



To: Planning Committee **Date:** October 2, 2013
From: Wayne Craig, Director of Development **File:** RZ 10-528877
Re: RZ 10-528877 - SmartCentres/Walmart Rezoning Application - Referral

This memo provides a response to the Planning Committee referral dated September 17, 2013 regarding the First Richmond North Shopping Centres Ltd., (SmartCentres) rezoning application (RZ 10-528877). The specific referral items were as follows:

“That the application by First Richmond North Shopping Centres Ltd. for Rezoning at 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road be referred back to staff and staff to report back at the Committee’s next scheduled meeting with the following information:

- 1) types of activities expected in the proposed development which is envisioned as a regional centre;***
- 2) details of the traffic study, e.g. projections on (i) number of people living in the area, (ii) volume of people going into the development; (iii) ingress to and egress from the development including: Alderbridge Way, Garden City Road, No. 4 Road and Cambie St.;***
- 3) back up plans, excluding expropriation, in the event that the City would not be able to acquire the two required lots for the Connector Road;***
- 4) rationalization of staff’s position that the Connector Road will not be needed in 10 years;***
- 5) comments whether the proposed landscaping is adequate, in particular the suitability of tree species to be planted; and***
- 6) advise on how City taxpayers and Council will be protected in the future in terms of the cost associated with the purchase of the two required lots for the construction of the Connector Road.”***

Staff was also directed to provide the Committee and all members of Council with the following:

- i. traffic study on the proposed development;
- ii. report of SmartCentres’ and the City’s environmental consultants on the Environmentally Sensitive Area (ESA);
- iii. Report on the status of trees on site; and
- iv. map showing Agricultural Land Reserve Areas where a buffer is not required.

The request for additional information will be addressed in this memo and separate memos from the Director of Transportation, Manager of Real Estate Services and the Manager of Policy Planning.

SmartCentres Revised Proposal:

SmartCentres now proposes two key changes to the previous (September 17, 2013) development proposal including:

- Shifting May Drive approximately 6m to the west, which would increase the park dedication plus a cash contribution totalling \$52,125 for environmental enhancements to the proposed park. Parks staff would utilize these funds as part of a comprehensive approach to improve this future park area when the final configuration of the park has been determined; and
- Provision of a separate cash contribution totalling \$186,155 for ecological enhancements to the West Cambie Park (WCP). Parks staff would use these funds to extend the north-south ecological corridor with the West Cambie Park (WCP).

See also the SmartCentres revised Site Plan (**Attachment 1**), the revised Landscape Plan (**Attachment 2**) and a detailed comparison table of the previous SmartCentres previous proposal dated August 29, 2013 and the revised proposal dated September 26, 2013 (**Attachment 3**).

Referral Item 1:

- *“Types of activities expected in the proposed development, which is envisioned as a regional centre”*

Regarding this referral item staff can report the following:

Background:

In 2003, SmartCentres submitted a rezoning application (RZ 03-235259) for properties located at 4660 to 4740 Garden City Road and 9040 to 9500 Alexandra Road in order to facilitate an automobile-oriented shopping centre. Council consideration of this rezoning application initiated a review of the West Cambie Area Plan (WCAP). The updated WCAP was adopted by Council on July 24, 2006 after considerable public input and makes provisions for the significant redevelopment of this planning area, including the creation of an urban village retail/commercial centre in the vicinity of the Alderbridge Way and Garden City Road intersection.

The WCAP - Alexandra Neighbourhood Land Use Plan includes 2 mixed use areas to ensure the creation of a vibrant, pedestrian-friendly urban village centre within the West Cambie Area (WCA). “Mixed Use Area A” to the west of the future ‘High Street’ envisions a compact, urban, street-fronting retail/commercial area while “Mixed Use Area B” to the east of the future ‘High Street’ permits large and small floor plate retail commercial units (CRU’s). SmartCentres proposal east of the ‘High Street’ would include a Walmart store with a total floor area of 14,975 m² (161,188 ft²), which is consistent with the requirements of “Mixed Use Area B” in the WCAP. SmartCentres proposal also includes site planning, architectural and landscape design elements that would contribute to the creation of a complete and balanced community within the WCA.

Open space enhancements within the proposed SmartCentres development include the extension of the Alexandra Way pedestrian corridor connecting the Alderbridge Way/Garden City Road intersection with the Alexandra Road/High Street intersection and the WCA to the north via wider sidewalks, pedestrian plazas, extensive decorative paving and raised pedestrian crossings within the proposed development west of the ‘High Street’. However, further design development is required at the Development Permit stage to ensure a high quality design with an appropriate level of pedestrian amenities.

Types of Proposed Retail/Commercial Development:

SmartCentres proposes a shopping centre type development with a gross floor area of 36,018 m² (387,692 ft²). The proposed land use would be exclusively retail/commercial development in predominantly 1-storey buildings with the exception of the Walmart store (2 to 3-storeys), Building A, the 2-storey anchor building west of the 'High Street' along Garden City Road and the parkade (4-storeys) along Alexandra Road.

SmartCentres proposes two anchor tenants within the overall development. The Walmart store totalling approximately 161,188 ft² would be the anchor tenant on the east side of the 'High Street'. The anchor tenant on the west side of the 'High Street' would be a department-type store of approximately 35,000 ft² offering household, pharmacy, cosmetics, and electronics sales as well as other ancillary uses on the ground floor of Building A. In total, Building A would contain 3 to 4 tenants with 2 tenants on the second floor including a fashion retailer and a house wares retailer, roughly 26,000 ft² in size each. SmartCentres indicates that approximately 34% or 130,888 ft² of the proposed total floor area would consist of smaller CRU's ranging in size from 2,000 to 4,000 ft². SmartCentres anticipates that the smaller CRU's will offer a variety of products and services typically found in a shopping centre, including retail (e.g., fashion, shoes, house wares, electronics, cosmetics), restaurants, services (e.g., personal services such as hair salons, optical, medical), as well as financial services such as banks and credit unions.

The products and services would be appropriate to service the local scale needs of nearby residents, with some fashion and other offerings that will serve a broader clientele. SmartCentres has indicated that 15% of anticipated traffic would be from outside Richmond, predominately from south Vancouver as they anticipate the market south of the Fraser River would be served by the Tsawwassen First Nations retail/entertainment complex, with the region east of Richmond being serviced by the Queensborough Walmart store.

Proposed Streetscape Design:

SmartCentres proposes differing streetscape design responses to the various surrounding perimeter road conditions. Pedestrian/bike greenways are proposed along Alderbridge Way and Garden City Road. Street fronting CRU's are not proposed along the majority of surrounding perimeter roads however, the 'High Street' would feature 2 continuous blocks of small CRU's fronting the street and the site plan would set the proposed Walmart store back from adjacent streets to permit small CRU's along both Alderbridge Way and the 'High Street'.

The architectural design proposes enhanced facade treatments at key corner locations surrounding the overall site as well as elaborate architectural and landscape screening techniques of proposed parking and loading facilities along Alexandra Road. The design of the 'High Street' proposes many small CRU's, appealing architectural façades, variety in streetscape design and high quality pedestrian amenities, which are important components of a village centre concept intended to generate and attract pedestrian activity.

Further design enhancements could be achieved through the Development Permit stage including more extensive building façade enhancements along perimeter streets, more effective screening and buffering of parking, loading and service areas and boulevard landscape refinements.

Referral Item 2 & 4:

- *“Details of the traffic study, e.g. projections on (i) number of people living in the area, (ii) volume of people going into the development; (iii) ingress to and egress from the development including: Alderbridge Way, Garden City Road, No. 4 Road and Cambie St.;”*
- *“Rationalization of staff’s position that the Connector Road will not be needed in 10 years;”*

These referral items are addressed in a separate memo from the Director of Transportation.

Referral Item 3 & 6:

- *“Back up plans, excluding expropriation, in the event that the City would not be able to acquire the two required lots for the Connector Road;”*
- *“Advise on how City taxpayers and Council will be protected in the future in terms of the cost associated with the purchase of the two required lots for the construction of the Connector Road.”*

These referral items are addressed in a separate memo from Manager of Real Estate Services.

Referral Item 5:

“Comment whether the proposed landscaping is adequate, in particular the suitability of tree species to be planted;”

Regarding this referral item staff can report the following:

West Cambie Park (WCP):

The OCP designated “Park” area on the development lands is approximately 1.51 ac (see **Attachment 4**). SmartCentres proposes to shift May Drive west by 5.89 metres, in order to maximize the area east of May Drive that could be consolidated with the City's future park area. The previous SmartCentres proposal included 1.08 acres of public space in the form of the elevated ‘green deck’ (36,360 ft² or 0.835 ac), “Area E” (3,702 ft² or 0.085 ac) and “Area J” (7,039 ft² or 0.16 ac). This does not include other publicly accessible open space within the development site, such as the Alexandra Way pedestrian corridor, which would be secured via a Statutory Right-of-Way (SRW). The net difference between the WCAP designated “Park” area and the SmartCentres proposed publicly accessible open space in the previous proposal was approximately 0.44 acres.

SmartCentres proposal to shift May Drive to the west would increase the size of “Area J” from 7,039 ft² to 13,733 ft² but slightly reduce the size of “Area E” from 3,702 ft² to 3,605 ft². SmartCentres revised proposal would increase the provision of public open space from 1.08 to 1.23 acres, consisting of the ‘green deck’ (36,360 ft² or 0.835 ac), “Area E” (3,605 ft² or 0.083 acres) and “Area J” (13,773 ft² or 0.316 ac). This results in a net increase of 0.16 ac from the previous proposal and would reduce the park area deficit from 0.44 to 0.28 acres.

Parks staff have recently completed habitat enhancement work totalling 7,809 m² (1.93 ac) in the WCP to extend a north-south ecological corridor. These enhancements consisted of drainage, soil placement, plant material supply and installation and establishment maintenance. Further extension of the north-south ecological corridor in the WCA will require additional habitat enhancement in the WCP. Parks staff estimate that the unit cost of this habitat enhancement is approximately \$46.00 per m² or \$186,155 per acre.

In order to address the park area deficit of 0.28 acres, SmartCentres proposes a cash contribution of \$52,125 (0.28 ac x \$186,155/ac) for habitat enhancement of the future park area east of May Drive.

SmartCentres has also agreed to the provision of the following sustainability features within the proposed development:

- Compact development with the majority of the parking stalls (683 of 1,153) located within structures or under buildings;
- Approximately 70% of the floor area or roughly 271,384 ft² within the proposed development would connect to the Alexandra District Energy Utility;
- Four (4) parking stalls (2 east and 2 west of the 'High Street') would be designated and equipped with electric vehicle charging equipment and 10% of the remaining parking stalls or approximately 115 stalls would be pre-ducted for future installation of charging stations;
- Two separate end-of-trip bicycle facilities (1 east and 1 west of the 'High Street') would be provided including a minimum of 3 water closets per gender, 2 wash basins per gender and 3 showers per gender in each facility;
- A total of 291 bike parking spaces (119 Class I secure storage spaces plus 172 Class II bike racks), which is a 25% increase or 59 bike parking spaces above the minimum bylaw requirement;
- Three (3) new or upgraded bus stops location (bus shelters and accessible pads) within the surrounding vicinity of the proposed development lands;
- Minimum LEED Silver equivalent building design standard;
- Reduced stormwater discharge through permeable paving, rooftop detention and bio-swales;
- Reduced water consumption through water efficient plumbing fixtures;
- Reduced energy consumption and efficiencies due to enhanced building envelope and HVAC systems; and
- Reduced light pollution through high-efficiency, night-sky friendly lighting.

Environmentally Sensitive Area (ESA):

The existing OCP designated "ESA" consists of approximately 2.57 ac within the proposed development site (see **Attachment 4**). SmartCentres environmental consultant (Stantec Consultants Ltd.) has conducted a detailed assessment of the designated ESA and recommends the ESA be reduced by approximately 1.0 ac (see **Attachment 5**) due to "disturbance (roads and existing development), limited connectivity to additional habitat, small ... size, and the presence of invasive weeds in the understorey".

The City's external environmental consultant (Raincoast Applied Ecology) was asked to review the extent of the ESA in the SmartCentres assessment. The external environmental consultant agreed with the Stantec findings regarding the extent and condition of the ESA with the recognition that there is a core area of higher value birch forest to the south, with areas of more disturbed vegetation to the north, which supports the Stantec recommendation to reduce the size of the ESA from approximately 2.57 to 1.57 ac. This reduced ESA area (1.57 ac) on the proposed development site is further impacted by the existing May Drive alignment in the WCAP, which would result in a further 0.35 ac reduction in the size of the ESA area (see **Attachment 6**). Therefore, the net ESA deficit is 1.22 ac (2.57 – 1.0 – 0.35 ac).

SmartCentres revised proposal would provide the equivalent of approximately 1.22 acres of ESA compensation consisting of the following components:

- land dedication of “Area J” totalling 0.32 acres; and
- a cash contribution of \$186,155 for 1 acre of habitat enhancement to extend the north-south ecological corridor in the West Cambie Park, which is more than the estimated 0.9 acres of the ESA land deficit.

It is also noteworthy that SmartCentres proposal does not take into account the ‘green deck’ and associated transition space (“Area E”), which total approximately 0.92 ac (0.835 + 0.083 ac).

Existing Vegetation:

SmartCentres has submitted a Tree Survey and Arborist Report for the proposed development site (see **Attachment 7**). The existing vegetation on the development site consists of 172 bylaw sized trees including 3 significant trees and 1 high value tree (i.e., 1-80cm caliper Douglas Fir, 1-111cm caliper Douglas Fir, 1-100cm Linden and 1-35cm Balsam Fir). These significant and high value large trees are generally located along the north edge of the site on the south side of the Alexandra Road drainage ditch.

The existing grades on the proposed development lands are generally between 0.9 and 1.2 m geodetic, which is approximately 1.6 m below the bylaw required flood proof elevation of 2.6 m geodetic in the WCA. City required improvements to Alexandra Road would involve widening and raising the road grade from the existing 1.0 m to minimum 2.0 m geodetic and higher in some locations, which would result in the elimination of the ditch along the south side of the road and in turn impact the existing large trees in close proximity to this ditch.

Richmond’s Tree Preservation Coordinator has reviewed SmartCentres tree survey and arborist report, conducted a separate on-site assessment of the existing trees, reviewed the SmartCentres development proposal and concurs with SmartCentres proposal to remove all existing trees on the development portions of the site subject to the provision of a minimum 2 replacement trees for each 1 proposed tree removal including no less than 344 replacement trees on-site as well as four (4) specimen coniferous trees at minimum 5 m height as replacements for the 3 significant and 1 high value trees.

SmartCentres has also submitted a supplemental arborist report to assess the suitability of relocating existing on-site trees and to provide more details regarding the condition of vegetation along the north side of Alderbridge Way (see **Attachment 8**). This report indicates that the 3 significant trees, the 1 high value tree and the other on-site trees are not suitable for retention or relocation due to the:

- proposed new north-south roads including May Drive and the ‘High Street’;
- road widening along Alexandra Road and Alderbridge Way;
- approximately 1.6 m increase in the elevation of existing site grades over the entire site to meet the flood protection bylaw requirements;
- spreading root zone of trees in a high water table condition and the anticipated root damage associated with relocation;
- susceptibility of wind-throw after transplanting;
- low probability of survival; and
- high cost of tree relocation.

Therefore, SmartCentres proposes to remove all existing on-site trees within the proposed development lands with the exception of “Area J”. Planning and Parks staff concur with this assessment.

Proposed Planting:

The revised landscape planting strategy (see **Attachment 2**) proposed by SmartCentres relies extensively on native plant material including the provision of 672 equivalent trees (556 on-site trees plus a cash contribution for an additional 116 off-site trees) consisting of 34 different tree species. This would represent 3.9 times the proposed 172 tree removals. The majority of the proposed replacement trees would be planted at larger than the minimum required bylaw replacement tree size and would be primarily native tree species. SmartCentres also proposes to plant approximately 6,662 primarily native shrubs, grasses, vines and groundcovers. See the comparison of proposed tree and shrub planting between the previous and current SmartCentres proposals in **Attachment 3**. SmartCentres proposed tree and shrub planting consists of the following components:

- A minimum of 556 trees to be planted on-site or along streets plus the provision of a cash contribution in the amount of \$40,600 (116 trees x \$350/each) for tree planting enhancements within “Area J”;
- A minimum of 6,201 shrubs to be planted on-site plus the provision of a cash contribution in the amount of \$11,525 (461 shrubs x \$25/each) for shrub planting enhancements within “Area J”;
- A voluntarily cash contribution \$186,155 (approximately 1 acre x \$186,155/ac based on \$46/m²) for environmental enhancements within the WCP in order to extend an existing north-south ecological corridor within the West Cambie Park. This cash contribution would compensate for approximately 0.1 acre more than the currently estimated 0.9 acre ESA deficit; and
- There would be no Development Cost Charge (DCC) credits for the above ecological enhancement contributions.

Since the site grades would generally be raised by approximately 1.6 m, the proposed trees and shrubs would not be planted in the existing native soils or ground water conditions. All proposed plant materials would be supplied from BC Nursery Trade Association (BCNTA) affiliated nurseries and grown in conditions that would be compatible with anticipated on-site conditions. The details regarding specific on-site tree planting practices would be further addressed at the Development Permit stage and likely would include measures such as tree vaults, continuous trenching to provide increased soil volume for root growth, no pocket planting of trees or shrubs, increased soil depths, automatic irrigation and other measures intended to ensure the proposed tree and shrub plantings continue to flourish and thrive into the future. The proposed planting scheme along the perimeter fronting roads, particularly along Alderbridge Way and Garden City Road, envision multiple rows of formal native street trees with grass boulevards within the road right-of-ways in combination with informal, native tree and shrub plantings within the building setbacks.

The program of use for the proposed elevated ‘green deck’ area is envisioned as a passive recreation area but the activity program would be addressed in more detail at the Development Permit stage. It is also anticipated that the planting strategy would feature predominantly native, drought tolerant tree and shrub planting subject to Parks staff review through detailed landscape design development during the Development Permit stage.

Recent Correspondence:


The City has received approximately 26 emails regarding the SmartCentres rezoning application, since the Planning Committee meeting of September 17, 2013. See **Attachment 9** for copies of this correspondence.

Conclusion

Staff recommend that the bylaws associated with the SmartCentres rezoning application (RZ 10-528877) be introduced, given first reading and forwarded to Public Hearing, recognizing that the revised SmartCentres proposal would:

- reduce the ESA deficit from 1.06 to 0.92 acres, with SmartCentres providing a cash contribution of \$186,155 to the City in order to further extend the north-south ecological corridor in other parts of the West Cambie Park. This contribution is based on recent City enhancements to the West Cambie Park in order to extend the north-south ecological corridor through the West Cambie Area (i.e., \$46/m² or \$186,155/acre); and
- reduce the park land deficit from 0.44 to 0.28 acres, with SmartCentres providing a cash contribution of \$52,125 for the enhancement of the future park on the east side of May Drive once the park boundaries are finally established. This contribution would be approximately equivalent to the value of recent park enhancements by the City to extend the north-south ecological corridor in other parts of the West Cambie Park (i.e., 0.28 ac x \$186,155/ac).

The current development proposal regarding the SmartCentres rezoning application (RZ 10-528877) including the above improvements are now reflected in the revised Rezoning Considerations, which have executed by SmartCentres (see **Attachment 10**).

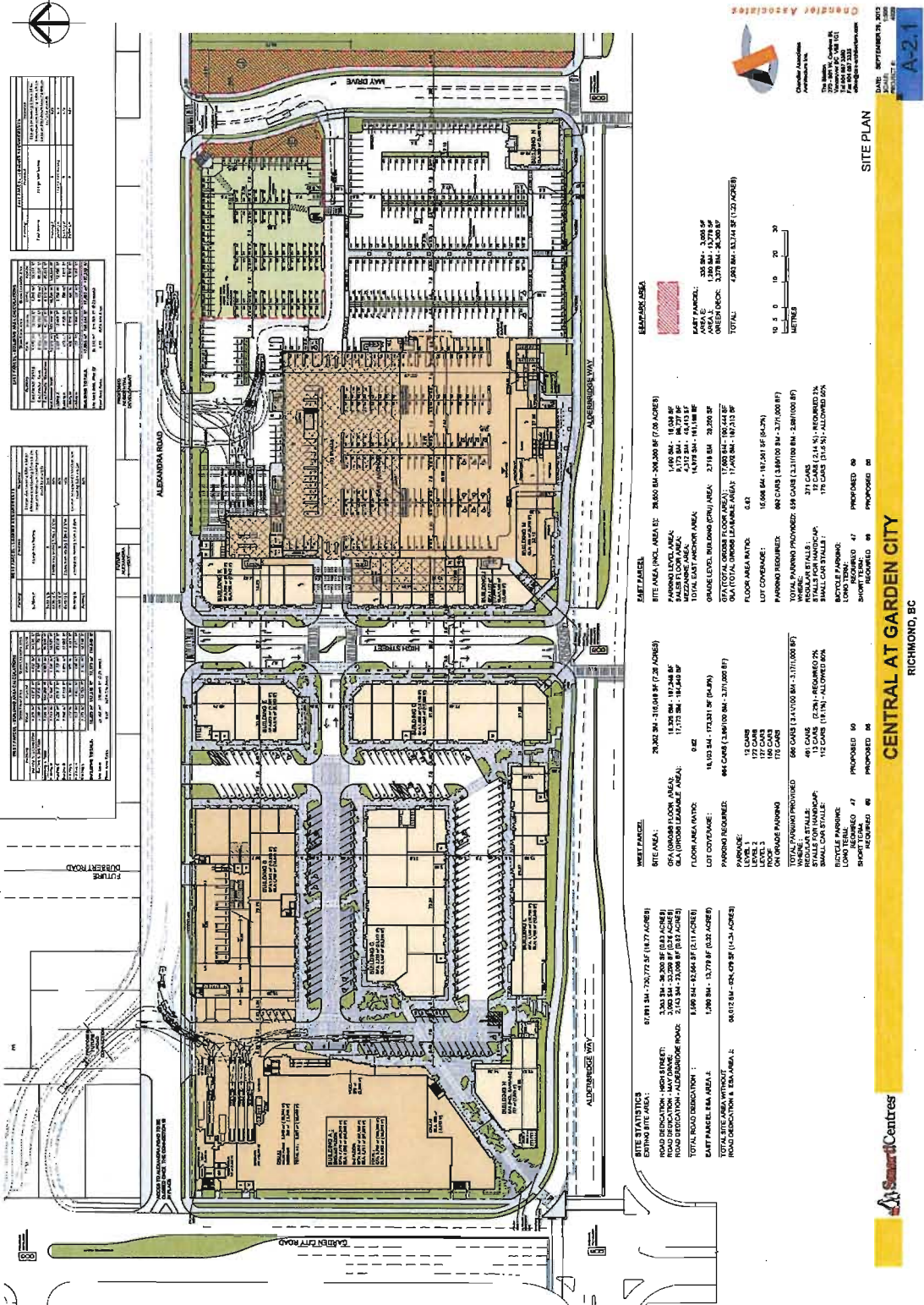


Wayne Craig
Director of Development

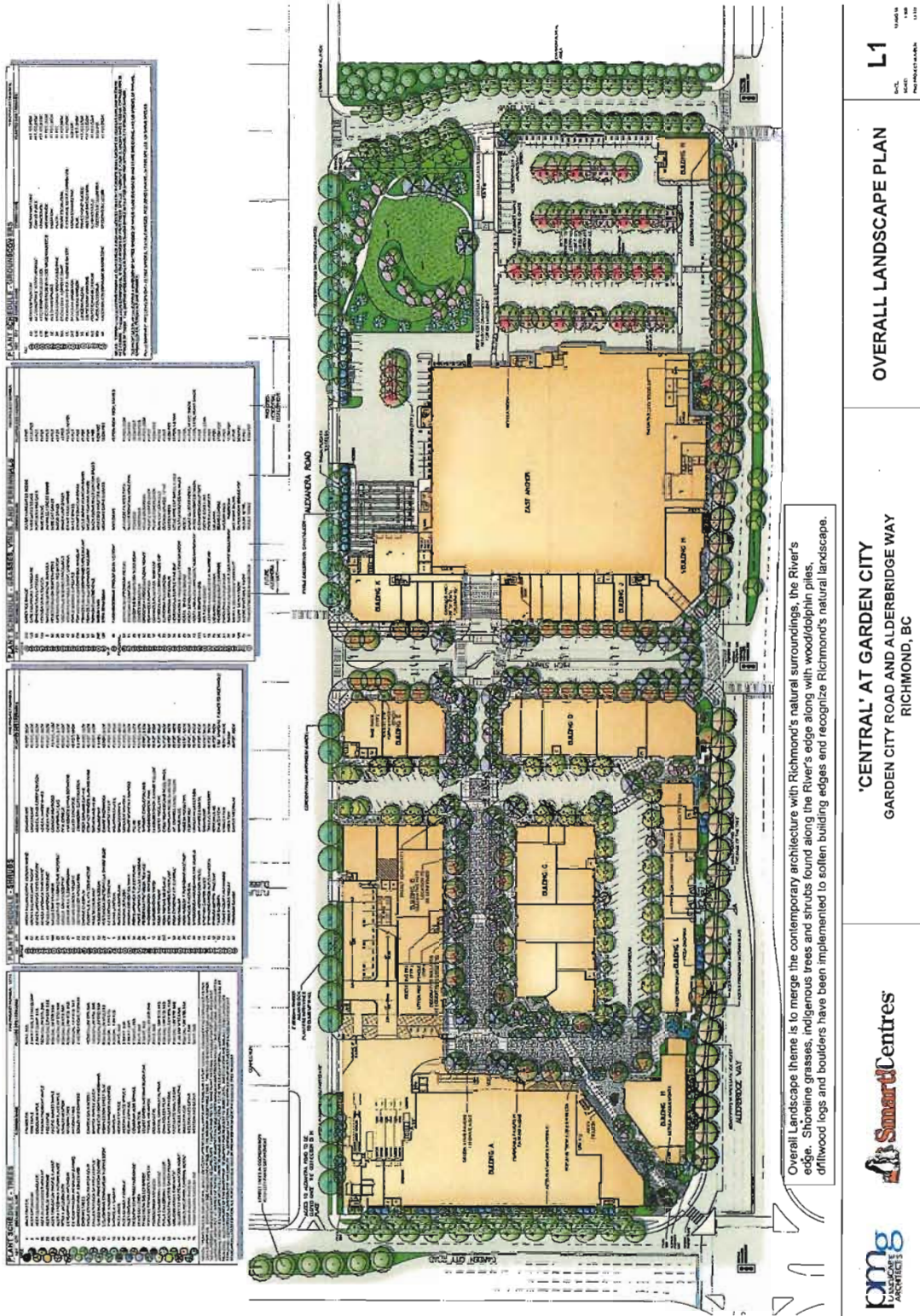
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Att. 10

- Attachment 1: SmartCentres Current Site Plan – September 26, 2013
- Attachment 2: SmartCentres Current Landscape Plan – September 26, 2013
- Attachment 3: Comparison Table of SmartCentres Proposals
- Attachment 4: SmartCentres Proposed Park & ESA Adjustments
- Attachment 5: SmartCentres ESA Assessment (Stantec Consultants Ltd.)
- Attachment 6: ESA Overlap with WCAP May Drive Alignment
- Attachment 7: SmartCentres Tree Survey & Arborist Report
- Attachment 8: SmartCentres Supplemental Vegetation Report
- Attachment 9: Public Correspondence (received since September 17, 2013)
- Attachment 10 Revised Rezoning Considerations – September 30, 2013

SmartCentres Current Site Plan Proposal (September 26, 2013)



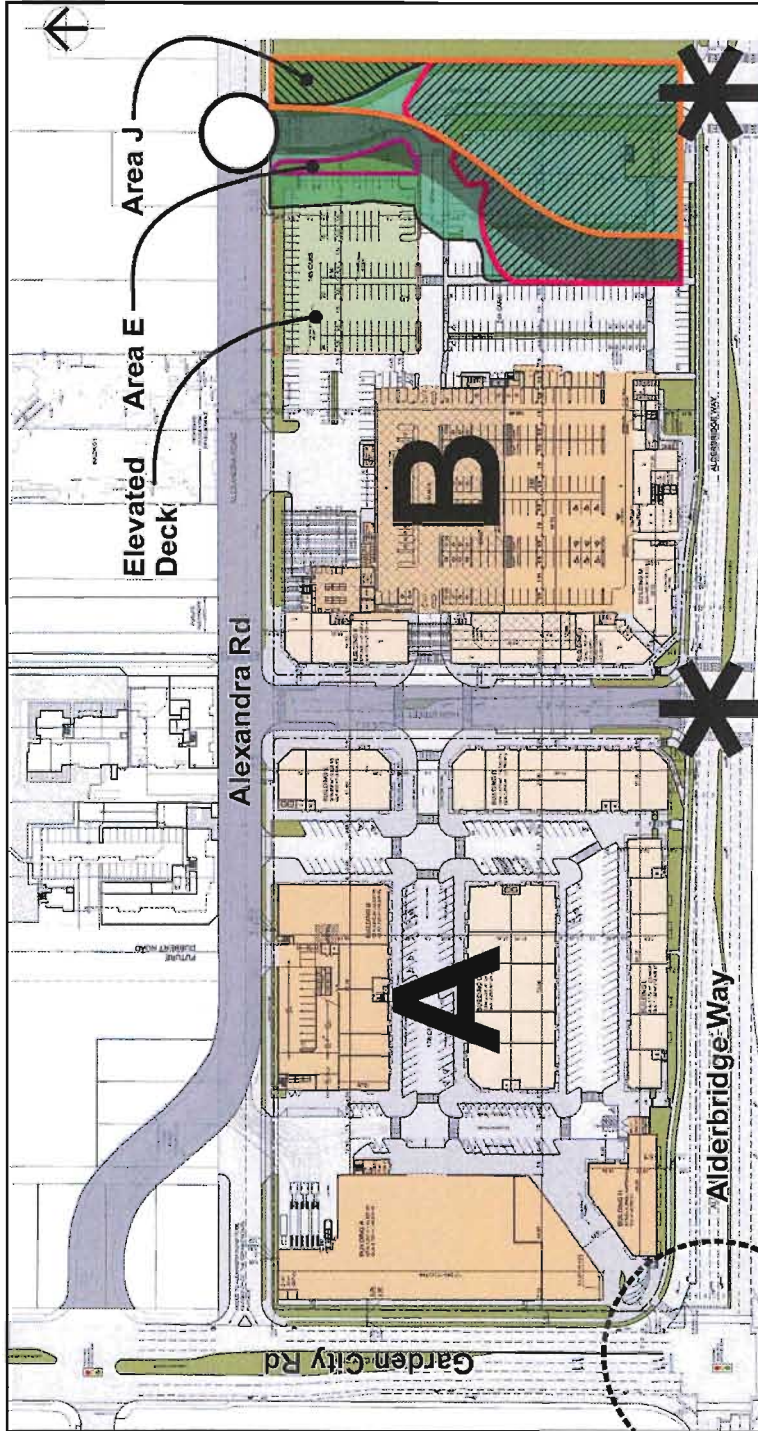
SmartCentres Current Landscape Plan Proposal (September 26, 2013)



Comparison of SmartCentres Proposals

Comparison Criteria	Previous SmartCentres Proposal (Aug. 29/13)		Current SmartCentres Proposal (Sept. 26/13)		Net Change
Gross Site Area	67,89094 m ² (730,772 ft ² or 16.77 acres)		same		no change
Dedications & SRW's (Road & Other)	9,286.03 m ² (99,954 ft ² or 2.29 acres)		9,879.59 m ² (106,343 ft ² or 2.44 acres)		Increased by +0.15 acres
Net Site Area	58,604.91 m ² (630,818 ft ² or 14.48 acres)		58,011.35 m ² (624,429 ft ² or 14.33 acres)		reduced by -0.15 acres
Gross Floor Area	36,017.77 m ² (387,692 ft ²)		same		no change
Gross Leasable Floor Area	34,574.98 m ² (372,162 ft ²)		same		no change
FAR (West versus East Side)	West Side 0.62	East Side 0.61	West Side 0.62	East Side 0.62	East Side +0.01 FAR
Proposed Total Parking	1,153 parking stalls		1,145 parking stalls		reduced by 8 stalls
Proposed "Area J"	653.94 m ² (7,039 ft ² or 0.16 acres)		1,280.11 m ² (13,779 ft ² or 0.32 acres)		increased by + 0.16 acres
Proposed "Area E"	343.93 m ² (3,702 ft ² or 0.085 acres)		334.92 m ² (3,605 ft ² or 0.083 acres)		Reduced by - 0.002 acres
Proposed 'Green Deck'	3,377.95 m ² (36,360 ft ² or 0.83 acres)		3,377.95 m ² (36,360 ft ² or 0.83 acres)		no change
Designated Park Area On-site	1.51 acres		same		no change
Proposed Park Deficit	0.44 acres (1.51 - 0.16 - 0.83 - 0.085 ac)		0.28 acres (1.51 - 0.32 - 0.83 - 0.083 ac)		improved by 0.16 acres less
Proposed Tree Removals	172 bylaw sized trees		same		same
Proposed Tree Planting	588		672 (556 onsite plus cash contribution for 116 off-site)		improved by 84 more trees
Proposed Shrub Planting	6,201		6,662 (6,201 onsite plus cash contribution for 461 off-site)		improved by 461 more shrubs
Tree Replacement Ratio	3.4 replacements x removals		3.9 replacements x removals		improved by 0.5 x tree removal
Designated ESA On-site	1.22 acres (2.57 - 1.0 - 0.35 ac)		same		no change
Proposed ESA Deficit	1.06 acres (1.22 - 0.16 ac)		0.9 acres (1.22 - 0.32 ac)		improved by 0.16 acres less
Park Habitat Enhancement	none		\$186,155		improved by \$186,155 more
Other Sustainability Features	compact development, 70% participation in district energy utility, 4 electric vehicle stalls & pre- ducting for 10% of stalls, 2 end-of- trip bike facilities, additional bike parking, 3 new/upgraded bus shelters, LEED Silver equivalent buildings, reduced stormwater, energy consumption & reduced light pollution		same		no change

SmartCentres Proposed Park & ESA Adjustments



July 25, 2012

- ESA & West Cambie Natural Park on proposed development site approx. 6,116.80 sq meters (1.51 ac)
- Existing ESA (Based on OCP) on proposed development site approx. 10,400 sq meters (2.57 ac)
- Proposed ESA (Based on detailed field investigation) approx. 6,354 sq meters (1.57 ac)
- Applicant proposed Area J Land Transfer approx. 654 sq meters (0.16 ac)
- Applicant proposed Elevated Deck as PROP-SRW approx. 3,378 sq meters (0.83 ac)
- Application proposed Area E as PROP-SRW approx. 344 sq meters (0.09 ac)



SmartCentres ESA Assessment



Stantec

Stantec Consulting Ltd.
4370 Dominion Street, 5th Floor
Burnaby, BC V5G 4L7
Tel: (604) 436-3014
Fax: (604) 436-3752

VIA EMAIL

March 4, 2013

Project No: 1231-10550

First Richmond North Shopping Centres Limited
#201 – 11120 Horseshoe Way
Richmond, BC V7A 5H7

Attention: Alan Lee

Dear Alan:

Reference: ESA Update for Rezoning Application at 9440, 9480, and 9500 Alexandra Road, Richmond, BC

1 INTRODUCTION

First Richmond North Shopping Centres Limited is submitting a rezoning application to develop a portion of the quarter section 34-5-6 adjacent to Alderbridge Way and Garden City Road in Richmond, British Columbia. A portion of this site is designated as an Environmentally Sensitive Area (ESA) by the City of Richmond (the City).

Stantec Consulting Ltd (operating as Jacques Whitford) conducted a preliminary habitat assessment in 2002 and a desktop review update in 2005 to evaluate the ecological function of the ESA within the proposed development area. As part of the City's requirements for the rezoning application, an additional report was prepared by Stantec in 2010 which summarized the existing habitat and highlighted any changes that had occurred since the previous assessment. The 2010 report also provided an assessment of the existing and potential ecological services provided by the ESA, the implications of removal or relocation of a portion of it, and recommendations for compensation and/or mitigation.

In November 2012, the City adopted their new 2041 Official Community Plan (OCP) Bylaw 9000. A draft technical report included with the 2041 OCP was the 2012 Environmentally Sensitive Area Management Strategy. This strategy expanded the existing ESA outlined in the City's previous OCP to encompass more than half of 9440 Alexandra Road and all of 9480 and 9500 Alexandra Road and was based on 1:4,000 high level orthophoto interpretation. Appendix C and Part 4 of the 2012 Environmentally Sensitive Area Management Strategy suggests that all applicants for development permits involving ESAs should conduct a vegetation survey of the sites to confirm the appropriate ESA boundary.

One Team. Infinite Solutions.

Stantec

First Richmond North Shopping Centres Limited
 Attention: Alan Lee
 Project No: 1231-10550

March 4, 2013
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Reference: **ESA Update for Rezoning Application at 9440, 9480, and 9500 Alexandra Road, Richmond, BC**

This report provides a detailed survey of the vegetation types on the site and makes recommendations for areas that should be included in the ESA.

2 METHODS

Preliminary vegetation polygons were created from orthophoto interpretation and were then ground truthed during the site visit. The three properties were surveyed on January 29, 2013 by two Stantec biologists to determine what the vegetation types were on the site. Dominant understory species within each polygon were recorded and photographs were taken at various locations on the site. Cover was estimated for the dominant species observed. Tree species were recorded during an existing tree assessment (MJM Consulting 2012). There were four different vegetation polygons identified on the site (Figure 1, Appendix A) and descriptions of these are provided below. None of the ecological communities observed on the site are considered at-risk within the province.

3 RESULTS

3.1 Polygon 1—Birch Forest

The southern half of the properties consists of an approximately 1.6 acre upland forest dominated by paper birch (*Betula papyrifera*) with an open understory of grasses and rushes (Photo 1, Appendix A). There are minor amounts of invasive Himalayan blackberry (*Rubus armeniacus*). Table 1 below provides a list of the dominant species within this polygon.

Table 1: Dominant Understory Species within Polygon 1

Common Name	Scientific Name	% Cover
reed canarygrass	<i>Phalaris arundinacea</i>	15
hardhack	<i>Spiraea douglasii</i>	15
common rush	<i>Juncus effusus</i>	15
Himalayan blackberry*	<i>Rubus armeniacus</i>	10
bentgrass	<i>Agrostis sp.</i>	5
bracken fern	<i>Pteridium aquilinum</i>	2

NOTE:

* Invasive species

One Team. Infinite Solutions.

Stantec

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Project No: 1231-10550

March 4, 2013
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Reference: ESA Update for Rezoning Application at 9440, 9480, and 9500 Alexandra Road, Richmond, BC

6 REFERENCES

BC Ministry of Agriculture. 2011. Field Guide to Noxious and Other Selected Weeds of British Columbia. Accessed (February 2013): <http://www.agf.gov.bc.ca/cropprot/iknotweed.htm>

Michael J. Mills Consulting 2012 Existing Tree Assessment Summary Plan MJM File #923
February 16, 2012

One Team. Infinite Solutions.

SmartCentres
Attention: Alan Lee

Appendix A: Photographs



Photo 3 Looking southwest into Polygon 3 at 9440 Alexandra Road.



Photo 4: Looking southeast into Polygon 3 at 9480 Alexandra Road.

SmartCentres
Attention: Alan Lee

Appendix A: Photographs



Photo 7: Looking south into Polygon 3 at 9500 Alexandra Road

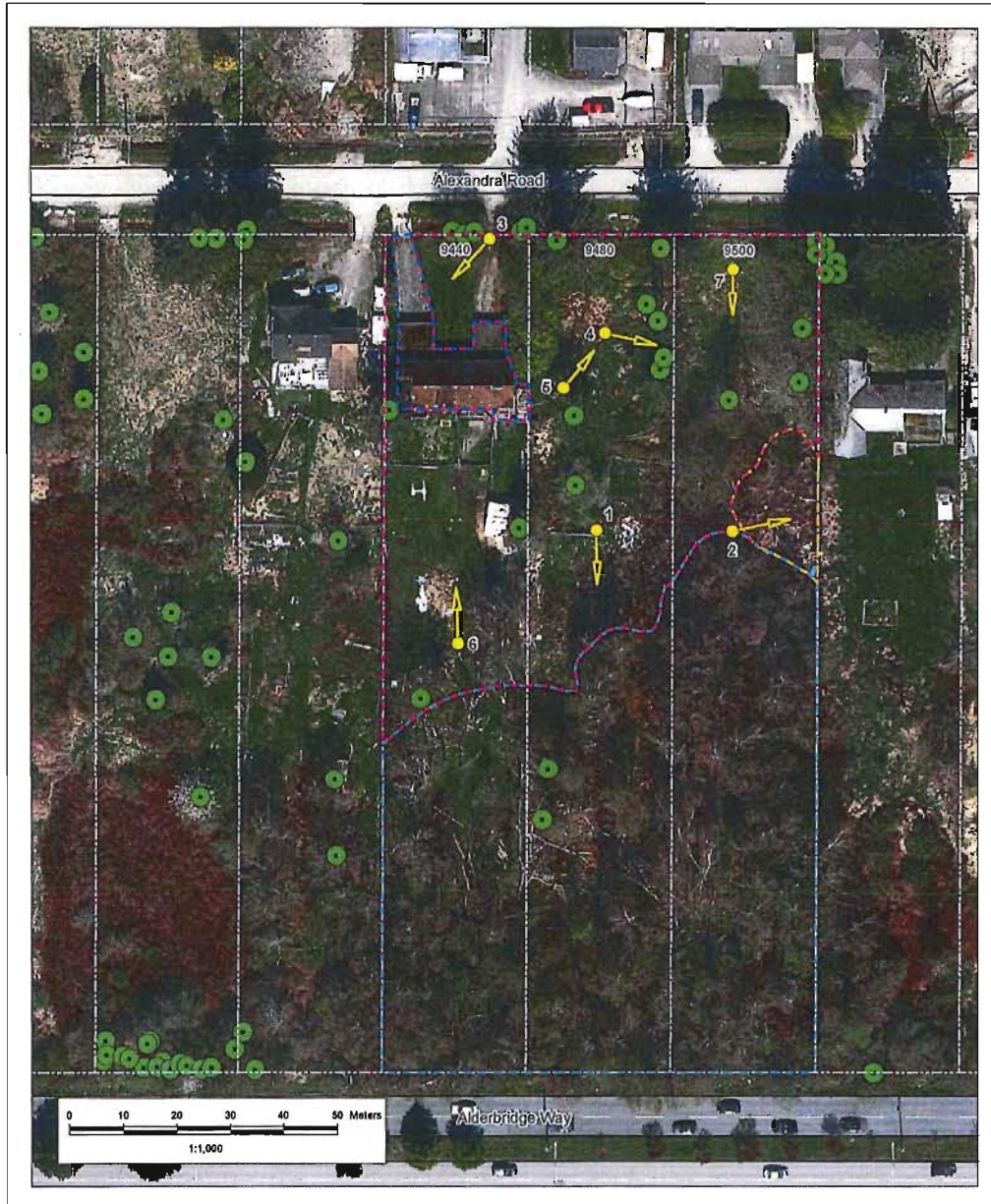


APPENDIX B

Site Plan

One Team. Infinite Solutions.

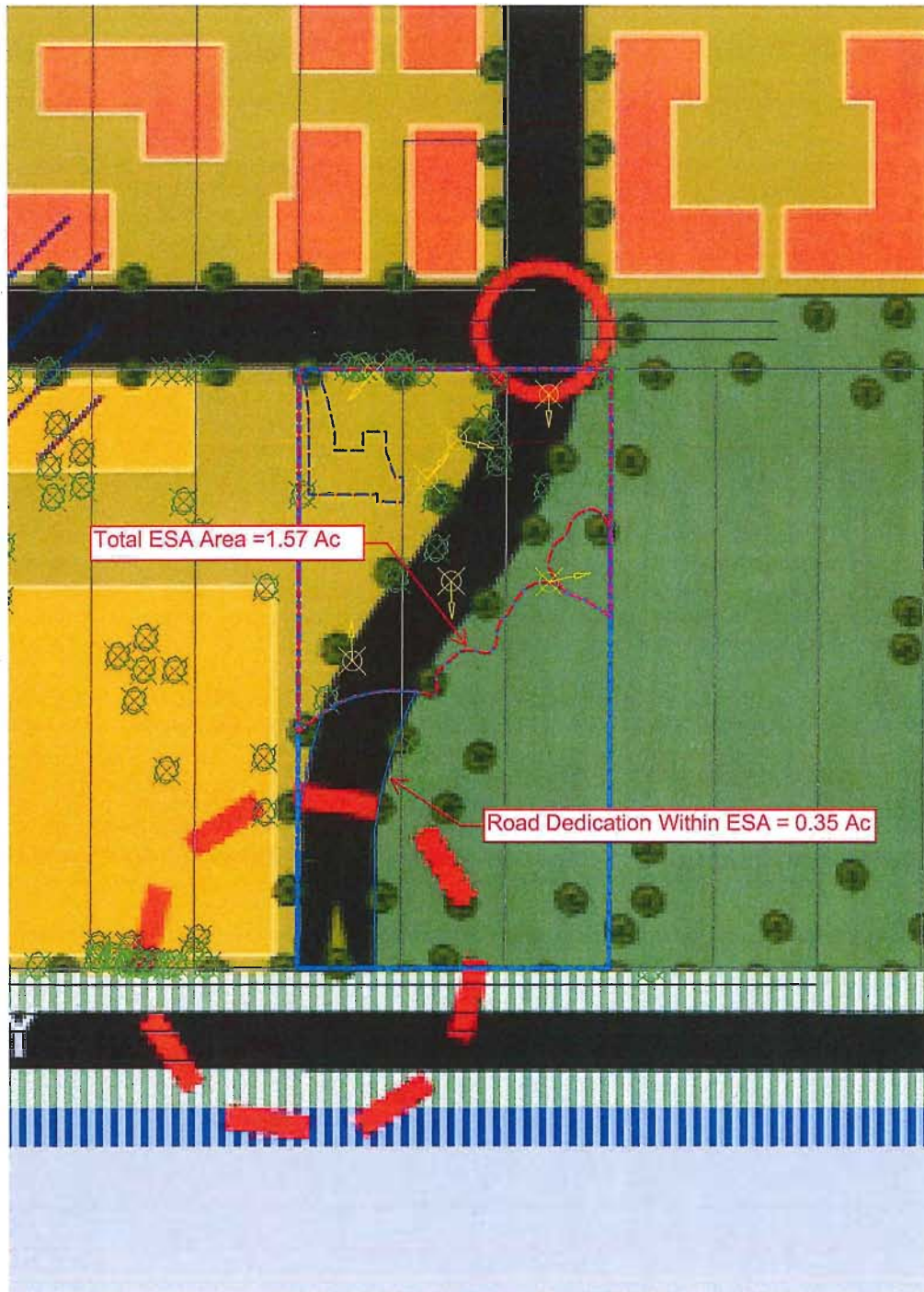




Legend - Cadastral - Sidewalk ● Photo Location ● City of Richmond By-Law 8057 Sized Trees ■ Polygon 1 - Birch Forest ■ Polygon 2 - Japanese Knotweed ■ Polygon 3 - Anthropogenically Disturbed ■ Polygon 4 - Impervious Surface	ESA Update 9440, 9480, 9500 Alexandra Road Richmond, BC SITE PLAN		PREPARED BY:
	<small>Sources: Base CAD drawing provided by Michan J. Keffs Consulting. Title: Existing Tree Assessment Summary Plan, MJM No #923, Dated: Feb 16, 2012</small> <small>Although there is no reason to believe that there are any errors associated with the data used to generate this product or in the production, users of these data are advised that errors in the data may be present.</small>		PREPARED FOR: SmartCentres
DATE: 21-FEB-13 FIGURE ID: 123110550 DRAWN BY: G. HUYNH	PROJECTION: UTM - ZONE 10 DATUM: NAD 83 CHECKED BY: T. ANDERSON	FIGURE NO: <div style="text-align: center; font-size: 24pt; font-weight: bold;">1</div>	

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May Drive & ESA Overlap



SmartCentres Tree Survey & Arborist Report

Michael J Mills Consulting
Arboriculture and Urban Forestry

**Arboricultural Assessment Report
Proposed Commercial Development
Alexander Road & Garden city Way
City of Richmond**

For Smart Centres
201 – 11120 Horseshoe Way Richmond

MJM File #923

February 18th 2010

1.0 Introduction:

Site visit was requested to review the condition and preservation potential of the existing trees located on the above noted Commercial property in the City of Richmond. We understand the proposal will involve a mixed commercial development with multi tenant retail buildings with surface parking.

We were requested to provide opinion with respect to the relative quality of the existing trees and to make recommendation for preservation in context with this proposal. Tree survey information was provided by Murray and Associates and Wedler Engineering. Development information was provided to us by Smart Centres. We have visited the site on several occasions with details assessment of the trees completed on February 1st, 2010. A modified version of the tree survey plan is appended to this report. Refer to this plan for all tree numbers referenced in this report.

2.0 Observation:

The property is bordered by Alexandra Road to the north, Alderbridge Way to the south and Garden City way to the west. The site area is comprised of 20 existing legal lots of varying size. The majority of these lots have had the pre existing homes removed over the past few years with only four houses remaining.



Aerial Photos of the subject area, from City of Richmond GIS web site, May 2007.

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The site is relatively flat and level with no distinct grade changes. No watercourses other than the open ditches along Alexandra Road were observed. The majority of the site area is open with disturbed conditions that according to the Jacques Whitford Stage 1 PSI of March 2003 are associated with residential uses dating back to the 1930s. Large open areas of un maintained grass are located through the central portions of the site. Dense areas of blackberry are located along much of the southern edges and sporadically throughout the site. A large number of fruit trees are located throughout the site that have not been maintained in recent years resulting in generally poor form and structure. There are several areas where a predominantly pure stand of Paper Birch has established. Within these areas, the Birch are generally of a uniform size and character and as such not all of these trees have been individually identified on the survey plan. There are presently no formal street trees located along any of the surrounding roadways.

The site contains two trees that have been identified on the old (1988) city list of significant trees, a Linden tree (#30) at 9260 Alexandra and a Douglas Fir (#11) at 9400 Alexandra. We also identified an interesting large old Black Walnut (#104) toward the south edge of the site, this tree was up rooted and although it has laid on its side for many years it continues to grow.

The most common tree species on the site is Paper Birch. Individual specimens are found throughout the site with a rather dense stand of trees occurring in the south east corner of the site and along much of the Alderbridge Way frontage. Most of the Birch within these stands are infested with Bronze Birch Borer and many are in advanced stages of decline. There are also several areas where cluster of the native Black Locust have established.

We comment that tree instability appears to be an issue in many areas. Trees have failed and uprooted in a number of areas and it was evident by the leaning nature of many trees that recent wind storms have affected the trees to some extent. The fact that the trees are leaning in many different directions would suggest that wet soils are also a contributing factor in the propensity for leaning.

We offer the brief comment with respect to the character of the existing trees associated with this application. Refer to the appended modified survey plan for the location of all trees referenced. The appended survey plan has been divided into 6 sections for the purpose of plan legibility.

Note: Commentary has not been provided for each individual Birch tree and several of the low value trees of other species. Trees were viewed during winter conditions making species identification and assessment of tree health of the deciduous tree species more difficult.

Tree Ref #	Species	Size	Comment
Trees within plan section #1			
1	Mixed Cluster	55cm -- 90cm	Cluster of trees located in the extreme north east corner of the site. Comprised of 2 Western Red Cedar, 2 Douglas Fir, 1 Maple and 1 Black Locust. All in relative good condition. Locust is a multi stem specimen. Must be considered as a single tree due to the crowded one sided form.
2	Black Locust	60cm	Part of a cluster of small Locust along the property line of the existing house, tall and thin form. Good health with a tendency for leaning out toward the light.
3	Black Locust	35cm	Part of a cluster of small Locust along the property line of the existing house, tall and thin form. Good health with a tendency for leaning out toward the light.
4	Black Locust	multi	Open grown tree with multi stem form. Broken form resulting in limited landscape value.

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5	Douglas Fir	60cm	Small Fir with broken wind swept form. Broken top. Metal brackets and nails embedded in the lower trunk.
6	Douglas Fir	40cm	Small Fir with broken form. Crowded by adjacent trees, poor condition.
7	Douglas Fir	75cm	Open grown form. Tall and thin with limited live canopy ration. Good health.
8	Douglas Fir	90cm	Fir located adjacent to ditch along Alexandra Road. Lean toward the north east. Some structural defects but overall in good condition.
9	Sitka Spruce	100cm	Large spruce growing close to ditch adjacent to Alexandra Road. Open grown, somewhat sparse canopy but overall in good condition.
10	Douglas Fir	55cm	Growing quite close to an existing house. Broken top form but overall in good health.
11	Large Douglas Fir in mixed group	50cm -- 110cm	Cluster of trees close to Alexandra Road, dominated by the large Fir that was listed on the significant tree list of 1988. Large secondary limb sweeping out from side. Large root visible along the edge of the ditch. Lots of dead wood in the upper canopy and somewhat one sided form due to shading from adjacent trees. Also within this group there are two Firs and a Cedar with co dominant main stem form from the ground. One of the smaller Firs is growing out of the side of the ditch. All trees within the group are in good health but maintain somewhat one sided form due to crowding. Good as a group.
12	Apple	80cm	Big old fruit tree. Extensive main stem decay, tree is in the process of splitting into two parts.
13	Austrian Pine	50cm	Small tree that was added to the landscape of this property. Multi stem with short bushy form. Good health.
14	Cherry	50cm	Old fruiting cherry, part of an old orchard area. Not well maintained. Poor condition.
15	English Oak	30cm	Nice young tree with good form and structure. Clothes line was attached to the lower trunk and has girdled the stem.
Trees within plan section #2			
16	Shore Pine	35cm	Small Pine with scrubby form typical for the species, poor form and structure. Growing close to Alexandra Road. Good health.
17	Balsam Fir	35cm	Small ornamental Fir with tall thin form. Good form, attractive small tree.
18	Weeping Willow	100cm	Big old tree with lots of dead wood and main stem decay. Extensive pruning has left blunt end with profuse suckering. Poor condition.
19	Western Red Cedar	45cm	Small tree with a notable lean from vertical toward the north.
20	Cluster of Maple	varies	Multiple specimens of small Maples (No foliage for identification but estimated to be Red Maple cultivar). Most of the trees in the cluster have multi stem form, low individual value.
21	Western Red Cedar	varies	Hedge row of Cedar along Alexandra Road frontage. Bushy form. Good health but of limited landscape value.

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22	Linden (Tilia)	70cm	Large Linden growing close to Alexandra Road. Growing beside the open ditch. Roots exposed along the edge of the ditch. Good condition with good upright form. Some inclusion between main stems. Sucker shoots from the base of the trunk. Considered to be one of the higher value landscape trees within the site.
23	Maple sp.	50cm	Small Maple with co dominant main stem form. Good health, poor structure.
24	Western Red Cedar	60cm	Large Cedar growing close to Alexandra Road. Growing beside the open ditch. Tree was topped and has developed multiple leaders. Good health.
25	Mixed Hedge Row	varies	Mixed hedge row comprised of Hazelnut, Cherry and Cedars. Installed as a landscape barrier along pre existing property lines. Hazelnuts have typical multi stem form. Cedars and Cherry are over crowded. Poor condition.
26	Pear	40cm & 20cm	Two small Pear trees growing close together. Not well maintained, poor condition.
27	Cherry	30cm	Small fruiting Cherry, poorly maintained, poor condition.
28	Incense Cedar	Multi	Cedar with broken form, appears to have been damaged by a fire?
29	Incense Cedar	Multi	Cedar with broken form, appears to have been damaged by a fire?
30	Linden (Tilia)	100cm	Large open grown Linden. Listed on the City list of significant trees. Good open grown form. Some inclusion between the main stems but overall in good health.
31	Cherry	20cm	One of three small Cherries growing together, overgrown in blackberry. Poor condition.
32	Cherry	25cm	One of three small Cherries growing together, overgrown in blackberry. Poor condition.
33	Cherry	30cm	One of three small Cherries growing together, overgrown in blackberry. Poor condition.
34	Cherry	40cm	Open grown fruiting cherry, overgrown by blackberry, very poor condition.
35	No tree		
36	Maple sp.	30cm	Small ornamental Maple with co dominant main stem form, good health, less than specimen form.
37	Western Red Cedar	40cm	Topped form, only a remnant of the original tree is left.
38	Pear	30cm	Older tree that has not been well maintained.
39	No tree		
40	Blue Spruce	15cm	Small tree with tall and thin form, good health.
41	Western Red Cedar	45cm	Small tree infested with blackberry, notable lean toward the north.
42	Maple sp.	40cm	Ornamental Maple engulfed in blackberry, notable lean toward the south.
43	Blue Spruce	30cm	Small tree with tall and thin form, good health. Infested with blackberry.
44	Mountain Ash	60cm	Small tree with good upright open grown form.
45	Western Red Cedar	40cm	Multi stem form, part of a short hedge row. Low value.
46	Norway Spruce	25cm	Small tree engulfed in blackberry, poor form.
47	Pear	30cm	Small tree, poor quality.
48	Hazelnut	multi	Multi stem shrubby form, good health.
49	Pin Cherry	40cm	Small native cherry growing in blackberry.

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50	Hazelnut	multi	Multi stem shrubby form, engulfed in blackberry, poor health.
51	Cottonwood	55cm	Young tree with open grown form, good health.
52	Sweetgum	40cm	Ornamental tree growing as part of a row of trees along a pre existing property line. Co dominant main stem form, twisted form.
53	Western Red Cedar	40cm	Small tree crowded out by adjacent Birch. Poor condition.
Trees within plan section #3			
54	Purple Leaf Plum	65cm	Large old flowering plum growing close to cedar hedge row along Alexandra. Extensive sucker shoots. Good condition but somewhat one sided due to crowing.
55	No tree		
56	Norway Maple	30cm	Small tree beside ditch along edge of Alexandra. Co dominant main stem form, good health.
57	Purple Leaf Plum	35cm	Small flowering plum in good condition.
58	Horse Chestnut	100cm	Large multi stem chestnut. Some main stem decay and past pruning damage but otherwise in good health.
59	Cherry	25cm	Small fruit cherry, poor condition.
60	Serbian Spruce	30cm	Typical tall and thin form, co dominant main stem form, good health.
61	Hazelnut	multi	Typical shrubby form for hazelnut, good condition.
62	Mountain Ash	60cm	Co dominant main stems with notable inclusion between, twisted form, good health.
63	Mountain Ash	60cm	Co dominant main stems, tree is in decline, poor condition.
64	Western Hemlock		Part of a hedge row of trees along the edge of Alexandra. Co dominant main stems, poor health, in decline.
65	Western Hemlock		Part of a hedge row of trees along the edge of Alexandra. Top broken off, almost dead.
66	English Holly	cluster	Cluster of multi stem Holly. Crowded and bushy form, good health.
67	Austrian Pine	35cm	Open grown Pinc. Good condition.
68	Cedar hedge row	multi	Hedge row comprised of five surveyed size trees with multi stem form. Considered to be of limited landscape value due to crowded form.
69	English Oak	45cm	Tall and thin form, one sided and leaning toward the south, relative good health.
70	Purple Leaf Plum	60cm	Old flowering Plum, extensive decay in the main stem, engulfed in blackberry poor condition.
71	Western Red Cedar	70cm	Crowded among cluster of smaller Birch. Crowded form but ok health.
72	Shore Pine	25cm	Small Pine in the far north west corner of the site close to Alexandra. Small tree with contorted form, good health.
73	Lombardy Poplar	varies	Cluster of Lombardy Poplar close to Garden City Way. Tall thin form typical of the species. All trees in the line along the edge of the row have been topped at a low height for overhead wire clearance. Generally in good health. One Douglas Fir within this group that has a notable lean away from the Poplars.

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74	Purple Leaf Plum	30cm	Part of a short hedge row of vegetation comprised of Plum, Hemlock and Cedar, poor condition due to crowded form.
75	Western Red Cedar	40cm	Part of a short hedge row of vegetation comprised of Plum, Hemlock and Cedar, poor condition due to crowded form.
Trees within plan section #4			
76	Douglas Fir	80cm	Large tree with open grown form, some wind damage. Notable lean toward the south east, large surface roots in lawn area. Good condition.
77	Western Red Cedar Hedge row	varies	Hedge row of Cedar along the pre existing property line. Intermixed with Birch. Little individual value due to crowded form.
78	Hedge Row	varics	Mixed hedge row of trees planted along the rear property line of the pre existing lot. Comprised primarily of Cedar and Fir but with specimens of Lawson Cypress, Moss Cypress, Birch and Apple mixed in. No trees within this row are considered to be of high landscape value.
79	Douglas Fir	35cm	Small Fir, good health, leaning form.
80	English Holly	45cm	Multi stem form, crowded by nearby trees, poor condition.
81	Incense Cedar	multi	Located along edge of Garden City, co dominant main stem form, damage to the trunk. Good health.
82	Purple Leaf Plum	45cm	Poor condition, infested with English Ivy. Pruned for overhead wires.
83	Lawson Cypress	60cm	Cypress located close to Garden City, topped off at low height for overhead wire clearance.
84	Sawara Cypress	60cm	Poor condition due to shaded site conditions.
85	Japanese Maple	multi	Good size specimen but with broken branching and other damage resulting in limited landscape value.
86	Linden	50cm	Poor condition due to shaded site conditions.
87	Western Red Cedar	multi	Tree with multiple stem form from the ground. Ivy infestation.
88	Weeping Willow	70cm	Large old tree, dominant tree in this corner of the site. Tree has been pruned back in the past but remains in good condition.
89	Purple Leaf Plum	multi	Large multi stem tree located close to Garden City. Poor condition.
90	Lawson Cypress	multi	Smaller multi stem tree with poor form due to crowding.
91	Black Locust	multi	One of a series of multi stemmed Robinia in this area, leaning out toward the open light areas, good health, poor form.
92	London Plane	multi	Comprised of 10 stems all topped at low height for overhead wire clearance. Low value.
93	Norway Spruce	25cm	Small tree, part of a group of trees in this corner of the site, crowded form, low value.
94	Lebanese Cedar	65cm	Good upright form, somewhat crowded by large Willow (#88). Top was damaged in the past but the tree has recovered an upright crooked form. Good health.
95	Hazelnut	multi	Large cluster of hazelnut stems at intersection of Alderbridge Way and Garden City. Lots of dead wood, good health.

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96	Norway Maple	2 x 50cm	Co dominant main stems, good open canopy form, located close to Alderbridge way, good health
97	Cluster of Locust		Dense stand of small multi stem Black Locust. All leaning out toward open edges looking for light. Good health, poor form. Several small Cedars within the cluster of trees, crowded and shaded out by larger locust.
98	Purple Leaf Plum	45cm	Crowded in amongst the locust. Co dominant main stem form. Poor condition.
99	Cluster of mix trees	varies	Cluster of trees along the edge of Alderbridge Way comprised of Norway Maple, Birch and Hazelnut. No high value trees.
Trees within plan section #5			
100	Norway Maple	40cm	Located close to Alderbridge Way, good form and health, leaning toward the north.
101	Cherry	35cm	Old Cherry located close to Alderbridge Way, tree has been impacted by a car in the past. Not considered a high value tree.
102	Apple	50cm	Un maintained form, engulfed in blackberry. Good health.
103	Black Walnut	150cm	Large old tree, tree fell over many years but continue to grow while lying over on its side. Canopy has provided for homeless shelter in the past. Small tree fort in the canopy. Interesting tree and uncommon at this size.
104	Black Walnut	25cm	Smaller Walnut, likely a seedling from tree 103. Good condition.
105	Linden (Tilia)	50cm	Good open grown form, young tree, likely a seedling from tree #30.
106	Cherry	40cm	Small tree, poor condition.
107	Cherry	20cm	Small tree, poor condition
108	Black Walnut	40cm	Leaning over, broken limbs, poor form, good health.
109	Western Red Cedar	45cm	Small tree with open grown form, good health.
110	Cherry	30cm	Fully engulfed in blackberry, poor condition.
111	Western Red Cedar	60cm	Co dominant main stem form, good health, open grown form, fully branched.
Trees within plan section #6			
112	Apple	45cm	Old tree, not maintained, poor form and health.
113	Western Red Cedar	40cm	Short and bushy form, good health.
114	Pin Cherry	30cm	Native Cherry growing on the edge of the Birch stand, good condition.
115	Pear	60cm	Old tree, not maintained, poor form and health.

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Photos:



Overview of the site looking north from the open field to the south of the site.



View of tree #22, Linden tree beside Alexandra Road.

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Detail showing the relationship between tree #22 and the open ditch along the edge of Alexandra Road.

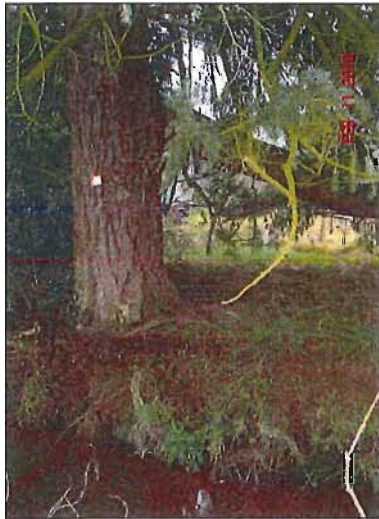


View showing the large Firs and Cedars within group #11

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Detail showing the large sweeping limb off the side of the large Fir and the relationship between the tree and the ditch along Alexandra Road

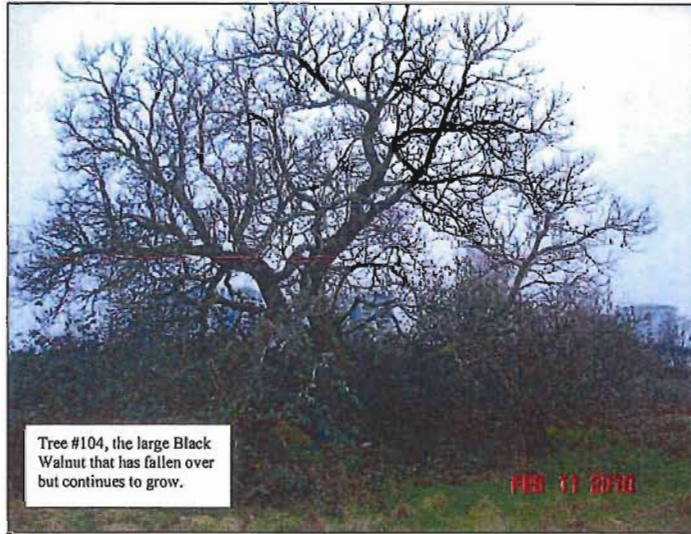


View showing the open pasture conditions through the centre of the site and the stand of small Birch in the south east corner.

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3.0 Recommendation:

We have been provided with a conceptual plan for the site that shows a large retail building and open parking across the eastern half of the site and a mix of smaller retail buildings and parking in the west. A new north south road (High Street) will be developed to separate the two portions of the site. The ultimate concept calls for Alexandra Road to curve to the north west of High Street. As part of the current application, Alexandra Road will continue along the existing alignment in a straight line across to Garden City Way. The existing interim road alignment has tree retention implications.

The limiting factor in the determination of tree preservation within this site is the requirement to increase grades over the full site area and along Alexandra Road to meet flood level standards. The site must be raised from the current levels of .9 – 1.2m above sea level to 2.6m finished floor elevations with the centre line of Alexandra Road ranging from 2.0 to 2.6 metres. The development of this site will also require the removal of the soft organic surface soil layers and the preloading of the area to meet geotechnical requirements.

Prior to completing our detailed assessment of the tree resources, we met on site with the City of Richmond Tree Preservation Coordinator, Gordon Jaggs. The limiting factors in retaining trees on this site was discussed and it was generally agreed that there were three trees of particular interest to the city on the site, the large Linden in the middle of the site (#30), the somewhat smaller Linden along Alexandra Road (#22) and the large Fir and other associated conifers along the edge of Alexander Road (#11). It was agreed that we would review these trees in more detail to determine if preservation might be possible.

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After further study it was determined that the retention of the large Linden in the centre of the site would not be feasible due to the increase in grades and the fact that the tree is positioned in an area which would significantly impact locating buildings along High Street and creating the feel and functionality central to that from the Area Plan. The only realistic opportunity was thought to be along the edge of Alexandra Road. We have been in discussion with Aplin & Martin Consulting, the civil engineer to review the possibility of retaining trees in this area. We requested that sections be provided to help understand how the grades might be adjusted to suit the trees (sections are appended).

The situation adjacent to the large Fir (#11) is unrealistic. The grades and the sidewalk result in a situation where we can find no means to retain this group of trees. Even if the sidewalk were to be moved to the back of curb for physical clearance, the relationship between these trees and the open ditch that would be filled does not result in a situation where we believe the trees would have a reasonable chance of success.

