional charge to the affordable housing tenants (i.e. no monthly rents or other fees shall apply for the casual, shared, or exclusive use of the parking spaces, bike storage, EV charging stations, or related facilities by affordable housing tenants), which features may be secured via legal agreement(s) on title prior to Development Permit\* issuance or as otherwise determined to the satisfaction of the City. (For clarity, those occupants of the affordable units who utilize the vehicle EV charging stations may be required to pay for the cost of their utility usage, but not for their use of the EV charging equipment or associated parking.)

4. **Building Features:** The affordable housing units, related uses (e.g., parking, garbage/recycling, hallways, amenities, lobbies), and associated landscaped areas shall be completed, on a lot-by-lot basis, to a turnkey level of finish, at the sole cost of the developer, to the satisfaction of the Director of Development and Manager, Community Social Development. Building features shall include, but may not be limited to the following items.
- a) Indoor amenity space shall be provided, on a lot-by-lot basis, within and around the affordable housing buildings; which spaces shall be designed and secured for the exclusive use of the affordable housing occupants and satisfy standard City OCP and City Centre Area Plan (CCAP) policies with respect to minimum amenity size, which for clarity shall:
    - i) Be calculated based on a rate of at least 100 m<sup>2</sup> (1,076 ft<sup>2</sup>) per affordable housing building or 2.0 m<sup>2</sup> (21.5 ft<sup>2</sup>) per affordable housing unit, whichever is greater, for some combination of social, recreational, cultural, and/or educational purposes; and
    - ii) In addition to the above, include at least 19 m<sup>2</sup> (200 ft<sup>2</sup>) per building for as administrative (e.g., office) space for the use of the housing operator.
  - b) Outdoor residential amenity space shall be provided for the shared use of the affordable housing occupants, on a lot-by-lot basis, in compliance with standard City OCP and City Centre Area Plan (CCAP) policies (e.g., at least 6 m<sup>2</sup> / 65 ft<sup>2</sup> per affordable housing unit, together with additional landscaped space).
  - c) The affordable housing buildings, including their housing units and common areas (e.g., circulation, lobbies, indoor/outdoor amenity spaces, parking, bike storage, and waste management areas), shall be accessible to people with disabilities, in compliance with the BC Building Code or as otherwise determined to the satisfaction of the Manager of Community Social Development and Manager of Building Approvals.
  - d) The affordable housing buildings, including their common areas and housing units, shall be equipped with an audio/visual alarm systems.
5. “No development” shall be permitted on Lot 1 (West) or Lot 2 (East), restricting Development Permit\* issuance on a lot-by-lot basis for a building on Lot 1 (West) and Lot 2 (East), in whole or in part, that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City’s discretion (excluding parking intended as an ancillary use to non-parking uses), until the developer, to the City’s satisfaction:
- a) Submits, for consideration by the City, a memorandum of understanding with a non-profit operator demonstrating, among other things, support for the developer’s proposed clustered affordable housing unit arrangement and unit mix on the lot; and
  - b) Designs the lot to provide for the affordable housing units and ancillary spaces and uses;
  - c) Amends or replaces the Housing Covenant to accurately reflect the specifics of the affordable housing units and ancillary spaces and uses as per the approved Development Permit\*; and
  - d) As required, registers additional legal agreements on title to the lot(s) to facilitate the detailed design, construction, operation, and/or management of the affordable housing units and/or ancillary spaces and uses (e.g., parking) as determined by the City via the Development Permit\* review and approval processes.
6. No Building Permit\* shall be issued for a building on Lot 1 (West) or Lot 2 (East) that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, in whole or in part (excluding parking intended as an ancillary use to non-parking uses), until, on a lot-by-lot basis, the developer provides for the required affordable housing units and ancillary spaces and uses to the satisfaction of the City.
7. “No occupancy” shall be permitted on Lot 1 (West) or Lot 2 (East), restricting final Building Permit\* inspection granting occupancy for any residential uses on Lot 1 (East Lot) and /or Lot 2 (West Lot), in whole or in part (except for parking), until, on a lot-by-lot basis, the required affordable housing units and ancillary spaces and uses are completed to the satisfaction of the City and have received final Building Permit\* inspection granting occupancy.

**District Energy Utility (DEU)**  
Terms & Conditions

Registration of a restrictive covenant and statutory right of way and/or alternative legal agreement(s) on title to Lot 1 (West) and Lot 2 (East), to the satisfaction of the City, securing the owner's commitment to connect to District Energy Utility (DEU) and granting the statutory right of way(s) necessary for supplying the DEU services to the building(s), which covenant and statutory right of way and/or legal agreement(s) will include, at minimum, the following terms and conditions:

1. No Building Permit will be issued for a building on the subject site (excluding any commercial portions of the existing enclosed mall) unless;
  - a) the building is designed with the capability to connect to and be serviced by a DEU; and
  - b) the owner has provided an energy modelling report satisfactory to the Director of Engineering.
2. If a district energy utility service area bylaw which provides for owner construction of an energy generation plant (a "DEU Bylaw"), and which applies to the site, has been adopted by Council prior to the issuance of the development permit for the subject site, no building permit will be issued for a building on the subject site unless:
  - a) the owner designs, to utility grade specification and the satisfaction of the City and the City's DEU service provider, Lulu Island Energy Company Ltd. (LIEC), a low carbon energy plant(s) which provides a minimum 70% of space heating, space cooling and domestic hot water annual energy use from a renewable (non-carbon) energy source, to be constructed and installed on the site, with the capability for the low carbon energy plant(s) and the building side HVAC systems for the site (excluding any commercial portions of the enclosed mall) to connect to and be serviced by a DEU; and
  - b) the owner enters into an asset transfer agreement with the City and/or the City's DEU service provider on terms and conditions satisfactory to the City, which provides, without limitation:
    - i) that the owner will transfer ownership of the low carbon energy plant(s), the distribution piping system, and all other ancillary components on the subject site used to generate or convey space heating, space cooling and domestic hot water heating up to and including energy transfer stations, to the City or as directed by the City, including to the City's DEU service provider, at no cost to the City or City's DEU service provider, LIEC, on a date prior to final building inspection permitting occupancy of the first building on the site; and
    - ii) that the City and/or the City's DEU service provider will have final approval of all design elements, equipment specifications, construction inspections and work approvals for the low carbon energy plants.
3. The owner agrees that the building(s) (excluding any commercial portions of the enclosed mall) will connect to a DEU when a DEU is in operation, unless otherwise directed by the City and the City's DEU service provider, LIEC.
4. If a DEU is available for connection and the City has directed the owner to connect, no final building inspection permitting occupancy of a building will be granted unless, and until:
  - a) the building (excluding any commercial portions of the enclosed mall) is connected to the DEU;
  - b) the owner enters into a Service Provider Agreement for that building with the City and/or the City's DEU service provider, LIEC, executed prior to depositing any Strata Plan with LTO and on terms and conditions satisfactory to the City which provides, without limitation, that the City or the City's DEU service provider will be the exclusive provider of space heating and domestic hot water heating, and when available space cooling, services for the building (excluding any commercial portions of the enclosed mall), unless otherwise agreed to by the City Engineer and set out in the Service Provider Agreement; and
  - c) prior to subdivision (including Air Space parcel subdivision and Strata Plan filing, but excluding the subdivision to create the Lot 1(West) and Lot 2 (East)), the owner grants or acquires, and registers, all Statutory Right-of-Way(s) and/or easements necessary for supplying the DEU services to the building.

5. If a DEU is not available for connection, but a DEU Bylaw which applies to the site has been adopted by Council prior to the issuance of the development permit for the subject site, no final building inspection permitting occupancy of a building will be granted unless and until:
  - a) the City receives a professional engineer's certificate stating that the building (excluding any commercial portions of the enclosed mall) has the capability to connect to and be serviced by a DEU;
  - b) the building (excluding any commercial portions of the enclosed mall ) is connected to a low carbon energy plant(s) supplied and installed by the owner, at the owner's sole cost, to provide space heating, space cooling and domestic hot water heating to the building(s), which energy plant(s) will be designed, constructed and installed on the subject site to the satisfaction of the City and the City's service provider, LIEC;
  - c) the owner transfers ownership of the low carbon energy plant(s), the distribution piping system, and all other ancillary components on the subject site used to generate or convey space heating, space cooling and domestic hot water heating up to and including energy transfer stations, to the City or as directed by the City to the City's DEU service provider, LIEC, at no cost to the City or the City's DEU service provider, LIEC, on terms and conditions satisfactory to the City;
  - d) prior to depositing a Strata Plan, the owner enters into a Service Provider Agreement for the building with the City and/or the City's DEU service provider, LIEC, on terms and conditions satisfactory to the City which provides, without limitation, that the City or the City's DEU service provider will be the exclusive provider of space heating, space cooling and domestic hot water heating services for the building (excluding any commercial portions of the enclosed mall), unless otherwise agreed to by the City Engineer and set out in the Service Provider Agreement; and
  - e) prior to subdivision (including Air Space parcel subdivision and Strata Plan filing, but excluding the subdivision to create the Lot 1(West) and Lot 2 (East)), the owner grants or acquires, and registers, all additional Covenants, Statutory Right-of-Way(s) and/or easements necessary for supplying the services to the building and the operation of the low carbon energy plant(s) by the City and/or the City's DEU service provider, LIEC.
6. If a DEU is not available for connection, and a DEU Bylaw which applies to the site has not been adopted by Council prior to the issuance of the Development Permit for the subject site, no final building inspection permitting occupancy of a building will be granted until:
  - a) the City receives a professional engineer's certificate stating that the building (excluding any commercial portions of the enclosed mall) has the capability to connect to and be serviced by a DEU; and
  - b) the owner grants or acquires any additional Statutory Right-of-Way(s) and/or easements necessary for supplying DEU services to the building, registered prior to subdivision (including Air Space parcel subdivision and strata plan filing).
7. The City, at the City's sole discretion can elect to exclude all of the commercial floor space of the buildings (including the common HVAC system of the commercial floor space of the residential buildings) from the conditions set out in sections 1 to 6 above, provided that:
  - a) the owner agrees that, subject to any exceptions agreed to by the City, the HVAC system(s) of all such excluded new commercial floor space in the buildings and the entirety of the southern portion of the enclosed mall (comprising of approximately 440,00ft<sup>2</sup> existing space plus new construction area (the "South Commercial HVAC Loop") will connect to a DEU to provide available heat rejection, at no cost to the City or the City's service provider, for the benefit of the City's service provider, LIEC, to utilize in its DEU, when a DEU is in operation, unless otherwise directed by the City and the City's DEU service provider, LIEC. For clarity, all mechanical equipment for commercial space will remain the property of the owner, and the owner will not be required to transfer ownership of same.
  - b) no building permit will be issued for a building on the subject site unless the South Commercial HVAC Loop is designed with the capability to reject heat to a DEU system (which includes, without limitation, the low carbon energy plant(s) in each of the residential buildings on the site) to the satisfaction of the City and the City's DEU service provider, LIEC;

- c) if a DEU Bylaw which applies to the site has been adopted by Council prior to the issuance of the development permit for the subject site, no building permit will be issued for a building on the subject site unless the owner designs, to the satisfaction of the City and the City's DEU service provider, LIEC, a heat rejection system from the South Commercial HVAC Loop. Connection points from the South Commercial HVAC Loop will be provided by the owner to enable the City or the City's service provider to capture and transfer the available commercial rejected heat to a DEU system(s) (which includes, without limitation, the low carbon energy plant(s) in residential buildings on the site);
- d) if a DEU is available for connection and the City has directed the owner to connect, no final building inspection permitting occupancy of a building will be granted unless, and until:
- i) the South Commercial HVAC Loop is connected to a DEU (which includes, without limitation, the low carbon energy plant(s) in residential buildings on site) to provide available rejected heat to the DEU;
  - ii) the owner enters into a Service Provider Agreement for such excluded commercial floor space in the building and the entire enclosed mall with the City and/or the City's DEU service provider, LIEC, executed prior to depositing any Strata Plan with LTO and on terms and conditions satisfactory to the City which provides, without limitation, that the City or the City's DEU service provider will be the exclusive recipient of available rejected heat, at no cost to the City or the City's DEU service provider, from the South Commercial HVAC Loop; and
  - iii) prior to subdivision (including Air Space parcel subdivision and Strata Plan filing, but excluding the subdivision to create the Lot 1(West) and Lot 2 (East)), the owner grants or acquires, and registers, all Statutory Right-of-Way(s) and/or easements necessary for the City or the City's DEU service provider to receive available rejected heat from the South Commercial HVAC Loop;
- e) if a DEU is not available for connection, but a DEU Bylaw which applies to the site has been adopted by Council prior to the issuance of the Development Permit for the subject site, no final building inspection permitting occupancy of a building will be granted unless and until:
- i) the City receives a professional engineer's certificate stating that the South Commercial HVAC Loop has the capability to, and will immediately, connect to and provide rejected heat to a DEU (which includes, without limitation, the low carbon energy plant(s) in residential buildings on the site);
  - ii) prior to depositing a Strata Plan, the owner enters into a Service Provider Agreement for such excluded commercial floor space of the building and the entire enclosed mall with the City and/or the City's DEU service provider, LIEC, on terms and conditions satisfactory to the City which provides, without limitation, that the City or the City's DEU service provider will be the exclusive recipient of available rejected heat, at no cost to the City or the City's DEU service provider, from the South Commercial HVAC Loop; and
  - iii) prior to subdivision (including Air Space parcel subdivision and Strata Plan filing, but excluding the subdivision to create the Lot 1(West) and Lot 2 (East)), the owner grants or acquires, and registers, all Statutory Right-of-Way(s) and/or easements necessary for the City or the City's DEU service provider to receive available rejected heat from the South Commercial HVAC Loop; and
- f) if a DEU is not available for connection, and a LCDEU service area bylaw which applies to the site has not been adopted by Council prior to the issuance of the Development Permit for the subject site, no final building inspection permitting occupancy of a residential building will be granted until:
- i) the City receives a professional engineer's certificate stating that the South Commercial HVAC Loop has the capability to, and will immediately, connect to and provide available rejected heat to a DEU (which includes, without limitation, the low carbon energy plant(s) in residential buildings on the site); and
  - ii) the owner grants or acquires, and registers, all Statutory Right-of-Way(s) and/or easements necessary for the City or the City's DEU service provider to receive available rejected heat from the South Commercial HVAC Loop.
8. The owner may on notice to the City elect to opt out of Section 7 above, and in such case, sections 1 through 6 above shall govern.

## **Servicing Agreement Requirements – Engineering Servicing Terms & Conditions**

These requirements were written with the intention of being constructed in two phases, with phase 1 (generally located on the west side of the site) preceding phase 2 (generally located on the east side of the site). The developer is required to enter into Servicing Agreement 1 (outlined below) prior to the Building Permit for phase 1 being issued. The works under Servicing Agreement 1 must be completed prior to the occupancy of the first building of phase 1 unless otherwise determined to the City's sole satisfaction and secured with legal agreement(s) on title. Similarly, the developer is required to enter into Servicing Agreement 2 (outlined below) prior to the Building Permit for phase 2 being issued. The works under Servicing Agreement 2 must be completed prior to the occupancy of the first building of phase 2 unless otherwise determined to the City's sole satisfaction and secured with legal agreement(s) on title.

### **Servicing Agreement #1**

#### **1) Water Works:**

- a) Using the OCP Model, there is 326.0 L/s of water available at a 20 psi residual at the Minoru Boulevard frontage. Based on your proposed development, your site requires a minimum fire flow of 220 L/s.
- b) The Developer is required to:
  - i) Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage Building designs.
  - ii) Upgrade the existing 250 mm AC water main to 300 mm PVC along the entire Minoru Boulevard frontage of the development site, approximately 450 m.
  - iii) Install approximately 135 m of new 300 mm water main along the new east-west road, complete with fire hydrants to achieve City spacing requirements, from the proposed water main in Minoru Boulevard to the extent of the phase 1 roadworks, complete with blow-off.
  - iv) Review hydrant spacing on all road frontages and install new fire hydrants as required to meet City spacing requirements for commercial land use.
  - v) Install one new water service connection, complete with meter and meter box, for each new parcel. Meters to be located onsite (i.e. in a mechanical room).
  - vi) Confirm which existing service connections are not required to serve the existing mall that is to remain and cut, cap, and remove unused connections.
- c) At Developer's cost, the City is to:
  - i) Complete all tie-ins for the proposed works to existing City infrastructure.

#### **2) Storm Sewer Works:**

- a) The Developer is required to:
  - i) Perform a drainage analysis to the major conveyance along Murdoch Avenue at Minoru Boulevard. Upgrade the existing storm sewer along the Minoru Boulevard frontage as necessary to address OCP flows, and reconnect all existing connections. The drainage analysis shall be included in the servicing agreement drawing set.
  - ii) Install approximately 140 m of minimum 600 mm or OCP size storm sewer along the new east-west road, complete with catch basins, from the proposed storm sewer in Minoru Boulevard to the extent of the phase 1 roadworks.
  - iii) Install one new storm service connection, complete with inspection chamber, for each new parcel.
- b) At Developer's cost, the City is to:
  - i) Complete all tie-ins for the proposed works to existing City infrastructure.

#### **3) Sanitary Sewer Works:**

- a) The Developer is required to:
  - i) Install approximately 175 m of new 250 mm sanitary sewer, 195 m of new 300 mm sanitary sewer, and 25 m of new 375 mm sanitary sewer along the Minoru Boulevard frontage from approximately the new east-west road to tie-in to the existing main along Murdoch Avenue. The main shall be designed to accommodate for the future sanitary flows from lots 6551/6631/6651 Minoru Boulevard, the City Hall, and 7811 Granville Avenue, based on OCP densities. The upstream invert shall be designed so that an extension of the main to service

7811 Granville Avenue & the City Hall, with adequate slopes and cover, is possible. (Development Cost charge credits may apply.)

- ii) Design the Murdoch Road extension to accommodate the future relocation of the sanitary forcemain from the north property line of the development site.
  - iii) There is an existing City sanitary sewer onsite near the Murdoch Road extension that will need to be removed to facilitate site preparation. Prior to start of site preparation (including but not limited to soil densification, excavation, and DSM wall construction), the developer is required to do the following:
    - A. Provide, as part of the phase 1 development permit application, a construction sequence plan for the installation of the new sanitary sewer in Murdoch, relocation of onsite sanitary service, and the removal/abandonment of the existing City sanitary sewer, for City review/approval.
    - B. Ensure that the existing mall remains serviced during and after the removal of the onsite City-owned sanitary sewer.
    - C. Provide a manhole and capped stub at the property line to serve the existing mall on the remainder lot. The sanitary sewer within the Murdoch Road extension required to connect to the existing mall is to be owned and maintained by the developer (i.e. private onsite service)..
    - D. Cut, cap, and remove the existing 200 mm AC sanitary main and manholes located within the development site, and legally dispose offsite. The extents of the removal shall be from manhole SMH587 to SMH588.
    - E. Enter into a legal agreement to transfer ownership, maintenance, and liability from the City to the property owner for any portion of the sanitary sewer that cannot be removed due to proximity to the existing mall.
    - F. Provide a signed and sealed letter from the developer's civil consultant stating that the AC sanitary main and related appurtenances have been removed and properly and legally disposed offsite.
  - iv) Install one new sanitary service connection, complete with inspection chamber, for each new parcel.
- b) At Developer's cost, the City is to:
- i) Complete all tie-ins for the proposed works to existing City infrastructure.

#### 4) Frontage Improvements:

- a) The Developer is required to:
  - i) Design the new east-west road to accommodate for a future 4.38 m-wide District Energy Utility corridor. The DEU corridor shall be within the roadway and clear of all other underground utilities.
  - ii) Incorporate future District Energy Utility corridors within the design of the No 3 Road and Minoru Boulevard cross-sections. The Minoru Boulevard DEU corridor width shall be 4.38 m, and the No 3 Road DEU corridor width shall be 4.2 m. The DEU corridors shall be clear of trees and all other underground utilities.
  - iii) Coordinate with BC Hydro, Telus and other private communication service providers:
    - A. To pre-duct for future hydro, telephone and cable utilities along all road frontages.
    - B. When relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
    - C. To locate/relocate all above ground utility cabinets and kiosks required to service the proposed development, and all above ground utility cabinets and kiosks located along the development's frontages, within the developments site (see list below for examples). The locations of the proposed & relocated infrastructure shall be shown on the development permit drawings. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements (e.g., statutory right-of-way dimensions) and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of statutory right-of-ways that shall be shown on the architectural plans/functional plan, the servicing agreement drawings, and registered prior to SA design approval:
      - a. BC Hydro PMT – 4.0 x 5.0 m
      - b. BC Hydro LPT – 3.5 x 3.5 m
      - c. Street light kiosk – 1.5 x 1.5 m
      - d. Traffic signal kiosk – 2.0 x 1.5 m
      - e. Traffic signal UPS – 1.0 x 1.0 m
      - f. Shaw cable kiosk – 1.0 x 1.0 m
      - g. Telus FDH cabinet – 1.1 x 1.0 m

- iv) Provide street lighting along all road frontages according to the following street light types:
- B. City Streets
- a. Minoru Boulevard
- i. Pole colour: Blue
  - ii. Roadway lighting @ median: City Centre Type Roadway/Pedestrian Luminaire Pole (LED) – Drawing L12.3 **INCLUDING** 2 street luminaires (set perpendicular to the direction of travel), banner arms, 1 flower basket holder, 1 duplex receptacle, and irrigation, but **EXCLUDING** pedestrian luminaires.
  - iii. Pedestrian lighting between sidewalk & bike path: City Centre Type Laneway Luminaire Pole (LED) – Drawing L12.1 **INCLUDING** 2 pedestrian luminaires (set perpendicular to the direction of travel), duplex receptacle, and flower basket holder, but **EXCLUDING** banner arms and irrigation.
- b. Murdoch Avenue (South side)
- i. Pole colour: Grey
  - ii. Roadway lighting @ back of curb (Both sides of street): Type 7 (LED) **INCLUDING** 1 street luminaire, banner arms, and 1 duplex receptacle, but **EXCLUDING** any pedestrian luminaires, flower basket holders, or irrigation.
  - iii. Pedestrian lighting @ back of ultimate bike path: Type 8 (LED) **INCLUDING** 1 pedestrian luminaire, but **EXCLUDING** any duplex receptacle, banner arms, flower basket holders, or irrigation.
  - iv. *NOTE: Murdoch & Cook will be constructed within SRWs; however, both streets shall be constructed to City standards to facilitate potential future dedication (as per the CCAP). Staff must confirm the streetlight requirements in coordination with cross-section & landscape design. Requirements may change.*
- c. New City Hall Street (Both sides of street)
- i. Pole colour: Grey
  - ii. Roadway lighting @ back of curb (Both sides of street): Type 7 (LED) **INCLUDING** 1 street luminaire, banner arms, and 1 duplex receptacle, but **EXCLUDING** any pedestrian luminaires, flower basket holders, or irrigation.
  - iii. Pedestrian lighting @ back of multi-use path (South side of street only): Type 8 (LED) **INCLUDING** 2 pedestrian luminaires and 1 duplex receptacle, but **EXCLUDING** any banner arms, flower basket holders, or irrigation. (NOTE: "Pedestrian luminaire" arms shall be set perpendicular to the direction of travel to light both the multi-use path and the adjacent City property.)
- NOTE: Staff must confirm the New City Hall Street streetlight requirements in coordination with cross-section & landscape design. Requirements may change.*
- C. Off-Street Publicly-Accessible Walkways & Open Spaces
- a. Park Road Plaza (SRW): To be determined through the Development Permit & SA processes (Note: Lighting to be privately owned & operated)
- D. Traffic Signals
- a. Minoru Boulevard @ Parkade Entrance, and Minoru Gate
    - i. Pole colour: Blue
    - ii. Style: To match City Centre Type Roadway/Pedestrian Luminaire Pole (LED) – Drawing L12.3
- E. Private Streets (Secured via SRW) – Developer owned/maintained
- a. Pole colour: Grey
  - b. Roadway lighting: Type 7 (LED) **INCLUDING** 1 street luminaire and **MAY INCLUDE** banner arms, duplex receptacles, pedestrian luminaires, flower basket holders, and/or irrigation.
  - c. Pedestrian lighting: Type 8 (LED) **INCLUDING** 1 or 2 pedestrian luminaires and **MAY INCLUDE** duplex receptacles, flower basket holders, and/or irrigation, but **EXCLUDING** banner arms.)
- NOTE: Staff must confirm the Private Street streetlight requirements in coordination with cross-section & landscape design through the Development Permit & SA approval processes. Requirements may change.*

**5) General Items:**

- a) The Developer is required to:
  - i) Relocate all private onsite infrastructure outside of the proposed road dedication/utility SRWs and into the development site.
  - ii) Provide, prior to start of site preparation works or within the first servicing agreement submission, whichever comes first, a geotechnical assessment of site preparation (including excavation, preload, dewatering, and soil densification) impacts on the existing utilities fronting the development site and provide mitigation recommendations.
  - iii) Conduct pre- and post-site preparation elevation surveys of all surrounding roads, utilities, and structures. Any damage, nuisance, or other impact to be repaired at the developer's cost. The post-site preparation elevation survey shall be incorporated within the servicing agreement design.
  - iv) Monitor the settlement at the adjacent utilities and structures during pre-loading, dewatering, and soil preparation works per a geotechnical engineer's recommendations, and report the settlement amounts to the City for approval.
  - v) Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

**Servicing Agreement #2****1) Water Works:**

- a) Using the OCP Model, there is 755.0 L/s of water available at a 20 psi residual at the No 3 Rd frontage. Based on your proposed development, your site requires a minimum fire flow of 220 L/s.
- b) The Developer is required to:
  - i) Submit Fire Underwriter Survey (FUS) or International Organization for Standardization (ISO) fire flow calculations to confirm development has adequate fire flow for onsite fire protection. Calculations must be signed and sealed by a Professional Engineer and be based on Building Permit Stage Building designs.
  - ii) Review hydrant spacing on all road frontages and install new fire hydrants as required to meet City spacing requirements for commercial land use.
  - iii) Install a new water service connection, complete with meters and meter boxes, for each new parcel. Meters to be located onsite (i.e. in a mechanical room). Note that the service connections and fire hydrant lead are to tie in to the existing 300 mm water main on the east side of No 3 Road. Service connections are not to tie in to the large diameter water mains (i.e. the 550 mm water main on the west side of No 3 Road), per the Engineering Design Specifications.
  - iv) Install approximately 120 m of new 300 mm water main along the new east-west road, complete with fire hydrants to achieve City spacing requirements, from the new water main built in phase 1 to the existing 300 mm water main in No 3 Road.
  - v) Confirm which existing service connections are not required to serve the existing mall that is to remain and cut, cap, and remove unused connections.
- c) At Developer's cost, the City is to:
  - i) Complete all tie-ins for the proposed works to existing City infrastructure.

**2) Storm Sewer Works:**

- a) The Developer is required to:
  - i) Install approximately 140 m of minimum 600 mm or OCP size storm sewer along the new east-west road, complete with catch basins, from the new storm sewer built in phase 1 to the existing storm sewer in No 3 Road.
  - ii) Install one new storm service connection, complete with inspection chamber, for each new parcel.
- b) At Developer's cost, the City is to:
  - i) Complete all tie-ins for the proposed works to existing City infrastructure.

