



CITY OF RICHMOND

REPORT TO COMMITTEE

TO: Planning Committee
FROM: Terry Crowe
Manager, Policy Planning Department
RE: Update to Area Plans

DATE: December 19, 2000
FILE: -

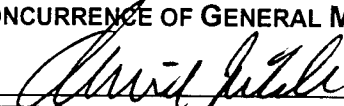
STAFF RECOMMENDATION

1. That Bylaw No. 7190, which amends Official Community Plan, by substituting the following Area Plans for the existing Area Plans:
 - Thompson (Dover Crossing Sub-Area) as Schedule 2.2A;
 - Thompson (Terra Nova Sub-Area) as Schedule 2.2B;
 - Blundell (Laurelwood Sub-Area) as Schedule 2.5A;
 - Blundell (East Livingstone Sub-Area) as Schedule 2.5B;
 - Broadmoor (Ash Street Sub-Area) as Schedule 2.6A;
 - Broadmoor (Central West Broadmoor Sub-Area) as Schedule 2.6B;
 - Broadmoor (Sunnymede North Sub-Area) as Schedule 2.6C; and
 - Shellmont (Ironwood Sub-Area) as Schedule 2.8A,

be introduced and given first reading.
2. That Bylaw No. 7190, having been examined in conjunction with the Capital Expenditure Program, the Waste Management Plan, the Economic Strategy Plan, and the 5 Year Financial Plan, is hereby deemed to be consistent with said program and plans, in accordance with Section 882(3) of the Local Government Act.
3. That Bylaw No. 7190, having been examined in accordance with the City Policy on referral of Official Community Plan Amendments, is hereby deemed to have no effect upon an adjoining Municipality nor function or area of the Greater Vancouver Regional District, in accordance with Section 882(3)(d) and (e) of the Local Government Act.
4. That Bylaw No. 7190 be referred to the Vancouver International Airport Authority in accordance with the Richmond YVR Accord.
5. That Bylaw No. 7190 be referred to the Agricultural Land Commission in accordance with Section 882(1)(c) of the Local Government Act.
6. That Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 7113 which replaced Schedule 2.8A (Shellmont-Ironwood Sub-Area Plan) be abandoned.


Terry Crowe
Manager, Policy Planning Department

Att.

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CONCURRENCE OF GENERAL MANAGER


STAFF REPORT

ORIGIN

This report presents eight updated Sub-Area Plans for approval by Council. This work was initiated following the adoption of the new Richmond Official Community Plan (OCP). The new plans are contained in the blue binder labelled "Schedule A to Bylaw 7190". For reference purposes, Attachment 1 to this report (black binder) contains the original Area Plans with margin notes indicating the main changes that are being proposed.

FINDINGS OF FACT

Format Changes

The following format changes are made to be consistent with the new OCP:

- a portrait rather than a landscape orientation;
- revised Table of Contents;
- revised headings, page layout, headers and footers; and
- re-organizing the Development Permit guidelines to follow a consistent format.

Minor Content Changes

Changes to the content of the plans were made to:

- correct grammatical errors;
- remove out of date information;
- remove implementation sections; and
- eliminate Acknowledgement sections.

Additionally, information was added to Area Plans with regard to the Agricultural Land Reserve and the Airport. Specifically, the Ash Street and Terra Nova Sub-Area Plans were amended by adding policies and guidelines to address adjacency issues with regard to the ALR. The Dover and Terra Nova Sub-Area Plans were amended by adding policies regarding the airport.

Proposed Bylaw 7113 to be Abandoned and Replaced

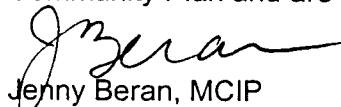
As part of an earlier rezoning application, Bylaw 7113, now between 3rd and 4th readings, proposes to add development guidelines to the Ironwood Sub-Area Plan for the north side of Steveston Hwy across from Ironwood. The adoption of proposed Bylaw 7113 has been held up with the final reading requirements for the rezoning application. In the meantime, another re-formatted version of the Ironwood Plan is presented here as part of Bylaw 7190. Therefore, in order to adopt the updated version of the Ironwood Plan as part of Bylaw 7190, the earlier proposed bylaw needs to be abandoned. This new proposed bylaw will ensure that the Ironwood Plan is presented in a consistent format with the other Plans and will not affect the proposed rezoning.

FINANCIAL IMPACT

None.

CONCLUSION

Eight Sub-Area Plans have been amended in order to make them consistent with the Official Community Plan and are presented to Council for adoption.



Jenny Beran, MCIP
Planner

JMB:jmb

Richmond Official Community Plan With Notated Changes

Schedule 2 Area Plans

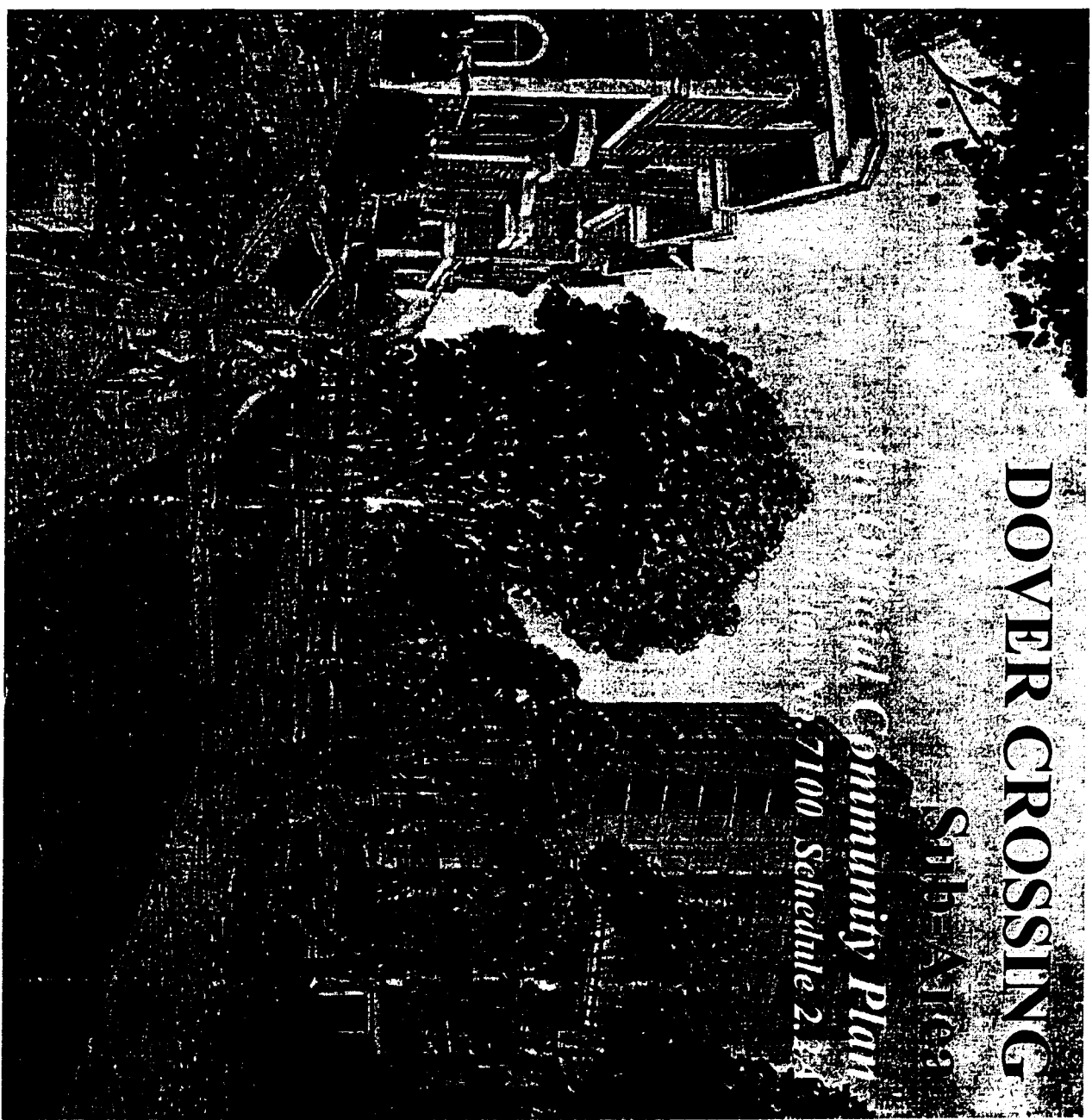
- 2.2A Thompson (Dover Crossing Sub-Area)
- 2.2B Thompson (Terra Nova Sub-Area)
- 2.5A Blundell (Laurelwood Sub-Area)
- 2.5B Blundell (East Livingstone Sub-Area)
- 2.6A Broadmoor (Ash Street Sub-Area)
- 2.6B Broadmoor (Central West Sub-Area)
- 2.6C Broadmoor (Sunnymede North Sub-Area)
- 2.8A Shellmont (Ironwood Sub-Area Plan)

City of Richmond



THOMPSON Area Plan

OCF-GOVENF:\SHALED\OCFCover\OCFCover91A.p65



DOVER CROSSING

Sub-Area 3

Community Plan

7100 Schedule 2, 2014

THOMPSON AREA

Dover Crossing Sub-Area Plan

now in 1.1 purpose

This plan sets out the goals, objectives, policies and development guidelines for the Dover Crossing Sub-Area, a part of the Thompson Planning Area.

now in plan interpretation

Additional Conservation Area and Environmentally Sensitive Area (ESA) policies, guidelines, and locations are included in Schedule 1 of this Bylaw and its attachments (Schedule 1 is a separate document which applies to the entire City). Readers should check Schedule 1 as it takes precedence over this plan in the case of Conservation Areas and ESAs.

The plan may be amended from time to time and users should check with the City's Urban Development Division to make sure that they have an up-to-date version containing all of the adopted amendments.

not included

ACKNOWLEDGEMENTS

The City of Richmond's Urban Development Division prepared this plan based on the Dover Crossing Neighbourhood Study completed by Civitas Urban Design and Planning Inc., and the input received from the Dover Crossing Citizen's Committee and the Staff Steering Committee. Members of the Staff Steering Committee included: Marga Betz, Don Brown, Doug Hystad, Ken Morris, Ron Mann, Henry Pelzer, Lauren Schibler, Dave Semple, and Rick Stene. Kiyoshi Otsuji and Frank Sciberras produced some of the graphics and assembled the report. Ruby Nishi was responsible for the word processing.

A special appreciation is extended to the enthusiastic and dedicated members of the Dover Crossing Citizen's Committee who contributed significantly to the contents of this plan. Members of the Committee include: Julie Halfnights, Glen Hoban, Wynne Gledstone, Walter Schinke, John Van Vaulkenberg, Laurie Svensson, Erika Simm, Mary-Lou Naval, Michael Conrad, Dan Pasacreta and Brian Stroud.

Appreciation is also extended to the staff of Civitas Urban Design and Planning Inc. who undertook the Dover Crossing Neighbourhood Study and who allowed us to use some of their graphics for this plan.

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*now in
List of Maps*

LIST OF ATTACHMENTS

ATTACHMENTS

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VI

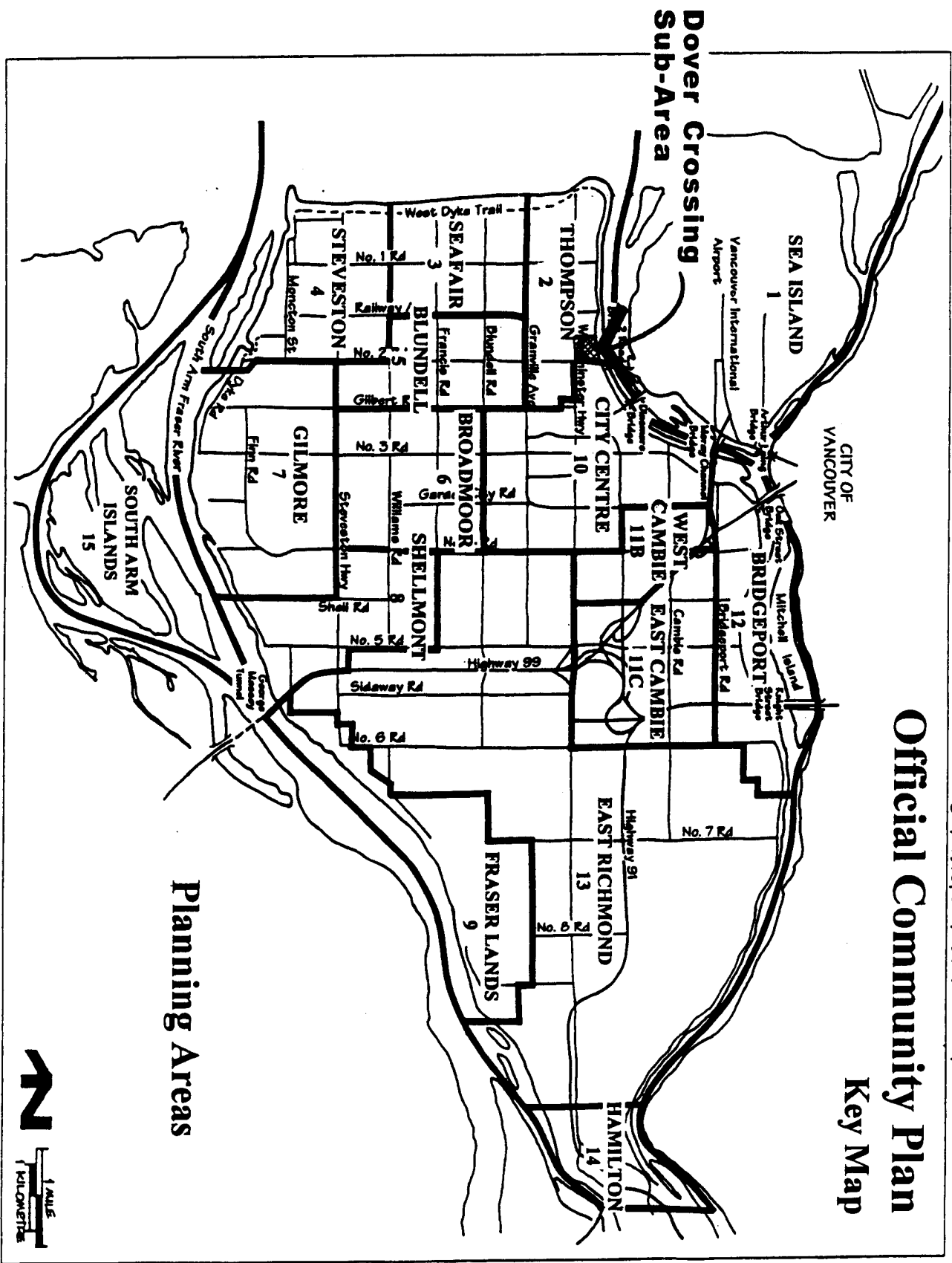
Original Adoption: March 15, 1999



see inside front cover

Official Community Plan

Key Map



1.0 INTRODUCTION

now 1.0 plan

overview

1.1 SETTING

The Dover Crossing Neighbourhood is located within the Thompson Planning Area and is bounded by River Road, No. 2 Road, Westminster Highway and Lynas Lane. (See Attachment 1). *added reference to Airport* The subject neighbourhood is surrounded by the Middle Arm of the Fraser River to the north, undeveloped land, a recreation vehicle park and industrial warehousing to the east, single-family residential to the south, and the City Works Yard to the west.

The subject neighbourhood occupies about 14.5 ha (36 ac.). Existing land use at the time of plan preparation consisted of 11 homes on large lots, the Vancouver-Austrian Club, a C.P.R. right-of-way and vacant undeveloped lands. The total resident population of the neighbourhood was approximately 33 people.

The new bridge crossing from the north end of No. 2 Road to Sea Island provides a direct link from Richmond to the Vancouver International Airport. This link makes the Dover Crossing Neighbourhood one of the major visual gateways into West Richmond.

The Middle Arm of the Fraser River is the most prominent natural feature of the Dover Crossing Neighbourhood. The river serves as an important marine habitat. The dyke and foreshore provide a valuable recreation and natural amenity that has a strong influence on land use in the Dover Crossing Neighbourhood.

now 1.1 purpose

1.2 PLAN PURPOSE

The purpose of this plan is to provide a decision making framework to guide the development of the Dover Crossing Neighbourhood. This plan establishes goals, policies and objectives for managing future change in the neighbourhood.

not included - not required or in app

1.3 PLAN ORGANIZATION

This plan is organized into four sections. The first section provides an overview of the plan. The second section contains the vision and goals for the Dover Crossing Neighbourhood. The third section contains the policies and objectives that are necessary to achieve the vision. The fourth section of the plan is an implementation section which provides measures to ensure that orderly development occurs in the Dover Crossing Neighbourhood that is consistent with the overall vision, goals, objectives and policies stated in this plan.

1.4 TIME FRAME

Plans include statements of goals, policies and objectives for the future. Plans are based on knowledge of the past and emerging situations. Since the future is always uncertain, it is important to set forth policies which are deemed to be realistically achievable. Therefore, this plan should be reviewed every five years.

1.5 DOVER CROSSING PLANNING PROCESS

The Dover Crossing Neighbourhood Plan process began in February 1990. Civitas Urban Design and Planning Inc., in consultation with City staff prepared a development concept plan for the neighbourhood. Council then directed staff to seek public input on the concept plan. A Citizen's Committee was formed on August 29, 1991 to review the development options for the neighbourhood. In addition, staff held two public information meetings to obtain the views of other property owners in the plan area.

This plan is based on the work of Civitas Urban Design and Planning Inc. and inputs provided by the public.

2.0 VISION AND GOALS

2.1 A VISION FOR THE DOVER CROSSING NEIGHBOURHOOD

The population of Richmond is expected to grow to 212,000 people by the year 2021. In order to preserve farmland and stabilize single-family neighbourhoods, the majority of this growth or approximately 42,000 people need to be housed in and around the City Centre Planning Area.

Although Richmond's history is linked to the Fraser River, its City centre is not located to take advantage of the waterfront. As Richmond grows, there is an opportunity to create an exciting urban waterfront that is well connected to the city core in terms of open space and transportation linkages. This may be achieved by eventually expanding the higher density residential and commercial developments of the City Centre Planning Area towards the Middle Arm of the Fraser River.

removed

The Dover Crossing Neighbourhood is located directly adjacent to the most westerly extension of the City Centre ~~study~~ area. The size of the Dover Crossing Neighbourhood 14.5 ha (36 ac.) affords an opportunity to create a liveable, well co-ordinated mixed housing community.

The Dover Crossing Neighbourhood will be transformed from an area with a high proportion of vacant land and large single-family lots to a residential neighbourhood with a variety of multiple-family (townhouses and apartments) housing types.

The general development concept will consist of the following characteristics. There will be a gradual transition in housing form from north to south in order to minimize the impacts on the existing single-family neighbourhoods to the south of the sub-area. Taller buildings will be located adjacent to the river and the No. 2 Road Bridge which will create a visual gateway into West Richmond and permit more open space in the neighbourhood. Townhouses will be primarily developed adjacent to Westminster Highway but will also be integrated with the apartments to the north to help create a high quality streetscape.

The Dover Crossing Sub-Area will be well connected to the riverfront. Linear parkways accessible by pedestrians, wheelchairs and cyclists will extend from *changed to* Westminster Highway through the neighbourhood to River Road. *Plastered* A Land Bridge could provide a safe and convenient connection from the neighbourhood over River Road to *Crossing* the dyke trail.

The importance of preserving views to the riverfront and mountains will be recognized. Views of the river and mountains will be preserved through the linear parkways referred to above and by terracing of apartment buildings.

A neighbourhood park and school site will be centrally located in the neighbourhood. Community services, such as childcare, will be encouraged and facilitated as part of development approvals.

Specialized landscaping, fountains and public art will help create special places for people to meet in the sub-area.

Developments in the area will maintain, enhance and preserve air, water and soil quality.

2.2 GOALS FOR THE DOVER CROSSING NEIGHBOURHOOD

1. To permit residential development that:
 - a) Recognizes the area's close proximity to the No. 2 Road Bridge, the Middle Arm of the Fraser River and the City Centre;
 - b) Preserves views of the waterfront and the mountains;
 - c) Provides public access to the waterfront;
 - d) Includes community services and open space that enhances liveability; and
 - e) Maintains, enhances and preserves air, water, and soil quality.
2. To encourage and facilitate features which create a "heart and soul" for the neighbourhood, such as land-bridges, special landscaping, fountains and public art.
3. To provide roads, walkways, and bicycle paths that help to connect the neighbourhood with the riverfront and the surrounding areas in a convenient and safe manner.



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Original Adoption: March 15, 1999

3.0 POLICIES AND OBJECTIVES

*now 3.0
Neighbourhood
Housing*

3.1 RESIDENTIAL LAND USE

3.1.1 Housing Forms

This plan prescribes a mix of townhouses and apartments. The densities of the development will range from .55 (F.A.R.)¹ to a maximum of 1.6 (F.A.R.). Thus, the overall density will be in the medium range.

As shown on Attachment 1, townhouses are located along the southern edge of the neighbourhood to provide a gradual transition from the low density single-family area along the south side of Westminster Highway. The apartments are deliberately located in the north and north-east portions of the neighbourhood in order to create a visual gateway into Richmond. The taller buildings allow for more green space, and in conjunction with the linear walkways enhances both private and public views and access to the waterfront.

OBJECTIVE 1: TO PROVIDE A RANGE OF HOUSING TYPES FOR A VARIETY OF HOUSEHOLDS AND AGE GROUPS.

It is Council's policy to:

- 1.1 Allow development such that a mixture of housing types are provided as per Attachment 1;

¹ F.A.R. (Floor Area Ratio: means the figure obtained when the total area of the floors of the building on a lot, measured to the outer limits of the building or buildings, is divided by the area of the lot.)

-
- 1.2 Require that all buildings shall not exceed 24.38 m (80 ft.) in height; and
 - 1.3 Encourage and facilitate the provision of affordable housing.

3.1.2 Views and Vistas

Views of the airport and the mountains are unobstructed from the neighbourhood. The physical height of the dyke restricts views to the north for low level residences.

OBJECTIVE 2: TO UTILIZE OPPORTUNITIES FOR VIEWS OF THE FRASER RIVER AND MOUNTAINS.

It is Council's policy to:

- 2.1 Encourage building designs which maximize views of the river and mountains; and
- 2.2 Encourage designs that minimize building shadowing of public and private open spaces and walkways.

3.1.3 Airport

The neighbourhood's close proximity to the Vancouver International Airport has both advantages and disadvantages. The views of seaplanes and other aircraft landing and descending adjacent to the neighbourhood adds to the interest of the area. However, the Dover Crossing Neighbourhood is subject to a certain amount of noise and disruptions created by the float planes, other air craft and ancillary uses to the airport. ~~The noise level in the neighbourhood is projected at 30-35 N.E.F. by the year 2001, a slight decrease resulting from the building of the third runway.~~

In order to protect flight operations, Transport Canada imposes building height restrictions in the vicinity of the Airport. Specific height restrictions are contained in plans filed by Transport Canada in the Provincial Land Titles Office.

OBJECTIVE 3: TO ENSURE THAT THE HEIGHTS AND DESIGNS OF ALL NEW BUILDINGS ARE IN ACCORDANCE WITH THE VANCOUVER INTERNATIONAL AIRPORT ZONING REGULATIONS.

It is Council's policy to:

- 3.1 Encourage builders of new residential buildings to comply with Canada Mortgage and Housing Corporation's guidelines for noise mitigation.

1 N.E.F. (Noise Exposure Forecast): N.E.F. is the summation of all noise that takes place in a 24 hour period based on the perceived level of noise. It considers some tonal qualities of sound and is intended to rate the "noisiness" or annoyance level of a sound rather than its loudness.

removed reference to specific NEF levels changed to "the planning area, or a part thereof, is subject to a noise level of 30 NEF or above."

added new policy regarding the city's practice to require covenants for areas affected by aircraft noise.

new NEF definition as requested by VNR

3.1.4 No. 2 Road Bridge

In March 1990, Richmond City Council approved a new bridge crossing from the north end of No. 2 Road to Sea Island, providing a direct link to the Vancouver International Airport. Taller buildings are situated in the north-east corner of the neighbourhood to provide a visual gateway into Richmond. Buildings are set back from the northern foot of No. 2 Road as a noise mitigation measure as well as to reserve open space in case bridge loop ramps need to be developed from the bridge to River Road at some future date.

It is Council's policy to:

1. Ensure that all residential buildings have appropriate setbacks from No. 2 Road Bridge to mitigate against vehicular traffic noise and for privacy of residential uses; and
2. Require that sufficient land is retained as open space at the north-east portion of the neighbourhood which can be used to develop bridge loop ramps if and when the need occurs.

U.S.O **3.2 ENVIRONMENT**

Mar **3.2.1 Fraser River**

The most distinctive feature of the sub-area is its relationship to the Fraser River. The dyke and foreshore of the Fraser River serves as both a recreational and natural amenity that must be protected and enhanced. The dyke and trail system is an important part of Richmond's Open Space System.

OBJECTIVE 4: TO ACKNOWLEDGE THE FRASER RIVER AS A RESOURCE FOR MANY USERS WHILE PRESERVING AND PROTECTING THE FORESHORE.

It is Council's policy to:

4.1 Support policies and actions directed towards improving water quality and preserving marsh areas, fish and wildlife habitats in and along the Middle Arm of the Fraser River; and

4.2 Retain the dyke adjacent to the Dover Crossing Neighbourhood for open space purposes.

*now 5.0 Natural
& Human Environment*
3.3 PARKS AND OPEN SPACE

Over 9.14 ha (10 ac.) of land is devoted to open space in this plan. The open space consists of natural areas, landscaping, a 2.43 ha (6 ac.) neighbourhood park and three linear parkways extending from Westminster Highway through the neighbourhood. As shown on Attachment 1, the three linear parkways totalling 1.62 ha (4 ac.) with a pedestrian ~~land bridge~~ provide connections from the surrounding residential areas through the Dover Crossing Neighbourhood over River Road to the dyke trail and Fraser River. A second ~~land bridge~~ extending over No. 2 Road may eventually be required to further connect the lands to the east with the Dover Crossing Neighbourhood.

The open spaces created by the linear parks and the pedestrian land bridge improve both the physical and visual connections between the site, the surrounding areas and the dyke trail.

changed to crossing
"

OBJECTIVE 5: TO ENSURE THAT SUFFICIENT PUBLIC AND PRIVATE PARK AND OPEN SPACE IS PROVIDED FOR RECREATIONAL USES, PEDESTRIAN AND BICYCLE MOVEMENT AND TO PROMOTE BETTER AIR QUALITY.

It is Council's policy to:

- 5.1 Designate no less than 2.43 ha (6 ac.) of land for an active neighbourhood park as per Attachment 1;
- 5.2 Require that three linear parkways extending from Westminster Highway to the waterfront are provided for public access as part of the development site as per Attachment 1;¹
- 5.3 *changed to crossing* Ensure that a ~~land bridge~~ accessible to pedestrians, cyclists and wheelchairs is built as part of the development over River Road to connect the Dover Crossing Neighbourhood to the riverfront, and investigate the possibility of developing a second land bridge over No. 2 Road to connect the lands to the east with the Dover Crossing Neighbourhood;
- 5.4 Require that the Dover Crossing Neighbourhood is accessible by cyclists and that bicycle racks are provided within the public areas;
- 5.5 Encourage the preservation of mature trees in the neighbourhood; and

¹ Where the linear parkways cross private lands, the linear parkways will be designated for the same land use as the contiguous private properties.

5.6 Utilize the C.P.R. right-of-way as shown on Attachment 1, for park purposes if it is vacated by C.P.R. and is not required by the City for transportation purposes.

3.4 COMMUNITY FACILITIES AND SERVICES

*now b.o
community facilities
and services*

Nearly all community service and facility needs of the residents are met outside of this sub-area. The Thompson Community Centre is located approximately 0.8 km (0.5 mi.) away from the neighbourhood.

Additional residential development will increase pressure on childcare services and schools in the Thompson Area. Childcare facilities in the area should therefore be encouraged. The Richmond School Board has indicated that it is interested in acquiring an elementary school site in the sub-area as shown on Attachment 1. Traditionally, joint school and park sites are developed in Richmond to benefit the neighbourhood as a whole.

The Dover Crossing Neighbourhood is located in close proximity to the City Centre which is the focus of high density residential and commercial development in Richmond. Therefore, major commercial facilities are not warranted in the Dover Crossing Neighbourhood.

OBJECTIVE 6: TO ENSURE THAT A RANGE OF NEIGHBOURHOOD FACILITIES AND SERVICES ARE MADE AVAILABLE WITHIN THE SUB-AREA.

It is Council's policy to:

- 6.1 Expand the services of the Thompson Community Centre rather than duplicate the same services in the sub-area;

-
- 6.2 Encourage the provision of space for childcare and private amenity space in the sub-area;
 - 6.3 Encourage the Richmond School Board to provide an elementary school in the sub-area;
 - 6.4 Permit small scale, pedestrian-oriented commercial services that are architecturally integrated within the residential buildings along River Road; and
 - 6.5 Encourage the provision of special landscaping, a fountain, and public art in the Dover Crossing Sub-Area.

wd 4.0 **3.5 PUBLIC UTILITIES AND ROADS**

importation

Westminster Highway, No. 2 Road and River Road are important to the City network as major and local arterial routes. Access to these roads from properties in the neighbourhood should be restricted to maintain a free flow of traffic. Lynas Lane and internal neighbourhood roads will be upgraded to full City standards as development occurs. The local road system provides access to the entire neighbourhood from Lynas Lane. A signalized intersection will be required at the corner of Lynas Lane and Westminster Highway and at Lynas Lane and River Road.

The dyke trail system will accommodate pedestrian traffic on the north side of River Road. Thus, a sidewalk will only be required on the south side of River Road.

Sanitary sewers, storm sewers, underground wiring, sidewalks, curbs and gutters will need to be provided in the neighbourhood with costs apportioned to all properties in the sub-area.

OBJECTIVE 7: TO IMPROVE THE QUALITY OF THE EXISTING PUBLIC UTILITIES AND ROADS CONCURRENT WITH NEW RESIDENTIAL DEVELOPMENT.

It is Council's policy to:

- 7.1 Close Dover Road for linear park purposes as shown on Attachment I;
- 7.2 Deny direct access to the sub-area from No. 2 Road;
- 7.3 Restrict direct access to the sub-area from Westminster Highway and River Road;
- 7.4 Protect River Road as a local arterial road; and
- 7.5 Accommodate pedestrians and cyclists on all roads with adequate sidewalks, and bicycle lanes.



UD.11.9826

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Original Adoption: March 15, 1999



not included - not required, in ASP or alone

4.0 IMPLEMENTATION

The goals, objectives and policies of the Dover Crossing Sub-Area Plan are a guide for the development of the area. The responsibility for implementing these policies rests mainly with Richmond staff and Council, but certain objectives and policies depend on the support and cooperation of the entire community and other governmental agencies.

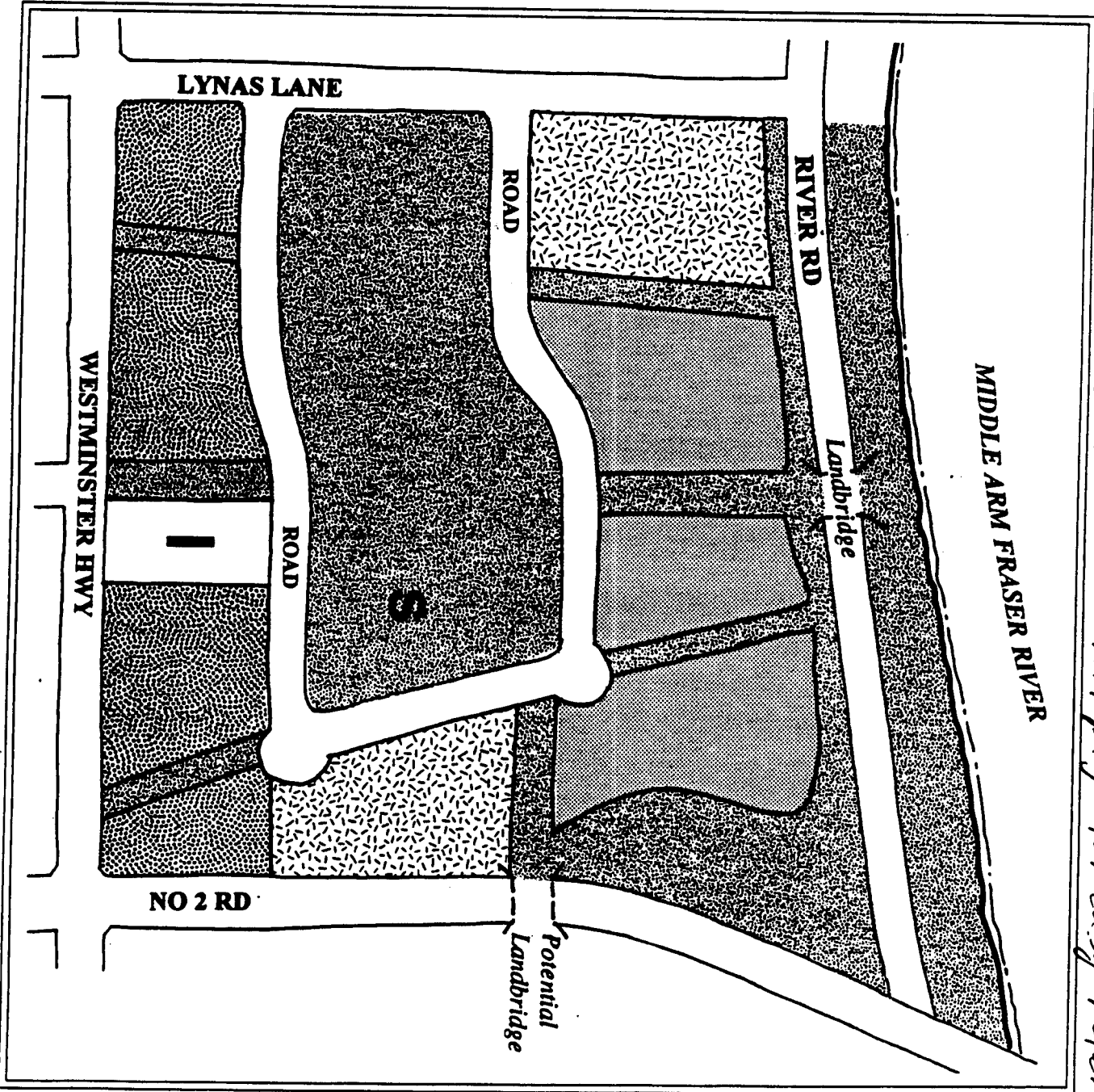
Achievement of the land use concepts described in this plan will take a number of years depending upon the general economic conditions of the region.

The actions needed to implement the Objectives and Policies of the Dover Crossing Plan are as follows:

1. Ensure that Development Permit guidelines for the sub-area which ensure that the objectives presented in this plan and the concerns of the Dover Crossing Citizen's Committee are adhered to;
2. Issue Development Permits for the sites within the designated Development Permit Areas, which are developed in conformance with the Development Permit Guidelines;
3. Initiate negotiations with the land owners adjacent to the No. 2 Road Bridge to acquire the lands needed for the development of the bridge;
4. Liaise with private property owners and developers to encourage the provision of amenity spaces, and childcare facilities in the neighbourhood;
5. Rezone the School/Park site to School and Public Use District (SPU) zone; and
6. Review the plan, every five years.





~~Conditions may change and amendments to the plan may be required from time to time.
In these cases, Council will hold a Public Hearing as required by the Municipal Act.~~

now located on last page for easy reference



Attachment 1

LAND USE

-  Townhouses (0.55 FAR max.)
-  Townhouses and Medium Density Apartments (1.2 FAR max.)
-  Townhouses and High Density Apartments (1.6 FAR max.)
-  Park/Open Space
- S** Possible School Site within the Sub-Area
- I** Institutional

Doyer Crossing Sub-Area
Official Plan
 Community Plan





UD.11.9826

20

Original Adoption: March 15, 1999



5.0 DEVELOPMENT PERMIT AREA GUIDELINES AND REQUIREMENTS

new 8.1.1

Application

These guidelines set out criteria for evaluating the form and character of the multiple-family residential buildings and associated open spaces within the Dover Crossing Sub-Area (also known as the Dover Crossing Neighbourhood, Attachment 1).

These guidelines are to be used in conjunction with:

1. The Comprehensive Development Bylaws contained in the Zoning and Development Bylaw 5300 for each development site located within the neighbourhood;
2. The general development permit guidelines contained in Schedule 1 of this bylaw; and
3. The detailed *character guidelines* for each development site located within the neighbourhood as shown on Attachment 2.

new 8.1.2

Intent

All the multiple-family developments in the Dover Crossing Neighbourhood merit site-by-site consideration of form and character to ensure:

1. That an appropriate, high quality built form is developed adjacent to the waterfront;

-
2. That the open spaces and the form of development in the Dover Crossing Neighbourhood provide a gateway to Richmond from the No. 2 Road Bridge crossing;
 3. That the intermediate views to the Fraser River and airport, as well as the distant views to the North Shore mountains are preserved and enhanced by the building forms and provision of unobstructed view corridors; and
 4. That the forms and materials applied to the buildings, and the type of street furniture, landscaping, signage, and light standards erected in the open spaces are co-ordinated throughout the area to form a distinct and cohesive urban neighbourhood character.

5.1 SECTION A: RESIDENTIAL DEVELOPMENT

~~5.1.1 ARCHITECTURAL COMPONENTS~~

not included as referenced in following guidelines

~~The Dover Crossing Neighbourhood is characterized by its prominent waterfront location adjacent to the Vancouver International Airport, Richmond's City Centre, and the No. 2 Road Bridge crossing. This waterfront location should be reflected in a marine style of architecture and in the choice of street furniture and lighting standards provided throughout the neighbourhood.~~

kept

The development in the Dover Crossing Neighbourhood should contribute to a residential and pedestrian scale and atmosphere while accomplishing a gateway image at this new entrance to Richmond. The design, form and character of the new buildings should not be monotonous, but rather achieve a sense of diversity, variety and interest without sacrificing visual compatibility.

Guidelines

new 8.2.1

5.1.1.1 Dwelling Unit Type

Multiple residential developments should provide a broad range of unit types and sizes. Included as part of this housing mix are the following minimum requirements for particular unit types:

- a) A minimum of 20% of the units to be no larger than 92.9 square metres (1,000 square feet) of net area;
- b) A minimum of 50% of the ground-oriented units with direct access to a linear walkway shall contain two bedrooms;
- c) A minimum of 10% of the ground-oriented units with direct access to a linear walkway shall contain three or more bedrooms; and
- d) A minimum of 5% of the units shall be "adaptable housing" to accommodate individuals with disabilities. A minimum of 25% of these units provided shall be ground-oriented.

in our definitions

~~"Adaptable housing unit" means a unit that is wheelchair accessible, and has both fixed and adjustable features that allow fixtures to be added, adjusted or removed as necessary to accommodate the needs of persons whether or not they are disabled.~~

5.1.1.2 Companion Animal

8.2.1

Multiple residential development should recognize the importance of the "companion animal" (e.g. dogs and cats) to people living in the community, as well as the health and emotional benefits of the human-bond animal. "Companion animal" means a household animal that people can live with comfortably in a residential unit.

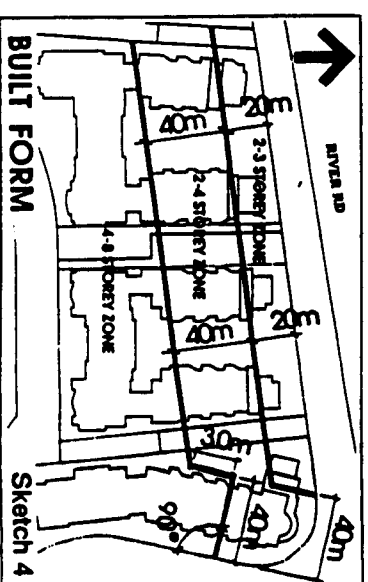
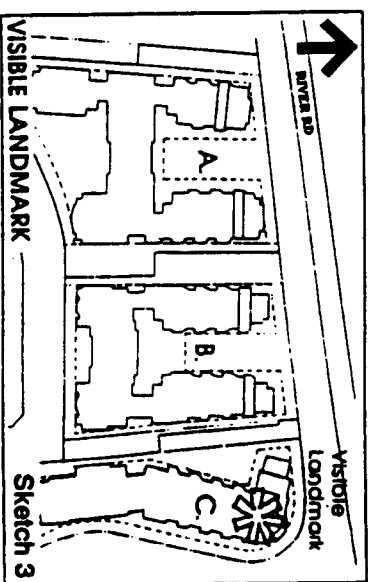
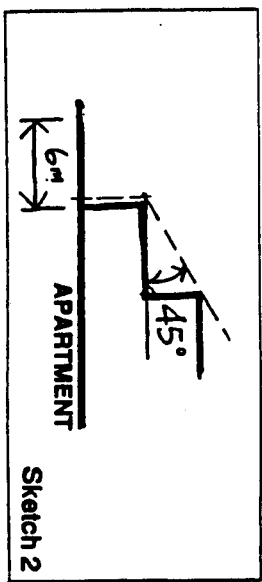
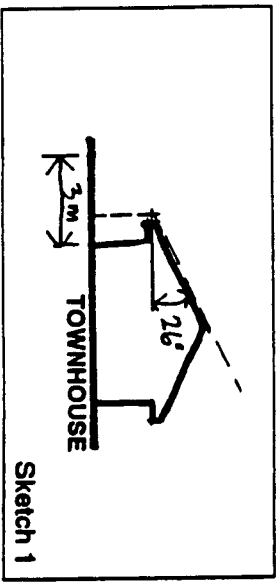
4.2.2 5.1.1.3 Roofscapes

A variety of overall roof heights and forms should be employed to give interest and residential scale to the building forms:

- a) Establish for *townhouse development* a maximum transitional height gradient of no more than 26° to all property lines* (Sketch 1); and
- b) Establish for *apartment development* a maximum transitional height gradient of no more than 45° to all property lines* (Sketch 2).

* *(These gradients may be varied provided privacy, sunlight, view and human scale criteria are met.)*

- c) Employ pitched roofs to express a traditional residential character as the theme of the development. Flat roof elements, in combination with these pitched roofs, may be introduced as a means to create diversity.
- d) Sculpt and terrace upper floors in buildings over four-storeys in height to reduce mass, and create a transition in height and maximize views.
- e) Reduce the scale of the buildings along the walkways by providing secondary roofs or trellises over entries and patios at lower levels.
- f) Provide a major landmark roof feature on the building located at the north-east corner of the Dover Crossing Neighbourhood (Sketch 3).
- g) Create terraced roof forms along River Road by stepping the building stories back from the riverfront (Sketch 4).



5.1.1.4 Entrances, Stairs and porches

Establishing clear and appropriate pedestrian connections between the private residences and the public areas is essential to the vitality and liveability of the neighbourhood.

- B.2.1
- a) Locate major entrances of the buildings onto the linear walkways.
 - b) Provide secondary entrances from the building courtyards and from the private units onto the linear walkways where possible.
 - c) Locate secondary entrances from the private residences onto common walkways located in between development sites.

- B.2.3
- d) Create "front stair" connections between a unit's private outdoor space, and the linear walkway it faces, provided that the grade between the two areas is no greater than 1.5 m (4.92 ft.).

- ←
- e) Create highly visible and identifiable building entrances through the use of landscape and prominent architectural components to create gateways into the courtyards.

5.1.1.5 Windows

The residential character of the neighbourhood should be expressed through appropriately scaled and proportioned windows:

- B.2.3
- a) Use various forms of projections, such as bay windows; and
 - b) Orient interior spaces as well as primary windows of units towards views of the mountains or the school and park site, rather than directly across the linear walkways and courtyards.

5.1.1.6 Balconies and Patios (Private Open Space)

Public and private outdoor space should be clearly defined to enhance the feeling of privacy and the pedestrian experience on the neighbourhood walkways.

2.4 a) Provide substantial landscaping, terracing, screening and low level hedges between private ground-oriented outdoor spaces and the public spaces (Sketch 5).

2.3 b) Articulate building edges to define private balconies and patios that become a natural extension to the residential unit.

5.1.1.7 Acoustics

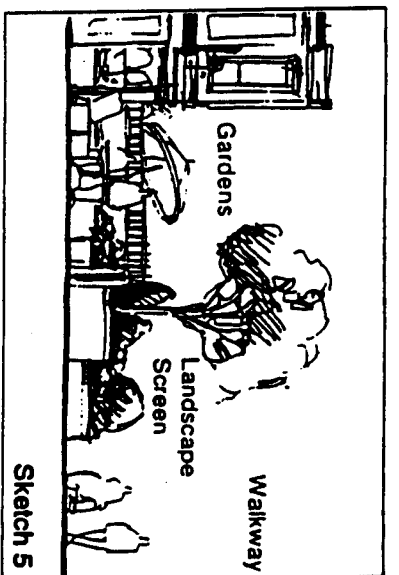
2.3 All Development Permit applications shall require evidence in the form of a report and recommendations prepared by a person trained in acoustics and current techniques of noise measurement demonstrating that the noise level in those portions of the dwelling units listed below shall not exceed the noise level is the A-weighted 24-hour equivalent (Leq) sound level and will be defined simply as noise level in decibels.

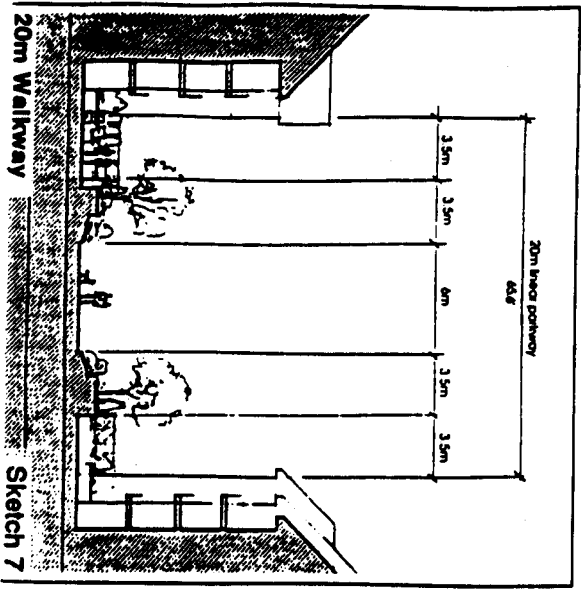
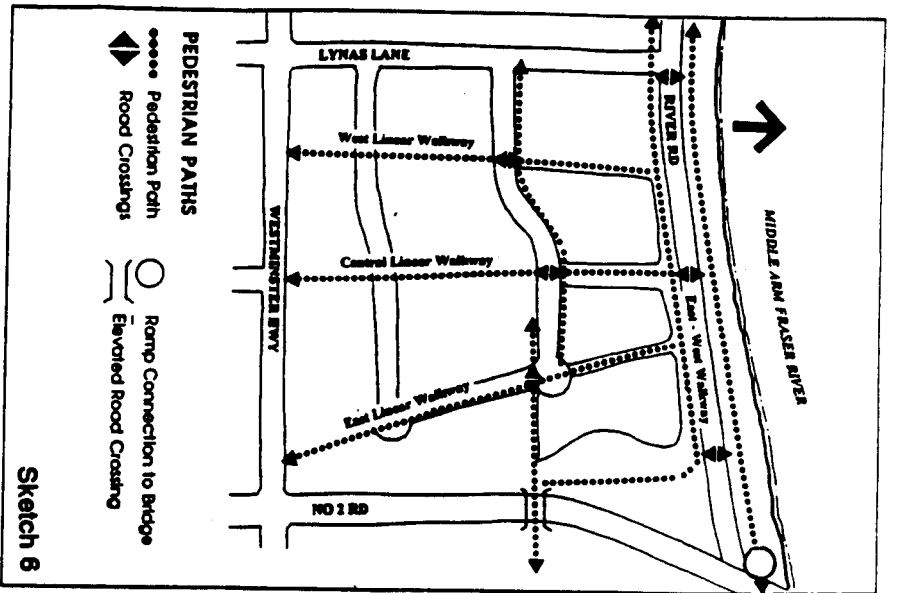
Portions of Dwelling Units

Noise Levels (Decibels)

- | | |
|-------------------------------------|----|
| a) Bedrooms | 35 |
| b) Living, dining, recreation rooms | 40 |
| c) Kitchen, bathrooms, hallways | 45 |

In addition to the above, the trained professional is to assist in the design of the private patios and balconies to minimize the noise levels with recommendations for building material selection and space planning.





5.2 SECTION B: PEDESTRIAN-ORIENTED STREETS CAPES

5.2.1 Linear Walkways (Sketch 6)

B.2.4

The prioritization of the pedestrian is a major element of the neighbourhood theme and character. Three linear walkways extending from Westminster Highway through the area form the most significant landscape component in the Dover Crossing Neighbourhood.

Each of these three walkways will have focal areas in the form of arrival plazas, and entry courts. The central and east linear walkway will also offer viewing points at their termination at River Road. Each of the linear walkways will rise gradually up from the south to the north to maximize viewing opportunities to the waterfront and mountains. The central walkway is the most significant of the three walkways, because it will provide a direct connection through the neighbourhood, across River Road to the riverfront and trail system.

Prominent at-grade crossings accessible by pedestrians, cyclists and people with disabilities will be provided over River Road to provide distinct and safe crossings to the dyke.

An east-west walkway shall also be provided along the south side of River Road to link the three linear walkways to No. 2 Road, as well as to the pedestrian crossings over River Road.

Guidelines

B.2.4

5.2.1.1 Width of the Linear Walkways

- a) Ensure that the centre linear walkway is a minimum of 20 m (65.62 ft.) in width and is comprised of 6 m (19.69 ft.) of hard walking surface, 3.5 m (11.48 ft.) of low level landscaping, and 3.5 m of terraced planting (Sketch 7).

3.2.4

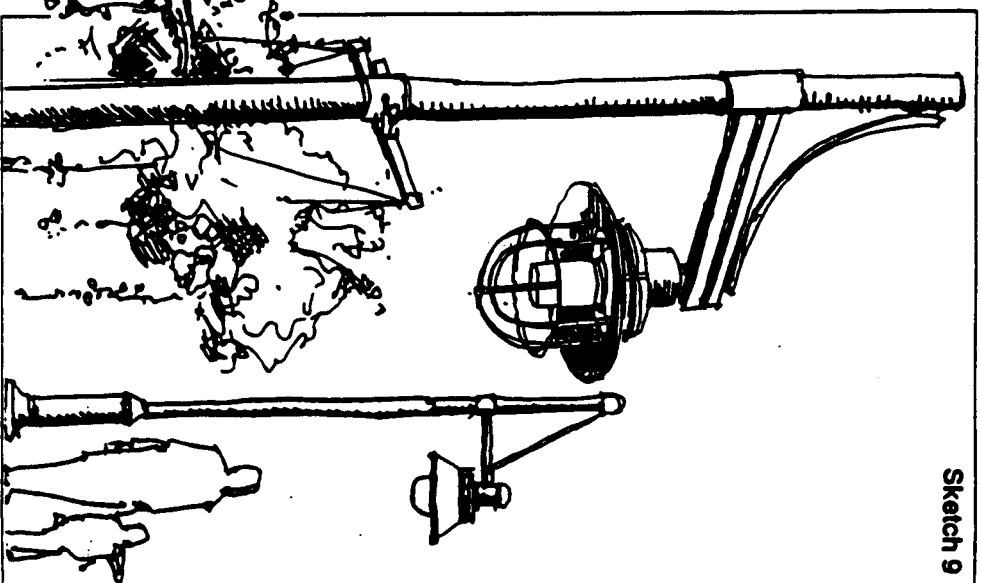
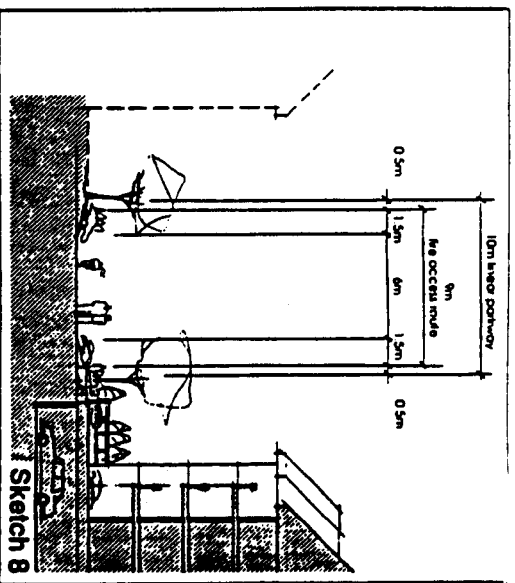
- b) Ensure that the east and west linear walkways are a minimum of 10 m (32.81 ft.) in width and are comprised of 3 m (9.84 ft.) of hard walking surface, and 3.5 m (11.48 ft.) of planters and low level landscaping (Sketch 8).
- c) Require that all elements and connections in the pedestrian circulation system be accessible by bicycles, and people with disabilities including provisions for the visually impaired.

5.2.1.2 Paving Materials

- a) Use the same decorative and durable walking surface on all the linear walkways, and the pedestrian road crossings throughout the neighbourhood.
- b) Provide textured surfaces as part of the paving patterns to provide a guide route for the visually impaired.

5.2.1.3 Lighting

- a) Install low level, possibly wall mounted pedestrian lighting which provides light and security onto the walkways, but does not produce glare into the adjacent residential buildings.
- b) Erect ornamental lights at the focal areas (arrival plazas, entry forecourts, viewing areas) along the entire pedestrian circulation system. These lights should have a maritime design character and include: post and bracket support system; a pendant fixture with metal hood; and a painted metal finish in a maritime colour. All the lighting standards and street furniture should be finished in a common colour scheme throughout the neighbourhood to enhance the area's special character (Sketch 9).



8.2.4

5.2.1.4 Signage

- a) Install directional signage at strategic locations throughout the neighbourhood which provides information relating to the public areas (linear walkways, crossings, school, park, etc.), as well as to the private residences (building entries and facility signs). The signs should be co-ordinated with the design character and location of the lighting standards throughout the entire neighbourhood. The signs should be visible during day time and evening hours.

5.2.1.5 Landscaping

- a) Plant ornamental, possibly flowering trees along the both sides of the three linear walkways. Trees should have a minimum calliper of 50 mm (1.97 in.), and be spaced at 6.0 m (19.69 ft.) to 7.0 m (22.97 ft.) intervals.
- b) Provide terraced planters along the linear walkways containing plants that create a height gradient separation between the public and private areas. The dimensions of the planters should be at least 1.2 m (3.94 ft.) wide and 0.9 m (2.95 ft.) deep.

5.2.1.6 Arrival Plazas

Arrival Plazas are enlarged circular seating areas which are intended to welcome and orient the visitor (Sketch 10).

- a) Locate the "arrival plazas" at the entrances of the linear walkways from Westminster Highway and Dover Crescent.

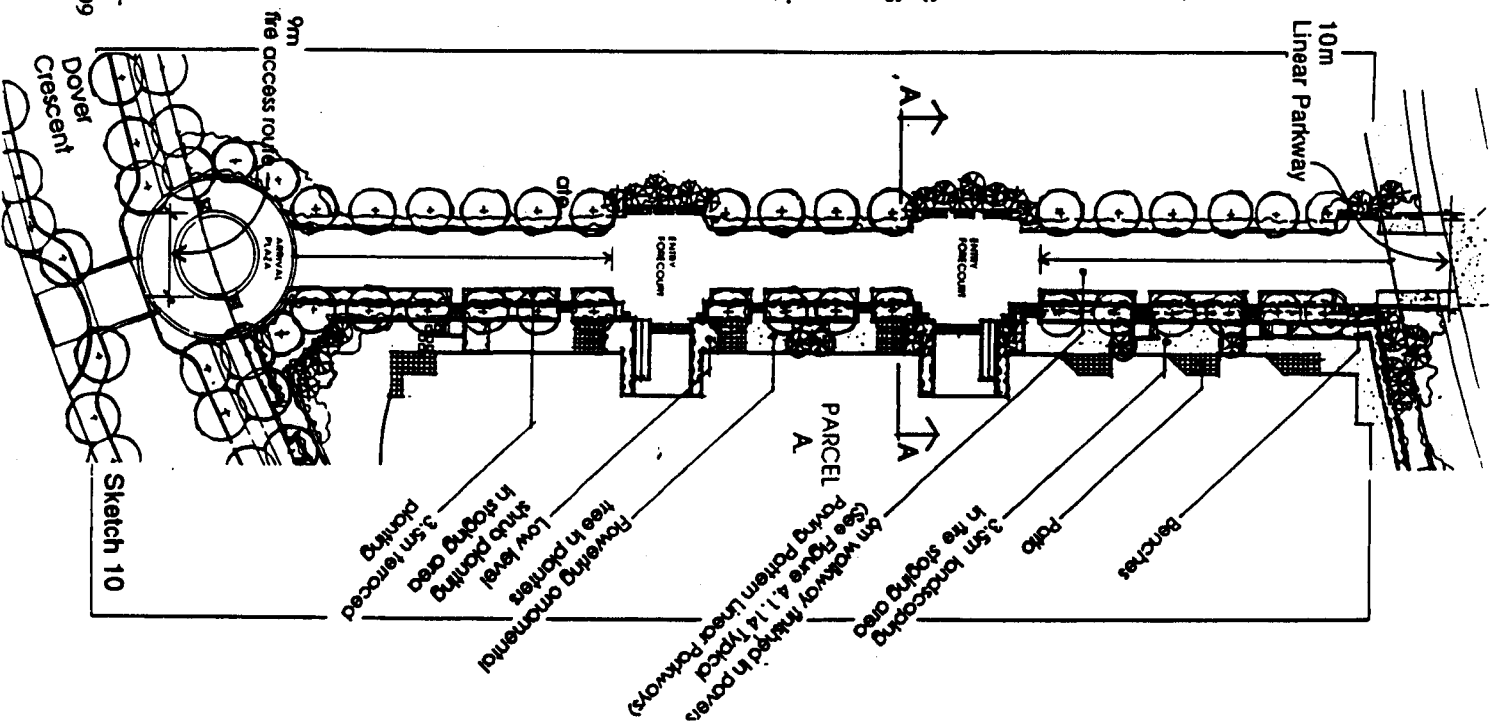
b) Design the "arrival plazas" to contain the following elements:

- A widened, hard circular surface with a distinct paving pattern;
- Bollards to prevent non-essential vehicular traffic;
- High quality vandal resistant street furniture, including benches and garbage receptacles;
- Accent planting; and
- Signage and ornamental light standards.

