

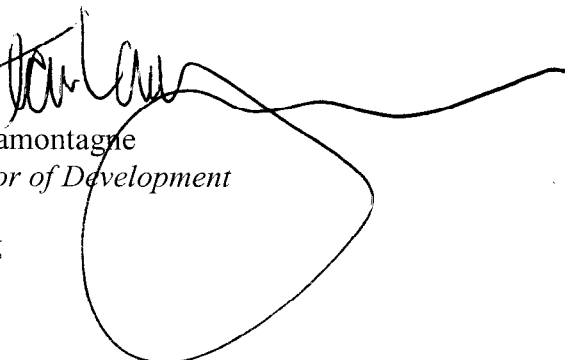


To: Development Permit Panel
From: Jean Lamontagne
Director of Development
Date: May 23, 2007
File: DP 05-317013
Re: **Application by Lawrence Doyle Architect Inc. for a Development Permit at
8200 Corvette Way**

Staff Recommendation

That a Development Permit be issued which would:

1. Permit the construction of a mixed-use commercial residential development with two (2) 16-storey residential high-rise towers totalling 231 units and a 14-storey hotel with 176 rooms on a site zoned Comprehensive Development District (CD/173); and
2. Vary the provisions of Zoning and Development Bylaw No. 5300 to:
 - a) Vary the maximum height for the residential buildings from 45 m permitted to 45.5 m and the maximum height of the hotel building from 38 m to 44.5 m; and
 - b) Vary the side yard setback to Corvette Way from 6 m required to 5.75 m for a portion of Tower B.



Jean Lamontagne
Director of Development

CA:blg
Att.

Staff Report

Origin

Lawrence Doyle Architect Inc. has applied to the City of Richmond for permission to develop a mixed-use commercial residential development with two (2) 16-storey residential high-rise towers totalling 231 units and a 14-storey hotel with 176 rooms at 8200 Corvette Way on a site zoned Comprehensive Development District (CD/173). The site is currently used to provide additional parking for the existing hotel on 3031 No. 3 Road.

The site is being rezoned from “Automobile-Oriented Commercial District (C6)” and “Comprehensive Development District (CD/85)” to “Comprehensive Development District (CD/173)” for this project under Bylaw 8073 (RZ 04-275910).

Development Information

Please refer to attached Development Application Data Sheet (**Attachment 1**) for a comparison of the proposed development data with the relevant bylaw requirements.

Background

Development surrounding the subject site is as follows:

- To the north: Moray Channel Bridge coming onto Sea Island Way and an existing mixed-use building zoned Marina District 2 (MA2) and light industrial and commercial uses/surface parking lot zoned Automobile-Oriented Commercial District (C6) and Land Use Contract 126;
- To the east: Corvette Way and existing hotel zoned Comprehensive Development District (CD/85);
- To the south, new Corvette Way extension, existing light industrial uses zoned Automobile-Oriented Commercial District (C6) and Capstan Way; and
- To the west, CP Rail Right-of-Way (ROW) and existing marina zoned Marine District 1 (MA1) and MA2.

Rezoning and Public Hearing Results

During the rezoning process, staff identified design improvements to be resolved at the Development Permit stage. Design development has been incorporated in the Development Permit submission that has satisfactorily addressed staff concerns.

The Public Hearing for the rezoning of this site was held on July 7, 2006. No written submissions were received in response to the public notice. At the Public Hearing, staff responded to an inquiry regarding driveway access from Capstan Way onto the site by noting that two (2) separate accesses to the development from an extension to Corvette Way which will connect to Sea Island Way. There were no follow up questions.

Staff Comments

The proposed scheme attached to this report has satisfactorily addressed the significant urban design issues and other staff comments identified as part of the review of the subject Development Permit application. In addition, it complies with the intent of the applicable sections of the Official Community Plan (OCP) and is in compliance with Comprehensive Development District (CD/173). No zoning variances have been requested.

Zoning Compliance/Variations (staff comments in bold)

The applicant requests to vary the provisions of the Zoning and Development Bylaw No. 5300 to:

- 1) Vary the maximum height for the residential buildings from 45 m permitted to 47 m (geodetic).

(Staff supports the proposed variances. The mechanical penthouses are well setback from the roof of the residential tower. The proposed height variances is for the mechanical penthouses only and do not pose any additional impact with respect to increased shadowing on the landscaped roof podiums and open space at grade. The buildings are less than 45 m in height without the elevator penthouses.

The rezoning submission included an 11-storey hotel. Consequently, the Comprehensive Development (CD) Bylaw was drafted to accommodate the original proposed scheme. In the interim, the design of the hotel has changed substantially to a high-rise for greater efficiency of the floor plates. The increased height does not increase the permitted density. The hotel is 42 m in height without the elevator penthouse. The proposed massing is appropriate in a City Centre and a gateway location. No negative impact has been created by the increased height.

Transport Canada has been consulted and has no objection to the height variances as they are within 47 m (geodetic). Transport Canada also noted that the developer will have to apply for an exemption for construction cranes from Transport Canada when the time comes.

- 2) Vary the side yard setback to Corvette Way from 6 m to 5.75 m for a portion of Tower B.

(Staff supports the proposed variances. The parking podium has to be pulled back from the western edge to provide a right-of-way (ROW) for an existing Greater Vancouver Sewer and Drainage District (GVS & DD) truck line. The trunk line is in existence, but no right-of-way (ROW) has been secured for it until this development proposal. A portion of the GVS & DD ROW will be developed with a paved path way as part of the Richmond Trail System. The minor setback variance does not create any negative impact on the interface with the street. The setback is adequately landscaped to create a pedestrian friendly environment).

Advisory Design Panel Comments

The Advisory Design Panel supported this project to be moved forward to Development Permit on October 5, 2005. A copy of the relevant excerpt from the Advisory Design Panel Minutes is attached for reference (**Attachment 2**). The Panel did not request any significant design development to be incorporated in the Development Permit submission.

Analysis

Conditions of Adjacency

- The project comprises of two (2) residential towers and a hotel building on top of a shared parking podium and roof top amenities and will be the first redevelopment site at this gateway location.
- The site is flanked by streets on the north, east and south, and a greenway/utility right-of-way, marina and the Fraser River to the west. This is a neighbourhood in transition. It is anticipated that the neighbourhood will be undergoing significant urban transformation as the City Centre Area Plan (CCAP) concept takes shape in the future. Consequently, the proposed development does not create any negative impact on the surrounding existing buildings, which are predominantly one-storey or two-storey industrial buildings except for the Skyline Hotel at 3031 No. 3 Road, owned by the same developer.
- The developer will be building a greenway suitable for bicycle and pedestrian along the west side of the site and a small staging area will be incorporated at the intersection of the greenway and Sea Island Way. The greenway is built on a Greater Vancouver Sewer and Drainage District (GVS & DD) right-of-way (ROW) that accommodates a regional trunk line. The design of the greenway and street beautification along Sea Island Way are being completed in consultation with Parks Department. The details of the design are described in the Servicing Agreement (SA 06-332908).
- The parkade wall to the west will be screened by a continuous row of trees and shrubs to soften the façade and to provide some landscaping along the trail. A metal trellis system will be incorporated onto the small portion of the exposed parkade façade on the north elevation for vines to climb up to create a “living green wall” effect, and a triple row (where practical) of street trees will be planted along Sea Island Way to provide a canopy lined pedestrian environment.

Urban Design and Site Planning

- The access onto the site from Sea Island Way is a right-in/right-out only T-intersection onto Corvette Way from the north or via Capstan Way south of the site. There are two (2) principal vehicular entrances proposed for this development. A drop-off/turn-around area that is integrated with loading and garbage and recycling facilities adjacent to a parkade entrance has been provided along the north-south leg of Corvette Way to primarily serve the hotel. A second parkade entrance has been provided along the east-west leg of Corvette Way (to be constructed) to provide vehicular entrance for the residential towers.
- Residential loading will be accommodated along the parking lane on east-west leg of Corvette Way on a temporary permitting basis for moving trucks. Delivery trucks will share the hotel’s loading facilities.
- Separate residential and commercial garbage and recycling rooms have been located adjacent to the two (2) vehicular entrances for easy access for pick-up operations.
- The residential and hotel parking are separated by security gate for added safety.
- The parking meets the provisions of the Comprehensive Development (CD/173) Bylaw. A total of 306 residential parking spaces have been provided. Of the total, there are 67 tandem (134 spaces) arrangements proposed. The developer is required to register a Restrictive Covenant on title to ensure that tandem parking is only assigned to the same dwelling units that have a minimum of two (2) bedrooms prior to the issuance of the Development Permit.
- In addition, a total of 165 parking spaces have been provided for hotel use. Of this total, 130 spaces are provided for the proposed hotel on this site and an additional 35 spaces are

secured under a Restrictive Covenant on title for the exclusive use of the existing hotel (Skyline Hotel) at 3031 No. 3 Road.

- Eight (8) wheelchair accessible residential units have been incorporated into the residential towers. In addition, some universal accessibility measures such as lever handles and taps, and backing board in the showers and bathtubs have been incorporated to encourage aging in place. The project provides twelve (12) handicap parking stalls which is in excess of the ten (10) handicap spaces required by the Parking Bylaw.
- The hotel has been designed to meet the National Building Code requirements for accessibility by providing five (5) wheelchair-accessible and nine (9) audio-impaired enhanced guest rooms. A total of 12 handicap parking spaces are provided.
- All the building facades have been adequately animated with active use, such as commercial along Sea Island Way and townhouses along Corvette Way or landscaped, such as along the greenway and the staging area, to provide good streetscape interface.
- Sea Island Way is a primary vehicular entrance into the City. It is also designated as a bicycle route and identified for street beautification. A small staging area has been incorporated into the northwest corner of the site at the mouth of the greenway. A “plaza” has been proposed at the north east side at the intersection of Sea Island Way and Corvette Way. Details of the street and boulevard treatment are provided in the Servicing Agreement drawings.
- A restaurant with outdoor patio seating at the northeast corner of the ground floor has been introduced along the Sea Island frontage to animate the street. The hotel lobby and townhouses at the base of the two (2) residential towers brings activities to the street level and create a safe pedestrian environment. Landscape buffer and private patios creates a buffer and provides the necessary separation between the private and public realm at grade.
- The developer has provided ample bicycle storage in various bicycle locker rooms on Levels P1, ground, second and third floors for use by the residential towers and the hotel building. These areas are for bicycle storage only.

Architectural Form and Character

- The design of the hotel has been significantly improved to create an iconic gateway building that takes its inspiration both from YVR and the proximity to the open water. The colour fins along the building façade is reminiscent of the airport traffic signalling used at the airport and the “ship” motif reference the site’s proximity to the water. The hotel includes a restaurant at grade facing Sea Island Way to animate the street.
- A shadowing diagram has been provided to illustrate the shadows cast by the proposed development.
- The towers on the site are located to provide the maximum separation from each other and to minimize shadowing on the greenway that runs along the western edge of the site. The tower separations provided meet the intent of the City Centre Area Plan guidelines. They are between 22.5 m and 23.2 m at the closest points and exceed 24 m in the remaining locations.
- The different heights and architectural treatments of the hotel and the residential buildings provide differentiation between residential and non-residential uses and provide variety of building types on the site.
- The principal materials used are architectural concrete with various treatments and colours and glazing with aluminium frames. The facades are accented with alucabond metal cladding and soffits and metal paneling for the portions of the hotel podium designed to look like the hull of a ship. The stylized bow faces south with the stern terminating along the north façade.

- The portion of exposed north facing exterior concrete wall of the parkade is finished with architectural concrete and clad with metal trellis to support vines. Over time, this façade will be softened with a “living wall”.
- The hotel has a ballroom and meeting rooms for hotel events on Level 4.
- A green roof is provided on the lower roof (Level 5) of the hotel.
- In addition to providing exclusive residential indoor amenities for each of the residential towers, a gymnasium and indoor pool is proposed on Level 4 that will be shared between the hotel guests and the residents in the “ship portion of the building” along the greenway. The gymnasium is fully integrated with the outdoor amenities on the roof podium. The use of the gymnasium and indoor pool by the residents will be secured by a Restrictive Covenant registered on title prior to the issuance of the Development Permit.
- Hotel signage will be designed in accordance with the Richmond Sign Bylaw.

Landscape Design and Open Space Design

- A double/triple row of street trees will be planted along Sea Island Way on both City and private land to provide a lush canopy as the trees mature. The street trees will be augmented by planting along the building face and climbing plants up the trellis on the small portion of parkade wall fronting onto Sea Island Way.
- The street level outdoor areas are well landscaped with generous planting and special treatment to paved surfaces to create a high quality public realm and to set the standards for future developments in the area.
- The roof podium is fully landscaped to provide amenities ranging from children’s play area located in close proximity to the residential amenity room and positioned to encourage natural surveillance from surrounding residential units; common paved patios for shared use to promote social interaction among residents; and universally accessible pathways to encourage strolling and to connect the various indoor amenities spread among the three buildings. Lighting has been incorporated to provide safety for night use.
- Each residential tower has its own indoor amenity rooms in addition to the shared amenities with the hotel previously described.
- A green roof is provided above Level 5 of the hotel building. Both the green roof and the landscaped podium reference the airport runways in the design of the planting and paving patterns and the lighting.
- In total, 173 trees are proposed in addition to the numerous shrubs, ground covers and vines.
- Planting along the greenway is provided immediate adjacent to the proposed west building facade as the greenway is over the GVS & DD truck line, so no large shrubs or trees are permitted. Lighting will be mounted on the building façade to illuminate the greenway.

Amenity Contribution

- The developer has provided voluntary amenity contribution totalling \$863,558 as part of the rezoning condition (RZ 04-275910).

Utilities and Site Servicing

- Utility capacity analysis and Site Servicing Agreement were conditions of the rezoning and have been addressed to the satisfaction of the Director of Engineering.
- The Site Servicing Agreement includes frontage work along all four (4) perimeters of the site for roads and greenways and servicing upgrades.

- In addition, the developer has provided a voluntary contribution of \$983,407.83 for utility services upgrades towards utility capacity upgrades for water, storm and sanitary services based on an estimate provided by the Engineering Department. If the Building Permit is issued after the new Development Cost Charges (DCCs) come into place in July, 2007, then the developer will pay the new DCCs instead of this voluntary contribution. This solution is deemed to be sufficient by the Director of Engineering to move this project forward for rezoning.