The situation around the second Linden (#22) is somewhat better. The tree is set back further from the road and the grading is not as severe in this location as the road will only be raised to 1.41 metres. Even so, the retention of the tree would require relaxation of the City of Richmond's engineering design standards. Given the existing temporary condition of the Alexandra Road alignment, it is hoped that the City would accept a slightly modified condition where the north gutter line is shifted north, the existing Alexandra Road grades in the vicinity of the Linden tree are maintained with a slightly steeper centreline profile, sidewalk would be pulled back to the back of curb and a retaining wall (with railing) installed to provide as much clearance from the tree (5.3 metres) as possible. It would also be necessary for the city to agree to a catch basin being installed to drain excess water from around the tree into the new storm line proposed for the north side of Alexandra Road. The layout and drainage is demonstrated in the appended sketch from Aplin & Martin. It is important to note that the conditions surrounding the tree could be improved in the future when Alexandra Road is constructed to the ultimate design and adjusted to sweep away from the tree toward the north.

In addition to cooperation from the city, a portion of the site to the south and west of the tree will need to be designed suitably to optimize site conditions for the preservation of the Linden tree. After the site and the roadways have been raised to design grades, the retained Linden tree will, in effect, be left in a shallow depression in the site. In discussion with the landscape architect, it has been envisioned that the area surrounding the tree could be treated as a landscape amenity area with a pedestrian connection made to encourage public use of the space.

The preservation of the Linden tree will not be easy to accomplish and would require careful attention to preservation details and tree protection through all phases of development to ensure success. If the decision is made to preserve the tree, detailing of the on site tree preservation measures will need to be coordinated by all consultants. Minor pruning of the tree to improve form and structure would be recommended. If the City agrees to make adjustment to the engineering of Alexandra Road, we will work to ensure that the on site design is adjusted to ensure the best opportunity for the retention of this tree.

Refer to the appended Aplin & Martin sketch sections for additional information: Tree #11 - (1200fir.pdf) Tree #22 - (Linden tree drainage.pdf)

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4.0 Limitations

We attach the following clauses to this document to ensure you are fully aware of what is technically and professionally realistic in the assessment and preservation of trees.

This Arboricultural field review report is based only on site observations on the date noted. Effort has been made to ensure that the opinions expressed are a reasonable and accurate representation of the condition of all trees reviewed, however, conditions influencing the opinion and recommendation as provided in this report can change quickly and without warning. Any trees retained should be reviewed on a regular basis to ensure reasonable safety and to minimize the associated risk.

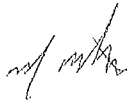
The assessment was completed based on visual review only. None of the trees were dissected, cored, probed or climbed. All trees or groups of trees have the potential to fail. No guarantees are offered or implied by Michael J Mills Consulting or their employees that the trees are safe given all conditions. Trees can be managed, but they cannot be controlled. To live work or play near trees is to accept some degree of risk. The only way to eliminate all risk associated with trees is to eliminate all trees.

The assessment provided was based on preliminary information only. No detailed information with respect to the final form of the development, site grading or the location of the site servicing was provided.

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Please contact the undersigned if you have any questions or concerns regarding this matter.

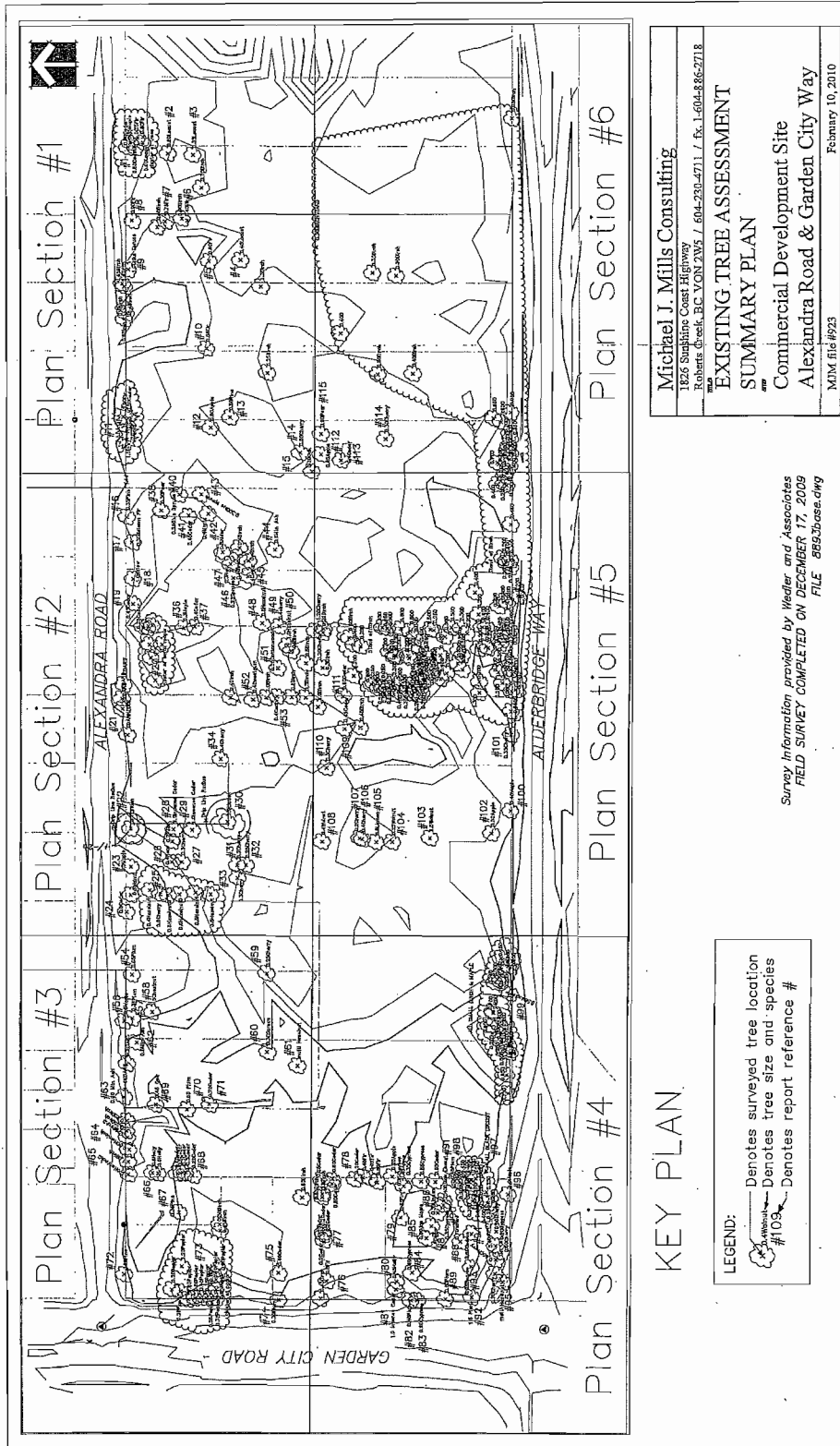
Yours Truly,



Michael J Mills
ISA Certified Arborist PN #0392
Certified Tree Risk Assessor #187

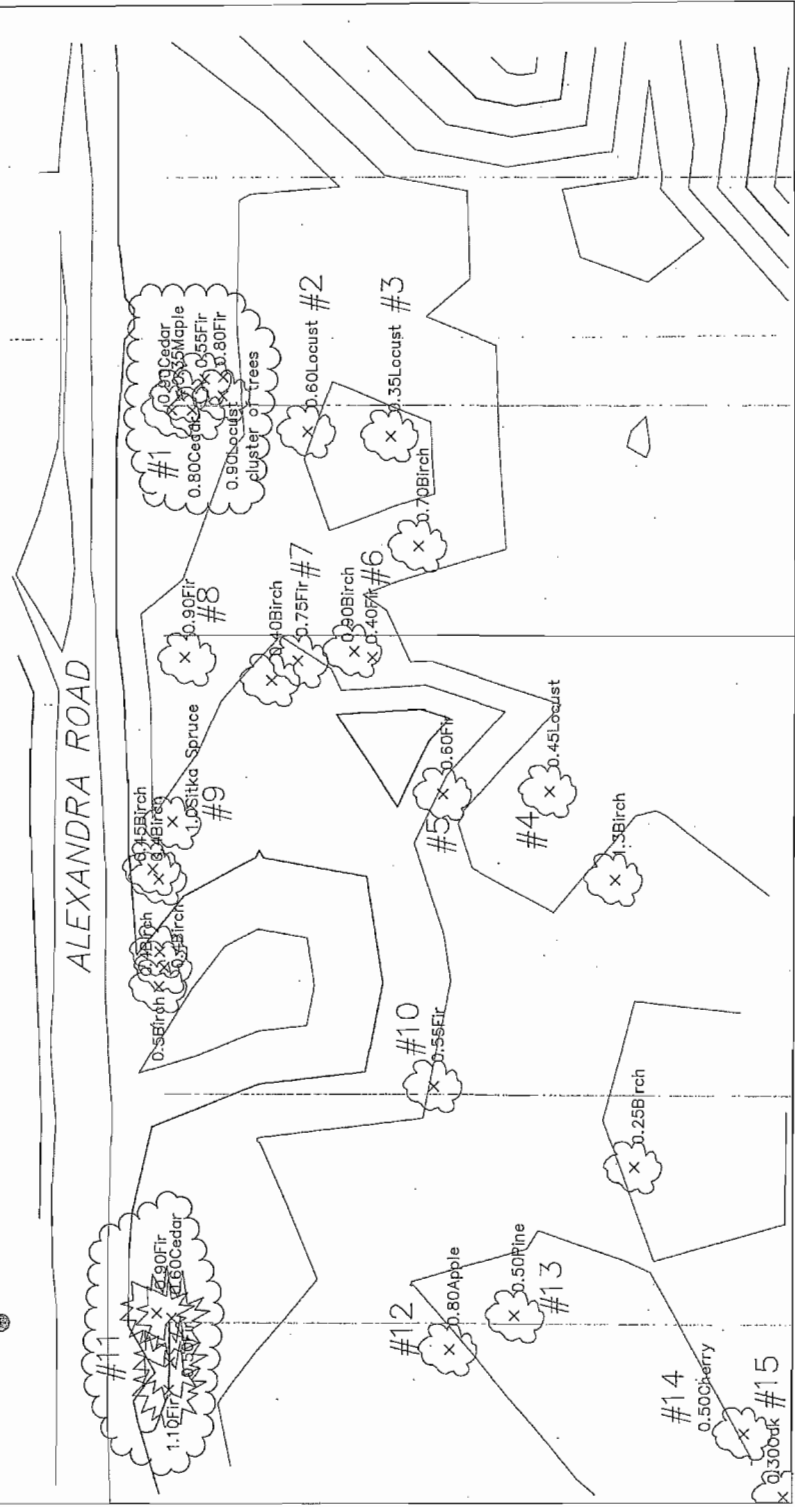
Appendix: #1 - Key plan, modified tree survey
#2 - 7 - Plan sections 1 - 6, enlargement of survey plan
Aplin & Martin Sketches Tree #11 - (1200fir.pdf) Tree #22 - (Linden tree drainage.pdf)

1826 Sunshine Coast Highway, Roberts Creek BC V0N 2W5
Phone 604-230-4711 Fax 604-886-2718 email mills@dccnet.com

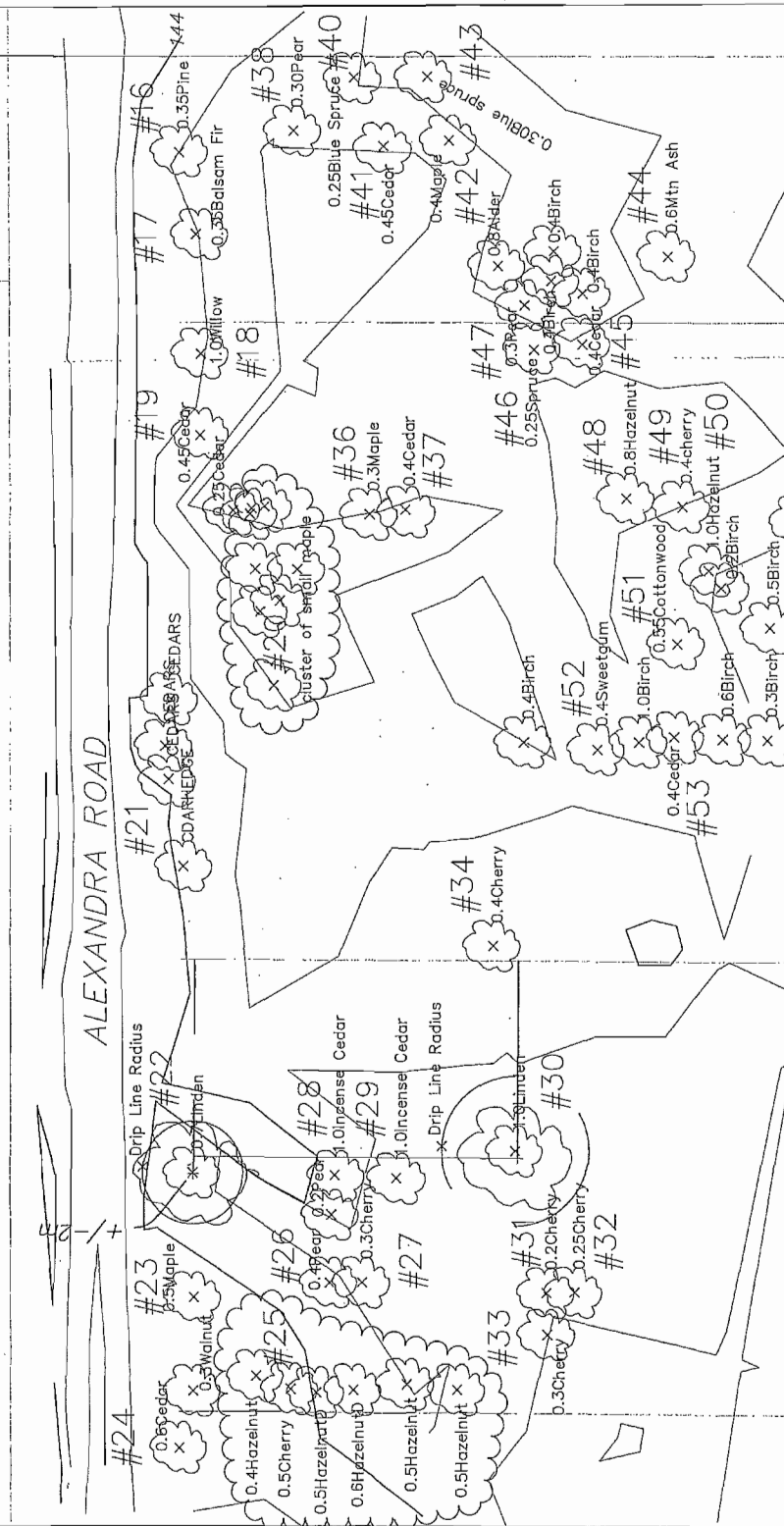
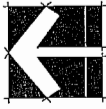


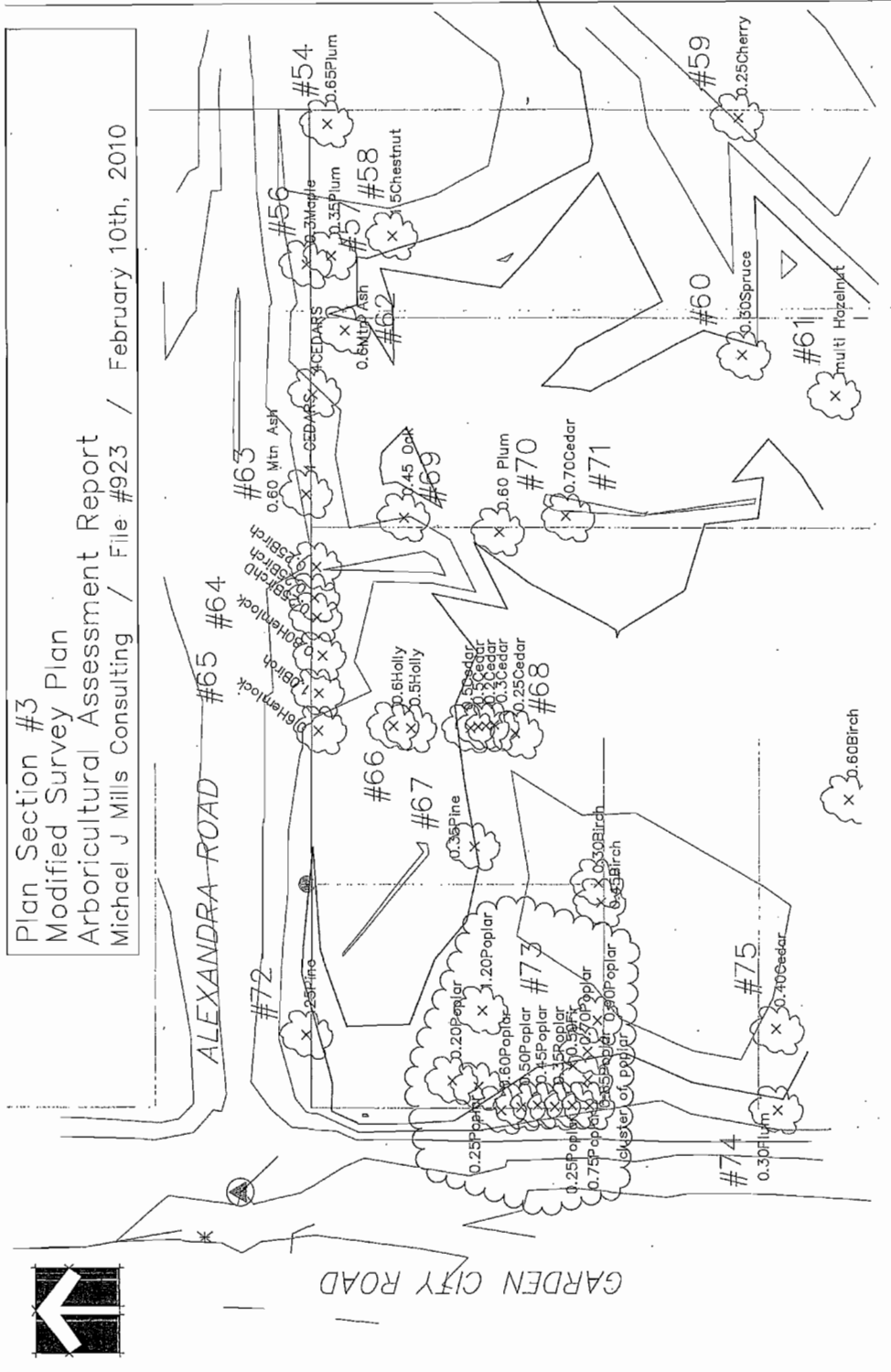


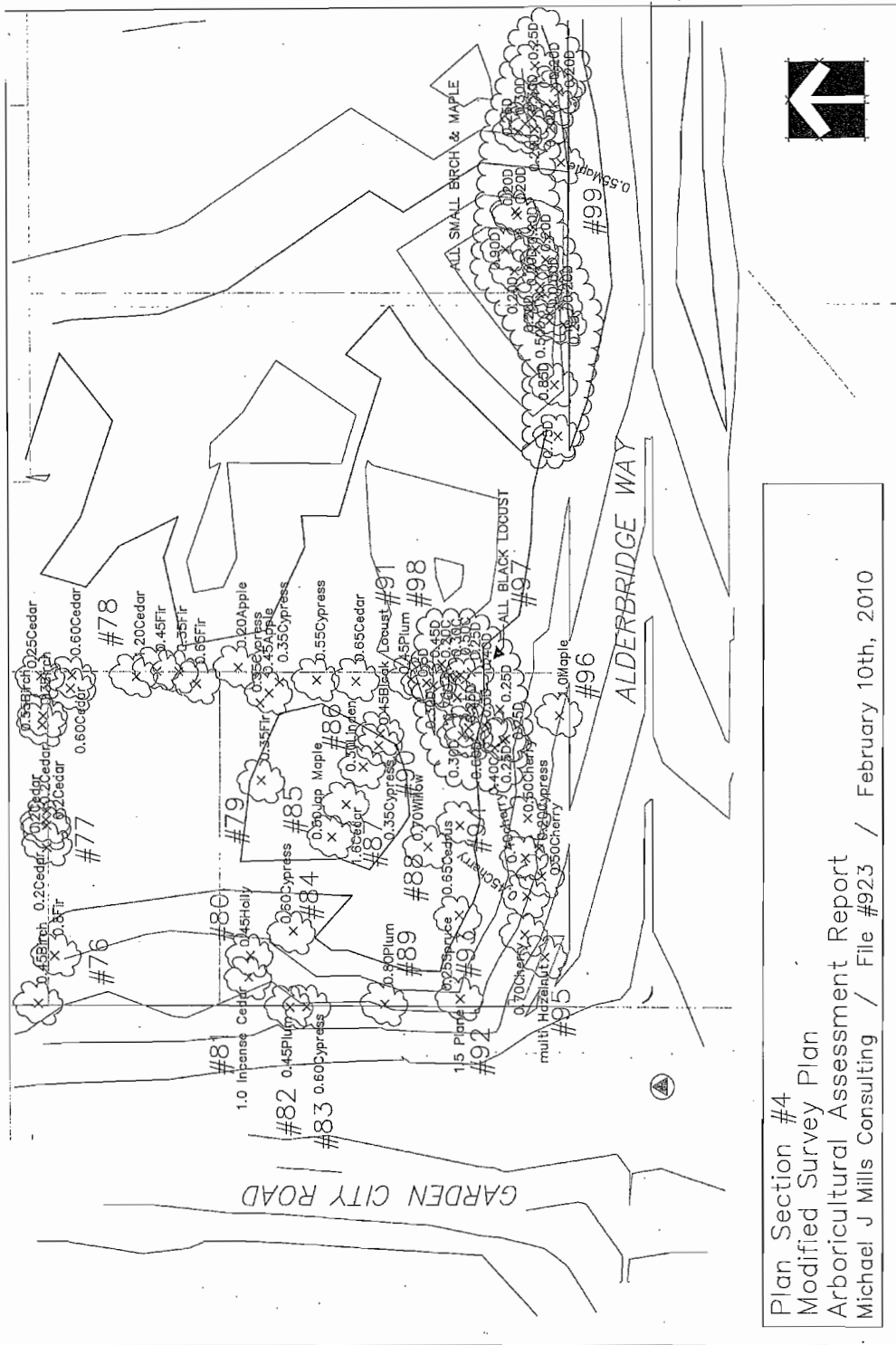
Plan Section #1
 Modified Survey Plan
 Arboricultural Assessment Report
 Michael J Mills Consulting / File #923 / February 10th, 2010



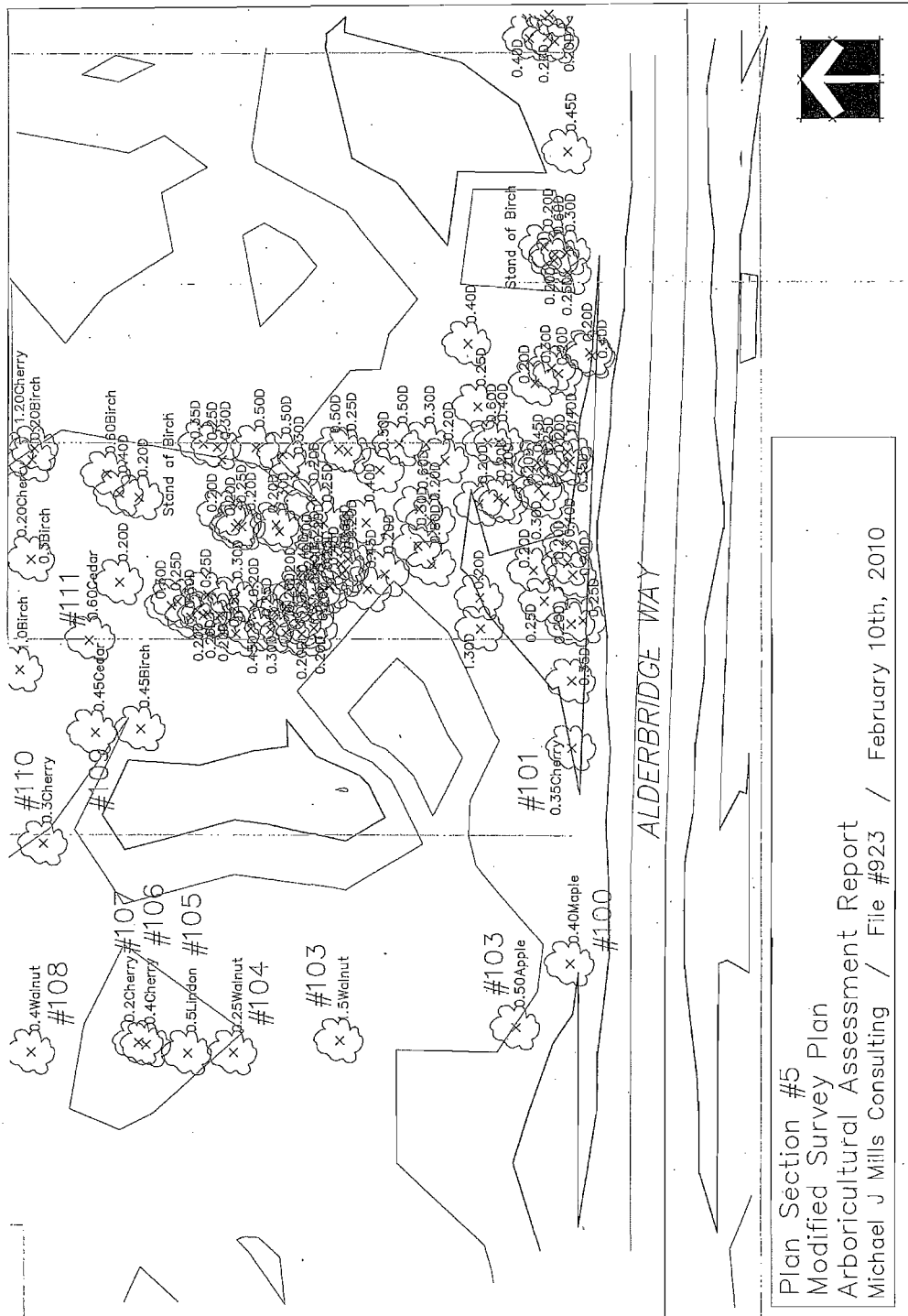
Plan Section #2
 Modified Survey Plan
 Arboricultural Assessment Report
 Michael J Mills Consulting / File #923 / February 10th, 2010



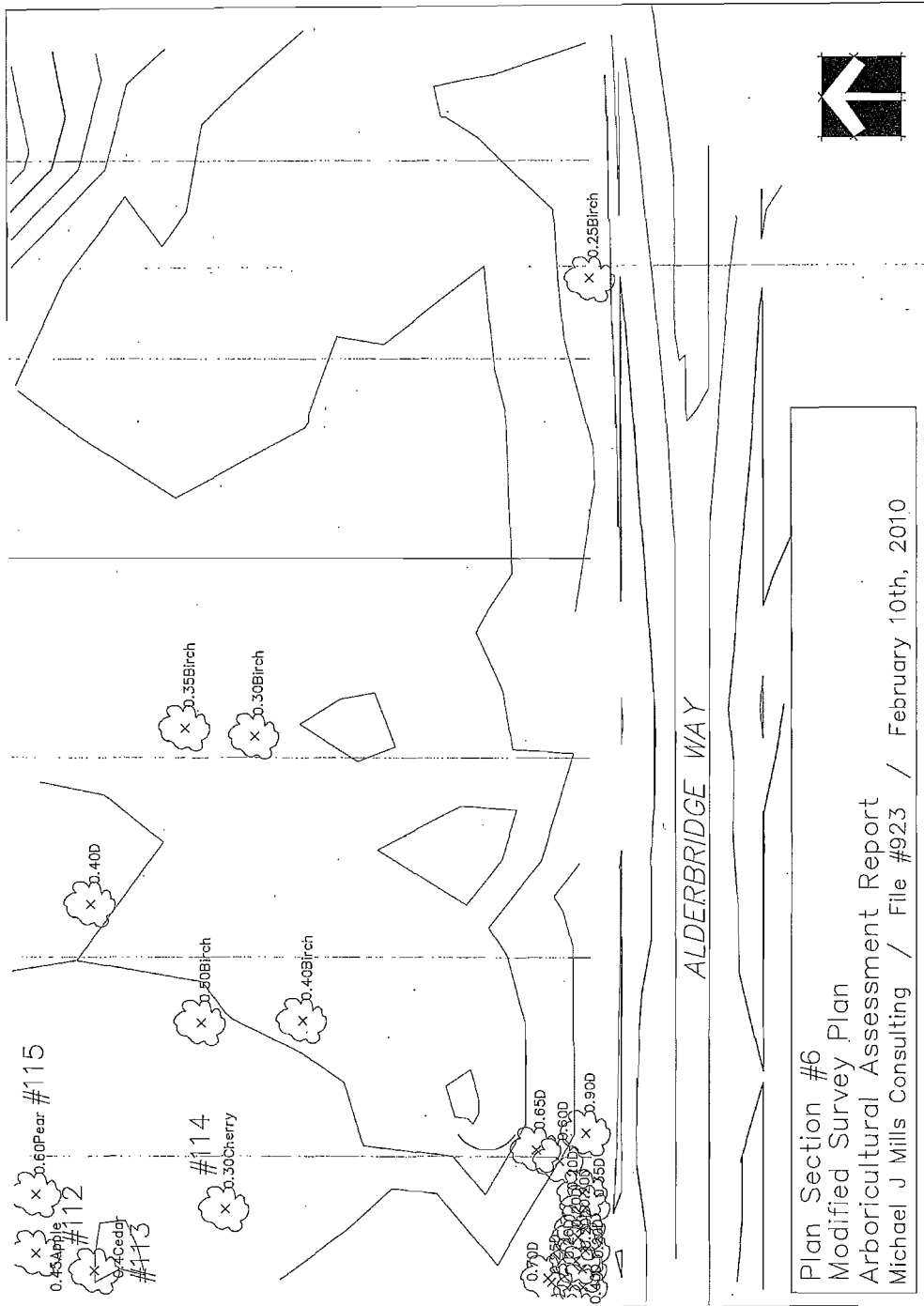


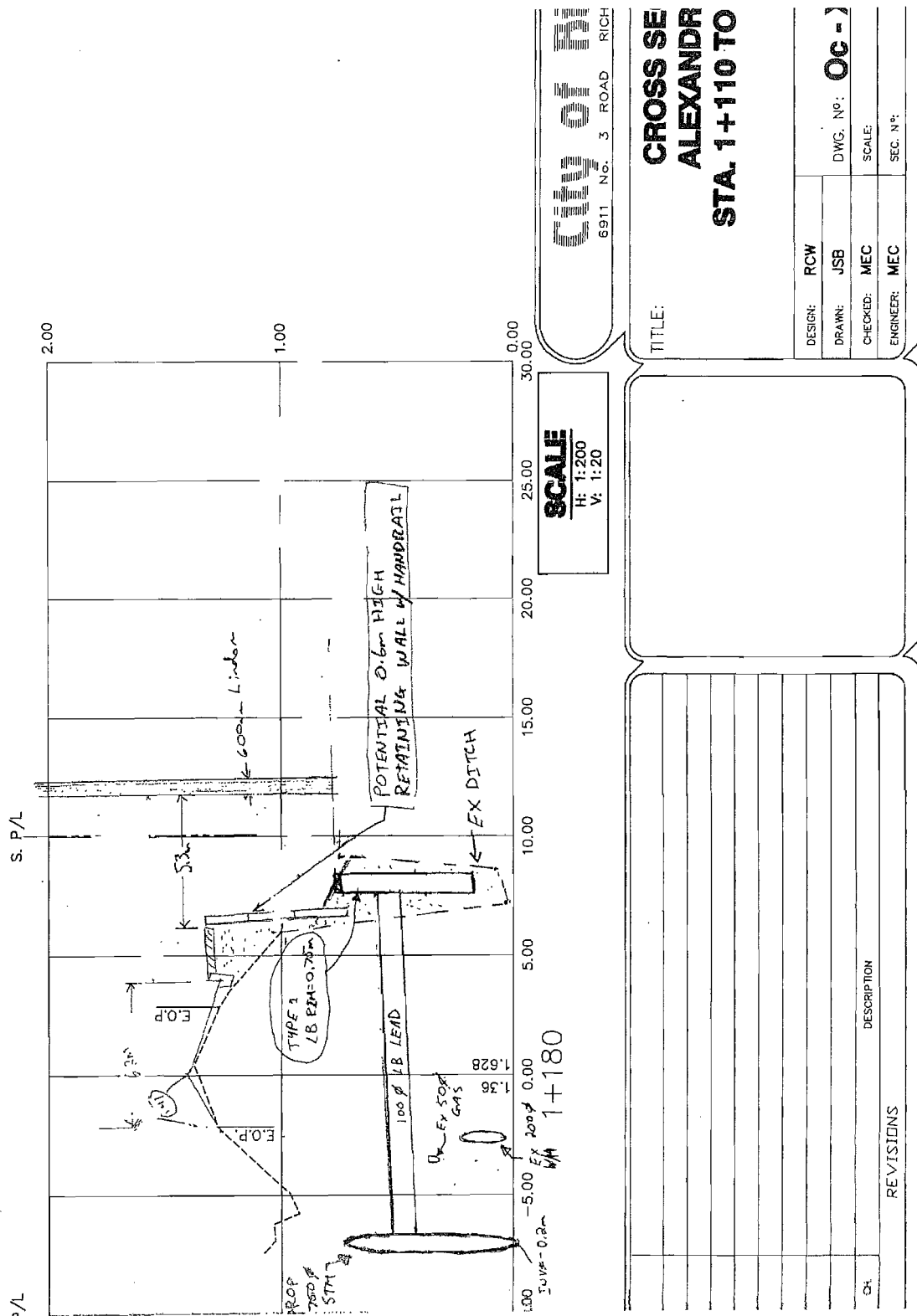


Plan Section #4
Modified Survey Plan
Arboricultural Assessment Report
Michael J Mills Consulting / File #923 / February 10th, 2010



Plan Section #5
Modified Survey Plan
Arboricultural Assessment Report
Michael J Mills Consulting / File #923 / February 10th, 2010





SCALE
H: 1:200
V: 1:20

City of Alexandria
6911 No. 3 ROAD RICH

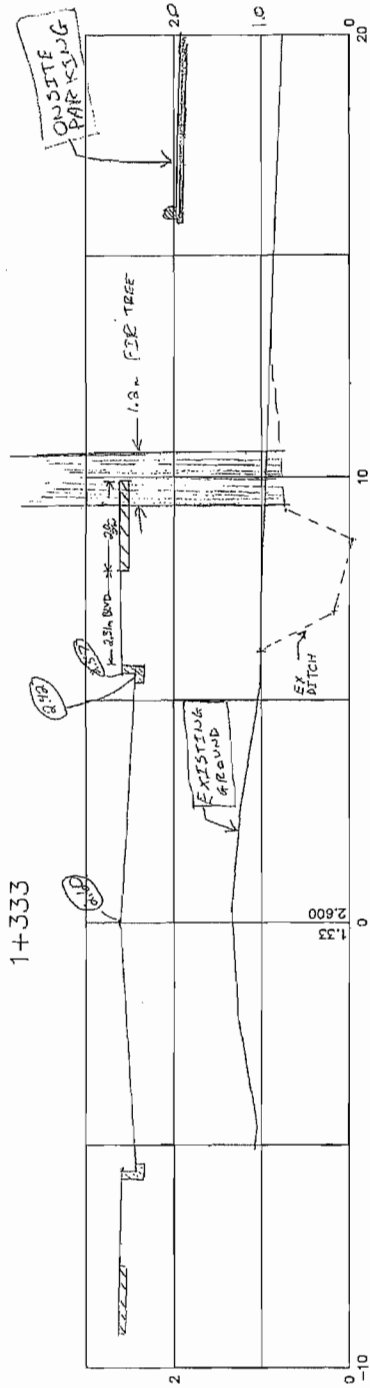
TITLE:
CROSS SECTION
ALEXANDRIA
STA. 1+110 TO 1+180

DESIGN:	RCW
DRAWN:	JSB
CHECKED:	MEC
ENGINEER:	MEC
DWG. No.:	Oc - 1
SCALE:	
SEC. No.:	

NO.	DESCRIPTION

NO.	DESCRIPTION

H 1:100
V 1:50



SmartCentres Supplemental Vegetation Assessment

Michael J Mills Consulting
Certified Arborist

Date:	September 27 th 2013	Project File No.:	923
Attention:	Alan Lee	Project Name:	Richmond Smart Centre Commercial Development
Firm Name:	Smart Centres	From:	Michael Mills

We were requested to provide comment and advice with respect to several specific tree related questions that have been asked of Smart Centres during the approval process with the City of Richmond. We revisited the site earlier on today's date to ensure we were familiar with the current site conditions prior to responding.

Question 1:

Can the trees identified as large significant trees be relocated?

There are three trees on this site that have been identified as large significant trees; these include two Linden trees (report reference #22 & 30) and a large Douglas Fir (tree #11). We understand that tree #8 (Douglas Fir) was mentioned in a staff report, however, we have never considered this specimen to be a high value tree due to poor form and structural issues.

We premise our response by stating that with enough time, money and resources, almost any tree can be moved. I have personally been involved in the successful relocation of many small and moderate scale trees but never any of the scale of the three trees in question.

Tree #30 is a substantial Linden tree. The trunk diameter is approximately 90cm dbh and the tree is over 35metres in height. The minimum size of root ball that would need to be excavated to relocate this tree would be approximately the same as the drip line radius. We measured this radius to be approximately 15metres. We are not sure what the weight of a 30 metre wide package of soil might be but we can reasonably estimate that there is no single crane in BC with the capacity to lift the associated weight, even if a truck could be found that could move such a large tree.



Tree #30.

The other Linden tree (#22) is slightly smaller in scale but still substantial in size. We comment that this tree is suffering this growing season from an insect infestation that has defoliated a notable extent of the canopy. No insect activity was apparent on the tree to allow for precise diagnosis but this species of tree is known to be susceptible to Winter Moth and the damage appears to be consistent with the feeding habit of this small caterpillar.

644 Bay Road Gibsons BC V0N 1V8 Phone 604-230-4711 / mills@dccnet.com

sment

S



*Tree #22:
Note light foliage density
and ditch along Alexandra
Street.*

The Fir tree (#11) is the largest of all three trees.



*Tree #11:
Part of a group of three
trees, 2 Firs and 1 Cedar.*

The limiting factor for both of these trees, beyond the large scale, is the association with the open ditch along the edge of Alexandra Street. Both trees have spent their life with unlimited access to the water that stands in this ditch year round. Based on our knowledge of how trees grow, we can reasonably assume that there is an extensive root system associated with both of these trees that extends along the edge and under this ditch. The relocation of trees that have grown under such site specific conditions would not be recommended as there is no realistic way for a tree to compensate for the abrupt change in the localized environment that would result. Even if one could physically move these trees (which again would be extremely difficult and expensive) the chances of survival would be very low.

In our opinion, the three large trees are not realistic candidates to be relocated.

September 27th 2013
 MJM Project No: 923
 Garden City and Alderbridge Way Commercial Development

Page 3 of 6

Question 2:

We were asked to comment on the type and condition of the vegetation along the south edge of the site adjacent to Alderbridge Way.

We understand that comment has been made with respect to the green edge that is present along the north side of Alderbridge way. There are presently three more or less distinct vegetation zones along this section of the road. First there is the corner area close to Garden City Road, there is the central section and the forested area at the east end.

The pre-existing property at the corner of Alderbridge Way and Garden City Way was previously occupied with a single family home. Over the years, trees were planted and have naturally occurred along this edge of the property. Tree species in this area include Hazelnut, Cherry, Maple, London plane and Black Locust. Many of the trees have been damaged by past pruning but they remain relatively healthy. There are no trees in this area of high individual landscape value. The understory vegetation in this area is dominated with Blackberry and Japanese Knotweed, two undesirable plant invasive species. We comment that the site plan indicates that Richmond intends to widen Alderbridge Way in this area such that all of the existing trees along the edge of the road will need to be removed.



View of the vegetation at the intersection of Alderbridge Way and Garden City.

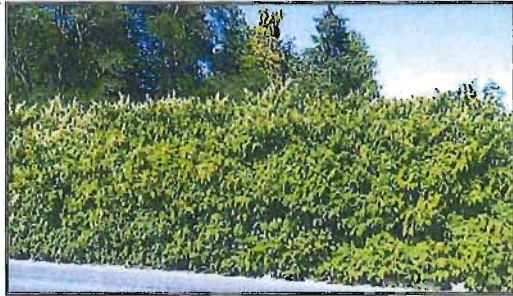
In the central area, the vegetation is dominated by invasive plant species. Both Blackberry and Japanese Knotweed have established in substantial clusters. Many of the existing trees have died or are in advanced decline. While the edge could be described as green, the quality of the vegetation would be rated as very poor.



Central area showing Blackberry infestation and dead and declining trees.

September 27th 2013
MJM Project No: 923
Garden City and Alderbridge Way Commercial Development

Page 4 of 6



View of the Japanese Knotweed infestation through the central area.

Toward the eastern end, there is a dense stand of immature Paper Birch that has naturally established close to the edge of the road. Most of the trees are quite small. Most of the smaller trees remain in relative good health, however, the species as a whole is under attack in the Richmond area from an insect referred to as the Bronze Birch Borer. The trees in this area remain in overall good health only because of their relative small size. The insect is attracted to the top of birch trees and as such the insect tends to attack larger trees first. There is no viable means to contain or control this pest and as such the long term prognosis for all of the Birch trees in Richmond is in question.



Overview of the forested area along Alderbridge Way at the east end.



Example of the damage caused by Bronze Birch Borer. Trees in this photo are located along the edge of Alderbridge Way.

September 27th 2013
MJM Project No: 923
Garden City and Alderbridge Way Commercial Development

Page 5 of 6

Question 3:

We were asked to review the site for possible candidates for the relocation of trees to the proposed ESA dedication area at the northeast corner of the proposed development.

Unfortunately, there are very few quality trees within the development area. Most of the trees suffer from a variety of ailments such as poor form, past pruning practices, poor health etc. We have reviewed the site and can confirm that there are no small or moderate scale trees with potential to be relocated that would meet the traditional definition of "high value tree".

There are two moderate scale trees that could potentially be relocated to the north east corner. Tree #67 is an Austrian Pine with good open grown form. Tree #17 is a Balsam Fir that maintains good open form despite the double leader.



Tree #67, Austrian Pine



Tree #17, Balsam Fir

Both trees are of relative large size with an estimated trunk diameter of 40cm dbh. Relocation would be challenging but if completed with care under winter dormancy, relocation could be a viable option. In our opinion, neither tree is of sufficient quality or landscape value to warrant the significant cost associated with the relocation of trees of this scale.

There are several smaller trees on the site that could be relocated such as Locust and Poplar seedlings, however, trees of this scale could be purchased new from nurseries more cost effectively than relocation.

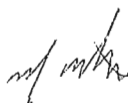
September 27th 2013
MJM Project No: 923
Garden City and Alderbridge Way Commercial Development

Page 6 of 6

Additional photos of the existing site trees were recorded and are available for use if required.

I trust this information will help to address the questions that have been put to us. Please feel free to contact us if you have any questions or concerns with the information provided.

Regards



Michael Mills
ISA Certified Arborist PN0392

Recent Correspondence (since September 17, 2013)

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 10:59
To: 'Katie Eliot'
Subject: RE: No to Walmart development - again NO

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Katie Eliot [<mailto:keliot@langara.bc.ca>]
Sent: Monday, 16 September 2013 4:30 PM
To: MayorandCouncillors
Subject: No to Walmart development - again NO

Good afternoon,

I understand there is another Walmart proposal coming to you tomorrow afternoon. Once again, there is no need for another shopping centre and more traffic congestion in downtown Richmond.

After the extreme rainfall we had last night, isn't it more important to have natural areas such as the Garden City Lands bog to soak up all this water? More blacktop and concrete will just exacerbate such problems and put more strain on city sewers.

Please think sustainably and keep the Garden City Lands area completely in the ALR - no exceptions!

Thank you,
Katie Eliot
Katie Eliot
Division Assistant
Creative Arts & Humanities
(604) 323-5005

Langara College
100 West 49th Avenue, Vancouver, BC, V5Y 2Z6

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 11:02
To: 'Lorne Brandt'
Subject: RE: Walmart/Smart Centres Mall

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Lorne Brandt [<mailto:lorne.brandt@shaw.ca>]
Sent: Monday, 16 September 2013 4:36 PM
To: MayorandCouncillors
Subject: Walmart/Smart Centres Mall

Honourable Mayor Brodie and Council,

I am a resident of the city centre, writing about a meeting Tomorrow, Sept 17, night in which I understand you will be discussing the Walmart wishes...

in the first place, with Target just opening and 2 other major malls in the area, not to mention 3 lesser ones, with Walmart being just down the road in New Est, I don't think we need another big shopping centre on Alexandra/Alderbridge. We have plenty of stores downtown - lots of dollar type stores and other discounts to compete with Walmart too.

If you do approve Walmart, you lose my support. But if you do approve it, P-L-E-A-S-E, PLEASE, please, get them to tone it down so they leave at least a 15 metre green strip of what is now growing there. I am one of those who really enjoy having Garden City Lands there and I don't want to be at the lands and looking across the street at a mall. Imagine doing that at Stanley Park or Garry Point!

Thank you.

Richmond BC.
lorne.brandt@shaw.ca
https://www.facebook.com/lorne.brandt.1?ref=tn_tnmn
<http://reflect-lulu-isle.blogspot.ca>
This message may have been dictated by Macspeech

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 12:55
To: 'Pam Price'
Subject: RE: September 17th meeting

Follow Up Flag: Follow up
Flag Status: Flagged

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Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Pam Price [<mailto:pamofgwent@shaw.ca>]
Sent: Monday, 16 September 2013 4:44 PM
To: MayorandCouncillors
Subject: September 17th meeting

To the Mayor and Councillors:

As a resident of Richmond for 30 years I urge you, at the meeting on September 17th regarding the Walmart project, to stop this continuous ceding to developers. They keep up the pressure time after time to get what they want and city staff encourage them in this. What about what the citizens want? We seem to be asked time and again the same questions. I urge the Mayor and council to reconsider this development of the Walmart Mall and protect the viewscape and the wildlife corridor.

Sincerely,
Pam Price

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 12:58
To: 'Kathryn and Gord'
Subject: RE: Re Walmart on Alderbridge proposal

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson

Manager, Legislative Services

City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1

Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Kathryn and Gord [<mailto:potdoug@shaw.ca>]
Sent: Monday, 16 September 2013 5:07 PM
To: MayorandCouncillors
Subject: Re Walmart on Alderbridge proposal

To the Mayor and Councillors of Richmond,

As a 20-year citizen of Richmond I would like to share my thoughts on this proposal. We have a large and vibrant shopping area in Richmond that is not currently fully utilized – witness the various For Lease signs in malls and on 3 Road. We do not need to expand retail in to parts of Richmond that are currently designated as green and that should remain green.

Let us focus our attention in a central area of the city.

Regards, Kathryn Potter
potdoug@shaw.ca
604-274-3777

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 12:58
To: 'Bell, Yvonne [HSSBC]'
Subject: RE: Smart Centre Mall

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

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Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Bell, Yvonne [HSSBC] [<mailto:Yvonne.Bell@hssbc.ca>]
Sent: Monday, 16 September 2013 5:24 PM
To: MayorandCouncillors
Subject: Smart Centre Mall

I would like the Mayor and Councillors to make sure there is a buffer of forested land kept along the north side of Alderbridge Way between 4 road and Garden City road. This forested land would be a continuation of a buffer that already exists along Alderbridge Way between 4 road and Shell road. The city has already allowed over 1000 trees to be removed from inside the Garden City road, 4 road, Cambie Road, and Alderbridge Way area (this from the developers arborist reports). Please don't let this become another treeless mall site. Mall developers promise landscaping but never deliver. They're just acres of blacktop with the odd tree. As a lifelong resident of Richmond and commuter cyclist, I am thankful for every urban forest we have left here in Richmond. Please do not let another urban forest be paved over, we have so few left. As you all well know, paving over forests with blacktop is one of the causes of climate change. Thank you for your time.

Yvonne Bell
10431 Mortfield Road
Richmond, BC
V7A 2W1

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 12:59
To: 'Pantelis Karaplis'
Subject: RE: WallMart Mall

Follow Up Flag: Follow up
Flag Status: Flagged

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Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

-----Original Message-----

From: Pantelis Karaplis [mailto:pantelis_karaplis@shaw.ca]
Sent: Monday, 16 September 2013 7:45 PM
To: MayorandCouncillors
Subject: WallMart Mall

Please let Planning Staff know that another mall is not needed in Richmond. If a Richmond resident wishes to shop at Walmart, they can drive to Vancouver or New Westminster. Please, not another Costco type project here. We have enough of these. They are ugly.

With respect,
Pantelis Karaplis
6260 Skaha Crescent

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:01
To: 'Penny Budd'
Subject: RE: Garden City Lands

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

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Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Penny Budd [<mailto:pobudd@telus.net>]
Sent: Monday, 16 September 2013 8:02 PM
To: MayorandCouncillors
Subject: Garden City Lands

I understand that Wal-Mart is trying to buy Garden City Lands!
Don't we have enough concrete in Richmond?
Enough is enough! Everywhere you look there is building going on but I don't see any new parks being built. Even the old railway track has gone to make way for MORE paving over when we already had a bicycle path right there on the road and the natural greenery and blackberry bushes have been ripped down, covered by blacktop. I can't imagine what Richmond will be like in 10 years time! We certainly don't need any more shopping centres so why don't the council concentrate on leaving the Garden City Lands GREEN.

Sincerely, Penelope Budd
8160 Fairbrook Crescent, Richmond.

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:04
To: 'brian phillips'
Subject: RE: Walmart Mall - Alderbridge Way

Follow Up Flag: Follow up
Flag Status: Flagged

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Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson

Manager, Legislative Services

City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1

Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: brian phillips [<mailto:brianmaryphillips@hotmail.com>]
Sent: Tuesday, 17 September 2013 7:56 AM
To: MayorandCouncillors
Subject: Walmart Mall - Alderbridge Way

Dear Mayor and Councillors,

I would like to add my voice to the other citizens of Richmond especially those who live in the City Centre who have begged you not to approve the Walmart Mall as proposed by the developers and City staff. You will be destroying a priceless city viewscape for the benefit of a retailer like Walmart that has the worst of reputations for how it keeps its prices so low. (Low pay and few benefits for its employees as well as sourcing its products from Asian sweatshops like the one that killed hundreds in a fire in Bangladesh)

Please do not do as you have in Steveston for Onni. The lovely waterside walkway that I walk most days has been irretrievably spoilt by the ugly concrete buildings and already cracking concrete pathways (no park, no new community library as once promised). If Onni's proposal is accepted, Steveston will have big retail stores and all the truck and vehicle traffic that involves – Moncton St will be transformed and not for the better. Yet the City and Tourism BC continues to advertise the "gem" of historic Steveston to visitors.

The wildlife corridor along Alderbridge Way from Garden City Rd to No 4 Rd and north to Alexandra Rd should be preserved. The remains of the urban forest and the ESA should be preserved. What a mockery of the City's Tree By-Law such destruction will be.

Please do the right thing for Richmond and its residents not the developers for once.

Mary Phillips

219- 5500 Andrews Road, Richmond. 604-271-8794

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:06
To: 'Janice'
Subject: RE: Walmart - SmartCenter Malls Application

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Janice [<mailto:jann.lambert@gmail.com>]
Sent: Tuesday, 17 September 2013 8:44 AM
To: MayorandCouncillors
Subject: Walmart - SmartCenter Malls Application

Dear Mayor and Councillors:

I respectfully request that you and your colleagues deny the application to build a new Walmart or any other such enterprise in the space suggested opposite the Garden City Lands.

1. We need to protect the beauty that is Richmond, what's left of the beautiful green space is enhanced by the gorgeous views of the North Shore mountains.
2. There is a huge Walmart only 15 minutes drive along the East West connector that was recently enlarged, there really is no need for another big box store so close.
3. Traffic, which is already challenging in that area during busy peak times will be even more difficult to navigate and large trucks of merchandise will be clogging one of the main arteries in and out of Richmond even further.

Many folks in Richmond are not aware of the proposal to build in that space, my fear is that if the proposal is not denied, we will see more big box stores spreading across that whole city block over the next few years - we don't need more in Richmond. If this is encouraged, it will kill the smaller stores in the central Richmond corridor, stores that have been there for decades.

Thank you for taking the time to read my message,

Sincerely
Janice Lambert

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:08
To: 'Bonnie Eliot'
Subject: RE: Re Garden City Lands and Walmart proposal

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

-----Original Message-----

From: Bonnie Eliot [<mailto:bonnieeliot@shaw.ca>]
Sent: Tuesday, 17 September 2013 8:45 AM
To: MayorandCouncillors
Subject: Re Garden City Lands and Walmart proposal

Dear Sirs and Madams,

Once again our remaining natural lands are under attack by multi-national corporations. Walmart is once again proposing developing on the Garden City Lands. Everyone I talk to in my neighbourhood (Seafair) is very much opposed to any more development. We all thought that you have firm plans for urban gardens/agriculture through Kwantlen, and to leave other areas as is.

There is so much development in Richmond that it is rare and very desirable to have nature close. These bog lands are a precious natural resource that cannot be replaced. Once encroached upon, it will be too late to save this beautiful landscape.

I ask that you represent my views in this afternoon's proposal meeting. Pls continue to stand firm and block any Walmart development.

Sincerely,
Bonnie Eliot
8151 Fairbrook Cres

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:10
To: 'Kate E'
Subject: RE: Walmart - not again!

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

-----Original Message-----

From: Kate E [<mailto:katekate88@shaw.ca>]
Sent: Tuesday, 17 September 2013 8:52 AM
To: MayorandCouncillors
Subject: Walmart - not again!

Dear Mayor and Councillors,

I can't believe that Walmart is once again proposing to develop part of the Garden City Lands. Didn't you turn them down last December? How much longer will Walmart waste your time with plans that do not fit with The Garden City, our beautiful Richmond?

Pls continue to stand up for the Kwantlen University plan, and for all citizens of Richmond who need clean air to breathe, and open spaces to de-stress. With all the current and imminent future traffic in Richmond Centre, the bog lands are needed more than ever!

I'd be grateful if you would represent my viewpoint at today's meeting.
Special Hello to Harold and Bill!!

All the very best,
Kate Eliot

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:12
To: 'Berda, Betty [RH]'
Subject: RE: Garden City Lands/Walmart

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 17, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Berda, Betty [RH] [<mailto:Betty.Berda@vch.ca>]
Sent: Tuesday, 17 September 2013 9:21 AM
To: MayorandCouncillors
Subject: Garden City Lands/Walmart

Please consider keeping the trees that line the north side of Alderbridge Way between 4 rd and Garden City Road. With so much development, and a soon to be large complex in this area, it would provide a better view for the public, visitors to the area, as well as keeping these lovely trees intact.

Sincerely,

Betty Berda
58- 8640 Bennett Rd.
Richmond, BC

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:13
To: 'Barbara Allan'
Subject: RE: Walmart Mall/SmartCentres Mall

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 16, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson

Manager, Legislative Services

City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1

Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Barbara Allan [<mailto:bjallan@hotmail.com>]
Sent: Tuesday, 17 September 2013 10:31 AM
To: MayorandCouncillors
Subject: Walmart Mall/SmartCentres Mall

Dear Mayor and Councillors,

it is my responsibility as a resident of the City of Richmond to voice my opposition to the construction of Walmart or similar big box stores in the Alderbridge area. Continued destruction of the natural habitat will soon make Richmond another lifeless, predictable, black-topped wasteland, quite contrary to the goal of being a livable city where man and nature can still connect. There is more to life than shopping. Let's not continue to throw up barriers between ourselves, and our children, to nature.

This afternoon's meeting is your opportunity to think about the "bigger picture."

Thank you for your consideration.

Barbara Allan
6460 Swift Ave,
Richmond

Guzzi, Brian

From: MayorandCouncillors
Sent: Tuesday, 17 September 2013 13:14
To: 'Tim H'
Subject: RE: Walmart Mall or SmartCentres Mall

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 17, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Tim H [<mailto:kwazimoto@msn.com>]
Sent: Tuesday, 17 September 2013 11:52 AM
To: MayorandCouncillors
Subject: Walmart Mall or SmartCentres Mall

Hello Mayor & Councillors

Please leave the Garden City Lands as a green space , putting in a Walmart could be devastating to small businesses in the area not to mention a big box store in a already concrete jungle complete with all the congested traffic and smog , I love Richmond but it's starting to look like a big metropolitan city like Vancouver or Toronto

Thanks for listening Tim

From: Yvonne Harwood [mailto:mail@yvonneharwood.mygbiz.com]
Sent: Tuesday, 17 September 2013 12:12 PM
To: MayorandCouncillors
Subject: "Wal-Mart mall" or "Smart Centres mall"



Good Morning, Your Worship, Mr. Mayor & Honourable Council Members:

I am writing to speak AGAINST the application of Walmart and their associates for approval of plans to locate in Richmond. I am writing to you, in the hope that you will listen to your constituents, rather than your staff, many of whom don't live in Richmond.

There are many reasons for my strong opposition, not all of which I write here (not necessarily in order of priority):

PROPERTY VALUES:

Installation of a Walmart store/mall has a huge negative impact on its surrounding property values, which, in turn, negatively affects the City's tax base income. Ask yourselves, how anxious would you be to purchase a residence in close proximity to a Walmart.

Cheap retail outlets cheapen their area. Isn't the rent charged in mall locations of high-end retailers higher than their opposites? This indicates the low desirability of proximity to low-end retailers.

Currently, the World, many organizations and the public hold Richmond in high esteem as a desirable place to live. Allowing Walmart into Richmond would seriously and negatively impact that position. As a Richmond REALTOR® I am sensitive to the impact any development or change may have on the 'desirability' of Richmond as a place to live. One only has to look at the variance of property values between the different areas of Richmond and the reasons behind those differences, to see how just one characteristic can impact an area's values; I ask you to think about the impact such a huge characteristic as a Walmart Mall would have on the entire Garden City Area.

POLLUTION – visual; air; water and wildlife habitat loss:

Walmart and its associates do not have a reputation for inspiring, or even attractive architecture or concepts. Not only are the actual structures and their surrounding parking areas repelled by the eye, but the destruction of the current natural vistas this project would necessitate will deplete Richmond's quantity of natural beauty; such destruction quadruples the negative visual impact the subject project would have.

The negative impact this loss of natural terrain will have on Richmond's air quality and rain water absorption is incalculable. Do we really want so many additional acres of asphalt serving as an oil additive to our storm water, our Mighty Fraser River and the Straights of Georgia? I think not.

What will the Walmart or the City do to re-house, feed and protect the thousands of creatures which call that area home? We will lose so many birds and other creatures to what, an unwanted edifice to the "love of money" – the root of all evil.

While there are many hundreds speaking against this project, there are a hundred times that number who feel the same way but remain silent.

It is possible that those of Council who are seen* to be in favour of this project may have difficulty in any future bid for a Council position.

*"Those who sit on the fence are, in reality, on the side of the oppressor." *Unknown author.*

Sincerely,
 Yvonne Harwood
 Parsons Road, Richmond
 A Richmond Resident for 25 years

From: Sharon MacGougan [<mailto:sharonmacg@telus.net>]
Sent: Tuesday, 17 September 2013 12:20 PM
To: MayorandCouncillors
Subject: Walmart mall

Dear Mayor and Councillors,

I am writing against the proposed Walmart development. Walmart is not the type of corporate citizen that I want to see in Richmond and especially not in such a key area as has been proposed.

According to a Globe and Mail editorial (September 16, 2013) Walmart has done nothing to assist the victims of the devastating fire in a clothing factory in Bangladesh that killed more than 1,100 workers. Walmart took advantage of cheap labour but has taken no action in the five months since the fire to compensate the victims.

“Shockingly, only nine of the 29 brands whose products were made in the Rana Plaza complex attended a meeting last week that was called to discuss compensation for the victims. The talks, chaired by the International Labour Organization in Geneva, were intended to figure out how to help the injured and the families of those killed.”

“Many big retailers, including Walmart ... didn’t bother to send anyone to the meeting, although they were invited.”

To date, only one of the 29 companies has given out any compensation and it was not Walmart. “Perhaps some companies think that because the Rana Plaza disaster is no longer in the headlines, they can slink away from their responsibility to those who suffered.”

I’m sure that Walmart representatives will be out in force as this proposal is discussed. But is this the type of company we want in Richmond’s heart (centre of Richmond)? I don’t think so; not in the Richmond I grew up in, know and love.

Saying no to Walmart, and saying yes to preserving a mixed urban forest of the Alderbridge wildlife corridor, would create a legacy for Richmond worthy of the slogan that invites people into our community: Island City, by nature. I want our future generations to hear songbirds; not just hear about what we lost.

Respectfully yours,

Sharon MacGougan

7411 Ash Street

Richmond, BC V6Y 2R9

Guzzi, Brian

From: MayorandCouncillors
Sent: Wednesday, 18 September 2013 11:53
To: 'C Southgate'
Subject: RE: Walmart

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 17, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

-----Original Message-----

From: C Southgate [<mailto:cgate@telus.net>]
Sent: Tuesday, 17 September 2013 2:55 PM
To: MayorandCouncillors
Subject: Walmart

Mayor and Council,

Please do not allow the destruction of the existing natural area along Alderbridge way for the proposed Walmart. To replace this important esa and wildlife corridor with a few token trees is not a solution. We continue to lose much of Richmond's natural legacy to development, and this unique area deserves protection. Natural spaces in cities has been proven to add to the quality of life, and aside from the proponents of this project, there is little support from the community on the needless destruction of this important esa and wildlife corridor.

Sincerely,

C. Southgate

Guzzi, Brian

From: MayorandCouncillors
Sent: Wednesday, 18 September 2013 11:54
To: 'Janis Hindman'
Subject: RE: Walmart Mall

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 17, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

-----Original Message-----

From: Janis Hindman [<mailto:janishindman@shaw.ca>]
Sent: Tuesday, 17 September 2013 3:29 PM
To: MayorandCouncillors
Subject: Walmart Mall

As a Richmond resident, I would like to state my opposition to the proposed Walmart development. The Garden City Lands and the Wildlife corridor along Alderbridge are areas of not only great beauty, but they are also part of our Natural Capital. The sphagnum area provides some flood protection and filtering of particulates - restoration would provide more.

They are a habitat for many bird and other animal species and they give a focus to our beautiful skylines.

The traffic on Alderbridge is already stupefying at most times of the day, a Walmart development would only serve to compound this.

Please consider these concerns when making this decision on our behalf and on behalf of the wildlife which has no say.

Guzzi, Brian

From: MayorandCouncillors
Sent: Wednesday, 18 September 2013 12:18
To: 'Margie&gary1067'
Subject: RE: No more Walmarts PLEASE!

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 17, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

-----Original Message-----

From: Margie&gary1067 [mailto:margie_gary@telus.net]
Sent: Tuesday, 17 September 2013 6:34 PM
To: MayorandCouncillors
Subject: No more Walmarts PLEASE!

Please keep some of Richmond green and natural!
We need more parks not less, and have more than enough shopping malls.
Preserve what we have at least.

Thank you,
Margaret Campbell
Gary Almhjell
And family

From: steve sangha [mailto:stevesangha@shaw.ca]
Sent: Wednesday, 18 September 2013 11:05 PM
To: MayorandCouncillors
Cc: rxshawn@yahoo.com
Subject: Walmart Development RE HOLDOUT PROPERTY FOR CONNECTOR ROAD
Importance: High

My parents (family Mr and Mrs B Sangha) own 4560/4562 Garden City Road. This property is opposite Leslie Road and is an integral part of the connector road to the Walmart project. We were quite shocked and dismayed about reading recent submissions about the development in the local paper.

Firstly, Smartcentres has been accumulating property in the neighborhood for over 10 years. This has destroyed the neighborhood that I grew up from the early 1970's. There were vacant houses many break-ins, homeless living in the area for the past few years. They have held the entire area hostage for the past few years.

We were quite shocked that Smartcentres has said that there are holdout owners. This is not true. Over the past year three years, my parents have signed real estate purchase agreements with the developer (we have copies which we can send you) for the sale of our property. Smartcentres or their agents acting on their behalf sign these legal real estate agreements that agree to a purchase price and terms for the purchase. They let the term expire and they have locked up the property for the past two years. But what happened last year is that developer assumed the CITY of Richmond was going to pay for the purchase price they agreed upon for the connector road. When the City OF Richmond refused to build a road for the richest corporation on Earth, the developer let the purchase agreement expire. My parents tired of being give the run around (they are in their late seventies), were presented with new offers by the developers which is less than the half of the original offer they presented. Because the City of Richmond refused to pay for the road, they use intimidation and threats (expropriation or eminent domain via the City) to buy our property. They are now offering even less than the appraisal price. Their current offer is less than half of their original offer which they signed and agreed upon. They say the property value is only worth for road/asphalt because that is what it is zoned for. (For all the properties they bought ten years ago, they will not accept the appraisal price for their own properties but they ask that of all the other home owners.)

This has been very stressful for my elderly parents. They rent out the duplex to students, young families and aboriginal for affordable housing.

To read in the paper that the city planners have come to an agreement for the road to be built in ten years is unacceptable. If this road is critical then it cannot wait ten years.

The logistics of having Alexandra/ Garden City Road and Garden City Road/ Alderbridge intersections 20 feet apart and as an access to the property (size of Richmond Oval) will not work. There will be 50 stores (London drugs, wall mart) major anchors – the current road system will not handle the volume and congestion of traffic in the area. It will be ten times worse than Ironwood (No 5 road nad Steveston mess). I do not understand how the city planners will allow this to be pushed aside for ten years while Wal Mart gets its way. How will the City enforce Walmart to build the road in ten years if they won't do it now? No road no permit.

My family feels Smartcentres has a more sinister plan to make the neighbourhood suffer horribly by increasing traffic congestion to our property so that we will be forced to give in. How will the residents that currently live on Garden City enter their properties? Currently there is a back alley that goes form Alexandra off of garden city to get to the homes. It will be impossible for families in the neighborhood to get in or out of their houses. The way Walmart has proposed - to leave the current roads in use to access their shopping centre will not work.

I would appreciate something in writing that the councilors and major have received this email.

My phone number is 778-228-6872.

Thank you for your time in this matter.

Dr Steven Sangha on behalf of Mr and Mrs B Sangha

From: MayorandCouncillors
Sent: Monday, 23 September 2013 15:30
To: 'chiuamy@live.ca'
Subject: RE: City of Richmond BC - General Comments, Compliments and Questions - Case [0913-CS-COMMENT-002062] Received

This is to acknowledge and thank you for your web submission of September 20, 2013, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
 City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
 Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: InfoCentre
Sent: Monday, 23 September 2013 11:26 AM
To: MayorandCouncillors
Subject: FW: City of Richmond BC - General Comments, Compliments and Questions - Case [0913-CS-COMMENT-002062] Received

From: donotreply@richmond.ca [<mailto:donotreply@richmond.ca>]
Sent: Friday, 20 September 2013 15:26
To: InfoCentre
Subject: City of Richmond BC - General Comments, Compliments and Questions - Case [0913-CS-COMMENT-002062] Received



Attention: Administrator

A general comment, compliment, or question has been submitted through the City of Richmond online Feedback Form. Below is the information which was provided by the person submitting the feedback.

General Comments, Compliments and Questions

Category: Comment

Comment/Compliment/Question:
 RE: Walmart entering Richmond - say NO

I've lived in Richmond for 20+ years and saw the drastic changes of a low profile small city to a busy modern medium size city. I kept an open mind to welcome and adjusted to the changes over the years, except this time I must voice my strong objection to house Walmart in Richmond. Richmond has Target, Superstore, Winners, London Drugs, Shopper's Drug Mart, Homesense, Price Smart and many other Asian supermarkets in the near by neighbourhood of the Garden City land. These stores provide very competitive pricing. Walmart in Richmond isn't going to offer us more competitive choices but rather traffic congestion and a waste of a precious piece of land in the centre of Richmond. What does Walmart has to offer to people in Richmond??? Nothing! Walmart will only kill the business in the neighbourhood Lansdowne Shopping Mall.

Many people in my neighbourhood don't want another big box in the community. We don't buy in big box to loose our community environment. The traffic along Cambie Road, Alderbridge Way, Garden City Road, No 3 & 4 Road, especially Alexandra Road and Hazelbridge Way is very busy. In a 15 to 20 min traffic we have access to a Walmart store in New Westminster as an option for the Walmart fans. Why do we want Walmart to take the centre piece of our precious land when it is a store that the residents of City of Vancouver voted to ban? In fact Walmart is a sign of lower and cheaper neighbourhood. Its existing downgrade Richmond. Please say NO to Walmart.

Any

Personal Information:
 Any

604-723-0738

chiuamy@live.ca

Tech Information:
 Submitted By: 216.232.17.3
 Submitted On: Sep 20, 2013 03:25 PM

Click [Here](#) to open this message in the case management system. You should immediately update the Case Status either to Received to leave the case open for further follow-up, or select the appropriate status based on your activity and work protocols. Click Save to generate the standard received message to the customer, add any additional comments you wish to and click Save & Send Email. Close the browser window to exit.