5.2.1.7 Entry Forecourts

Entry forecourts are located along the linear walkways at the building entrances. These forecourts announce the location of the building entries as well as provide seating areas along the walkways (Sketch 10).

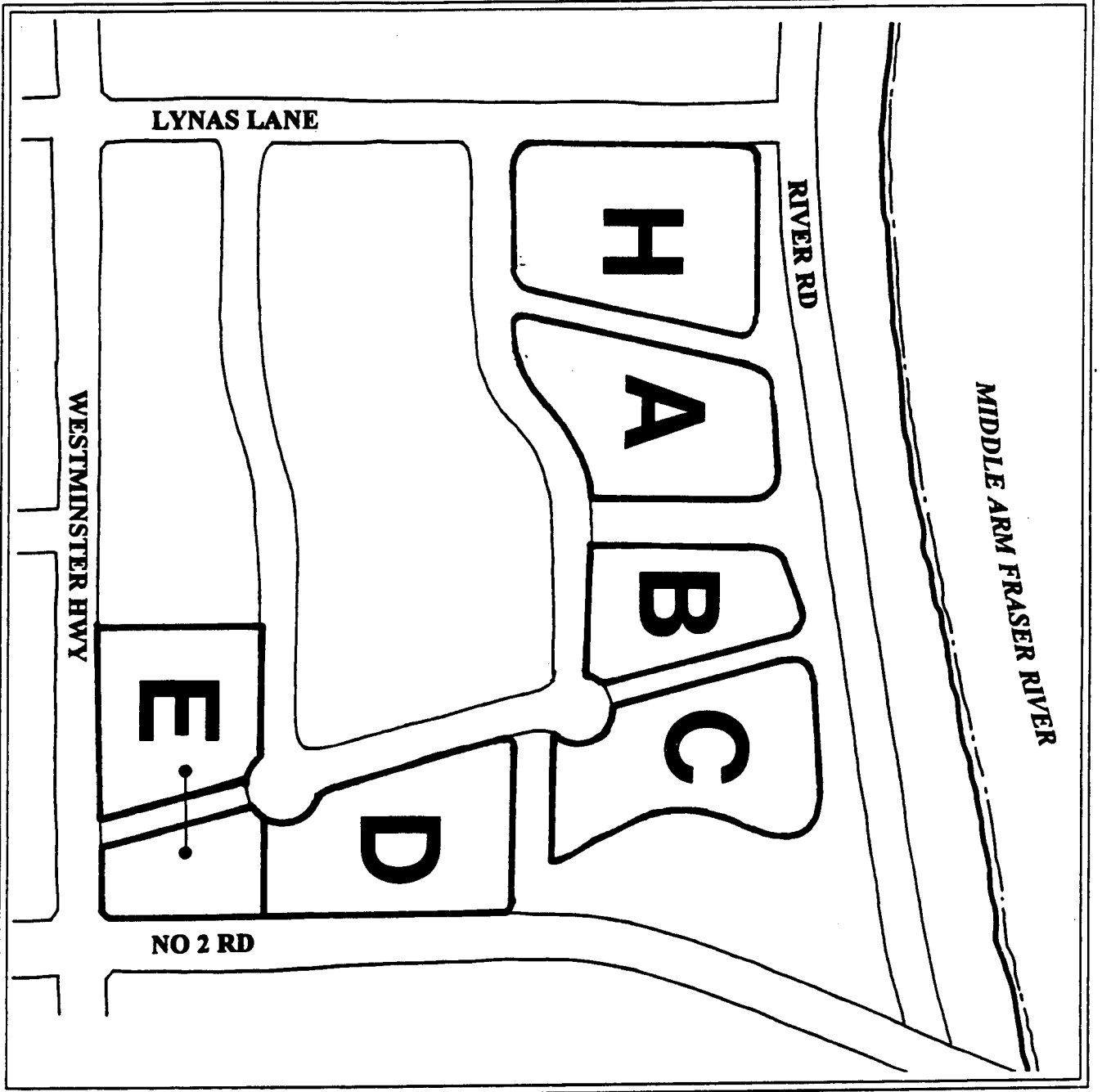
- a) Locate entry forecourts at all major entrances to buildings along the linear walkways.
- b) Design the entry forecourts to contain the following elements:
 - A widened, hard circular surface with a distinct paving pattern;
 - Ornamental light standards and signage;
 - Benches at the edge of the space;
 - Bicycle racks;
 - Accent planting; and
 - Garbage receptacles.



8.1.1

Note: More detailed information and criteria for evaluating the design, form and character of the buildings and open spaces within the Dover Crossing Neighbourhood is included in the site specific *character guidelines*. Developers seeking Development Permit approval must consult both these guidelines and the detailed *character guidelines* which follow.

Attachment 2
LOCATION OF DEVELOPMENT SITES WITH ADOPTED DETAILED CHARACTER GUIDELINES



DETAILED CHARACTER GUIDELINES

§. 2.5

The Detailed Character Guidelines for the specific development sites forming part of the Official Community Plan Bylaw 7100, Schedule 2.2A are available at the Urban Development Division.



UD.11.9826

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Original Adoption: March 15, 1999

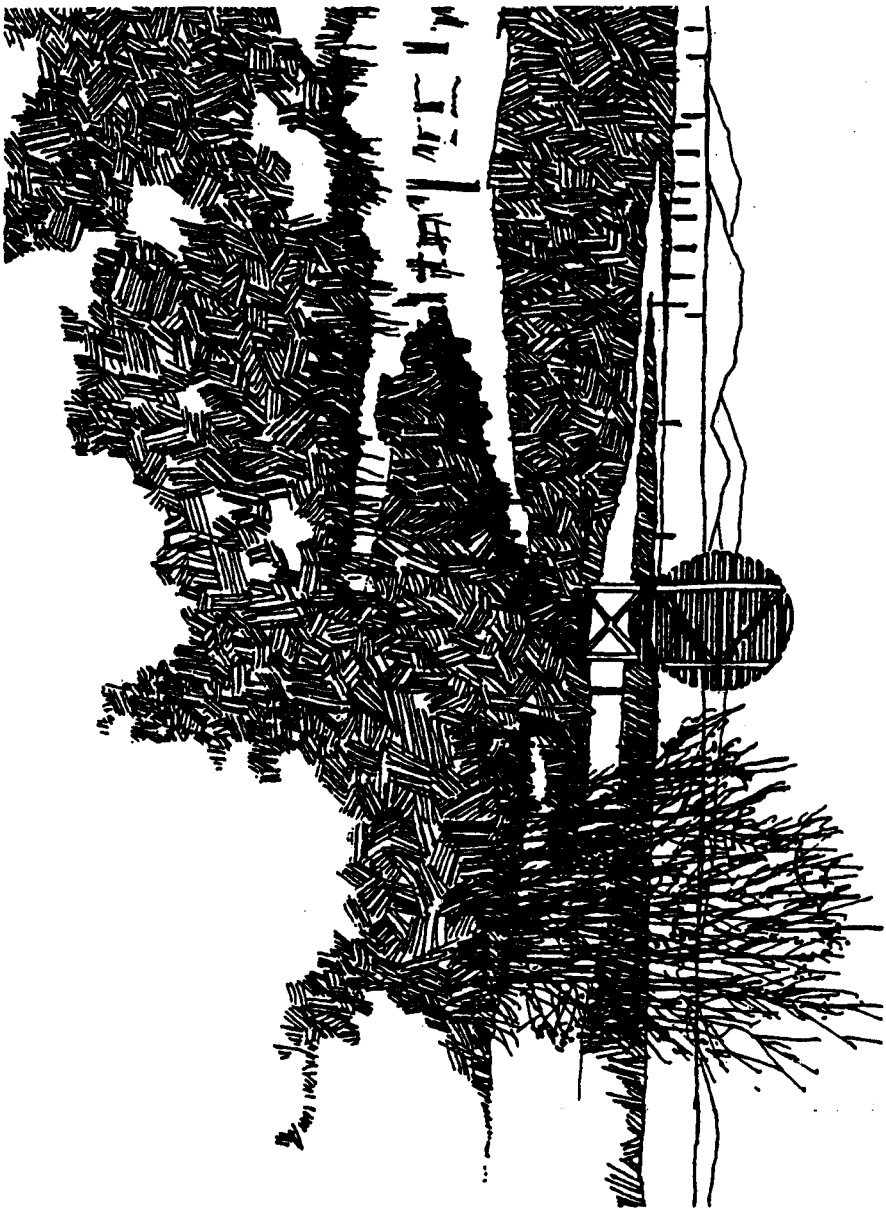


City of Richmond



THOMPSON Area Plan

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TERRANOVA Sub-Area

An Official Community Plan

Bylaw No. 7100 Schedule 2.2B

THOMPSON AREA

Terra Nova Sub-Area Plan

1.1 This plan sets out the goals, objectives, policies and development guidelines for the Terra Nova Sub-Area, a part of the Thompson Planning Area.

plan interpretation
Additional Conservation Area and Environmentally Sensitive Area (ESA) policies, guidelines, and locations are included in Schedule 1 of this Bylaw and its attachments (Schedule 1 is a separate document which applies to the entire City). Readers should check Schedule 1 as it takes precedence over this plan in the case of Conservation Areas and ESAs.

The plan may be amended from time to time and users should check with the City's Urban Development Division to make sure that they have an up-to-date version containing all of the adopted amendments.

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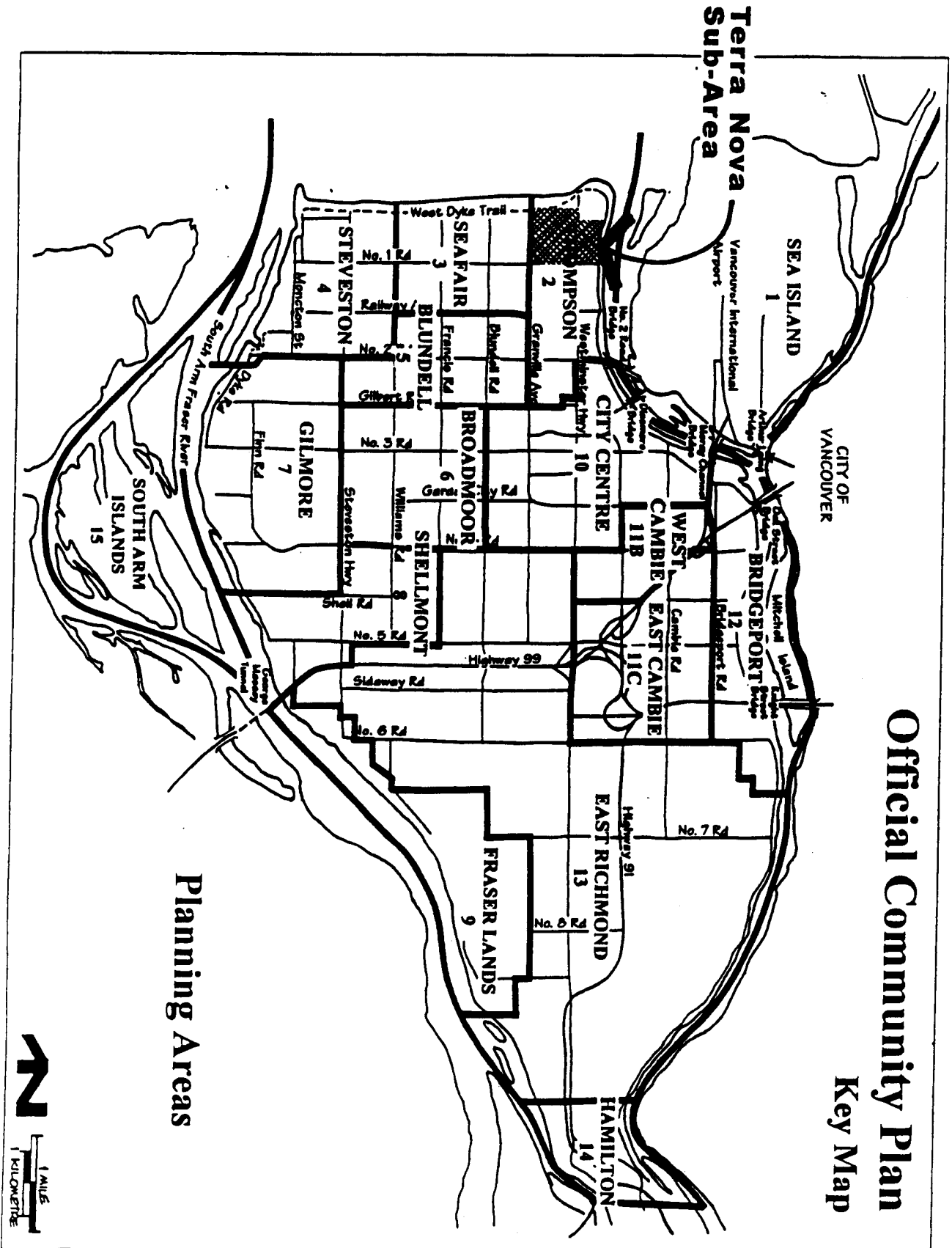
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LIST OF ATTACHMENTS *- see list of Maps*

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Official Community Plan Key Map



Planning Areas

UD: 11.9827

VI

Original Adoption: March 15, 1999

1.0 INTRODUCTION

1.1

This plan provides a series of policies, objectives and guidelines consistent with the development of the Terra Nova Area as a residential neighbourhood adjacent to sensitive environmental areas and significant natural amenities. The plan area currently consists of approximately 112 hectares (276.75 ac.) of dyked floodplain in the north-west portion of Lulu Island, occupied by single-family residences, townhouses, fallow agricultural land, small commercial premises, a golf course and City park land. The extent of the plan area is noted on Attachment 1.



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Original Adoption: March 15, 1999



2.0 GOALS

1.2

The goals of this plan are to:

- Provide public access to the waterfront;
- Preserve significant natural areas;
- Preserve and enhance recreational facilities;
- Provide a range of residential household sizes and types at a modest density;
- Enhance and preserve air, water and soil quality; and
- To provide safe and efficient means for pedestrians, cyclists and vehicles to circulate through the area.

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Original Adoption: March 15, 1999

5.0

3.0 PUBLIC, INSTITUTIONAL AND OPEN SPACE

OBJECTIVE 1: TO PROVIDE A VARIETY OF OPEN SPACES AND PUBLIC FACILITIES TO SERVE THE NEIGHBOURHOOD AND THE CITY AS A WHOLE.

It is Council's policy to:

- 1.1 Provide an open space between River Road and the Fraser River to enhance the dyke trail system upon which buildings will not be erected;
- 1.2 Establish a childcare facility on the central open space within the plan area;
- 1.3 Preserve and enhance the western open space along the perimeter dyke as natural wildlife habitat;
- 1.4 Provide for the active and passive recreational needs of the plan area within the central open space and a smaller park in the south portion of the plan area;
- 1.5 Permit the establishment of a public elementary school within the central open space, if warranted;
- 1.6 Protect archaeological resources within the plan area; and
- 1.7 Designate land as public, institutional and open space as indicated on Attachment 2.

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Original Adoption: March 15, 1999

3.0

4.0 RESIDENTIAL

OBJECTIVE 2: TO PROVIDE GROUND-ORIENTED HOUSING IN A RANGE OF SIZES AND FORMS TO A RANGE OF HOUSEHOLD TYPES

It is Council's policy to:

- 2.1 Permit the establishment of single-family residences on those lands designated "Residential (Single-Family)" on Attachment 2;
- 2.2 Permit the establishment of townhouses and small-lot single-family residences on those lands designated "Residential (Townhouse)" on Attachment 2;
- 2.3 Permit the establishment of new housing only with the full range of urban utilities and services;
- 2.4 Encourage and facilitate the provision of affordable housing; and
- 2.5 Require the issuance of a Development Permit prior to the construction of any townhouses in the plan area.

*insert.
new policies.
dealing with
airport - same
as in Dover plan* →



UD.11.9827

8

Original Adoption: March 15, 1999



5.0 COMMERCIAL

OBJECTIVE 3: TO MAINTAIN LOCAL COMMERCIAL SERVICES IN THE TERRA NOVA AREA.

It is Council's policy to:

- 3.1 Permit the establishment of commercial uses on lands designated "Commercial" on Attachment 2.;
- 3.2 Require the issuance of a Development Permit prior to the issuance of a building permit for any commercial buildings or structures in the plan area; and
- 3.3 Require that Development Permits issued for commercial uses comply with the following guidelines:
 - a) Commercial uses shall be screened from residential uses by means of walls, solid fences and landscaping;
 - b) Landscaping shall be provided adjacent to public roads, lanes and walkways; and
 - c) Buildings and structures shall be sited in a manner that would minimize the impact of the commercial uses on neighbouring residences.



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Original Adoption: March 15, 1999



2.0

6.0 AGRICULTURE

OBJECTIVE 4: TO MAINTAIN THE AGRICULTURAL LAND BASE IN THE AREA.

It is Council's policy to:

- 4.1 Maintain lands presently designated as "Agricultural Land Reserve" as part of the agricultural land base of Richmond; and
- 4.2 Permit open land recreational uses in areas designated "Agriculture" on Attachment 2, subject to the Agricultural Land Commission Act.

→

*insert
new policy
dealing with
urban/rural
boundary
next to ACR*



UD.11.9827

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Original Adoption: March 15, 1999



4.0

7.0 CIRCULATION SYSTEM

OBJECTIVE 5: TO PROVIDE A SAFE AND EFFICIENT MEANS OF ACCESS FOR PEDESTRIANS, CYCLISTS AND VEHICLES THROUGH AND AROUND THE AREA.

It is Council's policy to:

- 5.1 Restrict direct access to the area from No. 1 Road;
- 5.2 Provide a hierarchy of roads and trails as indicated on Attachment 3;
- 5.3 Provide a 20 m (65.617 ft.) wide road allowance where a trail is to be provided in a new local street in order that the boulevard of said street can be landscaped with trees between the curb and the sidewalk; and
- 5.4 Accommodate pedestrians, cyclists and vehicles on all roads within the plan area.



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Original Adoption: March 15, 1999



8.0 DEVELOPMENT PERMIT GUIDELINES

8.1.1

Schedule 1 of this bylaw designates areas where a development permit is required prior to the commencement of a development and sets out objectives and general guidelines for development in such areas. Area and sub-area plans may contain additional development permit guidelines complementing those contained in Schedule 1. This plan contains such additional development permit guidelines for the three sites in Terra Nova shown on Attachment 2A. The guidelines in Section 8.2 below apply to the two multiple-family sites; those in Attachment 5 below apply to the commercial site.

8.1.2

A key housing policy of Richmond's Official Community Plan is to carefully integrate new housing development into existing neighbourhoods. All multiple-family projects merit site-by-site consideration of form and character in order to achieve the desired relationship to surrounding areas, and to ensure high standards of new housing design.

Terra Nova in particular requires special consideration. The neighbourhood's location, traffic considerations, and significant redevelopment potential present issues which need to be addressed through design guidelines for both multiple-family and commercial sites.

8.1 APPLICATION OF CITY-WIDE GUIDELINES

8.1.1

Schedule 1 of this bylaw contains a set of city-wide development permit guidelines for both multiple-family and commercial developments, and these also apply in the Terra Nova area. Those wishing to develop in Terra Nova are advised to consult and follow the guidelines stipulated in this and all other pertinent documents.

8.2 ADDITIONAL DEVELOPMENT PERMIT GUIDELINES FOR MULTIPLE-FAMILY RESIDENTIAL AREAS IN THE TERRA NOVA SUB-AREA SHOWN ON ATTACHMENT 2A

8.2.1 Architectural Guidelines

The intent of these guidelines is to achieve an urbanized single-family residential scale and character, incorporating elements of craftsman style; and to ensure a compatible interface with nearby single-family dwellings, particularly to the west of the site.

A. Building Scale and Form

- 8.2.2 a) Provide a smooth transition between townhouse units and single-family homes to the west. Build duplexes (structures with no more than two attached units together) along the western property line. A space of at least 3 m (9.8 ft.) should separate each duplex structure along the street front.

- 8.2.3 b) Avoid the appearance of blank walls. Suggested treatment includes (but is not limited to) the use of: trim; more intense colour accents; secondary roof elements; building recesses; and stepped building forms emphasizing lowered height on ends and fronts.

- 8.2.2 c) Townhouse units should be designed in clusters of 25 units or less, and have less than six units in a row. However, exceptions can be made to allow up to eight units in a row IF larger clusters are separated from other clusters on all sides by a considerable amount of open space (or combination of open space and internal roadways). Figure 1 gives one example of appropriate open space separation.

- d) Townhouse units should have a maximum transitional height gradient of 26° to all property lines. These gradients may be varied provided privacy, sunlight, view and human scale criteria are met. Buildings should have an apparent length of less than 70 m (229.66 ft.).

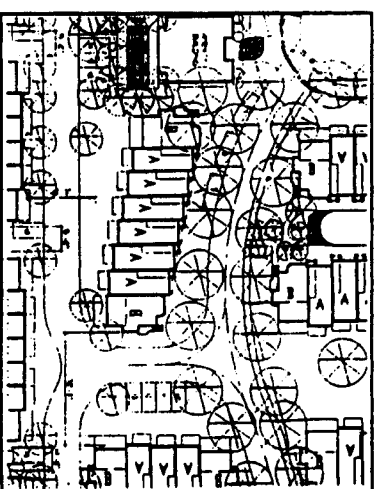


Figure 1.

This block of 8 units is separated from other units on three sides by an internal roadway, semi-public and private open space. On the fourth side it is separated by an amenity building and profuse landscaping.

Source: Ramsey Warden Architects,
Terra Nova South

B. Roofscapes

B. 2. 2

- a) All roofs should be pitched, with a minimum slope of 45°. Use gable ends and dormers to create diversity in the roof line and to present the area's craftsman character.

B. 2. 3

- b) Select roofing materials which suit the level of articulation desired in roof forms. Cedar shingles would facilitate this, whereas heavy-tiled roofing would not.

C. Windows

Windows should be operable, of residential scale, with multiple panels, and should add to the sense of neighbourliness and safety and security by providing visual interest and surveillance:

- a) Reinforce the smaller components of buildings which express strong unit identity through appropriate window placement;
- b) Use bay windows, window boxes, small balconies, and similar features to avoid a flat, box-like building;
- c) Windows should be visually prominent, separated from the building face and defined with colour, trim/shutters, or other decoration; and
- d) Non-traditional windows such as plastic bubble windows or tinted glaze are discouraged. If skylights are used, they should not be visible from the street or from internal roadways.

D. Entrances, Stairs, and Porches

8.2.1

Entrances should enhance the pedestrian-scale character of the area, by strengthening a unit's connection with public streets and internal roadways; and by masking the appearance of garage doors (see below under Landscape Guidelines - Internal Roadways).

- a) Townhouses adjacent to public roads and trails should have their main front doors visible and accessible from that road or trail (see Attachment 4), using stairs and new grading if required to do so. Direct access for each unit should also be provided to the internal north-south pedestrian and view corridors (see below under Internal Roadways).
- b) Where stairs are used to provide street or trail access from units, embellish stairs with decorative wooden handrails.

8.2.2 Landscaping and Open Space

A. General

8.2.1

- a) Special views, such as of the mountains or waterfront, should be taken advantage of where possible.
- b) Sunlight should not be blocked for a minimum of 75% of dwellings in each development on every day of the year. The minimum north-south spacing between residences to conserve available sunlight is determined by the sun angle of 17° at noon during the winter solstice.

8.2.4

- c) Screening and landscaping, including street trees, should be provided in each townhouse development.

8.2.4

- d) Townhouse units should have a minimum private outdoor space of 9 m (29.53 ft.) in depth and 37 m² (398.28 ft²) in area. The depth of private outdoor space may be reduced to 5.25 m (17.22 ft.) where adequate privacy screening is provided.

B. Public Streets

Enhance the pedestrian character and visual interest of public streets.

- a) Along the public street right-of-way, provide sidewalks on both sides of the street. Make these sidewalks attractive by providing a grass strip with street trees between the sidewalk and the street, and placing all utilities underground.
- b) Avoid the appearance of a solid wall of buildings along No. 1 Road, by varying street edge treatment through use of strategically placed open spaces with intensely clustered plantings; staggered or varied building setbacks; and other techniques.

C. Internal Roadways

8.2.1 Use internal roadways to maximize the sense of open space and enhance the area's pedestrian character. Where possible, vehicle and pedestrian access should be separated from each other and appropriately located.

8.2.4

- a) Create at least one north-south view corridor and pedestrian connection through a central boulevard running north-south through each development site. Treat the boulevard as if it were a public street, by planting large calliper trees on both sides, and providing a sidewalk on at least one side. An example of this concept is shown in Figure 2.
- b) Vary paving treatments through the use of special pavers, cobblestones or stamped concrete as accents at driveway entrances.

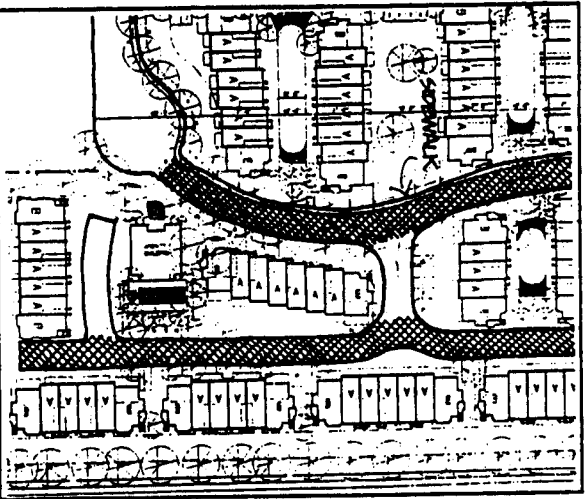


Figure 2.

8.2.3 c) Reduce the visual impact of garages along internal roads and driveways by interrupting garage doors with covered secondary pedestrian entrances, as shown in Figure 3. Pedestrian entrances should occupy approximately half the width of each garage door.

D. Pedestrian Paths

8.2.4 a) Enhance pedestrian, wheelchair and cycling access to No. 1 Road by providing east-west corridors for non-automotive users in strategic locations.

8.2.1 b) In Residential Area 2, include a semi-public walkway with lights and low landscaping. The walkway should be ungated, and connect to No. 1 Road and to the new road to the east. This corridor on the north property line, should eventually be matched by a similar corridor on the adjacent site.

8.2.4 c) Landscape all pedestrian paths with trees and other plant materials.

8.2.3 Other

8.2.3 a) A minimum of 75% of the units should have single garage doors. Tandem parking is encouraged.

8.2.1 b) There should be no vehicle gates at entrances to the site.

8.2.5 c) Access provision should be made for emergency vehicles, moving vans, and service vehicles.

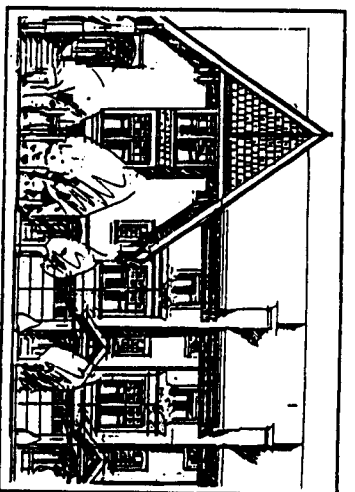
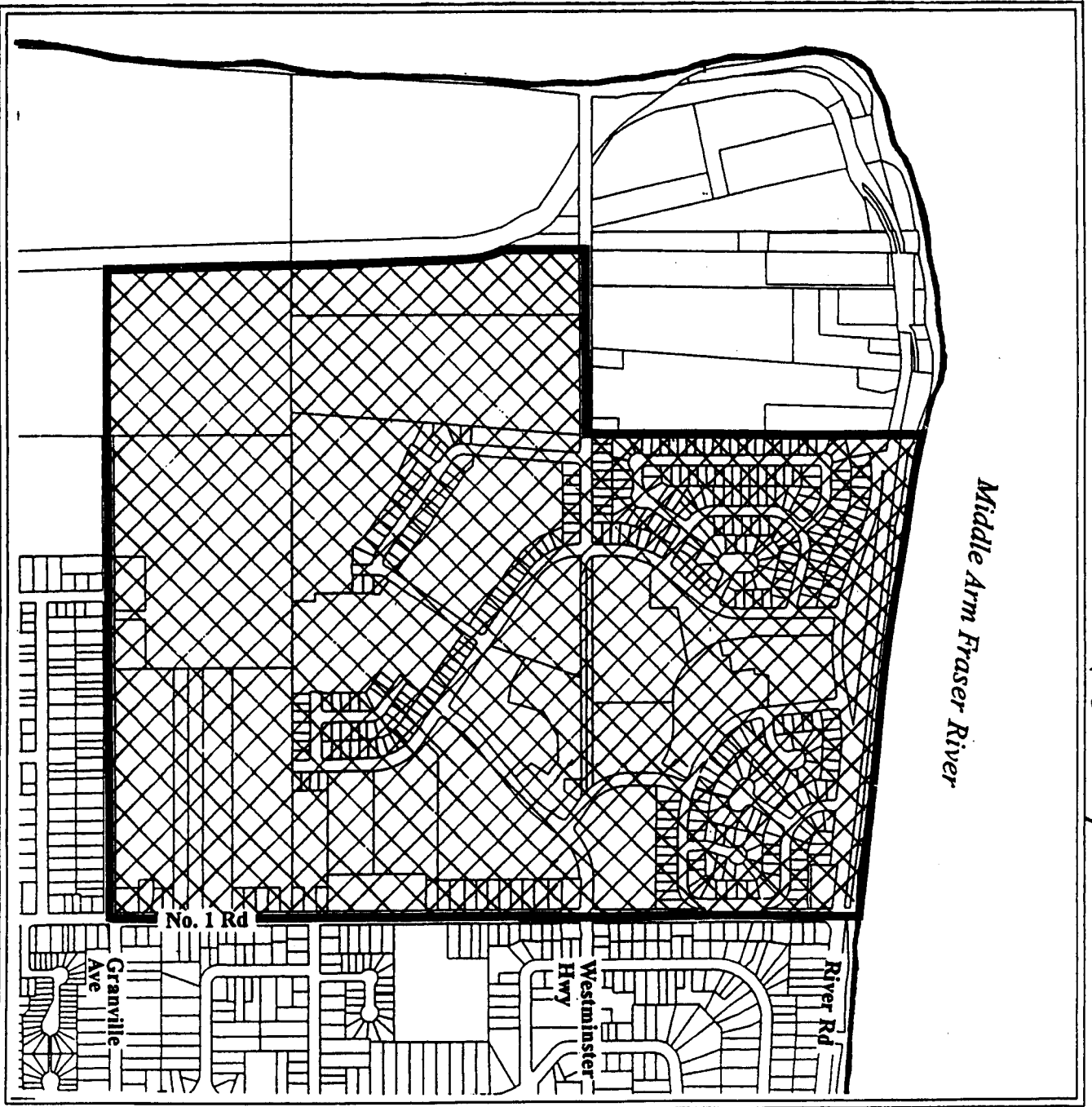


Figure 3.

Source: Ramsey Worden Architects,
Terra Nova South

See Plan Area Map

Middle Arm Fraser River



Attachment 1

AREA PLAN

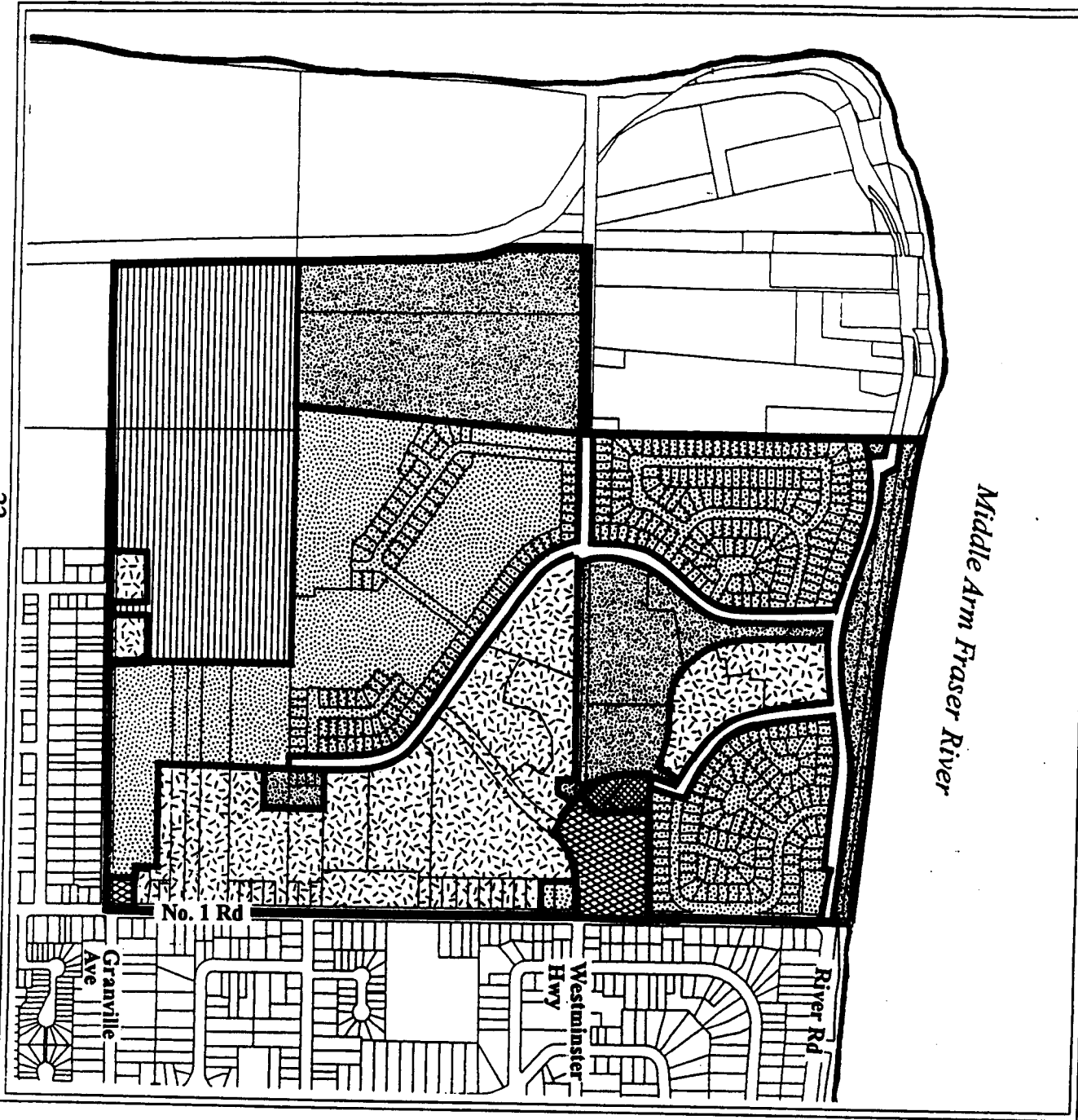


Area Plan
Boundary







Terra Nova
Sub - Area
Official
Community Plan

See Land Use Map

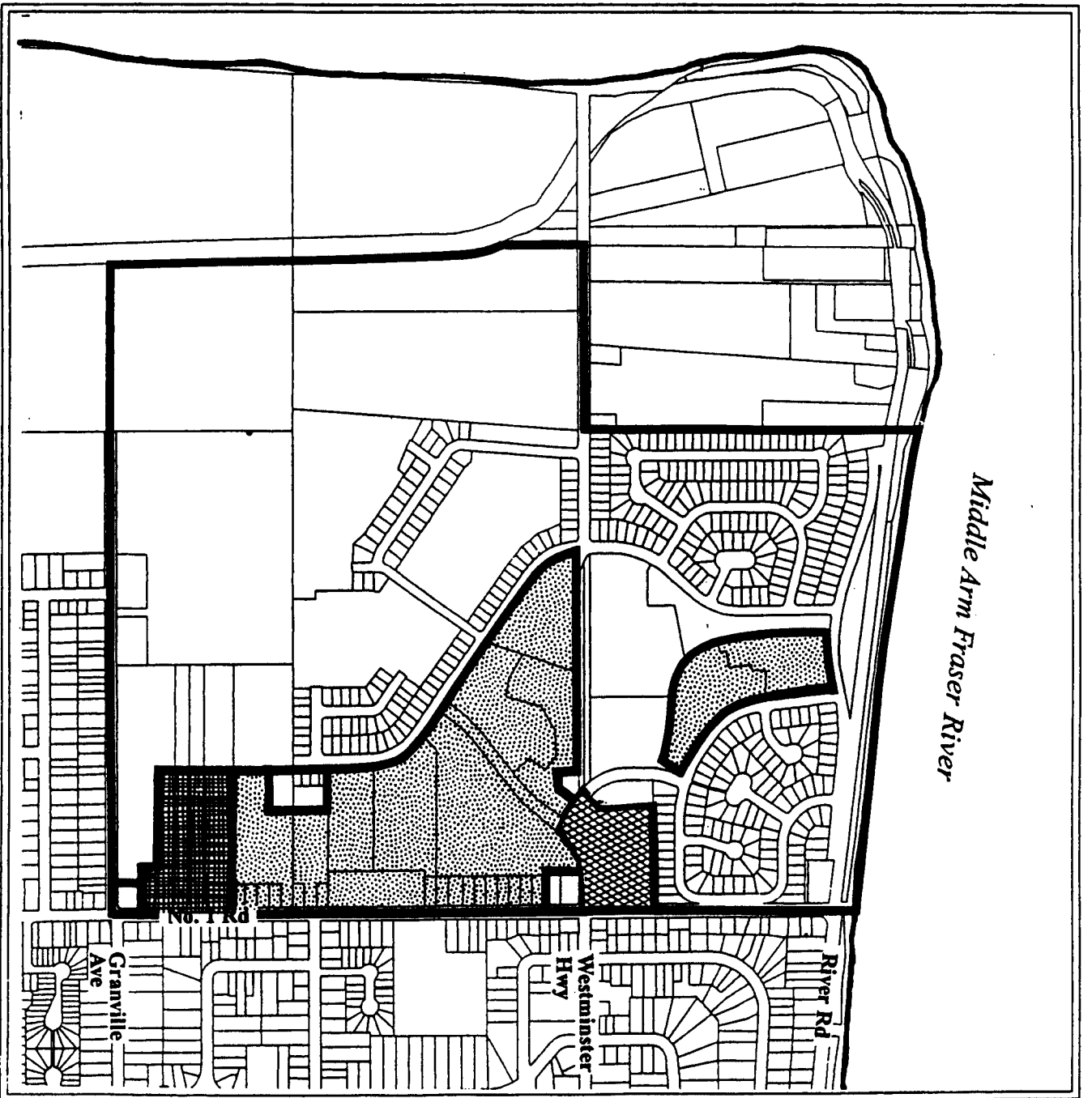
Middle Arm Fraser River



**Attachment 2
LAND USE
DESIGNATION**

-  Residential (Single Family)
-  Residential (Townhouse)
-  Public, Institutional and Open Space
-  Agriculture
-  Commercial
-  Residential Reserve








Middle Arm Fraser River

River Rd

Westminster Hwy

Granville Ave

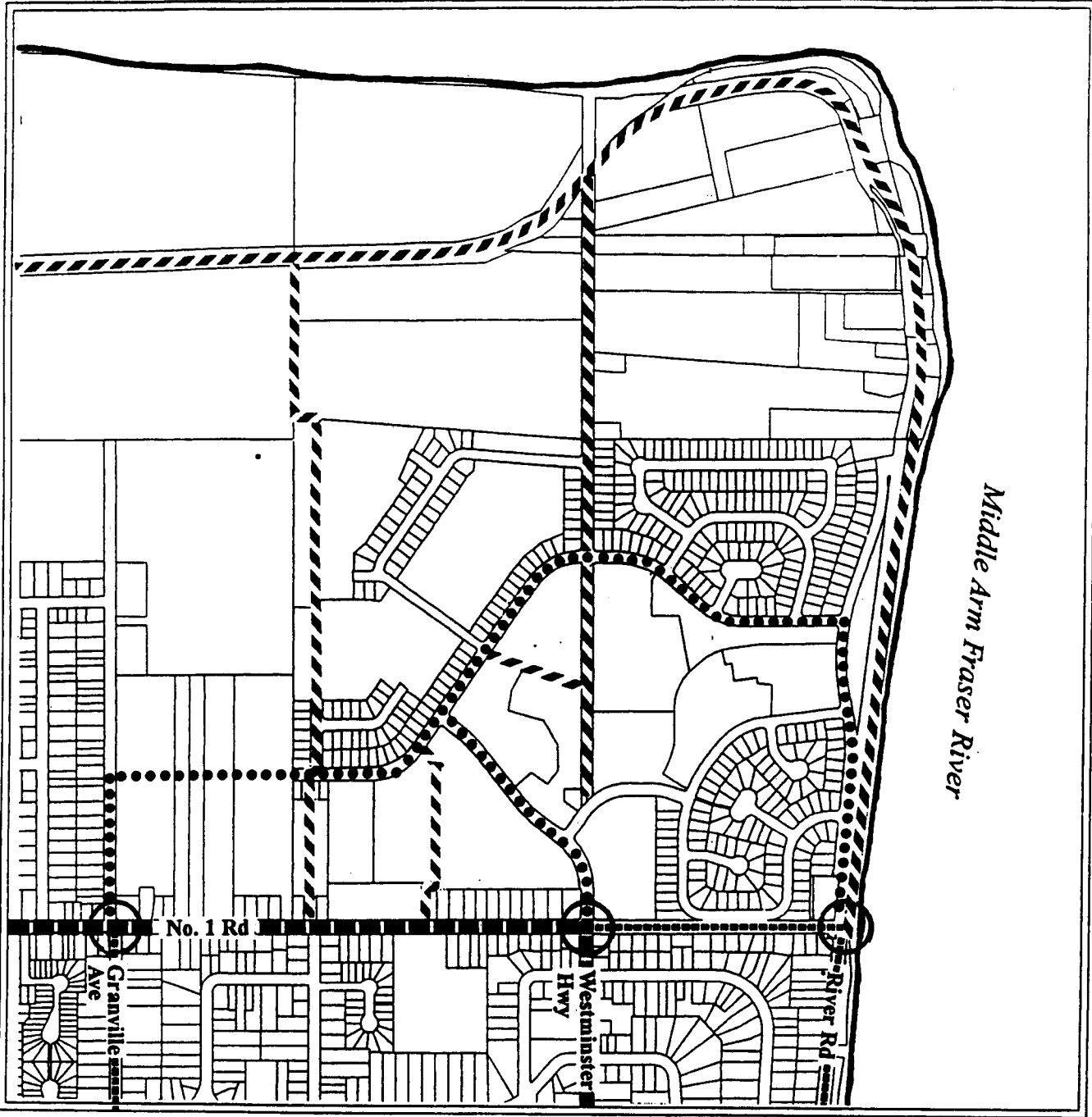
**Attachment 2A
DEVELOPMENT
PERMIT
AREAS**

-  Residential Development Permit Area 1
-  Residential Development Permit Area 2
-  Commercial

Terra Nova
Sub-Area
Official
Community Plan



Middle Arm Fraser River



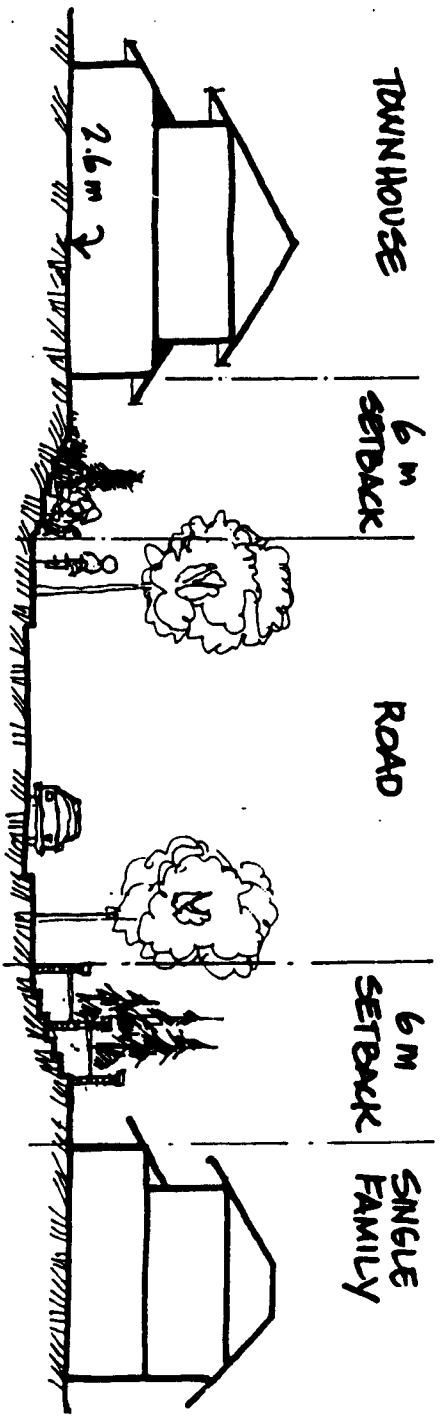
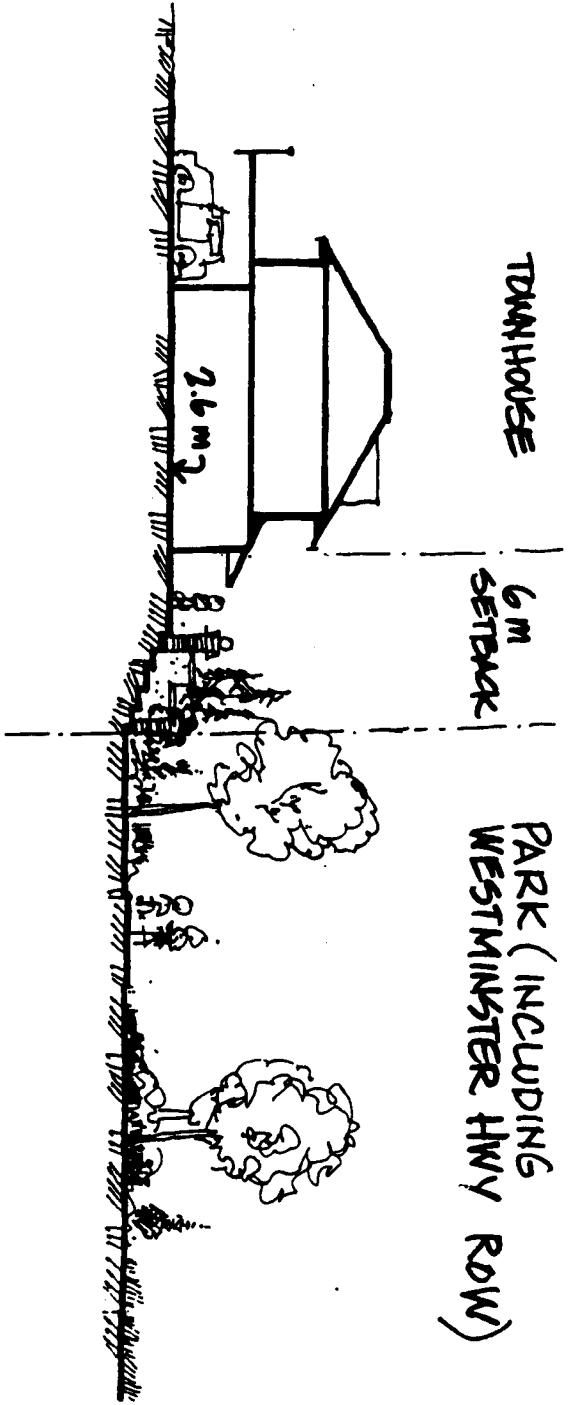
Attachment 3

CIRCULATION SYSTEM

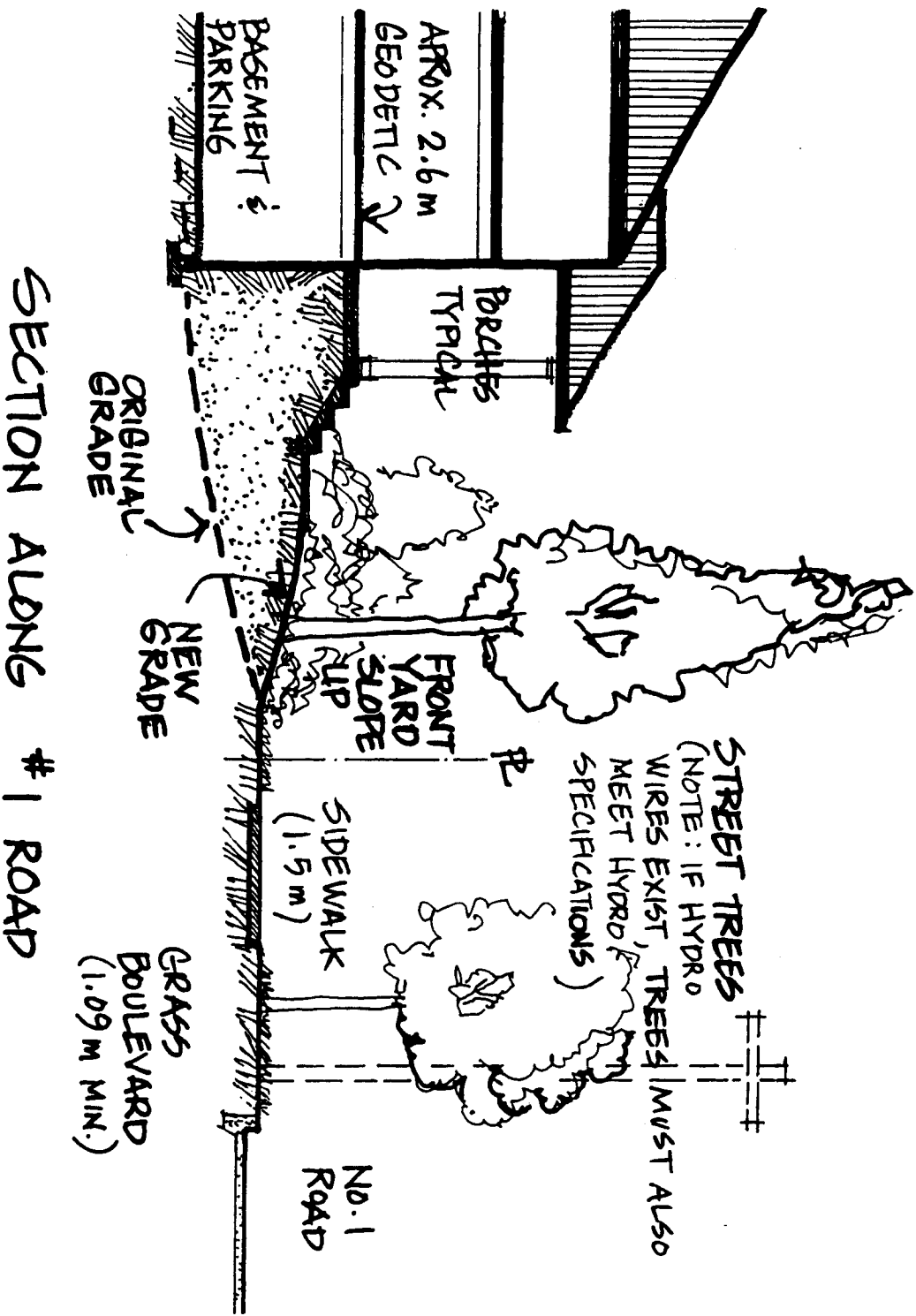
- Major Arterial Roads
- Local Arterial Roads
- Collector Roads
- /// Trails
- Major Intersections

Terra Nova
Sub - Area
Official
Community Plan

**GUIDELINES FOR TOWNHOUSING ADJACENT TO
PUBLIC OPEN SPACE AND ROADS**



See Development Permit Guidelines



See Development Permit Guidelines

**ATTACHMENT 5 - ADDITIONAL COMMERCIAL
DEVELOPMENT PERMIT GUIDELINES**

Application and Intent

8.1.1

The guidelines contained in this attachment set out criteria for evaluating the form and character of the commercial buildings and open spaces for the commercial site at the north-west corner of No. 1 Road and Westminster Highway, shown on Attachment 2A. These guidelines are to be used in conjunction with the general development permit guidelines for commercial areas contained in Schedule 1 of this bylaw and the regulations contained in the Richmon Zoning and Development Bylaw No. 5300.

8.1.2

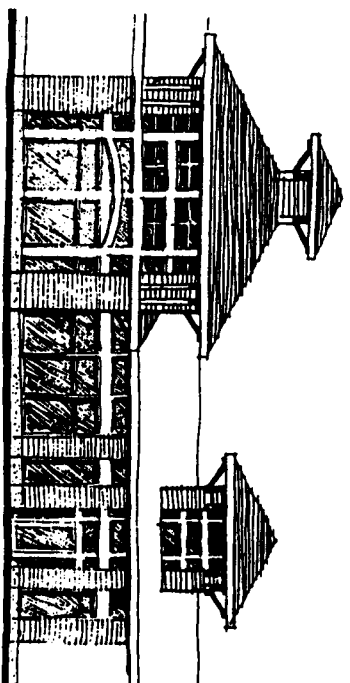
The special conditions that justify the need for additional development permit guidelines for the subject site are as follows:

1. That an appropriate, high quality built form is developed at this important gateway location to the Terra Nova Planning area;
2. That the single-family neighbourhoods to the north and east be protected by ensuring that the future commercial development blends with the character and scale of the surrounding area, and that sufficient buffers are provided between uses;
3. That an attractive and accessible pedestrian environment be created which provides a continuation of the Terra Nova Trail system and helps ensure a blending of the private and public realms;
4. That the development adds to the overall beautification of the City through the provision of special features in focal areas such as plazas, water fountains and the greening of streets, and parking areas; and

5. That the materials applied to the buildings, and the type of street furniture, landscaping, signage, and light standards erected in the open spaces are coordinated to form a distinct development.

Architectural Guidelines

1. Articulate the facades of the buildings on all four sides to create the appearance of a series of small scale commercial units that each have their own identity and interest without sacrificing the overall visual compatibility of the development. (See Sketch 1).



Sketch 1

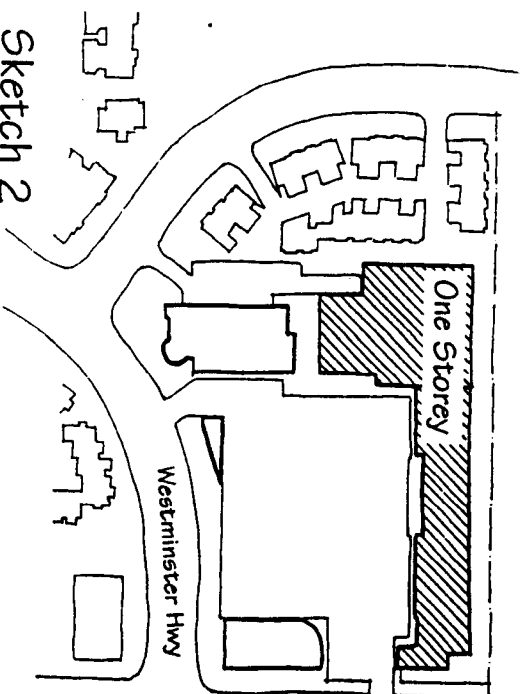
2. Design individual store entrances in a highly visible manner through the use of landscaping and architectural features such as recesses and projections.