Flood Indemnity Covenant

- In accordance with the City's Flood Management Strategy, the applicant is required to register a Flood Indemnity Covenant on title referencing the minimum habitable elevation for the area which is 0.9 m (geodetic) as a condition of the Development Permit.

Aircraft Noise Sensitive Development (ANSD) Policy

- The subject site is designated as "Area 3 – Moderate Aircraft Noise", which allows for all new aircraft noise sensitive land uses to be considered, provided that the developer registers on a Restrictive Covenant are registered on title regarding aircraft noise as a condition of the Development Permit; provides an acoustical report by a certified consultant as part of the Building Permit submission and that the developments incorporate noise mitigation as recommended by the Acoustical consultant in the Building Permit plans.

Crime Prevention Through Environmental Design

- The applicant has adequately address security and safety on-site to the satisfaction of the Advisory Design Panel and meets the intent of the Richmond Official Community Plan (OCP) guidelines.
- The parkade has been designed with secured accesses, glazing in the doors of the elevator lobbies and good lighting to enhance visibility and safety. The mailboxes are located adjacent to the lobby in high traffic areas to promote casual surveillance in addition to the security camera provided.

Conclusions

Staff recommend support for this Development Permit application. The proposed development sets high architectural standards and the applicant has proactively improved the architectural expression and form of the proposed hotel building from the rezoning to create an iconic building to enhance Sea Island Way entrance into the city.



Cecilia Achiam, MCIP, BCSLA
Senior Coordinator, Major Projects & Development Applications
(Local 4122)

CA:blg

Attachment 1: Development Application Sheet

Attachment 2: Design Panel Minutes

The following are to be met prior to forwarding this application to Council for approval:

- Receipt of a Letter-of-Credit for landscaping in the amount of \$210,108 (based on total floor area of 105,054 ft²).
- Prior to the issuance of the Development Permit, the developer is required to register the following on title:
 - a) a Restrictive Covenant to ensure that tandem parking are only assigned to the same dwelling units that has a minimum of two (2) bedrooms.
 - b) a Restrictive Covenant to secure the use of shared amenities with the hotel including the gymnasium and indoor pool on Level 4 by the residents.
 - c) a Flood Indemnity Covenant.

The following are to be addressed at Building Permit submission:

- The applicant to provide an acoustical report by a certified consultant and to incorporate noise mitigation as recommended by the acoustical consultant in the Building Permit submission.



City of Richmond

6911 No. 3 Road
 Richmond, BC V6Y 2C1
 www.richmond.ca
 604-276-4000

**Development Application
 Data Sheet**
 Development Applications Division

DP 05-317013

Attachment 1

Address: 8200 Corvette Way

Applicant: Lawrence Doyle Architect Inc. Owner: Skyline Airport Hotel (1977) Ltd.

Planning Area(s): 2.10 City Centre

Floor Area Gross: 32,305.7 m² Floor Area Net: 30,115.4 m²

	Existing	Proposed
Site Area:	10,975 m ²	9,295 m ² (after road dedication)
Land Uses:	Vacant	Mixed-use (residential and hotel)
OCP Designation:	Mixed Use	Mixed Use
Zoning:	CD/173 (Bylaw 8073 RZ 04-275910)	CD/173
Number of Units:	0	231 dwelling units 176 hotel rooms

	Bylaw Requirement	Proposed	Variance
Floor Area Ratio:	Total 3.2 (Max. 2.15 for residential)	Total 3.20 (comprised of 2.15 for residential and 1.05 for hotel)	Complies
Lot Coverage:	Max. 90%	67.3%	Complies
Setback – Side (Corvette Way-East):	Min. 6 m	6.12 m (hotel) 5.75 m (Tower B)	Variance supported
Setback – Rear Yard (Sea Island Way-north):	Min. 6 m	6 m (hotel)	Complies
Setback – Front Yard (Corvette Way-south):	Min. 6 m	6.51 m (Building A)	Complies
Setback – Side Yard:	Min. 6 m	6.09 m (Building A)	Complies
Height (m):	Max. 45 m (residential)	45.5 m and 45.4 m (to elevator penthouses of Tower A and B)	Variance supported
	Max. 38 m (all other buildings)	44.5 m (to elevator penthouse of hotel)	Variance supported
Lot Size:	N/A	9,295 m ²	N/A
Off-street Parking Spaces – Residential/hotel:	306 (residential) and 130 (proposed hotel) (+ 35 additional parking for the use of the existing hotels at 3031 No. 3 Road)	306 (residential) and 130 (proposed hotel) (+ 35 additional parking for the use of the existing hotels at 3031 No. 3 Road)	Complies

Off-street Parking Spaces – Accessible:	9	12	
Total off-street Spaces:	471	471	
Tandem Parking Spaces*	Permitted for 2 or more bedroom residential units only	67 tandem (i.e. 134 parking spaces)	Complies
Amenity Space – Indoor:	Min. 70 m ²	645 m ²	Complies

* A Restrictive Covenant is required to be registered on title prior to Development Permit to ensure that tandem is only allocated to the same unit with two (2) or more bedrooms.

Excerpt from the Minutes from
The Design Panel Meeting

Wednesday, October 5th, 2005 – 4:00 p.m.
Rm. M.1.003
Richmond City Hall

5. 2-Residential High-Rises with 1 Hotel Tower

Lawrence Doyle, Arch.
8200 Corvette Way
(Formal)

RZ 04-275910

Ms. Cecilia Achiam, reviewed the staff comments provided for the project, (Schedule 3).

Mr. Doyle introduced his team consisting of Peter Cook, and Bruno Wall. Mr. Doyle with the aid of a model and artist's renderings, described the project.

Mr. Peter Cook, Landscape Architect, reviewed the details of the conceptual landscape plan.

General questions put forth by the Panel included:

- What was the relationship between the site and the marina. **The marina sites were owned by someone else.**

The comments of the Panel were as follows:

- There were no accessible/adaptable units in towers. Unit B - 2 bedrooms looked as though it may be adaptable. Hotel needs accessible units. Needs increased accessibility.
- Well executed scheme. Good to see well documented contextual information. Project well resolved. This was a gateway to Sea Island Way – do something to enhance landscape and mark with additional signage. Appreciate strong concept – well handled – development at podium has potential for adjacency. Conceptual nature to shading of building – possibly express more glazing in apartment building.
- Podium levels as well could take on expression of activity centres along edges of Sea Island. On podium – three different areas of landscape – is there a way of connecting through hard landscape edges. Like notion of steps on podium edge.
- Great scheme. Should enhance this area of the City. Great plan for roof – neat features – access awkward from towers. Create stronger connection from elevator to amenity or have outdoor space more visible. There is lots of amenity space – not necessary to provide in towers, if provided in hotel. Focus on large amenity space – safer to use. **Individual amenity areas created a smaller subcommunity.**
- Take into account pedestrian and cyclist interaction on pedestrian walkway and at entrance.

- Very dense scheme – likes townhouses along rail tracks. If land issue resolved, could take advantage of this. Hotel unique in comparison to towers.
- Consider parking areas – any blind hallways – eliminate if possible. Bright walls and well lit. Have windows in access doors from parking lobby and stairwells. Open stairwells for visibility. Consider safety when bikes and vehicles share ingress/egress.
- Well done – usable space. Play area large enough to be meaningful. Water feature focal point – east of towers- as you come out on terrace, each building should have access to it. Well done conceptual plan.
- Integrate signage into the architectural scheme.
- Express more glazing in the residential buildings.

Mr. Doyle advised that he appreciated Panel's comments. He will negotiate accessible units with staff. Will look at elevator lobby and provide glazed walls from amenity area to landscape as much possible.

Panel was very appreciative of the quality of the presentation.

Discussion then ensued that resulted in the following motion:

It was moved and seconded

That the project proceed with the full support of the Panel.

**CARRIED
Unanimous**



No. DP 05-317013

To the Holder: LAWRENCE DOYLE ARCHITECT INC.
Property Address: 8200 CORVETTE WAY
Address: LAWRENCE DOYLE
 C/O 200 – 1450 CREEKSIDE DRIVE
 VANCOUVER, BC V6J 5B3

1. This Development Permit is issued subject to compliance with all of the Bylaws of the City applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Development Permit applies to and only to those lands shown cross-hatched on the attached Schedule "A" and any and all buildings, structures and other development thereon.
3. The "Richmond Zoning and Development Bylaw No. 5300" is hereby varied to:
 - a) increase the maximum height for the residential buildings from 45 m permitted to 45.5 m and the maximum height of the hotel building from 38 m to 44.5 m; and
 - b) reduce the side yard setback to Corvette Way from 6 m required to 5.75 m for a portion of Tower B.
4. Subject to Section 692 of the Local Government Act, R.S.B.C.: buildings and structures; off-street parking and loading facilities; roads and parking areas; and landscaping and screening shall be constructed generally in accordance with Plans attached hereto.
5. Sanitary sewers, water, drainage, highways, street lighting, underground wiring, and sidewalks, shall be provided as required.
6. As a condition of the issuance of this Permit, the City is holding the security in the amount of \$210,108 to ensure that the landscaping is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Holder if the security is returned. The condition of the posting of the security is that should the Holder fail to carry out the landscaping hereby authorized, according to the terms and conditions of this Permit within the time provided, the City may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Holder. Should the Holder carry out the landscaping permitted by this permit within the time set out herein, the security shall be returned to the Holder. The City may retain the security for up to one year after inspection of the completed landscaping in order to ensure that plant material has survived.
7. If the Holder does not commence the construction permitted by this Permit within 24 months of the date of this Permit, this Permit shall lapse and the security shall be returned in full.

Development Permit

No. DP 05-317013

To the Holder: LAWRENCE DOYLE ARCHITECT INC.
Property Address: 8200 CORVETTE WAY
Address: LAWRENCE DOYLE
C/O 200 – 1450 CREEKSIDE DRIVE
VANCOUVER, BC V6J 5B3

8. The land described herein shall be developed generally in accordance with the terms and conditions and provisions of this Permit and any plans and specifications attached to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

AUTHORIZING RESOLUTION NO.
DAY OF , .

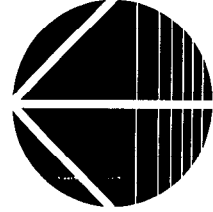
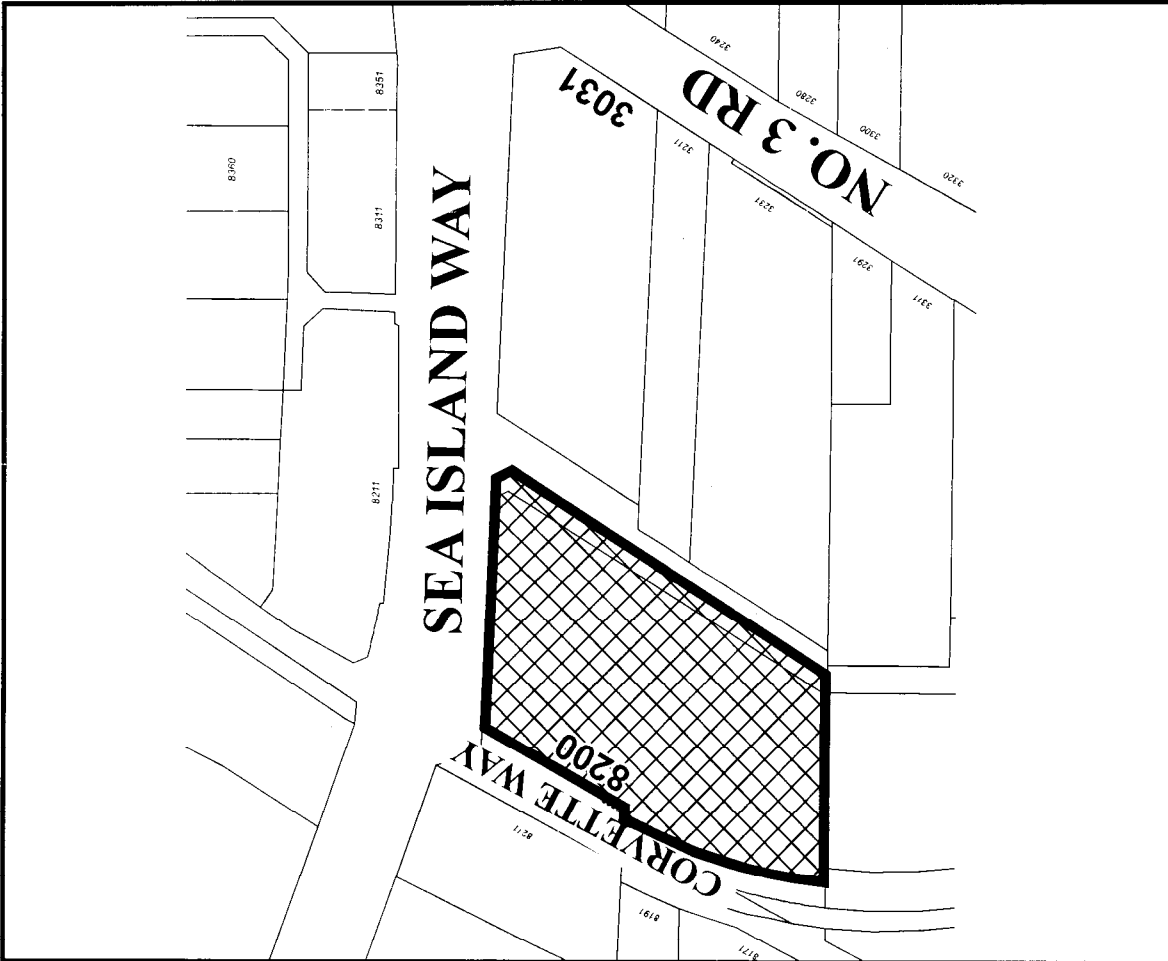
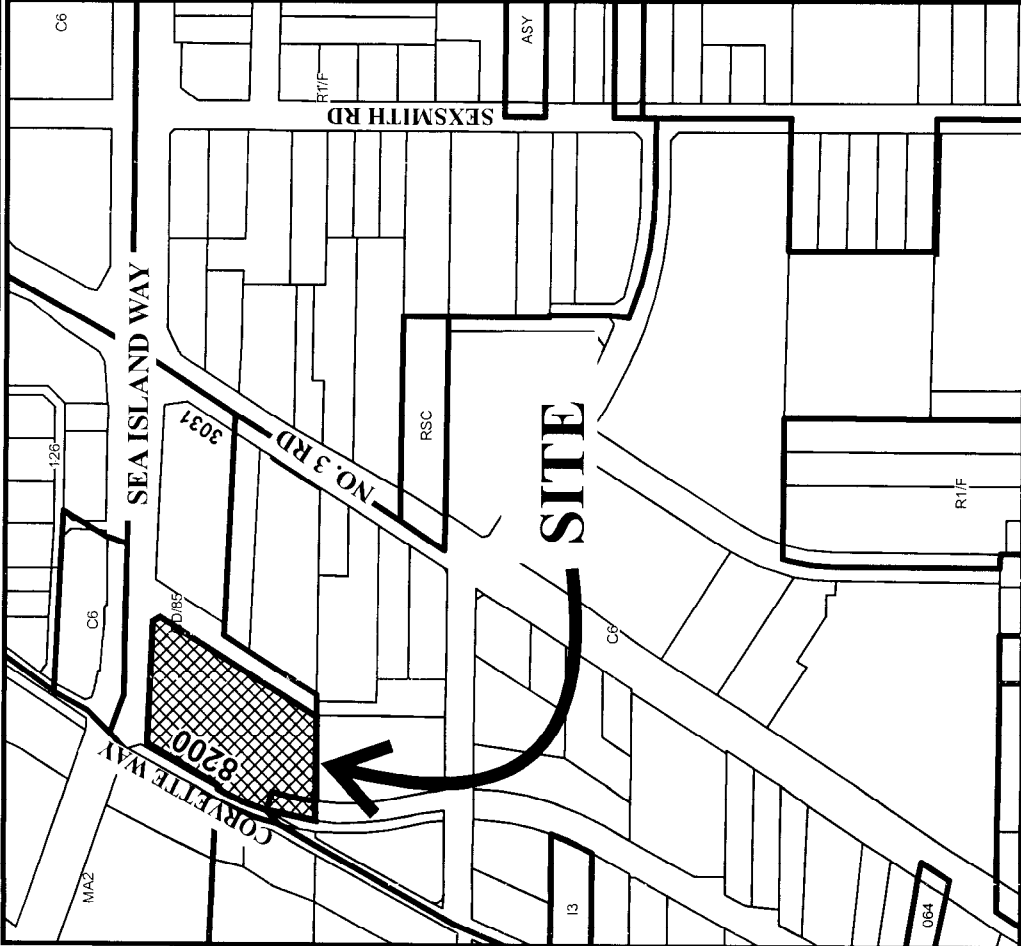
ISSUED BY THE COUNCIL THE