### 3) Sanitary Sewer Works:

- a) The Developer is required to:
- i) Install approximately 330 m of new sanitary sewer along No 3 Road in the roadway. The sizes shall range between 250-375 mm based on the existing & future catchment of the pipe, to be confirmed at the servicing agreement stage.
  - ii) Upgrade the Richmond Centre Sanitary Pump Station to accommodate the increased flows from this development, including but not limited to the following:
    - A. A new electrical kiosk and all related appurtenances, including conduits and SCADA antenna. The electrical kiosk shall be located close to the wet well.
    - B. A back-up generator and all related appurtenances, including conduits and exhaust.
    - C. Upgrades to the power supply as required by the upgraded kiosk, including but not limited to new conduits.
    - D. A parking area for the service vehicles (crane truck, vector truck, etc.) in front of the wet well. The parking area must be located such that the pumps can be removed from the wet well via the crane mounted on the service vehicle. The parking area must provide safe and adequate traffic and pedestrian flow during weekly maintenance, maintaining southbound traffic in the two-way lane, without requiring traffic control.
    - E. An approximately 13 m by 10 m utility right-of-way for the pump station and related equipment and parking area as described above and as shown in **Schedule F**.
    - F. A secondary designated parking stall designed for LSU vehicles, as shown in **Schedule F**, secured by a legal agreement.
  - iii) Design the Cook Road extension and No 3 Road cross-sections, and pump station configuration, to accommodate the future relocation of the sanitary forcemain from the north property line of the development site.
  - iv) Install a new sanitary service connection off of the proposed mains, complete with inspection chambers, for each new parcel.
  - v) Expose and locate all utilities in No 3 Road west of the median, to confirm that there is a suitable alignment available for the proposed sanitary sewer. If the utility locate determines that there is no suitable alignment within the roadway to the satisfaction of Engineering, the developer must either provide an additional right-of-way to accommodate the sanitary sewer as identified under section 2.7.3 *Additional City Utilities (No. 3 Road)*, or relocate such utilities that conflict with the proposed sanitary sewer (as identified by the required utility locate) so that the proposed sanitary sewer can be installed to meet the applicable standards and specifications (particularly in regards to clearance and cover).
- b) At Developer's cost, the City is to:
- i) Complete all tie-ins for the proposed works to existing City infrastructure.

### 4) Frontage Improvements:

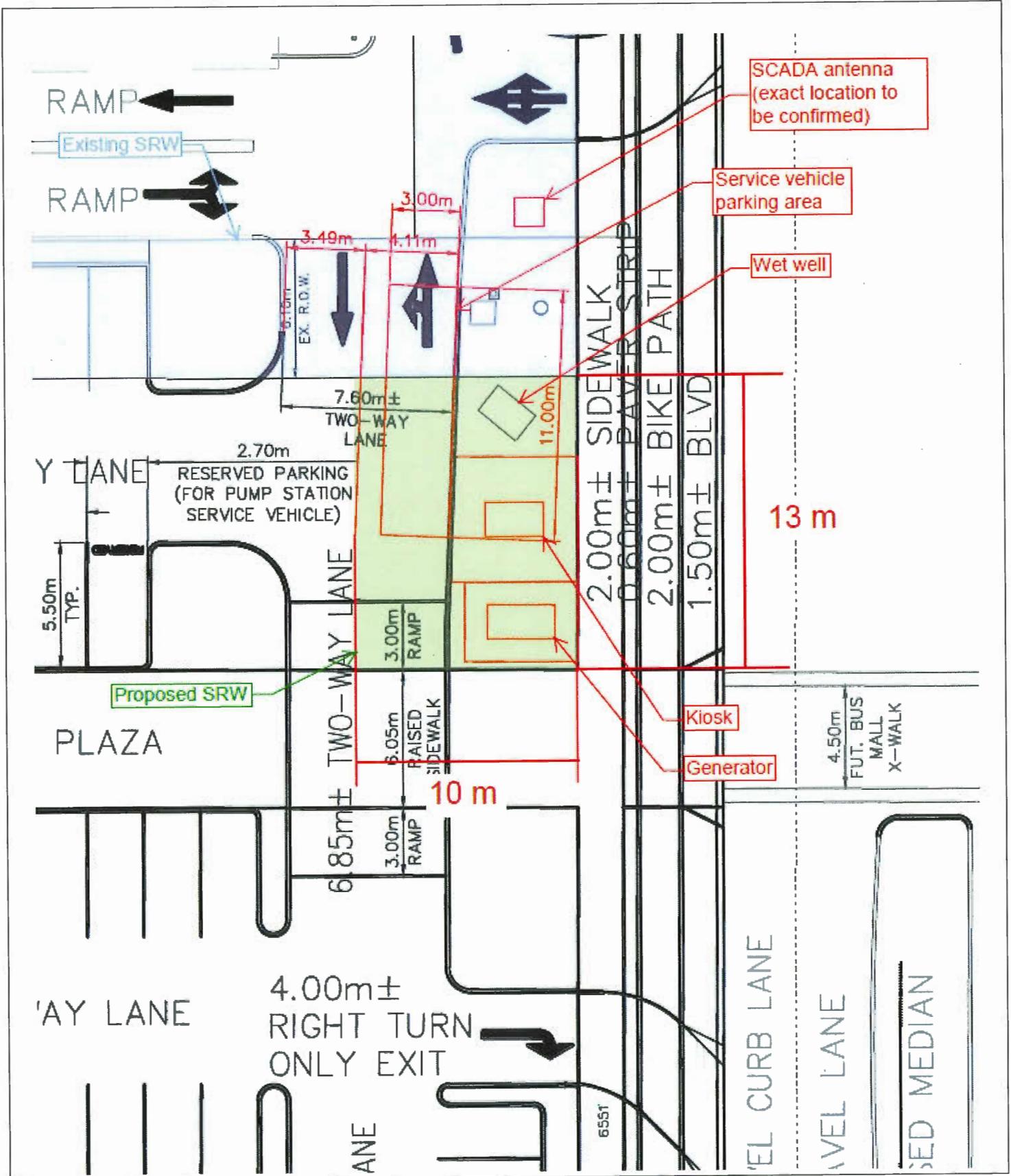
- a) The Developer is required to:
- i) Incorporate future District Energy Utility corridors within the design of the No 3 Road and Minoru Boulevard cross-sections. The Minoru Boulevard DEU corridor width shall be 4.38 m, and the No 3 Road DEU corridor width shall be 4.2 m. The DEU corridors shall be clear of trees and all other underground utilities.
  - ii) Coordinate with BC Hydro, Telus, and other private utility companies to relocate the existing structures (including, but not limited to, the Telus cabinets and LPT near the bus shelter) along No 3 Road out of the ultimate frontage improvements and into a suitable location onsite (i.e. outside of the public realm). The proposed locations shall be shown on the development permit plans.
  - iii) Coordinate with the City's Traffic and Engineering departments, and the project's lighting and traffic signal consultants, to relocate the existing traffic and street light kiosks located along No 3 Road out of the ultimate frontage improvements and into a suitable location onsite (i.e. outside of the public realm). The proposed locations shall be shown on the development permit plans.
  - iv) Coordinate with BC Hydro to relocate the existing structures (including, but not limited to, Vista Switch and LPT) located within the proposed intersection of the new east-west road and No 3 Road, into the ultimate location within the development site. The estimated BC Hydro right-of-way for the existing above-ground equipment is 14.0 m by 6.0 m; actual dimensions to be provided by BC Hydro following their detailed design. Please note that this does not include the above-ground structures (i.e. Vista Switches, PMTs, etc.) that are required to service the proposed development. The new location should be coordinated with BC Hydro and the City's Planning Department early to avoid future conflicts with the building design, delays, or other expenses for the Developer.

- v) Coordinate with BC Hydro, Telus and other private communication service providers:
- A. To pre-duct for future hydro, telephone and cable utilities along all road frontages.
  - B. When relocating/modifying any of the existing power poles and/or guy wires within the property frontages.
  - C. To locate/relocate all above ground utility cabinets and kiosks required to service the proposed development, and all above ground utility cabinets and kiosks located along the development's frontages, within the development's site (see list below for examples). The locations of the proposed & relocated infrastructure shall be shown on the development permit drawings. Please coordinate with the respective private utility companies and the project's lighting and traffic signal consultants to confirm the requirements (e.g., statutory right-of-way dimensions) and the locations for the aboveground structures. If a private utility company does not require an aboveground structure, that company shall confirm this via a letter to be submitted to the City. The following are examples of statutory right-of-ways that shall be shown on the architectural plans/functional plan, the servicing agreement drawings, and registered prior to SA design approval:
    - a. BC Hydro PMT – 4.0 x 5.0 m
    - b. BC Hydro LPT – 3.5 x 3.5 m
    - c. Street light kiosk – 1.5 x 1.5 m
    - d. Traffic signal kiosk – 2.0 x 1.5 m
    - e. Traffic signal UPS – 1.0 x 1.0 m
    - f. Shaw cable kiosk – 1.0 x 1.0 m
    - g. Telus FDH cabinet – 1.1 x 1.0 m
- vi) Provide street lighting along all road frontages according to the following street light types:
- A. City Streets
    - a. No 3 Road (West side of street)
      - i. Pole colour: Grey
      - ii. Roadway lighting: N/A (No change to existing lighting in centre median)
      - iii. Pedestrian lighting between sidewalk & bike path: Type 8 (LED) INCLUDING 2 pedestrian luminaires set perpendicular to the roadway, flower basket holders, and 1 duplex receptacle, but EXCLUDING any banner arms or irrigation.
    - b. Cook Road (Both sides)
      - i. Pole colour: Grey
      - ii. Roadway lighting @ back of curb (Both sides of street): Type 7 (LED) INCLUDING 1 street luminaire, banner arms, and 1 duplex receptacle, but EXCLUDING any pedestrian luminaires, flower basket holders, or irrigation.
      - iii. Pedestrian lighting @ back of ultimate bike path: Type 8 (LED) INCLUDING 2 pedestrian luminaires and 1 duplex receptacle, but EXCLUDING any banner arms, flower basket holders, or irrigation. (NOTE: "Pedestrian luminaire" arms shall be set perpendicular to the direction of travel to light both the ultimate bike path and the adjacent sidewalk.)
      - iv. *NOTE: Murdoch & Cook will be constructed within SRWs; however, both streets shall be constructed to City standards to facilitate potential future dedication (as per the CCAP). Staff must confirm the streetlight requirements in coordination with cross-section & landscape design. Requirements may change.*
    - c. New City Hall Street (Both sides of street)
      - i. Pole colour: Grey
      - ii. Roadway lighting @ back of curb (Both sides of street): Type 7 (LED) INCLUDING 1 street luminaire, banner arms, and 1 duplex receptacle, but EXCLUDING any pedestrian luminaires, flower basket holders, or irrigation.
      - iii. Pedestrian lighting @ back of multi-use path (South side of street only): Type 8 (LED) INCLUDING 2 pedestrian luminaires and 1 duplex receptacle, but EXCLUDING any banner arms, flower basket holders, or irrigation. (NOTE: "Pedestrian luminaire" arms shall be set perpendicular to the direction of travel to light both the multi-use path and the adjacent City property.)
      - iv. *NOTE: Staff must confirm the New City Hall Street streetlight requirements in coordination with cross-section & landscape design. Requirements may change.*
  - B. Traffic Signals
    - a. No. 3 Road @ Cook Road & Park Road
      - i. Pole colour: Grey
      - ii. Style: To match Type 7

- C. Private Streets (Secured via SRW) – Developer owned/maintained
  - a. Pole colour: Grey
  - b. Roadway lighting: Type 7 (LED) INCLUDING 1 street luminaire and MAY INCLUDE banner arms, duplex receptacles, pedestrian luminaires, flower basket holders, and/or irrigation.
  - c. Pedestrian lighting: Type 8 (LED) INCLUDING 1 or 2 pedestrian luminaires and MAY INCLUDE duplex receptacles, flower basket holders, and/or irrigation, but EXCLUDING banner arms.)  
*NOTE: Staff must confirm the Private Street streetlight requirements in coordination with cross-section & landscape design. Requirements may change.*

**5) General Items:**

- a) The Developer is required to:
  - i) Relocate all private onsite infrastructure outside of the proposed road dedication/utility SRWs and into the development site.
  - ii) Provide, prior to start of site preparation works or within the first servicing agreement submission, whichever comes first, a geotechnical assessment of site preparation (including excavation, dewatering, and soil densification) impacts on the existing utilities fronting the development site and provide mitigation recommendations.
  - iii) Conduct pre- and post-site preparation elevation surveys of all surrounding roads, utilities, and structures. Any damage, nuisance, or other impact to be repaired at the developer's cost. The post-site preparation elevation survey shall be incorporated within the servicing agreement design.
  - iv) Monitor the settlement at the adjacent utilities and structures during pre-loading, dewatering, and soil preparation works per a geotechnical engineer's recommendations, and report the settlement amounts to the City for approval.
  - v) Enter into, if required, additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the satisfaction of the Director of Engineering, including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.



**Servicing Agreement Requirements – Transportation**  
Terms & Conditions

Developer is responsible for the design and construction of the following frontage improvements and transition between those improvements and the existing condition outside the development site frontage (at a minimum 30:1 taper rate for No. 3 Road and a minimum 20:1 taper rate for all other roads) to the satisfaction of the City. Note that while the list below provides a general description of the minimum frontage work requirements (which are schematically shown in the Preliminary Functional Road Plan (**Schedule H**), the exact details and scope of the frontage works to be completed by the developer shall be confirmed via the detailed design (SA) process to the satisfaction of the City.

**1) New City Hall Street Cross-Sections:**

- a. Minoru Boulevard, from Murdoch Avenue to the proposed East/West Street (from west to east):
  - Maintain two existing southbound traffic lanes
  - 5.6m wide area for:
    - 1) 3.3m wide intersection turning lanes; and
    - 2) 2.3m wide landscaped/treed median with curb and gutter on both sides
  - 6.6m wide driving surface for two northbound traffic lanes
  - 0.15m wide curb and gutter
  - 2.4m wide grassed/treed boulevard
  - 1.8m wide asphalt bike path
  - 1.1m wide buffer/lighting strip
  - 2.5m wide concrete sidewalk
- b. Proposed East/West Street, from Minoru Boulevard to No. 3 Road (from south to north):
  - 3.0m wide concrete sidewalk
  - 1.4m wide grassed/treed boulevard
  - 0.15m wide curb and gutter
  - 7.0m wide driving surface for traffic lanes (one in each direction)
  - 0.15m wide curb and gutter
  - 1.5m wide grassed/treed boulevard (a portion of the area would be used as a parking/loading layby)
  - 2.0m wide concrete sidewalk
- c. No. 3 Road, from Saba Road to the proposed East/West Street (from east to west):
  - Maintain two existing southbound traffic lanes
  - 0.15m wide curb and gutter
  - 1.5m wide grassed/treed boulevard
  - 2.0m wide asphalt bike path
  - 0.6m wide buffer/lighting strip
  - 2.0m wide concrete sidewalk
  - Note that the above may be refined in the context of the building setback SRW review to further enhance the pedestrian realm

**Note:** Interim works as described below along No. 3 Road, from northern limit of the site to approximately 30m south of the future Bus Mall intersection shall be required prior to 1c) being completed:

- Widen the sidewalk along west side of No. 3 Road to min. 3.0m wide;
- As necessary, removal of the existing hedge and fence at the northern property line to provide a continuous min. 3.0m wide sidewalk to the neighbouring site to the north;
- Modify the existing vehicular access off the parkade ramp to physically restrict egress traffic movements onto No. 3 Road; and
- Install a new vehicular access approximately 30m south which will only allow right-out traffic movement onto No. 3 Road.

**2) Private (SRW) Street Cross-Sections:**

- a. Cook Road, from No. 3 Road to the western limit (from north to south):
  - 2.0m wide concrete sidewalk
  - 0.5m wide buffer/lighting strip
  - 1.8m wide concrete bike path
  - 3.0m wide grassed/treed boulevard

- 0.15m wide curb and gutter
- 16.1m wide pavement width
- 0.15m wide curb and gutter
- 3.0m wide grassed/treed boulevard
- 1.8m wide concrete bike path
- 0.5m wide buffer/lighting strip
- 2.0m wide concrete sidewalk

b. Murdoch Avenue, from Minoru Boulevard to the eastern limit (from north to south):

Ultimate cross-section

- 2.5m wide concrete sidewalk
- 0.5m wide buffer/lighting strip
- 1.8m wide concrete bike path
- 2.5m wide treed boulevard
- 0.15m curb and gutter
- 9.25m pavement width
- 0.15m curb and gutter
- 2.5m wide treed boulevard (including parking lay-by)
- 0.85m wide buffer
- 1.8m wide concrete bike path
- 0.5m wide buffer/lighting strip
- 2.5m wide concrete sidewalk

Interim cross-section shall be permitted to maintain the existing sidewalk along the street's north side and determine the pavement width based on required traffic operations, as determined to the City's satisfaction.

c. All other internal SRW streets: Generally shown in the preliminary road functional plan attached, with varying pavement widths to accommodate two-way traffic, curb and gutter, on-street parking, on-street lay-bys, treed/grassed boulevards and min. 2.0m wide sidewalk as appropriate.

### 3) Intersection Upgrades:

- a. Upgrade of the existing traffic signals / special crosswalks at the following locations to accommodate the road enhancements noted above. Work to include but not limited to: Install new, upgrade and/or replace signal pole, controller, base and hardware, pole base, detection, conduits (electrical & communications), signal indications, communications cable, electrical wiring, service conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).
- Minoru Boulevard/Minoru Gate: Upgrade of the existing traffic signal
  - Minoru Boulevard/Proposed parkade entrance: Install a new traffic signal (and removal of the existing special crosswalk)
  - Minoru Boulevard/Murdoch Avenue: Upgrade of the existing traffic signal
  - No 3 Road/Park Road: Upgrade of the existing traffic signal
  - No 3 Road/Cook Road: Upgrade of the existing traffic signal
  - No 3 Road/future Bus Mall access: Upgrade of the traffic signal (DCC credits will apply.)
- b. At each of the intersections, all existing pedestrian crosswalks should be upgraded to meet City Centre standards (min. 4.5m wide) as necessary with universal accessibility features (e.g., tactile treatments or equivalent) installed on all wheelchair ramps.

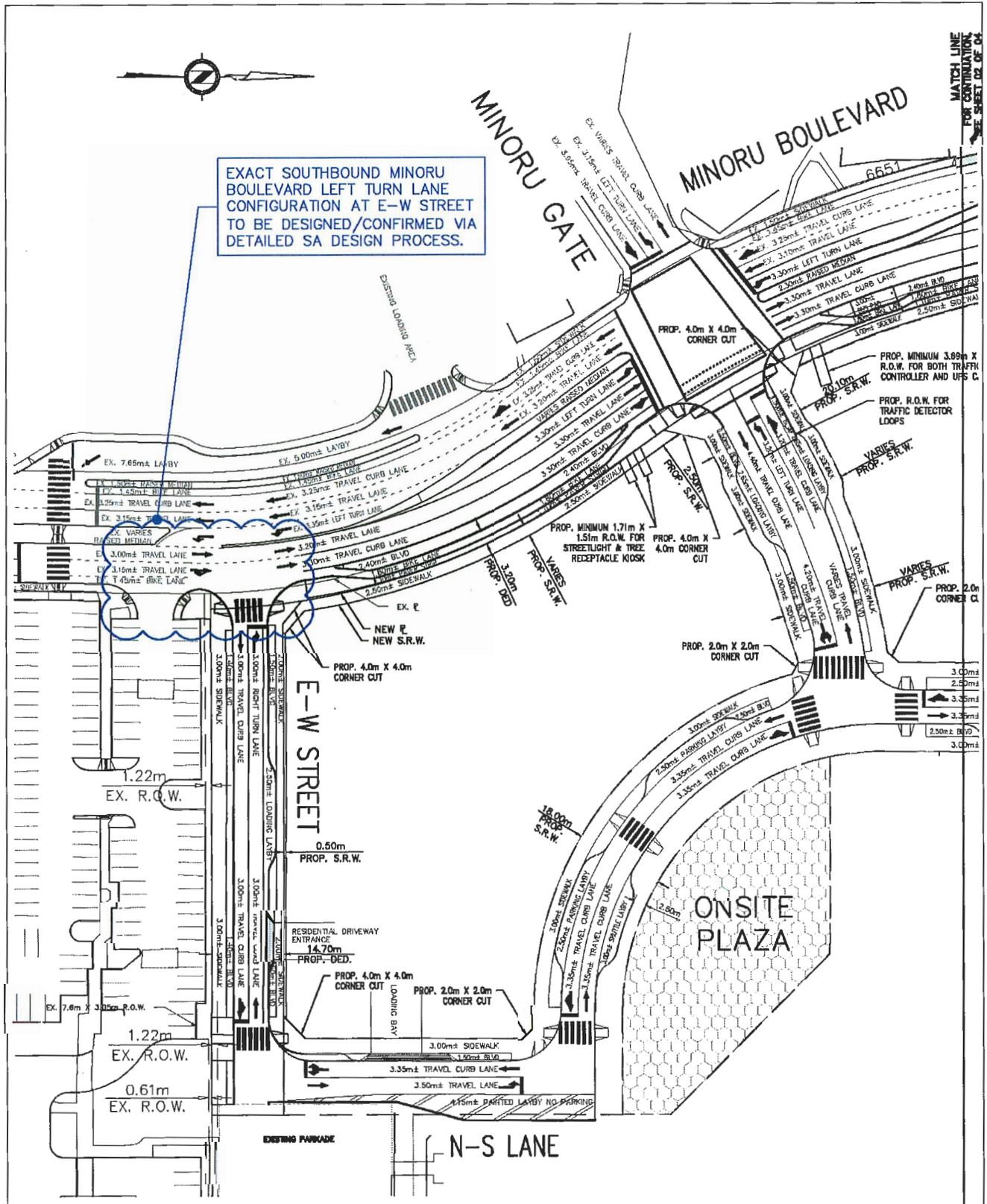
### 4) Timing of Works:

In general, the improvements noted above shall be completed on a phase-by-phase basis as follows:

- a. Servicing Agreement #1 (generally works within the western portion of the site):
- Minoru Boulevard, from Murdoch Avenue to the proposed East/West Street (as described in 1a)
  - Western ½ of the proposed East/West Street, from Minoru Boulevard to No. 3 Road (as described in 1b)
  - Murdoch Avenue, from Minoru Boulevard to the eastern limit (as described in 1e)
  - All other internal SRW streets within the western ½ of the site (as described in 1f)
  - Intersection upgrades, all intersections along Minoru Boulevard (as described in 1g)
  - Interim works along No. 3 Road, from northern limit of 6088 Minoru Boulevard to approximately 30m south of the future Bus Mall intersection:

- Widen the sidewalk along west side of No. 3 Road to min. 3.0m wide;
  - As necessary, removal of the existing hedge and fence at the northern property line to provide a continuous min. 3.0m wide sidewalk to the neighbouring site to the north;
  - Modify the existing vehicular access off the parkade ramp to physically restrict egress traffic movements onto No. 3 Road; and
  - Install a new vehicular access approximately 30m south which will only allow right-out traffic movement onto No. 3 Road.
- b. Servicing Agreement #2 (generally works within the eastern portion of the site):
- Eastern ½ of the proposed East/West Street, from Minoru Boulevard to No. 3 Road (as described in 1b)
  - No. 3 Road, from northern limit of the site to the proposed East/West Street (as described in 1c)
  - Cook Road, from No. 3 Road to the western limit (as described in 1d)
  - All other internal SRW streets within the eastern ½ of the site (as described in 1f)
  - Intersection upgrades, all intersections along No. 3 Road (as described in 1g)

SCHEDULE H  
 Preliminary Functional Road Plan (Phase 1 & Phase 2)



<b>City of Richmond</b> 8811 No. 3 Road Richmond B.C. V6Y 3C1				
ISSUE	DESCRIPTION	DR	CH	DATE
A	ISSUED FOR OCP AMENDMENT CONSIDERATIONS	RM	MY	10 SEP/18

**APLIN MARTIN**  
 ENGINEERING ARCHITECTURE PLANNING SURVEYING

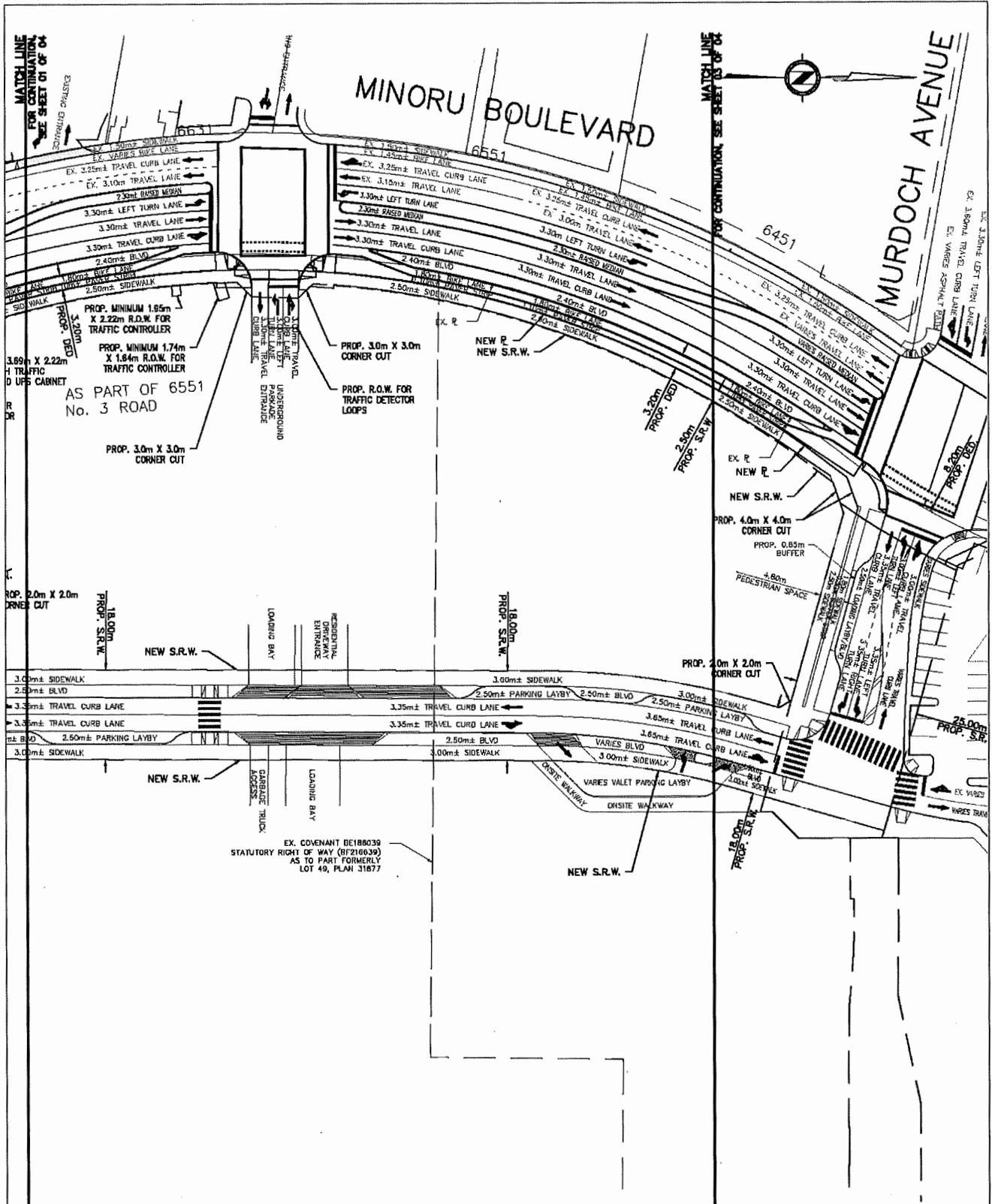
1818 - 1177 West Hastings Street, Vancouver, B.C. Canada V6E 2H0  
 Tel: (604) 679-4424, Fax: (604) 679-4425