3. Protect the privacy of the single-family neighbours to the north by:

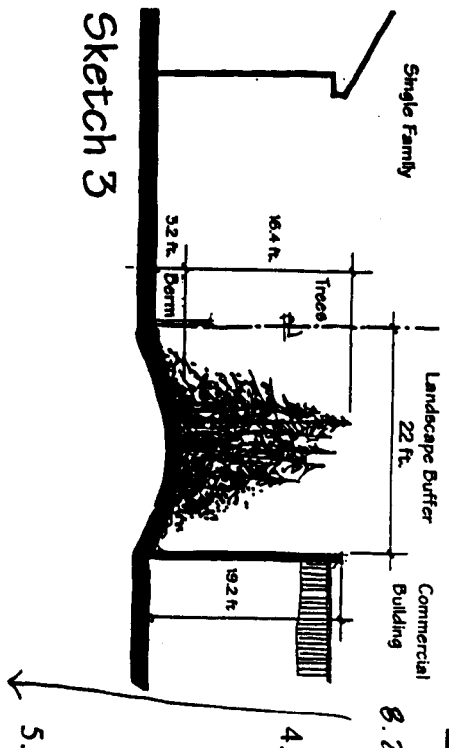
- a) Ensuring that the commercial units located in the northern portion of the site do not exceed 9 m (29.5 ft) in height or contain more than 1 storey with the exception of special entry features which should not exceed 10.21 m (33.5 ft) in height. (See Sketch 2);

- b) Providing a minimum 6.7 m (22 ft) wide landscaped buffer between the commercial buildings and the backyards of the single-family homes (see Building Setback Plan accompanying Comprehensive Development District (CD/65) zone). A total of 111 conifers including Douglas Fir and Pine should be planted within a 1 m (3 ft) high berm. Sixty of these 111 trees are currently located on the site and should be relocated into the buffer area. The remaining 51 trees should be 5 m (16.4 ft) high at the time of planting. (See Sketch 3);

- c) planting climbing vines on the rear exterior walls of the buildings located in the northern portion of the site; and



Sketch 2



Sketch 3

§.2.7 d) insulating the buildings along the northern property line to help minimize noise.

Establish a northwest style of architecture and a sense of quality and solidity through the use of natural building material. Brick, stone, and wood siding all express a sense of quality and permanence. Stucco should only be used in combination with other natural materials, and should not be used within 1m (3.28 ft.) of the ground. Stucco should be treated to prevent discoloration and particular care should be taken in the detailing of north facing facades.

5. Ensure that a variety of complementary colours are used on the buildings and street furniture.

§.2.6 6. Add interest by constructing pitched roof forms at all public facades (i.e. facing No. 1 Road, Westminster Highway and the principal parking areas), except where otherwise concealed by landscaping.

§.2.7 7. Conceal all mechanical equipment within the sloped roofs.

§.2.9 8. Locate the garbage and recycling containers as well as utility metres in areas which do not visually or physically conflict with pedestrian traffic along the pedestrian circulation system. The garbage and recycling containers should be contained within an enclosed building.

9. Limit commercial signage that is well integrated into the building designs to:

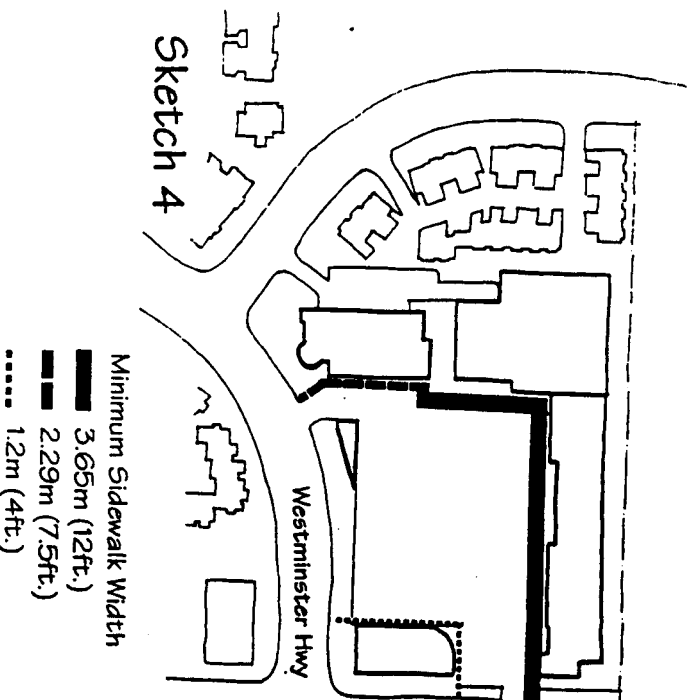
- a) Facade signs comprised on letters and logos affixed to the building, or in internally illuminated boxes - the latter to be limited to sign bands immediately above the main floor and second floor windows/doors or copy on awnings, and to consist of light-coloured or white text on a dark background; and
- b) Freestanding signs limited to directional signs and signs indicating the name and general nature of the services, to a maximum of 2 m (6.56 ft.) in height.

(These guidelines for commercial signage are to be used in addition to the Richmond Sign Bylaw requirements.)

Pedestrian System and Landscaping Guidelines

Internal Walkway System

1. Provide a continuous sidewalk between the store fronts and the parking lot. (See Sketch 4).
2. Enhance the pedestrian experience along the internal sidewalk by:
 - Providing low level landscaping planters along the building edge;
 - Incorporating low-level dog tying hitches to the planter boxes;
 - Using a special decorative paving treatment on the internal walkway sidewalk;
 - Expanding the width of the walkway by 3m (9.84 ft.) into the parking area where feasible in order to provide seating areas in front of commercial units envisioned for restaurant use;



9. Limit commercial signage that is well integrated into the building designs to:
- 8.2.7

a) Facade signs comprised on letters and logos affixed to the building, or in internally illuminated boxes - the latter to be limited to sign bands immediately above the main floor and second floor windows/doors or copy on awnings, and to consist of light-coloured or white text on a dark background; and

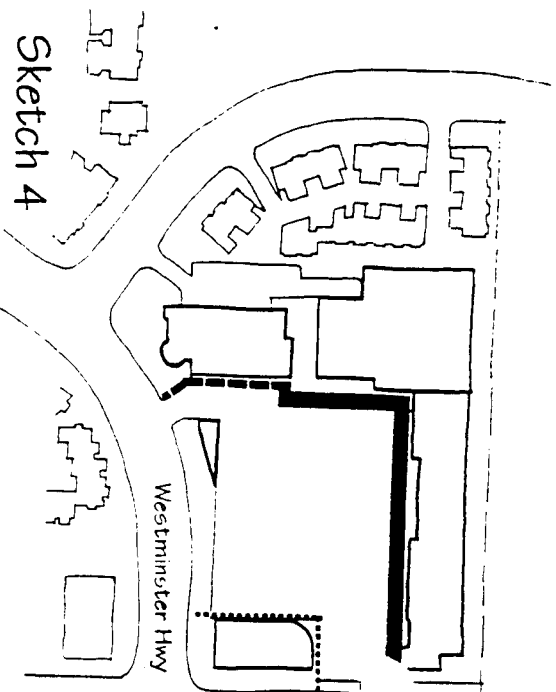
b) Freestanding signs limited to directional signs and signs indicating the name and general nature of the services, to a maximum of 2 m (6.56 ft.) in height.

(These guidelines for commercial signage are to be used in addition to the Richmond Sign Bylaw requirements.)

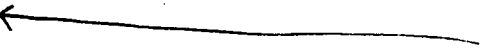
Pedestrian System and Landscaping Guidelines

Internal Walkway System

- 8.2.8
1. Provide a continuous sidewalk between the store fronts and the parking lot. (See Sketch 4).
 2. Enhance the pedestrian experience along the internal sidewalk by:
 - Providing low level landscaping planters along the building edge;
 - Incorporating low-level dog tying hitches to the planter boxes;
 - Using a special decorative paving treatment on the internal walkway sidewalk;
 - Expanding the width of the walkway by 3m (9.84 ft.) into the parking area where feasible in order to provide seating areas in front of commercial units envisioned for restaurant use;



8.7-8



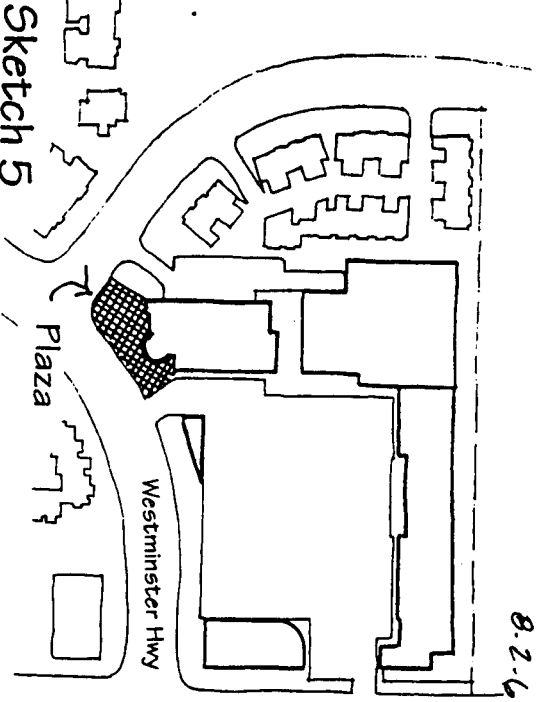
- Providing benches, garbage containers and bicycle racks along the sidewalk at major building entries finished in a colour scheme complementary to the building (The bicycle racks should be located within 15.0 m (49.02 ft.) of a principal building entry. The bicycle racks should be constructed of sturdy, theft-resistant material, securely anchored to the ground. The racks should be designed to support the bicycle frame, not just the wheels, and allow for both the frame and the front wheel to be locked to rack with an U-style lock);
- Providing continuous weather protection over the store fronts by including either overhangs, canopies or awnings that are of a sufficient depth (minimum 2 m) to shelter outdoor displays, seating and walking areas on all the internal walkways except for the single building located at the corner of No. 1 Road and Westminster Highway;
- Connecting the internal sidewalk to the perimeter trail system; and
- Ensuring that the sidewalks and thresholds are accessible to disabled people.

Plazas

1. Locate a 595 m² (6,440 ft²) plaza at the southwest corner of the shopping centre extending from the building entry to the curb. (See Sketch 5).

2. Design the plaza to contain the following elements:

- a) A widened hard circular surface with a distinct paving pattern measuring at least 10.9 m (36 ft.) in diameter;
- b) Benches and garbage containers;
- c) Accent planting;
- d) Decorative lighting; and

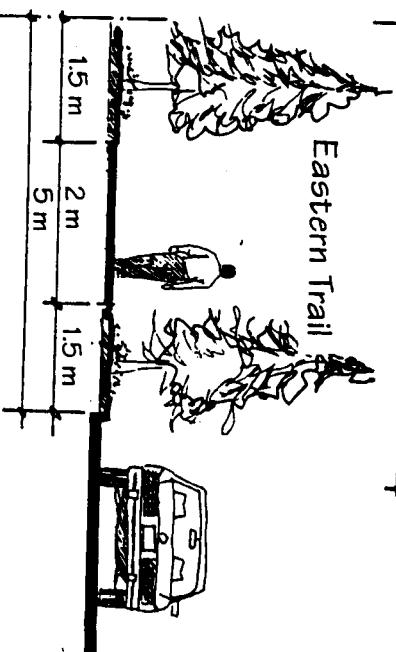
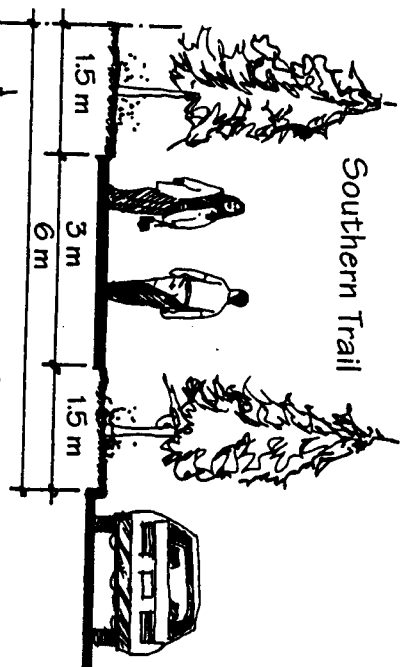
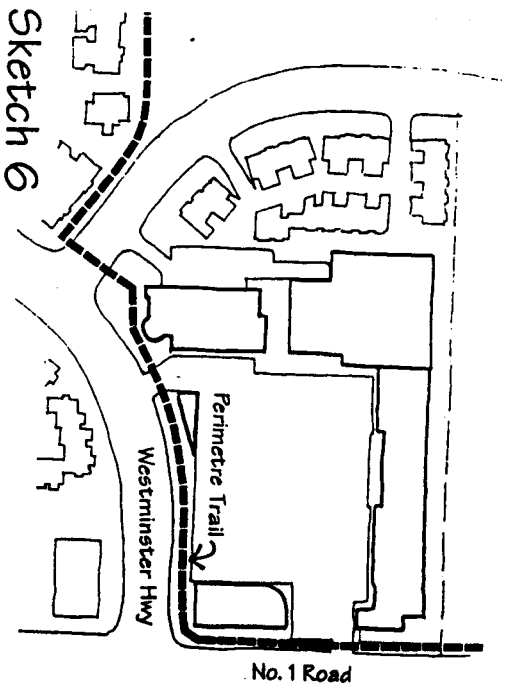


Sketch 5

8.2.8 e) A water feature.

Perimeter Trail System

1. Construct a 6 m (19.6 ft.) wide trail along the southern perimeter of the site and a 5 m (16.4 ft.) wide trail along the eastern perimeter of the site capable of accommodating pedestrians and recreational cyclists. The southern trail should consist of 3 m (9.84 ft.) of hard walking surface located between a double row of street trees planted in two 1.5 m wide grassed boulevards. The eastern perimeter trail should consist of 2 m (6.56 ft.) of hard walking surface located between a double row of street trees planted in two 1.5 m grassed boulevards. (See Sketch 6). The street trees should have a 8 cm (3.14 in.) calliper at the time of planting and be spaced no more than 7 m to 9 m apart. Honey Locust trees should be planted along the No. 1 Road and Ash trees should be planted along the Westminster trail.
2. Construct the walking surface of the trail with a decorative paving material.
3. Use a highly textured paving material and raised crossings at driveways to alert pedestrians and motorists of crossings.
4. Erect automatic gates at delivery entrance driveways to help ensure pedestrian safety along the trail.
5. Include the curb cuts at the driveway entrances within the boulevards so that the sidewalks remain level.
6. Erect low masonry walls or hedges along the perimeter of the site only if they do not exceed 1 m (3.28 ft.) in height measured at the highest elevation of the trail, and pedestrian access to shopping centre is not impaired at the corner of No. 1 Road and Westminster Highway.



8.2.6

7. Develop an entry feature at the corner of No. 1 Road and Westminster Highway that provides a focal point to the shopping centre as well as a gateway to the Terra Nova neighbourhood. Although not limited to, suggested entry features include:

- A water feature with accent planting and a Terra Nova Neighbourhood sign. The sign should be designed to reflect the northwest style of architecture used on the buildings. If the sign is lit, it should be done so in a manner that creatively and positively contributes to the nighttime impression of the area.

Parking Lot

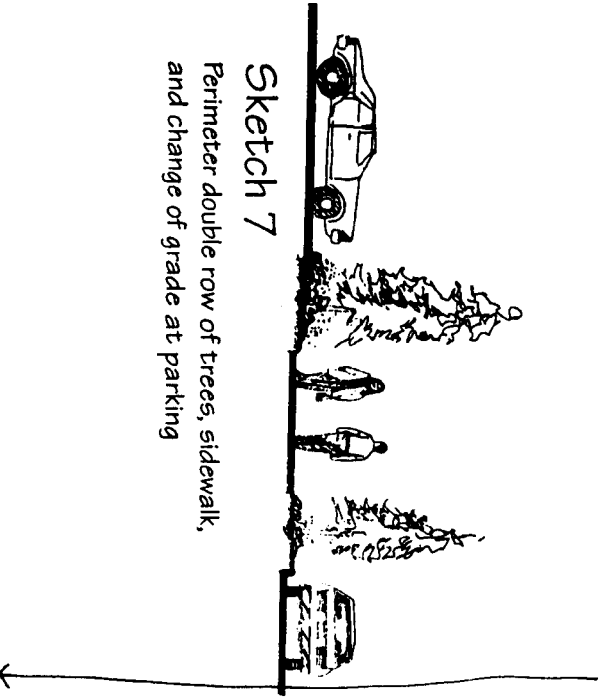
8.2.8

1. Soften the parking lot by planting 1 tree per every 3 parking spots so that after approximately 10 years after planting, at least 70 % of the parking lot will be covered by a canopy of leaves in the summer.
2. Provide decorative lighting in the parking lot that is capable of accommodating hanging flower baskets with irrigation and/or banners. The colour of the lighting standards should be coordinated with the colour of the street furnishings throughout the development. Lighting should not exceed 3 foot candles at the north, west and south property lines.
3. Screen the parked cars from the perimeter trail without compromising safety and surveillance by providing a slight change in grade and low shrubs at the edge of the parking lot. (See Sketch 7)

Tree Preservation

1. Prepare a tree survey and hire an arborist to determine which of the existing trees on the site (in addition to the conifers used in the northern buffer area) can be retained or relocated on the site.

Sketch 7
Perimeter double row of trees, sidewalk,
and change of grade at parking



2. Erect protective fencing around the trees that are to be retained before commencing with construction. The fencing should be shown on the Landscape Plans.

Amenities

8.2.9

1. Provide on-site amenities such as an employee lunch room or lounge, employee locker and shower room, public washrooms, and bicycle parking and end-of-trip facilities. The bicycle parking and end of trip facilities should include the following:

- a) Parking facilities shall: be at-grade, have uniform 160 lux (min.) lighting which yields true colours; and, be within sight of building entry, elevator, and/or security;

- b) Bicycle rooms should provide: lockable door(s) with window(s); tamper proof, motion-activated security lighting; and unobstructed view of each room from its entry; and, facilities for no more than 20 bicycles per room (enabling owners to identify one another); and

- c) Bicycle lockers shall: be constructed of solid, opaque, weather-proof and theft-resistant material, with no exposed fittings or connectors; have lockable doors which open to full height and width of each locker; be grouped together; not be located at the head of parking spaces; and, have clear minimum dimensions of:

Length	180 m (5.91 ft.)
End of Width at Door	0.60 m (1.97 ft.)
End of Width Opposite Door	0.22 m (0.72 ft.)
Height	1.20 m (3.94 ft.)

Adoption of guidelines dealing with adjacency to ALR

BLUNDELL Area Plan



City of Richmond

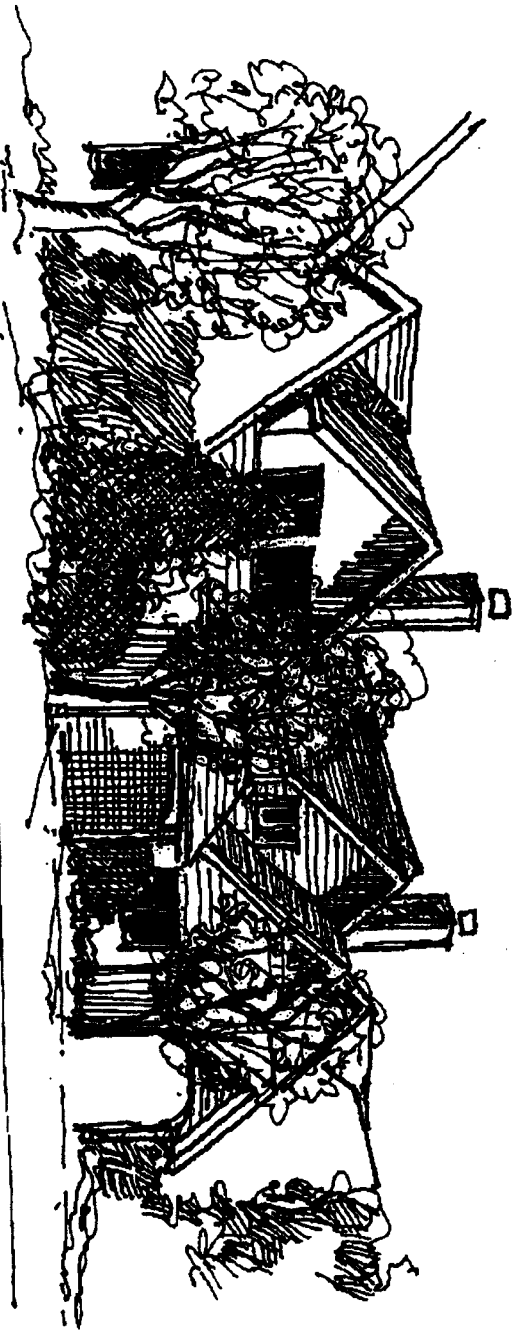
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Laurelwood

Sub-Area

An Official Community Plan

Bylaw 7100 Schedule 2.5A



BLUNDELL AREA PLAN

Laurelwood Sub-Area Plan

1.1

*Plan
Interpretation*

This plan sets out the goals, objectives, policies and development guidelines for the Laurelwood Sub-Area, a part of the Blundell Planning Area.

Additional Conservation Area and Environmentally Sensitive Area (ESA) policies, guidelines, and locations are included in Schedule 1 of this Bylaw and its attachments (Schedule 1 is a separate document which applies to the entire City). Readers should check Schedule 1 as it takes precedence over this plan in the case of Conservation Areas and ESAs.

The plan may be amended from time to time and users should check with the City's Urban Development Division to make sure that they have an up-to-date version containing all of the adopted amendments.

UD.11.9829

ii

Original Adoption: March 15, 1999

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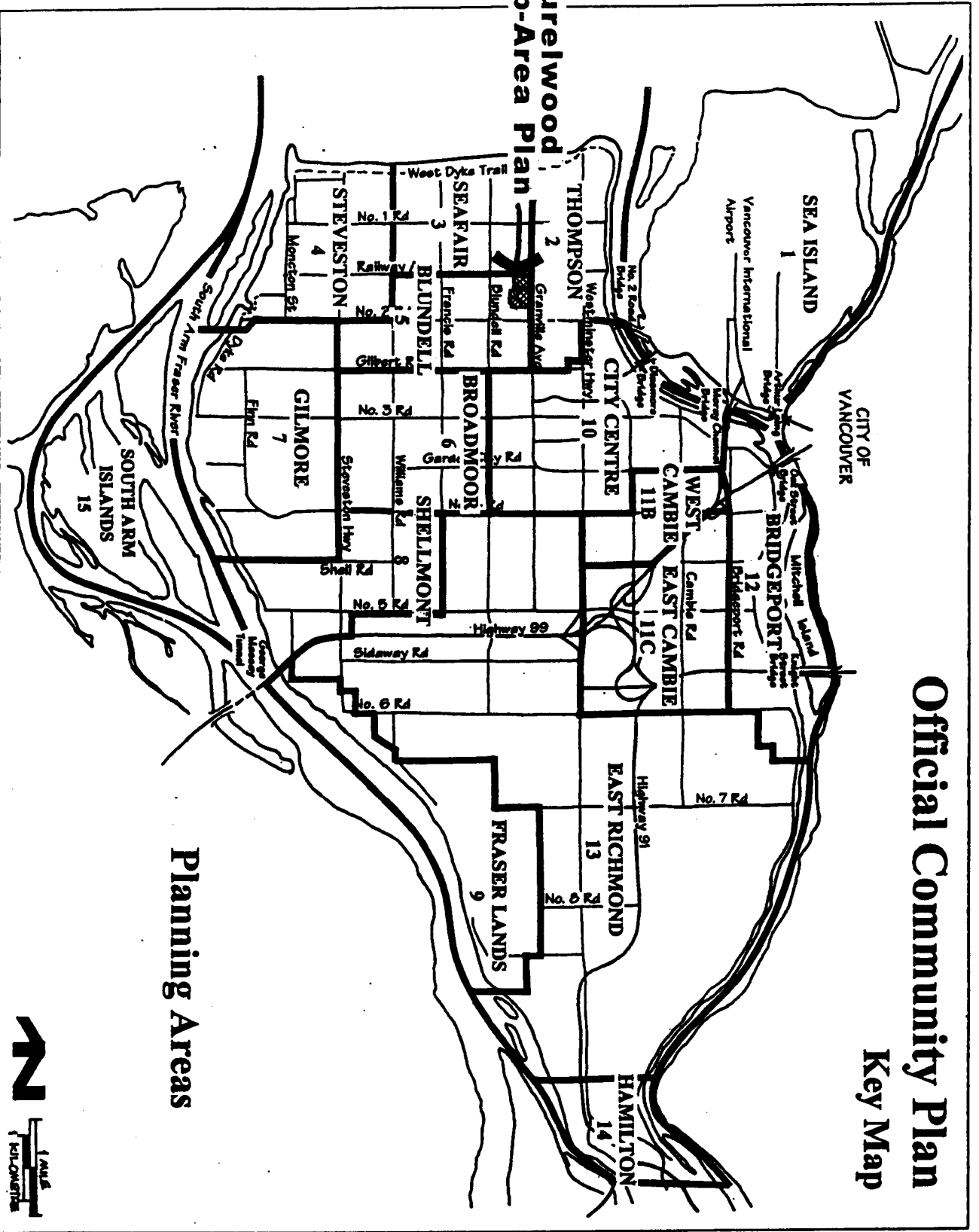
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Official Community Plan Key Map



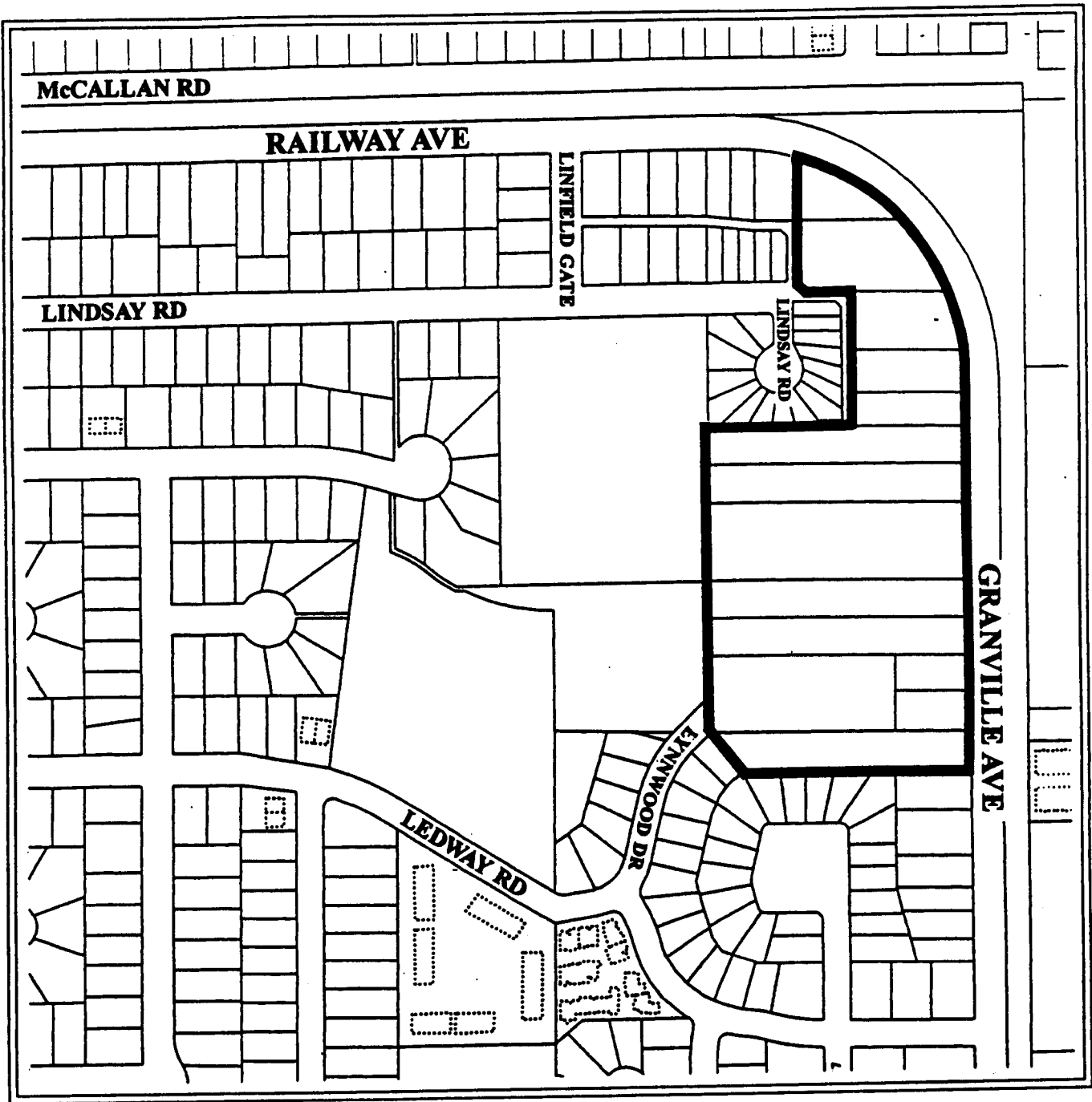
**Laurelwood
Sub-Area Plan**

Planning Areas

Original Adoption: March 15, 1999

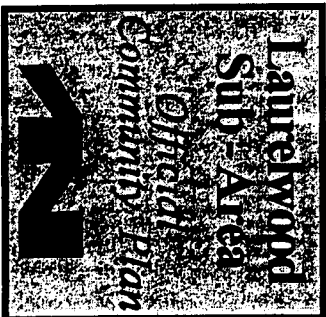
1.0 INTRODUCTION

1.1
This plan provides a series of policies, objectives and guidelines consistent with the development of the Laurelwood Sub-Area as a neighbourhood adjacent to a major arterial road, near the Granville Avenue/Railway Avenue bend. The area to the south and east is a mix of single-family homes on a variety of lot sizes, three-storey apartments, an elementary school and park space. To the north, across Granville Avenue, there is a secondary school, community park and a community centre. The extent of the plan area is noted on Attachment 1.



Attachment 1
PLAN AREA

— Area Boundary

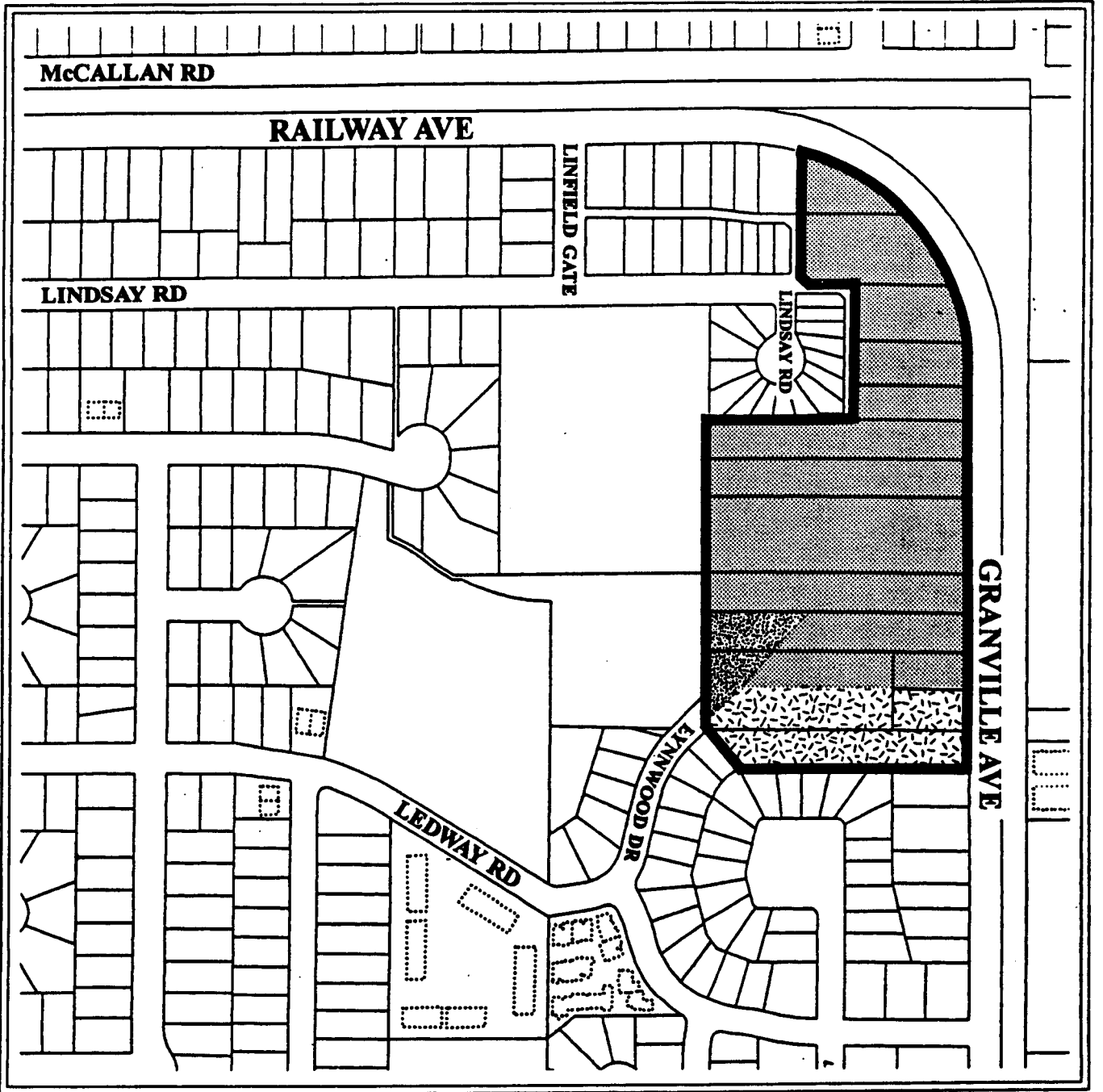


2.0 GOAL

1.2

The goals of this plan are to:





- Provide safe and efficient means for pedestrians, cyclists and vehicles to circulate to facilities, parks and arterial roads in the area; and
- Provide a range of residential household sizes and types at a modest density.



4

Attachment 2

LAND USE

-  Area Boundary
-  Residential (Single Family)
-  Residential (Townhouses)
-  Public Open Space

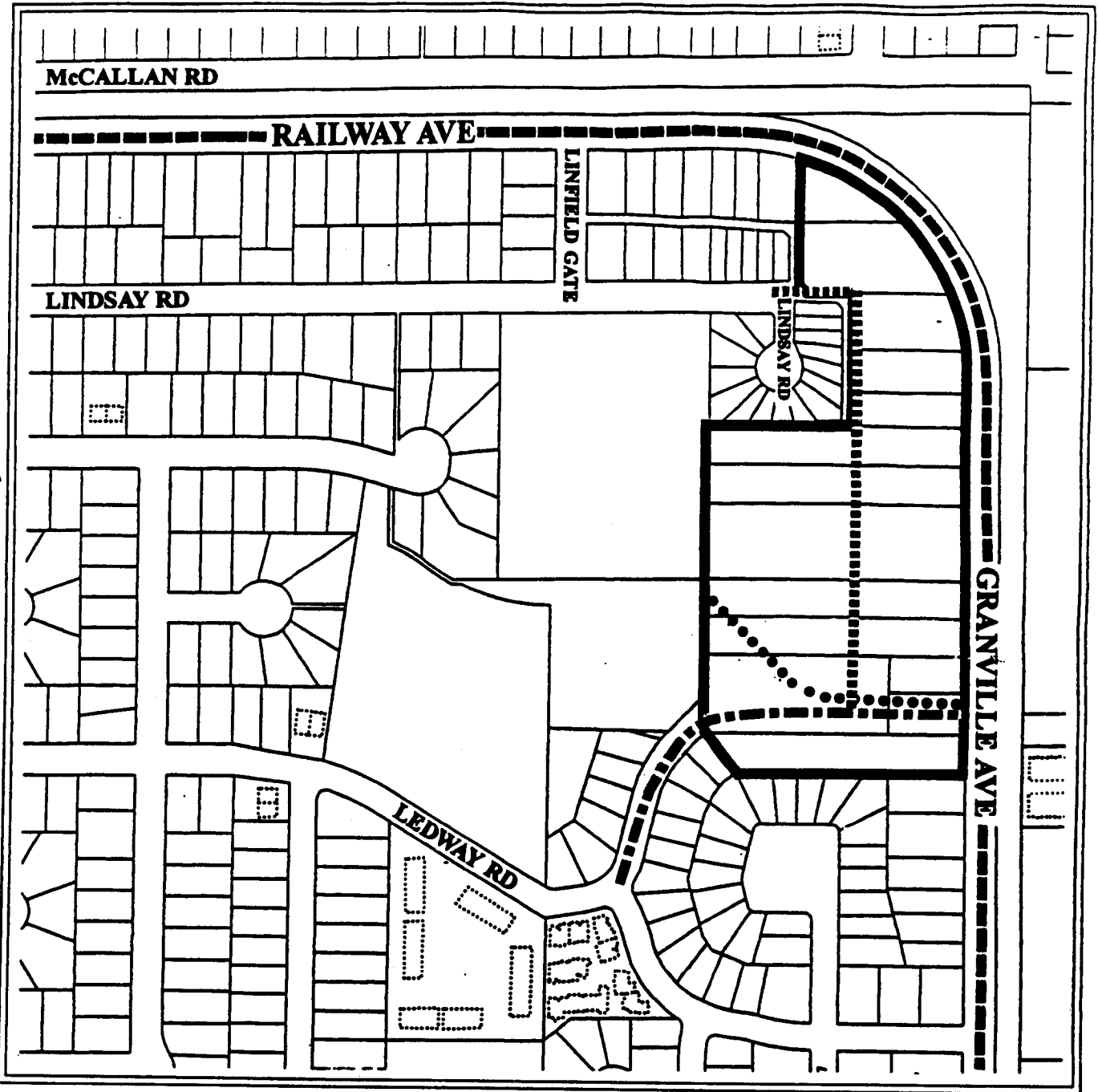


3.0 PUBLIC OPEN SPACE

It is Council's objective to:

5.0

- 3.1 Designate a minimum of 0.2 ha. (0.5 ac.) of land for public open space as shown on Attachment 2.



6

Attachment 3

CIRCULATION SYSTEM

- Area Boundary
- ▬ Major Arterial Road
- ▬ Collector Road
- ▬ Local Road
- Trail

LEWIS & CLARK
COMMUNITY PLAN
2000-2010

Original Adoption: March 15, 1999

4.0 CIRCULATION SYSTEM

4.0

It is Council's objective to:

- 4.1 Restrict direct access from individual lots to Granville and Railway Avenues; and
- 4.2 Provide a hierarchy of roads and trails as indicated on Attachment 3.

UD.11.9829

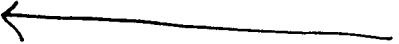
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Original Adoption: March 15, 1999

5.0 RESIDENTIAL

3.0

It is Council's objective to:

- 
- 5.1 Permit the establishment of single-family residences on lands designated "Residential (Single-Family)" on Attachment 2;
 - 5.2 Permit the establishment of townhouses on those land designated "Residential (Townhouse)" on Attachment 2;
 - 5.3 Permit the establishment of new housing only with the full range of urban utilities, street access, and services;
 - 5.4 Encourage and facilitate the provision of affordable housing;
 - 5.5 Require the issuance of a Development Permit prior to construction of any new townhouses in the plan area; and
 - 5.6 Require that Development Permits issued for townhouses comply with the following guidelines:
 - 8.2.2.2 (a) Townhouses should be designed in small clusters and have less than six units in a row.
 - 8.2.2.4 (b) Townhouses should have a minimum private outdoor space of 6.0 m (19.685 ft.) in depth and 36 m² (387.5 ft²) in the area. The depth of private outdoor space may be reduced to 5.25 m (17.22 ft.) where adequate privacy screening can be provided. (Not recommended where private outdoor spaces abut a public street).
 - 8.2.2.3 (c) Townhouses adjacent to public roads and trails should have their main frontage doors visible and accessible from that road or trail with minimal changes in grade. Use minimal changes in level and landscaping to ensure the privacy of the individual units open space is protected without the use of high fences.

8.2.3 (d) Townhouses should have a maximum transitional height gradient of 26° starting at 4.5 m (14.76 ft.) height at 3.0 m (9.84 ft.) setback from all property lines that do not abut a public road. These gradients may be varied provided privacy, sunlight, view and human scale are met.

8.2.1 (e) Special views, such as of mountains should be taken advantage of where possible.

8.2.4 (f) Screening and landscaping, including street trees, should be provided in each townhouse development. Define the vehicular entrance to each residential unit by providing a street tree between each driveway/garage entry point. Trees should be a minimum calliper of 100 mm (4 in.) measured 1.4 m (4.7 ft.) above grade. Use special landscape features such as trellises to conceal garages and visitor parking stalls from surrounding private semi-public and public areas.

8.2.5 (g) Visitor parking should be in public view and easily accessible near the main entrances. Parking lots should have landscaping to separate every fourth parking space and be separated from children's play areas.

↓
(h) Provision should be made for the storage and collection of garbage and recyclable materials.

8.2-4 (i) Retain and incorporate mature landscaping into the development where possible.

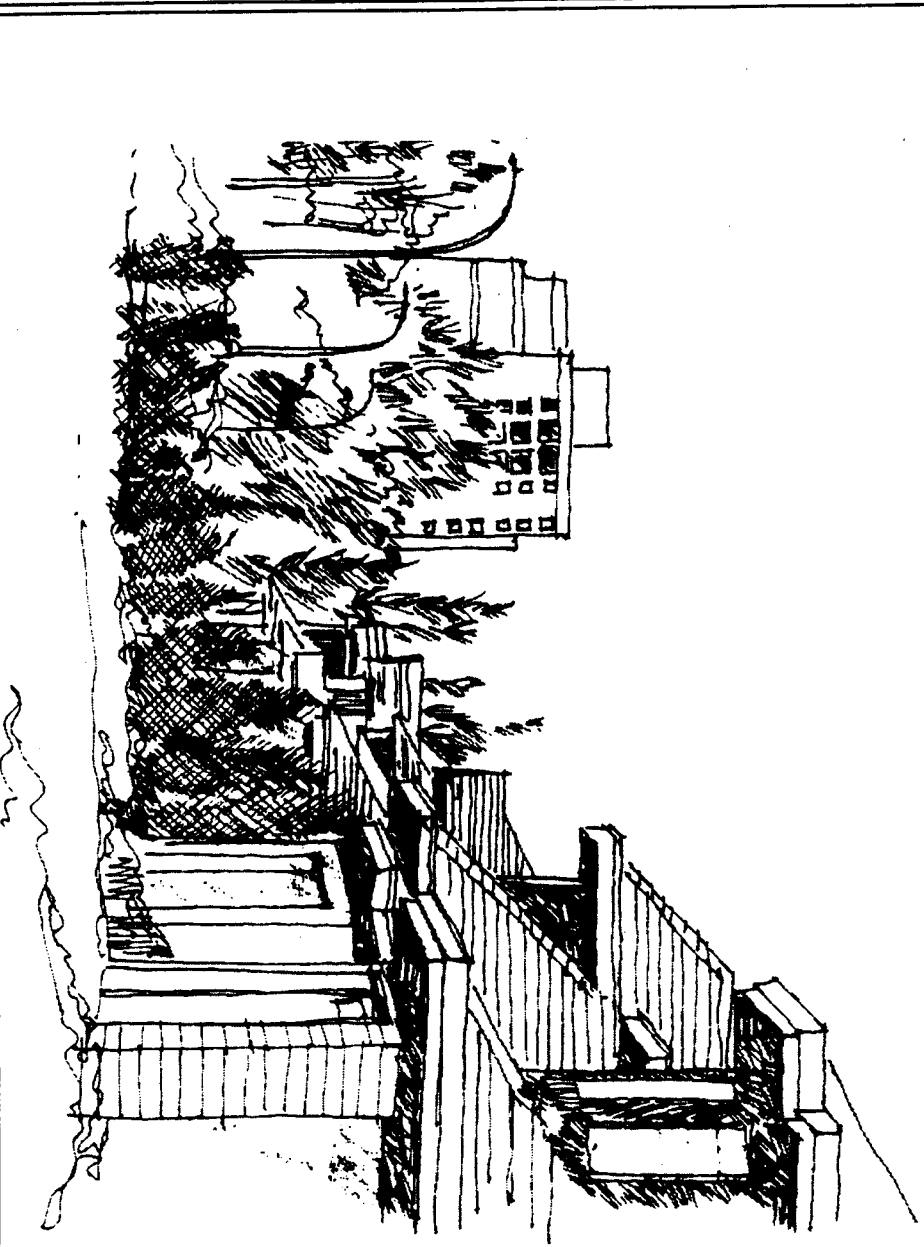
8.2.5 (j) Light semi-private open spaces, pedestrian and vehicular circulation routes and building entries to provide security, safety and convenient access without producing glare into adjacent properties.

City of Richmond



BLUNDELL Area Plan

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EAST LIVINGSTONE Sub-Area

An Official Community Plan
Bylaw 7100 Schedule 2.5B

BLUNDELL AREA

East Livingstone Sub-Area Plan

1.1
This plan sets out the goals, objectives, policies and development guidelines for the East Livingstone Sub-Area, a part of the Blundell Planning Area.

Plan Interpretation
Additional Conservation Area and Environmentally Sensitive Area (ESA) policies, guidelines, and locations and included in Schedule 1 of this Bylaw and its attachments (Schedule 1 is a separate document which applies to the entire City). Readers should check Schedule 1 as it takes precedence over this plan in the case of Conservation Areas and ESAs.

The plan may be amended from time to time and users should check with the City's Urban Development Division to make sure that they have an up-to-date version containing all of the adopted amendments.



UD.11.9828

ii

Original Adoption: March 15, 1999



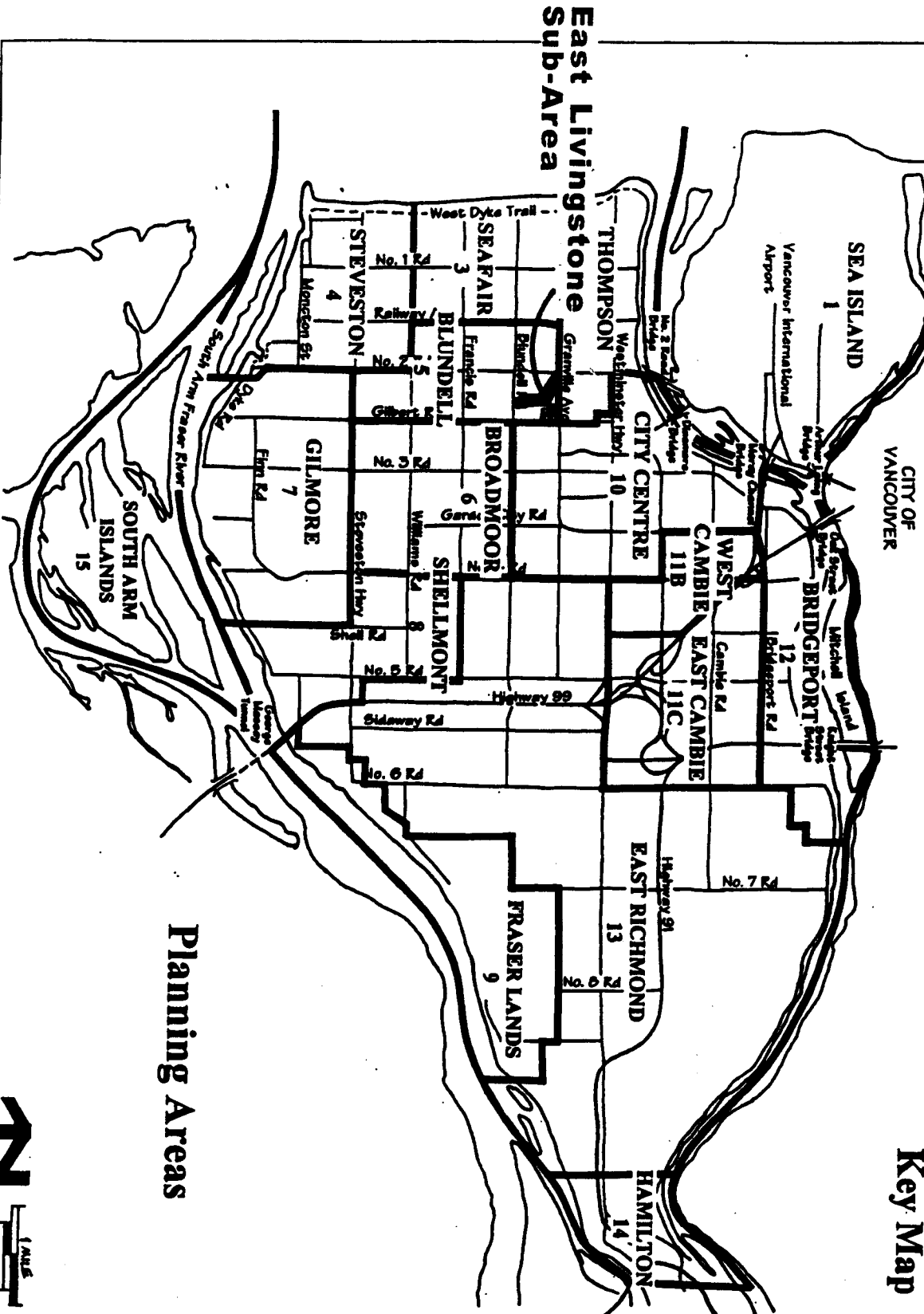
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Official Community Plan

Key Map



**East Livingstone
Sub-Area**

Planning Areas



Original Adoption: March 15, 1999

1.0 INTRODUCTION

1.1
The East Livingstone Sub-Area is bounded by Granville Avenue to the north, Comstock Road to the south, Gilbert Road to the east, and extends about 170 m (558 ft.) westward from the western edge of Gilbert Road.

This Plan contains appropriate land uses, designations and Development Permit Guidelines for the East Livingstone Sub-Area.

UD.11.9828

2

Original Adoption: March 15, 1999

2.0 GOALS AND POLICIES

2.1 GOAL

1.2

TO CREATE A LIVABLE, PEDESTRIAN-FRIENDLY, RESIDENTIAL NEIGHBOURHOOD WHICH ACCOMMODATES A VARIETY OF HOUSING TYPES, HOUSEHOLDS AND AGE GROUPS, AND FULFILLS A TRANSITION FUNCTION BETWEEN THE HIGHER DENSITY CITY CENTRE AREA AND THE ADJACENT SINGLE-FAMILY NEIGHBOURHOODS.

2.2 POLICIES

2.2.1 Land Use

3.1
See Land Use Map →
The north-east corner of the East Livingstone Sub-Area is designated for townhouse or low-rise apartment residential use. This corner extends from Granville Avenue to a point approximately parallel to the southern edge of the Livingstone Place alignment, and westward from the western property line at Gilbert Road for approximately 85 m (279 ft.).

All remaining land within the East Livingstone Sub-Area is designated small-lot single-family, two-family, or townhouse residential use.

~~Small lot single family residential refers to a single detached dwelling unit of a lot size conforming to Single-Family Housing District, Subdivision Area A (R1/A) zoning specifications.~~

~~Two family residential consists of a duplex, or two semi-attached residential units on a single lot, in conformance with Two-Family Housing District (R5) zoning specifications.~~

Townhouse residential consists of a building or buildings containing two or more dwelling units, where each has a separate entrance at first-storey level.

2.2.2 Neighbourhood Character

Create a tranquil, serene neighbourhood distinguished by its visible, abundant trees and colourful, inviting gardens. Unify street character through the repetition of certain common landscaping elements.

Ensure the neighbourhood is compatible with adjacent single-family neighbourhoods.

Ensure the neighbourhood is well-connected by providing additional walkway or trail connections which enhance access for East Livingstone and the adjacent single-family area to important City Centre amenities, such as Minoru Park:

- Secure public rights-of-way to create pedestrian/bicycle paths from Livingstone Place through to Granville Avenue and Gilbert Road, on the approximate locations designated in the land use map.
- Ensure these paths are attractive and safe.

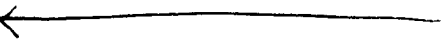
2.2.3 Traffic

Ensure that vehicular access to new buildings from Granville Avenue or Gilbert Road is not permitted.

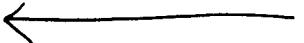
Ensure vehicular access to new residential developments within the sub-area is provided through an extension of Livingstone Place through to Comstock Road. Ideally, the Comstock link will be setback at least 100 m (328 ft.) from Gilbert Road.

Provide vehicular access to single-family and two-family residential developments through lanes. Where possible, also use lanes to provide vehicular access to multiple-family development, to minimize the visual and physical impact of the automobile on neighbourhood streets and the pedestrian realm.

3.1



4.6



3.0 DEVELOPMENT PERMIT GUIDELINES

3.1 APPLICATION AND INTENT

Schedule 1 of this bylaw designates areas for the establishment of objectives and provision of guidelines for the form and character of multi-family residential development. The East Livingstone Official Community Plan Land Use Map designates areas for multiple-family use.

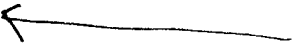
A key housing policy of Richmond's Official Community Plan is to carefully integrate new housing development into existing neighbourhoods. All multiple-family projects merit site-by-site consideration of form and character in order to achieve the desired relationship to surrounding areas, and to ensure high standards of new housing design.

East Livingstone in particular requires special consideration in order to fulfil the Sub-Area Plan goal. The neighbourhood's location, traffic considerations, mature landscaping, and significant redevelopment potential present issues which need to be addressed through design guidelines. Guidelines are also required to ensure that stipulated policies for neighbourhood character are realized (refer to Section 2.2.2).

3.2 APPLICATION OF CITY-WIDE GUIDELINES

Schedule 1 of this bylaw sets out objectives and general guidelines for development in multiple-family residential areas, and these apply in the East Livingstone area. Those wishing to develop in East Livingstone are advised to consult and follow the guidelines stipulated in these and all other pertinent documents.

8.1.1



8.1.2

8.1.1

3.3 ADDITIONAL GUIDELINES SPECIFIC TO EAST LIVINGSTONE

3.3.1 General

A. Densities

Densities should be compatible with adjacent single-family neighbourhoods while providing some transition between those neighbourhoods and the high rise apartment directly to the east.

3.1 a) In the area designated for apartment use, densities of up to 0.90 FAR are permitted.

3.1 b) In the area designated for townhouse use, densities of up to 0.55 are permitted.

3.1 c) Where a site includes both apartment and townhouse uses, consideration may be given to averaging densities across the site.

B. Setbacks

3.1 Setbacks should complement those on single-family properties and should provide space for plant landscaping that enhances the pedestrian realm.

a) Along all public streets the minimum property line setback is 6 m (19.7 ft.).

b) Setbacks for side and rear yards not directly facing a public street are a minimum of 2 m (6.6 ft.).

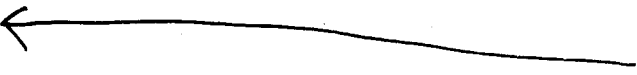
3.3.2 Architectural Guidelines

Create a human-scale built environment that complements the adjacent single-family neighbourhood and enhances the streetscape.

A. Building Scale and Form

Ensure that the scale, massing and form of new buildings complement adjacent and facing single-family residential areas and contribute to a pedestrian-friendly street environment.

8.2.2

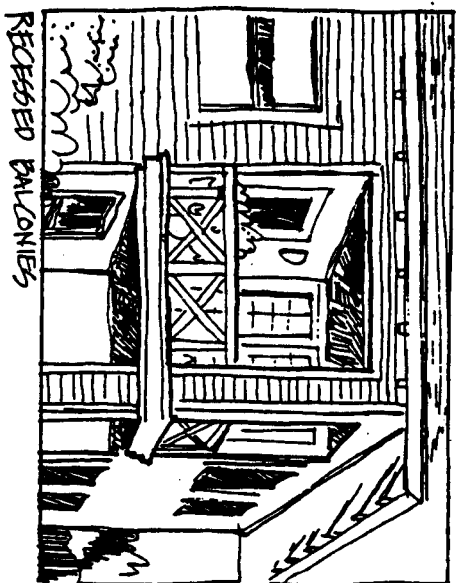
- 
- a) Avoid facing blank building walls onto the street. Orient windows and doors streetward, and create an attractive frontage using windows and doors to help animate the building facade and promote a more human-scale character.
 - b) Reduce the apparent height of buildings with architectural treatment that promotes recognition of individual storeys and avoids the appearance of sheer blank walls. Such treatment might include (but is not limited to) the use of: trim; more intense colour accents; secondary roof elements; building recesses; and stepped building forms emphasizing lowered height on ends with fronts.
 - c) Where apartments are permitted, limit height to a maximum of four storeys over parking.
 - d) Provide a side yard height step-back where apartments are adjacent to townhouse or single-family dwellings, as illustrated in diagram. Also provide height step backs at street corners, in the form of lower (eg one or two-storey) building elements.

