



NOTE: ATTACHMENT 1 TO THIS REPORT IS AVAILABLE EITHER ON THE WEB THROUGH THE JANUARY 16, 2001 PLANNING COMMITTEE AGENDA, OR THE CITY CLERK'S OFFICE

CITY OF RICHMOND

REPORT TO COMMITTEE

TO: Planning Committee
FROM: Terry Crowe
Manager, Policy Planning Department
RE: Update to Area Plans

To Planning Committee - Jan 16, 2001
DATE: December 19, 2000
FILE: - 4045-01(xr)
8060-20-7190

STAFF RECOMMENDATION

- 1. That Bylaw No. 7190, which amends Official Community Plan, by substituting the following Area Plans for the existing Area Plans:
Thompson (Dover Crossing Sub-Area) as Schedule 2.2A;
Thompson (Terra Nova Sub-Area) as Schedule 2.2B;
Blundell (Laurelwood Sub-Area) as Schedule 2.5A;
Blundell (East Livingstone Sub-Area) as Schedule 2.5B;
Broadmoor (Ash Street Sub-Area) as Schedule 2.6A;
Broadmoor (Central West Broadmoor Sub-Area) as Schedule 2.6B;
Broadmoor (Sunnymede North Sub-Area) as Schedule 2.6C; and
Shellmont (Ironwood Sub-Area) as Schedule 2.8A,
be introduced and given first reading.
2. That Bylaw No. 7190, having been examined in conjunction with the Capital Expenditure Program, the Waste Management Plan, the Economic Strategy Plan, and the 5 Year Financial Plan, is hereby deemed to be consistent with said program and plans, in accordance with Section 882(3) of the Local Government Act.
3. That Bylaw No. 7190, having been examined in accordance with the City Policy on referral of Official Community Plan Amendments, is hereby deemed to have no effect upon an adjoining Municipality nor function or area of the Greater Vancouver Regional District, in accordance with Section 882(3)(d) and (e) of the Local Government Act.
4. That Bylaw No. 7190 be referred to the Vancouver International Airport Authority in accordance with the Richmond YVR Accord.
5. That Bylaw No. 7190 be referred to the Agricultural Land Commission in accordance with Section 882(1)(c) of the Local Government Act.
6. That Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 7113 which replaced Schedule 2.8A (Shellmont-Ironwood Sub-Area Plan) be abandoned.

Terry Crowe
Manager, Policy Planning Department

Att.

FOR ORIGINATING DIVISION USE ONLY
CONCURRENCE OF GENERAL MANAGER
[Signature]

STAFF REPORT

ORIGIN

This report presents eight updated Sub-Area Plans for approval by Council. This work was initiated following the adoption of the new Richmond Official Community Plan (OCP). The new plans are contained in the blue binder labelled "Schedule A to Bylaw 7190". For reference purposes, Attachment 1 to this report (black binder) contains the original Area Plans with margin notes indicating the main changes that are being proposed.

FINDINGS OF FACT

Format Changes

The following format changes are made to be consistent with the new OCP:

- a portrait rather than a landscape orientation;
- revised Table of Contents;
- revised headings, page layout, headers and footers; and
- re-organizing the Development Permit guidelines to follow a consistent format.

Minor Content Changes

Changes to the content of the plans were made to:

- correct grammatical errors;
- remove out of date information;
- remove implementation sections; and
- eliminate Acknowledgement sections.

Additionally, information was added to Area Plans with regard to the Agricultural Land Reserve and the Airport. Specifically, the Ash Street and Terra Nova Sub-Area Plans were amended by adding policies and guidelines to address adjacency issues with regard to the ALR. The Dover and Terra Nova Sub-Area Plans were amended by adding policies regarding the airport.

Proposed Bylaw 7113 to be Abandoned and Replaced

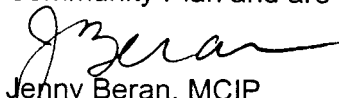
As part of an earlier rezoning application, Bylaw 7113, now between 3rd and 4th readings, proposes to add development guidelines to the Ironwood Sub-Area Plan for the north side of Steveston Hwy across from Ironwood. The adoption of proposed Bylaw 7113 has been held up with the final reading requirements for the rezoning application. In the meantime, another re-formatted version of the Ironwood Plan is presented here as part of Bylaw 7190. Therefore, in order to adopt the updated version of the Ironwood Plan as part of Bylaw 7190, the earlier proposed bylaw needs to be abandoned. This new proposed bylaw will ensure that the Ironwood Plan is presented in a consistent format with the other Plans and will not affect the proposed rezoning.

FINANCIAL IMPACT

None.

CONCLUSION

Eight Sub-Area Plans have been amended in order to make them consistent with the Official Community Plan and are presented to Council for adoption.



Jenny Beran, MCIP
Planner

JMB:jmb

CITY OF RICHMOND
BYLAW 7190
RICHMOND OFFICIAL COMMUNITY PLAN BYLAW 7100
AMENDMENT BYLAW 7190

The Council of the City of Richmond, enacts as follows:

1. Richmond Official Community Plan Bylaw 7100 is amended by:

a) Deleting:

- Schedule 2 to Bylaw 7100 Table of Contents
- Schedule 2.2A (Thompson (Dover Crossing Sub-Area));
- Schedule 2.2B (Thompson (Terra Nova Sub-Area));
- Schedule 2.5A (Blundell (Laurelwood Sub-Area));
- Schedule 2.5B (Blundell (East Livingstone Sub-Area));
- Schedule 2.6A (Broadmoor (Ash Street Sub-Area));
- Schedule 2.6B (Broadmoor (Central West Broadmoor Sub-Area));
- Schedule 2.6C (Broadmoor (Sunnymede North Development Permit Guidelines)); and
- Schedule 2.8A (Shellmont (Ironwood Development Permit Guidelines));

and

b) Adding a new:

- Schedule 2 to Bylaw 7100 Table of Contents
- Schedule 2.2A (Thompson (Dover Crossing Sub-Area));
- Schedule 2.2B (Thompson (Terra Nova Sub-Area));
- Schedule 2.5A (Blundell (Laurelwood Sub-Area));
- Schedule 2.5B (Blundell (East Livingstone Sub-Area));
- Schedule 2.6A (Broadmoor (Ash Street Sub-Area));
- Schedule 2.6B (Broadmoor (Central West Broadmoor Sub-Area));
- Schedule 2.6C (Broadmoor (Sunnymede North Sub-Area)); and
- Schedule 2.8A (Shellmont (Ironwood Sub-Area));

which are attached as Schedule A to this Bylaw.

2. This Bylaw may be cited as **“Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 7190”**.

FIRST READING

PUBLIC HEARING

SECOND READING

THIRD READING

ADOPTED

	CITY OF RICHMOND
	APPROVED for content by originating dept.
	APPROVED for legality by Solicitor

MAYOR

CITY CLERK

Richmond Official Community Plan

Schedule 2 Area Plans

2.2A	Thompson (Dover Crossing Sub-Area)
2.2B	Thompson (Terra Nova Sub-Area)
2.4	<i>Steveston</i>
2.5A	Blundell (Laurelwood Sub-Area)
2.5B	Blundell (East Livingstone Sub-Area)
2.6A	Broadmoor (Ash Street Sub-Area)
2.6B	Broadmoor (Central West Sub-Area)
2.6C	Broadmoor (Sunnymede North Sub-Area)
2.8A	Shellmont (Ironwood Sub-Area Plan)
2.10	<i>City Centre</i>
2.10A	<i>City Centre (St. Albans Sub-Area)</i>
2.10B	<i>City Centre (Acheson Bennett Sub-Area)</i>
2.10C	<i>City Centre (McLennan North Sub-Area)</i>
2.10D	<i>City Centre (McLennan South Sub-Area)</i>
2.11A	<i>West Cambie</i>
2.11B	<i>East Cambie</i>
2.12	<i>Bridgeport</i>
2.13A	<i>East Richmond (McLennan Sub-Area)</i>
2.14	<i>Hamilton</i>

Note: The Area Plans shown in italics remain in its original sideways binder and will be included in this binder at a later date.

Richmond Official Community Plan



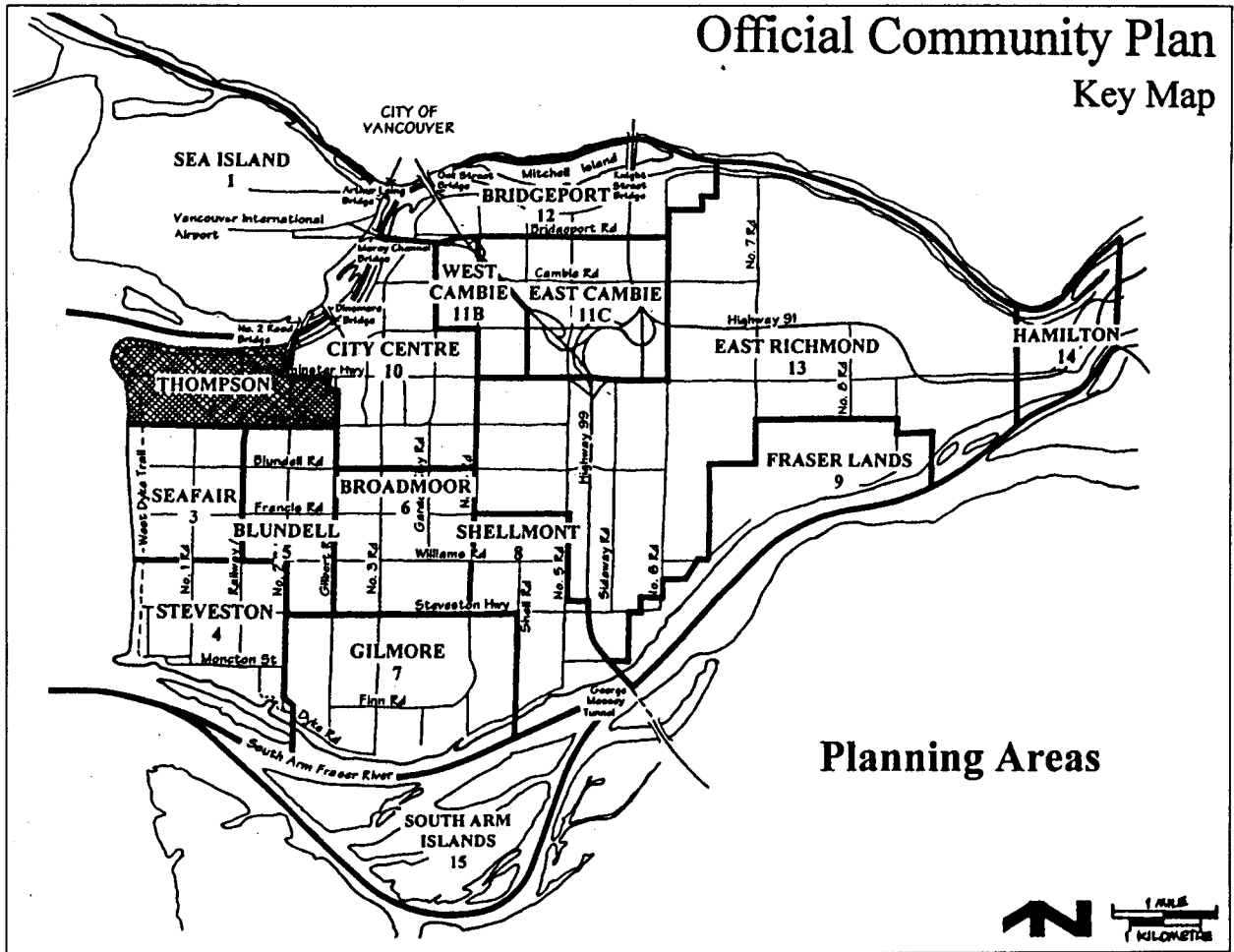
City of Richmond

THOMPSON AREA DOVER CROSSING SUB-AREA PLAN

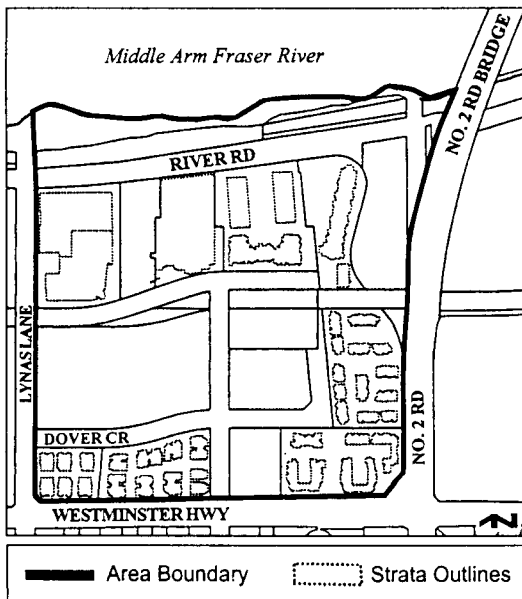
Bylaw 7100 Schedule 2.2A



KEY MAP



PLAN AREA MAP



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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City's social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

The Dover Crossing Neighbourhood is located within the Thompson Planning Area and is bounded by River Road, No. 2 Road, Westminster Highway and Lynas Lane. (See Plan Area Map). The subject neighbourhood is surrounded by the Middle Arm of the Fraser River and the Vancouver International Airport to the north, undeveloped land, a recreation vehicle park and industrial warehousing to the east, single-family residential to the south, and the City Works Yard to the west.

The subject neighbourhood occupies about 14.5 ha (36 ac.). Existing land use at the time of plan preparation (1990) consisted of 11 homes on large lots, the Vancouver-Austrian Club, a C.P.R. right-of-way and vacant undeveloped lands. The total resident population of the neighbourhood was approximately 33 people.

The new bridge crossing from the north end of No. 2 Road to Sea Island provides a direct link from Richmond to the Vancouver International Airport. This link makes the Dover Crossing Neighbourhood one of the major visual gateways into West Richmond.

The Middle Arm of the Fraser River is the most prominent natural feature of the Dover Crossing Neighbourhood. The river serves as an important marine habitat. The dyke and foreshore provide a valuable recreation, trail and natural amenity that has a strong influence on land use in the Dover Crossing Neighbourhood.

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

This plan sets out the goals, objectives, policies and development guidelines for the Dover Crossing Sub-Area, a part of the Thompson Planning Area.

1.2 VISION

The population of Richmond is expected to grow to 212,000 people by the year 2021. In order to preserve farmland and stabilize single-family neighbourhoods, the majority of this growth needs to be housed in and around the City Centre Planning Area.

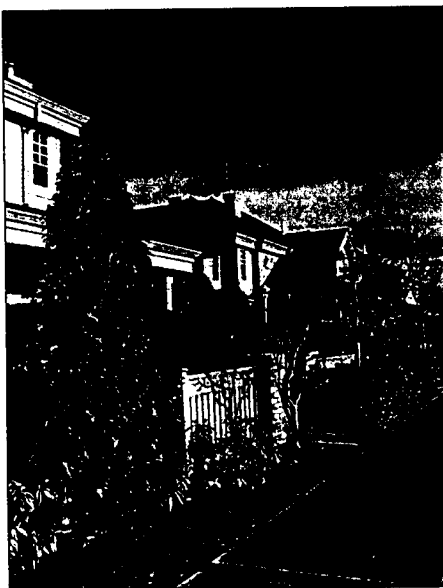
Although Richmond's history is linked to the Fraser River, its City Centre is not located to take advantage of the waterfront. As Richmond grows, there is an opportunity to create an exciting urban waterfront that is well connected to the city core in terms of open space and transportation linkages. This may be achieved by eventually expanding the higher density residential and commercial developments of the City Centre Planning Area towards the Middle Arm of the Fraser River.

The Dover Crossing Neighbourhood is located directly adjacent to the most westerly part of the City Centre area. The size of the Dover Crossing Neighbourhood 14.5 ha (36 ac.) affords an opportunity to create a liveable, well co-ordinated mixed housing community.

The Dover Crossing Neighbourhood will be transformed from an area with a high proportion of vacant land and large single-family lots to a residential neighbourhood with a variety of multiple-family (townhouses and apartments) housing types.

The general development concept will consist of the following characteristics:

- There will be a gradual transition in housing form from north to south in order to minimize the impacts on the existing single-family neighbourhoods to the south of the sub-area.
- Taller buildings will be located adjacent to the river and the No. 2 Road Bridge which will create a visual gateway into West Richmond and permit more open space in the neighbourhood.
- Townhouses will be primarily developed adjacent to Westminster Highway but will also be integrated with the apartments to the north to help create a high quality streetscape.



Linear Walkways Connect Housing to the Waterfront

The Dover Crossing Sub-Area will be well connected to the riverfront. Linear parkways accessible by pedestrians, wheelchairs and cyclists will extend from Westminster Highway through the neighbourhood to River Road. A pedestrian crossing will provide a safe and convenient connection from the neighbourhood over River Road to the dyke trail.

The importance of preserving views to the riverfront and mountains will be recognized. Views of the river and mountains will be preserved through the linear parkways referred to above and by terracing of apartment buildings.

A neighbourhood park and school site will be centrally located in the neighbourhood. Community services, such as childcare, will be encouraged and facilitated as part of development approvals.

Specialized landscaping, fountains and public art will help create special places for people to meet in the sub-area.

Developments in the area will maintain, enhance and preserve air, water and soil quality.

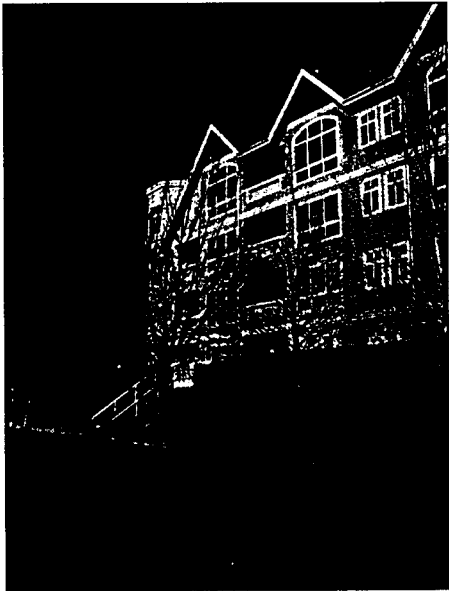
1.3 GOALS

1. To permit residential development that:
 - a) Recognizes the area's close proximity to the No. 2 Road Bridge, the Middle Arm of the Fraser River and the City Centre;
 - b) Preserves views of the waterfront and the mountains;
 - c) Provides public access to the waterfront;
 - d) Includes community services and open space that enhances liveability;
 - e) Maintains, enhances and preserves air, water, and soil quality.
2. To encourage and facilitate features which create a "heart and soul" for the neighbourhood, such as land-bridges, special landscaping, fountains and public art.
3. To provide roads, walkways, and bicycle paths that help to connect the neighbourhood with the riverfront and the surrounding areas in a convenient and safe manner.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING



New Apartment Units

ISSUE:

Housing Forms

This plan prescribes a mix of townhouses and apartments. The densities of the development will range from .55 (F.A.R.)¹ to a maximum of 1.6 (F.A.R.). Thus, the overall density will be in the medium range.

As shown on the Land Use Map, townhouses are located along the southern edge of the neighbourhood to provide a gradual transition from the low density single-family area along the south side of Westminster Highway. The apartments are deliberately located in the north and north-east portions of the neighbourhood in order to create a visual gateway into Richmond. The taller buildings allow for more green space, and in conjunction with the linear walkways enhances both private and public views and access to the waterfront.

OBJECTIVE 1:

To provide a range of housing types for a variety of households and age groups.

POLICIES:

- a) Allow development such that a mixture of housing types are provided as per the Land Use Map;
- b) Require that all buildings shall not exceed 24.38 m (80 ft.) in height;
- c) Encourage and facilitate the provision of affordable housing.

¹ F.A.R. (Floor Area Ratio: means the figure obtained when the total area of the floors of the building on a lot, measured to the outer limits of the building or buildings, is divided by the area of the lot.)

ISSUE:

Views and Vistas

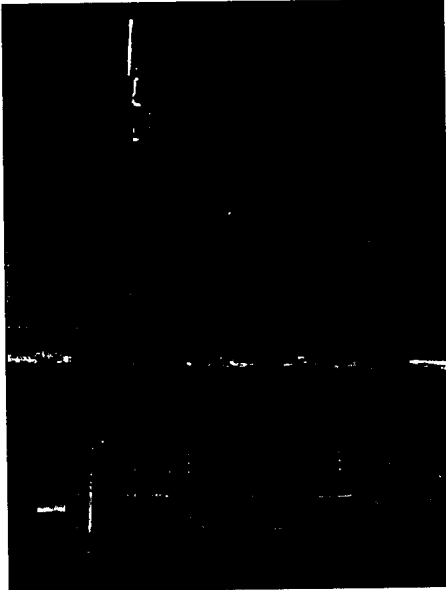
Views of the airport and the mountains are unobstructed from the neighbourhood. The physical height of the dyke restricts views to the north for low level residences.

OBJECTIVE 2:

To utilize opportunities for views of the Fraser River and mountains.

POLICIES:

- a) Encourage building designs which maximize views of the river and mountains;
- b) Encourage designs that minimize building shadowing of public and private open spaces and walkways.



Views to the River

ISSUE:

Airport

The neighbourhood's close proximity to the Vancouver International Airport has both advantages and disadvantages. The views of seaplanes and other aircraft landing and descending adjacent to the neighbourhood adds to the interest of the area. However, the Dover Crossing Neighbourhood is subject to a certain amount of noise and disruptions created by the float planes, other air craft and ancillary uses to the airport and therefore the planning area, or a part thereof, is subject to a noise level of 30 NEF or above.²

In order to protect flight operations, Transport Canada imposes building height restrictions in the vicinity of the Airport. Specific height restrictions are contained in plans filed by Transport Canada in the Provincial Land Titles Office.

OBJECTIVE 3:

To ensure that the heights and designs of all new buildings are in accordance with the Federal Airport Registered Zoning Regulations relative to the Vancouver International Airport.

² N.E.F. (Noise Exposure Forecast): N.E.F. is the summation of all noise that takes place in a 24 hour period based on the perceived level of noise. It considers some tonal qualities of sound and is intended to rate the "noisiness" or annoyance level of a sound rather than its loudness.

POLICIES:

- a) Encourage builders of new residential buildings to comply with Canada Mortgage and Housing Corporation's guidelines for noise mitigation;
- b) Through rezoning and subdivision applications, require restrictive covenants to hire professionals qualified in acoustics to determine and implement noise mitigation measures.

ISSUE:

No. 2 Road Bridge

In March 1990, Richmond City Council approved a new bridge crossing from the north end of No. 2 Road to Sea Island, providing a direct link to the Vancouver International Airport. Taller buildings are situated in the north-east corner of the neighbourhood to provide a visual gateway into Richmond. Buildings are set back from the northern foot of No. 2 Road as a noise mitigation measure as well as to reserve open space in case bridge loop ramps need to be developed from the bridge to River Road at some future date.

POLICIES:

- a) Ensure that all residential buildings have appropriate setbacks from No. 2 Road Bridge to mitigate against vehicular traffic noise and for privacy of residential uses;
- b) Require that sufficient land is retained as open space at the north-east portion of the neighbourhood which can be used to develop bridge loop ramps if and when the need occurs.

4.0 TRANSPORTATION

ISSUE:

Westminster Highway, No. 2 Road and River Road are important to the City network as major and local arterial routes. Access to these roads from properties in the neighbourhood should be restricted to maintain a free flow of traffic.

Lynas Lane and internal neighbourhood roads will be upgraded to full City standards as development occurs. The local road system provides access to the entire neighbourhood from Lynas Lane. A signalized intersection will be required at the corner of Lynas Lane and Westminster Highway and at Lynas Lane and River Road.

The dyke trail system will accommodate pedestrian traffic on the north side of River Road. Thus, a sidewalk will only be required on the south side of River Road.

Sanitary sewers, storm sewers, underground wiring, sidewalks, curbs and gutters will need to be provided in the neighbourhood with costs apportioned to all properties in the sub-area.

OBJECTIVE 1:

To improve the quality of the existing public utilities and roads concurrent with new residential development.

POLICIES:

- a) Close Dover Road for linear park purposes as shown on the Land Use Map;
- b) Deny direct access to the sub-area from No. 2 Road;
- c) Restrict direct access to the sub-area from Westminster Highway and River Road;
- d) Protect River Road as a local arterial road;
- e) Accommodate pedestrians and cyclists on all roads with adequate sidewalks, and bicycle lanes.

5.0 NATURAL & HUMAN ENVIRONMENT

ISSUE:

Fraser River

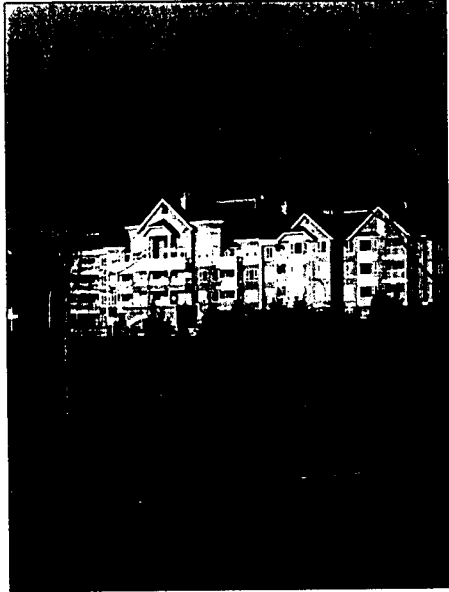
The most distinctive feature of the sub-area is its relationship to the Fraser River. The dyke and foreshore of the Fraser River serve as both a recreational and natural amenity that must be protected and enhanced. The dyke and trail system is an important part of Richmond's Open Space System.

OBJECTIVE 1:

To acknowledge the Fraser River as a resource for many users while preserving and protecting the foreshore.

POLICIES:

- a) Support policies and actions directed towards improving water quality and preserving marsh areas, and fish and wildlife habitats in and along the Middle Arm of the Fraser River;
- b) Retain the dyke adjacent to the Dover Crossing Neighbourhood for open space purposes.



Open Space

ISSUE:

Open Space and Parks

Over 9.14 ha (10 ac.) of land is devoted to open space in this plan. The open space consists of natural areas, landscaping, a 2.43 ha (6 ac.) neighbourhood park and three linear parkways extending from Westminster Highway through the neighbourhood. As shown on the Land Use Map, the three linear parkways totalling 1.62 ha (4 ac.) with a pedestrian crossing provide connections from the surrounding residential areas through the Dover Crossing Neighbourhood over River Road to the dyke trail and Fraser River. A second pedestrian crossing extending over No. 2 Road may eventually be required to further connect the lands to the east with the Dover Crossing Neighbourhood.

The open spaces created by the linear parks and the pedestrian crossing improve both the physical and visual connections between the site, the surrounding areas and the dyke trail.

OBJECTIVE 1:

To ensure that sufficient public and private park and open space is provided for recreational uses, pedestrian and bicycle movement and to promote better air quality.

POLICIES:

- a) Designate no less than 2.43 ha (6 ac.) of land for an active neighbourhood park as per the Land Use Map;
- b) Require that three linear parkways extending from Westminster Highway to the waterfront are provided for public access as part of the development site as per the Land Use Map³;
- c) Ensure that a crossing accessible to pedestrians, cyclists and wheelchairs is built as part of the development across River Road to connect the Dover Crossing Neighbourhood to the riverfront, and investigate the possibility of developing a second crossing across No. 2 Road to connect the lands to the east with the Dover Crossing Neighbourhood;
- d) Require that the Dover Crossing Neighbourhood is accessible by cyclists and that bicycle racks are provided within the public areas;
- e) Encourage the preservation of mature trees in the neighbourhood;

³ Where the linear parkways cross private lands, the linear parkways will be designated for the same land use as the contiguous private properties.

- f) Utilize the C.P.R. right-of-way as shown on the Land Use Map, for park purposes if it is vacated by C.P.R. and is not required by the City for transportation purposes.

6.0 COMMUNITY FACILITIES & SERVICES

ISSUE:

Nearly all community service and facility needs of the residents are met outside of this sub-area. The Thompson Community Centre is located approximately 0.8 km (0.5 mi.) away from the neighbourhood.

Additional residential development will increase pressure on childcare services and schools in the Thompson Area. Childcare facilities in the area should therefore be encouraged. The Richmond School Board has indicated that it is interested in acquiring an elementary school site in the sub-area as shown on the Land Use Map. Traditionally, joint school and park sites are developed in Richmond to benefit the neighbourhood as a whole.

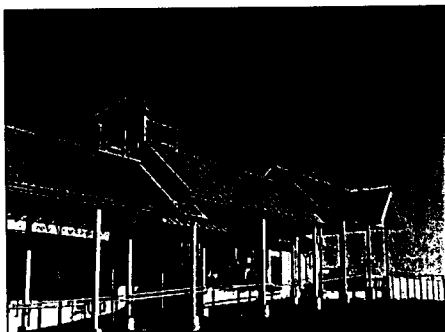
The Dover Crossing Neighbourhood is located in close proximity to the City Centre which is the focus of high density residential and commercial development in Richmond. Therefore, major commercial facilities are not warranted in the Dover Crossing Neighbourhood.

OBJECTIVE 1:

To ensure that a range of neighbourhood facilities and services are made available within the sub-area.

POLICIES:

- a) Expand the services of the Thompson Community Centre rather than duplicate the same services in the sub-area;
- b) Encourage the provision of space for childcare and private amenity space in the sub-area;
- c) Encourage the Richmond School Board to provide an elementary school in the sub-area;
- d) Permit small scale, pedestrian-oriented commercial services that are architecturally integrated within the residential buildings along River Road;
- e) Encourage the provision of special landscaping, a fountain, and public art in the Dover Crossing Sub-Area.



Childcare Facility

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated for multiple-family residential use on the Dover Crossing Land Use Map.