CITY FILE #: CP 16-752923	ABM PROJECT No: 15-954B-PH1-OCP-FP-02			
TITLE: <b>PRELIMINARY FUNCTIONAL PLAN PHASE 1</b>				
DESIGN:MY/RM	CHECK:MY	DATE: SEP/18	PROJECT ADDRESS: 6551 No. 3 ROAD	SHEET No. 01 OF 04
DRAWN:RM	APPR:MY	SCALE: 1:1000		

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Sep 11, 2018 10:08am No. 201515-954B-PH1-ENGINEERING-DWG-OFFSITE/JUNCTIONAL PLAN/15-954B-PH1-FUNCTIONAL PLAN DESIGN MASTER.dwg/FP-02-PH1 (6.5x11) 1 M:yang

SCHEDULE H  
 Preliminary Functional Road Plan (Phase 1 & Phase 2)



<b>City of Richmond</b> 8811 No. 3 Road Richmond B.C. V6Y 3C1				
ISSUE	DESCRIPTION	DR	CH	DATE
A	ISSUED FOR OCP AMENDMENT CONSIDERATIONS	RM	MY	10 SEP/18

**APLIN MARTIN**  
 ENGINEERING ARCHITECTURE PLANNING SURVEYING

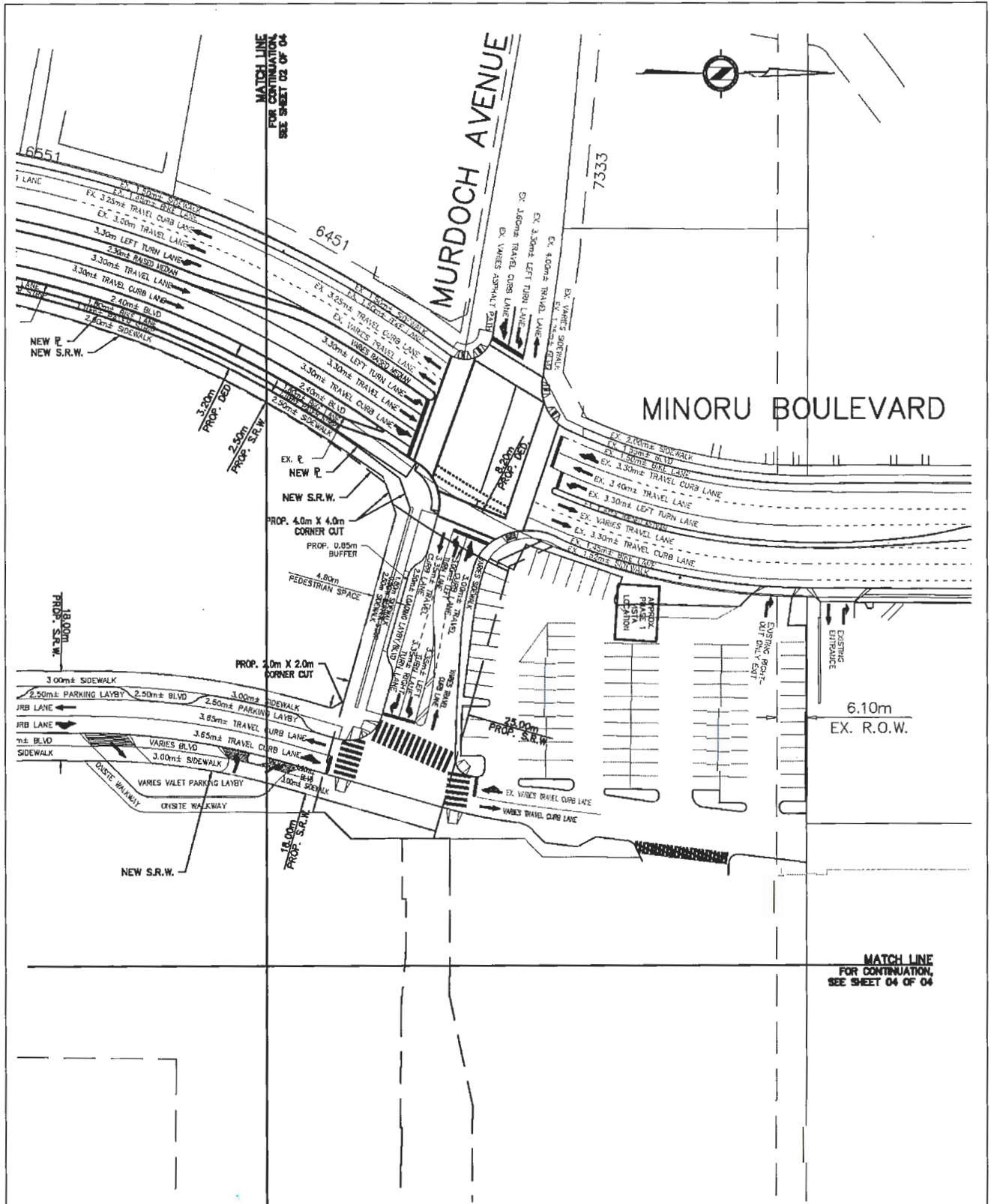
#1818 - 1177 West Hastings Street, Vancouver, B.C. Canada V6E 2K0  
 Tel: (604) 678-9424, Fax: (604) 276-2021, Email: info@aplinmartin.com

CITY FILE #: CP 16-752923	A&M PROJECT No: 15-954B-PH1-OCP-FP-03
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DESIGN:MY/RM CHECK:MY DATE: SEP/18	PROJECT ADDRESS: 6551 No. 3 ROAD
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SCHEDULE H  
 Preliminary Functional Road Plan (Phase 1 & Phase 2)



**City of Richmond**  
 8911 No. 3 Road Richmond B.C. V6Y 3E1

ISSUE	DESCRIPTION	DR	CH	DATE
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**APLIN MARTIN**  
 ENGINEERING ARCHITECTURE PLANNING SURVEYING

#1818 - 1177 West Hastings Street, Vancouver, B.C. Canada V6E2K0  
 Tel: (604) 678-6434, Fax: (604) 681-2041, Email: info@aplinmartin.com

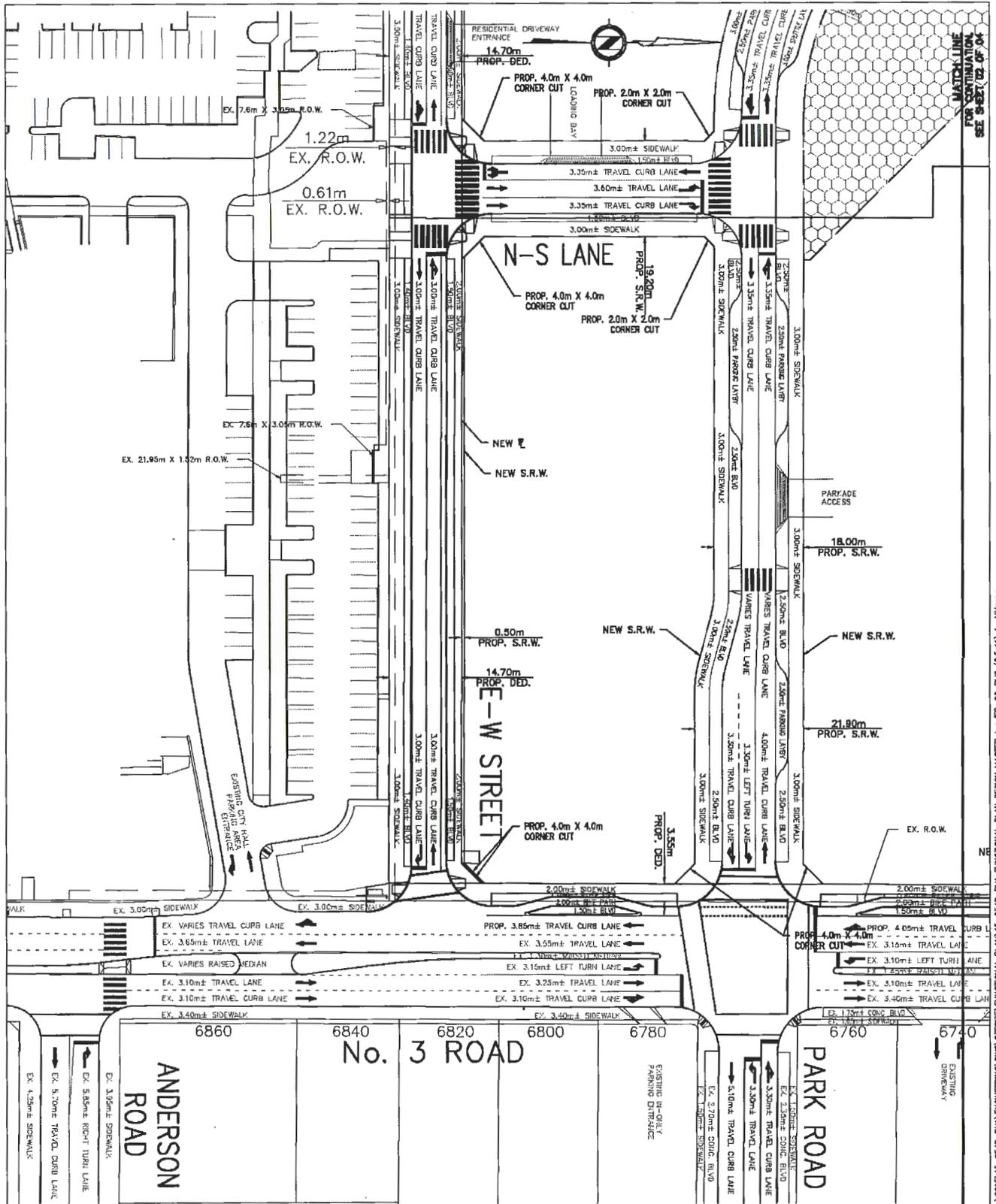
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DESIGN:MY/RM CHECK:MY DATE: SEP/18	PROJECT ADDRESS: 6551 No. 3 ROAD
DRAWN:RM APPR: MY SCALE: 1:1000	SHEET No. <b>03 OF 04</b>

GNCL - 159

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 Sep 11, 2018 - 10:14am



SCHEDULE H  
Preliminary Functional Road Plan (Phase 1 & Phase 2)



<b>City of Richmond</b> <small>9811 No. 3 Road Richmond B.C. V6V 3C1</small>				
ISSUE	DESCRIPTION	DR	CH	DATE
A	ISSUED FOR OCP AMENDMENT CONSIDERATIONS	RM	MY	10 SEP/18



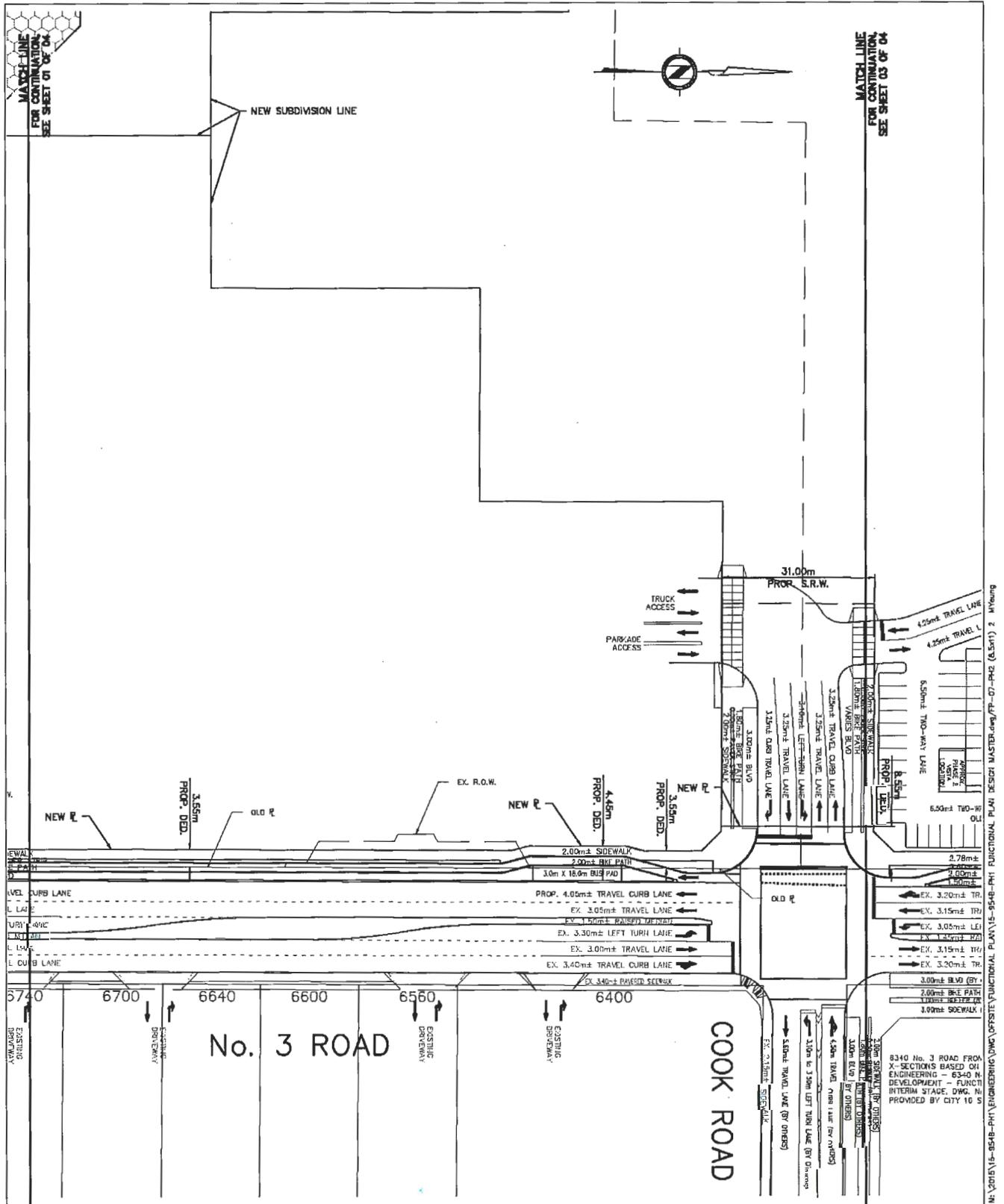
#1818 - 1177 West Hastings Street, Vancouver, B.C. Canada V6E 2K0  
Tel: (604) 678-8434, Fax: (604) 678-8435 Email: info@aplinmartin.com

CITY FILE #: <b>CP 16-752923</b>	AM PROJECT No: <b>15-954B-PH1-OCF-FP-06</b>
TITLE: <b>PRELIMINARY FUNCTIONAL PLAN PHASE 2</b>	
DESIGN:MY/RM CHECK:MY DATE: SEP/18	PROJECT ADDRESS: <b>6551 No. 3 ROAD</b>
DRAWN:RM APPR: MY SCALE: 1:1000	SHEET No. <b>01 OF 04</b>

CNCL - 161

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 Sep 11 2018 - 10:27am

SCHEDULE H  
Preliminary Functional Road Plan (Phase 1 & Phase 2)



<b>City of Richmond</b>				
8811 No. 3 Road Richmond B.C. V6Y 3G1				
ISSUE	DESCRIPTION	DR	CH	DATE
A	ISSUED FOR OCP AMENDMENT CONSIDERATIONS	RM	MY	10 SEP/18

**APLIN MARTIN**  
ENGINEERING ARCHITECTURE PLANNING SURVEYING

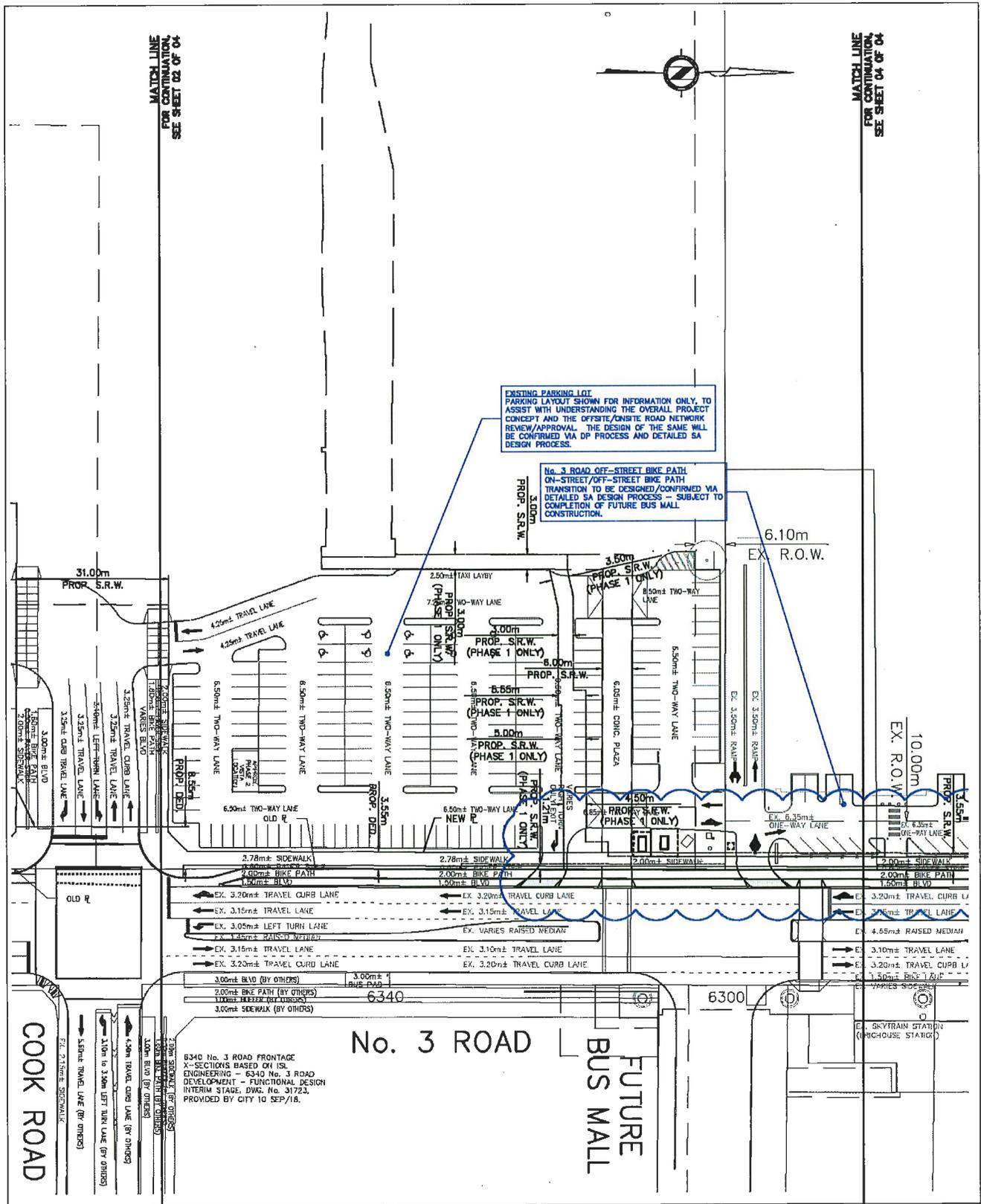
#1818 - 1177 West Hastings Street, Vancouver, B.C. Canada V6E 2K3  
Tel: (604) 670-9414, Fax: (604) 670-9415, Email: info@aplinmartin.com

CITY FILE #: <b>CP 16-752923</b>	ARM PROJECT No: <b>15-954B-PH1-OCP-FP-07</b>		
TITLE: <b>PRELIMINARY FUNCTIONAL PLAN PHASE 2</b>			
DESIGN: WY/RM	CHECK: MY	DATE: SEP/18	PROJECT ADDRESS: <b>6551 No. 3 ROAD</b>
DRAWN: RM	APPR: MY	SCALE: 1:1000	SHEET No. <b>02 OF 04</b>

**CNCL - 162**

Sep 11, 2018 10:20am N:\2016\15-954B-PH1\ENGINEERING\DWG\OFFSITE\FUNCTIONAL PLAN\15-954B-PH1-FUNCTIONAL PLAN DESIGN MASTER.dwg/FP-07-PH2 (A5x11) 2 MyYoung

SCHEDULE H  
Preliminary Functional Road Plan (Phase 1 & Phase 2)



**City of Richmond**  
8811 No. 3 Road Richmond B.C. V6V 3C1

ISSUE	DESCRIPTION	DR	CH	DATE
A	ISSUED FOR OCP AMENDMENT CONSIDERATIONS	RM	MY	10 SEP/18

**APLIN MARTIN**  
ENGINEERING ARCHITECTURE PLANNING SURVEYING

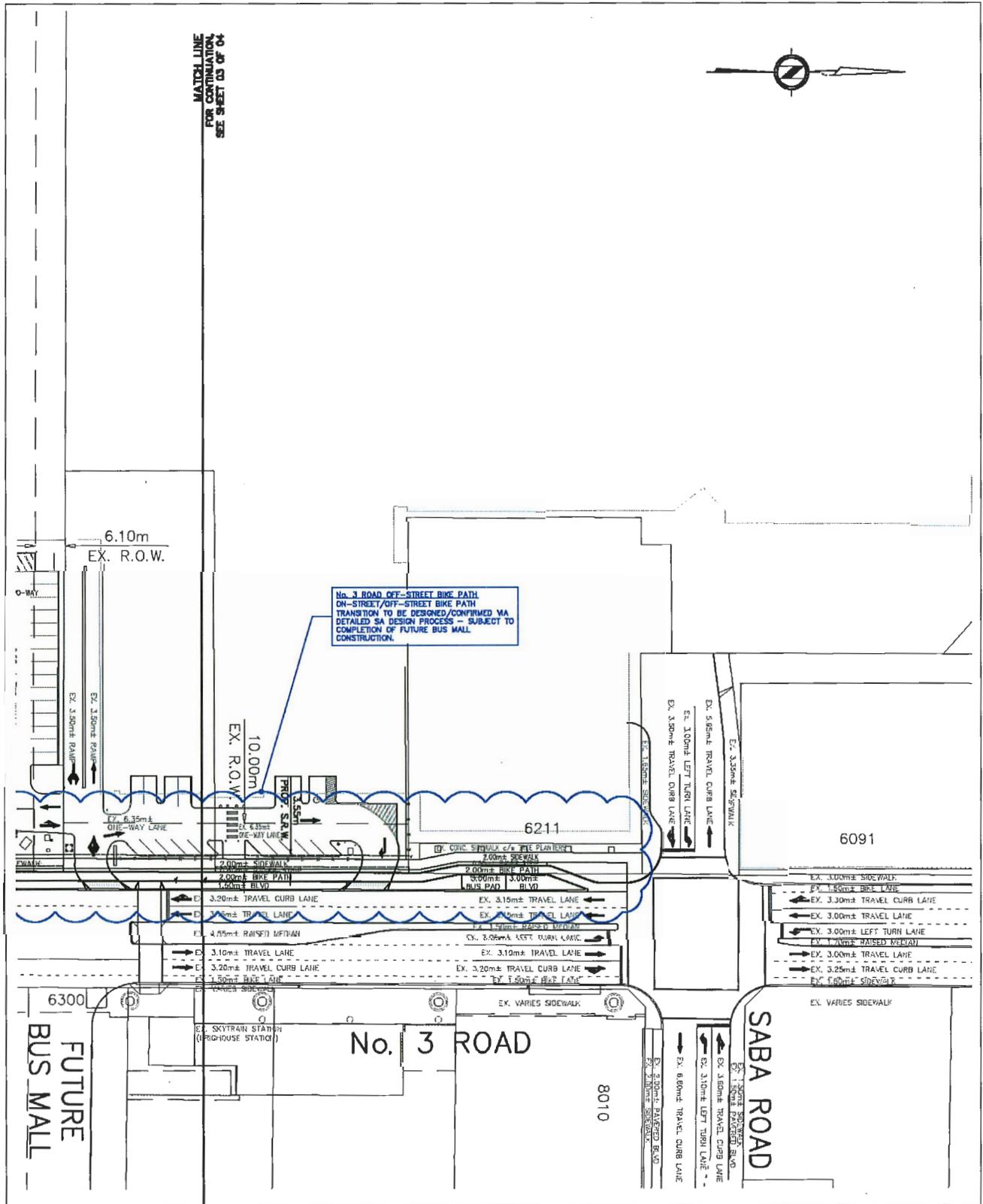
#1818 - 1177 West Hastings Street, Vancouver, B.C. Canada V6E 2K3  
Tel: (604) 679-8424, Fax: (604) 597-9061, Email: general@aplinmartin.com

CITY FILE #: CP 16-752923	A&M PROJECT No: 15-954B-PH1-OCF-FP-08
TITLE: <b>PRELIMINARY FUNCTIONAL PLAN PHASE 2</b>	
DESIGN:MY/RM CHECK:MY DATE: SEP/18	PROJECT ADDRESS: 6551 No. 3 ROAD
DRAWN:RM APPR: MY SCALE: 1:1000	SHEET No. <b>03 OF 04</b>

**CNCL - 163**

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 Sep 11 2018 - 10:20am

SCHEDULE H  
Preliminary Functional Road Plan (Phase 1 & Phase 2)



<b>City of Richmond</b> <small>8111 No. 3 Road Richmond B.C. V6V 3C1</small>				
ISSUE	DESCRIPTION	DR	CH	DATE
A	ISSUED FOR OCP AMENDMENT CONSIDERATIONS	RM	MY	10 SEP/18

**APLIN MARTIN**  
ENGINEERING ARCHITECTURE PLANNING SURVEYING

#1010 - 1177 West Hastings Street, Vancouver, B.C. Canada V6E2K3  
Tel: (604) 678-9434, Fax: (604) 597-9061, Email: gsaera@aplinmartin.com

CITY FILE #: CP 16-752923	ABM PROJECT No: 15-954B-PH1-OCF-FP-10			
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DESIGN:MY/RW	CHECK:MY	DATE: SEP/18	PROJECT ADDRESS: 6551 No. 3 ROAD	SHEET No. 04 OF 04
DRAWN:RM	APPR: MY	SCALE: 1:1000		

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**City Centre “Parking Zone 1” & TDM Strategy Requirements**  
Terms & Conditions

The following Transportation Demand Management (TDM) measures shall be provided in support of the developer’s proposed reduction in parking, as provided for in the Zoning Bylaw (i.e. maximum 10% reduction, based on City Centre Parking Zone 1 rates):

**1) TDM Measures:**

- a. Mobility hubs, including:
  - Mobility Hub 1 (Local Hub) within the western portion of the site, with typical elements/features summarized in the Mobility Hub Vision (**Schedule J**), with exact details to be finalized as part of the Phase 1 DP application.
  - Mobility Hub 2 (Regional Hub) within the eastern portion of the site, with typical elements/features summarized in the Mobility Hub Vision (**Schedule J**), with exact details to be finalized as part of the Phase 2 DP application.
- b. For each Phase 1 and Phase 2, provide an end of trip bicycle facilities (showers and changing rooms for retail uses) and maintenance tools located in the bicycle storage area. (Sizes and features to be confirmed through the DP approval processes.)
- c. Bicycle maintenance and repair facilities in each of the residential towers. (Sizes and features to be confirmed through the DP approval processes.)
- d. Transit passes:
  - Residential: monthly transit passes (2-zone for one year) offered to 25% of the market units and 100% of affordable units
  - Retail: \$100,000 for the purchase of 2-zone transit passes or equivalent for use by the employees and customers
- d. Complete off-site improvements to enhance pedestrian walkability at the following locations:
  - Minoru Boulevard/Granville Avenue: Upgrade/enhance existing pedestrian crosswalks and upgrade of the existing traffic signal
  - Minoru Boulevard/Library Crossing: Upgrade of existing pedestrian crosswalk to include stamped and coloured asphalt pavement surface with Duratherm or equivalent
  - No. 3 Road/ Granville Avenue: Upgrade/enhance existing pedestrian crosswalks and upgrade of the existing traffic signal
  - No. 3 Road/ Anderson Road: Upgrade of existing pedestrian crosswalk to include stamped and coloured asphalt pavement surface with Duratherm or equivalent

Note: Pedestrian crosswalk enhancements/upgrades include a wider crosswalk (i.e., min. 4.5m wide) and universal accessibility features installed on all wheelchair ramps. Traffic signal upgrades include the following works but not limited to: install new, upgrade and/or replace signal pole, controller, base and hardware, pole base, detection, conduits (electrical & communications), signal indications, communications cable, electrical wiring, service conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).