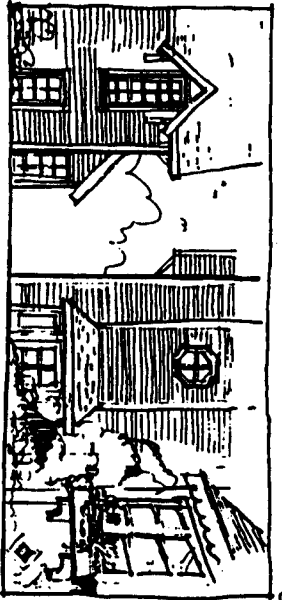
B. Porches, Balconies, Patios

8.2.3

Use porches and balconies to provide visual interest to buildings while complementing existing neighbourhood character and providing usable private open space to residents.

- a) Large projecting balconies are discouraged along the streetfront, as they emphasize building bulk. However, recessed balconies are encouraged (see illustration).
 - b) Porches are encouraged along Livingstone Place or Comstock Road, and may project up to 2.5 m (8.2 ft.) into front yard setbacks along these streets. Porches are discouraged along Granville Avenue and Gilbert Road, and may not project into setbacks along these streets.
 - c) Porches and covered stairs should be at least 2.5 m (8.2 ft.) deep to allow for usability, and should incorporate prominent main entries.
 - d) Porches, porch entrances, and related accents provide desired opportunities for varied and creative design expression. They should enhance individual buildings and be accented with traditional materials such as natural wood or wrought iron. They should also be integrated into the facade rather than appearing "tacked on".
- ### C. Windows
- Windows should be operable, of residential scale, with multiple panels, and should add to the sense of neighbourliness and safety and security by providing visual interest and surveillance.
- a) Reinforce the smaller components of buildings which express strong unit identity through appropriate window placement.





WINDOWS SHOULD BE RESIDENTIAL SCALE

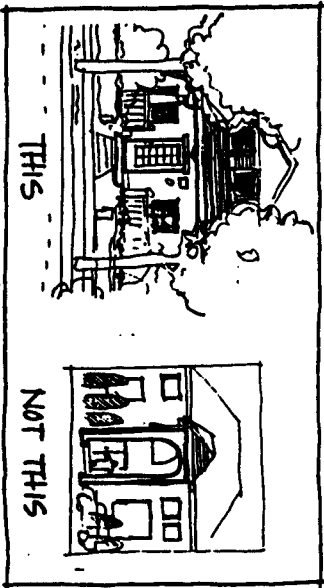
8.2.3

- b) Windows, used singly or in combination, should be apparent at eye level and should be clear-glazed (not tinted) to reinforce the sense of surveillance over the street.
- c) Windows should be visually prominent, separated from the building face and defined with colour, trim/shutters, or other decoration. Bay windows, window boxes and similar features are also encouraged.
- d) Traditional window treatment should be used where visible from the street. Skylights, plastic bubble windows, tinted glaze and other non-traditional windows should not be used in prominent locations on the building that are visible from the street.

D. Entrances

Strongly tie the new multiple-family development to quiet residential streets, and use entrances to reinforce and achieve a human-scale and pedestrian-friendly environment along these streets.

- a) Provide front door entries with direct grade access to new townhouse and apartment developments that are oriented to, and clearly visible from either Livingstone Place or Comstock Road. Ensure that entries are accessible to disabled persons.
- b) Architectural treatment of unit entrances should reinforce proximity to grade level and avoid two-storey features.
- c) Entrance features, such as trellises and small entry courts edged with ornamental trees, shrubs and plants, are encouraged.



THIS

NOT THIS

RESIDENTIAL ENTRIES SHOULD BE VISIBLE FROM THE SIDEWALK, AT GRADE, AND BE PART OF A SINGLE-STORY ELEMENT

E. Materials

- a) Roofing material should emphasize natural or west-coast produced materials such as cedar. Metal roofs are discouraged and the use of roof tiles should be avoided.
- b) Particularly desirable building materials include: natural stone, cedar siding, and wood shingles. Good quality vinyl-coated aluminum or solid vinyl reproductions of traditional siding materials are acceptable. The use of brick is discouraged and the use of stucco should be minimized, particularly on surfaces visible from the street.

3.3.3 Landscape Guidelines

Use landscaping to create a uniform street appearances, and share the visual benefits of attractive landscaping on private property among all neighbours.

A. Streetscape

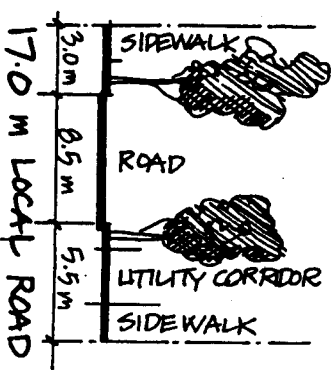
Along all streets:

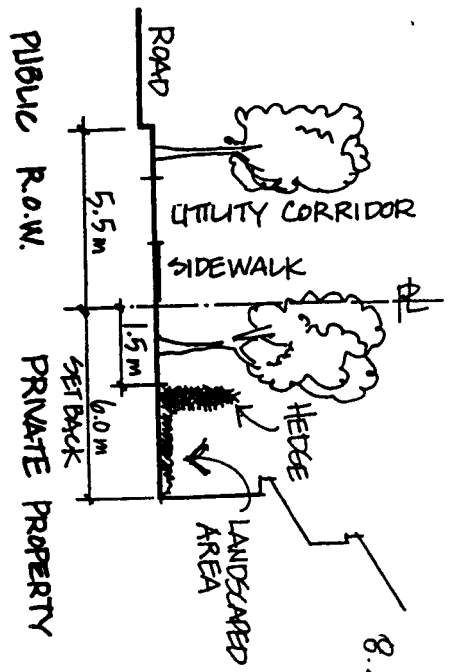
- a) Ensure that windows and doors remain visible from the street and are not hidden by vegetation.

Along Comstock Road and Livingstone Place:

- b) Along the public right-of-way, provide sidewalks on both sides of Livingstone Place and on the north side of Comstock Road. Make these sidewalks attractive by providing street trees, and placing all utilities underground.

8.2.4





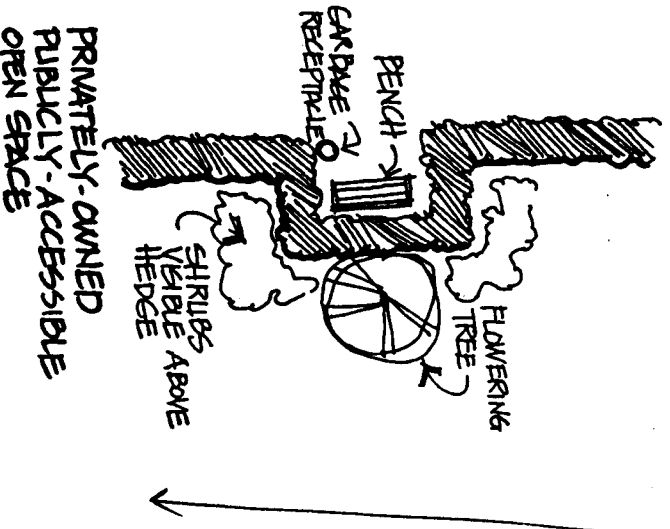
8.2.4

- c) On private property, provide a 1.5 m (4.9 ft.) grass strip at the front property line directly beside the sidewalk and include a second row of trees along this grass strip. Trees should be spaced approximately 10 m (32.8 ft.) apart, with oak varieties recommended, such as red oak (*quercus rubra*) or willow oak (*quercus phellos*). Directly behind this grass strip provide contiguous flowering or evergreen hedging (eg. wild rose, boxwood, laurel) of 0.6 m (2 ft.) in height, and at least 0.6 m (2 ft.) wide.
- d) Although driveways should be minimized (see "Driveways" below), where they create a break in the grass strip and hedging, reduce the visual impact of this break by edging at least the first 3 m (9.8 ft.) of any driveway (leading directly from these streets) with the grass strip and hedge described above.

- e) Along the North and East sides of Livingstone Place, and the North side of Comstock Road, provide privately-owned, publicly-accessible open spaces (P.O.P.A.S.) in the form of small seating areas approximately 2 m by 2 m (6.6 ft. x 6.6 ft.) in size (see accompanying illustration). Provide benches and trash receptacles in these areas. These areas should be spaced approximately every 30 m (98.4 ft.). Divert hedging accordingly, and behind the hedging provide at least one flowering fruit tree, of rounded canopy and moderate to rapid-growing. Create a profusion of vegetation around these open spaces, by clustering taller shrubs and other understorey around the tree and behind the hedge.

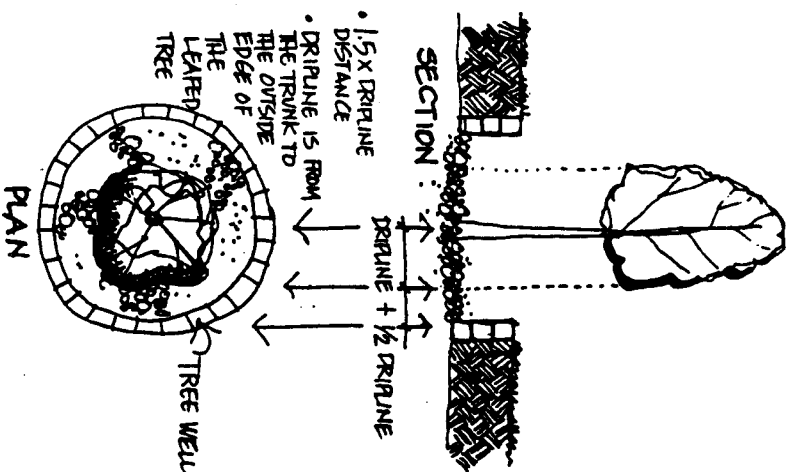
Along Granville Avenue and Gilbert Road:

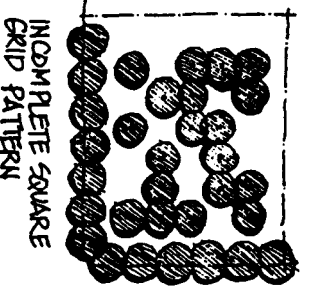
- f) Create a tree grove along these streets (see below under tree preservation). Setbacks should be completely landscaped with plant materials.



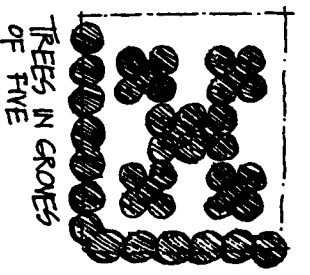
B. Tree Preservation

- a) A detailed survey of existing trees and vegetation should be conducted, following the requirements set out in Richmond's Tree Survey Bulletin.
 - b) Take special efforts to preserve trees and mature vegetation. Plan open spaces and walkways with landscaping first. Then group buildings around the spaces. Most mature trees exist along Granville Avenue, Gilbert Road, and existing rear property lines. It should be possible to create a grove effect along these streets (large contiguous lines of mature trees/vegetation) through retention and enhancement of existing plant materials. Any new plant materials used should be indigenous or native species which require little maintenance.
 - c) Tree wells may be used to facilitate retention of existing trees. Where these are used, they should be a minimum distance of 1.5 times the distance from the trunk of the tree to the drip line (see illustration).
- ### C. Common Open Space
- Create open spaces that are usable and practical while also enhancing the streetscape of the neighbourhood.
- a) Special effort should be taken to create and coordinate the retention of contiguous existing mature vegetation on adjacent properties.
 - b) Landscape the ground plane intensely with flower beds, flowering trees, shrubs and other plant materials which will foster a garden-like quality. Shrubs or small trees should be clustered between units or planted a minimum of every third unit in the front yards. Include species that will attract birds (which in turn enhance both biodiversity and create pleasant soundscapes). Examples of such plants include: crab apple, cherry, and dogwood trees; holly, serviceberry, elderberry, bayberry, honeysuckle and rose bushes. Where possible, plant tall columnar trees in side yards.

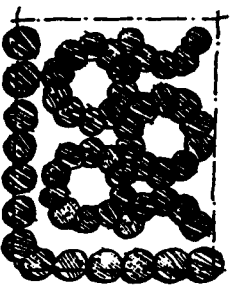




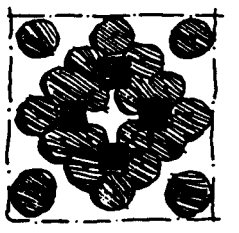
INCOMPLETE SQUARE GRID PATTERN



TREES IN GROVES OF FIVE



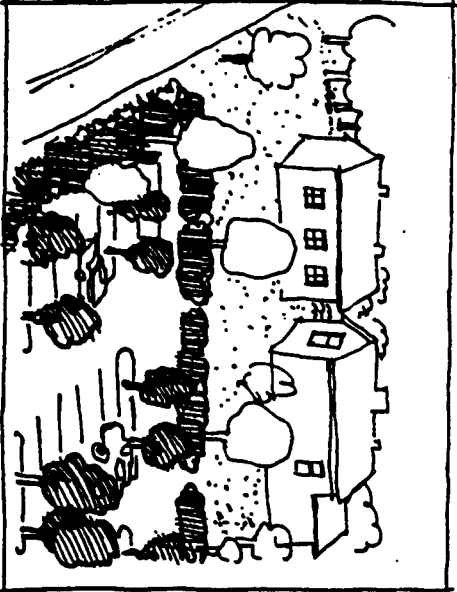
MULTIPLE CIRCLE PATTERN



20 TREES IN 14 ROWS OF 4 TREES WITH FOUNTAINS OR POOLS.

EXAMPLES OF TREES CLUSTERED TO CREATE 'OUTDOOR ROOMS'

ADAPTED FROM ARRARD, HENRY E. 1980. TREES IN URBAN DESIGN.



SCREEN PARKING WITH MIN. 2.0m WIDE X 1.5m HIGH LANDSCAPE STRIP. ONE TREE SHOULD BE PLANTED FOR EVERY LINE METER

c) Cluster trees to create "outdoor rooms" or to divide yards into smaller, more intimate areas for people to gather in while avoiding a heavily partitioned character.

D. Driveways and Parking

8.2.1 a) Gated driveways are not permitted.

Reduce the visual presence of the automobile.

b) Driveways are not permitted along Granville Avenue or Gilbert Road.

8.2.5 c) Minimize the surface area of black top parking and access driveways through alternate paving treatments.

d) Resident parking should not be visible from the street. On townhouse sites, it should be covered or placed behind buildings and screened. Screening should be a minimum of 2 m (6.6 ft.) in length times 1.5 m (4.9 ft.) landscape in height planting or trellis strips. Trees should also be planted a minimum of one tree for every four parking stalls. In apartment developments, parking should be covered and concealed through earth-berming landscaped intensely with plant materials.

Along Comstock Road and Livingstone Place:

8.2.1 e) Encourage shared driveways between properties, and limit all driveways to a maximum width of 3 m (9.8 ft.).

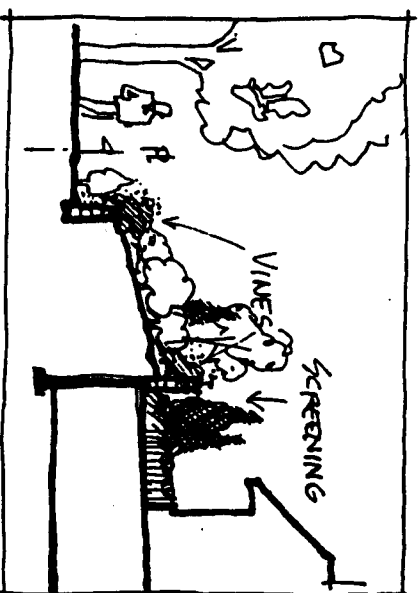
f) Driveways should be lined with trees or shrubs.

g) The first 10 m (32.8 ft.) of any driveway leading directly from a public road should be treated with grey pavers or stamped concrete.

E. Fences, Retaining Walls and Hedges

Use hedges and fencing to demarcate private property while avoiding a walled-off effect.

- a) Fences are discouraged along Comstock Road or Livingstone Place, and may not project into 6 m (19.7 ft.) setbacks. If they are provided along these streets, fences should be no more than 1 m in height, and of a transparent quality, such as wood picket or wrought iron railing.
- b) Fences may be used along Granville Avenue and Gilbert Road, but should not be continuous. They should be screened with landscaping, routed around existing vegetation, and limited to a maximum height of 1.1 m (3.6 ft.). Fences may project up to 2 m (6.6 ft.) into the street setbacks, but may be no closer than 4 m (13.1 ft.) to the property line along these streets.
- c) Side yard fences are discouraged.
- d) Hedges and retaining walls may be used along Granville Avenue and Gilbert Road, and should be no more than 1.1 m in height along those streets. On apartment sites, retaining walls are encouraged as part of the earth-berming scheme. Where retaining walls are used they should be softened through the planting of with vines or other cover. Hedges are required along Comstock Road and Livingstone Place (see under "Streetscape"), but retaining walls along those streets are discouraged.

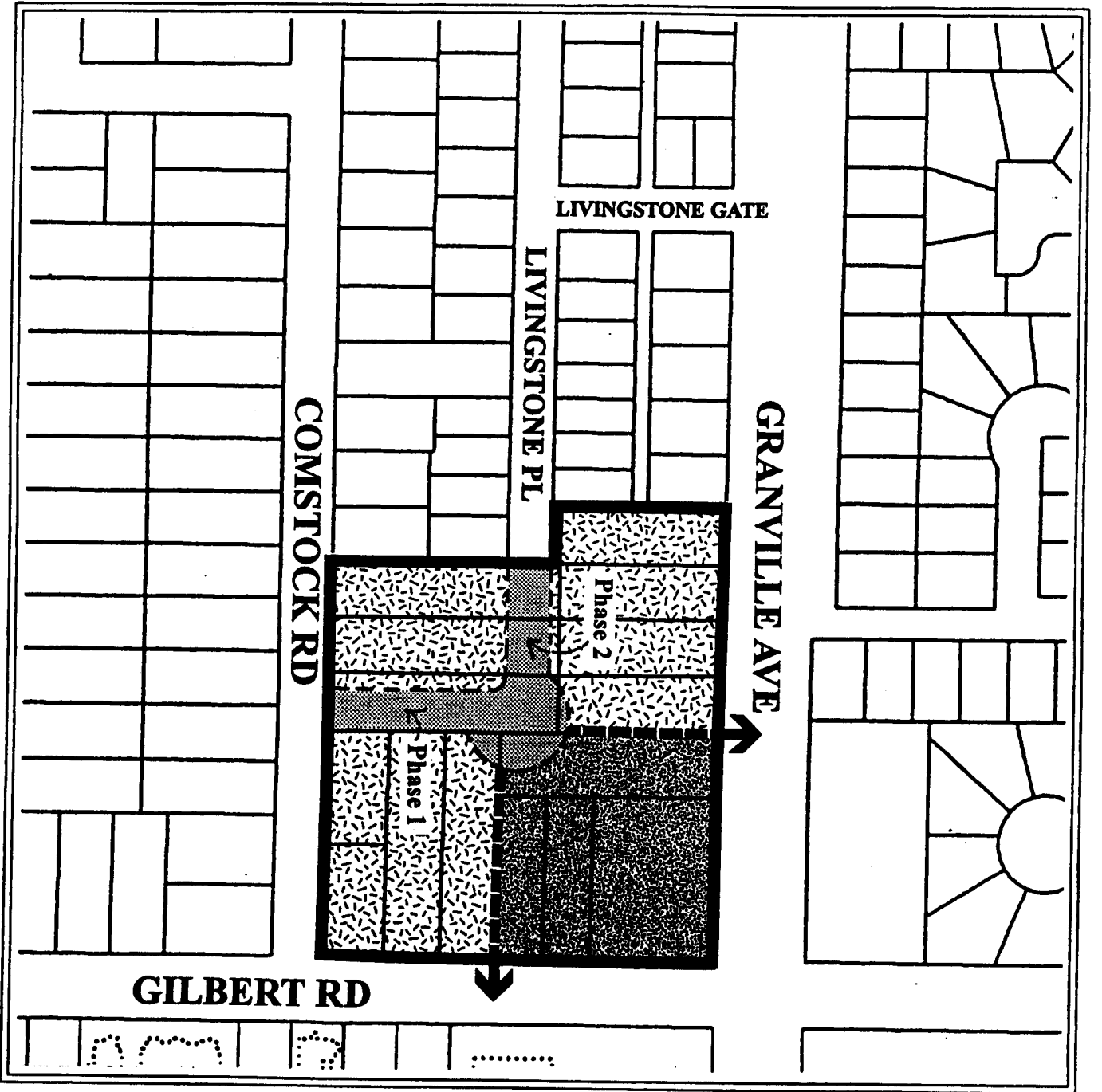


USE VINES AND OTHER VEGETATION TO SCREEN AND SOFTEN RETAINING WALLS.

F. Pathways





8.2.1 Create safe and pleasant pathways between Livingstone Place and the two major arterial streets.

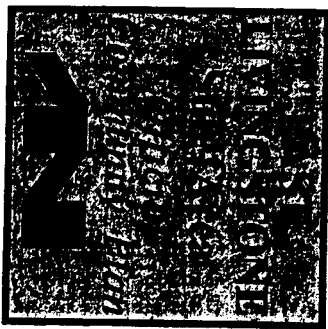
- a) Pathways should be a minimum of 3 m (9.8 ft.) wide. They should be treated with special pavers, and landscaped with small-scale plant materials and shrubs; however, where possible, existing vegetation should be preserved along these pathways.
- b) Maximize visibility and animation along pathways by orienting windows, entries and balconies on adjacent buildings towards paths.
- c) Visually enhance pedestrian linkages and create a sense of arrival through the use of arrival plazas, special plantings and benches at the intersection of Livingstone Place and the pedestrian paths leading to Granville Avenue and Gilbert Road.
- d) Ensure that pathways are well-lit, visible from the street and clearly marked with entrances embellished by ornamental planters or small (columnar or narrow pyramid-shaped) trees.



Attachment 1

LAND USE

-  Small lot single family, two family or Townhouse residential
-  Townhouse or low rise apartment residential
-  Public path/pedestrian right-of-way (approximate)
-  Future road, exact alignment to be determined. Road provision may be phased, with phase one consisting of the connection to Comstock Rd.

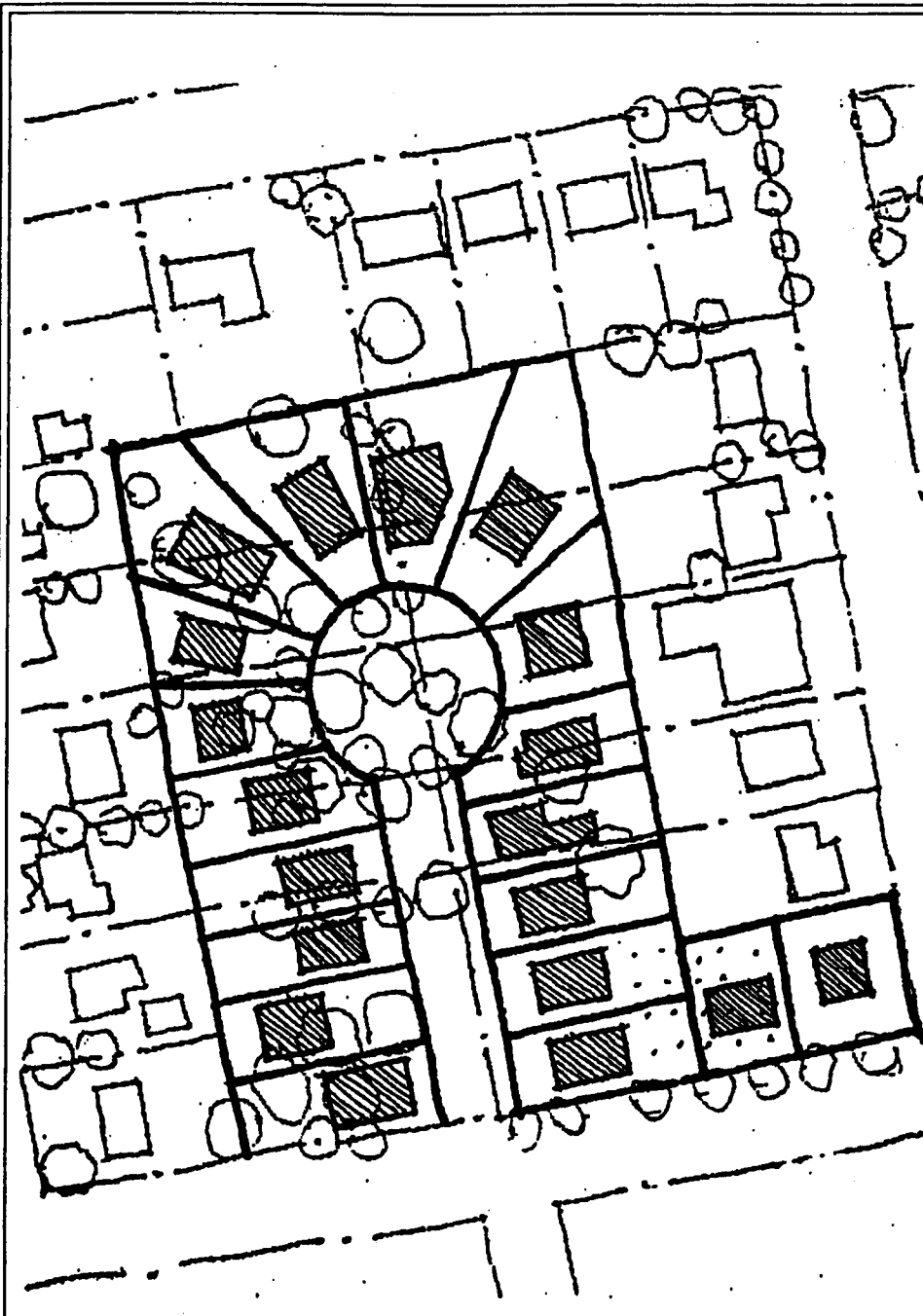


City of Richmond



BROADMOOR Area Plan

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**ASH STREET
Sub-Area**
An Official Community Plan
Bylaw 7100 Schedule 2.6A

BROADMOOR AREA PLAN

Ash Street Sub-Area Plan

1.1 This plan sets out the goals, objectives, policies and development guidelines for the Ash Street Sub-Area, a part of the Broadmoor Planning Area.

Plan Interpretation
Additional Conservation Area and Environmentally Sensitive Area (ESA) policies, guidelines, and locations are included in Schedule 1 of this Bylaw and its attachments (Schedule 1 is a separate document which applies to the entire City). Readers should check Schedule 1 as it takes precedence over this plan in the case of Conservation Areas and ESAs.

The plan may be amended from time to time and users should check with the City's Urban Development Division to make sure that they have an up-to-date version containing all of the adopted amendments.

UD.11.9830

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Original Adoption: March 15, 1999

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ATTACHMENTS

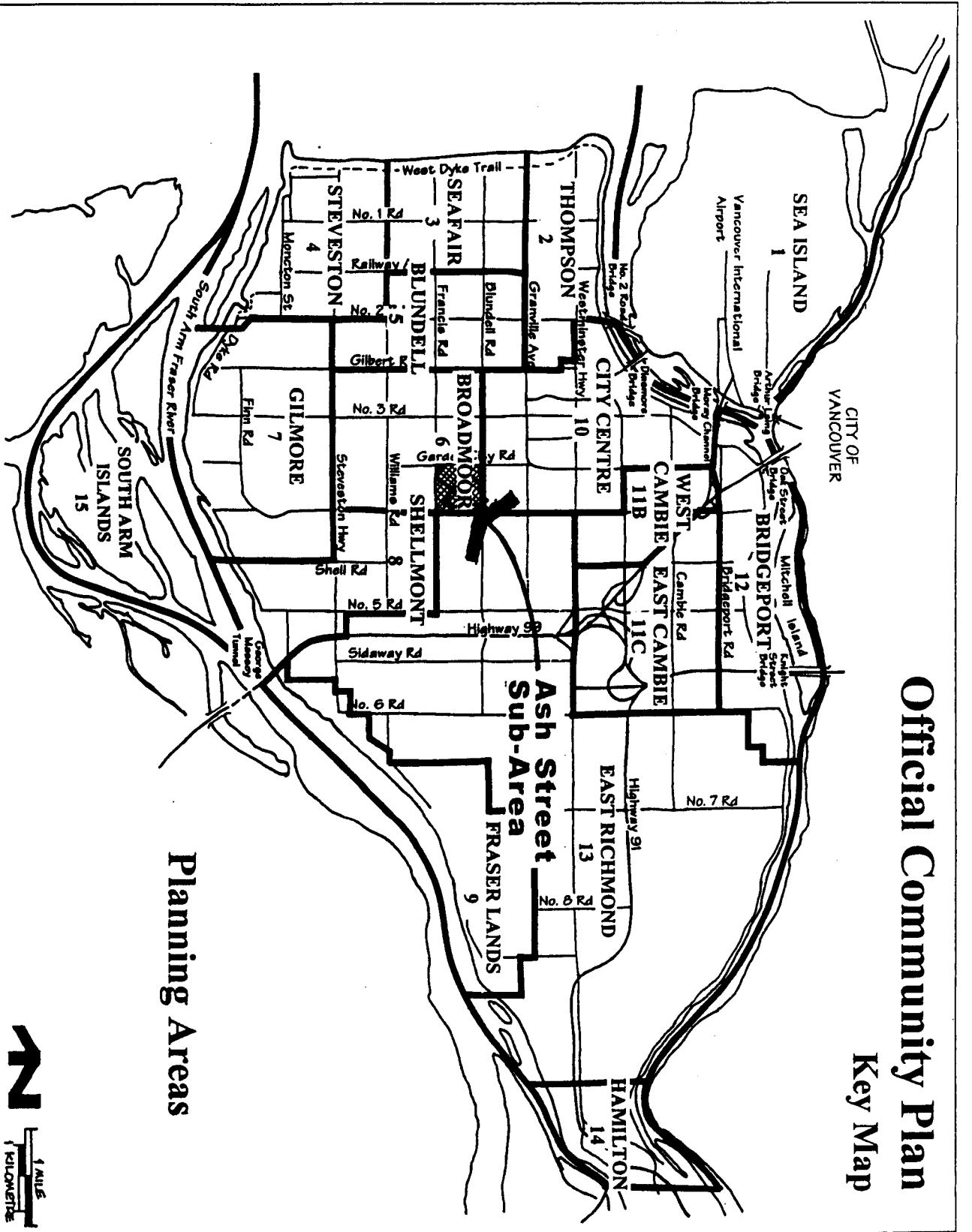
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VI

Original Adoption: March 15, 1999

Official Community Plan Key Map



Planning Areas

1.0 INTRODUCTION

1.1 PREAMBLE

././

The Ash Street Sub-Area is an area where residential growth has led to a need for an expanded sanitary sewer system, closed in ditches and improved roads. This land use plan will allow more precise design of these City services. The plan also includes projected needs for commercial, parks, schools, and other community facilities.

The Ash Street Sub-Area is located in the Broadmoor Planning Area. The Ash Street Sub-Area was planned in advance of the rest of the Broadmoor Area because of development.

This sub-area plan sets out a goal for the area. From this a series of achievable and realistic objectives covering basic planning issues are formulated which together will attain the goal. Specific policy statements then detail the ways in which Council could achieve the objectives. Finally, the process for implementing, monitoring, and evaluating the plan is described so that residents are aware that the document is dynamic. The plan is capable of being amended by Council from time to time in response to the unique needs of the community.

not included - using ocp zoning definitions

1.2 INTERPRETATION

1.2.1 The Plan

The Ash Street Sub-Area Plan Bylaw comprises sections 1 to 4.

1.2.2 Definitions

NOTE: Schedule 1 of the Official Community Plan (OCP) contains a definitions section which applies to the entire OCP. The following definitions apply to this area plan only.

Allow: To approve and permit action by others directed towards achieving objectives.

Control or Limit: Measures taken to restrain, limit, regulate or direct actions within specific bounds or criteria.

Development: Means residential, commercial, industrial or community buildings and structures complete with urban infrastructure such as roads, underground services, parks and open space that are developed on the lands described within this plan.

May or Should: Shall be construed as expressing a desirable voluntary action.

Preserve: Means to protect or keep intact.

Shall or Will: Shall be construed as expressing an imperative course of action.

Use: The purpose for which a lot, parcel, tract of land, building or structure is designed, arranged or intended, or for which it is occupied or maintained.

1.2

2.0 GOAL

The goal of the Ash Street Sub-Area Plan is to maintain and improve the existing residential community by upgrading the quality of services and facilities concurrent with new development.



UD.11.9830

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Original Adoption: March 15, 1999



3.0 OBJECTIVES & POLICY STATEMENTS

Objectives

The major objectives of the Ash Street Sub-Area Plan are to:

1. Provide a population base which will support community facilities such as the school and park;
2. Provide a range of housing types and tenures for a variety of households and age groups;
3. Provide the opportunity for a community commercial area to serve the shopping needs of residents of the immediate area;
4. Provide open space and parks for the benefit of area residents;
5. Ensure that a range of neighbourhood facilities and services are made readily available to area residents;
6. Provide an efficient transportation system which serves the area without channelling traffic from other sources through the neighbourhood; and
7. Improve the quality of public utilities concurrent with population growth.

3.0

3.1 RESIDENTIAL LAND USE

The trend of residential development in the Ash Street area has been one of gradually increasing densities.

This is all repeated in following objectives

The Ash Street area was first subdivided into 1/2 ac. lots about 35 years ago. In the late 1970s and early 1980s, two large subdivisions of 12 m (40 ft.) wide lots were built, and the character of the area began to change.

In 1984, for the first time, medium density residential housing was constructed in the area. This housing took the form of detached homes, two storey townhouses and zero lot-line units on small lots about 9 m (30 ft.) wide. This is a trend which is also occurring in other parts of Richmond, and provides more economical family accommodation than traditional large single-family residential lots. The trend is expected to continue in other parts of the Ash Street area, but with variations.

One characteristic of the existing residential neighbourhood is its age which is dramatically emphasized by the many fine old trees located around the homes and lining the streets. In preserving existing homes, care must be taken to also preserve as many of these trees as possible. New forms of housing can be added to the community in a sensitive manner so that the scale of buildings and life styles of residents are compatible with those existing in the area.

This plan recommends forms of new housing which can be built utilizing the rear portions of 1/2 ac. lots, while still allowing the existing residents to retain their homes. This is sometimes referred to as "infill" housing. In most cases two alternative types of housing are recommended for each designated infill area as shown on the attached area plan map (Attachment 1).

at time of plan adoption

The present population of the area is about 1850 people, living in about 613 dwelling units. ~~If all of the new dwelling units are added as per Attachment 1, the population could increase to about 2300 persons.~~

OBJECTIVE 1: TO PROVIDE A POPULATION BASE WHICH WILL SUPPORT COMMUNITY FACILITIES SUCH AS THE SCHOOL AND PARK.

It is Council's policy to:

- 1.1 Allow development such that the population increase is commensurate with school and park capacity in the area.

OBJECTIVE 2: TO PROVIDE A RANGE OF HOUSING TYPES AND TENURES FOR A VARIETY OF HOUSEHOLDS AND AGE GROUPS.

It is Council's policy to:

- 2.1 Enhance and protect existing single-family areas;
- 2.2 Encourage residential infill development which is compatible with existing single-family areas;
- 2.3 Encourage a variety of dwelling types and tenures by permitting the types and maximum densities indicated for each of the "infill" sites shown on Attachment 1; and
- 2.4 Permit the use and development of lands outside of the "infill" sites shown on Attachment 1 to be governed by the City's normal development application process.

3.2 COMMERCIAL LAND USE

As the population in this area grows, there will be an increasing demand for commercial establishments to meet the weekly shopping needs of residents' . . . (hereafter referred to as community commercial). Based on planning studies, a site of 1.2 to 2 ha (3 to 5 ac.) should be located approximately at the corner of Garden City Road and Blundell Road, which is the middle of the market area. One alternative could include an expansion of the existing commercial centre. The main component of such an expanded centre would be a major grocery store, and possibly a pharmacy. Should the expansion of the shopping centre fail to occur, the site could be developed with residential townhousing.

A pub and two convenience stores are located just outside the Ash Street area.

note
at
note

1 Richmond Urban Development Division, Local Neighbourhood and Community Commercial: Proposed New Zoning Districts (unpublished), 1984.

2.0

OBJECTIVE 3: TO PROVIDE AN OPPORTUNITY FOR A COMMUNITY COMMERCIAL AREA TO SERVE THE WEEKLY SHOPPING NEEDS OF RESIDENTS OF THE IMMEDIATE AREA.

It is Council's policy to:

- 3.1 Permit a community commercial area located as per Attachment 1 to serve the shopping needs of residents of the neighbourhood; and

3.3 OPEN SPACE AND PARKS

As the population of the area grows, there will be an increasing need for parks and open space to meet leisure time needs of residents.

5.0

Three kinds of parks serve most Richmond areas: neighbourhood, community, and City facilities. Because the Ash Street area is small, it only has one kind of facility; neighbourhood parks. Residents travel outside of the area for other recreational opportunities. The closest community park is South Arm Park. ~~However, there is an opportunity for better utilization of both Heather Park and DeBeck School/Park site. Heather Park is much smaller than the average neighbourhood park. There have been some problems on the site due to the lack of facilities such as restrooms. One option would be to develop the park to small lot housing, and fully expand DeBeck School/Park site as the major neighbourhood facility for this quarter section. The other option is to leave it as is.~~

*Information
out of date*

~~There are less than 2.5 ha (5 ac.) of open space for every 1000 people in the Ash Street area at present, which is at the low end of the City standard, however, full exploration of this problem should be carried out during preparation of the Broadmoor Area Plan.~~

Residents have expressed a need for mini-parks or open spaces in each infill area which is developed for medium density housing, to provide a play space for small children and green space with seating for adults.

out of the
Residents have also asked for a plan to upgrade ~~and~~ landscape the properties recently added to DeBeck School/Park site. ~~The unused portion of Bridge Street right-of-way has potential for use as part of the park, which would eliminate the potential for crime in the undeveloped bush land.~~

OBJECTIVE 4: TO PROVIDE OPEN SPACE AND PARKS FOR THE BENEFIT OF AREA RESIDENTS.

It is Council's policy to:

- 4.1 Maintain and enhance the DeBeck Park site;
- 4.2 Add adjacent properties to the DeBeck Park site as City finances permit, or use funds from sale of Heather Park to purchase more parkland at DeBeck;
- 4.3 Close the Bridge Street right-of-way and add the land to DeBeck Park site;
- 4.4 Request the South Arm Community Association to work with the School Board and with local residents in formulating a development plan for DeBeck School/Park site;
- 4.5 Provide for the open space needs of small children and adults in each block where medium density residential infill is developed, by providing a small play area with landscaping and benches; and
- 4.6 Re-examine parkland requirements when the Broadmoor Area Plan is prepared.

3.4 COMMUNITY FACILITIES AND SERVICES

Most of the community service and facility needs of Ash Street residents are met outside of this sub-area, however as the population grows more of these needs can be met within the area. ~~For example if the school-aged population grows sufficiently, DeBeck may be once again utilized as a public school. (DeBeck School is presently being used as a pre-school.)~~

info out of data

~~There are no other community facilities in the area except for a group home at 8180 Ash Street. Childcare services are also lacking in the area and for this reason childcare facilities, particularly family day care and group day care need to be encouraged, especially in light of additional residential development.~~

b.o

OBJECTIVE 5: TO ENSURE THAT A RANGE OF COMMUNITY FACILITIES AND SERVICES ARE MADE READILY AVAILABLE TO RESIDENTS OF THE AREA.

It is Council's policy to:

- 5.1 Encourage the Richmond School Board to retain and utilize the DeBeck site for an elementary school to serve children in the catchment area if the need arises;
- 5.2 Encourage the existing group home to continue to serve the community; and
- 5.3 Encourage the provision of additional space for childcare.

3.5 TRANSPORTATION

The Ash Street Sub-Area is bounded by four arterial roads, Garden City, Blundell, No. 4, and Francis Roads. It is generally City policy that all traffic within the area should be local traffic only, and travel through the area for purposes other than local access should be discouraged. Satisfactory vehicular access should be provided to arterial roads without impeding the flow of vehicular traffic. ~~In order to provide a more orderly road network it is preferable that the main access to the Ash Street Sub Area be from the intersection of the arterials with Dayton Avenue and Ash Street.~~

^{for}
^{it of}
^{etc}
~~No. 4 Road is considered a "major arterial" because it is a high capacity road whose primary function is to carry traffic between major traffic generating areas, and between such areas and the Provincial Highway System. No. 4 Road has already been improved to full standards. Blundell Road is also a major arterial and is being upgraded to a full four lanes.~~

Ash Street and Dayton Avenue are expected to be upgraded also. In order to prevent their use for through traffic and to make the area safer for local residents, ~~some changes to the intersection of Dayton and Ash Streets are being proposed. Because it is a 1/2 mi. long north-south through street, Heather Street has been singled out as a potential route for unwanted through traffic, and some changes are also in order there.~~

In all cases, it is important that residents have equal opportunity to drive from one part of the quarter section to any other part, for example to visit friends or to go to DeBeck Park, without using the arterial roads. This is both a transportation and a social consideration and will help maintain the cohesiveness of the neighbourhood.

The Urban Development Division and the Public Works Division have investigated various methods of accomplishing this objective and the proposed traffic pattern shown in Attachment 2 is considered to be the best.

Finally, the amount of pedestrian traffic along Dayton Avenue to and from Garden City School, Palmer Secondary School, and to the bus justifies improved pedestrian facilities on Dayton Avenue.

OBJECTIVE 6: TO PROVIDE AN EFFICIENT TRANSPORTATION SYSTEM WHICH SERVES THE AREA WITHOUT CHANNELLING TRAFFIC FROM OTHER SOURCES THROUGH THE NEIGHBOURHOOD.

It is Council's policy to:

- 6.1 Maintain the integrity of arterial roads by providing access at appropriate locations;
- 6.2 Discourage through traffic on Dayton, Heather, and Ash Streets by use of barriers, cul-de-sacs or other means, as indicated in Attachment 2;
- 6.3 *out of date* Accommodate pedestrian movement on Dayton Avenue; and
- 6.4 Develop a transportation system which achieves the results shown on the attached transportation plan, Attachment 2.

3.6 PUBLIC UTILITIES

When the first 1/2 ac. lots were subdivided and the first homes were constructed in the Ash Street Sub-Area many years ago, public utilities were minimal. Because servicing in new subdivisions now includes sanitary sewers, storm sewers, curbs, gutters, sidewalks and underground wiring, the level of expectation for services in older areas is rising. Problems with drainage and septic tanks in older areas has also led people to request improvements to their utilities.

As infill development occurs it will be possible to economically install better public utilities for the surrounding areas. Residents would prefer to have sanitary sewers, especially in areas where the sub-soil may contain peat, and where septic tile fields may become flooded during periods of high water table. The installation of sanitary sewers to service 1/2 ac. lots is not economical. Alternatively, the acquisition of a sewer easement at the rear of existing homes may be difficult until land assembly has occurred prior to infill development.

**OBJECTIVE 7: TO IMPROVE THE QUALITY OF PUBLIC UTILITIES
CONCURRENT WITH POPULATION GROWTH.**

It is Council's policy to:

- 7.1 Install sanitary sewers to serve the Ash Street area, as redevelopment occurs;
- 7.2 Install a sanitary sewer system for the area north-west of Dayton Avenue and Ash Street concurrent with development, and in cases where it is impractical to wait for residential infill to occur, install sanitary sewers prior to development of new homes; and
- 7.3 Take necessary measures to maintain public health and safety in older areas.

not included - either completed or included in ocp

4.0 IMPLEMENTATION

Section 3 provides a guide for decisions that will shape the future of the Ash Street community. The concept and policies of the plan are developed in conjunction with the community and the achievement of the plan's goal, objectives and policies depends upon the support of the whole community. Richmond Council has the ultimate responsibility to ensure the plan's successful implementation. Because implementation will happen over time, a procedure is required.

Council's role in implementation is primarily to ensure orderly development consistent with the overall goal, objectives and policies stated in this plan. In approving rezonings or development permits Council will be guided by the stated policies of the plan.

The City of Richmond, in matters under its jurisdiction, shall be governed by the Ash Street Sub-Area Plan in the formulation of specific programs and in the determination of capital expenditures.

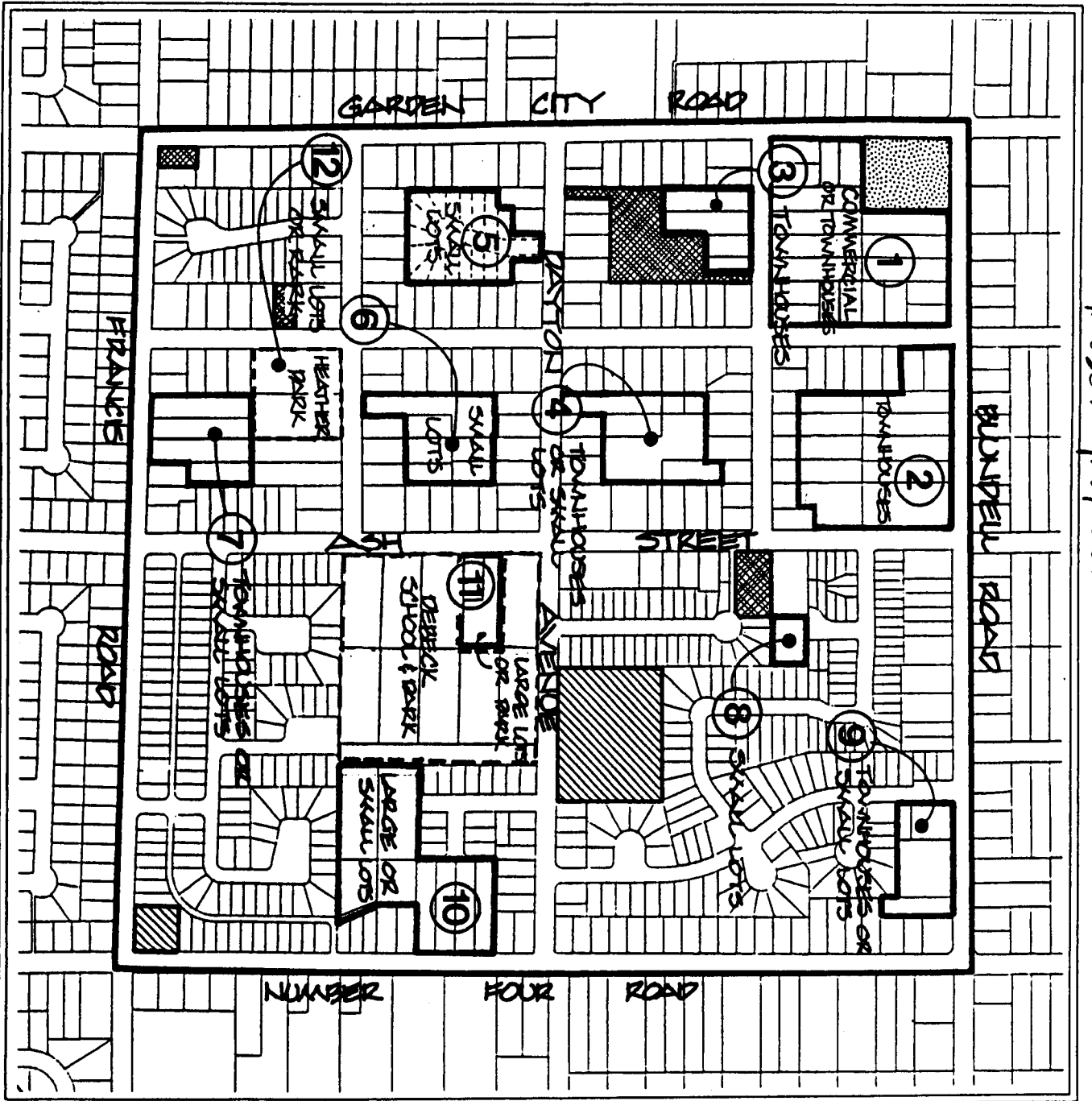
In order to implement the plan, it is Council's intent to:

1. Rezone properties to conform with the plan at the time that development occurs;
2. Rezone Bridge Street and other publicly owned parks to School and Park Site District;
3. Establish priorities for all public works, services (including public safety) to be included in the City five year capital program based on the plan;
4. Monitor the plan on a continuing basis with a periodic review and incorporation of changes, if any, by Council; and

-
5. ~~Review~~ the entire plan every five years to determine whether the major policy issues ~~continue~~ to be addressed in the plan.





*Development permit studliness added
to address adjacency issues
with the AR*

base map updated



Attachment 1

Land Use

-  Commercial
-  Institutional
-  Townhouses
-  Designated infill areas. Refer to table : 1

Ash Street Sub - Area
 Official Community Plan



TABLE 1
 OFFICIAL COMMUNITY PLAN - DESCRIPTION OF DEVELOPMENT INFILL AREAS
 ASH STREET (SECTION 22-4-6)

KEY TO AREA PLAN MAP	APPROXIMATE AREA HA (A.C.)	DENSITY UNITS/HA (UNITS/A.C.)	MAXIMUM NUMBER OF UNITS	LAND USE	OTHER
1	24 (6)		N.A.	Commercial or townhouses	Maximum 2 storey height
2	1.86 (4.61)	29 (12)	77	Townhouses or small lots	Open space and children's play area with townhouses or small lots
3	0.502 (1.25)	*	*	Townhouses or duplex	Open space and children's play area with townhouses
4	1.07 (2.64)	35 (14)	37	Townhouses or small lots	Open space and children's play area with townhouses or small lots
5	.95 (2.34)	18 (7)	18	Small lots	Open space and children's play area
6	.81 (2)	18 (7)	14	Small lots	Open space and children's play area
7	0.830 (2.05)	29 (12)	25	Townhouses or small lots	Open space and children's play area
8	0.12 (0.3)	18 (7)	3	Small lots	
9	0.645 (1.6)	19 (12)	24	Townhouses or small lots with lane	Access to Blundell restricted. Open space and children's play area. Possible lane.
10	1.8 (4.45)	18 (7)	31	Small lots or large lots	
11	N.A.	N.A.	N.A.	Large lots or park	Open up corner for public view of DeBeck school/park
12	N.A.	N.A.	N.A.	Park or small lots	
TOTAL			246		

*.55FAR

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not included - out of date

APPENDIX A - PARKS AND OPEN SPACE

Park Sites

Two park sites are located in the area; DeBeck School/Park (2.53 ha (6.25 ac.)) and a smaller park known as Dolphin Park (0.81 ha (2.0 ac.)).

Dolphin Park is grassed and has a small playground and baseball diamond. Originally proposed for sale by the City because of its low usage by the public, Council has recently reaffirmed its retention (March 12th, 1984). At the moment no further park development on the site is planned.

DeBeck School/Park site, contains a two classroom primary annex. The School Board owns the major portion (2.17 ha (5.35 ac.)) of site. The site serves as a neighbourhood park for about 2300 residents within a 0.6 km (3/8 mi.) radius of the site. In 1983, 0.36 ha (0.9 ac.) was added to the site by the City to increase its size.

Council recently voted to acquire an additional 0.36 ha (0.9 ac.) to add to the northwest portion of the site, and has recommended that the rear portion of the property at the south end of the park also be acquired at some time in the future. The site is presently smaller than the average for neighbourhood parks which is 2.83 ha to 3.6 ha (7 to 9 ac.)

• DeBeck School/Park	2.53 ha	6.25 ac.
• Heather (Dolphin) Park	0.81 ha	2.0 ac.
• Addition to DeBeck	0.36 ha	0.9 ac.
• Bridge Street unused road	<u>0.41 ha</u>	<u>1.0 ac.</u>
TOTAL	4.11 ha	10.15 ac.

not included - descriptive not policy

APPENDIX B - COMMUNITY FACILITIES

Context

Much of a family's daily life focuses on the neighbourhood. Local community services are a vital way in which daily life is maintained and enhanced. A range of community services are available in Richmond including: Education, Leisure Services, Health, Library Services, Religious Organizations, Social Services, Childcare and Public Safety and Administration. Some services are established in local areas, while others are available only in the City Centre and serve all of Richmond.

Community Facilities in Ash Street Area

The Ash Street area is located within the larger neighbourhood known as Broadmoor. The quarter section is too small to have services to serve only that area. However, several community services are located here, but serve a wider area, or all of Richmond.

Assisted Housing

A group home, administered by Touchstone Family Association is located at 8180 Ash Street. Under a contract with the Ministry of Human Resources, the Association provides 8 beds for "at risk" youth. The Centre is used for Treatment and Assessment with 6 beds for long term stay (up to three months) and 2 beds for emergency placement.

Religious Organizations

Two religious organizations are located in the Ash Street area at 9051 Dayton Avenue and 8851 Heather Street. The site on Heather Street is a single-family home. Neither has extensive community facilities which are open to the general public.

Childcare

Two pre-schools are planned for the DeBeck School. No other childcare facilities of any type are located in the neighbourhood. In fact, no childcare facilities are located within the larger Broadmoor area. Two family day cares are located 1/2 mi. away from the north-west and south-west corners of the study area, and three nursery schools are located to the south-west of the study area, all in the Broadmoor planning area.

Health Services

Public health nurses, long term and home care staff visit clients in the home.

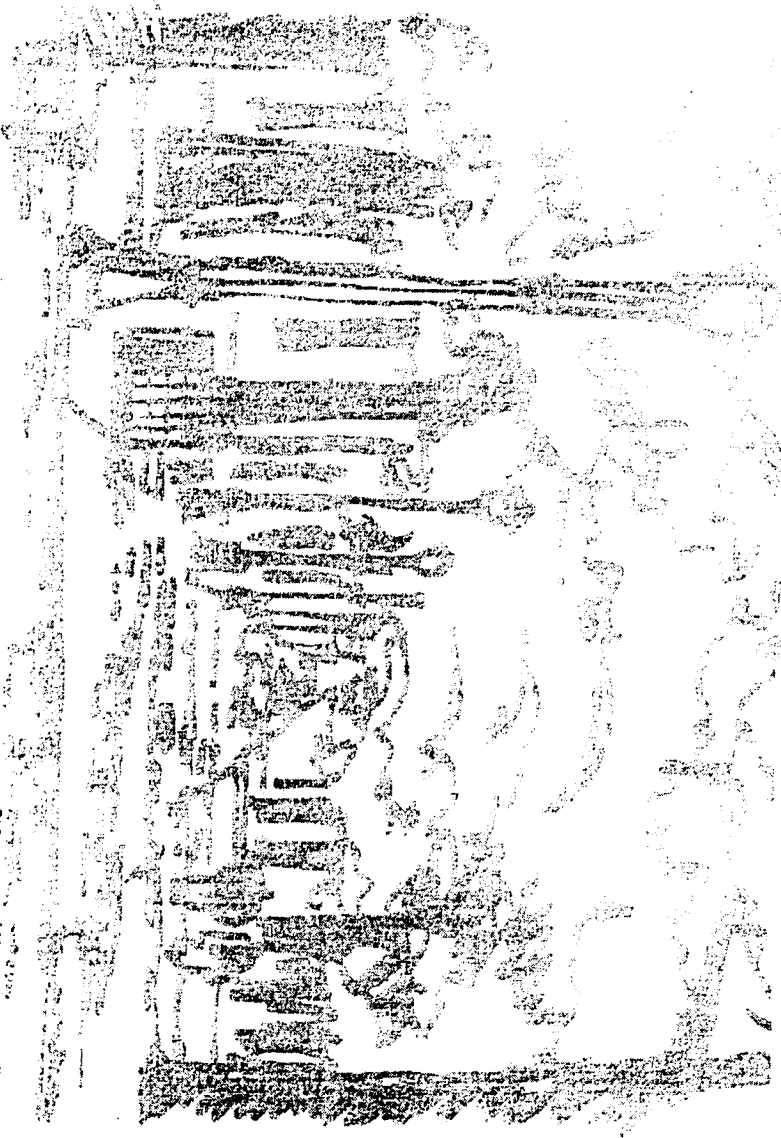
BROADMOOR Area Plan



City of Richmond

CENTRAL WEST BROADMOOR Sub-Area

*An Official Community Plan
Bylaw 7100 Schedule 2.6B*



BROADMOOR AREA

Central West Broadmoor Sub-Area Plan

1.1 This plan sets out the goals, objectives, policies and development guidelines for the Central West Broadmoor Sub-Area, a part of the Broadmoor Planning Area.

Plan Interpretation Additional Conservation Area and Environmentally Sensitive Area (ESA) policies, guidelines, and locations are included in Schedule 1 of this Bylaw and its attachments (Schedule 1 is a separate document which applies to the entire City). Readers should check Schedule 1 as it takes precedence over this plan in the case of Conservation Areas and ESAs.

The plan may be amended from time to time and users should check with the City's Urban Development Division to make sure that they have an up-to-date version containing all of the adopted amendments.