It is intended that these guidelines be used in conjunction with:

- The City wide Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw);
- With the site specific guidelines for each development site located within the neighbourhood as shown on the “Location of Development Sites with Detailed Character Guidelines” Map.

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

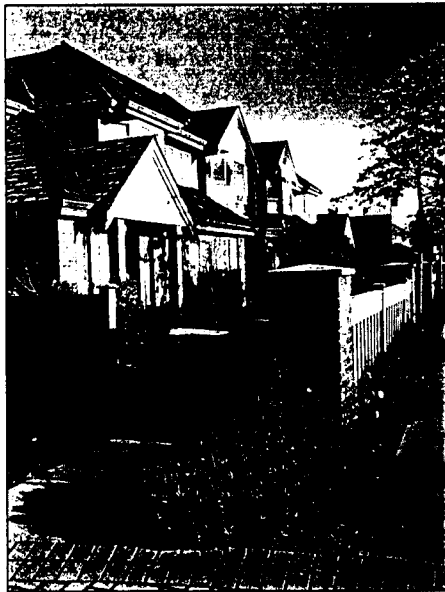
8.1.2 JUSTIFICATION

All the multiple family developments in the Dover Crossing Neighbourhood merit site-by-site consideration of form and character to ensure:

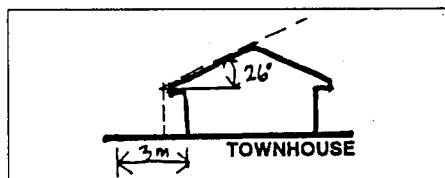
- That an appropriate, high quality built form is developed adjacent to the waterfront;
- That the open spaces and the form of development in the Dover Crossing Neighbourhood provide a gateway to Richmond from the No. 2 Road Bridge crossing;
- That the intermediate view to the Fraser River and airport, as well as the distant views to the North Shore mountains are preserved and enhanced by the building forms and provision of unobstructed view corridors;

- That the forms and materials applied to the buildings, and the type of street furniture, landscaping, signage, and light standards erected in the open spaces are co-ordinated throughout the area to form a distinct and cohesive urban neighbourhood character.

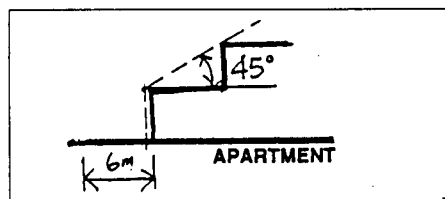
8.2 DEVELOPMENT PERMIT GUIDELINES



Secondary Entrance from Private Residences onto Common Walkway



Townhouse Transitional Height Gradient



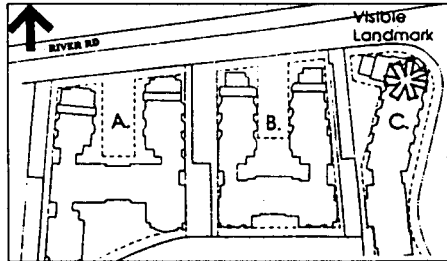
Apartment Transitional Height Gradient

8.2.1 SETTLEMENT PATTERNS

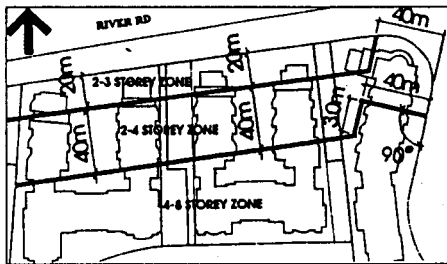
- Establish clear and appropriate pedestrian connections between the private residences and the public areas;
- Locate major entrances of the buildings onto the linear walkways;
- Provide secondary entrances from the building courtyards and from the private units onto the linear walkways where possible;
- Locate secondary entrances from the private residences onto common walkways located in between development sites;
- Multiple residential developments should provide a broad range of unit types and sizes. Included as part of this housing mix are the following minimum requirements for particular unit types:
 - A minimum of 20% of the units to be no larger than 92.9 square metres (1,000 square feet) of net area;
 - A minimum of 50% of the ground-oriented units with direct access to a linear walkway shall contain two bedrooms;
 - A minimum of 10% of the ground-oriented units with direct access to a linear walkway shall contain three or more bedrooms;
 - A minimum of 5% of the units shall be "adaptable housing" to accommodate individuals with disabilities. A minimum of 25% of these units provided shall be ground-oriented.
- Multiple residential development should recognize the importance of the "companion animal" (e.g. dogs and cats) to people living in the community, as well as the health and emotional benefits of the human-bond animal.

8.2.2 MASSING AND HEIGHT

- A variety of overall roof heights and forms should be employed to give interest and residential scale to the building forms.



Landmark



Terraced Massing

- b) Establish a maximum transitional height gradient for *townhouse development* of no more than 26° (see Townhouse Transitional Height Gradient sketch) and no more than 45° for *apartment development* to all property lines (see Apartment Transitional Height Gradient sketch); (These gradients may be varied provided privacy, sunlight, view and human scale criteria are met.)
- c) Employ pitched roofs to express a traditional residential character as the theme of the development. Flat roof elements, in combination with these pitched roofs, may be introduced as a means to create diversity;
- d) Sculpt and terrace upper floors in buildings over four-storeys in height to reduce mass, and create a transition in height and maximize views;
- e) Reduce the scale of the buildings along the walkways by providing secondary roofs or trellises over entries and patios at lower levels;
- f) Provide a major landmark roof feature on the building located at the north-east corner of the Dover Crossing Neighbourhood (see Landmark sketch);
- g) Create terraced roof forms along River Road by stepping the building stories back from the riverfront (see Terrace Massing sketch).

8.2.3 ARCHITECTURAL ELEMENTS

- a) The waterfront location should be reflected in a marine style of architecture and in the choice of street furniture and lighting standards throughout the neighbourhood;
- b) Create "front stair" connections between a unit's private outdoor space, and the linear walkway it faces, provided that the grade between the two areas is no greater than 1.5 m (4.92 ft.);
- c) Create highly visible and identifiable building entrances through the use of landscape and prominent architectural components to create gateways into the courtyards;
- d) The residential character of the neighbourhood should be expressed through appropriately scaled and proportioned windows;
- e) Use various forms of projections, such as bay windows;
- f) Orient interior spaces as well as primary windows of units towards views of the mountains or the school and park site, rather than directly across the linear walkways and courtyards;
- g) Public and private outdoor space should be clearly defined to enhance the feeling of privacy and the pedestrian experience on the neighbourhood walkways;
- h) Articulate building edges to define private balconies and patios that become a natural extension to the residential unit;

- i) All Development Permit applications shall require evidence in the form of a report and recommendations prepared by a person trained in acoustics and current techniques of noise measurement demonstrating that the noise level in those portions of the dwelling units listed below shall not exceed the noise level in the A-weighted 24-hour equivalent (Leq) sound level and will be defined simply as noise level in decibels.

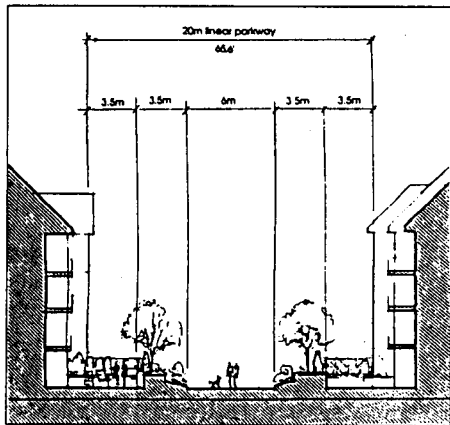
<u>Portions of Dwelling Units</u>	<u>Noise Levels (Decibels)</u>
Bedrooms	35
Living, dining, recreation rooms	40
Kitchen, bathrooms, hallways	45

In addition to the above, the trained professional is to assist in the design of the private patios and balconies to minimize the noise levels with recommendations for building material selection and space planning.

8.2.4 LANDSCAPE ELEMENTS

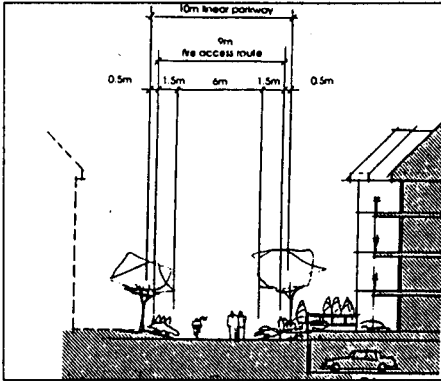
- a) Provide three linear walkways extending from Westminster Highway through to the water. Each of these three walkways will have:
 - Focal areas in the form of arrival plazas and entry courts;
 - Will rise gradually up from the south to the north to maximize viewing opportunities to the waterfront and mountains.

The central walkway is the most significant of the three walkways because it will provide a direct connection through the neighbourhood, across River Road to the riverfront and trail system, and the central and east linear walkway will also offer viewing points at their termination at River Road;

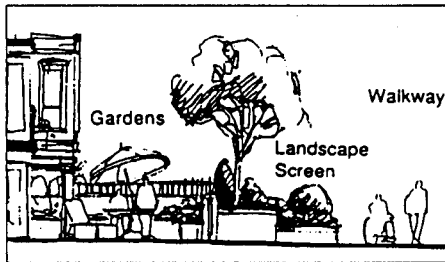


20 m Walkway

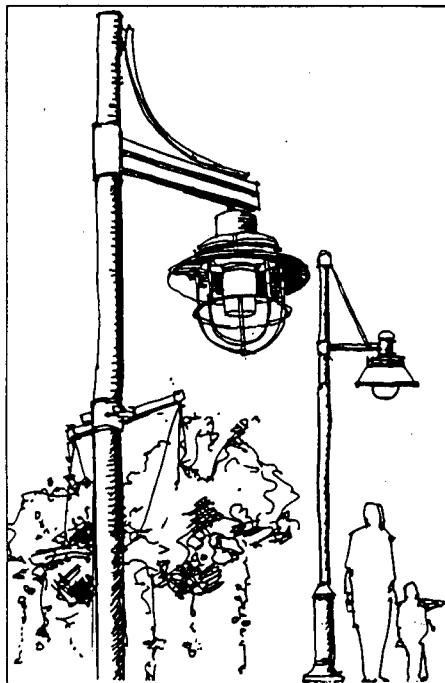
- b) Prominent at-grade crossings accessible by pedestrians, cyclists and people with disabilities will be provided over River Road to provide distinct and safe crossings to the dyke;
- c) An east-west walkway shall also be provided along the south side of River Road to link the three linear walkways to No. 2 Road, as well as to the pedestrian crossings over River Road;
- d) Ensure that the centre linear walkway is a minimum of 20 m (65.62 ft.) in width and is comprised of 6 m (19.69 ft.) of hard walking surface, 3.5 m (11.48 ft.) of low level landscaping, and 3.5 m of terraced planting (see 20 m Walkway sketch);



East and West Linear Walkway

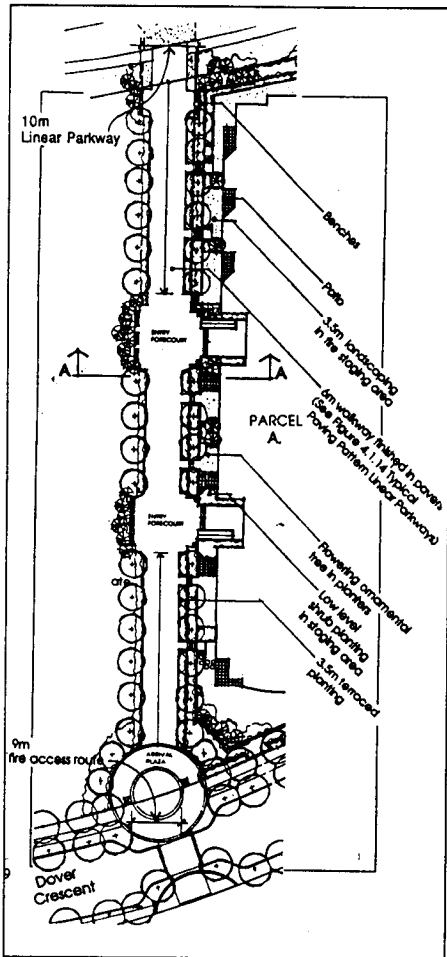


Landscape Private Open Space



Lighting

- e) Ensure that the east and west linear walkways are a minimum of 10 m (32.81 ft.) in width and are comprised of 3 m (9.84 ft.) of hard walking surface, and 3.5 m (11.48 ft.) of planters and low level landscaping (see East and West Linear Walkway sketch);
- f) Provide substantial landscaping, terracing, screening and low level hedges between private ground-oriented outdoor spaces and the public spaces (see Landscape Private Open Space sketch);
- g) Require that all elements and connections in the pedestrian circulation system be accessible by bicycles, and people with disabilities including provisions for the visually impaired;
- h) Use the same decorative and durable walking surface on all the linear walkways, and the pedestrian road crossings throughout the neighbourhood;
- i) Provide textured surfaces as part of the paving patterns to provide a guide route for the visually impaired;
- j) Install low level, possibly wall mounted pedestrian lighting which provides light and security onto the walkways, but does not produce glare into the adjacent residential buildings;
- k) Erect ornamental lights at the focal areas (arrival plazas, entry forecourts, viewing areas) along the entire pedestrian circulation system. These lights should have a maritime design character and include:
 - Post and bracket support system;
 - A pendant fixture with metal hood;
 - A painted metal finish in a maritime colour.
 All the lighting standards and street furniture should be finished in a common colour scheme throughout the neighbourhood to enhance the area's special character (see Lighting sketch);
- l) Install directional signage at strategic locations throughout the neighbourhood which provides information relating to the public areas (linear walkways, crossings, school, park, etc.), as well as to the private residences (building entries and facility signs). The signs should be co-ordinated with the design character and location of the lighting standards throughout the entire neighbourhood. The signs should be visible during day time and evening hours;
- m) Plant ornamental, possibly flowering trees along the both sides of the three linear walkways. Trees should have a minimum calliper of 50 mm (1.97 in.), and be spaced at 6.0 m (19.69 ft.) to 7.0 m (22.97 ft.) intervals;



Arrival Plazas and Entry Forecourts



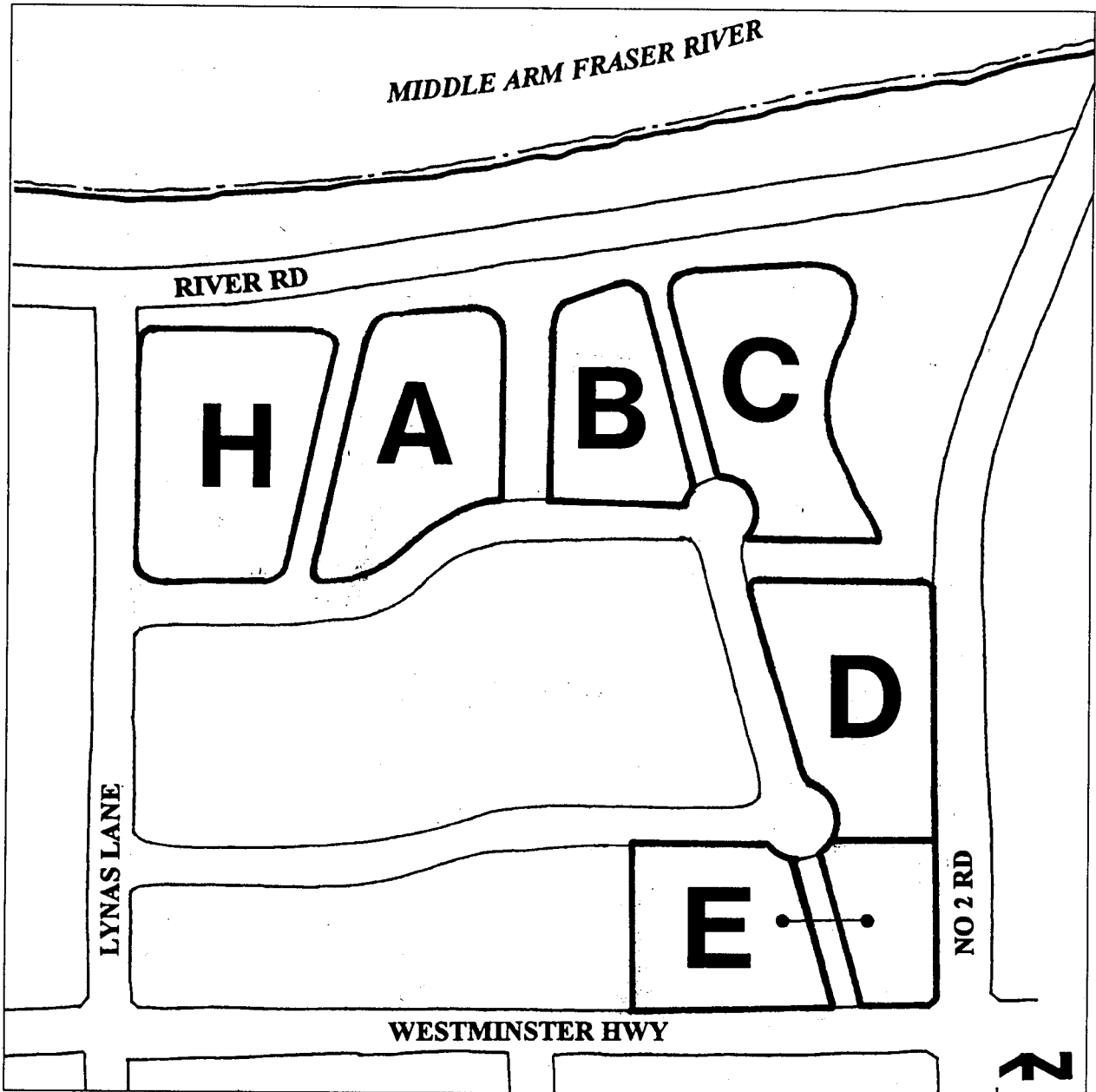
Linear Walkway

- n) Provide terraced planters along the linear walkways containing plants that create a height gradient separation between the public and private areas. The dimensions of the planters should be at least 1.2 m (3.94 ft.) wide and 0.9 m (2.95 ft.) deep;
- o) Locate the "arrival plazas" (enlarged circular seating areas which are intended to welcome and orient the visitor) at the entrances of the linear walkways from Westminster Highway and Dover Crescent (see Arrival Plazas and Entry Forecourts sketch);
- p) Design the "arrival plazas" to contain the following elements:
 - A widened, hard circular surface with a distinct paving pattern;
 - Bollards to prevent non-essential vehicular traffic;
 - High quality vandal resistant street furniture, including benches and garbage receptacles;
 - Accent planting;
 - Signage and ornamental light standards.
- q) Locate entry forecourts at all major entrances to buildings along the linear walkways (see sketch);
- r) Design the entry forecourts to contain the following elements:
 - A widened, hard circular surface with a distinct paving pattern;
 - Ornamental light standards and signage;
 - Benches at the edge of the space;
 - Bicycle racks;
 - Accent planting;
 - Garbage receptacles.

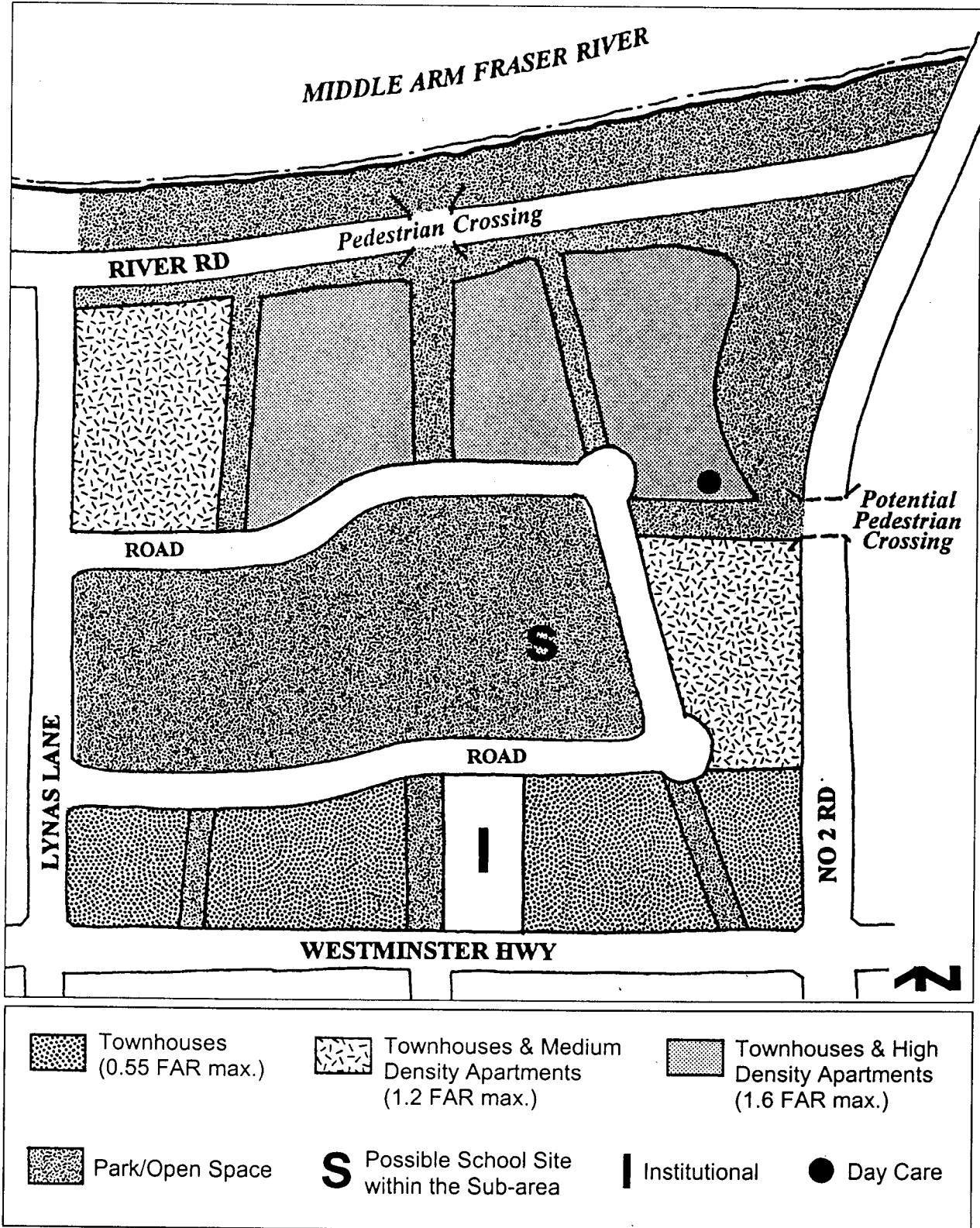
8.2.5 SITE SPECIFIC GUIDELINES AND MAP

Note: There are additional Detailed Character Guidelines for the specific development sites as shown on the following page. The guidelines form part of the Official Community Plan Bylaw 7100, Schedule 2.2A and are available at the Urban Development Division.

Location of Development Sites with Site Specific Guidelines Map



Land Use Map



Richmond Official Community Plan

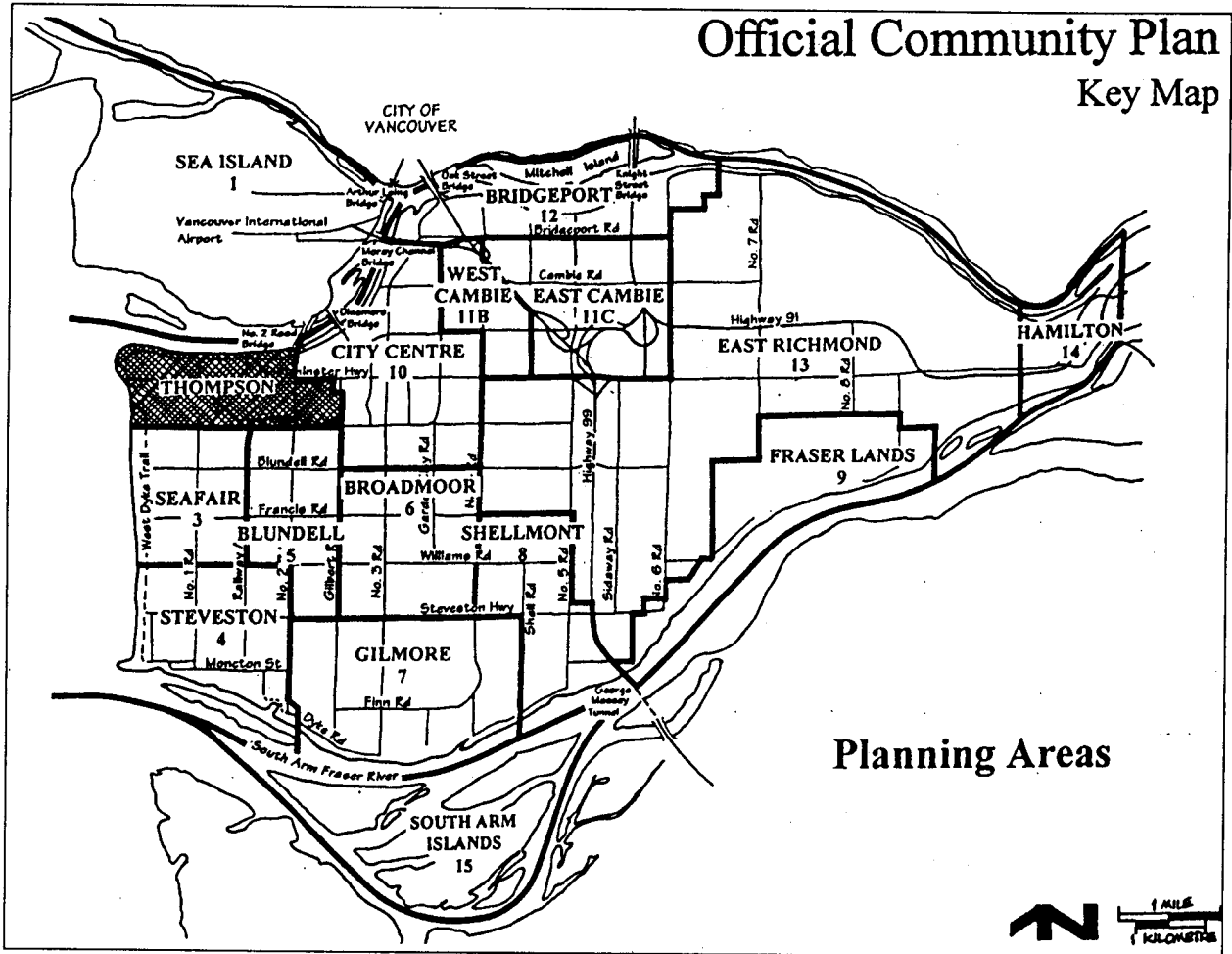


City of Richmond

THOMPSON AREA TERRA NOVA SUB-AREA PLAN Bylaw 7100 Schedule 2.2B



KEY MAP



PLAN AREA MAP

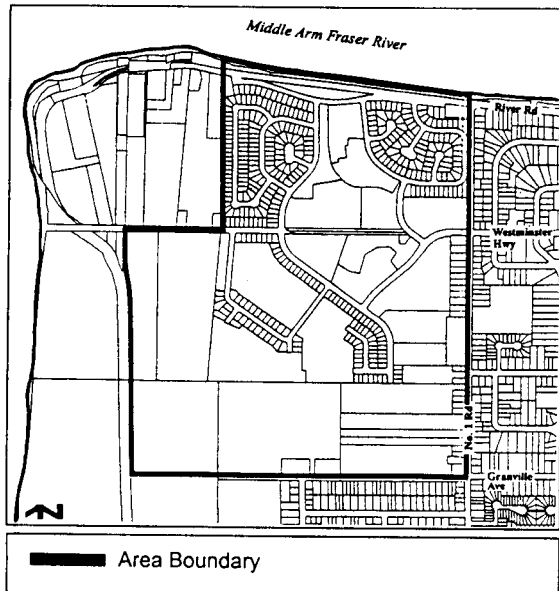


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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City's social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

This plan provides a series of goals, objectives, policies and guidelines consistent with the development of the Terra Nova Sub-Area as a residential neighbourhood adjacent to sensitive environmental areas and significant natural amenities.

The plan area currently consists of approximately 112 ha (276.75 ac.) of dyked floodplain in the north-west portion of Lulu Island, occupied by single-family residences, townhouses, fallow agricultural land, small commercial premises, a golf course and City park land. The extent of the plan area is noted on the Plan Area Map.

1.2 GOALS

The goals of this plan are to:

- Provide public access to the waterfront;
- Preserve significant natural areas;
- Preserve and enhance recreational facilities;
- Provide a range of residential household sizes and types at a modest density;
- Enhance and preserve air, water and soil quality;
- To provide safe and efficient means for pedestrians, cyclists and vehicles to circulate through the area.

2.0 JOBS & BUSINESS



Commercial Services

OBJECTIVE 1:

To maintain local commercial services in the Terra Nova Area.

POLICIES:

- a) Permit the establishment of commercial uses on lands designated "Commercial" on the Land Use Map;
- b) Require the issuance of a Development Permit prior to the issuance of a building permit for any commercial buildings or structures in the plan area;
- c) Require that Development Permits issued for commercial uses comply with the following guidelines:
 - Commercial uses shall be screened from residential uses by means of walls, solid fences and landscaping;
 - Landscaping shall be provided adjacent to public roads, lanes and walkways;
 - Buildings and structures shall be sited in a manner that would minimize the impact of the commercial uses on neighbouring residences.

OBJECTIVE 2:

To maintain the agricultural land base in the area.

POLICIES:

- a) Maintain lands presently designated as "Agricultural Land Reserve" as part of the agricultural land base of Richmond;
- b) Permit open land recreational uses in areas designated "Agriculture" on the Land Use Map, subject to the Agricultural Land Commission Act;
- c) Establish buffers along the urban/rural boundary consistent with the OCP.