**2) Timing of TDM Implementation:**

- a. Phase 1:
  - Mobility Hub 1- Local Hub
  - Minoru Boulevard/Granville Avenue: Upgrade/enhance existing pedestrian crosswalks and upgrade of the existing traffic signal
  - Minoru Boulevard/New City Hall Street: Upgrade of existing pedestrian crosswalk to include stamped and coloured asphalt with Duratherm or equivalent
- b. Phase 2:
  - Mobility Hub 2- Regional Hub
  - No. 3 Road/ Granville Avenue: Upgrade/enhance existing pedestrian crosswalks and upgrade of the existing traffic signal
  - No. 3 Road/ Anderson Road: Upgrade of existing pedestrian crosswalk to include stamped and coloured asphalt pavement surface with Duratherm or equivalent

# CF Richmond Centre

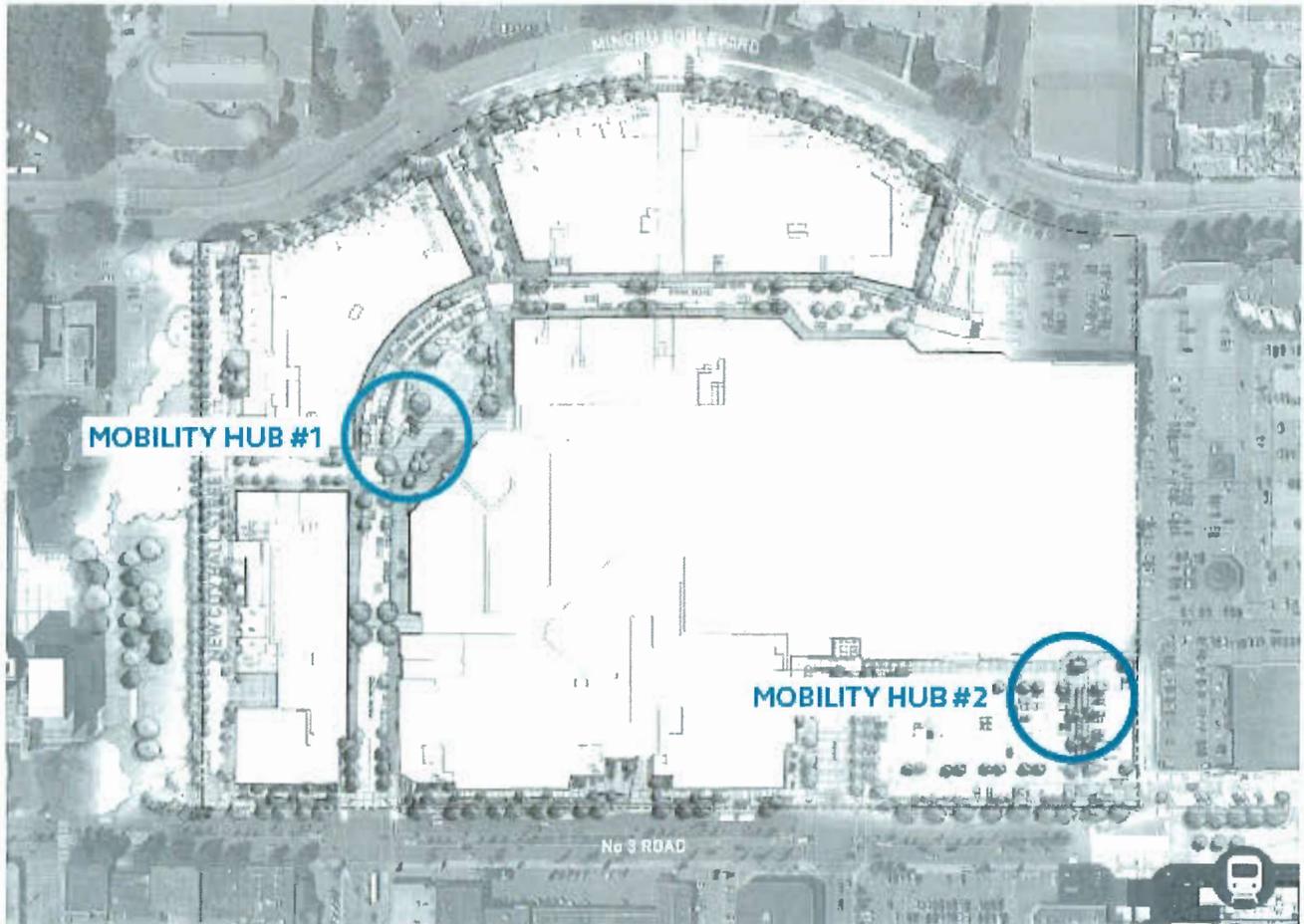
## MOBILITY HUB VISION

- PEDESTRIAN CONNECTIONS
- BICYCLE CONNECTIONS
- TRANSIT CONNECTIONS
- VEHICLE CONNECTIONS

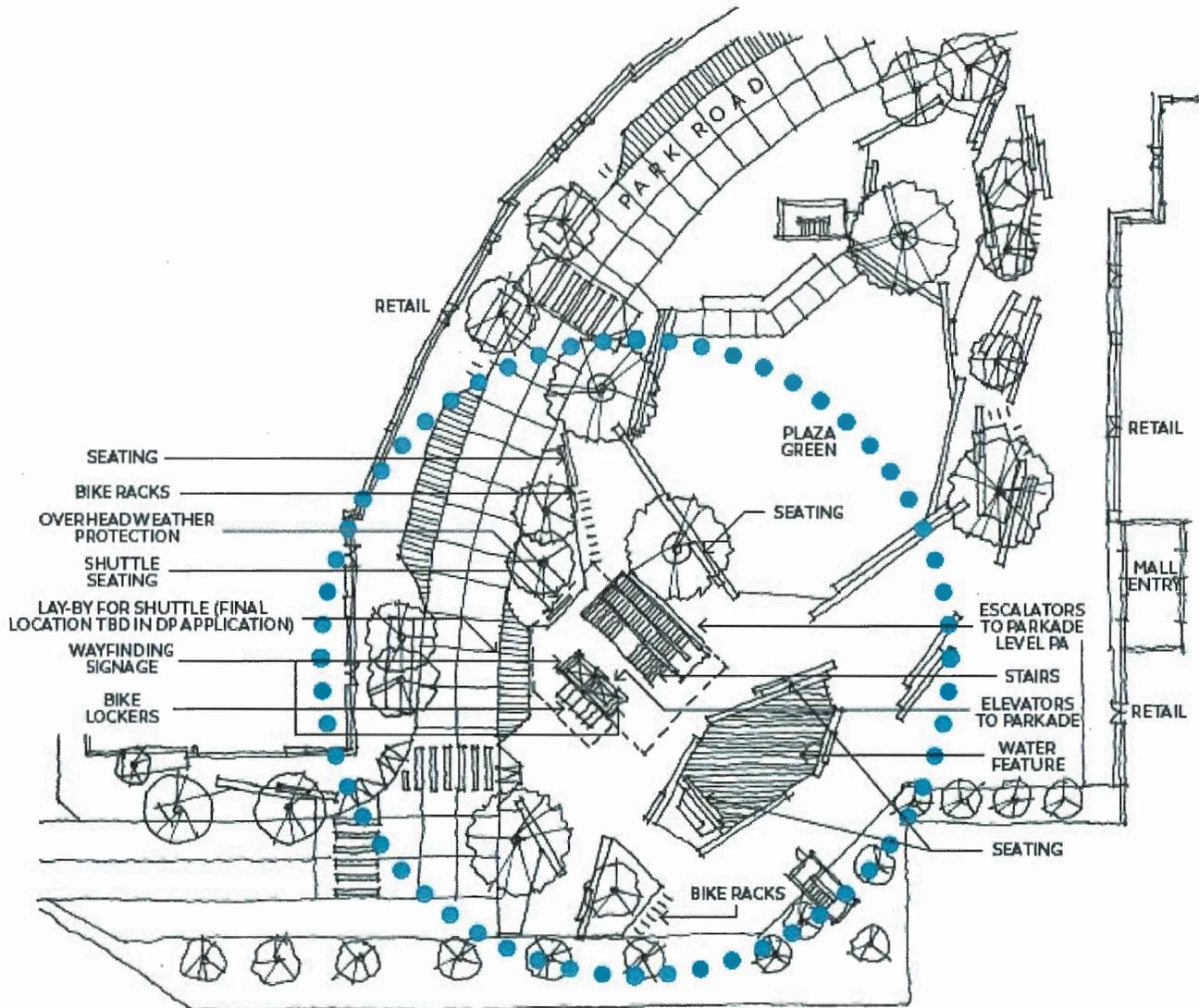
19 June 2018



# CF RICHMOND CENTRE SITE PLAN



# MOBILITY HUB #1 (LOCAL HUB) CONCEPTUAL PLAN



PLAZA LEVEL PLAN



## MOBILITY HUB FEATURES @ GROUND LEVEL

1. SHUTTLE LAY-BY
2. SHUTTLE SEATING AREA
3. BIKE LOCKERS (4-5)
4. BIKE RACKS
5. OVERHEAD WEATHER PROTECTION
6. LIGHTING
7. WAY-FINDING SIGNAGE

\*PRELIMINARY ESTIMATE OF MOBILITY MODES AND FEATURES; FINAL PROGRAM TO BE DETERMINED WITH PHASE 1 DP APPLICATION

# MOBILITY HUB #1 (LOCAL HUB) CONCEPTUAL VISION



ILLUSTRATIVE VISION

WEATHER  
PROTECTION

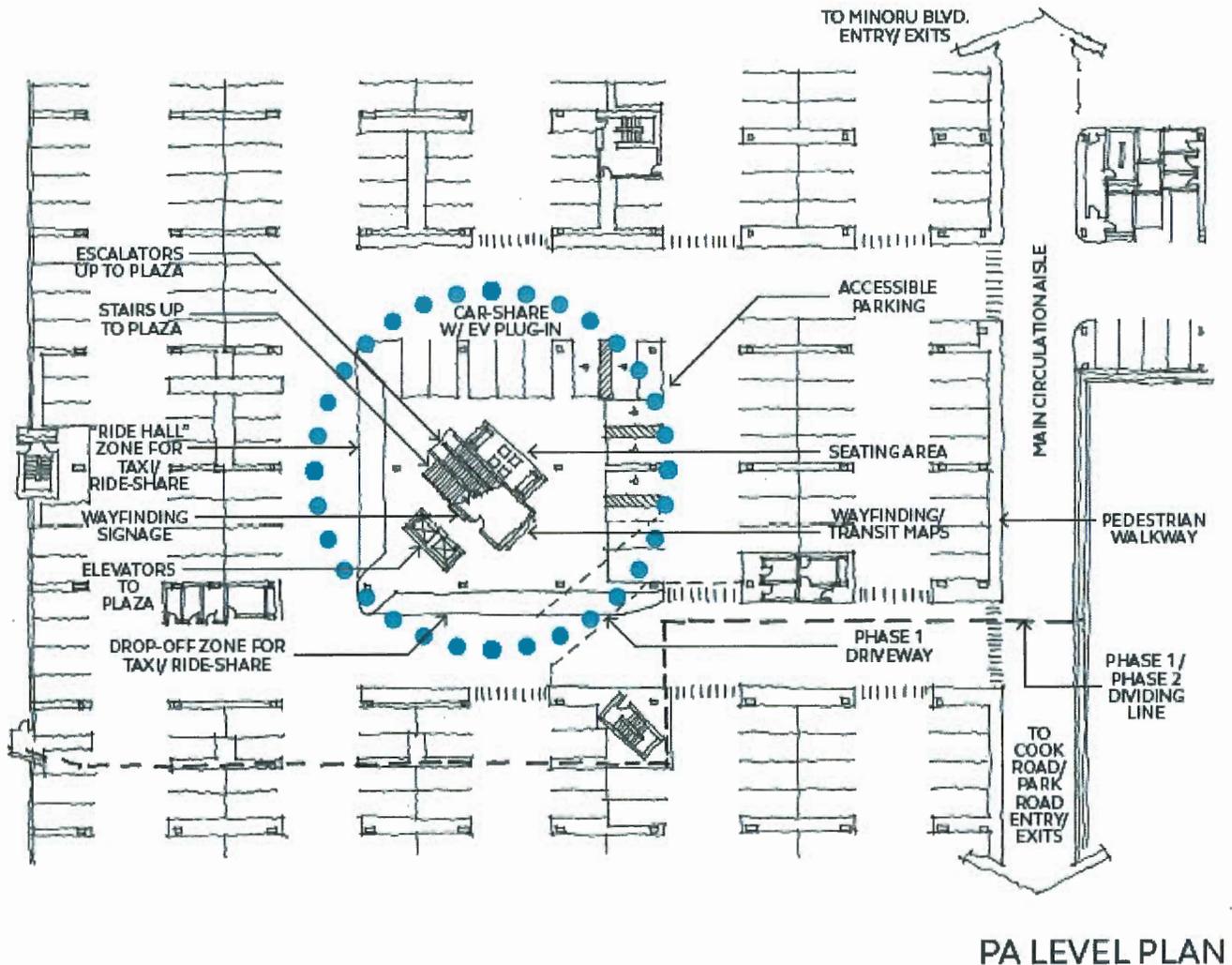
BIKE  
LOCKERS

WAYFINDING  
SIGNAGE

SEATING

ELEVATORS/ STAIRS/  
ESCALATORS/  
DOWN TO PA LEVEL  
PARKING

# MOBILITY HUB #1 (LOCAL HUB) CONCEPTUAL PLAN

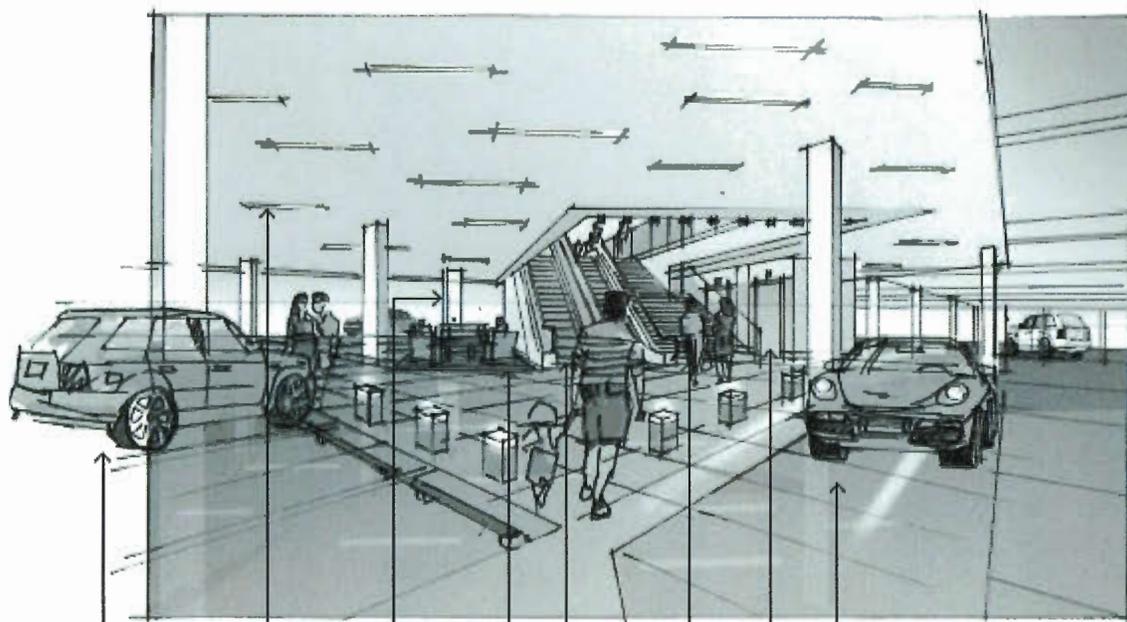


## MOBILITY HUB FEATURES UNDERGROUND @ LEVEL PA

1. CAR SHARE VEHICLES (3-4)
2. CAR SHARE PARKING SPACES (W/ EV PLUG-IN) (RESERVED PARKING FOR CAR-SHARE VEHICLES) (3-4)
3. TAXI PARKING / PICK-UP / DROP-OFF (4-5)
4. SEATING / WAITING AREA
5. WAY-FINDING SIGNAGE
6. TRANSIT MAPS / SCHEDULES
7. LIGHTING

\*PRELIMINARY ESTIMATE OF MOBILITY MODES AND FEATURES; FINAL PROGRAM TO BE DETERMINED WITH PHASE 1 DP APPLICATION

# MOBILITY HUB #1 (LOCAL HUB) CONCEPTUAL VISION



ILLUSTRATIVE VISION

CAR-SHARE  
PARKING  
(W/ EV)

QUALITY  
LIGHTING

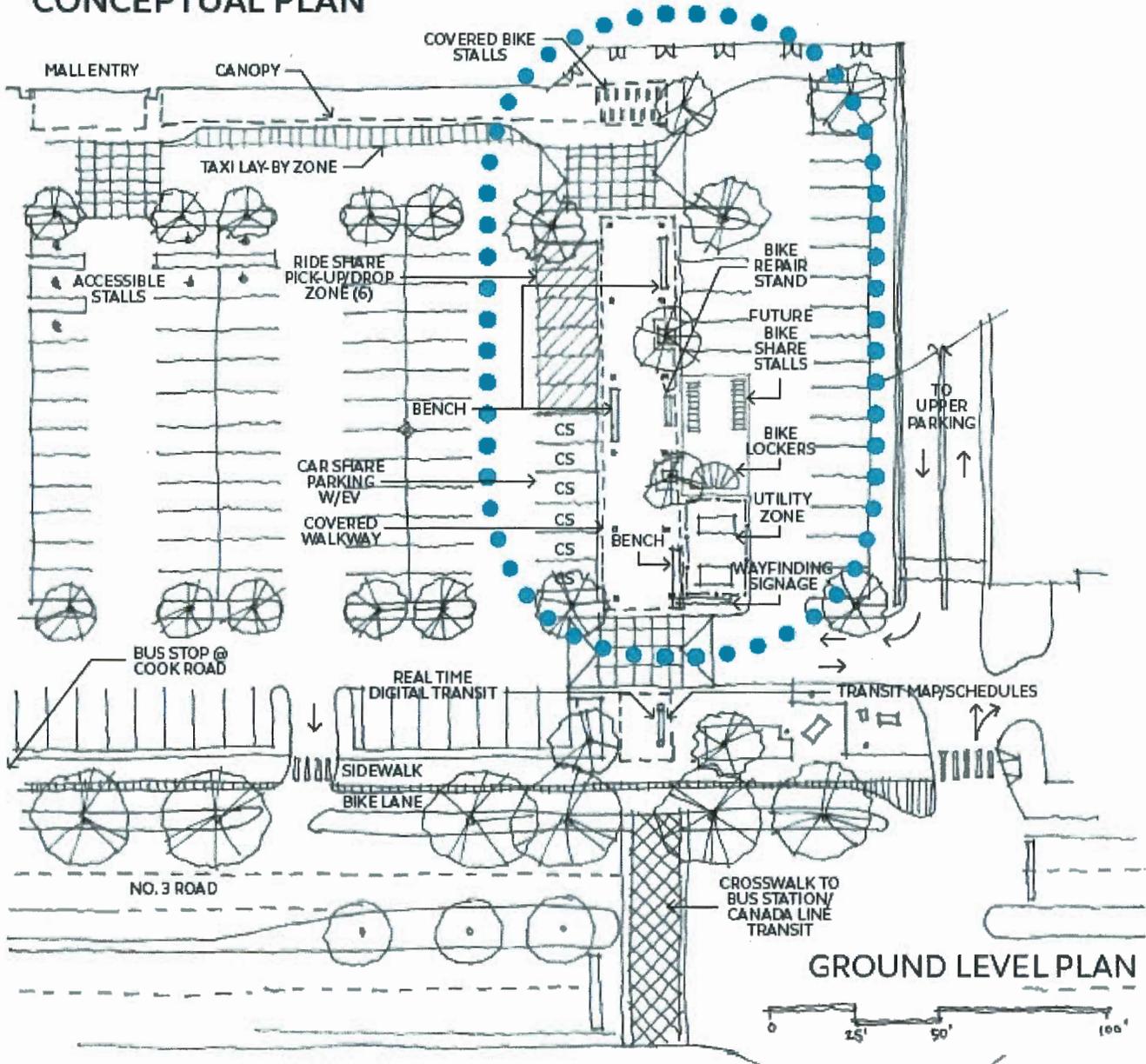
WAYFINDING  
SIGNAGE

SEATING  
AREA

ESCALATORS/  
STAIRS/ ELEVATORS  
UP TO PLAZA LEVEL/  
MALL ENTRY

"RIDE HALL"  
ZONE FOR  
TAXI/ RIDE-  
SHARE

# MOBILITY HUB #2 (REGIONAL HUB) CONCEPTUAL PLAN

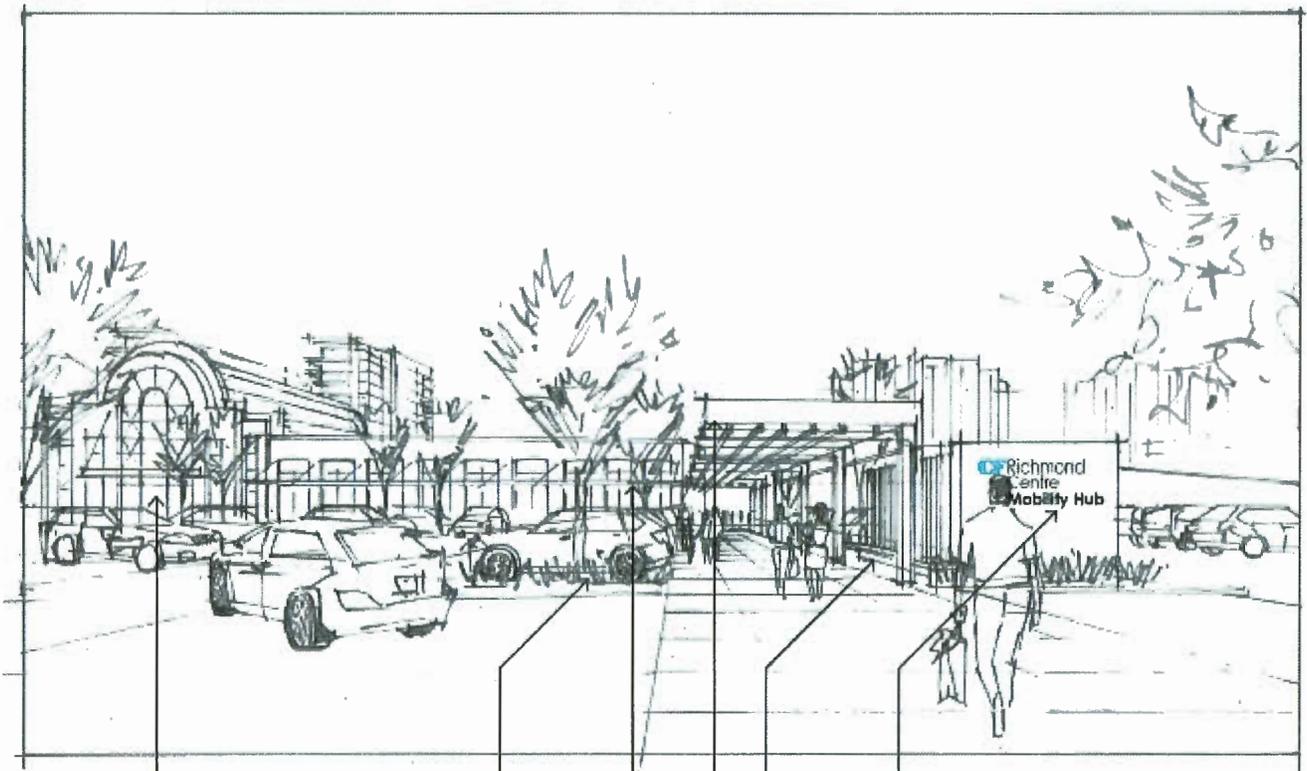


GROUND LEVEL PLAN

## MOBILITY HUB FEATURES

- |  |   |   |
|--|---|---|
| <ol style="list-style-type: none"> <li>1. RIDE SHARE VEHICLES (5-6)</li> <li>2. CAR SHARE PARKING SPACES (W/ EV PLUG-IN) (RESERVED PARKING FOR CAR-SHARE VEHICLES) (5-6)</li> <li>3. TAXI PARKING / PICK-UP / DROP-OFF (4-5)</li> <li>4. COVERED BICYCLE STALLS (20-25)</li> <li>5. BICYCLE LOCKERS (4-5)</li> <li>6. BIKE SHARE SPACES DOCKED SYSTEM OR IF DOCKLESS; PROVIDE "PARKING ZONE" (10-12)</li> <li>7. BICYCLE REPAIR STAND (TOOLS: I.E. WRENCHES, PUMP, ETC) (1)</li> </ol> | <ol style="list-style-type: none"> <li>8. WATER FOUNTAIN / BOTTLE FILL STATION</li> <li>9. COVERED WALKWAYS</li> <li>10. BENCHES / SEATING AREA</li> <li>11. LIGHTING</li> <li>12. WAY-FINDING SIGNAGE</li> <li>13. MAP / TRANSIT MAP / SCHEDULES</li> <li>14. REAL TIME DIGITAL TRANSIT MESSAGE BOARD</li> <li>15. BUS SHELTER W/ BENCHES (ADJACENT @ COOK ROAD AND AT BUS STATION)</li> </ol> | <p>*PRELIMINARY ESTIMATE OF MOBILITY MODES AND FEATURES; FINAL PROGRAM TO BE DETERMINED WITH PHASE 1 DP APPLICATION</p> |
|--|---|---|