From: MayorandCouncillors
Sent: Monday, 23 September 2013 15:15
To: 'Sundeep Gill'
Subject: RE: Proposed Walmart on Alderbridge and Garden City

This is to acknowledge and thank you for your email of September 22, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Sundeep Gill [<mailto:s.gill@queensu.ca>]
Sent: Sunday, 22 September 2013 10:13 AM
To: MayorandCouncillors
Subject: Proposed Walmart on Alderbridge and Garden City

Dear Mayor and Councillors,

It's frustrating to see this project once again being placed on hold. If anyone has been to the area as of late I think that they would agree that it might as well be deemed uninhabitable. The plans that were put forth over 10 years ago are a direct result of why the properties have been neglected, abandoned, and in most cases torn down. The level of vagrant and criminal activity has increased dramatically; will this be the Downtown Eastside of Richmond? The two councillors that were against this proposal need to look at the overall impact to society, not simply the environmental impact. The economic benefits to Richmond far outweigh the environmental concerns (of which some are addressed by the developer) that residents and councillors are putting forth. Are we forgetting about the hundreds of jobs that will be created throughout the life of this project, and thereafter? What else can be done with these vacant, abandoned properties? The taxpaying owners of these properties have spoken: "Develop the land, or let it remain as a vacant eyesore in Richmond, attracting unwanted activity".

SG

Guzzi, Brian

From: MayorandCouncillors
Sent: Wednesday, 25 September 2013 10:40
To: 'Katie Eliot'
Subject: RE: WalMart Mall - Still Too Much Development near the Garden City Lands

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 24, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: Katie Elliot [<mailto:keliot@langara.bc.ca>]
Sent: Tuesday, 24 September 2013 2:42 PM
To: MayorandCouncillors
Subject: WalMart Mall - Still Too Much Development near the Garden City Lands

Dear Mayor and Councillors:

I understand that the Walmart developer will be making yet another presentation to you on Oct 8/13. Once again, the revised plan will not be in Richmond's best interests.

1. From a physical standpoint: Increased traffic congestion, pollution, stress, water runoff, reduced drainage, more heat-trapping structures. None of this is desirable to us as citizens nor to the various creatures still trying to inhabit our municipality. There really is no wildlife corridor remaining in the plan - current wildlife will find their habitat greatly reduced and compromised by more blacktop, buildings, and people.
2. From a sociological view: There are already enough malls in central Richmond. A new Walmart will put more stress on people living and driving in/through the area. The crowding/densification will also reduce liveability and civility.
3. From an economic perspective: Advantages will be outweighed by disadvantages. There will be more civic costs for policing and infrastructure upgrades that won't be covered by developer fees. However the main problem is with less-quantifiable costs such as the loss of natural flood control. The greatest of those costs will be the loss of viewscape from the Garden City Lands across Alderbridge Way. That will be a loss for the City Centre Area forever.

Have you already done future budget projections which include (imagined) revenue from this Walmart project? Did you quantify and subtract the losses to community wellness and tourism? I suggest that the City does not need to augment its budget by including any potential Walmart revenue.

Thank you for considering my views on this subject.

Sincerely,
Katie Eliot
Longtime West Richmond resident

Guzzi, Brian

From: MayorandCouncillors
Sent: Friday, 27 September 2013 15:00
To: 'john terborg'
Subject: RE: Alderbridge Wildlife Corridor

Follow Up Flag: Follow up
Flag Status: Flagged

This is to acknowledge and thank you for your email of September 26, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson

Manager, Legislative Services

City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1

Phone: 604-276-4006 | Email: mjansson@richmond.ca

From: john terborg [mailto:john_terborg@hotmail.com]

Sent: Thursday, 26 September 2013 9:08 PM

To: MayorandCouncillors

Subject: Alderbridge Wildlife Corridor

Hello council members, especially the planning committee,

I spoke at the most-recent planning committee meeting about the proposed Walmart development and the loss of Environmentally Sensitive Areas. I now wish to provide some additional information to support your decision making (slide presentation attached).

For decades the location that became the north edge of Alderbridge Way has been shown on City maps as a strip of ESA. This has been included in official documents (including Richmond's 2005 State of the Environment Report) and online well into 2012; in fact, the GIS map showed ESA status for every lot on the north edge of Alderbridge from Garden City Road to Number Four Road. This status predates by many years the applicant's purchase of the property and subsequent development application. Also, the development application predates any change in ESA status in the OCP, and it has been pointed out by others that the long-standing ESA status continues to apply.

To me the most important factor is that the ESA strip is protecting sensitive mixed urban forest habitat. For that basic reason, in addition to the ESA status, it is very important that it be enforced. There are many positive benefits to the community.

If the current proposal is left unchallenged, Richmond will lose some unique environmentally sensitive areas. This ESA area has even greater relative value because of its accessibility and proximity to such a large number of urban residents living in and near the City Centre. Green spaces are already at a premium in the centre of Richmond.

I ask that you please require any development to include an appropriate restoration and enhancement approach for the wildlife corridor which also happens (very significantly too) to preserve the panoramic legacy views from the Garden City Lands area.

Thank you,

John ter Borg

Guzzi, Brian

From: Craig, Wayne
Sent: Monday, 30 September 2013 16:22
To: Guzzi, Brian
Subject: FW: WalMart Proposal Oct 8 - Still Not Suitable

Follow Up Flag: Follow up
Flag Status: Flagged

Another letter RE:smartcentres

-----Original Message-----

From: MayorandCouncillors
Sent: Monday, 30 September 2013 04:17 PM
To: 'Bonnie Eliot'
Subject: RE: WalMart Proposal Oct 8 - Still Not Suitable

This is to acknowledge and thank you for your email of September 30, 2013 to the Mayor and Councillors, in connection with the above matter, a copy of which has been forwarded to the Mayor and each Councillor for their information.

In addition, your email has been referred to Wayne Craig, Director of Development for response. If you have any questions or further concerns at this time, please call Mr. Craig at 604.276.4000.

Thank you again for taking the time to make your views known.

Yours truly,

Michelle Jansson
Manager, Legislative Services
City of Richmond, 6911 No. 3 Road, Richmond, BC V6Y 2C1
Phone: 604-276-4006 | Email: mjansson@richmond.ca

-----Original Message-----

From: Bonnie Eliot [<mailto:bonnieeliot@shaw.ca>]
Sent: Monday, 30 September 2013 11:45 AM
To: MayorandCouncillors
Subject: WalMart Proposal Oct 8 - Still Not Suitable

Good morning Mr Mayor and Councillors,

Once again I am writing to you to express my continued opposition to any development of the proposed Walmart site north of the Garden City Lands.

After the downpour we just had on the weekend, it's so important to retain as many natural areas in Richmond as possible. More tarmac for buildings, increased load on drainage & sewage, and more roads/parking lots are certainly not sustainable planning.

These weather episodes are increasing and it's not in anyone's best interest to try and downplay this fact.

I have lived in Richmond most of my life and find that this city is still a good place to live. But increased traffic, bigger houses taking more energy, more paving and less greenery to drain water and clean the air -- all this development does have a tipping point. Walmart is the tipping point here.

Letters to the Editor in our local weekly papers all point out that we don't need more shopping - Target will be opening soon and we already have Aberdeen, Yaohan, Lansdowne, Richmond Centre, and all the strip malls squeezed in-between.

Enough is enough!
Sincerely,
Mrs Bonnie Eliot

Kathleen Beaumont
6415 London Road
Richmond,
BC V7E 6V5

Re: RZ 10-528877, Walmart Development Proposal

Dear Mr Guzzi

I am writing this letter to voice my concerns about the rezoning application RZ 10-52887 which I would like included in the review process.

It seems somewhat cliché to be writing a citizens letter in response to a proposed Walmart development in Richmond but in fact the current debate has little to do with the brand and more to do with the location.

The site being proposed for Walmart is a wild life corridor, which amongst many things serves as a roosting area for large flocks of crows. It is also one of the last remaining mature treed areas in what was once a charming enclave of small family farms.

City staff may be content with the fact that this proposal has met all the technical criteria and that they will be handsomely compensated for any tree removal or additional road and utility services. This alone is no reason to approve a new development of any kind. A much broader view has to be taken on this issue, which takes into account the bigger picture and the impact it will have on Richmond as an evolving city. We can't go on expanding the urban boundaries with the view that it will generate monetary benefits and compensation therefore its acceptable.

In recent years, city has amassed huge reserves from previous developers who have paid compensation for the removal of trees and vegetation. In the absence of a long-term plan for the administration of a comprehensive tree program for the city, these funds have accumulated and little is being done with them. We are definitely not short of a few dollars for new trees. So this little bylaw doesn't carry much clout. A mature green space with existing trees is of more value to the citizens of Richmond than payouts to a tree fund or road development fund.

One doesn't have to go very far to see where the real opportunities exist for retail development in Richmond. A short ride on the Canada line to the north east quadrant of No 3rd provides the rider with birds-eye view of some of Richmond's oldest and unsightly developments. From the train the area look like a shantytown of flat roofs dotted with rusting utilities begging for redevelopment. This is where the city should be providing much needed incentives for the removal and upgrade of existing commercial retail and commercial property.

Moving the commercial enterprise further east is not going to address the ongoing need for reclamation and upgrade to what is already a sadly deteriorating

commercial area containing some of Richmond's worst commercial building inventory. There is a dire need to address reclamation and redevelopment of the oldest commercial, retail and light industrial malls prior to considering further expansion into virgin territory. Surely the city could partner with Walmart to redevelop this area which is on the Canada line and zones appropriate for Walmarts requirements.

Another suggestion, though probably politically charged, may be to trade a piece of the baron Garden City lands for this ecologically valuable property which would further allow us to retain the mature lands and in so doing provide an alternate location for Walmart.

I have no objection to a Walmart development; from a citizen's perspective, this is just the wrong location

Trusting you will take my comments and the comments of other concerned citizens into account when giving consideration this application.

Regards,

Kathleen Beaumont

Revised Rezoning Considerations



City of
Richmond

Rezoning Considerations

Development Applications Division
6911 No. 3 Road, Richmond, BC V6Y 2C1

Address: 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road

File No.: RZ 10-528877

Prior to final adoption of Zoning Amendment Bylaw 8864, the developer is required to complete the following:

1. Final Adoption of OCP Amendment Bylaws 8865 and 8973.
2. Required Road/Utility Dedications:
 - a) Alexandra/Leslie (Connector Road): Road dedication is required from 9071, 9091 and 9111 Alexandra Road in accordance with **Attachment 2**. The exact dedication is to be as per an acceptable functional design approved by the Director of Transportation.
 - b) Alderbridge Way: Road dedication is required to accommodate the frontage improvements noted in Servicing Agreement Items 16 below to the back of the 3.3m wide shared pedestrian/cyclist path on the north side of the road. Exact dedication to be determined through a road functional design drawing to be prepared by the Developer and to the approval of the Director of Transportation and the Director of Engineering.
 - c) Garden City Road: Road dedication is required to accommodate the frontage improvements noted in Servicing Agreement Item 16 below to the back of the 2.0 wide sidewalk on the east side of the road. Exact dedication to be determined through a road functional design drawing to be prepared by the Developer and to the approval of the Director of Transportation and the Director of Engineering.
 - d) May Drive: A minimum 20m wide road dedication is required between Alderbridge Way and Alexandra Road to accommodate the frontage improvements noted in Servicing Agreement Items 16 below and to the approval of the Director of Transportation and the Director of Engineering.
 - e) High Street: A minimum 22.7m wide road dedication is required between Alderbridge Way and Alexandra Road to accommodate the frontage improvements noted in Servicing Agreement Items 16 below and to the approval of the Director of Transportation and the Director of Engineering.
 - f) Provision of minimum 4m x 4m corner cuts (as dedication) required at all intersections where public roads intersect and approved by the Director of Transportation and the Director of Engineering except in locations where the proposed road geometry requires additional land dedication to ensure that the travel portion of the road and the adjacent sidewalks are within the road right of way to the approval of the Director of Development and the Director of Transportation. The corner cuts to be measured from the "new" property lines.
 - g) Final determination of the exact road dedications and construction requirements are subject to minor revisions as determined by the functional road design and to the approval of the Director of Transportation and Director of Development.
3. Required land transfers:
 - a) A land transfer of proposed "Area J" consisting of approximately 1,280.11 m² (13,779 ft² or 0.32 acres) is required from the Developer to the City as a fee simple lot for park purposes at a nominal cost (i.e., \$10) to the approval of the Manager of Real Estate Services, Director of Transportation and the Director of Development. "Area J" is located at the northeast corner of the development site on the east side of the proposed May Drive alignment (see **Attachment 1**). Final determination of the exact land transfer area is subject to minor revisions as determined by the functional road design, to be confirmed by survey plans and to the approval of the Director of Transportation and Director of Development. A legal agreement will be required for this land transfer.

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4. Required Statutory Rights of Way (SRW's):

- a) Granting of a variable width Statutory Right of Way (SRW) for sidewalk purposes that connects the northeast corner of the Alexandra Road/High Street intersection with the northeast corner of the Alexandra Way/Garden City Road intersection through the west development parcel for the purposes of establishing a public pedestrian walkway referred to as the 'Alexandra Way' pedestrian corridor in the WCAP. This SRW should include:
 - i. A minimum 3.5 m wide sidewalk, within the building setback between the property line and the proposed building façades on both sides of the High Street within the north block,
 - ii. A minimum 3.5 m wide sidewalk, within the west development parcel along the north side of the northerly east-west drive aisle including all necessary and associated pedestrian crossings that traverse parking lot drive aisles;
 - iii. A pedestrian plaza within the west development parcel at the west end of the northerly east-west drive aisle as shown on the Site Plan dated August 29, 2013 including all necessary and associated pedestrian crossings that traverse parking lot drive aisles;
 - iv. A minimum 3.5 m wide sidewalk on the east side of the westerly north-south drive aisle including all necessary and associated pedestrian crossings that traverse parking lot drive aisles within the west development parcel;
 - v. A minimum 3.5 m wide diagonal sidewalk connecting the west development parcel with the Alderbridge Way/Garden City Road intersection including the corner pedestrian plaza; and
 - vi. A reference plan is required to identify this proposed SRW to be confirmed by survey plan and a legal plan for registration in the land title office.

The design of Alexandra Way public pedestrian corridor requires further design development through the Development Permit process. This pedestrian corridor shall include decorative pedestrian and vehicle paving, decorative street lighting and banners, high quality retail signage, street furniture and continuous weather protection, street trees, shrub planting, decorative accent floral planting, high-quality public open spaces along the corridor and periodic focal elements such as public art, special effect night lighting, outdoor cafes/eating areas and/or other attractors and generator of pedestrian traffic and all to the approval of the Director of Development. The construction and maintenance of including liability for the Alexandra Way public pedestrian corridor shall be the responsibility of the Developer.

- b) Granting of an approximately 334.92 m² (3,605 ft² or 0.083 acres) Statutory Right of Way over proposed "Area E" for the purposes of establishing a passive recreation, public open space as a transition to the proposed elevated landscape deck (see item c. below). "Area E" is located at the northeast corner of the development site on the west side of May Drive (see **Attachment 2**). The design of this transition area requires further design development through the Development Permit process. The construction and maintenance costs including the on-going liability for this landscape transition area to the elevated landscape deck shall be the responsibility of the Developer.
- c) Granting of an approximately 3,377.95 m² (36,360 ft² or 0.83 acres) Statutory Right of Way (SRW) over the proposed elevated landscape deck for the purposes of establishing a passive recreation, public open space including the transition areas to the fronting streets (Alexandra Road and May Drive). The elevated landscape deck is located along Alexandra Road at the northeast corner of the proposed development site excluding "Area E" (see **Attachment 1**). A reference plan is required with the appropriate area shaded to identify this proposed SRW to be confirmed by survey plan and a volumetric legal plan to the approval of the Director of Development prior to registration in the land title office. The design of this elevated landscape deck and transition areas requires further design development through the Development Permit process but is intended to be an important feature of the site design including barrier free pedestrian access, multiple entry points including a stair connection to the surface parking lot below, decorative pedestrian paving, lighting, street furniture, numerous seating opportunities with abundant trees, shrub, groundcover and sodded grass planting, an all to the approval of the Director of Development. The construction and maintenance cost including the on-going liability for the elevated landscape deck shall be the responsibility of the Developer.
- d) Final determination of the exact PROP-SRW's and construction requirements are subject to minor revisions as determined by the functional road design and to the approval of the Director of Transportation, Director of Engineering and Director of Development.

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5. Consolidation of the following 20 lots (the table below) in order to create two (2) development parcels (east development parcel and west development parcel) plus "Area J" as identified in Attachment I. The existing dwellings have already been demolished.

Address	PID	Legal Address	Zoning	Owner
4660 Garden City Road	003-491-988	Lot 53 Section 34 Block 5 North Range 6 West New Westminster District Plan 41957	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
4680 Garden City Road	003-522-725	Lot 1 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
4700 Garden City Road	001-985-281	Lot 2 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
4720 Garden City Road	003-640-043	Lot 3 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
4740 Garden City Road	008-141-525	Lot 4 Except: Firstly, Parcel A (Bylaw Plan 73626), Secondly, Part on Plan LMP41468 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9040 Alexandra Road	003-514-889	Lot 54 Section 34 Block 5 North Range 6 West New Westminster District Plan 41957	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9060 Alexandra Road	007-133-138	Lot 37 Section 34 Block 5 North Range 6 West New Westminster District Plan 34867	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9080 Alexandra Road	004-192-141	Lot 38 Section 34 Block 5 North Range 6 West New Westminster District Plan 34867	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9180 Alexandra Road	012-032-476	North 249.3 Feet Lot 3 Except: Parcel "A" (Explanatory Plan 8738), Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9200 Alexandra Road	003-498-433	Parcel "A" (Explanatory Plan 8738), Lot 3 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9260 Alexandra Road	012-032-522	Parcel "One" (Explanatory Plan 9711) Lots 3 and 4 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9280 Alexandra Road	012-032-557	West Half Lot 5 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9320 Alexandra Road	004-079-124	East Half Lot 5 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9340 Alexandra Road	000-868-655	Lot "B" Section 34 Block 5 North Range 6 West New Westminster District Plan 11945	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9380 Alexandra Road	000-556-939	Lot A Section 34 Block 5 North Range 6 West New Westminster District Plan 11945	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9400 Alexandra Road	012-032-573	West Half Lot 7 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.

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Address	PID	Legal Address	Zoning	Owner
8420 Alexandra Road	004-204-662	East Half Lot 7 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9440 Alexandra Road	012-032-581	West Half Lot 8 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9480 Alexandra Road	001-084-372	East Half Lot 8 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9500 Alexandra Road	008-130-990	West Half Lot 9 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.

6. Registration of an aircraft noise indemnity covenant for non-sensitive uses on title.
7. Registration of a flood plain covenant on title identifying a minimum habitable elevation of 2.6 m GSC. The proposed development is designed to 2.6m GSC with the exception of the proposed Walmart loading dock/service area, main lobby including the proposed clinic area and Buildings M and N along Alderbridge Way. The Developer will be required to submit a survey of Alderbridge Way (May Drive to High Street), set these finished floor elevations as high as possible and provide a supportable rationale, which are all subject to the approval of the Manager of Buildings and the Director of Engineering.
8. Registration of a legal agreement on title ensuring that the only means of vehicle access is to Alexandra Road, the proposed High Street and the proposed extension of May Drive and that there be no direct vehicle access to Alderbridge Way or Garden City Road and to the approval of the Director of Development.
9. Registration of a legal agreement that ensures the provision of the following required Transportation Demand Management (TDM) measures to the approval of the Director of Transportation including:
 - a. Bicycle storage (in addition to the bylaw requirements): The Developer to provide a 25% increase in the total number of Class 1 and Class 2 bicycle spaces (i.e. an additional 25 Class 1 stalls and an additional 33 Class 2 stalls);
 - b. Two (2) separate end-of-trip bicycle facilities: The Developer to provide facilities consisting of three (3) water closets per gender, 2 wash basins per gender and 3 showers per gender. Based on the layout of the development, these facilities should be spread out between the western and eastern portions of the site; and
 - c. Electric Vehicle (EV) Plug-ins: The Developer to provide pre-ducting to 10% of the total number of parking stalls provided on-site for future installation of charging stations and designated as such. In addition, and as part of the proposed development, equip a minimum of four (4) parking stalls (i.e., 2 parking stalls on the west development parcel and 2 parking stalls on the east development parcel near the proposed Walmart Store) with EV charging stations (240V).
10. Submission of a voluntary cash contribution of \$3,450,000 to the City for acquisition of 4560/62 and 4580 Garden City Road. This is to be accompanied with a legal agreement, which indicates that the City is not obliged to acquire these properties by any specific date. The City will reimburse the Developer with any surplus funds from their \$3,450,000 contribution for these 2 properties, if there is any residual funding for these lots after all City costs have been paid.
11. Submission of a Letter of Credit (LOC) acceptable to the City, in the amount of for the construction of the Connector Road. The LOC is to be replaced with a cash contribution based on the construction value in the year that the City constructs the Connector Road. The estimated construction value in 2013 is \$2,166,382, which has been escalated by an assumed 4% annual inflation factor to arrive at the estimated construction value of \$3,206,774 in 2023. The LOC is to be accompanied with a legal agreement enabling the City to use the LOC for road construction.

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a. Alexandra/Leslie Connector Road Construction Cost Forecasts are as follows:

Estimated 2012 Construction Cost = \$ 2,083,059
 Forecasted Annual Inflation = 4%
 Forecasted 2013 Construction Cost = \$ 2,166,381
 Forecasted 2023 Construction Cost = \$ 3,206,774

Year	Forecasted Construction Cost	Annual Inflation
2013	\$ 2,166,381	4%
2014	\$ 2,253,037	4%
2015	\$ 2,343,158	4%
2016	\$ 2,436,884	4%
2017	\$ 2,534,360	4%
2018	\$ 2,635,734	4%
2019	\$ 2,741,164	4%
2020	\$ 2,850,810	4%
2021	\$ 2,964,842	4%
2022	\$ 3,083,436	4%
2023	\$ 3,206,774	4%

12. The Developer has voluntarily agreed to incorporate on-site public art installation(s) along the Alexandra Way public pedestrian corridor in accordance with the City's Public Art Policy with an approximate value of \$155,077 (387,692 ft² x \$0.40/ft²) and to the approval of the Director of Development. A letter of credit in the amount of \$155,077 (387,692 ft² x \$0.40/ft²) is a requirement as security for public art to be installed on-site. If the Developer elects not to install on-site public art, then the Developer must agree to voluntarily contribute \$0.40 per buildable square foot or \$155,077 (387,692 ft² x \$0.40/ft²) to the City's public art fund.
13. City acceptance of the Developer's offer to voluntarily contribute \$0.60 per buildable square foot for City Beautification or \$232,615 (i.e. 387,692 ft² x \$0.60/ft²) as part of the City's West Cambie Area – Alexandra Interim Amenity Charges. A reduction to this contribution for the design and construction costs related to the Alexandra Way pedestrian corridor if any is to be determined by the Director of Development.
14. City acceptance of the Developer's offer to voluntarily contribute \$0.07 per buildable square foot for Community and Engineering Planning or \$27,138.44 (i.e. 387,692 ft² x \$0.07/ft²) as part of the City's West Cambie – Alexandra Interim Amenity Charges.
15. City acceptance of the Developer's offer to voluntarily contribute \$90,000 for 3 bus shelters (\$25,000 each for the bus shelter plus \$5,000 each for the bus landing pad) proposed at each of the following locations, if the Developer does not upgrade these bus stop locations through the Servicing Agreement and to the approval of the Director of Transportation:
 - a) north of Alexandra Road on the west side of Garden City Road,
 - b) south of Alderbridge Way on the west side of Garden City Road, and
 - c) south of Odlin Road on the west side of Garden City Road or along Alderbridge Way if TransLink and Coast Mountain Bus Company agree to the necessary bus route revisions. In the event that the necessary bus route revisions are not made by Translink and Coast Mountain Bus company, the location for the bus shelter and landing pad will be pursued elsewhere near the vicinity of the subject site
16. City acceptance of the Developer's offer to voluntarily contribute \$40,600 (116 trees x \$350/each) for tree planting enhancement of "Area J" plus a minimum of 556 (672 – 116) on-site trees and street trees that consist of primarily native species with the majority of proposed tree planting to be larger than the minimum replacement tree planting sizes. This cash contribution will not be eligible for Development Cost Charge (DCC) credits.

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17. City acceptance of the Developer's offer to voluntarily contribute \$11,525 (461 shrubs x \$25/each) for shrub planting enhancement of "Area J" plus a minimum of 6,201 (6,662 - 461) on-site shrubs. This cash contribution will not be eligible for Development Cost Charge (DCC) credits.
18. City acceptance of the Developer's offer to voluntarily contribute \$186,155 (approximately 1 acre x \$186,155/ac based on \$46/m²) for environmental enhancements within the West Cambie Park in order to extend a proposed north-south ecological corridor within the West Cambie Area. This cash contribution will not be eligible for Development Cost Charge (DCC) credits.
19. Registration of a legal agreement(s) regarding the Developer's commitment to connect to the Alexandra District Energy Utility (ADEU), including the operation of and use of the ADEU and all associated obligations and agreement as determined by the Director of Engineering. The Developer has committed that between 63-69% of the proposed floor area or approximately 70% of the total annual heating and cooling energy demand will be serviced by the ADEU but this is subject to Council approval of amendments to the ADEU bylaw to allow less than 70% participation. However, participation in the ADEU will be limited to the large format tenants (Buildings A and the East Anchor Building - Walmart Store). More detailed energy modeling will be required to establish the extent of the energy demand represented by those tenants. The Developer will coordinate with Engineering staff to determine this demand as part of the Servicing Agreement process.
20. Processing of a Development Permit advanced to a sufficient level of detailed design and to the approval of the Director of Development.
21. Enter into a Servicing Agreement* for the design and construction of road improvements and site servicing. Works include, but may not be limited to the following:


A. Transportation & Traffic Requirements

1. Applicant responsible for the design and construction of the following frontage improvements and transition between those improvements and the existing condition outside the development site frontage (at a minimum 30:1 taper rate for Alderbridge Way and Garden City Road, and a minimum 20:1 taper rate for all internal roads) to the approval of the City. Please refer to Item 2 for additional frontage improvements at intersections. Note that while Servicing Agreement Items A1 and A2 provide a general description of the minimum frontage work requirements, the exact details and scope of the frontage works to be completed by the Developer would be confirmed via a functional road design to be prepared by the Developer and to the approval of the Director of Development, the Director of Transportation and the Director of Engineering.
 - a) Alderbridge Way, from Garden City Road to May Drive (from south to north):
 - enhance existing medians with decorative/gateway treatments, including but not limited to banners, landscaping, trees, hard landscaping, street lighting, etc.;
 - maintain two existing westbound traffic lanes;
 - maintain existing curb/gutter on the north side;
 - 1.5 m wide treed boulevard; and
 - 3.3 m wide shared pedestrian/cyclist path.
 - b) Garden City Road, from Alderbridge Way to Alexandra Road (from west to east):
 - enhance existing medians with decorative/gateway treatments, including but not limited to banners, landscaping, trees, hard landscaping, street lighting, etc.;
 - maintain two existing northbound traffic lanes;
 - shift the existing northbound bicycle lane allowance onto the east boulevard;
 - 0.15 m wide curb/gutter;
 - 1.85 m wide treed boulevard;
 - 2.0 m wide bike lane;
 - 1.77 m wide grass buffer strip to separate sidewalk and bike lane;
 - 2.0 m wide sidewalk at property line; and
 - minimum 3.0 m wide building setback from property line (west to east) sloped and landscaped with dense plant material to the proposed building wall with perpendicular walkway connections to the public sidewalk including stairs or ramps from the required emergency exit doors along the back of the building as required by code; and

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- c) Alexandra Road, from Garden City Road to eastern limit of the development site (from south to north):
- 2.0 m wide sidewalk;
 - 1.5 m wide boulevard;
 - 0.15 m wide curb;
 - minimum 9 m wide vehicular driving/parking surface (this pavement may be reduced to min. 6.2 m at mid-block locations where feasible); and
 - minimum 1.0 m wide shoulder.
- d) May Drive, from Alderbridge Way to Alexandra Road (from west to east):
- 2.0 m wide sidewalk;
 - 1.5 m wide boulevard;
 - 0.15 m wide curb;
 - 12.7 m wide vehicular driving/parking surface;
 - 0.15 m wide curb,
 - 1.5 m wide boulevard; and
 - 2.0 m wide sidewalk.
- e) High Street, from Alderbridge Way to Alexandra Road (from west to east):
- 2.0 m wide sidewalk;
 - 0.15 m wide curb;
 - 18.4 m wide vehicular driving/parking surface (i.e., 2 x 2.5 m wide parking lane or landscaped boulevard near intersections, 4 x 3.35 m wide traffic lanes);
 - 0.15 m wide curb; and
 - 2.0m wide sidewalk.
2. In addition to the frontage improvements noted in Item 1, the Developer is responsible for the design and construction of the following intersection improvements and to the approval of the Director of Transportation and the Director of Engineering.
- a) Alderbridge Way / May Drive
- Installation of a new traffic signal to include but not limited to the followings: signal pole, controller, base, hardware, pole base (City Centre decorative pole and street light fixture), detection, conduits (electrical and communications), signal indications, communications cable, electrical wiring and service conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).
 - Installation of an eastbound to northbound left-turn lane, with a minimum storage length of 60 m. Please note that while a portion of the left-turn lane may be accommodated within existing median, a minimum 1.2m wide median should be maintained.
- b) Alderbridge Way / High Street
- Installation of a new traffic signal to include but not limited to the followings: signal pole, controller, base, hardware, pole base (City Centre decorative pole and street light fixture), detection, conduits (electrical and communications), signal indications, communications cable, electrical wiring and service conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).
 - Installation of an eastbound to northbound left-turn lane, with a minimum storage length of 60 m. Please note that while a portion of the left-turn lane may be accommodated within the existing median, a minimum 1.2 m wide median should be maintained.
 - Installation of a westbound to northbound right-turn lane, with a minimum storage length of 50 m, while maintaining the two westbound through lanes as noted in Item A1a) above.
- c) Alderbridge Way / Garden City Road
- Upgrade of the existing traffic signal to include but not limited to the followings: signal pole, controller, base, hardware, pole base (City Centre decorative pole and street light fixture), detection, conduits (electrical and communications), signal indications, communications cable, electrical wiring and service conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).

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- Installation of dual left-turn lanes on the southbound (a minimum total storage length of 200 m), northbound (a minimum total storage length of 200 m) and westbound approaches (a minimum total storage length of 190 m), while maintaining all other existing traffic lanes. Please note that while a portion of the left-turn lanes may be accommodated within existing medians, a minimum 1.2 m wide median should be maintained on all intersection approaches.
 - Installation of a westbound to northbound right-turn lane, with a minimum storage length of 50 m, while maintaining the two westbound through lanes as noted in Item A1a) above.
 - Provision of an acceptable transition between the above noted intersection improvements, the proposed boulevard treatment and the building setback landscape design (at a minimum 30:1 taper rate for Alderbridge Way and Garden City Road) to the approval of the Director of Development and the Director of Transportation.
 - Installation of an accessible bus shelter and landing pad (9 m x 3 m) on the east side of Garden City Road, just north of Alderbridge Way.
- d) Garden City Road / Alexandra Road
- Closure of existing median opening with curb/gutter and decorative median treatments.
- e) Installation of special crosswalks with downward lighting and associated equipments at the following locations:
- High Street, at the proposed access to the development site;
 - High Street, at Alexandra Road; and
 - Alexandra Road, at High Street.
- f) Construction Timing: all frontage improvements should be completed prior to opening of development.
3. All Transportation requirements shall be as per City requirements and approved by the Director of Transportation.

B. Site Servicing & Connection Requirements

1. Storm: Required storm sewer improvements include the following:
 - a) All storm drainage must be directed to Alexandra Road and west to Garden City Road except for road run-off from the south half of High Street and May Drive that may be drained to Alderbridge Way;
 - b) Provide a 600mm diameter storm sewer from existing manhole (manhole D26 in the analysis) located at the intersection of Alderbridge Way and future May Drive to proposed manhole D4 located at the intersection of Alexandra Road and future May Drive with an approximate length of 170m;
 - c) Provide a 600mm diameter storm sewer from existing manhole (manhole D29 in the analysis) located at the intersection of Alderbridge Way and future High Street to proposed manhole D6+ located at the intersection of Alexandra Road and future High Street with an approximate length of 170m;
 - d) Upgrade the existing ditch at Alexandra Road to a 600mm diameter storm main from intersection of Alexandra Road and future May Drive (manhole D4 in the analysis) west to manhole D5 with an approximate length of 100m;
 - e) Upgrade the existing ditch at Alexandra Road to a 900mm diameter storm main from manhole D5 west to manhole D6 with an approximate length of 100m;
 - f) Upgrade the existing ditch at Alexandra Road to a 900mm diameter storm main from manhole D6 west to manhole D8 with an approximate length of 50m;
 - g) Upgrade the existing ditch at Alexandra Road to a 1050mm diameter storm main from manhole D8 west to manhole D15 with an approximate length of 95m;
 - h) Upgrade the existing ditch at Alexandra Road to a 1050mm diameter storm main from manhole D15 west to manhole D16 with an approximate length of 80m; and

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- i) A new tie-in will be required to convey flow from the proposed drainage system in Alexandra Road to the existing 1200mm diameter storm sewer located at the west side of Garden City. Details and location of the new crossing/tie-in will be determined via the Servicing Agreement and to the approval of the Director of Engineering.
2. Sanitary: Required sanitary sewer improvements include the following:
- a) Alexandra Road: Construct a 250mm diameter sanitary sewer from the proposed transition point (west of Dubbert Street) to May Drive;
May Drive (future road extension): Construct a 375mm diameter sanitary sewer from Alexandra Rd to Tomicki Ave and connect to the existing system. If a road dedication does not exist, then a minimum 6.0 m wide right-of-way will be required. The pipe sizes may be revised at the Servicing Agreement stage as additional information becomes available for the servicing requirements of the proposed adjacent developments; and
- b) High Street: Construct a 200mm diameter sanitary sewer and connect to the system on Alexandra Road. The upstream end of the sanitary sewer will be determined by the location of the service connection for this development.
- c) Provision of a minimum 6.0 m wide utility Statutory Right-of-Way (SRW) is required for the proposed sanitary sewer at future May Drive between Alexandra Road and Tomicki Avenue. The location of the required SRW is to be determined later either within 9451 and 9471 Alexandra Road and measured 6.0 m from the east property lines of these 2 properties or located on 9491 Alexandra Road and measured 6.0 m from the west property line.
3. Water: Required water service improvements include the following:
- a) Using the OCP 2021 Maximum Day Model, there is 717.10 L/s available at 20 psi residual at 4740 Garden City Road, 529.40 L/s at 20 psi residual at 4600 Garden City Road and 220.50 L/s at 20 psi residual at 9411 Alexandra Road;
- b) Based on the proposed rezoning, the site requires a minimum fire flow of 200 L/s;
- c) Water analysis is not required to determine upgrades to achieve minimum requirements;
- d) Once the building design is confirmed at the Building Permit stage, the Developer is required to submit fire flow calculations signed and sealed by a professional engineer based on the Fire Underwriter Survey or ISO Standards to confirm that there is adequate available flow;
- e) A new watermain is required on Alexandra Road, High Street and May Drive along the development frontages (design to be via the servicing agreement); and
- f) Via the Servicing Agreement the City will review the impact of the proposed works on the existing 300mm diameter asbestos-cement (AC) watermain on Garden City Road. The City will work with the Developer to coordinate the replacement/relocation of the AC watermain, if required.
4. Hydro/Telephone: Pre-ducting works are required on the following proposed roads subject to confirmation from BC Hydro and telecom providers:
- a) proposed May Drive (from Alderbridge Way to Alexandra Road); and
- b) proposed High Street (from Alderbridge way to Alexandra Road).
- The removal of existing power poles and installation of underground pre-duct along the east side of Garden City Road and along the north side of Alexandra Road will be at the discretion of BC Hydro.
5. All servicing infrastructure works shall be as per City requirements and to the approval of the Director of Engineering.
6. The Developer is required to contact private utility companies to learn of their requirements; the developer must provide rights-of-ways to accommodate their equipment (kiosks, vista, transformers, etc.) on the development site (i.e. not within City road dedication or right-of-way), subject to concurrence from the private utility companies.

Initial: 

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Prior to a Development Permit* being forwarded to the Development Permit Panel for consideration, the developer is required to:

1. Prior to issuance of a tree cutting permit after Public Hearing, the developer is required to submit a letter of credit acceptable to the City in the amount of \$192,000 (i.e., 344 replacement trees x \$500 each plus 3 significant trees and 1 high value tree x \$5,000 each) as security for replacement trees.
2. Further design development of the architectural facade designs, site planning and landscape design are required at the Development Permit stage. Advance the concept design and submit more detailed design drawings to ensure the establishment of a compact, vibrant, pedestrian oriented, urban village centre that will become the retail/commercial heart of the Alexandra Neighbourhood and to the approval of the Director of Development.
3. Expand the design concept and submit more detailed design drawings to ensure the creation of an attractive, accessible, activated, comfortable, pedestrian-friendly retail/commercial environment with strong pedestrian scale streetwall definition, the possibility for restaurants/shops to extend out toward the back of sidewalk including numerous small shops plus an interesting mix and variety of retail shopping opportunities along the High Street and to the approval of the Director of Development.
4. Improve the concept design and submit more detailed design drawings to ensure the continuation of the Alexandra Way pedestrian corridor through the proposed development with high-quality pedestrian enhancements, punctuated with periodic pedestrian plaza areas, activated to attract pedestrian traffic and facilitate seasonal events, designed with ample pedestrian space and opportunities to encourage pedestrians to sit/linger and incorporating other features such as public art and focal elements that add interest and variety to the pedestrian experience and to the approval of the Director of Development.
5. Neighbourhood Plan, Design Guidelines Compliance and Urban Design Improvements: Proposed deviations from WCAP neighbourhood structure and design guidelines can be dealt with at the Development Permit stage. Urban design improvements required at the Development Permit stage include advancing the concept design and resubmission of more detailed design drawings to ensure:
 - the establishment of a compact, vibrant, pedestrian oriented, urban village centre that is integral part of the neighbourhood and will become the retail/commercial heart of the Alexandra Neighbourhood;
 - an attractive, accessible, activated, comfortable, pedestrian-friendly retail/commercial environment with strong pedestrian scale streetwall definition, the possibility for restaurants/shops to extend out to the back of sidewalk including numerous small neighbourhood scale character shops plus an interesting mix and variety of retail shopping opportunities along the High Street;
 - a higher quality architectural expression around the entire perimeter of the development site by extending the signature corner treatments (e.g. Alderbridge Way and Garden City Road) further along the building faces on all perimeter building facades including greater horizontal articulation and permeability of perimeter building facades to add more visual interest through enhanced architectural character and an appropriate proportion of transparent and opaque combination of surfaces for the proposed buildings that face the perimeter streets around the exterior of the proposed development;
 - the strong presence and continuation of the Alexandra Way pedestrian corridor, and neighbourhood pedestrian spine, through the proposed development with high-quality pavements and contrasting colours that identify the direction of Alexandra Way to and from the neighbourhood to the urban plaza at the corner of Garden City Road and Alderbridge Way. The Alexandra Way pedestrian corridor should be punctuated with periodic pedestrian plaza areas and pedestrian amenities to activate and attract pedestrian traffic and facilitate seasonal events, designed with ample pedestrian space and focused on creating opportunities to encourage pedestrians to sit and linger. The plaza spaces should incorporate other features such as public art and focal elements that add interest and variety to the pedestrian experience. The ground plane paving treatment along the Alexandra Way pedestrian corridor through the proposed development site should include a distinctive and continuous decorative paving treatment extending from building face to building face (along this route through the proposed development) with significant differentiation between the Alexandra Way corridor and other the other internal streets and sidewalks within the overall development;

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- a reduction in the amount of signage that is coordinated with the proposed floor plans including the better integration or elimination of redundant signage such as the proposed "Directional Signage" pylons and stronger coordination with the enhanced architectural character of proposed buildings at corner locations;
 - safe and efficient pedestrian movement that reflects the direction of the pedestrian traffic toward the Walmart store within the parking area including consideration of east-west oriented parking aisles within the open parking area on the east development parcel with wider bio-swales;
 - better coordination between the landscape and architectural design, including a stronger reliance on the informal clustering of large coniferous tree planting around the perimeter of the proposed development site to enhance the massing and materials articulation/treatment of the building facades, particularly along the Alderbridge Way frontage that is visible from the Garden City Lands to the south; and
 - acceptable resolution of any non-compliance with all relevant design guidelines.
6. Provision of adequate and appropriate refuse and recycling facilities for each building to and to the approval of the Director of Development and the Director of Public Works.
 7. CPTED: All parkade areas (walls, columns and ceiling) to be painted with reflective white paint and come with lighting levels as required by the BC Building Code. The open parking areas will be well lit with fixtures providing good colour rendition. A complete and comprehensive CPTED review of the development will be provided with the Development Permit submission.
 8. Submission of a landscape plan prepared by a BCSLA registered landscape architect to the approval of the Director of Development including the deposit of a landscape security based on 100% of the cost estimate provided by the landscape architect. The Landscape Plan should:
 - comply with the OCP guidelines regarding Lane Establishment and Arterial Road Redevelopment Policies and should not include hedges along the front property line;
 - include a mix of coniferous and deciduous trees;
 - provide 4 large specimen trees on the planting plan to replace the existing 3 significant trees plus 1 high value tree proposed for removal; and
 - include the 344 (172 x 2) required replacement trees with the following minimum sizes:

No. of Replacement Trees	Minimum Caliper of Deciduous Tree	or	Minimum Height of Coniferous Tree
344	10 cm caliper		3.5 m height

If required replacement trees cannot be accommodated on-site, a cash-in-lieu contribution in the amount of \$500/tree to the City's Tree Compensation Fund for off-site planting is required or \$5,000 each for significant or high value trees not provided on site.

Prior to Building Permit Issuance, the developer must complete the following requirements:

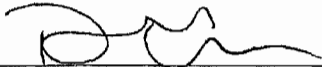
1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Division. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570 and approved by of the Director of Transportation.
2. Payment of the Supplementary Local Area DCC for the Alexandra Neighbourhood.
3. Payment of the \$480,738.08 (387,692 ft² x \$1.24/ft²) indexed at the applicable rate, in accordance with the Alexandra Neighbourhood Development Agreement.
4. If applicable, payment of latecomer agreement charges associated with eligible latecomer works.
5. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Division at 604-276-4285.

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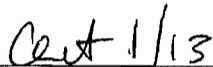
- 12 of 14-

Notes:

- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.
- All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.
- The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.
- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the approval of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.



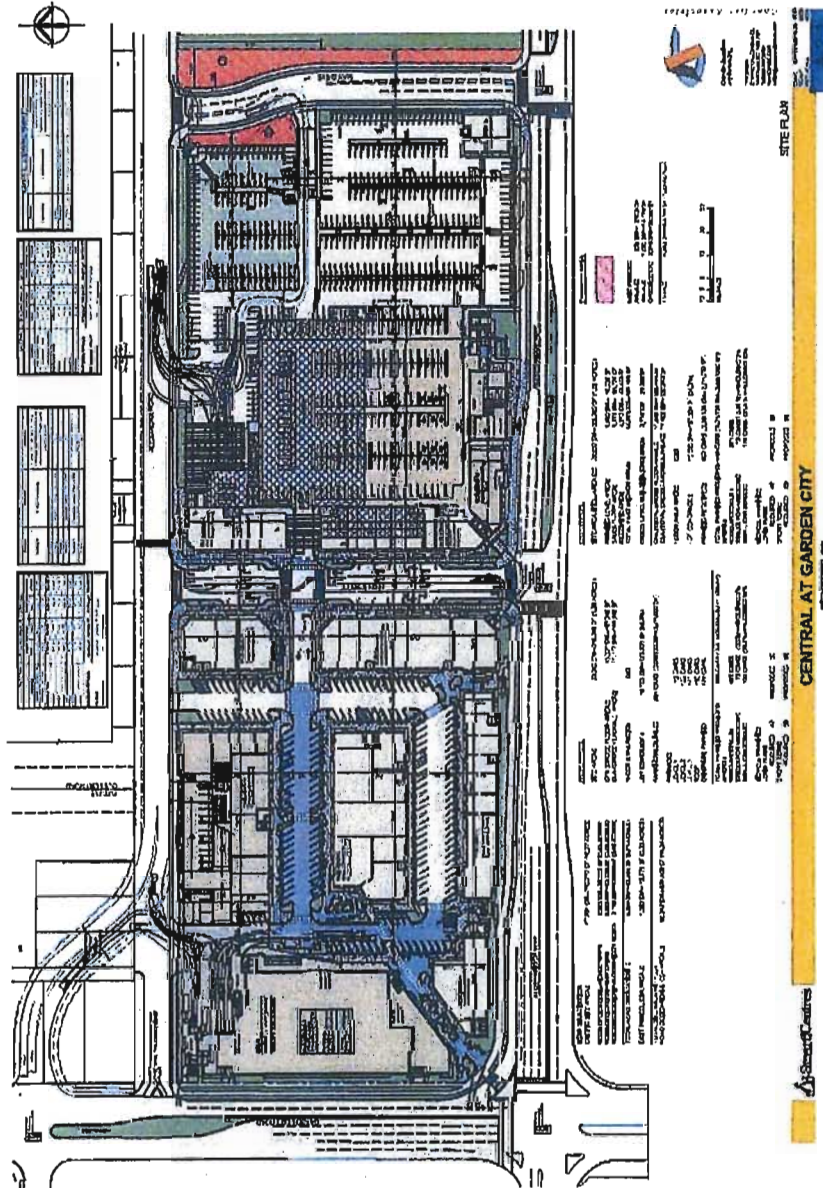
Signed



Date

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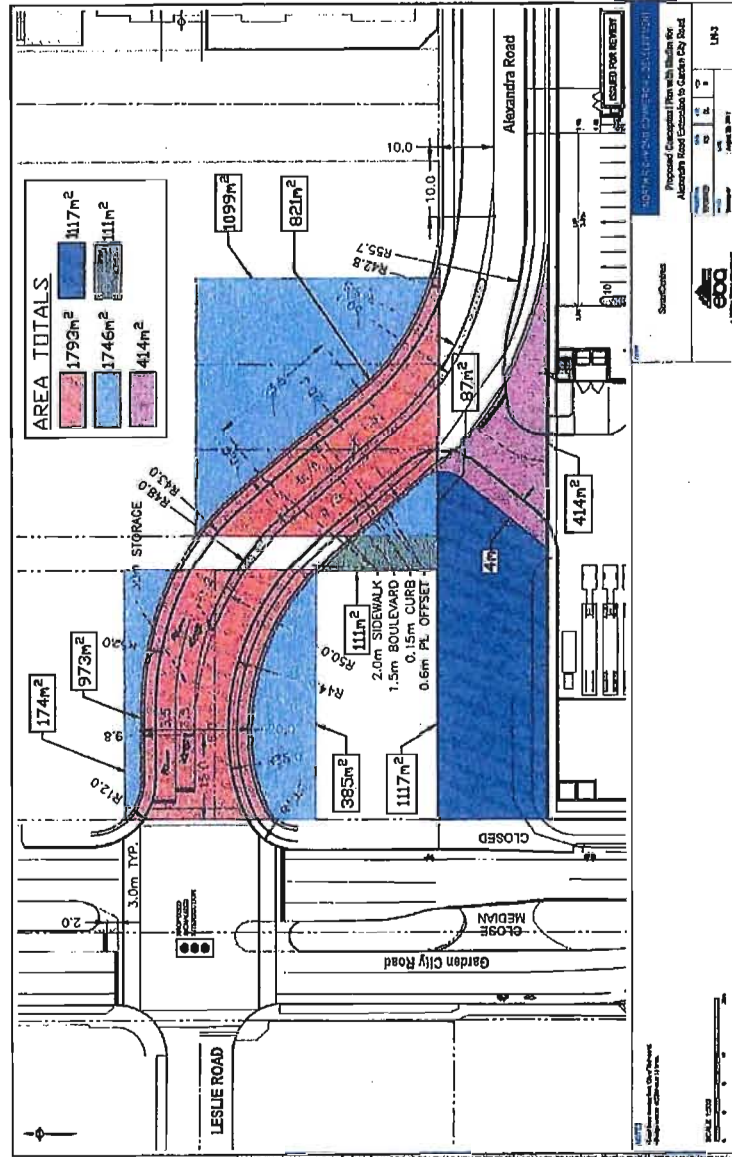
Revised Concept Site Plan (September 26, 2013)



Initial: JS

Attachment 2

Alexandra/Leslie Connector Road Realignment & Land Requirements





To: Planning Committee **Date:** October 2, 2013
From: Victor Wei, P. Eng. **File:** RZ 10-528877
Director, Transportation
**Re: Rezoning Application by First Richmond North Shopping Centres Ltd. –
Response to September 17, 2013 Referral on Transportation-Related Items**

Following the consideration of the rezoning application by First Richmond North Shopping Centres Ltd at the September 17, 2013 meeting of Planning Committee, staff were directed to report back at the Committee's next scheduled meeting with further information on various topics. This memorandum responds to the following transportation-related items of the referral:

- (2) details of the traffic study, e.g. projections on (i) number of people living in the area; (ii) volume of people going into the development; (iii) ingress to and egress from the development including: Alderbridge Way, Garden City Road, No. 4 Road, and Cambie Road;*
- (4) rationalization of staff's position that the Connector Road will not be needed in 10 years;*
- (i) provision of the traffic study on the proposed development;*

The remaining referral items (i.e., Items 1, 3, 5, 6, and (ii) through (iv)) are addressed in separate memoranda.

1. Traffic Impact Assessment (TIA) Projections

1.1 Number of People Living in the Area

Based on 2011 Census data, the Alexandra Neighbourhood (bounded by Alderbridge Way, Cambie Road, Garden City Road, and No. 4 Road) had 580 dwelling units and a population of 1,465. In addition to accounting for background traffic growth at three horizon years (2012, 2017 and 2022), sensitivity analysis undertaken as part of the TIA also incorporated the future levels of new development in the area envisaged in the West Cambie Area Plan (WCAP). Based on the guidelines of the WCAP and a previous traffic study undertaken for a multi-family residential development at Cambie Road-Stolberg Street, the TIA assumed a total of 3,210 additional dwelling units for the Alexandra Neighbourhood at full build-out for the 2022 horizon year. Based on estimates provided by Policy Planning, the residential areas of the Alexandra Neighbourhood could accommodate approximately 3,070 dwelling units with an estimated population of 6,750 by 2023. Therefore, the recommended road improvements resulting from the traffic analysis can accommodate a marginally higher build-out population in West Cambie than what the area plan envisages.

1.2 Traffic Volumes to and from Proposed Development

In assessing traffic impacts from new developments, the projected volume of vehicles (as opposed to volume of people) is typically used as standard measure to determine the required transportation improvements. To convert vehicle trips to number of people, an average vehicle occupancy of 1.2 to 2.5 can be assumed for weekdays and weekends respectively. Table 1 identifies the projected number of vehicular trips that will be generated at opening day based on a development of 36,018 m² (387,692 ft²) gross floor area, which is consistent with the current proposal. Overall, the development is projected to generate two-way vehicle trips of approximately 300 in the weekday morning peak hour, 1,355 in the weekday afternoon peak hour and 1,780 in a Saturday afternoon peak hour.

Table 1: Projected 2-Way Trips

Time Period	Total Trips
AM Peak Hour	298
PM Peak Hour	1,353
Sat Peak Hour	1,779

1.3 Ingress to and Egress from the Proposed Development

Table 2 lists and **Attachment 1** illustrates the percentage distribution of vehicle trips travelling to the site from the arterial roadway system. The projected volumes of vehicle trips to the only three access points to the site on opening day are:

- Garden City Road at Alexandra Road: unsignalized right-in/right-out movement only would serve 16 per cent of site traffic;
- Alderbridge Way at High Street: full movement at new signalized access east of Garden City Road would serve 49 per cent of site traffic; and
- Alderbridge Way at May Drive: full movement at new signalized access east of High Street would serve 35 per cent of site traffic.

It is estimated that the only external traffic generated by the development would be from Vancouver which would consist of approximately 15% of the total site traffic. These trips are distributed as noted in Table 2. With the Tsawwassen First Nations retail development, which is five times bigger than this development and most likely to proceed, it is expected that the site would generate negligible amount of traffic from Delta.

Table 2: Distribution of Trips

Approach	% of Trips (% Vancouver Trips)
Garden City Road north of Cambie Road	15% (9%)
No. 4 Road north of Cambie Road	2.5% (1.5%)
Cambie Road east of No. 4 Road	2.5% (1.5%)
Cambie Road west of Garden City Road	5% (3%)
Alderbridge Way east of No. 4 Road	10%
No. 4 Road south of Alderbridge Way	25%
Garden City Road south of Westminster Highway	25%
Lansdowne Road west of Garden City Road	5%
Alderbridge Way west of Kwantlen Gate	10%
Total	100% (15%)

In addition, based on the recommended road improvement plan for the proposed development, it is anticipated that within ten years of opening of the proposed development, Alexandra Road will be realigned to create a full-movement signalized intersection at Garden City Road and Leslie Road facilitating all turning movements. This future signalized intersection is expected to divert 10 per cent of the site access volumes from the two Alderbridge Way access points to this location.

2. Future Timing of Connector Road

Given the challenge experienced by the applicant in acquiring all of the necessary properties to implement the connector road (re-aligned Alexandra Road) on opening day of the proposed development, the applicant's traffic consultant and staff jointly identified an interim alternative for up to ten years. This alternative involves a number of intersection improvements at Garden City Road and Alderbridge Way designed to provide equivalent traffic capacity to accommodate the additional vehicular trips generated by the proposed development for up to ten-years after opening day. Specifically, these intersection improvements are:

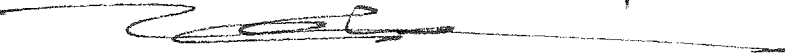
- additional westbound left-turn lane on Alderbridge Way (resulting in double left-turn lanes);
- additional northbound left-turn lane on Garden City Road (resulting in double left-turn lanes);
- additional southbound left-turn lane on Garden City Road (resulting in double left-turn lanes); and
- new exclusive westbound to northbound right-turn lane on Alderbridge Way.

Based on staff's analysis, the implementation of these improvements would be adequate in accommodating the projected traffic volumes for a period of 10 years, after which the connector road would be required to accommodate on-going growth.

3. Traffic Impact Assessment of Proposed Development

The original traffic impact study for the development is a 50+ page document that was subsequently revised several times by the applicant's consultant in response to staff comments to arrive at the current proposed traffic plan supported by staff. As these are technical documents, for ease of comprehension, **Attachment 2** is a summary of the key findings with respect to site traffic and access as well as the recommended intersection improvements extracted from this original version (dated July 2011) of the TIA prepared by the applicant's consultants. It should be noted that the final recommended traffic improvements in the staff report presented at the September 17, 2013 Planning Committee consist of additional intersection improvements at Garden City Road/Alderbridge Way, as outlined in Section 2, which are not listed in the attachment.

If you have any questions or would like further information regarding the above, please contact me at 604-276-4131.



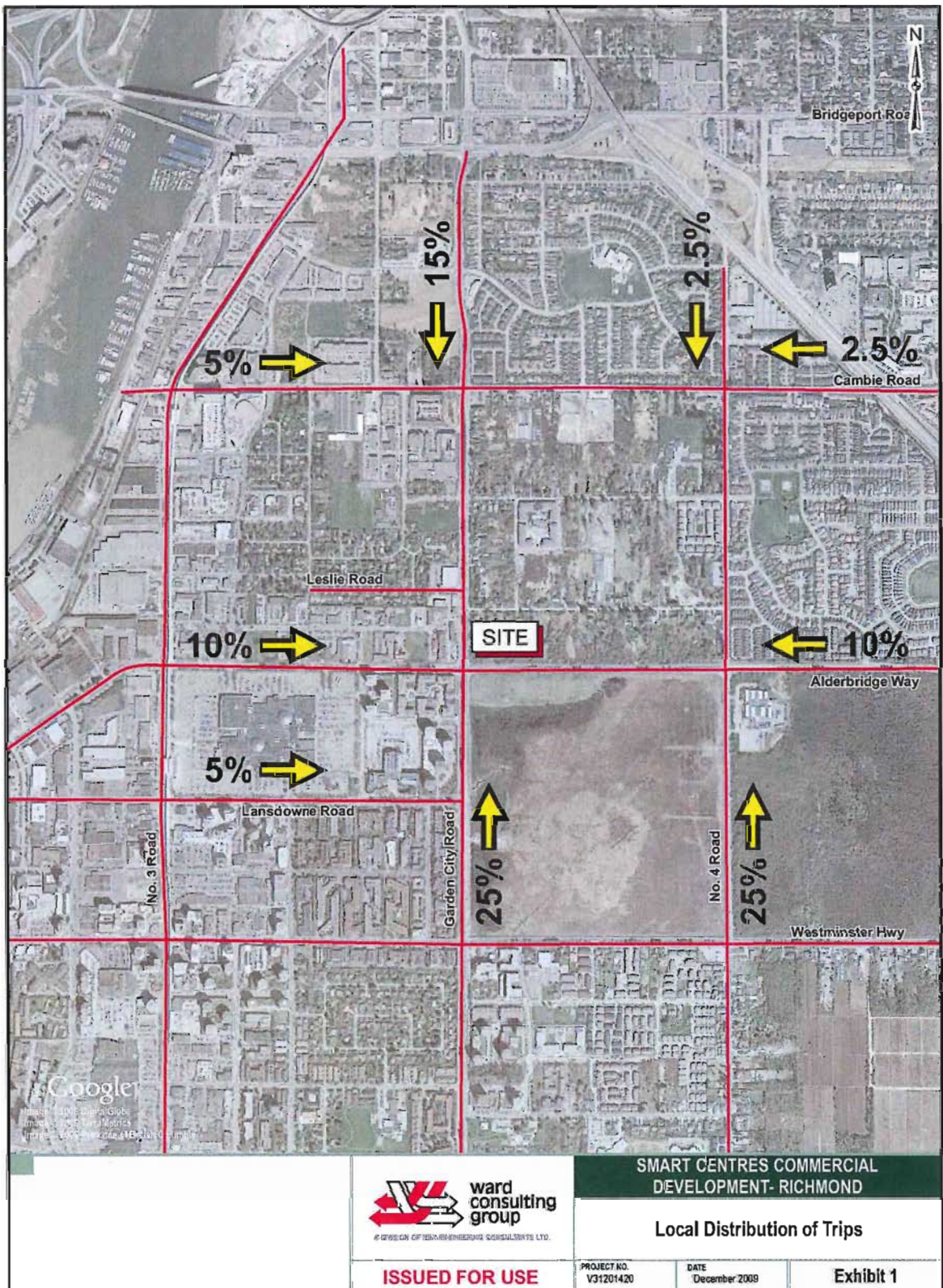
Victor Wei, P. Eng.
Director, Transportation

Att. 2

VW:jc

pc: SMT

Local Distribution of Trips to the Development



**Summary of Key Findings of Traffic Impact Assessment
(Extracted from SmartCentre's Traffic Consultant Report Dated July 2011)**

Site Traffic

- The site will have access via Alderbridge Way (two signalized accesses) and Garden City Road.
- Ultimately, access to Garden City Road will be provided by an extension of Leslie Road to the east (High Street) as given in the Cambie West Area Plan. With this extension, the intersection of Leslie Road and Garden City Road will form a regular four-legged intersection under signal control.
- Traffic generation for the site was estimated using standard trip rates. These were adjusted by 5% to account for the high potential of non-auto traffic for the site. A subsequent sensitivity analysis indicated that, had this 5% reduction not been included, the conclusions and recommendations would have been the same.
- Overall, the site is estimated to generate in the order of 1, 350 vehicle trips two-way in the weekday p.m. peak and 1,780 vehicle trips in the Saturday afternoon peak hour. Trip generation during the weekday a.m. peak hour will be considerably less at 300 vehicle trips two-way.
- Allowing for pass-by trips, i.e., trips already on the road network that divert into the site, the net new trips are projected to be approximately 1,015 vehicle trips during the p.m. peak hour and 1,600 trips two-way during the Saturday afternoon peak hour.
- Site traffic will be distributed across nine possible approach routes. The majority of site traffic is expected to be oriented to the south of the site via No. 4 Road south of Alderbridge Way and Garden City Road south of Westminster Highway.
- Less than 20% of site traffic is expected to be oriented to and from the north. Based on SmartCentres' market study, this percentage oriented to the north is likely on the high side.
- Approximately 50% of site traffic is expected to pass through the intersection of Alderbridge Way/Garden City Road. This translates to over 500 veh/h in the weekday p.m. peak and 800 veh/h on a Saturday afternoon peak hour.

Site Access

- Without the High Street access to Garden City Road, approximately 80% of the site traffic is expected to use Alderbridge Way to access the site.
- With High Street constructed through to Garden City Road, up to 30% of the site traffic will use Garden City Road to access the site, and 70% will use Alderbridge Way.
- The two site accesses on Alderbridge Way and the access on Garden City Road will all operate at an acceptable level of service at build out during both the p.m. and Saturday afternoon peak hours. At the two accesses on Alderbridge Way, separate eastbound left-turn lanes should be provided on Alderbridge Way.
- The two accesses on Alderbridge Way should be designed with three lanes on the north leg, i.e., one entrance lane and two exit lanes.
- 37.5 metres of magazine storage should be provided at the two Alderbridge Way accesses.
- At the west access on Alderbridge Way, it is recommended that a separate northbound left-turn lane be provided at the first driveway on High Street. The east side of this intersection should be restricted to right turns only.
- At the High Street access on Garden City Road, a minimum of 30.0 metres of magazine storage is recommended.
- Sidewalks should be provided on both sides of High Street between Alderbridge Way and Garden City Road.

**Summary of Key Findings of Traffic Impact Assessment
(Extracted from SmartCentre's Traffic Consultant Report Dated July 2011)**

Recommended Improvements

- To mitigate the impact of site traffic on the adjacent road network, it is recommended that from opening day a second westbound left-turn lane be added at the intersection of Garden City Road/Alderbridge Way, together with a protected/permissive phase for the southbound left-turn movement. These improvements will allow the signal timing to be modified to accommodate the additional north-south traffic created by the development.
- The opening of the High Street link through to Alexandra Road cannot occur until the properties affected by this link are developed. Until such time as this link is constructed, access to the development from Garden City Road could be via Alexandra Road. This intersection should be restricted to right-in and right-out movements as a minimum. Left-turn exit movements should be prohibited from opening year regardless of what other movements may be permitted.
- When the High Street connection is constructed and access to the development is available from this road, this upgraded intersection should be signalized and Alexandra Road/Garden City Road should be restricted to right-turn entrance movements only or closed off completely.
- It is recommended that sidewalks be constructed on the north side of Alderbridge Way between Garden City Road and the eastern property line. A sidewalk should also be constructed on the east side of Garden City Road between Alderbridge Way and Alexandra Road.



To: Planning Committee **Date:** September 20, 2013
From: Kirk Taylor **File:** 2275-20-416
Manager, Real Estate Services
**Re: Rezoning Application by First Richmond North Shopping Centres Ltd. -
Response to September 17, 2013 Referral on Real Estate Related Items**

Following the consideration of the rezoning application by First Richmond North Shopping Centres Ltd at the September 17, 2013 meeting of Planning Committee, staff were directed to report back at the Committee's next scheduled meeting with further information on various topics.

This memorandum responds to the following Real Estate-related items of the referral:

- (3) back up plans, excluding expropriation, in the event that the City would not be able to acquire the two required lots for the Connector Road;*
- (6) advise on how City taxpayers and Council will be protected in the future in terms of the cost associated with the purchase of the two required lots for the construction of the Connector Road.*

The remaining referral items (i.e., Items 1, 2, 4, and 5) are addressed in separate memoranda.

Background

In late 2010/early 2011 Smart Centres presented offers to the five (5) owners of the required properties in the Alexandra Road Alignment. While they had a number of responses and put one property under contract, based on not being successful with others, they let the agreement lapse. In February 2013 they again went forward and have the three (3) properties on Alexandra Road under contract. The offers on the two (2) properties at 4560/62 and 4580 Garden City resulted in one counter offer to the developer and one rejection (see Attachment 1 & 2).

RES Analysis

There were two (2) separate appraisals completed in 2012 (by Altus and HCLC) for these properties, one of which (Altus) was utilized in the Developers Analysis. We have further consulted two (2) appraisers with respect to the current valuation of the two (2) Garden City properties specifically, one who completed appraisals on the City's behalf prior, and another that had not. The challenge to get any revisions to the two (2) Garden City appraisals at this juncture is that Appraisers need facts to conduct a comparative analysis. While we do not doubt that Smart Centres will close on the Alexandra properties if their project moves forward, they are currently firm on only one (1) of the three (3) contracts. In the event this application proceeds,

we would have third party value verification of the two (2) remaining Garden City properties, after such time as the Alexandra properties close.

Based on the information we currently have on land, here is our analysis:

Property	Lot Size	2013 Assessed Value	PSF	Appraisal 1 (ALTUS)	PSF	Appraisal 2 (HCLG)	PSF	Forecasted Purchase Price Based on Average Price PSF of Alexandra Sales	PSF
4560/4562 Garden City	11,593 sq.ft.	\$793,800	\$68.28	\$870,000	\$75	\$1,150,000	\$99.14	\$1,902,250	\$164
4580 Garden City	8,686 sq.ft.	\$587,200	\$67.50	\$696,000	\$80	\$825,000	\$94.83	\$1,424,504	\$164
Total	20,279 sq.ft.	\$1,381,000		\$1,566,000		\$1,975,000		\$3,326,754	\$164

*PSF – price per square foot

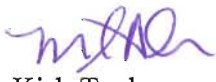
The Forecasted Purchase Price is based on the average dollar per square foot of land that the Developer is reportedly paying for the three (3) properties on Alexandra Road. We have utilized this information as a basis for analysis. Upon receipt of the \$3,450,000 contribution from the developer, staff would commence negotiations with the two (2) remaining property owners in hopes of coming up with an amicable agreement with both parties.

The City has a further level of protection in this situation based on the following:

- 4560/62 and 4580 Garden City have remnant portions of land available which have value; and
- City would also have the ability to sell the existing Alexandra Road right of way and a portion of the lane, neither of which is required under this proposal.

Summary

In summary, staff would commence negotiations with owners of 4560/62 and 4580 Garden City at the direction of Council, and upon receipt of the developer funds. These monies represent a significantly higher dollar figure in total value than either of the two appraisals that staff have on hand. Staff hope to resolve this matter in an amicable manner and the City also has other options available both in the form of value as identified above, and other potentially innovative ideas. Further details are included in a Closed Memorandum to Planning Committee dated September 20, 2013.


 per Kirk Taylor
 Manager, Real Estate Services
 (604-276-4212)
 KT:lv

ATTACHMENT 1**Richmond Assembly Notes:****Sangha – 4560/ 4562 Garden City**

- 2010 Property is assessed for property taxes at \$752,600.
- 2010-12-10 SmartCentres makes offer to purchase property at total price of \$1,020,800.
- 2010-12-10
to 2011-05-30 Vendor counter-offers at \$2,000,000. SmartCentres counters at \$1,370,000.
- 2011 Property is assessed for property taxes at \$894,400
- 2011-05-30 SmartCentres puts property under contract at \$2,000,000, conditional for 7 months
- 2011-12 After discussion with City staff and agreement that price is unreasonable, SmartCentres drops the contract to purchase the property.
- 2012 Property is assessed for property taxes at \$794,600.
- 2012 SmartCentres and City agree to a process to acquire the properties required by the road based on contributions from benefitting land owners (which was presented at Planning Committee Dec 2012).
- 2012-08 Altus appraises property value at \$870,000.
- 2012-12-18 SmartCentres application presented at Planning Committee including road acquisition funding strategy based on contributions from benefitting land owners. Funding strategy referred back to staff.
- 2013 Property is assessed for property taxes at \$793,800.
- 2013-02-01 SmartCentres makes offer \$1,159,700 (33% premium to appraised value)
- 2013-02-04 SC's broker meets vendor. Vendor doesn't think SC project will get built, doesn't want to sell. Won't counter our offer. Says if they did sell, would only tie up for 2 months max., wants \$2,000,000.
- 2013-03-07 SC's broker called son, Shawn Sangha to attempt to get counter-offer in writing.
- 2013-03-11 Shawn Sangha responded to SC's broker by email demanding \$2,400,000 with no conditional time.