3.0 NEIGHBOURHOODS & HOUSING

ISSUE:

The Terra Nova neighbourhood is in close proximity to the Vancouver International Airport and therefore parts of the area are subject to a noise level of 30 NEF or above.

OBJECTIVE 1:

To provide ground-oriented housing in a range of sizes and forms to a range of household types.

POLICIES:

- a) Permit the establishment of single-family residences on those lands designated "Residential (Single-Family)" on the Land Use Map;
- b) Permit the establishment of townhouses and small-lot single-family residences on those lands designated "Residential (Townhouse)" on the Land Use Map;
- c) Permit the establishment of new housing only with the full range of urban utilities and services;
- d) Encourage and facilitate the provision of affordable housing;
- e) Require the issuance of a Development Permit prior to the construction of any townhouses in the plan area;



New Single-Family Residences

ISSUE:

The Terra Nova neighbourhood is in close proximity to the Vancouver International Airport and therefore parts of the area are subject to a noise level of 30 NEF or above.¹

OBJECTIVE 2:

To ensure that new building is in accordance with standards and agreements.

- a) Require builders of new residential buildings to comply with Canada Mortgage and Housing Corporation's guidelines for noise mitigation;
- b) Through rezoning and subdivision applications, require restrictive covenants to hire professionals qualified in acoustics to determine and implement noise mitigation measures.

¹ The Noise Exposure Forecast (NEF) is the official metric used in Canada for airport noise assessment. The NEF contours are calculated (or modelled) by time-averaging aircraft operations considering aircraft fleet mix, flight distances, runway utilization, flight path, and time of day as night-time operations are more heavily weighted. The NEF was designed to encourage compatible land uses in the airport vicinity and to predict human annoyance to airport operations within the noise contours.

4.0 TRANSPORTATION

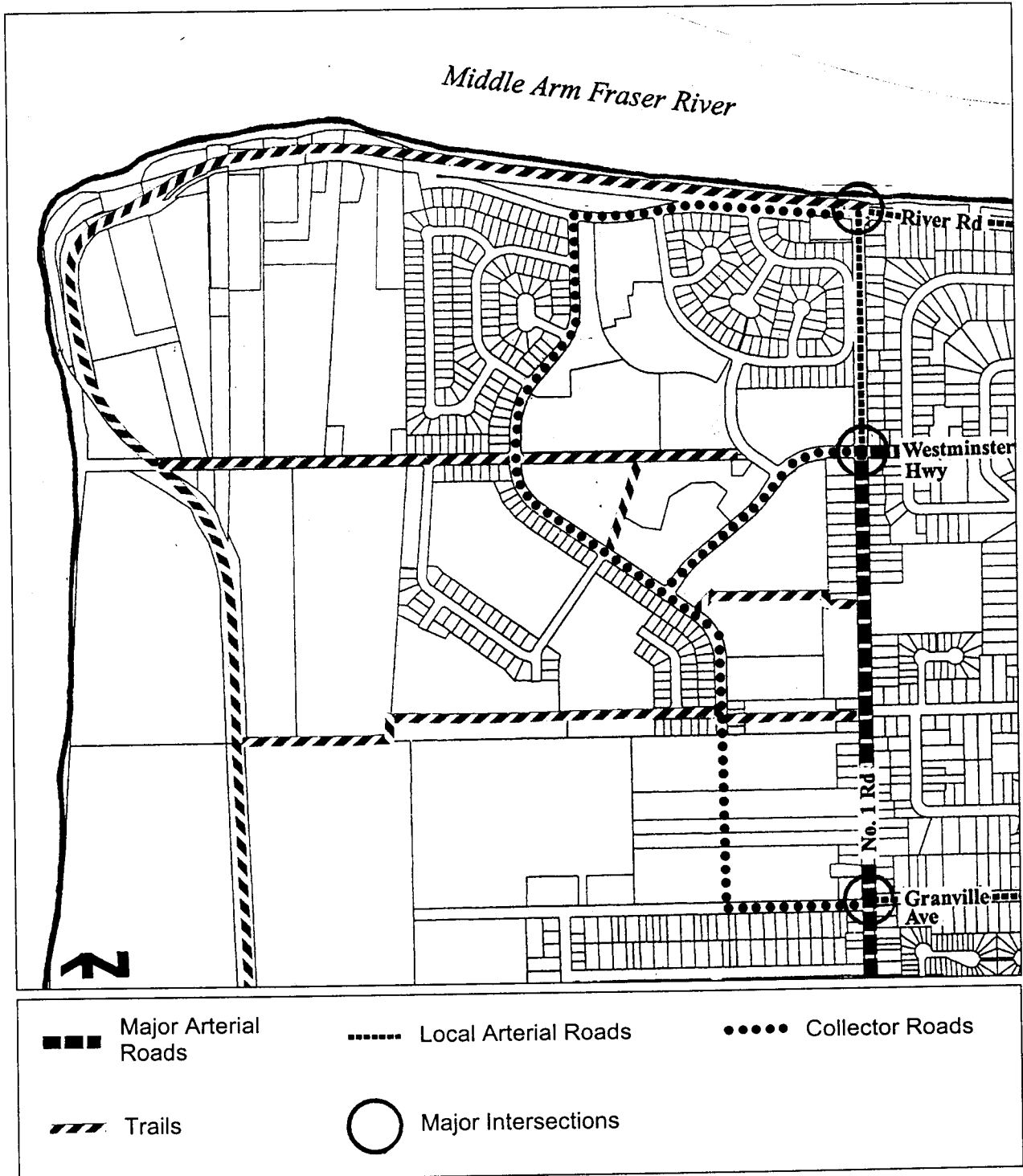
OBJECTIVE 1:

To provide a safe and efficient means of access for pedestrians, cyclists and vehicles through and around the area.

POLICIES:

- a) Restrict direct access to the area from No. 1 Road;
- b) Provide a hierarchy of roads and trails as indicated on the Circulation System Map;
- c) Provide a 20 m (65.617 ft.) wide road allowance where a trail is to be provided in a new local street in order that the boulevard of said street can be landscaped with trees between the curb and the sidewalk;
- d) Accommodate pedestrians, cyclists and vehicles on all roads within the plan area.

Circulation System Map



5.0 NATURAL & HUMAN ENVIRONMENT

OBJECTIVE 1:

To provide a variety of open spaces and public facilities to serve the neighbourhood and the city as a whole.

POLICIES

- a) Provide an open space between River Road and the Fraser River to enhance the dyke trail system upon which buildings will not be erected;
- b) Establish a childcare facility on the central open space within the plan area;
- c) Preserve and enhance the western open space along the perimeter dyke as natural wildlife habitat;
- d) Provide for the active and passive recreational needs of the plan area within the central open space and a smaller park in the south portion of the plan area;
- e) Permit the establishment of a public elementary school within the central open space, if warranted;
- f) Protect archaeological resources within the plan area;
- g) Designate land as public, institutional and open space as indicated on the Land Use Map.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated for multiple-family residential and commercial use on the Terra Nova Land Use Map.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Terra Nova Area.

It is intended that these guidelines be used in conjunction with the City's more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

Residential

A key housing policy of Richmond's Official Community Plan is to carefully integrate new housing development into existing neighbourhoods. All multiple-family projects merit site-by-site consideration of form and character in order to achieve the desired relationship to surrounding areas, and to ensure high standards of new housing design.

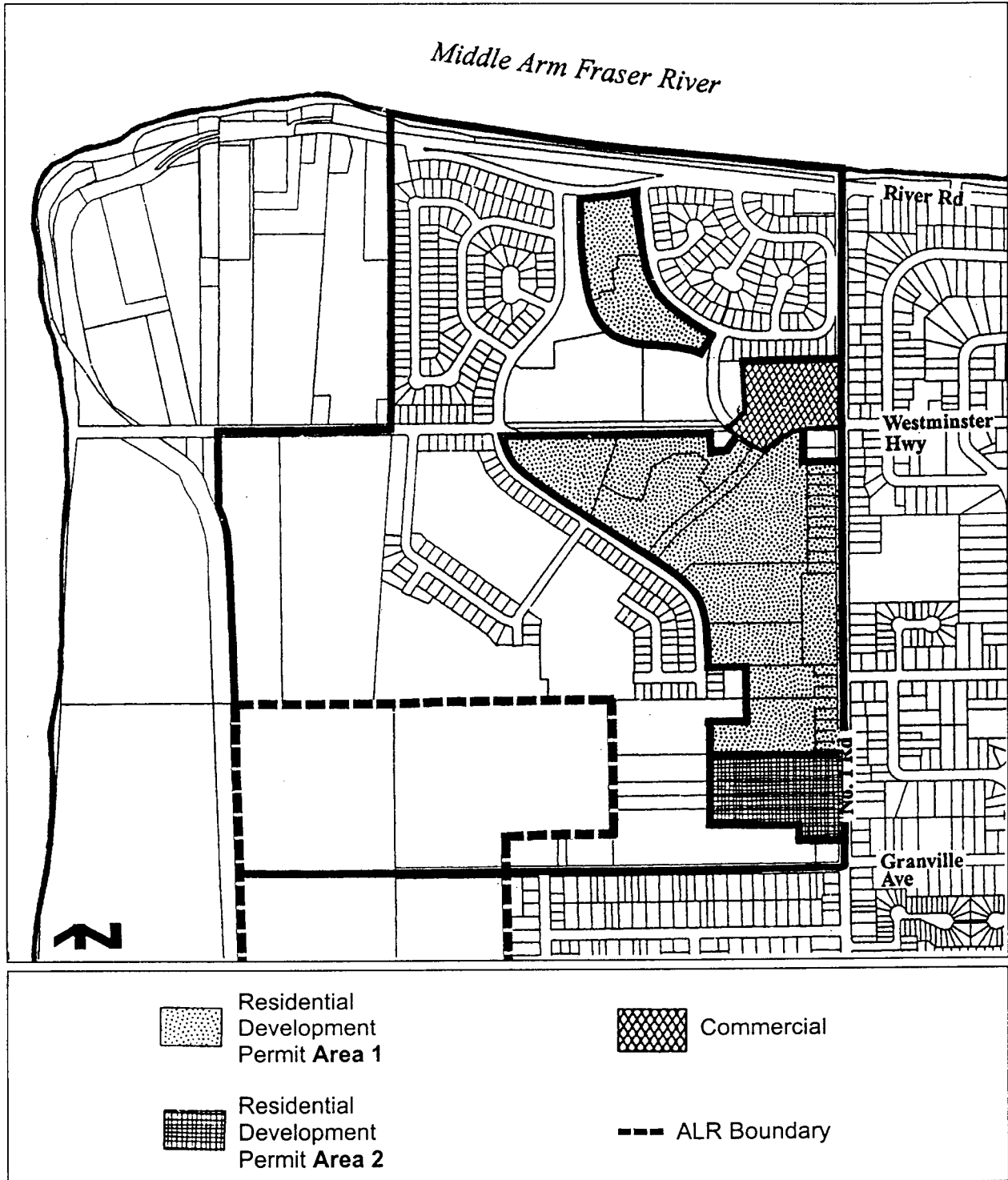
Terra Nova in particular requires special consideration. The neighbourhood's location, traffic considerations, and significant redevelopment potential present issues which need to be addressed through design guidelines for both multiple-family and commercial sites.

Commercial

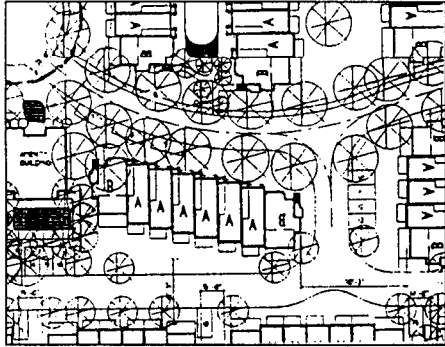
The special conditions that justify the need for additional development permit guidelines for the commercial site are as follows:

- a) That an appropriate, high quality built form is developed at this important gateway location to the Terra Nova Planning area;
- b) That the single-family neighbourhoods to the north and east be protected by ensuring that the future commercial development blends with the character and scale of the surrounding area, and that sufficient buffers are provided between uses;
- c) That an attractive and accessible pedestrian environment be created which provides a continuation of the Terra Nova Trail system and helps ensure a blending of the private and public realms;
- d) That the development adds to the overall beautification of the City through the provision of special features in focal areas such as plazas, water fountains and the greening of streets, and parking areas;
- e) That the materials applied to the buildings, and the type of street furniture, landscaping, signage, and light standards erected in the open spaces are coordinated to form a distinct development.

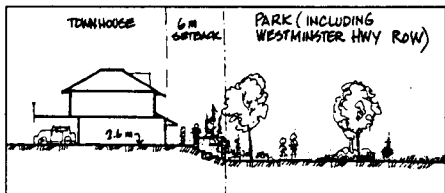
Development Permit Areas Map



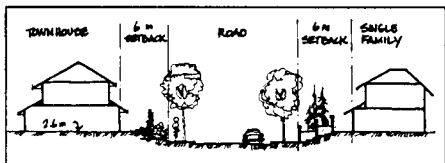
8.2 DEVELOPMENT PERMIT GUIDELINES



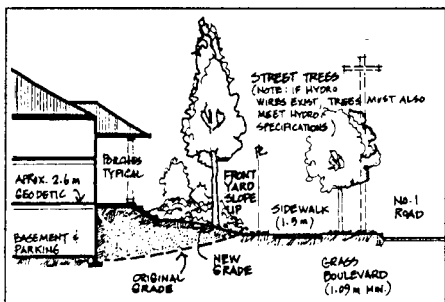
Open Space Separation



Townhouses adjacent to Public Open Space



Townhouses adjacent to Public Roads



No. 1 Road

Area 1: Residential Development

The following guidelines apply to residential development as shown on the Development Permit Areas map in addition to the Guidelines located in the OCP (Schedule 1 to this Bylaw).

8.2.1 SETTLEMENT PATTERNS

- a) Townhouse units should be designed in clusters of 25 units or less, and have less than six units in a row. However, exceptions can be made to allow up to eight units in a row IF larger clusters are separated from other clusters on all sides by a considerable amount of open space (or combination of open space and internal roadways). (See Open Space Separation Sketch);
- b) Entrances should enhance the pedestrian-scale character of the area, by strengthening a unit's connection with public streets and internal roadways; and by masking the appearance of garage doors;
- c) Townhouses adjacent to public roads and trails should have their main front doors visible and accessible from that road or trail (see sketch showing Townhouses Adjacent to Public Open Space and Roads), using stairs and new grading if required to do so (see No. 1 Road Sketch). Direct access for each unit should also be provided to the internal north-south pedestrian and view corridors;
- d) Special views, such as of the mountains or waterfront, should be taken advantage of where possible;
- e) Sunlight should not be blocked for a minimum of 75% of dwellings in each development on every day of the year. The minimum north-south spacing between residences to conserve available sunlight is determined by the sun angle of 17° at noon during the winter solstice;
- f) Use internal roadways to maximize the sense of open space and enhance the area's pedestrian character. Where possible, vehicle and pedestrian access should be separated from each other and appropriately located;
- g) In Residential Area 2, include a semi-public walkway with lights and low landscaping. The walkway should be ungated, and connect to No. 1 Road and to the new road to the east. This corridor on the north property line, should eventually be matched by a similar corridor on the adjacent site;
- h) There should be no vehicle gates at entrances to the site.



Townhouses with Front Doors Visible and Accessible from the Road



Use of Gable Ends and Dormers



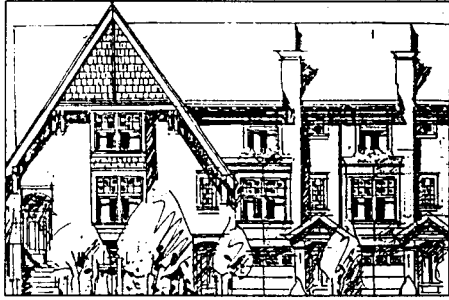
New Townhouse Development showing use of Trim, Colour Accents and Secondary Roof Elements

8.2.2 MASSING AND HEIGHT

- a) Provide a smooth transition between townhouse units and single-family homes to the west. Build duplexes (structures with no more than two attached units together) along the western property line. A space of at least 3 m (9.8 ft.) should separate each duplex structure along the street front;
- b) Townhouse units should have a maximum transitional height gradient of 26 to all property lines. These gradients may be varied provided privacy, sunlight, view and human scale criteria are met. Buildings should have an apparent length of less than 70 m (229.66 ft.);
- c) All roofs should be pitched, with a minimum slope of 45 . Use gable ends and dormers to create diversity in the roof line and to present the area's craftsman character.

8.2.3 ARCHITECTURAL ELEMENTS

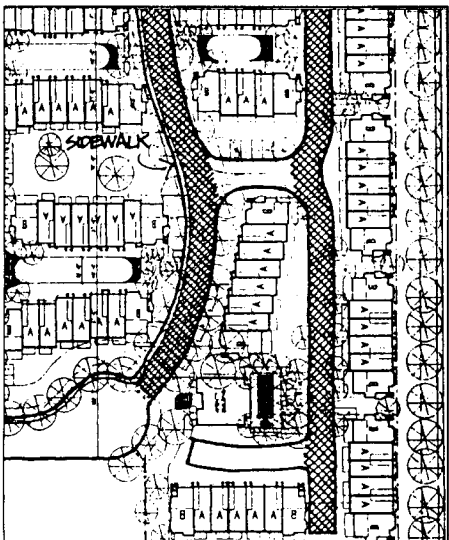
- a) Avoid the appearance of blank walls. Suggested treatment includes (but is not limited to) the use of: trim; more intense colour accents; secondary roof elements; building recesses; and stepped building forms emphasizing lowered height on ends and fronts;
- b) Select roofing materials which suit the level of articulation desired in roof forms. Cedar shingles would facilitate this, whereas heavy-tiled roofing would not;
- c) Windows should be operable, of residential scale, with multiple panels, and should add to the sense of neighbourliness and safety and security by providing visual interest and surveillance:
 - Reinforce the smaller components of buildings which express strong unit identity through appropriate window placement;
 - Use bay windows, window boxes, small balconies, and similar features to avoid a flat, box-like building;
 - Windows should be visually prominent, separated from the building face and defined with colour, trim/shutters, or other decoration;
 - Non-traditional windows such as plastic bubble windows or tinted glaze are discouraged. If skylights are used, they should not be visible from the street or from internal roadways;
- d) Reduce the visual impact of garages along internal roads and driveways by interrupting garage doors with covered secondary pedestrian entrances (see Sketch). Pedestrian entrances should occupy approximately half the width of each garage door;



Garages along Internal Roads and Driveways



Entrance Gateway



North-South View Corridors

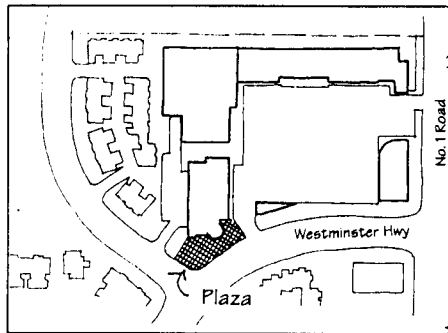
- e) A minimum of 75% of the units should have single garage doors. Tandem parking is encouraged.

8.2.4 LANDSCAPE ELEMENTS

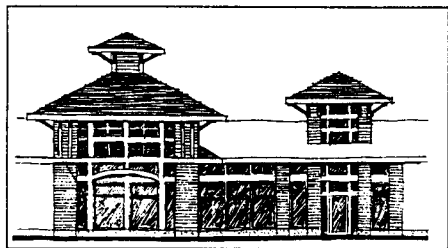
- a) Where stairs are used to provide street or trail access from units, embellish stairs with decorative wooden handrails;
- b) Screening and landscaping, including street trees, should be provided in each townhouse development;
- c) Townhouse units should have a minimum private outdoor space of 9 m (29.53 ft.) in depth and 37 m² (398.28 ft²) in area. The depth of private outdoor space may be reduced to 5.25 m (17.22 ft.) where adequate privacy screening is provided;
- d) Along the public street right-of-way, provide sidewalks on both sides of the street. Make these sidewalks attractive by providing a grass strip with street trees between the sidewalk and the street, and placing all utilities underground;
- e) Avoid the appearance of a solid wall of buildings along No. 1 Road, by varying street edge treatment through use of strategically placed open spaces with intensely clustered plantings; staggered or varied building setbacks; and other techniques;
- f) Create at least one north-south view corridor and pedestrian connection through a central boulevard running north-south through each development site. Treat the boulevard as if it were a public street, by planting large calliper trees on both sides, and providing a sidewalk on at least one side (see North-South View Corridors Sketch);
- g) Vary paving treatments through the use of special pavers, cobblestones or stamped concrete as accents at driveway entrances;
- h) Enhance pedestrian, wheelchair and cycling access to No. 1 Road by providing east-west corridors for non-automotive users in strategic locations;
- i) Landscape all pedestrian paths with trees and other plant materials.

8.2.5 PARKING AND SERVICES

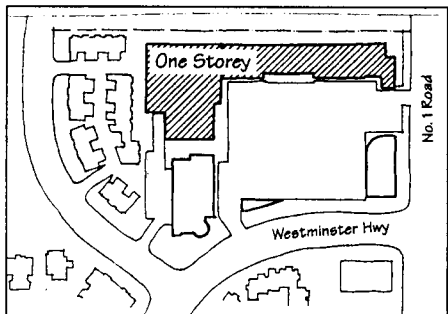
- a) Access provision should be made for emergency vehicles, moving vans, and service vehicles.



Plaza



Massing



Building Heights

Area 2: Commercial Development

The following guidelines apply to commercial development as shown on the Development Permit Areas map in addition to the Guidelines located in Schedule 1 to this Bylaw.

8.2.6 SETTLEMENT PATTERNS

- a) Locate a 595 m² (6,440 ft²) plaza at the south-west corner of the shopping centre extending from the building entry to the curb. (See Plaza Sketch);
- b) Develop an entry feature at the corner of No. 1 Road and Westminster Highway that provides a focal point to the shopping centre as well as a gateway to the Terra Nova neighbourhood. Although not limited to, suggested entry features include:
 - A water feature with accent planting and a Terra Nova Neighbourhood sign. The sign should be designed to reflect the northwest style of architecture used on the buildings. If the sign is lit, it should be done so in a manner that creatively and positively contributes to the nighttime impression of the area;
- c) Articulate the facades of the buildings on all four sides to create the appearance of a series of small scale commercial units that each have their own identity and interest without sacrificing the overall visual compatibility of the development. (See Massing Sketch);
- d) Ensure that the commercial units located in the northern portion of the site do not exceed 9 m (29.5 ft) in height or contain more than 1 storey with the exception of special entry features which should not exceed 10.21 m (33.5 ft.) in height. (See Building Heights Sketch);
- e) Add interest by constructing pitched roof forms at all public facades (i.e. facing No. 1 Road, Westminster Highway and the principal parking areas), except where otherwise concealed by landscaping.

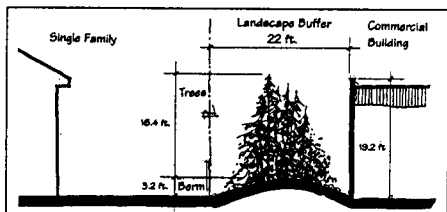
8.2.7 ARCHITECTURAL ELEMENTS

- a) Design individual store entrances in a highly visible manner through the use of landscaping and architectural features such as recesses and projections;
- b) Insulate the buildings along the northern property line to help minimize noise;

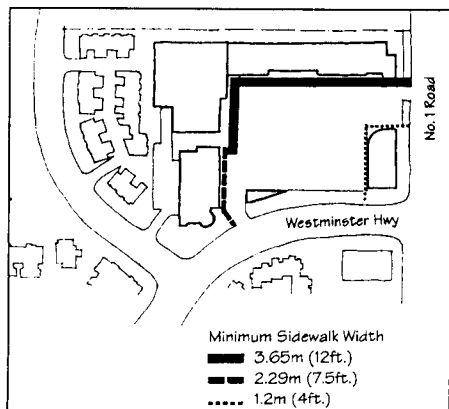
- c) Establish a northwest style of architecture and a sense of quality and solidity through the use of natural building material. Brick, stone, and wood siding all express a sense of quality and permanence. Stucco should only be used in combination with other natural materials, and should not be used within 1m (3.28 ft.) of the ground. Stucco should be treated to prevent discoloration and particular care should be taken in the detailing of north facing facades;
- d) Ensure that a variety of complementary colours are used on the buildings and street furniture;
- e) Conceal all mechanical equipment within the sloped roofs;
- f) Limit commercial signage that is well integrated into the building designs to:
 - Facade signs comprised on letters and logos affixed to the building, or in internally illuminated boxes - the latter to be limited to sign bands immediately above the main floor and second floor windows/doors or copy on awnings, and to consist of light-coloured or white text on a dark background;
 - Freestanding signs limited to directional signs and signs indicating the name and general nature of the services, to a maximum of 2 m (6.56 ft.) in height;

(These guidelines for commercial signage are to be used in addition to the Richmond Sign Bylaw requirements.)

- g) Providing continuous weather protection over the store fronts by including either overhangs, canopies or awnings that are of a sufficient depth (minimum 2 m) to shelter outdoor displays, seating and walking areas on all the internal walkways except for the single building located at the corner of No. 1 Road and Westminster Highway.



Landscape Buffer



Continuous Walkway

- ### 8.2.8 LANDSCAPE ELEMENTS
- a) Provide a minimum 6.7 m (22 ft) wide landscaped buffer between the commercial buildings and the backyards of the single-family homes (see Building Setback Plan accompanying Comprehensive Development District (CD/65) zone). A total of 111 conifers including Douglas Fir and Pine should be planted within a 1 m (3 ft) high berm. Sixty of these 111 trees are currently located on the site and should be relocated into the buffer area. The remaining 51 trees should be 5 m (16.4 ft) high at the time of planting. (See Landscape Buffer Sketch);
 - b) Plant climbing vines on the rear exterior walls of the buildings located in the northern portion of the site;
 - c) Provide a continuous sidewalk between the store fronts and the parking lot. (See Continuous Walkway Sketch);

- d) Enhance the pedestrian experience along the internal sidewalk by:
- Providing low level landscaping planters along the building edge;
 - Incorporating low-level dog tying hitches to the planter boxes;
 - Using a special decorative paving treatment on the internal walkway sidewalk;
 - Expanding the width of the walkway by 3 m (9.84 ft.) into the parking area where feasible in order to provide seating areas in front of commercial units envisioned for restaurant use;
 - Providing benches, garbage containers and bicycle racks along the sidewalk at major building entries finished in a colour scheme complimentary to the building (The bicycle racks should be located within 15.0 m (49.02 ft.) of a principal building entry. The bicycle racks should be constructed of sturdy, theft-resistant material, securely anchored to the ground. The racks should be designed to support the bicycle frame, not just the wheels, and allow for both the frame and the front wheel to be locked to rack with an U-style lock);
 - Connecting the internal sidewalk to the perimeter trail system;
 - Ensuring that the sidewalks and thresholds are accessible to disabled people;
- e) Design the plaza to contain the following elements:
- A widened hard circular surface with a distinct paving pattern measuring at least 10.9 m (36 ft.) in diameter;
 - Benches and garbage containers;
 - Accent planting;
 - Decorative lighting;
 - A water feature;

- f) Construct a 6 m (19.6 ft.) wide trail along the southern perimeter of the site and a 5 m (16.4 ft.) wide trail along the eastern perimeter of the site capable of accommodating pedestrians and recreational cyclists. The southern trail should consist of 3 m (9.84 ft.) of hard walking surface located between a double row of street trees planted in two 1.5 m wide grassed boulevards. The eastern perimeter trail should consist of 2 m (6.56 ft.) of hard walking surface located between a double row of street trees planted in two 1.5 m grassed boulevards. The street trees should have a 8 cm (3.14 in.) calliper at the time of planting and be spaced no more than 7 m to 9 m apart. Honey Locust trees should be planted along the No. 1 Road trail and Ash trees should be planted along the Westminster trail. (See Perimeter Trail Sketch);
- g) Construct the walking surface of the trail with a decorative paving material;
- h) Use a highly textured paving material and raised crossings at driveways to alert pedestrians and motorists of crossings;
- i) Erect automatic gates at delivery entrance driveways to help ensure pedestrian safety along the trail;
- j) Include the curb cuts at the driveway entrances within the boulevards so that the sidewalks remain level;
- k) Erect low masonry walls or hedges along the perimeter of the site only if they do not exceed 1 m (3.28 ft.) in height measured at the highest elevation of the trail, and pedestrian access to shopping centre is not impaired at the corner of No. 1 Road and Westminster Highway;
- l) Soften the parking lot by planting 1 tree per every 3 parking spots so that after approximately 10 years after planting, at least 70 % of the parking lot will be covered by a canopy of leaves in the summer;
- m) Provide decorative lighting in the parking lot that is capable of accommodating hanging flower baskets with irrigation and/or banners. The colour of the lighting standards should be coordinated with the colour of the street furnishings throughout the development. Lighting should not exceed 3 foot candles at the north, west and south property lines;
- n) Screen the parked cars from the perimeter trail without compromising safety and surveillance by providing a slight change in grade and low shrubs at the edge of the parking lot. (See Screen Parking Sketch);

- o) Prepare a tree survey and hire an arborist to determine which of the existing trees on the site (in addition to the conifers used in the northern buffer area) can be retained or relocated on the site;
- p) Erect protective fencing around the trees that are to be retained before commencing with construction. The fencing should be shown on the Landscape Plans.