# MOBILITY HUB #2 (REGIONAL HUB) CONCEPTUAL VISION



ILLUSTRATIVE VISION

MALL ENTRY

CAR-SHARE  
PARKING  
(W/ EV)

COVERED  
WALKWAY

BENCH  
SEATING

WAYFINDING  
SIGNAGE

**Additional Development Requirements**  
Terms & Conditions

1. **NAV Canada Building Heights:** Prior to Development Permit\* issuance, on a lot-by-lot basis, the developer shall submit a letter of confirmation from a registered surveyor assuring that the proposed building heights are in compliance with Transport Canada regulations.
2. **Family-Friendly Housing Unit Mix:** Prior to Development Permit\* issuance, on a lot-by-lot basis, in whole or in part, the developer shall demonstrate that the development provides for a housing unit mix that includes at least 50% 2- and 3-bedroom, "family-friendly" units (in some combination of market-ownership, market rental, and affordable housing units) or as otherwise determined to the satisfaction of the Director of Development and Manager of Community Social Services through the Development Permit\* approval processes. Prior to Development Permit\* issuance, the developer may be required to register legal agreement(s) on title to one or both lots to secure the developer's commitment to designing and constructing the approved housing unit mix, as determined to the City's satisfaction.
3. **Public Art:** Prior to Development Permit\* issuance, on a lot-by-lot basis, covenant(s) and/or alternative legal agreement(s) shall be registered on title to Lot 1 (West) and Lot 2 (East), to the satisfaction of the City, securing the owner's commitment to voluntarily contribute towards public art, on a lot-by-lot basis, in compliance with the Council-approved private development public art program policy and/or related requirements in effect at the time of development approval. The covenant and/or alternative legal agreement(s) shall include various development holds for the purpose of securing the developer's public art contribution in accordance with City policy and shall include, but may not be limited to, the preparation of a detailed public art plan for each lot, Council and/or advisory committee approval(s), the delivery of the developer's contribution in some combination of cash and/or security (Letter of Credit), and the installation and maintenance of the public art on City property and/or within statutory rights of way(s) on the lots, all at the developer's/owner's sole cost. More specifically:
  - 3.1. "No development" shall be permitted on Lot 1 (West) and/or Lot 2 (East), restricting Development Permit\* issuance, on a lot-by-lot basis, for any building on Lot 1 (West) and Lot 2 (East) that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, until the developer:
    - 3.1.1. Submits a Detailed Public Art Plan for the lot, to the satisfaction of the City, that:
      - a) Includes the entirety of the lot, together with related public open space and public road, as determined to the City's satisfaction;
      - b) Is prepared by an appropriate professional and based on the Richmond Public Art Program, City Centre Public Art Plan, and any relevant supplementary public art and heritage planning undertaken by the City for Brighthouse Village, to the satisfaction of the Director of Development and Director, Arts, Culture, and Heritage Services (including review(s) by the Public Art Advisory Committee and presentation for endorsement by Council, as required by the Director, Arts, Culture, and Heritage Services); and
      - c) Account for the full value of the developer's voluntary public art contribution for the lot, which value shall be based on:
        - i) The maximum buildable floor area approved under the Development Permit\* for the lot, excluding standard floor area ratio (FAR) exemptions and affordable housing; and
        - ii) Minimum rates of:
          - For Lot 1 (West): \$0.83 per buildable square foot of residential uses and \$0.44 per buildable square foot of non-residential uses; and
          - For Lot 2 (East): The applicable Council-approved rate(s) in effect at the time of Development Permit\* issuance;
    - 3.1.2. Registers legal agreement(s) on title to facilitate the implementation of the City-approved Detailed Public Art Plan for the lot, to the City's satisfaction; and

- 3.1.3. Submits a Letter of Credit and/or cash (as determined at the sole discretion of the City) to secure the developer’s implementation of the City-approved Detailed Public Art Plan for the lot, the value of which shall be at least the full value of the developer’s voluntary public art contribution for the lot as set out in the City-approved Public Art Plan.
- 3.2. “No occupancy” shall be permitted on Lot 1 (West) or Lot 2 (East), restricting final Building Permit\* inspection granting occupancy, on a lot-by-lot basis, for any building on the lot that includes any residential use and/or increase in gross leasable floor area on the lot, as determined in the City's discretion, in whole or in part (excluding parking intended as an ancillary use to non-parking uses), for which the City-approved Detailed Public Art Plan requires the developer’s implementation of a public artwork(s) until:
  - 3.2.1. The developer, at the developer’s sole expense, commissions an artist(s) to conceive, create, manufacture, design, and oversee or provide input about the manufacturing of the public artwork, and causes the public artwork to be installed on City property, if expressly permitted by the City, or within a statutory right-of-way on the developer’s lands (which right-of-way shall be to the satisfaction of the City for rights of public passage, public art, and related purposes, in accordance with the City-approved Detailed Public Art Plan);
  - 3.2.2. The developer, at the developer’s sole expense and within thirty (30) days of the date on which the public art is installed, executes and delivers to the City a transfer of all of the developer’s rights, title, and interest in the public artwork to the City if on City property or to the subsequent Strata or property owner if on private property (including transfer of joint world-wide copyright) or as otherwise determined to be satisfactory by the City Solicitor and Director, Arts, Culture, and Heritage Services; and
 

NOTE: It is the understanding of the City that the artist’s rights, title, and interest in the public artwork will be transferred to the developer upon acceptance of the artwork based on an agreement solely between the developer and the artist. These rights will in turn be transferred to the City if on City property, subject to approval by Council to accept the transfer of ownership of the artwork.
  - 3.2.3. The developer, at the developer’s sole expense, submits a final report to the City promptly after completion of the installation of the public art in respect to the City-approved Detailed Public Art Plan, which report shall, to the satisfaction of the Director of Development and Director, Arts, Culture, and Heritage Services, include:
    - a) Information regarding the siting of the public art, a brief biography of the artist(s), a statement from the artist(s) on the public art, and other such details as the Director of Development and Director, Arts, Culture, and Heritage Services may require;
    - b) A statutory declaration, satisfactory to the City Solicitor, confirming that the developer’s financial obligation(s) to the artist(s) have been fully satisfied;
    - c) The maintenance plan for the public art prepared by the artist(s); and
    - d) Digital records (e.g., photographic images) of the public art, to the satisfaction of the Director of Development and Director, Arts, Culture, and Heritage Services.
- 4. **Electric Vehicle (EV) Charging Infrastructure for Vehicles & “Class 1” Bicycle Storage:** Prior to Development Permit\* issuance, on a lot-by-lot basis, covenant(s) and/or alternative legal agreement(s) shall be registered on title to Lot 1 (West) and Lot 2 (East), to the satisfaction of the City, securing the owner's commitment to voluntarily provide, install, and maintain EV charging infrastructure within the building on Lot 1 (West), Lot 2 (East), and Remainder Lot (North) for the use of the building’s residents, commercial tenants, and others as determined to the satisfaction of the City through an approved Development Permit\*. More specifically, the minimum permitted rates for EV charging infrastructure shall be as indicated in the following table or as per the Official Community Plan or Zoning Bylaw rates in effect at the time of Development Permit\* approval , whichever is greatest.

User/Use	Energized Outlet – Minimum Permitted Rates	
	Vehicle Parking (1)	“Class 1” (Secured) Bike Storage (2)
Market Residential (i.e. resident parking & bike storage)	1 per parking space (for exclusive use)	1 per each 10 bikes or portion thereof in a bike storage room or locker (which Energized Outlet shall be located to facilitate shared use with bikes in the room/locker)
Affordable Housing (i.e. resident parking & bike storage)	1 per parking space (for exclusive use)	
Non-Residential (i.e. tenant/employee bike storage)	N/A	1 per each 10 bikes or portion thereof in a bike storage room or locker (which Energized Outlet shall be located to facilitate shared use by bikes when secured in the room/locker)

(1) “Vehicle Parking” “Energized Outlet” shall mean all the wiring, electrical equipment, and related infrastructure necessary to provide Level 2 charging (as per SAE International’s J1772 standard) or higher to an electric vehicle.

*NOTE: Richmond’s Electric Vehicle Charging Infrastructure Bylaw provides that, where an electric vehicle energy management system is implemented, the Director of Engineering may specify a minimum performance standard to ensure a sufficient rate of electric vehicle charging. For the purposes of the Bylaw, electric vehicle energy management system means a system to control electric vehicle supply equipment electrical loads comprised of monitor(s), communications equipment, controller(s), timer(s), and other applicable devices.*

(2) “Class 1 (Secured) Bike Storage” “Energized Outlet” shall mean an operational 120V duplex outlet for the charging of an electric bicycle and all the wiring, electrical equipment, and related infrastructure necessary to provide the required electricity for the operation of such an outlet.

5. **Tree Retention, Removal & Replacement:** Prior to Development Permit\* issuance, on a lot-by-lot basis, as per standard City policy and procedures, the developer shall, based on a Certified Arborist’s Report approved by the City, register legal agreement(s) on title to the lot, submit security and/or cash-in-lieu compensation, and/or implement other measures, to the satisfaction of the City, with respect to the retention, removal, and replacement of on-site and off-site (City) trees impacted by the proposed development.

**Standard City Legal Requirements**  
Terms & Conditions

1. **Flood Construction Covenants:** Registration of flood indemnity covenants on title to Lot 1 (West), Lot 2 (East), and Remainder Lot (North), as per Flood Plain Designation and Protection Bylaw, Area “A” (i.e. minimum flood construction level of 2.9 m GSC).
2. **Aircraft Noise Covenants:** Registration of the City’s standard aircraft noise sensitive development (ANSD) covenants on title to Lot 1 (West), Lot 2 (East), and Remainder Lot (North). The owner-developer shall notify all initial purchasers of the potential aircraft noise impacts. Furthermore, on a phase-by-phase basis, prior to each Development Permit\* and Building Permit\* issuance, the owner-developer shall submit a report(s) and/or letter(s) of assurance prepared by an appropriate registered professional, which demonstrates that the interior noise levels and thermal conditions comply with the City’s Official Community Plan and Noise Bylaw requirements. The standard required for air conditioning systems and their alternatives (e.g. ground source heat pumps, heat exchangers and acoustic ducting) is the ASHRAE 55-2004 “Thermal Environmental Conditions for Human Occupancy” standard and subsequent updates as they may occur.

Maximum interior noise levels (decibels) within dwelling units must achieve CMHC standards follows:

Portions of Dwelling Units	Noise Levels (decibels)
Bedrooms	35 decibels
Living, dining, recreation rooms	40 decibels
Kitchen, bathrooms, hallways, and utility rooms	45 decibels

Applicable ANSD covenants shall include:

- a) Lot 1 (West): Mixed use covenant;
  - b) Lot 2 (East): Mixed use covenant; and
  - c) Remainder Lot (North): Non-sensitive use covenant.
3. **Canada Line Covenants:** Registration of a legal agreement(s) on title to Lot 2 (East) and Remainder Lot (North) requiring that the proposed development on the lots must be designed and constructed in a manner that mitigates potential Canada Line impacts (e.g., noise from trains and public areas, vibration, overlook, light spillage) on proposed adjacent dwelling units and other potential sensitive uses. The owner-developer shall notify all initial purchasers of the potential Canada Line impacts. Furthermore, on a phase-by-phase basis, prior to each Development Permit\* and Building Permit\* issuance, the owner-developer shall submit a report(s) and/or letter(s) of assurance prepared by an appropriate registered professional, which demonstrate that, among other things, for residential uses the interior noise levels and thermal conditions comply with City objectives including, for air conditioning systems and their alternatives (e.g. ground source heat pumps, heat exchangers and acoustic ducting), compliance with the ASHRAE 55-2004 “Thermal Environmental Conditions for Human Occupancy” standard and subsequent updates as they may occur and, for maximum interior noise levels (decibels) within dwelling units, CMHC standards as per the above table with respect to the “Aircraft Noise” agreement.
  4. **View Blockage & Other Development Impacts Covenants:** Registration of a restrictive covenant(s) on title to Lot 1 (West) and Lot 2 (East), to the satisfaction of the City, requiring that development on the lots must be designed and constructed in a manner that mitigates potential development impacts including without limitation view obstruction, increased shading, increased overlook, reduced privacy, increased ambient noise, increased ambient night-time light, and increased public use of fronting streets, sidewalks, and open spaces caused by or experienced as a result of, in whole or in part, development on the lands and future development on or the use of surrounding properties. In particular, the covenant shall notify residential tenants in mixed use buildings of potential noise and/or nuisance that may arise due to proximity to retail, restaurant, other commercial, and community uses and activities. The owner-developer shall notify all initial purchasers of the potential development impacts. Through the Development Permit\* approval processes, the developer shall submit a report(s) to the City, to be attached to the legal agreement(s), prepared by an appropriate registered professional, which demonstrates that adequate development impact mitigation measures will be incorporated into the building design. Prior to Building Permit\* issuance and final Building Permit\* inspection granting occupancy, on a lot-by-lot basis, in whole or in part, the developer shall submit letter(s) of

confirmation prepared by an appropriate registered professional, which confirms that the building design and construction comply with the report(s) approved at Development Permit\* stage.

5. **Tandem Parking Covenants:** Registration of a legal agreement(s) on title to Lot 1 (West) and Lot 2 (East) for the purpose of ensuring that:
- a) For Valet Commercial Parking: Where the owner operates a valet parking service, to the satisfaction of the City, two parking spaces may be provided in a tandem arrangement for the use of commercial business owners, tenants, employees, and/or customers and/or the general public as determined to the satisfaction of the City and approved through a Development Permit\*, which may include the registration of legal agreement(s) on title if required by the City;
  - b) For Market-Ownership Residential Parking: Where two parking spaces are provided in a tandem arrangement for the use of the occupants of residential dwellings (excluding affordable housing and market rental dwellings), as per the Zoning Bylaw, both parking spaces must be assigned to the same dwelling unit; and
  - c) Elsewhere: Tandem parking shall be prohibited for all other purposes including, but not limited to, parking for residential visitors, affordable housing and market rental dwellings, commercial uses (except those commercial uses served by Valet Commercial Parking, as determined to the satisfaction of the City and approved through a Development Permit\*), child care, community amenity uses, and car-share spaces.





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**Richmond Official Community Plan Bylaw 7100 and Bylaw 9000  
Amendment Bylaw 9892 (CP 16-752923)  
6551 No. 3 Road**

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

1. Richmond Official Community Plan Bylaw 9000 is amended at Attachment 1 to Schedule 1, 2041 OCP Land Use Map, for the area marked as “A” on “Schedule A attached to and forming part of Bylaw 9892”, by replacing the existing land use designation of area “A” with “Park”.
2. Richmond Official Community Plan Bylaw 7100, in Schedule 2.10 (City Centre Area Plan), is amended by:
  - 2.1. Replacing page 2-20, including the Pedestrian-Oriented Retail Precincts Map, with “Schedule B attached to and forming part of Bylaw 9892”;
  - 2.2. On page 2-24, entitled 2.3 Mobility & Access – Objective, replacing the last bullet with “Car-Free Measures: encourage measures that support car-free lifestyles through enhanced first-to-last kilometre connectivity, alternative transportation options, and home delivery services and facilities.”
  - 2.3. On page 2-25, entitled 2.3 Mobility & Access, Policies, following 2.3.8(a), inserting the following as 2.3.8(b): “Multi-Modal Mobility Hubs – Establish transportation network nodes that seamlessly integrate multiple travel options, supportive infrastructure, and placemaking strategies to create pedestrian-friendly centres that help to maximize first-to-last kilometre connectivity without need for private motor vehicles.”
  - 2.4. Replacing page 2-27, including the Street Network Map (2031), with “Schedule C attached to and forming part of Bylaw 9892”;
  - 2.5. Replacing page 2-36, including Pedestrian Environment Map (2031), with “Schedule D attached to and forming part of Bylaw 9892”;
  - 2.6. Replacing page 2-38, including the Cycling Network Map (2031), with “Schedule E attached to and forming part of Bylaw 9892”;
  - 2.7. Replacing page 2-42, including the Goods Movement & Loading Map (2031), with “Schedule F attached to and forming part of Bylaw 9892”;

- 2.8. Replacing page 2-44, entitled 2.3.8 Fostering a Car-Free Lifestyle, with “Schedule G attached to and forming part of Bylaw 9892”;
- 2.9. Replacing page 2-46, including the Arts & Culture Map (2031), with “Schedule H attached to and forming part of Bylaw 9892”;
- 2.10. Replacing page 2-50, including the Public Spaces & Places Map (2031), with “Schedule I attached to and forming part of Bylaw 9892”;
- 2.11. Replacing page 2-65, including the Base Level Parks & Open Space Map (2031), with “Schedule J attached to and forming part of Bylaw 9892”;
- 2.12. Replacing page 2-68, including the Neighbourhood Parks Map, with “Schedule K attached to and forming part of Bylaw 9892”;
- 2.13. Replacing page 2-71, including the Pedestrian Linkages Map, with “Schedule L attached to and forming part of Bylaw 9892”;
- 2.14. Replacing page 2-88, including the Public Realm Areas Map, with “Schedule M attached to and forming part of Bylaw 9892”;
- 2.15. Replacing page 2-113, including the Tower Spacing & Floorplate Size Map, with “Schedule N attached to and forming part of Bylaw 9892”;
- 2.16. Replacing page 2-116, including the Preferred Frontage Conditions Map, with “Schedule O attached to and forming part of Bylaw 9892”;
- 2.17. On page 3-1 (3.0 Development Permit Guidelines), following the Sub-Area Guidelines bullet, inserting a new bullet as follows: “Special Precinct Guidelines: Special development standards intended to contribute towards placemaking by encouraging distinct identities supportive of local mobility, ecology, culture, and urban form opportunities”;
- 2.18. On page 3-4, inserting a new Development Permit Special Precinct Key Map as shown in “Schedule P attached to and forming part of Bylaw 9892”;
- 2.19. Following section 3.2 Sub-Area Guidelines, inserting section 3.3.1 Special Precinct 1.0 – Richmond Centre South (Brighthouse Village) as shown in “Schedule Q attached to and forming part of Bylaw 9892”;
- 2.20. Replacing the Generalized Land Use Map (2031) with “Schedule R attached to and forming part of Bylaw 9892”;
- 2.21. Replacing the Specific Land Use Map: Brighthouse Village (2031) with “Schedule S attached to and forming part of Bylaw 9892”; and

- 2.22. Making various text and graphic amendments to accommodate the identified bylaw amendments and to ensure consistency with the Generalized Land Use Map (2031) and Specific Land Use Map: Brighthouse Village (2031) as amended.
- 3. This Bylaw may be cited as **“Richmond Official Community Plan Bylaw 7100 and Bylaw 9000, Amendment Bylaw 9892”**.

FIRST READING

\_\_\_\_\_

PUBLIC HEARING

\_\_\_\_\_

SECOND READING

\_\_\_\_\_

THIRD READING

\_\_\_\_\_

OTHER CONDITIONS SATISFIED

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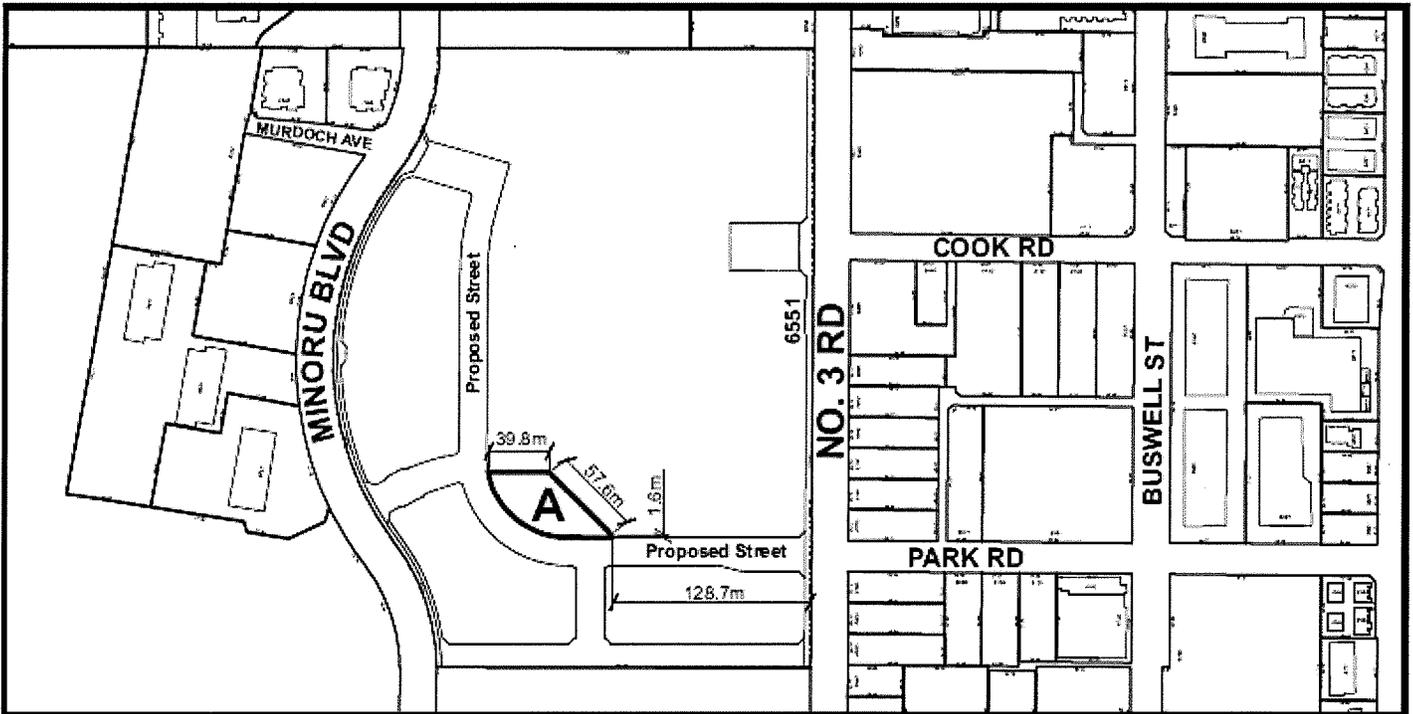
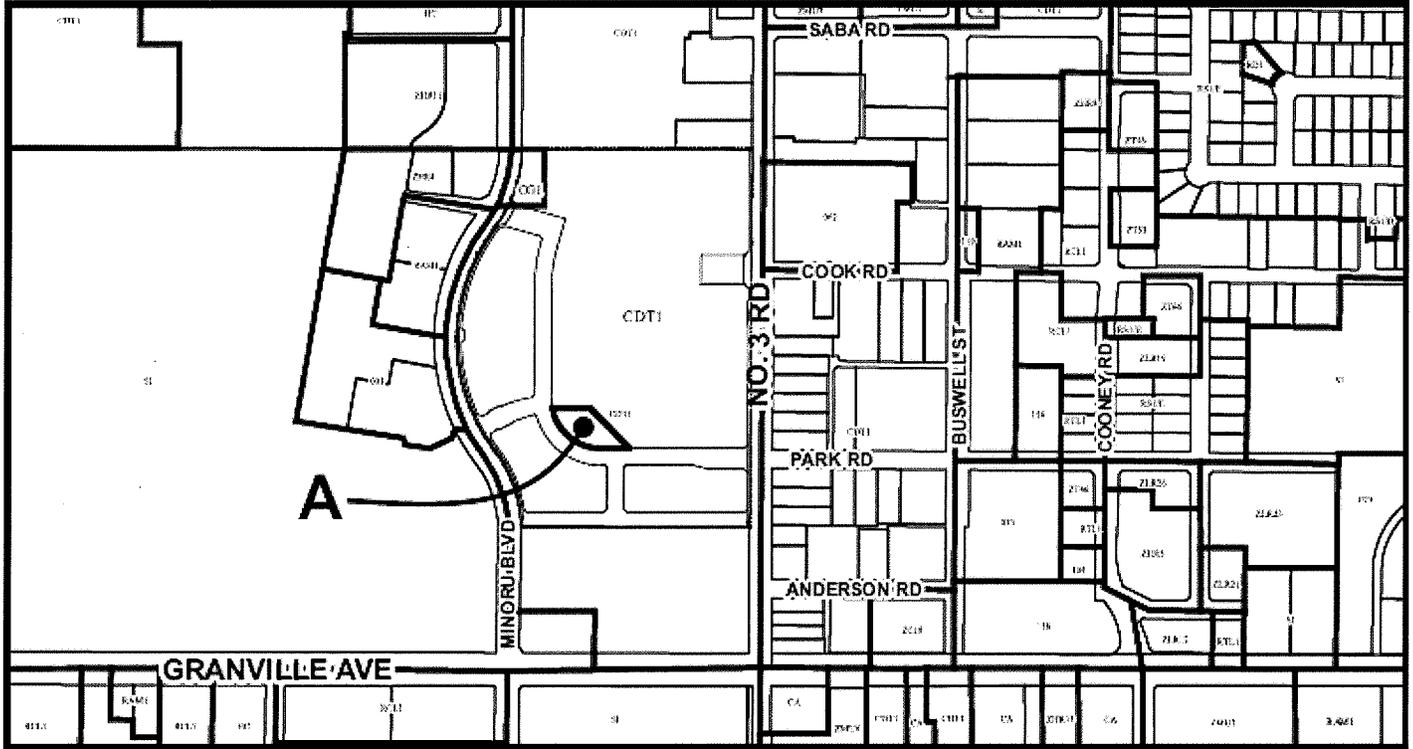
ADOPTED

\_\_\_\_\_

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CORPORATE OFFICER





OCP Amendment Bylaw 9892  
(CP 16-752923)

Original Date: 09/12/18

Revision Date:

Note: Dimensions are in METRES

### 2.2.3(d) Pedestrian-Oriented Retail Precincts

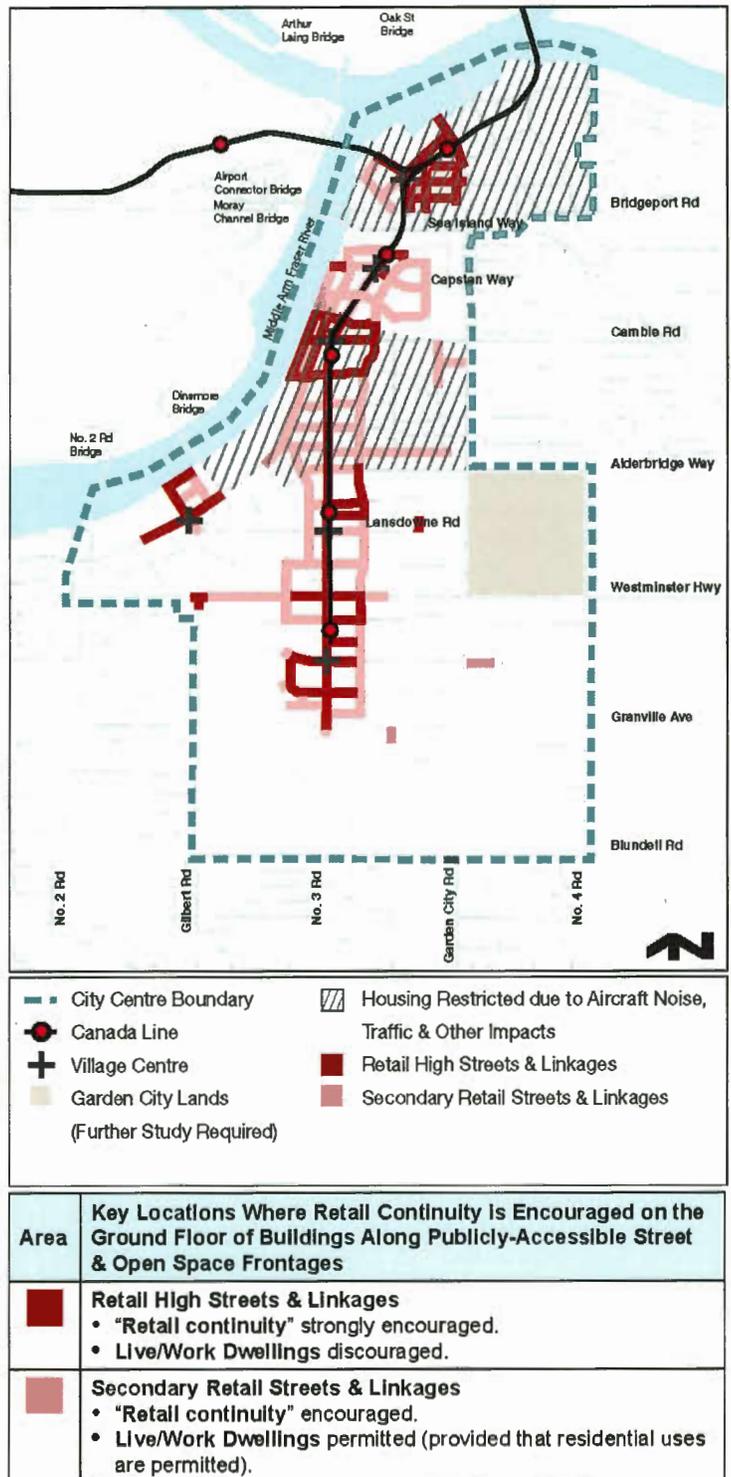
Lively, urban retail areas require “retail continuity”: the continuity of a substantial amount of ground floor frontages that are attractive, pedestrian-oriented, rich in detail, and engaging – in other words, frontages that encourage people to walk and linger, and include:

- a diversity of activities (e.g., shops and restaurants);
- a high degree of transparency enabling interaction between activities inside the building and the fronting sidewalk or open space (e.g., display windows and views into shop interiors);
- small unit frontages, typically no more than 10 m (33 ft.) wide, each with its own entry;
- multi-tenant building entries, hotels, and large commercial units with ground floor frontage widths of no more than 10 m (33 ft.), unless special measures are employed to maintain retail continuity;
- office and similar uses situated above the ground floor;
- pedestrian weather protection;
- pedestrian-oriented and scaled signage and lighting;
- public art, seating, and other public amenities;
- quality, durable materials and construction.