Source: Smart Centres

ATTACHMENT 2**Basi – 4580 Garden City**

- 2010 Property is assessed for property taxes at \$464,000.
- 2010-12-10 SmartCentres makes offer to purchase the property for \$765,600. Vendor rejects offer, refuses to counter-offer. Vendor says their intent is to build high-rise hotel on the property.
- 2011 Property is assessed for property taxes at \$584,000.
- 2012 Property is assessed for property taxes at \$587,700.
- 2012 SmartCentres and City agree to a process to acquire the properties required by the road based on contributions from benefitting land owners (which was presented at Planning Committee Dec 2012).
- 2012-08 Altus appraises the property value at \$696,000.
- 2012-12-18 SmartCentres application presented at Planning Committee including road acquisition funding strategy based on contributions from benefitting land owners. Funding strategy referred back to staff.
- 2013 Property is assessed for property taxes at \$587,200.
- 2013-02-01 SmartCentres writes offer to purchase the property for \$930,900 (34% premium to appraised value), forwards offer to SC's broker to present to Vendor.
- 2013-02 to 2013-03 Vendor refuses to meet SC's broker to present offer. Broker tries to reach Vendor's daughter.
- 2013-03-25 SCs's broker meets with Vendor over the weekend and presents offer. Vendor rejects offer and refuses to sell.

Source: Smart Centres



To: Mayor and Council
From: Terry Crowe, Manager
Policy Planning
Date: October 3, 2013
File:
Re: 2041 Official Community Plan ALR Buffer Requirements

Purpose

The purpose of this memorandum is to provide ALR buffer information which was requested by the Planning Committee at its September 17, 2013 meeting. The Committee requested information regarding the City's Agricultural Land Reserve (ALR) buffer requirements as they apply to the West Cambie SmartCentres site.

For the SmartCentres Proposal

At the Planning Committee meeting, staff advised that the 2041 OCP and West Cambie Area Plan do not require an ALR buffer for the Smart Centres site. Instead, to meet the minimum zoning 2m (6 ft) yard setback, Smart Centre is proposing a 3m (10 ft) wide densely landscaped yard setback. It is to consist of native coniferous and deciduous trees, and near the road, primarily broad leaved evergreen shrubs. The aim is to assist in minimizing potential nuisances from Alderbridge Way and the ALR.

Purpose of City ALR Buffer

The purpose of the ALR boundary is to support agriculture and minimize urban complaints against farm operations in the ALR. An ALR buffer is a special landscaped setback area on a non agricultural site outside of, but adjacent to, the ALR boundary or road abutting the ALR boundary in which (1) urban buildings are to be set back from the ALR boundary or adjacent road which abuts the ALR, (2) special landscaping is required and (3) a legal agreement is registered to notify all future building occupants (e.g., residential, commercial, industrial) that a buffer has been provided to mitigate against typical agricultural nuisances (e.g., noise, dust, odour). The 2041 OCP Land Use Map shows the ALR (**Attachment 1**).

2041 OCP ALR Buffer Requirements (OCP pp 7-4 and 7-5):

The 2041 OCP, Chapter 7.0 Agriculture and Food policies include the following ALR buffer policies:

- f) ensure that land uses adjacent to, but outside of, the ALR are compatible with farming by establishing effective buffers on the non-agricultural lands;
- g) designate all parcels abutting, but outside of, the ALR boundary as Development Permit Areas with Guidelines for the purpose of protecting farming;
- h) where there is an intervening road between the ALR and the non-ALR lands:
 - encourage appropriate landscaping within the road right-of-way in front of the non-agricultural lands (e.g., between the road curb, any sidewalk and the property line) through the servicing agreement process;
 - encourage an appropriate landscaped setback on the non-agricultural lands (e.g., 3 m or 9.84 ft. to parking and 6 m or 19.68 ft. to buildings) through the:
 - Rezoning and/or Building Permit process for industrial and institutional uses;
 - Rezoning and/or Development Permit process for commercial and multiple family residential sites;
 - Rezoning and/or Subdivision process for single family residential sites.

Area Plan ALR Buffering Policies

As well, the following Area Plan addresses the ALR buffer in various ways, Bridgeport, East Cambie, Hamilton, McLennan, McLennan North, McLennan South and Steveston. Typically they refer to the above 2041 OCP policies and mention that fences, trees and berms can be used to reinforce the boundary between the ALR and other uses. There is no ALR buffer requirement in the West Cambie Area Plan.

Summary

- (1) For residential uses: As residential land uses have the potential to generate complaints from residents about typical noise, dust and odour activities associated with normal farm practices, staff review all residential development applications that have an adjacency to the ALR and secure buffers (e.g., as the site yard and street boulevard areas) and accompanying legal agreements where appropriate. This ALR buffer approach has been applied to a variety of residential redevelopments, for example:
 - (a) where the urban developing site directly abuts the ALR (i.e., single-family and multi-family redevelopment in the London/Princess Area), and
 - (b) where there is an intervening opened road between the development site and ALR (i.e., west side of No. 2 Road, north side of Steveston Highway, west side of No. 4 Road in the McLennan North and South Sub-Area Plans).
- (2) For non residential urban uses (e.g., commercial, uses): The 2041 OCP requires that where the developing site:
 - (a) directly abuts the ALR (i.e., no intervening road), a Development Permit application is required to establish an on-site buffer (a minimum of 15 m [50 feet] wide, or an alternative width deemed suitable based on specific site conditions and proposed landscaped approach), and
 - (b) is separated by an existing opened intervening road right of way and the ALR, the 2041 OCP does not require a Development Permit application to secure an on-site buffer, unless there is direction in an Area Plan to do so.

In each case, staff work with developers to: (1) tailor the ALR buffer requirement to meet the needs and circumstances, or (2) where is no ALR buffer is required, to achieve a well landscaped front, side or rear yard which may have buffering and screening attributes. Buffers typically consist of a minimum setback for buildings, and landscaping and fencing treatments specifically designed for the proposed urban use.

For development scenarios that do not require an ALR buffer based on policies contained in the 2041 OCP and are located outside of and across from the ALR, staff work with the developers to encourage appropriate setbacks for buildings and on-site landscaping features to ensure compatibility of land uses.

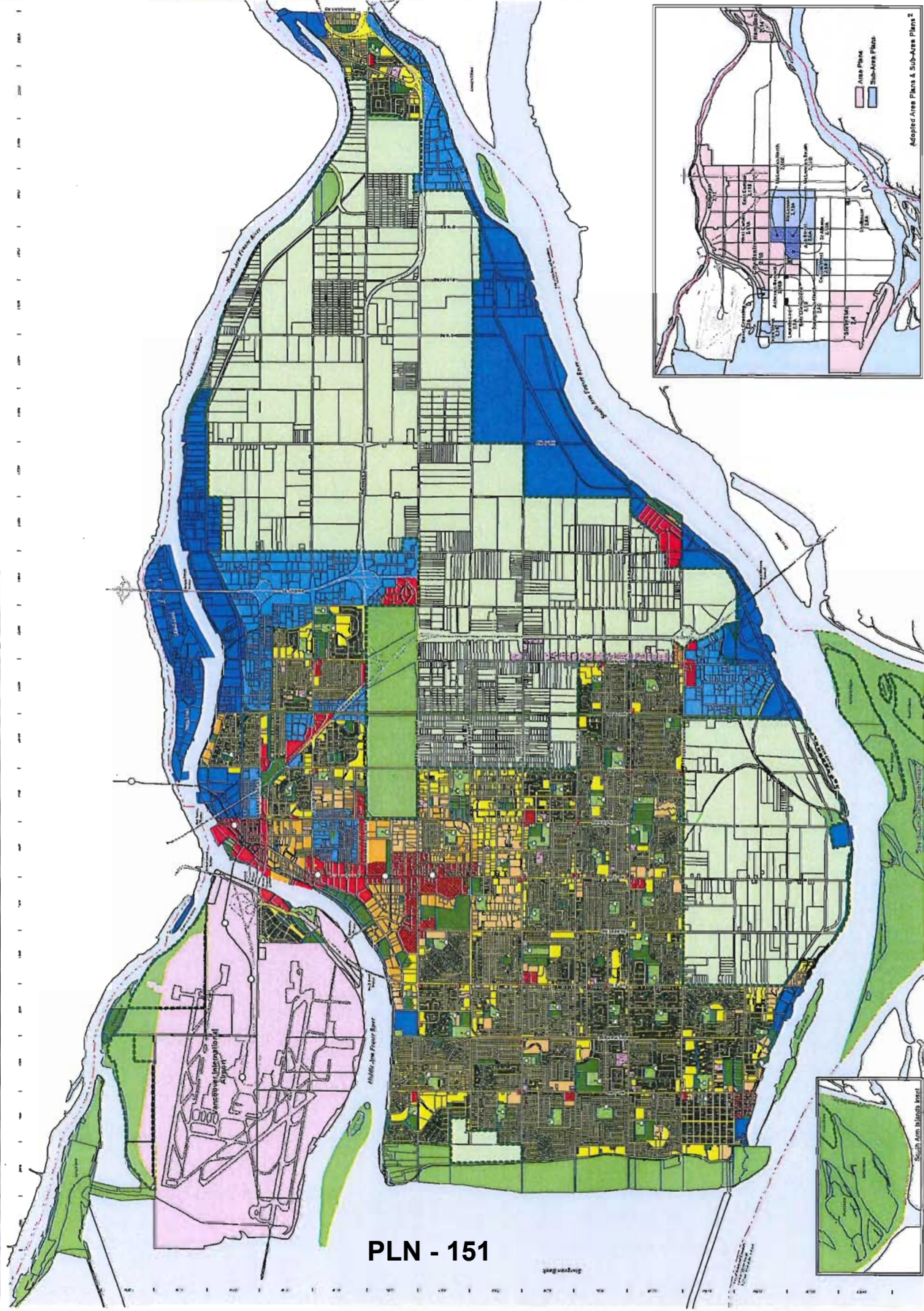
Please feel free to contact me directly (604-276-4139; tcrowe@richmond.ca) if you have any questions.



Terry Crowe
Manager, Policy Planning

TTC:ke

pc: Joe Erceg, MCIP, Deputy CAO
Wayne Craig, Director of Development
Brian Guzzi, Senior Planner/Urban Design
Kevin Eng, Planner 1



PLN - 151

Attachment 1 to Schedule 1 of Bylaw 9000¹

Legend

Land Use	Description
[Symbol]	Agriculture
[Symbol]	Agricultural Land Reserve Boundary
[Symbol]	Alipant
[Symbol]	Apartment Residential
[Symbol]	Commercial
[Symbol]	Community Institutional
[Symbol]	Conservation Area
[Symbol]	Development Mixed Use
[Symbol]	Industrial
[Symbol]	Limited Mixed Use
[Symbol]	Mixed Employment
[Symbol]	Mixed Use
[Symbol]	Neighbourhood Residential
[Symbol]	Neighbourhood Service Centre
[Symbol]	Park
[Symbol]	School ⁴

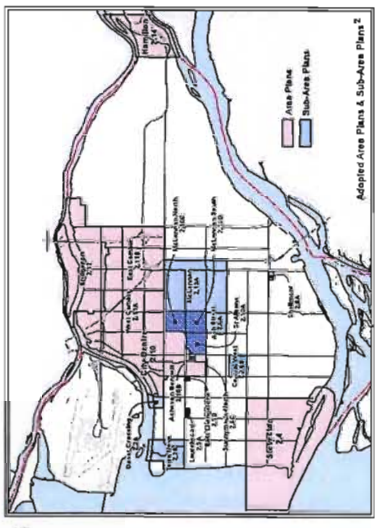
¹ This map is an attachment to the Official Community Plan (OCP). You are reminded that the OCP is a legal document and that its provisions, including its terms, conditions, and restrictions, are subject to amendment from time to time.

² Where there is a conflict with respect to the use of land, the provisions of the OCP, and other OCPs or Sub-Area Plans and Land Use Plans, shall prevail. The City of Richmond is not responsible for the accuracy of the information shown on this map. The City of Richmond is not responsible for the accuracy of the information shown on this map.

³ Contact VTR, Fort Meke Vancouver and the Information Services Authority to see their plans.

⁴ Buildings shown building from specific Richmond neighbourhood maps in the OCP.

Adopted November 10, 2012
Revised July 8, 2013
Prepared by Frank Schmitt



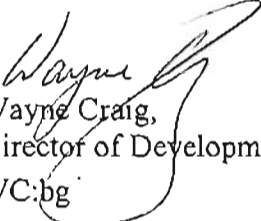


To: Planning Committee **Date:** September 5, 2013
From: Wayne Craig **File:** RZ 10-528877
Director of Development
Re: Application by First Richmond North Shopping Centres Ltd. for Rezoning at 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road from "Single Detached ((RS1/F))" to "Neighbourhood Commercial (ZC32) - West Cambie Area" and "School & Institutional (SI)"

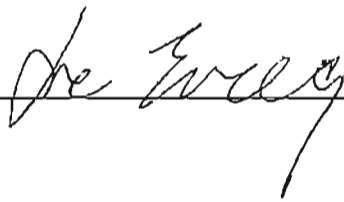

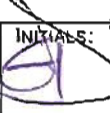
Staff Recommendation

1. That Official Community Plan Bylaw 7100, Amendment Bylaw 8865, to amend the Alexandra Neighbourhood Land Use Map in Schedule 2.11.A of West Cambie Area Plan (WCAP) as shown on the proposed amendment plan to:
 - a. reduce the minimum density permitted from 1.25 to 0.60 FAR in Mixed Use Area A;
 - b. adjust the proposed alignment of May Drive within the development lands; and
 - c. reduce the "Park" designation over portions of 9440, 9480 and 9500 Alexandra Road, be introduced and given first reading.
2. That Official Community Plan Bylaw 9000, Amendment Bylaw 8973, to amend Attachment 2 to Schedule 1 of the Official Community Plan "2041 OCP ESA Map" to eliminate the Environmentally Sensitive Area (ESA) designation for 9440, 9480 and 9500 Alexandra Road, be introduced and given first reading.
3. That Official Community Plan Bylaw 7100 Amendment Bylaw 8865 and Official Community Plan Bylaw 9000 Amendment Bylaw 8973, having been considered in conjunction with:
 - a. the City's Financial Plan and Capital Program; and
 - b. the Greater Vancouver Regional District Solid Waste and Liquid Waste Management Plans; is hereby deemed to be consistent with said program and plans, in accordance with Section 882(3)(a) of the Local Government Act.
4. That Official Community Plan Bylaw 7100 Amendment Bylaw 8865 and OCP Bylaw 9000 Amendment Bylaw 8973 having been considered in accordance with OCP Bylaw Preparation Consultation Policy 5043, are hereby deemed not to require further consultation.

5. That Richmond Zoning Bylaw 8500, Amendment Bylaw 8864 to create the "Neighbourhood Commercial (ZC32) – West Cambie Area" zone and rezone 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480 and 9500 Alexandra Road from "Single Detached (RS1/F)" to "Neighbourhood Commercial (ZC32) – West Cambie Area" and "School & Institutional (SI)", be introduced and given first reading.


 Wayne Craig,
 Director of Development
 WC:bg

Att. 11

REPORT CONCURRENCE			
ROUTED TO:	CONCURRENCE	CONCURRENCE OF GENERAL MANAGER	
Finance Division	<input checked="" type="checkbox"/>		
Real Estate Services	<input checked="" type="checkbox"/>		
Parks Services	<input checked="" type="checkbox"/>		
Engineering	<input checked="" type="checkbox"/>		
Law	<input checked="" type="checkbox"/>		
Policy Planning	<input checked="" type="checkbox"/>		
Transportation	<input checked="" type="checkbox"/>		
Sustainability	<input checked="" type="checkbox"/>		
DIRECTORS	INITIALS: DW	REVIEWED BY CAO 	INITIALS: 

Staff Report

Origin

First Richmond North Shopping Centres Ltd., (SmartCentres) has applied to the City of Richmond to rezone 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480 and 9500 Alexandra Road from "Single Detached (RS1/F)" to "Neighbourhood Commercial (ZC32) – West Cambie Area" and "School & Institutional (SI)" in order to develop a shopping centre with a gross floor area of 36,018 m² (387,692 ft²) and a gross leasable floor area of 34,575 m² (372,162 ft²) and a small lot to be transferred to the City for "Park" purposes. This proposed development is intended to become the urban village centre for the West Cambie Area (WCA). This proposal would consolidate 20 lots creating two (2) development parcels approximately 7¼ acres each, separated by a new north-south road (i.e., the "High Street") linking Alderbridge Way and Alexandra Road. The east development parcel includes a proposed Walmart Store consisting of approximately 14,975 m² (161,188 ft²) of floor space. The following table provides an overview statistical summary of the overall proposed development.

Category	Proposed West Parcel	Proposed East Parcel	Totals
Gross Site Area - before dedications	-	-	67,891 m ² (730,772 ft ²) 16.8 ac.
Net Site Area - after dedications	29,362 m ² (316,049 ft ²) 7.26 ac.	29,243 m ² (314,769 ft ²) 7.22 ac.	58,605 m ² (630,818 ft ²) 14.48 ac.
Gross Floor Area	18,325 m ² (197,248 ft ²)	17,693 m ² (190,444 ft ²)	36,018 m ² (387,692 ft ²)
Gross Leasable Area	17,173 m ² (184,849 ft ²)	17,402 m ² (187,313 ft ²)	34,575 m ² (372,162 ft ²)
Major Anchors	8,883 m ² (95,616 ft ²)	14,975 m ² (161,188 ft ²)	23,858 m ² (256,805 ft ²)
Proposed FAR	0.62 FAR	0.61 FAR	0.62 FAR (overall)
Parkade Parking	411	-	1,153 stalls
Parking Under	-	314 under structures	
Parking On-Grade	175	253 open to the sky	
Total Parking	586	567	1,153 stalls

A staff report regarding the SmartCentres rezoning application was presented to Planning Committee on December 18, 2012 and was subsequently referred back to staff in order to address the following three main issues:

1. *Review arrangement for payment of cost for immediate construction of the road and any possible distribution of cost between developments.*

Staff can advise Committee that:

- **In response to Council direction, SmartCentres has now agreed to pay the entire estimated land costs for the Connector Road. This is a significant improvement over the proposal presented to Planning Committee on December 18, 2012.**
- **The City would have to acquire the remaining 2 properties (4560/62 and 4580 Garden City Road) required for the Connector Road using the funding provided by**

SmartCentres, which City staff estimate is sufficient to acquire these 2 remaining properties at a reasonable purchase price.

- **SmartCentres has also agreed to pay the entire construction cost for the Connector Road via a Letter of Credit (LOC) based on construction costs in 2023 to allow the construction any time between now and 2023.**
- **No Connector Road costs would be deferred to other development sites in the WCA.**

2. *Look at the potential arrangement to purchase residences on the road pathway and other further alternatives if any.*

Staff can advise Committee that:

- **SmartCentres will acquire 3 of the 5 properties (9071, 9091 and 9111 Alexandra Road) required for the Connector Road and dedicate to the City the required right-of-way across these properties.**
- **SmartCentres has agreed to provide a cash contribution to the City in order to fund the City acquisition of the remaining 2 properties (4560 4562 and 4580 Garden City Road).**

3. *Review alternatives to the proposed May Drive alignment and the proposed structure with the green space.*

Staff can advise Committee that:

- **The Official Community Plan (OCP) designated Environmentally Sensitive Area (ESA) on the development site has been assessed by SmartCentres environmental consultant and supported by City's environmental consultant that the size of the designated "ESA" within the proposed development site should be reduced by approximately 1 acre from 2.57 to 1.57 acres (ac) due to human disturbance and the presence of invasive species;**
- **The designated "Park" area on the proposed development lands is approximately 1.51 ac and SmartCentres proposes the provision of 1.08 ac compensation (0.16 ac as dedication and 0.92 ac as SRW). It is also noted that the existing designated "Park" is compromised by human disturbance, invasive species and the recently reduced size of the "Park" beyond the boundaries of the site diminishes the ecological value of the "Park" area.**
- **The additional development land resulting from the SmartCentres proposed realignment of May Drive would be used primarily for parking that supports the proposed retail/commercial uses.**
- **This proposal has been reviewed by Parks staff and the reduction in the "Park" area is acceptable as alternative publicly accessible area is being provided on-site.**

The following is a report that brings forward the SmartCentres rezoning application and responds to the Planning Committee referral.

Findings of Fact

See **Attachment 1** - Location Map and **Attachment 2** - Air Photo. See **Attachment 3** for the Development Application Data Sheet. **Attachment 4** illustrates the SmartCentres Concept Site Plan. All the Conceptual Design Drawings are attached at the end of this document (see **Drawing Sheets 1 to 60**).

A Servicing Agreement will be required for this proposed rezoning application and SmartCentres has agreed to the associated frontage improvements and site servicing requirements, which are outlined in the rezoning considerations (see **Attachment 5** for details). A signed copy of the Rezoning Considerations is located in the rezoning file.

Surrounding Development

To the North: across Alexandra Road is an area of older single-family residential lots - some occupied and others vacant - zoned "Single Detached (RS1/F)" and "Two Unit Dwellings (RD1)" plus one mixed-use (residential/commercial) development site recently rezoned to "Residential/Limited Commercial (ZMU16)" (see rezoning file RZ 12-598503) and one property zoned "School and Institutional Use (SI)". The Alexandra Neighbourhood Land Use Map calls for 3 different land uses on the north side of Alexandra Road along the frontage of the proposed development site:

- west portion: Business/Office with office over retail at a maximum 1.25 FAR.
- central area: Mixed-Uses abutting the High Street at medium density residential over retail and for the lands not abutting the High Street, medium density residential.
- east portion: Residential Area 1 with a base 1.5 FAR (maximum 1.70 FAR with density bonusing for affordable housing) for townhouses and low-rise apartments (4-storey typical).

There are currently 3 active rezoning applications involving 11 properties on the north side of Alexandra Road (opposite the proposed development) consisting of several 4 to 6-storey mixed-use (residential/commercial) and residential buildings with approximately 950 housing units:

To the East: across the proposed extension of May Drive the adjacent lot (9540 Alexandra Road) is designated "Park" and beyond is an area of older single-family residential lots - either occupied or vacant zoned - "Single Detached (RS1/F)" and recently designated in the WCAP as Residential Area 2 for townhouse development with 0.65 base FAR at a maximum 0.75 FAR with density bonusing for affordable housing;

To the South: across Alderbridge Way is the City-owned "Garden City Lands" within the Agricultural Land Reserve (ALR) and zoned "Agriculture (AG1)"; and

To the West: across Garden City Road is an area of retail/commercial land uses zoned "Auto-Oriented Commercial (CA)" and "Gas & Service Stations (CG1)".

Related Policies & Studies

1. West Cambie Area Plan Referral: Planning Committee made the following referral to staff on September 18, 2012

"that staff explore the best use of the land that is bounded by Alexandra Road to the south; Garden City Road to the west; Cambie Road to the north; and Dubbert Street to the east, and report back to the Planning Committee."

A staff report regarding the West Cambie – Alexandra Neighbourhood – Business/Office Area dated June 24, 2013 was presented to Council on July 8, 2013 but this report was subsequently referred back to staff “to further consider mixed use including commercial, residential and office use and the appropriate proportion and numbers of units for each use”. Policy Planning staff are reviewing this area and a separate report to Planning Committee on the land use referral will be presented for consideration at a later date. Staff believe that this application can proceed at this time, without any impact on the referral.

2. West Cambie Park designation to Townhouse Residential: The properties on the south side of Alexandra Road and east of the proposed May Drive extension were previously designated in the WCAP as “Park” area within the West Cambie Park. However, a recent amendment to the WCAP has re-designated the majority of these properties to “Residential Area 2” for townhouse development with base 0.65 FAR (maximum 0.75 FAR with density bonus for affordable housing) similar to the properties on the north side of Alexandra Road. The “Park” designation is retained on 9540 Alexandra Way so that the north-south trail connection through the area is maintained. The proposed SmartCentres development would be compatible with these recent nearby land designations.
3. Connector Road Alignment: City staff have determined that the Connector Road as envisioned in the WCAP – Alexandra Neighbourhood is a critical component of this development since this is the single largest anticipated redevelopment within the immediate vicinity of the Connector Road. The Connector Road has been realigned to reduce the impact on nearby development sites, which has also reduced the number of properties required for the road realignment. However the road realignment still impacts 5 properties, specifically: 9071, 9091 and 9111 Alexandra Road and 4560/62 and 4580 Garden City Road. See **Attachment 6** for the Connector Road realignment land requirements.
4. Connector Road Funding Strategy: City staff previously recommended that SmartCentres acquire the necessary property for the Connector Road and pay for all construction costs. SmartCentres has now agreed to acquire 3 of the 5 properties (9071, 9091 and 9111 Alexandra Road) and dedicate the required road right-of-way (ROW) for the Connector Road and voluntarily contribute approximately \$3,450,000 to the City for the estimated acquisition of the remaining 2 properties (4560/62 and 4580 Garden City Road). The proposed contribution amount is estimated to enable the City to acquire these properties including all associated costs such as land, legal and demolition costs. The City will reimburse SmartCentres with any surplus funds from their contribution for these 2 properties if there is any residual funding for these lots after all City costs have been paid. SmartCentres has also agreed to pay for the entire construction cost of the Connector Road.
5. West Cambie Park and Environmentally Sensitive Area (ESA): The SmartCentres proposed alignment of May Drive would reduce the “Park” designated in the WCAP (see **Attachment 7**).
 - a. The existing OCP “ESA” designation consists of approximately 2.57 ac.
 - b. The Developer’s environmental consultant (Stantec Consultants Ltd.) has conducted a detailed assessment of the designated “ESA” and suggests that the designated “ESA” be reduced by approximately 1.0 ac, which is also supported by the City’s external environmental consultant.

- c. Both the OCP designated “ESA” and the suggested reduction of the designated “ESA” proposed by Stantec includes the area required for the May Drive extension as identified in the WCAP.
 - d. The designated “Park” area on the development lands is approximately 1.51 ac.
 - e. As a result of the Developer’s proposal the City would receive
 - 0.16 ac as “Area J” for “Park” purposes, which is also designated “ESA”; and
 - 0.92 ac in SRW’s over the proposed elevated landscaped deck and transition areas.
 - 1.08 ac total of publicly accessible open space for the loss of approximately 1.51 ac of “Park”.
 - The reduced compensation for “Park” is acceptable since the designated “Park” area, which is overlapped by “ESA” is compromised by invasive species and the relatively small size of this area diminishes the ecological value of the “Park” area.
 - Sustainability initiatives proposed by the Developer include participation in the Alexandra District Energy Utility, provision of electric vehicle stall with plug-in charging equipment, storm water management measures, additional bus stops and shelters, end-of-trip bicycle facilities with additional bike parking and storage plus the incorporation of native trees and plantings wherever possible. These initiatives in combination with the proposed land dedication and SRW’s further enhance the environmental sustainability of this proposed development.
6. Alexandra Mixed-Use Area A – Proposed Reduction of Minimum Density: The WCAP Alexandra Neighbourhood Land Use Plan, for “Mixed-Use Area A” specifies a minimum 1.25 FAR and a maximum of 2.0 FAR (i.e., the proposed west side of the development site). The intent of the minimum 1.25 FAR for “Mixed-Use Area A” was established to require that “Development along Alderbridge must be a compact, urban form and meet high standards of site planning and urban design” and “... all development must demonstrate an appropriate site, building and landscaping response as an integral component of a ‘complete and balanced’ community.” This area is intended as a vibrant, pedestrian-friendly urban village centre for the WCA. SmartCentres proposes a 0.62 FAR, which is approximately half of the minimum 1.25 FAR in the WCAP which requires an OCP amendment. See **Attachment 8** for the existing Alexandra Neighbourhood Land Use Map and **Attachment 9** for the proposed changes to the Alexandra Neighbourhood Land Use Map. The following points are noted:
- The proposal provides for buildings along all frontages and the majority of parking areas are concealed or screened behind buildings from views along fronting streets;
 - If the proposed parking structures consisting of approximately 15,938 m² (171,561 ft²) were included in the FAR calculation the proposed density would be 0.86 FAR and if the parking area under the building was included in the calculation the FAR would be even higher; and
 - SmartCentres has committed to further enhance and activate the pedestrian realm along the Alexandra Way pedestrian corridor and the High Street during the Development Permit stage as indicated in the Rezoning Considerations.
- In summary, an amendment to the WCAP can be supported since SmartCentres has agreed to further enhance and activate the pedestrian realm during the Development Permit process.

7. Alexandra Neighbourhood Amenity Charges: SmartCentres must contribute the following prior to final adoption of the rezoning in keeping with the West Cambie – Alexandra Interim Amenity Guidelines:
 - City beautification amenity charges of \$232,615.20 (387,692 ft² x \$0.60/ft²). Credits will be applied to the Alexandra Interim Amenity Charges City Beautification for the design and construction costs related to the Alexandra Way pedestrian corridor; and
 - Community planning and engineering planning charges of \$27,138.44 (387,692 ft² x \$0.07/ft²).
8. Alexandra Neighbourhood Development Agreement: Council, on June 25, 2007 authorized the execution of the "Alexandra Neighbourhood Development Agreement" for the provision of required off-site sanitary and storm sewer utility works. The subject development is required to provide their proportionate share of the costs associated with the execution of the "Alexandra Neighbourhood Development Agreement" prior to connecting the utility works covered by this agreement. The required payment will be calculated and collected prior to issuance of a building permit for the subject development and will include current interest charges as defined by the agreement. SmartCentres must contribute \$480,738.08 (387,692 ft² x \$1.24/ft²) indexed at the applicable rate, in accordance with the Alexandra Neighbourhood Development Agreement for previously constructed infrastructure improvements in the Alexandra Neighbourhood.
9. Local Area Development Cost Charges (Alexandra DCC's): In addition to City-wide Development Cost Charges (DCCs) applicable to the application, the applicant is required to pay the Supplementary Local Area DCC for the Alexandra Neighbourhood, to fund local north-south roads (including associated infrastructure), supplemental funding for the High Street, to achieve standards over and above the City standard and the acquisition and development of lands for the Alexandra Neighbourhood Park.
10. Aircraft Noise Policy: The proposed development is located in Area 1A of the Aircraft Noise Sensitive Development Map, which prohibits any new developments that contain aircraft noise sensitive uses such as residential, school, daycare and hospital uses. The proposed development does not include any such uses but registration of a restrictive noise indemnity covenant for non-noise sensitive development and SRW in favour of the Vancouver International Airport (YVR) is required as part of the rezoning considerations. Provision of an acoustic report will be required as part of the Development Permit process.
11. Flood Plain Management Policy: The Flood Construction Level (FCL) for the site is 2.6m GSC in the WCAP. The proposed development is designed to 2.6m GSC with the exception of the proposed Walmart lobby and Buildings M and N along Alderbridge Way. SmartCentres will be required to submit a survey of Alderbridge Way (May Drive to High Street), set these finished floor elevations as high as possible and provide a supportable rationale for the lower elevation to the satisfaction of the Senior Manager - Buildings and the Director of Engineering. A floodplain covenant will be secured as a condition of rezoning.
12. Neighbourhood Plan, Design Guidelines Compliance and Urban Design Improvements: Proposed deviations from WCAP neighbourhood structure and design guidelines can be dealt with at the Development Permit stage. Urban design improvements required at the Development Permit stage include advancing the concept design and resubmission of more detailed design drawings to ensure:

- the establishment of a compact, vibrant, pedestrian oriented, urban village centre that is integral part of the neighbourhood and will become the retail/commercial heart of the Alexandra Neighbourhood;
- an attractive, accessible, activated, comfortable, pedestrian-friendly retail/commercial environment with strong pedestrian scale streetwall definition, the possibility for restaurants/shops to extend out to the back of sidewalk including numerous small neighbourhood scale character shops plus an interesting mix and variety of retail shopping opportunities along the High Street;
- a higher quality architectural expression around the entire perimeter of the development site by extending the signature corner treatments (e.g., Alderbridge Way and Garden City Road) further along the building faces on all perimeter building facades including greater horizontal articulation and permeability of perimeter building facades to add more visual interest through enhanced architectural character and an appropriate proportion of transparent and opaque combination of surfaces for the proposed buildings that face the perimeter streets around the exterior of the proposed development;
- the strong presence and continuation of the Alexandra Way pedestrian corridor, and neighbourhood pedestrian spine, through the proposed development with high-quality pavements and contrasting colours that identify the direction of Alexandra Way to and from the neighbourhood to the urban plaza at the corner of Garden City Road and Alderbridge Way. The Alexandra Way pedestrian corridor should be punctuated with periodic pedestrian plaza areas and pedestrian amenities to activate and attract pedestrian traffic and facilitate seasonal events, designed with ample pedestrian space and focused on creating opportunities to encourage pedestrians to sit and linger. The plaza spaces should incorporate other features such as public art and focal elements that add interest and variety to the pedestrian experience. The ground plane paving treatment along the Alexandra Way pedestrian corridor through the proposed development site should include a distinctive and continuous decorative paving treatment extending from building face to building face (along this route through the proposed development) with significant differentiation between the Alexandra Way corridor and other the other internal streets and sidewalks within the overall development;
- a reduction in the amount of signage that is coordinated with the proposed floor plans including the better integration or elimination of redundant signage such as the proposed “Directional Signage” pylons and stronger coordination with the enhanced architectural character of proposed buildings at corner locations;
- safe and efficient pedestrian movement that reflects the direction of the pedestrian traffic toward the Walmart store within the parking area including consideration of east-west oriented parking aisles within the open parking area on the east development parcel with wider bio-swales;
- improved coordination between the landscape and architectural design, including a stronger reliance on the informal clustering of large coniferous tree planting around the perimeter of the proposed development site to enhance the massing and materials articulation/treatment of the building facades, particularly along the Alderbridge Way frontage that is visible from the Garden City Lands to the south; and
- acceptable resolution of any non-compliance with all relevant design guidelines.

Consultation

Public Input

Development signs have been posted as notification of the intent to rezone these 20 properties.

Prior to the initial presentation of this rezoning proposal to Planning Committee on December 18, 2012, a letter was received from Polygon Homes Ltd., dated June 2, 2011 expressing concern regarding the SmartCentres proposed frontage conditions along the south side of Alexandra Road with specific reference to the easterly service and loading area. Polygon has recently acquired several properties on the north side of Alexandra Road including 9393, 9431, 9451, 9471, 9491, 9511 and 9531 Alexandra Road and these 7 properties are on the opposite (north) side of Alexandra Road from the proposed service/loading area of the proposed Walmart store. In general, Polygon proposes 5 to 6-storey residential development on the north side of Alexandra Road. The east development parcel of the SmartCentres proposal has been modified in the following ways to address the Polygon concerns. The Walmart Store service and loading area has been blocked from the majority of views along Alexandra Road by a solid screen wall and overhead by an open trellis structure to carry a vine planting. In addition the surface parking lot has been largely screened from views by an elevated and landscaped deck for public use and enjoyment. While the design improvements will limit the impact of the loading/service area across from the residential uses, relocating the loading function within the site would represent a substantial design improvement and will be further investigated at the Development Permit stage.

Since the Planning Committee referral of December 18, 2012, the City has received 2 additional letters and 22 emails from individuals regarding the SmartCentres rezoning application. In general, these comments can be summarized as follows:

- Expressions of concern and opposition to the SmartCentres rezoning application;
- The majority of correspondence regarding this rezoning application express appreciation and support for the adjacent Garden City Land to be retained within the Agricultural Land Reserve (ALR) and designed to accommodate compatible uses such as for wildlife, farming, garden plots and recreation;
- Concerns expressed regarding the impact of the SmartCentres proposed development on the Garden City Lands and in particular the views to the north from the Garden City Lands;
- Requests for the retention of the existing native vegetation along the north side of Alderbridge Way on the south side of the SmartCentres (Walmart) site; and
- See **Attachment 10** for all public correspondence received to date since June, 2011 regarding this rezoning application.

In response it is noted that the SmartCentres proposed commercial development is located within the Alexandra area of the WCAP, which allows for substantial commercial development including large floor plate retail stores and general merchandise retailers such as the proposed Walmart store. The initial SmartCentres rezoning application in 2003 triggered the referral from Planning Committee to update the WCAP, which was formulated by staff and consultants and approved by Council in 2006 after a lengthy process that involved substantial public consultation.

The Alexandra Neighbourhood Land Use Plan establishes the vision of a complete and balanced community within for the area bounded by Garden City Road, No. 4 Rd, Alderbridge Way and Cambie Road. The proposed development site is separated from the Garden City Lands by Alderbridge Way and the proposed development would not encroach into the ALR. In order

address public concerns regarding the loss of existing native vegetation along the north side of Alderbridge Way on the development site and the anticipated visual impact of the proposed development on views to the north from the Garden City Lands, SmartCentres has proposed a planting strategy along the north side of Alderbridge Way that includes a combination of native coniferous and deciduous tree planting. SmartCentres drawings include simulated views of the proposed development from the Garden City Lands, which demonstrate that the proposed informal, native planting along Alderbridge Way, in combination with the relatively low proposed building heights would not block skyline or profile views of the north shore mountains from the south or central portions of the Garden City Lands.

Staff Comments

Analysis

Conditions of Adjacency

North Edge: The future Connector Road will ultimately result in 1 consolidated lot between the connector road and the SmartCentres proposed development site (west development parcel). The City has on file a schematic concept for the redevelopment of this future consolidated lot submitted by SmartCentres and they propose various screening techniques along the south side of Alexandra Road to address the buffering of the 2 proposed service/loading areas, open parking areas and the parkade. The loading/service area for the west side of the development site along Alexandra Road includes proposed architectural and landscape screening and will be set back from the road when the future Connector Road is constructed. The Walmart loading/service area incorporates more elaborate screening that includes a proposed building wall extension and overhead trellis system with vine planting together with a dense landscape planting scheme along the boulevard. The open parking area within the east development parcel is screened along Alexandra Road with a proposed elevated landscape deck. The proposed parkade screening on the west parcel includes a multi-layered, mature landscape planting treatment consisting of coniferous and deciduous trees and dense shrub planting.

East Edge: The proposed open parking area within the east parcel is screened along May Drive with dense evergreen shrub plantings on the ground plane plus an over-storey of canopy trees.

South Edge: The proposed open parking area within the east parcel would be screened with dense shrub planting and a double row of trees while buildings block views of parking areas in the west parcel. There is an off-street combined pedestrian/bicycle greenway on the boulevard. The planting strategy along the north side of Alderbridge Way would consist of a formal arrangement of native street trees together with informal groupings of native coniferous trees within the building setback zones in combination with dense native shrub plantings. The WCAP does not require any additional ALR setback requirements and none have been included in the proposed zoning district "Neighbourhood Commercial (ZC32) – West Cambie Area".

West Edge: The west edge of the development site includes the required greenway treatment on the boulevard along Garden City Road and provides an appropriate, dense, evergreen foundation planting in combination with a double row of street trees to the proposed buildings.

Legal Agreements & Land Requirements

Land dedications are required for road purposes along Alderbridge Way, Garden City Road, May Drive, High Street and the Alexandra Road realignment. "Area J" as shown on **Attachment 4** at the northeast corner of the proposed development site is required to be transferred to the City as a fee simple lot for uses to be determined by the City and to the satisfaction of the Manager of Real Estate Services and the Director of Development. Statutory Right-of-Ways (SRW's) are required for "Area E" and the elevated landscaped deck over a portion of the surface parking area on the east development parcel as shown on **Attachment 4** plus the proposed Alexandra Way pedestrian corridor, which would connect the Alexandra Road/High Street intersection to the southwest corner of the site, through the proposed west development parcel. In addition, various other legal agreements will be required. See **Attachment 5** - Rezoning Considerations.

Transportation & Traffic

1. Connector Road Realignment:

- a) The 2003 SmartCentres rezoning application (RZ 02-235259) was one of the principal reasons to initiate the West Cambie Area Plan (WCAP) update. SmartCentres participated in the area planning process and as such they were aware of the area plan goals and objectives. The WCAP was adopted on July 24, 2006 and the Connector Road realignment was identified as a key component in the area plan. See **Attachment 6** for road realignment land requirements.
- b) In June 2011, the City adjusted the alignment of the Connector Road as part of a development application (RZ 10-534751 and DP 12-613923) for a mixed-use development consisting of 132 residential units including a small commercial-retail unit fronting the north side of Alexandra Road located at 9251 and 9291 Alexandra Road. The Connector Road realignment involved utilizing the existing Alexandra Road right-of-way as much as possible, which had the added benefit of reducing the land requirements and the construction costs for the Connector Road realignment.

2. Connector Road Funding Strategy:

- a) The Developer has now agreed to pay for all the estimated costs associated with the Connector Road (land and construction) and will provide:
 - the required road dedication needed to facilitate the Connector Road realignment from 9071, 9091 and 9111 Alexandra Road;
 - a cash contribution of \$3,450,000 for the future acquisition by the City of the remaining lands required (4560/62 and 4580 Garden City Road) at a reasonable cost; and
 - LOC for the estimated construction cost of the Connector Road realignment. The City will utilize the LOC to fund road construction after all required property has been acquired.
- b) The current funding strategy for the Connector Road significantly reduces the risk to the City and defers no costs to other development sites within the catchment area.

3. Summary Assessment: The following provides a summary of the current Connector Road proposed funding strategy:

a. Pros:

- Comparing with the previous proposal presented to Planning Committee, the current proposal from SmartCentres significantly reduces the risk to the City for implementing the Connector Road.
- The other road improvements proposed by SmartCentres will ensure acceptable performance of the existing road network for up to 10 years, which allows time to acquire the remaining properties and construct the Connector Road.
- This approach provides the City with the ability to acquire the 2 remaining properties and build the Connector Road realignment without seeking additional funding from other development based on the currently estimated acquisition cost.
- The City could process the construction of the Connector Road once all the property is secured since the City would have SmartCentres LOC for the full cost of construction costs based on the 2023 estimated construction value.

b. Cons:

- The City will need to negotiate the purchase of the remaining 2 properties (4560/62 and 4580 Garden City Road).
- Over time, if property values escalate at a significant rate, the contribution provided may not be sufficient. Staff would begin property negotiations shortly after the funding contribution has been provided to the City.

4. Other Improvements & Land Dedications: Since the Connector Road will not be constructed by the opening day of the proposed development, the following road improvements are required along the following streets:

- Alderbridge Way to ensure the required road widening and provision of a minimum 4.8 m wide shared pedestrian/cyclist path and boulevard on the north side of the road from the back of curb;
- Garden City Road to ensure the required road widening and the provision of a minimum 7.77 m wide shared pedestrian/cyclist greenway and boulevard on the east side of the road from the back of curb;
- Alexandra Road to ensure the required road widening and provision of a minimum 3.65 m wide boulevard/sidewalk on the south side of the road plus allowances for a 9 m wide driving/parking surface and 1.0 m wide shoulder within the road dedication;
- May Drive to ensure the provision of the full road width or a minimum 20 m wide north-south road extension connecting Alexandra Road and Alderbridge Way;
- High Street to ensure the provision of the full road width or a minimum 22.7 m wide new north-south road connecting Alexandra Road and Alderbridge Way;
- Various road improvements at the following intersections: Alderbridge Way/May Drive; Alderbridge Way/High Street; Alderbridge Way/Garden City Road and Garden City Road/Alexandra Road plus special crosswalks on the High Street at the proposed access to the site and at Alexandra Road including Alexandra Road at the High Street; and

- g. The exact width of all required road/intersection improvements and the associated land dedications are based on functional road designs, subject to the approval of the Director of Transportation and to be confirmed by survey plans.

5. Parking & Loading:

- a. The required parking rate for this proposed development is 3.0 spaces per 100 m² the first 350 m² of floor area and 4.0 spaces per 100 m² of floor area for the remaining floor area. The proposed parking is less than the Zoning Bylaw by 16% (1,382 required versus 1,153 proposed) but the WCAP includes the allowance for a 20% reduction in parking subject to a Transportation Impact Study and acceptable Transportation Demand Management (TDM) measures. SmartCentres has proposed the provision of the following TDM measures:
- Bicycle storage (25% more than the bylaw requirements);
 - Two (2) end-of-trip cycling facilities with a total three (3) water closets per gender, 2 wash basins per gender and 3 showers per gender with 1 end-of-trip cycling facility to be provided on the west development parcel and 1 on the east development parcel;
 - Three (3) bus stop upgrades (bus shelters and accessible bus landing pads for each) within the vicinity of the site; and
 - Ten percent (10%) of the total parking spaces pre-ducted for electrical vehicle (EV) plug-ins plus a minimum of 4 EV parking stalls (i.e., 2 on the west development parcel and 2 on the east development parcel) be equipped with charging stations (240V).

6. Summary:

- a. The currently proposed funding strategy for the Connector Road assumes that the Developer will pay for the entire cost of the Connector Road (land and construction) with no costs deferred to other development sites within the catchment area.
- b. Compared with the previous proposal presented to Planning Committee, SmartCentres has agreed to pay for the 41% of the Connector Road costs that would have been contributed by other development sites within the catchment area.

Engineering & Servicing

1. Storm Sewer: All site storm drainage must be directed to Alexandra Road except for road run-off from the south half of High Street and May Drive that may be drained to Alderbridge Way. The storm sewer along the Alexandra Road must be upgraded to a minimum 600mm diameter pipe including a new connection across Garden City Road to the existing 1200mm diameter storm drain.
2. Sanitary Sewer: Sanitary analysis is required for the Odlin West sanitary pump station. Sanitary sewer improvements are required on Alexandra Road, May Drive and High Street. A 6.0 m wide Statutory Right of Way (SRW) for utility purposes is required for the proposed sanitary sewer within the future May Drive connecting Alexandra Road and Tomicki Avenue. The required SRW is located within 9451 and 9471 Alexandra Road and is to be measured 6.0 m from the east property lines of these 2 properties.
3. Water Service: A new watermain is required on Alexandra Road, High Street and May Drive and asbestos-cement (AC) watermain replacement is required along Garden City Road.

4. Hydro/Telephone: Pre-ducting works are required on the following proposed roads subject to confirmation from BC Hydro and telecom providers:

- a) proposed May Drive (from Alderbridge Way to Alexandra Road), and
- b) proposed High Street (from Alderbridge Way to Alexandra Road).

The removal of existing power poles and installation of underground pre-ducting along the east side of Garden City Road and along the north side of Alexandra Road will be at the discretion of BC Hydro.

5. Summary: The City has defined the scope of work description for required frontage improvements and site servicing for the Servicing Agreement in the Rezoning Considerations (see **Attachment 5**). All servicing infrastructure works shall be as per City requirements and to final approval by the Director of Engineering and the Director of Transportation.

Site Planning & Urban Design

1. Pedestrian-Oriented Village Centre: The WCAP envisions a compact, urban, pedestrian friendly village centre for the Alexandra Neighbourhood – Area A (proposed west parcel). There are enhanced pedestrian environments within the development that include wider sidewalks, raised pedestrian crossings, permanent and seasonal plaza areas and a moderate level of pedestrian enhancements. Further design development is required at the Development Permit stage to ensure a high quality design with an appropriate level of pedestrian amenities.
2. Streetscape Design: The proposed streetscape design responds to the various edge conditions surrounding the site including the 2 greenways (along Alderbridge Way and Garden City Road). However, further design development is required through the Development Permit stage to ensure effective screening of parking areas, adequate buffering of the parkade and loading/service areas, building façade enhancements and boulevard treatments, the elimination of stairs in the public realm and high quality streetscape design. The High Street is an important urban design component of the Alexandra neighbourhood village centre concept that should be designed to generate and attract pedestrian activity with retail uses, appealing streetfront architectural façades, variety in streetscape design and high quality pedestrian amenities. While the proposed site plan allows for adequate pedestrian circulation space, careful attention to detailed design at a larger scale is required during the Development Permit stage to ensure the WCAP vision is achieved with an appropriate level of activation for this important pedestrian retail street.
3. Design Development: Further design development of the architectural facade designs, site planning and landscape design are required at the Development Permit stage as indicated above and in the rezoning considerations (see **Attachment 5**).

Architectural Form & Character

Street Fronting Building Façades: The proposed building façades include design variety and visual interest that break long retail frontages into smaller CRU's. The streetfront façade design also attempts to replicate retail storefronts, along streets with rear facing buildings however, further design development is required at the Development Permit stage to ensure there is:

- correlation between storefront façade design and proposed CRU floor areas;
- appropriately scaled building streetwalls with a visual appearance taller than 1-storey;

- sufficient architectural variety with appropriate design commonalities;
- incorporation of high quality building materials with abundant storefront transparency;
- a complementary mix of retail uses and an integrated streetscape design;
- acceptable pedestrian activation with potential for retail uses expanding onto the boulevard;
- adequate streetscape improvements with appropriate pedestrian comforts and amenities;
- creation of a vibrant, attractive and pedestrian friendly retail/commercial street; and
- a coordinated streetscape design with ample visual interest and pedestrian scaled signage.

See also Design Guidelines Compliance above and Rezoning Conditions - **Attachment 5**.

Landscape & Open Space Design

1. Existing Trees:

- a. SmartCentres has provided a tree survey and arborist report with an assessment of all on-site trees.
- b. Staff and the proponent investigated the retention of significant and high quality trees but tree retention could not be achieved due to road improvements, grade changes and the form of development.
- c. SmartCentres proposes the removal of all site trees but will provide 344 replacement trees as part of the landscape plan and/or cash-in-lieu if the total number of replacement trees cannot be located on-site. The Tree Preservation Coordinator agrees with the proposed tree removal. The existing site trees can be removed following the Public Hearing with the appropriate tree removal permit and bonding for replacement trees on a 2 for 1 basis.
- d. There are 3 significant trees and 1 high value tree (i.e. 1-80cm caliper Douglas Fir, 1-111cm caliper Douglas Fir, 1-100cm Linden and 1-35cm Balsam Fir), which SmartCentres proposes to remove. SmartCentres proposes to plant 4 specimen replacement coniferous trees (minimum 5 m high or 20cm caliper for deciduous trees) and this will be addressed at the Development Permit stage.
- e. Through the Development Permit process, staff will ensure the landscape plan includes native tree species diversity to provide increased bio-diversity plus year round screening and visual interest.

2. Landscape & Open Space Design:

- a. Alexandra Way Pedestrian Corridor: This important neighbourhood pedestrian corridor is proposed to extend along the both sides (east and west boulevards) of the High Street north block with a connection between the east and west development parcels at the both ends of the High Street north block. This pedestrian corridor continues through the west development parcel, eventually connecting with the northeast corner of the Garden City Road and Alderbridge Way intersection. The proposed design allows for a minimum 3.5m wide pedestrian walkway including the following features, decorative paving, lighting, banners, hanging baskets, street furniture, wayfinding signage, weather-protection, tree, shrub and floral plantings, seasonal displays and public art.

- b. Further design development is required during the Servicing Agreement and Development Permit stages to ensure a high quality design with a full range of pedestrian amenities and comforts. A SRW will be required over this area to ensure public pedestrian access. Maintenance of this pedestrian corridor will be the responsibility of SmartCentres.
- c. **Boulevard Design:** Both Alderbridge Way and Garden City Road are designated greenways with boulevard planting strips, minimum 2 rows of street trees and off-street bike lanes. The design proposes a shared pedestrian/bike path along Alderbridge Way and a separated pedestrian sidewalk and bike lane along Garden City Road. The proposed landscape design behind the sidewalk (within the building setback) includes a zone of tree and shrub planting that varies in width along the fronting roads in order to enhance the rear building facades. Along Alexandra Road, the Building A (west parcel) loading area would be screened by a minimum 2m wide landscape strip between the sidewalk and the screen wall while the Walmart loading area (east parcel) would be screened by a minimum 4.0 m wide landscape strip between the sidewalk and the screen wall including an overhead trellis for additional screening from above. The proposed High Street design features wider sidewalks (min. 4.5m wide) for cafes and retail activities to expand onto the boulevard, with street trees and median plantings to add variety and seasonal interest to the streetscape design. May Drive is proposed as a standard City street with typical sidewalk and boulevard plantings. More design development of these street frontages is required through the Servicing Agreement and Development Permit stages to ensure high quality streetscapes.
- d. **Raised Landscape Deck:** The design proposal includes an elevated landscape deck at the northeast corner of the site to screen open parking areas and add usable green space for the future adjacent higher density residential projects and the general public. The grade transition from Alexandra Road onto the landscape deck has been reduced to a maximum slope of 3:1. The proposed deck design incorporates multiple pedestrian entry points including a stair connection to the parking area below. The proposed design character is informal and predominately green incorporating passive recreation opportunities with numerous seating areas adjacent to the pathway system. The pathway system includes minimum 2.0 m wide sidewalks and pedestrian scale lighting for safety. The proposed planting includes abundant tree and evergreen shrub planting complete with an automatic irrigation system. Further design development will be necessary to ensure a high quality design and appropriate crime prevention measures are incorporated. Maintenance of this SRW area will be the responsibility of SmartCentres.
- e. See also Design Guidelines Compliance above and Rezoning Conditions - **Attachment 5**.

Alexandra District Energy Utility (ADEU), Sustainability & Environmental Design

1. Alexandra District Energy Utility (ADEU):

- a. SmartCentres has agreed that 63% to 69% of the proposed floor area or approximately 70% of the total annual heating and cooling energy demand will be serviced by the ADEU but this is subject to Council approval of amendments to the ADEU bylaw to allow less than 70% participation. Furthermore, obligations to connect to the ADEU will be subject to Council's future approval of capital funding for the expansion of ADEU infrastructure necessary to service the development. Upon Council's support for this rezoning, staff will

bring forward expansion options for consideration. The participation of this development proposal in the ADEU will be limited to the large format tenants (Buildings A and the East Anchor Building – Walmart Store). More detailed energy modelling will be required to establish the extent of the energy demand represented by those tenants. SmartCentres will be required to coordinate with Engineering staff to determine this demand as part of the Servicing Agreement process.

2. Sustainability & Environmental Design: SmartCentres has agreed to provide the following environmental and sustainability features:
 - LEED Silver equivalency for the project;
 - Compact development with the majority of stalls in 3 covered/structured parking areas;
 - Electric vehicle stalls with plug-in charging equipment;
 - Improved on-site pedestrian circulation (Development Permit refinements required);
 - Reduced storm water discharge through rooftop detention, permeable paving, bio-swales and the storm water discharge treatment through oil and water separators;
 - Water efficient plumbing fixtures and drought tolerant planting; and
 - Reduced energy consumption and attention paid to the efficiency of the building envelope and HVAC systems plus high-efficiency night-sky friendly lighting.

Public Art & Crime Prevention Through Environmental Design (CPTED)

1. Public Art: SmartCentres will either provide public art on-site along the Alexandra Way pedestrian corridor in accordance with the City's Public Art Policy or provide cash-in-lieu to the City Public Art fund, which is currently estimated to be approximately \$155,077 (387,692 ft² x \$0.40/ft²).
2. CPTED: The inside of the parkade (walls, columns and ceilings) will be painted with reflective white paint with lighting levels as required by the BC Building Code. The open parking areas will be well lit with fixtures providing good colour rendition. A complete and comprehensive list of CPTED enhancements will be provided during the Development Permit stage.

Refuse & Recycling

The proposed refuse/recycling facilities meet the City minimum requirements. Each separate building will have a designed refuse/recycling room, including grease bins for restaurants, if appropriate. Garbage rooms will be provided with 2 large containers (for garbage and cardboard) and separate carts for food scraps, paper, glass and plastics (4 carts in total). Plans will be provided at the Development Permit stage with the layout and location of all facilities.

Richmond Advisory Design Panel (ADP)

This rezoning application was presented to the ADP on December 8th, 2011. See **Attachment 11** for ADP comments followed by SmartCentres responses in *bold italics*. The ADP expressed concerns regarding the form and character of the SmartCentres proposal, which are summarized in the following statements:

- Project would benefit from increased density to reflect a more urban 'Village' character;
- More storefronts and retail development that front onto the surrounding perimeter roads;
- Improved architectural retail streetfront design to create an improved 'Gateway' experience;
- More intense urban design required to create a more sophisticated urban character;
- Stronger architectural expression is required to balance the size and scale of Walmart store;
- High Street requires more vertical definition and should be the retail heart of the project;
- Hierarchy of linked outdoor rooms is required on the Alexandra Way pedestrian corridor;
- Pedestrian improvements are needed at entries/crossings plus continuous rain protection;
- The elevated landscape deck should expand the variety of uses and improve linkages;
- More attention to CPTED issues is required under the elevated landscape deck; and
- Inadequate screening of the loading areas.

The Development Permit process will include a more detailed presentation to the ADP.

Financial Impact

The financial implications to the City are as follows:

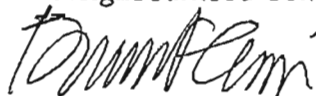
1. It is estimated that the SmartCentres current proposal includes sufficient funding to acquire all the land at a reasonable cost and construct the Connector Road as soon as all the land has been acquired. Effectively, SmartCentres is offering to pay for the entire cost of the Connector Road (land and construction).
2. Under the SmartCentres proposal the City would have to acquire the remaining 2 properties (4560/62 and 4580 Garden City Road). The total estimated value to acquire these 2 properties is estimated to be approximately \$3,450,000 (land, legal and demolition costs) in 2013 taking into consideration SmartCentres accepted offers for the other 3 properties. SmartCentres has agreed to a voluntarily cash contribute to the City of \$3,450,000 for the acquisition of these 2 properties. After all City costs related to the acquisition of these properties then any residual funding would be reimbursed to the Developer. This cash contribution should be sufficient funding to acquire all the land.
3. The Connector Road is not required to be constructed for 10 years based on the other transportation and traffic improvements to the surrounding road network that SmartCentres has agreed to install. SmartCentres will provide a LOC for the construction amount, which will enable the City to construct the Connector Road at any time between now and 2023.

Conclusion

SmartCentres has now agreed to pay for the entire Connector Road costs (land and construction). This is a significant concession in excess of the previous proposal and would defer no Connector Road costs to other development sites within the catchment area. The proposed project design responds positively to the Area Plan urban design objectives and provides for substantial commercial development in keeping with the WCAP.

Therefore, it is recommended that the following bylaws be introduced, given first reading and forwarded to Public Hearing:

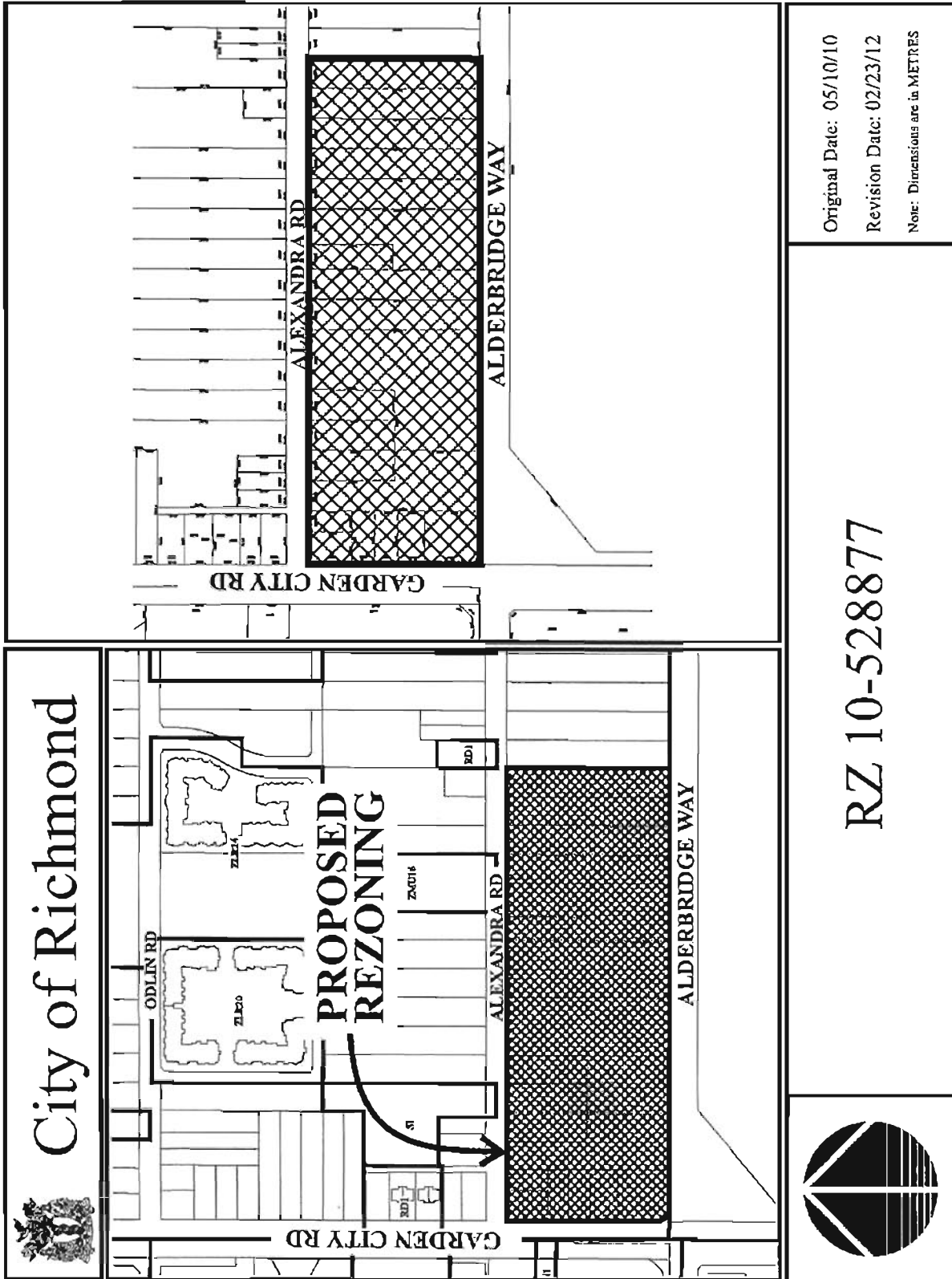
- Official Community Plan Bylaw 7100, Amendment Bylaw 8865, to amend the Alexandra Neighbourhood Land Use Map in Schedule 2.11.A of West Cambie Area Plan (WCAP);
- Official Community Plan Bylaw 9000, Amendment Bylaw 8973, to amend Attachment 2 to Schedule 1 of the Official Community Plan "2041 OCP ESA Map" to eliminate the Environmentally Sensitive Area (ESA) designation for 9440, 9480 and 9500 Alexandra Road; and
- Richmond Zoning Bylaw 8500, Amendment Bylaw 8864 to create the "Neighbourhood Commercial (ZC32) – West Cambie Area" zone and rezone 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480 and 9500 Alexandra Road from "Single Detached (RS1/F)" to "Neighbourhood Commercial (ZC32) – West Cambie Area" and "School & Institutional (SI)".



Brian Guzzi, MCIP, MCSLA
Senior Planner - Urban Design

BG:cas

- Attachment 1: Location Plan
- Attachment 2: Aerial Photo
- Attachment 3: Development Application Data Sheet
- Attachment 4: Concept Site Plan
- Attachment 5: Rezoning Considerations
- Attachment 6: Connector Road Realignment & Land Requirements
- Attachment 7: Park & Environmental Sensitive Area (ESA) – Proposed Adjustments
- Attachment 8: Alexandra Neighbourhood Land Use Map – WCAP (existing)
- Attachment 9: Alexandra Neighbourhood Land Use Map – WCAP (proposed)
- Attachment 10: Public Input Summary
- Attachment 11: Advisory Design Panel Comments and Applicant Responses



City of Richmond



PROPOSED
REZONING


RZ 10-528877



Original Date: 05/10/10
Revision Date: 02/23/12
Note: Dimensions are in METRES

Air Photo



	<h1>RZ 10-528877</h1>	<p>Original Date: 05/10/10 Amended Date: Note: Dimensions are in METRES</p>
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City of Richmond
 6911 No. 3 Road
 Richmond, BC V6Y 2C1
 www.richmond.ca
 604-276-4000

Development Application Data Sheet

RZ 10-528877

Attachment 3

4660, 4680, 4700, 4720, 4740 Garden City Road and
 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480,
 Addresses: 9500 Alexandra Road

Applicant: First Richmond North Shopping Centres Ltd.

Planning Area(s): West Cambie Area – Alexandra Neighbourhood

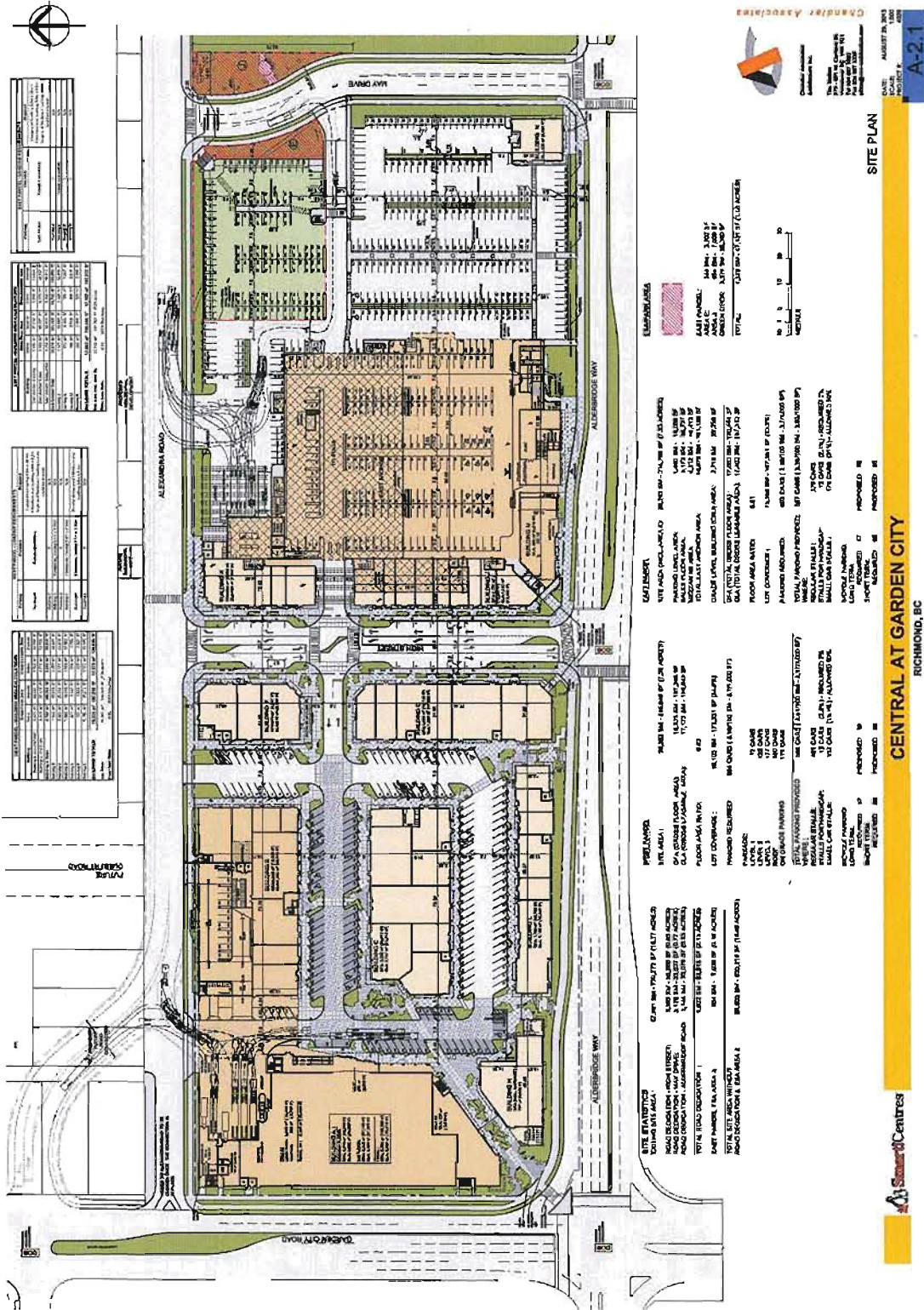
	Existing	Proposed	Variance
Owner:	First Richmond North Shopping Centres Ltd.		-
Site Size (m2):	67,891 m ² (16.8 ac.)	58,631 m ² (14.5 ac.)	-
Land Dedications:			
High Street:	N/A	3,363 m ² (36,200 ft ²)	-
May Drive:	N/A	3,125 m ² (33,637 ft ²)	
Alderbridge Way:	N/A	2,144 m ² (23,078 ft ²)	
Area J:	N/A	654 m ² (7,039 ft ²)	
Land Uses:	Vacant	Mixed Use: Retail/Commercial	-
OCP Designation:	Commercial	Commercial	-
Area Plan Designation:	Mixed Use: Retail/Commercial	Mixed Use: Retail/Commercial	-
Zoning:	Single Detached (RS1/F)	Neighbourhood Commercial (ZC32) – West Cambie Area	
Gross Floor Area:	none	36,018 m ² (387,692 ft ²)	-
Leasable Floor Area:	none	34,575 m ² (372,162 ft ²)	-
Other Designations:	Partial ESA Designation	Elimination of ESA Designation	OCP amendment
Floor Area Ratio:	Area A - Min. 1.25 to Max. 2.0 FAR	Area A - 0.62 FAR	OCP amendment
	Area B – Max. 1.0 FAR	Area B - 0.61 FAR	none
Building Lot Coverage (Max. 55%)	0%	Area A - 54.8%	none
	0%	Area B - 53.2% (with deck)	none

Table continued on next page

	On Future Subdivided Lots Bylaw Requirement	Proposed	Variance
Lot Size (min. dimensions):	2 ha (4.94 ac.)	West Parcel - 29,362 m ² (316,049 ft ²) or 7.26 ac. East Parcel - 29,243 m ² (314,769 ft ²) or 7.22 ac.	none
Road Setbacks ~ Alderbridge Way: Garden City Road: Alexandra Road: High Street: May Drive:	Min. 2.0 m Min. 3.0 m Min. 1.0 m Min. 3.0 m Min. 5.0 m	Min. 2.0 m Min. 3.0 m Min. 1.0 m Min. 3.6 m Min. 5.0 m	none
Height (m):	Area A - Max. 20 m	Area A - 17.81 m	none
	Area B - Max. 20 m	Area B - 16.23 m	
Off-street Parking: Regular (R), Small (S), Accessible (A):	3 stall per 100 m ² (R), Max. 50% (S) allowed, Min. 2% (A) required, Required Total = 1,382 With TDM Package = 1,153	840 (R), 288 (S), 25 (A) Total = 1,153	none
Parking Rate	3 stalls/100m ² (first 350 m ²) 4.0 stalls/100m ² (remainder)	3 stalls/100m ² (first 350 m ²) 4.0 stalls/100m ² (remainder)	none
Loading Spaces:	5 large (WB-17) spaces & 7 medium (SU9) spaces	8 large (WB-17) spaces & 6 temporary small (5.5 m x 2.65 m)	none
Bicycle Parking: Class 1 (Long Term), Class 2 (Short Term)	Class 1 - 94 Class 2 - 138	Class 1 - 119 Class 2 - 172	none

Other: Tree replacement compensation required for loss of 172 existing/former site trees (on 2 for 1 basis at \$500 per tree) plus 3 existing significant trees and 1 existing high value tree (on a 1 for 1 basis at \$5,000 per tree).