8.2.9 PARKING AND SERVICES

- a) Locate the garbage and recycling containers as well as utility metres in areas which do not visually or physically conflict with pedestrian traffic along the pedestrian circulation system. The garbage and recycling containers should be contained within an enclosed building;
- b) Provide on-site amenities such as an employee lunch room or lounge, employee locker and shower room, public washrooms, and bicycle parking and end-of-trip facilities;
- c) The bicycle parking and end of trip facilities should include the following:

- Parking facilities shall: be at-grade, have uniform 160 lux (min.) lighting which yields true colours; and, be within sight of building entry, elevator, and/or security;
- Bicycle rooms should provide: lockable door(s) with window(s); tamper proof, motion-activated security lighting; and unobstructed view of each room from its entry; and, facilities for no more than 20 bicycles per room (enabling owners to identify one another);
- Bicycle lockers shall: be constructed of solid, opaque, weather-proof and theft-resistant material, with no exposed fittings or connectors; have lockable doors which open to full height and width of each locker; be grouped together; not be located at the head of parking spaces; and, have clear minimum dimensions of:

Length	180 m (5.91 ft.)
End of Width at Door	0.60 m (1.97 ft.)
End of Width Opposite Door	0.22 m (0.72 ft.)
Height	1.20 m (3.94 ft.)

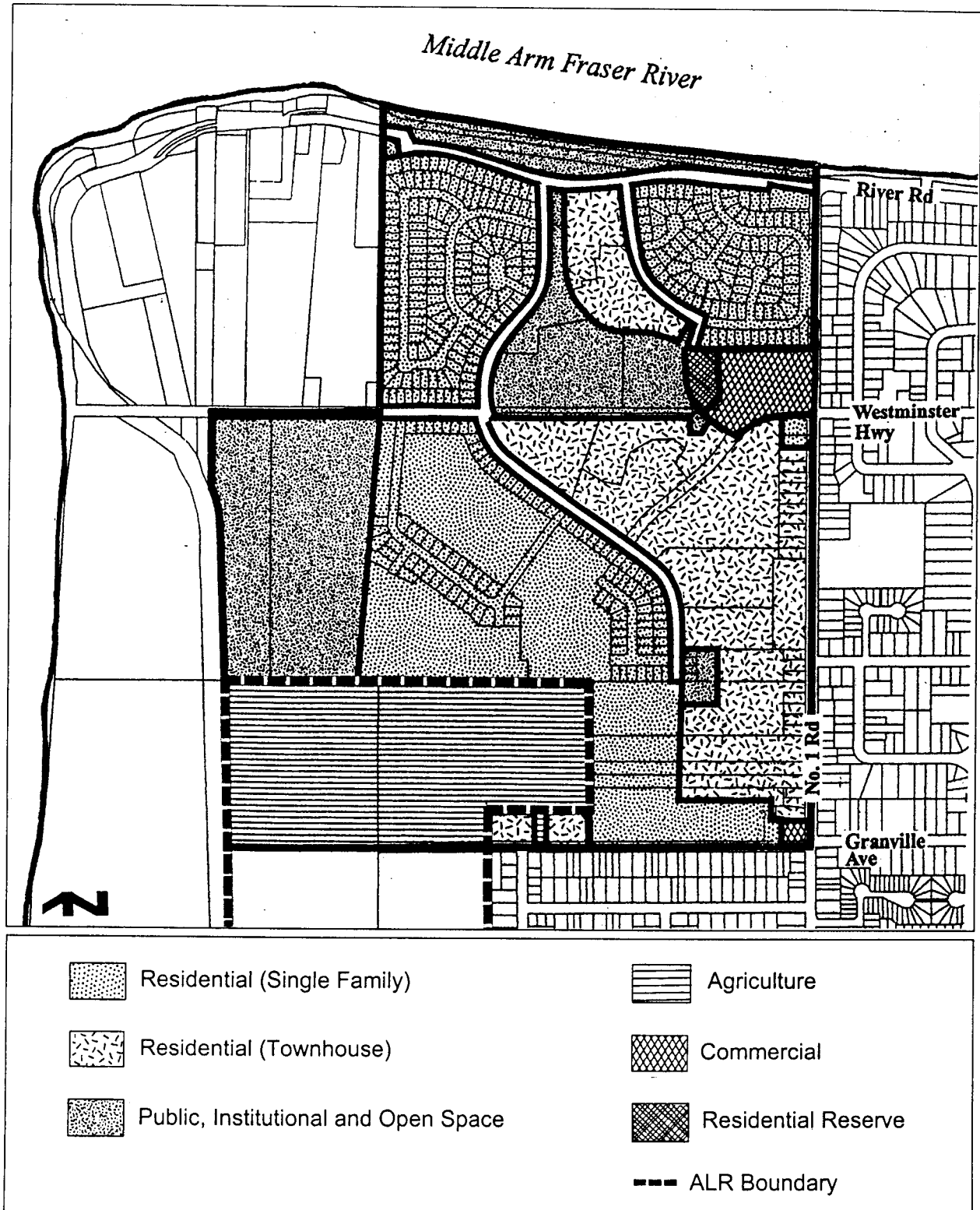
ALR Boundary

The following guidelines apply to development adjacent to the ALR boundary as shown on the Development Permit Areas Map in addition to the guidelines located in the OCP.

8.2.10 SETTLEMENT PATTERNS

- a) For development adjacent to sites within the ALR boundary a landscaped buffer 15 m (49.2 ft.) wide must be provided between the development and the agricultural land.

Land Use Map



Richmond Official Community Plan

BLUNDELL AREA LAURELWOOD SUB-AREA PLAN Bylaw 7100 Schedule 2.5A

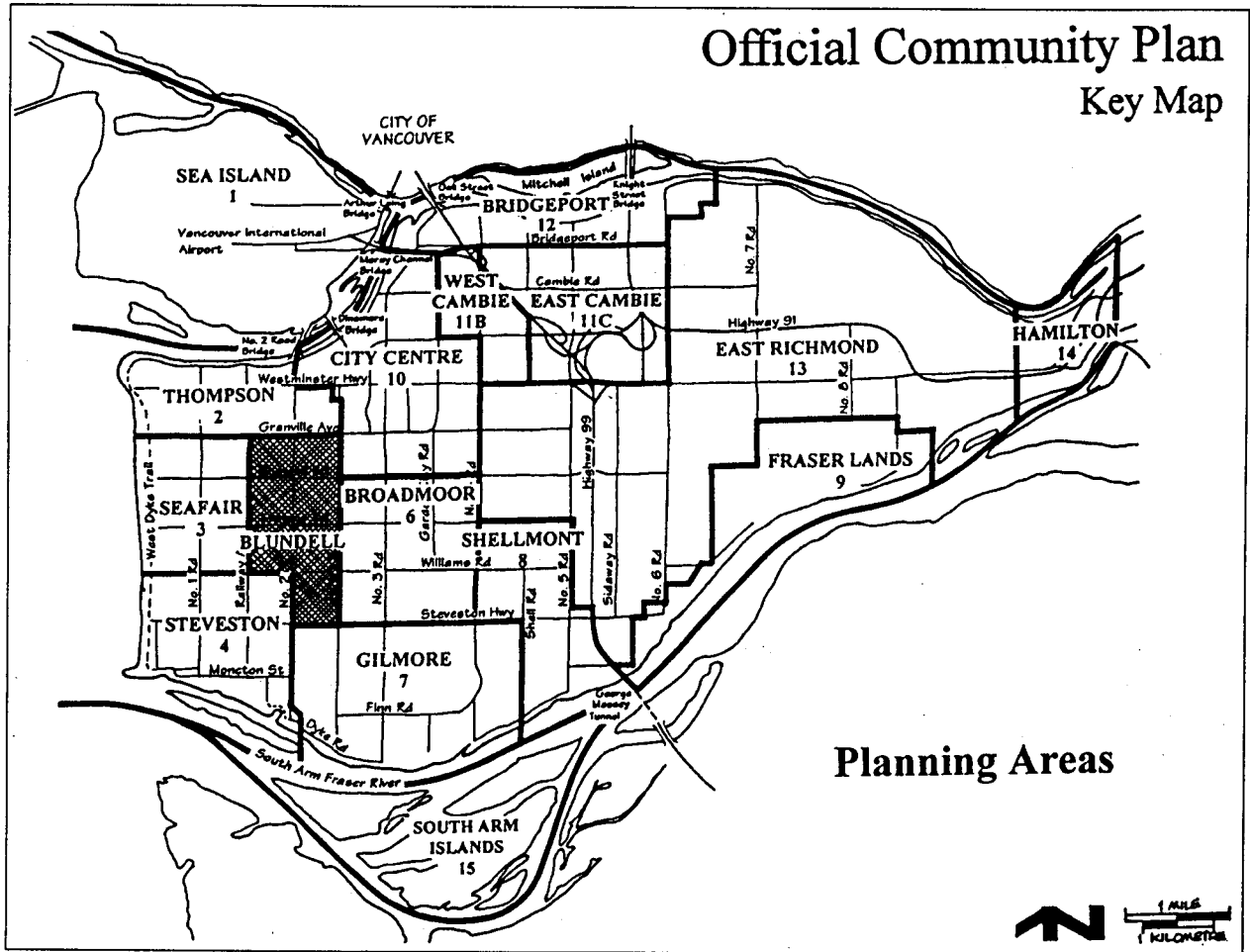


City of Richmond

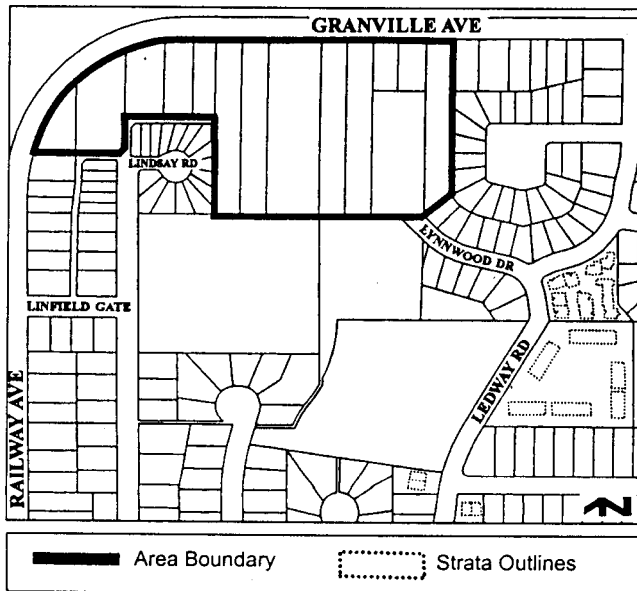


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KEY MAP



PLAN AREA MAP



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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City’s social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City’s Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

This plan provides goals, policies and guidelines consistent with the development of the Laurelwood Sub-Area as a neighbourhood adjacent to a major arterial road, near the Granville Avenue/Railway Avenue bend.

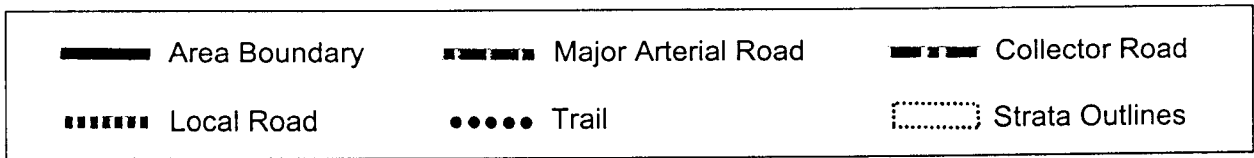
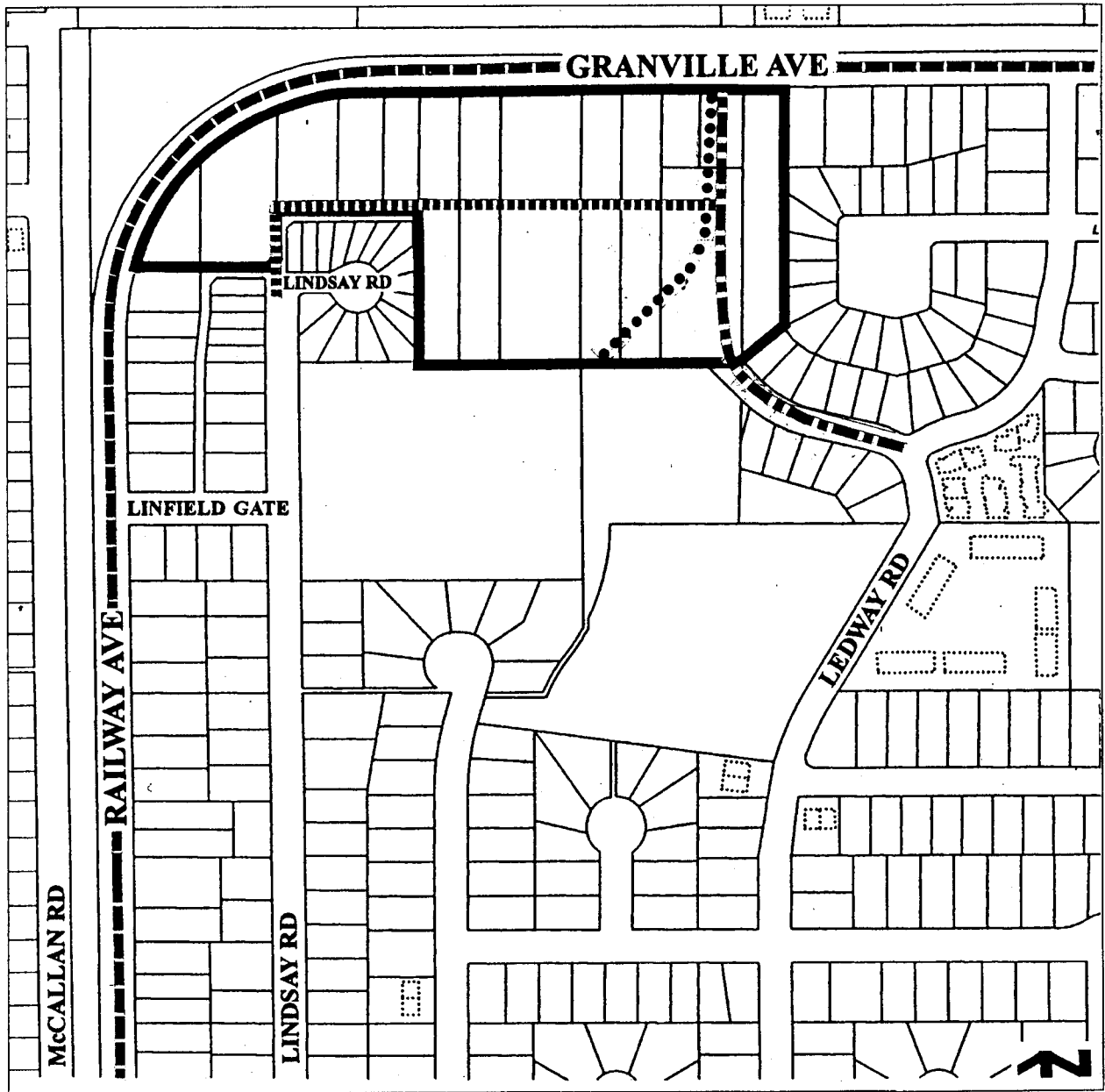
The area to the south and east is a mix of single-family homes on a variety of lots sizes, three-storey apartments, an elementary school and park space. To the north, across Granville Avenue, there is a secondary school, community park and community centre.

1.2 GOALS

The goals of this plan are to:

- Provide a range of residential household sizes and types at a modest density;
- Provide safe and efficient means for pedestrians, cyclists and vehicles to circulate to facilities, parks and arterial roads in the area.

Circulation Map



2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING



New Townhouses

POLICIES:

- a) Permit the establishment of single-family residences on lands designated “Residential (Single-Family)” on the Land Use Map;
- b) Permit the establishment of townhouses on those lands designated “Residential (Townhouse)” on the Land Use Map;
- c) Permit the establishment of new housing only with the full range of urban utilities, street access, and services;
- d) Encourage and facilitate the provision of affordable housing;
- e) Require the issuance of a Development Permit prior to construction of any new townhouses in the plan area.

4.0 TRANSPORTATION



Pedestrian Circulation

POLICIES:

- a) Restrict direct access from individual lots to Granville and Railway Avenues;
- b) Provide a hierarchy of roads and trails as indicated on the Circulation Map.

5.0 NATURAL & HUMAN ENVIRONMENT

POLICIES:

- a) Designate a minimum of 0.2 ha (0.5 ac.) of land for public open space as shown on the Land Use Map.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated “Residential (Townhouse)” on the Land Use Map.

The purpose of the guidelines is to supplement the City-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Laurelwood area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in order to fulfil the sub-area plan goals and objectives.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

- a) Special views, such as of mountains should be taken advantage of where possible.

8.2.2 MASSING AND HEIGHT

- a) Townhouses should be designed in small clusters and have less than six units in row.

8.2.3 ARCHITECTURAL ELEMENTS

- a) Townhouses adjacent to public roads and trails should have their main frontage doors visible and accessible from that road or trail with minimum changes in grade. Use minimal changes in level and landscaping to ensure the privacy of the individual units open space is protected without the use of high fences;
- b) Townhouses should have a maximum transitional height gradient of 26° starting at 4.5 m (14.76 ft.) height at 3.0 m (9.84 ft.) setback from all property lines that do not abut a public road. These gradients may be varied provided privacy, sunlight, view and human scale are met.



Transitional Height Gradient

8.2.4 LANDSCAPE ELEMENTS

- a) Townhouses should have a minimum private outdoor space of 6.0 m (19.685 ft.) in depth and 36 m² (387.5 ft²) in area. The depth of private outdoor space may be reduced to 5.25 m (17.22 ft.) where adequate privacy screening can be provided. (Not recommended where private outdoor spaces abut a public street.);
- b) Screening and landscaping, including street trees, should be provided in each townhouse development;
- c) Define the vehicular entrance to each residential unit by providing a street tree between each driveway/garage entry point. Trees should be a minimum calliper of 100 mm (4 in.) measured 1.4 m (4.7 ft.) above grade;
- d) Use special landscape features such as trellises to conceal garages and visitor parking stalls from surrounding private, semi-public and public areas;
- e) Retain and incorporate mature landscaping into the development where possible.

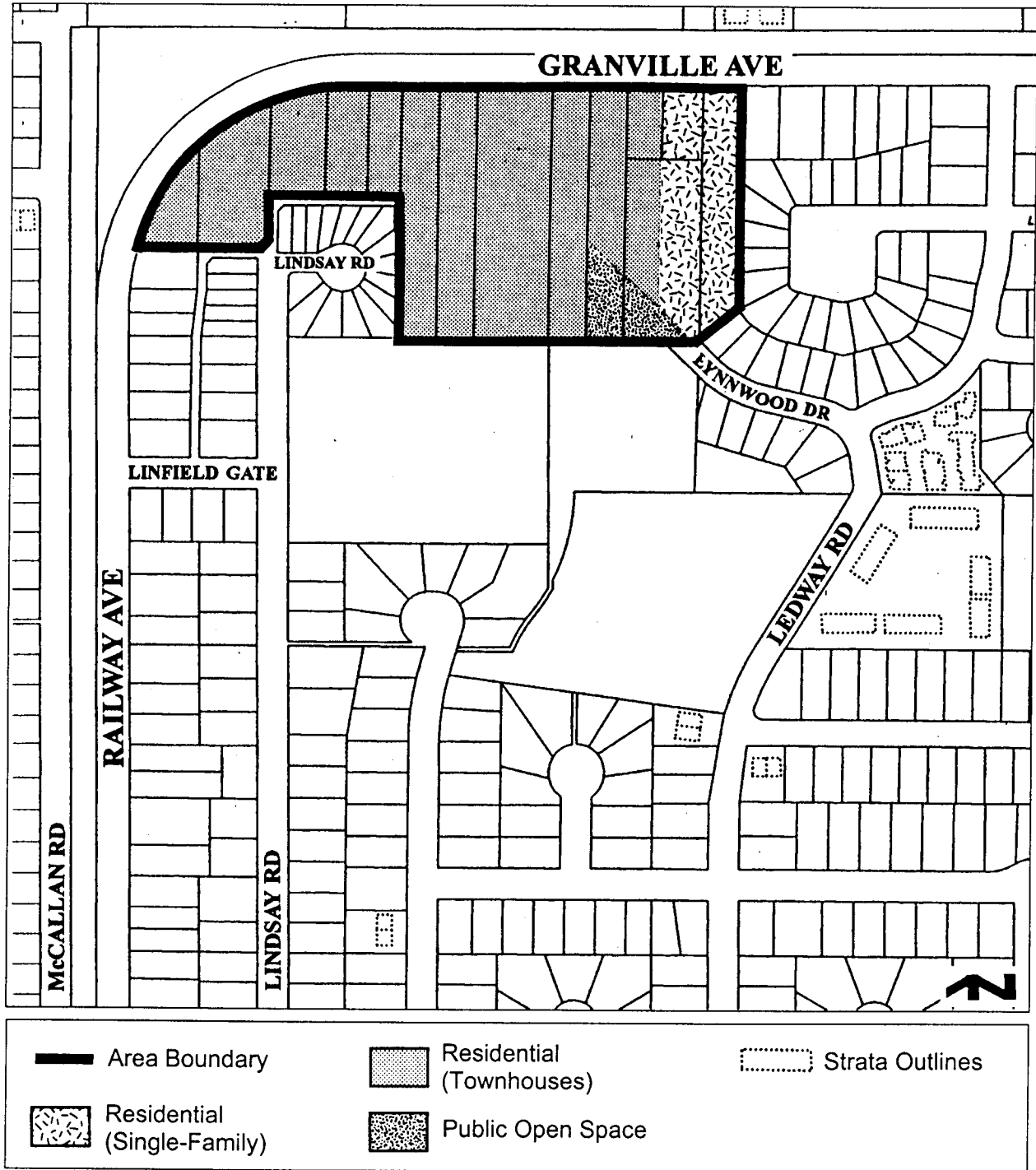
8.2.5 PARKING AND SERVICES



Internal Circulation and Parking

- a) Visitor parking should be in public view and easily accessible near the main entrances. Parking lots should have landscaping to separate every fourth parking space and be separated from children's play areas;
- b) Provision should be made for the storage and collection of garbage and recyclable materials;
- c) Light semi-private open spaces, pedestrian and vehicular circulation routes and building entries to provide security, safety and convenient access without producing glare into adjacent properties.

Land Use Map



Richmond Official Community Plan



City of Richmond

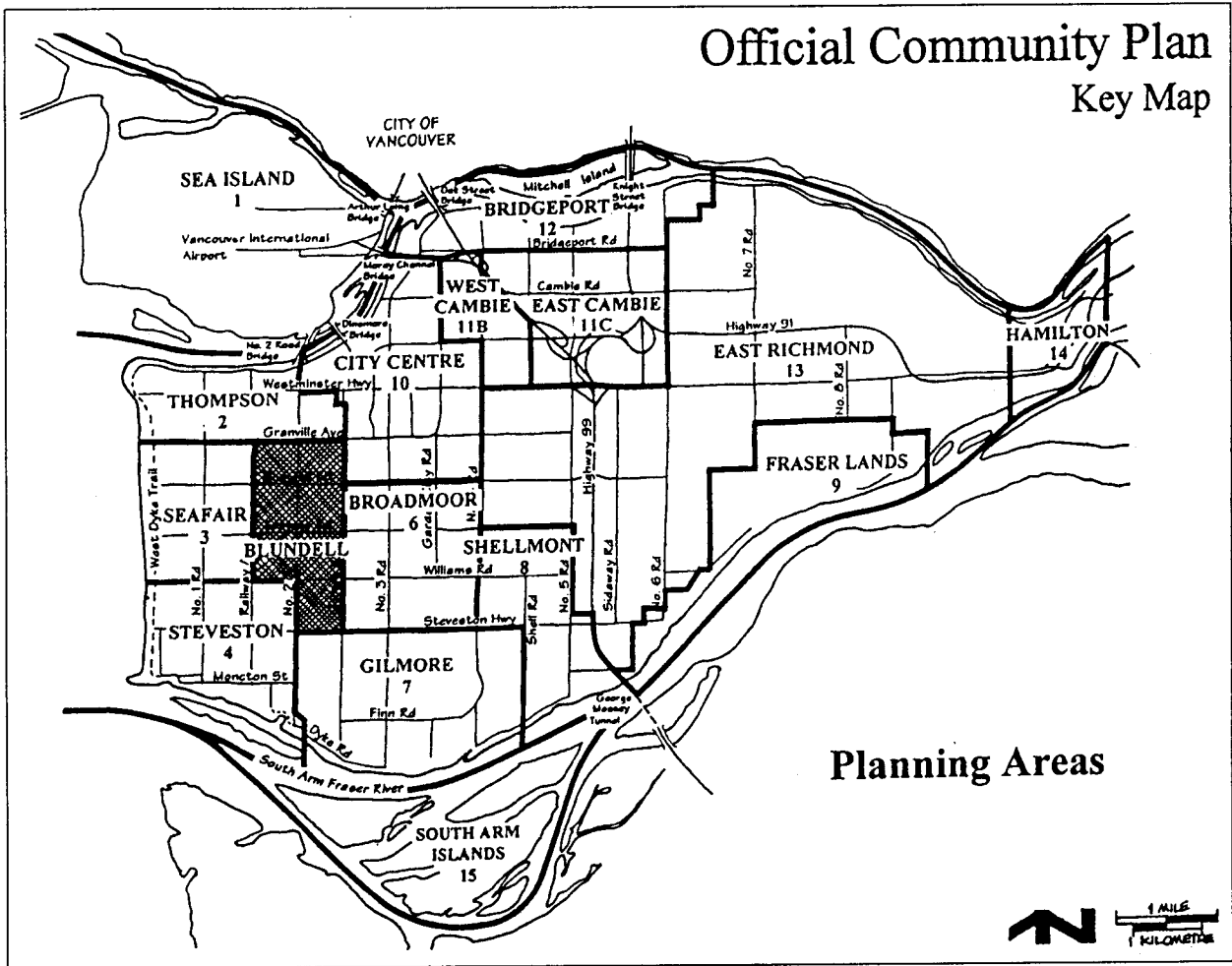
BLUNDELL AREA EAST LIVINGSTONE SUB-AREA PLAN

Bylaw 7100 Schedule 2.5B



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KEY MAP



PLAN AREA MAP

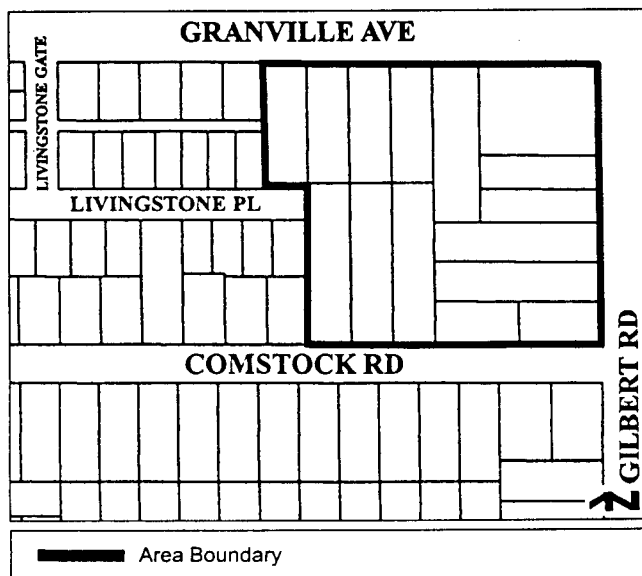


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Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

The East Livingstone Sub-Area is bounded by Granville Avenue to the north, Comstock Road to the south, Gilbert Road to the east, and extends about 170 m (558 ft.) westward from the western edge of Gilbert Road.

This plan contains the goals, policies, Development Permit Guidelines and land use designations for the East Livingstone Sub-Area.

1.2 GOAL

To create a livable, pedestrian-friendly, residential neighbourhood which accommodates a variety of housing types, households and age groups, and fulfils a transition function between the higher density City Centre and the adjacent single-family neighbourhoods.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING

3.1 LAND USE

POLICIES:

- a) Create a tranquil, serene neighbourhood distinguished by its visible, abundant trees and colourful, inviting gardens. Unify street character through the repetition of certain common landscaping elements;
- b) Ensure the neighbourhood is compatible with adjacent single-family neighbourhoods;
- c) Permit the establishment of townhouse or low-rise apartment residential in the north-east corner of the Livingston Sub-Area as shown on the Land Use Map. The maximum permitted density is 0.9 FAR; however, densities lower than 0.9 FAR may be necessary in some cases to achieve livability and other objectives;
- d) Permit the establishment of small-lot single-family, two-family, or townhouse residential use as shown on the Land Use Map at a maximum density of .55 FAR. Where a site includes both apartment and townhouse designations, consideration may be given to averaging densities across the site;
- e) Ensure that setbacks along all public streets is 6 m (19.7 ft.) and setbacks for side and rear yards not directly facing a public street are a minimum of 2 m (6.6 ft.) in order to reinforce the single-family character and provide space for plant landscaping that enhances the pedestrian realm.
- f) Ensure that the neighbourhood is well-connected by providing additional walkway or trail connections which enhance access for East Livingstone and the adjacent single-family area to important City Centre amenities, such as Minoru Park:
 - Secure a public right-of-way to create pedestrian/bicycle paths from Livingstone Place through to Granville Avenue and Gilbert Road, on the approximate locations designated in the land use map;
 - Ensure that these paths are attractive and safe.

4.0 TRANSPORTATION

POLICIES:

- a) Ensure that vehicular access to new buildings from Granville Avenue or Gilbert Road is not permitted;
- b) Ensure that vehicular access to new residential developments within the sub-area is provided through an extension of Livingstone Place through to Comstock Road. Ideally, the Comstock link will be setback at least 100 m (328 ft.) from Gilbert Road;
- c) Provide vehicular access to single-family and two-family residential developments through lanes. Where possible, also use lanes to provide vehicular access to multiple-family development, to minimize the visual and physical impact of the automobile on neighbourhood streets and the pedestrian realm.