DELIVERED THIS DAY OF , .

MAYOR

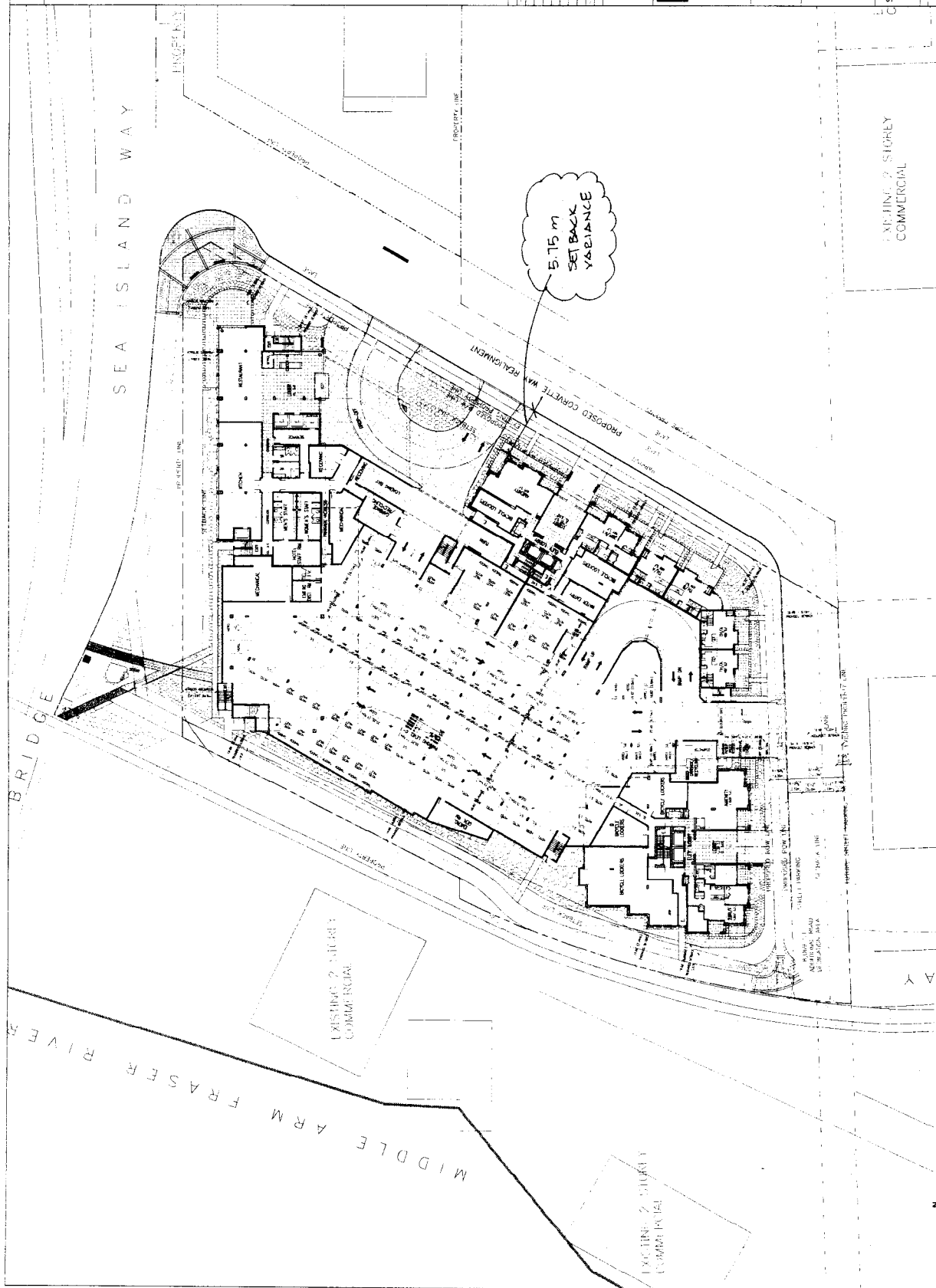


City of Richmond



DP 05-317013 SCHEDULE "A"

Original Date: 12/06/05
 Revision Date:
 Note: Dimensions are in METRES



EXISTING 2 STOREY
COMMERCIAL

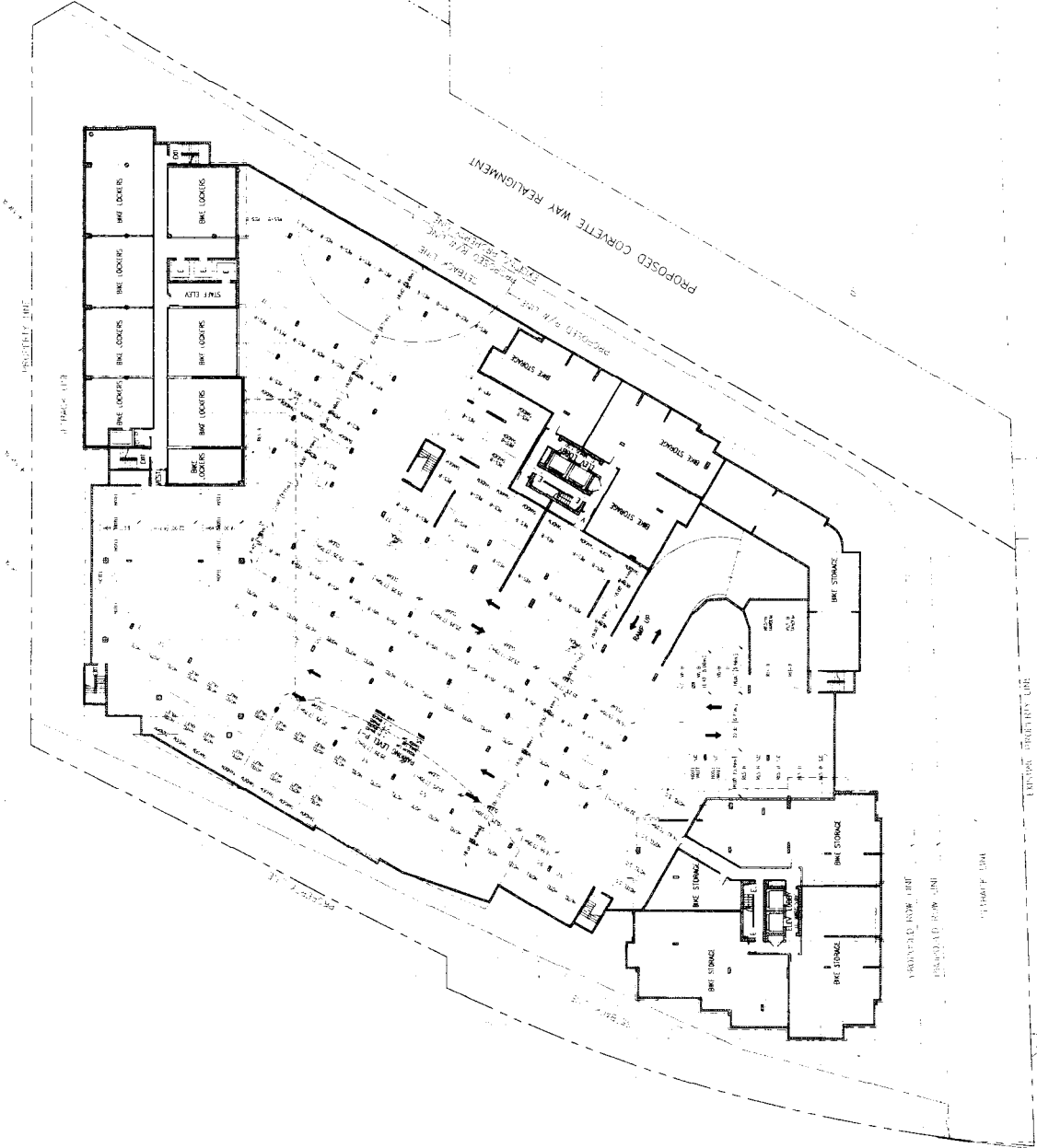
DP 05317013

MAY 23 2007

SITE PLAN
SCALE: 1/8" = 1'-0"



MIDDLE ARM FRASER



PARKING LEVEL P-1
 SCALE: 1/16" = 1'-0"