Concept Site Plan



NO.	DESCRIPTION	AREA (SQ. METERS)	AREA (SQ. FEET)
1	Site Area	1,000,000	10,764,000
2	Building Footprint	150,000	1,620,000
3	Parking	100,000	1,076,400
4	Landscaping	50,000	538,200
5	Other	100,000	1,076,400
6	Total	1,300,000	13,974,600

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SITE PLAN
 SCALE: AS SHOWN
 PROJECT # A-2.1

CENTRAL AT GARDEN CITY
 RICHMOND, BC



Rezoning Considerations

4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road

RZ 10-528877

Prior to final adoption of Zoning Amendment Bylaw 8864, the developer is required to complete the following:

1. Final Adoption of OCP Amendment Bylaws 8865 and 8973.
2. Required Road/Utility Dedications:
 - a) Alexandra/Leslie (Connector Road): Road dedication is required from 9071, 9091 and 9111 Alexandra Road in accordance with **Attachment 6**. The exact dedication is to be as per an acceptable functional design approved by the Director of Transportation.
 - b) Alderbridge Way: Road dedication is required to accommodate the frontage improvements noted in Servicing Agreement Items 16 below to the back of the 3.3m wide shared pedestrian/cyclist path on the north side of the road. Exact dedication to be determined through a road functional design drawing to be prepared by the Developer and to the approval of the Director of Transportation and the Director of Engineering.
 - c) Garden City Road: Road dedication is required to accommodate the frontage improvements noted in Servicing Agreement Item 16 below to the back of the 2.0 wide sidewalk on the east side of the road. Exact dedication to be determined through a road functional design drawing to be prepared by the Developer and to the approval of the Director of Transportation and the Director of Engineering.
 - d) May Drive: A minimum 20m wide road dedication is required between Alderbridge Way and Alexandra Road to accommodate the frontage improvements noted in Servicing Agreement Items 16 below and to the approval of the Director of Transportation and the Director of Engineering.
 - e) High Street: A minimum 22.7m wide road dedication is required between Alderbridge Way and Alexandra Road to accommodate the frontage improvements noted in Servicing Agreement Items 16 below and to the approval of the Director of Transportation and the Director of Engineering.
 - f) Provision of minimum 4m x 4m corner cuts (as dedication) required at all intersections where public roads intersect and approved by the Director of Transportation and the Director of Engineering except in locations where the proposed road geometry requires additional land dedication to ensure that the travel portion of the road and the adjacent sidewalks are within the road right of way to the approval of the Director of Development and the Director of Transportation. The corner cuts to be measured from the “new” property lines.
 - g) Final determination of the exact road dedications and construction requirements are subject to minor revisions as determined by the functional road design and to the approval of the Director of Transportation and Director of Development.

3. Required land transfers:

- a) A land transfer of proposed "Area J" consisting of approximately 654 m² is required from the Developer to the City as a fee simple lot for park purposes at a nominal cost (i.e., \$10) to the approval of the Manager of Real Estate Services, Director of Transportation and the Director of Development. "Area J" is located at the northeast corner of the development site on the east side of the proposed May Drive alignment (see **Attachment 4**). Final determination of the exact land transfer area is subject to minor revisions as determined by the functional road design, to be confirmed by survey plans and to the approval of the Director of Transportation and Director of Development. A legal agreement will be required for this land transfer.

4. Required Statutory Rights of Way (SRW's):

- a) Granting of a variable width Statutory Right of Way (SRW) for sidewalk purposes that connects the northeast corner of the Alexandra Road/High Street intersection with the northeast corner of the Alexandra Way/Garden City Road intersection through the west development parcel for the purposes of establishing a public pedestrian walkway referred to as the 'Alexandra Way' pedestrian corridor in the WCAP. This SRW should include:
 - i. A minimum 3.5 m wide sidewalk, within the building setback between the property line and the proposed building façades on both sides of the High Street within the north block,
 - ii. A minimum 3.5 m wide sidewalk, within the west development parcel along the north side of the northerly east-west drive aisle including all necessary and associated pedestrian crossings that traverse parking lot drive aisles;
 - iii. A pedestrian plaza within the west development parcel at the west end of the northerly east-west drive aisle as shown on the Site Plan dated August 29, 2013 including all necessary and associated pedestrian crossings that traverse parking lot drive aisles;
 - iv. A minimum 3.5 m wide sidewalk on the east side of the westerly north-south drive aisle including all necessary and associated pedestrian crossings that traverse parking lot drive aisles within the west development parcel;
 - v. A minimum 3.5 m wide diagonal sidewalk connecting the west development parcel with the Alderbridge Way/Garden City Road intersection including the corner pedestrian plaza; and
 - vi. A reference plan is required to identify this proposed SRW to be confirmed by survey plan and a legal plan for registration in the land title office.

The design of Alexandra Way public pedestrian corridor requires further design development through the Development Permit process. This pedestrian corridor shall include decorative pedestrian and vehicle paving, decorative street lighting and banners, high quality retail signage, street furniture and continuous weather protection, street trees, shrub planting, decorative accent floral planting, high-quality public open spaces along the corridor and periodic focal elements such as public art, special effect night lighting, outdoor cafes/eating areas and/or other attractors and generator of pedestrian traffic and all to the approval of the Director of Development. The construction and maintenance of including liability for the Alexandra Way public pedestrian corridor shall be the responsibility of the Developer.

including liability for the Alexandra Way public pedestrian corridor shall be the responsibility of the Developer.

- b) Granting of an approximately 344 m² Statutory Right of Way over proposed “Area E” for the purposes of establishing a passive recreation, public open space as a transition to the proposed elevated landscape deck (see item c. below). “Area E” is located at the northeast corner of the development site on the west side of May Drive (see **Attachment 4**). The design of this transition area requires further design development through the Development Permit process. The construction and maintenance costs including the on-going liability for this landscape transition area to the elevated landscape deck shall be the responsibility of the Developer.
 - c) Granting of an approximately 3,378 m² (52 m x 64.9 m) Statutory Right of Way (SRW) over the proposed elevated landscape deck for the purposes of establishing a passive recreation, public open space including the transition areas to the fronting streets (Alexandra Road and May Drive). The elevated landscape deck is located along Alexandra Road at the northeast corner of the proposed development site excluding “Area E” (see **Attachment 4**). A reference plan is required with the appropriate area shaded to identify this proposed SRW to be confirmed by survey plan and a volumetric legal plan to the approval of the Director of Development prior to registration in the land title office. The design of this elevated landscape deck and transition areas requires further design development through the Development Permit process but is intended to be an important feature of the site design including barrier free pedestrian access, multiple entry points including a stair connection to the surface parking lot below, decorative pedestrian paving, lighting, street furniture, numerous seating opportunities with abundant trees, shrub, groundcover and sodded grass planting, an all to the approval of the Director of Development. The construction and maintenance cost including the on-going liability for the elevated landscape deck shall be the responsibility of the Developer.
 - d) Final determination of the exact PROP-SRW’s and construction requirements are subject to minor revisions as determined by the functional road design and to the approval of the Director of Transportation, Director of Engineering and Director of Development.
5. Consolidation of the following 20 lots (the table below) in order to create two (2) development parcels (east development parcel and west development parcel) plus “Area J” as identified in **Attachment 4**. The existing dwellings have already been demolished.

Address	PID	Legal Address	Zoning	Owner
4660 Garden City Road	003-491-986	Lot 53 Section 34 Block 5 North Range 6 West New Westminster District Plan 41957	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
4680 Garden City Road	003-522-725	Lot 1 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
4700 Garden City Road	001-985-281	Lot 2 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.

Address	PID	Legal Address	Zoning	Owner
4720 Garden City Road	003-640-043	Lot 3 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
4740 Garden City Road	008-141-525	Lot 4 Except: Firstly, Parcel A (Bylaw Plan 73626), Secondly, Part on Plan LMP41468 Section 34 Block 5 North Range 6 West New Westminster District Plan 15498	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9040 Alexandra Road	003-514-889	Lot 54 Section 34 Block 5 North Range 6 West New Westminster District Plan 41957	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9060 Alexandra Road	007-133-138	Lot 37 Section 34 Block 5 North Range 6 West New Westminster District Plan 34867	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9080 Alexandra Road	004-192-141	Lot 38 Section 34 Block 5 North Range 6 West New Westminster District Plan 34867	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9180 Alexandra Road	012-032-476	North 249.3 Feet Lot 3 Except: Parcel "A" (Explanatory Plan 8738), Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9200 Alexandra Road	003-498-433	Parcel "A" (Explanatory Plan 8738), Lot 3 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9260 Alexandra Road	012-032-522	Parcel "One" (Explanatory Plan 9711) Lots 3 and 4 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9280 Alexandra Road	012-032-557	West Half Lot 5 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9320 Alexandra Road	004-079-124	East Half Lot 5 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9340 Alexandra Road	000-868-655	Lot "B" Section 34 Block 5 North Range 6 West New Westminster District Plan 11945	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9360 Alexandra Road	000-556-939	Lot A Section 34 Block 5 North Range 6 West New Westminster District Plan 11945	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9400 Alexandra Road	012-032-573	West Half Lot 7 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9420 Alexandra Road	004-204-662	East Half Lot 7 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9440 Alexandra Road	012-032-581	West Half Lot 8 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.
9480 Alexandra Road	001-084-372	East Half Lot 8 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.

Address	PID	Legal Address	Zoning	Owner
9500 Alexandra Road	008-130-990	West Half Lot 9 Block "C" Section 34 Block 5 North Range 6 West New Westminster District Plan 1224	Single Detached (RS1/F)	First Richmond North Shopping Centres Ltd.

6. Registration of an aircraft noise indemnity covenant for non-sensitive uses on title.
7. Registration of a flood plain covenant on title identifying a minimum habitable elevation of 2.6 m GSC. The proposed development is designed to 2.6m GSC with the exception of the proposed Walmart loading dock/service area, main lobby including the proposed clinic area and Buildings M and N along Alderbridge Way. The Developer will be required to submit a survey of Alderbridge Way (May Drive to High Street), set these finished floor elevations as high as possible and provide a supportable rationale, which are all subject to the approval of the Manager of Buildings and the Director of Engineering.
8. Registration of a legal agreement on title ensuring that the only means of vehicle access is to Alexandra Road, the proposed High Street and the proposed extension of May Drive and that there be no direct vehicle access to Alderbridge Way or Garden City Road and to the approval of the Director of Development.
9. Registration of a legal agreement that ensures the provision of the following required Transportation Demand Management (TDM) measures to the approval of the Director of Transportation including:
 - a. Bicycle storage (in addition to the bylaw requirements): The Developer to provide a 25% increase in the total number of Class 1 and Class 2 bicycle spaces (i.e. an additional 25 Class 1 stalls and an additional 33 Class 2 stalls);
 - b. Two (2) separate end-of-trip bicycle facilities: The Developer to provide facilities consisting of three (3) water closets per gender, 2 wash basins per gender and 3 showers per gender. Based on the layout of the development, these facilities should be spread out between the western and eastern portions of the site; and
 - c. Electric Vehicle (EV) Plug-ins: The Developer to provide pre-ducting to 10% of the total number of parking stalls provided on-site for future installation of charging stations and designated as such. In addition, and as part of the proposed development, equip a minimum of four (4) parking stalls (i.e., 2 parking stalls on the west development parcel and 2 parking stalls on the east development parcel near the proposed Walmart Store) with EV charging stations (240V).
10. Submission of a voluntary cash contribution of \$3,450,000 to the City for acquisition of 4560/62 and 4580 Garden City Road. This is to be accompanied with a legal agreement, which indicates that the City is not obliged to acquire these properties by any specific date. The City will reimburse the Developer with any surplus funds from their \$3,450,000 contribution for these 2 properties, if there is any residual funding for these lots after all City costs have been paid.
11. Submission of a Letter of Credit (LOC) acceptable to the City, in the amount of for the construction of the Connector Road. The LOC is to be replaced with a cash contribution based on the construction value in the year that the City constructs the Connector Road. The estimated construction value in 2013 is \$2,166,382, which has been escalated by an assumed 4% annual inflation factor to arrive at the estimated construction value of \$3,206,774 in

2023. The LOC is to be accompanied with a legal agreement enabling the City to use the LOC for road construction.

a. Alexandra/Leslie Connector Road Construction Cost Forecasts are as follows:

Estimated 2012 Construction Cost = \$ 2,083,059
 Forecasted Annual Inflation = 4%
 Forecasted 2013 Construction Cost = \$ 2,166,381
 Forecasted 2023 Construction Cost = \$ 3,206,774

Year	Forecasted Construction Cost	Annual Inflation
2013	\$ 2,166,381	4%
2014	\$ 2,253,037	4%
2015	\$ 2,343,158	4%
2016	\$ 2,436,884	4%
2017	\$ 2,534,360	4%
2018	\$ 2,635,734	4%
2019	\$ 2,741,164	4%
2020	\$ 2,850,810	4%
2021	\$ 2,964,842	4%
2022	\$ 3,083,436	4%
2023	\$ 3,206,774	4%

12. The Developer has voluntarily agreed to incorporate on-site public art installation(s) along the Alexandra Way public pedestrian corridor in accordance with the City’s Public Art Policy with an approximate value of \$155,077 (387,692 ft² x \$0.40/ft²) and to the approval of the Director of Development. A letter of credit in the amount of \$155,077 (387,692 ft² x \$0.40/ft²) is a requirement as security for public art to be installed on-site. If the Developer elects not to install on-site public art, then the Developer must agree to voluntarily contribute \$0.40 per buildable square foot or \$155,077 (387,692 ft² x \$0.40/ft²) to the City’s public art fund.
13. City acceptance of the Developer’s offer to voluntarily contribute \$0.60 per buildable square foot for City Beautification or \$232,615 (i.e. 387,692 ft² x \$0.60/ft²) as part of the City’s West Cambie Area – Alexandra Interim Amenity Charges. A reduction to this contribution for the design and construction costs related to the Alexandra Way pedestrian corridor if any is to be determined by the Director of Development.
14. City acceptance of the Developer’s offer to voluntarily contribute \$0.07 per buildable square foot for Community and Engineering Planning or \$27,138.44 (i.e. 387,692 ft² x \$0.07/ft²) as part of the City’s West Cambie – Alexandra Interim Amenity Charges.
15. City acceptance of the Developer’s offer to voluntarily contribute \$90,000 for 3 bus shelters (\$25,000 each for the bus shelter plus \$5,000 each for the bus landing pad) proposed at each of the following locations, if the Developer does not upgrade these bus stop locations through the Servicing Agreement and to the approval of the Director of Transportation:
 - a) north of Alexandra Road on the west side of Garden City Road,
 - b) south of Alderbridge Way on the west side of Garden City Road, and

- c) south of Odlin Road on the west side of Garden City Road or along Alderbridge Way if TransLink and Coast Mountain Bus Company agree to the necessary bus route revisions. In the event that the necessary bus route revisions are not made by Translink and Coast Mountain Bus company, the location for the bus shelter and landing pad will be pursued elsewhere near the vicinity of the subject site
16. Registration of a legal agreement(s) regarding the Developer's commitment to connect to the Alexandra District Energy Utility (ADEU), including the operation of and use of the ADEU and all associated obligations and agreement as determined by the Director of Engineering. The Developer has committed that between 63-69% of the proposed floor area or approximately 70% of the total annual heating and cooling energy demand will be serviced by the ADEU but this is subject to Council approval of amendments to the ADEU bylaw to allow less than 70% participation. However, participation in the ADEU will be limited to the large format tenants (Buildings A and the East Anchor Building – Walmart Store). More detailed energy modeling will be required to establish the extent of the energy demand represented by those tenants. The Developer will coordinate with Engineering staff to determine this demand as part of the Servicing Agreement process.
17. Processing of a Development Permit advanced to a sufficient level of detailed design and to the approval of the Director of Development.
18. Enter into a Servicing Agreement* for the design and construction of road improvements and site servicing. Works include, but may not be limited to the following:

A. Transportation & Traffic Requirements

1. Applicant responsible for the design and construction of the following frontage improvements and transition between those improvements and the existing condition outside the development site frontage (at a minimum 30:1 taper rate for Alderbridge Way and Garden City Road, and a minimum 20:1 taper rate for all internal roads) to the approval of the City. Please refer to Item 2 for additional frontage improvements at intersections. Note that while Servicing Agreement Items A1 and A2 provide a general description of the minimum frontage work requirements, the exact details and scope of the frontage works to be completed by the Developer would be confirmed via a functional road design to be prepared by the Developer and to the approval of the Director of Development, the Director of Transportation and the Director of Engineering.
- a) Alderbridge Way, from Garden City Road to May Drive (from south to north):
- enhance existing medians with decorative/gateway treatments, including but not limited to banners, landscaping, trees, hard landscaping, street lighting, etc.;
 - maintain two existing westbound traffic lanes;
 - maintain existing curb/gutter on the north side;
 - 1.5 m wide treed boulevard; and
 - 3.3 m wide shared pedestrian/cyclist path.
- b) Garden City Road, from Alderbridge Way to Alexandra Road (from west to east):
- enhance existing medians with decorative/gateway treatments, including but not limited to banners, landscaping, trees, hard landscaping, street lighting, etc.;
 - maintain two existing northbound traffic lanes;

- shift the existing northbound bicycle lane allowance onto the east boulevard;
 - 0.15 m wide curb/gutter;
 - 1.85 m wide treed boulevard;
 - 2.0 m wide bike lane;
 - 1.77 m wide grass buffer strip to separate sidewalk and bike lane;
 - 2.0 m wide sidewalk at property line; and
 - minimum 3.0 m wide building setback from property line (west to east) sloped and landscaped with dense plant material to the proposed building wall with perpendicular walkway connections to the public sidewalk including stairs or ramps from the required emergency exit doors along the back of the building as required by code; and
- c) Alexandra Road, from Garden City Road to eastern limit of the development site (from south to north):
- 2.0 m wide sidewalk;
 - 1.5 m wide boulevard;
 - 0.15 m wide curb;
 - minimum 9 m wide vehicular driving/parking surface (this pavement may be reduced to min. 6.2 m at mid-block locations where feasible); and
 - minimum 1.0 m wide shoulder.
- d) May Drive, from Alderbridge Way to Alexandra Road (from west to east):
- 2.0 m wide sidewalk;
 - 1.5 m wide boulevard;
 - 0.15 m wide curb;
 - 12.7 m wide vehicular driving/parking surface;
 - 0.15 m wide curb,
 - 1.5 m wide boulevard; and
 - 2.0 m wide sidewalk.
- e) High Street, from Alderbridge Way to Alexandra Road (from west to east):
- 2.0 m wide sidewalk;
 - 0.15 m wide curb;
 - 18.4 m wide vehicular driving/parking surface (i.e., 2 x 2.5 m wide parking lane or landscaped boulevard near intersections, 4 x 3.35 m wide traffic lanes);
 - 0.15 m wide curb; and
 - 2.0m wide sidewalk.
2. In addition to the frontage improvements noted in Item 1, the Developer is responsible for the design and construction of the following intersection improvements and to the approval of the Director of Transportation and the Director of Engineering.
- a) Alderbridge Way / May Drive
- Installation of a new traffic signal to include but not limited to the followings: signal pole, controller, base, hardware, pole base (City Centre decorative pole and street light fixture), detection, conduits (electrical and communications), signal indications, communications cable, electrical wiring and service

conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).

- Installation of an eastbound to northbound left-turn lane, with a minimum storage length of 60 m. Please note that while a portion of the left-turn lane may be accommodated within existing median, a minimum 1.2m wide median should be maintained.

b) Alderbridge Way / High Street

- Installation of a new traffic signal to include but not limited to the followings: signal pole, controller, base, hardware, pole base (City Centre decorative pole and street light fixture), detection, conduits (electrical and communications), signal indications, communications cable, electrical wiring and service conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).
- Installation of an eastbound to northbound left-turn lane, with a minimum storage length of 60 m. Please note that while a portion of the left-turn lane may be accommodated within the existing median, a minimum 1.2 m wide median should be maintained.
- Installation of a westbound to northbound right-turn lane, with a minimum storage length of 50 m, while maintaining the two westbound through lanes as noted in Item A1a) above.

c) Alderbridge Way / Garden City Road

- Upgrade of the existing traffic signal to include but not limited to the followings: signal pole, controller, base, hardware, pole base (City Centre decorative pole and street light fixture), detection, conduits (electrical and communications), signal indications, communications cable, electrical wiring and service conductors, APS (Accessible Pedestrian Signals) and illuminated street name sign(s).
- Installation of dual left-turn lanes on the southbound (a minimum total storage length of 200 m), northbound (a minimum total storage length of 200 m) and westbound approaches (a minimum total storage length of 190 m), while maintaining all other existing traffic lanes. Please note that while a portion of the left-turn lanes may be accommodated within existing medians, a minimum 1.2 m wide median should be maintained on all intersection approaches.
- Installation of a westbound to northbound right-turn lane, with a minimum storage length of 50 m, while maintaining the two westbound through lanes as noted in Item A1a) above.
- Provision of an acceptable transition between the above noted intersection improvements, the proposed boulevard treatment and the building setback landscape design (at a minimum 30:1 taper rate for Alderbridge Way and Garden City Road) to the approval of the Director of Development and the Director of Transportation.
- Installation of an accessible bus shelter and landing pad (9 m x 3 m) on the east side of Garden City Road, just north of Alderbridge Way.

- d) Garden City Road / Alexandra Road
 - Closure of existing median opening with curb/gutter and decorative median treatments.
 - e) Installation of special crosswalks with downward lighting and associated equipments at the following locations:
 - High Street, at the proposed access to the development site;
 - High Street, at Alexandra Road; and
 - Alexandra Road, at High Street.
 - f) Construction Timing: all frontage improvements should be completed prior to opening of development.
3. All Transportation requirements shall be as per City requirements and approved by the Director of Transportation.

B. Site Servicing & Connection Requirements

1. Storm: Required storm sewer improvements include the following:
 - a) All storm drainage must be directed to Alexandra Road and west to Garden City Road except for road run-off from the south half of High Street and May Drive that may be drained to Alderbridge Way;
 - b) Provide a 600mm diameter storm sewer from existing manhole (manhole D26 in the analysis) located at the intersection of Alderbridge Way and future May Drive to proposed manhole D4 located at the intersection of Alexandra Road and future May Drive with an approximate length of 170m;
 - c) Provide a 600mm diameter storm sewer from existing manhole (manhole D29 in the analysis) located at the intersection of Alderbridge Way and future High Street to proposed manhole D6+ located at the intersection of Alexandra Road and future High Street with an approximate length of 170m;
 - d) Upgrade the existing ditch at Alexandra Road to a 600mm diameter storm main from intersection of Alexandra Road and future May Drive (manhole D4 in the analysis) west to manhole D5 with an approximate length of 100m;
 - e) Upgrade the existing ditch at Alexandra Road to a 900mm diameter storm main from manhole D5 west to manhole D6 with an approximate length of 100m;
 - f) Upgrade the existing ditch at Alexandra Road to a 900mm diameter storm main from manhole D6 west to manhole D8 with an approximate length of 50m;
 - g) Upgrade the existing ditch at Alexandra Road to a 1050mm diameter storm main from manhole D8 west to manhole D15 with an approximate length of 95m;
 - h) Upgrade the existing ditch at Alexandra Road to a 1050mm diameter storm main from manhole D15 west to manhole D16 with an approximate length of 80m; and
 - i) A new tie-in will be required to convey flow from the proposed drainage system in Alexandra Road to the existing 1200mm diameter storm sewer located at the west side of Garden City. Details and location of the new crossing/tie-in will be determined via the Servicing Agreement and to the approval of the Director of Engineering.

2. Sanitary: Required sanitary sewer improvements include the following:
 - a) Alexandra Road: Construct a 250mm diameter sanitary sewer from the proposed transition point (west of Dubbert Street) to May Drive;
 - b) May Drive (future road extension): Construct a 375mm diameter sanitary sewer from Alexandra Rd to Tomicki Ave and connect to the existing system. If a road dedication does not exist, then a minimum 6.0 m wide right-of-way will be required. The pipe sizes may be revised at the Servicing Agreement stage as additional information becomes available for the servicing requirements of the proposed adjacent developments; and
 - c) High Street: Construct a 200mm diameter sanitary sewer and connect to the system on Alexandra Road. The upstream end of the sanitary sewer will be determined by the location of the service connection for this development.
 - d) Provision of a minimum 6.0 m wide utility Statutory Right-of-Way (SRW) is required for the proposed sanitary sewer at future May Drive between Alexandra Road and Tomicki Avenue. The location of the required SRW is to be determined later either within 9451 and 9471 Alexandra Road and measured 6.0 m from the east property lines of these 2 properties or located on 9491 Alexandra Road and measured 6.0 m from the west property line.
3. Water: Required water service improvements include the following:
 - a) Using the OCP 2021 Maximum Day Model, there is 717.10 L/s available at 20 psi residual at 4740 Garden City Road, 529.40 L/s at 20 psi residual at 4600 Garden City Road and 220.50 L/s at 20 psi residual at 9411 Alexandra Road;
 - b) Based on the proposed rezoning, the site requires a minimum fire flow of 200 L/s;
 - c) Water analysis is not required to determine upgrades to achieve minimum requirements;
 - d) Once the building design is confirmed at the Building Permit stage, the Developer is required to submit fire flow calculations signed and sealed by a professional engineer based on the Fire Underwriter Survey or ISO Standards to confirm that there is adequate available flow;
 - e) A new watermain is required on Alexandra Road, High Street and May Drive along the development frontages (design to be via the servicing agreement); and
 - f) Via the Servicing Agreement the City will review the impact of the proposed works on the existing 300mm diameter asbestos-cement (AC) watermain on Garden City Road. The City will work with the Developer to coordinate the replacement/relocation of the AC watermain, if required.
4. Hydro/Telephone: Pre-ducting works are required on the following proposed roads subject to confirmation from BC Hydro and telecom providers:
 - c) proposed May Drive (from Alderbridge Way to Alexandra Road); and
 - d) proposed High Street (from Alderbridge way to Alexandra Road).

The removal of existing power poles and installation of underground pre-duct along the east side of Garden City Road and along the north side of Alexandra Road will be at the discretion of BC Hydro.

5. All servicing infrastructure works shall be as per City requirements and to the approval of the Director of Engineering.
6. The Developer is required to contact private utility companies to learn of their requirements; the developer must provide rights-of-ways to accommodate their equipment (kiosks, vista, transformers, etc.) on the development site (i.e. not within City road dedication or right-of-way), subject to concurrence from the private utility companies.

Prior to a Development Permit' being forwarded to the Development Permit Panel for consideration, the developer is required to:

1. Prior to issuance of a tree cutting permit after Public Hearing, the developer is required to submit a letter of credit acceptable to the City in the amount of \$192,000 (i.e., 344 replacement trees x \$500 each plus 3 significant trees and 1 high value tree x \$5,000 each) as security for replacement trees.
2. Further design development of the architectural facade designs, site planning and landscape design are required at the Development Permit stage. Advance the concept design and submit more detailed design drawings to ensure the establishment of a compact, vibrant, pedestrian oriented, urban village centre that will become the retail/commercial heart of the Alexandra Neighbourhood and to the approval of the Director of Development.
3. Expand the design concept and submit more detailed design drawings to ensure the creation of an attractive, accessible, activated, comfortable, pedestrian-friendly retail/commercial environment with strong pedestrian scale streetwall definition, the possibility for restaurants/shops to extend out toward the back of sidewalk including numerous small shops plus an interesting mix and variety of retail shopping opportunities along the High Street and to the approval of the Director of Development.
4. Improve the concept design and submit more detailed design drawings to ensure the continuation of the Alexandra Way pedestrian corridor through the proposed development with high-quality pedestrian enhancements, punctuated with periodic pedestrian plaza areas, activated to attract pedestrian traffic and facilitate seasonal events, designed with ample pedestrian space and opportunities to encourage pedestrians to sit/linger and incorporating other features such as public art and focal elements that add interest and variety to the pedestrian experience and to the approval of the Director of Development.
5. Neighbourhood Plan, Design Guidelines Compliance and Urban Design Improvements: Proposed deviations from WCAP neighbourhood structure and design guidelines can be dealt with at the Development Permit stage. Urban design improvements required at the Development Permit stage include advancing the concept design and resubmission of more detailed design drawings to ensure:
 - the establishment of a compact, vibrant, pedestrian oriented, urban village centre that is integral part of the neighbourhood and will become the retail/commercial heart of the Alexandra Neighbourhood;
 - an attractive, accessible, activated, comfortable, pedestrian-friendly retail/commercial environment with strong pedestrian scale streetwall definition, the possibility for restaurants/shops to extend out to the back of sidewalk including numerous small

neighbourhood scale character shops plus an interesting mix and variety of retail shopping opportunities along the High Street;

- a higher quality architectural expression around the entire perimeter of the development site by extending the signature corner treatments (e.g. Alderbridge Way and Garden City Road) further along the building faces on all perimeter building facades including greater horizontal articulation and permeability of perimeter building facades to add more visual interest through enhanced architectural character and an appropriate proportion of transparent and opaque combination of surfaces for the proposed buildings that face the perimeter streets around the exterior of the proposed development;
 - the strong presence and continuation of the Alexandra Way pedestrian corridor, and neighbourhood pedestrian spine, through the proposed development with high-quality pavements and contrasting colours that identify the direction of Alexandra Way to and from the neighbourhood to the urban plaza at the corner of Garden City Road and Alderbridge Way. The Alexandra Way pedestrian corridor should be punctuated with periodic pedestrian plaza areas and pedestrian amenities to activate and attract pedestrian traffic and facilitate seasonal events, designed with ample pedestrian space and focused on creating opportunities to encourage pedestrians to sit and linger. The plaza spaces should incorporate other features such as public art and focal elements that add interest and variety to the pedestrian experience. The ground plane paving treatment along the Alexandra Way pedestrian corridor through the proposed development site should include a distinctive and continuous decorative paving treatment extending from building face to building face (along this route through the proposed development) with significant differentiation between the Alexandra Way corridor and other the other internal streets and sidewalks within the overall development;
 - a reduction in the amount of signage that is coordinated with the proposed floor plans including the better integration or elimination of redundant signage such as the proposed “Directional Signage” pylons and stronger coordination with the enhanced architectural character of proposed buildings at corner locations;
 - safe and efficient pedestrian movement that reflects the direction of the pedestrian traffic toward the Walmart store within the parking area including consideration of east-west oriented parking aisles within the open parking area on the east development parcel with wider bio-swales;
 - better coordination between the landscape and architectural design, including a stronger reliance on the informal clustering of large coniferous tree planting around the perimeter of the proposed development site to enhance the massing and materials articulation/treatment of the building facades, particularly along the Alderbridge Way frontage that is visible from the Garden City Lands to the south; and
 - acceptable resolution of any non-compliance with all relevant design guidelines.
6. Provision of adequate and appropriate refuse and recycling facilities for each building to and to the approval of the Director of Development and the Director of Public Works.
 7. CPTED: All parkade areas (walls, columns and ceiling) to be painted with reflective white paint and come with lighting levels as required by the BC Building Code. The open parking areas will be well lit with fixtures providing good colour rendition. A complete and

comprehensive CPTED review of the development will be provided with the Development Permit submission.

8. Submission of a landscape plan prepared by a BCSLA registered landscape architect to the approval of the Director of Development including the deposit of a landscape security based on 100% of the cost estimate provided by the landscape architect. The Landscape Plan should:
 - comply with the OCP guidelines regarding Lane Establishment and Arterial Road Redevelopment Policies and should not include hedges along the front property line;
 - include a mix of coniferous and deciduous trees;
 - provide 4 large specimen trees on the planting plan to replace the existing 3 significant trees plus 1 high value tree proposed for removal; and
 - include the 344 (172 x 2) required replacement trees with the following minimum sizes:

No. of Replacement Trees	Minimum Caliper of Deciduous Tree	or	Minimum Height of Coniferous Tree
344	10 cm caliper		3.5 m height

If required replacement trees cannot be accommodated on-site, a cash-in-lieu contribution in the amount of \$500/tree to the City's Tree Compensation Fund for off-site planting is required or \$5,000 each for significant or high value trees not provided on site.

Prior to Building Permit Issuance, the developer must complete the following requirements:

1. Submission of a Construction Parking and Traffic Management Plan to the Transportation Division. Management Plan shall include location for parking for services, deliveries, workers, loading, application for any lane closures, and proper construction traffic controls as per Traffic Control Manual for works on Roadways (by Ministry of Transportation) and MMCD Traffic Regulation Section 01570 and approved by of the Director of Transportation.
2. Payment of the Supplementary Local Area DCC for the Alexandra Neighbourhood.
3. Payment of the \$480,738.08 (387,692 ft² x \$1.24/ft²) indexed at the applicable rate, in accordance with the Alexandra Neighbourhood Development Agreement.
4. If applicable, payment of latecomer agreement charges associated with eligible latecomer works.
5. Obtain a Building Permit (BP) for any construction hoarding. If construction hoarding is required to temporarily occupy a public street, the air space above a public street, or any part thereof, additional City approvals and associated fees may be required as part of the Building Permit. For additional information, contact the Building Approvals Division at 604-276-4285.

Notes:

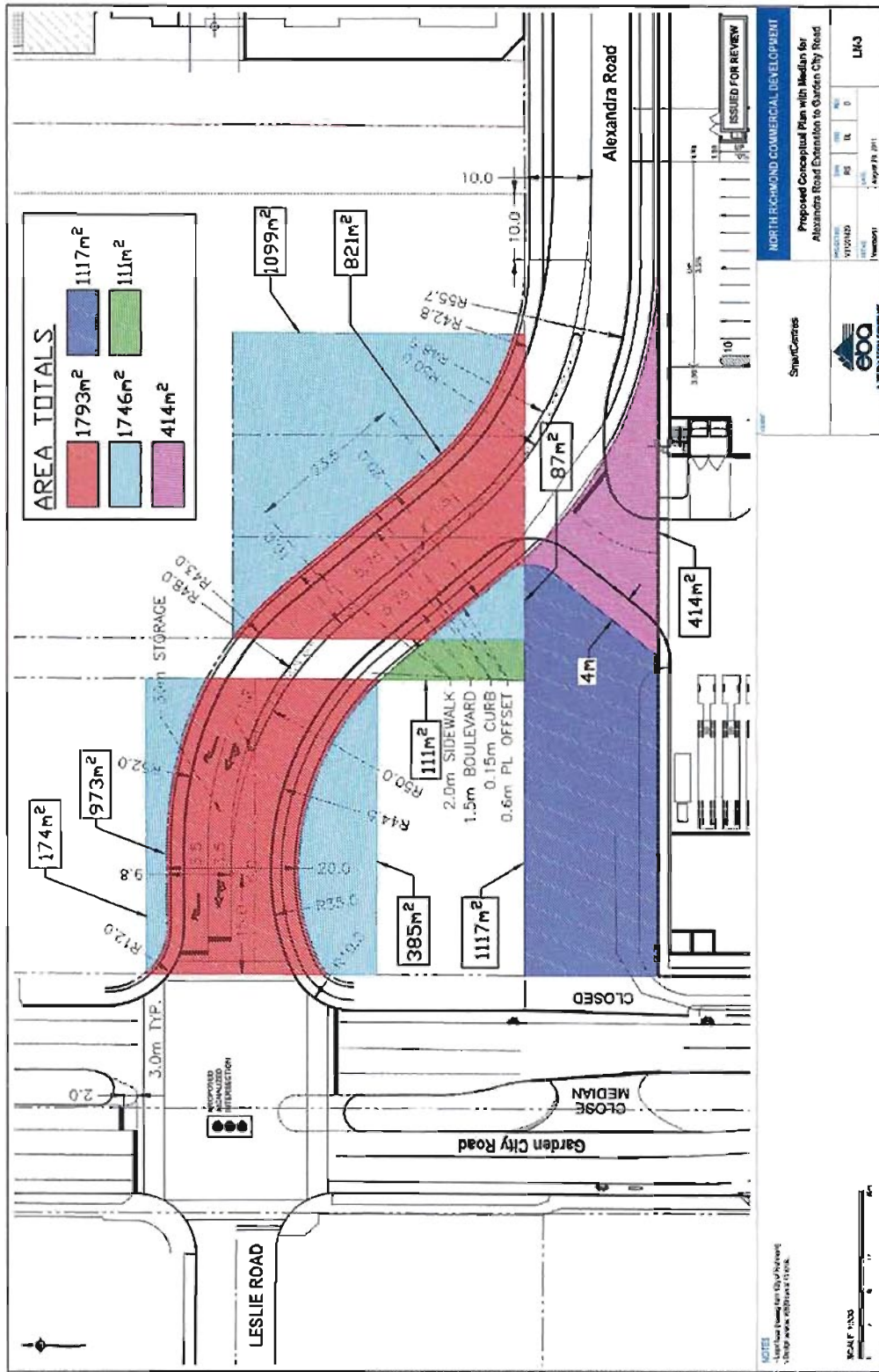
- * This requires a separate application.
- Where the Director of Development deems appropriate, the preceding agreements are to be drawn not only as personal covenants of the property owner but also as covenants pursuant to Section 219 of the Land Title Act.
- All agreements to be registered in the Land Title Office shall have priority over all such liens, charges and encumbrances as is considered advisable by the Director of Development. All agreements to be registered in the Land Title Office shall, unless the Director of Development determines otherwise, be fully registered in the Land Title Office prior to enactment of the appropriate bylaw.
- The preceding agreements shall provide security to the City including indemnities, warranties, equitable/rent charges, letters of credit and withholding permits, as deemed necessary or advisable by the Director of Development. All agreements shall be in a form and content satisfactory to the Director of Development.
- Additional legal agreements, as determined via the subject development's Servicing Agreement(s) and/or Development Permit(s), and/or Building Permit(s) to the approval of the Director of Engineering may be required including, but not limited to, site investigation, testing, monitoring, site preparation, de-watering, drilling, underpinning, anchoring, shoring, piling, pre-loading, ground densification or other activities that may result in settlement, displacement, subsidence, damage or nuisance to City and private utility infrastructure.

[Signed original on file]

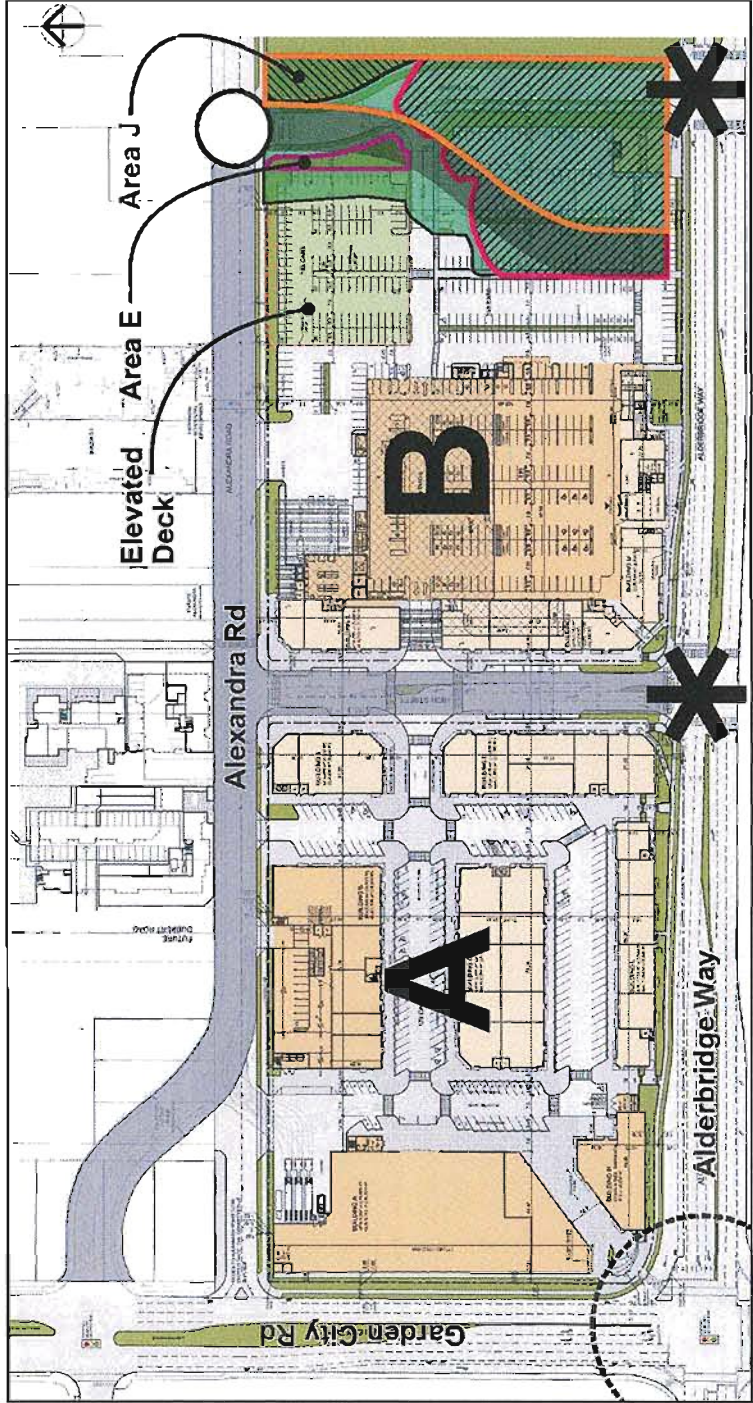
Signed

Date

Alexandra/Leslie Connector Road Realignment & Land Requirements



West Cambie Park & Environmentally Sensitive Area – Proposed Adjustments



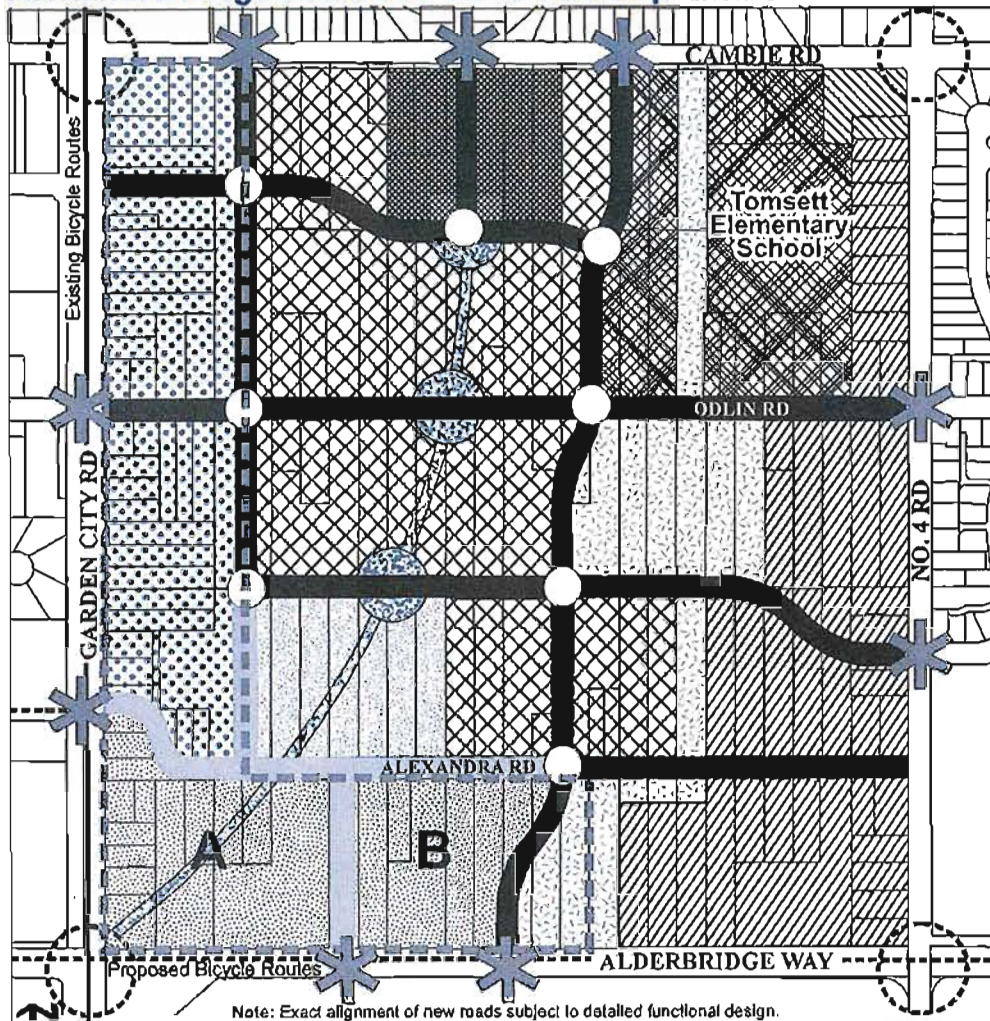
July 25, 2012

- ESA & West Cambie Natural Park on proposed development site approx. 6,116.80 sq meters (1.51 ac)
- Existing ESA (Based on OCP) on proposed development site approx. 10,400 sq meters (2.57 ac)
- Proposed ESA (Based on detailed field investigation) approx. 6,354 sq meters (1.57 ac)
- Applicant proposed Area J Land Transfer approx. 654 sq meters (0.16 ac)
- Applicant proposed Elevated Deck as PROP-SRW approx. 3,378 sq meters (0.83 ac)
- Application proposed Area E as PROP-SRW approx. 344 sq meters (0.09 ac)

West Cambie Alexandra Neighbourhood Land Use Map (existing)

City of Richmond

Alexandra Neighbourhood Land Use Map *Bylaw 8945 2012/10/15*



Note: Exact alignment of new roads subject to detailed functional design.

Area of No Housing Affected by Aircraft Noise	Residential Area 2 0.65 base FAR (Max. 0.75 FAR with density bonusing for affordable housing). 2 & 3-storey Townhouses.	Park: North Park Way, Central Park, South Park Way
Business/Office – office over retail FAR up to 1.25	Mixed Use: Hotel, office and streetfront retail commercial. Area A: Min. 1.25 FAR up to 2.0. Area B: Large and small floor plate up to 1.0 FAR.	Alexandra Way (Public Rights of Passage Right-of-way)
Convenience Commercial	Mixed Use: • abutting the High Street, medium density residential over retail; • not abutting the High Street, medium density residential. 1.25 base FAR. Building heights low to mid-rise. (Max. 1.60 FAR with density bonusing for affordable housing).	Proposed Roadways
Residential Area 1 1.50 base FAR (Max. 1.70 FAR with density bonusing for affordable housing). Townhouse, low-rise Apts. (4-storey typical).	Community Institutional	High Street
Residential Area 1A 1.60 base FAR (Max. 1.75 FAR with density bonusing for affordable housing). Townhouse, low-rise Apts. (6-storey maximum).		New Traffic Signals
		Feature Intersections – details to be developed
		Feature Landmarks in combination with Traffic Calming Measures

Also refer to Section 8.4.5 – Alexandra District Energy Unit regarding district energy density bonusing policies.

Original Adoption: September 12, 1988 / Plan Adoption: July 24, 2006
3186783

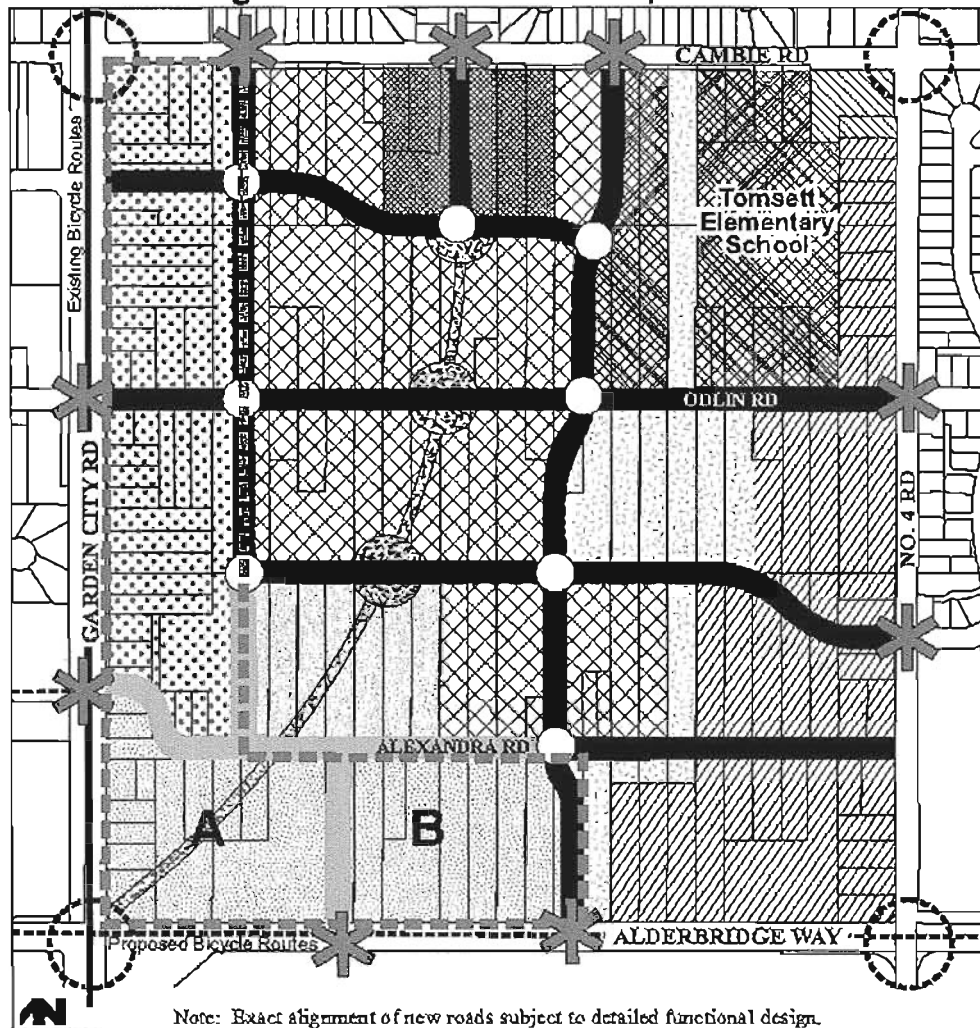
West Cambie Area Plan 50

West Cambie Alexandra Neighbourhood Land Use Map (Proposed)

City of Richmond

Schedule A attached to and forming part of Bylaw 8865

Alexandra Neighbourhood Land Use Map



Note: Exact alignment of new roads subject to detailed functional design.

	Area of No Housing Affected by Aircraft Noise		Residential Area 2 0.65 base FAR (Max. 0.75 FAR with density bonusing for affordable housing). 2 & 3-storey Townhouses.		Park: North Park Way, Central Park, South Park Way
	Business/Office - office over retail FAR up 1.25		Mixed Use: Hotel, office and street-front retail/commercial. Area A: Min. 0.60 FAR up to 2.0 Area B: Large and small floor plate up to 1.0 FAR		Alexandra Way (Public Rights of Passage Right-of-way)
	Convenience Commercial		Mixed Use: - abutting the High Street, medium density residential over retail; - not abutting the High Street, medium density residential. 1.25 base FAR. Building heights low to mid-rise. (Max. 1.50 FAR with density bonusing for affordable housing).		Proposed Roadways
	Residential Area 1 1.50 base FAR (Max. 1.70 FAR with density bonusing for affordable housing). Townhouse, low-rise Apts. (4-storey typical)				High Street
	Residential Area 1A 1.50 base FAR (Max. 1.75 FAR with density bonusing for affordable housing). Townhouse, Low-rise Apts. (6-storey maximum).				New Traffic Signals
	Community Institutional				Feature Intersections - details to be developed
					Feature Landmarks In Combination with Traffic Calming Measures

West Cambie Area Plan

Summary of Public Comments & Correspondence

The following is a chronological listing of public correspondence received by the City regarding this rezoning application since June 2011

Date	Sender	Type
June 2, 2011	Polygon	Letter
Dec. 19, 2012	S. Sangha	Email
Dec. 24, 2012	M. Woodward	Email
Dec. 26, 2012	L. Jones	Email
Dec. 28, 2012	A. Gauld	Email
Dec. 29, 2012	J. Cross	Email
Jan. 2, 2013	R. Mathias	Email
Jan. 5, 2013	D. Burgess	Email
Jan. 21, 2013	D. Loveland	Email
Jan. 21, 2013	R. Vetter	Email
Feb. 25, 2013	C. May	Email
Mar. 8, 2013	D. Whalen	Letter
Mar. 17, 2013	O. Tkatcheva	Email
Mar. 17, 2013	B. Mathias	Email
Mar. 17, 2013	R. Mathias	Email
Mar. 18/22, 2013	K. Eliot	Email
Mar. 18, 2013	W. So	Email
Mar. 18, 2013	P. Price	Email
Mar. 18, 2013	A. May	Email
Mar. 18/22, 2013	B. & N. Houle	Email
Mar. 18, 2013	J. Terborg	Email
Mar. 18, 2013	R. Xavier	Email
Mar. 19, 2013	R. McCallion	Email
Mar. 20, 2013	D. Whalen	Email
April 9, 2013	C. Day	Letter

Note: There were articles or letters to the editor in the local newspapers on the following dates January 11 and 23, 2013, March 15 and 27, 2013 that appear to coincide with the timing of the majority of correspondence from the general public on this rezoning application.

The follow pages of this attachment contain copies of the actual public correspondence.



June 2, 2011

City of Richmond
6911 No. 3 Road
Richmond, BC
V6Y 2C1

Attention: *Brian Jackson*
Director of Development,
Development Applications Division

Dear Brian,

Re: Proposed SmartCentres West Cambie Project

This letter is to confirm our concern regarding the current design of the proposed SmartCentres project located between Garden City and May Drive and, Alderbridge Way and Alexandra Road.

As neighbors on Alexandra Road, we have appreciated the effort that SmartCentres has put into the current design to create internally pedestrian friendly neighborhood streetscapes, resulting in a vast improvement over the typical suburban big box power centre. Unfortunately, this responsible and pedestrian friendly planning has not been applied externally to their frontage along Alexandra Road.

Alexandra Road is an important neighborhood street that has existing and future residential front doors and living areas. Design detailing and care is required to create a friendly streetscape that allows for the transition between retail and residential. Alexandra Road is not and can not become a service alley. Commercial 24 hour loading docks simply can not front directly on neighborhood streets such as Alexandra Road. We have raised this legitimate planning concern to SmartCentres. However, no proposed resolution to this has been presented to us.

In the spirit of cooperation, we are willing to work and contribute our time in developing an acceptable and proper design solution with both SmartCentres and the City of Richmond. Our goal is to create a vibrant and pedestrian friendly neighborhood for the existing and future residents of the West Cambie neighborhood.

Sincerely,
Polygon Development 269 Ltd.

Chris Ho
Vice President, Development
/ch

From: steve sangha [mailto:stevesangha@shaw.ca]
Sent: Wednesday, 19 December 2012 7:23 PM
To: MayorandCouncilors
Subject: Re Walmart development Attn Bill McNulty
Importance: High

Our family has owned properties 4560/4562 Garden City Road for over 40 years. It is directly across from Leslie Road. Over the past few years realtors (which I believe act on behalf of smart centres) have offered real estate contracts which we have signed over the past 3 years. The contract expires then they want us to sign again for another period of time – never actually following through with the purchase. After being tirelessly fed on with these real estate contracts that never get fulfilled or expire without purchaser fulfilling their commitment, my family is sick of being used as a pawn to extract money or use us as a leverage to get money from the city. Smart centres should buy the property and build the road it was intended from the beginning. There is huge traffic congestion already having Alexandra/ Garden City Road intersection so close to Alderbridge Way/Garden City Road – throw in a new development without figuring the road accessibility would be ludicrous.

The worst thing about this is that Smart Centres has totally ruined a neighbourhood I grew up as a child. Many houses are boarded up. Vagrants and homeless people wander and search for things to sell or pawn. Our tenants (one which lived there for 14 years) moved because of theft and break ins. We had nice families leave because the children no longer felt safe. It has been especially bad since last two years. A house actually exploded after a homeless person tried to make a fire or steal metal from a gas line. It has been over ten years and smart Centres bought house for \$200,000 and now they want City to pay for land acquisition costs for a road. Another disturbing aspect is how the City of Richmond planners agreed to even consider the proposal of splitting the costs of land acquisition. Smart Centres has already leased out the entire commercial project to other tenants at a substantial profit.

If they have made their final offer then let it be. Don't hold the neighborhood hostage for another 5 years. Let them sell it to another developer or scrap the entire project and bring back single family homes. But the idea that the properties are not obtainable is totally false. We have real estate contracts and correspondence to prove otherwise.

S. Sangha

778-228-6872

From: Meredith Woodward [mailto:mlbw09@gmail.com]
Sent: Monday, 24 December 2012 8:23 AM
To: MayorandCouncillors
Subject: Re: Walmart Proposal

Dear Mayor and Councillors

I am writing to voice opposition to the proposal to build a Walmart in Richmond. We do not need another big box store. We have enough. Walmart's profits are derived from cheap goods being manufactured offshore in unacceptable working conditions. Ultimately this model is bad news for Canadian manufacturing and retail businesses. Let's draw the line here.

Council has a good track record for making responsible decisions around controversial issues, decisions that have a rational human basis, rather than a solely economic one. Taking a stand against a Walmart invasion would be courageous, forward-thinking and supportive of the local and Canadian economy.

With this in mind, I would also support the retaining and resorationg of the lands along the north side of Alderbridge Way from Garden City to No. 4 Road. The events of the last few years underlines that we need to take better care of our environment. This would be one small step in that direction.

Thank you all for your service to this community.

Sincerely,
Meredith Woodward



422-4500 Westwater Drive, Richmond, BC V7E 6S1
604-274-7601
www.mytripjournal.com/mmsw

From: lv.jones@telus.net [mailto:lv.jones@telus.net]

Sent: Wednesday, 26 December 2012 1:17 PM

To: Mayor and Councillors

Subject: walmart land use

to whom it may concern, the use of the walmart land must include taking care of the wooded area left. The digging and earth movement has disturbed the ground and drainage, causing flooding of trees, flora and fauna and destruction. There are coyotes, owls, hawks and many other smaller species that call this place home.

A few weeks ago I watched as the last two beautiful mature trees in all their fall colour, on the construction site on Minoru blvd. across from the mall, as they were torn down branch by branch with a digger. They stood for at least 30 to 40 years with many others keeping our air clean providing shade for us, and home to many song birds and squirrels. I stood alone in the cool morning sunshine, helpless to stop it not a tree or a blade of grass was left just waste and barren. My heart still aches to have witness that total disregard to living earth. What we are doing to this once lovely quiet healthy city? Please do something to save what little is left at walmart land where the coyote, owls and hawks live, and garden city land where they hunt. We are soon going to need more than higher dykes to save us from ourselves. We are caretakers of this earth not owners. What are we leaving for our children and grandchildren? Thank you for your help, caring resident Linda Jones.



From: Angela Gauld [mailto:angelag@shaw.ca]
Sent: Friday, 28 December 2012 11:08 AM
To: Mayor and Councillors
Subject: Proposed Walmart Mall

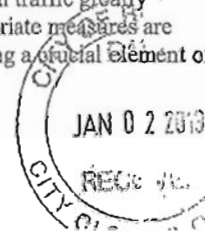
Dear Mr. Brodie and Councillors

It now seems that Richmond residents are to have another Walmart, in spite of that company's well-known dubious business practices, and their tolerance of dangerous working conditions in third world countries. And, what is to happen to Lansdowne when we will have yet another mall selling cheap imported goods? Shall we have a giant white elephant on our hands?

If this new mall is a *fait accompli*, then please at least ensure that the mixed urban forest along the north side of Alderbridge Way from Garden City Road to No. 4 Road is retained and restored. This is an important wildlife corridor and provides a natural viewscape, a commodity fast disappearing in our city. Please also insist upon retaining as much natural park as possible in the Alexandra area around the proposed Walmart Mall. Part of the fast-diminishing pleasure of being a Richmond resident is the enjoyment of what little wildlife is left to us. Natural viewscales and abundant green spaces and wildlife are a major part of what makes any city "appealing and liveable".

Finally, because I live close to Garden City Road, the effect of another major mall on local traffic greatly concerns me, and if this project must go ahead, I hope that it will not be built until appropriate measures are already well-established to cope with the inevitable increase in traffic, such measures being a crucial element of "well-managed community".

Respectfully
Angela Gauld



From: Joseph Cross [mailto:josephcrossart@me.com]
Sent: Saturday, 29 December 2012 3:40 PM
To: Mayor and Councillors
Subject: Walmart Mall

Dear Mayor & members of Council,

At a time of shifts and changes to municipal landscapes due to extreme weather conditions and climate weirding, it puzzles me as to why Richmond Council would even consider building a huge box store (one with a dubious corporate reputation at that), near the Garden City Lands.

Parks and green spaces are necessary for the health and wellbeing of residents, for diversity of wildlife, for the mitigation of flooding, recharging aquifers, and for cooling and cleansing the air in summer. Priceless.

The Garden City Lands serve an important ecological balance to the commercial landscape that has swallowed up valuable green spaces, and impacted our infrastructure. Municipalities are rethinking the amount of green space versus hard surfaces in their boundaries, and I would encourage you to reconsider this plan.

Looking forward to a forward thinking approach to land use.



Regards,

Joseph & Sharon Cross

-----Original Message-----

From: Mathias, Richard [<mailto:richard.mathias@ucb.ca>]
Sent: Sunday, 23 December 2012 9:48 PM
To: MayorandCouncillors
Subject: Walmart Site

We need to do much better in order to live up to our motto than to destroy the wild (undeveloped) lands proposed for the Walmart site.

Please reconsider this unnecessary project and protect our City by Nature.

Richard Mathias
6280 Doulton Ave
Richmond



From: DON BURGESS [mailto:dondoc13@hotmail.com]
Sent: Saturday, 05 January 2013 10:19 AM
; Mayor and Councillors; gardencitylands@shaw.ca
Subject: Garden City lands and Walmart proposal

Dear Mayor and Councillors,

I have been following the proposals for the Wal-mart development. I would like to request that the Council ask that Wal-mart create as much green space and trees as possible if it builds, and that the council be resolute to enforce this condition. Richmond is already becoming a high-rise asphalt jungle, and this is a tragedy in a new city with the possibility a good living environment.

I would also propose that the City retain a green corridor in the area north of Alderbridge Way from Garden City Rd across to Number 4 road.

Minoru Park remains the only good parkland area in central Richmond, and we need more.

I thank you for your attention,

Yours sincerely,

Donald Burgess
10857 Canso Crescent
Richmond BC
V6E 5 B6.

PHOTOCOPIED

JAN 8/2013

& DISTRIBUTED



From: Damien Loveland [mailto:dee10101@yahoo.ca]
Sent: Monday, 21 January 2013 11:54 AM
To: Mayor and Councillors
Subject: Walmart - Garden City

Dear Mayor and Councillors:

With respect to the Walmart proposal, I'd like to put in a request for Richmond to retain and restore the mixed urban forest along the north side of Alderbridge Way from Garden City Rd to No. 4 Rd (as a wildlife corridor and an important part of the natural viewscape as seen from Garden City Rd, the Garden City Lands, Westminster Hwy, etc.) and retain as much natural park as possible in the Alexandra Area around the Walmart mall (if it is built).

Best Regards,

Damien Loveland

PHOTOCOPIED

J. 23/2013

REBUTED



From: Richard Vetter [mailto:Richard.Vetter@manulifesecurities.ca]
Sent: Monday, 21 January 2013 9:17 PM
To: Mayor and Councillors
Subject: FW: Wildlife corridor / mixed urban forest

JAN 23/2013
W
& DISTRIBUTED

Dear mayor and councillors:

First off, I have to admit, my involvement in our community isn't a shadow of what you and many others have and I am very grateful for what you do. I also know that you make decisions guided by the people who elect you into office and I'm going to give you my feelings on the Walmart shopping centre development and surrounding areas.

Long story short, you have the ability to mandate development guidance that could help preserve something human and peaceful in this corridor.

There's a page on Facebook called "You know you're from Richmond when..." Unfortunately, most of the posters are people who are glad they left. I'm one of the smaller group of people who are glad they stayed. I love living in and doing business in Richmond and rarely obsess about the past. My only desire is to see a sustainable and more human future.

Whatever happens to the Garden City Lands is still unknown at this stage. Whatever form it takes will either be beautifully framed or debauched by your decision on the wildlife corridor.

I would ask you to choose wisely and support those who are recommending a more sensitive solution than the path that unimpeded development will invariably take.

Thanks once again,

Richard Vetter

Richard Vetter, BA, CFP, CLU | WealthSmart Financial Group | Manulife Securities Incorporated | 3251 Chatham Street,
Richmond, BC V7E 6B8
Phone 604.241.4357 | Fax 604.676.2288 | www.wealthsmart.ca

This message is only to be read by the addressee and is not for public distribution. The sender is not responsible for distribution of this message beyond the addressee intended. All information in this message is confidential to the addressee and should be treated as such. To ensure that trading instructions are received and executed in a timely and accurate manner, please do not send any trading instructions via e-mail nor leave any such instructions on voicemail. Please contact me directly at 604-241-4357 in order to verbally confirm your instructions.

-----Original Message-----

From: Curtis May [mailto:c_may@shaw.ca]
Sent: Friday, 22 February 2013 4:38 PM
To: Mayor and Councillors
Subject: Walmart Development on Garden City

To whom it may concern of the Richmond Municipality,

It has come to my attention that a likely deal with Walmart developers to build a Walmart and other shops at Garden City and Alderbridge will happen in the near future.

I'd just like to know what you think are the benefits of allowing a Walmart to be placed in West Richmond? Are there any other options? Are we considering the consequences of allowing the development of a super centre that has been known to shut down small businesses, care little of the environment, and contribute to the population poor quality food, among other things?

I apologize if my question is open and I know little of the decision making process at this time or of potential public involvement,

I'd appreciate a response,
Thank you,

Curtis, Richmond Resident.

Mayor and Councillors

From: De Whalen [de_whalen@hotmail.com]
Sent: Friday, 08 March 2013 10:12 PM
To: Mayor and Councillors; Carlile, Cathryn; Semple, Dave
Cc: Margaret Hewlett; John Roeder; Alex Nixon RFB; Lynda Brummitt
Subject: Richmond Poverty Response Committee Letter re: Garden City Lands
Attachments: Mar 2013 PRC Letter on GCL to Mayor & Council.doc; City of Richmond Plan Committee Mins Feb 6 07.doc

Categories: 06-2280-20-142 - Garden City Lands - Management

March 8, 2013

Greetings:
Mayor and Council,
Cathy Carlile,
Dave Semple

Please see attached, a letter and request from the Richmond Poverty Response Committee regarding the Garden City Lands Study. I will hand deliver a hard-copy to City Hall c/o Mayor and Council on March 11, 2013.

I look forward to your response.

Sincerely,

De Whalen
Chair, Richmond PRC
cc. PRC Exco Committee

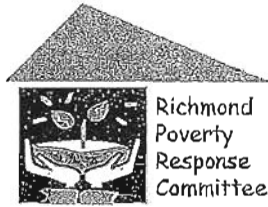
C 604.230.3158

"Food is the moral right of all who are born into this world." Norman Borlaug, Agronomist, Nobel Laureate...