5.0 NATURAL & HUMAN ENVIRONMENT

See OCP.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to:

- the lands designated “Townhouse or low-rise apartment residential”; or
- to those sites within the “Small lot single-family, two-family or Townhouse residential” designation that are zoned for townhouse use.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the East Livingstone area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in order to fulfil the sub-area plan goals and objectives.

The neighbourhood’s location, traffic considerations, mature landscaping, and significant redevelopment potential present issues which need to be addressed through design guidelines.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

Vehicular Treatments

- a) Gated driveways are not permitted;
- b) Driveways are not permitted along Granville Avenue or Gilbert Road;
- c) Encourage shared driveways between properties, and limit all driveways to a maximum width of 3 m (9.8 ft.);
- d) Driveways should be lined with trees or shrubs;
- e) The first 10 m (32.8 ft.) of any driveway leading directly from a public road should be treated with grey pavers or stamped concrete;

Pedestrian Treatments

- f) Create safe and pleasant pathways between Livingstone Place and the two major arterial streets;
- g) Pathways should be a minimum of 3 m (9.8 ft.) wide. They should be treated with special pavers, and landscaped with small-scale plant materials and shrubs; however, where possible, existing vegetation should be preserved along these pathways;
- h) Maximize visibility and animation along pathways by orienting windows, entries and balconies on adjacent buildings towards paths;
- i) Visually enhance pedestrian linkages and create a sense of arrival through the use of arrival plazas, special plantings and benches at the intersection of Livingstone Place and the pedestrian paths leading to Granville Avenue and Gilbert Road;
- j) Ensure that pathways are well-lit, visible from the street and clearly marked with entrances embellished by ornamental planters or small (columnar or narrow pyramid-shaped) trees.

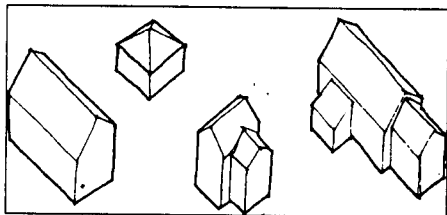
8.2.2 MASSING AND HEIGHT

- a) Ensure that the scale, massing and form of new buildings complement adjacent and facing single-family residential areas and contribute to a pedestrian-friendly street environment;
- b) Avoid facing blank building walls onto the street. Orient windows and doors streetward, and create an attractive frontage using windows and doors to help animate the building facade and promote a more human-scale character;



Stepped Building Form

- c) Reduce the apparent height of buildings with architectural treatment that promotes recognition of individual storeys and avoids the appearance of sheer blank walls. Such treatment might include (but is not limited to) the use of: trim; more intense colour accents; secondary roof elements; building recesses; and stepped building forms emphasizing lowered height on ends with fronts;
- d) Where apartments are permitted, limit height to a maximum of four storeys over parking;
- e) Provide a side yard height step-back where apartments are adjacent to townhouse or single-family dwellings. Also provide height step backs at street corners, in the form of lower (eg one or two-storey) building elements (see Building Massing sketch).

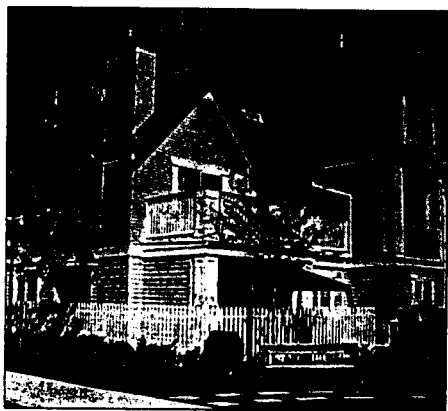


Building Massing

8.2.3 ARCHITECTURAL ELEMENTS

Porches, Balconies, Patios

- a) Use porches and balconies to provide visual interest to buildings while complementing existing neighbourhood character and providing usable private open space to residents;
- b) Large projecting balconies are discouraged along the streetfront, as they emphasize building bulk. However, recessed balconies are encouraged (see Recessed Balconies sketch);
- c) Porches are encouraged along Livingstone Place or Comstock Road, and may project up to 2.5 m (8.2 ft.) into front yard setbacks along these streets. Porches are discouraged along Granville Avenue and Gilbert Road, and may not project into setbacks along these streets;
- d) Porches and covered stairs should be at least 2.5 m (8.2 ft.) deep to allow for usability, and should incorporate prominent main entries;
- e) Porches, porch entrances, and related accents provide desired opportunities for varied and creative design expression. They should enhance individual buildings and be accented with traditional materials such as natural wood or wrought iron. They should also be integrated into the facade rather than appearing "tacked on";



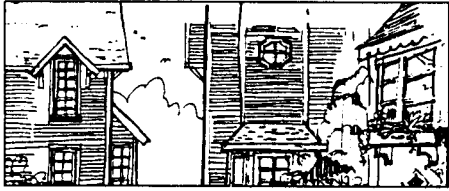
Porches and Balconies

Windows

- f) Windows should be operable, of residential scale, with multiple panels, and should add to the sense of neighbourliness and safety and security by providing visual interest and surveillance (see Residential Scale Windows sketch);



Recessed Balconies



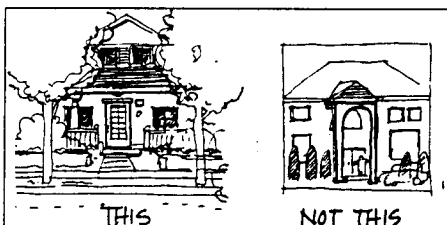
Residential Scale Windows



Prominent Main Entries



Grade Access Entry



Single Storey Features

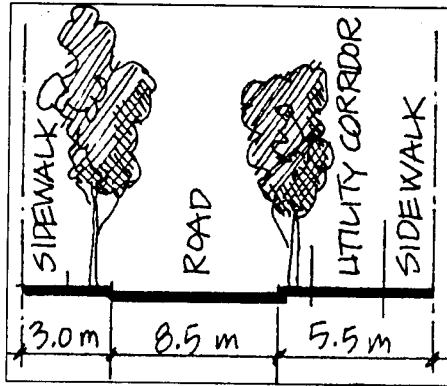
- g) Reinforce the smaller components of buildings which express strong unit identity through appropriate window placement;
- h) Windows, used singly or in combination, should be apparent at eye level and should be clear-glazed (not tinted) to reinforce the sense of surveillance over the street;
- i) Windows should be visually prominent, separated from the building face and defined with colour, trim/shutters, or other decoration. Bay windows, window boxes and similar features are also encouraged;
- j) Traditional window treatment should be used where visible from the street. Skylights, plastic bubble windows, tinted glaze and other non-traditional windows should not be used in prominent locations on the building that are visible from the street;

Entrances

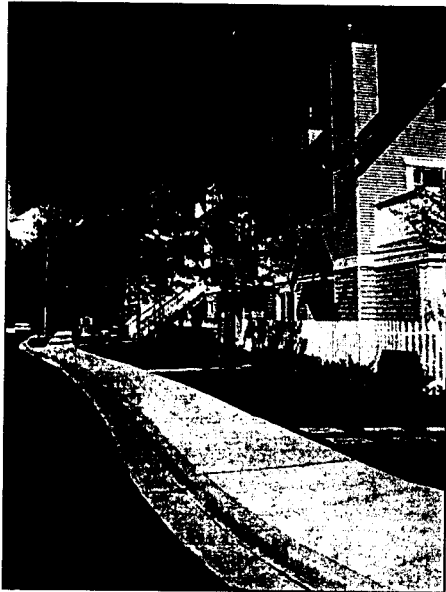
- k) Strongly tie new multiple-family development to quiet residential streets, and use entrances to reinforce and achieve a human-scale and pedestrian-friendly environment along these streets (see Prominent Main Entrance sketch);
- l) Provide front door entries with direct grade access to new townhouse and apartment developments that are oriented to, and clearly visible from either Livingstone Place or Comstock Road. Ensure that entries are accessible to disabled persons;
- m) Architectural treatment of unit entrances should reinforce proximity to grade level and avoid two-storey features (see Single Storey Features sketch);
- n) Entrance features, such as trellises and small entry courts edged with ornamental trees, shrubs and plants, are encouraged;

Materials

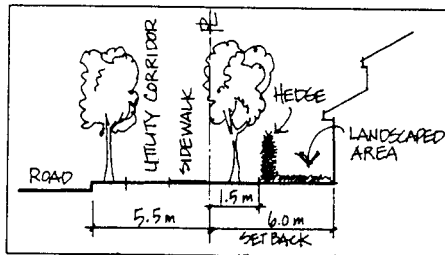
- o) Roofing material should emphasize natural or west-coast produced materials such as cedar. Metal roofs are discouraged and the use of roof tiles should be avoided;
- p) Particularly desirable building materials include: natural stone, cedar siding, and wood shingles. Good quality vinyl-coated aluminum or solid vinyl reproductions of traditional siding materials are acceptable. The use of brick is discouraged and the use of stucco should be minimized, particularly on surfaces visible from the street.



Local Road



Sidewalk along Livingstone Place



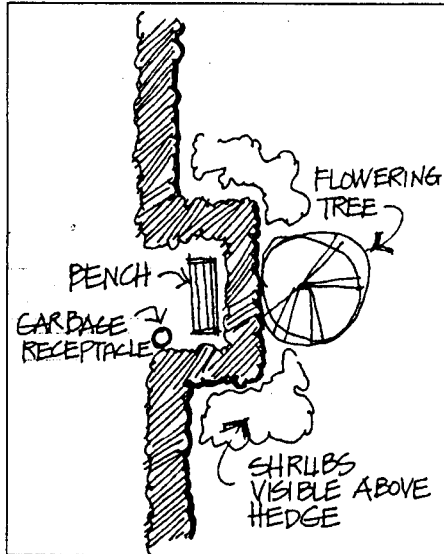
Landscaped Setback

8.2.4 LANDSCAPE ELEMENTS

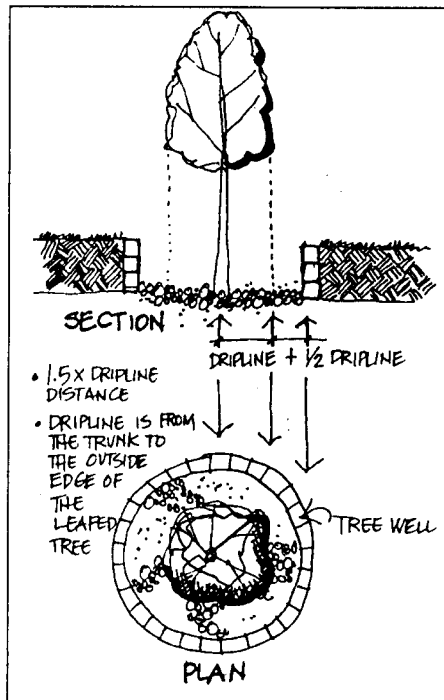
- a) Use landscaping to create a uniform street appearances, and share the visual benefits of attractive landscaping on private property among all neighbours;

Streetscape

- b) Ensure that windows and doors remain visible from the street and are not hidden by vegetation;
- c) Provide sidewalks on both sides of Livingstone Place and on the north side of Comstock Road. Make these sidewalks attractive by providing street trees, and placing all utilities underground (see Local Road sketch);
- d) Along Comstock and Livingstone Place provide a 1.5 m (4.9 ft.) grass strip at the front property line directly beside the sidewalk and include a second row of trees along this grass strip. Trees should be spaced approximately 10 m (32.8 ft.) apart, with oak varieties recommended, such as red oak (*quercus rubra*) or willow oak (*quercus phellos*). Directly behind this grass strip provide contiguous flowering or evergreen hedging (eg. wild rose, boxwood, laurel) of 0.6 m (2 ft.) in height, and at least 0.6 m (2 ft.) wide (see Landscaped Setback sketch);
- e) Although driveways should be minimized, where they create a break in the grass strip and hedging, reduce the visual impact of this break by edging at least the first 3 m (9.8 ft.) of any driveway (leading directly from these streets) with the grass strip and hedge described above;
- f) Along the North and East sides of Livingstone Place, and the North side of Comstock Road, provide privately-owned, publicly-accessible open spaces (P.O.P.A.S.) in the form of small seating areas approximately 2 m by 2 m (6.6 ft. x 6.6 ft.) in size. Provide benches and trash receptacles in these areas. These areas should be spaced approximately every 30 m (98.4 ft.). Divert hedging accordingly, and behind the hedging provide at least one flowering fruit tree, of rounded canopy and moderate to rapid-growing. Create a profusion of vegetation around these open spaces, by clustering taller shrubs and other understorey around the tree and behind the hedge (see P.O.P.A.S. sketch);
- g) Create a tree grove along Granville Avenue and Gilbert Road. Setbacks should be completely landscaped with plant materials;



Privately-owned Publicly-accessible Open Space (P.O.P.A.S.)



Tree Well

Tree Preservation

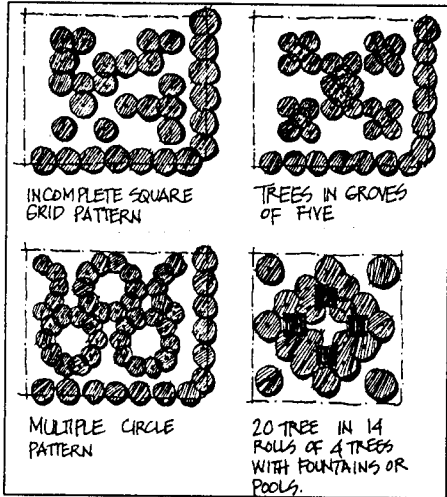
- h) A detailed survey of existing trees and vegetation should be conducted, following the requirements set out in Richmond's Tree Survey Bulletin;
- i) Take special efforts to preserve trees and mature vegetation. Plan open spaces and walkways with landscaping first. Then group buildings around the spaces. Most mature trees exist along Granville Avenue, Gilbert Road, and existing rear property lines. It should be possible to create a grove effect along these streets (large contiguous lines of mature trees/vegetation) through retention and enhancement of existing plant materials. Any new plant materials used should be indigenous or native species which require little maintenance;
- j) Tree wells may be used to facilitate retention of existing trees. Where these are used, they should be a minimum distance of 1.5 times the distance from the trunk of the tree to the drip line (see Tree Well sketch);

Common Open Space

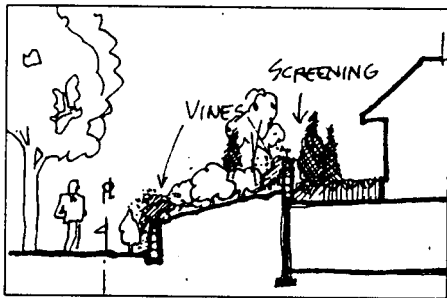
- k) Create open spaces that are usable and practical while also enhancing the streetscape of the neighbourhood;
- l) Special effort should be taken to create and coordinate the retention of contiguous existing mature vegetation on adjacent properties;
- m) Landscape the ground plane intensely with flower beds, flowering trees, shrubs and other plant materials which will foster a garden-like quality. Shrubs or small trees should be clustered between units or planted a minimum of every third unit in the front yards. Include species that will attract birds (which in turn enhance both biodiversity and create pleasant soundscapes). Examples of such plants include: crab apple, cherry, and dogwood trees; holly, serviceberry, elderberry, bayberry, honeysuckle and rose bushes. Where possible, plant tall columnar trees in side yards;
- n) Cluster trees to create "outdoor rooms" or to divide yards into smaller, more intimate areas for people to gather in while avoiding a heavily partitioned character (see Outdoor Rooms sketch);

Fences, Retaining Walls and Hedges

- o) Use hedges and fencing to demarcate private property while avoiding a walled-off effect.



"Outdoor Rooms" adapted by Arnold, Henry E. 1980. *Trees in Urban Design*



Soften Retaining Walls



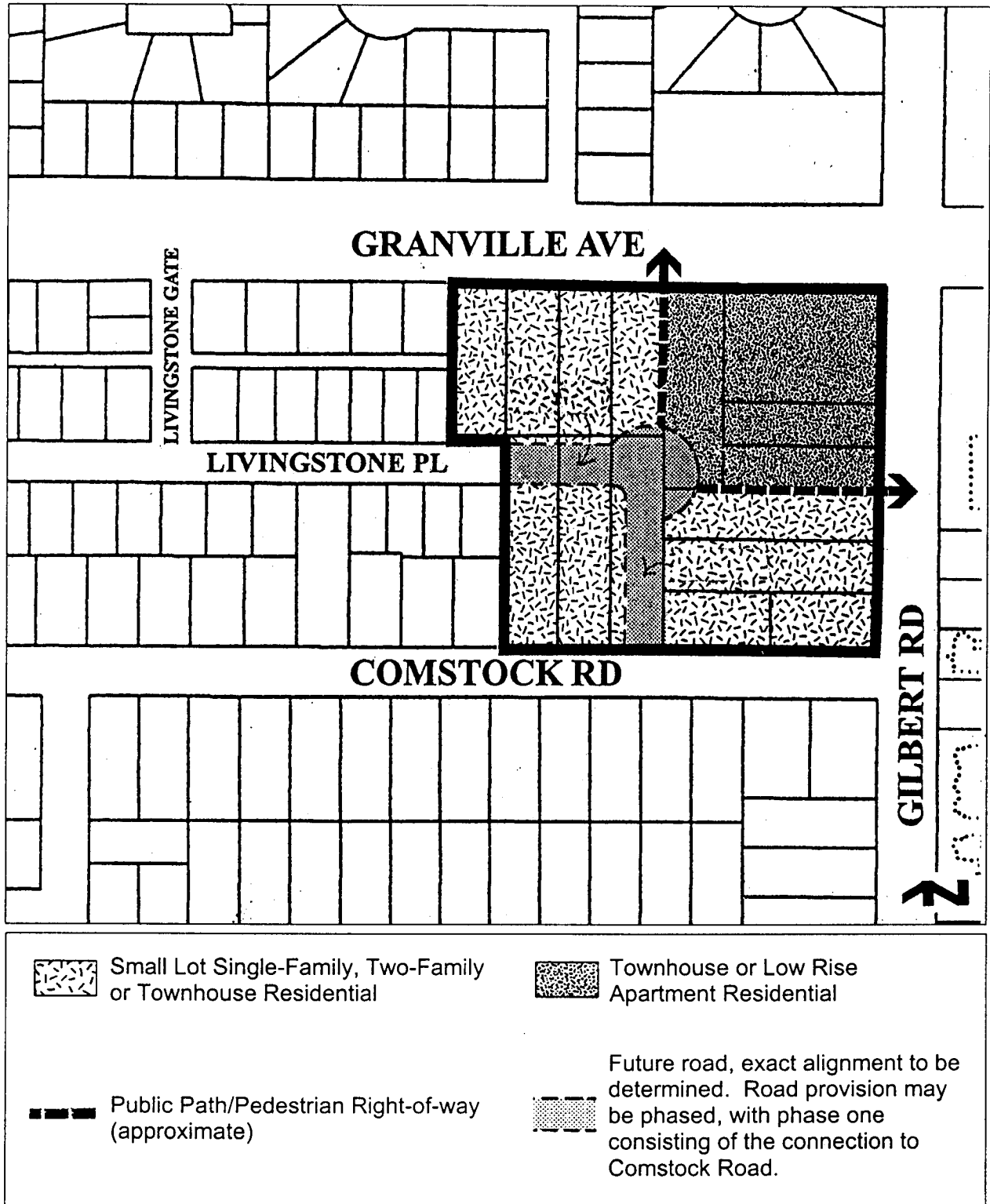
Screen Parking

- p) Fences are discouraged along Comstock Road or Livingstone Place, and may not project into 6 m (19.7 ft.) setbacks. If they are provided along these streets, fences should be no more than 1 m in height, and of a transparent quality, such as wood picket or wrought iron railing;
- q) Fences may be used along Granville Avenue and Gilbert Road, but should not be continuous. They should be screened with landscaping, routed around existing vegetation, and limited to a maximum height of 1.1 m (3.6 ft.). Fences may project up to 2 m (6.6 ft.) into the street setbacks, but may be no closer than 4 m (13.1 ft.) to the property line along these streets;
- r) Side yard fences are discouraged;
- s) Hedges and retaining walls may be used along Granville Avenue and Gilbert Road, and should be no more than 1.1 (3.6 ft.) in height along those streets. On apartment sites, retaining walls are encouraged as part of the earth-berming scheme. Where retaining walls are used they should be softened through the planting of with vines or other cover. Hedges are required along Comstock Road and Livingstone Place, but retaining walls along those streets are discouraged (see Soften Retaining Walls sketch).

8.2.5 PARKING AND SERVICES

- a) Minimize the surface area of black top parking and access driveways through alternate paving treatments;
- b) Resident parking should not be visible from the street. On townhouse sites, it should be covered or placed behind buildings and screened. Screening should be a minimum of 2 m (6.6 ft.) in length times 1.5 m (4.9 ft.) landscape in height planting or trellis strips. Trees should also be planted a minimum of one tree for every four parking stalls. In apartment developments, parking should be covered and concealed through earth-berming landscaped intensely with plant materials (see Screen Parking sketch).

Land Use Map



City Council Meeting - Jan 16, 2001

Richmond Official Community Plan



City of Richmond

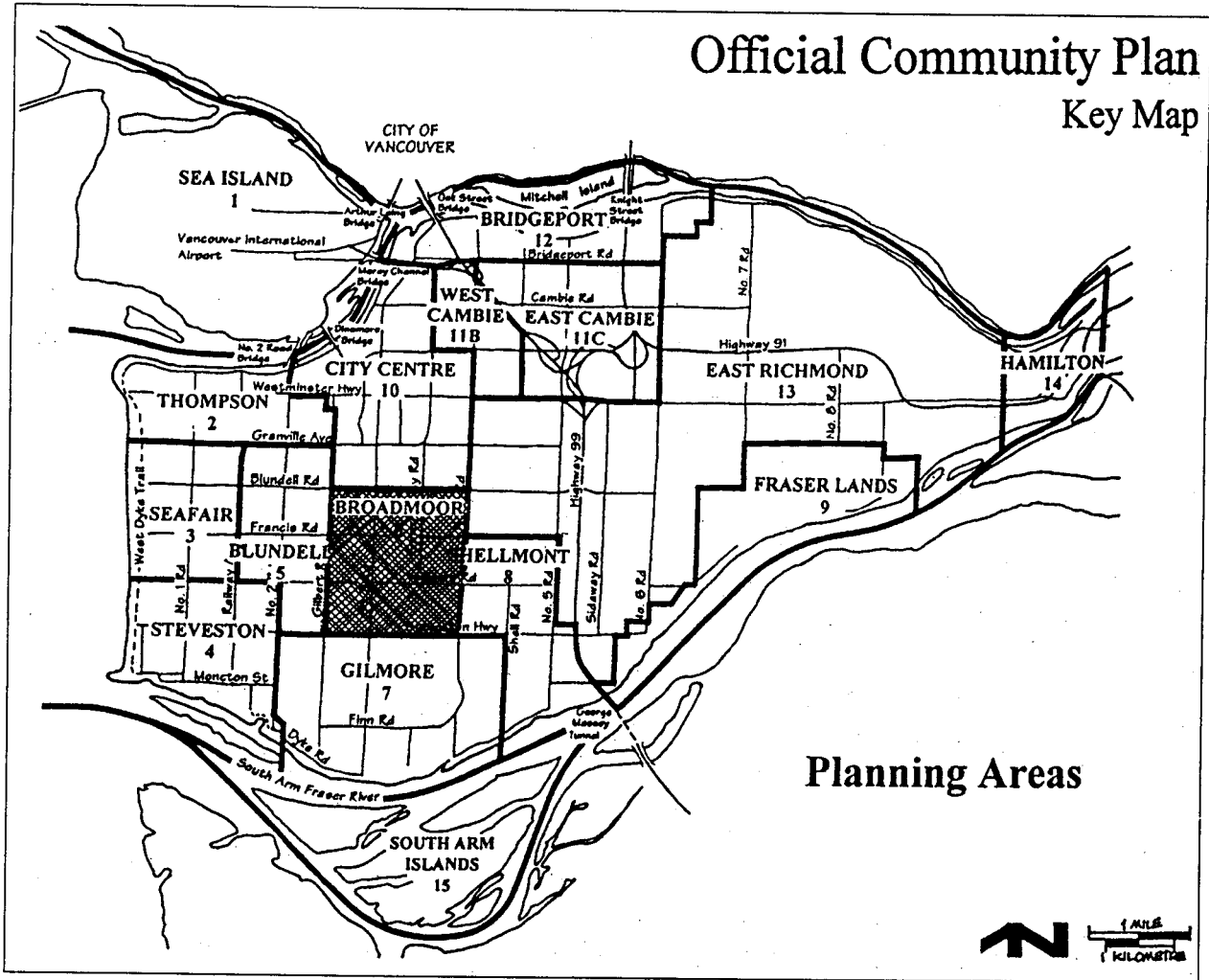
BROADMOOR AREA ASH STREET SUB-AREA PLAN

Bylaw 7100 Schedule 2.6A



280

KEY MAP



PLAN AREA MAP

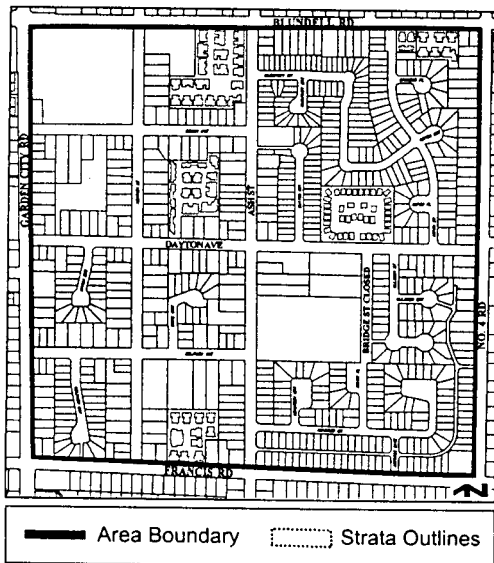


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PLAN INTERPRETATION

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- 1) Schedule 1: the overall OCP;
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Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City’s Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

The Ash Street Sub-Area is located in the Broadmoor Planning Area and is bounded by Blundell, Francis, Garden City and No.4 Roads.

The Ash Street Sub-Area is located in an area where residential growth has led to a need for an expanded sanitary sewer system, closed in ditches and improved roads.

This land use plan will allow for a more precise design of these City services.

The plan also includes projected needs for commercial, parks schools and other community facilities.

This sub-area plan sets out a goal for the area. From this a series of achievable and realistic objectives covering basic planning issues are formulated which together will attain the goal. Specific policy statements then detail the ways in which Council can achieve the objectives.

1.2 GOAL

The goal of the Ash Street Sub-Area Plan is to maintain and improve the existing residential community by upgrading the quality of services and facilities concurrent with new development.

2.0 JOBS & BUSINESS

OBJECTIVE 1:

To provide an opportunity for a community commercial area to serve the weekly shopping needs of residents of the immediate area.

POLICIES:

- a) Permit a community commercial area located as per the Land Use Map to serve the shopping needs of residents of the neighbourhood;
- b) Establish buffers along No. 4 Road (the urban/rural boundary) for the purpose of defining the urban/rural edge.

3.0 NEIGHBOURHOODS & HOUSING

3.1 RESIDENTIAL LAND USE



Single Family Development



Townhouse Development

ISSUE:

The trend of residential development in the Ash Street area has been one of gradually increasing densities.

The Ash Street area was first subdivided into half acre lots then in the late 1970s and early 1980s, two large subdivisions of 12 m (40 ft.) wide lots were built, and the character of the area began to change.

In 1984, for the first time, medium density residential housing was constructed in the area. This housing took the form of detached homes, two storey townhouses and zero lot-line units on small lots about 9 m (30 ft.) wide. This is a trend which is also occurring in other parts of Richmond, and provides more economical family accommodation than traditional large single-family residential lots. The trend is expected to continue in other parts of the Ash Street area, but with variations.

One characteristic of the existing residential neighbourhood is its age which is dramatically emphasized by the many fine old trees located around the homes and lining the streets. In preserving existing homes, care must be taken to also preserve as many of these trees as possible.

New forms of housing can be added to the community in a sensitive manner so that the scale of buildings and life styles of residents are compatible with those existing in the area.

This plan recommends forms of new housing which can be built utilizing the rear portions of 1/2 ac. lots, while still allowing the existing residents to retain their homes. This is sometimes referred to as "infill" housing. In most cases two alternative types of housing are recommended for each designated infill area as shown on the attached Land Use Map.

The population of the area at the time of plan adoption (1986) was about 1850 people, living in about 613 dwelling units.

OBJECTIVE 1:

To balance population with community facilities such as the school and park.

POLICIES:

- c) Allow development such that the population increase is commensurate with school and park capacity in the area.

OBJECTIVE 2:

To provide a range of housing types and tenures for a variety of households and age groups.

POLICIES:

- a) Enhance and protect existing single-family areas;
- b) Encourage residential infill development which is compatible with existing single-family areas;
- c) Encourage a variety of dwelling types and tenures by permitting the types and maximum densities indicated for each of the "infill" sites shown on the Land Use Map;
- d) Permit the use and development of lands outside of the "infill" sites shown on the Land Use Map to be governed by the City's normal development application process.



Small Lot Redevelopment

4.0 TRANSPORTATION

ISSUE:

The Ash Street Sub-Area is bounded by four arterial roads, Garden City, Blundell, No. 4, and Francis Roads. It is generally City policy that all traffic within the area should be local traffic only, and travel through the area for purposes other than local access should be discouraged. Satisfactory vehicular access should be provided to arterial roads without impeding the flow of vehicular traffic.

Heather Street, Ash Street and Dayton Avenue have been upgraded, in order to prevent their use for through traffic and to make the area safer for local residents.