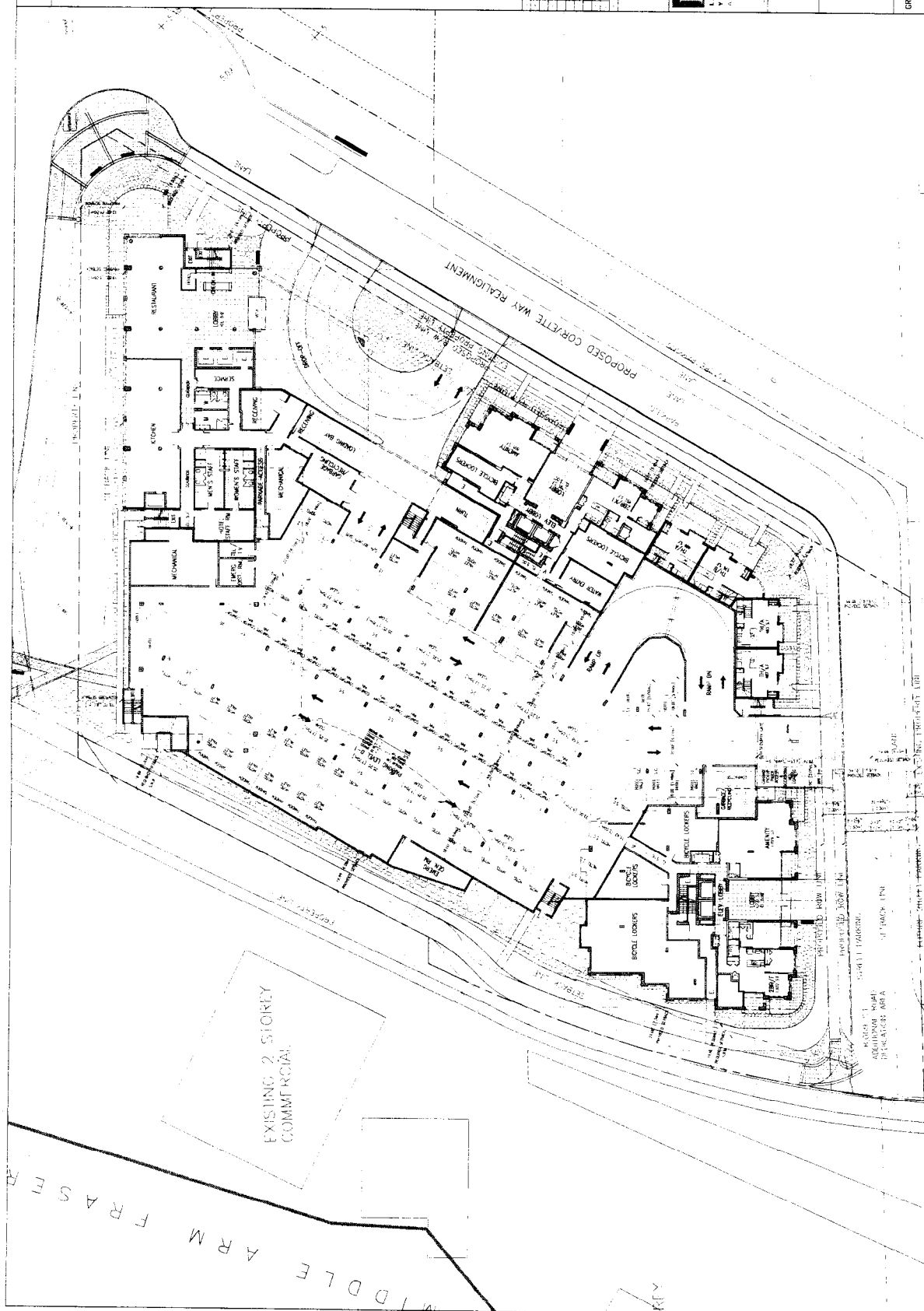
DP 05317013
 MAY 23 2007



**CORVETTE WAY
 DEVELOPMENT**
 Richmond, E.C.

PARKING LEVEL P-1

A.2.1



DP 05317013

MAY 23 2007

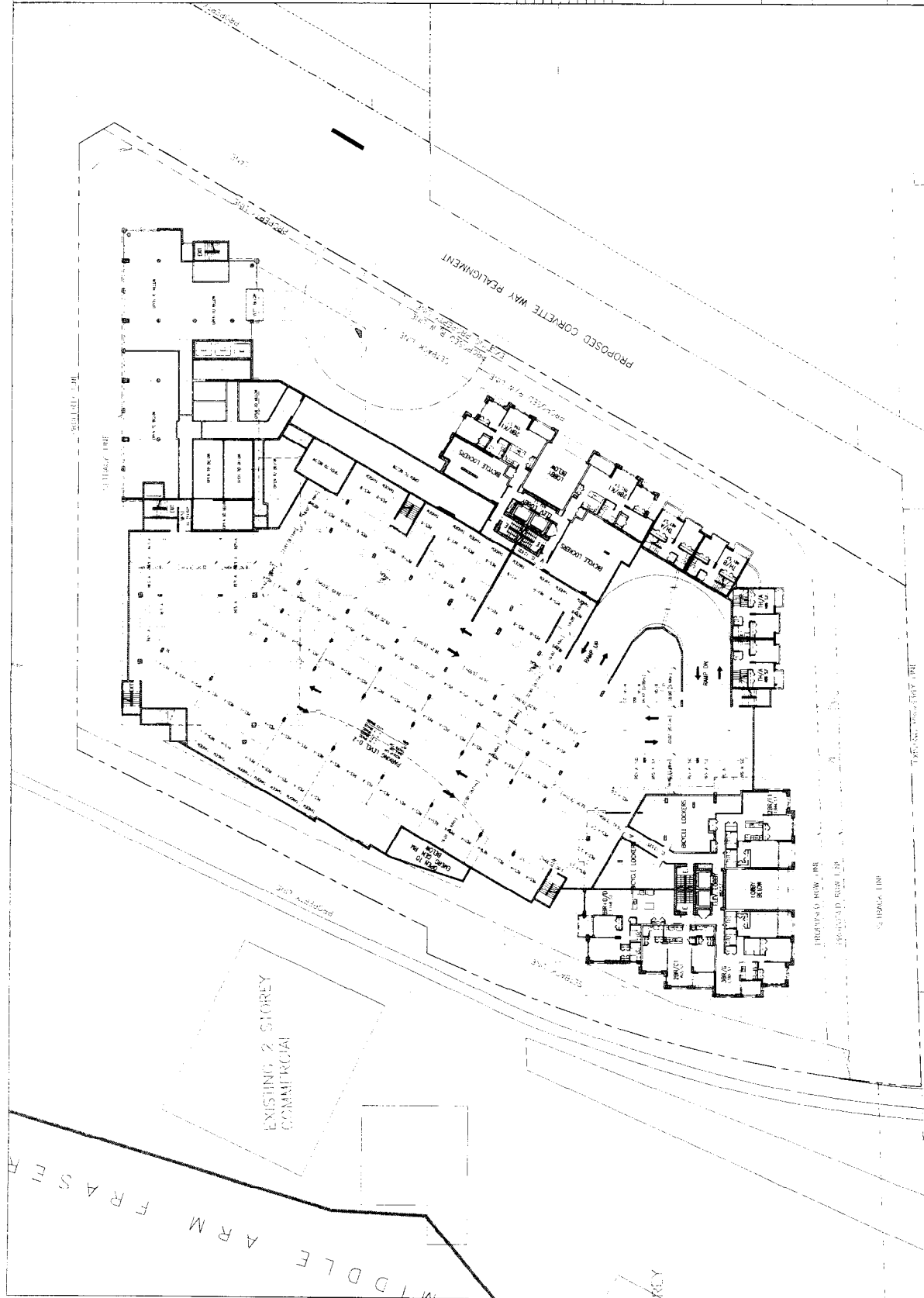
GROUND FLOOR PLAN
SCALE: 1/16" = 1'-0"



CORPETTE WAY DEVELOPMENT
RICHMOND, B.C.

GROUND FLOOR PLAN

A.2.2



DP 05217013

MAY 23 2007

SECOND FLOOR PLAN
SCALE: 1/8" = 1'-0"



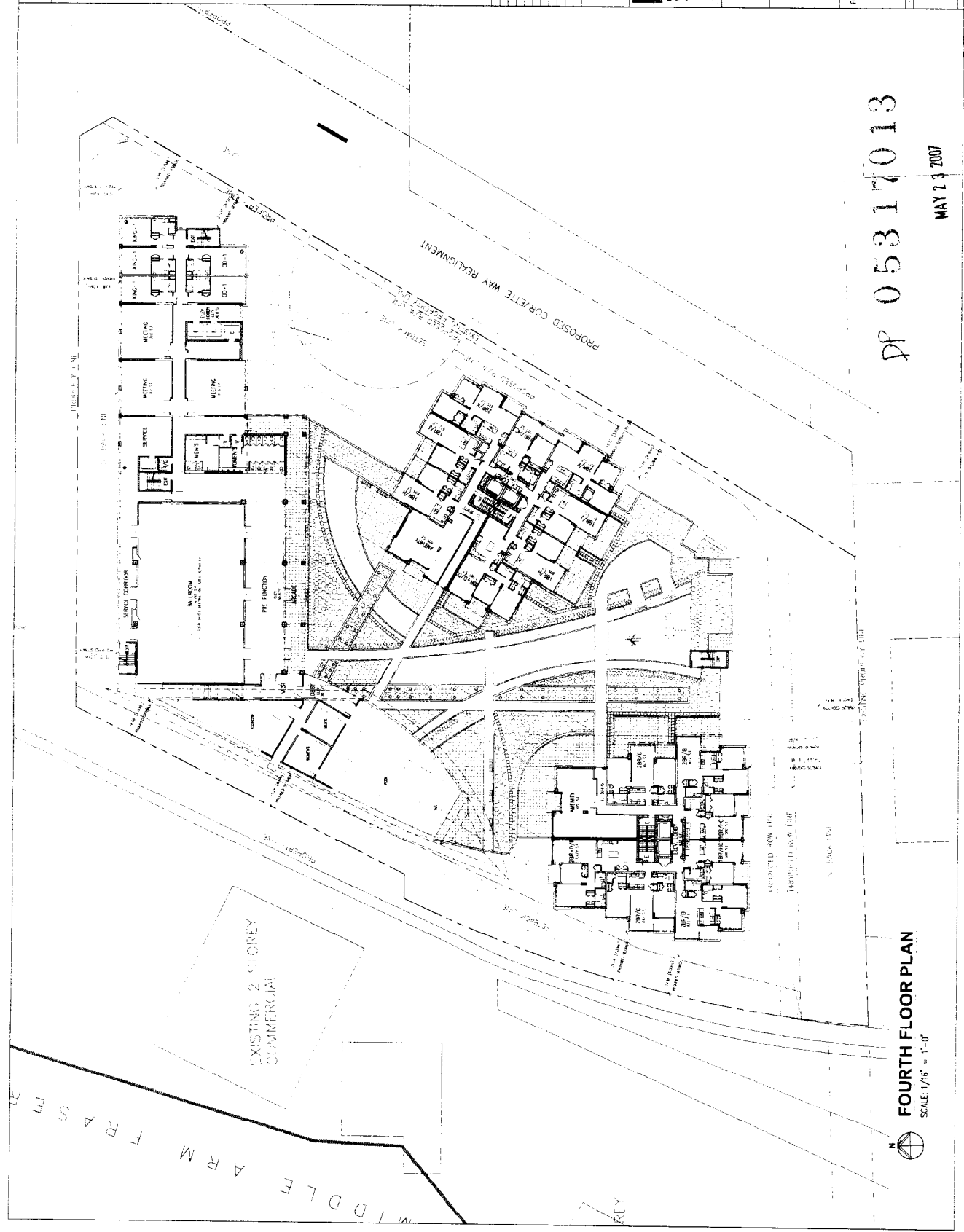
CORRETTE WAY DEVELOPMENT
PARKING, B.C.

SECOND FLOOR PLAN

A.2.3

PP 05317013

MAY 23 2007



FOURTH FLOOR PLAN
SCALE: 1/8" = 1'-0"

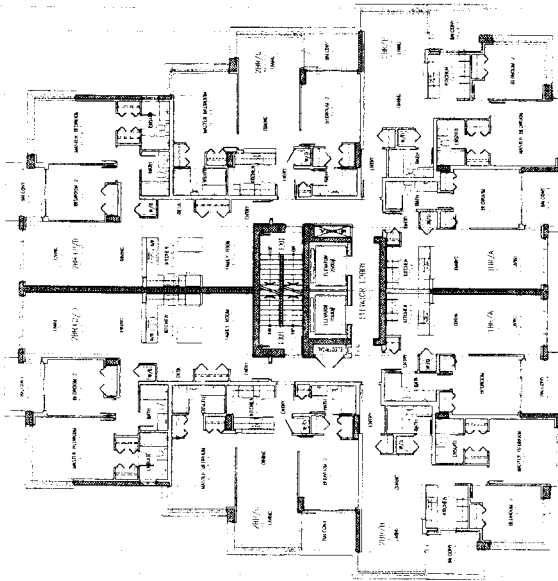
MIDDLE ARM FRASER

DP 05317013

MAY 23 2007

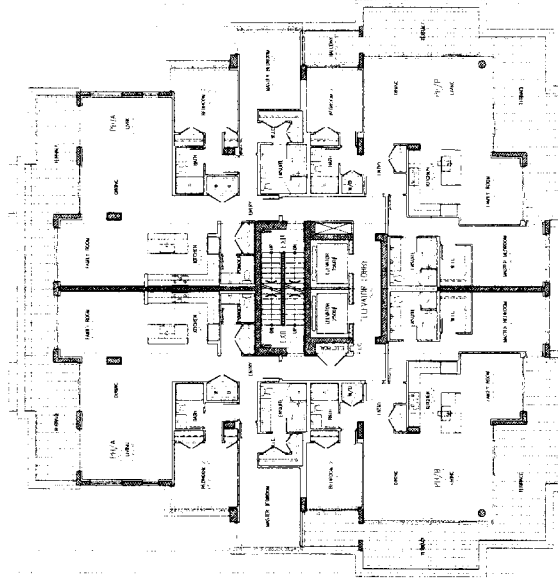


FIFTH FLOOR PLAN
 SCALE: 1/16" = 1'-0"



SUMMARY	TOWER A, TYPICAL FLOOR
NET FLOOR AREA	5,000 S.F.
COMMON AREA	1,777 S.F.
GRG. AREA	1,208 S.F.
CON. RESERVATION	298 S.F.
AREA W/ I.P.	7,283 S.F.

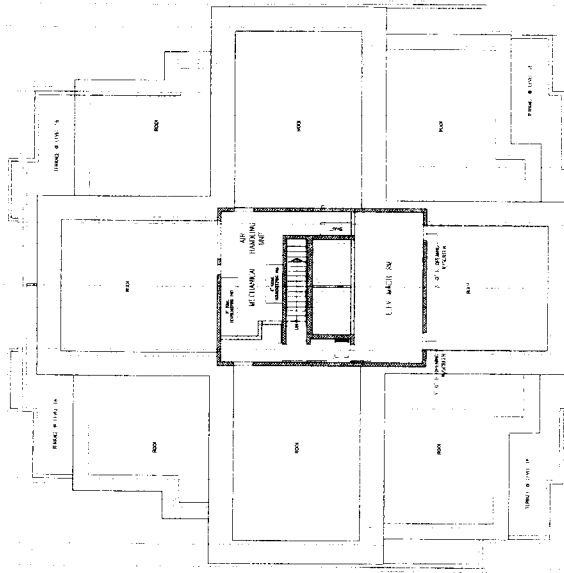
TOWER A TYPICAL FLOOR PLAN (5th-15th Floors)
SCALE: 1/8" = 1'-0"



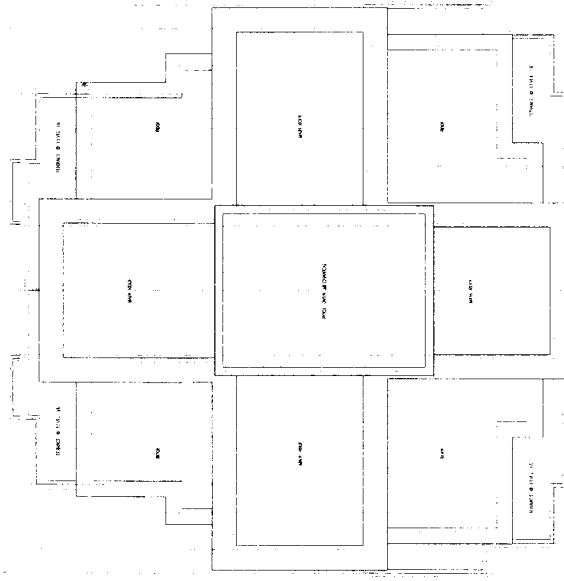
SUMMARY	TOWER A, 16th FLOOR
NET FLOOR AREA	5,000 S.F.
COMMON AREA	1,777 S.F.
GRG. AREA	1,208 S.F.
CON. RESERVATION	298 S.F.
AREA W/ I.P.	7,283 S.F.

PR 05317013
MAY 23 2007

TOWER A PENTHOUSE PLAN (16th Floor)
SCALE: 1/8" = 1'-0"



MECHANICAL RM. & EL. MACH. RM. PLAN
SCALE: 1/8" = 1'-0"



ROOF PLAN
SCALE: 1/8" = 1'-0"

DP 05317013

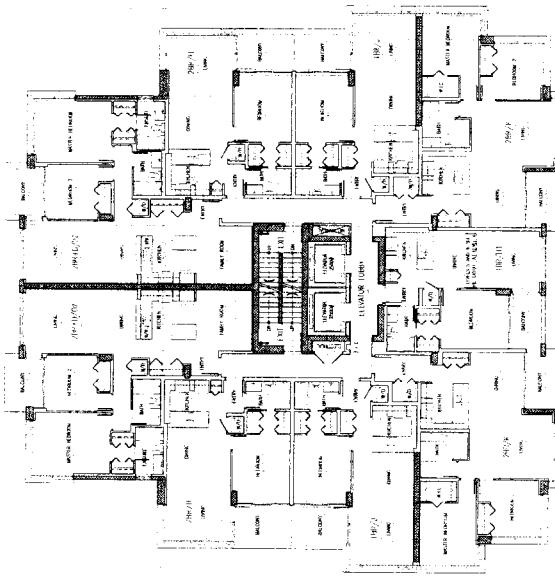
MAY 23 2007

A.3.1

LDA
LAWRENCE DOYLE
ARCHITECTS INC.