"Anyone who has ever struggled with poverty knows how extremely expensive it is to be poor." James Baldwin

"Once you see it, you can't un-see it. And once you've seen it, keeping quiet, saying nothing, becomes as political an act as speaking out. There's no innocence. Either way, you're accountable." Arundhati Roy

TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE



March 8, 2013

Mayor & Councillors
City of Richmond
6911 No. 3 Road
Richmond, BC
V6Y 2C1

Lo

PHOTOCOPIED

MAR 11 2013

& DISTRIBUTION



Dear Mayor & Councillors:

Re: Garden City Lands
Richmond Poverty Response Committee's Report on Sustainable Food Systems

On behalf of the Richmond Poverty Response Committee (PRC) I am writing to request the City of Richmond and the Parks & Recreation Department review a 2006 report on Urban Agriculture from the Richmond Food Security Task Force (a subcommittee of the Richmond PRC).

This request is in light of the 2041 Official Community Plan which requires the City to consult with stakeholders in areas such as "Park and Open Space Strategy" and "Agriculture and Food." We are particularly interested in the city's future plans for the Garden City Lands.

Entitled "*Richmond Food System Assessment*," our report and recommendations introduced a vision for a local food system initiative that would see an increase in community gardens/farms and sustainable economic enterprises on the Garden City Lands. A link to the report is at:
http://www.vch.ca/media/CPAI_Full_Richmond.pdf.

In the minutes of a Feb 6, 2007 City Planning Committee meeting (enclosed), the following motion was passed: "That the Richmond Poverty Response Committee and the Richmond Food Security Task Force be included on any stakeholders list for the Garden City Lands Study."

The Richmond PRC has a solid track record of work in food security. We established the Food Security Task Force (which has since become the Richmond Food Security Society) "to promote the understanding of food security, which means: when all people in the community, at all times, have access to nutritious, safe, personally acceptable and culturally appropriate foods, produced in ways that are environmentally sound and socially just."

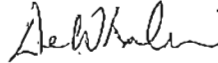
We completed the Richmond Food Security Assessment in 2006, sponsored the Garden City Lands Town Hall in 2007, and held the Food for All Conference in 2008, producing a dialogue report of the conference. A pocket market was also launched in 2008 as well a Local Food Guide to make locally grown produce more accessible.

c/o Richmond Food Bank Society, #100-5800 Cedarbridge Way, Richmond, BC V6X 2A7
Tel 604-205-4700 www.richmondprc.ca

We ask that the City honour its resolution to include the Richmond PRC in any community consultations related to the Garden City Lands Study.

Thank you for your attention to this matter. Should you have questions or comments, please do not hesitate to contact the undersigned at da_whalen@hotmail.com or at 604.230.3158.

Yours Truly,



De Whalen
Chair, Richmond PRC

Enclosed: City of Richmond Planning Committee minutes Feb 6, 07

Cc PRC Executive Committee
Cathy Carlile, Manager Community Services (by email)
Dave Scmple, Manager Parks & Recreation (by email)

2007 AGENDAS & MINUTES
February 6, 2007 - Minutes



City of Richmond
6711 No. 3 Road
Richmond, BC V6Y 2C1

Minutes

Planning Committee

Date: Tuesday, February 6, 2007
Place: Anderson Room
Richmond City Hall
Present: Councillor Harold Steves, Chair
Councillor Bill McNulty, Vice-Chair
Councillor Linda Barnes
Councillor Sue Halsey-Brandt
Councillor Rob Howard
Mayor Malcolm Brodie

Call to Order: The Chair called the meeting to order at 4:00 p.m.

MINUTES

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defined.. It was moved and seconded

That the minutes of the meeting of the Planning Committee held on Tuesday, January 16, 2007, be adopted as circulated.

CARRIED

The Chair advised that the Food Security Task Force delegation would be removed from Item 7 and would be heard before Committee addressed Item 3.

NEXT COMMITTEE MEETING DATE

2. The next meeting of the Committee will be held on Tuesday, February 20th, 2007, at 4:00 p.m. in the Anderson Room.

DELEGATION

Ms. Arzeena Hamir spoke on behalf of The Richmond Food Security Task Force (RFSTF) and was accompanied by Mary Gazetas, Jason O'Brien and David Reay.

Ms. Hamir advised that the RFSTF was introducing a vision for a local food system initiative, and she distributed a Proposal for a Sustainable Food Systems Centre/Park, Garden City Lands, Richmond, B.C. and a Richmond Food System Assessment Report. (A copy of the proposal and a copy of the report are on file in the City Clerk's Office.)

The RFSTF defines food security as: "Being assured when all people in the community, at all times, have access to nutritious, safe, personally acceptable and culturally appropriate foods, produced in ways that are environmentally sound and socially just."

Ms. Hamir made the point that in order to ensure that Richmond has food security, the RFSTF supports food production locally.

To ensure local food production, the RFSTF proposed a Sustainable Food Systems Centre on the Garden City Lands. The Centre would promote sustainable agriculture, would include space for growing native edible plants on land reserved for organic farming; a restaurant featuring locally grown food; and a teaching kitchen, and would have Community Supported Agriculture. In addition, the Centre would house a new Food Bank with garden plots accessible to clients, and would also provide space for cooking clubs, and other activities that support food access and security, while fostering self-reliance and a sense of belonging and community.

Ms. Hamir concluded the presentation by stating that the plan is an innovative use of space and that a Food Systems Centre of the kind proposed would encourage young people to go into farming.

In response to inquiries the RFSTF representatives advised:

- the proposal for the Food Systems Centre does not include a formula for economic benefit, but the commercial production of food at the Centre would create some economic benefit;
 - the Garden City lands are desirable for the proposed Centre because the concept is to practise urban agriculture in an urban area, not agriculture in a rural area; bearing in mind that the City does not own the Garden City Lands, the RFSTF would work with City staff if other arable lands were available;
 - the Farmers' Institute and local farmers were consulted during research for the Richmond Food System Assessment Report;
 - the Richmond Food System Assessment Report, funded by a grant from the Province of BC's "Act Now - Smart Fund", and administered through the Vancouver Coastal Health Authority, was completed in 2003, and in 2007 the RFSTF will work on a 10-month action plan;
 - the RFSTF envisions the Centre as fully accessible by both the general public and by those in the farming business.
- Joe Erceg, General Manager, Planning and Development, advised that the Garden City Lands Memorandum of Understanding was issued to the City of Richmond and its two partners, the Canada Lands Company and the Musqueam First Nation. Any discussion of uses of the Garden City Lands in terms of the Richmond Food Security Task Force, would be problematic without consultation with other interested parties. Mr. Erceg advised that the Garden City Lands Master Plan process would be an open public process.

It was moved and seconded

That the urban agriculture proposal be received for information.

CARRIED

It was moved and seconded
That the Richmond Poverty Response Committee and the Richmond Food Security Task Force be included on any stakeholders list for the Garden City Lands Study.

CARRIED
Opposed: Cllr. Howard

It was moved and seconded

That both the Richmond Food System Assessment report and the Proposal for a Sustainable Food Systems Centre/Part be received for information, and that staff be directed to review and gather comments, including possible locations and community partners, and that City staff report to a future Planning Committee meeting on the outcome.

CARRIED

TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

MayorandCouncillors

From: Olga [olga.tkatcheva@gmail.com]
Sent: Sunday, 17 March 2013 11:58 PM
To: MayorandCouncillors
Cc: gardencitylands@shaw.ca
Subject: Walmart site planning in connection with Garden City Lands
Categories: 12-8060-20-8864 - Walmart/Smart Centre - Garden City & Alderbridge

Dear Council members,

I am respectfully asking you to disapprove of the plan that destroys the views from the Garden City Lands and allow the Walmart mall to be built exposed to the GCI side with a row of stores. Leaving a green belt between the Walmart complex and the Garden City Lands would make it more delicate and less intrusive.

Please make a conscious effort to keep in harmony the future site of the community parks and gardens with the commercial and residential areas surrounding it. The Garden City Lands has an enormous potential to become a jewel, a focal point of our town, an envy for all municipalities that did not pay attention and failed to preserve the natural areas of the same significance in its center, but it needs a bit of special attitude in planning to realize its potential in a best possible way. Right now the view from the Garden City Lands to the mountains is the best one in the city, please, keep it!

with best regards,
proud member of the Garden City Lands coalition, Olga Tkatcheva Richmond, BC.

PHOTOCOPIED
MAR 18 2013
& DISTRIBUTED



TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

MayorandCouncillors

From: Barbara Mathias [barbmathias@telus.net]
Sent: Sunday, 17 March 2013 7:50 PM
To: MayorandCouncillors
Subject: Views in Richmond

Categories: 08-4100-02-01 - Development - Inquiries and Complaints - General

Precious. Special. Let's preserve them. I'm writing about the views to the north.
Specifically from West Hwy across the open lands.
The plan for the shopping centre looks reasonable. If we give them that, we can insist that
the trees and corridor remain as a natural and existing blind.
So they lose some square footage. Really, we can look at what they get.
And, what we preserve. That is your job, and our mindfulness as this city grows.
I ask this of you, my representatives for my home.
Thank you.
Barbara Mathias
Richmond.

PHOTOCOPIED *L*

MAR 18 2013

& DISTRIBUTED



TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

Mayor and Councillors

From: Richard Mathias [richard.mathias@ubc.ca]
Sent: Sunday, 17 March 2013 7:34 PM
To: Mayor and Councillors
Subject: Alderbridge Corridor

Categories: 08-4100-02-01 - Development - Inquiries and Complaints - General

Mayor Brodie and Councillors

Although I do not believe that Richmond needs another big box store for any reason, this message is more about the sustainability of a healthy community through conservation. The Alderbridge corridor is another unique Richmond feature that could be preserved or sacrificed depending on your responses to Walmart's blandishments. Their position is one of profit not liveability. I urge you to consider our generation who have already seen much of what was Richmond sacrificed to profit and our future generations who will never be able to see what we did. Sustaining our community is in your hands. I understand that resistance is difficult and under very persuasive threat from those who see only their own motives. I also understand that resistance must be maintained, as once a decision is made to allow this corridor to be removed, it can never be reconstituted. As one developer said, the regulatory authority can refuse many times; but once agreement to develop is given, it cannot be revoked as development is not reversible. Please resist another degradation of our environment through unwise and unnecessary development.

Thank you

Richard Mathias
Professor of Public Health, UBC
Richmond Resident since 1980

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MAR 18 2013

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TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

MayorandCouncillors

From: Katie Elliot [kellot@langara.bc.ca]
Sent: Monday, 18 March 2013 11:14 AM
To: MayorandCouncillors
Cc: gardencitylands@shaw.ca
Subject: Garden City Lands Preservation
Categories: 06-2280-20-142 - Garden City Lands - Management

Dear Mayor and Councillors,

You have acted steadfastly in the past to protect the integrity of the Garden City Lands. This area has so much potential and value to its citizens, mainly as an accessible natural landscape.

The bog drains so much rain which is particularly important, vital- in Richmond. All over Richmond, so many yards and fields keep being cemented over -- to have a large area with good drainage, especially around the concrete jungle city centre, is obviously necessary!

Planners and developers may wave around cash and say they can fix any problems they create. (But they won't want to talk about all those tidal-wash wet parking garages in Steveston, for example.)

We know that nothing can replace the no-charge, free and efficient action of nature at work.

Please keep this area for urban gardens, a wildlife corridor, and other priceless assets that enhance civic wellbeing.

This will be a legacy to be proud of.

Thank you,
Katie Elliot

--
Katie Elliot
Division Assistant
Creative Arts & Humanities
(604) 323-5005

Langara College
100 West 49th Avenue, Vancouver, BC, V5Y 2Z6

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MAR 18 2013

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Guzzi, Brian

From: Katie Eliot [kellot@langara.bc.ca]
Sent: Friday, 22 March 2013 10:12
To: Guzzi, Brian
Subject: Re: RZ 10-528877 - Katie Elliot email re Proposed SmartCentres/Walmart Rezoning Application

Thanks, Brian.

I think the main point is not encroachment on the ALR but (1) the unnecessary and potentially harmful increase of traffic, (2) additional unnecessary stores, and (3) paving of natural drainage areas.

Thank you for emphasizing these points.

Sincerely,

Katie

--

Katie Eliot
Division Assistant
Creative Arts & Humanities
(604) 323-5005

Langara College
100 West 49th Avenue, Vancouver, BC, V5Y 2Z6

TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

Mayor and Councillors

From: Winnie [so.winnie@gmail.com]
Date: Monday, 18 March 2013 10:43 AM
To: Mayor and Councillors
Cc: Garden City News
Subject: Large plan on the north side of Garden City Lands
Categories: 12-8060-20-8864 - Walmart/Smart Centre - Garden City & Alderbridge

Dear Mayor and Councillors,

I recently read about and saw the video of the presentation video of "Garden City Smart Centres". It was shocking to see this destruction.
http://www.youtube.com/watch?v=R_UoH-lwdfk

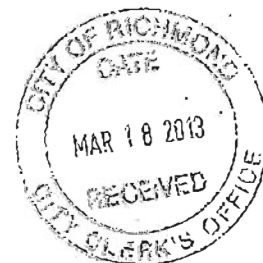
I can't describe how upset I am with this plan to destroy our beautiful neighbourhood. Does Richmond need such a large big box ugly outlet style mall so close to our city centre? Why would the developers be allowed to cut down so many trees and destroy the habitat of wildlife in Richmond. This will also change the viewscape of the Lands and worsen the already bad traffic along Gardencity road. It may be alright to have a Walmart store in Richmond (which I am not really happy with) but we DO NOT need an extension of box stores for the cost of destroying the important wildlife corridor along Alderbridge way.

As a resident of Richmond for more than 10 years, I would urge all councillors and the mayor to stop and reconsider this development and protect the viewscape and this important wildlife corridor.

If this ever will happen, I would have left no choice but to move out of this city.

Sincerely,
Resident of Richmond
Winnie W. Y. So
1105 - 6233 Katsura Street,
Richmond, BC

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MAR 18 2013
& DISTRIBUTED



TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

Mayor and Councillors

From: Pam Price [pamofgwent@shaw.ca]
Sent: Monday, 18 March 2013 10:00 AM
To: Mayor and Councillors
Cc: gardencitylands@shaw
Subject: garden city lands

Categories: 08-4100-02-01 - Development - Inquiries and Complaints - General

To the Mayor and Council:

Please think twice about "boxing" in the area around the Garden City Lands and ruining the view from that area. It seems like it is a constant fight to keep green space in Richmond.

It started years ago with the attempt by developers to build on Garry Park. Another was the big fight over Terra Nova, and yet another over building right to the dyke on Bayview in Steveston.

That is just on the West side of Richmond!

Now that Richmond is building skywards with all the architecturally-devoid glass towers, (and god knows what is going to be built on the Russ Baker Way property,) it is time to call a halt and start doing something to make this city one with which we can be proud.

I fear that once the Garden City Lands get boxed in it will be an excuse to build on the Lands themselves.

Sincerely,

Pam Price

Steveston

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MAR 18 2013

& DISTRIBUTED



TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

Mayor and Councillors

From: Andrea May [ajmay2003@hotmail.com]
Sent: Monday, 18 March 2013 3:08 AM
To: Mayor and Councillors
Cc: GardenCityLands@shaw.ca; audrey hall
Subject: Protecting the Integrity of the Garden City Lands Park

Categories: 08-4100-02-01 - Development - Inquiries and Complaints - General

Honourable Leaders,
A majority of citizens want to keep spectacular views from the city
centre to the North Shore Mountains. Certainly, we do NOT want a row of
hundreds of metres of boxy stores with big signs ruining that view for
everyone, on, or near, the Garden City Lands park.

Many, many people have kept up the efforts to safeguard the Garden City Lands in the ALR to conserve, not waste.
We would appreciate a confident stand in this matter, in your present directions to respect our perspective for the
future enjoyment of all.

Thank you,
Andrea May

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MAR 18 2013

& DISTRIBUTED



TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

Mayor and Councillors

From: normah@telus.net
Sent: Monday, 18 March 2013 12:04 PM
To: Mayor and Councillors
Cc: GardenCityLands@shaw.ca
Subject: Proposed development encroaching Garden City Lands

Categories: 12-8060-20-8864 - Walmart/Smart Centre - Garden City & Alderbridge

To Richmond Mayor and Councillors:

We were dismayed to hear of the proposed Walmart development to go along the perimeter of the Garden City Lands. We have fought to keep this parcel in the ALR and, as such, thought it was a protected corridor. The Garden City Lands area is a small space and the idea of a development of the magnitude of the one including Walmart would be an encroachment that would not only dwarf the site but might also be the 'leading edge of the wedge' that would determine, disqualify or limit plans for future use. We do not have a scarcity of land set aside for mall use. Number 3 Road has several malls to choose from and the Costco development is only a short distance away. In addition, East Richmond already has a huge Walmart Mall along with the congestion of traffic that goes along with it. However, we do have a very limited amount of land within the ALR. That land is always under pressure of development and, while the development proposed at the present time is not actually using the land site known as the Garden City Lands it will affect it and the future use of it. We do not have the power to stop this development but we can appeal to our elected representatives, which we are doing. Please remember that you are the custodians for this land and act accordingly.

A
Norma & Norm Houle
1/291 Westminster Hwy.
Richmond, B.C.
V6V 1A9

PHOTOCOPIED *fb*

MAR 18 2013

& DISTRIBUTED



Guzzi, Brian

From: normah@telus.net
Sent: Friday, 22 March 2013 11:16
To: Guzzi, Brian
Subject: Re: RZ 12-528877 - SmartCentres/Walmart Proposed Rezoning

With respect Mr Guzzi, I'm sure we have both seen, over the years, developments which were legal and within zoning development by-laws but which were not well thought out in terms of the future. An example of this is when residential development is allowed to locate right up to farm use land and "all of a sudden" harvesters at night are too noisy and over-head spraying is unthinkable, not to mention the difficulty of moving farm equipment around: or a small Church in East Richmond which is quietly allowed to become a school and "all of a sudden" council wakes up and wonders "when did that happen?" There cannot be a school of children surrounded by farm land and, consequently, land is removed from the ALR and a corridor - now occupied by commercial interests, is established. Perhaps you can understand a citizen wondering if there is someone at the helm or even someone who understands what long-term planning means.

I am not questioning the present zoning of the land in question and I realize it is not within the ALR. I worry that the proposed development will affect or limit the proposals for the Garden City Lands. Once this development is established, there is nothing stopping any one of the retailers from going to council to ask for permission to add extra stories onto their buildings. Increased height of buildings is happening all over Richmond. If traffic congestion became an issue, it is not too hard to imagine needing to widen some roads.

Where might that land come from? The worries are real.

Thank you for your considerate response.

Norma and Bernie Houle

TO: MAYOR & EACH
COUNCILLOR
CITY CLERK'S OFFICE

MayorandCouncillors

From: John terborg [john_terborg@hotmail.com]
Sent: Monday, 18 March 2013 7:53 PM
To: MayorandCouncillors
Subject: Alderbridge Way - Conserve Viewscapes
Categories: 12-8060-20-8864 - Walmart/Smart Centre - Garden City & Alderbridge

Richmond City Council Members,

Please conserve the legacy views when you are making decisions regarding the Walmart proposal.

Development is currently buffered by the environmentally sensitive areas along Alderbridge Way which have been described for decades by the City's ESA maps. Prior to the Walmart proposal and prior to the land being purchased.

The Garden City Lands have the opportunity to be a destination for community health and wellness and this potential will be impacted by large box stores and commercial development that will take away from the natural scenes.

The community's experience of the natural, farming, and recreational uses on the Garden City park lands will be impacted by the neighbouring viewscapes.

Please retain the natural views.

Thank you,

John ter Borg

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MAR 19 2013

& DISTRIBUTED



Mayor and Councillors

TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

From: Rick Xavier [whisper@xworks.ca]
Sent: Monday, 18 March 2013 6:23 PM
To: Mayor and Councillors
Subject: Garden City Lands

Categories: 12-8060-20-8864 - Walmart/Smart Centre - Garden City & Alderbridge

Déar Councillors,

So I hear city council is debating whether to approve development of a Walmart store, and possibly a strip mall, along Alderbridge across from the Garden City lands. We have plenty of opportunity to admire large scale urban architecture in Richmond, must we have a large view-dominating eyesore next to this natural space? My opinion as a Richmond resident, for what it's worth, is no. Big box stores so close to central Richmond seem a little out of place - in fact a waste of opportunity for urban coolness. Please use the Lands for more natural purposes on a more natural scale.

Rick Xavier

PHOTOCOPIED
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MAR 19 2013
& DISTRIBUTED



From: rick [<mailto:rick@resist.ca>]
Sent: Tuesday, 19 March 2013 04:11 PM
To: DevApps
Subject: smartcentres development application "12 627046 000 00 SA"

Hi

I live at 9420 Alexandra Road. After being issued an eviction notice by SmartCentres for development purposes, i phoned Rob Campbell at their offices in Edmonton and was told the permit had been denied and that i should continue bringing rent cheques.

Would you please tell me the reason for withholding the permit and anything you can tell me about steps they will need to follow to get approval. Also what might be the time window for the approval.

Thanks very much.

Rick McCallion
9420 alexandra road, richmond

TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

Mayor and Councillors

From: De Whalen [de_whelen@hotmail.com]
Sent: Wednesday, 20 March 2013 10:41 PM
To: Bhreandain Clugston; Mayor and Councillors
Subject: Letter to the Editor
Categories: 06-2280-20-142 - Garden City Lands - Management

March 20, 2013

Bhreandain Clugston
Richmond Review

Letter to the Editor

Re: "Respect the people, nature and legacies"

PHOTOCOPIED
MAR 21 2013
& DISTRIBUTED



I enjoyed reading Jim Wright's column on March 15th about the Garden City Lands. As a member of the Richmond Poverty Response Committee I can confirm the group discussed the future viewscape and some even toured the land one weekend in the rain.

I consider it an absolute disgrace for the City to propose building a Walmart right next door to this pristine environment. They have already allowed the developer to dump fill on the so-called "environmentally sensitive area" parallel to Alderbridge and kill all the trees that would have hidden the concrete backside of Walmart-to-

What would I rather see? I'd like to see the City invite all Richmondites to participate in the planning of the Garden City Lands and the Alderbridge corridor.

I can envision hundreds of community garden plots as well as incubator farms for new young farmers and larger test plots for established farmers to grow organic produce. The Garden City Lands could be a centre for neighbouring Kwantlen Polytechnic University's Urban Agriculture program. Young urban farmers could rent land and employ new practices that produce more per acre than traditional practices as well as supply some of Richmond's food needs.

It could also be a tourist attraction. The land already has the catchy title of the "Garden City." Visitors wouldn't need much convincing to come and experience the view, natural environment and food production methods. Local restaurants would be a market for fresh organic produce, especially considering we are next to the third largest city in Canada with the most food conscious residents and the best restaurants in the land.

And let's not forget that the land includes delta bog with rare bog plant species and unique animals. Raised walkways, seating and educational signposts similar to the Nature Park could meander through the area and allow families to relax, reinvigorate and learn to respect this land.

Come on Mayor and Council, Richmondites deserve to be involved in planning the future of the Garden City Lands and environs. Don't leave us out of the viewscape!

De Whalen

C 604.230.3158

"Food is the moral right of all who are born into this world." Norman Borlaug, Agronomist, Nobel Laureate

"Anyone who has ever struggled with poverty knows how extremely expensive it is to be poor." James Baldwin

"Once you see it, you can't un-see it. And once you've seen it, keeping quiet, saying nothing, becomes as political an act as speaking out. There's no innocence. Either way, you're accountable." Arundhati Roy

TO: MAYOR & EACH
COUNCILLOR
FROM: CITY CLERK'S OFFICE

MayorandCouncillors

From: Carol Day (carol@catsigns.ca)
Sent: Tuesday, 09 April 2013 2:30 PM
To: MayorandCouncillors
Subject: Walmart proposal
Attachments: Walmart To City Council April 8, 2013.docx
Categories: 12-8060-20-8864 - Walmart/Smart Centre - Garden City & Alderbridge

To Mayor and Council

Please see my letter attached.

Thanks

Carol Day

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PHOTOCOPIED

APR 9 2013

& DISTRIBUTED



To Mayor and Council City of Richmond

April 9th, 2013

Re : Walmart application

As a founding member of the Garden City Lands Coalition I am very proud of the hard work we all put in to save the Garden City lands from the proposed high-density residential development. It is with great sadness I view the plans for a massive shopping center to include Walmart on the north side of the Garden City Lands. Today you can look north and see the pristine snow-capped Coast Mountains, but if the proposed plan is approved by Richmond City Council then that will be changed forever.

The green space and trees could be saved if the development was scaled back. The views of the mountains could be completely saved too. The neighborhood could be improved if city council would listen to our concerns.

We have very little open green space left in the concrete jungle we call Richmond City Centre. We do however have a massive amount of shopping and a massive amount of concrete towers. Today's council will determine forever how that changes for future generations.

We can do better than this. Please direct the developer and the planning staff to try a little harder and come up with a less imposing plan for the land north of the Garden City Lands.

Carol Day

Advisory Design Panel (ADP) Comments

(Excerpted from ADP Minutes of December 8th, 2011)

Item 3. RZ 10-528877 – Major low-rise commercial development approximately 30,569 m² (330,000 ft²) with 10 one-storey buildings, 1 two-storey building, and 1 three-level parking structure including a Walmart Store.

Architect: Chandler Associates Architecture Inc.

Property Location: 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500 Alexandra Road

Panel Discussion

Advisory Design Panel comments are followed by SmartCentres response in *bold italics*:

- very large and challenging project; has many interior and perimeter edges; ***Agreed.***
- screening of loading bays along Alexandra Road is inadequate; needs more work as loading bays remain exposed and trucks would still be visible but the western loading/service area is more successful; applicant also needs to address overlook issues; ***The eastern loading bay is screened by a 14' high screen wall, with dense planting which includes numerous mature trees. The loading bay is further screened from above by the incorporation of a metal trellis structure, which will be covered with vines, thus addressing the overlook issues. The western loading bay is entirely encapsulated by solid walls on three sides, limiting any views into the loading bay except for only west bound traffic along Alexandra Road (at less than a 30 degree cone of vision). Overlook at the west loading bay is not an issue.***
- higher density two-storey buildings are more appropriate to achieve streetscape continuity; one-storey buildings on the perimeter of the site are far less successful; sameness of expression and slight height variations do not interrupt or establish strong punctuations; ***Two storey spaces will be integrated where feasible.***
- replicating retail streetfronts is complicated and more work is required such as changes in materiality; simplify and use high quality materials; increase the amount of vision glass around the perimeter of the site; this applies to a large proportion of the proposed design; ***Changes in materials, simplification of design and integration of more vision glass will be expressed in our Development Permit submission.***
- internal elevations appear flat and box-like; design development is needed to reflect a village character; massing needs to be broken down and further articulated to provide visual interest to pedestrians; introduce structures to draw pedestrians to the site; using high quality materials is appropriate for a new shopping center; ***Increased articulation to the internal facades will be introduced in our Development Permit submission.***
- one-storey buildings need substantial design development; consider layering of design elements to achieve a more pedestrian-friendly environment; ***As noted in previous items, further design development of the facades will be pursued with our Development Permit submission.***
- transition in the architectural character from the outside to the inside of the site should be significant and shocking; the design quality of the interior should be sensational and dramatically different from the exterior; ***As per panel notes above, the exterior facades will be further developed to create larger areas of quality materials to relate better to the automobile oriented traffic along the***

perimeter, and the internal facades will be further articulated to add visual interest, creating a striking difference in expression between "inside" and "outside".

- landscaped deck works well when viewed from the outside; however, it looks disconnected from the rest of the project when viewed from the inside; appears like a floating mass; poses a CPTED challenge underneath; connect the landscaped deck into the project; *The deck will receive a stair on the south side, connecting the podium to the pedestrian walkway in the at grade parking area. The north-east corner will be opened up to the street to allow visual connection and surveillance from Alexandra Road/May Drive into the covered parking area. The covered parkade area walls, ceiling and columns will be painted white and will include bright lighting levels.*
- Garden City Road and Alderbridge Way portal is weak; Building H is small and low in relation to the other buildings; portal should be more powerful at this important gateway to the City Centre; *The entry way will be further developed, with a more powerful expression of Building H.*
- Building N appears lost and disconnected from the rest of the project; does not provide continuity to what is happening along the street; *Building N will be integrated into the development through creating a connection with the remainder of the development with architectural screening landscape elements (similar to Alexandra Road).*
- project has a lot of challenges; relationship between the outside and inside edges needs to be improved; project is intended to create the 'Village' centre for the West Cambie Area and should reflect a more urban and commercial character; *As noted in previous items, the outside facades will be treated differently from the inside facades (larger areas of materials with a reduced material palette versus more finer articulation). A "village" centre experience will be expressed through greater detailing and layering of landscape and architectural elements along the Alexandra Way through the site.*
- a challenging commercial project; project needs a more consistent and intense urban design response to fit with the emerging more sophisticated urban character of Richmond; *This will be achieved through the changes to the building facades and massing as noted previously, and through the enhancement of Alexandra Way through the development.*
- landscape needs to have a sense of hierarchy and one dominant theme to unify spaces; needs anchor points or nodes that attract pedestrians and encourage them to linger, e.g. water elements or public art; *Public nodes are being integrated along Alexandra Way to encourage people to linger along the way. The nodes will be connected through the use of consistent elements and materials and will be further highlighted as "special" areas through the integration of public art elements.*
- the project's high street does not read as a retail/commercial heart of the project; built form on the west side of high street (1-storey buildings) is weak; high street should have a stronger sense of enclosure and a more intense urban design response, continuous streetfront retail with high quality paving including furnishings and appointments that increase visual interest and enhance pedestrian comfort; should serve as the principal organizing spine; *High Street will be further developed with the enhancement of the landscape treatment including special paving, planting and furnishings. The building along the west side of High Street will be designed with two-storey loft-style elements along the street façade in order to increase its visual and physical presence.*
- a model would be helpful in future assessment of this design proposal; *We will present a series of large scale models to demonstrate the detailing and articulation of architectural and landscape elements that have been integrated into our design. A 3-d "fly-thru" of the overall project has also been produced to further explain the overall project.*
- Alexandra Way as the primary pedestrian linkage through the site should encompass more than just decorative paving but a series of outdoor pedestrian-scaled rooms with a hierarchy and a central 'Village Green'; *As noted in previous items, we will present a detailed investigation of*

Alexandra Way with our Development Permit submission that will include a series of pedestrian nodes through the length of Alexandra Way

- landscaped roof deck needs programming (is it ESA compensation or active use area); roof deck elements should be carried through the development (linked in some way with the design of other on-site pedestrian open space areas); should have linkage with Walmart; needs a more urban response; can introduce native plantings and stormwater elements; seating and amenities should be located adjacent to paved areas; consider the landscape deck as the 'Garden Centre'; ***We are awaiting direction from the City to determine the character of the landscaped deck. We will provide a stair access from the deck to the at grade parking to the south.***
- consider May Drive as a 'sustainable' street, particularly adjacent to the designated park and ESA area on the east site of May Drive; ***City engineering requirements won't allow for sustainable street design.***
- consider stronger pedestrian crossing elements at intersections with the vehicle circulation system throughout the site, not just painted stripes; ***All pedestrian crossings within the site are raised platforms to meet the adjacent sidewalk grades.***
- grade differences should be handled with ramps (rather than stairs) at the entrances to the site; integrate with other elements; ***Ramped sidewalks will be the primary accesses to the site. Any stairs used to access the site will be secondary to the ramped sidewalks.***
- pedestrian scale signage is all right but pylon type signs need further design development; integrate signage with public art; ***The pylon sign designs will receive additional design development. Their design will work in conjunction to the public art rather than being integrated with it.***
- architecture is too busy; suggest one linking and dominant material, preferably brick that is carried through the entire project and combined with other elements; ***As noted in previous items, the exterior façade design will be refined with larger areas of materials used.***
- rain protection needs to be continuous through the project along pedestrian routes; could be at different levels; could be a combination of awnings and canopies; ***Agreed. Rain protection is applied along Alexandra Way and along the sidewalks adjacent to the retail buildings. We will use a combination of awnings and canopies as suggested.***
- a very complex project; intent of creating an urban village is more successful internally; ***Agreed***
- life-style center concept is not achieved in the project; landscape elements, e.g. art elements, seating opportunities, and plazas are missing; consider recessing some of the street fronting buildings to create space for seating areas; ***As noted in previous items, the landscape design will be further developed in our Development Permit submission, integrating plazas, pedestrian nodes, furnishings, public art etc.***
- urban village character is not achieved on the Alderbridge Way, Garden City Road and Alexandra Road elevations; broken-down mass is not appropriate externally as these streets are vehicle and not pedestrian-oriented; street elevations do not achieve the objective of becoming a landmark; external elevations need more work; ***As noted in previous items, the exterior elevations will be refined to suit the vehicular oriented nature of the facades.***
- character of external and internal elevations should be different; internal elevations are like a suburban mall but should be more organized and urban; need to be consistent in order to achieve an urban village character; ***As per panel notes above, the exterior facades will be redeveloped to reflect a more urbane nature through the use of larger areas of materials with the internal facades being further articulated to add visual interest, creating a striking difference in expression between "inside" and "outside". A "village" centre experience will be expressed through greater detailing and layering of landscape and architectural elements along the Alexandra Way through the site.***
- a very challenging project; one of the most complex projects considered by the Panel; ***Agreed***

- project does not look like a village; appears like a shopping centre precinct; does not have the tactility and texture of a village; *The "village" centre experience will be expressed through greater detailing and layering of landscape and architectural elements and be further enhanced through the creation of a central plaza area, along with a number of pedestrian nodes established through the length of Alexandra Way.*
- consider a diagonal cut-thru for pedestrians at the corner of Alderbridge Way and Garden City Road; could become the high street; will link Alexandra neighbourhood to downtown Richmond neighbourhood; *We have created a diagonal "cut-thru" at the south-west corner of the site which will connect Alexandra Way from the residential area to the north of our site, down and through the site to connect with the Richmond downtown neighbourhood.*
- project is pedestrian in nature; heavy pedestrian movement is expected in the neighbourhood; design narrative is needed for the project to achieve a pedestrian and village character; proposed project needs to be defined; does not have the feel of quality; signage works well but disappears in other parts of the development; *The "village" centre experience will be expressed through greater detailing and layering of landscape and architectural elements and be further enhanced through the creation of a central plaza area, along with a number of pedestrian nodes established through the length of Alexandra Way.*
- introduce glazing at the backs of buildings adjacent to pedestrian walkways; look at Aberdeen Center as precedent; *All buildings will have vision glass oriented to the pedestrian walkways within the site. Vision glass will be integrated into the external facades, adjacent to sidewalks where feasible. Much of the vision glass in the above noted project is obscured by tenant posters etc., or is open to inactive exit stair corridors. Our intent is to locate vision glass where it will have a positive contribution to interact with adjacent sidewalks.*
- link landscaped deck to Walmart; *We will link the landscaped deck to the at-grade parking area. Linking it to the store will create security issues.*
- articulation of buildings looks flat; needs visual clarity defined by the project's design narrative; look at Park Royal as precedent for a pedestrian-friendly project; *As noted in previous items, the internal facades will be further articulated to add visual interest and be integrated with landscaping elements such as seating, planting, paving etc.*
- proposed development is a huge project in Richmond; Planning is requested to assist applicant in defining Richmond character; project has to serve pedestrians; *We have been working closely with the Planning Department to define the development's character and create a richer pedestrian experience through articulation of the building facades and integration of landscape elements throughout the site.*
- with the exception of various stair locations, grade changes are handled well; introduce nodes in the project; further design development of the landscaped deck will encourage pedestrians to pause; benches and picnic tables need to be connected to impermeable surface; *Grade changes are being redesigned to be accessible primarily by ramped sidewalks with stairs being used secondarily. Pedestrian nodes and plazas are being integrated throughout the site, and primarily along Alexandra Way.*
- continuous rain protection will encourage pedestrians to stay on the site; *As noted in previous items, continuous rain protection is provided through a combination of awnings and canopies.*
- project has the potential and should become a major gateway to Richmond's city center; project as presented is not successful as major entry point or gateway to the city centre; *The design development of the project as described in the items above will establish this project as a major and successful gateway to the City Centre.*
- project design needs to evolve and not just mirror Lansdowne (suburban shopping centre design approach is no longer appropriate particularly adjacent to the city centre); *The design development*

of the project as described in the items above will create a project that does not mirror the above cited development.

- project needs to have an iconic element; should stand out to differentiate it from other Walmart projects; *We will work to find an acceptable solution.*
- project is a different interpretation of the village character for the Alexandra Neighbourhood as described in the Alexandra Neighbourhood design guidelines for this area; scale of the project should set the tone for the neighbourhood; *The design development and integration of the architectural and landscape treatments and elements listed in the responses above will create a development that is more in keeping with the Alexandra Neighbourhood guidelines.*
- project should accommodate the inclusion of Walmart but not let Walmart dictate the design character; *The design of the Walmart store has strayed substantially from the prototypical design. The store has been integrated into the retail store designs along High Street and Alderbridge Way, and where exposed along the north and east facades, have been enriched with articulation of the facades and have incorporated material used throughout the rest of the development.*
- ESA area in the project could be spread out rather than concentrated on the corner; *In order for the ESA to have any significance for the integration of natural habitat, the area must be consolidated into one single area, rather than being diluted and lessen its value by spreading the area throughout the site.*
- the 'Village' character unifying design narrative is missing in the project; needed to anchor the design concept of the project; *The "village" centre experience will be expressed through greater detailing and layering of landscape and architectural elements and be further enhanced through the creation of a central plaza area, along with a number of pedestrian nodes established through the length of Alexandra Way.*



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LANDSCAPE:

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Sheet No. 1
September 5, 2013
RZ 10-528877



CENTRAL AT GARDEN CITY

RICHMOND, BC

AUGUST 29, 2013 REZONING APPLICATION #RZ10-528877

COVER PAGE

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DATE: AUGUST 29, 2013
SCALE: NIS
PROJECT #: 4029



CONTEXT PLAN - AERIAL VIEW
SCALE 1:3000



CONTEXT PLAN
SCALE 1:3000

Sheet No. 2
September 5, 2013
RZ 10-528877



CENTRAL AT GARDEN CITY

RICHMOND, BC

CONTEXT PLAN



SITE ALONG ALDERBRIDGE WAY



SITE ALONG GARDEN CITY ROAD



GARDEN CITY ROAD & ALEXANDRA ROAD
LOOKING SOUTH - EAST

PLN - 239



SITE ALONG ALEXANDRA ROAD



GARDEN CITY ROAD & ALDERBRIDGE ROAD
LOOKING NORTH - EAST



GARDEN CITY ROAD
LOOKING NORTH - WEST



ALDERBRIDGE ROAD
LOOKING EAST



ALEXANDRA STREET
LOOKING EAST

Sheet No. 3
September 5, 2013
RZ 10-528877



CENTRAL AT GARDEN CITY

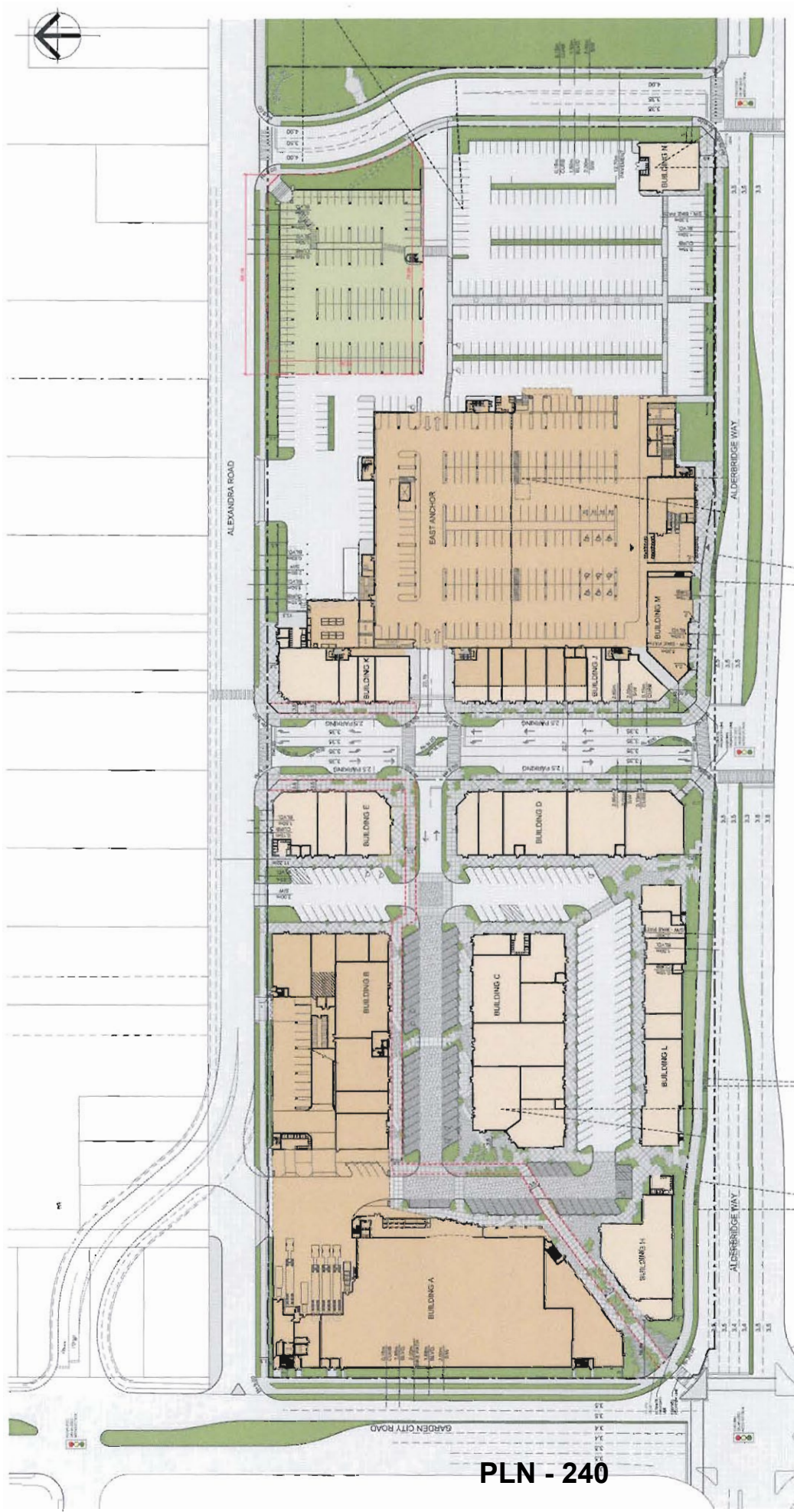
RICHMOND, BC

CONTEXT PHOTOGRAPHS



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14100 1st Ave. Suite 101
Richmond, BC V6V 1G1
Phone: 604-271-3305
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DATE: AUGUST 29, 2013
SCALE: NTS
PROJECT #: 4029



- 3.5 m WIDE ALEXANDRA WAY SRW
- GREEN DECK SRW

EAST PARCEL SETBACKS			
Building	North	West	South
Building A	13.50 m	22.15 m	2.00 m
Building J	- m	3.00 m	9.02 m
Building K	3.00 m	3.00 m	- m
Building M	- m	10.50 m	2.00 m
Building N	- m	- m	1.00 m

WEST PARCEL SETBACKS			
Building	North	West	South
Building A	1.30 m	3.00 m	- m
Building B	1.00 m	- m	- m
Building C	- m	- m	5.29 m
Building D	1.00 m	- m	2.00 m
Building H	- m	19.00 m	3.00 m
Building L	- m	- m	2.00 m



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Sheet No. 4
 September 5, 2013
 RZ 10-S28877



CENTRAL AT GARDEN CITY
 RICHMOND, BC

SETBACK PLAN

DATE: AUGUST 29, 2013
 SCALE: 1:500
 PROJECT #: A-1.3

PLN - 240

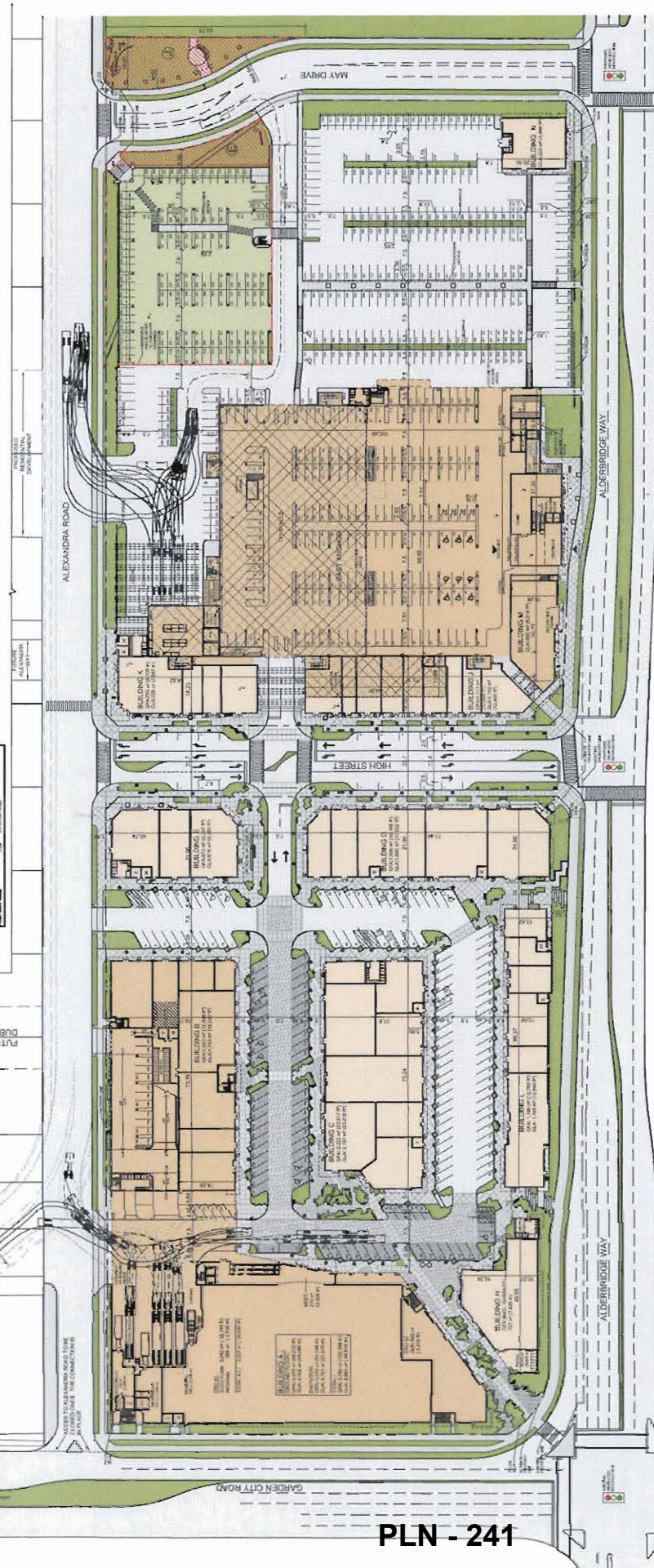


WEST PARCEL - BUILDING AREA CALCULATION	
Building Area	1,490.00 SF
Roof Area	1,490.00 SF
Other Area	0.00 SF
Total Area	2,980.00 SF

WEST PARCEL - BUILDING AREA CALCULATION	
Building Area	1,490.00 SF
Roof Area	1,490.00 SF
Other Area	0.00 SF
Total Area	2,980.00 SF

WEST PARCEL - BUILDING AREA CALCULATION	
Building Area	1,490.00 SF
Roof Area	1,490.00 SF
Other Area	0.00 SF
Total Area	2,980.00 SF

WEST PARCEL - BUILDING AREA CALCULATION	
Building Area	1,490.00 SF
Roof Area	1,490.00 SF
Other Area	0.00 SF
Total Area	2,980.00 SF



SITE STATISTICS

EXISTING SITE AREA:
 ROAD DEDICATION - HIGH STREET: 3,303 SM - 36,200 SF (0.93 ACRES)
 ROAD DEDICATION - MAY DRIVE: 3,125 SM - 33,837 SF (0.77 ACRES)
 ROAD DEDICATION - ALDERBRIDGE ROAD: 2,144 SM - 23,076 SF (0.53 ACRES)
 TOTAL ROAD DEDICATION: 8,572 SM - 92,913 SF (2.23 ACRES)

EAST PARCEL - ESA AREA J:
 654 SM - 7,039 SF (0.16 ACRES)
 TOTAL SITE AREA WITHOUT ROAD DEDICATION & ESA AREA J: 46,605 SM - 630,818 SF (14.28 ACRES)

WEST PARCEL

SITE AREA:
 20,362 SM - 216,040 SF (7.26 ACRES)
GFA (GROSS FLOOR AREA):
 18,325 SM - 197,248 SF
GFA (GROSS LEASABLE AREA):
 17,173 SM - 184,849 SF
FLOOR AREA RATIO:
 0.62

PARKING REQUIRED:
 684 CARS (3,086,100 SM - 3,711,000 SF)
 12 CARS
 127 CARS
 LEVEL 2
 LEVEL 3
ON GRADE PARKING:
 175 CARS

TOTAL PARKING PROVIDED:
 596 CARS (3,431,100 SM - 3,711,000 SF)
REGULAR STALLS:
 481 CARS (2.2%) - REQUIRED 2%
STALLS FOR HANDICAP:
 13 CARS (0.6%) - REQUIRED 2%
SMALL CAR STALLS:
 112 CARS (5.1%) - ALLOWED 50%

BICYCLE PARKING:
 LONG TERM: 47 PROPOSED 59
 SHORT TERM: 47 PROPOSED 69

EAST PARCEL

SITE AREA (INCL. AREA E):
 29,243 SM - 314,769 SF (7.22 ACRES)
PARKING LEVEL AREA:
 1,490 SM - 16,038 SF
SALES FLOOR AREA:
 8,173 SM - 86,737 SF
TOTAL EAST ANCHOR AREA:
 14,573 SM - 161,188 SF

GRADE LEVEL BUILDING (ORU) AREA:
 2,718 SM - 29,256 SF
GFA (TOTAL GROSS FLOOR AREA):
 17,693 SM - 190,444 SF
GFA (TOTAL GROSS LEASABLE AREA):
 17,492 SM - 187,213 SF

FLOOR AREA RATIO:
 0.61

LOT COVERAGE:
 15,598 SM - 167,551 SF (83.2%)

PARKING REQUIRED:
 692 CARS (3,089,100 SM - 3,711,000 SF)

TOTAL PARKING PROVIDED:
 597 CARS (3,269,100 SM - 3,030,000 SF)
REGULAR STALLS:
 379 CARS (2.1%) - REQUIRED 2%
STALLS FOR HANDICAP:
 17 CARS (0.9%) - REQUIRED 2%
SMALL CAR STALLS:
 176 CARS (31%) - ALLOWED 50%

BICYCLE PARKING:
 LONG TERM: 47 PROPOSED 69
 SHORT TERM: 47 PROPOSED 86

ESA PARK AREA

EAST PARCEL:
 AREA E: 344 SM - 3,705 SF
 AREA J: 654 SM - 7,039 SF
 GREEN DECK: 3,376 SM - 36,360 SF
TOTAL:
 4,376 SM - 47,101 SF (1.08 ACRES)



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Sheet No. 5
 September 5, 2013
 RZ-10-S28877



CENTRAL AT GARDEN CITY
 RICHMOND, BC

SITE PLAN

DATE: AUGUST 29, 2013
 SCALE: 1:500
 PROJECT #:
A-2.1



EAST ELEVATION
SCALE: 1/120



NORTH ELEVATION
SCALE: 1/120

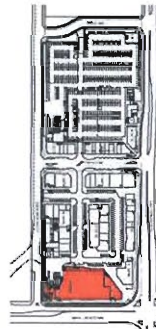
FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - BRUSHED GLASS IN ALUMINIUM FRAME
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINIUM FRAME
- 9 - METAL CANOPY
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS

PRELIMINARY SIGNAGE ANALYSIS

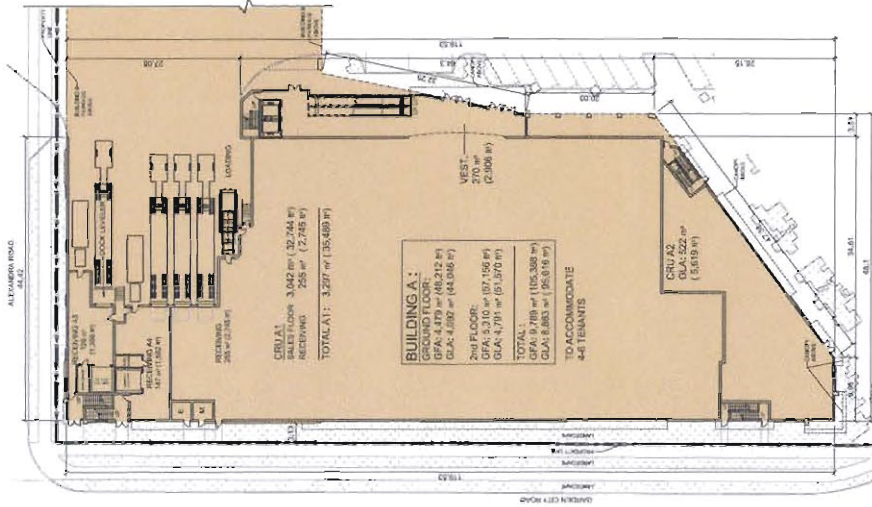
EAST ELEVATION		NORTH ELEVATION	
REQUIRED	PROPOSED	REQUIRED	PROPOSED
ELEV. LENGTH	52.28 m	BUILD. WIDTH	41.72 m
PERMITTED SIGN AREA / LINEAR SPACING	1 m ²	PERMITTED SIGN AREA / SPACING	1 m ²
TOTAL SIGN AREA / PERMITTED	52.28 m ²	TOTAL SIGN AREA / PERMITTED	41.72 m ²

EAST ELEVATION		NORTH ELEVATION	
PROPOSED SIGNAGE	AREA	PROPOSED SIGNAGE	AREA
SIGNAGE 1	3.2 x 4.0	SIGNAGE 1	3.2 x 4.0
SIGNAGE 2	1.3 x 1.2	SIGNAGE 2	1.3 x 1.2
SIGNAGE 3	1.3 x 1.2	SIGNAGE 3	1.3 x 1.2
SIGNAGE 4	0.3 x 1.2	SIGNAGE 4	0.3 x 1.2
SIGNAGE 5	1.6 x 6.6	SIGNAGE 5	1.6 x 6.6
SIGNAGE 6	0.8 x 2.2	SIGNAGE 6	0.8 x 2.2
SIGNAGE 7	1.3 x 1.0	SIGNAGE 7	1.3 x 1.0
SIGNAGE 8	1.3 x 1.0	SIGNAGE 8	1.3 x 1.0
SIGNAGE 9	1.6 x 6.6	SIGNAGE 9	1.6 x 6.6
TOTAL	51.2 m ²	TOTAL	38.4 m ²

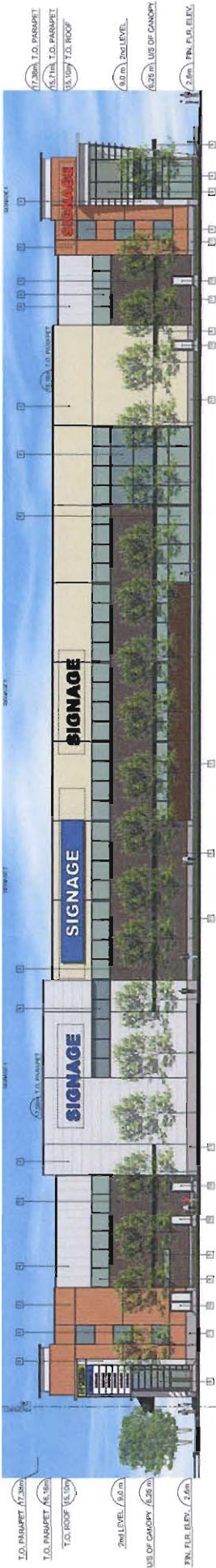


KEYPLAN
NTS

Sheet No. 6
September 5, 2013
RZ 10-528877



GROUND FLOOR PLAN
SCALE: 1/300



WEST ELEVATION
SCALE: 1:100



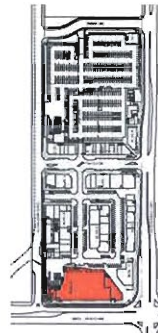
SOUTH ELEVATION
SCALE: 1:100

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EIFS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CHALKBOARD FINISH
- 7 - CONCRETE UPSTAND
- 8 - GLAZING IN BRUSHED ALUMINIUM FRAME
- 9 - BRUSHED GLASS IN ALUMINIUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS

PRELIMINARY SIGNAGE ANALYSIS:

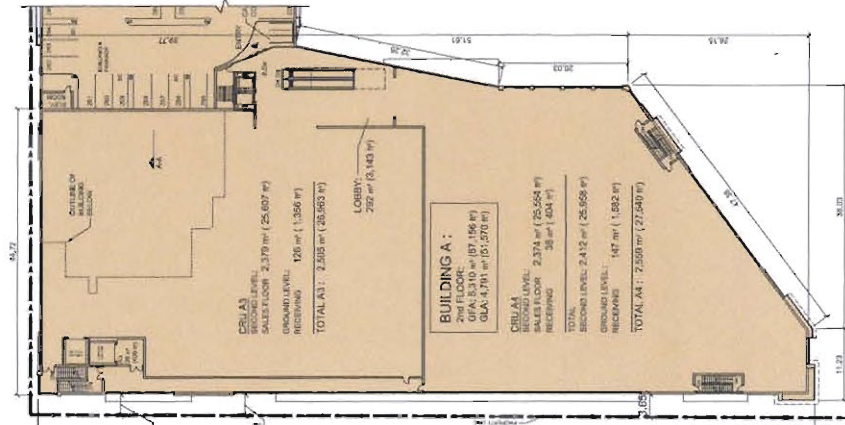
WEST ELEVATION		SOUTH ELEVATION	
REQUIRED	PROPOSED	REQUIRED	PROPOSED
BLDG. LENGTH	119.53 m	BLDG. WIDTH	47.28 m
PERMITTED SIGN AREA / LINEAR SIGNAGE	119.53 m ² / 1.17'	PERMITTED SIGN AREA / LINEAR SIGNAGE	47.28 m ² / 1.17'
TOTAL SIGN AREA	119.53 m ²	TOTAL SIGN AREA	47.28 m ²
PERMITTED SIGN AREA	119.53 m ²	PERMITTED SIGN AREA	47.28 m ²
TOTAL SIGN AREA	119.53 m ²	TOTAL SIGN AREA	47.28 m ²



Sheet No. 7
September 5, 2013
RZ 10-528877



BUILDING A PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC



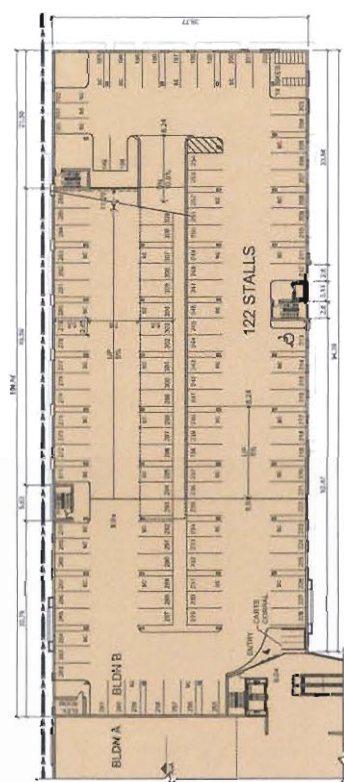
SECOND FLOOR PLAN
SCALE: 1:500



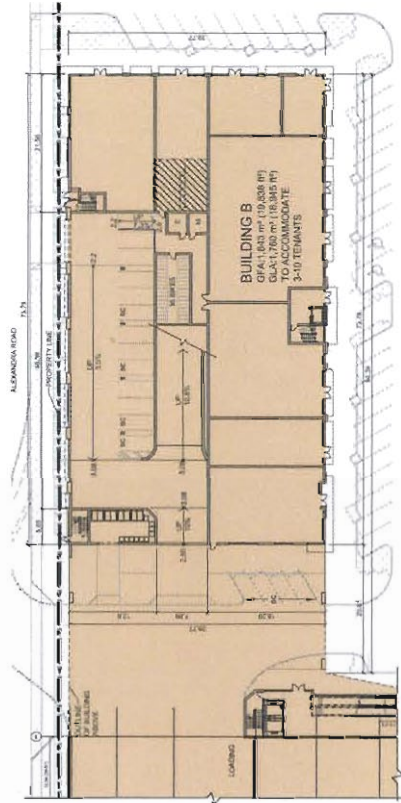
DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #:
A-3.2



SOUTH ELEVATION
SCALE 1:150



SECOND LEVEL PLAN
SCALE 1:200



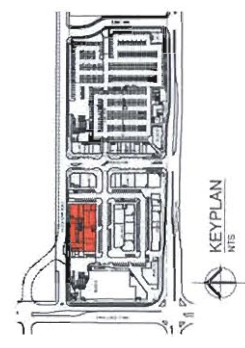
GROUND LEVEL PLAN
SCALE 1:200

FINISH LEGEND AND COLOUR SCHEDULE:

- | | | | | | | | |
|---|--------------------|---|-----------------------------------|----|------------------------------------|----|--------------------|
| 1 | - EIFS | 5 | - METAL CANOPY | 9 | - SPANDREL GLASS IN ALUMINUM FRAME | 13 | - PAINTED CONCRETE |
| 2 | - BRICK VENEER | 6 | - CANVAS AWNING | 10 | - METAL DOOR | 14 | - METAL SCREEN |
| 3 | - CORRUGATED METAL | 7 | - METAL & GLASS CANOPY | 11 | - CONCRETE UPSTAND | 15 | - METAL PLANTER |
| 4 | - METAL PANELS | 8 | - BLAZED UNBRUSHED ALUMINUM FRAME | 12 | - PRESFINISHED WOOD PANELS | | |

PRELIMINARY SIGNAGE ANALYSIS

REQUIRED		PROPOSED	
BLDG. LENGTH	PERMITTED SIGN AREA / PERMITTED SIGNAGE	BLDG. NO.	SIGNAGE AREA
74.20m	11 m ²	1	3.8 m ²
	94.20 m ²	2	1.0 x 5.5
		3	5.0 m ²
		4	5.5 m ²
		5	5.5 m ²
		6	5.5 m ²
		TOTAL	30.3 m²



KEY PLAN
NTS

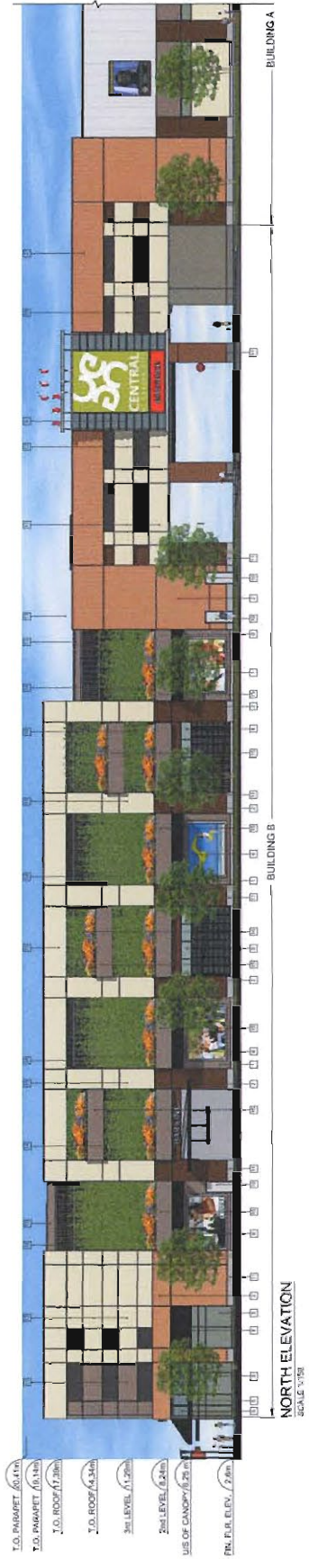
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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT: # 2029
A-3.3

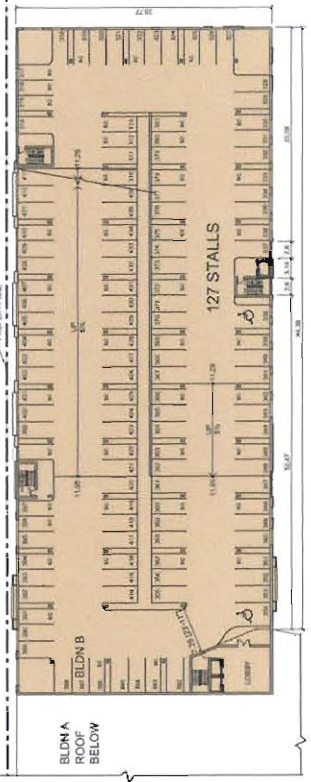
Sheet No. 8
September 5, 2013
RZ 10-528877

BUILDING B PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC

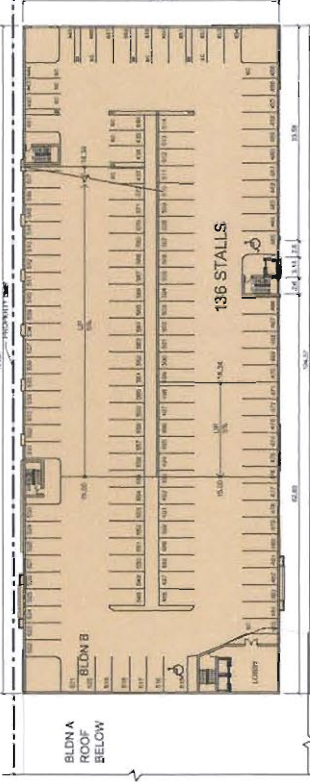




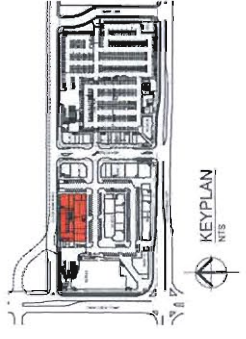
- FINISH LEGEND AND COLOUR SCHEDULE:**
- | | | | | | | | |
|---|--------------------|---|-------------------------------------|----|------------------------------------|----|--------------------|
| 1 | • EPS | 8 | • METAL CANOPY | 9 | • SPANDREL GLASS IN ALUMINUM FRAME | 17 | • PAINTED CONCRETE |
| 2 | • BRICK VENEER | 9 | • CANVAS AWNING | 10 | • METAL DOOR | 18 | • METAL SCREEN |
| 3 | • CORRUGATED METAL | 7 | • METAL & GLASS CANOPY | 11 | • CONCRETE UPSTAND | 19 | • METAL PLANTER |
| 4 | • METAL PANELS | 8 | • GLAZING IN BRUSHED ALUMINUM FRAME | 12 | • PREFINISHED WOOD PANELS | | |



THIRD LEVEL PLAN
SCALE 1/300

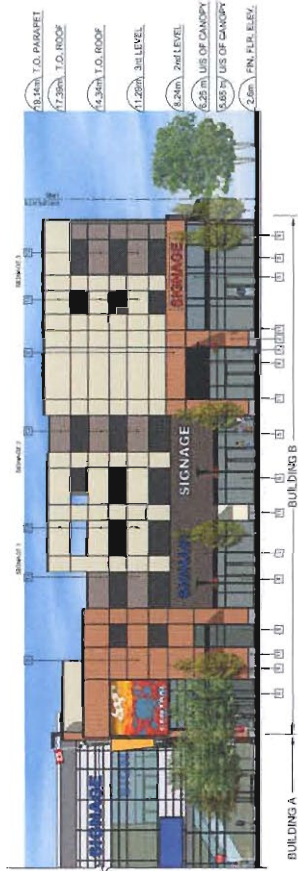


ROOF LOWER LEVEL PLAN
SCALE 1/300

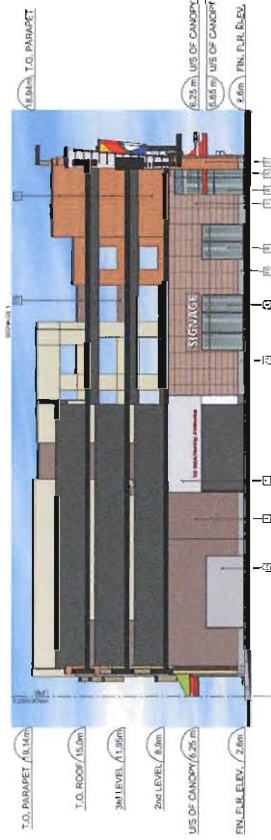


Sheet No. 9
September 5, 2013
RZ 10-528877

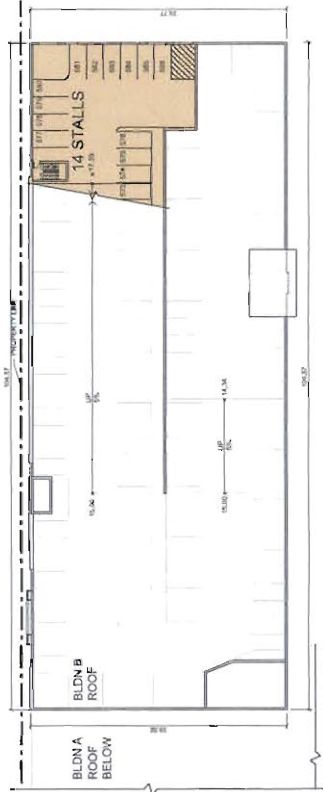
BUILDING B PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC



EAST ELEVATION
SCALE 1:150



WEST ELEVATION
SCALE 1:150



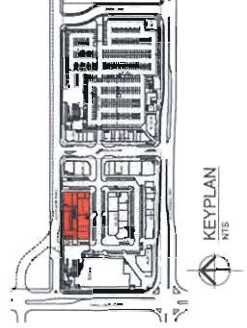
ROOF UPPER LEVEL PLAN
SCALE 1:300

PRELIMINARY SIGNAGE ANALYSIS

EAST ELEVATION		WEST ELEVATION	
REQUIRED	PROPOSED	REQUIRED	PROPOSED
BLDG. WIDTH	10.5 x 6.0	BLDGL. LENGTH	1 x 4.5
PERMITTED SIGN AREA / FRONTAGE	1.0 x 6.0 6.0 m ²	PERMITTED SIGN AREA / FRONTAGE	1 x 4.5 4.5 m ²
TOTAL	18.0 m ²	TOTAL	4.5 m ²

FINISH LEGEND AND COLOUR SCHEDULE:

1	EPS	3	METAL CANOPY	9	BRANDED GLASS IN ALUMINIUM FRAME	17	PAINTED CONCRETE
2	BRICK VENEER	6	CONCRETE UPSTAND	10	METAL DOOR	14	METAL SCREEN
3	CORRUGATED METAL	7	METAL & GLASS CANOPY	11	CONCRETE UPSTAND	15	METAL PLANTER
4	METAL PANELS	8	GLAZING IN BRUSHED ALUMINIUM FRAME	12	PRE-FINISHED WOOD PANELS		



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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4029

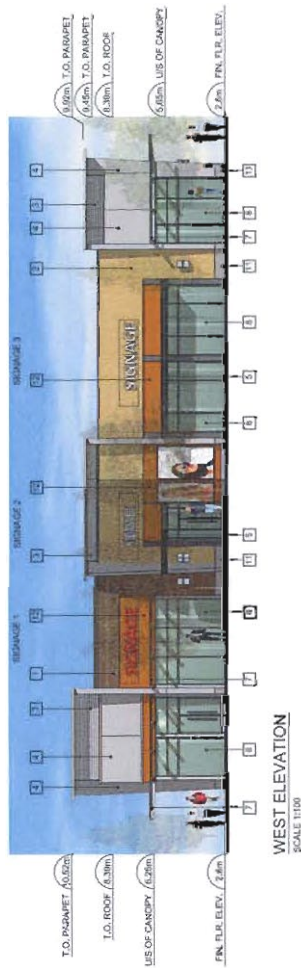
Sheet No. 10
September 5, 2013
RZ 10-528877

BUILDING B PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC

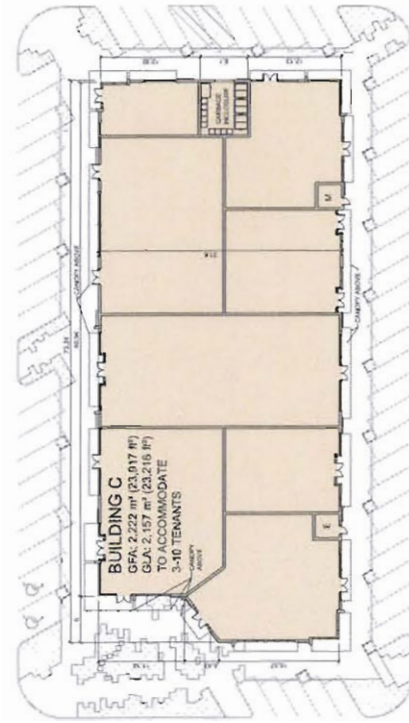




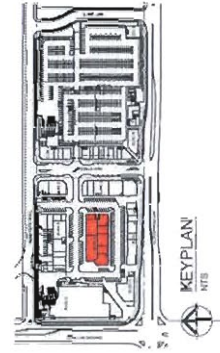
NORTH ELEVATION
SCALE 1:100



WEST ELEVATION
SCALE 1:100



BUILDING C - FLOOR PLAN
SCALE 1:200



PRELIMINARY SIGNAGE ANALYSIS

NORTH ELEVATION		WEST ELEVATION	
REQUIRED:		REQUIRED:	
BLDG. LENGTH	73.32 m	BLDG. WIDTH	31.45 m
PERMITTED FRONTAGE	1.7m	PERMITTED FRONTAGE	1.7m
TOTAL SIGN AREA	73.32 m²	TOTAL SIGN AREA	31.45 m²
PROPOSED:		PROPOSED:	
SIGNAGE 1	1 x 4.5	SIGNAGE 1	1 x 4.5
SIGNAGE 2	1 x 4.5	SIGNAGE 2	1 x 4.5
SIGNAGE 3	1 x 4.5	SIGNAGE 3	1 x 4.5
SIGNAGE 4	1 x 4.5	SIGNAGE 4	1 x 4.5
SIGNAGE 5	1 x 4.5	SIGNAGE 5	1 x 4.5
TOTAL	22.5 m²	TOTAL	22.5 m²

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - SPANDREL GLASS IN ALUMINUM FRAME
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINUM FRAME
- 9 - CANVAS AWNING
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PRE-FINISHED WOOD PANELS

Sheet No. 11
September 5, 2013
RZ 10-528877

BUILDING C PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY

RICHMOND, BC



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DATE: AUGUST 20, 2013
SCALE: AS NOTED
PROJECT #: 4779

A-3.6



SOUTH ELEVATION
SCALE 1:100



EAST ELEVATION
SCALE 1:100

PRELIMINARY SIGNAGE ANALYSIS

SOUTH ELEVATION

REQUIRED	PERMITTED	TOTAL SIGN
BLDG. LENGTH	73.28 m	73.28 m
PERMITTED LINEAR FRONTAGE	1.2m	73.28 m

PROPOSED	PERMITTED	TOTAL SIGN
SIGNAGE 1	1 x 4.5	4.5 m ²
SIGNAGE 2	1 x 4.5	4.5 m ²
SIGNAGE 3	1 x 4.5	4.5 m ²
SIGNAGE 4	1 x 4.5	4.5 m ²
SIGNAGE 5	1 x 4.5	4.5 m ²
TOTAL		22.5 m²

EAST ELEVATION

REQUIRED	PERMITTED	TOTAL SIGN
BLDG. WIDTH	31.75 m	31.75 m
PERMITTED SIGN AREA / FRONTAGE	1 m ²	31.75 m ²

PROPOSED	PERMITTED	TOTAL SIGN
SIGNAGE 1	1 x 4.5	4.5 m ²
SIGNAGE 2	1 x 4.5	4.5 m ²
TOTAL		9.0 m²

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - METAL CANOPY
- 3 - SPANDREL GLASS IN ALUMINUM FRAME
- 4 - BRICK VENEER
- 5 - CANVAS AWNING
- 6 - METAL DOOR
- 7 - CORRUGATED METAL
- 8 - METAL & GLASS CANOPY
- 9 - GLASS IN BRUSHED ALUMINUM FRAME
- 10 - CONCRETE UPSTAND
- 11 - METAL PANELS
- 12 - PREFINISHED WOOD PANELS



Sheet No. 12
September 5, 2013
RZ 10-S28877

BUILDING C PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC



DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4029

A-3.7

PROPERTY LINE
 10.0m T.O. PARAPET
 6.0m T.O. ROOF
 5.0m US OF CANOPY
 2.0m FIN. F.L.R. ELEV.