In all cases, it is important that residents have equal opportunity to access from one part of the quarter section to any other part, for example to visit friends or to go to DeBeck Park, without using the arterial roads. This is both a transportation and a social consideration and will help maintain the cohesiveness of the neighbourhood.

Various measures have been identified on the Transportation Plan Map.

OBJECTIVE 1:

To provide an efficient transportation system which serves the area without channelling traffic from other sources through the neighbourhood.

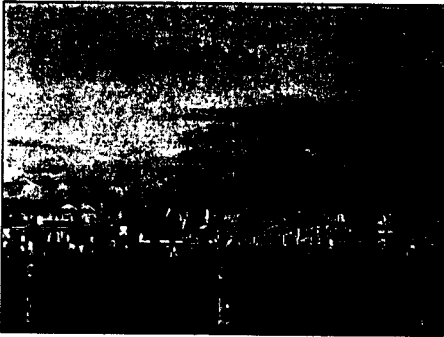
POLICIES:

- a) Maintain the integrity of arterial roads by providing access at appropriate locations;
- b) Discourage through traffic on Dayton, Heather, and Ash Streets by the use of barriers, cul-de-sacs or other means, as indicated on the Transportation Plan Map;
- c) Achieve a transportation system as shown on the attached Transportation Plan Map.



Traffic Barrier

5.0 NATURAL & HUMAN ENVIRONMENT



DeBeck School / Park Site

ISSUE:

As the population of the area grows, there will be an increasing need for parks and open space to meet leisure time needs of residents.

Three kinds of parks serve most Richmond areas: neighbourhood, community, and City facilities.

Because the Ash Street area is small, it has only one kind of facility; neighbourhood parks. Residents travel outside of the area for other recreational opportunities. The closest community park is South Arm Park.

Residents have expressed a need for mini-parks or open spaces in each infill area which is developed for medium density housing, to provide a play space for small children and green space with seating for adults.

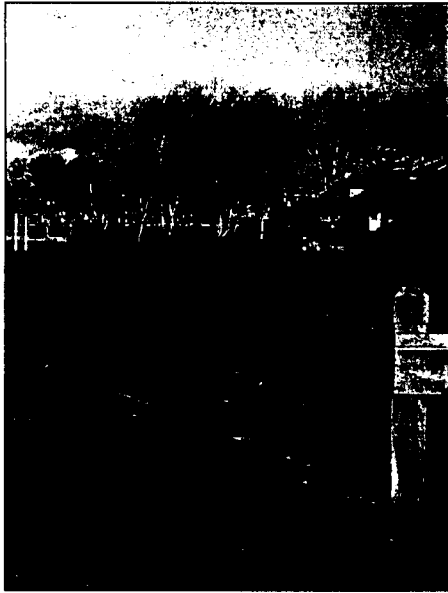
OBJECTIVE 1:

To provide open space and parks for the benefit of area residents.

POLICIES:

- a) Maintain and enhance the DeBeck Park site;
- b) Add adjacent properties to the DeBeck Park site as City finances permit;
- c) Provide for the open space needs of small children and adults in each block where medium density residential infill is developed, by providing a small play area with landscaping and benches.

6.0 COMMUNITY FACILITIES & SERVICES



DeBeck School / Park Site

ISSUE:

Most of the community service and facility needs of Ash Street residents are met outside of this sub-area, however as the population grows more of these needs can be met within the area.

OBJECTIVE 1:

To ensure that a range of community facilities and services are made readily available to residents of the area.

POLICIES:

- a) Encourage the Richmond School Board to retain and utilize the DeBeck site for an elementary school to serve children in the catchment area;
- b) Encourage the existing group home to continue to serve the community;
- c) Encourage the provision of additional space for childcare.

7.0 CITY INFRASTRUCTURE

ISSUE:

When the first half acre lots were subdivided and the first homes were constructed in the Ash Street Sub-Area many years ago, public utilities were minimal. Because servicing in new subdivisions now includes sanitary sewers, storm sewers, curbs, gutters, sidewalks and underground wiring, the level of expectation for services in older areas is rising. Problems with drainage and septic tanks in older areas have also led people to request improvements to their utilities.

As infill development occurs it will be possible to economically install better public utilities for the surrounding areas. Residents would prefer to have sanitary sewers, especially in areas where the sub-soil may contain peat, and where septic tile fields may become flooded during periods of high water table. The installation of sanitary sewers to service half acre lots is not economical. Alternatively, the acquisition of a sewer easement at the rear of existing homes may be difficult until land assembly has occurred prior to infill development.

OBJECTIVE 1:

To improve the quality of public utilities concurrent with development and population growth.

POLICIES:

- a) Install sanitary sewers to serve the Ash Street area, as redevelopment occurs;
- b) Install a sanitary sewer system for the area north-west of Dayton Avenue and Ash Street concurrent with development, and in cases where it is impractical to wait for residential infill to occur, install sanitary sewers prior to development of new homes;
- c) Take necessary measures to maintain public health and safety in older areas.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands which are adjacent (within 30 m (98.4 ft.) to the edge of the Agricultural Land Reserve (ALR) for the purpose of protecting farming (development permits are not required for construction of single family dwellings)).

It is intended that these guidelines be used in conjunction with the City's more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to protect farming on the adjacent ALR lands.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

- a) All development across a road from sites designated as Agricultural Land Reserve (ALR) must provide a minimum 5 m (16.4 ft.) landscaped strip as measured from the back of curb.

8.2.2 LANDSCAPE ELEMENTS

- a) The landscaped strip should reinforce the character of agricultural lands, including large-scale trees, a combination of shrubs and ground cover, and where appropriate, a sidewalk.

Transportation Plan Map

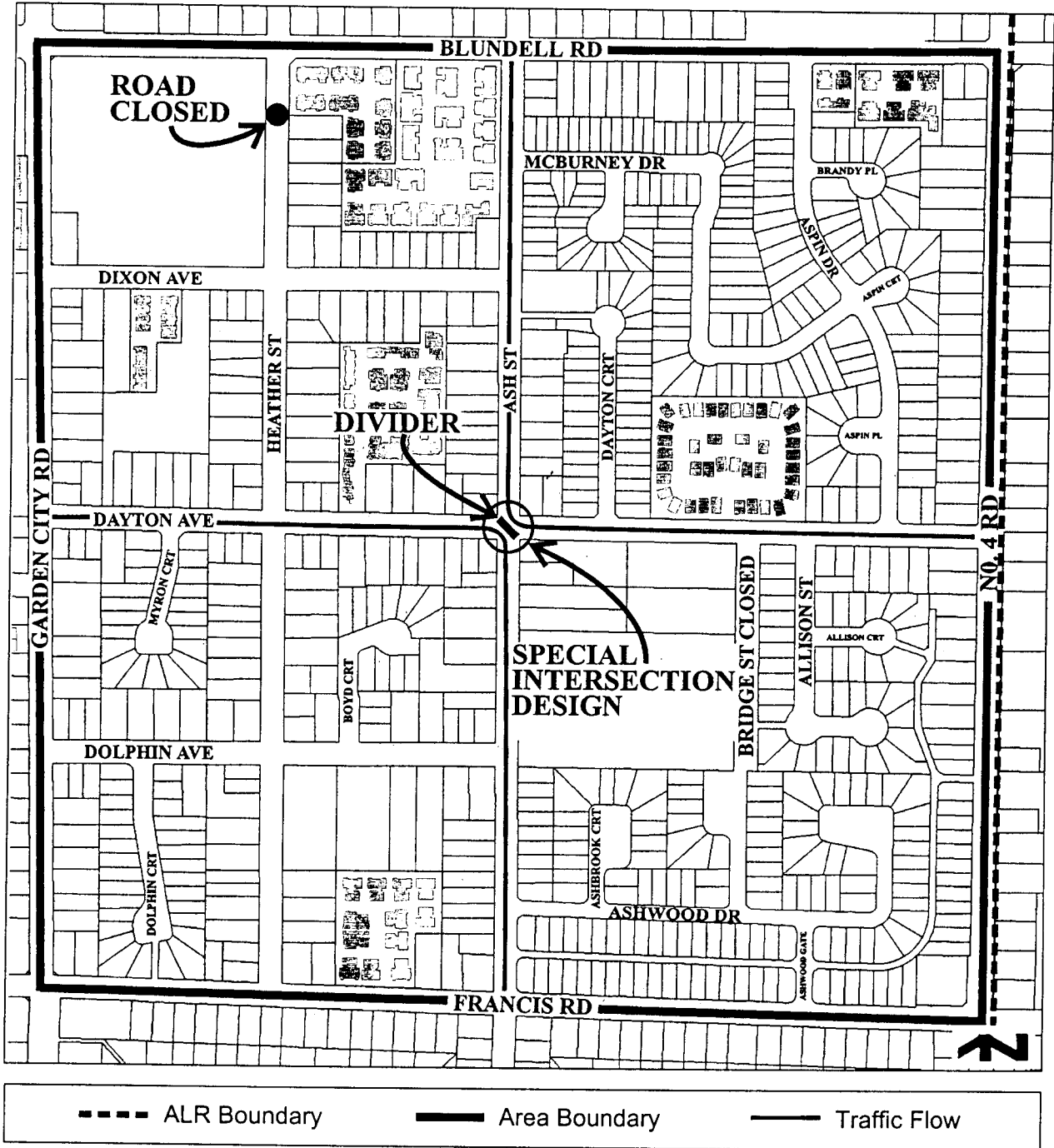
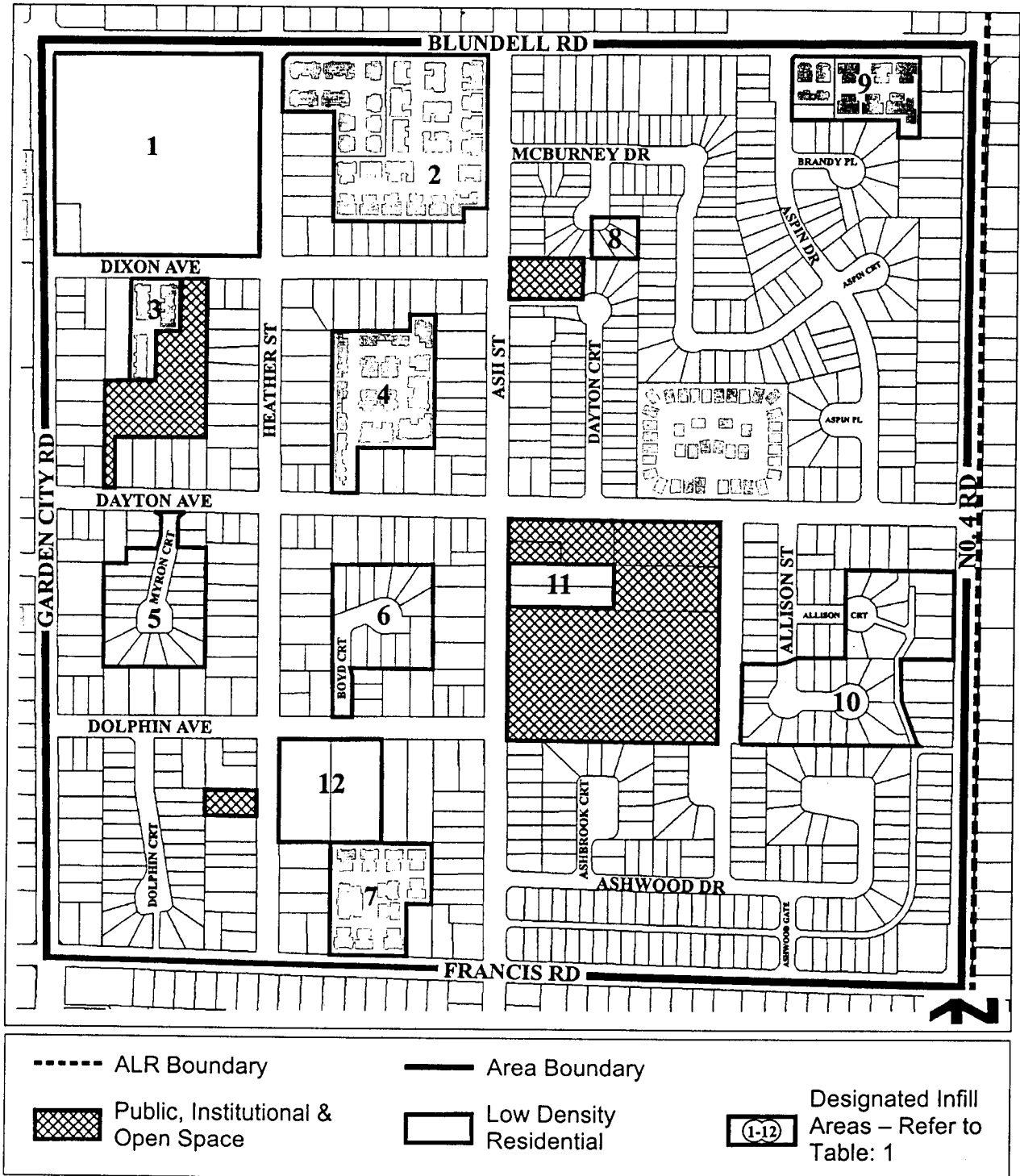


Table 1:

Official Community Plan - Specific Infill Land Use Designations Ash Street (Section 22-4-6)					
KEY TO AREA PLAN MAP	APPROXIMATE AREA HA (AC.)	DENSITY UNITS/HA (UNITS/AC.)	MAXIMUM NUMBER OF UNITS	LAND USE	OTHER
1	24 (6)		N.A.	Commercial or townhouses	Maximum two-storey height
2	1.86 (4.61)	29 (12)	77	Townhouses or small lots	Open space and children's play area with townhouses or small lots
3	0.502 (1.25)	*.55 FAR	*.55 FAR	Townhouses or duplex	Open space and children's play area with townhouses
4	1.07 (2.64)	35 (14)	37	Townhouses or small lots	Open space and children's play area with townhouses or small lots
5	.95 (2.34)	18 (7)	18	Small lots	Open space and children's play area
6	.81 (2)	18 (7)	14	Small lots	Open space and children's play area
7	0.830 (2.05)	29 (12)	25	Townhouses or small lots	Open space and children's play area
8	0.12 (0.3)	18 (7)	3	Small lots	
9	0.645 (1.6)	19 (12)	24	Townhouses or small lots with lane	Access to Blundell restricted. Open space and children's play area. Possible lane.
10	1.8 (4.45)	18 (7)	31	Small lots or large lots	
11	#	N.A.	N.A.	Large lots or park	Open up corner for public view of DeBeck school/park
12	#	N.A.	N.A.	Park or small lots	
TOTAL			246		

Note: FAR = Floor Area Ratio

Land Use Map



Richmond Official Community Plan

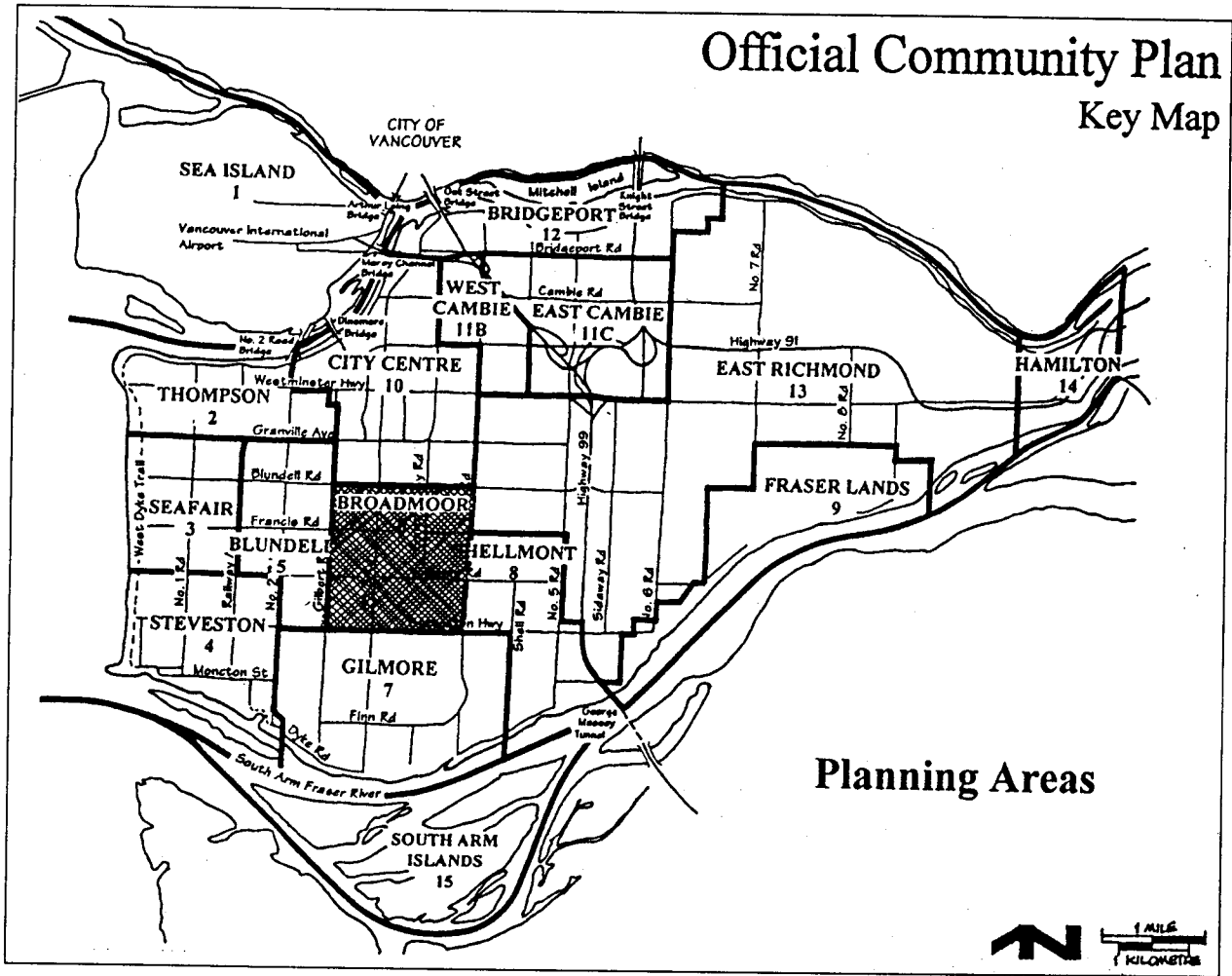


City of Richmond

BROADMOOR AREA CENTRAL WEST SUB-AREA PLAN Bylaw 7100 Schedule 2.6B



KEY MAP



PLAN AREA MAP

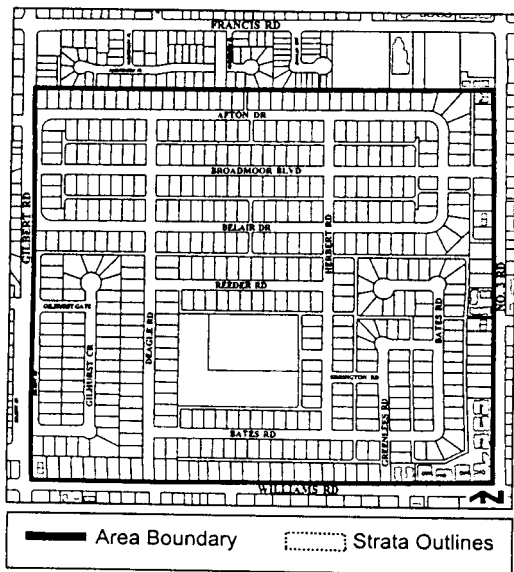


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This plan applies to the area shown on the Plan Area Map.

The Central West Sub-Area is bounded by Williams Road to the south, No. 3 Road to the east, Gilbert Road to the west and to the north side of Afton Drive.

This plan contains the goals, policies, Development Permit Guidelines and land use designations for the Central West Sub-Area.

1.2 GOALS

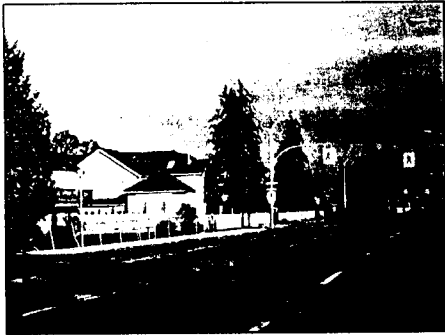
To create a livable and stable, family oriented residential neighbourhood while consolidating a distinct urban corridor with strong linkages to adjacent neighbourhoods outside Central West Broadmoor, on the neighbourhood's eastern perimeter.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING

3.1 HOUSING AND NEIGHBOURHOOD CHARACTER



Multi and Single-family Homes along No. 3 Road.



Pedestrian Connections

POLICIES:

- a) Maintain the large lot qualities of the neighbourhood while providing a range of housing types on the eastern perimeter that can accommodate a variety of families and households;
- b) Strengthen the identity of the No. 3 Road corridor as part of an emerging neighbourhood village;
- c) Ensure that multiple-family residential is compatible with adjacent single-family neighbourhoods;
- d) Foster a green neighbourhood by encouraging the retention of trees on single-family properties and requiring tree retention or replacement on multiple-family properties;
- e) Ensure that the neighbourhood is well-connected by maintaining existing pedestrian connections from the centre portion of the neighbourhood through to No. 3 Road, and by providing additional pedestrian and cycling connections as opportunities arise. Ensure that these paths are attractive and safe.

4.0 TRANSPORTATION

4.1 CIRCULATION

POLICIES:

- a) Ensure that vehicular access to new developments from No. 3 Road is limited;
- b) Provide vehicular access to single-family and two-family residential developments through lanes. Where possible, also use lanes to provide vehicular access to multiple-family development, to minimize the visual and physical impact of the automobile on neighbourhood streets and the pedestrian realm;
- c) Improve lane access to No. 3 Road at mid-block. Extend the lane west of Bates Road through the City-owned property directly west of 9511 No. 3 Road;
- d) Improve traffic safety on neighbourhood streets. Place stop signs in appropriate locations along Broadmoor Boulevard and consult with residents on additional traffic safety measures in other neighbourhood locations.

5.0 NATURAL & HUMAN ENVIRONMENT

5.1 PARKS AND OPEN SPACE

POLICIES:

- a) Improve access to recreational activities. Maximize the use of park space outside the neighbourhood by making access to these parks safer, through crosswalks and other measures.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated “Townhouse Residential” on the Land Use Map.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Broadmoor-Central West area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

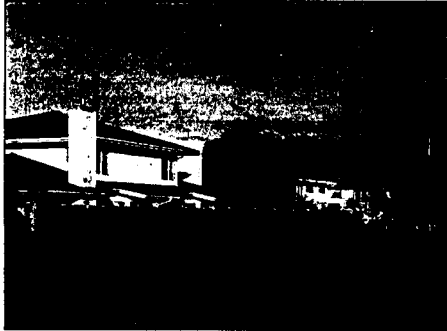
It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in order to fulfil the sub-area plan goals and objectives. Specifically, the goal of ensuring compatibility between multiple-family and existing single-family neighbourhoods needs to be addressed through design guidelines.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

- a) Ensure compatibility with single-family housing across the lane and to the north through complementary scale, massing and building form;
- b) Each dwelling should have a near, middle, and distant view;

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Limit Access to No. 3 Road through the Use of Lanes

- c) Provide a quiet and livable residential environment by reducing the impact of traffic noise on residential units. All developments on properties adjacent to No. 3 Road must provide a minimum 6 m (19.68 ft.) setback from this road or provide other remedial measures acceptable to staff;
- d) Minimize the visual presence of, and land occupied by, motorized vehicles on sites:
 - Cover or screen resident parking from the street and encourage tandem parking;
 - Limit access driveways to 3 m (10 ft.) width and combine them where possible;
- e) Reduce parking visibility from the street by discouraging placement of garages facing No. 3 Road. Where this is not possible, driveway access to garages must be treated exclusively with special pavers and embellished with ornamental shrubs and trees;
- f) Use measures to ensure that vehicular access from No. 3 Road does not endanger or inconvenience pedestrians or the mobility impaired;
- g) Improve No. 3 Road as a pedestrian thoroughfare, and enhance its visual interest through the built environment;
- h) Enhance safety on public pedestrian paths, by orienting windows and doors towards these paths and limiting fences and hedges along such paths to 1.1 m (3.6 ft.) in height;
- i) Recognize that significant pedestrian activity will also occur on lanes, and improve their appearance and safety.

8.2.2 MASSING AND HEIGHT

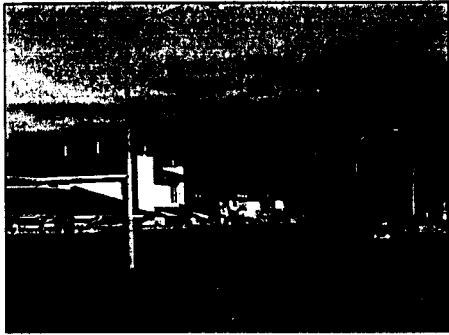
- a) Encourage the orientation of front doors and windows towards No. 3 Road. Avoid facing blank building walls onto the street;
- b) Buildings should be set back from streets and open spaces in a manner that promotes continuity of local street definition, landscape, and residential character.

8.2.3 ARCHITECTURAL ELEMENTS

- a) Provide richer detailing, high quality, durable materials and special lighting which will enhance the pedestrian's close-up view of buildings along No. 3 Road;
- b) Encourage the use of sloped roof forms and visually interesting, high quality roofing materials. Heavy tiled roofing is not appropriate;
- c) Discourage the treatment of buildings with decorative patterned stucco, imitation brick, or vinyl siding;
- d) Clearly define and number entrances to each unit.



Sloped Roofs and Detailing



Encourage Retention of Mature Trees

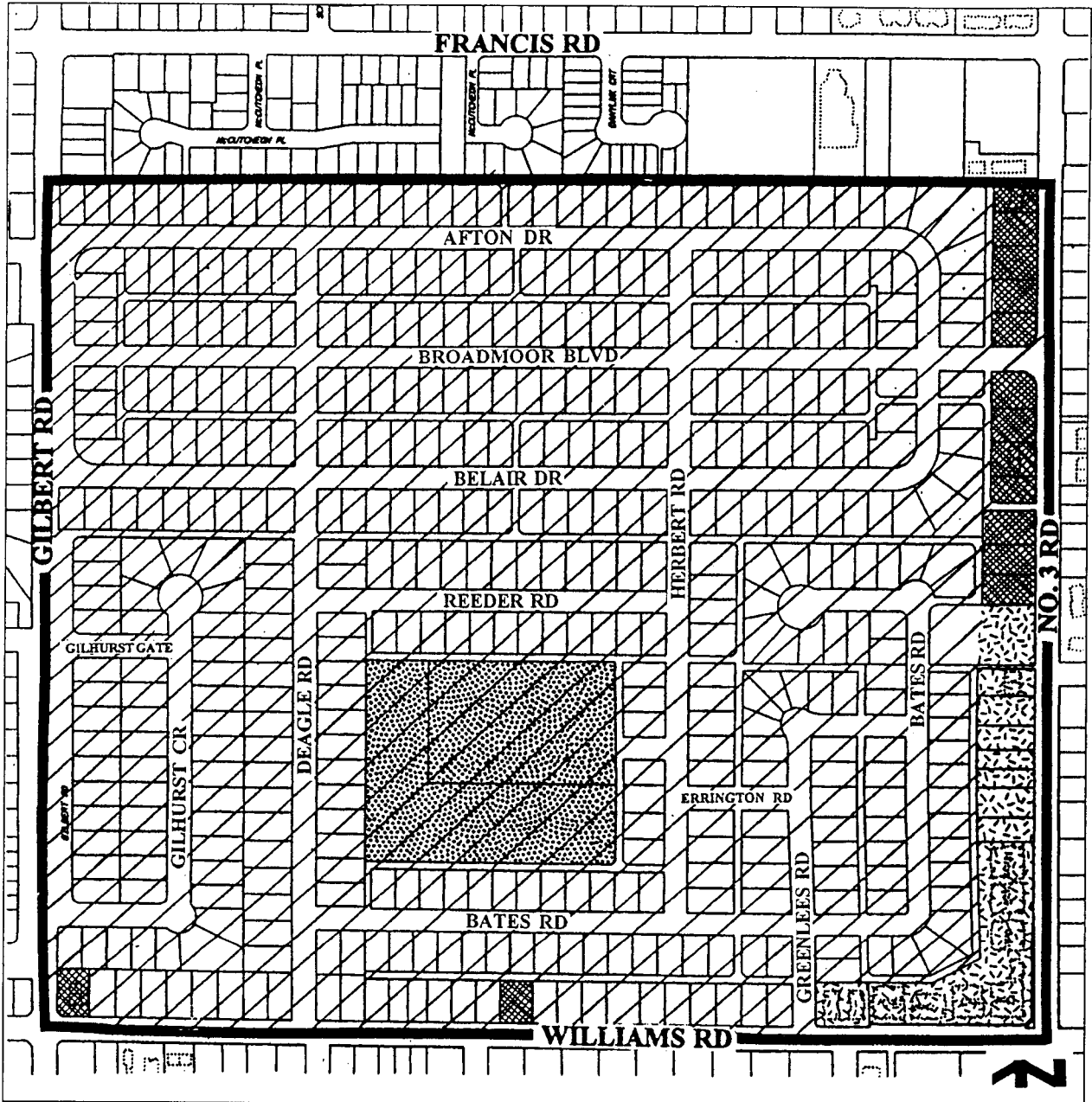
8.2.4 LANDSCAPE ELEMENTS

- a) Reinforce the natural environment on private property:
 - Retain existing trees and mature plants, and augment them with ornamental flowering trees and shrubs;
 - Provide each dwelling unit with its own private outdoor space, and clearly demarcate this space at ground levels;
 - Permit no more than 75% of a site to be covered by buildings and non-porous surfaces. A minimum of 25% of the site must be covered with soft landscaping including plant materials;
- b) Minimize the surface area of blacktop parking and access driveways through alternate paving treatments;
- c) Use special landscaping features, such as trellises, to conceal garages and visitor parking stalls from surrounding private, semi-private and public areas;
- d) Fences and hedges along No. 3 Road should be limited to 1.1 m (3.6 ft.) in height;
- e) Provide pedestrian-oriented lighting along lanes and internal roadways on private developments;
- f) Include landscape materials, complete with necessary space, soil depth and irrigation.