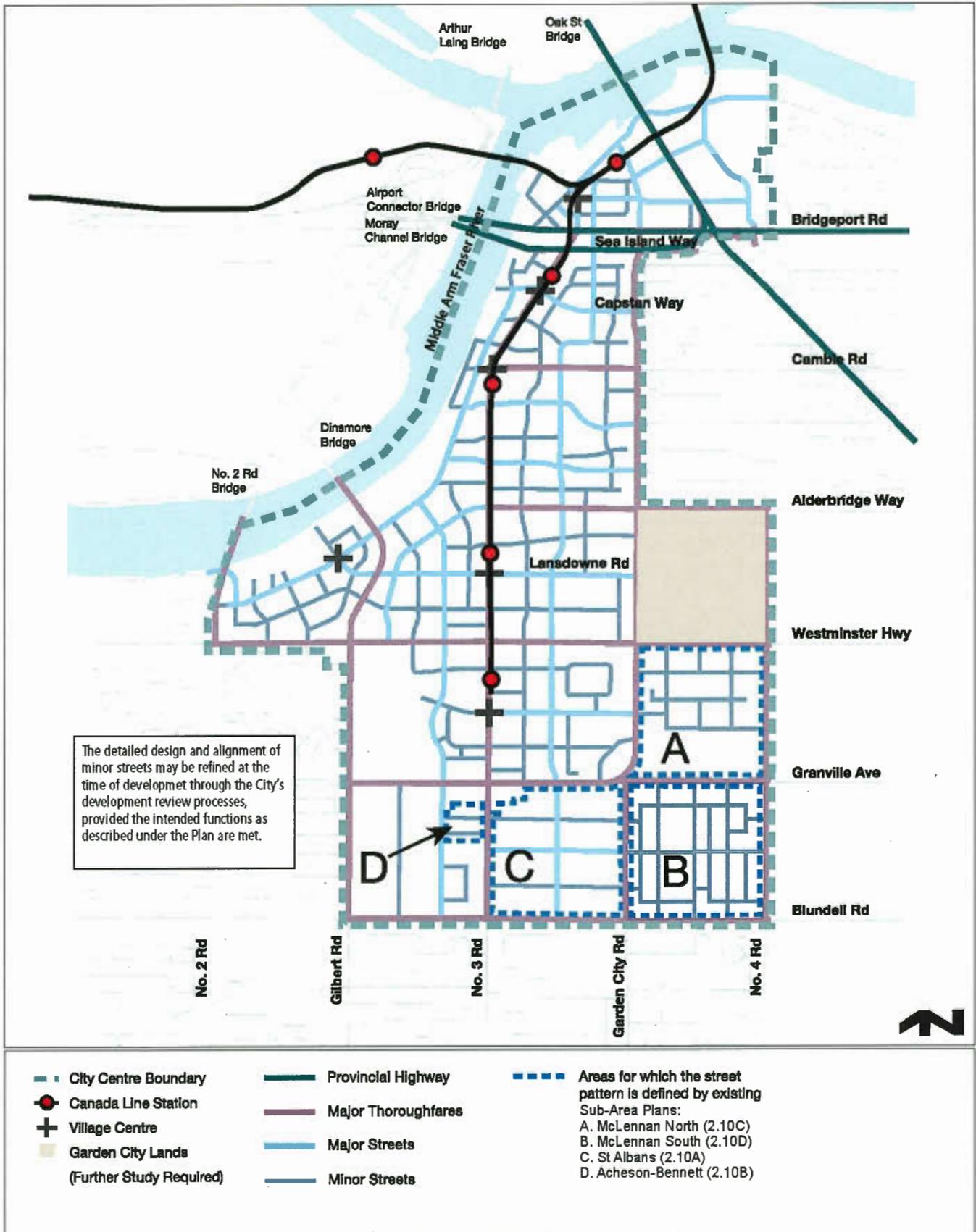
In addition, a successful retail area requires commercial units that can accommodate and adapt to the needs of a variety of businesses over time. To help achieve this, commercial retail units should have a depth of:

- typical - 18 m (59 ft.) or more;
- minimum - 9 m (30 ft.).

### Pedestrian-Oriented Retail Precincts Map



## Street Network Map (2031)



**Walking Features**

**Street Network**

- Every street is walkable and has a sidewalk, a minimum of 2.0 m (6.5 ft.) wide and preferably 2.5 m (8.2 ft.) wide, with street trees, boulevards and pedestrian lighting.
- Shorter city blocks, narrower street crossings and conveniently timed pedestrian signals.
- Increased curbside parking on minor streets acts as a buffer from adjacent vehicle traffic.
- A wayfinding system to guide pedestrians to key destinations.
- An enhanced pedestrian-cyclist crossings at selected locations, particularly near schools.

**Streetscape**

- A creative, fun and welcoming environment for pedestrians via landscaping, artwork, attractive street furniture, open spaces, gathering places, and resting areas.
- Orient ground level businesses to pedestrian access from the sidewalk.
- Continuous store awnings provide weather protection.

**Transit Villages & Connections**

- Transit schedules and route information available at transit stations and bus stops.
- Fully accessible transit stops conveniently located and easily recognizable with sufficient space for waiting passengers.
- Covered walkways provided between transit stops and village centres.

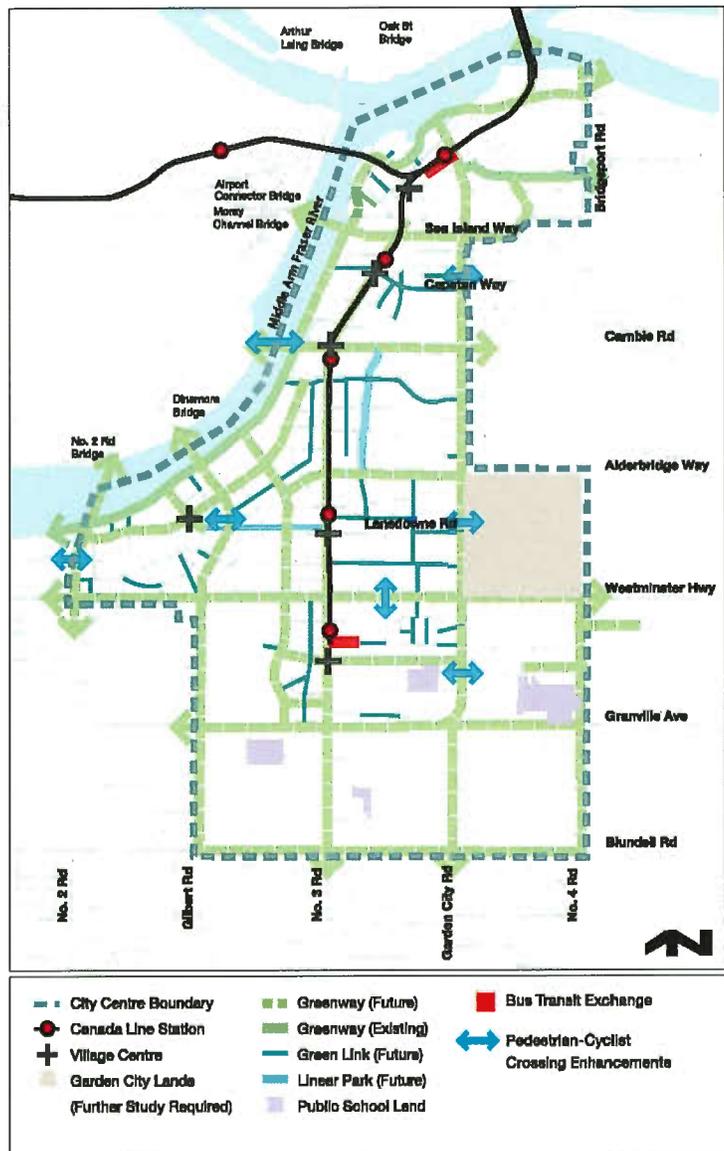
**Urban Greenways & Trails**

- Enhanced streetscape features along urban greenways and within pedestrian precincts around transit villages.
- Improved trails along the dyke and new links across water boundaries (e.g., Middle and North Arms of the Fraser River).

**Accessibility**

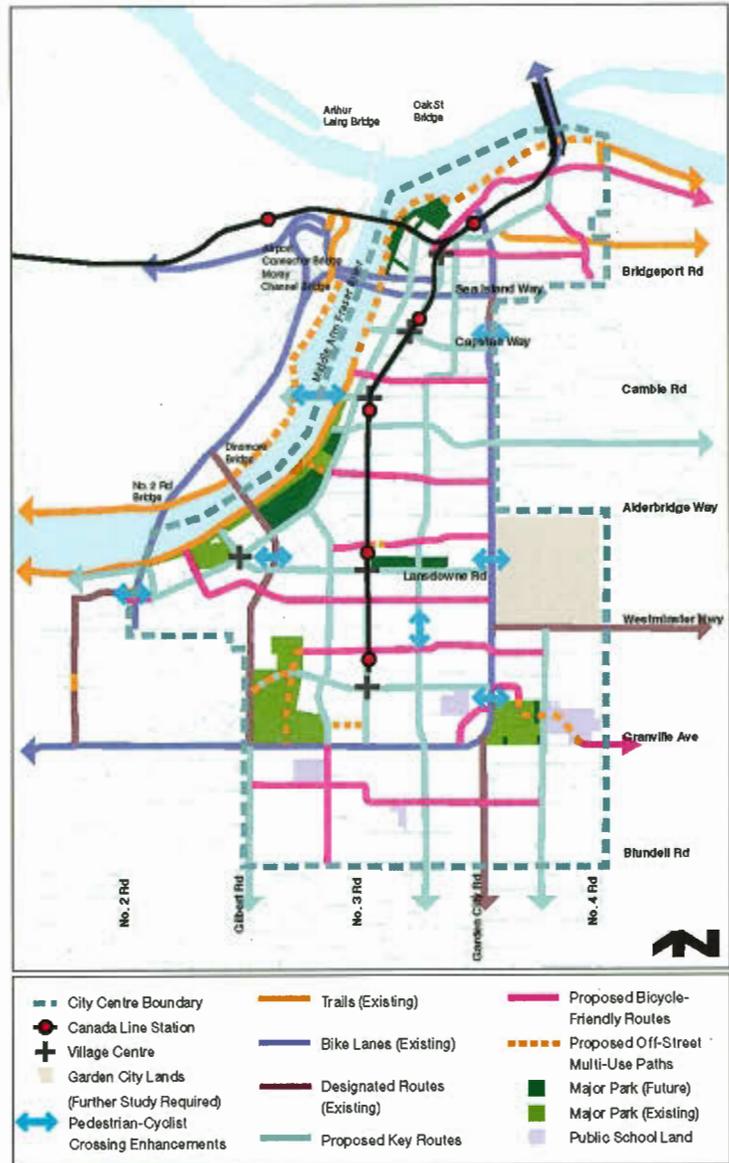
- Enhanced use of universal accessible design features such as accessible pedestrian signals and tactile wayfinding.
- Lighting along trail networks where feasible.
- Priority given to pedestrian access and safety through parking lots.
- Installation of ramps at all intersections.

**Pedestrian Environment Map (2031)**



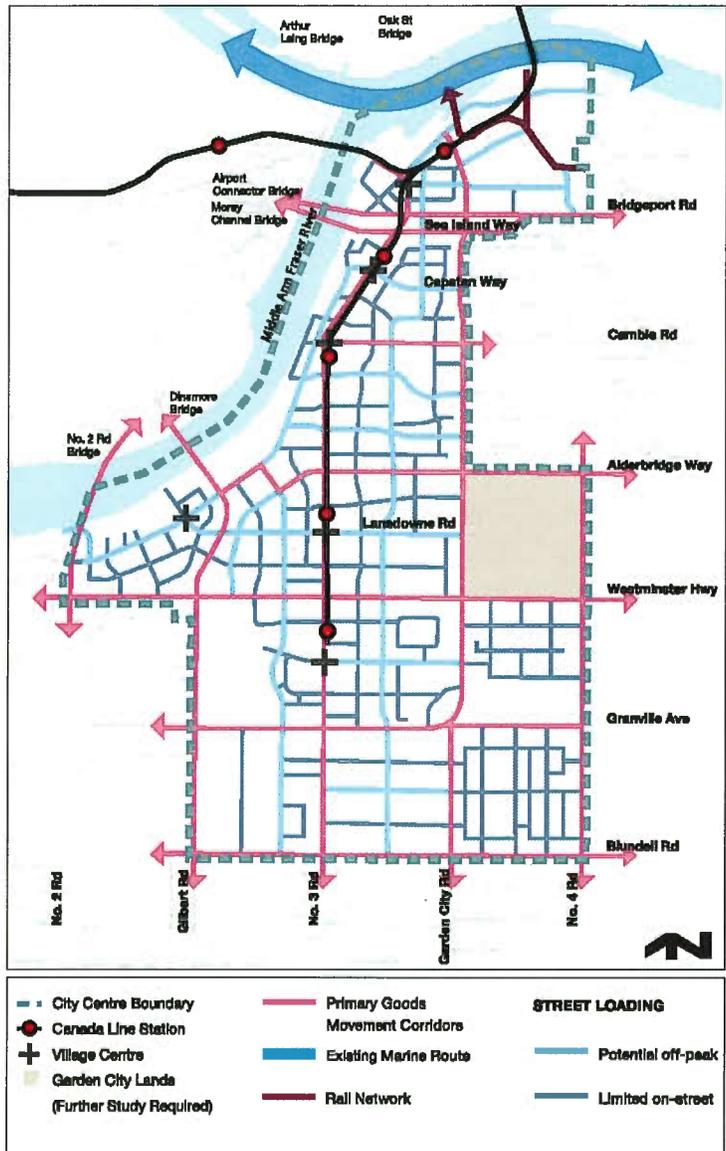
Cycling Network Features	
<b>Accommodation on Street Network</b>	<ul style="list-style-type: none"> <li>• Provide signage and pavement markings to clearly delineate cycling facilities from other street components.</li> <li>• Minimize potential conflicts and safely accommodate multiple road users such as transit service and cycling.</li> <li>• Enhanced pedestrian-cyclist crossings at selected locations, particularly near schools.</li> </ul>
<b>Designated Cycling Routes</b>	<ul style="list-style-type: none"> <li>• Designated routes feature signage, pavement markings and bicycle-friendly traffic signals.</li> <li>• Designated bike lanes on major thoroughfares and some major streets with a typical width of 1.5 m to 1.8 m (5 ft. to 6 ft.).</li> <li>• Cycling routes are physically separated from vehicle traffic on major thoroughfares and major streets where feasible.</li> <li>• Shared wide curb lanes on some major streets and on minor streets with typical width of 4.3 m (14.1 ft.).</li> <li>• Bicycle-friendly routes feature pavement markings, signage and signal loop detectors but road is not widened.</li> </ul>
<b>Trails &amp; Bridges</b>	<ul style="list-style-type: none"> <li>• Integration of on-street cycling network with off-street trails and pathways including the Canada Line Bridge over the North Arm of the Fraser River.</li> <li>• Off-street pathways have typical width of 3.0 m to 4.0 m (10 ft. to 13.1 ft.).</li> <li>• Proposed new pedestrian/cycling bridge from the west end of Cambie Road to Sea Island.</li> </ul>
<b>End-of-Trip Facilities</b>	<ul style="list-style-type: none"> <li>• Secure end-of-trip facilities (bike racks, lockers, cages) at civic sites, parks, transit villages, and activity centres.</li> <li>• Bylaw requirement for all new developments to provide short-term and long-term secure bicycle parking.</li> </ul>
<b>Integration with Transit</b>	<ul style="list-style-type: none"> <li>• Bicycle accommodation on the Canada Line and all buses during all hours of operation.</li> <li>• Bike racks and bike lockers at all rapid transit stations and transit exchanges.</li> </ul>
<b>Promotion &amp; Education</b>	<ul style="list-style-type: none"> <li>• Safe cycling courses for adults and children.</li> <li>• Area-wide event to promote cycling for all trips.</li> <li>• Education and enforcement programs to encourage sharing the road among motorists and cyclists.</li> </ul>

Cycling Network Map (2031)



<p><b>Goods Movement &amp; Emergency Services Features</b></p> <p><b>Goods Movement Corridors</b></p> <ul style="list-style-type: none"> <li>Major thoroughfares and streets act as the primary goods movement corridors with minor streets and lanes providing access for local deliveries and loading.</li> <li>Support other modes of goods movement such as rail in the Bridgeport area and the potential for short-sea shipping routes along the Fraser River.</li> </ul>
<p><b>Loading Locations</b></p> <ul style="list-style-type: none"> <li>Provide off-street loading docks within parking areas for zones of high trucking activity.</li> <li>Construction loading zones provided where feasible to facilitate pick up and drop off of construction materials and minimize traffic disruption.</li> <li>Service lanes and mews are the preferred on-street locations.</li> <li>Limited to areas adjacent to on-street parking on minor streets.</li> <li>Available on some major streets in off-peak periods but not permitted on major thoroughfares.</li> </ul>
<p><b>Emergency Services</b></p> <ul style="list-style-type: none"> <li>Priority is given to emergency service access and timely response.</li> <li>Major thoroughfares and some major and minor street intersections incorporate traffic signal pre-emption capability.</li> <li>Parking regulations ensure that lanes and mews are kept accessible for emergency vehicles.</li> <li>Consider response time requirements for emergency services when identifying priority routes.</li> </ul>
<p><b>Planning &amp; Policy</b></p> <ul style="list-style-type: none"> <li>Maintain liaison with the Provincial Emergency Program to protect local disaster response routes as part of the regional network.</li> <li>Restrict unnecessary dangerous goods movement in City Centre.</li> <li>Seek to minimize response times when planning the site of future emergency service facilities.</li> <li>On-going liaison with stakeholders (e.g., trucking industry) to enhance goods movement.</li> </ul>

## Goods Movement & Loading Map (2031)



### 2.3.8 Fostering a Car-Free Lifestyle

The key success indicator for fostering a car-free lifestyle is:

*An improved shift to sustainable travel modes resulting from the enhanced convenience, flexibility, attractiveness, and integration of alternative transportation options within the urban fabric.*

#### Challenges

- People often make multi-purpose trips or need to carry bulky items, which can make giving up a car difficult.
- People may need their cars for work.
- Some activities can be difficult to access without a car (e.g., skiing, team sports, child care).
- It can be challenging to introduce new travel options in developed, high density areas.

The principles of transit-oriented development and complete communities together with the complementary policies and key directions for each component of the transportation system jointly seek to foster a “car-free” lifestyle as a viable option for City Centre households over time.

Ideally, with more choices, it will be possible and even desirable for residents to have only one or perhaps no private car at all. If an effective range of mobility choices, infrastructure, services, and supporting initiatives are in place, the car-free lifestyle becomes feasible and public investments in the Canada Line and the transit system, as well as the City’s commitment to sustainability, are maximized.

#### Strategies

- Implement measures, such as multi-modal mobility hubs, to encourage people to walk, cycle, and take transit.
- Foster attractive, pedestrian-friendly, transit-orient, urban villages.
- Ensure convenient access to alternative travel options and superior first-to-last kilometre connectivity.
- Encourage customer service and residential development models that make it easier to shop and do day-to-day activities without a car.



Checklist for a Car-Free Lifestyle	
<b>Transit Villages</b>	<ul style="list-style-type: none"> <li>• Compact, mixed use development that enables easy walking to convenient transit linkages.</li> <li>• The daily needs of City Centre residents and workers are within reach of walking and transit.</li> </ul>
<b>Access to Transportation Options</b>	<ul style="list-style-type: none"> <li>• Enable people to conveniently access multiple travel modes when needed so that owning a motor vehicle is not necessary.</li> <li>• Encourage all developments to support car-free lifestyles by supporting transit, car- and bike-share, ride hailing, taxis, autonomous cars, and multi-modal mobility hubs.</li> <li>• Encourage retail and other destination-type uses to provide priority space for travel modes that support car-free lifestyles.</li> </ul>
<b>Access to Retail Goods and Services</b>	<ul style="list-style-type: none"> <li>• Encourage retail uses to provide home pick-up and delivery services, ideally at no or minimal cost.</li> <li>• Encourage retailers to schedule delivery and pick-up at times when residents are most likely to be at home and traffic volumes are low (e.g., evenings).</li> <li>• Encourage retailers and other service providers (e.g., furniture movers) to avoid the use of large vehicles that are difficult to accommodate in dense urban areas.</li> <li>• Encourage co-ordinated delivery services for multi-tenant retail developments.</li> <li>• Ensure that sidewalks and pathways have sufficient width to accommodate pedestrian modes including scooters and handcarts.</li> </ul>
<b>Home Delivery &amp; Pick-Up Services</b>	<ul style="list-style-type: none"> <li>• Encourage residential developments to provide spaces for concierge services to enable home deliveries and pick-up (e.g., groceries, drycleaning, etc.).</li> <li>• Ensure sufficient common space/secure areas for the temporary storage of goods to be picked-up and deliveries until the owner arrives home.</li> <li>• Ensure that loading areas are publicly accessible for larger delivery trucks and publicly accessible.</li> <li>• Provide on-street loading zones, where feasible, to allow for home delivery/pick-up in higher density projects without off-street parking or service lanes are not readily available.</li> </ul>

## Multi-Modal Mobility Hubs

**Purpose:** multi-modal mobility hubs are key transportation network nodes designed to seamlessly integrate multiple travel modes, supportive infrastructure, and placemaking strategies with the aim of creating pedestrian-oriented centres that help to maximize first-to-last kilometre connectivity.

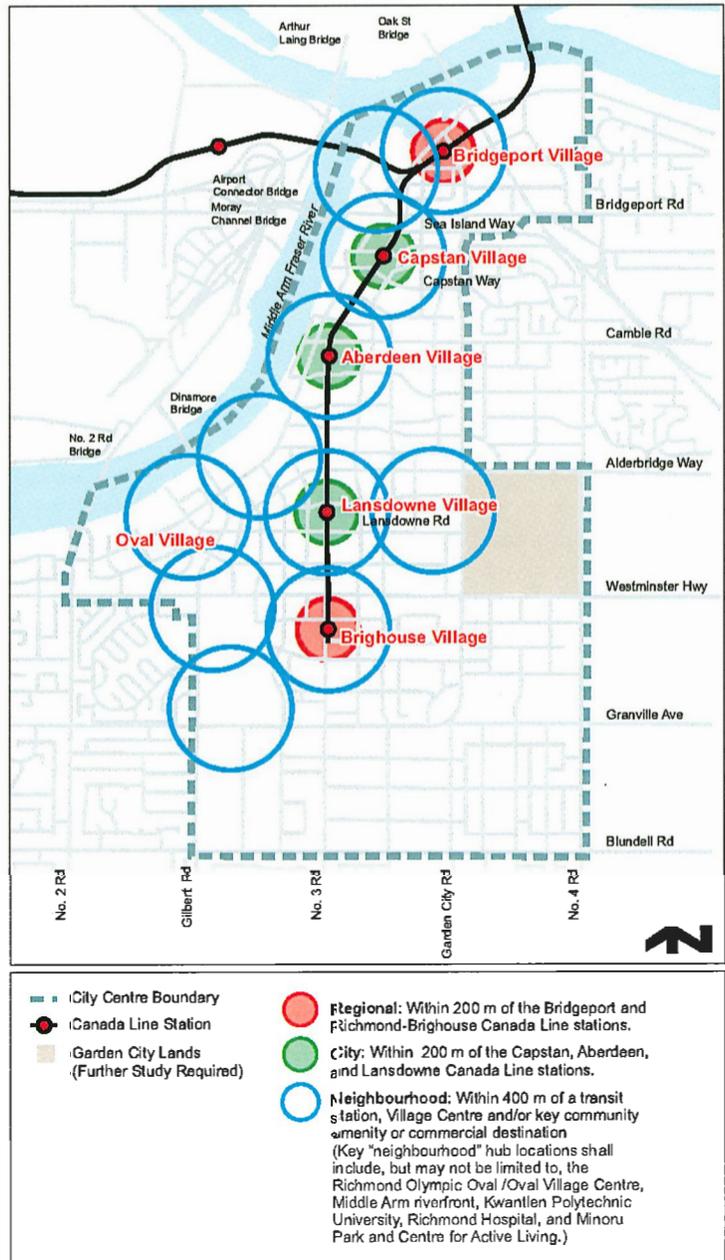
**Features:** effective mobility hubs require an integrated suite of pedestrian-friendly, transportation and related features, which may include, but may not be limited to:

- public transit stops for rail, bus, community shuttle, and HandyDart with supporting amenities such as shelters and real-time arrival information;
- bike- and car-share facilities;
- taxi and ride-hailing services;
- secure bike storage and repair services;
- kiss-and-ride;
- shops and services;
- Wi-Fi, weather protection, seating, wayfinding signage, public washrooms, and special features such as public art;
- personal safety and security features (e.g., Crime Prevention through Environmental Design measures).