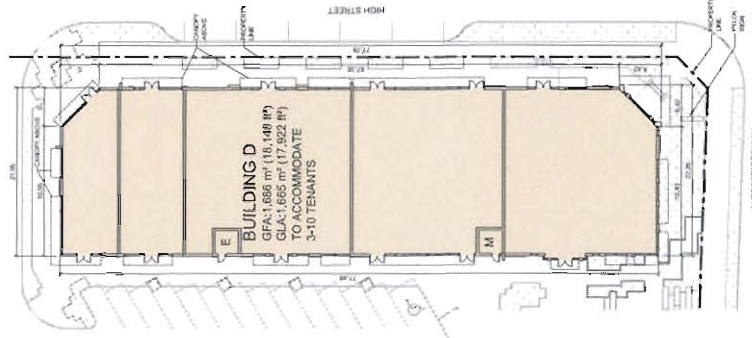
WEST ELEVATION
 SCALE 1:100



SOUTH ELEVATION
 SCALE 1:100

PRELIMINARY SIGNAGE ANALYSIS

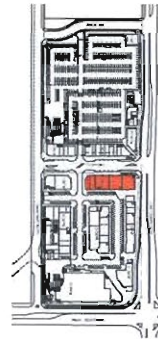
WEST ELEVATION		SOUTH ELEVATION	
REQUIRED	PROPOSED	REQUIRED	PROPOSED
BLDG. LENGTH	11.42	BLDG. WIDTH	1.8 x 1.8
LINEAR SIGNAGE AREA PERMITTED	11.42	LINEAR SIGNAGE PERMITTED	1.8 x 1.8
LINEAR SIGNAGE	11.42	FRONTAGE	1.8 x 1.8
FRONTAGE	11.42	FRONTAGE	1.8 x 1.8
TOTAL	11.42	TOTAL	11.42



GROUND FLOOR PLAN
 SCALE 1:200

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLASS UNFINISHED ALUMINUM FRAME
- 9 - SPANDREL GLASS IN ALUMINUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS



Sheet No. 13
 September 5, 2013
 RZ 10-528877



BUILDING D PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
 RICHMOND, BC

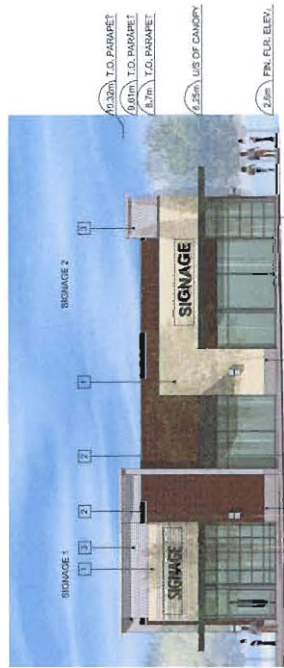
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DATE: AUGUST 29, 2013
 SCALE: AS NOTED
 PROJECT #: 8759

A-3.8



EAST ELEVATION
SCALE 1:100



NORTH ELEVATION
SCALE 1:100

PRELIMINARY SIGNAGE ANALYSIS

EAST ELEVATION

REQUIRED	PERMITTED	TOTAL SIGN AREA
77.78 m	1.0 ²	77.78 m ²

PROPOSED	AREA
SIGNAGE 1	14.45
SIGNAGE 2	14.45
SIGNAGE 3	14.45
SIGNAGE 4	14.45
SIGNAGE 5	14.45
SIGNAGE 6	14.45
TOTAL	85.50

SOUTH ELEVATION

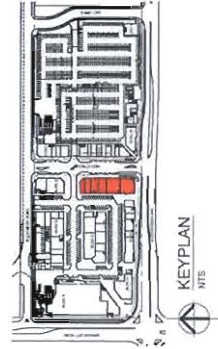
REQUIRED	PERMITTED	TOTAL SIGN AREA
21.06 m	1.0 ²	21.06 m ²

PROPOSED	AREA
SIGNAGE 1	14.45
SIGNAGE 2	14.45
TOTAL	28.90

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EIFS
- 2 - CORRUGATED METAL
- 3 - METAL PANELS
- 4 - METAL CANOPY
- 5 - CANVAS AWNING
- 6 - METAL & GLASS CANOPY
- 7 - METAL DOOR
- 8 - CONCRETE UPSTAND
- 9 - BRICK VENEER
- 10 - PRE-FINISHED WOOD PANELS
- 11 - STAINLESS STEEL IN ALUMINUM FRAME
- 12 - METAL DOOR

Sheet No. 14
September 5, 2013
RZ 10-528877



KEY PLAN
N/E

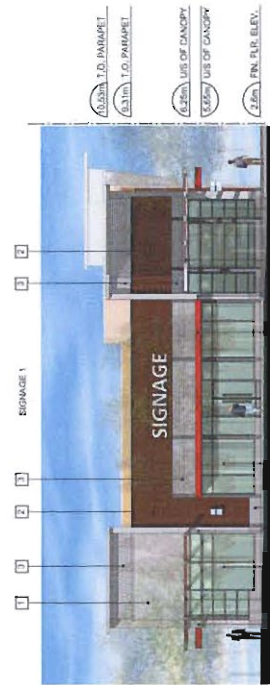
Chandler Associates
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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT # 407

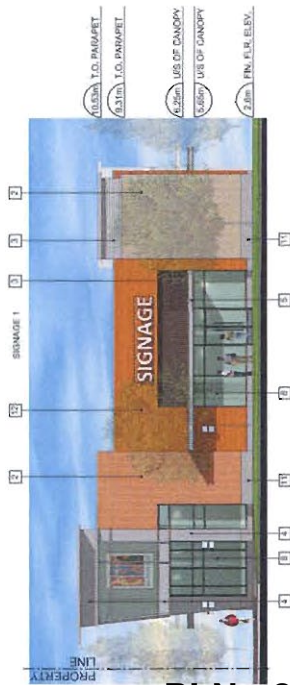
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BUILDING D PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC

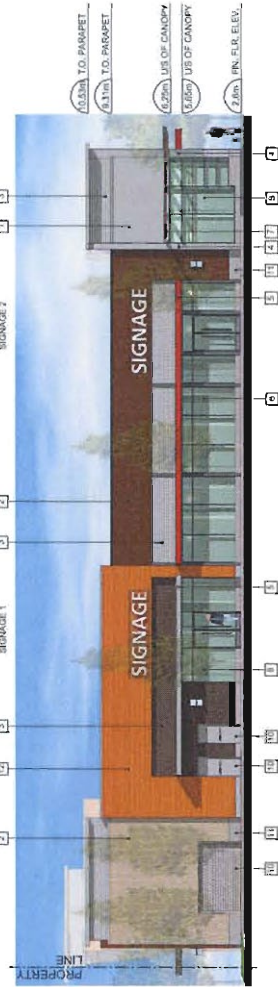




SOUTH ELEVATION
SCALE 1:100



NORTH ELEVATION
SCALE 1:100



WEST ELEVATION
SCALE 1:100



EAST ELEVATION
SCALE 1:100

- FINISH LEGEND AND COLOUR SCHEDULE:**
- 1 - EPS
 - 2 - BRICK VENEER
 - 3 - CORRUGATED METAL
 - 4 - METAL PANELS
 - 5 - METAL CANOPY
 - 6 - CANVAS AWNING
 - 7 - METAL & GLASS CANOPY
 - 8 - CLADDING IN BRUSHED ALUMINIUM FRAME
 - 9 - SPANDREL GLASS IN ALUMINIUM FRAME
 - 10 - METAL DOOR
 - 11 - CONCRETE UPLIFT AND
 - 12 - PREFINISHED WOOD PANELS

PRELIMINARY SIGNAGE ANALYSIS

REQUIRED:		TOTAL SIGN AREA / PERMITTED	
BUILDING WIDTH	FRONTAGE	PERMITTED	FRONTAGE
27.56m	1 m ²	21.56 m ²	

REQUIRED:		TOTAL SIGN AREA / PERMITTED	
BUILDING LENGTH	FRONTAGE	PERMITTED	FRONTAGE
42.86m	1 m ²	40.86 m ²	

REQUIRED:		TOTAL SIGN AREA / PERMITTED	
BUILDING WIDTH	FRONTAGE	PERMITTED	FRONTAGE
27.57m	1 m ²	21.57 m ²	

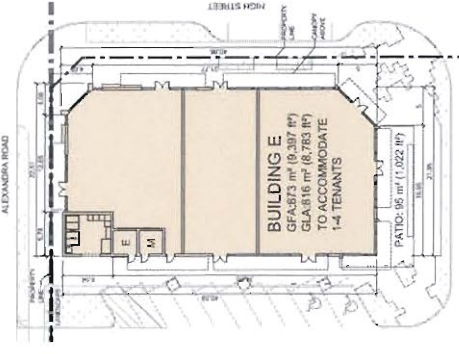
REQUIRED:		TOTAL SIGN AREA / PERMITTED	
BUILDING LENGTH	FRONTAGE	PERMITTED	FRONTAGE
42.86m	1 m ²	40.86 m ²	

PROPOSED:		TOTAL SIGN AREA / PERMITTED	
SIGNAGE 1	FRONTAGE	PERMITTED	FRONTAGE
1 x 4.5	4.5 m ²		
TOTAL		4.5 m ²	

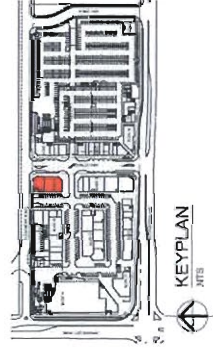
PROPOSED:		TOTAL SIGN AREA / PERMITTED	
SIGNAGE 2	FRONTAGE	PERMITTED	FRONTAGE
1 x 4.5	4.5 m ²		
TOTAL		4.5 m ²	

PROPOSED:		TOTAL SIGN AREA / PERMITTED	
SIGNAGE 1	FRONTAGE	PERMITTED	FRONTAGE
1 x 4.5	4.5 m ²		
TOTAL		4.5 m ²	

PROPOSED:		TOTAL SIGN AREA / PERMITTED	
SIGNAGE 2	FRONTAGE	PERMITTED	FRONTAGE
1 x 4.5	4.5 m ²		
TOTAL		4.5 m ²	



GROUND FLOOR PLAN
SCALE 1:200



KEY PLAN
SCALE 1:200

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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4705

A-3.10

BUILDING E PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC

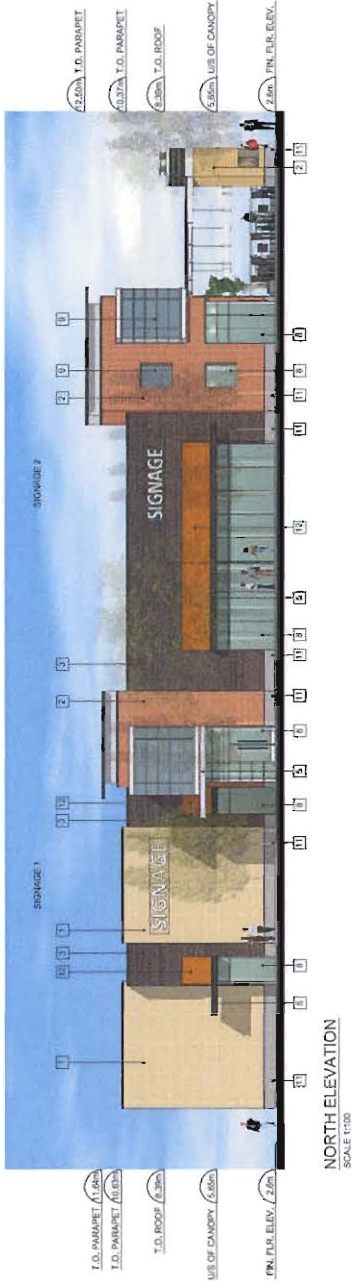
Sheet No. 15
September 5, 2013
RZ 10-528877



SIGNAGE ANALYSIS

NORTH ELEVATIONS

REQUIRED	PERMITTED	TOTAL SIGN
BLDG. SIGNAGE LENGTH	22.62 m	1 x 5
FRONTAGE	1 m ²	5 m ²
TOTAL	22.62 m	10 m²



NORTH ELEVATION
SCALE 1:100



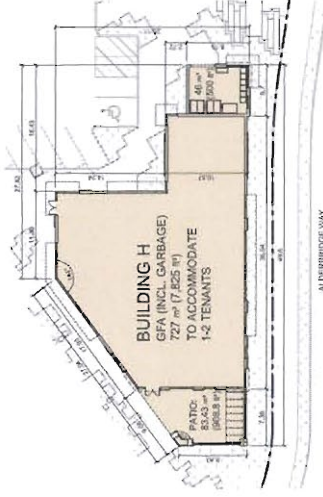
WEST ELEVATION
SCALE 1:100

FINISH LEGEND AND COLOUR SCHEDULE:

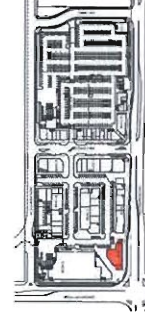
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| 1 | - EPS | 8 | - METAL CANOPY | 9 | - SPANREL GLASS IN ALUMINIUM FRAME |
| 2 | - BRICK VENEER | 9 | - CANVAS AWNING | 10 | - METAL DOOR |
| 3 | - CORRUGATED METAL | 10 | - METAL & GLASS CANOPY | 11 | - CONCRETE UPSTAND |
| 4 | - METAL PANELS | 11 | - GLAZING IN BRUSHED ALUMINIUM FRAME | 12 | - PREFINISHED WOOD PANELS |

PRELIMINARY SIGNAGE ANALYSIS

REQUIRED	PERMITTED	TOTAL SIGN
BLDG. SIGNAGE WIDTH	2.62 m	1 x 5
FRONTAGE	1 m ²	5 m ²
TOTAL	2.62 m	5 m²



GROUND FLOOR PLAN
SCALE 1:200



KEY PLAN
SCALE 1:50

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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4009

A-3.11

BUILDING H PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC

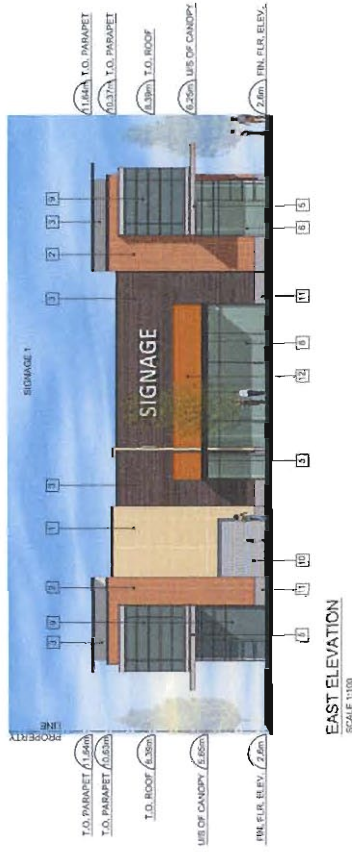
Sheet No. 16
September 5, 2013
RZ 10-528877





SIGNAGE ANALYSIS

SOUTH ELEVATION	
REQUIRED:	PERMITTED:
BLDG. LENGTH / 49.5 m	SIGN AREA / 14.5
1.7m ²	AREA / 5 m ²
	FRONTAGE / 5 m ²
	TOTAL / 48.8 m ²



PRELIMINARY SIGNAGE ANALYSIS

EAST ELEVATION	
REQUIRED:	PERMITTED:
BLDG. WIDTH / 26.11 m	SIGN AREA / 14.5
1.7m ²	AREA / 5 m ²
	FRONTAGE / 5 m ²
	TOTAL / 26.11 m ²

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EIFS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINUM FRAME
- 9 - SPANDREL GLASS IN ALUMINUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS

Sheet No. 17
 September 5, 2013
 RZ 10-528877



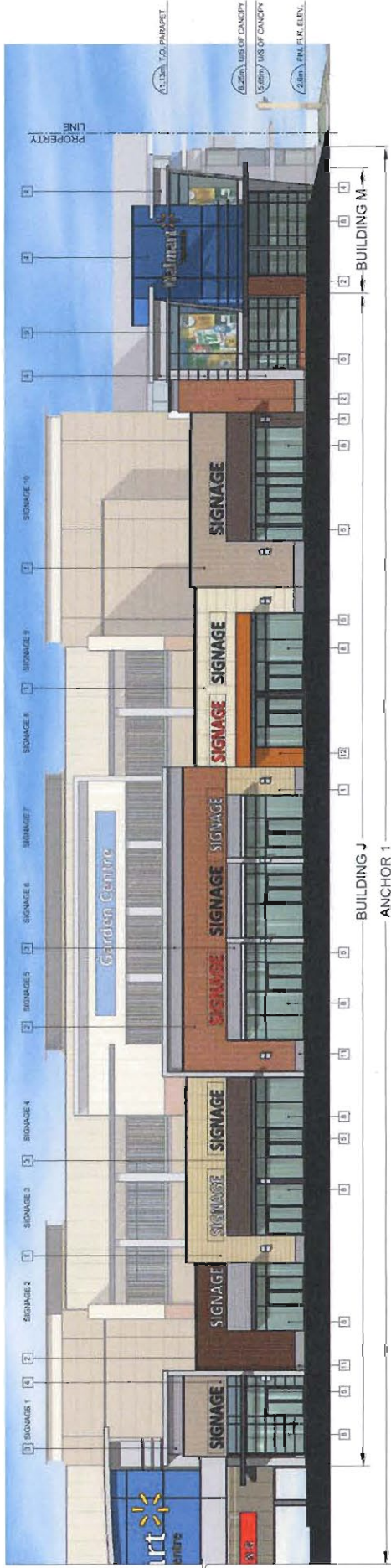
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DATE: AUGUST 20, 2013
 SCALE: AS NOTED
 PROJECT #: 4070

A-3.12

BUILDING H PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
 RICHMOND, BC





WEST ELEVATION
SCALE 1/100

PLN - 254

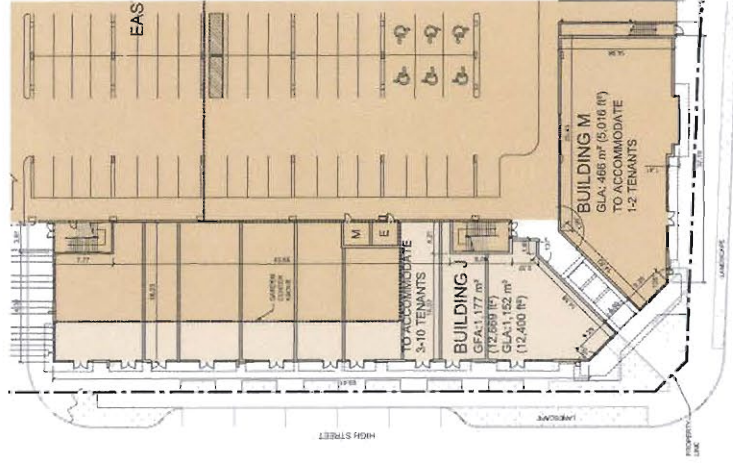
FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS RIBBING
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINUM FRAME
- 9 - SPANDREL GLASS IN ALUMINUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS

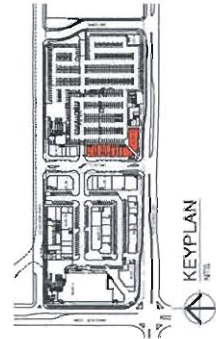
PRELIMINARY SIGNAGE ANALYSIS

WEST ELEVATION REQUIRE:		TOTAL SIGN AREA / PERMITTED FRONTOFACE	
BLDG. LENGTH	1 m ²	PERMITTED	FRONTOFACE
60.41 m	1 m ²	60.41 m ²	60.41 m ²

PROPOSED:	AREA
SIGNAGE 1	1.4.5
SIGNAGE 2	1.4.5
SIGNAGE 3	1.4.5
SIGNAGE 4	1.4.5
SIGNAGE 5	1.4.5
SIGNAGE 6	1.4.5
SIGNAGE 7	1.4.5
SIGNAGE 8	1.4.5
SIGNAGE 9	1.4.5
SIGNAGE 10	1.4.5
TOTAL	14.5 m ²



GROUND FLOOR PLAN
SCALE 1/200



Sheet No. 18
September 5, 2013
RZ 10-528877

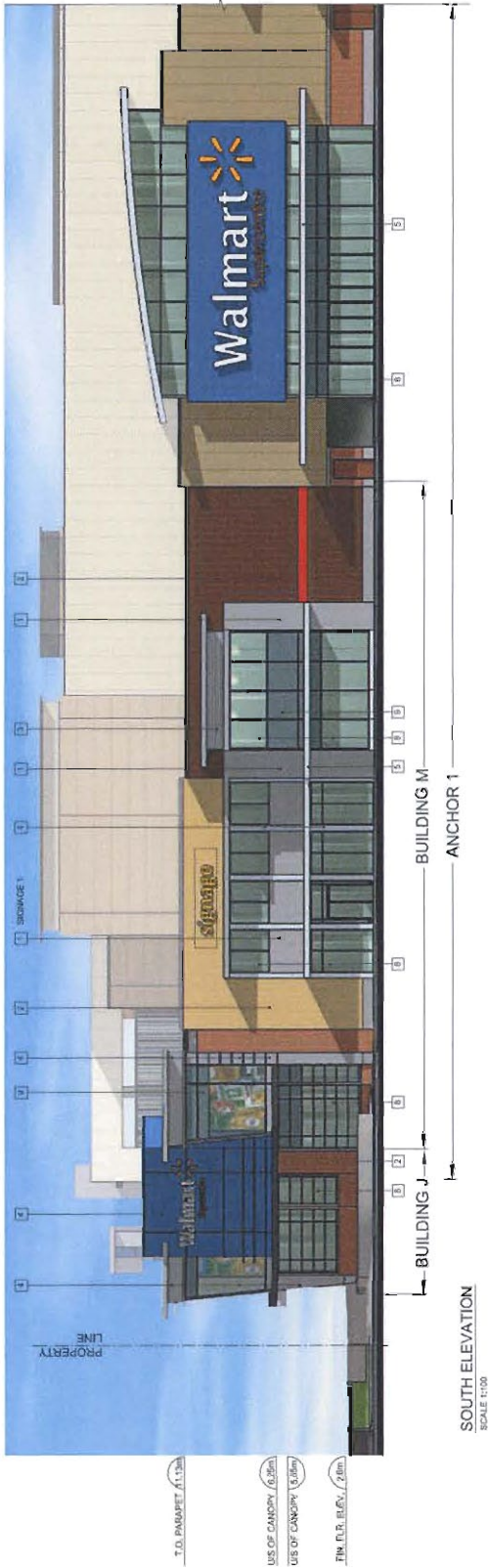


BUILDING J & M PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC

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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 1079

A-3.13



SOUTH ELEVATION
SCALE 1/100



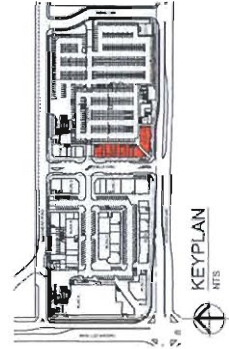
NORTH ELEVATION
SCALE 1/100

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - METAL CANOPY
- 3 - SPANDREL GLASS IN ALUMINIUM FRAME
- 4 - BRICK VENEER
- 5 - CANVAS AWNING
- 6 - METAL DOOR
- 7 - CORRUGATED METAL
- 8 - METAL & GLASS CANOPY
- 9 - METAL PANELS
- 10 - CONCRETE UPSTAIR
- 11 - GLAZING IN BRUSHED ALUMINIUM FRAME
- 12 - PREFINISHED WOOD PANELS

PRELIMINARY SIGNAGE ANALYSIS

REQUIRED:		PROPOSED:	
BUILD. LENGTH	SIGN AREA / PERMITTED PACKAGE	SPACE 1	SPACE 2
32.15 m	1 m ²	6.5 x 1.2	6.6 m ²
		TOTAL	6.6 m ²
			31.19 m ²



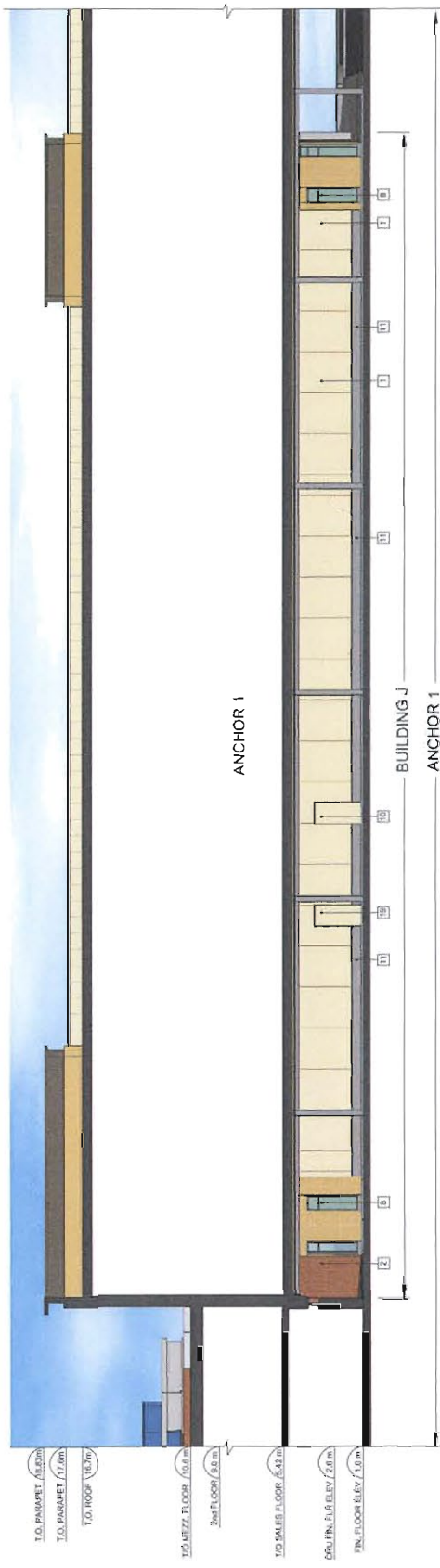
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September 5, 2013
RZ 10-528877

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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4078
A-3.14

BUILDING J & M PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC



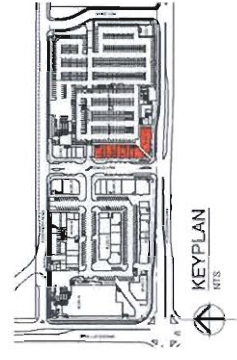


EAST ELEVATION
SCALE: 1:100

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINUM FRAME
- 9 - SPANDREL GLASS IN ALUMINUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAIR
- 12 - PRETREATED WOOD PANELS

Sheet No. 20
September 5, 2013
RZ 10-528877



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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 1079

A-3.15

BUILDING J & M PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC





WEST ELEVATION
SCALE 1:100



EAST ELEVATION
SCALE 1:100

FINISH LEGEND AND COLOUR SCHEDULE:

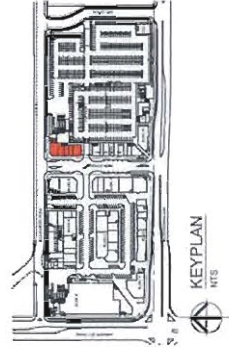
- | | | | | | |
|---|--------------------|---|-------------------------------------|----|-----------------------------------|
| 1 | - EIFS | 5 | - METAL CANOPY | 9 | - GRANITE GLASS IN ALUMINUM FRAME |
| 2 | - BRICK VENEER | 6 | - CANVAS AWNING | 10 | - METAL DOOR |
| 3 | - CORRUGATED METAL | 7 | - METAL & GLASS CANOPY | 11 | - CONCRETE UPSTAND |
| 4 | - METAL PANELS | 8 | - GLAZING IN BRUSHED ALUMINUM FRAME | 12 | - PREFINISHED WOOD PANELS |

PRELIMINARY SIGNAGE ANALYSIS

REQUIRED:		PROPOSED:	
BUILDING LENGTH	44.5 m	SIGNAGE 1	6.0 x 1.3
BUILDING FRONTAGE	1 m	SIGNAGE 2	5.0 x 1.2
TOTAL SIGN AREA	44.5 m ²	SIGNAGE 3	5.0 x 1.2
TOTAL SIGN AREA PERMITTED	44.5 m ²	TOTAL	16.0 m ²



GROUND FLOOR PLAN
SCALE 1:200



KEY PLAN
SCALE 1:10

PRELIMINARY SIGNAGE ANALYSIS

REQUIRED:

BLOCK WIDTH	LINEAR FRONTAGE	PERMITTED SIGN AREA	TOTAL SIGN AREA PERMITTED
18.5 m	1 m ²	18.5 m ²	18.5 m ²

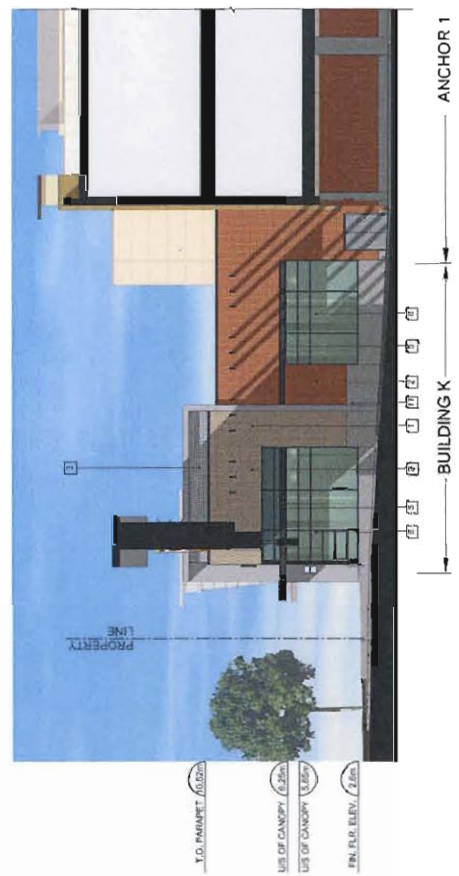
PROPOSED:

SIGNAGE 1	5.8 x 1.2	6.0 m ²
TOTAL		6.0 m ²

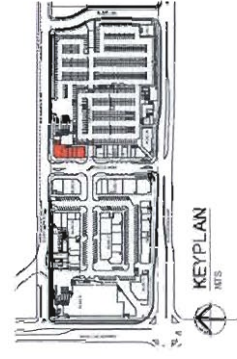


NORTH ELEVATION
SCALE 1:100

- FINISH LEGEND AND COLOUR SCHEDULE:**
- 1 - EPS
 - 2 - BRICK VENEER
 - 3 - CORRUGATED METAL
 - 4 - METAL PANELS
 - 5 - METAL CANOPY
 - 6 - SPANDREL GLASS IN ALUMINIUM FRAME
 - 7 - CANVAS AWNING
 - 8 - METAL & GLASS CANOPY
 - 9 - METAL DOOR
 - 10 - CONCRETE UPSTAND
 - 11 - GLAZING IN FINISHED ALUMINIUM FRAME
 - 12 - PREFABRICATED WOOD PANELS



SOUTH ELEVATION
SCALE 1:100



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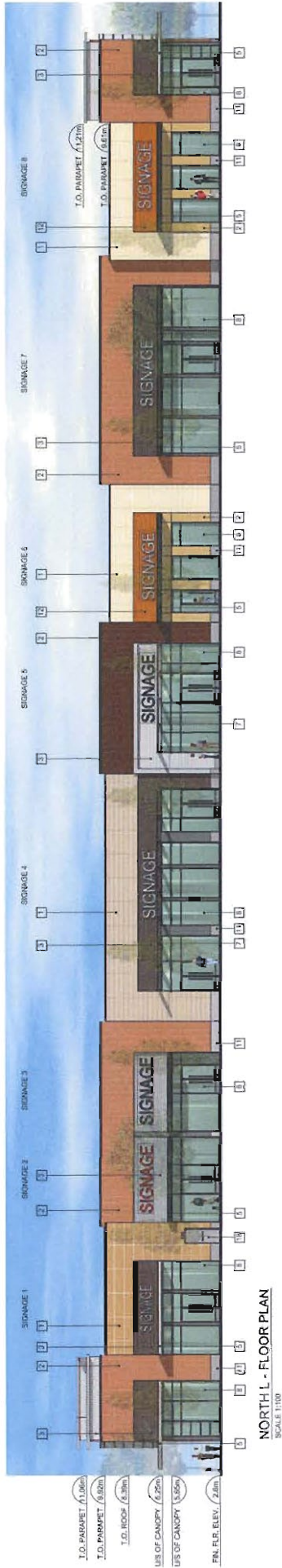
DATE: AUGUST 20, 2013
SCALE: AS NOTED
PROJECT # 4029

A-3.17

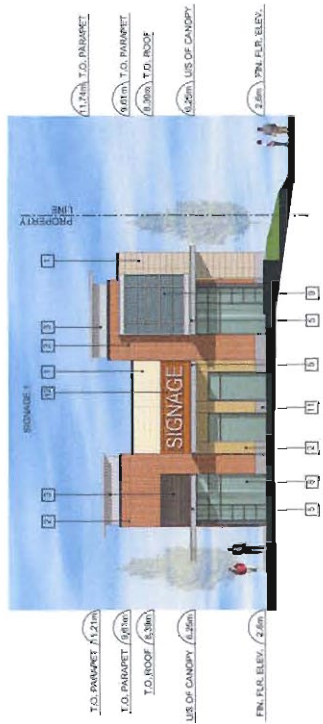
BUILDING K PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC

Sheet No. 22
September 5, 2013
RZ 10-528877

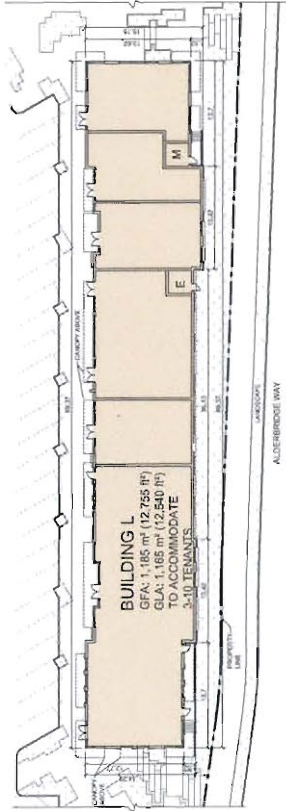




NORTH L - FLOOR PLAN
SCALE 1:100



WEST ELEVATION
SCALE 1:100



BUILDING L - FLOOR PLAN
SCALE 1:200



KEY PLAN
N

PRELIMINARY SIGNAGE ANALYSIS

REQUIRED:		TOTAL SIGN AREA / PERMITTED:	
BIDS LENGTH	LINEAR FOOTAGE	PERMITTED	PERMITTED
80.37m	171'	80.37 m ²	80.37 m ²

REQUIRED:		TOTAL SIGN AREA / PERMITTED:	
BIDS WIDTH	FRONTAGE	PERMITTED	PERMITTED
14.29m	171'	14.29 m ²	14.29 m ²

PROPOSED:		TOTAL SIGN AREA / PERMITTED:	
SIGNAGE 1	LINEAR FOOTAGE	PERMITTED	PERMITTED
14.25	171'	14.25 m ²	14.25 m ²
SIGNAGE 2	14.25	14.25 m ²	14.25 m ²
SIGNAGE 3	14.25	14.25 m ²	14.25 m ²
SIGNAGE 4	14.25	14.25 m ²	14.25 m ²
SIGNAGE 5	14.25	14.25 m ²	14.25 m ²
SIGNAGE 6	14.25	14.25 m ²	14.25 m ²
SIGNAGE 7	14.25	14.25 m ²	14.25 m ²
SIGNAGE 8	14.25	14.25 m ²	14.25 m ²
TOTAL		114.00	114.00

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EIFS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINUM FRAME
- 9 - SPANISH GLASS IN ALUMINUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS

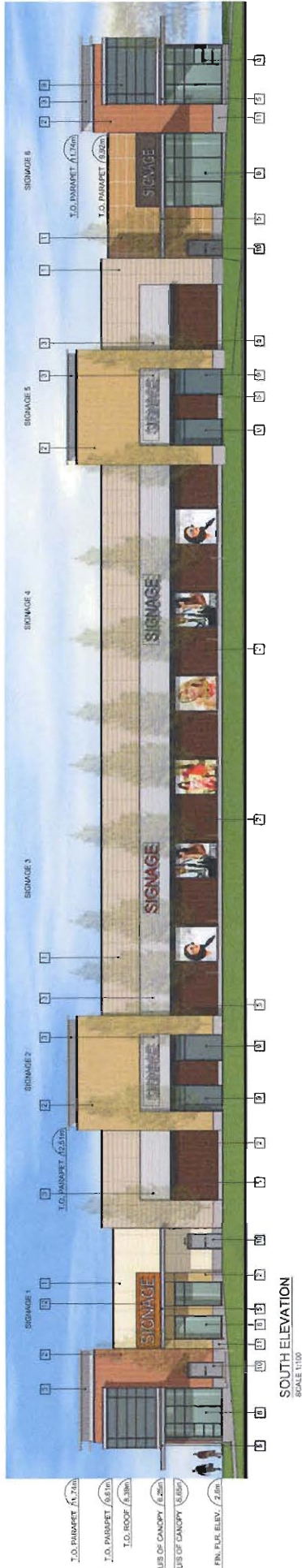
Sheet No. 23
September 5, 2013
RZ 10-528877



BUILDING L PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC



DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: A-3.18



SOUTH ELEVATION
SCALE 1:100



EAST ELEVATION
SCALE 1:100

PRELIMINARY SIGNAGE ANALYSIS

SOUTH ELEVATION

REQUIRED	PERMITTED SIGN AREA / SIGNAGE	TOTAL SIGN AREA / PERMITTED
80.27 m	1 m ²	80.27 m ²
1 x 4.5	4.5 m ²	4.5 m ²
1 x 4.5	4.5 m ²	4.5 m ²
1 x 4.5	4.5 m ²	4.5 m ²
1 x 4.5	4.5 m ²	4.5 m ²
1 x 4.5	4.5 m ²	4.5 m ²
TOTAL		27 m²

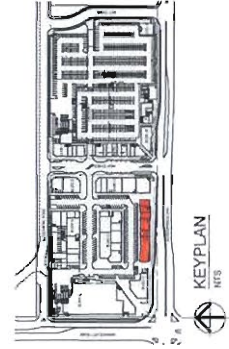
EAST ELEVATION

REQUIRED	PERMITTED SIGN AREA / SIGNAGE	TOTAL SIGN AREA / PERMITTED
15.15 m	1 m ²	15.15 m ²
1 x 4.5	4.5 m ²	4.5 m ²
TOTAL		4.5 m²

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANALS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINIUM FRAME
- 9 - SPANREL GLASS IN ALUMINIUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS

Sheet No. 24
September 5, 2013
RZ 10-528877



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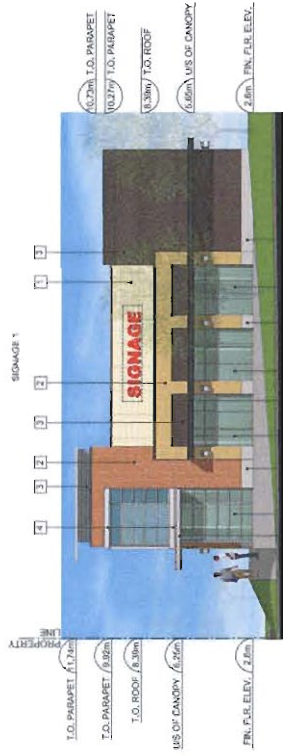
DATE: AUGUST 26, 2013
SCALE: AS NOTED
PROJECT #: 4070
A-3.19

BUILDING L PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC





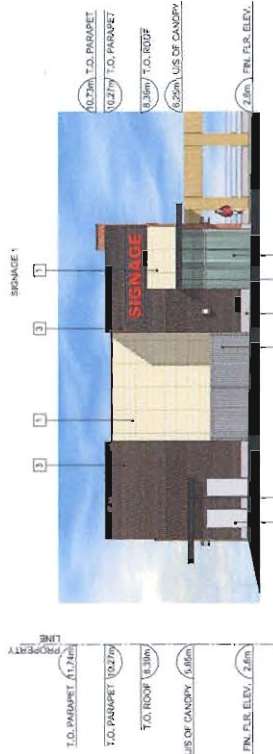
SOUTH ELEVATION
SCALE 1:100



EAST ELEVATION
SCALE 1:100



WEST ELEVATION
SCALE 1:100



NORTH ELEVATION
SCALE 1:100

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLAZING IN BRUSHED ALUMINUM FRAME
- 9 - SPANDREL GLASS IN ALUMINUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS

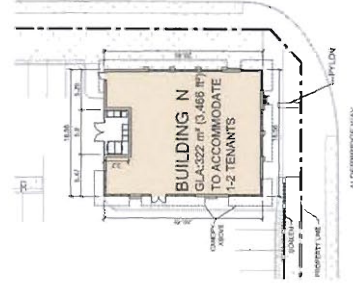
PRELIMINARY SIGNAGE ANALYSIS

WEST ELEVATION			
REQUIRED:		PROPOSED:	
BLDG. LENGTH	PERMITTED FRONTAGE	SIGNAGE 1	TOTAL
20.45 m	1 m*	1 x 6	6 m*
TOTAL SIGN AREA / PERMITTED FRONTAGE		TOTAL SIGN AREA / PERMITTED FRONTAGE	
18.55 m*		20.45 m*	

NORTH ELEVATION			
REQUIRED:		PROPOSED:	
BLDG. WIDTH	PERMITTED FRONTAGE	SIGNAGE 1	TOTAL
16.58 m	1 m*	1 x 6	6 m*
TOTAL SIGN AREA / PERMITTED FRONTAGE		TOTAL SIGN AREA / PERMITTED FRONTAGE	
16.55 m*		20.45 m*	

SOUTH ELEVATION			
REQUIRED:		PROPOSED:	
BLDG. WIDTH	PERMITTED FRONTAGE	SIGNAGE 1	TOTAL
20.45 m	1 m*	1 x 6	6 m*
TOTAL SIGN AREA / PERMITTED FRONTAGE		TOTAL SIGN AREA / PERMITTED FRONTAGE	
18.55 m*		20.45 m*	

EAST ELEVATION			
REQUIRED:		PROPOSED:	
BLDG. LENGTH	PERMITTED FRONTAGE	SIGNAGE 1	TOTAL
20.45 m	1 m*	1 x 6	6 m*
TOTAL SIGN AREA / PERMITTED FRONTAGE		TOTAL SIGN AREA / PERMITTED FRONTAGE	
18.55 m*		20.45 m*	



BUILDING N - FLOOR PLAN
SCALE 1:250

Sheet No. 25
September 5, 2013
RZ 10-528877

BUILDING N PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC



DATE: AUGUST 28, 2013
SCALE: AS NOTED
PROJECT #: 402

A-3.20



WEST ELEVATION
SCALE 1:150

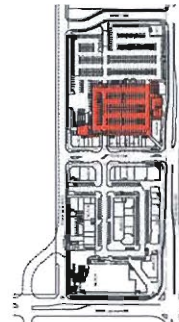


SOUTH ELEVATION
SCALE 1:150

PLN - 262

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - SPANDREL GLASS IN ALUMINUM FRAME
- 9 - METAL DOOR
- 10 - CONCRETE UPSTAND
- 11 - GLASS IN BRUSHED ALUMINUM FRAME
- 12 - PAINTED CONCRETE
- 13 - METAL SCREEN
- 14 - METAL PLANTER
- 15 - FINISHED WOOD PANELS



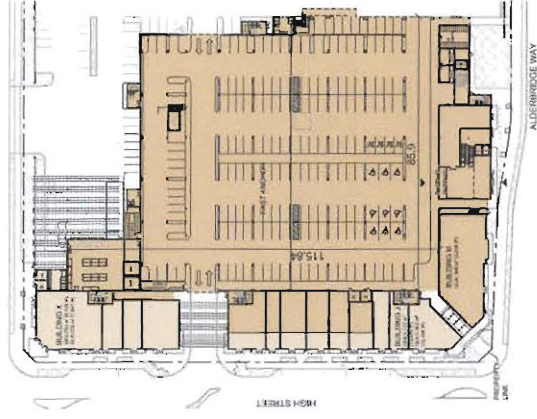
KEYPLAN
N.T.S.

PRELIMINARY SIGNAGE ANALYSIS

WEST ELEVATION			SOUTH ELEVATION		
REQUIRED:	PERMITTED:	TOTAL SIGN AREA / LINEAR FRONTAGE	REQUIRED:	PERMITTED:	TOTAL SIGN AREA / LINEAR FRONTAGE
BLDG. LENGTH: 115.8 m	1.1 m ²	115.8 m ²	BLDG. WIDTH: 85.9 m	1.1 m ²	86.0 m ²
PROPOSED:	PROPOSED:		PROPOSED:	PROPOSED:	
SIGNAGE 1: 11.0 x 3.7	SIGNAGE 1: 12.6 x 4.0	40.7 m ²	SIGNAGE 1: 5.0 x 1.5	SIGNAGE 1: 5.0 x 1.5	7.5 m ²
SIGNAGE 2: 8.0 x 1.3	SIGNAGE 2: 5.0 x 1.5	9.6 m ²	SIGNAGE 2: 5.0 x 1.5	SIGNAGE 2: 5.0 x 1.5	7.5 m ²
TOTAL:	TOTAL:	50.3 m ²	TOTAL:	TOTAL:	15.0 m ²

Sheet No. 26
September 5, 2013
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PARKING LEVEL PLAN
SCALE 1:500





EAST ELEVATION
SCALE 1/16"

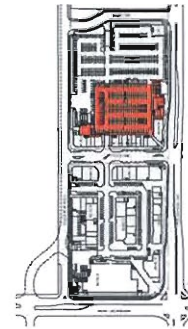


NORTH ELEVATION
SCALE 1/16"

PLN - 263

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EPS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - SPANDREL GLASS IN ALUMINUM FRAME
- 9 - METAL DOOR
- 10 - CONCRETE UPSTAND
- 11 - BRUSHED WOOD PANELS
- 12 - PAINTED CONCRETE
- 13 - METAL SCREEN
- 14 - METAL PLANTER
- 15 - METAL PLANTER

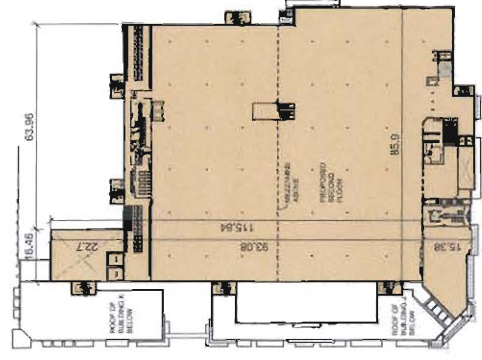


KEYPLAN
N/S

PRELIMINARY SIGNAGE ANALYSIS

EAST ELEVATION REQUIRED:		PROPOSED:	
PERMITTED SIGN AREA	115.8 sq' (1.3%)	SIGNAGE 1	7.5 x 22.5
PERMITTED FRONTAGE	115.8 sq'	SIGNAGE 2	7.5 x 22.5
		TOTAL	15.8 sq'

SALES LEVEL PLAN
SCALE 1/800



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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4079

A-3.22

Sheet No. 27
September 5, 2013
RZ 10-528877
EAST ANCHOR PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC





NORTH INTERNAL ELEVATION
SCALE 1:100

FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - EBS
- 2 - BRICK VENEER
- 3 - CORRUGATED METAL
- 4 - METAL PANELS
- 5 - METAL CANOPY
- 6 - CANVAS AWNING
- 7 - METAL & GLASS CANOPY
- 8 - GLASS IN BRUSHED ALUMINUM FRAME
- 9 - BRANDED GLASS IN ALUMINUM FRAME
- 10 - METAL DOOR
- 11 - CONCRETE UPSTAND
- 12 - PREFINISHED WOOD PANELS
- 13 - PAINTED CONCRETE
- 14 - METAL SCREEN
- 15 - METAL PLANTER

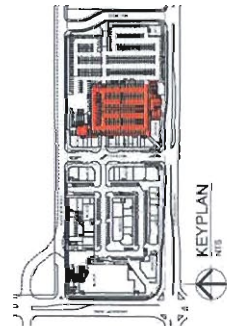
PRELIMINARY SIGNAGE ANALYSIS

NORTH ELEVATION		PROPOSED:	
PERMITTED SIGN LENGTH	115.8 m	SIGNAGE 1	5.2 x 2.0
PERMITTED FRONTAGE	115.8 m	SIGNAGE 2	1.0 x 3.0
		SIGNAGE 3	1.0 x 3.0
		TOTAL	18.0 m ²



MEZZANINE LEVEL PLAN
SCALE 1:100

Sheet No. 28
September 5, 2013
RZ 10-528877



KEY PLAN
N

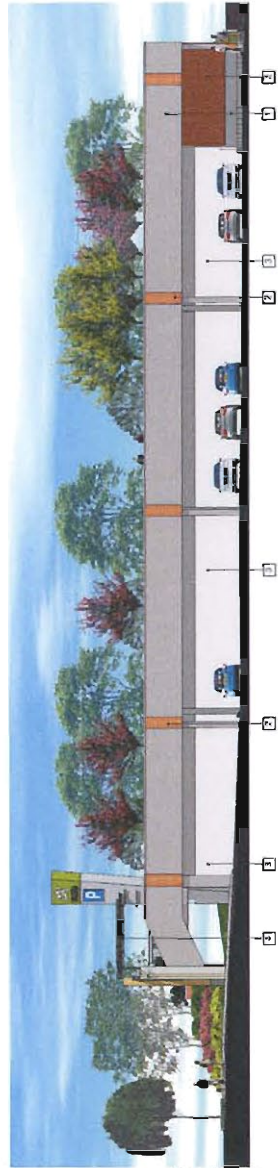
EAST ANCHOR PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY
RICHMOND, BC



NORTH ELEVATION

SCALE 1/150

4.6 m
TO CONCRETE DECK
0.9 m
TO FIN. FLOOR
4.0 m
TO PARKADE FLOOR



WEST ELEVATION

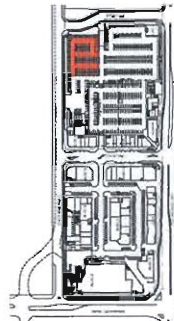
SCALE 1/150

4.6 m
TO CONCRETE DECK
0.9 m
TO FIN. FLOOR
4.0 m
TO PARKADE FLOOR

PLN - 265

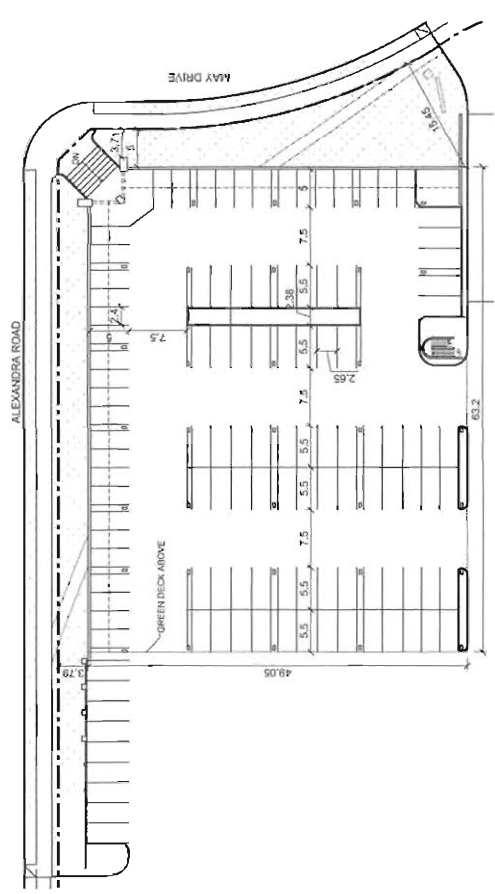
FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - PAINTED CONCRETE
- 2 - BRICK VENER
- 3 - PAINTED CONCRETE WHITE
- 4 - METAL QUADRIMEL
- 5 - METAL TRELLIS



KEYPLAN

NTS



GROUND LEVEL PLAN

SCALE 1/250

Sheet No. 29
September 5, 2013
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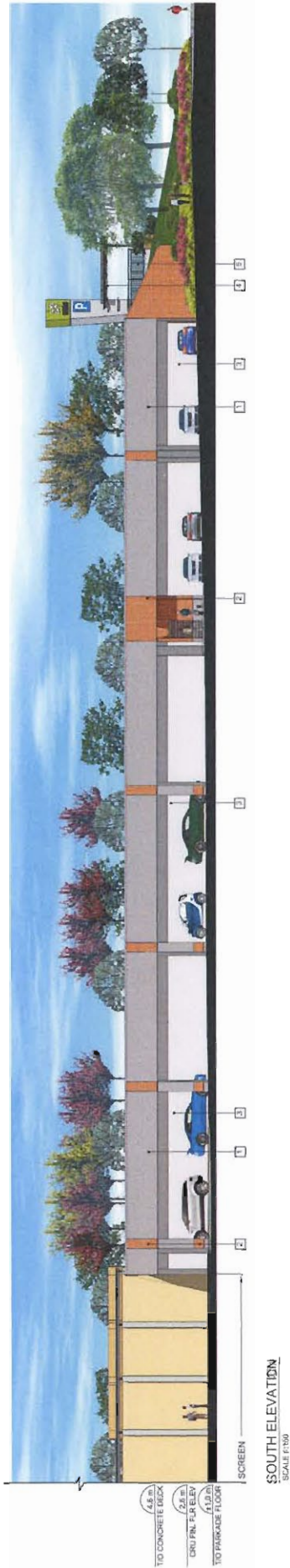
EAST ANCHOR PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY

RICHMOND, BC



DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4279

A-3.24



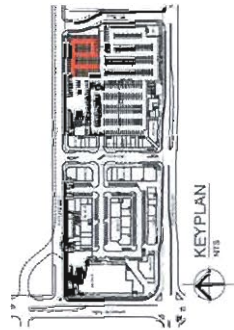
SOUTH ELEVATION
SCALE 1/150



EAST ELEVATION
SCALE 1/150

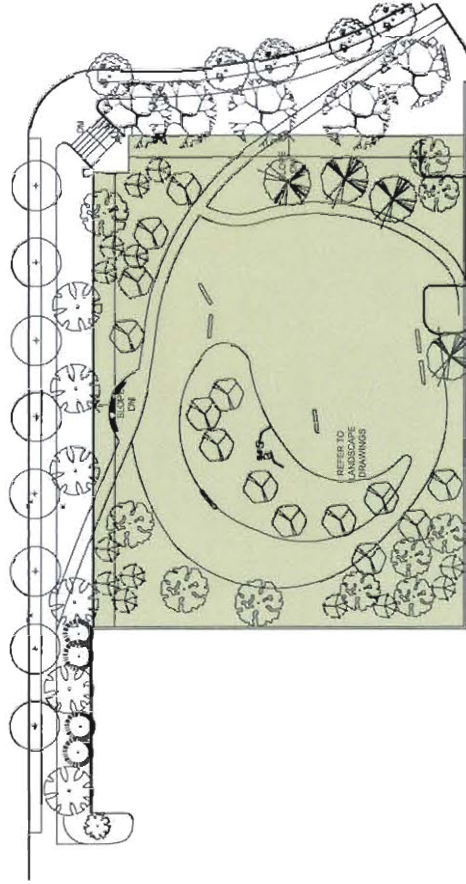
FINISH LEGEND AND COLOUR SCHEDULE:

- 1 - PAINTED CONCRETE
- 2 - BRICK VIMBER
- 3 - PAINTED CONCRETE W/RTZ
- 4 - METAL GUARDRAIL
- 5 - METAL TRELLIS



KEY PLAN
1/125

Sheet No. 30
September 5, 2013
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DECK PLAN
SCALE 1/200

EAST ANCHOR PLANS AND ELEVATIONS
CENTRAL AT GARDEN CITY

RICHMOND, BC





1 - STREET ELEVATIONS ALONG ALEXANDRA ROAD LOOKING SOUTH



2 - STREET ELEVATIONS ALONG ALDERBRIDGE WAY LOOKING NORTH

PLN - 267



KEY PLAN
SCALE: NTS

Sheet No. 31
September 5, 2013
RZ 10-528877

STREET ELEVATIONS WITHOUT LANDSCAPE

CENTRAL AT GARDEN CITY

RICHMOND, BC

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DATE: AUGUST 29, 2013
SCALE: 1/100
PROJECT #: 4029

A-3.26



VIEW 1 - EXISTING STREET ELEVATIONS ALONG ALDERBRIDGE WAY LOOKING NORTH



VIEW 1 - PROPOSED STREET ELEVATIONS ALONG ALDERBRIDGE WAY LOOKING NORTH



VIEW 2 - EXISTING STREET ELEVATIONS ALONG ALDERBRIDGE WAY LOOKING NORTH



VIEW 2 - PROPOSED STREET ELEVATIONS ALONG ALDERBRIDGE WAY LOOKING NORTH



VIEW 3 - EXISTING STREET ELEVATIONS ALONG ALDERBRIDGE WAY LOOKING NORTH



VIEW 3 - PROPOSED STREET ELEVATIONS ALONG ALDERBRIDGE WAY LOOKING NORTH



GARDEN CITY AND ALDERBRIDGE WAY
INTERSECTION LOOKING NORTH

Sheet No. 32
September 5, 2013
RZ 10-528877

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DATE: SEPTEMBER 8, 2013
SCALE: NTS
PROJECT #: A-3.27

CENTRAL AT GARDEN CITY
RICHMOND, BC

ALDERBRIDGE STREET ELEVATIONS



BUILDING A WEST ELEVATION ALONG GARDEN CITY ROAD
SCALE: 1:1500

1.0. PARAPET (7.20m)

2ND LEVEL (6.00m)

FIN. F.L.R. ELEV. (2.00m)



BUILDING H AND L SOUTH ELEVATIONS ALONG ALDERBRIDGE ROAD
SCALE: 1:1500

1.0. PARAPET (6.80m)

US OF CANOPY (6.20m)

US OF CANOPY (6.50m)

FIN. F.L.R. ELEV. (2.80m)

FIN. F.L.R. ELEV. (1.00m)



ANCHOR I SOUTH ELEVATION ALONG ALDERBRIDGE ROAD
SCALE: 1:1500

BUILDING M

ANCHOR I



KEY PLAN
NTS



BUILDING N SOUTH ELEVATION
SCALE: 1:1500

1.0. PARAPET (7.20m)

FIN. F.L.R. ELEV. (2.00m)

Sheet No. 33

September 5, 2013

RZ 10-528877

LANDSCAPE GROWTH WITHIN 10 YEARS

CENTRAL AT GARDEN CITY

RICHMOND, BC



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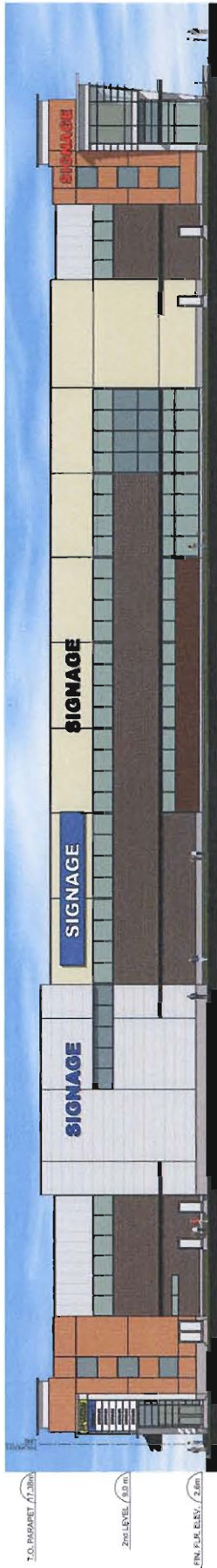
DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4022

A-3.28



SmartCentres

PLN 269



BUILDING A WEST ELEVATION
SCALE: 1:150

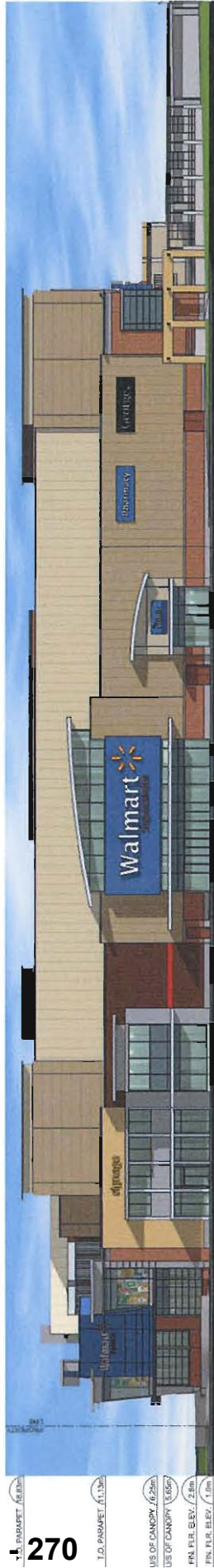
1.0. PARAPET 17.3m

2nd LEVEL 6.0 m

FIN. F.L.R. ELEV. 2.6m



BUILDING H AND L SOUTH ELEVATIONS
SCALE: 1:150

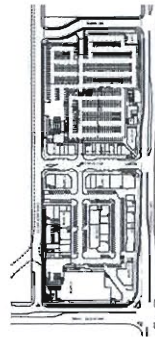


ANCHOR 1 SOUTH ELEVATION
SCALE: 1:150

BUILDING J

BUILDING M

ANCHOR 1



KEY PLAN
NTS



BUILDING N SOUTH ELEVATION
SCALE: 1:150

1.0. PARAPET 17.3m

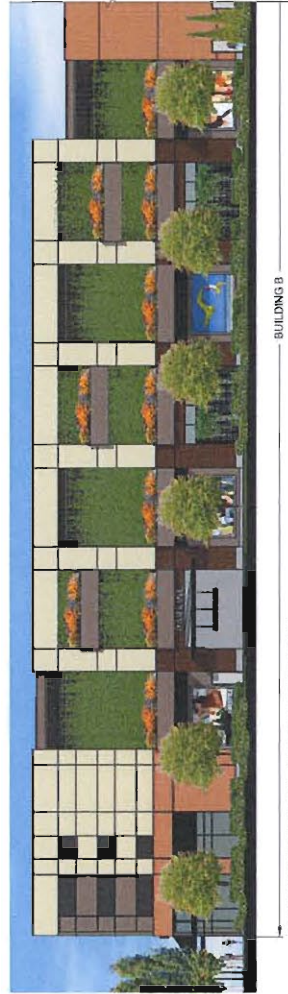
FIN. F.L.R. ELEV. 2.6m

Sheet No. 34
September 5, 2013
RZ 10-528877

STREET ELEVATIONS WITHOUT LANDSCAPE
CENTRAL AT GARDEN CITY
RICHMOND, BC



EAST ANCHOR NORTH ELEVATION
SCALE: 1/150



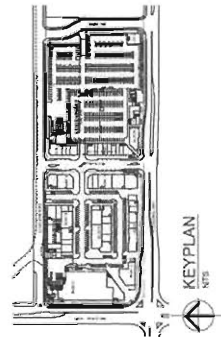
BUILDING B PARTIAL NORTH ELEVATION
SCALE: 1/150



BUILDING E NORTH ELEVATION
SCALE: 1/150



BUILDING B PARTIAL NORTH ELEVATION AND BUILDING A NORTH ELEVATION
SCALE: 1/150



Sheet No. 35
September 5, 2013
RZ 10-528877

LANDSCAPE GROWTH WITHIN 10 YEARS ALONG ALEXANDRA ROAD

CENTRAL AT GARDEN CITY

RICHMOND, BC



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DATE: AUGUST 29, 2013
SCALE: AS NOTED
PROJECT #: 4079

A-3.30



EAST ANCHOR NORTH ELEVATION
SCALE: 1/150



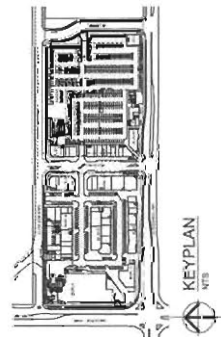
BUILDING B PARTIAL NORTH ELEVATION.
SCALE: 1/150



BUILDING E NORTH ELEVATION
SCALE: 1/150



BUILDING B PARTIAL NORTH ELEVATION AND BUILDING A NORTH ELEVATION
SCALE: 1/150



Sheet No. 36
September 5, 2013
RZ 10-528877

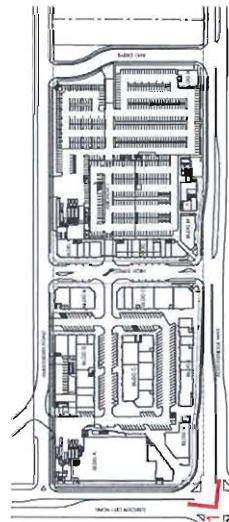
ELEVATIONS WITHOUT LANDSCAPE ALONG ALEXANDRA ROAD
CENTRAL AT GARDEN CITY
RICHMOND, BC



PERSPECTIVE 1 - VIEW FROM GARDEN CITY ROAD @ ALDERBRIDGE WAY LOOKING NORTHEAST.