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Original Adoption: March 15, 1999



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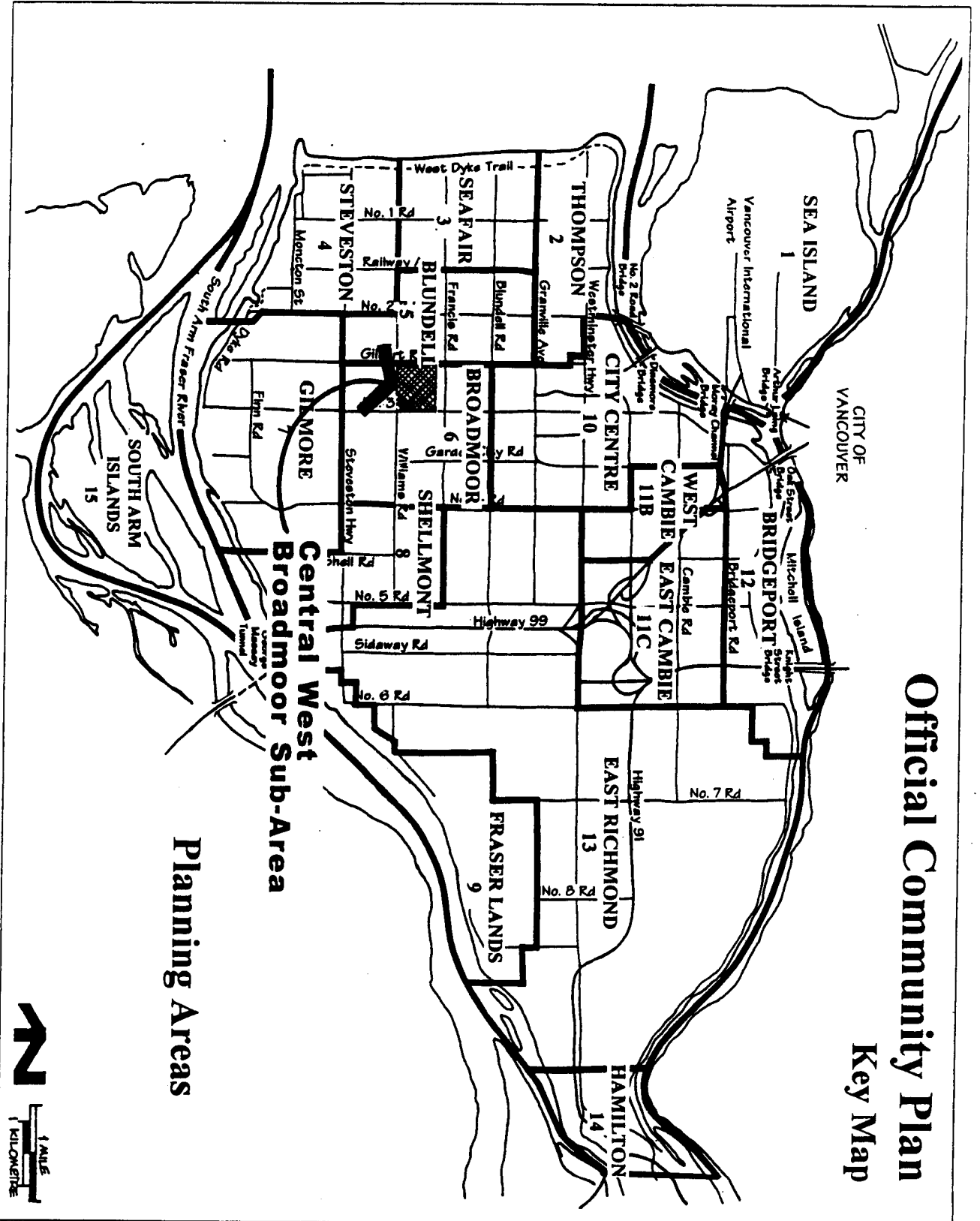
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LIST OF ATTACHMENTS

1 Land Use Map 11

Official Community Plan

Key Map



Original Adoption: March 15, 1999

1.0 INTRODUCTION

/.

The Central West Broadmoor Sub-Area Plan is bounded by Williams Road to the south, No. 3 Road to the east, Gilbert Road to the west, and the north side of Afon Drive.

This plan contains appropriate land use designations and Development Permit Guidelines for the Central West Broadmoor Sub-Area.

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2.0 GOAL AND POLICIES

2.1 GOAL

1.2

To create a livable and stable, family-oriented residential neighbourhood while consolidating a distinct urban corridor with strong linkages to adjacent neighbourhoods outside Central West Broadmoor, on the neighbourhood's eastern perimeter.

2.2 POLICIES

2.2.1 Land Use

~~The area bounded by Williams Road, No. 3 Road, Gilbert Road and the north side of Afton Drive is designated for large lot (Single-Family Housing District, Subdivision Area B (R1/E)) single-family residential use with the following exceptions:~~

*See
Land Use
Map*

- ~~• Portion 'B' of 9591 No. 3 Road is designated for large lot (Single-Family Housing District, Subdivision Area H (R1/H)) single-family residential use;~~
- ~~• Properties on No. 3 Road which are directly north of 9591 No. 3 Road are designated for small lot single-family residential use;~~
- ~~• Properties on No. 3 Road, south of 9591 No. 3 Road, lots fronting Williams Road between No. 3 Road and Greenless Road, and portion 'A' of 9591 No. 3 Road, are designated for townhouse residential use; and~~
- 7011 and 7371 Williams Road are designated for small lot single-family residential use.

(Land uses are shown more specifically on Attachment 1.)

~~Large lot single-family refers to a single detached dwelling unit of a lot size conforming to Single-Family Housing District, Subdivision Area E (R1/E) zoning specifications.~~

~~Small lot single-family residential refers to a single detached dwelling unit of a lot size conforming to Single-Family Housing District, Subdivision Area B (R1/B) zoning specifications.~~

~~Townhouse residential consists of a building or buildings containing two or more dwelling units, where each has a separate entrance at first-storey level.~~

2.2.2 Housing and Neighbourhood Character

~~While maintaining the large lot qualities of the neighbourhood, provide on the eastern perimeter a range of housing types that can accommodate a variety of families and households.~~

~~Strengthen the identity of the No. 3 Road corridor as part of an emerging neighbourhood village.~~

~~Ensure that multiple-family residential is compatible with adjacent single-family neighbourhoods.~~

~~Foster a green neighbourhood by encouraging the retention of trees on single-family properties, and requiring tree retention or replacement on multiple-family properties.~~

~~Ensure the neighbourhood is well-connected by maintaining existing pedestrian connections from the centre portion of the neighbourhood through to No. 3 Road, and by providing additional pedestrian and cycling connections as opportunities arise. Ensure these paths are attractive and safe.~~

3.1

5.1

2.2.3 Parks and Open Space

Improve access to recreational activities. Maximize use of park space outside the neighbourhood by making access to these parks safer, through crosswalks and other measures.

2.2.4 Circulation

Ensure that vehicular access to new developments from No. 3 Road is limited.

4.1

Provide vehicular access to single-family and two-family residential developments through lanes. Where possible, also use lanes to provide vehicular access to multiple-family development, to minimize the visual and physical impact of the automobile on neighbourhood streets and the pedestrian realm.

Improve lane access to No. 3 Road at mid-block. Extend the lane west of Bates Road through the City-owned property directly west of 9511 No. 3 Road.

Improve traffic safety on neighbourhood streets. Place stop signs in appropriate locations along Broadmoor Boulevard and consult with residents on additional traffic safety measures in other neighbourhood locations.

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3.0 DEVELOPMENT PERMIT GUIDELINES

3.1 APPLICATION AND INTENT

8.1.1

Schedule 1 of this bylaw designates areas for the establishment of objectives and provision of guidelines for the form and character of multiple-family residential development. The Central West Broadmoor Sub-Area Plan map (Attachment 1) designates areas for multiple-family use.

A key housing policy of Richmond's Official Community Plan is to carefully integrate new housing development into existing neighbourhoods. All multiple-family projects merit site-by-site consideration of form and character in order to achieve the desired relationship to surrounding areas, and to ensure high standards of new housing and design.

8.1.2

Central West Broadmoor in particular requires special consideration in order to fulfil the sub-area plan goal, and to achieve the specific policy of ensuring compatibility between multiple-family and existing single-family neighbourhoods.

3.2 APPLICATION OF CITY-WIDE GUIDELINES

8.1.1

Schedule 1 of this bylaw contains general guidelines for development of multiple-family areas, and these apply to multiple-family sites in the Central West Broadmoor Sub-Area. Those wishing to develop in Central West Broadmoor's designated multiple-family areas are advised to consult and follow the guidelines stipulated in this and all other pertinent documents.

3.3 ADDITIONAL GUIDELINES SPECIFIC TO CENTRAL WEST BROADMOOR'S MULTIPLE-FAMILY AREAS

3.3.1 Residential Livability

Provide a quiet and livable residential environment by reducing the impact of traffic noise on residential units. All developments on properties adjacent to No. 3 Road must provide a minimum 6 m (19.68 ft.) setback from this road or provide other remedial measures acceptable to staff.

Ensure compatibility with single-family housing across the lane and to the north through complementary scale, massing and building form.

Each dwelling should have a near, middle, and distant view.

Enhance the safety of internal pedestrian circulation by orienting windows to any internal walkways.

8.2.4 Reinforce the natural environment on private property:

- Retain existing trees and mature plants, and augment them with ornamental flowering trees and shrubs;
- Provide each dwelling unit with its own private outdoor space, and clearly demarcate this space at ground levels; and
- Permit no more than 75% of a site to be covered by buildings and non-porous surfaces. A minimum of 25% of the site must be covered with soft landscaping including plant materials.

8.2.5 Provide appropriate garbage facilities. Erect a covered, gated structure to contain residents' garbage and recycling materials. The design of this structure should complement the design of units in the project. Landscape screening of this structure should be provided.

8.2.1 Minimize the visual presence of, and land occupied by, motorized vehicles on sites:

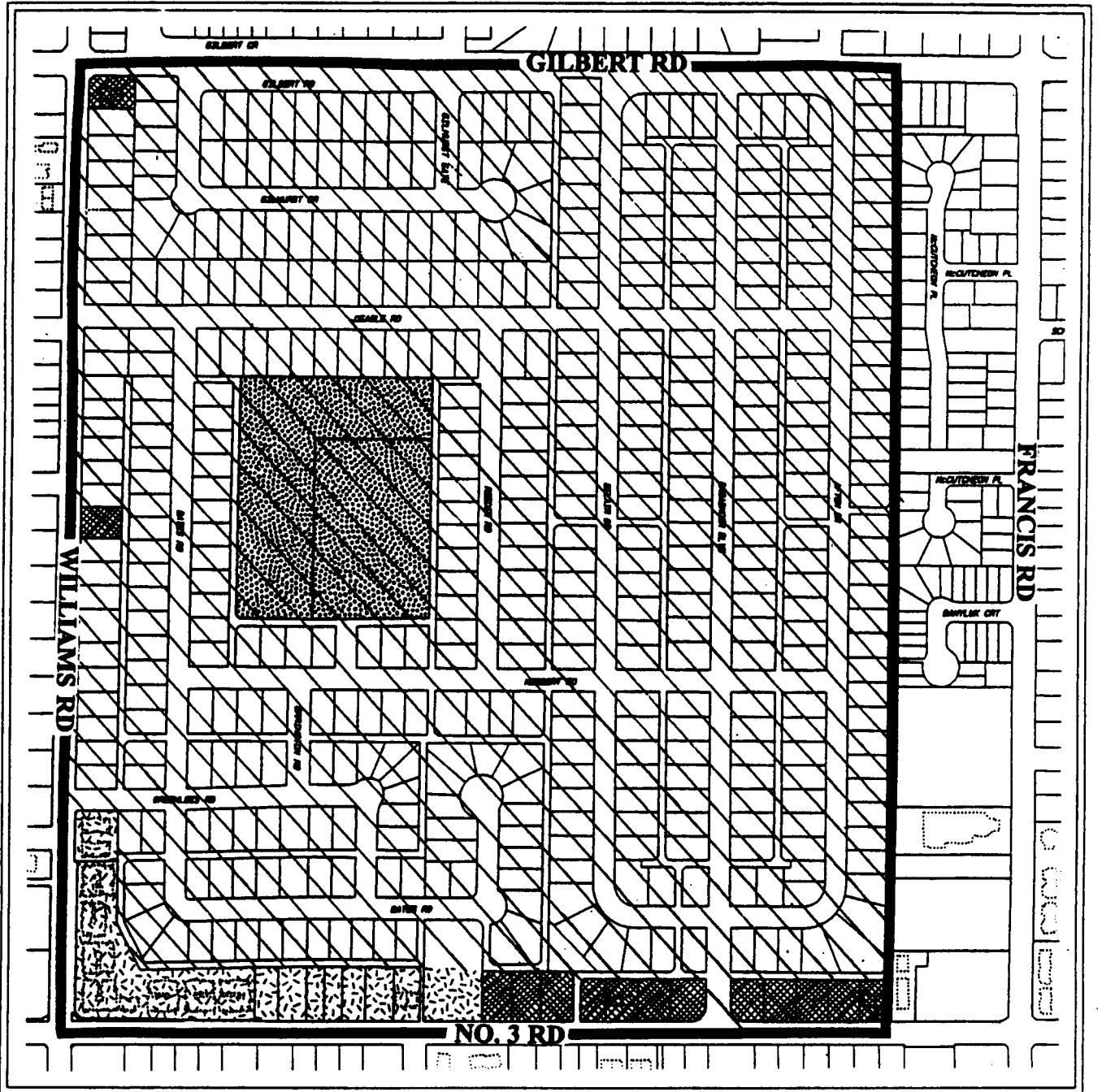
- Cover or screen resident parking from the street and encourage tandem parking;
- Limit access driveways to 3 m (10 ft.) width and combine them where possible;
- Minimize the surface area of blacktop parking and access driveways through alternate paving treatments; and
- Use special landscaping features, such as trellises, to conceal garages and visitor parking stalls from surrounding private, semi-private and public areas.

3.3.2 Street and Lane Character

8.2.1 Improve No. 3 Road as a pedestrian thoroughfare, and enhance its visual interest through the built environment.





- **8.2.3** Clearly define and number entrances to each unit, and encourage the orientation of front doors and windows towards No. 3 Road. Avoid facing blank building walls onto the street.
- **8.2.2** Reduce parking visibility from the street by discouraging placement of garages facing No. 3 Road. Where this is not possible, driveway access to garages must be treated exclusively with special pavers and embellished with ornamental shrubs and trees.
- **8.2.1**

-
- 8.2.2 Buildings should be set back from streets and open spaces in a manner that promotes continuity of local street definition, landscape, and residential character.
 - 8.2.4 Fences and hedges along No. 3 Road should be limited to 1.1 m (3.6 ft.) in height.
 - 8.2.3 Provide richer detailing, high quality, durable materials and special lighting which will enhance the pedestrian's close-up view of buildings along No. 3 Road.
 - 8.2.1 Use measures to ensure that vehicular access from No. 3 Road does not endanger or inconvenience pedestrians or the mobility impaired.
 - 8.2.3 Encourage the use of sloped roof forms and visually interesting, high quality roofing materials. Heavy tiled roofing is not appropriate.
 - ↓ Discourage the treatment of buildings with decorative patterned stucco, imitation brick, or vinyl siding.
 - 8.2.5 Place all utilities underground.
 - 8.2.1 Enhance safety on public pedestrian paths, by orienting windows and doors towards these paths and limiting fences and hedges along such paths to 1.1 m (3.6 ft.) in height.
 - ↓ Recognize that significant pedestrian activity will also occur on lanes, and improve their appearance and safety.
 - 8.2.4 Provide pedestrian-oriented lighting along lanes and internal roadways on private developments.
 - ↓ Include landscape materials, complete with necessary space, soil depth and irrigation.
 - 8.2.5 Screen garbage facilities and related uses.



Attachment 1

LANDUSE

-  Small Lot Single-Family (R1/B)
-  Large Lot Single-Family (R1/E)
-  Townhouse Residential (R2)
-  Public and Openspace Use

Central West
Broadmoor
Sub-Area
Official
Community Plan





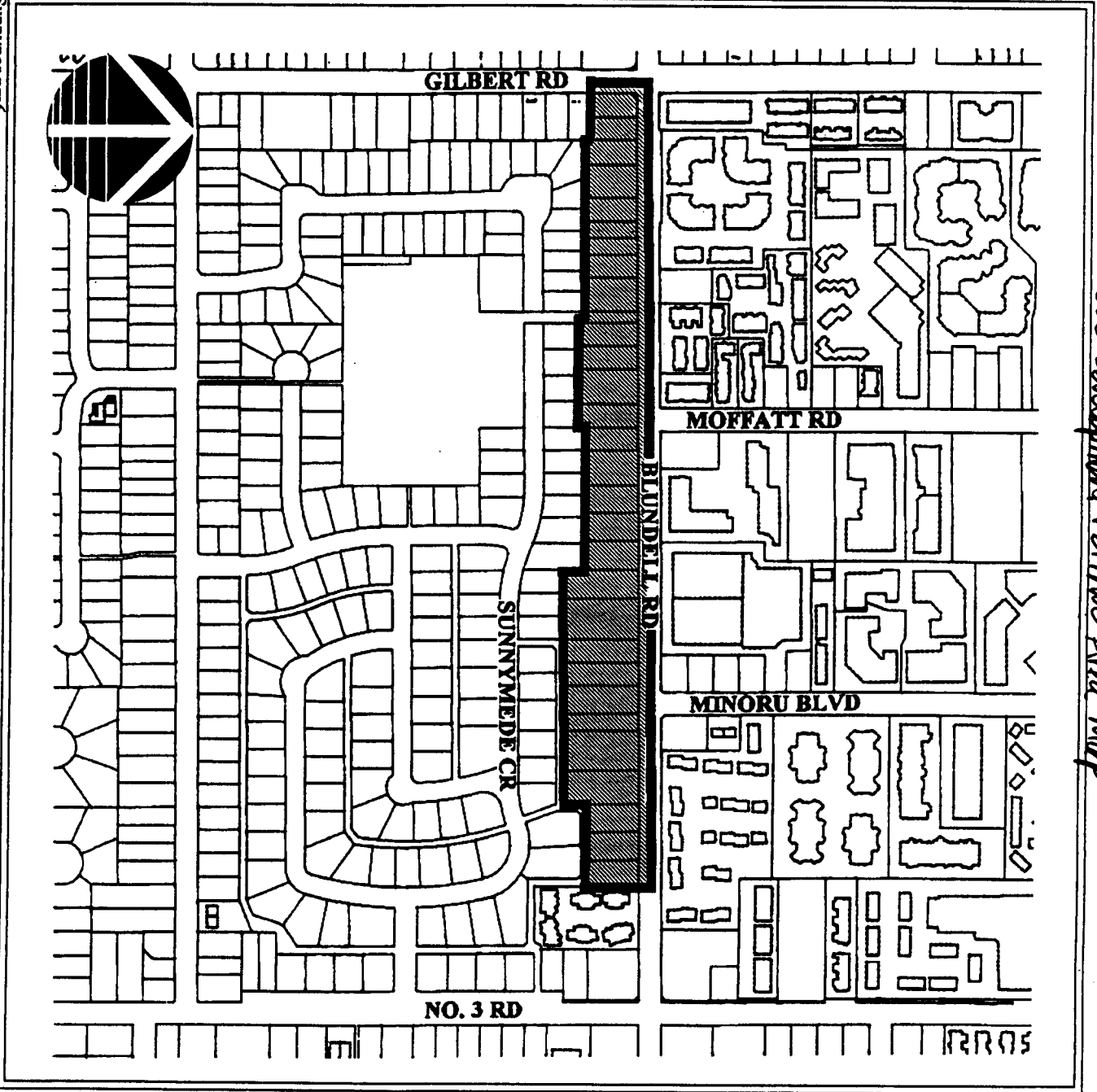
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see Development Permit Area Map



Sunnymede.pnt6

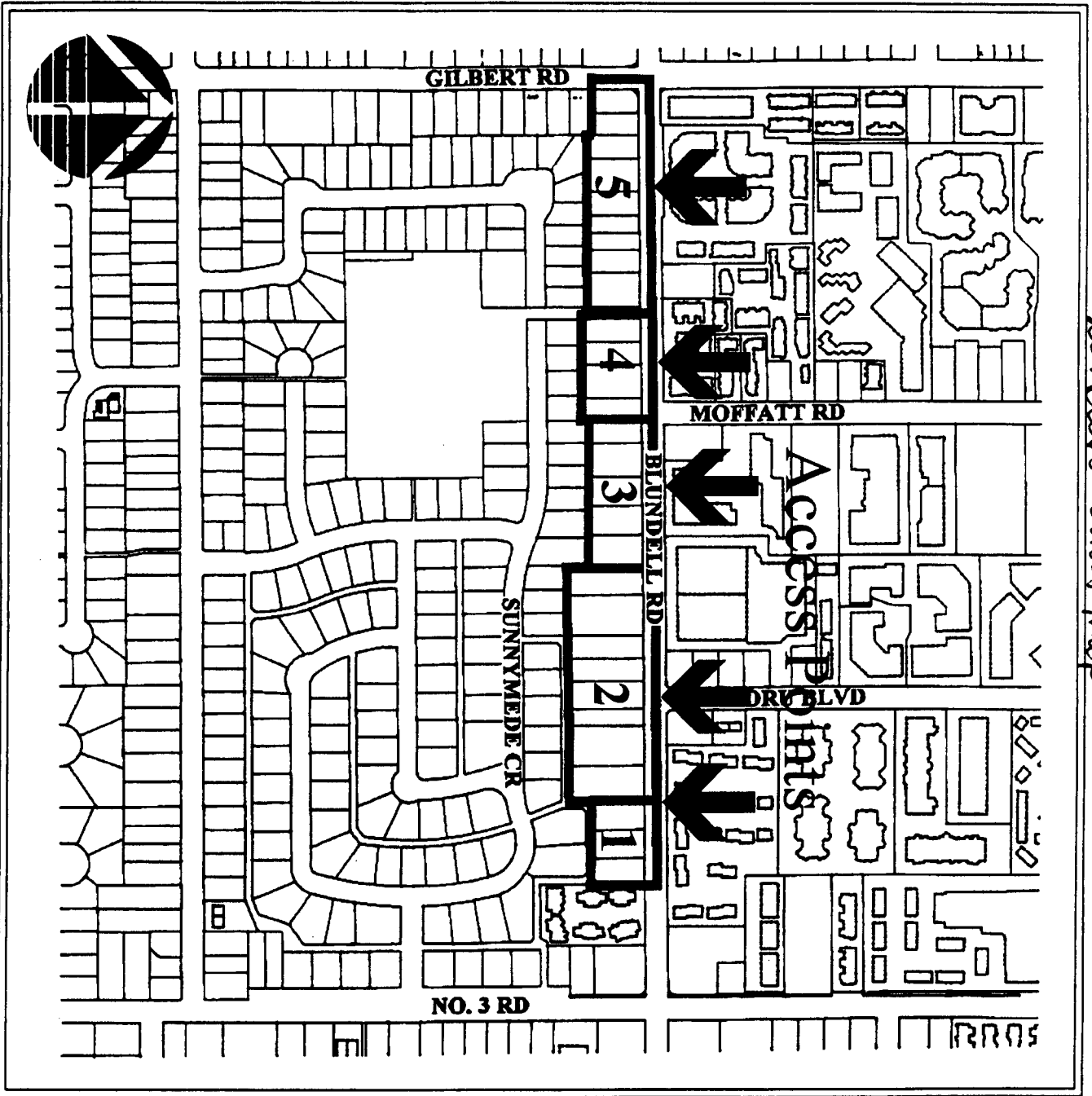


Attachment 1
Bylaw 7100 Schedule 2.6C



Original Adoption: March 15, 1999

see Access Points Map



Sunny/nor.pnt6

Attachment 2

Bylaw 7100 Schedule 2.6C



Original Adoption: March 15, 1999

**SCHEDULE 2.6C OF BYLAW NO. 7100
SUNNYMEDE NORTH SUB-AREA
DEVELOPMENT PERMIT GUIDELINES**

JUSTIFICATION

~~*This is a description of the special conditions and objectives that justify the designation of the south side of 7,000 block of Blundell Road as a special area for multiple-family residential.*~~

The south side of the 7,000 block of Blundell Road forms a critical boundary between the medium density residential district to the north in the City Centre Planning Area, and the low density single-family to the south in the Broadmoor Planning Area. Currently, the area is occupied by older single-family homes surrounded by mature vegetation. The single-family lots in the area presently access Blundell Road which is classified as a major arterial road.

The area abuts an established single-family neighbourhood to the south. New development along the south side of the 7,000 block of Blundell Road must blend with the character and the scale of this adjacent single-family area.

There is a need to control access onto Blundell Road, as well as to exercise greater control over the form and character of development in this area to ensure that a harmonious transition occurs between a medium density neighbourhood and a low density neighbourhood. These objectives justify the imposition of additional guidelines to control and direct development in this area.

8.1.2

8.1.1

The area shown shaded on Attachment 1 is subject to the Development Permit guidelines contained herein.

**GUIDELINES FOR THE ISSUANCE OF DEVELOPMENT PERMITS FOR
MULTIPLE-FAMILY RESIDENTIAL DEVELOPMENTS ON THE SOUTH SIDE
OF THE 7,000 BLOCK OF BLUNDELL ROAD**

SECTION A: FORM AND CHARACTER

1. Roofscapes

8.2.3 a) Construct pitched roofs of various heights with a minimum slope of 6 to 12 with gable ends and dormers to create diversity.

↓
b) Provide soffits with overhangs to add interest and help protect the siding of the buildings.

8.2.2 c) Reduce the scale of building forms adjacent to semi-private open spaces, pedestrian walkways, main vehicular entrances and adjacent to the Sunnymede single-family neighbourhood to the south through the use of secondary roof elements of a human scale such as covered porches or trellis elements.

8.2.3 d) Select roofing materials which are suitable for the level of articulation desired in the roof forms. Cedar shingles would facilitate this, whereas heavy-tiled roofing would not.

2. Exterior Design and Finish of Buildings and Structures

8.2.3 a) Use natural building materials such as wood, stone, brick or stucco on building exteriors. Unacceptable finish materials include imitation brick and highly patterned stucco.

↓
b) Employ a combination of two exterior materials on facades to help maintain a human scale, create texture and enhance design.

B.2.3

- c) Account for the effects of the Richmond climate in the choice of material and detailing. Stucco should be treated to prevent discolouration and particular care should be taken in detailing the north facades facing Blundell Road.



B.2.1

- d) Use a wide range of natural earth tones commonly produced as "heritage series" by a number of commercial paint manufacturers on the main exterior of the buildings. Compliment and coordinate the range of colours used on the various units with accent colours on the trim. Reserve the use of vivid colours to "highlighting" areas such as the trims and doors.
- e) Use decorative trim, cornices, reveals and projections where appropriate.

B.2.2

- f) Design buildings with a main orientation towards Blundell Road and the internal roadways of the projects. Fenestration in facades facing the internal roadways and open spaces should be carefully arranged to provide for internal unit privacy, yet convey a perception of "eyes of the street" enhancing safety and security for residents.
- g) Units should have their massing designed to provide articulation to the streetscape both vertically and horizontally. Flat street fronts are unacceptable, as are continuous cornice lines.

B.2.1

- 3. Entrances, Stairs and Porches
 - a) Create highly visible and identifiable principal building entrances through the use of landscape and prominent architectural components such as gateways and covered porches to emphasize the transition from public and semi-private spaces to each unit's private space. Entrances should provide protection from the elements of the weather. Use low level landscaping and/or consider the use of changes in grade to ensure the privacy of an individual unit's private open space without the use of high fences. (The grade change should be no more than .61 m (2 ft.) to .91 m (3 ft.) and should occur between the curb and the main door of each unit).

8.2.1 b) Establish clear and appropriate pedestrian connections between private residences and semi-public areas.

c) Ensure that the units located directly adjacent to Blundell Road have their front doors facing Blundell Road with a pathway connecting from the sidewalk to each doorway. All of the other units in each development should have their front doors facing the internal roadways.

d) Locate secondary entrances from private residences onto common walkways located between developments.

4. Windows

8.2.3 a) Express a residential character by using a variety of appropriately scaled and proportioned windows.

b) Articulate building facades through the use of various forms of specialty window projections such as sky lights, bay windows, and dormers to achieve design interest and improved interior light.

c) Encourage the provision of window treatments such as flower boxes and shutters.

d) Orient windows of living spaces towards pedestrian walkways and streets to help facilitate supervision of the street and increase pedestrian safety.

5. Balconies, Decks and Patios (Private Open Spaces)

8.2.4 a) Provide each unit with a private outdoor space that is at least 37 m² (398.28 ft²) in area and 5.25 m (17.22 ft.) in depth.

8.2.3 b) Ensure that balconies to upper floors facing Blundell Road are small and cantilevered, giving minimal exterior access. Their design should be treated like a decorative exterior element enhancing the building's articulation and massing.

- 8.2.4
- c) Design decks and patios as natural extensions of each unit into the landscape. A maximum of 40% of each unit's private space may have a deck, with the remainder in soft landscaping including grass, shrubs and trees. The provision of decks and patios developed at a maximum of 45.72 cm (18 in.) above grade are preferable to second-storey balconies on units located adjacent to the existing Sunnymede single-family neighbourhood.
- d) Separate each unit's rear private open space with a fence no higher than 1.8 m (6 ft.). Consider the importance and the safety of small household pets by ensuring that a gap no larger than 5.08 cm (2 in.) is provided between the ground and the base of the fence.

SECTION B: LANDSCAPING AND OPEN SPACE

1. Amenity Space

- 8.2.5
- a) Locate a highly visible amenity space in each development area along with an indoor amenity space in projects which include more than 70 bedrooms. Locate children's play apparatuses and benches within the open space along with other urban design features such as gardens, fountains, arbours and art. Locate the outdoor amenity area to take advantage of sun and natural shelter from the elements of the weather. Ensure that surveillance of the area is provided from adjacent units and the area has barrier free access. Provide a mixture of hard-surfaced and natural landscaping in the outdoor amenity area.

2. Internal Walkways

- 8.2.1
- a) Provide internal walkways extending from Blundell Road through each development area to the internal east-west road systems. Define the entrances to the internal north-south walkways by providing arrival plazas. The arrival plazas should consist of the following elements: widened hard surfaces with distinctive paving patterns, arbours, and accent planting.

8-2-1 b) Use walkways, rather than fences, to separate the development areas. Provide low level landscaping and lighting on both sides of the walkways.

3. Trees

8-2-4 Incorporate mature trees and landscaping into the development area. Whenever a major tree is removed, the replacement ratio should be 2:1. The replacement trees should have a minimum calliper of 15 cm (5.9 in.) in diameter.

4. Garbage and Recycling

8-2-5 Locate enclosed garbage container buildings away from pedestrian accesses and children's play areas. The area should be easily accessible for collection purposes and should provide adequate maneuvering space for 13.7 m (45 ft.) long garbage trucks. A covered recycling area 2.4 m (7.9 ft.) by 3.5 m (11.5 ft.) should also be provided near garbage areas. It should be wheel-chair accessible.

5. Acoustics

8-2-1 Traffic noise should be screened from residential units in order to maintain acceptable ambient indoor sound levels. All Development Permit Applications shall require evidence in the form of a report and recommendations prepared by a person trained in acoustics. This report shall demonstrate that noise levels in A-weighted 24-hour equivalent sound level and will be defined simply as noise level in decibels.

Dwelling Unit Room/Area

Noise Level (Decibels)

Bedrooms	35
Living, Dining and Recreation	40
Kitchen, Bathroom and Hallways	45

6. Vehicular Circulation System and Parking

8.2.1

- a) Minimize the number of vehicular access points from Blundell Road as shown on Attachment 2.

8.2.4

- b) Enhance the main shared vehicular/pedestrian entry into each of the development areas by planting large specimen trees and accent planting without obstructing sightlines. Trees should frame the main entry driveways as seen from Blundell Road to each of the development area's internal road system.

8.2.1

- c) Consider the use of traffic circles or landscaped medians to visually and physically direct people to the separate projects included in each development area.

8.2.4

- d) Plant large feature trees with a minimum calliper of 150 mm (6 in.) measured 1.4 m (4.7 ft.) above grade along with appropriate low level accent planting in the middle of traffic circles or medians.

8.2.1

- e) No gates are permitted at the main vehicular accesses to the internal roadways or between projects within each development area.



- f) Use "gentle" tangent curves along internal road systems to create more interesting roadways.

8.2.4

- g) Create visual focal points by providing landscaped areas at both the curves of the internal roads, and at the "dead-ends" of each of the development sites east-west internal road. Allow for future pedestrian access to adjacent development areas within the landscaped areas at the east-west road ends.

h) In order to enhance the pedestrian experience and minimize the presence of garage doors along the internal roadways:

8.2.3

i) Incorporate front door entry features that extend .61 m (2 ft.) beyond the garage doors;

ii) Minimize the width of garage doors to a maximum of 4.87 m (16 ft.) x 2.13 m (7 ft.);

8.2.4

iii) Separate the individual garages with landscaped areas including trees with a minimum calliper of 100 mm (4 in.) measured 1.4 m (4.7 ft.) above grade and/or with the careful siting of the residential portions of the buildings;

iv) Plant street trees spaced no more than 9 m (29.52 ft.) apart within landscaped boulevards along the internal roadways;

8.2.1

v) Stagger units along main east-west internal roads;

8.2.4

vi) Use changes in paving materials such as decorative pavers or cobblestones near entrances to walkways and children's play areas;

vii) Soften hard surfaces through the use of landscaping, trellises and tree planting;

viii) Install pedestrian-oriented lighting along the internal roadways of the developments;

8.2.3

ix) Incorporate human scale elements (windows, doors, roof elements, trellis, etc.) into the building facade visible from the street;

x) Incorporate decorative architectural treatments, such as windows, on and above garage doors that are complementary to unit finishes;

8.2.3

xi) Ensure that the garage door encompasses no more than 60% of the building width as visible from the internal road systems;

xii) Where units have both the front and back facade fronting a public road or pedestrian access route, ensure that:

- no more than two garage doors are placed in a row without interrupting with a portion of the building or side yard equal to the width of one garage door; and
- no building block has more than six garage parking spaces;

xiii) Where units have only one facade fronting an internal road system road or pedestrian access route, ensure that:

- for 50% of the units, no more than one garage door is placed in a row where units are attached without interrupting with an amount of building or side yard equal to the width of one garage door; and
- no building block has more than four garage parking spaces.

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Original Adoption: March 15, 1999

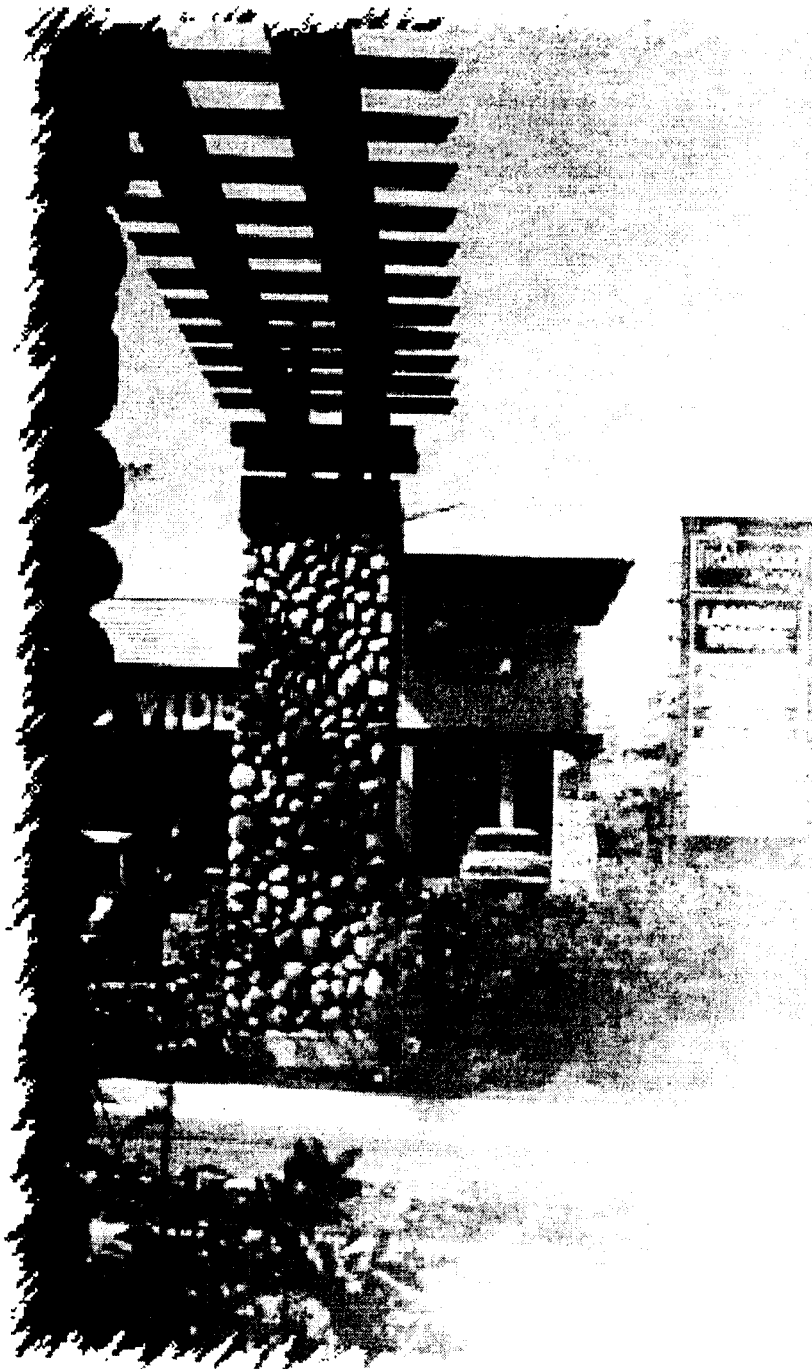
Richmond Official Community Plan



City of Richmond

SHELLMONT-IRONWOOD SUB-AREA PLAN

Bylaw 7100 Schedule 2.8A



KEY MAP

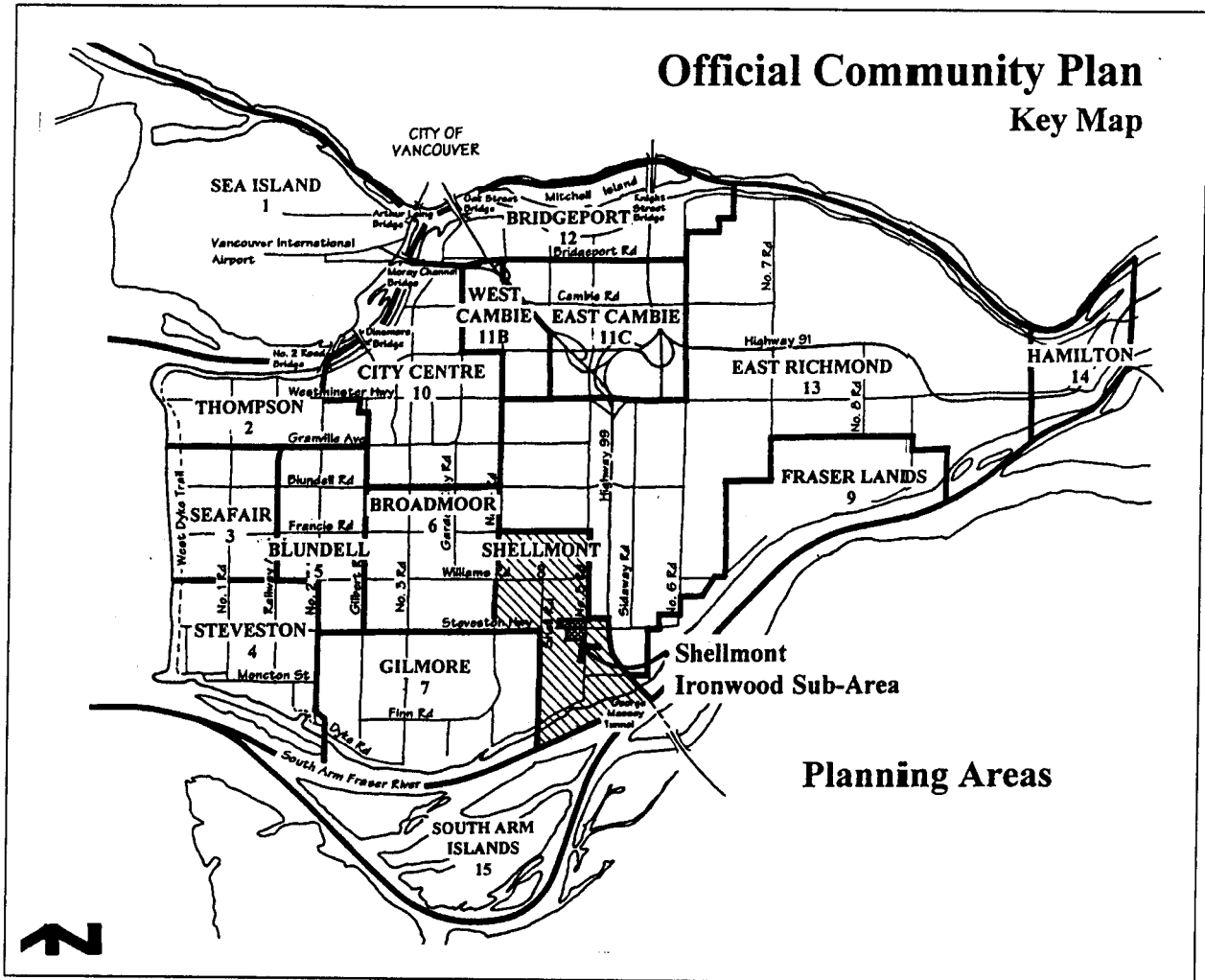


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updated

PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal document for planning and managing the City's social, economic and physical future. It sets out a vision, goals and objectives that reflect overall community values that have been determined through a public consultation process.

Attached to the OCP are "Area-Plans" and "Sub-Area Plans" for specific locations within Richmond. Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Map). Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP, Area Plans and Sub-Area Plans typically contain policies guiding land use and may contain Development Permit Guidelines directing form and character of development. The OCP addresses broad community issues that affect the city as a whole while the Area Plans and Sub-Area Plans are used to address local neighbourhood issues.

The OCP forms Schedule 1 to Bylaw 7100. Area Plans and Sub-Area Plans form Schedule 2 to Bylaw 7100.

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Map shall take precedence.

What is the Purpose of this document?

This document sets out Development Permit Guidelines for the Ironwood Sub-Area (see Development Permit Area Map), a part of the Shellmont Planning Area. In this case there are no Sub-Area Plan policies for Ironwood, leaving the Development Permit Guidelines to stand on their own.

It is important to note that guidelines do not direct land use, they only suggest appropriate form and character. To check the land use designation for a particular piece of land, refer to the Generalized and Specific Land Use Maps attached to Schedule 1 of Bylaw 7100.

Environmentally Sensitive Areas

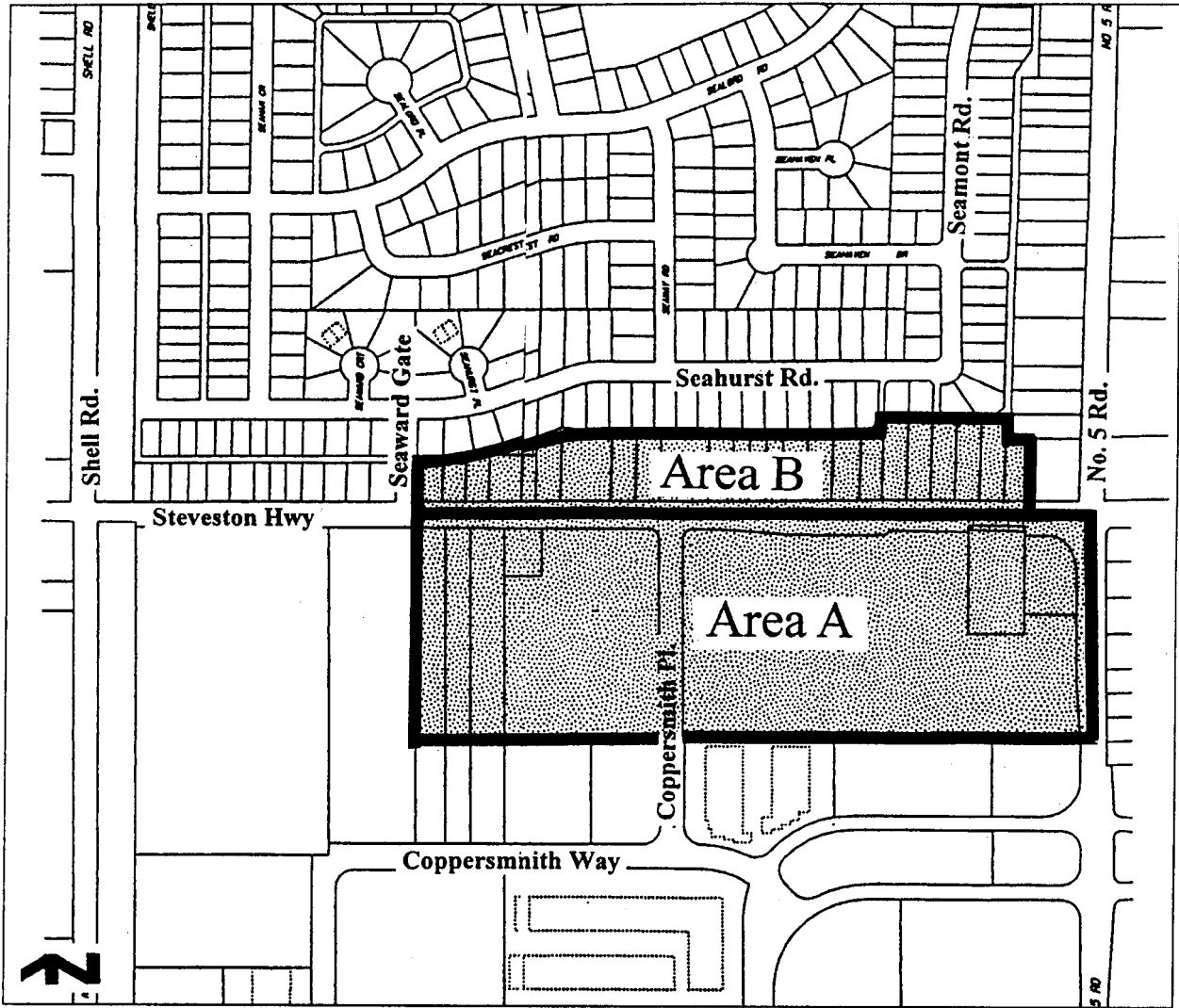
Additional Conservation Area and Environmentally Sensitive Area (ESA) policies, guidelines, and locations are included in Schedule 1 of Bylaw 7100 and its attachments. Readers should check Schedule 1 as it takes precedence over this plan in the case of Conservation Areas and ESA's.

Changes to this Document

This Plan may be amended from time to time. Check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

now on inside front cover

Development Permit Area Map



1.0 DEVELOPMENT PERMIT GUIDELINES

1.1 APPLICATION AND INTENT

1.1.1 DEVELOPMENT PERMIT AREAS

The Ironwood Sub-Area contains two “character areas” situated along the north and south side of Steveston Highway to the east of No. 5 Road (Refer to the Development Permit Area Map). This section contains Development Permit Guidelines which apply to these character areas. The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Ironwood Sub-Area.

*re-organised
into new
format*

1.1.2 APPLICATION AND EXEMPTION

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in Schedule 1 of this Bylaw. Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

1.1.3 OBJECTIVES AND JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to commercial development on the south side of Steveston Highway and multiple-family development along the north side of the Steveston Highway due to the following factors:

- a) The intersection of Steveston Highway and No. 5 Road is an important gateway into Richmond. New commercial and residential development should provide an attractive, welcoming entrance appropriate to this role as a key entrance to Richmond;
- b) Due to the traffic volumes there is a need to control access to and ensure safe and convenient pedestrian circulation along Steveston Highway; and

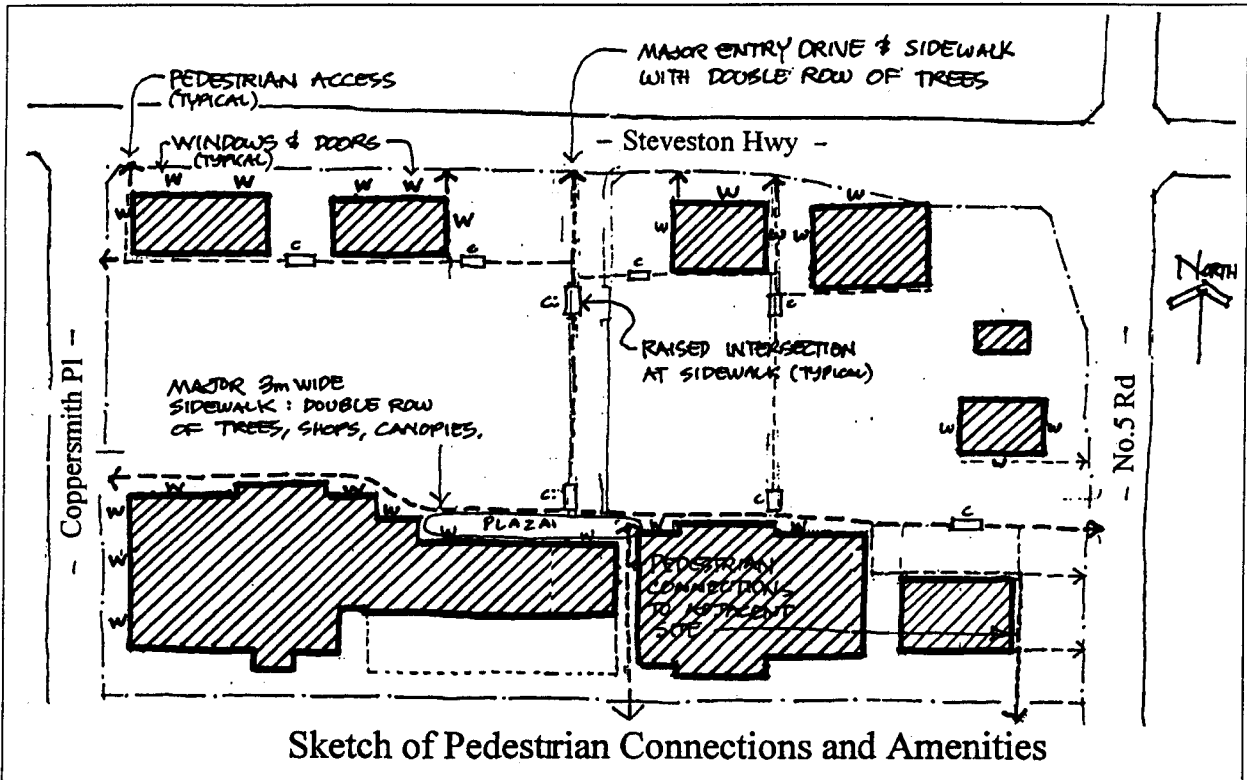
- c) Whenever different land uses adjoin, there is a need to ensure that new development blends in with the character and scale of existing developments and a need to buffer or mitigate potential impacts. Traffic, noise, lighting and other environmental conditions must be taken into consideration in the design of the commercial development. The multi-family development should be designed to provide a boundary between the quiet, low density residential uses on the north side of Steveston Highway and the increasingly active, commercial and business park uses on the south side of Steveston Highway.

1.2 AREA A – COMMERCIAL DEVELOPMENT ALONG THE SOUTH SIDE OF STEVESTON HIGHWAY

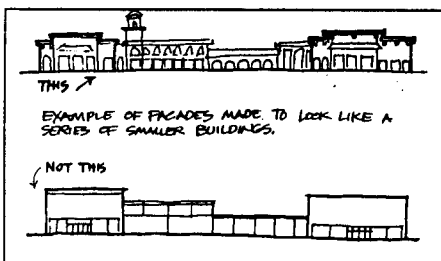
The following guidelines apply to commercial development as shown in Area A on the Development Permit Area Map in addition to the General and Commercial Guidelines for all Development Permits located in Schedule 1 to this Bylaw.

1.2.1 SETTLEMENT PATTERNS

- a) Pedestrian access to the main buildings on each site on both sides of Coppersmith Place should be provided as follows:
 - Minimum 1.5 m (4.9 ft.) wide sidewalks;
 - Located so as to provide a minimum of two accesses from Steveston Highway, one from No. 5 Road, two from Coppersmith Place, and two from Coppersmith Way;
 - Where walkways intersect vehicle paths, the roadways should be raised to the walkway level, and should have a non-asphalt paving material;
 - At least one walkway connecting to the Steveston Highway sidewalk should be fully covered and lighted; and
 - All walkways should be accessible to disabled persons.
- b) Vehicle connections to the streets on each site on both sides of Coppersmith Place should be as follows:
 - To Steveston Highway: one right-in-right-out;
 - To Coppersmith Place: one full movements (at south end) and one loading bay access;
 - To No. 5 Road: one right-in-right-out, one full movements. One to be shared with adjacent property on the south;
 - To Coppersmith Way: one full movement; and
 - Width to be minimized, to promote pedestrian safety.
- c) Pedestrian/vehicle connections to the adjacent properties are to be encouraged. There should be at least one pedestrian connection through the main block of buildings (see Pedestrian Connection sketch).
- d) The design should create defensible spaces, which provide for easy surveillance and safety both day and night.



Pedestrian Connection Sketch



Building Façades

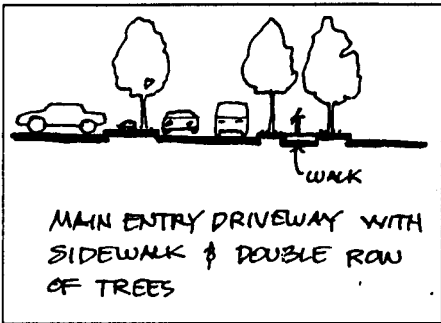
1.2.2 ARCHITECTURAL ELEMENTS

- a) Buildings should be designed so as to break down large façades into smaller elements to create an appearance of a series of smaller buildings (see Building Façades sketch).
- b) Shopfronts should have a minimum of 40% glazing, and all display windows and entrances should be protected from the weather by minimum 2 m (6.6 ft.) deep colonnades or canopies. The north side second floor should have an open corridor or colonnade across its full frontage.

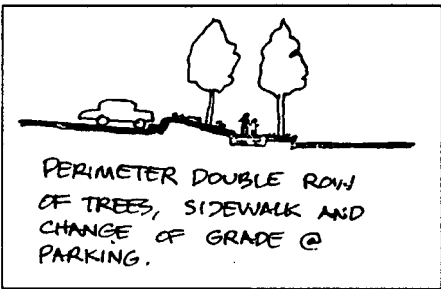
- c) Buildings which front onto public streets should have display windows or glass doors for a minimum of 60% of the building edge. These areas should be paved for a minimum of 2 m (6.6 ft.) in front of the windows or doors.
- d) Pedestrian access areas (sidewalks) between parking lots and store entrances should be a minimum of 3 m (9.8 ft.) wide. Pedestrian amenities should be provided at regular intervals.
- e) On-site employee or public amenities should include change rooms, showers, lockers, a lounge, and a covered outdoor seating area.
- f) Signs should be integrated with the architecture, and limited to the following (in addition to the Sign Bylaw requirements):
 - Façade signs comprised of letters and logos affixed to the building, or in internally illuminated boxes - the latter to be limited to sign bands immediately above main floor windows/doors or copy on awnings; and
 - Freestanding signs limited to directional signs and signs indicating the name and general nature of the services, to a maximum of 2 m (6.6 ft.) in height.

1.2.3 LANDSCAPE ELEMENTS

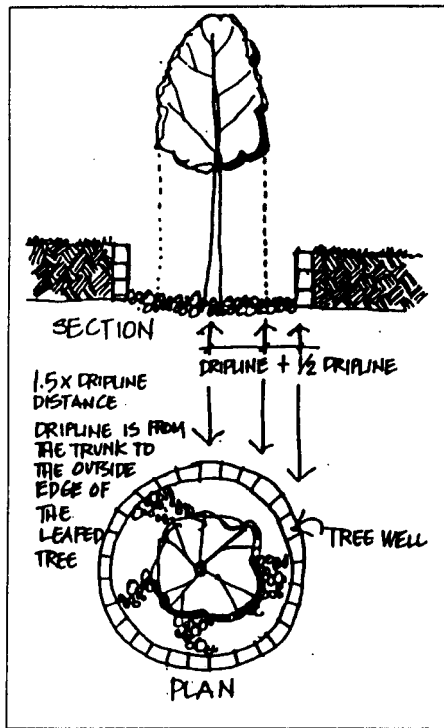
- a) Edge conditions should create a high-quality pedestrian environment, by provision of boulevards, street trees, pedestrian amenities, lighting, and landscaping. Pedestrian/vehicle conflicts should be minimized.
- b) Landscaping should be used to create a predominant green aspect of the site and also to soften the presence of large numbers of vehicles, both in the parking lot and on the surrounding streets, by:
 - Planting a double row of trees around the perimeter of the site and on the two main entry driveways, to form a canopy over the sidewalks and driveways (see Tree Planting sketch);
 - Planting "groves" of trees and shrubs in the parking lot so that, approximately 10 years after planting, at least 50% of the parking lot will be covered by a canopy of leaves in summer; and
 - Creating a change of grade at the edge of the parking lot and planting low shrubs so that, without compromising visibility and surveillance, parked cars are screened from the road (see Grade Change sketch).