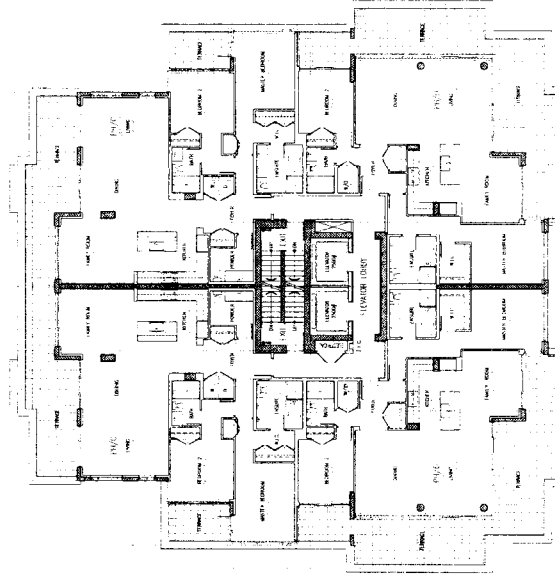
**CORVETTE WAY
DEVELOPMENT**
Richmond, B.C.

MECHANICAL RM. &
EL. MACH. RM. PLAN
ROOF PLAN



SUMMARY - TOWER B, TYPICAL FLOOR
 NET AREA 6,024 SF
 COMMON AREA 1,074 SF
 CROSS AREA 7,100 SF
 CORE RELIEF 2,760 SF
 AREA IN PLAN 7,427 SF

TOWER B TYPICAL FLOOR PLAN (5th-15th Floors)
 SCALE: 1/8" = 1'-0"



SUMMARY - TOWER B, 16TH FLOOR
 NET AREA 5,555 SF
 COMMON AREA 1,074 SF
 CROSS AREA 6,630 SF
 CORE RELIEF 2,760 SF
 AREA IN PLAN 6,069 SF

TOWER B PENTHOUSE PLAN (16th Floor)
 SCALE: 1/8" = 1'-0"



DP 05317013

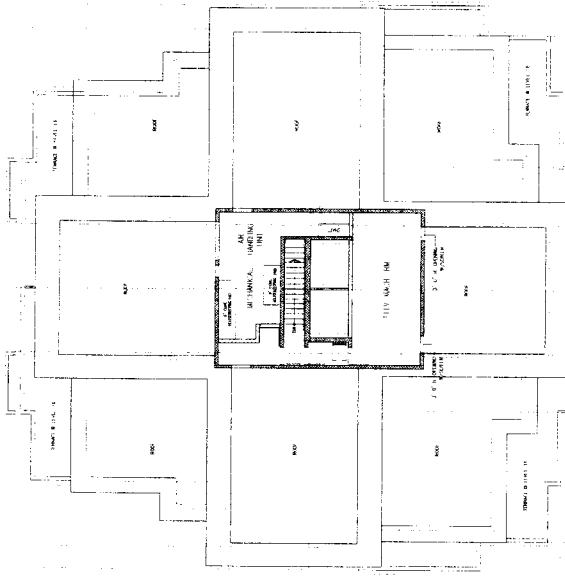
MAY 23, 2007



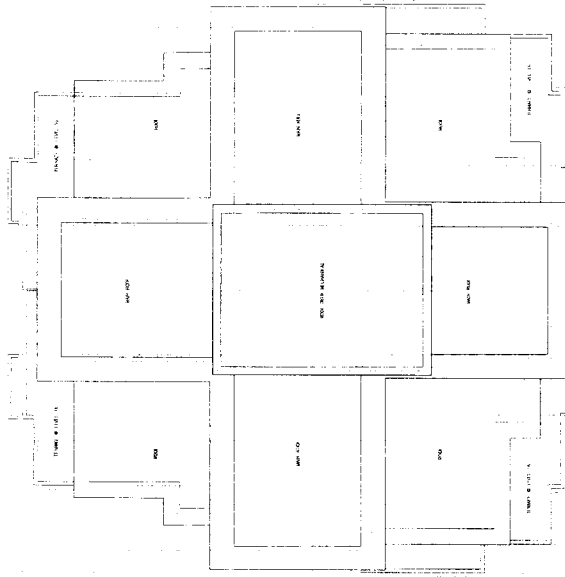
CORNETTE WAY DEVELOPMENT
 PHASES B & C

TYPICAL FLOOR PLAN
 PENTHOUSE PLAN

A.3.7



Mechanical/Elev. Mach. Room Floor Plan
 SCALE: 1/8" = 1'-0"



Mechanical/Elev. Mach. Room Roof Plan
 SCALE: 1/8" = 1'-0"

DP 05317013

MAY 23 2007

A.3.8



LEONARD D. ANDERSON
 ARCHITECTS INC.

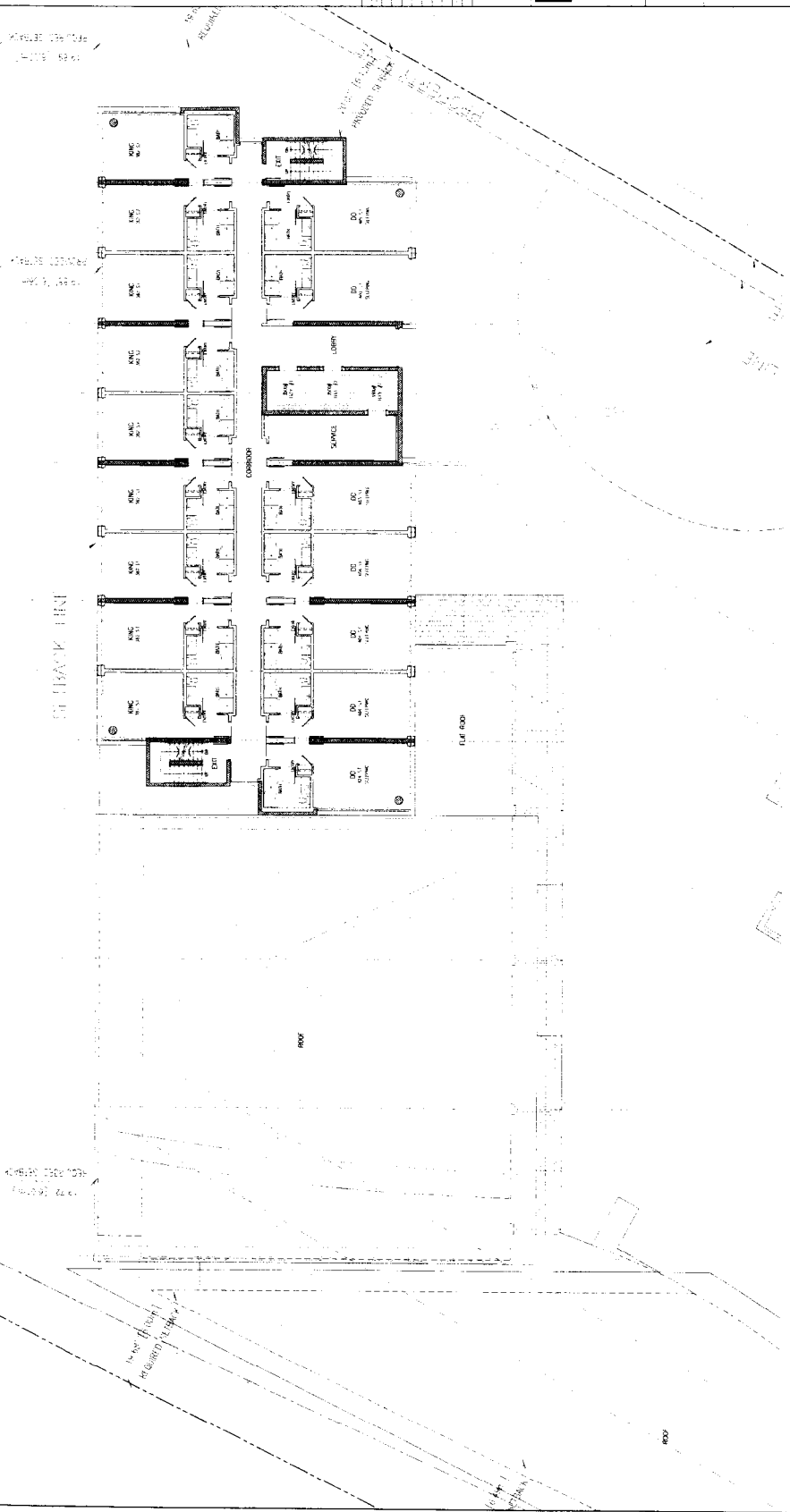
CORVETTE WAY
 DEVELOPMENT
 Richmond, B.C.

MECHANICAL / ELEVATOR
 MACHINE ROOM &
 ROOF PLAN

SEA I

PROPERTY LINE

BACK HKT



COMPANY: PERKINS+WILL
 PROJECT: HOTEL CORVETTE WAY
 LOCATION: CORVETTE WAY, BOSTON, MA
 DATE: 05/23/07
 DRAWN BY: J. B. BROWN
 CHECKED BY: J. B. BROWN
 SCALE: 1/8" = 1'-0"

DP 05317013

MAY 23 2007

HOTEL TYPICAL FLOOR PLAN (5th-11th Floors)

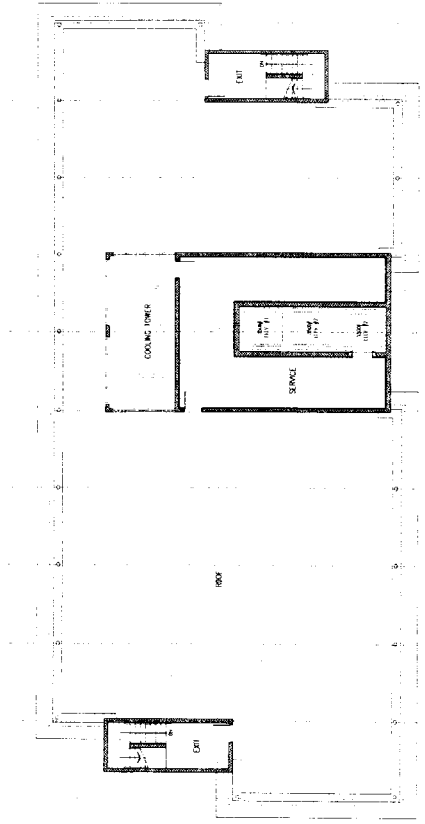
SCALE: 1/8" = 1'-0"



CORVETTE WAY DEVELOPMENT
 BOSTON, MA

HOTEL CORVETTE WAY
 (5th-11th Floors)

A.3.15

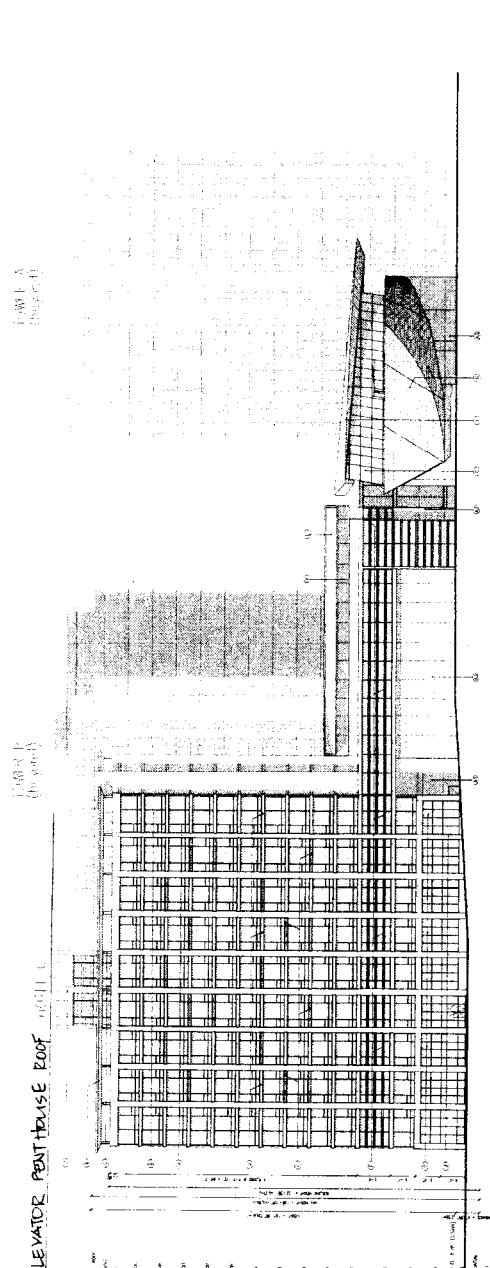


DP 05317013

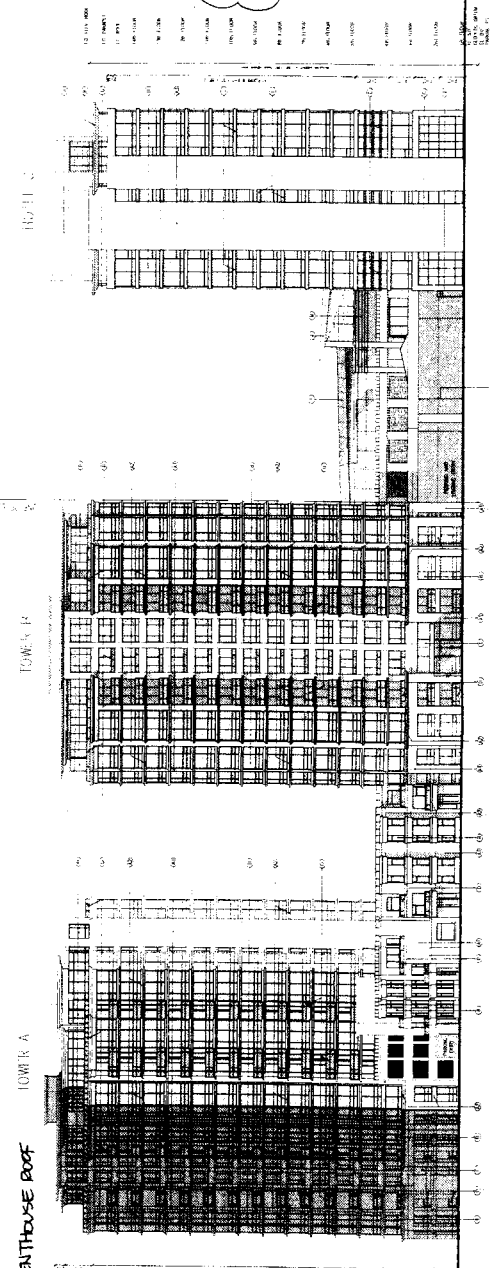
MAY 23 2007

ROOF PLAN - HOTEL
SCALE: 1/8" = 1'-0"