8.2.5 PARKING AND SERVICES

- a) Provide appropriate garbage facilities. Erect a covered, gated structure to contain residents' garbage and recycling materials. The design of this structure should complement the design of units in the project;
- b) Place all utilities underground;
- c) Screen garbage facilities and related uses.

Land Use Map



Small Lot Single-Family (R1/B)



Large Lot Single-Family (R1/E)



Townhouse (R2) Residential

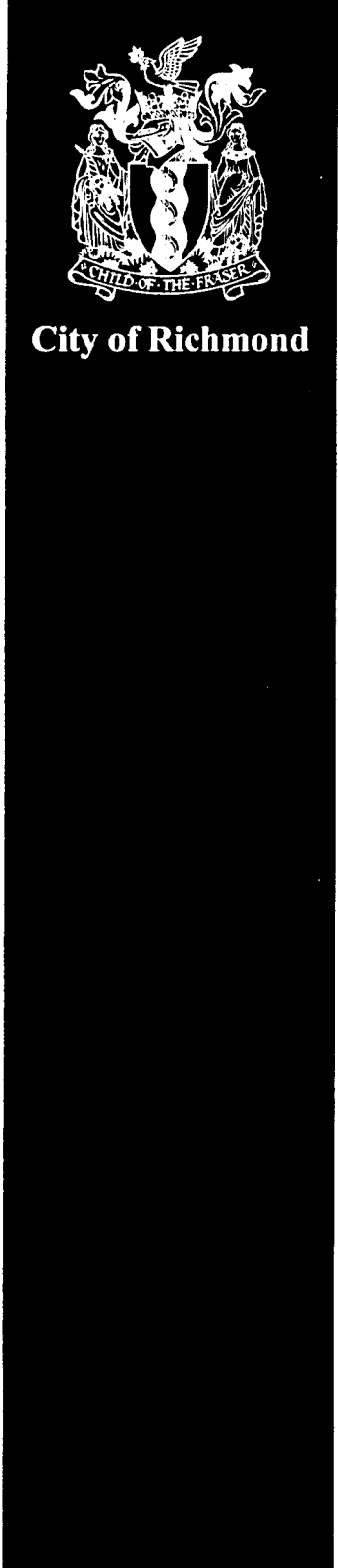


Public and Open Space Use



Strata Outlines

Richmond Official Community Plan

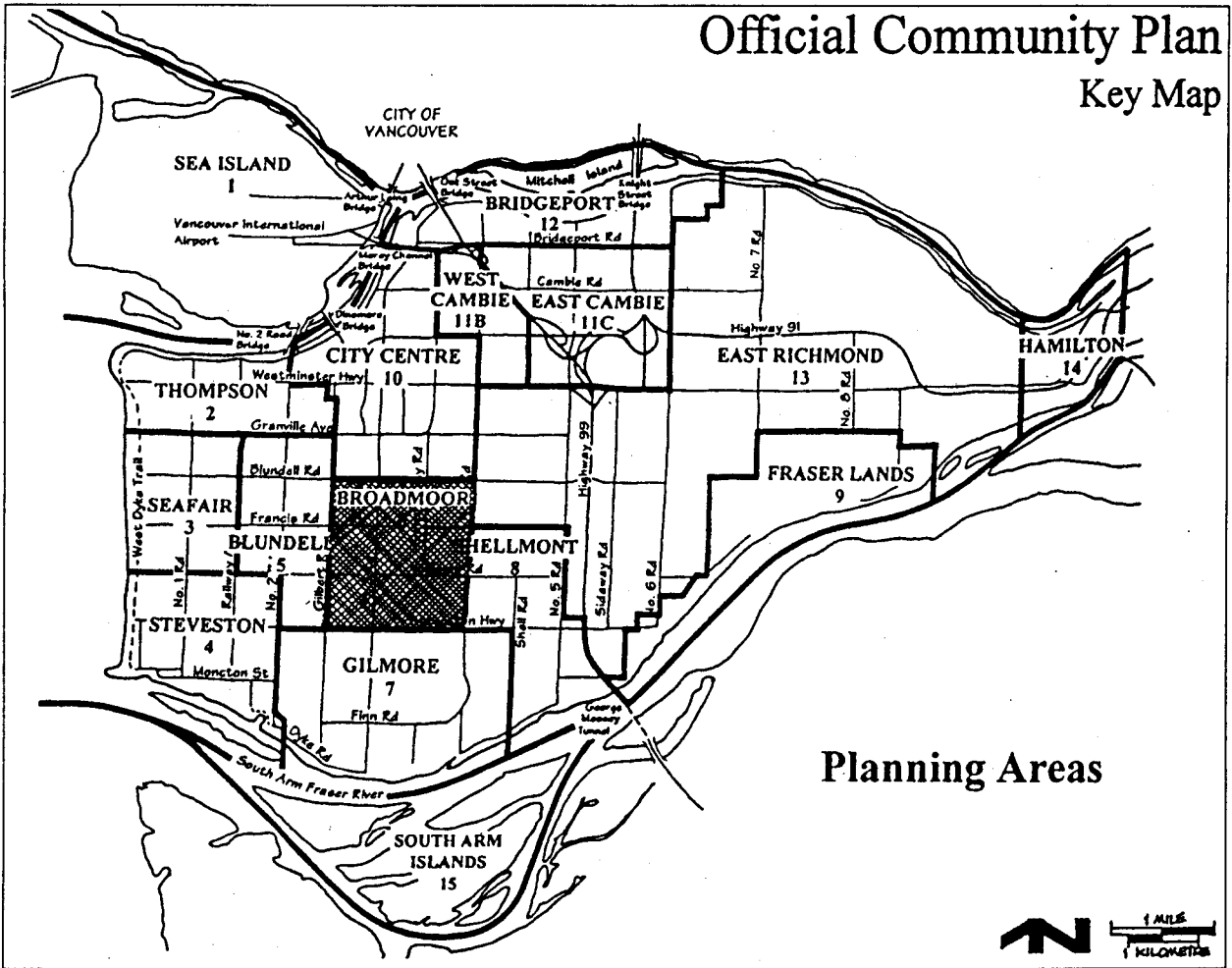


BROADMOOR AREA
SUNNYMEDE NORTH
SUB-AREA PLAN
 Bylaw 7100 Schedule 2.6C

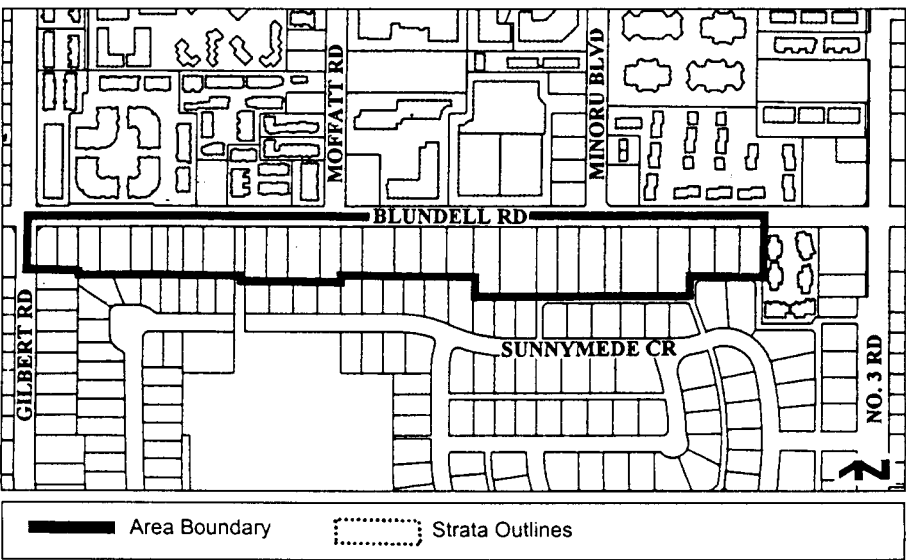


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KEY MAP



DEVELOPMENT PERMIT AREA MAP



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PLAN INTERPRETATION

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The OCP is a legal community planning document for managing the City’s social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

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Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City’s Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

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1.0 PLAN OVERVIEW

See OCP.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING

See OCP.

4.0 TRANSPORTATION

See OCP.

5.0 NATURAL & HUMAN ENVIRONMENT

See OCP.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the Sunnymede North Sub-Area as shown on the Development Permit Area Map.

The purpose of the guidelines is to supplement the City-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Sunnymede North area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

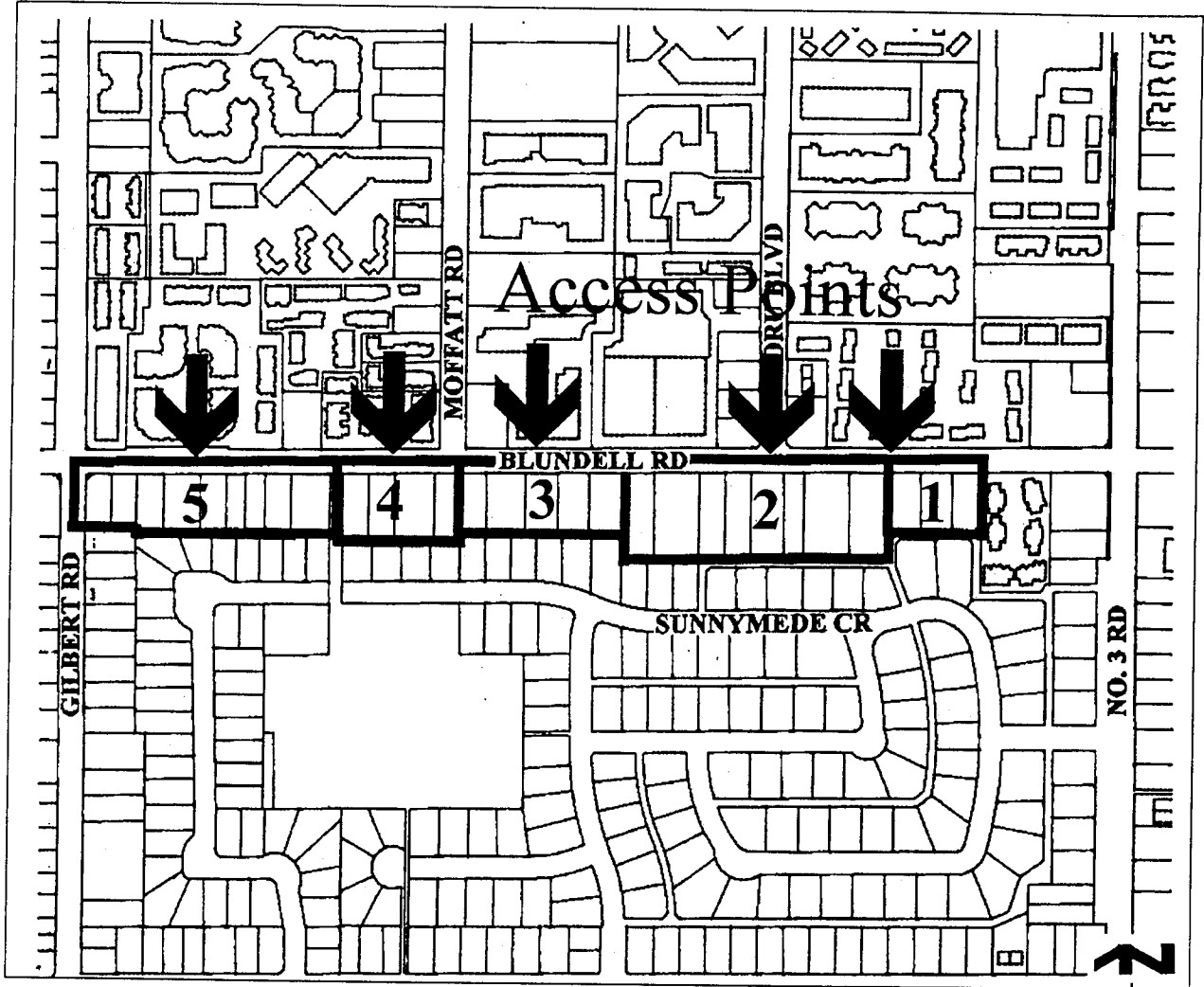
Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in the Sunnymede North Sub-Area.

There is a need to designate this area for special conditions in order to control access onto Blundell Road as well as to exercise greater control over the form and character of development to ensure a harmonious transition between the medium density residential district to the north in the City Centre and the low density single-family area to the south in Broadmoor.

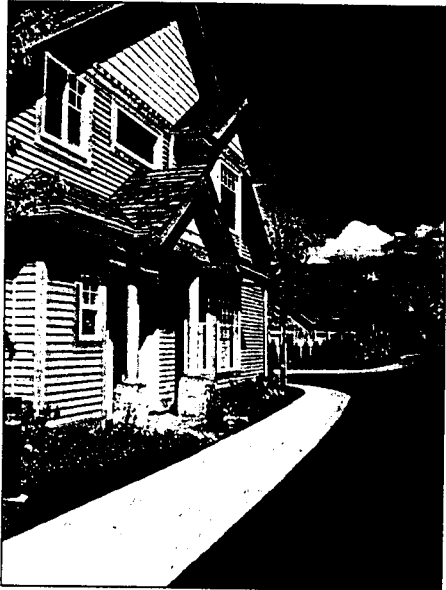
Access Points Map



8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

Building Treatment



Internal Roadway

- a) Design buildings with a main orientation towards Blundell Road and the internal roadways of the projects. Fenestration in facades facing the internal roadways and open spaces should be carefully arranged to provide for internal unit privacy, yet convey a perception of "eyes of the street" enhancing safety and security for residents;
- b) Create highly visible and identifiable principal building entrances through the use of landscape and prominent architectural components such as gateways and covered porches to emphasize the transition from public and semi-private spaces to each unit's private space. Entrances should provide protection from the elements of the weather. Use low level landscaping and/or consider the use of changes in grade to ensure the privacy of an individual unit's private open space without the use of high fences. (The grade change should be no more than .61 m (2 ft.) to .91 m (3 ft.) and should occur between the curb and the main door of each unit);
- c) Stagger units along main east-west internal roads;

Pedestrian Treatment



Prominent Building Entrance

- d) Establish clear and appropriate pedestrian connections between private residences and semi-public areas;
- e) Ensure that the units located directly adjacent to Blundell Road have their front doors facing Blundell Road with a pathway connecting from the sidewalk to each doorway. All of the other units in each development should have their front doors facing the internal roadways;
- f) Locate secondary entrances from private residences onto common walkways located between developments;
- g) Provide internal walkways extending from Blundell Road through each development area to the internal east-west road systems. Define the entrances to the internal north-south walkways by providing arrival plazas. The arrival plazas should consist of the following elements: widened hard surfaces with distinctive paving patterns, arbours, and accent planting;
- h) Use walkways, rather than fences, to separate the development areas. Provide low level landscaping and lighting on both sides of the walkways;

Vehicular Treatment

- i) Minimize the number of vehicular access points from Blundell Road as shown on Attachment 2.
- j) Consider the use of traffic circles or landscaped medians to visually and physically direct people to the separate projects included in each development area;
- k) No gates are permitted at the main vehicular accesses to the internal roadways or between projects within each development area;
- l) Use "gentle" tangent curves along internal road systems to create more interesting roadways;
- m) Traffic noise should be screened from residential units in order to maintain acceptable ambient indoor sound levels. All Development Permit applications shall require evidence in the form of a report and recommendations prepared by a person trained in acoustics. This report shall demonstrate that noise levels in a weighted 24-hour equivalent sound level (defined simply as noise level in decibels) will not exceed:
 - bedrooms – 35;
 - living, dining and recreation rooms – 40;
 - kitchen, bathroom and hallways – 45.

8.2.2 MASSING AND HEIGHT

- a) Reduce the scale of building forms adjacent to semi-private open spaces, pedestrian walkways, main vehicular entrances and adjacent to the Sunnymede single-family neighbourhood to the south through the use of secondary roof elements of a human scale such as covered porches or trellis elements;
- b) Units should have their massing designed to provide articulation to the streetscape both vertically and horizontally. Flat street fronts are unacceptable, as are continuous cornice lines;



Secondary Roof Elements

8.2.3 ARCHITECTURAL ELEMENTS

- a) Incorporate human scale elements (windows, doors, roof elements, trellis, etc.) into the building facade visible from the street;
- b) Ensure that balconies to upper floors facing Blundell Road are small and cantilevered, giving minimal exterior access. Their design should be treated like a decorative exterior element enhancing the building's articulation and massing;

Roofscapes

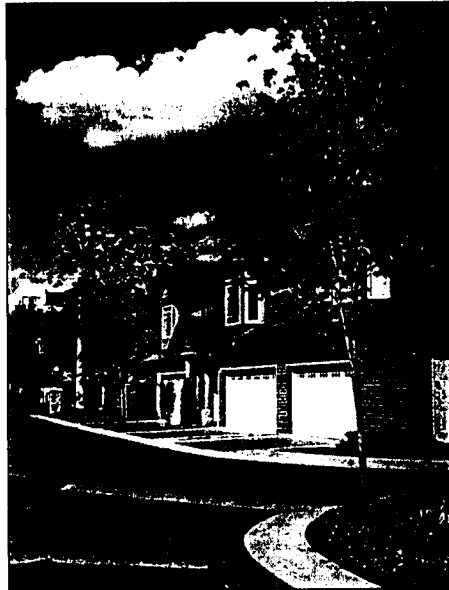
- c) Construct pitched roofs of various heights with a minimum slope of 6 to 12 with gable ends and dormers to create diversity;
- d) Provide soffits with overhangs to add interest and help protect the siding of the buildings;
- e) Select roofing materials which are suitable for the level of articulation desired in the roof forms. Cedar shingles would facilitate this, whereas heavy-tiled roofing would not;

Exterior Finish

- f) Use natural building materials such as wood, stone, brick or stucco on building exteriors. Unacceptable finish materials include imitation brick and highly patterned stucco;
- g) Employ a combination of two exterior materials on facades to help maintain a human scale, create texture and enhance design;
- h) Account for the effects of the Richmond climate in the choice of material and detailing. Stucco should be treated to prevent discolouration and particular care should be taken in detailing the north facades facing Blundell Road;
- i) Use a wide range of natural earth tones commonly produced as "heritage series" by a number of commercial paint manufacturers on the main exterior of the buildings. Compliment and coordinate the range of colours used on the various units with accent colours on the trim. Reserve the use of vivid colours to "highlighting" areas such as the trims and doors;
- j) Use decorative trim, cornices, reveals and projections where appropriate;

Windows

- k) Express a residential character by using a variety of appropriately scaled and proportioned windows;
- l) Articulate building facades through the use of various forms of specialty window projections such as sky lights, bay windows, and dormers to achieve design interest and improved interior light;
- m) Encourage the provision of window treatments such as flower boxes and shutters;
- n) Orient windows of living spaces towards pedestrian walkways and streets to help facilitate supervision of the street and increase pedestrian safety;



Garage Doors

Garages

- o) Incorporate front door entry features that extend 0.61 m (2 ft.) beyond the garage doors;
- p) Minimize the width of garage doors to a maximum of 4.87 (16 ft.) x 2.13 m (7 ft.);
- q) Incorporate decorative architectural treatments, such as windows, on and above garage doors that are complementary to unit finishes;
- r) Ensure that the garage door encompasses no more than 60% of the building width as visible from the internal road systems;
- s) Where units have both the front and back facade fronting a public road or pedestrian access route, ensure that:
 - No more than two garage doors are placed in a row without interrupting with a portion of the building or side yard equal to the width of one garage door;
 - No building block has more than six garage parking spaces;
- t) Where units have only one facade fronting an internal road system road or pedestrian access route, ensure that:
 - For 50% of the units, no more than one garage door is placed in a row where units are attached without interrupting with an amount of building or side yard equal to the width of one garage door;
 - No building block has more than four garage parking spaces.

8.2.4 LANDSCAPE ELEMENTS

- a) Provide each unit with a private outdoor space that is at least 37 m² (398.28 ft²) in area and 5.25 m (17.22 ft.) in depth;
- b) Design decks and patios as natural extensions of each unit into the landscape. A maximum of 40% of each unit's private space may have a deck, with the remainder in soft landscaping including grass, shrubs and trees. The provision of decks and patios developed at a maximum of 45.72 cm (18 in.) above grade are preferable to second-storey balconies on units located adjacent to the existing Sunnymede single-family neighbourhood;
- c) Separate each unit's rear private open space with a fence no higher than 1.8 m (6 ft.). Consider the importance and the safety of small household pets by ensuring that a gap no larger than 5.08 cm (2 in.) is provided between the ground and the base of the fence;



Private Outdoor Space

- d) Create visual focal points by providing landscaped areas at both the curves of the internal roads, and at the "dead-ends" of each of the development sites east-west internal road. Allow for future pedestrian access to adjacent development areas within the landscaped areas at the east-west road ends;

Tree Planting

- e) Use changes in paving materials such as decorative pavers or cobblestones near entrances to walkways and children's play areas;
- f) Soften hard surfaces through the use of landscaping, trellises and tree planting;
- g) Install pedestrian-oriented lighting along the internal roadways of the developments.
- h) Incorporate mature trees and landscaping into the development area. Whenever a major tree is removed, the replacement ratio should be 2:1. The replacement trees should have a minimum calliper of 15 cm (5.9 in.) in diameter;
- i) Enhance the main shared vehicular/pedestrian entry into each of the development areas by planting large specimen trees and accent planting without obstructing sightlines. Trees should frame the main entry driveways as seen from Blundell Road to each of the development area's internal road system;
- j) Plant large feature trees with a minimum calliper of 150 mm (6 in.) measured 1.4 m (4.7 ft.) above grade along with appropriate low level accent planting in the middle of traffic circles or medians;
- k) Plant street trees spaced no more than 9 m (29.52 ft.) apart within landscaped boulevards along the internal roadways;
- l) Separate the individual garages with landscaped areas including trees with a minimum calliper of 100 mm (4 in.) measured 1.4 m (4.7 ft.) above grade and/or with the careful siting of the residential portions of the buildings;



Mature Tree

8.2.5 PARKING AND SERVICES

- a) Locate enclosed garbage container buildings away from pedestrian accesses and children's play areas. The area should be easily accessible for collection purposes and should provide adequate manoeuvring space for 13.7 m (45 ft.) long garbage trucks. A covered recycling area 2.4 m (7.9 ft.) by 3.5 m (11.5 ft.) should also be provided near garbage areas. It should be wheel-chair accessible;

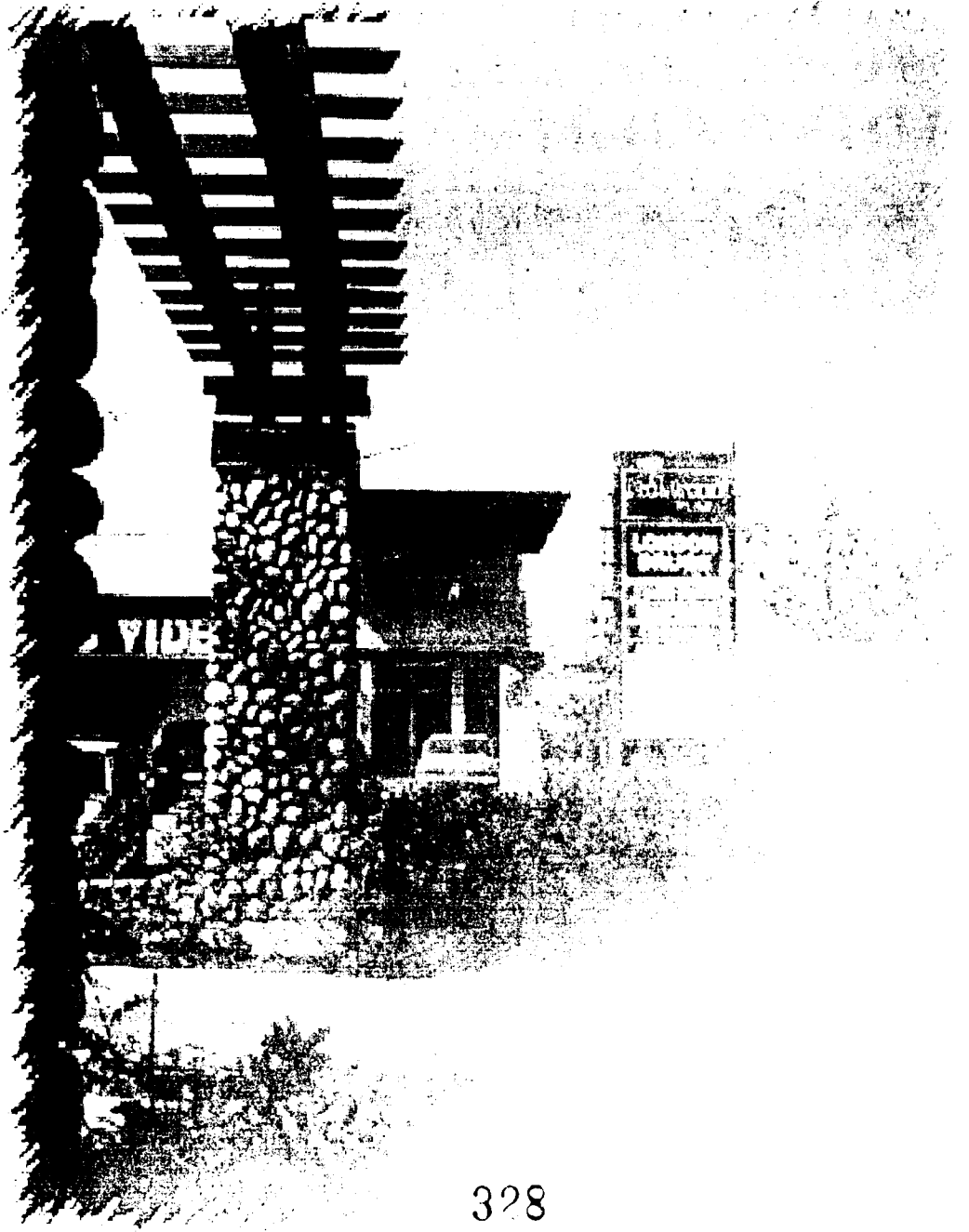
- b) Locate a highly visible amenity space in each development area along with an indoor amenity space in projects which include more than 70 bedrooms.
- c) Locate children's play apparatuses and benches within the open space along with other urban design features such as gardens, fountains, arbours and art.
- d) Locate the outdoor amenity area to take advantage of sun and natural shelter from the elements of the weather.
- e) Ensure that surveillance of the area is provided from adjacent units and the area has barrier free access.
- f) Provide a mixture of hard-surfaced and natural landscaping in the outdoor amenity area.