**Development Approach:** successful mobility hubs are integrated with the urban fabric and responsive to surrounding land uses and evolving community needs. This requires a strategic development approach that:

- encourages site-specific mobility programming tailored to the distinct needs of users in “regional”, “city”, and “neighbourhood” locations;
- offers flexible, cost-effective design options that support the establishment of hubs in high-demand, high density locations;
- can readily adapt to growth and increasing demands for new and emerging technologies and travel options (e.g., bike-share, ride hailing, and autonomous vehicles);
- employs placemaking strategies supportive of attractive, safe, and pedestrian-friendly facilities that complement the quality and character of the public realm.

## Multi-Modal Mobility Hub Map



### Alternative Mobility Hub Strategies

Multi-modal mobility hubs are places of connectivity that help to bridge the gap between high-frequency transit and each person’s origin and destination by co-locating an integrated suite of mobility services, amenities, and technologies with a concentration of employment, housing, shopping, and/or recreation uses.

Mobility hub designs may vary based on user needs and location-specific opportunities. Development strategies may include, but may not be limited to:

- the street-level clustering of transportation amenities and complementary urban uses on one or several adjacent sites;
- a stand-alone hub that concentrates transportation amenities in a purpose-built facility in proximity to complementary urban uses; or
- a hybrid approach that integrates transportation amenities with complementary urban uses in the form of a high-amenity, transit-oriented, mixed use development.



*Clustered, street-level transportation amenities.*  
Source: Hamburger Hochbahn AG



*CF Richmond Centre – Proposed mobility hub integrated with underground parking and convenient public access to grade.*



*Marine Gateway and the Marine Drive Canada Line Station, Vancouver. Photo: Andrew Latreille / Courtesy: Perkins+Will*

## OBJECTIVE:

Provide a framework for the City Centre as a **“thriving and creative community”** that is empowered, engaged and diverse, and where arts, culture, and heritage are inextricably linked with and support:

- **a strong community voice and engaged community** that enhances the relevance and responsiveness of urban and economic development, planning, and governance;
- **placemaking**, with a mosaic of appealing, lively, and distinctive urban villages, vibrant public spaces, festivals, events, and activities;
- **an increased creative capacity** which enriches the quality of life and attracts progressive business opportunities which support:
  - the arts, heritage and cultural practitioners;
  - the identification, conservation, and interpretation of heritage resources;
  - spaces for residents and visitors to work and participate in arts, culture and heritage activities;
- **an enhanced enjoyment** of the urban realm and respect for and connectivity among citizens and cultures.

## Arts & Culture Map (2031)



### 2.4.1(b) Places to Gather & Celebrate

Public open space and streetscape will play a key role in supporting interaction within the City Centre linking people, buildings & activities. Public spaces are important “mixing places” for community residents, artists & visitors and serve as “stages” for showcasing the work of local artists.

Celebrations form an important part of vibrant urban living & provide opportunities for residents & visitors to come together bringing understanding and a sense of belonging. Many celebrations are intentionally small and community focused. In other cases however, the intent is to invite the City, the region and the world, which requires special accommodation and co-location with City facilities and private developments.

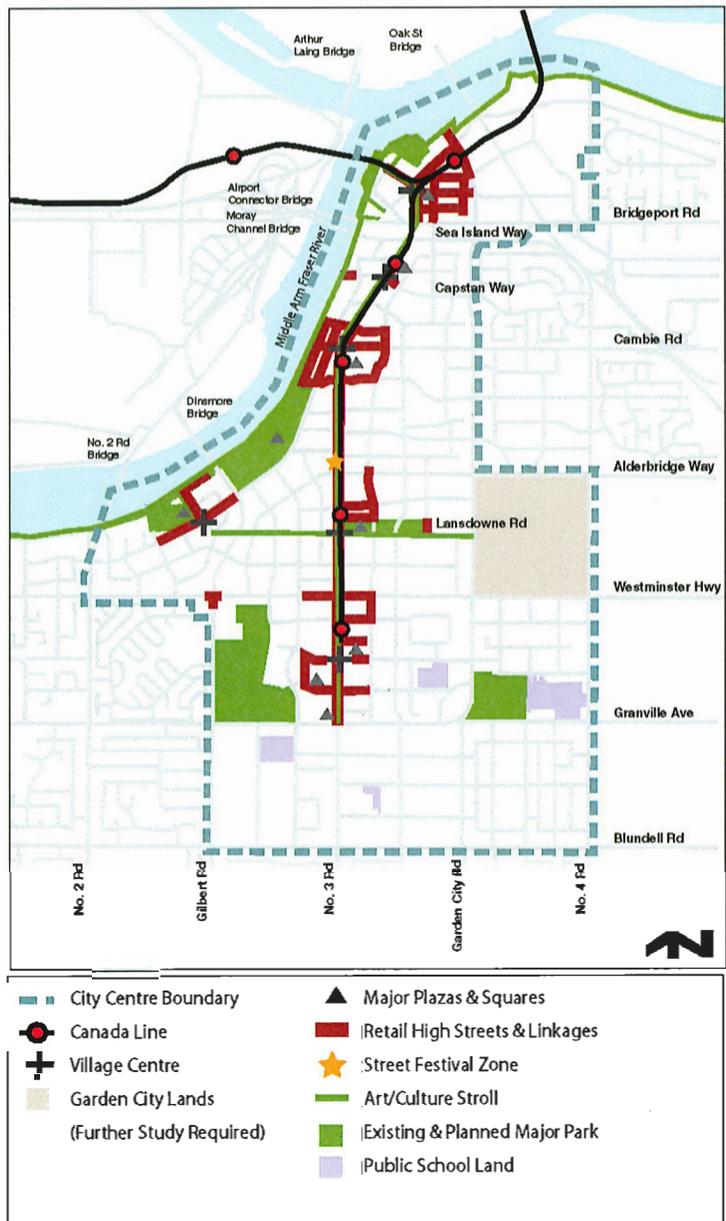
#### Challenges/Opportunities

With the Canada Line, the Oval Plaza & the Middle Arm Park in the development phase, the infrastructure to provide facilities to host events can be built into the design of the spaces instead of having to adapt spaces and bring in infrastructure for each event.

#### Proposed Strategy

- Prepare a festival/events plan including appropriately designed spaces and parade routes.
- Design spaces that ensure staging, view corridors, seating areas, power supply & lights that can flexibly accommodate events of different sizes & styles of community gatherings and festivals.
- Ensure the provision of public and private open spaces that are designed as people gathering and mixing spaces including elements such as conversation areas, public art, busker and performance space and informal play areas.

### Public Spaces & Places Map (2031)



## OBJECTIVE:

Provide a framework for a **complete parks and open space system** that will:

- provide the quantity of park and open space required to address social, recreational, and cultural needs;
- incorporate a rich diversity of experiences and landscapes that reflect the identity of the community and are rooted in local culture and environment;
- ensure an equitable distribution of parks and open space of each type;
- mitigate the environmental impacts of increasing urbanization and continually support the health of the urban environment;
- respond to the higher densities in the City Centre with a greater diversity of programming in each park and appropriate design and materials.

### *Strategic Investment for City Acquisition of Open Space*

*In order to optimize public resources, the strategic approach to the acquisition of City owned parks and open space is to secure investments rapidly. In the period ending in 2031, when the greatest growth and the greatest increase in land values is anticipated, 75% of the total land required to build-out will have been acquired.*

## Base Level Parks & Open Space Map (2031)



\* The Base Level Open Space Standard will be augmented in Capstan Village by publicly accessible areas secured for public park and related uses in respect to the Capstan Station Bonus.

	Year 2006	Year 2031	Build-out
Population	40,000	90,000	120,000
Quantity of Open Space	76.5 ha (189 ac.)	118.4 ha (292.5 ac.)	157.8 ha (390 ac.)
Ratio of Acreage to Population	4.75/1,000	3.25/1,000	3.25/1,000
Quantity of Additional Open Space	0	41.9 ha (103.5 ac.)	39.5 ha (97.5 ac.)

## 2.6.1 Neighbourhood Parks

Neighbourhood parks comprise 40% of the open space system and primarily serve the local needs of the immediate residential or commercial neighbourhood. Parks will determine the types which include:

### Residential Village Parks

**Location:** To serve residents within a 400 m (1,312 ft.) radius without crossing arterial roads or major streets.

**Program:** Social gatherings, informal recreation, environmental features &/or local storm water management features.

**Site Features:** 0.6 to 3.2 ha (1.5 ac. to 8 ac.), 40% urban forest &/or eco-amenity, 50% frontage on streets, south exposure with access to sunlight, outdoor fitness amenities, sport courts, playgrounds, community gardens, seating/gathering area.

### Commercial Village Parks

**Location:** To serve businesses within a 400 m (1,312 ft.) radius without crossing arterial roads or major streets.

**Program:** Daytime & evening gathering, social & cultural programming, informal recreation, urban character.

**Site Features:** 0.2 to 1.6 ha (0.5 ac. to 4 ac.), 30% urban forest, 50% frontage on streets, south exposure with access to sunlight, hard surface and seating areas, sport courts, soft landscape areas.

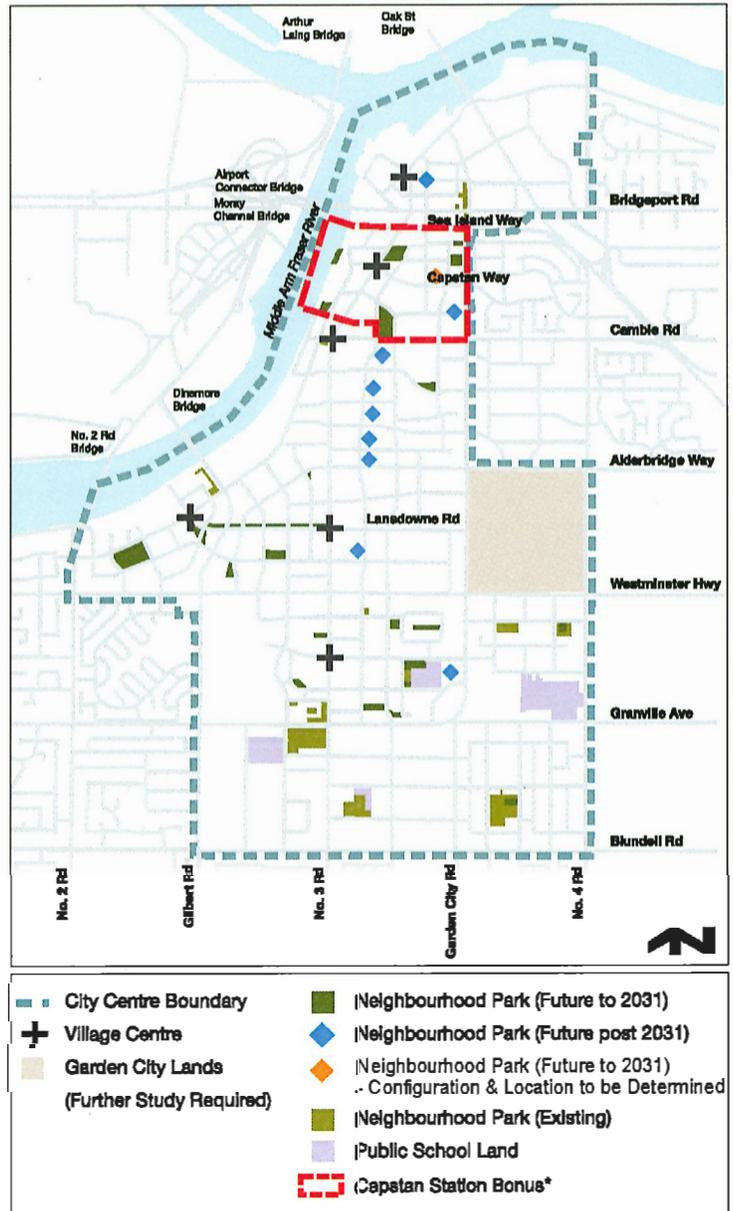
### Urban Plazas

**Location:** At prominent cross-roads within a village.

**Program:** Daytime & evening gathering, social & cultural programming, urban character.

**Site Features:** Less than 0.2 ha (0.5 ac.), 50% frontage on streets, south exposure with access to sunlight, hard surface and seating areas, soft landscape features.

## Neighbourhood Parks Map



\* The Base Level Open Space Standard will be augmented in Capstan Village by publicly accessible areas secured for public park and related uses in respect to the Capstan Station Bonus.

### Additional Study

*Urban Agriculture Strategy – to better understand effective ways of integrating urban agriculture within public open spaces and on private property.*

### 2.6.3(c) Pedestrian Linkages

The 2010 Richmond Trail Strategy provides the vision to guide continued development of the greenway system in City Centre. The intent is to “provide a variety of exciting opportunities for walking, rolling and cycling that will link people to each other, to their community, and to Richmond’s unique natural and cultural heritage”.

#### Greenways

**Location:** Along major streets and important recreational corridors.

**Program:** Link multiple destinations (e.g. between major open spaces and other significant destinations) and connect natural areas.

**Site Features:** Min. 10 m (33 ft.) wide, separate pedestrian and cycling paths, rest areas with street furnishings, public art, signage & wayfinding, integrated with wetlands & storm water features, hedgerows, significant tree planting.

#### Linear Parks

**Location:** Along key streets to create significant recreational and environmental corridors linking the waterfront to the heart of the downtown.

**Program:** Combined neighbourhood park and greenway functions to encourage movement through the neighbourhood (walking, jogging) and incorporating social and physical activity nodes.

**Site Features:** 30 to 40 m (100 to 131 ft.) wide, high quality landscape, broad pedestrian promenade, playgrounds, sports courts, water features, significant tree planting and multi-layered planting, site furnishings, public art.

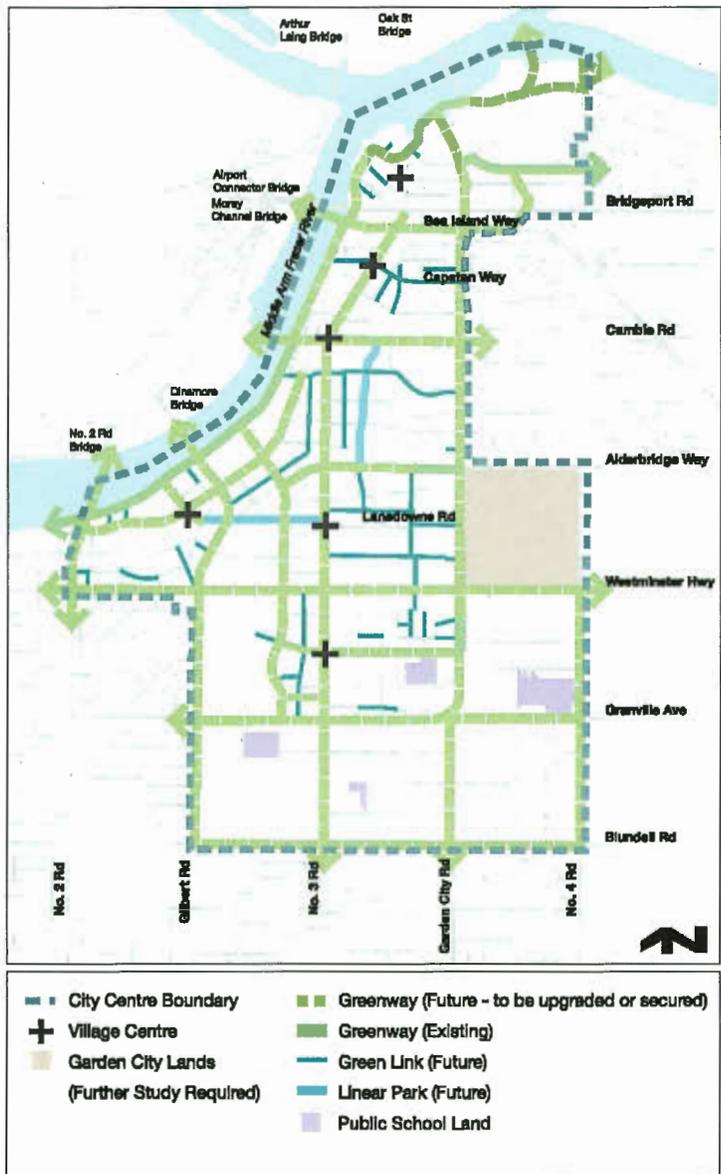
#### Green Links

**Location:** Along lanes and mews, through or between developments (which may include indoor routes).

**Program:** Provide connections within neighbourhoods to support a walkable urban environment, and to support ecological areas.

**Site Features:** Min. 6 m (20 ft.) to 20 m (65 ft.) wide, broad sidewalks with special paving at nodes and intersections, rest areas with street furniture, street trees and multi-layered planting, pedestrian scale street lighting, wayfinding, community art.

### Pedestrian Linkages Map



#### Additional Study

*Storm Water Management Strategy – to develop methods to better address stormwater and permeability in parks, greenways and streets.*

**OBJECTIVE:**

Provide a framework for a “**lively community**” that is rooted in a “culture of walking and cycling” and a collaborative, interdisciplinary approach to city building that is:

- diverse;
- engaging;
- attractive;
- safe;
- healthy;
- human-scaled.

*“... A good city can be compared to a good party-people stay for much longer than really necessary because they are enjoying themselves.”*

Public Spaces and Public Life, City of Adelaide: 2002. City of Adelaide, Gehl Architects ApS, 2002.

**Public Realm Areas Map**



**“Taming Tall Buildings”: Part 2  
Tower Spacing, Floorplate Size &  
Development Site Size**

Richmond’s OCP encourages a maximum tower floorplate size of 600 m<sup>2</sup> (6,459 ft<sup>2</sup>) and a minimum distance between towers of 24 m (79 ft.).

While these guidelines have been effective in encouraging a staggered distribution of point tower forms, new challenges are emerging, including a need for:

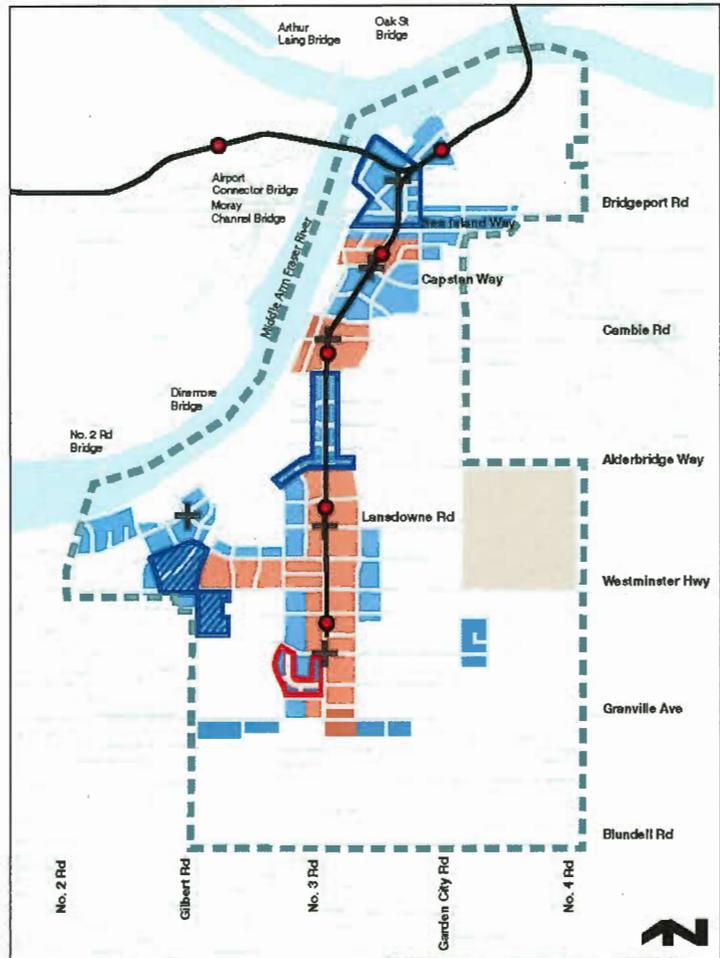
- larger floorplates that better reflect actual City Centre residential development practices (i.e., typically 650 m<sup>2</sup> (6,997 ft<sup>2</sup>)) and anticipated non-residential market needs;
- larger gaps between towers in some areas to reduce private view blockage, sunlight blockage, and the impression of a “wall” of buildings.

In addition, a minimum development site size for tower development is encouraged. This is intended to make clear that while a development site may be designated for building heights greater than 25 m (82 ft.) (i.e., towers), this form is discouraged where it may impact adjacent sites or affects the livability or attractiveness of the public realm.

**Minimum tower development site size (i.e., for buildings taller than 25 m (82 ft.):**

- Width: 45 m (148 ft.);
- Depth: 40 m (131 ft.);
- Area:
  - a) For less than 3 FAR: 4,000 m<sup>2</sup> (1 ac.);
  - b) For 3 FAR or more: 2,500 m<sup>2</sup> (0.6 ac.).

**Tower Spacing & Floorplate Size Map**



City Centre Boundary	Village Centre
Canada Line Station	Garden City Lands (Further Study Required)

**Tower Spacing: Typical Minimum\***

Above 25 m (82 ft.)		Above 30.5 m (100 ft.)	
	24 m (79 ft.)		24 m (79 ft.)
	35 m (115 ft.)		35 m (115 ft.)

\* Between towers on a single development site or adjacent development sites. Towers setbacks to interior property lines or to the centre line of abutting dedicated City lanes should be a minimum of 50% of the Typical Minimum Spacing, except where it can be demonstrated to the satisfaction of the City that a reduced setback will not impact the livability of a neighbouring site or its ability to develop.

NOTE: If tower development occurs outside the areas indicated here, the minimum spacing shall be 35 m (115 ft.).

**Tower Floorplate Size: Typical Maximum**

	For office: 1,800 m <sup>2</sup> (19,376 ft <sup>2</sup> ) above 25 m (82 ft.) For other uses: 650 m <sup>2</sup> (6,997 ft <sup>2</sup> ) above 25 m (82 ft.)
	For hospital: 1,800 m <sup>2</sup> (19,376 ft <sup>2</sup> ) above 25 m (82 ft.) For other uses: 650 m <sup>2</sup> (6,997 ft <sup>2</sup> ) above 25 m (82 ft.)
	650 m <sup>2</sup> (6,997 ft <sup>2</sup> ) above 25 m (82 ft.), EXCEPT may be increased to 1,200 m <sup>2</sup> (13,000 ft <sup>2</sup> ) above 30.5 m (100 ft.) where the Typical Minimum Tower Spacing is provided
Elsewhere	650 m <sup>2</sup> (6,997 ft <sup>2</sup> ) above 25 m (82 ft.)

### 2.10.2(a) Attractive, Accessible Street Frontages

The frontage of a development site is the area between the building and the curb of the fronting public street (or the boundary of a park). How this area is designed is critical to the pedestrian experience and the liveliness of the public realm – but in the City Centre, the design of this space is complicated by Richmond’s flood management policy that generally requires a minimum habitable floor elevation of 2.9 m (9.5 ft.) geodetic – which in many places is as much as 1.5 m (4.9 ft.) above the grade of the fronting street.

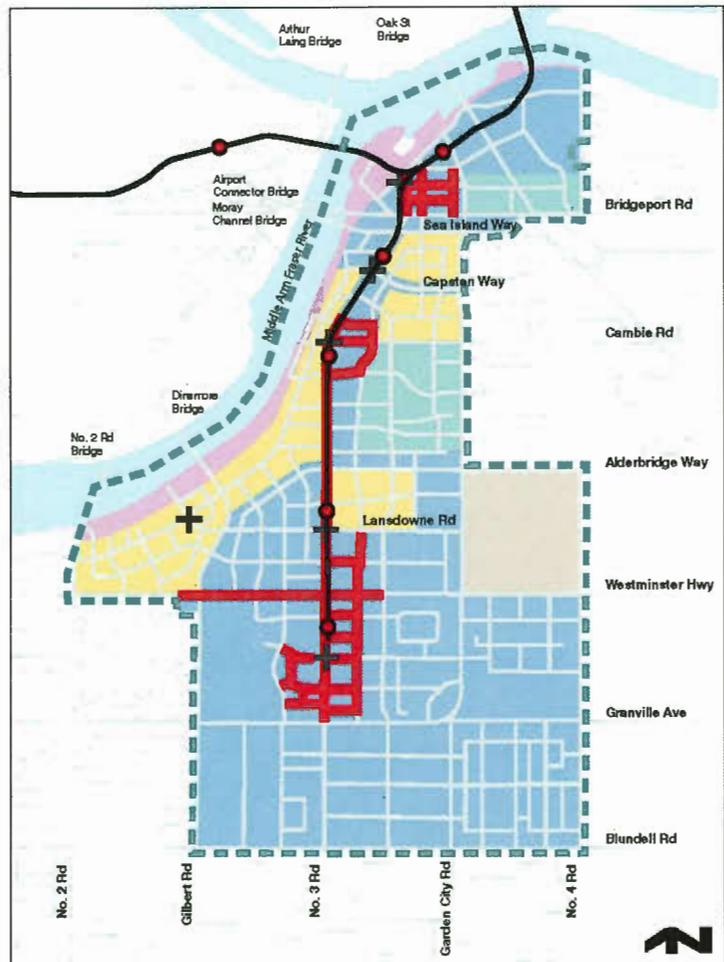
#### Challenge/Opportunity

The grade differential between the street and the minimum habitable floor elevation can enhance privacy for street-fronting dwellings; however, it can also impede pedestrian access, impair retail viability, and present other urban design challenges (e.g., concealing parking).

#### Proposed Strategy

- Raise riverfront areas to the level of the dyke or higher.
- Raise grades to 2.6 m (8.5 ft.) geodetic or higher wherever possible (e.g., transit plazas, new streets and parks, large sites).
- Relax minimum habitable floor elevations for select retail and industrial areas to 0.3 m (1.0 ft.) above the crown of the fronting street.
- Elsewhere, employ a variety of alternative frontage treatments, alone or in combination.