NORTH ELEVATION
 SCALE: 1/16" = 1'-0"



EAST ELEVATION
 SCALE: 1/16" = 1'-0"

DP 05317013
 MAY 23 2007

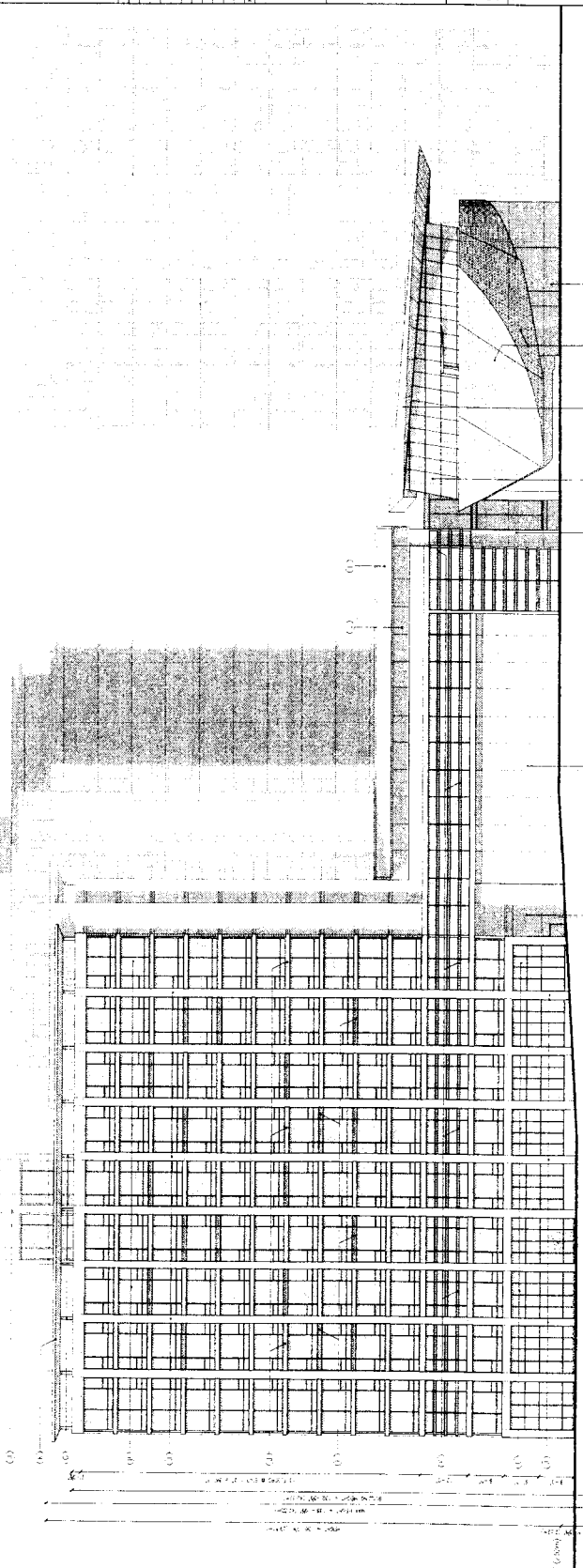
REVISIONS

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HOTEL C

TOWER B
(beeyond)

TOWER A
(beeyond)



NORTH ELEVATION
SCALE: 3/32" = 1'-0"

MAY 23 2007

DP 0531613

NORTH ELEVATION

A.53

LDA+
LAWRENCE DAVIS
YOUNG WRIGHT
ARCHITECTS INC.

ICONSTRUX
1000 W. 10TH AVENUE, SUITE 1000
DENVER, CO 80202
TEL: 303.733.1000
WWW.ICONSTRUX.COM

GROUP 111 WAY
1111 11TH AVENUE
DENVER, CO 80202

NO.	DATE	DESCRIPTION

DATE PLOTTED: 05/23/07	SCALE: 3/32" = 1'-0"
DATE PRINTED: 05/23/07	SCALE: 3/32" = 1'-0"

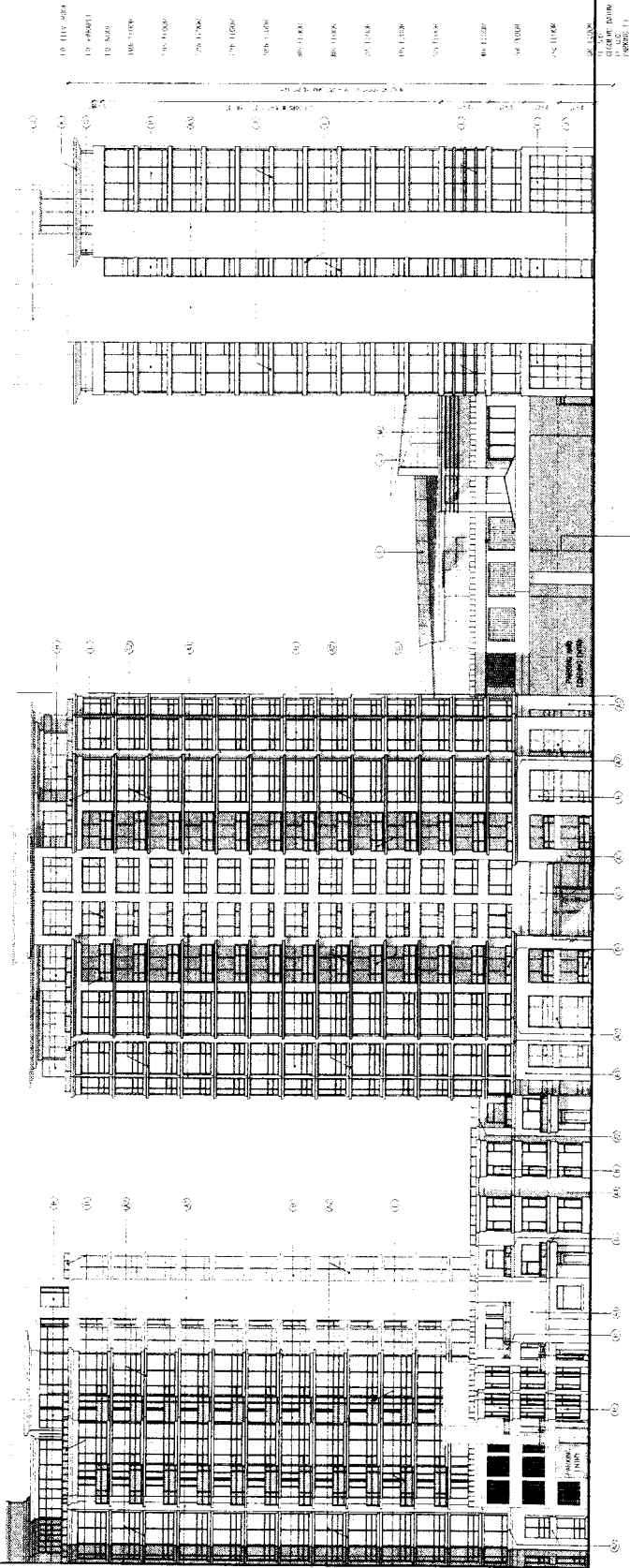
EXTERIOR FINISHES

- A. METAL PANEL FINISH
- B. BRICK
- C. TERRAZZO
- D. CONCRETE
- E. STAINLESS STEEL
- F. GLASS
- G. POLYURETHANE
- H. POLYURETHANE WITH METAL PANEL
- I. POLYURETHANE WITH TERRAZZO
- J. POLYURETHANE WITH BRICK
- K. POLYURETHANE WITH CONCRETE
- L. POLYURETHANE WITH STAINLESS STEEL
- M. POLYURETHANE WITH GLASS

LOWER A

LOWER B

HOTEL C



CORNER SIGNAGE WITH SPOKE-SPIN W/ ZEPHYRUS SIGN

MAY 23 2007
 DF 05317013

EAST ELEVATION
 SCALE: 3/32" = 1'-0"

EAST ELEVATION

A.54

LDPA
 LAWRENCE DOYLE
 YOUNG WRIGHT
 ARCHITECTS INC.

ICONSTRUX
 ARCHITECTS INC.

2700 N. 11th St.
 Suite 100
 Phoenix, AZ 85016

DATE: 05/23/07
 DRAWING NO.: 05317013

PROJECT: 05317013

SCALE: 3/32" = 1'-0"

DATE: 05/23/07

PROJECT: 05317013

SCALE: 3/32" = 1'-0"

DATE: 05/23/07

PROJECT: 05317013

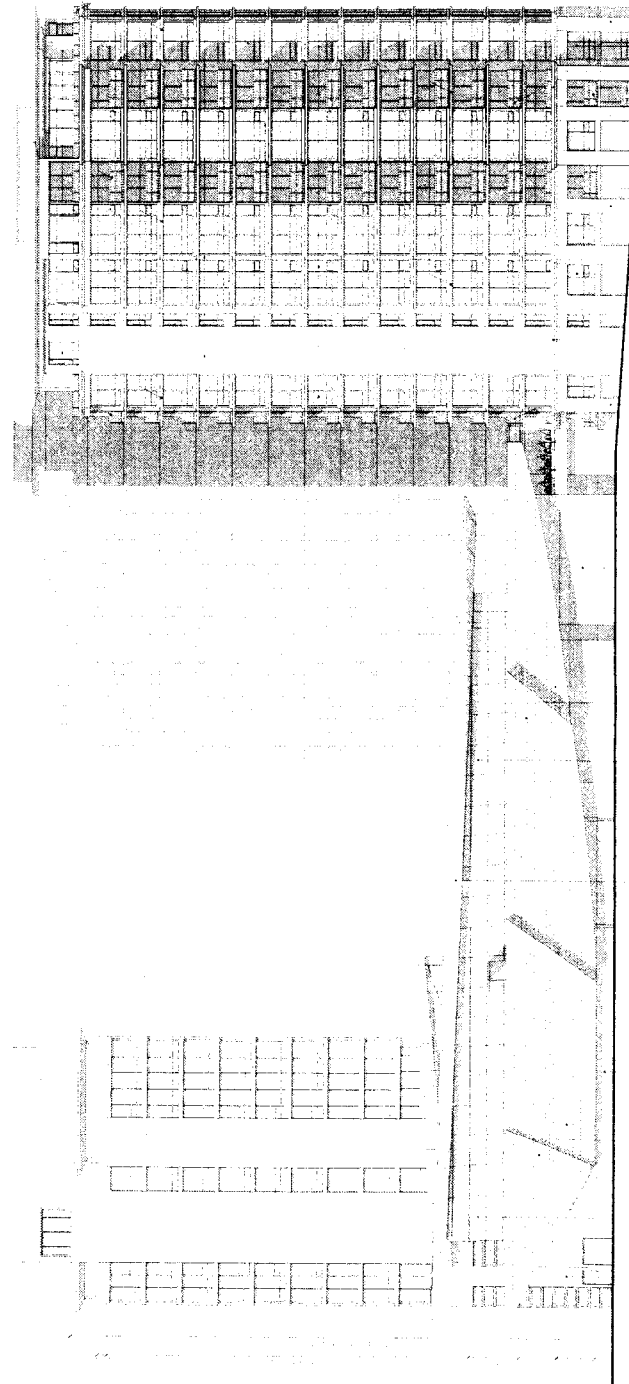
SCALE: 3/32" = 1'-0"

DATE: 05/23/07

DATE: 05/23/07

PROJECT: CORVETTE WAY DEVELOPMENT
DRAWN BY: J. HARRIS
CHECKED BY: J. HARRIS
SCALE: 1/8" = 1'-0"

WEST ELEVATION



WEST ELEVATION

SCALE 3/32" = 1'-0"

DF 05317013

MAY 23 2007

LDG
LAWRENCE DOYLE
YOUNG • WRIGHT
ARCHITECTS INT'L

ICONSTRUX

CORVETTE WAY
DEVELOPMENT
MEMPHIS, TN

WSP: JTH/ARK

A.55

EXHIBIT 2 - FINISHES

- 1. EXTERIOR WALLS: BRICK
- 2. EXTERIOR WALLS: CONCRETE
- 3. EXTERIOR WALLS: STUCCO
- 4. EXTERIOR WALLS: CLAY TILE
- 5. EXTERIOR WALLS: METAL PANELS
- 6. EXTERIOR WALLS: GLASS CURTAIN WALL
- 7. EXTERIOR WALLS: TERRAZZO
- 8. EXTERIOR WALLS: POLISHED CONCRETE
- 9. EXTERIOR WALLS: GRANITE
- 10. EXTERIOR WALLS: MARBLE
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- 200. EXTERIOR WALLS: SLATE

LEVEL A

LEVEL B
(ceyond)

LEVEL C
(ceyond)



LEVEL D
(ceyond)

MAY 23 2007

DP 05317013

SOUTH ELEVATION
SCALE: 3/32" = 1'-0"



2007 AND 2008 DESIGN BUREAU
LAWRENCE BOYLE
YOUNG WRIGHT
ARCHITECTS INC.
1000 EAST 17TH AVENUE, SUITE 1000
DENVER, CO 80202
TEL: 303.733.1100
WWW.LBYWA.COM