Richmond Official Community Plan



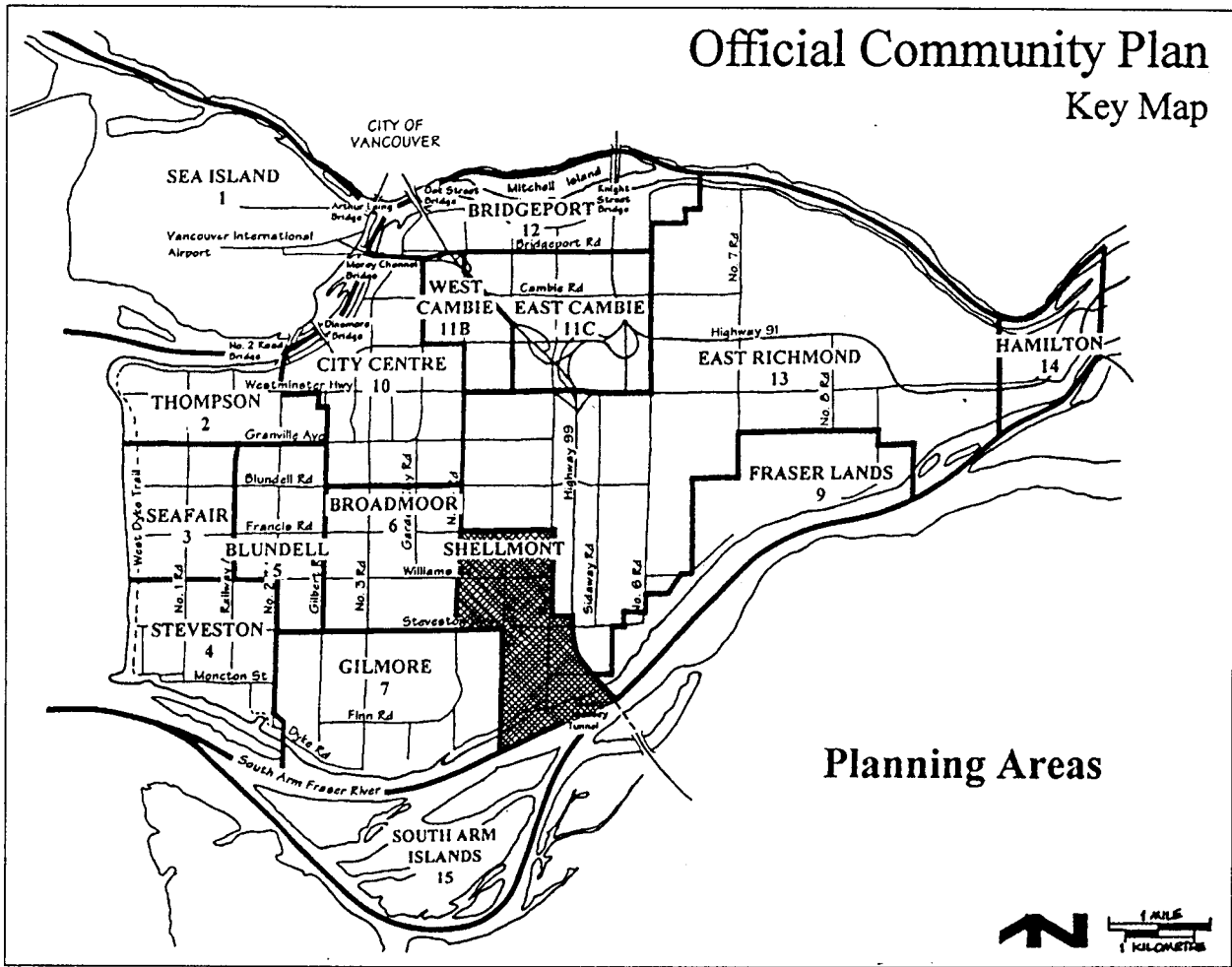
City of Richmond

SHELLMONT AREA IRONWOOD SUB-AREA PLAN Bylaw 7100 Schedule 2.8A



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KEY MAP



DEVELOPMENT PERMIT AREA MAP

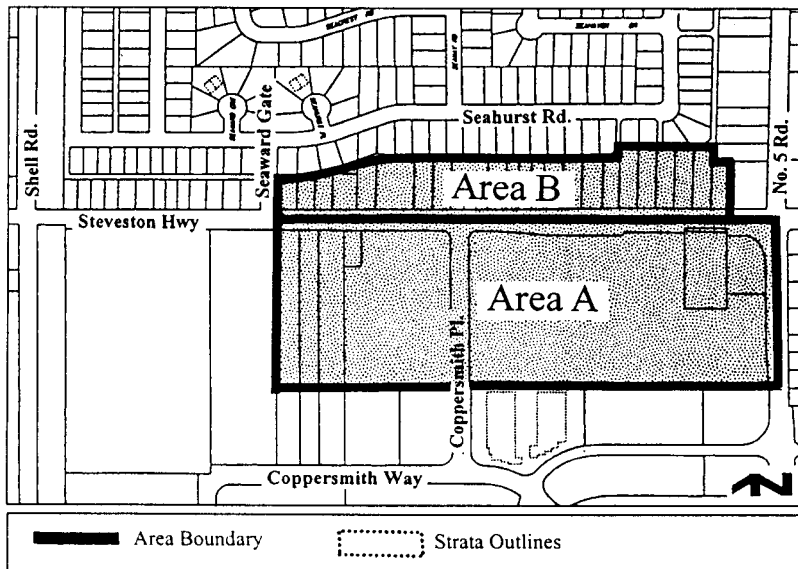


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See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

The Ironwood Sub-Area contains two “character areas” situated along the north and south side of Steveston Highway to the east of No. 5 Road (Refer to the Development Permit Area Map). This section contains Development Permit Guidelines which apply to these character areas.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Ironwood Sub-Area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to commercial development on the south side of Steveston Highway and multiple-family development along the north side of the Steveston Highway due to the following factors:

- a) The intersection of Steveston Highway and No. 5 Road is an important gateway into Richmond. New commercial and residential development should provide an attractive, welcoming entrance appropriate to this role as a key entrance to Richmond;

- b) Due to the traffic volumes there is a need to control access to and ensure safe and convenient pedestrian circulation along Steveston Highway; and
- c) Whenever different land uses adjoin, there is a need to ensure that new development blends in with the character and scale of existing developments and a need to buffer or mitigate potential impacts. Traffic, noise, lighting and other environmental conditions must be taken into consideration in the design of the commercial development. The multi-family development should be designed to provide a boundary between the quiet, low density residential uses on the north side of Steveston Highway and the increasingly active, commercial and business park uses on the south side of Steveston Highway.

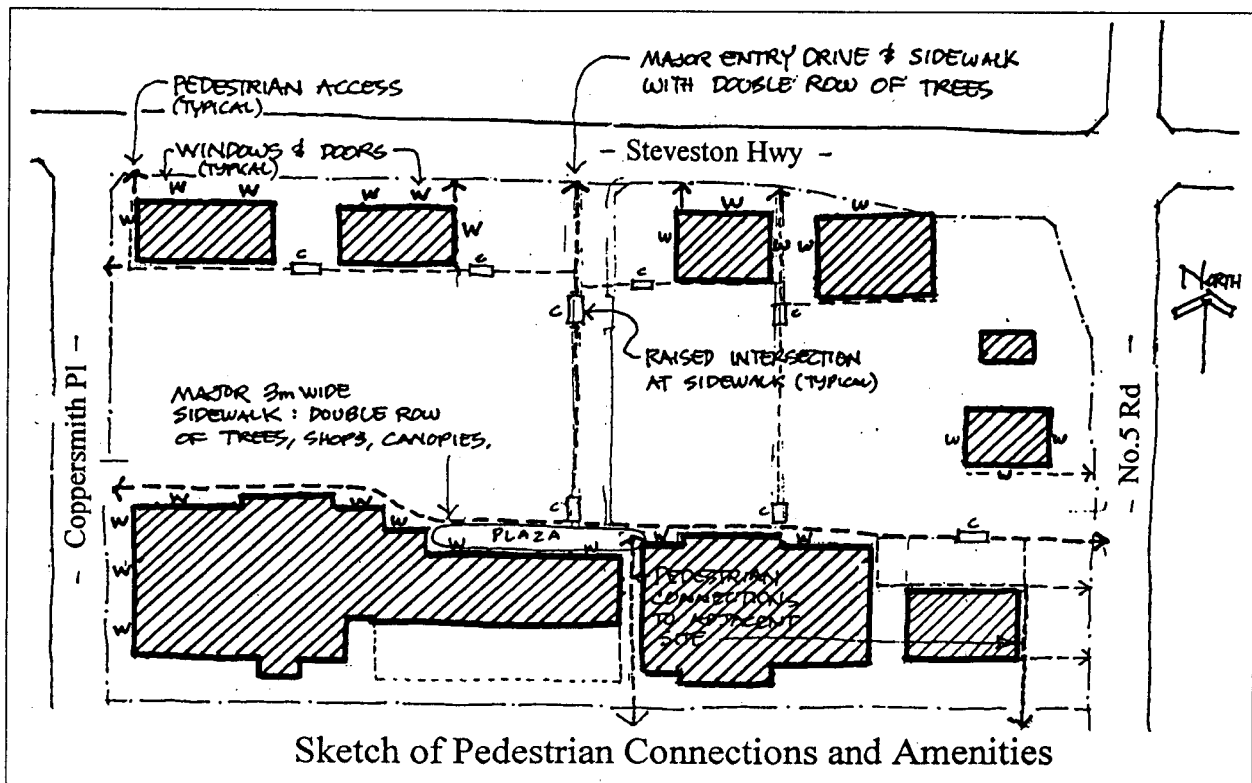
8.2 AREA A – COMMERCIAL DEVELOPMENT ALONG THE SOUTH SIDE OF STEVESTON HIGHWAY

The following guidelines apply to commercial development as shown in Area A on the Development Permit Area Map (inside front cover) in addition to the General and Commercial Guidelines for all Development Permits located in Schedule 1 to this Bylaw.

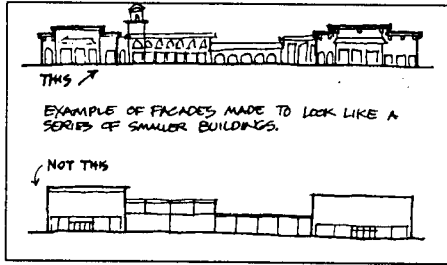
8.2.1 SETTLEMENT PATTERNS

- a) Pedestrian access to the main buildings on each site on both sides of Coppersmith Place should be provided as follows:
 - Minimum 1.5 m (4.9 ft.) wide sidewalks;
 - Located so as to provide a minimum of two accesses from Steveston Highway, one from No. 5 Road, two from Coppersmith Place, and two from Coppersmith Way;
 - Where walkways intersect vehicle paths, the roadways should be raised to the walkway level, and should have a non-asphalt paving material;
 - At least one walkway connecting to the Steveston Highway sidewalk should be fully covered and lighted; and
 - All walkways should be accessible to disabled persons.

- b) Vehicle connections to the streets on each site on both sides of Coppersmith Place should be as follows:
 - To Steveston Highway: one right-in-right-out;
 - To Coppersmith Place: one full movements (at south end) and one loading bay access;
 - To No. 5 Road: one right-in-right-out, one full movements. One to be shared with adjacent property on the south;
 - To Coppersmith Way: one full movement; and
 - Width to be minimized, to promote pedestrian safety.
- c) Pedestrian/vehicle connections to the adjacent properties are to be encouraged. There should be at least one pedestrian connection through the main block of buildings (see Pedestrian Connection sketch).
- d) The design should create defensible spaces, which provide for easy surveillance and safety both day and night.



Pedestrian Connection Sketch

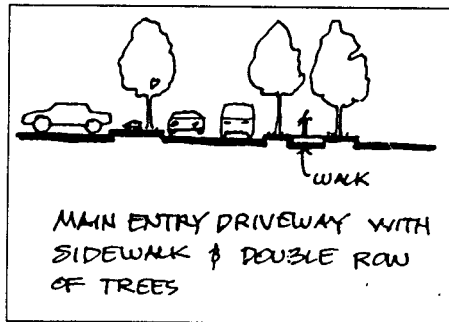


Building Façades

8.2.2 ARCHITECTURAL ELEMENTS

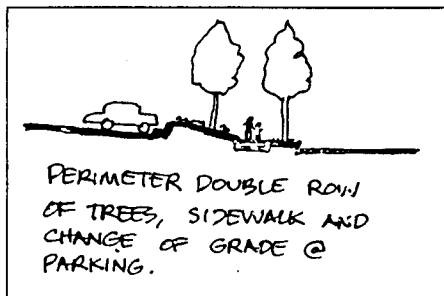
- a) Buildings should be designed so as to break down large façades into smaller elements to create an appearance of a series of smaller buildings (see Building Façades sketch).
- b) Shopfronts should have a minimum of 40% glazing, and all display windows and entrances should be protected from the weather by minimum 2 m (6.6 ft.) deep colonnades or canopies. The north side second floor should have an open corridor or colonnade across its full frontage.
- c) Buildings which front onto public streets should have display windows or glass doors for a minimum of 60% of the building edge. These areas should be paved for a minimum of 2 m (6.6 ft.) in front of the windows or doors.
- d) Pedestrian access areas (sidewalks) between parking lots and store entrances should be a minimum of 3 m (9.8 ft.) wide. Pedestrian amenities should be provided at regular intervals.
- e) On-site employee or public amenities should include change rooms, showers, lockers, a lounge, and a covered outdoor seating area.
- f) Signs should be integrated with the architecture, and limited to the following (in addition to the Sign Bylaw requirements):
 - Façade signs comprised of letters and logos affixed to the building, or in internally illuminated boxes - the latter to be limited to sign bands immediately above main floor windows/doors or copy on awnings; and
 - Freestanding signs limited to directional signs and signs indicating the name and general nature of the services, to a maximum of 2 m (6.6 ft.) in height.

8.2.3 LANDSCAPE ELEMENTS

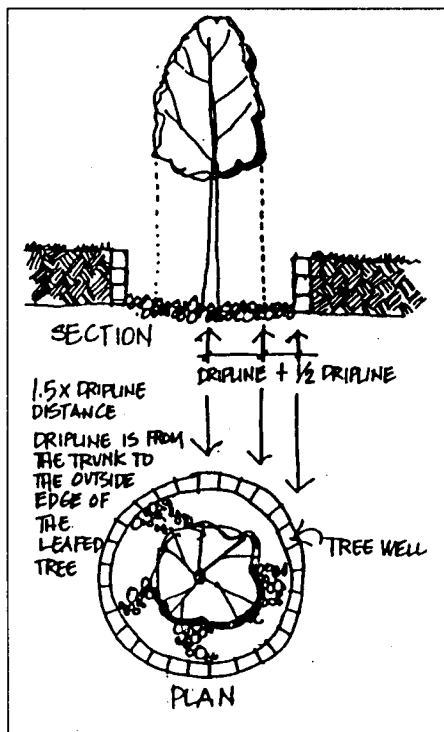


Tree Planting

- a) Edge conditions should create a high-quality pedestrian environment, by provision of boulevards, street trees, pedestrian amenities, lighting, and landscaping. Pedestrian/vehicle conflicts should be minimized.
- b) Landscaping should be used to create a predominant green aspect of the site and also to soften the presence of large numbers of vehicles, both in the parking lot and on the surrounding streets, by:



Grade Change



Tree Well

- Planting a double row of trees around the perimeter of the site and on the two main entry driveways, to form a canopy over the sidewalks and driveways (see Tree Planting sketch);
 - Planting "groves" of trees and shrubs in the parking lot so that, approximately 10 years after planting, at least 50% of the parking lot will be covered by a canopy of leaves in summer; and
 - Creating a change of grade at the edge of the parking lot and planting low shrubs so that, without compromising visibility and surveillance, parked cars are screened from the road (see Grade Change sketch).
- c) Existing trees should be preserved by:
- Retaining in situ complete with tree wells as necessary (see Tree Well sketch), or relocating on the site; and
 - Erecting construction fencing to City specs around all of the above trees prior to site pre-loading and to remain fenced throughout the construction period.
- d) Efforts should be made to retain, move offsite, or relocate other existing trees and shrubs. Provincial laws regarding cutting of trees containing songbird or raptor nests during the nesting season should be respected.
- e) The south edge of the property should be well landscaped, but should be carefully integrated with the adjoining properties. Fences or hedges on this perimeter should not exceed 1 m (3.3 ft.) in height.
- f) Landscape themes should include the following suggested mix of native and exotic plants, to maintain a consistent level of quality:
- Perimeter and parking lot tree types: Honey Locust, Chanticleer (Bradford) Pear, Sweet Gum, Katsura. Shrub types: Blueberry, Currant, Red Osier Dogwood, Oregon Grape, Native (Swamp) Rose, Rhododendron; and
 - Feature trees and plants: Windmill Palm, Persian Ironwood, Monkey Puzzle, Sitka Spruce, Pacific Crabapple, Yucca, Bamboo, native and exotic tall grasses.
- g) Preserve natural heritage by retaining, relocating and augmenting existing healthy on-site trees and shrubs. Each existing tree larger than 20.3 cm (8 in.) dbh which is unavoidably cut should be replaced by two suitable trees of minimum 10.2 cm (4 in.) dbh. Wherever possible, plant new landscaping which will be beneficial to native and migratory birds.

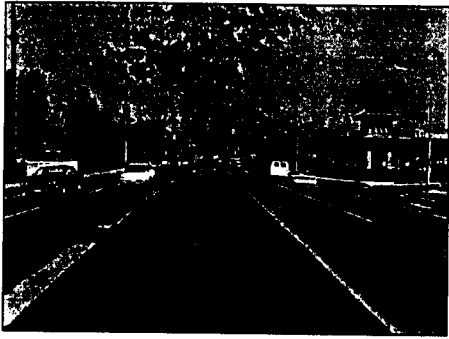
- h) At least one lighted shelter should be provided for people waiting for busses.
- i) There should be at least one pedestrian plaza of a minimum size of 550 m² (5,920 ft²), with a minimum of 50 linear metres (164 linear feet) of seating. The plaza should also include a drinking fountain. The plaza should be adjacent to a building containing shop windows and canopies.

8.2.4 PARKING, GARBAGE, RECYCLING AND RELATED ELEMENTS

- a) Parking lot lighting should be directed away from residential areas so that there is a maximum of three footcandles at the north property line. Certain light standards should be designed to accommodate hanging flower baskets complete with irrigation.
- b) Bicycle parking should be provided as per the following minimum standards:
 - Class 1, long-term secured parking: 0.27 spaces per each 100 m² (1,076.43 ft²) of gross leasable area; and
 - Class 2, short term parking: 0.27 spaces per each 100 m² (1,076.43 ft²) of gross leasable area.For details, refer to Appendix 1 – Bicycle Parking and End of Trip Facilities.
- c) Signs and parking lot lighting should be compatible with the adjacent residential area.
- d) Garbage, recycling and pick-up should be situated in areas which do not conflict with pedestrian traffic, and should either be fully enclosed or screened with solid walls higher than the bins.

8.3 AREA B – NORTH SIDE OF STEVESTON HIGHWAY

The following guidelines apply to Area B as shown on the Development Permit Area Map (inside front cover). It is the objective of these guidelines to help define the form and character of new townhouse development along the north side of Steveston Highway to ensure it is both appropriate to the area's "Richmond gateway" location and proximity to Ironwood Shopping Centre, while also addressing livability issues related to vehicular access and traffic impacts.



*Centre boulevard across from
Ironwood*

8.3.1 SETTLEMENT PATTERNS

To establish a pattern of dual fronting townhouse clusters (with sunny yards) linked by a pedestrian-friendly lane system and screened by a “formal front” oriented to Steveston Highway, new development should:

- a) Place emphasis on the establishment of a green, treed and landscaped streetscape along Steveston Highway punctuated by entranceways to individual townhouse clusters;
- b) Typically design townhouse clusters as a combination of rows of units aligned north-south (such that the fronts and rears of individual units are oriented east-west and unit sidewalls front onto Steveston Highway);
- c) Accommodate vehicular access via a public lane system situated along the north edge of Area B. In addition one temporary right-in/right-out access with a maximum dimension of 5 m (16.4 ft.) in width may be permitted on a temporary basis from Steveston Highway until such time as the lane is upgraded to City standards (see Landscape Elements for details regarding temporary vehicular access); and
- d) Setback:
 - Along Steveston Highway - 6 m (19.69 ft.), EXCEPT that where a berm is not provided (as described under section 1.3.3 Landscape Elements) or rows of units are aligned east-west, rather than north-south, the minimum setback shall be 12 m (39.37 ft.); and
 - Along the lane – 6.0 m (19.69 ft.), provided that porches and similar building features may project up to 1 m (3.28 ft.) into the setback.

8.3.2 ARCHITECTURAL ELEMENTS

To address noise-related traffic impacts and establish a pedestrian-friendly streetscape, new development should:

- a) Be designed to maintain an acceptable ambient noise level of 35 dB for indoor spaces and 55 dB for outdoor private spaces;
- b) Create a residential streetscape along the lane that reinforces its use and image as a special local, pedestrian-friendly street (rather than a service access) through features such as porches, front doors to individual townhouse units, bay windows, living/dining room windows, etc.; and

- c) Orient garages so as to be accessed via private, on-site driveways rather than directly from the public lane.

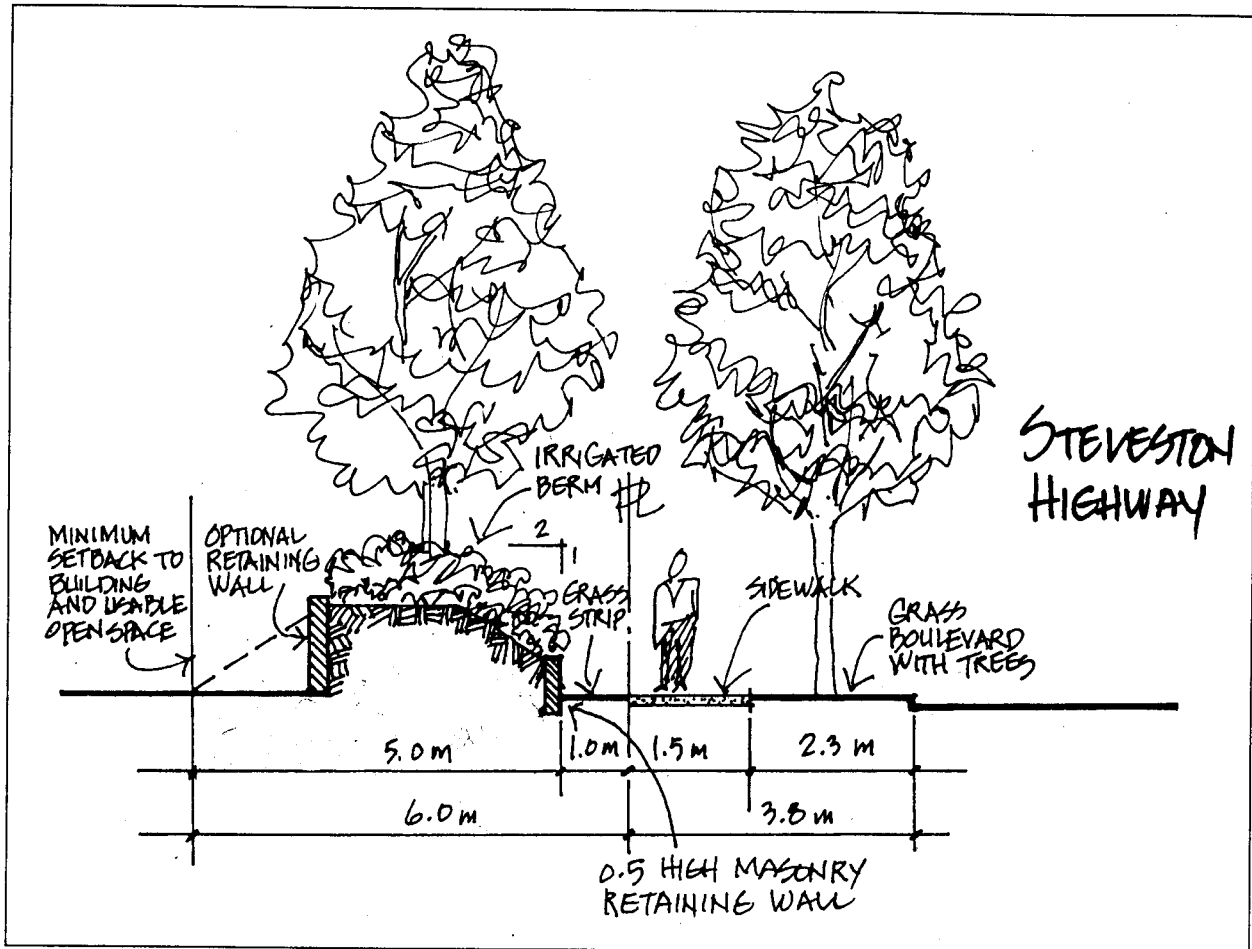
8.3.3 LANDSCAPE ELEMENTS

To enhance the appearance of Steveston Highway as a prominent vehicular artery, screen sensitive residential uses, and create a special, pedestrian-friendly lane environment, new development should:

- a) Provide clear signage on both Steveston Highway and the lane indicating addresses of developments. The Steveston Highway signage should specifically note that “lane access is available from Seaward Gate”;
- b) Along Steveston Highway, contribute a lush, green and pedestrian oriented landscape by accommodating:
 - Installation of a 2.3 m (7.55 ft.) wide grass boulevard (complete with a single row of Pin Oaks) at the back of curb and a 1.5 m (4.92 ft.) wide concrete sidewalk;
 - Within the minimum 6 m (19.69 ft.) building setback, a 1 m (3.28 ft.) wide grass strip at the back of sidewalk and a continuous landscaped berm at least 1.2 m (3.94 ft.) high (measured from the adjacent curb), EXCEPT as required to maintain existing mature trees (See diagram: *Steveston Highway Frontage*);
 - Any fencing incorporated as part of the berm should be located at a minimum of 4.4 m (14.43 ft.) from the south property line and not higher than 1.5 m (4.92 ft.) (measured from the curb) EXCEPT where a fence is adjacent to private outdoor space it may be as tall as 1.8 m (5.90 ft.);

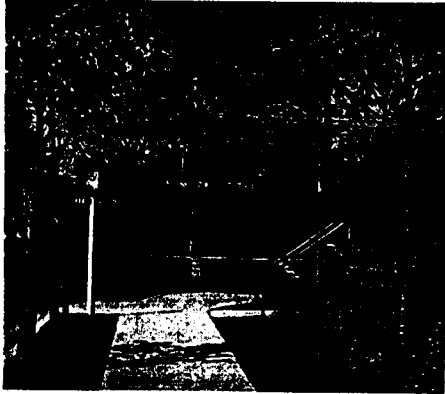


Berm on south side of Steveston Highway

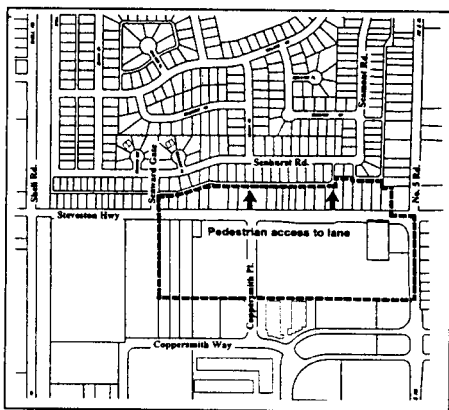


Steveston Highway Frontage

- Significant planting within the berm area, including large growing trees and plant material chosen for its seasonal colour, screening abilities, and visual interest;
- A minimal width and number of breaks in the berm for pedestrian and where necessary, temporary vehicular access. Pedestrian access should be shared by a number of units and typically be confined to a 3 m (13 ft.) break in the berm. One temporary vehicular access is permitted per development and should typically be confined to a 6 m (19.7 ft.) wide break in the berm to accommodate both vehicles and pedestrians. Temporary vehicular access should be designed to be readily convertible to pedestrian only use and character without significant cost (ie, decorative paving materials for the temporary driving surface, use of landscaping and/or bollards); and



Crosswalk at Coppersmith Way

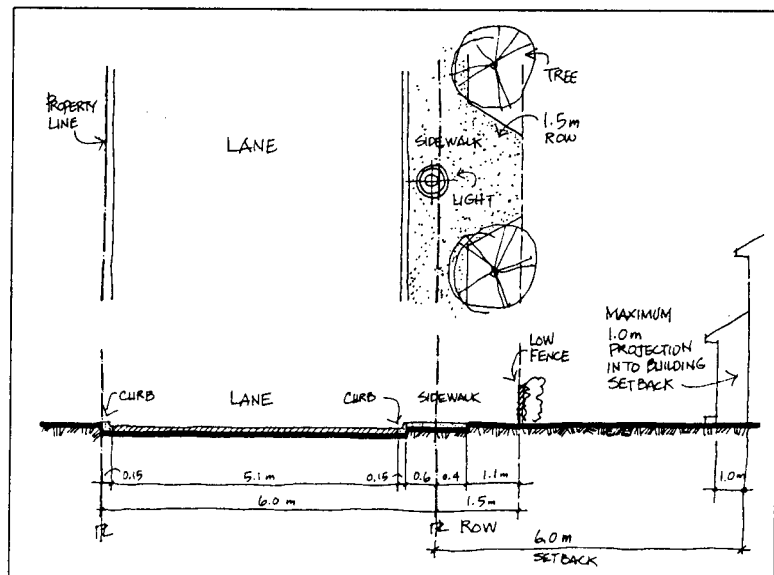


Pedestrian Pathways

- Pedestrian pathways linking the single-family neighbourhood to the north of Area B to the Ironwood shopping centre. Pathways should be located at Coppersmith Way and where the alignment of the lane turns north to Seahurst Road (see Pedestrian Pathways diagram). The pathways require a minimum of 2.4 m (7.87 ft.) of paved surface to accommodate pedestrian and bicycles with a minimum of 0.8 m (2.6 ft.) landscaping on either side for a total width of 4 m (13.12 ft.).

c) For the public lane provide:

- A 6.0 m (19.685 ft.) wide laneway with roll curbs and lighting strip;
- A 1.5 m (4.92 ft.) right-of-way on the southern edge of the laneway to accommodate a concrete aggregate sidewalk and a single row of trees at 9 m (29.53 ft.) on centre (see Lane Frontage sketch);
- High visibility between the lane and the adjacent properties by ensuring that fencing along the lane is no higher than 1.2 m (3.94 ft.). Additionally, any fencing should be located no closer than 1.5 m (4.92 ft.) from the northern property line; and
- Facilities for mail and garbage.



Lane Frontage

**BICYCLE PARKING AND
END OF TRIP FACILITIES**

APPENDIX 1

New development should accommodate the bicycle parking and end-of-trip facility needs of multiple-family residential dwellers, workers, and visitors.

a) CLASS 1 Parking

Secured, long-term bicycle parking shall be provided for the use of residential use and non-residential tenants in the form of waterproof bicycle lockers, or bicycle rooms complete with bicycle racks.

- (i) Parking facilities shall: be at-grade; have uniform 160 lux (min.) lighting which yields true colours; and, be within sight of building entry, elevator, and/or security.
- (ii) Bicycle rooms shall provide: lockable door(s) with window(s); tamper-proof, motion-activated security lighting; and unobstructed view of each room from its entry; and, facilities for no more than 20 bicycles per room (enabling owners to identify one another).
- (iii) Bicycle lockers shall: be constructed of solid, opaque, weather-proof and theft-resistant material, with no exposed fittings or connectors; have lockable doors which open to the full height and width of each locker; be grouped together; not be located at the head of parking spaces; and, have clear minimum dimensions of:

Length	1.80 m (5.91 ft.)
End Width at Door	0.60 m (1.97 ft.)
End Width Opposite Door	0.22 m (0.72 ft.)
Height	1.20 m (3.94 ft.)

b) CLASS 2 Parking

Unsecured, short-term bicycle parking shall be provided for visitors in the form of bicycle racks located within 15 m (49.2 ft.) of a principal building entry.

- (i) Parking shall be situated in well-lit locations, clearly visible from principal building entries and/or public roads.

- (ii) Bicycle racks shall be made of sturdy, theft-resistant material, securely anchored to the floor or ground.
- (iii) Bicycle racks shall be designed to support the bicycle frame, not the wheels, and allow both the frame and the front wheel to be locked to the rack with a U-style lock.