### Preferred Frontage Conditions Map

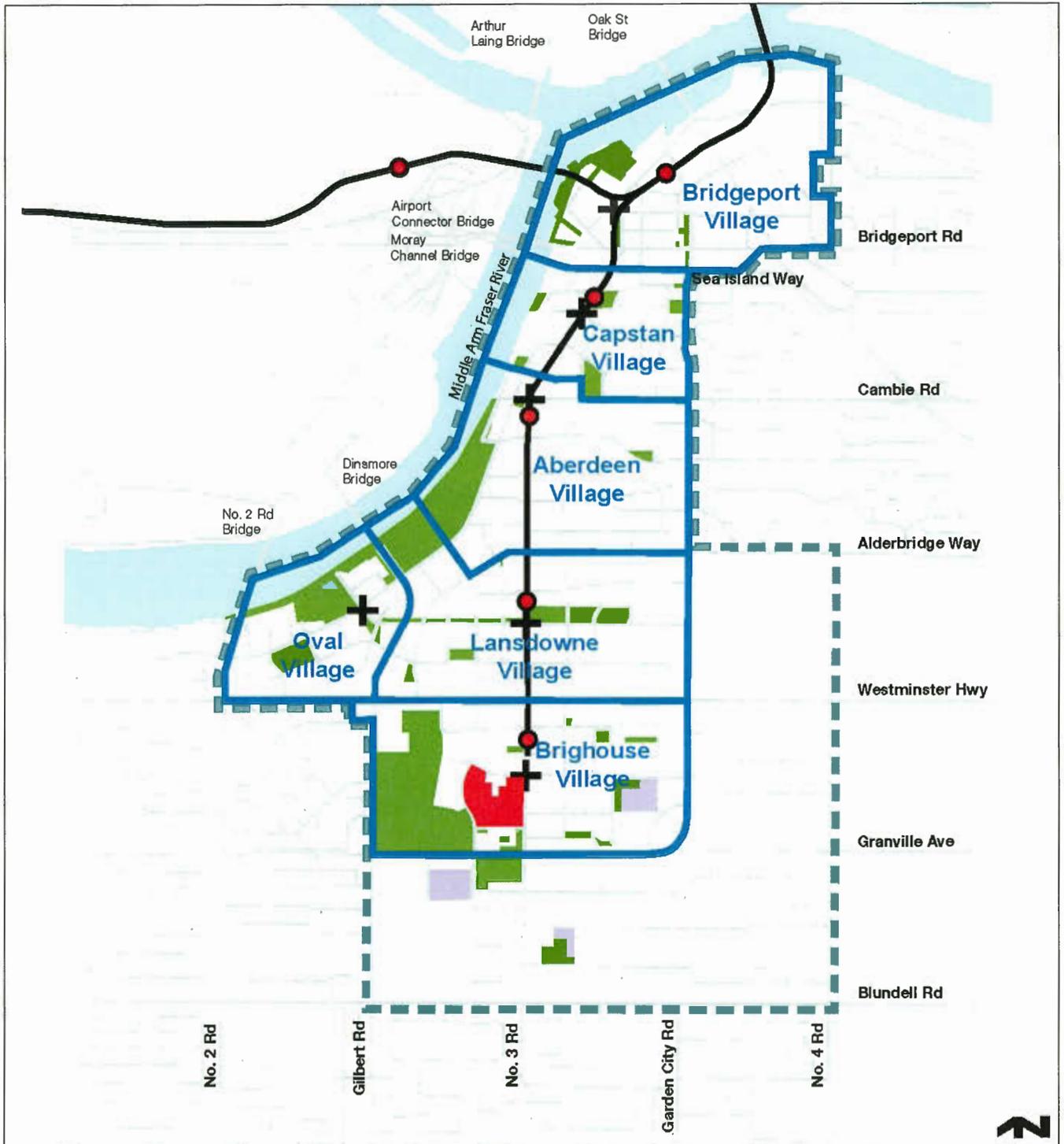


	City Centre Boundary		Riverfront
	Canada Line		Major Redevelopment Areas
	Village Centre		Key Retail Exempt Areas
	Garden City Lands (Further Study Required)		Industrial Exempt Areas
			General

#### Typical Area Descriptions & Minimum Recommended Elevations (Geodetic)

	<b>Riverfront</b> <ul style="list-style-type: none"> <li>• Parks &amp; Streets: 4 m (13.1 ft.) (i.e., dyke crest).</li> <li>• Habitable Floor Elevation: 4 m (13.1 ft.).</li> </ul>
	<b>Major Redevelopment Areas</b> <ul style="list-style-type: none"> <li>• Parks &amp; Streets: 2.6 m (8.5 ft.).</li> <li>• Habitable Floor Elevation: 2.9 m (9.5 ft.) minimum.</li> </ul>
	<b>Key Retail Exempt Areas</b> <ul style="list-style-type: none"> <li>• Parks &amp; Streets: Existing grade maintained.</li> <li>• Street-Fronting Commercial Habitable Floor Elevation: 0.3 m (1.0 ft.) above the crown of the fronting street.</li> <li>• Residential Habitable Floor Elevation: 2.9 m (9.5 ft.).</li> </ul>
	<b>Industrial Exempt Areas</b> <ul style="list-style-type: none"> <li>• Parks &amp; Streets: Existing grade maintained.</li> <li>• Industrial Habitable Floor Elevation: 0.3 m (1.0 ft.) above the crown of the fronting street.</li> <li>• Non-Industrial Habitable Floor Elevation: 2.9 m (9.5 ft.).</li> </ul>
	<b>General</b> <ul style="list-style-type: none"> <li>• Parks &amp; Streets: Existing grade maintained, but may be raised where this is feasible and it enhances livability, form of development, etc.</li> <li>• Habitable Floor Elevation: 2.9 m (9.5 ft.).</li> </ul>

## Development Permit Special Precinct Key Map

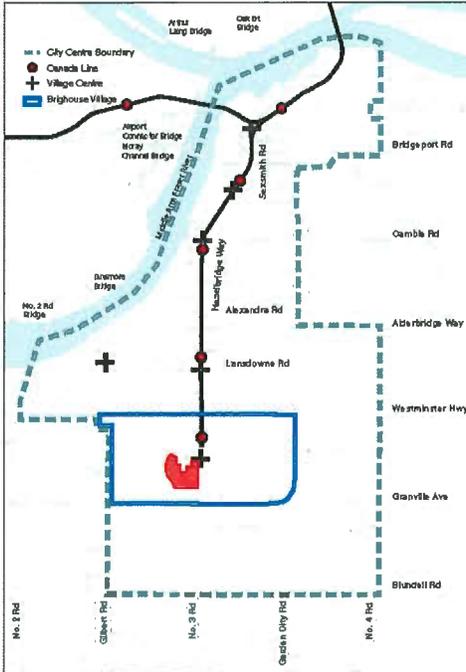


- - - City Centre Boundary
- Canada Line
- + Village Centre
- Parks
- School
- 1.0 Richmond Centre South (Brighthouse Village)

### 3.3 Special Precinct Guidelines

#### 3.3.1 Special Precinct 1.0

##### Richmond Centre South (Brighthouse Village)



This special precinct is envisioned as an animated, high density, high-rise, mixed use, urban place comprised of four distinct character areas:

- Two commercial “High Streets” offering a lively mix of pedestrian-friendly, public/private, outdoor/indoor, places to shop and play; and
- Two “Green Streets” bridging between the busy downtown and adjacent residential, civic, and park uses.

#### Predominant Land Uses:

- Mixed Residential/Commercial, including stand-alone affordable housing buildings

#### Key Land Use Restrictions:

- Pedestrian-Oriented Retail  
Precinct designation requires small commercial units along designated frontages

#### Maximum Net Density:

- 3.15 FAR, including affordable housing

#### Maximum Typical Height:

- 45 m (148 ft.)

### High Streets



A. No. 3 Road



B. Park Road

### Green Streets



C. Minoru Boulevard

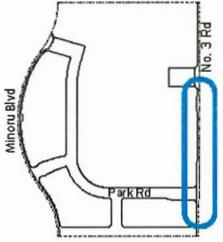
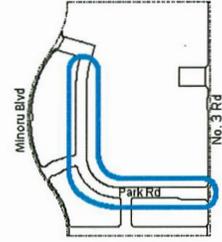
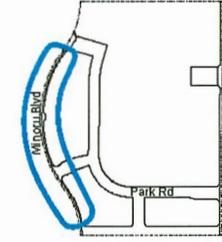
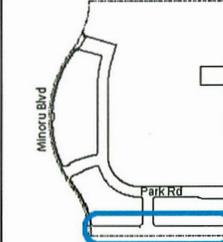


D. Civic Promenade



Possible massing at 3.15 FAR (including affordable housing) showing the precinct’s distinctive high-rise courtyards, accommodating large, family-friendly rooftop outdoor spaces, framed by slim towers that fan out from an animated pedestrian-oriented retail high street and central public plaza.

**“Schedule Q attached to and forming part of Bylaw 9892”**

Special Precinct 1.0	High Streets		Green Streets	
	A. No. 3 Road	B. Park Road	C. Minoru Boulevard	D. Civic Promenade
<b>Richmond Centre South (Brighthouse Village)</b>				
A. Typical Distribution of Uses	<ul style="list-style-type: none"> <li>Underground: Parking.</li> <li>Ground: Pedestrian-oriented retail &amp; limited lobbies.</li> <li>Above: Residential.</li> </ul>		<ul style="list-style-type: none"> <li>Underground: Parking.</li> <li>Ground: Townhouses &amp; limited lobbies.</li> <li>Above: Residential.</li> </ul>	<ul style="list-style-type: none"> <li>Underground: Parking.</li> <li>Ground: Lobbies &amp; limited pedestrian-oriented retail.</li> <li>Above: Residential.</li> </ul>
B. Maximum City Block Size	As defined by the Plan's proposed public street and pedestrian linkages networks, together with lanes and mews as required to achieve a roughly 100 m (328 ft.) circulation grid.			
C. Minimum Net Development Site	2.8 ha (7.0 ac).			
D. Net Development Site Coverage	90% max. exclusive of streets & open spaces secured for public access with Statutory Right-of-Ways (SRW).			
E. Maximum Building Height	45 m (148 ft.).			
F. Towers:	<ul style="list-style-type: none"> <li>35 m (115 ft.) min, above 30.5 m (100 ft.).</li> </ul>	<ul style="list-style-type: none"> <li>24 m (79 ft.) min, above 30.5 m (100 ft.).</li> </ul>	<ul style="list-style-type: none"> <li>35 m (115 ft.) min, above 30.5 m (100 ft.), EXCEPT that spacing may be reduced to 24 m (79 ft.) to reinforce Park Road gateway locations.</li> </ul>	
• Tower Width	20.0 m (65.6 ft.) max. (across the tower's narrow dimension) above 30.5 m (100 ft.).			
• Tower Floorplate	650 m <sup>2</sup> (6,997 ft <sup>2</sup> ) above 25 m (82 ft.), EXCEPT may be increased to 1,200 m <sup>2</sup> (13,000 ft <sup>2</sup> ) above 30.5 m (100 ft.) where (i) the recommended Tower Spacing is provided, (ii) larger floorplates do not impact key public spaces, and (iii) larger floorplates contribute towards larger, more family-friendly, rooftop (outdoor) amenity spaces.			
G. Habitable Floor Elevation	<ul style="list-style-type: none"> <li>Retail &amp; Lobbies: 0.3 m (1.0 ft.) above the crown of the fronting street.</li> </ul>	<ul style="list-style-type: none"> <li>Retail &amp; Lobbies: 0.3 m (1.0 ft.) above the crown of the fronting street.</li> </ul>	<ul style="list-style-type: none"> <li>Lobbies: 0.3 m (1.0 ft.) above the crown of the fronting street.</li> </ul>	<ul style="list-style-type: none"> <li>Retail &amp; Lobbies: 0.3 m (1.0 ft.) above the crown of the fronting street.</li> <li>Other residential: 2.9 m (9.5 ft.) GSC.</li> </ul>
H. Minimum Setbacks	<ul style="list-style-type: none"> <li>Underground: Nil</li> <li>Elsewhere: 6.0 m (19.7 ft.) to lot line &amp; greater at lobbies/primary entrances</li> <li>Flex Zone: Setbacks may be reduced by:                             <ol style="list-style-type: none"> <li>1/3 for 50% of the ground floor frontage if compensated for with public plaza of equivalent size;</li> <li>1/2 for 50% of the upper floor frontage.</li> </ol> </li> </ul>	<ul style="list-style-type: none"> <li>Underground: Nil.</li> <li>Plaza: 1.5 m (4.9 ft.) to SRW boundary.</li> <li>Street: 7.5 m (24.6 ft.) east of plaza &amp; 5.5 m (18.0 ft.) elsewhere measured to curb face (i.e. setback includes sidewalk &amp; parking).</li> <li>Flex Zone (east of plaza only): Setbacks may be reduced by 2.0 m (6.6 ft.) for 20% of ground floor &amp; 50% of upper floor frontages.</li> </ul>	<ul style="list-style-type: none"> <li>Underground: Nil.</li> <li>Elsewhere: 4.5 m (14.8 ft.) to the lot line or 2.0 m (6.6 ft.) to the back of the sidewalk (whichever is greater), EXCEPT greater at lobbies &amp; primary entrances.</li> </ul>	<ul style="list-style-type: none"> <li>Underground: Nil.</li> <li>Tower: 2.0 m (6.6 ft.) to lot line, EXCEPT setback may be reduced to 0.5 m (1.6 ft.) above the ground floor.</li> <li>Pedestrian-Oriented Retail: 0.5 m (1.6 ft.) to lot line.</li> <li>Elsewhere: 6.0 m (19.7 ft.) to lot line.</li> <li>Note: Sidewalk SRW encroaches into lot by 0.5 m (1.6 ft.).</li> </ul>
I. Build-to-Lines	<ul style="list-style-type: none"> <li>Setbacks should typically be treated as build-to-lines.</li> <li>High Street "Flex Zones" are intended to contribute towards more varied &amp; animated public spaces &amp; built forms.</li> </ul>			
J. Preferred Frontage Treatments	<ul style="list-style-type: none"> <li>"Shopfront &amp; Awning".</li> </ul>	<ul style="list-style-type: none"> <li>"Shopfront &amp; Awning".</li> </ul>	<ul style="list-style-type: none"> <li>"Stoops &amp; Porches".</li> <li>"Lawn &amp; Garden".</li> </ul>	<ul style="list-style-type: none"> <li>"Lawn &amp; Garden".</li> </ul>
K. Landscape Considerations	<ul style="list-style-type: none"> <li>A broad sidewalk framed by large trees, public seating, and amenities enhance No. 3 Road as premier retail avenue &amp; key civic space.</li> </ul>	<ul style="list-style-type: none"> <li>Narrow sidewalks, a public plaza &amp; special boulevard &amp; Flex Zone features confer an intimate scale &amp; vibrant retail character.</li> </ul>	<ul style="list-style-type: none"> <li>A park-like street is enhanced &amp; expanded with terraced planting &amp; landscape features that convey a garden-like character to the built form.</li> </ul>	<ul style="list-style-type: none"> <li>A narrow, pedestrian-oriented City street lined with lushly-planted courtyard gardens.</li> </ul>
L. Built Form Considerations	<ul style="list-style-type: none"> <li>Continuous retail enhanced by a bold, articulated &amp; diverse architectural expression creates a signature image for No. 3 Road.</li> </ul>	<ul style="list-style-type: none"> <li>Intimate scale &amp; articulated mixed-use buildings contribute towards a distinct local retail experience.</li> </ul>	<ul style="list-style-type: none"> <li>Dynamic, terraced streetwall buildings punctuated by widely spaced, slim towers &amp; gateway features.</li> </ul>	<ul style="list-style-type: none"> <li>Recessed streetwall buildings punctuated by widely spaced, slim towers set close to the sidewalk.</li> </ul>
	<ul style="list-style-type: none"> <li>Form &amp; character of stand-alone buildings must be to the same level of quality as other uses.</li> </ul>			

### 3.3.1 Special Precinct 1.0

#### Richmond Centre South (Brighthouse Village)

##### Development Features:

1. **Streetwalls:** Layered streetwalls, ranging in height from 5.0 m (16.4 ft.) to 30.5 m (100 ft.), break down the building mass vertically and horizontally and, together with variations in articulation, colour, materials, and fenestration, impart a vibrancy and fine grain to the streetscape.
2. **Flex Zone:** Varied setbacks and upper floor projections add to the street’s visual interest and define large/small outdoor spaces and plazas for public/private uses (e.g., dining), socializing, play, public art, seating, and public amenities.
3. **Towers:** Slim, regular tower slabs set perpendicular to the street imparts an order to the streetscape.
4. **Gateway Features:** An articulated tower at the Cook Road corner visually reinforces this location as the Village centre, while reduced building height at the site’s south end opens up views to Richmond City Hall and its significant trees.
5. **Retail Ground:**
  - Permeability and legibility are enhanced with a hierarchy of building entrances and transparent storefront glazing to active retail interiors.
  - A continuous animated retail frontage lines the high street, including frequent individual shop entrances, varied styles of fixed/operable display windows, and multi-tenant retail entrances enhanced with forecourts and pedestrian amenities.
  - Residential lobbies must be limited and must enhance the retail street with landscaped features and amenities.
6. **Landscape:** A high quality, elegant hardscape is enhanced with trees, planters, street furniture, public art, and special features.

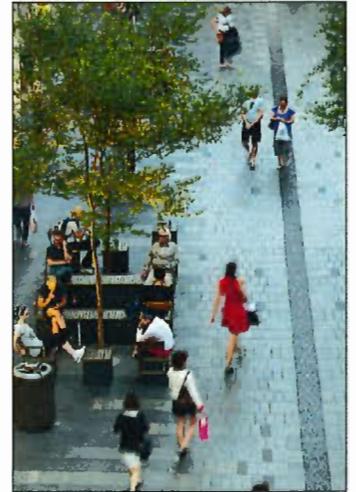
## High Streets

### A. No. 3 Road

*Richmond Centre South (Brighthouse Village) is intended to contribute towards No. 3 Road’s development as a “great street” and Richmond’s preeminent retail avenue through the combination of a bold streetwall, varied architectural expression, landmark features, and continuous pedestrian-oriented shops, amenities, public art, and landscaping.*



*Regular Pattern of Slim Towers*



*Active Retail Flex Zone & High-Amenity Landscape*



*Varied Heights & Setbacks*

### 3.3.1 Special Precinct 1.0

#### Richmond Centre South (Brighthouse Village)

##### Development Features:

1. **Streetwalls:** A stepped form unifies Park Road and ties together 3 sub-areas:
  - *South Leg:* A 2-storey façade lines both sides of the street, stepping up to mid-rise and tower forms behind.
  - *Plaza:* The south leg’s streetwall is extended in a bold arc that defines the plaza’s south side, reinforced by horizontal balconies and articulations.
  - *North Leg:* As the street narrows the streetwall rises, creating a sense of enclosure that is reinforced by the building’s lively vertical expression.
2. **Flex Zone:** Along the South Leg, the public sidewalk is expanded with a public/private zone suitable for café seating and retail displays, while upper floor projections add visual interest.
3. **Towers:** Slim towers fan out around the high street and plaza to enhance daylight and create a distinctive skyline feature.
4. **Retail Ground:** Continuous small-scale shops line the high street. Residential lobbies are limited and provide small forecourts and public amenities (e.g., art, seating) that enhance the high street.
5. **Community “Living Room”:** A flexible, year-round, outdoor space for dining, shopping, socializing, relaxing, entertaining, and playing, comprising:
  - At least 2,023.4 m<sup>2</sup> (0.5 ac) in size;
  - Hard/soft landscaping and special features supporting year-round use;
  - Multi-modal mobility hub integrated with underground parking/services;
  - Animated retail/restaurant edges; and
  - Public art, seating, and amenities.
6. **Back-of-House:** Necessary service uses and above-grade parking along the North Leg are made pedestrian-friendly and visually engaging with high quality materials, artful design features, and special street design features (e.g., catenary lighting, curb-less design).

## High Streets

### B. High Streets: Park Road

*Richmond Centre South (Brighthouse Village) is intended to contribute towards the establishment of a distinct downtown marketplace through a combination of intimate streetscapes, small-scaled shops, residential above, and a vibrant community “living room” in the form of a large central plaza.*



Community “Living Room” Plaza



Dynamic Back-of-House Uses along North Leg



South Leg with Cafe-Friendly Flex Zone

### 3.3.1 Special Precinct 1.0

#### Richmond Centre South (Brighthouse Village)

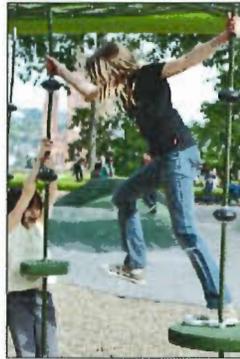
##### Development Features:

1. **Streetwall:** A highly articulated, low-rise streetwall (4 storeys typical) enhances the distinctive arc of Minoru Boulevard. Mid-rise forms are generally set well back from the street. Townhouses with stoops, in combination with tower lobbies, forecourts, and special entry features (e.g., public seating, glass canopies, and water features) provide for an appealing, pedestrian-friendly streetscape.
2. **Towers:** Slim, widely spaced towers punctuate the streetwall and extend to grade to vary the rhythm of the streetwall.
3. **Vertical Garden:**
  - Building articulations, podium and mid-rise rooftops, balconies, and terraces support a varied and visually appealing pattern of trees, planting, and landscape features that impart a “vertical garden” character and can be enjoyed by building occupants and from nearby buildings and the street.
  - A varied palette of trees and plants provides for year-round colour and visual interest, contributes towards the downtown’s urban forest, and offers wildlife and pollinator habitat.
  - Garden designs provide for ease of maintenance (to avoid overburdening building residents and ensure the health and longevity of the landscape) by including features that allow for lower maintenance and easy access and upkeep.
4. **Gateway Features:** Entrances to the precinct’s Park Road high street are marked at Murdoch Avenue and Minoru Gate with broad walkways framed by a double row of trees and special architectural features that take into consideration, among other things, views along Minoru Boulevard.

## Green Streets

### C. Minoru Boulevard

*Richmond Centre South (Brighthouse Village) is intended to enhance Minoru Boulevard as a key route linking the public and neighbours with park and civic amenities by making the landscape the predominant feature of the development’s architectural expression.*



*Family-Friendly Rooftop Courtyards*



*Vertical Gardens*



*Pedestrian-Oriented Urban Townhouses*



*Slim Towers & Gateway Features*

### 3.3.1 Special Precinct 1.0

#### Richmond Centre South (Brighthouse Village)

##### Development Features:

1. **Streetwalls:** An elegant, low-rise streetwall (3 storeys typical) is recessed to provide a backdrop for public gardens. Mid-rise forms are set well back from the street.
2. **Towers:** Slim, “light”, refined towers:
  - Are widely spaced to maximize views and daylight towards the north;
  - Pull close to the sidewalk (in front of the recessed streetwall) to vary the rhythm of the streetscape and frame the garden spaces;
  - Extend to grade on slim columns and/or in the form of glassy lobbies that are designed to visually blur the line between indoors and out; and
  - Are articulated above grade with projecting balconies, cantilevered roofs, and similar features above the sidewalk (secured by SRW) to impart texture, varied expression, and a finer grain.
3. **Public Gardens:** A variety of planting forms, including informal groupings of trees and indigenous plants and shrubs, changes in grade, water, and related landscape features complement the adjacent City Hall landscape, visually expand the public realm, make the gardens and fronting walkways attractive year-round, contribute towards the downtown’s urban forest, and provide for wildlife/pollinator habitat.
4. **Ground Floor Uses:**
  - Lobbies and public gardens preferred.
  - Restaurant/retail uses are encouraged near No. 3 Road, together with outdoor seating/dining within the “public garden” area.
  - Townhouses and amenity space are discouraged. Parking entrances and services uses must be minimized and are discouraged near City Hall.

## Green Streets

### D. Civic Promenades

*Richmond Centre South (Brighthouse Village) is intended to enhance Richmond’s civic precinct, including City Hall, its plaza, significant trees, and linkages with Minoru Park, by contributing towards the street’s development as a park-like, pedestrian promenade.*



*Public Promenade & Amenities*

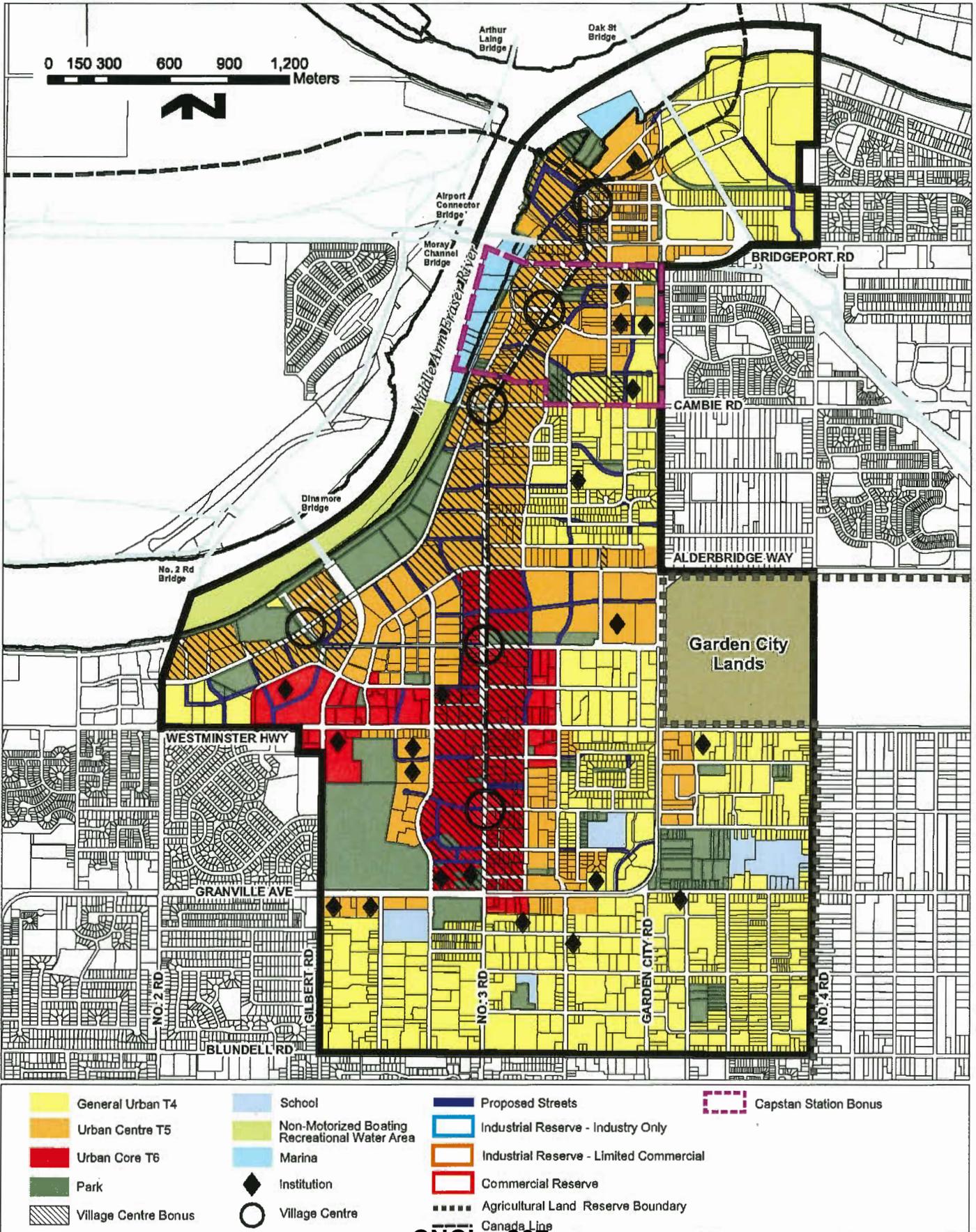


*Slim Towers & Indoor/Outdoor Lobbies*



*Low-Rise Streetwall set back along the Promenade*

# Generalized Land Use Map (2031)



### Specific Land Use Map: Brighthouse Village (2031)