CONSTRUCTION
1000 EAST 17TH AVENUE, SUITE 1000
DENVER, CO 80202
TEL: 303.733.1100
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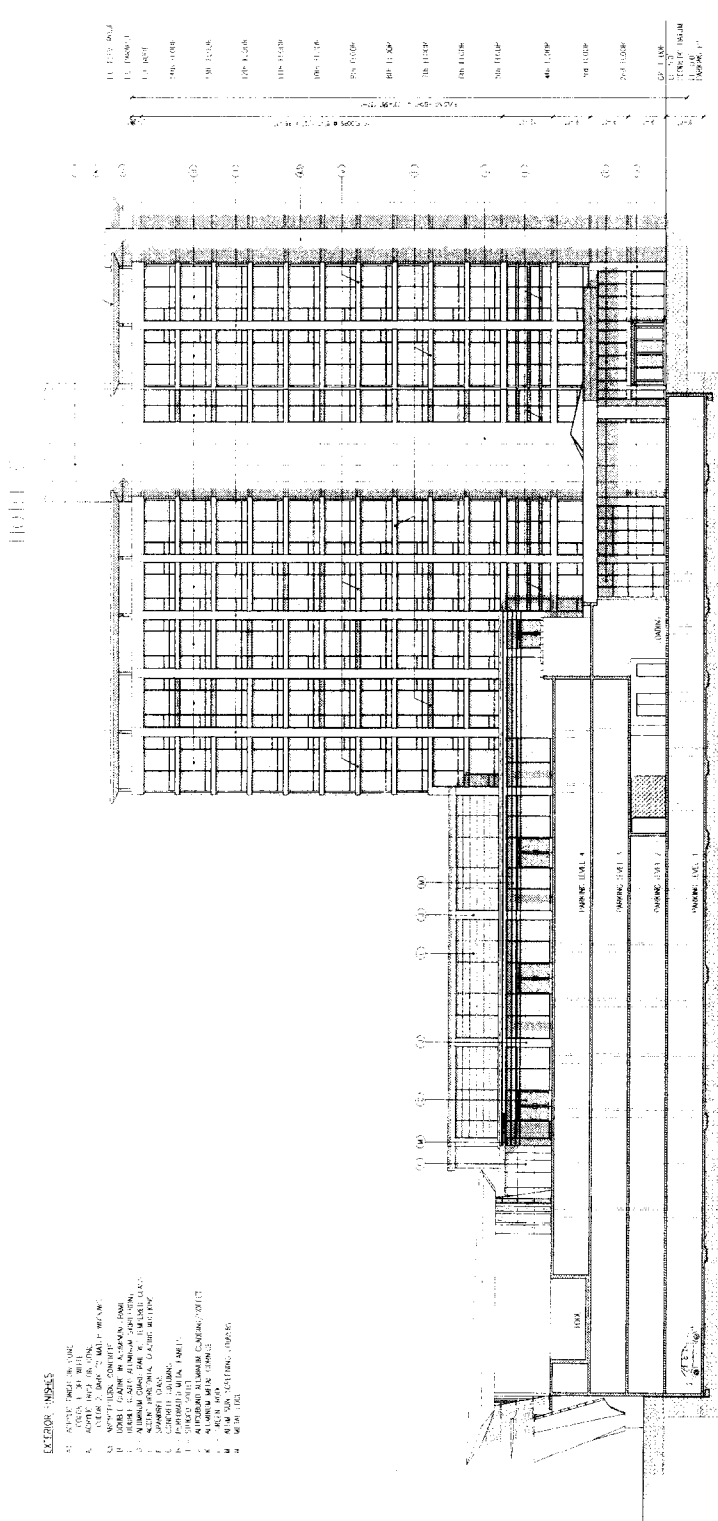
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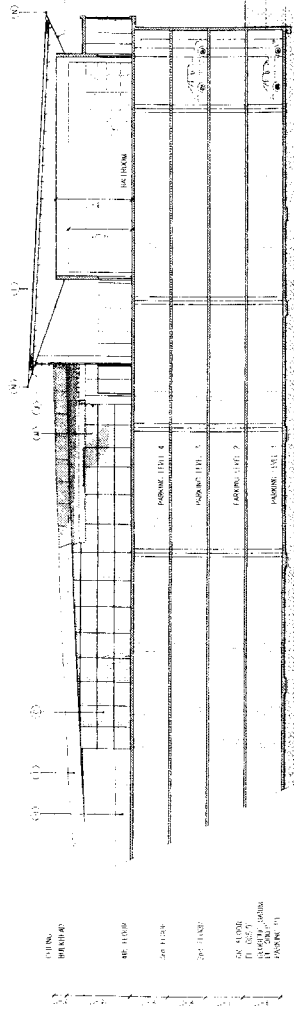
EXTERIOR FINISHES

- 1. EXTERIOR WALLS: BRICK
- 2. EXTERIOR WALLS: BRICK
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- 50. EXTERIOR WALLS: BRICK



HOTEL SOUTH ELEVATION/SECTION

SCALE: 3/32" = 1'-0"



AMENITY EAST ELEVATION/BALLROOM SECTION C

SCALE: 3/32" = 1'-0"

MAY 23 2007

DP 05317013

A.5.7

LD&A
LAWRENCE DOYLE
YOUNG WRIGHT
ARCHITECTS INC.

ICONSTRUX
CONSTRUCTION MANAGEMENT

CLAYTON H. WRIGHT
LAWRENCE DOYLE
ARCHITECTS INC.

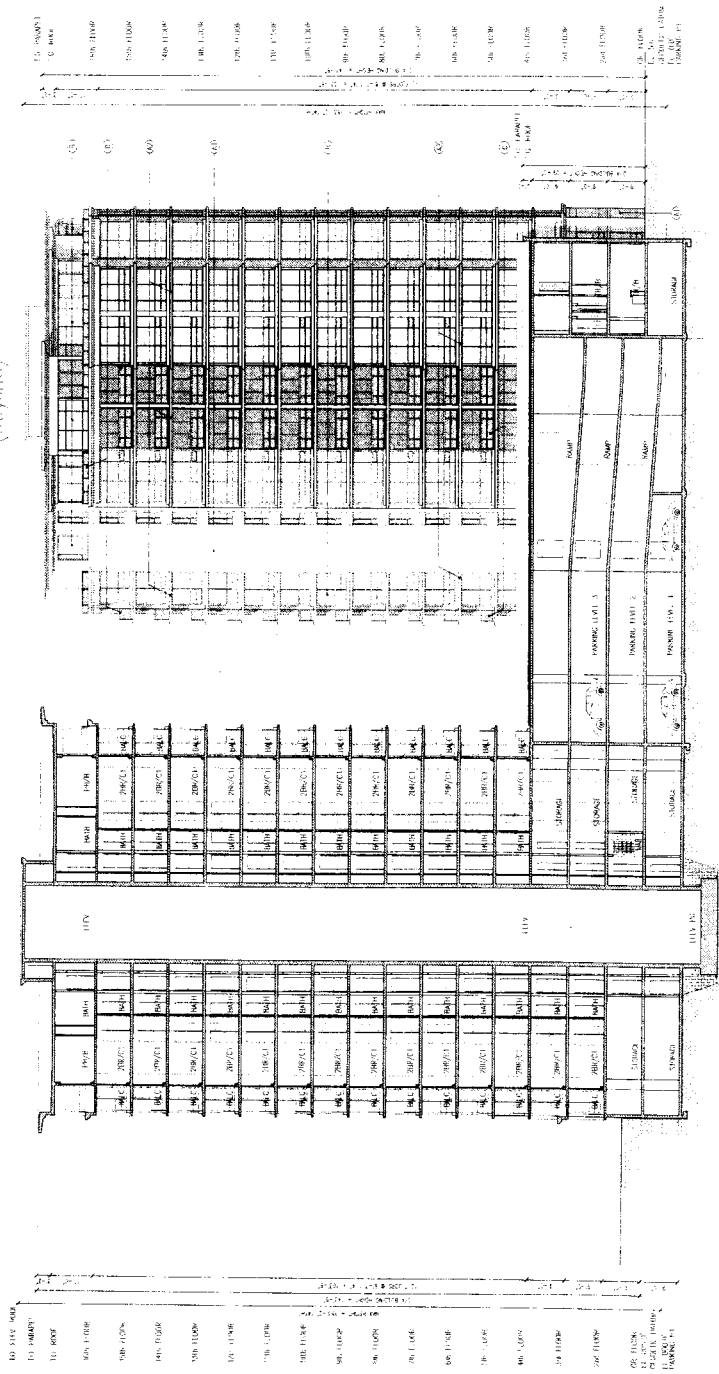
SOUTH & EAST HOTEL ELEVATIONS/SECTIONS	
DATE	05/23/07
PROJECT	SOUTH & EAST HOTEL
CLIENT	CLAYTON H. WRIGHT
DESIGNER	LAWRENCE DOYLE
CONTRACTOR	ICONSTRUX
SCALE	3/32" = 1'-0"
NO. OF SHEETS	13
SHEET NO.	A.5.7

EXTERIOR FINISHES

- A. BRICK, BRUSH ON, 1/2" DIA.
- B. BRICK, BRUSH ON, 1/2" DIA.
- C. BRICK, BRUSH ON, 1/2" DIA.
- D. BRICK, BRUSH ON, 1/2" DIA.
- E. BRICK, BRUSH ON, 1/2" DIA.
- F. BRICK, BRUSH ON, 1/2" DIA.
- G. BRICK, BRUSH ON, 1/2" DIA.
- H. BRICK, BRUSH ON, 1/2" DIA.
- I. BRICK, BRUSH ON, 1/2" DIA.
- J. BRICK, BRUSH ON, 1/2" DIA.
- K. BRICK, BRUSH ON, 1/2" DIA.
- L. BRICK, BRUSH ON, 1/2" DIA.
- M. BRICK, BRUSH ON, 1/2" DIA.
- N. BRICK, BRUSH ON, 1/2" DIA.
- O. BRICK, BRUSH ON, 1/2" DIA.
- P. BRICK, BRUSH ON, 1/2" DIA.
- Q. BRICK, BRUSH ON, 1/2" DIA.
- R. BRICK, BRUSH ON, 1/2" DIA.
- S. BRICK, BRUSH ON, 1/2" DIA.
- T. BRICK, BRUSH ON, 1/2" DIA.
- U. BRICK, BRUSH ON, 1/2" DIA.
- V. BRICK, BRUSH ON, 1/2" DIA.
- W. BRICK, BRUSH ON, 1/2" DIA.
- X. BRICK, BRUSH ON, 1/2" DIA.
- Y. BRICK, BRUSH ON, 1/2" DIA.
- Z. BRICK, BRUSH ON, 1/2" DIA.

TOWER B
(beyond)

TOWER A



MAY 2 2007

DP 05817013

TOWER B SOUTH ELEVATION/SECTION B-B

SCALE: 3/32" = 1'-0"

LDAS
LAWRENCE BOYD
YOUNG WRIGHT
ARCHITECTS INC

ICONSTRUX
CONSTRUCTION MANAGEMENT

CONSTRUCTION MANAGEMENT
1000 W. MAIN ST., SUITE 1000
ANN ARBOR, MI 48106-1000
TEL: 734.769.1000
FAX: 734.769.1001
WWW.ICONSTRUX.COM

CONSTRUCTION MANAGEMENT
1000 W. MAIN ST., SUITE 1000
ANN ARBOR, MI 48106-1000
TEL: 734.769.1000
FAX: 734.769.1001
WWW.ICONSTRUX.COM

**SOUTH TOWER B
ELEVATION/SECTION**

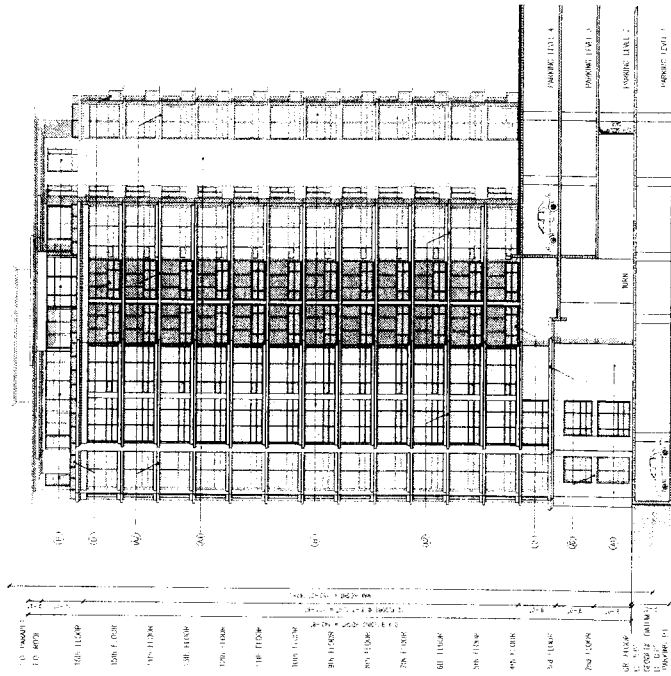
DATE	2007.05.02
BY	LDAS
CHECKED BY	LDAS
SCALE	3/32" = 1'-0"
PROJECT NO.	05817013
REVISIONS	

A.58

EXTERIOR FINISHES

- 1. ASPHALT ROOFING
- 2. ASPHALT ROOFING
- 3. ASPHALT ROOFING
- 4. ASPHALT ROOFING
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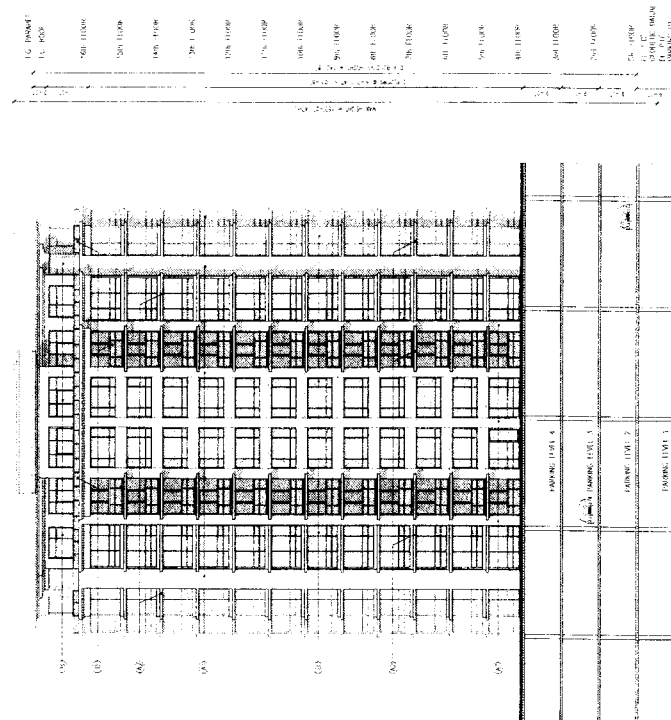
TOWER B