Tree Planting



Grade Change



Tree Well

- c) Existing trees should be preserved by:
 - Retaining in situ complete with tree wells as necessary (see Tree Well sketch), or relocating on the site; and
 - Erecting construction fencing to City specs around all of the above trees prior to site pre-loading and to remain fenced throughout the construction period.
- d) Efforts should be made to retain, move offsite, or relocate other existing trees and shrubs. Provincial laws regarding cutting of trees containing songbird or raptor nests during the nesting season should be respected.
- e) The south edge of the property should be well landscaped, but should be carefully integrated with the adjoining properties. Fences or hedges on this perimeter should not exceed 1 m (3.3 ft.) in height.
- f) Landscape themes should include the following suggested mix of native and exotic plants, to maintain a consistent level of quality:
 - Perimeter and parking lot tree types: Honey Locust, Chanticleer (Bradford) Pear, Sweet Gum, Katsura. Shrub types: Blueberry, Currant, Red Osier Dogwood, Oregon Grape, Native (Swamp) Rose, Rhododendron; and
 - Feature trees and plants: Windmill Palm, Persian Ironwood, Monkey Puzzle, Sitka Spruce, Pacific Crabapple, Yucca, Bamboo, native and exotic tall grasses.
- g) Preserve natural heritage by retaining, relocating and augmenting existing healthy on-site trees and shrubs. Each existing tree larger than 20.3 cm (8 in.) dbh which is unavoidably cut should be replaced by two suitable trees of minimum 10.2 cm (4 in.) dbh. Wherever possible, plant new landscaping which will be beneficial to native and migratory birds.
- h) At least one lighted shelter should be provided for people waiting for busses.
- i) There should be at least one pedestrian plaza of a minimum size of 550 m² (5,920 ft²), with a minimum of 50 linear metres (164 linear feet) of seating. The plaza should also include a drinking fountain. The plaza should be adjacent to a building containing shop windows and canopies.

1.2.4 PARKING, GARBAGE, RECYCLING AND RELATED ELEMENTS

- a) Parking lot lighting should be directed away from residential areas so that there is a maximum of three footcandles at the north property line. Certain light standards should be designed to accommodate hanging flower baskets complete with irrigation.
- b) Bicycle parking should be provided as per the following minimum standards:
 - Class 1, long-term secured parking: 0.27 spaces per each 100 m² (1,076.43 ft²) of gross leasable area; and
 - Class 2, short term parking: 0.27 spaces per each 100 m² (1,076.43 ft²) of gross leasable area.

For details, refer to Appendix 1 – Bicycle Parking and End of Trip Facilities.
- c) Signs and parking lot lighting should be compatible with the adjacent residential area.
- d) Garbage, recycling and pick-up should be situated in areas which do not conflict with pedestrian traffic, and should either be fully enclosed or screened with solid walls higher than the bins.

1.3 AREA B – NORTH SIDE OF STEVESTON HIGHWAY

The following guidelines apply to Area B as shown on the Development Permit Area Map. It is the objective of these guidelines to help define the form and character of new townhouse development along the north side of Steveston Highway to ensure it is both appropriate to the area's "Richmond gateway" location and proximity to Ironwood Shopping Centre, while also addressing livability issues related to vehicular access and traffic impacts.

1.3.1 SETTLEMENT PATTERNS



Centre boulevard across from Ironwood

To establish a pattern of dual fronting townhouse clusters (with sunny yards) linked by a pedestrian-friendly lane system and screened by a "formal front" oriented to Steveston Highway, new development should:

- a) Place emphasis on the establishment of a green, treed and landscaped streetscape along Steveston Highway punctuated by pedestrian entranceways to individual townhouse clusters;
- b) Typically design townhouse clusters as a combination of rows of units aligned north-south (such that the fronts and rears of individual units are oriented east-west and unit sidewalls front onto Steveston Highway);
- c) Accommodate all vehicular access via a public lane system situated along the north edge of Area B; and
- d) Setback:
 - Along Steveston Highway - 6 m (19.69 ft.), EXCEPT that where a berm is not provided (as described under section 1.3.3 Landscape Elements) or rows of units are aligned east-west, rather than north-south, the minimum setback shall be 12 m (39.37 ft.); and
 - Along the lane - 5.4 m (17.72 ft.), provided that porches and similar building features may project up to 1 m (3.28 ft.) into the setback.

1.3.2 ARCHITECTURAL ELEMENTS

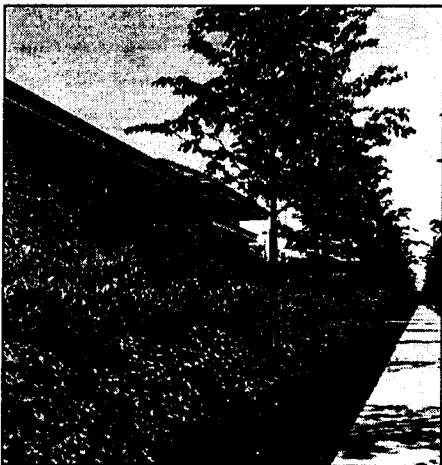
To address noise-related traffic impacts and establish a pedestrian-friendly streetscape, new development should:

- a) Be designed to maintain an acceptable ambient noise level of 35 dB for indoor spaces and 55 dB for outdoor private spaces;
- b) Create a residential streetscape along the lane that reinforces its use and image as a special local, pedestrian-friendly street (rather than a service access) through features such as porches, front doors to individual townhouse units, bay windows, living/dining room windows, etc.; and
- c) Orient garages so as to be accessed via private, on-site driveways rather than directly from the public lane.

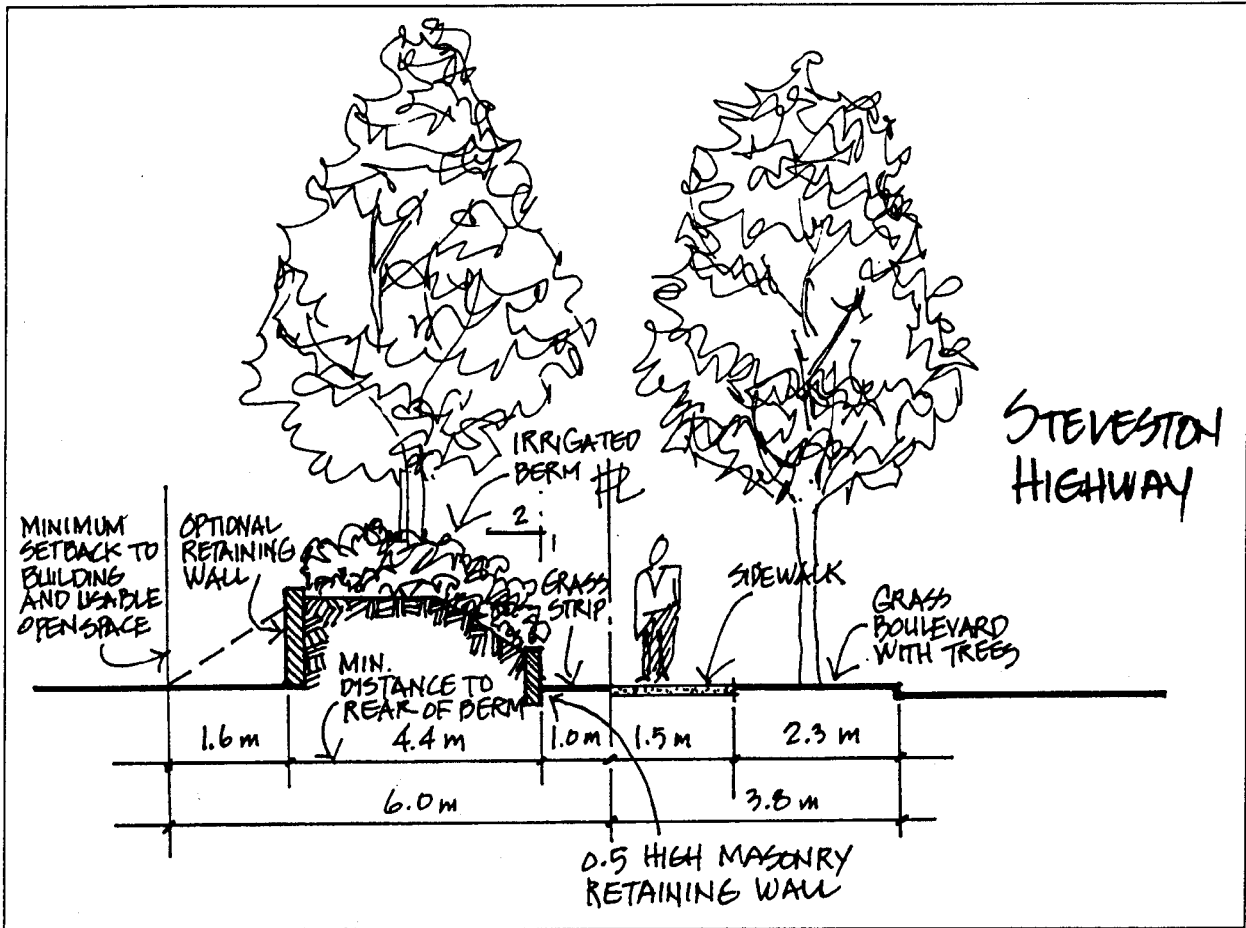
1.3.3 LANDSCAPE ELEMENTS

To enhance the appearance of Steveston Highway as a prominent vehicular artery, screen sensitive residential uses, and create a special, pedestrian-friendly lane environment, new development should:

- a) Along Steveston Highway, contribute a lush, green and pedestrian oriented landscape by accommodating:
 - Installation of a 2.3 m (7.55 ft.) wide grass boulevard (complete with a single row of Pin Oaks) at the back of curb and a 1.5 m (4.92 ft.) wide concrete sidewalk;
 - Within the minimum 6 m (19.69 ft.) building setback, a 1 m (3.28 ft.) wide grass strip at the back of sidewalk and a continuous landscaped berm at least 1.2 m (3.94 ft.) high (measured from the adjacent curb) and 4.4 m (14.44 ft.) deep (measured from the grass strip), EXCEPT at pedestrian entrances to townhouse clusters (and as required to maintain existing mature trees) (See diagram: *Steveston Highway Frontage*). Any fencing incorporated as part of the berm should be located at a minimum of 4.4 m (14.43 ft.) from the south property line and not higher than 1.5 m (4.92 ft.) (measured from the curb). Where a fence is adjacent to private outdoor space it may be as tall as 1.8 m (5.90 ft.);

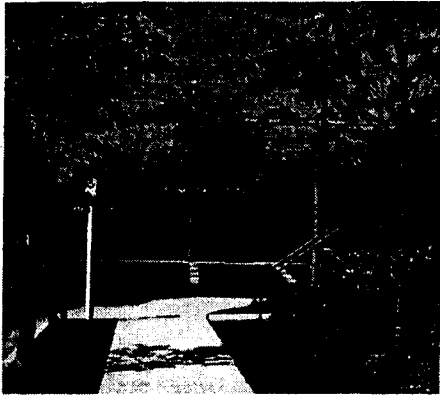


Berm on south side of Steveston Highway

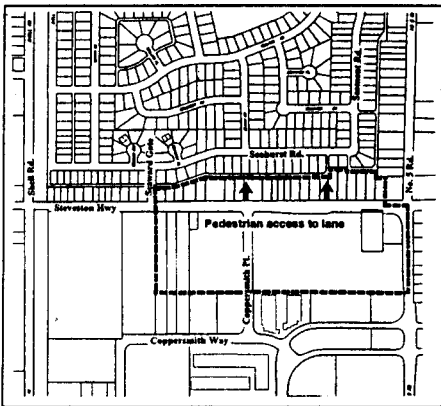


Steveston Highway Frontage

- Significant planting within the berm area, including large growing trees and plant material chosen for its seasonal colour, screening abilities, and visual interest;
- Special gateway features, incorporating address information and mail boxes, at the entrances to townhouse clusters. Breaks in the berm for entrances to individual developments should be minimized and care should be taken to ensure that they are at a pedestrian scale by lowering the height of adjacent fencing to 1.2 m (3.94 ft.); and



Crosswalk at Coppersmith Way

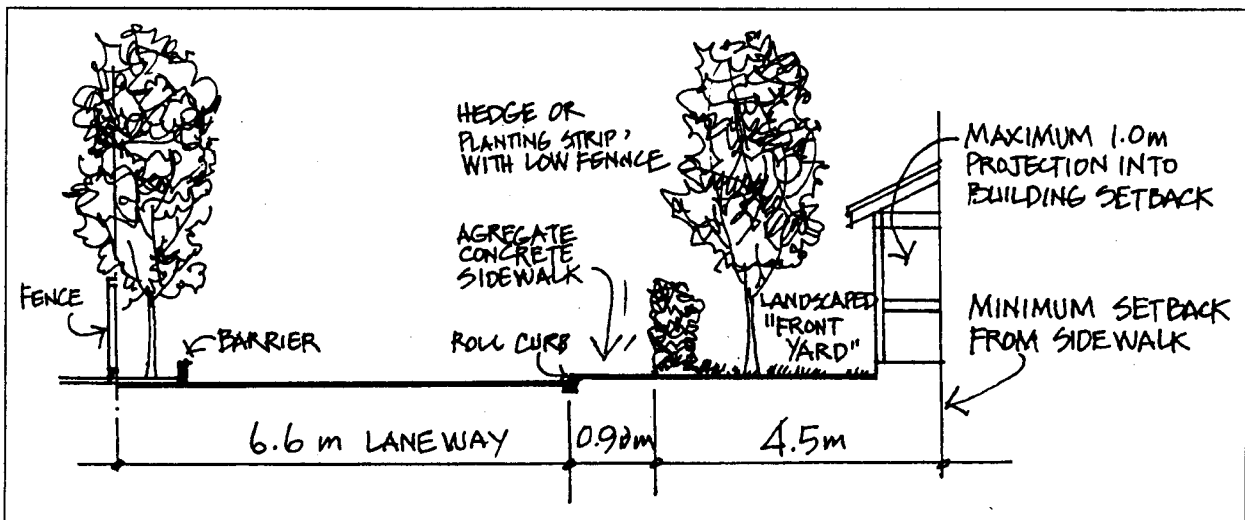


Pedestrian Connections

- Pedestrian pathways linking the single-family neighbourhood to the north of Area B to the Ironwood shopping centre. Pathways should be located at Coppersmith Way and where the alignment of the lane turns north to Seahurst Road (see Pedestrian Pathways diagram). The pathways require a minimum of 2.4 m (7.87 ft.) of paved surface to accommodate pedestrian and bicycles with a minimum of 0.8 m (2.6 ft.) landscaping on either side for a total width of 4 m (13.12 ft.).

b) For the public lane provide:

- A 6.6 m (21.65 ft.) wide laneway with a 0.6 m (1.97 ft.) wide raised setback along the northern edge to be planted with a single row of trees at 9 m (29.53 ft.) on centre (see Lane Frontage sketch);
- A 0.9 m (2.95 ft.) wide concrete aggregate sidewalk with roll curb on the southern edge of the laneway with a hedge or vertical planting next to the sidewalk;
- High visibility between the lane and the adjacent properties by ensuring that fencing along the lane is no higher than 1.2 m (3.94 ft.). Additionally, any fencing should be located no closer than 1.5 m (4.92 ft.) from the northern property line; and
- Clear addressing, street lighting and upgrades to the storm sewer line.



Lane Frontage

**BICYCLE PARKING AND
END OF TRIP FACILITIES** **APPENDIX 1**

New development should accommodate the bicycle parking and end-of-trip facility needs of multiple-family residential dwellers, workers, and visitors.

a) CLASS 1 Parking

Secured, long-term bicycle parking shall be provided for the use of residential use and non-residential tenants in the form of waterproof bicycle lockers, or bicycle rooms complete with bicycle racks.

- (i) Parking facilities shall: be at-grade; have uniform 160 lux (min.) lighting which yields true colours; and, be within sight of building entry, elevator, and/or security.
- (ii) Bicycle rooms shall provide: lockable door(s) with window(s); tamper-proof, motion-activated security lighting; and unobstructed view of each room from its entry; and, facilities for no more than 20 bicycles per room (enabling owners to identify one another).
- (iii) Bicycle lockers shall: be constructed of solid, opaque, weather-proof and theft-resistant material, with no exposed fittings or connectors; have lockable doors which open to the full height and width of each locker; be grouped together; not be located at the head of parking spaces; and, have clear minimum dimensions of:

Length	1.80 m (5.91 ft.)
End Width at Door	0.60 m (1.97 ft.)
End Width Opposite Door	0.22 m (0.72 ft.)
Height	1.20 m (3.94 ft.)

b) CLASS 2 Parking

Unsecured, short-term bicycle parking shall be provided for visitors in the form of bicycle racks located within 15 m (49.2 ft.) of a principal building entry.

- (i) Parking shall be situated in well-lit locations, clearly visible from principal building entries and/or public roads.

- (ii) Bicycle racks shall be made of sturdy, theft-resistant material, securely anchored to the floor or ground.
- (iii) Bicycle racks shall be designed to support the bicycle frame, not the wheels, and allow both the frame and the front wheel to be locked to the rack with a U-style lock.

CITY OF RICHMOND
BYLAW 7190
RICHMOND OFFICIAL COMMUNITY PLAN BYLAW 7100
AMENDMENT BYLAW 7190

The Council of the City of Richmond, enacts as follows:

1. Richmond Official Community Plan Bylaw 7100 is amended by:

a) Deleting:

- Schedule 2 to Bylaw 7100 Table of Contents
- Schedule 2.2A (Thompson (Dover Crossing Sub-Area));
- Schedule 2.2B (Thompson (Terra Nova Sub-Area));
- Schedule 2.5A (Blundell (Laurelwood Sub-Area));
- Schedule 2.5B (Blundell (East Livingstone Sub-Area));
- Schedule 2.6A (Broadmoor (Ash Street Sub-Area));
- Schedule 2.6B (Broadmoor (Central West Broadmoor Sub-Area));
- Schedule 2.6C (Broadmoor (Sunnymede North Development Permit Guidelines)); and
- Schedule 2.8A (Shellmont (Ironwood Development Permit Guidelines));

and

b) Adding a new:

- Schedule 2 to Bylaw 7100 Table of Contents
- Schedule 2.2A (Thompson (Dover Crossing Sub-Area));
- Schedule 2.2B (Thompson (Terra Nova Sub-Area));
- Schedule 2.5A (Blundell (Laurelwood Sub-Area));
- Schedule 2.5B (Blundell (East Livingstone Sub-Area));
- Schedule 2.6A (Broadmoor (Ash Street Sub-Area));
- Schedule 2.6B (Broadmoor (Central West Broadmoor Sub-Area));
- Schedule 2.6C (Broadmoor (Sunnymede North Sub-Area)); and
- Schedule 2.8A (Shellmont (Ironwood Sub-Area));

which are attached as Schedule A to this Bylaw.

2. This Bylaw may be cited as **“Richmond Official Community Plan Bylaw 7100, Amendment Bylaw 7190”**.

FIRST READING

PUBLIC HEARING

SECOND READING

THIRD READING

ADOPTED

	CITY OF RICHMOND
	APPROVED for content by originating dept.
	APPROVED for legality by Solicitor

MAYOR

CITY CLERK

AMENDMENT LIST BYLAW 7100
OFFICIAL COMMUNITY PLAN
 (Not a legal part of bylaw)
 Effective May 17, 1999

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
7100	N/A	OCP Main	New OCP.	Mar. 15/99	
7104	N/A	OCP Main	Amends Bylaw 7100 1. Adds policy "b)" under Policies Land Supply in Objective 1 in Sec. 2.3 INDUSTRY. 2. Reletters policy "b)" under Policies: Utilities and Transportation in Objective 1 in Sec. 2.3 INDUSTRY to policy "c)". 3. Adds policy "f)" under Policies in Objective 3 in Sec. 2.4 COMMERCIAL.	Apr. 17/99	
7115	2.10C	City Centre (McLennan North)	Amends Bylaw 7100 1. Amends Att. 1 to Sch. 2.10C. 2. Text Amendments to: <ul style="list-style-type: none"> • Policy 2.1 of Section 3.2; • Section 3.3; • Section 4.3.B (Items A, B, C, D and H); • Section 4.3.C (Item A); • Section 4.4.A (Items A and H). 	June 19/00	
7118	2.4	Steveston	Amends Bylaw 7100 (Amends Att. 1 to Sch. 2.4 by designating 11160, 11180, 11188 Railway Avenue and 5193, 5195 Hummingbird Drive as Multiple Family.)	June 12/00	

* Changes reflected on Urban Development's consolidated version of Attachment 1 (Generalized Land Use Map), Schedule 1 to Bylaw 7100 only.

* Detailed Character Design Guidelines (not included in your consolidation - see front counter).

PREVIOUS AMENDMENT LIST BYLAW 5400
OFFICIAL COMMUNITY PLAN
Effective November 14, 1983 to March 15, 1999

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
4211	4A	Steveston (Moncton)	Original OCP	14/11/83	22/12/86
4277	11A	Cambie (North Lansdowne)	Original OCP	09/10/84	22/12/86
4278	4	Steveston	Original OCP	22/04/85	22/12/86
4462	14	Hamilton	Original OCP	20/06/85	22/12/86
4469	11A	Cambie (North Lansdowne)	Amends Bylaw 4277 (Alexandra lots.)	25/11/85	03/04/89
4537	11A	Cambie (North Lansdowne)	Amends Bylaw 4277 (Road relocation.)	27/01/86	03/04/89
4559	4A	Steveston (Moncton)	Amends Bylaw 4211 (Map/Schedule)	10/03/86	03/04/89
4561	9A	McLennan (Ash Street)	Original OCP	10/03/86	22/12/86
4581	10	Town Centre	Original OCP	09/06/86	22/12/86
4679	4	Steveston	Amends Bylaw 4278 (Map/lots on No. 1 Rd.)	24/11/86	03/04/89
4695	10	Town Centre	Amends Bylaw 4581 (Extends C1 north.)	22/12/86	03/04/89
4700		RICHMOND AND ALL AREAS	Original OCP & areas	22/12/86	03/04/89
4719	4	Steveston	Amends Bylaw 4700 (Adds mixed comm/res uses to Steveston Community.)	09/02/87	03/04/89
4730	10	Town Centre	Amends Bylaw 4700 (Amends density and use Table from Plan bylaw.)	09/02/87	03/04/89
4777	9	McLennan	Amends Bylaw 4700 (Adds Schedule 2.9.)	25/05/87	03/04/89
4809		RICHMOND OCP and areas	Amends Bylaw 4700 (Adds Dev't Permit Areas and hskping changes.)	22/06/87	03/04/89
4810	10A	Town Centre (St. Alban's)	Amends Bylaw 4700 (Adds Schedule 2.10A.)	08/06/87	03/04/89
4814	4A	Steveston (Moncton)	Amends Bylaw 4700 (Deletes Health Care Facility amends phasing Table 1.)	14/09/87	03/04/89
4835	9A	McLennan (Ash Street)	Amends Bylaw 4700 (Designates 9311, 31, 51 Francis Rd. as 'Townhouse or small lots'.)	13/07/87	03/04/89
4886	4	Steveston	Amends Bylaw 4700 (Designates 11340 & 11360 No. 1 Rd. as Multiple Family.)	26/10/87	03/04/89
4894	11C	Cambie (East)	Amends Bylaw 4700 (Adds Schedule 2.11C.)	11/07/88	03/04/89
4907	3	Seafair	Amends Bylaw 4700 (Designates westerly portion of 4640 Cabot Dr. as Residential.)	26/10/87	03/04/89
4918	12	Bridgeport	Amends Bylaw 4700 (Designates portion of 2111 No. 4 Rd. as Residential.)	26/10/87	03/04/89
4920	11B	Cambie West	Amends Bylaw 4700 (Adds Schedule 2.11B. Also various revisions to Schedule 1 of Bylaw 4700.)	12/09/88	03/04/89
4950	12	Bridgeport	Amends Bylaw 4700 (Designates Beckwith to Bridgeport, Gage Rd. east as Nonresidential.)	04/01/88	03/04/89
4955	11	Cambie	Amends Bylaw 4700 (Designates portion of 3560 Garden City Rd. as Nonresidential.)	08/02/88	03/04/89

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
4973	4	Steveston (Moncton)	Amends Bylaw 4700 (Designates the extension of Area B of the Moncton Developments Plan as 'Residential' and 'Public Open Space'.)	25/01/88	03/04/89
4979	11A	Cambie (North Lansdowne)	Amends Bylaw 4700 (Designates properties on south side of Alexandra Rd. as Commercial.)	22/02/88	03/04/89
4992	4	Steveston	Amends Bylaw 4700 (Designates properties on north-east corner of Chatham St. and Seventh Ave. as 'Single Family'.)	25/01/88	03/04/89
5002	N/A	Various	Amends Bylaw 4700 (Amends Dev't Permit Guidelines in Area Plans & OCP.)	14/03/88	03/04/89
5006	10	Town Centre	Amends Bylaw 4700 (Designates 7220 Westminster Hwy. as Commercial and Nonresidential.)	22/02/88	03/04/89
5019	10	Town Centre	Amends Bylaw 4700 (Designates 7720 and 7740 Granville Ave. and 7031 Minoru Blvd. as 'Residential' and 7680, 7700, 7720 and 7740 Granville Ave. and 7031 Minoru Blvd. as 'Multiple-family High-rise'.)	28/03/88	03/04/89
5029	9A	McLennan (Ash Street)	Amends Bylaw 4700 (Enlarges parcel 4 and amends Table 1.)	22/02/88	03/04/89
5030	4	Steveston	Amends Bylaw 4700 (Designates properties on north-east corner of Chatham St. and Seventh Ave. as 'Single Family'.)	28/03/88	03/04/89
5032	4	Steveston	Amends Bylaw 4700 (Designates 4060 Steveston Hwy. as 'Multiple Family'.)	28/03/88	03/04/89
5035	12	Bridgeport	Amends Bylaw 4700 {Designates 9351 Bridgeport Rd. and 2760 Gage Rd. as 'Nonresidential'.}	09/05/88	03/04/89
5055	10	Town Centre	Amends Bylaw 4700 (Designates areas along the east side of No. 3 Rd. north of Alderbridge Way as Commercial and Commercial Service Districts as shown in Attachment 2.)	25/07/88	03/04/89
5076	10	Town Centre	Amends Bylaw 4700 (Designates 7300 & 7340 Granville Ave., 7031 & 7051 Moffatt Rd. to "Health Care" in Town Centre Area Plan and "P,I, & Open Space" in overall Schedule 1.)	29/06/88	03/04/89
5085	2.2A	Thompson (Dover Road)	Amends Bylaw 4700 (Adds Schedule 2.2A.)	26/09/88	03/04/89
5086	N/A	Various	Amends Bylaw 4700 (Changes Study Area designation in McLennan, amend ALR boundary and notes.)	25/04/88	03/04/89
5088	11A	Cambie (North Lansdowne)	Amends Bylaw 4700 (Designates 8400 Browndale as Nonresidential.)	25/04/88	03/04/89
5090	11C	Cambie (East)	Amends Bylaw 4700 (Designates part of 10340, 10380, 10400 Bridgeport Rd. as Nonresidential.)	24/05/88	03/04/89

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
5094	7	Gilmore	Amends Bylaw 4700 (Designates South of Steveston Hwy. Area as Nonresidential Reserve.)	25/04/88	03/04/89
5097	11A	Cambie (North Lansdowne)	Amends Bylaw 4700 (Designates north side of Alexandra Rd., between Hazelbridge and Garden City as Commercial.)	24/05/88	03/04/89
5098	4	Steveston	Amends Bylaw 4700 (Designates 11700, 11720, 11740, 11780 and 11800 No. 1 Rd. and 4020 and 4060 Garry St. as multiple-family.)	24/05/88	03/04/89
5110	2.2B	Thompson (Terra Nova)	Amends Bylaw 4700 (Adds Schedule 2.2B.)	26/09/88	02/12/88
5113	1	Sea Island	Amends Bylaw 4700 (Designates an area south of Lysander Lane as Airport.)	29/06/88	03/04/89
5127	14	Hamilton	Amends Bylaw 4700 (Amends Development Permit Guidelines in Area Plan and OCP.)	08/08/88	03/04/89
5144	11C	Cambie East	Amends Bylaw 4700 (Designates s.w. corner of Cambie Rd. and Shell Rd. Non-Residential.)	26/09/88	03/04/89
5154	11C	Cambie East	Amends Bylaw 4700 (Adds flood plain exemption note.)	12/09/88	03/04/89
5166	4	Steveston	Amends Bylaw 4700 (Designates 11120, 11140 and 11160 No. 1 Rd. as Multiple Family.)	22/08/88	03/04/89
5177	10A	St. Alban's	Amends Bylaw 4700 (Designates 8420 Granville Ave. and 8451, 8471, and 8491 Bennett Rd. to Multiple Family High Rise.)	11/10/88	03/04/89
5199	7A	Gilmore	Amends Bylaw 4700 (Designates 12151 Shell Rd. to Public, Institutional and Open Space.)	11/10/88	03/04/89
5226	9A	McLennan (Ash Street)	Amends Bylaw 4700 (Designates 8031 and 8051 No. 4 Rd. as Residential.)	24/10/88	03/04/89
5257	9A	McLennan (Ash Street)	Amends Bylaw 4700 (Designates 9311, 9331, 9351, 9371 and 9391 Dixon Ave. as Townhouse or Small Lots, except the southerly 30.48m (100 ft.))	22/11/88	03/04/89
5299	N/A	RICHMOND AND ALL AREAS (To facilitate new Zoning Bylaw 5300)	Repeals Bylaw 4700	03/04/89	
5400	N/A	RICHMOND AND ALL AREAS	OCP & areas (Replaces Bylaw 4700.)	22/06/89	
5413	5*	Blundell	Amends Bylaw 5400 (Designates 6560 Blundell Rd. as Public Institutional and Open Space.)	18/09/89	
5415	11B*	Cambie (West)	Amends Bylaw 5400 1. Designates areas along the east side of No. 4 Rd., west of Shell Rd., north of Alderbridge Way, south of Cambie Rd. to Residential and Non-Residential; 2. Adds Non-Residential definition to Interpretation Section; and 3. Amends Attachments 1 and 2 & 4 to reflect re-designation.	14/11/89	
5440	7A*	Gilmore	Amends Bylaw 5400 (Adds Schedule 2.7A.)	20/11/89	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
5449	11B*	Cambie (West)	Amends Bylaw 5400 and 5415 1. (Designates generally the east end of Odlin Rd. (north and south side) to Residential Reserve and Under Study; and 2. Amends Attachments 1 and 2 & 3 to reflect re-designation.	09/04/89	
5451	11B*	Cambie (West)	Amends Bylaw 5400 (Designates 8700/8920 Alderbridge Way as Residential.)	08/01/90	
5475	11C	Cambie (East)	Amends Bylaw 5400 (Substituting new definition of industrial in Interpretation Section.)	19/02/90	
5485	4*	Steveston	Amends Bylaw 5400 (Att. 1 to Sch. 1 & Att. 1 to Sch.2.4) (Designates 12011 7th Ave. accordingly to accommodate relocation of restaurant at Garry Point Park Site.)	19/02/90	
5503	10*	Town Centre	Amends Bylaw 5400 (Att. 2 to Sch. 2.10, designates 8300 Cook Rd., 6571, 6591, & 6611 Cooney Rd. from Multi – Family Low - Rise to Multi - Family High – Rise.)	28/01/91	
5552	4A*	Steveston	Amends Bylaw 5400 (Amends Table 1 and Att. 1, 2 & 3 to Sch. 2.4A to redesignate 12451 Trites Rd. for hi-rise & recreation use.)	25/06/90	
5554	N/A	Various	Amends Bylaw 5400 (Add Multi-Family Site definition Sect. 1.5.1.) (Amend Non-Residential definition to include marinas in Sect.1.5.2.) (Add DP Guidelines for marinas to Att. 9.)	18/06/90	
5581	N/A	Various	Amends Bylaw 5400 (Changes arterial designations to Att. 5 to Sch. 1.)	17/09/90	
5588	11B*	Cambie (West)	Amends Bylaw 5400 (Att. 1 & 2 to Sch. 2.11b) (Designates 9740 & 9760 Cambie Rd. on Att. 1 to Sch. 2.11b to Commercial and on Att. 2 to Sch. 2.11b to Commercial/Industrial.)	15/10/90	
5608	6*	Broadmoor	Amends Bylaw 5400 (Att.1 to Sch. 1) (Designates 10020, 10040, 10060 & 10080 Dunoon Dr. from Residential to Non-residential.)	11/02/91	
5614	8*	Shellmont	Amends Bylaw 5400 (Att. 1 to Sch. 1, designate certain properties at 9371 No. 5 Rd. and 11791 King Rd. from Non-Residential to Residential.)	19/11/90	
5617	12*	Bridgeport	Amends Bylaw 5400 (Att. 1 to Sch. 1, designate a portion of the property at 9920 River Dr. from Non-Residential to Residential.)	17/12/90	
5621	N/A	RICHMOND & ALL AREAS	Amends Bylaw 5400 (Replace Objective 15 & adds Att. 10 to Schedule 1.)	15/04/91	
5657	11C	Cambie (East)	Amends Bylaw 5400 (Att. 1 to Sch. 2.11c, designates 5731 No. 6 Rd. from Industrial to School / Park Institutional.)	21/01/91	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
5683	6*	Broadmoor	Amends Bylaw 5400 (Att. 1 to Sch.1, designates 8100 & 8120 Francis Rd. & 9080 No. 3 Rd. to Public, Institutional and Open Space.)	26/08/91	
5670	9	McLennan	Amends Bylaw 5400 (Att. 1 to Sch.1, change to Agriculture, Att.1 to Sch. 2.9 change to Agriculture, Institutional and Public. & various text amendments for 6000, 7000, 8000, 9000 & 10000 Block of No. 5 Rd. east side.)	16/3/92	
5685	7*	Gilmore	Amends Bylaw 5400 (Att. 1 to Sch.1, designates a portion of 11251 Dyke Rd. to Non – Residential.)	17/06/91	
5700	6*	Broadmoor	Amends Bylaw 5400 (Att. 1 to Sch.1, designates 8051 Garden City Rd. to Non – Residential.)	09/09/91	
5723	11A	North Lansdowne	Amends Bylaw 5400 (Att. 1 to Sch. 2.11A designates 4651 Garden City Rd. to Commercial, Att. 2 to Sch. 2.11A Development Permit required.)	10/02/92	
5734	12*	Bridgeport	Amends Bylaw 5400 (Att.1 to Sch.1, designates 9231, 9251, & 9271 Beckwith Rd. to Non – Residential.)	09/12/91	
5746	N/A	Various	Amends Bylaw 5400 Att. 9 to Sch.1, various text amendment.)	09/12/91	
5756	13	East Richmond	Amends Bylaw 5400 (Text amendment, policy section of Objective 3 in Section 4.1.)	22/6/92	
5764	14	Hamilton	Amends Bylaw 5400 (Att. 1 to Sch.1 designates 21,000 and 22,000 Block Westminster Hwy. and Dyke Rd. to Non-Residential, Public, Institutional, and Open Space.) Att. 1 to Sch. 2.14 designates 21,000 and 22,000 Block Westminster Hwy. and Dyke Rd. to Industrial, Park & Commercial.)	13/7/92	
5766	4	Steveston	Amends Bylaw 5400 (Att.1 to Sch. 2.4 designates 3028, 3040, 3044, 3048 Richmond St. to School and Park.)	12/11/91	
5766	10 A	Town Centre (St. Albans)	Amends Bylaw 5400 (Map 1.1 of Sch. 2.10 designates 8191, 8211, 8271, 8151 Jones Rd. to School and Park.)	12/11/91	
5766	11 C	Cambie East	Amends Bylaw 5400 (Att.1 to Sch. 2.11C designates 12320, 12300, 12180, 12160, 12140 & 12100 Cambie Rd. to School and Park.)	12/11/91	
5766	6*	Broadmoor	Amends Bylaw 5400 (Att.1 to Sch.1 designates 8231 Garden City Rd., 9060, 9200, 9180, 9140 Williams Rd. to Public Institutional and Open Space.)	12/11/91	
5769	4 A	Steveston (Moncton)	Amends Bylaw 5400 (Amend O.C.P. map Sch. A and Page 2 of Table 1 Sch. B.)	09/12/91	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
5777	7*	Gilmore	Amends Bylaw 5400 (Att.1 to Sch.1 designates westerly 40.0m of 9320 Steveston Hwy. to Public, Institutional and Open Space.)	13/1/92	
5795	11C	Cambie East	Amends Bylaw 5400 (Deletes Att.1 to Sch. 2.11C & substitutes Sch. A thereof.) (Deletes Att. 2 to Sch. 2.11C & substitutes Sch. B thereof.) (Deletes the definition of "Residential" in Sch.1 and replace with new definition.)	27/4/92	
5798	10	Town Centre	Amends Bylaw 5400 (Att. 2 to Sch. 2.10 designates 8000 block Cook Rd. and Anderson Rd., 6000 block Cooney Rd., Eckersley Rd., Park Rd. and Park Pl. to Multi-Family High Rise and Park.) (Att. 5 to Sch. 2.10 designates the 8000 block Anderson Rd., 6000 block Cooney Rd., Eckersley Rd., Park Rd. and Park Pl. as Development Permit Areas.)	17/2/92	
5801	5*	Blundell	Amends Bylaw 5400 (Att.1 to Sch.1 designates 7680/7700 No. 2 Rd. to Non-Residential.) (Att.1 & Att. 2 of Sch. A attached and forming part of Bylaw No. 5801 under the title Sch. 2.8A of Bylaw 5400.)	22/6/92	
5827	6*	Broadmoor	Amends Bylaw 5400 (Att.1 to Sch.1 designates 8080 Francis Rd. to Public, Institutional and Open Space.)	17/2/92	
5832	4	Steveston	Amends Bylaw 5400 (Att.1 to Sch. 2.4 designates 11771 Fentiman Pl. to Institutional.)	17/2/92	
5839	12	Bridgeport	Amends Bylaw 5400 (Establish an Area Plan for the Bridgeport Planning Area.)	16/3/92	
5843	11C	Cambie East	Amends Bylaw 5400 (Att. 1 to Sch. 2.11C designates southerly 57.07m of 12440 Woodhead Rd. to Residential.) (Att. 2 to Sch. 2.11C designates southerly 57.07m of 12440 Woodhead Rd. as a Development Permit Area.)	01/6/92	
5849	10	Town Centre	Amends Bylaw 5400 (Att. 2 to Sch. 2.10 designates part of 7191 Granville Ave. to Mult. Family High Rise.) (Att. 5 to Sch. 2.10 designates part of 7191 Granville Ave. as a Development Permit Area.)	27/7/92	
5854	10	Town Centre	Amends Bylaw 5400 (Att. 2 to Sch. 2.10 designates part of 6651 Minoru Blvd. to Civic Centre.)	27/7/92	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
5857	2.12* 2.14	Bridgeport / Hamilton	Amends Bylaw 5400 (Att. 1 to Sch. 1 designates 10120 Gilmore Cr., 10091 Finlason Dr. to Public, Institutional and Open Space.) (Att. 1 to Sch. 2.14 designates 22000 Block Westminster Hwy. to Park.)	01/6/92	
5891	2.10	Town Centre	Amends Bylaw 5400 (Att. 2 to Sch. 2.10 designates 7771, 7791 & 7811 Bennett Rd. to Civic Centre.)	28/9/92	
5897	2.14	Hamilton	Amends Bylaw 5400 (Att. 1 to Sch. 2.14 designates 22031 and 22051 Westminster Hwy. to R2 New Residential and replaces Table with "Sch. A attached to and forming part of Bylaw No. 5897 and Att. 3 to Sch. 2.14 Development Permit Guideline additions.)	26/10/93	
5902	2.12	Bridgeport	Amends Bylaw 5400 (Att. 8 to Sch. 2.12 designates part of Hwy. 99 North and South of Van Horne Way (<i>under Oak Street Bridge</i>) to Temporary Commercial Permit Area.)	15/6/92	
5912	2.11C	Cambie (East)	Amends Bylaw 5400 (Att. 1 to Sch. 2.11C designates part of Hwy. 99 South of 10700 Cambie Rd. to Commercial.)	11/04/94	
5924	2.11B	Cambie (West)	Amends Bylaw 5400 (Att.1 to Sch. 2.11B designates 3651 Sexsmith Rd. to Commercial/ Industrial.)	27/7/92	
5925	2.11B	Cambie (West)	Amends Bylaw 5400 (Att. 1 to Sch. 2.11B designates 3671 and 3751 Sexsmith Rd. & 8391 Cambie Rd. as Commercial / Industrial.)	25/5/93	
5928	2.11C	Cambie (East)	Amends Bylaw 5400 (Att. 1 to Sch. 2.11C designates 4000 block Jacombs Rd. and 12000 block Cambie Rd. to Residential, also Att. 2 to Sch. 2.11C for area above as a Development Permit Area.)	8/2/93	
5932	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Richmond O.C.P. Bylaw 5400, 1989 is amended by deleting Sch. 2.2A thereof and by substituting therefor "Sch. A attached to and forming part of Bylaw No. 5932" under the title "Sch. 2.2A of Bylaw 5400".)	20/7/92	
5943	2.12	Bridgeport	Amends Bylaw 5400 (Delete Att. 8 to Sch. 2.12 thereof and substitute "Sch. A attached to and forming part of Bylaw 5943.")	17/8/92	
5975	2.14	Hamilton	Amends Bylaw 5400 (Att. 1 to Sch. 2.14 designates 22580 and 22620 Westminster Hwy. to R2 New Residential and replaces Table 1 with "Sch. B attached to and forming part of Bylaw 5975".)	08/03/93	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
5990	2.2B	Thompson (Terra Nova)	Amends Bylaw 5400 (Adds "Sch. A attached to and forming part of Bylaw No. 5990 under the title "Sch. 2.2B of Bylaw 5400".)	22/02/93	
6012	2.14	Hamilton	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.14 by designating part of 22040 Westminster Hwy. to Industrial.)	28/06/93	
6019	211C	Cambie (East)	Amends Bylaw 5400 (Adds Attachment 5 for 13000 Block Sparwood Pl. and 5871 No. 6 Rd. as Temporary Industrial Uses.)	19/04/93	
6038	2.11B	Cambie (West)	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.11B by designating 10400, 10406, 10420, 10460, and 10480 Cambie Rd.; 10571, 10631, 10651 and 10671 Odlin Rd. to Residential.)	27/09/93	
6043	N/A	Main O.C.P.	Amends Bylaw 5400 (Amends Subsection 1.5 definition of "Multiple Family Site".)	19/04/93	
6062	2.14	Hamilton	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.14 by designating 22851 Westminster Hwy. and 22660 Rathburn Dr to R2 New Residential, also by deleting Table 1 of Sch. 2.14 thereof and by substituting Sch. A attached to and forming part of Bylaw No. 6062 and by adding paragraph 5, 6 and 7 to Att. 3 of Sch. 2.14, Development Permit Guidelines.)	22/11/93	
6079	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Delete Att. 1 to Sch. 2.2A, substituting "Sch. A attached to and forming part of Bylaw No. 6079".)	22/11/93	
6080	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Insert Section 5 to Sch. 2.2A thereto "Sch. A attached to and forming part of Bylaw No. 6080 and adding Table of Contents of Sch. 2.2A thereto Sch. B attached to and forming part of Bylaw No. 6080".)	22/11/93	
6081	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Amended by appending to Sch. 2.2A thereto "Sch. A attached to and forming part of Bylaw No. 6081".)	22/11/93	
6087	2.14	Hamilton	Amends Bylaw 5400 (Amended by repealing the existing land use designation of 22680 Westminster Hwy. in Att. 1 to Sch. 2.14 and by designating it Public Institutional & Open Space.)	22/08/94	
6097	2.14	Hamilton	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.14 by designating 22500 McLean Ave., part of 22591 Westminster Hwy., 22631 & 22651 Westminster Hwy. to Public, Institutional & Open Space.)	13/12/93	
6108	2.11C	Cambie (East)	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.11C by designating 10111, 10131 and 10151 Bird Rd. as School/Park, Institutional.)	09/01/95	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
6118*	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Amended by deleting Att. 3 to Sch. 2.2A thereto and inserting "Schedule A attached to and forming part of Bylaw No. 6118, also amended by adding Schedule B attached to and forming part of Bylaw No. 6118".)	28/02/94	
6120*	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Amended by deleting Att. 3 to Sch. 2.2A thereto and inserting "Schedule A attached to and forming part of Bylaw No. 6120, also amended by adding Schedule B attached to and forming part of Bylaw No. 6120".)	28/03/94	
6127	2.14	Hamilton	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.14 by designating 22220, 22280, 22320, 22360 and 22380 Westminster Hwy. to R2 (New Residential). Also amended by deleting Table 1 of Sch. 2.14 and substituting "Schedule A attached to and forming part of Bylaw No. 6127".)	27/02/95	
6129	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating 6451 Dyke Rd. to Single Family.)	08/11/93	
6131	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating 5791 and 5771 Steveston Hwy. to Multiple-Family.)	24/05/94	
6135	2.11C	Cambie (East)	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.11C by designating 11051/11071 Bird Rd. to Residential (Single Family Only.)	13/09/93	
6154	2.5*	Blundell	Amends Bylaw 5400 (Amends Att. 1 to Sch.1 designates 4780 Blundell Rd., Public, Institutional & Open Space.)	16/08/93	
6174	2.7*	Gilmore	Amends Bylaw 5400 (Amends Att 1.1 to Sch. 1 by designating 9120 Steveston Hwy. to Public, Institutional & Open Space.)	11/04/94	
6203	2.4A	Steveston (Moncton)	Amends Bylaw 5400 (Amends Sch. 2.4A by: 1. Replacing Att. 1 (O.C.P.Map); 2. Replacing Page 2 of Table 1 (Staging, Density, Proposed Uses and Housing Types); 3. Adding Att. 2A (Detailed Character Design Guidelines Area); and 4. Adding Schedule D Detailed Character Design Guidelines.*)	24/10/94	
6214	2.4A	Steveston (Moncton)	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4A by designating Development Area D to Industrial also revise Table 1 of Sch. 2.4A.)	28/03/94	
6215	2.6A	Broadmoor (Ash Street)	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.6A, amends Table 1 of Sch.2.6A and amends Clause 1.1 of Objective 1 of Subsection 3.1 of Sch. 2.6A.)	12/11/96	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
6224	2.4	Steveston	Amends Bylaw 5400 (Area bounded by Chatham St., 7th Ave., Moncton St. & 6th Ave.) (Amends Att. 1 to Sch. 2.4 by designating S.E. portion Industrial.) (Adds Att. 2A Additional D.P. Guideline Area and Att. 4 Additional Development Permit Area Guidelines and Requirements.)	28/11/94	
6245*	2.11B	Cambie West	Amends Bylaw 5400 (Amended by repealing existing land use designation of 5000 Fisher Dr. in Att. 1 to Sch. 2.11B and by designating it to Residential.)	22/08/94	
6248	2.7*	Gilmore	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 designates 11340 No. 5 Rd. (part only) to Non Residential.)	13/03/95	
6257	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating 4900 Steveston Hwy. to School and Park, also amends Att. 1 to Sch. 1 designating 4900 Steveston Hwy. to Public, Institutional & Open Space.)	21/03/94	
6265	2.9A	McLennan (Ash Street)	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.9A by designating 9160 Dixon Ave. to Institutional also amended by deleting Table 1 of Sch. 2.9A and inserting Schedule A attached to and forming part of Bylaw No. 6265.)	28/03/94	
6261	2.6C	Broadmoor (Sunnymede North)	Amends Bylaw 5400 (Development Permit Guidelines for south side of 7000 block Blundell Rd. between No. 3 Rd. and Gilbert Rd.)	26/08/96	
6284	2.12*	Bridgeport	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating 9471 Beckwith Rd. as Non-Residential.)	27/03/95	
6293	2.10B	City Centre (Acheson/Bennett)	Amends Bylaw 5400 (Acheson / Bennett Sub-Area Plan)	20/03/95	
6295	2.11	Cambie (West)	Amends Bylaw 5400 (Att. 1 to Sch. 2.11B designates a part of 10500 Odlin Rd. as Commercial/Industrial.)	13/02/95	
6319	2.9*	McLennan	Amends Bylaw 5400 (Amended by repealing the existing Environmentally Sensitive Area designation in Att. 11 to Sch. 1 for 6120 No. 5 Rd.)	20/06/94	
6323	2.3*	Seafair	Amends Bylaw 5400 (Amended by repealing the existing Environmentally Sensitive Area designation in Att. 11 to Sch. 1 for Granville Ave. & Railway Ave.)	20/06/94	
6326*	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Amended by deleting Att. 3 to Sch. 2.2A thereof and inserting Schedule A, also amended by adding Schedule B - Design Guidelines for Area H.)	22/09/94	
6333	2.12	Bridgeport	Amends Bylaw 5400 (Amended by adding Att. 9 to Sch. 2.12, designating 1999 Savage Rd. Temporary Industrial Use.)	18/07/94	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
6336	2.11B	Cambie (West)	Amends Bylaw 5400 (Amended by repealing the existing land use designation in Att.1 to Sch. 2.11B, the southerly 30m of 3240 No. 4 Rd. and by designating it Residential.)	22/09/94	
6350	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating part of 4360 Garry St. as Single Family.)	12/12/94	
6356	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating 11571/11591 7th Ave. as Single Family.)	17/10/94	
6360	2.11B	Cambie (West)	Amends Bylaw 5400 (Amended by inserting Att. 5 - Temporary Commercial Use Area Map for 8311 Cambie Rd.)	17/10/94	
6381	2.13*	East Richmond	Amends Bylaw 5400 (Adds Att. 12 - Temp. Industrial Use Area. (No. 6 Rd./Bridgeport R/W.)	19/12/94	
6387	2.2B	Thompson (Terra Nova)	Amends Bylaw 5400 (Amends Att. 2 to Sch. 2.2B by designating 6051 to 6411 No. 1 Rd. to Residential Townhouse.)	18/04/95	
6393	2.10	City Centre	Amends Bylaw 5400 (New City Centre Area Plan 2.10, also revise and adjust area boundaries to Planning Areas 2 (Thompson), 9 (McLennan), 11B (Cambie West).)	19/06/95	
6403	2.4 / 2.4A	Steveston / Moncton	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4-Trails / Att. 1 to Sch. 2.4A- Area D to Industrial.)	16/01/95	
6425	N/A	Main OCP	Amends Bylaw 5400 (Amended by replacing clause 2 of the Special Guidelines for Foreshores in Att. 9.)	15/05/95	
6429	2.11C	Cambie (East)	Amends Bylaw 5400 (Amends Att. 1 and Att. 2 to Sch. 2.11C by designating 10140, 10180, 10200, 10220, 10260, 10280 and 10300 Bridgeport Rd. to Commercial.)	20/03/95	
6430	2.11B	Cambie (West)	Amends Bylaw 5400 (Amends Att. 1 and Att. 2 to Sch. 2.11B by designating 9700, 9760, 9766 and 9780 Bridgeport Rd. to Commercial.)	20/03/95	
6450	2.14	Hamilton	Amends Bylaw 5400 (New Hamilton Area Plan 2.14.)	19/06/95	
6453	N/A	Main OCP	Amends Bylaw 5400 (Amends Objective 14 to Section 4.5 of Sch., also amends Item 4 of Section 5.3 of Sch., adds Item 19 at end of Section 5.3 of Sch. 1 and replaces Att. 4 of Sch. 1.)	18/09/95	
6459	2.14	Hamilton	Amends Bylaw 5400 (Amends Hamilton 2.14 by adding Lower Westminster Sub - Area.)	19/06/95	
6466	2.4	Steveston	Amends Bylaw 5400 (Insert Att. 1A to Sch. 2.4 by designating 6111 and 6225 London Rd. to Temporary Industrial Use.)	17/07/95	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
6474	2.1*	Sea Island	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating 7120 and 7140 Miller Rd. to Public, Institutional and Open space.)	28/08/95	
6479	2.6*	Broadmoor	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating 8211 and 8271 Garden City Rd. to Public, Institutional & Open Space.)	17/07/95	
6485	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating 12420 No. 1 Rd. to Commercial.)	25/09/95	
6505	2.5B	Blundell (Laurelwood Sub-Area)	Amends Bylaw 5400 (Amended by inserting Schedule 2.5B (Laurelwood Sub-Area) at Granville/Railway Ave.)	16/10/95	
6522	2.5C	Blundell (East Livingston Sub-Area)	Amends Bylaw 5400 (Amended by inserting Schedule 2.5C East Livingston Sub-Area.)	17/06/96	
6529	2.5A	Blundell (N.E. corner of Blundell & No. 2 Rd.)	Amends Bylaw 5400 (Delete the term 2.8A wherever it appears in Schedule 2.8A insert 2.5A.)	19/02/96	
6531	2.14	Hamilton	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.14 by designating south west corner of Hamilton Rd. and Queen Rd. to Natural Area / Open Space (Public and Private).)	20/11/95	
6531	2.11C	Cambie East	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.11C by designating westerly part of Lot A Section 11 Block 5 North Range 6 N.W.D. Plan 82430 to School / Park, Institutional.)	20/11/95	
6531	2.14	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating properties south of Dyke Rd. from No. 2 Rd. east to School and Park.)	20/11/95	
6531	N/A	Main OCP	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating southerly 147.83m of 6480 & 6500 Williams Rd. as Public, Institutional, and Open Space.) (Amends Att. 1 to Sch. 1 by designating lands north of River Rd. between Savage Rd. and No. 7 Rd. as Public Institutional, and Open Space.)	20/11/95	
6531	2.2A	Thompson (Dover Crossing)	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.2A by designating Lots 4 & 54 Section 1, 4-7 as Park / Open Space.)	20/11/95	
6534	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4, and adding Att. 2B and Att. 5 to Sch. 2.4.)	11/03/96	
6545	2.2B	Thompson (Terra Nova)	Amends Bylaw 5400 (New Terra Nova Sub-Area Plan 2.2B.)	08/01/96	
6555	2.6B	Broadmoor (Central West Broadmoor)	Amends Bylaw 5400 (New Central West Broadmoor Sub-Area Plan 2.6B.)	18/03/96	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
6561	N/A	Main OCP	Amends Bylaw 5400 (Add Section 4.1 under "Guidelines" in Att. 9.)	15/04/96	
6568	N/A	Main OCP	Amends Bylaw 5400 (Amend Att. 11 to Sch. 1 Environmentally Sensitive Area in Fraserwood Industrial Park.)	15/01/96	
6569	2.14	Hamilton	Amends Bylaw 5400 (Amends the Development Permit Area Map.)	15/01/96	
6575	2.7*	Gilmore	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating (part of) 11340 No. 5 Rd. as Non - Residential.)	10/03/97	
6589	2.10C	City Centre (McLennan North)	Amends Bylaw 5400 (New McLennan North Sub-Area Plan 2.10C.)	15/07/96	
6591	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating 6431 and 6451 Dyke Rd. (part only) as Single Family.)	22/07/96	
6597	2.10	City Centre	Amends Bylaw 5400 (Amends Official Community Plan Land Use Map to Sch. 2.10 by designating 9031 Blundell Rd. as Neighbourhood Pub.)	28/10/96	
6620	2.10D	City Centre (McLennan South)	Amends Bylaw 5400 (New McLennan South Sub-Area Plan 2.10D.)	21/05/96	
6668	2.10	City Centre	Amends Bylaw 5400 (Amends Official Community Plan Land Use Map to Sch. 2.10 by designating 8888 Odlin Cr., 8860 & 8880 Cambie Rd. as Auto-Oriented Commercial.)	12/11/96	
6675	2.13*	East Richmond	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating 5520 No. 6 Rd. as Non-Residential.)	26/05/97	
6682	2.7B	Gilmore	Amends Bylaw 5400 (Add Sch. 2.7B to Bylaw 5400, Guidelines for Commercial Development at No. 5 Rd. and Steveston Hwy.)	10/03/97	
6701	2.12*	Bridgeport	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating parts of 9280 and 9320 Beckwith Rd. as Non - Residential.)	10/02/97	
6712	N/A	Main OCP	Amends Bylaw 5400 (Amends Att. 11 to Sch. 1 by designating 2700 and 2800 Westminster Hwy. as an Environmentally Sensitive Area.)	17/02/97	
6737	2.2B	Thompson (Terra Nova)	Amends Bylaw 5400 (Amends Att. 2 to Sch. 2.2B, Att. 2A to Sch. 2.2B and insert Section 8 of Sch. 2.2B.)	09/06/97	
6740	2.11B	Cambie West	Amends Bylaw 5400 (Amended by inserting new Schedule 2.11B.)	23/06/97	
6743	2.10	City Centre	Amends Bylaw 5400 (Amends the Official Community Plan Land Use Map to Schedule 2.10 by designating 6871 and 6911 No. 3 Rd. Mixed Use-High Density.)	21/04/97	

Bylaw No.	Area No.	Area	Purpose	Adopted	Rescinded
6770	2.7*	Gilmore	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating part of 11340 No. 5 Rd. as Non –Residential.)	14/07/97	
6777	2.10 & 2.10A	City Centre City Centre (Ash Street)	Amends Bylaw 5400 (Amends the Official Community Plan Land Use Map in Sch. 2.10 & in Map 1.1 in Sch. 2.10A by designating 7060 and 7080 St. Albans Rd. & 8471 Bennett Rd. as Mixed Use-High Density.)	14/10/97	
6781	2.5*	Blundell	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating 6480 and 6500 Williams Rd. as Public, Institutional and Open Space.)	21/07/97	
6792	2.3*	Seafair	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating 4000 Blundell Rd. & 8060 No.1 Rd. as Residential.)	25/08/97	
6798	2.2B	Thompson (Terra Nova)	Amends Bylaw 5400 (Amends Att. 2 to Sch. 2.2B and adding Att. 4 to Sch. 2.2B Additional Development Permit Guidelines.)	25/05/98	
6822	2.8*	Shellmont	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating 11331 Williams Rd. as Public, Institutional and Open Space.)	23/02/98	
6839	N/A	Main OCP	Amends Bylaw 5400 (Amends by adding Richmond Regional Context Statement.)	16/03/98	
6841	2.3*	Seafair	Amends Bylaw 5400 (Amends Att. 1 to Sch. 1 by designating Area A of 9751 No. 1 Rd. Public, Institutional and Open Space and Area B of 9751 No. 1 Rd. as Residential.)	09/03/98	
6843	2.4	Steveston	Amends Bylaw 5400 (Amends Att. 1 to Sch. 2.4 by designating 10251 No. 1 Rd. as Multiple Family.)	08/06/98	
6846	2.6A	Broadmoor (Ash Street)	Amends Bylaw 5400 (Amends Table 1 of Sch. 2.6A.)	14/04/98	
6931	2.10	City Centre	Amends Bylaw 5400 (Amends the Official Community Plan Land Use Map by designating 8351, 8371 Brownwood Rd., 8220, 8240 Cambie Rd., parts of 8360, 8380, 8391 and 8431 Brownwood Rd., 8351 Browngate Rd. and 8280 Cambie Rd. as Auto-Oriented Commercial.)	09/11/98	
6937	2.6A	Broadmoor (Ash Street)	Amends Bylaw 5400 (Amends Policy 2.2 and Policy 2.3 to Sch. 2.6A.)	20/07/98	
7100	N/A	RICHMOND AND ALL AREAS	Richmond OCP (Replaces Bylaw 5400)	15/03/99	

- * Changes reflected on Urban Development's consolidated version of Attachment 1, Schedule 1 to Bylaw 5400 only.
 * Detailed Character Design Guidelines (not included in your consolidation - see front counter).