PLN - 273

Sheet No. 37
 September 5, 2013
 RZ 10-528877



KEY PLAN
 SCALE: 1:125



CENTRAL AT GARDEN CITY
 RICHMOND, BC

PERSPECTIVES

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DATE: AUGUST 29, 2013
 SCALE: NIS
 PROJECT #: 4029

A-4.1



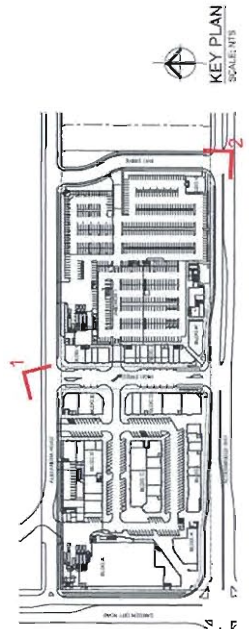
PERSPECTIVE 1 - ALEXANDRA ROAD @ HIGH STREET LOOKING SOUTH - WEST

PLN - 274



PERSPECTIVE 2 - ALDERBRIDGE WAY @ MAY DRIVE LOOKING NORTH - WEST

Sheet No. 38
September 5, 2013
RZ 10-528877



KEY PLAN
SCALE: 1:150



CENTRAL AT GARDEN CITY
RICHMOND, BC

SITE PERSPECTIVES



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DATE: AUGUST 26, 2013
SCALE: 1:150
PROJECT #: 4079

A-4.2



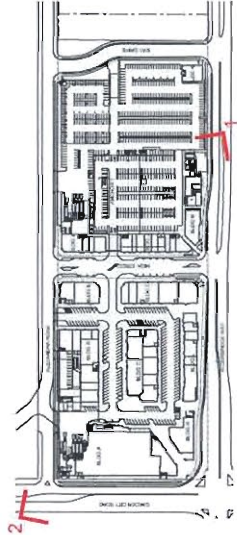
PERSPECTIVE 1 - ALDERBRIDGE WAY LOOKING WEST-NORTH

PLN - 275



PERSPECTIVE 2 - GARDEN CITY ROAD @ ALEXANDRA ROAD LOOKING SOUTH - EAST

Sheet No. 39
 September 5, 2013
 RZ 10-528877



KEY PLAN
 SCALES



CENTRAL AT GARDEN CITY
 RICHMOND, BC

SITE PERSPECTIVES

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DATE: AUGUST 28, 2013
 SCALE: 1:150
 PROJECT #:
 4029

A-4.3



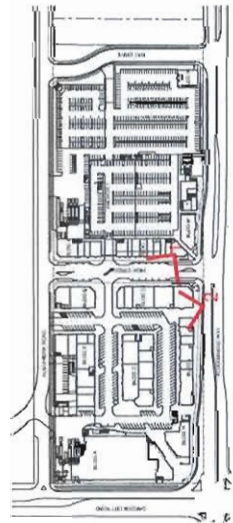
PERSPECTIVE 1 - BUILDING D LOOKING NORTH-WEST

PLN - 276



PERSPECTIVE 2 - LOOKING NORTH

Sheet No. 40
September 5, 2013
RZ 10-528877



CENTRAL AT GARDEN CITY

RICHMOND, BC

SITE PERSPECTIVES



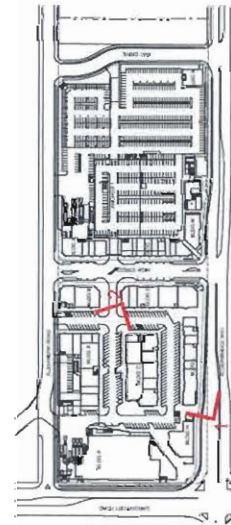
PERSPECTIVE 1 - ALDERBRIDGE WAY LOOKING NORTH-EAST

PLN - 277



PERSPECTIVE 2 - BUILDING B - HIGH STREET LOOKING NORTH - WEST

Sheet No. 41
September 5, 2013
RZ 10-528877



KEY PLAN
SCALE: NTS



CENTRAL AT GARDEN CITY
RICHMOND, BC



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DATE: AUGUST 26, 2013
SCALE: 1/1500
PROJECT#: 4029

A-4.5

SITE PERSPECTIVES



PERSPECTIVE 1 - ALDERBRIDGE WAY LOOKING NORTH -EAST

PLN - 278



PERSPECTIVE 2 - BUILDING A - LOOKING WEST-NORTH

Sheet No. 42
September 5, 2013
RZ 10-528877



KEY PLAN
SCALE: NTS



CENTRAL AT GARDEN CITY

RICHMOND, BC

SITE PERSPECTIVES



DATE: AUGUST 26, 2013
SCALE: 1:150
PROJECT #: 4079

A-4.6



PERSPECTIVE 1 - BUILDING C - LOOKING SOUTH-EAST

PLN - 279



PERSPECTIVE 2 - ALEXANDRA ROAD @ MAY DRIVE LOOKING SOUTH-WEST

Sheet No. 43
September 5, 2013
RZ 10-528877



KEY PLAN
SCALE: NTS



CENTRAL AT GARDEN CITY

RICHMOND, BC

SITE PERSPECTIVES

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chandler@chandlerassoc.com



DATE: AUGUST 29, 2013
SCALE: 1:150
PROJECT #: 4029

A-4.7



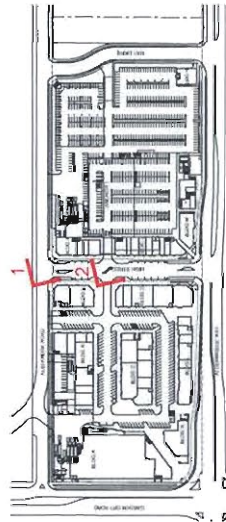
PERSPECTIVE 1 - ALEXANDRA ROAD @ HIGH STREET LOOKING EAST-SOUTH

PLN - 280



PERSPECTIVE 2 - HIGH STREET LOOKING SOUTH-EAST

Sheet No. 44
 September 5, 2013
 RZ. 10-528877



KEY PLAN
 SCALE: 1/8" = 1'-0"

SITE PERSPECTIVES

CENTRAL AT GARDEN CITY

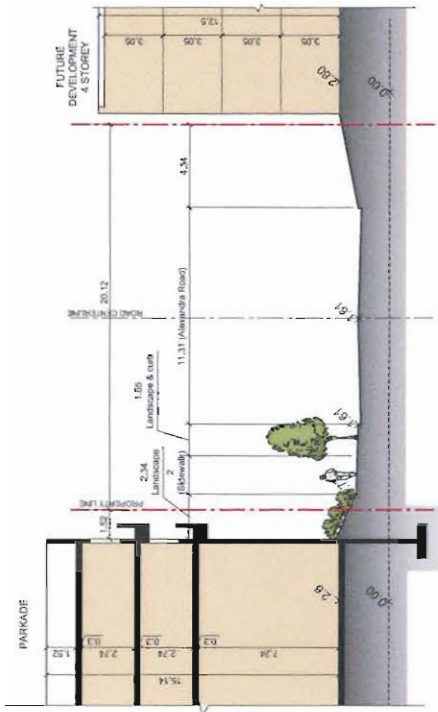
RICHMOND, BC



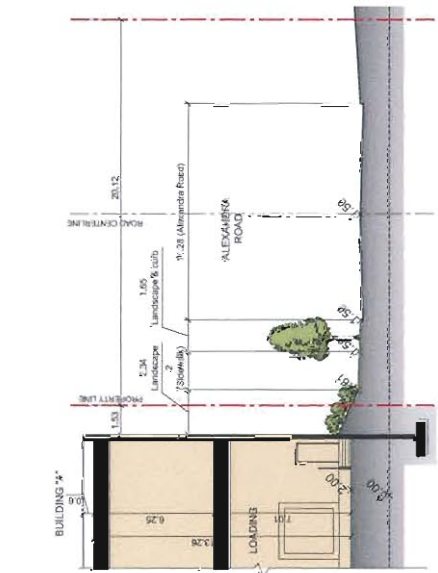
Chandler Associates
 Architects Inc.
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 Tel: 604.681.3300
 Fax: 604.681.3305
 cba@chandler-associates.com

DATE: AUGUST 29, 2013
 SCALE: 1/16" = 1'-0"
 PROJECT #: 4029

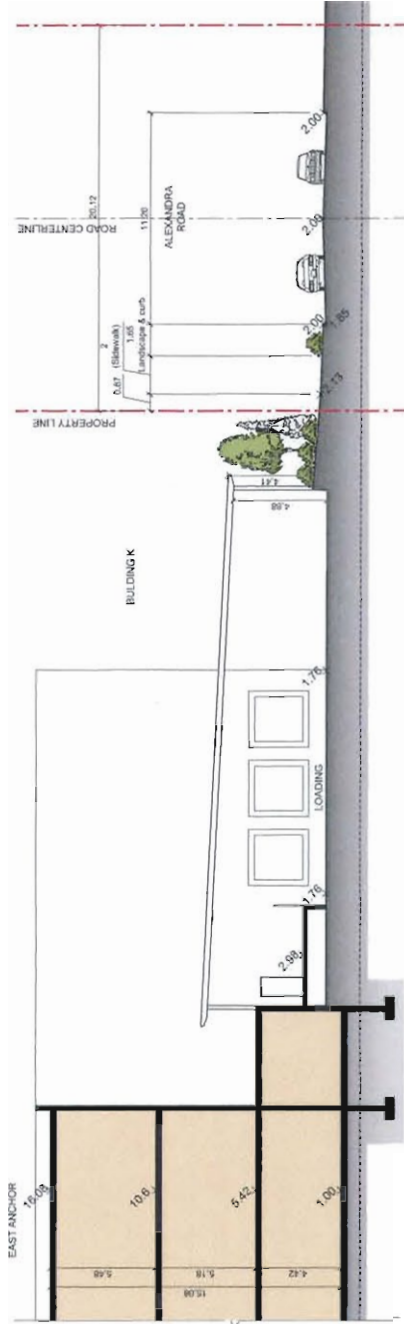
A-4.8



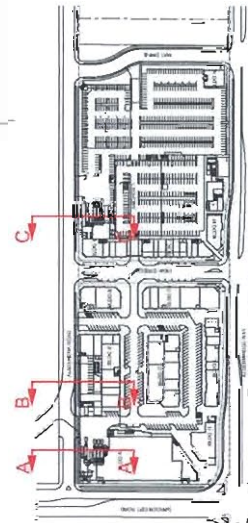
SECTION A-A
SCALE 1:100



SECTION B-B
SCALE 1:100



SECTION C-C
SCALE 1:100



KEY PLAN
SCALE 1:500

Sheet No. 45
September 5, 2013
RZ 10-528877

CENTRAL AT GARDEN CITY
RICHMOND, BC

SmartCentres

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DATE: AUGUST 29, 2013
SCALE: 1:100
PROJECT #: 4079

A-5.1



SECTION G-G
SCALE 1/80

PLN - 283

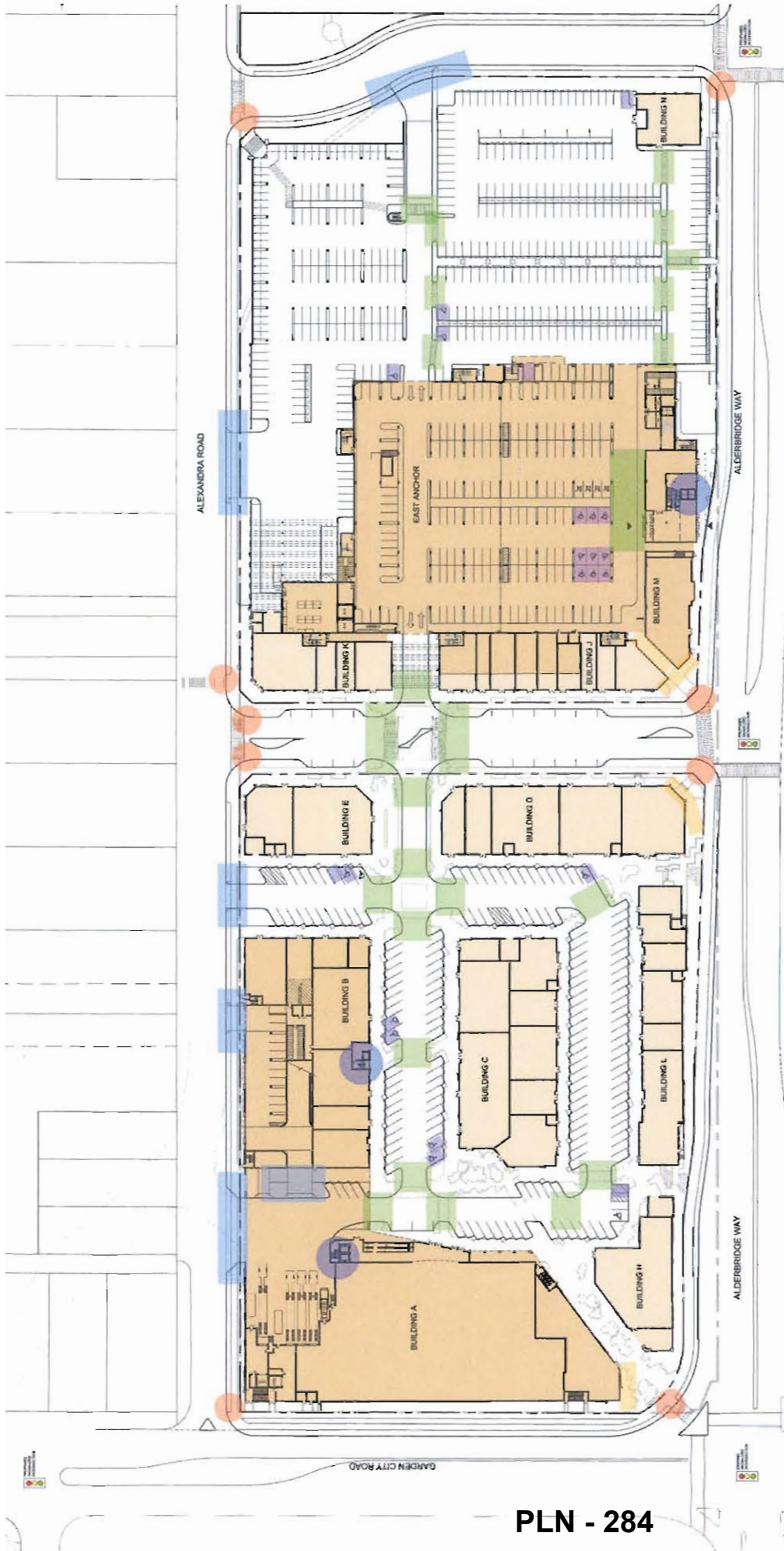


KEY PLAN
SCALE 1/50

Sheet No. 47
September 5, 2013
RZ 10-528877

CENTRAL AT GARDEN CITY
RICHMOND, BC



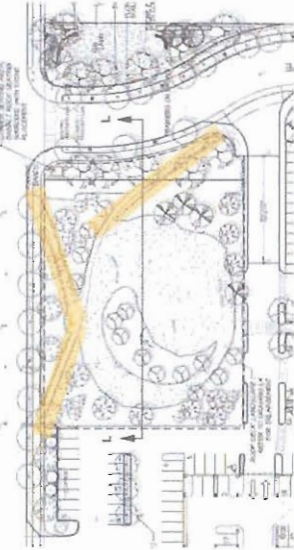


PLN - 284

CENTRAL AT GARDEN CITY - ACCESSIBLE DESIGN SITE PLAN

ACCESSIBLE DESIGN LEGEND

-  CURB LETDOWN AT CROSSWALKS
-  RAISED "TABLE" CROSSWALKS
-  RAMP
-  CONTINUOUS SIDEWALK WITH VEHICLE LETDOWN ACCESS
-  H/C STALLS
-  ELEVATORS



Sheet No. 48
 September 5, 2013
 RZ 10-528877

CENTRAL AT GARDEN CITY
 RICHMOND, BC

ACCESSIBLE DESIGN SITE PLAN

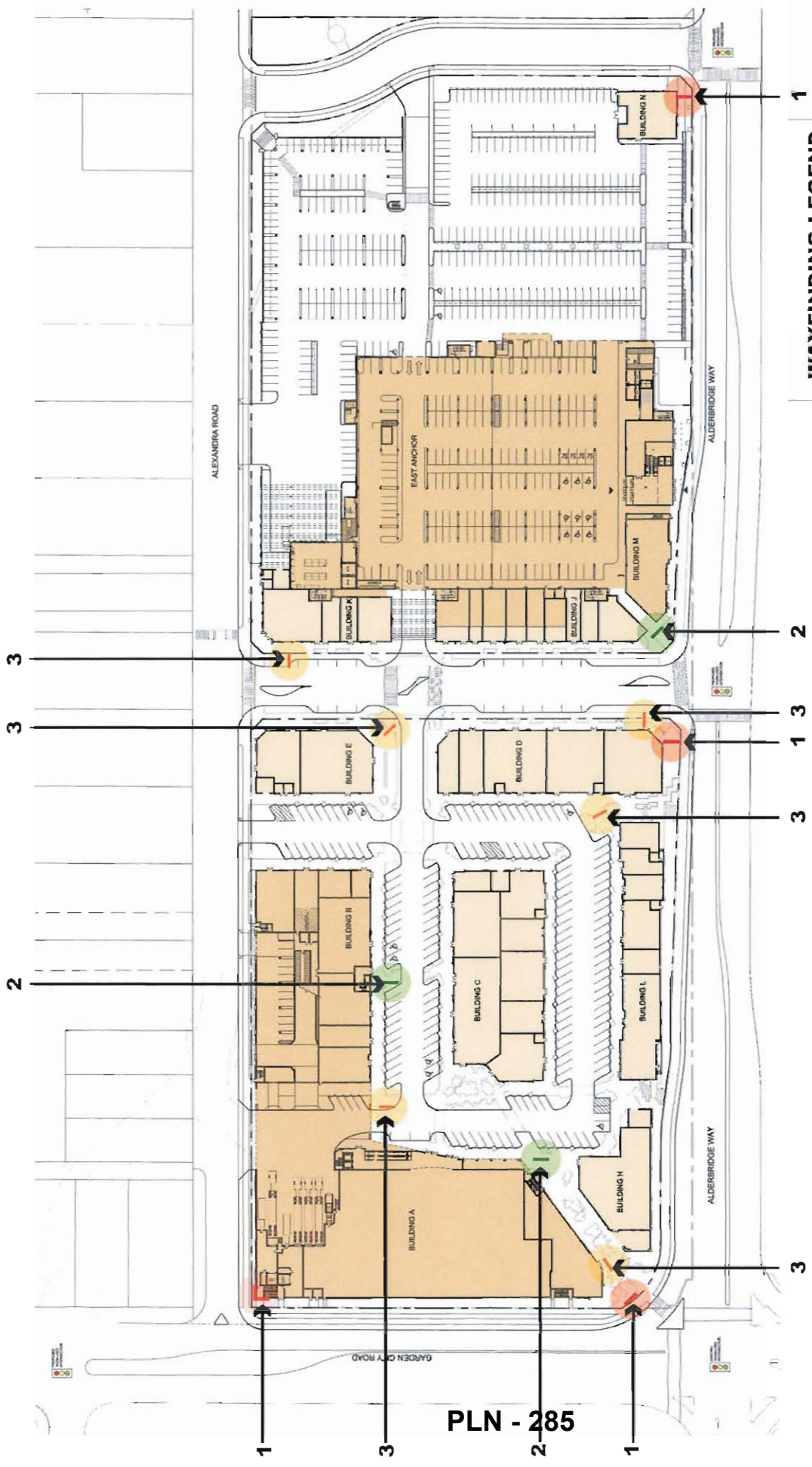
LANDSCAPED DECK AREA



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DATE: NOVEMBER 2012
 SCALE: A209
 PROJECT: A-6.1





PLN - 285

WAYFINDING LEGEND

- 1. PYLON SIGN
- 2. DIRECTORY PANEL
- 3. DIRECTIONAL SIGN

Chandler Associates
 Architects Inc.
 The Station, 1000-10th St.
 Vancouver, BC V6B 1G1
 604.681.7300
 office@chandlerassociates.com

DATE: NOVEMBER 2012
 SCALE: 1/8"=1'-0"
 PROJECT # 4077

Sheet No. 49
 September 5, 2013
 R/Z 10-528877

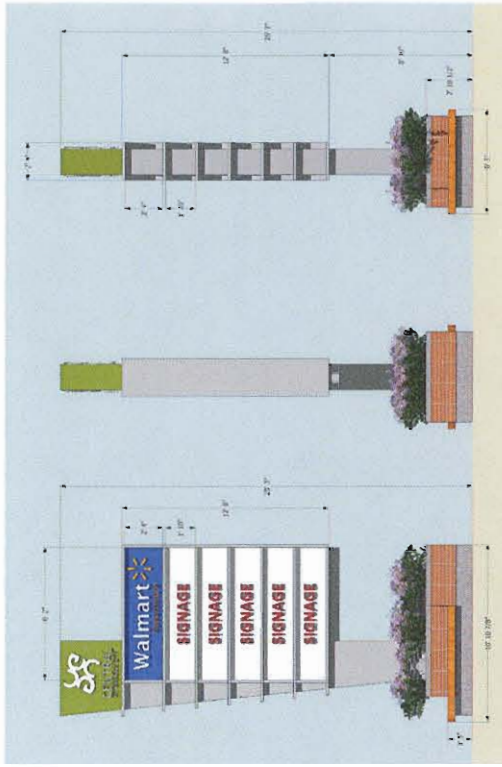


PERSPECTIVE VIEW - WAYFINDING SIGNAGE: PYLON, DIRECTORY AND DIRECTIONAL

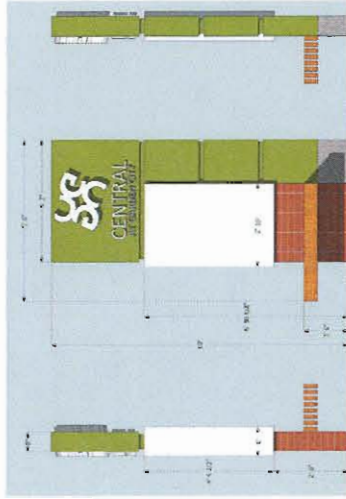
Sheet No. 50
 September 5, 2013
 RZ 10-528877



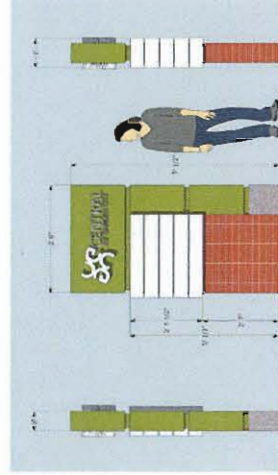
CENTRAL AT GARDEN CITY
 RICHMOND, BC



ELEVATIONS - WAYFINDING SIGNAGE: PYLON



ELEVATIONS - WAYFINDING SIGNAGE: DIRECTORY



ELEVATIONS - WAYFINDING SIGNAGE: DIRECTIONAL

TYPICAL WAYFINDING SIGNAGE



DATE: AUGUST 26, 2013
 SCALE:
 PROJECT # 4279

A-6.3

PLANT SCHEDULE - TREES	PLANT SCHEDULE - SHRUBS	PLANT SCHEDULE - GRASSES, VINES, AND PERENNIALS	PLANT SCHEDULE - GROUNDCOVERS
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September 5, 2013
RZ 10-528877



Overall Landscape theme is to merge the contemporary architecture with Richmond's natural surroundings, the River's edge. Shoreline grasses, indigenous trees and shrubs found along the River's edge along with wood/dolphin piles, driftwood logs and boulders have been implemented to soften building edges and recognize Richmond's natural landscape.



'CENTRAL' AT GARDEN CITY
GARDEN CITY ROAD AND ALDERBRIDGE WAY
RICHMOND, BC

OVERALL LANDSCAPE PLAN

L1

DATE: 09/05/13
SCALE: AS SHOWN
PROJECT/CLIENT NUMBER: 101112137



SHORELINE PLANTING



RICH AND POPULAR TREES ANCHOR THE SITE TO THE REGION

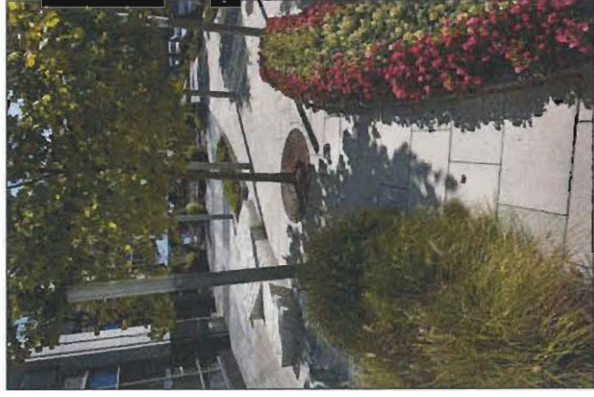


SHORELINE GRASSES PLANTINGS



PEEZED LOGS AND LOGS ALONGS SERVE AS BEATING ELEMENTS AND ARE EMBROIDERED IN PLANTING AREAS

Sheet No. 52
September 5, 2013
RZ 10-528877



PROVIDES OF PLANTING AREA AND CONNECTION TO THE PAVED AREAS



SITE LOCATOR

The pedestrian arrival plaza anchors the site while offering a sense of arrival. People are drawn inward by the openness of this space along with the native shoreline landscaping, and commencement of the Alexandra Wayfinding element. Traveling through the Village, one begins to experience the hierarchy of plaza spaces along the frontages of the shops, restaurants and services.



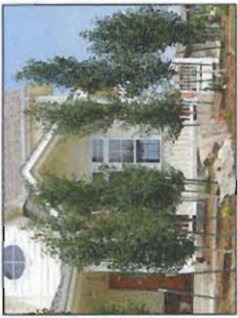
RENDERING LOOKING AT SW CORNER



PLAZA AREA WITH SEATING AREAS FOR GATHERING, CANOPY SHADE TREES AND SITE FURNITURE



CANOPY TREES FOR SHADE



PLAZA SEATING



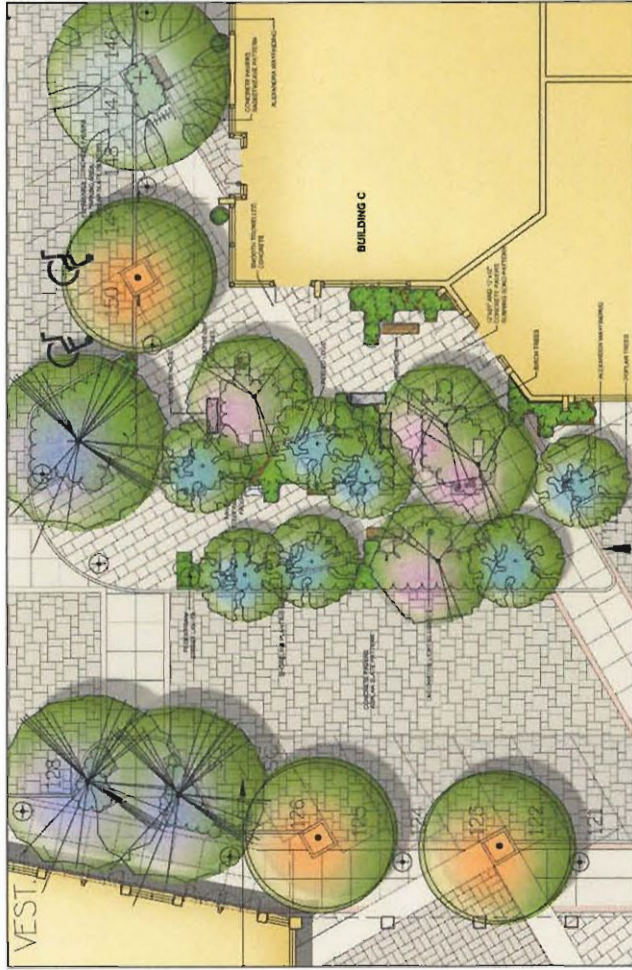
GRAVEL BENCHES EMBEDDED IN PLANTING AREA



BENCHES FOR SEATING



DRIVE RACK



A large central gathering area is created for pedestrians to socialize in a meeting place. Large tree groupings are introduced for shade. Seating areas including modern furniture or basalt benches allow for social interaction. Logs and boulders provide opportunities of play for children.



SITE LOCATION



LOGS FOR CLIMBING, BALANCING OR SITTING



CHILDREN'S PLAY AREA

Sheet No. 53
September 5, 2013
RZ 10-528877



'CENTRAL' AT GARDEN CITY
GARDEN CITY ROAD AND ALDERBRIDGE WAY
RICHMOND, BC

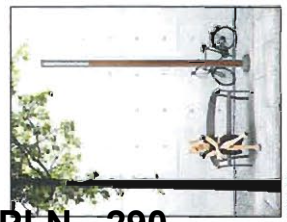
CENTRAL PLAZA

L3

DATE: 09/05/13
SCALE: 1:100
PROJECT SHEET NUMBER: 10/01/03/05



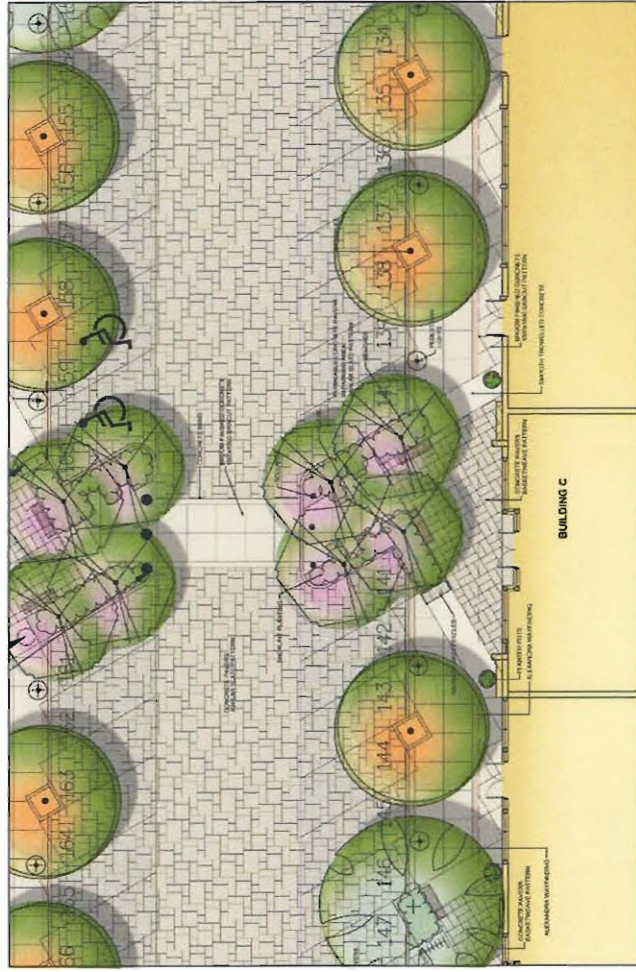
BOLLARD



LIGHTPOST

PLN - 290

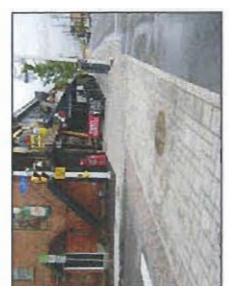
Sheet No. 54
September 5, 2013
RZ. 10-528877



Safe pedestrian crossings are located at "mid block" and intersections. The crossing are raised to establish priority for pedestrian circulation and sidewalks. Throughout the development, secondary plazas are generated to break up the long sidewalks offer nodes of greenery and seating areas. The incorporation of the Alexandra Way signature paving pattern and wayfinding signage along with the special paving in the vehicular plaza areas create an identifiable link through the site along Alexandra Way to the West Cambie area.



SITE LOCATOR



SPECIAL PAVING

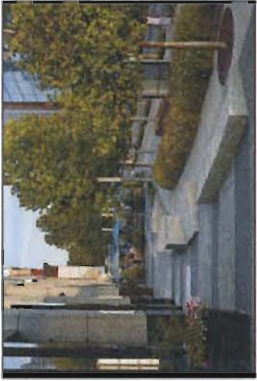


'CENTRAL' AT GARDEN CITY
GARDEN CITY ROAD AND ALDERBRIDGE WAY
RICHMOND, BC

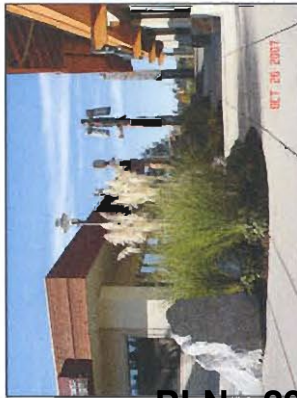
ALEXANDRA WAY
AND PEDESTRIAN CROSSINGS

L4

DATE: 09/05/13
SCALE: 1:100
PROJECT NUMBER: 101013-000



OUTDOOR AMENITY AREAS FOR SEATING AND SOCIALIZING



PLANTING AREAS TO SOFTEN OUTDOOR SPACES

Sheet No. 55
September 5, 2013
RZ 10-528877

PLN - 291



TRASH RECEPTACLE



PLANTER POTS



BIKE RACK



SITE LOCATOR

Secondary entrances are offered into the development from Alderbridge Way connecting the street edge into the site. The site is opened up in these locations with wide, shallow, generous steps or ramps that are used to ease entry into the Village. The site edges are layered with a tight spacing of street trees transitioning into an more natural planting of native trees, shrubs and ornamental grasses to soften the perspective from the street.

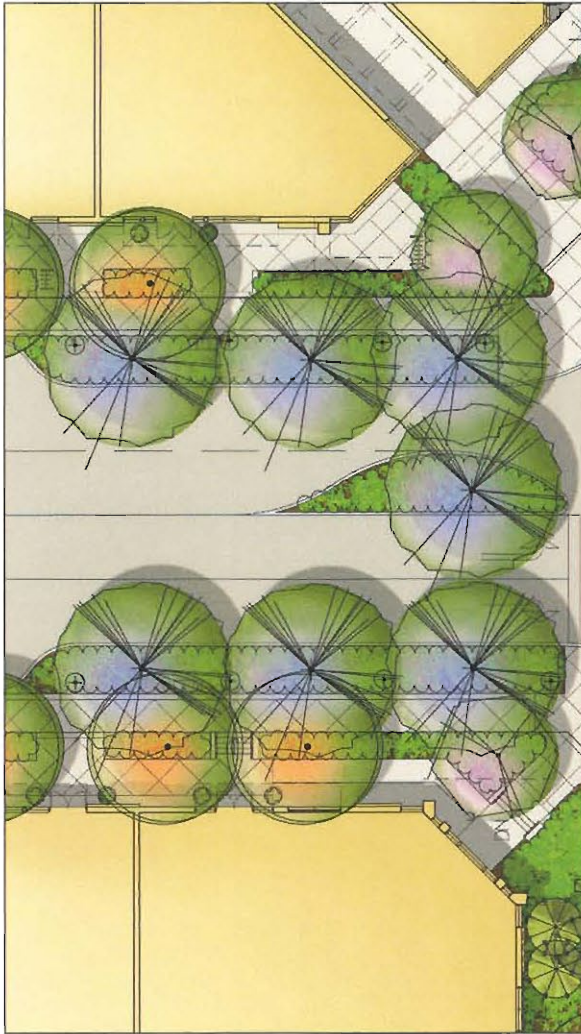


'CENTRAL' AT GARDEN CITY
GARDEN CITY ROAD AND ALDERBRIDGE WAY
RICHMOND, BC

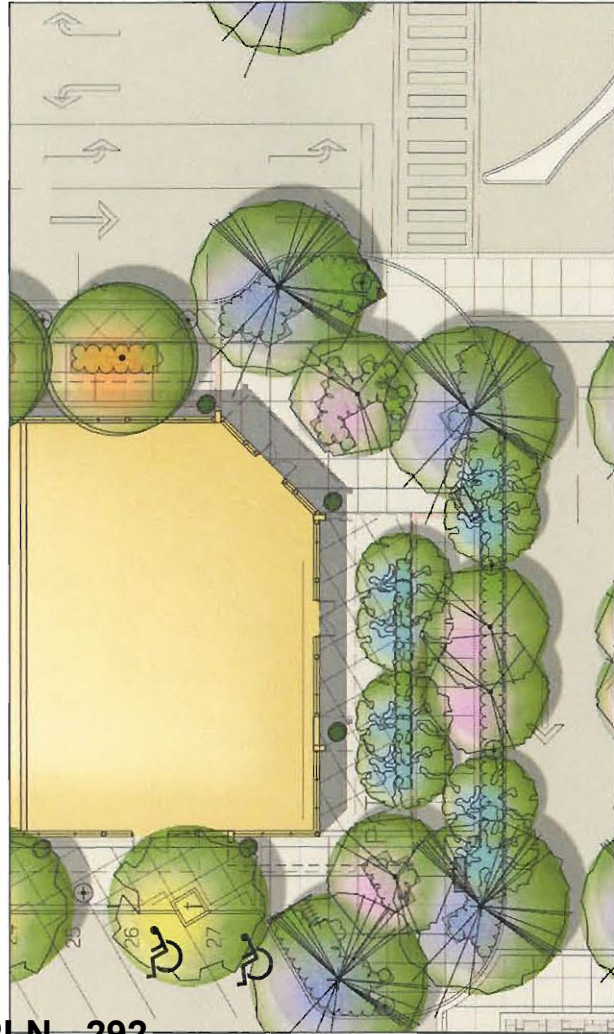
BUILDING L S.E. CORNER

L5

DATE: _____
SCALE: _____
PROJECT NUMBER: _____
DRAWN BY: _____
CHECKED BY: _____
DATE: _____



PLN - 292



TWO LAYERS OF OUTDOOR SPACES



PEDESTRIAN AREAS PULLED BACK FROM PARKING



PEDESTRIAN AREAS PULLED BACK FROM STREETS



OUTDOOR PATIO

High Street lined with large scale canopy trees to create a pedestrian friendly environment. Pedestrians are brought away from the street edge to allow for more intimate interaction with the CRU's allowing for seating and outdoor patio areas for coffee shops and restaurants.



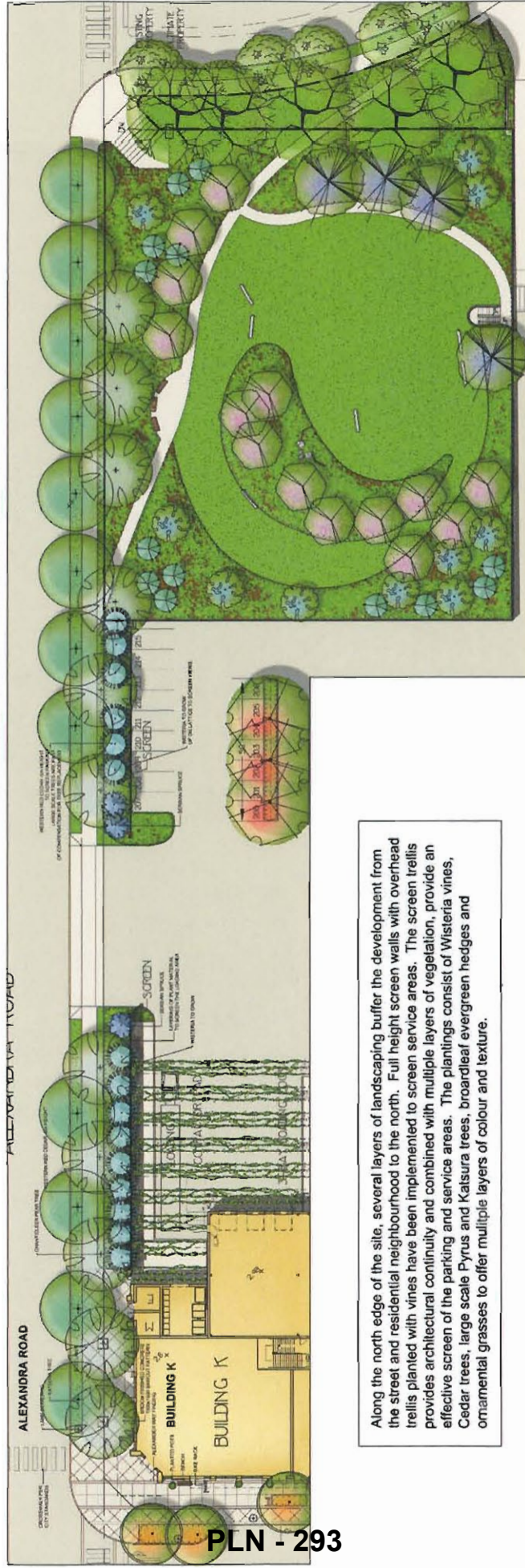
SITE LOCATION

Sheet No. 56
September 5, 2013
RZ 10-528877



LARGE SCALE TREES AND SHRUBS TO SCREEN LOADING AREA AND PROVIDE A GREEN STREET EDGE

WISTERIA VINES OVER TRELLIS STRUCTURE



Along the north edge of the site, several layers of landscaping buffer the development from the street and residential neighbourhood to the north. Full height screen walls with overhead trellis planted with vines have been implemented to screen service areas. The screen trellis provides architectural continuity and combined with multiple layers of vegetation, provide an effective screen of the parking and service areas. The plantings consist of Wisteria vines, Cedar trees, large scale Pynus and Katsura trees, broadleaf evergreen hedges and ornamental grasses to offer multiple layers of colour and texture.

A 36,360 sq. ft. public green space has been created on top of a portion of the eastern parking area to provide public recreation opportunities as well as a green buffer from the future residential neighbours to the north. This deck covers 103 parking spaces while offering a publicly accessible Green Park space. Currently, the design consists of pedestrian paths and seating areas to provide passive recreation use. The planting would be integrated with the native palette within the retail village. Final programming of the green deck space will be completed in conjunction with the City of Richmond Parks Dept.



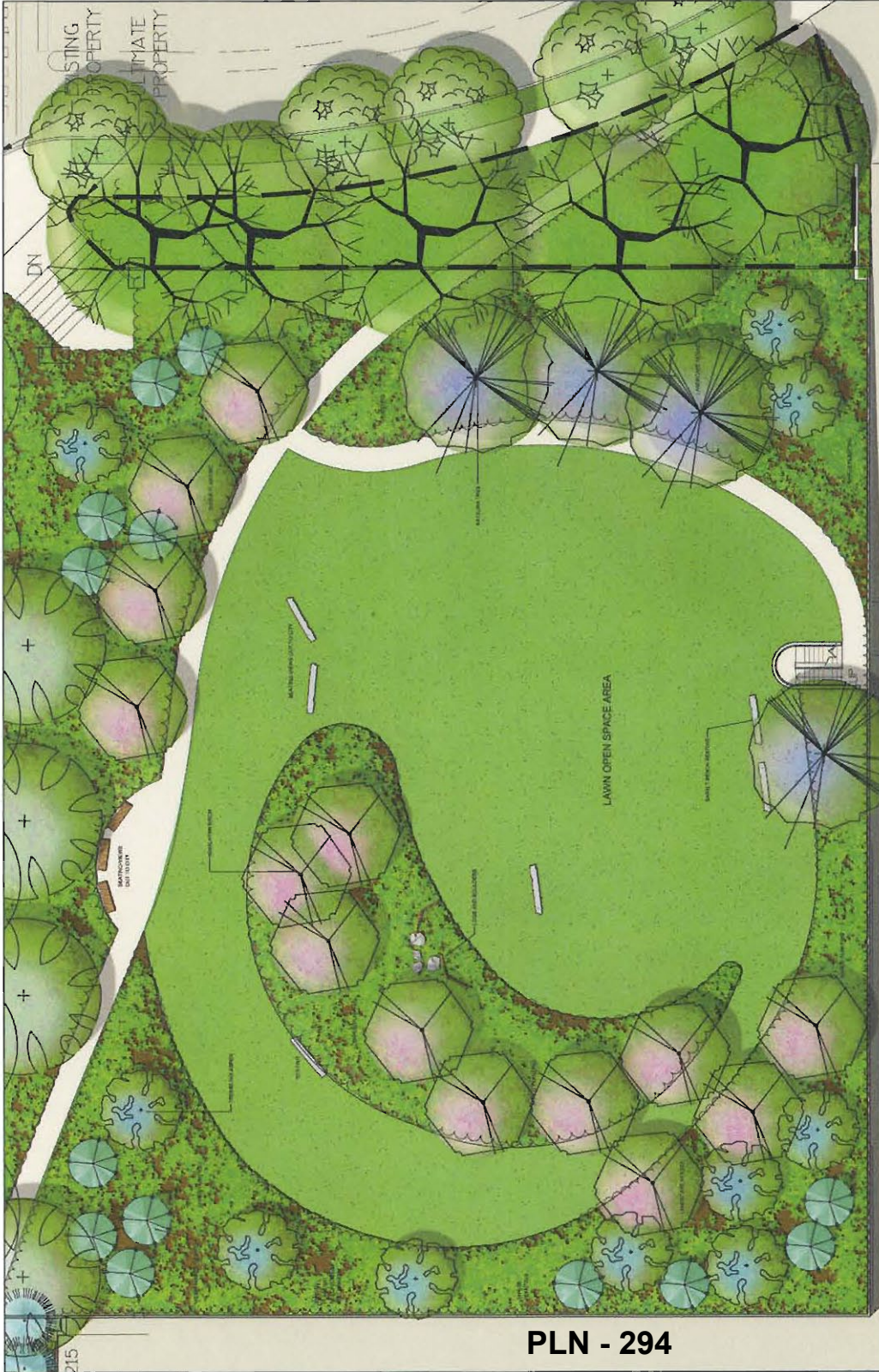
Sheet No. 57
September 5, 2013
RZ 10-528877

L7
DATE: 09/05/13
SCALE: 1:1000
PROJECT NUMBER: 10528877

NORTH EDGE BUFFERING

'CENTRAL' AT GARDEN CITY
GARDEN CITY ROAD AND ALDERBRIDGE WAY
RICHMOND, BC





PLN - 294



SHRUBS AND GROUNDCOVER



NATIVE TREES

Sheet No. 58
September 5, 2013
RZ 10-528877



GREEN ROOF DECK

A 36,360 sq. ft. public green space has been created on top of a portion of the eastern parking area to provide public recreation opportunities as well as a green buffer from the future residential neighbours to the north. This deck covers 103 parking spaces while offering a publicly accessible Green Park space. Currently, the design consists of pedestrian paths and seating areas to provide passive recreation use. The planting would be integrated with the native palette within the retail village. Final programming of the green deck space will be completed in conjunction with the City of Richmond Parks Dept.



SITE LOCATION

L8

DATE: 09/05/13
SCALE: 1:1000
DRAWN BY: J. HARRIS
CHECKED BY: J. HARRIS

GREEN DECK

'CENTRAL' AT GARDEN CITY
GARDEN CITY ROAD AND ALDERBRIDGE WAY
RICHMOND, BC



Sheet No. 59
 September 5, 2013
 RZ 10-528877



Surface parking areas have been minimized and is buffered from adjacent streets with a setback of several layers of planting consisting of an outer row of street trees, an inner row of deciduous and coniferous trees, planting beds of shrubs and ornamental grasses and perennials to provide seasonal interest. The parking areas are finished with bioswales to mitigate the stormwater prior to reaching the storm system. The plantings within the bioswale consists of canopy trees for moisture transpiration and shade to reduce heat island effect, shrubs and ornamental grasses to help clean and reduce the water charging into the storm system.



SITE LOCATOR



BIOSWALES TO MITIGATE STORMWATER



PARKING LOT ISLANDS



BOULEVARD PLANTS AT BUILDING FOUNDATION



BOULEVARD PLANTING TO SCREEN PARKING AREAS



'CENTRAL' AT GARDEN CITY
 GARDEN CITY ROAD AND ALDERBRIDGE WAY
 RICHMOND, BC

EAST PARKING

L9

DATE: 09/05/13
 SCALE: 1/8"=1'-0"
 PLAN PROJECT NUMBER: 10-528877



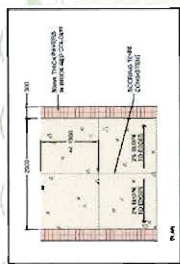
Sheet No. 60
September 5, 2013
RZ 10-528877



SITE LOCATOR

Planting:
A wide range of plantings are introduced to allow for biodiversity, personal interest and habitat opportunities.

Seating & Planters:
Plant areas have been provided to encourage pedestrian/bicyclist interaction and provide definition along the corridor.



ALEXANDRA WAYFINDING PAVING

PLN - 296
Alexandra Way pedestrian corridor has been integrated to provide continuity from the Alexandra neighborhood to City centre.

PLN - 296

L10
DATE: 10/10/13
SCALE: 1:1000
PROJECT NUMBER: 101010101

ALEXANDRA WAYFINDING

'CENTRAL' AT GARDEN CITY
GARDEN CITY ROAD AND ALDERBRIDGE WAY
RICHMOND, BC





**Richmond Zoning Bylaw 8500
Amendment Bylaw No. 8864 (RZ 10-528877)
4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080,
9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500
Alexandra Road**

The Council of the City of Richmond enacts as follows:

1. Richmond Zoning Bylaw 8500, as amended, is further amended by inserting Section 32.0 thereof the following:

“32.0 Neighbourhood Commercial (ZC32) – West Cambie Area

32.1 Purpose

The zone provides for a mix of commercial and related uses oriented to vehicular access.

32.2 Permitted Uses

- amusement centre
- animal grooming
- building or garden supply
- broadcasting studio
- child care
- education, commercial
- education, university
- entertainment, spectator
- government service
- greenhouse & plant nursery
- health service, minor
- manufacturing, custom indoor
- office
- parking, non-accessory
- recreation, indoor
- recycling depot
- restaurant
- retail, convenience
- retail, general
- retail, second hand
- service, business support
- service, financial
- service, household repair
- service, personal
- studio

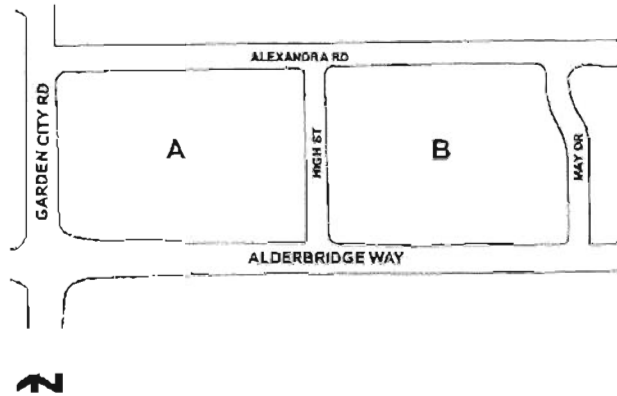
32.3 Secondary Uses

- amenity space, community

32.2 Permitted Uses

- veterinary service
- vehicle repair

Diagram 1



32.3 Secondary Uses

32.4 Permitted Density

1. The maximum **floor area ratio** is 2.0 FAR for the area identified as "A" in Diagram 1, Section 32.2.
2. The minimum **floor area ratio** is 0.60 for the area identified as "A" in Diagram 1, Section 32.2.
3. The maximum **floor area ratio** is 1.0 FAR for the area identified as "B" in Diagram 1, Section 32.2.

32.5 Permitted Lot Coverage

1. The maximum **lot coverage** is 60% for **buildings**.

32.6 Yards & Setbacks

1. The minimum **setbacks** to a public road shall be:
 - a. 2.0 m for Alderbridge Way;
 - b. 3.0 m for Garden City Road;
 - c. 1.0 m for Alexandra Road;
 - d. 5.0 m for May Drive; and
 - e. 3.0 m for High Street.

32.7 Permitted Heights

1. The maximum **height** for all **buildings** is 22.0 m.
2. The maximum **height** for **accessory structures** is 12.0 m.

32.8 Subdivision Provisions/Minimum Lot Size

- 1. The minimum **lot area** is 2 ha (4.94 ac.).

32.9 Landscaping & Screening

- 1. **Landscaping and screening** shall be provided according to the provisions of Section 6.0.

32.10 On-Site Parking and Loading

- 1. On-site **vehicle** loading and bicycle parking and loading shall be provided according to the standards set out in Section 7.0, except that:
 - a. On-site **vehicle** parking shall be provided at a minimum rate of 3.0 parking stalls per 100 m² of **gross leasable floor area** of a building in the areas identified as "A" and "B" separately, in Diagram 1, Section 32.2.

32.11 Other Regulations

- 1. The maximum **gross leasable floor area** for each individual **business** shall not exceed:
 - a. 9,900 m² for the area identified as "A" in Diagram 1, Section 32.2; and
 - b. 15,100 m² for the area identified as "B" in Diagram 1, Section 32.2.
 - 2. **Telecommunication antenna** must be located a minimum of 20.0 m above the ground (i.e. on a roof of a building).
 - 3. The overnight parking of **recreational vehicles** is prohibited.
 - 4. In addition to the regulations listed above, the General Development Regulations in Section 4.0 and the Specific Use Regulations in Section 5.0 apply.
2. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation and by designating it "**Neighbourhood Commercial (ZC32) - West Cambie Area**":
- That area shown as Area 'A' on "Schedule A attached to and forming Part of Bylaw 8864"
3. The Zoning Map of the City of Richmond, which accompanies and forms part of Richmond Zoning Bylaw 8500, is amended by repealing the existing zoning designation and by designating it "**School & Institutional (SI)**":
- That area shown as Area 'B' on "Schedule A attached to and forming Part of Bylaw 8864"

4. This Bylaw is cited as "Richmond Zoning Bylaw 8500 Amendment Bylaw 8864".

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

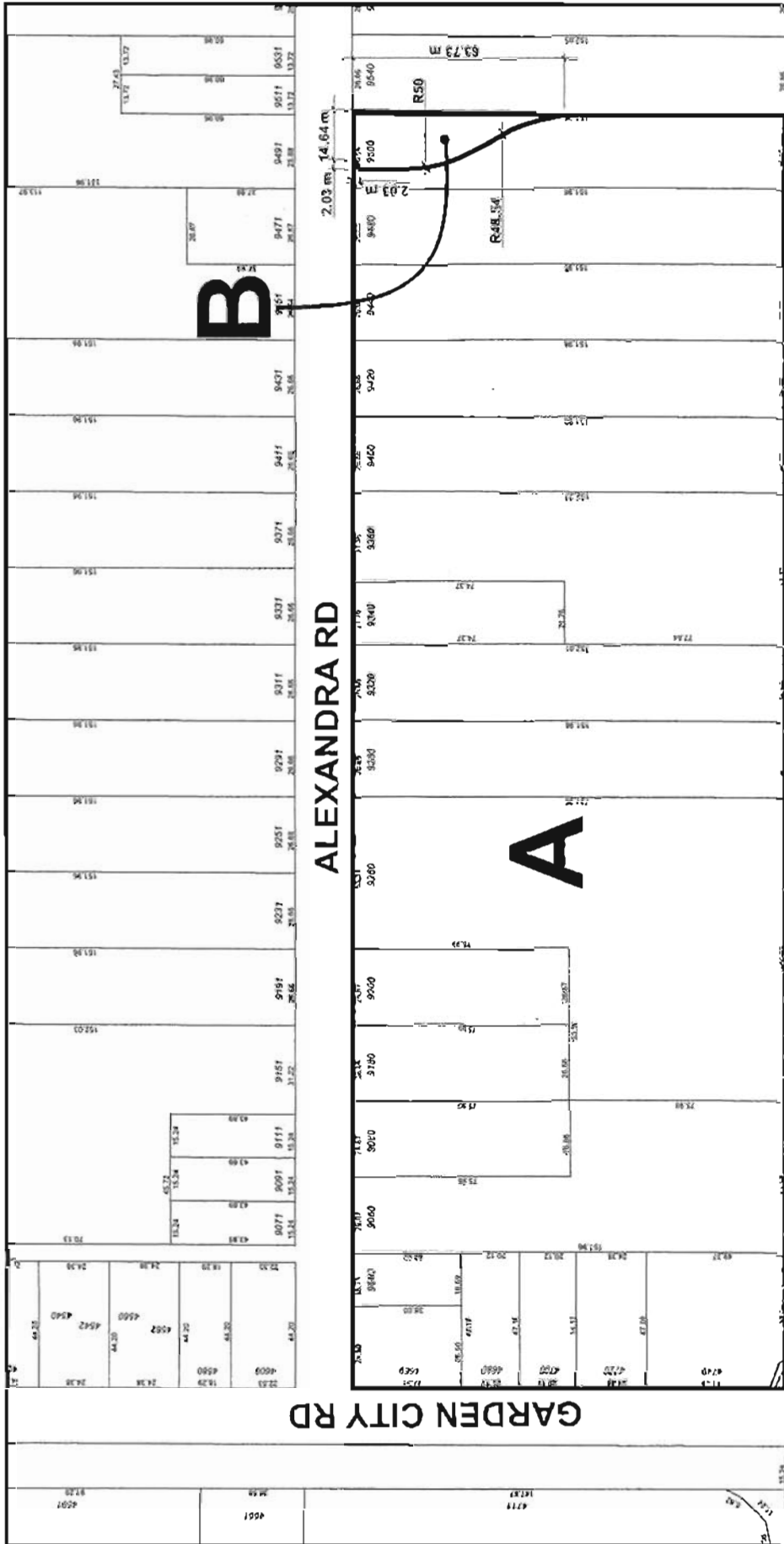
OTHER REQUIREMENTS SATISFIED

ADOPTED

MAYOR

CORPORATE OFFICER

CITY OF RICHMOND
APPROVED for content by originating dept. <i>ml</i>
APPROVED for legality by Solicitor <i>ml</i>



LEGEND

Area A to be rezoned to ZC32
 Area B to be rezoned to SI

Original Date: 11/27/12
 Revision Date: 12/05/12
 Note: Dimensions are in METRES

RZ 10-528877





Richmond Official Community Plan Bylaw 7100
Amendment Bylaw 8865 (RZ 10-528877)

4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080,
9180, 9200, 9260, 9280, 9320, 9340, 9360, 9400, 9420, 9440, 9480, 9500
Alexandra Road

The Council of the City of Richmond enacts as follows:

1. That Richmond Official Community Plan Bylaw 7100 is amended by repealing the area bounded by Alderbridge Road, Garden City Road, Alexandra Road and the proposed May Drive Extension on the existing Alexandra Neighbourhood Land Use Map in the Richmond Official Plan Bylaw 7100, Schedule 2.11A - West Cambie Area Plan and replacing it with the attached Schedule A to Amendment Bylaw 8865, in order to:
 - a) reduce the minimum density permitted from 1.25 to 0.60 FAR in the Mixed Use Area A on 4660, 4680, 4700, 4720, 4740 Garden City Road and 9040, 9060, 9080, 9180, 9200, 9260, 9280, 9320 Alexandra Road;
 - b) adjust the alignment of May Drive within the development lands over portions of 9440, 9480 and 9500 Alexandra Road; and
 - c) reduce the "Park" designation over portions of 9440, 9480 and 9500 Alexandra Road, be introduced and given first reading.
2. This Bylaw is cited as "Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 8865".

FIRST READING

A PUBLIC HEARING WAS HELD ON

SECOND READING

THIRD READING

OTHER REQUIREMENTS SATISFIED

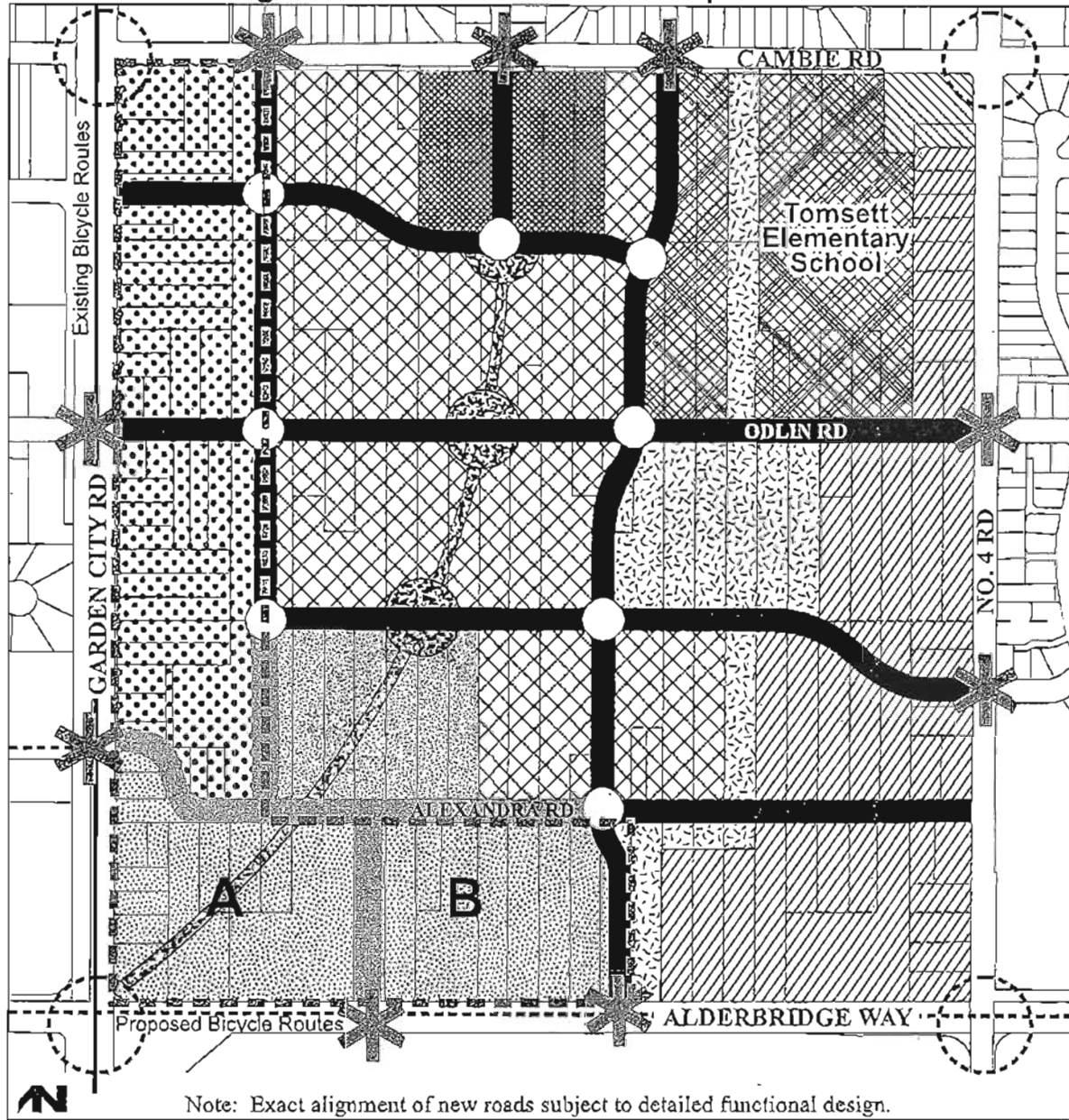
ADOPTED

CITY OF RICHMOND
APPROVED for consent by originating dept.
APPROVED for legality by Solicitor

MAYOR

CORPORATE OFFICER

Alexandra Neighbourhood Land Use Map



Note: Exact alignment of new roads subject to detailed functional design.

Area of No Housing Affected by Aircraft Noise	Residential Area 2 0.65 base FAR (Max. 0.75 FAR with density bonusing for affordable housing), 2 & 3-storey Townhouses.	Park: North Park Way, Central Park, South Park Way
Business/Office - office over retail FAR up to 1.25	Mixed Use: Hotel, office and streetfront retail commercial.	Alexandra Way (Public Rights of Passage Right-of-way)
Convenience Commercial	Area A: Min. 0.60 FAR up to 2.0	Proposed Roadways
Residential Area 1 1.50 base FAR (Max. 1.70 FAR with density bonusing for affordable housing). Townhouse, low-rise Apts. (4-storey typical)	Area B: Large and small floor plate up to 1.0 FAR	High Street
Residential Area 1A 1.50 base FAR (Max. 1.75 FAR with density bonusing for affordable housing). Townhouse, Low-rise Apts. (6-storey maximum),	Mixed Use: - abutting the High Street, medium density residential over retail; - not abutting the High Street, medium density residential.	New Traffic Signals
Community Institutional	1.25 base FAR. Building heights low to mid-rise. (Max. 1.50 FAR with density bonusing for affordable housing).	Feature Intersections - details to be developed
		Feature Landmarks In Combination with Traffic Calming Measures



Richmond Official Community Plan Bylaw 9000
Amendment Bylaw 8973 (10-528877)
9440, 9480 and 9500 Alexandra Road

The Council of the City of Richmond, in open meeting assembled, enacts as follows:

- 1. Richmond Official Community Plan Bylaw 9000 is amended by repealing the existing "Environmentally Sensitive Area (ESA)" designation in Attachment 2 to Schedule 1 from 9440, 9480 and 9500 Alexandra Road with the following legal addresses:

P.I.D. 012-032-581
West Half Lot 8 Block "C" Section 34 Block 5 North Range 6 West New Westminster
District Plan 1224

P.I.D. 001-084-372
East Half of Lot 8 Block "C" Section 34 Block 5 North Range 6 West New Westminster
District Plan 1224

P.I.D. 008-130-990
West Half Lot 9 Block "C" Section 34 Block 5 North Range 6 West New Westminster
District Plan 1224

- 2. This Bylaw may be cited as "Richmond Official Community Plan Bylaw 9000, Amendment Bylaw 8973".

FIRST READING

PUBLIC HEARING

SECOND READING

THIRD READING

OTHER REQUIREMENTS SATISFIED

ADOPTED

MAYOR

CORPORATE OFFICER

CITY OF RICHMOND
APPROVED by [Signature]
APPROVED by Manager or Solicitor [Signature]