TOWER B NORTH ELEVATION/SECTION D

SCALE: 3/32" = 1'-0"

TOWER B



TOWER B WEST ELEVATION/SECTION E

SCALE: 3/32" = 1'-0"

DP 05317013

MAY 23 2007

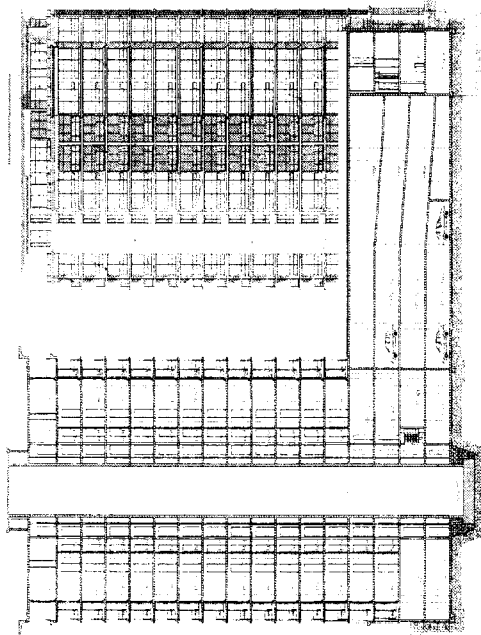
A.59

TDAF
LANE & ASSOCIATES
ARCHITECTS INC.
 1000 WEST 10TH AVENUE, SUITE 1000
 DENVER, CO 80202
 TEL: 303.733.1000
 FAX: 303.733.1001
 WWW.TDAF.COM

ICONSTRUX
 1000 WEST 10TH AVENUE, SUITE 1000
 DENVER, CO 80202
 TEL: 303.733.1000
 FAX: 303.733.1001
 WWW.ICONSTRUX.COM

CURTH WYN
HELMERT
 HIGHWAY 100, SUITE 1000
 DENVER, CO 80202
 TEL: 303.733.1000
 FAX: 303.733.1001
 WWW.CURTHWYNHELMERT.COM

NORTH & WEST TOWER ELEVATIONS/SECTIONS	
NO.	DATE
1	05/23/07
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SECTION A-A
SCALE: 1/8" = 1'-0"

NO.	DATE	DESCRIPTION
1	10/10/06	ISSUED FOR PERMIT
2	11/15/06	REVISIONS
3	12/15/06	REVISIONS
4	01/15/07	REVISIONS
5	02/15/07	REVISIONS
6	03/15/07	REVISIONS
7	04/15/07	REVISIONS
8	05/15/07	REVISIONS
9	06/15/07	REVISIONS
10	07/15/07	REVISIONS
11	08/15/07	REVISIONS
12	09/15/07	REVISIONS
13	10/15/07	REVISIONS
14	11/15/07	REVISIONS
15	12/15/07	REVISIONS
16	01/15/08	REVISIONS
17	02/15/08	REVISIONS
18	03/15/08	REVISIONS
19	04/15/08	REVISIONS
20	05/15/08	REVISIONS
21	06/15/08	REVISIONS
22	07/15/08	REVISIONS
23	08/15/08	REVISIONS
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28	01/15/09	REVISIONS
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96	09/15/14	REVISIONS
97	10/15/14	REVISIONS
98	11/15/14	REVISIONS
99	12/15/14	REVISIONS
100	01/15/15	REVISIONS

LDA+
LAWRENCE DOYLE
YOUNG WRIGHT
ARCHITECTS INC.

ICONSTRUX

CORNETTE WAY
DEVELOPMENT
RICHMOND, S.C.

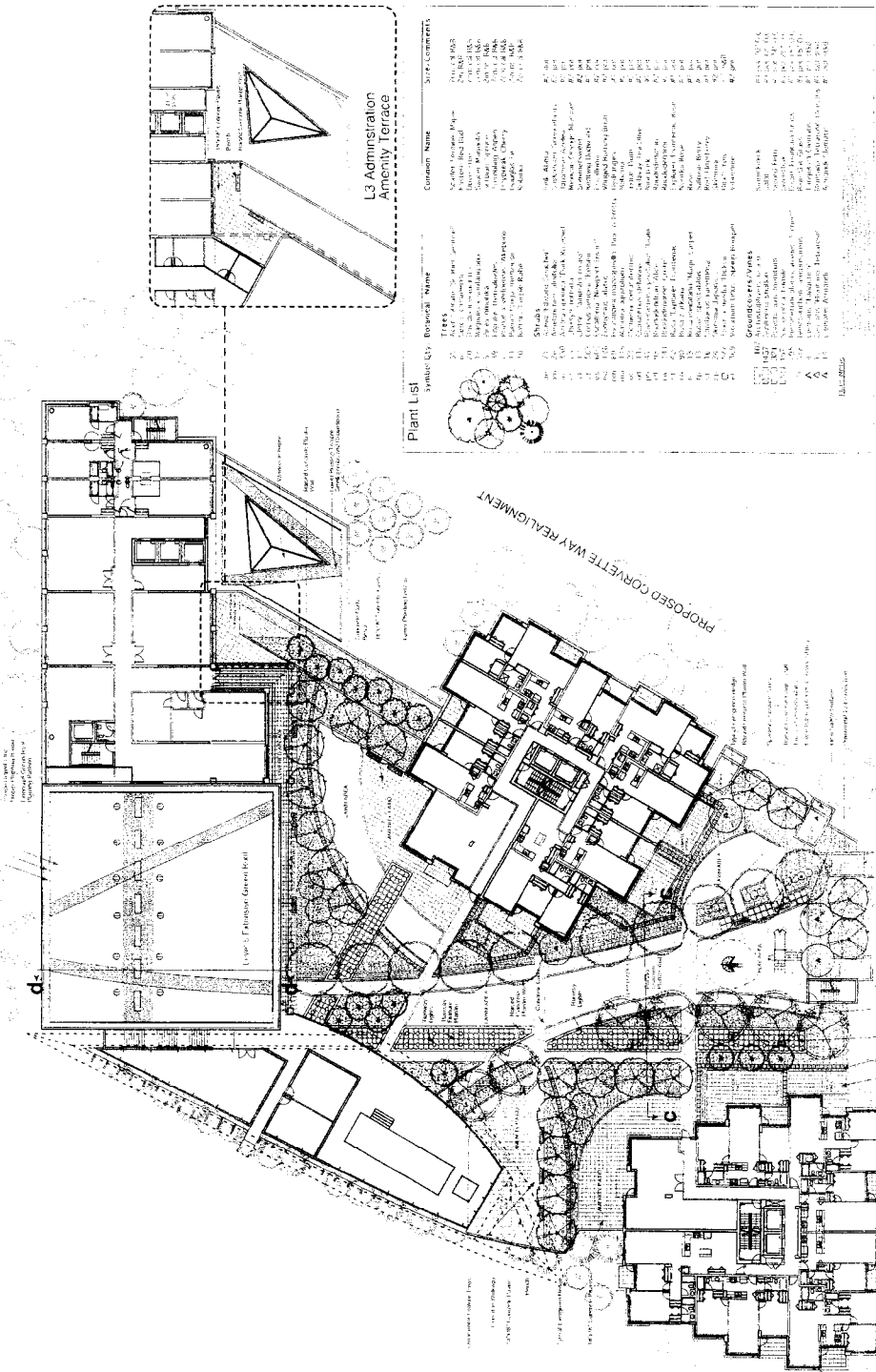
AS SHOWN
A.A.

A.6.1

DP 05317013

MAY 23 2007

SEA ISLAND WAY



Plant List

Symbol	Common Name	Plant Comments
T1	Tree 1	...
T2	Tree 2	...
T3	Tree 3	...
T4	Tree 4	...
T5	Tree 5	...
T6	Tree 6	...
T7	Tree 7	...
T8	Tree 8	...
T9	Tree 9	...
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T96	Tree 96	...
T97	Tree 97	...
T98	Tree 98	...
T99	Tree 99	...
T100	Tree 100	...

Extensive Green Roof (Level 3)

Planting: 100% Green Roof
 Species: 100% Green Roof
 Quantity: 100% Green Roof
 Location: 100% Green Roof
 Details: 100% Green Roof

DP 05317013

MAY 23 2007

DATE: 10/15/14
 DRAWN BY: J. B. [unreadable]
 CHECKED BY: [unreadable]
 PROJECT: [unreadable]

NO.	DESCRIPTION	DATE	BY	CHECKED
1	ISSUED FOR PERMITTING	10/15/14	J. B. [unreadable]	[unreadable]
2	ISSUED FOR CONSTRUCTION	10/15/14	J. B. [unreadable]	[unreadable]
3	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]
4	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]
5	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]
6	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]
7	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]
8	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]
9	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]
10	ISSUED FOR [unreadable]	10/15/14	J. B. [unreadable]	[unreadable]

LDMA+
 LAWRENCE DODD
 YOUNG WRIGHT
 ARCHITECTS INC.

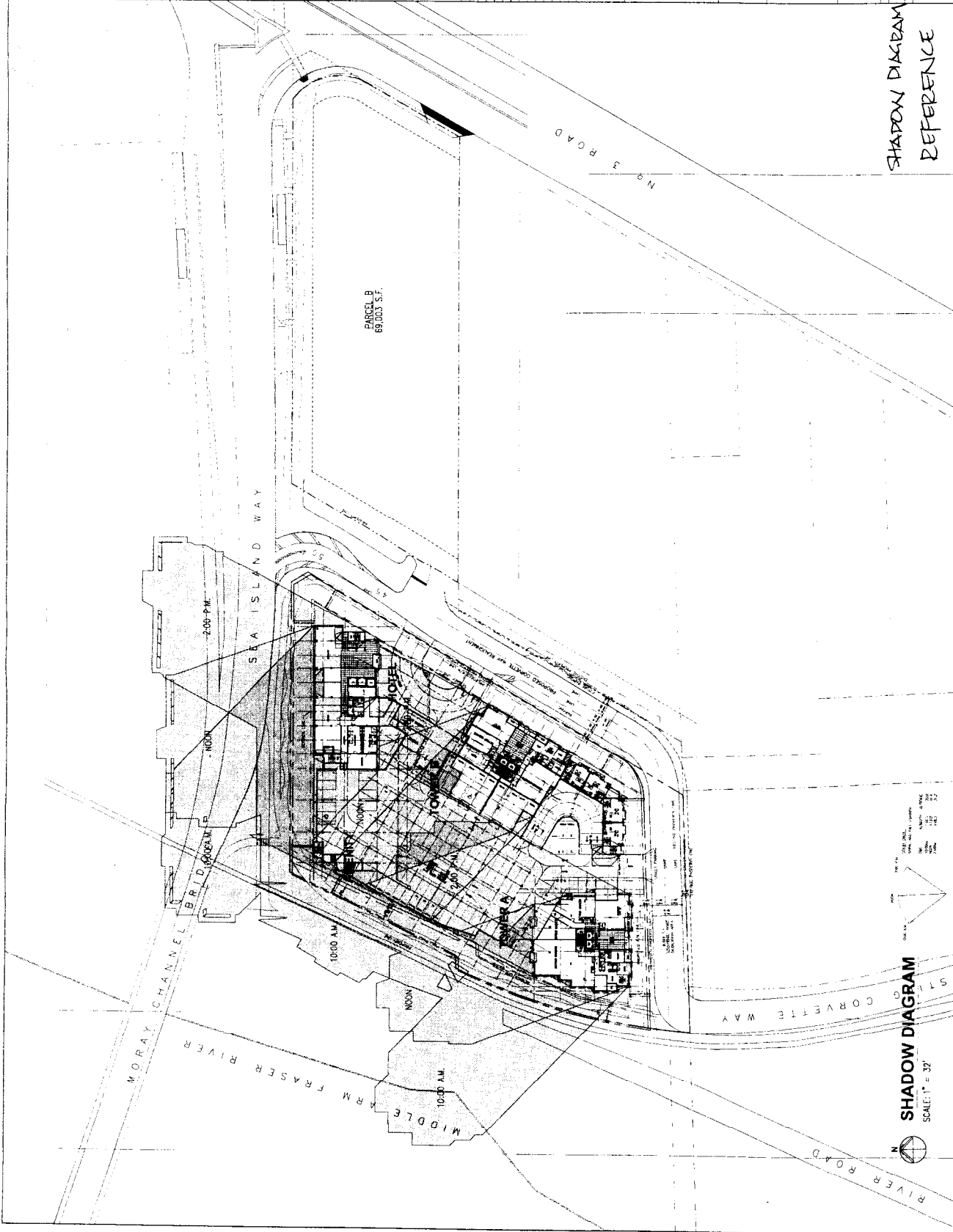
200 - EAST GARDNER ROAD
 SUITE 100
 WASHINGTON, DC 20001
 TEL: 202.462.1100
 FAX: 202.462.1101
 WWW.LDMA.COM

ICONETRUX
 1000 EAST 17TH AVENUE
 SUITE 1000
 DENVER, CO 80202
 TEL: 303.733.1100
 FAX: 303.733.1101
 WWW.ICONETRUX.COM

COMPASSIVE WAY
 DEVELOPMENT
 PHASE 1B, EC

PROJECT NO.
 SHEET NO.
 DATE
 SCALE
 DRAWN BY
 CHECKED BY

SHADOW DIAGRAM
 REFERENCE



SHADOW DIAGRAM
 SCALE: 1" = 32'



RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

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CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

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2:00 PM

PARCEL B
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SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

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NOON

10:00 AM

2:00 PM

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SEA ISLAND WAY

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SEA ISLAND WAY

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N 3 ROAD

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SEA ISLAND WAY

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CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

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10:00 AM

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SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

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ISTING CORVETTE WAY

NOON

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2:00 PM

PARCEL B
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SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

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CORVETTE WAY

RIVER ROAD

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PARCEL B
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SEA ISLAND WAY

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CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

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2:00 PM

PARCEL B
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SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

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NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

ISTING CORVETTE WAY

NOON

10:00 AM

2:00 PM

PARCEL B
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SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER

CORVETTE WAY

RIVER ROAD

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NOON

10:00 AM

2:00 PM

PARCEL B
 69,003 S.F.

N 3 ROAD

SEA ISLAND WAY

MORAY CHANNEL BRIDGEWAY

MIDDLE ARM FRASER RIVER