Richmond Official Community Plan

Schedule 2 Area Plans

2.2A	Thompson (Dover Crossing Sub-Area)
2.2B	Thompson (Terra Nova Sub-Area)
2.4	<i>Steveston</i>
2.5A	Blundell (Laurelwood Sub-Area)
2.5B	Blundell (East Livingstone Sub-Area)
2.6A	Broadmoor (Ash Street Sub-Area)
2.6B	Broadmoor (Central West Sub-Area)
2.6C	Broadmoor (Sunnymede North Sub-Area)
2.8A	Shellmont (Ironwood Sub-Area Plan)
2.10	<i>City Centre</i>
2.10A	<i>City Centre (St. Albans Sub-Area)</i>
2.10B	<i>City Centre (Acheson Bennett Sub-Area)</i>
2.10C	<i>City Centre (McLennan North Sub-Area)</i>
2.10D	<i>City Centre (McLennan South Sub-Area)</i>
2.11A	<i>West Cambie</i>
2.11B	<i>East Cambie</i>
2.12	<i>Bridgeport</i>
2.13A	<i>East Richmond (McLennan Sub-Area)</i>
2.14	<i>Hamilton</i>

Note: The Area Plans shown in italics remain in its original sideways binder and will be included in this binder at a later date.

Richmond Official Community Plan



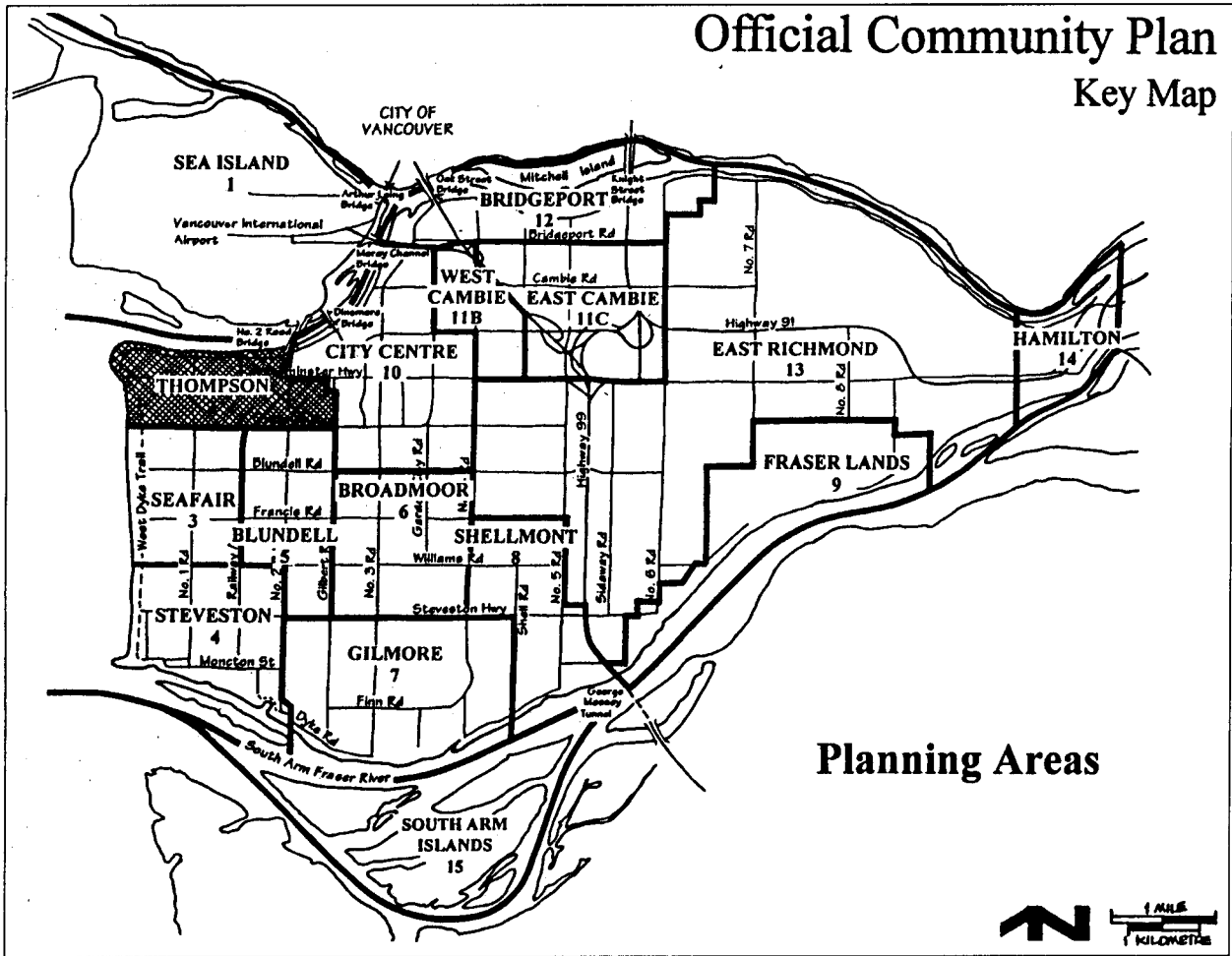
City of Richmond

THOMPSON AREA DOVER CROSSING SUB-AREA PLAN

Bylaw 7100 Schedule 2.2A



KEY MAP



PLAN AREA MAP

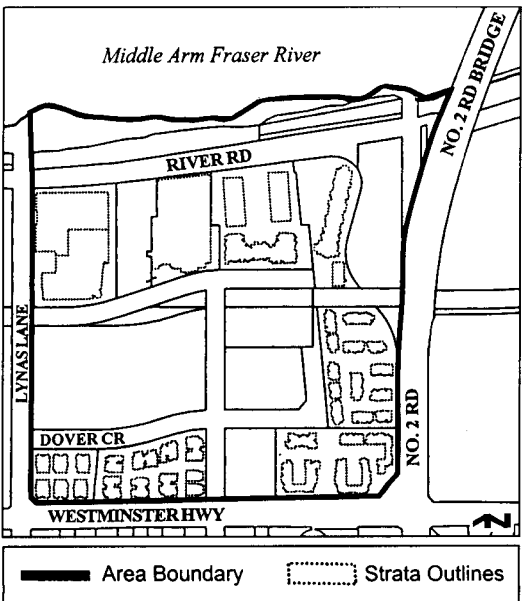


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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City’s social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City’s Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

The Dover Crossing Neighbourhood is located within the Thompson Planning Area and is bounded by River Road, No. 2 Road, Westminster Highway and Lynas Lane. (See Plan Area Map). The subject neighbourhood is surrounded by the Middle Arm of the Fraser River and the Vancouver International Airport to the north, undeveloped land, a recreation vehicle park and industrial warehousing to the east, single-family residential to the south, and the City Works Yard to the west.

The subject neighbourhood occupies about 14.5 ha (36 ac.). Existing land use at the time of plan preparation (1990) consisted of 11 homes on large lots, the Vancouver-Austrian Club, a C.P.R. right-of-way and vacant undeveloped lands. The total resident population of the neighbourhood was approximately 33 people.

The new bridge crossing from the north end of No. 2 Road to Sea Island provides a direct link from Richmond to the Vancouver International Airport. This link makes the Dover Crossing Neighbourhood one of the major visual gateways into West Richmond.

The Middle Arm of the Fraser River is the most prominent natural feature of the Dover Crossing Neighbourhood. The river serves as an important marine habitat. The dyke and foreshore provide a valuable recreation, trail and natural amenity that has a strong influence on land use in the Dover Crossing Neighbourhood.

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

This plan sets out the goals, objectives, policies and development guidelines for the Dover Crossing Sub-Area, a part of the Thompson Planning Area.

1.2 VISION

The population of Richmond is expected to grow to 212,000 people by the year 2021. In order to preserve farmland and stabilize single-family neighbourhoods, the majority of this growth needs to be housed in and around the City Centre Planning Area.

Although Richmond's history is linked to the Fraser River, its City Centre is not located to take advantage of the waterfront. As Richmond grows, there is an opportunity to create an exciting urban waterfront that is well connected to the city core in terms of open space and transportation linkages. This may be achieved by eventually expanding the higher density residential and commercial developments of the City Centre Planning Area towards the Middle Arm of the Fraser River.

The Dover Crossing Neighbourhood is located directly adjacent to the most westerly part of the City Centre area. The size of the Dover Crossing Neighbourhood 14.5 ha (36 ac.) affords an opportunity to create a liveable, well co-ordinated mixed housing community.

The Dover Crossing Neighbourhood will be transformed from an area with a high proportion of vacant land and large single-family lots to a residential neighbourhood with a variety of multiple-family (townhouses and apartments) housing types.

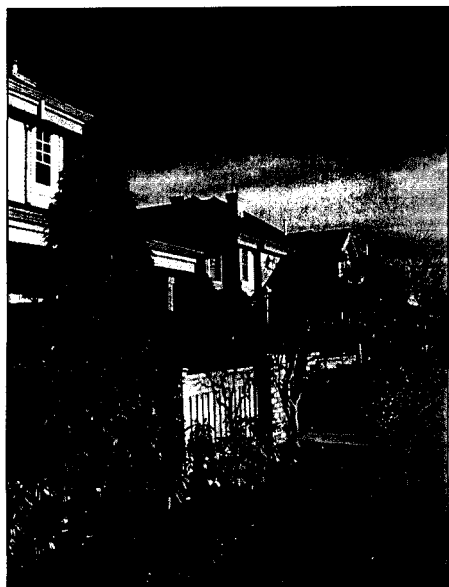
The general development concept will consist of the following characteristics:

- There will be a gradual transition in housing form from north to south in order to minimize the impacts on the existing single-family neighbourhoods to the south of the sub-area.
- Taller buildings will be located adjacent to the river and the No. 2 Road Bridge which will create a visual gateway into West Richmond and permit more open space in the neighbourhood.
- Townhouses will be primarily developed adjacent to Westminster Highway but will also be integrated with the apartments to the north to help create a high quality streetscape.

The Dover Crossing Sub-Area will be well connected to the riverfront. Linear parkways accessible by pedestrians, wheelchairs and cyclists will extend from Westminster Highway through the neighbourhood to River Road. A pedestrian crossing will provide a safe and convenient connection from the neighbourhood over River Road to the dyke trail.

The importance of preserving views to the riverfront and mountains will be recognized. Views of the river and mountains will be preserved through the linear parkways referred to above and by terracing of apartment buildings.

A neighbourhood park and school site will be centrally located in the neighbourhood. Community services, such as childcare, will be encouraged and facilitated as part of development approvals.



Linear Walkways Connect Housing to the Waterfront

Specialized landscaping, fountains and public art will help create special places for people to meet in the sub-area.

Developments in the area will maintain, enhance and preserve air, water and soil quality.

1.3 GOALS

1. To permit residential development that:
 - a) Recognizes the area's close proximity to the No. 2 Road Bridge, the Middle Arm of the Fraser River and the City Centre;
 - b) Preserves views of the waterfront and the mountains;
 - c) Provides public access to the waterfront;
 - d) Includes community services and open space that enhances liveability;
 - e) Maintains, enhances and preserves air, water, and soil quality.
2. To encourage and facilitate features which create a "heart and soul" for the neighbourhood, such as land-bridges, special landscaping, fountains and public art.
3. To provide roads, walkways, and bicycle paths that help to connect the neighbourhood with the riverfront and the surrounding areas in a convenient and safe manner.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING



New Apartment Units

ISSUE:

Housing Forms

This plan prescribes a mix of townhouses and apartments. The densities of the development will range from .55 (F.A.R.)¹ to a maximum of 1.6 (F.A.R.). Thus, the overall density will be in the medium range.

As shown on the Land Use Map, townhouses are located along the southern edge of the neighbourhood to provide a gradual transition from the low density single-family area along the south side of Westminster Highway. The apartments are deliberately located in the north and north-east portions of the neighbourhood in order to create a visual gateway into Richmond. The taller buildings allow for more green space, and in conjunction with the linear walkways enhances both private and public views and access to the waterfront.

OBJECTIVE 1:

To provide a range of housing types for a variety of households and age groups.

POLICIES:

- a) Allow development such that a mixture of housing types are provided as per the Land Use Map;
- b) Require that all buildings shall not exceed 24.38 m (80 ft.) in height;
- c) Encourage and facilitate the provision of affordable housing.

¹ F.A.R. (Floor Area Ratio: means the figure obtained when the total area of the floors of the building on a lot, measured to the outer limits of the building or buildings, is divided by the area of the lot.)

ISSUE:

Views and Vistas

Views of the airport and the mountains are unobstructed from the neighbourhood. The physical height of the dyke restricts views to the north for low level residences.

OBJECTIVE 2:

To utilize opportunities for views of the Fraser River and mountains.

POLICIES:

- a) Encourage building designs which maximize views of the river and mountains;
- b) Encourage designs that minimize building shadowing of public and private open spaces and walkways.



Views to the River

ISSUE:

Airport

The neighbourhood's close proximity to the Vancouver International Airport has both advantages and disadvantages. The views of seaplanes and other aircraft landing and descending adjacent to the neighbourhood adds to the interest of the area. However, the Dover Crossing Neighbourhood is subject to a certain amount of noise and disruptions created by the float planes, other air craft and ancillary uses to the airport and therefore the planning area, or a part thereof, is subject to a noise level of 30 NEF or above.²

In order to protect flight operations, Transport Canada imposes building height restrictions in the vicinity of the Airport. Specific height restrictions are contained in plans filed by Transport Canada in the Provincial Land Titles Office.

OBJECTIVE 3:

To ensure that the heights and designs of all new buildings are in accordance with the Federal Airport Registered Zoning Regulations relative to the Vancouver International Airport.

² N.E.F. (Noise Exposure Forecast): N.E.F. is the summation of all noise that takes place in a 24 hour period based on the perceived level of noise. It considers some tonal qualities of sound and is intended to rate the "noisiness" or annoyance level of a sound rather than its loudness.

POLICIES:

- a) Encourage builders of new residential buildings to comply with Canada Mortgage and Housing Corporation's guidelines for noise mitigation;
- b) Through rezoning and subdivision applications, require restrictive covenants to hire professionals qualified in acoustics to determine and implement noise mitigation measures.

ISSUE:

No. 2 Road Bridge

In March 1990, Richmond City Council approved a new bridge crossing from the north end of No. 2 Road to Sea Island, providing a direct link to the Vancouver International Airport. Taller buildings are situated in the north-east corner of the neighbourhood to provide a visual gateway into Richmond. Buildings are set back from the northern foot of No. 2 Road as a noise mitigation measure as well as to reserve open space in case bridge loop ramps need to be developed from the bridge to River Road at some future date.

POLICIES:

- a) Ensure that all residential buildings have appropriate setbacks from No. 2 Road Bridge to mitigate against vehicular traffic noise and for privacy of residential uses;
- b) Require that sufficient land is retained as open space at the north-east portion of the neighbourhood which can be used to develop bridge loop ramps if and when the need occurs.

4.0 TRANSPORTATION

ISSUE:

Westminster Highway, No. 2 Road and River Road are important to the City network as major and local arterial routes. Access to these roads from properties in the neighbourhood should be restricted to maintain a free flow of traffic.

Lynas Lane and internal neighbourhood roads will be upgraded to full City standards as development occurs. The local road system provides access to the entire neighbourhood from Lynas Lane. A signalized intersection will be required at the corner of Lynas Lane and Westminster Highway and at Lynas Lane and River Road.

The dyke trail system will accommodate pedestrian traffic on the north side of River Road. Thus, a sidewalk will only be required on the south side of River Road.

Sanitary sewers, storm sewers, underground wiring, sidewalks, curbs and gutters will need to be provided in the neighbourhood with costs apportioned to all properties in the sub-area.

OBJECTIVE 1:

To improve the quality of the existing public utilities and roads concurrent with new residential development.

POLICIES:

- a) Close Dover Road for linear park purposes as shown on the Land Use Map;
- b) Deny direct access to the sub-area from No. 2 Road;
- c) Restrict direct access to the sub-area from Westminster Highway and River Road;
- d) Protect River Road as a local arterial road;
- e) Accommodate pedestrians and cyclists on all roads with adequate sidewalks, and bicycle lanes.

5.0 NATURAL & HUMAN ENVIRONMENT

ISSUE:

Fraser River

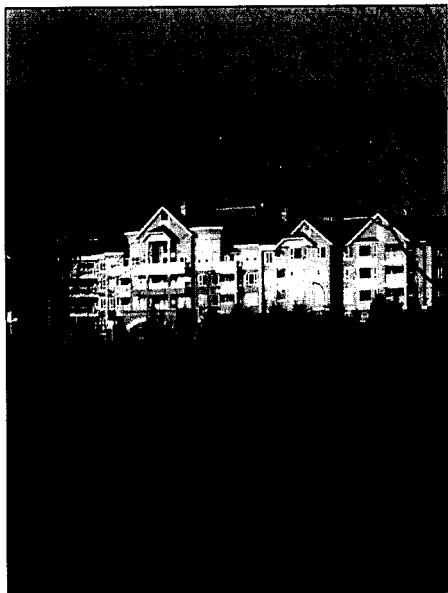
The most distinctive feature of the sub-area is its relationship to the Fraser River. The dyke and foreshore of the Fraser River serve as both a recreational and natural amenity that must be protected and enhanced. The dyke and trail system is an important part of Richmond's Open Space System.

OBJECTIVE 1:

To acknowledge the Fraser River as a resource for many users while preserving and protecting the foreshore.

POLICIES:

- a) Support policies and actions directed towards improving water quality and preserving marsh areas, and fish and wildlife habitats in and along the Middle Arm of the Fraser River;
- b) Retain the dyke adjacent to the Dover Crossing Neighbourhood for open space purposes.



Open Space

ISSUE:

Open Space and Parks

Over 9.14 ha (10 ac.) of land is devoted to open space in this plan. The open space consists of natural areas, landscaping, a 2.43 ha (6 ac.) neighbourhood park and three linear parkways extending from Westminster Highway through the neighbourhood. As shown on the Land Use Map, the three linear parkways totalling 1.62 ha (4 ac.) with a pedestrian crossing provide connections from the surrounding residential areas through the Dover Crossing Neighbourhood over River Road to the dyke trail and Fraser River. A second pedestrian crossing extending over No. 2 Road may eventually be required to further connect the lands to the east with the Dover Crossing Neighbourhood.

The open spaces created by the linear parks and the pedestrian crossing improve both the physical and visual connections between the site, the surrounding areas and the dyke trail.

OBJECTIVE 1:

To ensure that sufficient public and private park and open space is provided for recreational uses, pedestrian and bicycle movement and to promote better air quality.

POLICIES:

- a) Designate no less than 2.43 ha (6 ac.) of land for an active neighbourhood park as per the Land Use Map;
- b) Require that three linear parkways extending from Westminster Highway to the waterfront are provided for public access as part of the development site as per the Land Use Map³;
- c) Ensure that a crossing accessible to pedestrians, cyclists and wheelchairs is built as part of the development across River Road to connect the Dover Crossing Neighbourhood to the riverfront, and investigate the possibility of developing a second crossing across No. 2 Road to connect the lands to the east with the Dover Crossing Neighbourhood;
- d) Require that the Dover Crossing Neighbourhood is accessible by cyclists and that bicycle racks are provided within the public areas;
- e) Encourage the preservation of mature trees in the neighbourhood;

³ Where the linear parkways cross private lands, the linear parkways will be designated for the same land use as the contiguous private properties.

- f) Utilize the C.P.R. right-of-way as shown on the Land Use Map, for park purposes if it is vacated by C.P.R. and is not required by the City for transportation purposes.

6.0 COMMUNITY FACILITIES & SERVICES

ISSUE:

Nearly all community service and facility needs of the residents are met outside of this sub-area. The Thompson Community Centre is located approximately 0.8 km (0.5 mi.) away from the neighbourhood.

Additional residential development will increase pressure on childcare services and schools in the Thompson Area. Childcare facilities in the area should therefore be encouraged. The Richmond School Board has indicated that it is interested in acquiring an elementary school site in the sub-area as shown on the Land Use Map. Traditionally, joint school and park sites are developed in Richmond to benefit the neighbourhood as a whole.

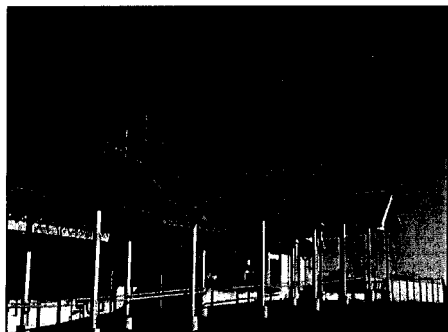
The Dover Crossing Neighbourhood is located in close proximity to the City Centre which is the focus of high density residential and commercial development in Richmond. Therefore, major commercial facilities are not warranted in the Dover Crossing Neighbourhood.

OBJECTIVE 1:

To ensure that a range of neighbourhood facilities and services are made available within the sub-area.

POLICIES:

- a) Expand the services of the Thompson Community Centre rather than duplicate the same services in the sub-area;
- b) Encourage the provision of space for childcare and private amenity space in the sub-area;
- c) Encourage the Richmond School Board to provide an elementary school in the sub-area;
- d) Permit small scale, pedestrian-oriented commercial services that are architecturally integrated within the residential buildings along River Road;
- e) Encourage the provision of special landscaping, a fountain, and public art in the Dover Crossing Sub-Area.



Childcare Facility

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated for multiple-family residential use on the Dover Crossing Land Use Map.

It is intended that these guidelines be used in conjunction with:

- The City wide Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw);
- With the site specific guidelines for each development site located within the neighbourhood as shown on the “Location of Development Sites with Detailed Character Guidelines” Map.

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

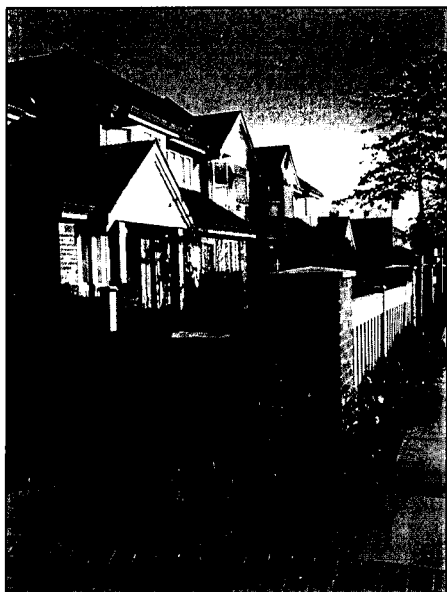
8.1.2 JUSTIFICATION

All the multiple family developments in the Dover Crossing Neighbourhood merit site-by-site consideration of form and character to ensure:

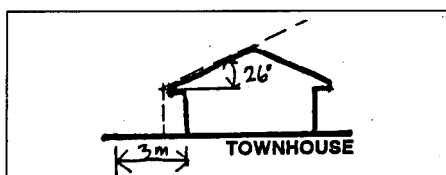
- That an appropriate, high quality built form is developed adjacent to the waterfront;
- That the open spaces and the form of development in the Dover Crossing Neighbourhood provide a gateway to Richmond from the No. 2 Road Bridge crossing;
- That the intermediate view to the Fraser River and airport, as well as the distant views to the North Shore mountains are preserved and enhanced by the building forms and provision of unobstructed view corridors;

- That the forms and materials applied to the buildings, and the type of street furniture, landscaping, signage, and light standards erected in the open spaces are co-ordinated throughout the area to form a distinct and cohesive urban neighbourhood character.

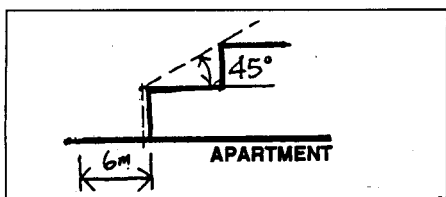
8.2 DEVELOPMENT PERMIT GUIDELINES



Secondary Entrance from Private Residences onto Common Walkway



Townhouse Transitional Height Gradient



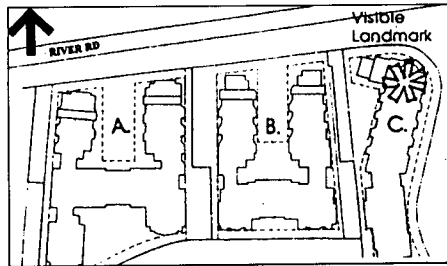
Apartment Transitional Height Gradient

8.2.1 SETTLEMENT PATTERNS

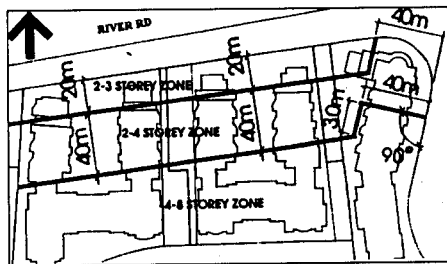
- Establish clear and appropriate pedestrian connections between the private residences and the public areas;
- Locate major entrances of the buildings onto the linear walkways;
- Provide secondary entrances from the building courtyards and from the private units onto the linear walkways where possible;
- Locate secondary entrances from the private residences onto common walkways located in between development sites;
- Multiple residential developments should provide a broad range of unit types and sizes. Included as part of this housing mix are the following minimum requirements for particular unit types:
 - A minimum of 20% of the units to be no larger than 92.9 square metres (1,000 square feet) of net area;
 - A minimum of 50% of the ground-oriented units with direct access to a linear walkway shall contain two bedrooms;
 - A minimum of 10% of the ground-oriented units with direct access to a linear walkway shall contain three or more bedrooms;
 - A minimum of 5% of the units shall be "adaptable housing" to accommodate individuals with disabilities. A minimum of 25% of these units provided shall be ground-oriented.
- Multiple residential development should recognize the importance of the "companion animal" (e.g. dogs and cats) to people living in the community, as well as the health and emotional benefits of the human-bond animal.

8.2.2 MASSING AND HEIGHT

- A variety of overall roof heights and forms should be employed to give interest and residential scale to the building forms.



Landmark



Terraced Massing

- b) Establish a maximum transitional height gradient for *townhouse development* of no more than 26° (see Townhouse Transitional Height Gradient sketch) and no more than 45° for *apartment development* to all property lines (see Apartment Transitional Height Gradient sketch); (These gradients may be varied provided privacy, sunlight, view and human scale criteria are met.)
- c) Employ pitched roofs to express a traditional residential character as the theme of the development. Flat roof elements, in combination with these pitched roofs, may be introduced as a means to create diversity;
- d) Sculpt and terrace upper floors in buildings over four-storeys in height to reduce mass, and create a transition in height and maximize views;
- e) Reduce the scale of the buildings along the walkways by providing secondary roofs or trellises over entries and patios at lower levels;
- f) Provide a major landmark roof feature on the building located at the north-east corner of the Dover Crossing Neighbourhood (see Landmark sketch);
- g) Create terraced roof forms along River Road by stepping the building stories back from the riverfront (see Terrace Massing sketch).

8.2.3 ARCHITECTURAL ELEMENTS

- a) The waterfront location should be reflected in a marine style of architecture and in the choice of street furniture and lighting standards throughout the neighbourhood;
- b) Create "front stair" connections between a unit's private outdoor space, and the linear walkway it faces, provided that the grade between the two areas is no greater than 1.5 m (4.92 ft.);
- c) Create highly visible and identifiable building entrances through the use of landscape and prominent architectural components to create gateways into the courtyards;
- d) The residential character of the neighbourhood should be expressed through appropriately scaled and proportioned windows;
- e) Use various forms of projections, such as bay windows;
- f) Orient interior spaces as well as primary windows of units towards views of the mountains or the school and park site, rather than directly across the linear walkways and courtyards;
- g) Public and private outdoor space should be clearly defined to enhance the feeling of privacy and the pedestrian experience on the neighbourhood walkways;
- h) Articulate building edges to define private balconies and patios that become a natural extension to the residential unit;

- i) All Development Permit applications shall require evidence in the form of a report and recommendations prepared by a person trained in acoustics and current techniques of noise measurement demonstrating that the noise level in those portions of the dwelling units listed below shall not exceed the noise level is the A-weighted 24-hour equivalent (Leq) sound level and will be defined simply as noise level in decibels.

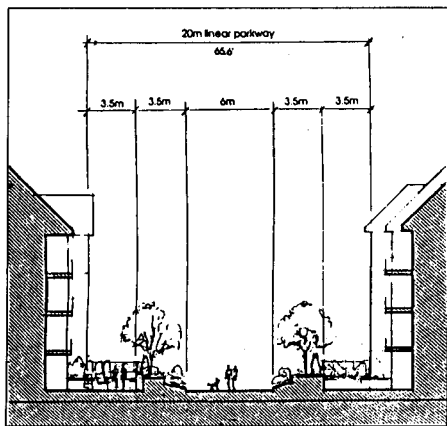
<u>Portions of Dwelling Units</u>	<u>Noise Levels (Decibels)</u>
Bedrooms	35
Living, dining, recreation rooms	40
Kitchen, bathrooms, hallways	45

In addition to the above, the trained professional is to assist in the design of the private patios and balconies to minimize the noise levels with recommendations for building material selection and space planning.

8.2.4 LANDSCAPE ELEMENTS

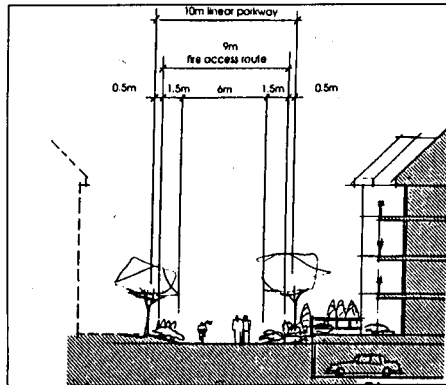
- a) Provide three linear walkways extending from Westminster Highway through to the water. Each of these three walkways will have:
- Focal areas in the form of arrival plazas and entry courts;
 - Will rise gradually up from the south to the north to maximize viewing opportunities to the waterfront and mountains.

The central walkway is the most significant of the three walkways because it will provide a direct connection through the neighbourhood, across River Road to the riverfront and trail system, and the central and east linear walkway will also offer viewing points at their termination at River Road;

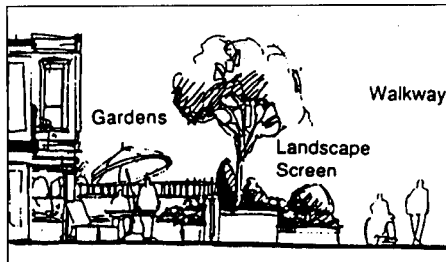


20 m Walkway

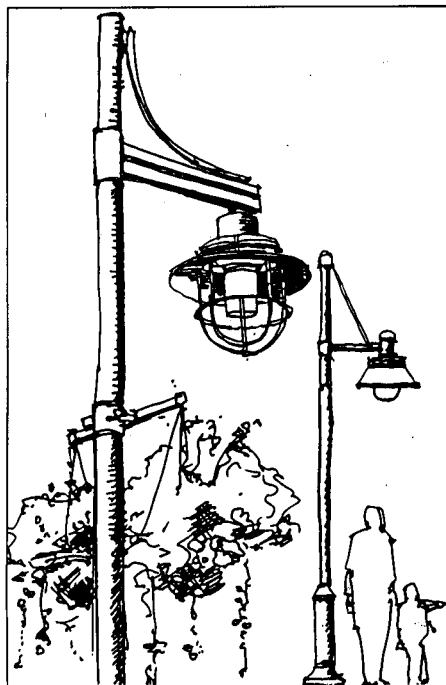
- b) Prominent at-grade crossings accessible by pedestrians, cyclists and people with disabilities will be provided over River Road to provide distinct and safe crossings to the dyke;
- c) An east-west walkway shall also be provided along the south side of River Road to link the three linear walkways to No. 2 Road, as well as to the pedestrian crossings over River Road;
- d) Ensure that the centre linear walkway is a minimum of 20 m (65.62 ft.) in width and is comprised of 6 m (19.69 ft.) of hard walking surface, 3.5 m (11.48 ft.) of low level landscaping, and 3.5 m of terraced planting (see 20 m Walkway sketch);



East and West Linear Walkway



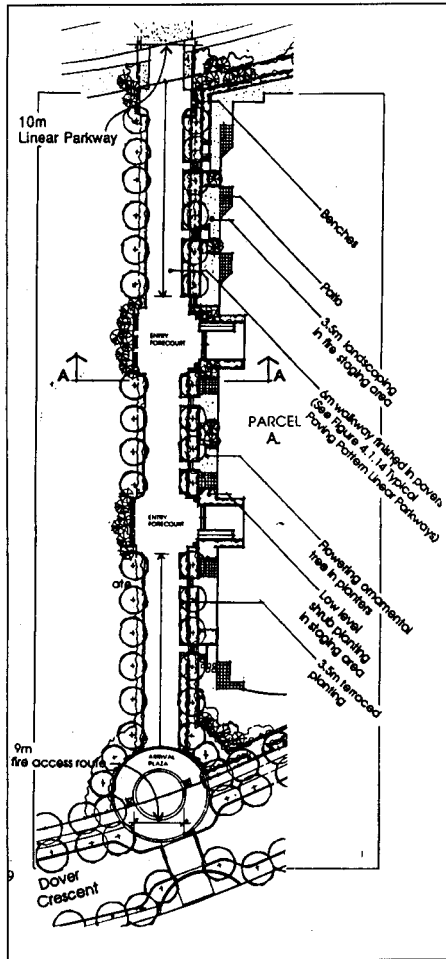
Landscape Private Open Space



Lighting

- e) Ensure that the east and west linear walkways are a minimum of 10 m (32.81 ft.) in width and are comprised of 3 m (9.84 ft.) of hard walking surface, and 3.5 m (11.48 ft.) of planters and low level landscaping (see East and West Linear Walkway sketch);
- f) Provide substantial landscaping, terracing, screening and low level hedges between private ground-oriented outdoor spaces and the public spaces (see Landscape Private Open Space sketch);
- g) Require that all elements and connections in the pedestrian circulation system be accessible by bicycles, and people with disabilities including provisions for the visually impaired;
- h) Use the same decorative and durable walking surface on all the linear walkways, and the pedestrian road crossings throughout the neighbourhood;
- i) Provide textured surfaces as part of the paving patterns to provide a guide route for the visually impaired;
- j) Install low level, possibly wall mounted pedestrian lighting which provides light and security onto the walkways, but does not produce glare into the adjacent residential buildings;
- k) Erect ornamental lights at the focal areas (arrival plazas, entry forecourts, viewing areas) along the entire pedestrian circulation system. These lights should have a maritime design character and include:
 - Post and bracket support system;
 - A pendant fixture with metal hood;
 - A painted metal finish in a maritime colour.

All the lighting standards and street furniture should be finished in a common colour scheme throughout the neighbourhood to enhance the area's special character (see Lighting sketch);
- l) Install directional signage at strategic locations throughout the neighbourhood which provides information relating to the public areas (linear walkways, crossings, school, park, etc.), as well as to the private residences (building entries and facility signs). The signs should be co-ordinated with the design character and location of the lighting standards throughout the entire neighbourhood. The signs should be visible during day time and evening hours;
- m) Plant ornamental, possibly flowering trees along the both sides of the three linear walkways. Trees should have a minimum calliper of 50 mm (1.97 in.), and be spaced at 6.0 m (19.69 ft.) to 7.0 m (22.97 ft.) intervals;



Arrival Plazas and Entry Forecourts

- n) Provide terraced planters along the linear walkways containing plants that create a height gradient separation between the public and private areas. The dimensions of the planters should be at least 1.2 m (3.94 ft.) wide and 0.9 m (2.95 ft.) deep;
- o) Locate the "arrival plazas" (enlarged circular seating areas which are intended to welcome and orient the visitor) at the entrances of the linear walkways from Westminster Highway and Dover Crescent (see Arrival Plazas and Entry Forecourts sketch);
- p) Design the "arrival plazas" to contain the following elements:
 - A widened, hard circular surface with a distinct paving pattern;
 - Bollards to prevent non-essential vehicular traffic;
 - High quality vandal resistant street furniture, including benches and garbage receptacles;
 - Accent planting;
 - Signage and ornamental light standards.
- q) Locate entry forecourts at all major entrances to buildings along the linear walkways (see sketch);
- r) Design the entry forecourts to contain the following elements:
 - A widened, hard circular surface with a distinct paving pattern;
 - Ornamental light standards and signage;
 - Benches at the edge of the space;
 - Bicycle racks;
 - Accent planting;
 - Garbage receptacles.

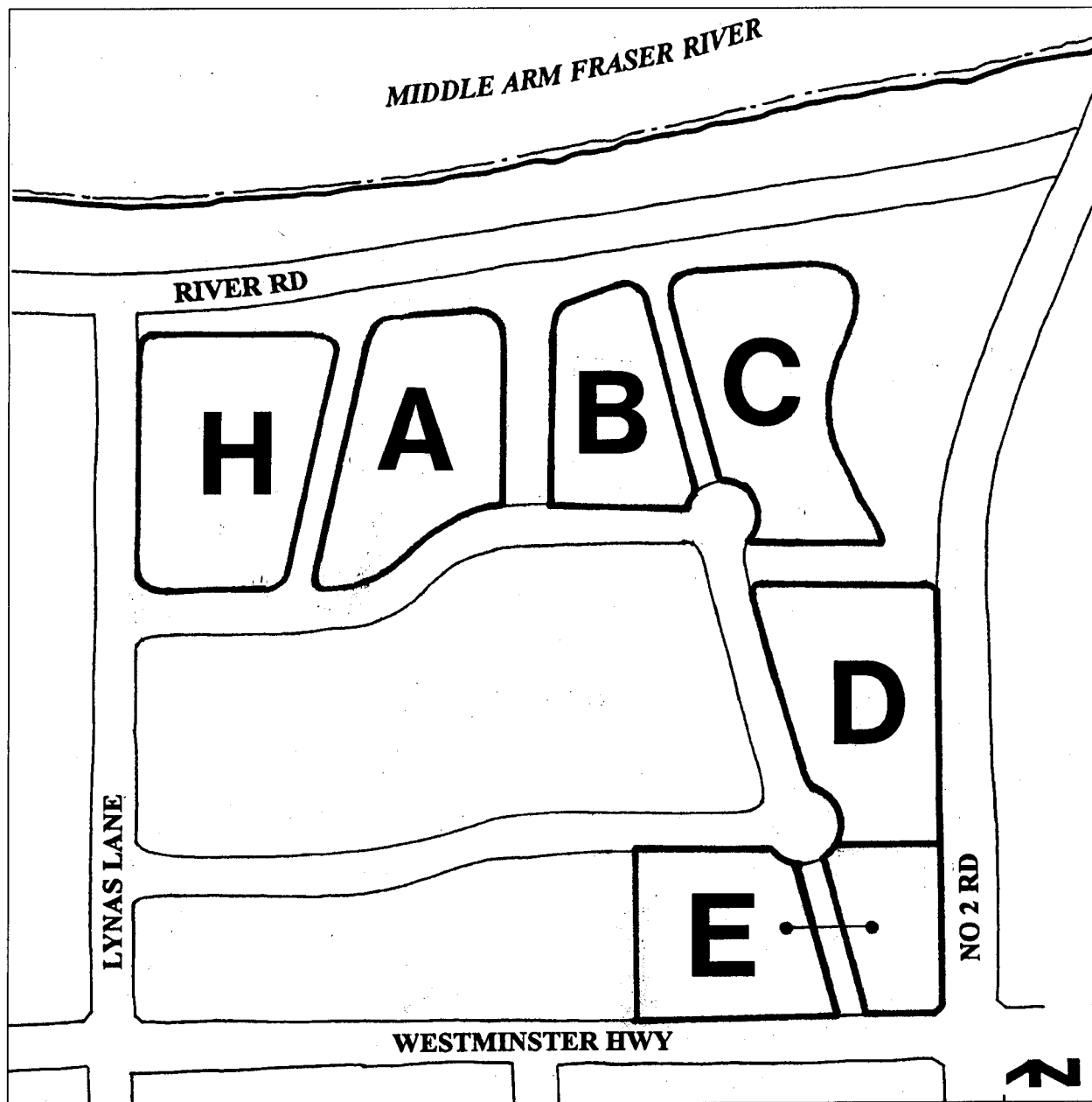


Linear Walkway

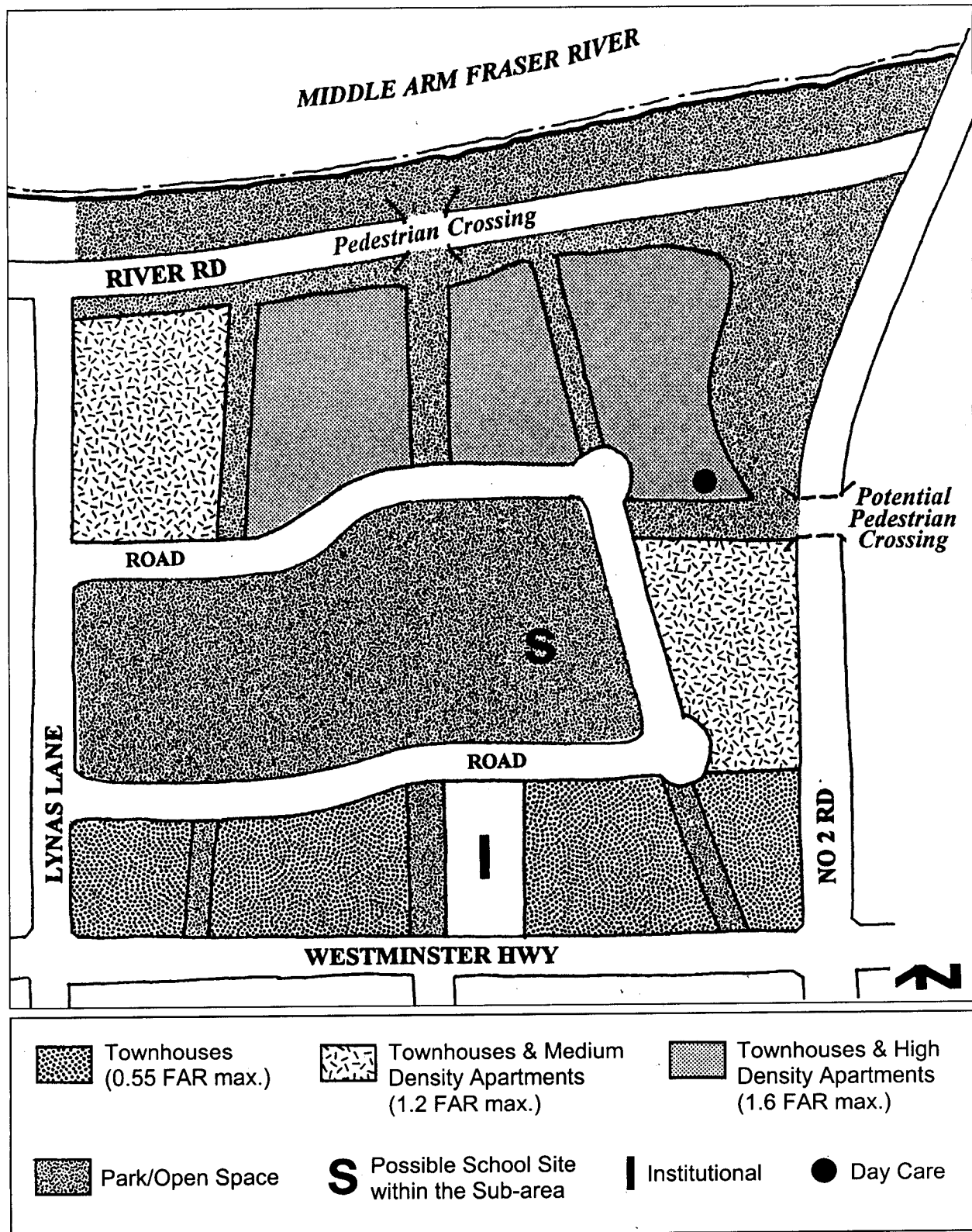
8.2.5 SITE SPECIFIC GUIDELINES AND MAP

Note: There are additional Detailed Character Guidelines for the specific development sites as shown on the following page. The guidelines form part of the Official Community Plan Bylaw 7100, Schedule 2.2A and are available at the Urban Development Division.

Location of Development Sites with Site Specific Guidelines Map



Land Use Map



Richmond Official Community Plan

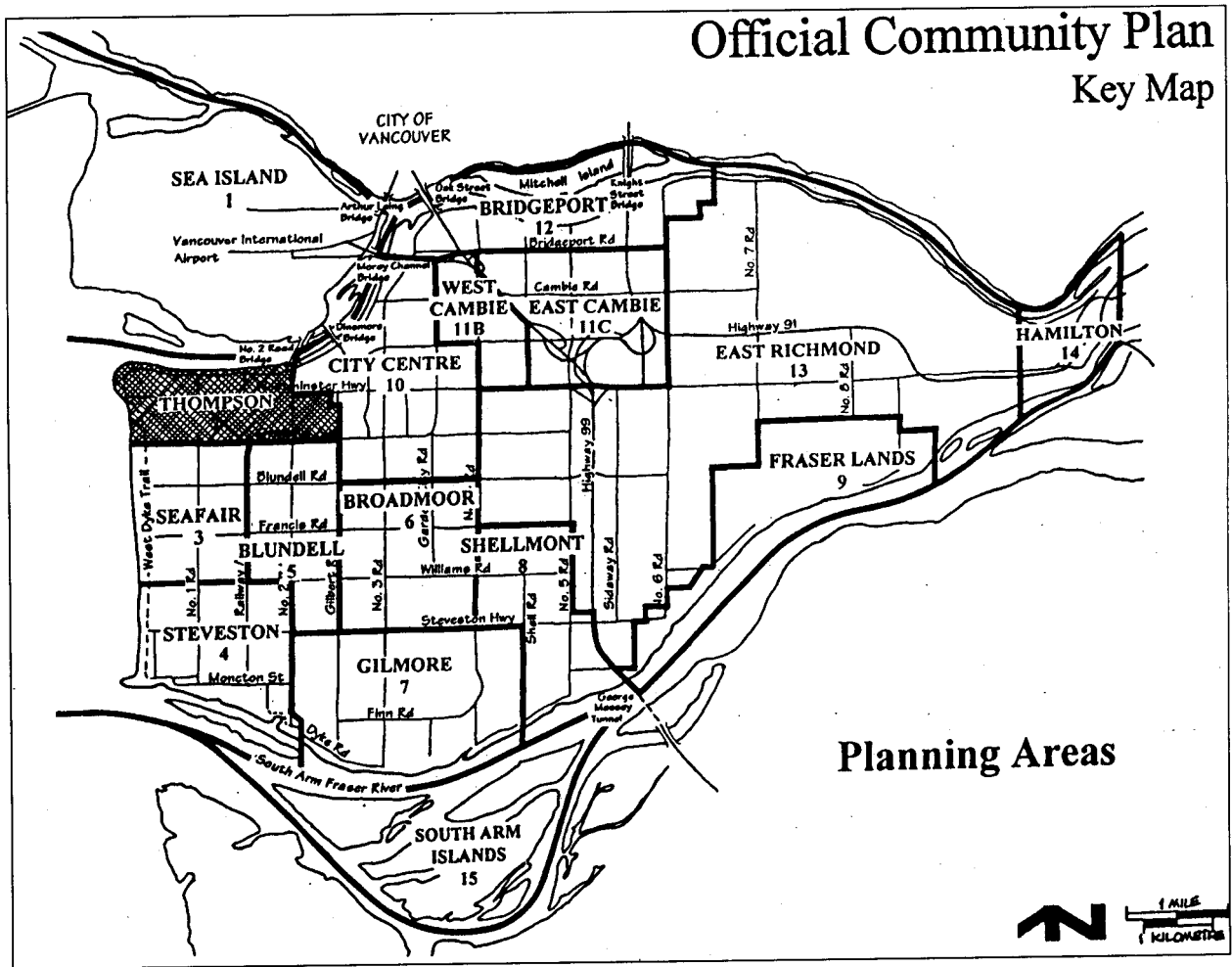


City of Richmond

THOMPSON AREA TERRA NOVA SUB-AREA PLAN Bylaw 7100 Schedule 2.2B



KEY MAP



PLAN AREA MAP

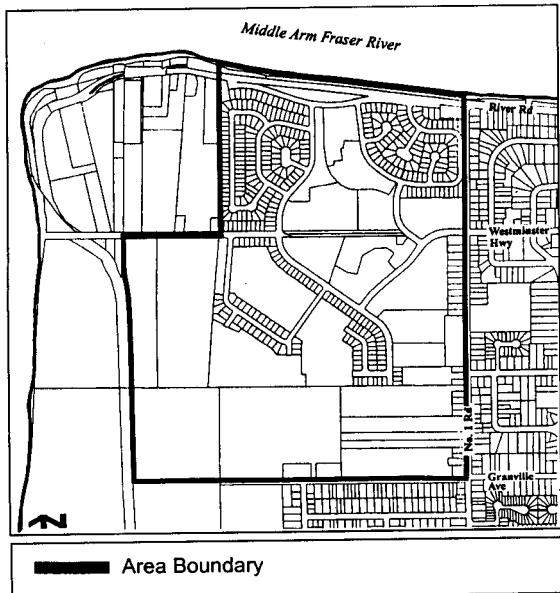


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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City's social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

This plan provides a series of goals, objectives, policies and guidelines consistent with the development of the Terra Nova Sub-Area as a residential neighbourhood adjacent to sensitive environmental areas and significant natural amenities.

The plan area currently consists of approximately 112 ha (276.75 ac.) of dyked floodplain in the north-west portion of Lulu Island, occupied by single-family residences, townhouses, fallow agricultural land, small commercial premises, a golf course and City park land. The extent of the plan area is noted on the Plan Area Map.

1.2 GOALS

The goals of this plan are to:

- Provide public access to the waterfront;
- Preserve significant natural areas;
- Preserve and enhance recreational facilities;
- Provide a range of residential household sizes and types at a modest density;
- Enhance and preserve air, water and soil quality;
- To provide safe and efficient means for pedestrians, cyclists and vehicles to circulate through the area.

2.0 JOBS & BUSINESS



Commercial Services

OBJECTIVE 1:

To maintain local commercial services in the Terra Nova Area.

POLICIES:

- a) Permit the establishment of commercial uses on lands designated "Commercial" on the Land Use Map;
- b) Require the issuance of a Development Permit prior to the issuance of a building permit for any commercial buildings or structures in the plan area;
- c) Require that Development Permits issued for commercial uses comply with the following guidelines:
 - Commercial uses shall be screened from residential uses by means of walls, solid fences and landscaping;
 - Landscaping shall be provided adjacent to public roads, lanes and walkways;
 - Buildings and structures shall be sited in a manner that would minimize the impact of the commercial uses on neighbouring residences.

OBJECTIVE 2:

To maintain the agricultural land base in the area.

POLICIES:

- a) Maintain lands presently designated as "Agricultural Land Reserve" as part of the agricultural land base of Richmond;
- b) Permit open land recreational uses in areas designated "Agriculture" on the Land Use Map, subject to the Agricultural Land Commission Act;
- c) Establish buffers along the urban/rural boundary consistent with the OCP.

3.0 NEIGHBOURHOODS & HOUSING

ISSUE:

The Terra Nova neighbourhood is in close proximity to the Vancouver International Airport and therefore parts of the area are subject to a noise level of 30 NEF or above.

OBJECTIVE 1:

To provide ground-oriented housing in a range of sizes and forms to a range of household types.

POLICIES:

- a) Permit the establishment of single-family residences on those lands designated "Residential (Single-Family)" on the Land Use Map;
- b) Permit the establishment of townhouses and small-lot single-family residences on those lands designated "Residential (Townhouse)" on the Land Use Map;
- c) Permit the establishment of new housing only with the full range of urban utilities and services;
- d) Encourage and facilitate the provision of affordable housing;
- e) Require the issuance of a Development Permit prior to the construction of any townhouses in the plan area;



New Single-Family Residences

ISSUE:

The Terra Nova neighbourhood is in close proximity to the Vancouver International Airport and therefore parts of the area are subject to a noise level of 30 NEF or above.¹

OBJECTIVE 2:

To ensure that new building is in accordance with standards and agreements.

- a) Require builders of new residential buildings to comply with Canada Mortgage and Housing Corporation's guidelines for noise mitigation;
- b) Through rezoning and subdivision applications, require restrictive covenants to hire professionals qualified in acoustics to determine and implement noise mitigation measures.

¹ The Noise Exposure Forecast (NEF) is the official metric used in Canada for airport noise assessment. The NEF contours are calculated (or modelled) by time-averaging aircraft operations considering aircraft fleet mix, flight distances, runway utilization, flight path, and time of day as night-time operations are more heavily weighted. The NEF was designed to encourage compatible land uses in the airport vicinity and to predict human annoyance to airport operations within the noise contours.

4.0 TRANSPORTATION

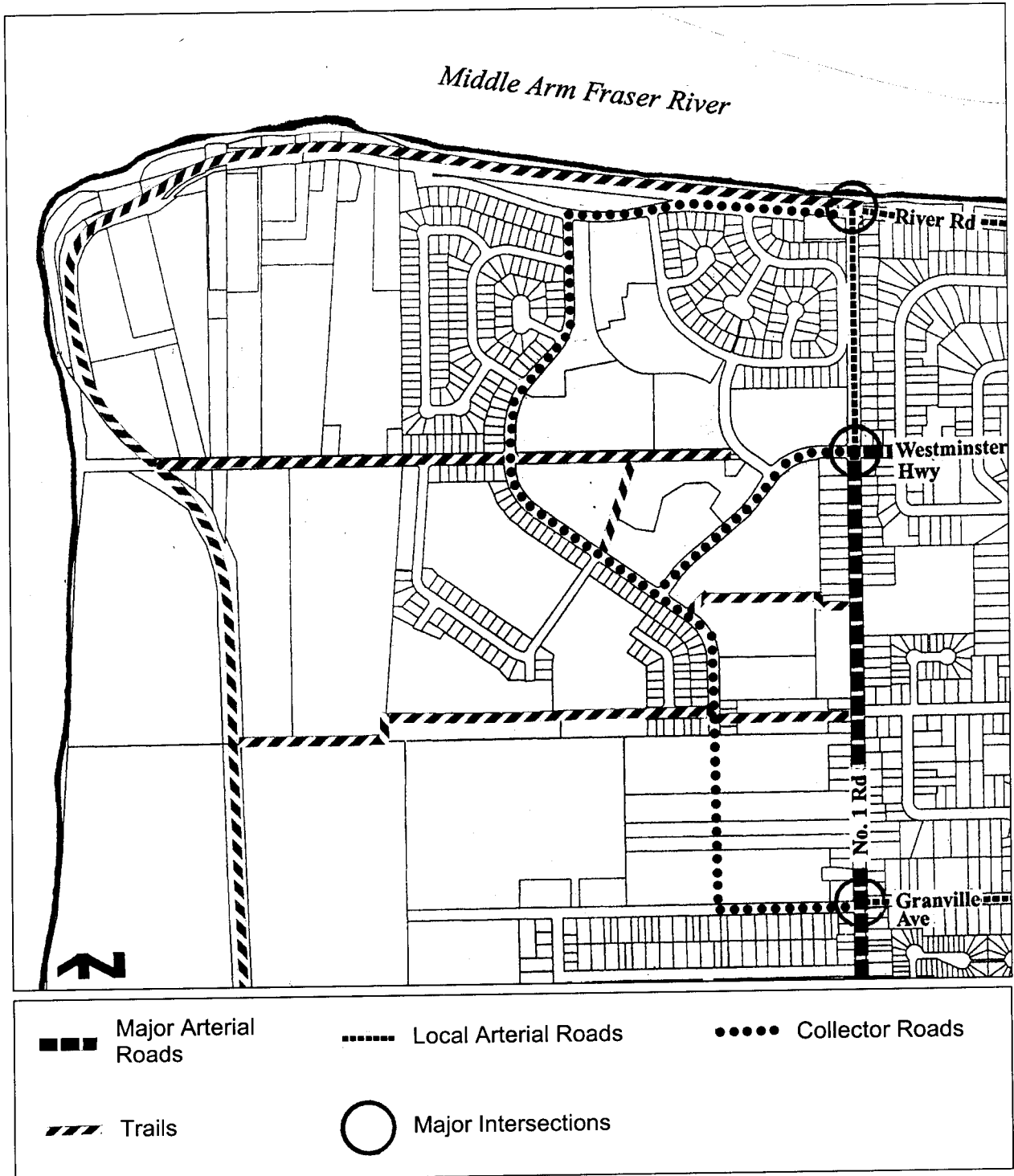
OBJECTIVE 1:

To provide a safe and efficient means of access for pedestrians, cyclists and vehicles through and around the area.

POLICIES:

- a) Restrict direct access to the area from No. 1 Road;
- b) Provide a hierarchy of roads and trails as indicated on the Circulation System Map;
- c) Provide a 20 m (65.617 ft.) wide road allowance where a trail is to be provided in a new local street in order that the boulevard of said street can be landscaped with trees between the curb and the sidewalk;
- d) Accommodate pedestrians, cyclists and vehicles on all roads within the plan area.

Circulation System Map



5.0 NATURAL & HUMAN ENVIRONMENT

OBJECTIVE 1:

To provide a variety of open spaces and public facilities to serve the neighbourhood and the city as a whole.

POLICIES

- a) Provide an open space between River Road and the Fraser River to enhance the dyke trail system upon which buildings will not be erected;
- b) Establish a childcare facility on the central open space within the plan area;
- c) Preserve and enhance the western open space along the perimeter dyke as natural wildlife habitat;
- d) Provide for the active and passive recreational needs of the plan area within the central open space and a smaller park in the south portion of the plan area;
- e) Permit the establishment of a public elementary school within the central open space, if warranted;
- f) Protect archaeological resources within the plan area;
- g) Designate land as public, institutional and open space as indicated on the Land Use Map.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated for multiple-family residential and commercial use on the Terra Nova Land Use Map.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Terra Nova Area.

It is intended that these guidelines be used in conjunction with the City's more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

Residential

A key housing policy of Richmond's Official Community Plan is to carefully integrate new housing development into existing neighbourhoods. All multiple-family projects merit site-by-site consideration of form and character in order to achieve the desired relationship to surrounding areas, and to ensure high standards of new housing design.

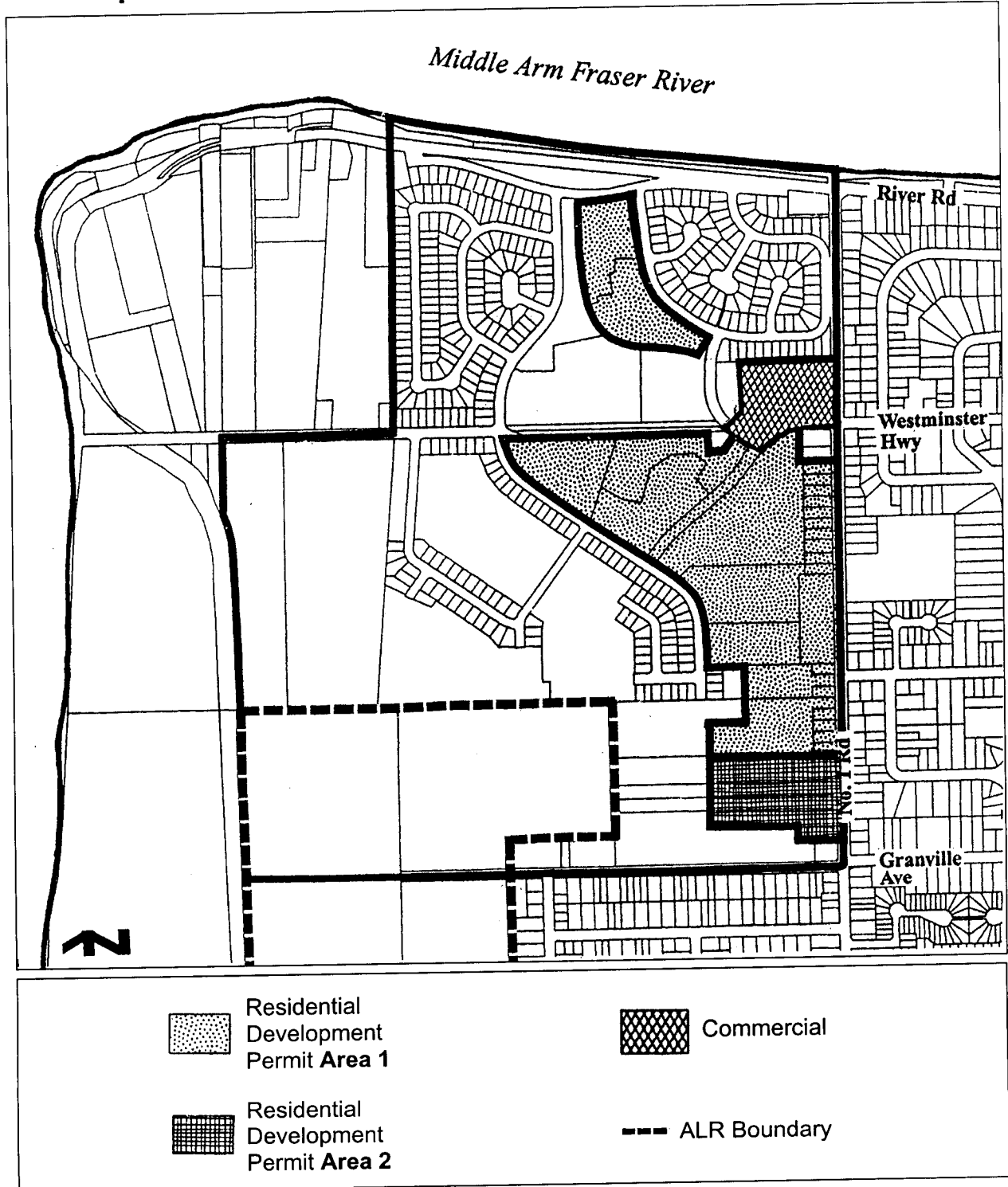
Terra Nova in particular requires special consideration. The neighbourhood's location, traffic considerations, and significant redevelopment potential present issues which need to be addressed through design guidelines for both multiple-family and commercial sites.

Commercial

The special conditions that justify the need for additional development permit guidelines for the commercial site are as follows:

- a) That an appropriate, high quality built form is developed at this important gateway location to the Terra Nova Planning area;
- b) That the single-family neighbourhoods to the north and east be protected by ensuring that the future commercial development blends with the character and scale of the surrounding area, and that sufficient buffers are provided between uses;
- c) That an attractive and accessible pedestrian environment be created which provides a continuation of the Terra Nova Trail system and helps ensure a blending of the private and public realms;
- d) That the development adds to the overall beautification of the City through the provision of special features in focal areas such as plazas, water fountains and the greening of streets, and parking areas;
- e) That the materials applied to the buildings, and the type of street furniture, landscaping, signage, and light standards erected in the open spaces are coordinated to form a distinct development.

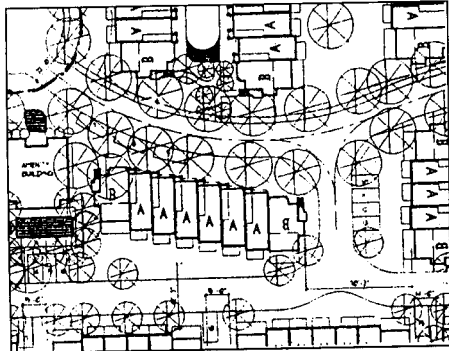
Development Permit Areas Map



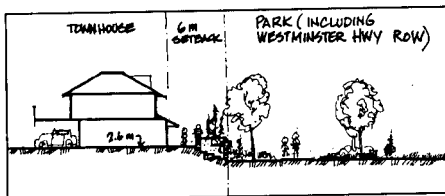
8.2 DEVELOPMENT PERMIT GUIDELINES

Area 1: Residential Development

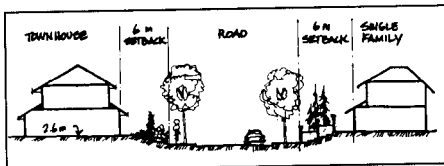
The following guidelines apply to residential development as shown on the Development Permit Areas map in addition to the Guidelines located in the OCP (Schedule 1 to this Bylaw).



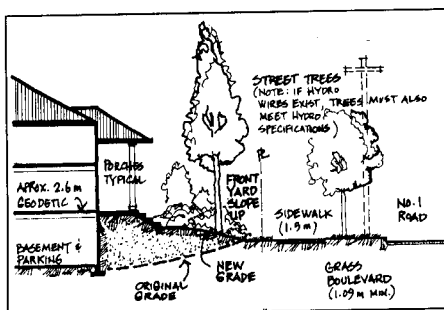
Open Space Separation



Townhouses adjacent to Public Open Space



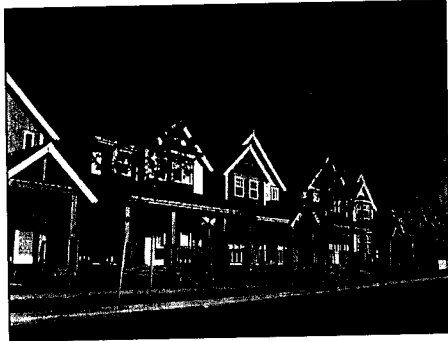
Townhouses adjacent to Public Roads



No. 1 Road

8.2.1 SETTLEMENT PATTERNS

- a) Townhouse units should be designed in clusters of 25 units or less, and have less than six units in a row. However, exceptions can be made to allow up to eight units in a row IF larger clusters are separated from other clusters on all sides by a considerable amount of open space (or combination of open space and internal roadways). (See Open Space Separation Sketch);
- b) Entrances should enhance the pedestrian-scale character of the area, by strengthening a unit's connection with public streets and internal roadways; and by masking the appearance of garage doors;
- c) Townhouses adjacent to public roads and trails should have their main front doors visible and accessible from that road or trail (see sketch showing Townhouses Adjacent to Public Open Space and Roads), using stairs and new grading if required to do so (see No. 1 Road Sketch). Direct access for each unit should also be provided to the internal north-south pedestrian and view corridors;
- d) Special views, such as of the mountains or waterfront, should be taken advantage of where possible;
- e) Sunlight should not be blocked for a minimum of 75% of dwellings in each development on every day of the year. The minimum north-south spacing between residences to conserve available sunlight is determined by the sun angle of 17° at noon during the winter solstice;
- f) Use internal roadways to maximize the sense of open space and enhance the area's pedestrian character. Where possible, vehicle and pedestrian access should be separated from each other and appropriately located;
- g) In Residential Area 2, include a semi-public walkway with lights and low landscaping. The walkway should be ungated, and connect to No. 1 Road and to the new road to the east. This corridor on the north property line, should eventually be matched by a similar corridor on the adjacent site;
- h) There should be no vehicle gates at entrances to the site.



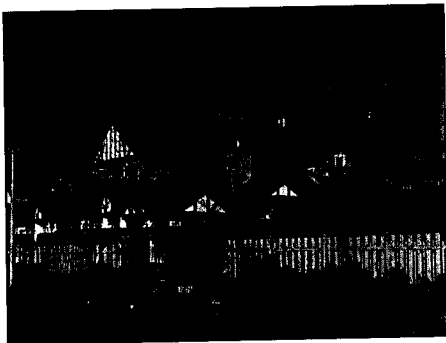
Townhouses with Front Doors Visible and Accessible from the Road

8.2.2 MASSING AND HEIGHT

- a) Provide a smooth transition between townhouse units and single-family homes to the west. Build duplexes (structures with no more than two attached units together) along the western property line. A space of at least 3 m (9.8 ft.) should separate each duplex structure along the street front;
- b) Townhouse units should have a maximum transitional height gradient of 26 to all property lines. These gradients may be varied provided privacy, sunlight, view and human scale criteria are met. Buildings should have an apparent length of less than 70 m (229.66 ft.);
- c) All roofs should be pitched, with a minimum slope of 45 . Use gable ends and dormers to create diversity in the roof line and to present the area's craftsman character.

8.2.3 ARCHITECTURAL ELEMENTS

- a) Avoid the appearance of blank walls. Suggested treatment includes (but is not limited to) the use of: trim; more intense colour accents; secondary roof elements; building recesses; and stepped building forms emphasizing lowered height on ends and fronts;
- b) Select roofing materials which suit the level of articulation desired in roof forms. Cedar shingles would facilitate this, whereas heavy-tiled roofing would not;
- c) Windows should be operable, of residential scale, with multiple panels, and should add to the sense of neighbourliness and safety and security by providing visual interest and surveillance:
 - Reinforce the smaller components of buildings which express strong unit identity through appropriate window placement;
 - Use bay windows, window boxes, small balconies, and similar features to avoid a flat, box-like building;
 - Windows should be visually prominent, separated from the building face and defined with colour, trim/shutters, or other decoration;
 - Non-traditional windows such as plastic bubble windows or tinted glaze are discouraged. If skylights are used, they should not be visible from the street or from internal roadways;
- d) Reduce the visual impact of garages along internal roads and driveways by interrupting garage doors with covered secondary pedestrian entrances (see Sketch). Pedestrian entrances should occupy approximately half the width of each garage door;



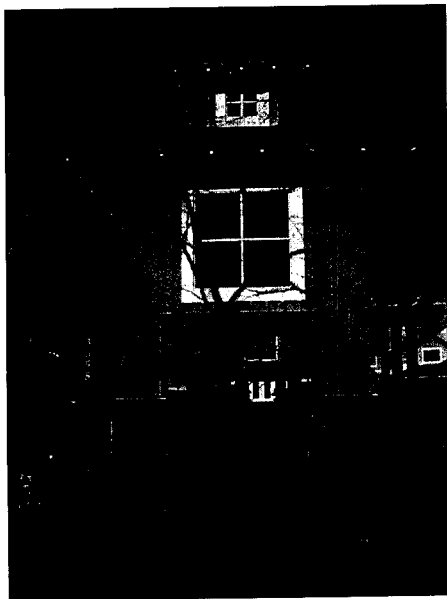
Use of Gable Ends and Dormers



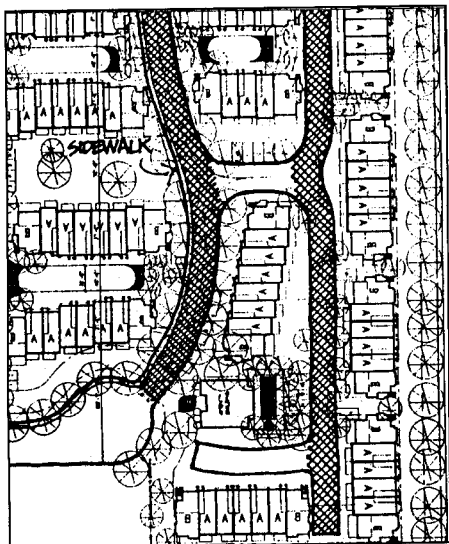
New Townhouse Development showing use of Trim, Colour Accents and Secondary Roof Elements



Garages along Internal Roads and Driveways



Entrance Gateway



North-South View Corridors

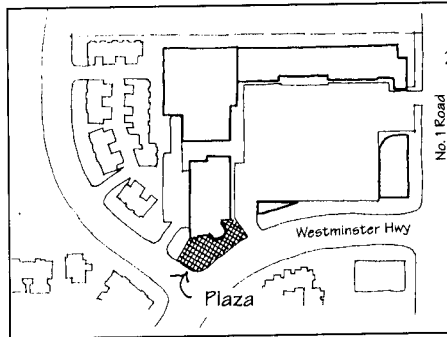
- e) A minimum of 75% of the units should have single garage doors. Tandem parking is encouraged.

8.2.4 LANDSCAPE ELEMENTS

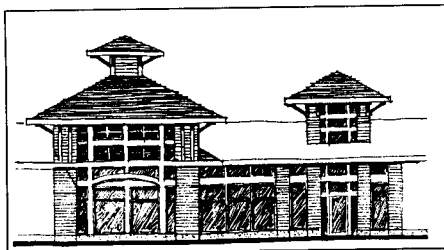
- a) Where stairs are used to provide street or trail access from units, embellish stairs with decorative wooden handrails;
- b) Screening and landscaping, including street trees, should be provided in each townhouse development;
- c) Townhouse units should have a minimum private outdoor space of 9 m (29.53 ft.) in depth and 37 m² (398.28 ft²) in area. The depth of private outdoor space may be reduced to 5.25 m (17.22 ft.) where adequate privacy screening is provided;
- d) Along the public street right-of-way, provide sidewalks on both sides of the street. Make these sidewalks attractive by providing a grass strip with street trees between the sidewalk and the street, and placing all utilities underground;
- e) Avoid the appearance of a solid wall of buildings along No. 1 Road, by varying street edge treatment through use of strategically placed open spaces with intensely clustered plantings; staggered or varied building setbacks; and other techniques;
- f) Create at least one north-south view corridor and pedestrian connection through a central boulevard running north-south through each development site. Treat the boulevard as if it were a public street, by planting large calliper trees on both sides, and providing a sidewalk on at least one side (see North-South View Corridors Sketch);
- g) Vary paving treatments through the use of special pavers, cobblestones or stamped concrete as accents at driveway entrances;
- h) Enhance pedestrian, wheelchair and cycling access to No. 1 Road by providing east-west corridors for non-automotive users in strategic locations;
- i) Landscape all pedestrian paths with trees and other plant materials.

8.2.5 PARKING AND SERVICES

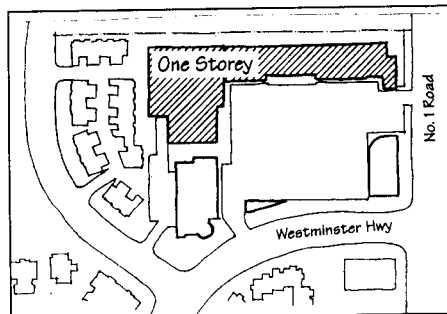
- a) Access provision should be made for emergency vehicles, moving vans, and service vehicles.



Plaza



Massing



Building Heights

Area 2: Commercial Development

The following guidelines apply to commercial development as shown on the Development Permit Areas map in addition to the Guidelines located in Schedule 1 to this Bylaw.

8.2.6 SETTLEMENT PATTERNS

- a) Locate a 595 m² (6,440 ft²) plaza at the south-west corner of the shopping centre extending from the building entry to the curb. (See Plaza Sketch);
- b) Develop an entry feature at the corner of No. 1 Road and Westminster Highway that provides a focal point to the shopping centre as well as a gateway to the Terra Nova neighbourhood. Although not limited to, suggested entry features include:
 - A water feature with accent planting and a Terra Nova Neighbourhood sign. The sign should be designed to reflect the northwest style of architecture used on the buildings. If the sign is lit, it should be done so in a manner that creatively and positively contributes to the nighttime impression of the area;
- c) Articulate the facades of the buildings on all four sides to create the appearance of a series of small scale commercial units that each have their own identity and interest without sacrificing the overall visual compatibility of the development. (See Massing Sketch);
- d) Ensure that the commercial units located in the northern portion of the site do not exceed 9 m (29.5 ft) in height or contain more than 1 storey with the exception of special entry features which should not exceed 10.21 m (33.5 ft.) in height. (See Building Heights Sketch);
- e) Add interest by constructing pitched roof forms at all public facades (i.e. facing No. 1 Road, Westminster Highway and the principal parking areas), except where otherwise concealed by landscaping.

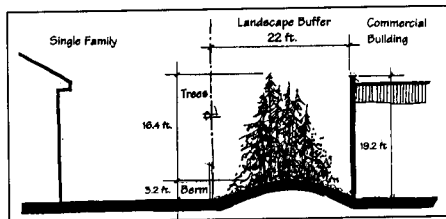
8.2.7 ARCHITECTURAL ELEMENTS

- a) Design individual store entrances in a highly visible manner through the use of landscaping and architectural features such as recesses and projections;
- b) Insulate the buildings along the northern property line to help minimize noise;

- c) Establish a northwest style of architecture and a sense of quality and solidity through the use of natural building material. Brick, stone, and wood siding all express a sense of quality and permanence. Stucco should only be used in combination with other natural materials, and should not be used within 1 m (3.28 ft.) of the ground. Stucco should be treated to prevent discoloration and particular care should be taken in the detailing of north facing facades;
- d) Ensure that a variety of complementary colours are used on the buildings and street furniture;
- e) Conceal all mechanical equipment within the sloped roofs;
- f) Limit commercial signage that is well integrated into the building designs to:
 - Facade signs comprised on letters and logos affixed to the building, or in internally illuminated boxes - the latter to be limited to sign bands immediately above the main floor and second floor windows/doors or copy on awnings, and to consist of light-coloured or white text on a dark background;
 - Freestanding signs limited to directional signs and signs indicating the name and general nature of the services, to a maximum of 2 m (6.56 ft.) in height;

(These guidelines for commercial signage are to be used in addition to the Richmond Sign Bylaw requirements.)

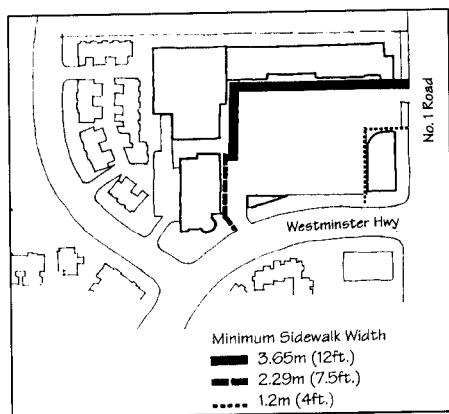
- g) Providing continuous weather protection over the store fronts by including either overhangs, canopies or awnings that are of a sufficient depth (minimum 2 m) to shelter outdoor displays, seating and walking areas on all the internal walkways except for the single building located at the corner of No. 1 Road and Westminster Highway.



Landscape Buffer

8.2.8 LANDSCAPE ELEMENTS

- a) Provide a minimum 6.7 m (22 ft) wide landscaped buffer between the commercial buildings and the backyards of the single-family homes (see Building Setback Plan accompanying Comprehensive Development District (CD/65) zone). A total of 111 conifers including Douglas Fir and Pine should be planted within a 1 m (3 ft) high berm. Sixty of these 111 trees are currently located on the site and should be relocated into the buffer area. The remaining 51 trees should be 5 m (16.4 ft) high at the time of planting. (See Landscape Buffer Sketch);
- b) Plant climbing vines on the rear exterior walls of the buildings located in the northern portion of the site;
- c) Provide a continuous sidewalk between the store fronts and the parking lot. (See Continuous Walkway Sketch);



Continuous Walkway

- d) Enhance the pedestrian experience along the internal sidewalk by:
 - Providing low level landscaping planters along the building edge;
 - Incorporating low-level dog tying hitches to the planter boxes;
 - Using a special decorative paving treatment on the internal walkway sidewalk;
 - Expanding the width of the walkway by 3 m (9.84 ft.) into the parking area where feasible in order to provide seating areas in front of commercial units envisioned for restaurant use;
 - Providing benches, garbage containers and bicycle racks along the sidewalk at major building entries finished in a colour scheme complimentary to the building (The bicycle racks should be located within 15.0 m (49.02 ft.) of a principal building entry. The bicycle racks should be constructed of sturdy, theft-resistant material, securely anchored to the ground. The racks should be designed to support the bicycle frame, not just the wheels, and allow for both the frame and the front wheel to be locked to rack with an U-style lock);
 - Connecting the internal sidewalk to the perimeter trail system;
 - Ensuring that the sidewalks and thresholds are accessible to disabled people;
- e) Design the plaza to contain the following elements:
 - A widened hard circular surface with a distinct paving pattern measuring at least 10.9 m (36 ft.) in diameter;
 - Benches and garbage containers;
 - Accent planting;
 - Decorative lighting;
 - A water feature;

- f) Construct a 6 m (19.6 ft.) wide trail along the southern perimeter of the site and a 5 m (16.4 ft.) wide trail along the eastern perimeter of the site capable of accommodating pedestrians and recreational cyclists. The southern trail should consist of 3 m (9.84 ft.) of hard walking surface located between a double row of street trees planted in two 1.5 m wide grassed boulevards. The eastern perimeter trail should consist of 2 m (6.56 ft.) of hard walking surface located between a double row of street trees planted in two 1.5 m grassed boulevards. The street trees should have a 8 cm (3.14 in.) calliper at the time of planting and be spaced no more than 7 m to 9 m apart. Honey Locust trees should be planted along the No. 1 Road trail and Ash trees should be planted along the Westminster trail. (See Perimeter Trail Sketch);
- g) Construct the walking surface of the trail with a decorative paving material;
- h) Use a highly textured paving material and raised crossings at driveways to alert pedestrians and motorists of crossings;
- i) Erect automatic gates at delivery entrance driveways to help ensure pedestrian safety along the trail;
- j) Include the curb cuts at the driveway entrances within the boulevards so that the sidewalks remain level;
- k) Erect low masonry walls or hedges along the perimeter of the site only if they do not exceed 1 m (3.28 ft.) in height measured at the highest elevation of the trail, and pedestrian access to shopping centre is not impaired at the corner of No. 1 Road and Westminster Highway;
- l) Soften the parking lot by planting 1 tree per every 3 parking spots so that after approximately 10 years after planting, at least 70 % of the parking lot will be covered by a canopy of leaves in the summer;
- m) Provide decorative lighting in the parking lot that is capable of accommodating hanging flower baskets with irrigation and/or banners. The colour of the lighting standards should be coordinated with the colour of the street furnishings throughout the development. Lighting should not exceed 3 foot candles at the north, west and south property lines;
- n) Screen the parked cars from the perimeter trail without compromising safety and surveillance by providing a slight change in grade and low shrubs at the edge of the parking lot. (See Screen Parking Sketch);

- o) Prepare a tree survey and hire an arborist to determine which of the existing trees on the site (in addition to the conifers used in the northern buffer area) can be retained or relocated on the site;
- p) Erect protective fencing around the trees that are to be retained before commencing with construction. The fencing should be shown on the Landscape Plans.

8.2.9 PARKING AND SERVICES

- a) Locate the garbage and recycling containers as well as utility metres in areas which do not visually or physically conflict with pedestrian traffic along the pedestrian circulation system. The garbage and recycling containers should be contained within an enclosed building;
- b) Provide on-site amenities such as an employee lunch room or lounge, employee locker and shower room, public washrooms, and bicycle parking and end-of-trip facilities;
- c) The bicycle parking and end of trip facilities should include the following:

- Parking facilities shall: be at-grade, have uniform 160 lux (min.) lighting which yields true colours; and, be within sight of building entry, elevator, and/or security;
- Bicycle rooms should provide: lockable door(s) with window(s); tamper proof, motion-activated security lighting; and unobstructed view of each room from its entry; and, facilities for no more than 20 bicycles per room (enabling owners to identify one another);
- Bicycle lockers shall: be constructed of solid, opaque, weather-proof and theft-resistant material, with no exposed fittings or connectors; have lockable doors which open to full height and width of each locker; be grouped together; not be located at the head of parking spaces; and, have clear minimum dimensions of:

Length	180 m (5.91 ft.)
End of Width at Door	0.60 m (1.97 ft.)
End of Width Opposite Door	0.22 m (0.72 ft.)
Height	1.20 m (3.94 ft.)

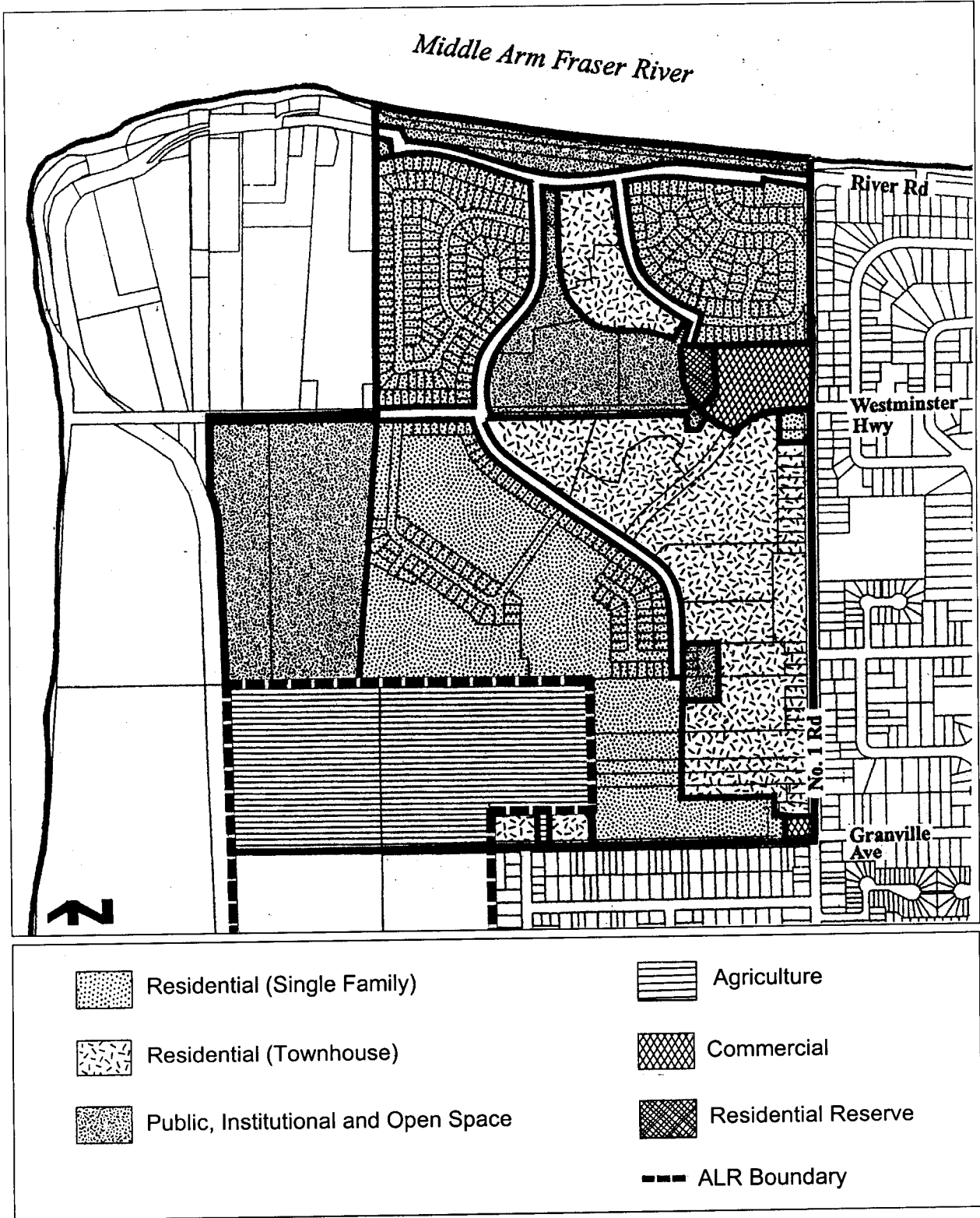
ALR Boundary

The following guidelines apply to development adjacent to the ALR boundary as shown on the Development Permit Areas Map in addition to the guidelines located in the OCP.

8.2.10 SETTLEMENT PATTERNS

- a) For development adjacent to sites within the ALR boundary a landscaped buffer 15 m (49.2 ft.) wide must be provided between the development and the agricultural land.

Land Use Map



Richmond Official Community Plan

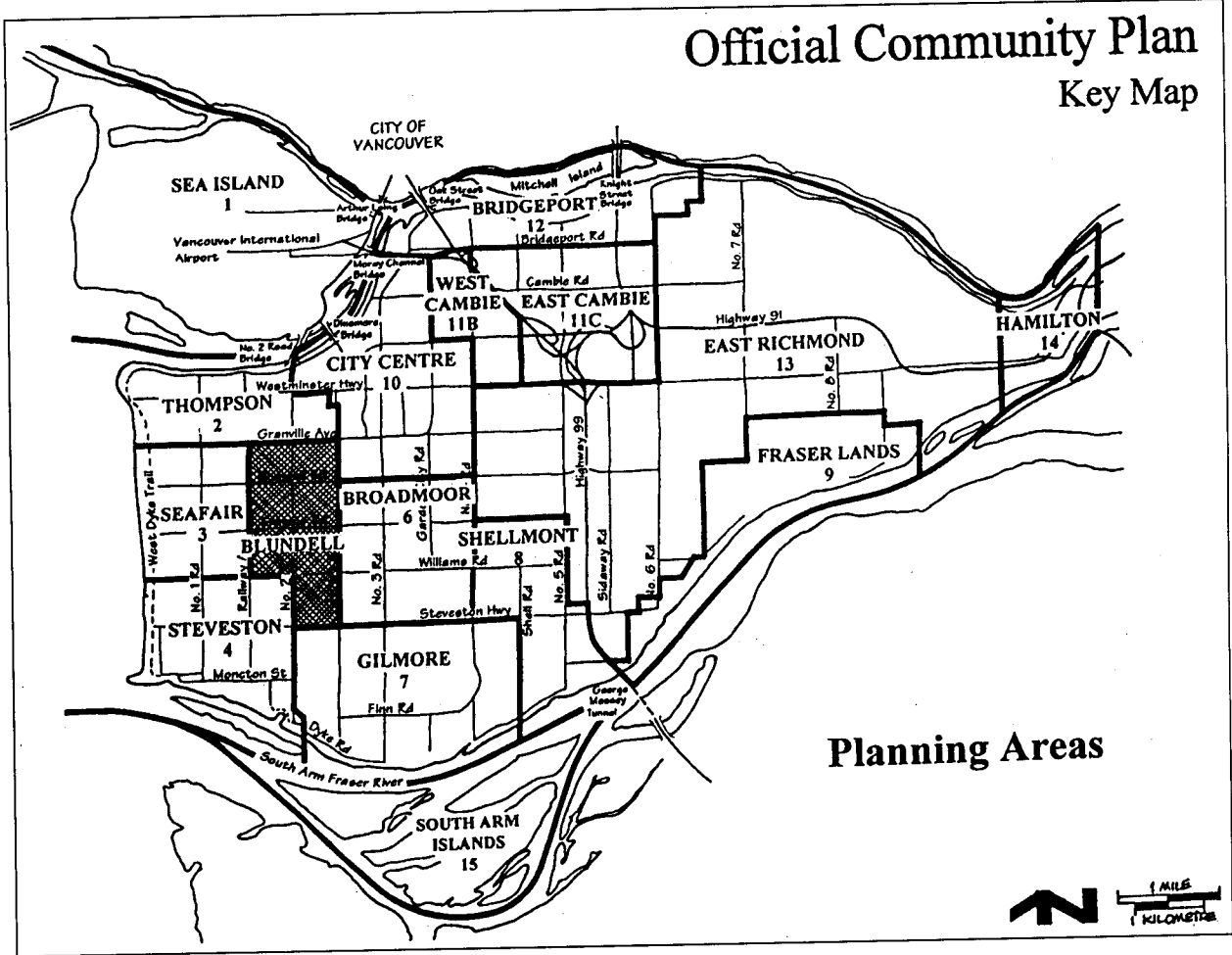


City of Richmond

BLUNDELL AREA LAURELWOOD SUB-AREA PLAN Bylaw 7100 Schedule 2.5A



KEY MAP



PLAN AREA MAP

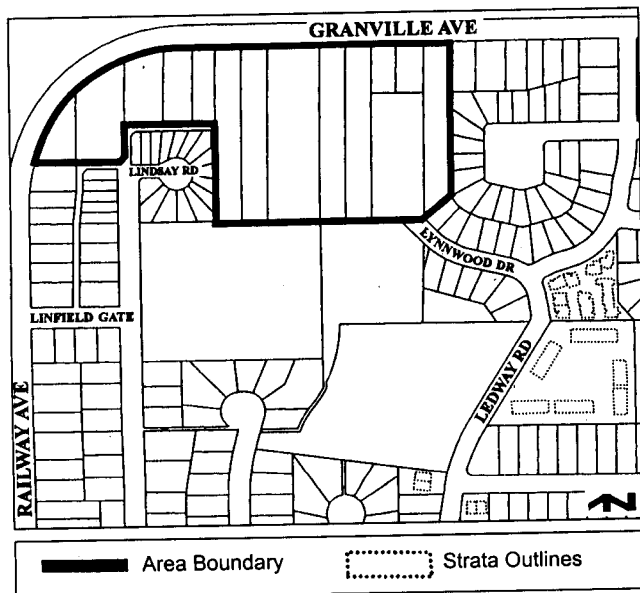


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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City's social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

This plan provides goals, policies and guidelines consistent with the development of the Laurelwood Sub-Area as a neighbourhood adjacent to a major arterial road, near the Granville Avenue/Railway Avenue bend.

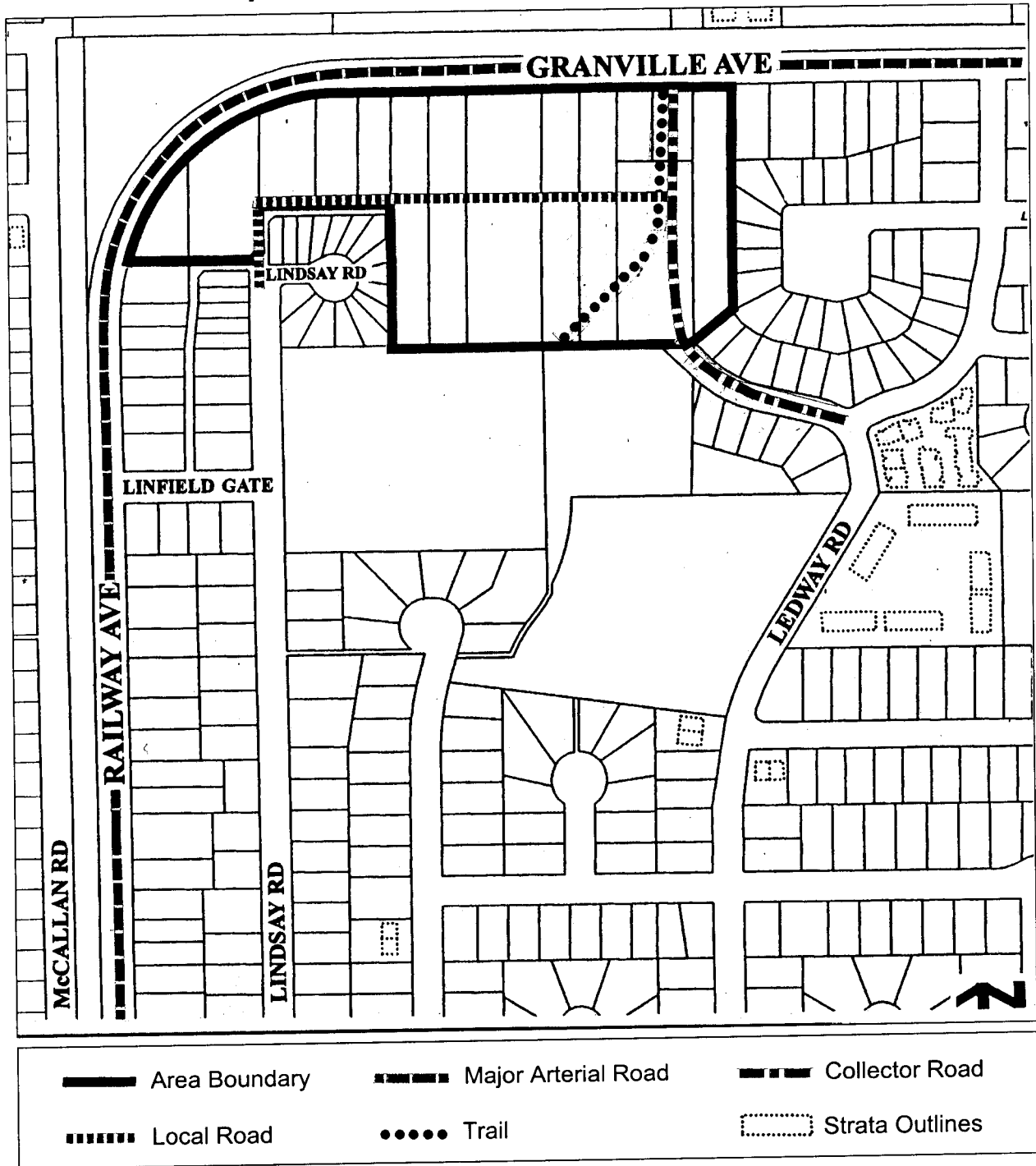
The area to the south and east is a mix of single-family homes on a variety of lots sizes, three-storey apartments, an elementary school and park space. To the north, across Granville Avenue, there is a secondary school, community park and community centre.

1.2 GOALS

The goals of this plan are to:

- Provide a range of residential household sizes and types at a modest density;
- Provide safe and efficient means for pedestrians, cyclists and vehicles to circulate to facilities, parks and arterial roads in the area.

Circulation Map



2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING



New Townhouses

POLICIES:

- a) Permit the establishment of single-family residences on lands designated “Residential (Single-Family)” on the Land Use Map;
- b) Permit the establishment of townhouses on those lands designated “Residential (Townhouse)” on the Land Use Map;
- c) Permit the establishment of new housing only with the full range of urban utilities, street access, and services;
- d) Encourage and facilitate the provision of affordable housing;
- e) Require the issuance of a Development Permit prior to construction of any new townhouses in the plan area.

4.0 TRANSPORTATION



Pedestrian Circulation

POLICIES:

- a) Restrict direct access from individual lots to Granville and Railway Avenues;
- b) Provide a hierarchy of roads and trails as indicated on the Circulation Map.

5.0 NATURAL & HUMAN ENVIRONMENT

POLICIES:

- a) Designate a minimum of 0.2 ha (0.5 ac.) of land for public open space as shown on the Land Use Map.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated “Residential (Townhouse)” on the Land Use Map.

The purpose of the guidelines is to supplement the City-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Laurelwood area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in order to fulfil the sub-area plan goals and objectives.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

- a) Special views, such as of mountains should be taken advantage of where possible.

8.2.2 MASSING AND HEIGHT

- a) Townhouses should be designed in small clusters and have less than six units in row.

8.2.3 ARCHITECTURAL ELEMENTS

- a) Townhouses adjacent to public roads and trails should have their main frontage doors visible and accessible from that road or trail with minimum changes in grade. Use minimal changes in level and landscaping to ensure the privacy of the individual units open space is protected without the use of high fences;
- b) Townhouses should have a maximum transitional height gradient of 26° starting at 4.5 m (14.76 ft.) height at 3.0 m (9.84 ft.) setback from all property lines that do not abut a public road. These gradients may be varied provided privacy, sunlight, view and human scale are met.

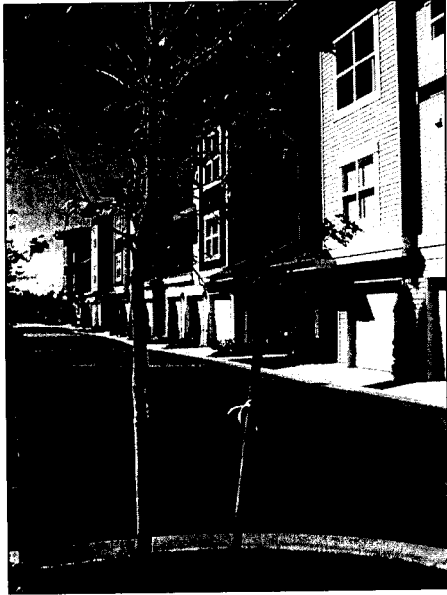


Transitional Height Gradient

8.2.4 LANDSCAPE ELEMENTS

- a) Townhouses should have a minimum private outdoor space of 6.0 m (19.685 ft.) in depth and 36 m^2 (387.5 ft^2) in area. The depth of private outdoor space may be reduced to 5.25 m (17.22 ft.) where adequate privacy screening can be provided. (Not recommended where private outdoor spaces abut a public street.);
- b) Screening and landscaping, including street trees, should be provided in each townhouse development;
- c) Define the vehicular entrance to each residential unit by providing a street tree between each driveway/garage entry point. Trees should be a minimum calliper of 100 mm (4 in.) measured 1.4 m (4.7 ft.) above grade;
- d) Use special landscape features such as trellises to conceal garages and visitor parking stalls from surrounding private, semi-public and public areas;
- e) Retain and incorporate mature landscaping into the development where possible.

8.2.5 PARKING AND SERVICES



Internal Circulation and Parking

- a) Visitor parking should be in public view and easily accessible near the main entrances. Parking lots should have landscaping to separate every fourth parking space and be separated from children's play areas;
- b) Provision should be made for the storage and collection of garbage and recyclable materials;
- c) Light semi-private open spaces, pedestrian and vehicular circulation routes and building entries to provide security, safety and convenient access without producing glare into adjacent properties.

Land Use Map



Richmond Official Community Plan

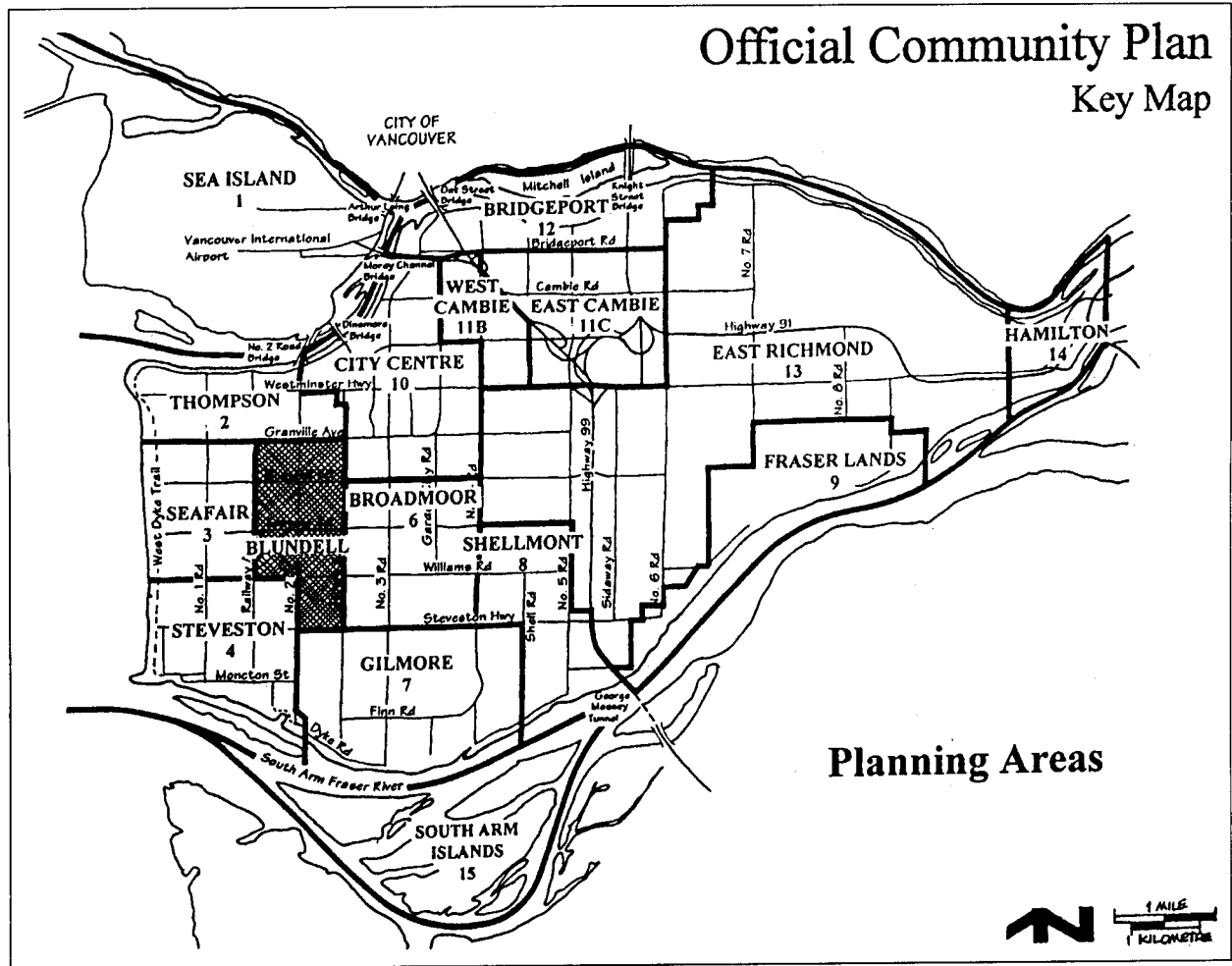


City of Richmond

BLUNDELL AREA EAST LIVINGSTONE SUB-AREA PLAN Bylaw 7100 Schedule 2.5B



KEY MAP



PLAN AREA MAP

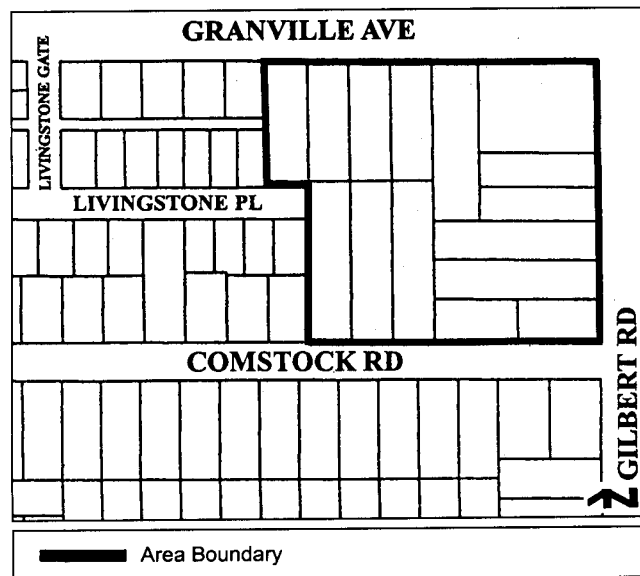


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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City's social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

The East Livingstone Sub-Area is bounded by Granville Avenue to the north, Comstock Road to the south, Gilbert Road to the east, and extends about 170 m (558 ft.) westward from the western edge of Gilbert Road.

This plan contains the goals, policies, Development Permit Guidelines and land use designations for the East Livingstone Sub-Area.

1.2 GOAL

To create a livable, pedestrian-friendly, residential neighbourhood which accommodates a variety of housing types, households and age groups, and fulfils a transition function between the higher density City Centre and the adjacent single-family neighbourhoods.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING

3.1 LAND USE

POLICIES:

- a) Create a tranquil, serene neighbourhood distinguished by its visible, abundant trees and colourful, inviting gardens. Unify street character through the repetition of certain common landscaping elements;
- b) Ensure the neighbourhood is compatible with adjacent single-family neighbourhoods;
- c) Permit the establishment of townhouse or low-rise apartment residential in the north-east corner of the Livingston Sub-Area as shown on the Land Use Map. The maximum permitted density is 0.9 FAR; however, densities lower than 0.9 FAR may be necessary in some cases to achieve livability and other objectives;
- d) Permit the establishment of small-lot single-family, two-family, or townhouse residential use as shown on the Land Use Map at a maximum density of .55 FAR. Where a site includes both apartment and townhouse designations, consideration may be given to averaging densities across the site;
- e) Ensure that setbacks along all public streets is 6 m (19.7 ft.) and setbacks for side and rear yards not directly facing a public street are a minimum of 2 m (6.6 ft.) in order to reinforce the single-family character and provide space for plant landscaping that enhances the pedestrian realm.
- f) Ensure that the neighbourhood is well-connected by providing additional walkway or trail connections which enhance access for East Livingstone and the adjacent single-family area to important City Centre amenities, such as Minoru Park:
 - Secure a public right-of-way to create pedestrian/bicycle paths from Livingstone Place through to Granville Avenue and Gilbert Road, on the approximate locations designated in the land use map;
 - Ensure that these paths are attractive and safe.

4.0 TRANSPORTATION

POLICIES:

- a) Ensure that vehicular access to new buildings from Granville Avenue or Gilbert Road is not permitted;
- b) Ensure that vehicular access to new residential developments within the sub-area is provided through an extension of Livingstone Place through to Comstock Road. Ideally, the Comstock link will be setback at least 100 m (328 ft.) from Gilbert Road;
- c) Provide vehicular access to single-family and two-family residential developments through lanes. Where possible, also use lanes to provide vehicular access to multiple-family development, to minimize the visual and physical impact of the automobile on neighbourhood streets and the pedestrian realm.

5.0 NATURAL & HUMAN ENVIRONMENT

See OCP.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to:

- the lands designated “Townhouse or low-rise apartment residential”; or
- to those sites within the “Small lot single-family, two-family or Townhouse residential” designation that are zoned for townhouse use.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the East Livingstone area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in order to fulfil the sub-area plan goals and objectives.

The neighbourhood’s location, traffic considerations, mature landscaping, and significant redevelopment potential present issues which need to be addressed through design guidelines.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

Vehicular Treatments

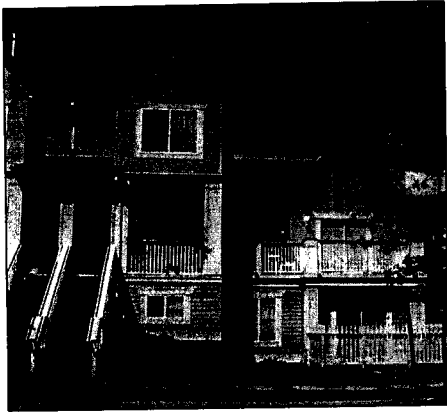
- a) Gated driveways are not permitted;
- b) Driveways are not permitted along Granville Avenue or Gilbert Road;
- c) Encourage shared driveways between properties, and limit all driveways to a maximum width of 3 m (9.8 ft.);
- d) Driveways should be lined with trees or shrubs;
- e) The first 10 m (32.8 ft.) of any driveway leading directly from a public road should be treated with grey pavers or stamped concrete;

Pedestrian Treatments

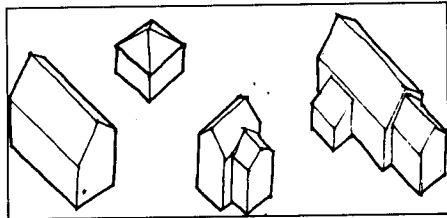
- f) Create safe and pleasant pathways between Livingstone Place and the two major arterial streets;
- g) Pathways should be a minimum of 3 m (9.8 ft.) wide. They should be treated with special pavers, and landscaped with small-scale plant materials and shrubs; however, where possible, existing vegetation should be preserved along these pathways;
- h) Maximize visibility and animation along pathways by orienting windows, entries and balconies on adjacent buildings towards paths;
- i) Visually enhance pedestrian linkages and create a sense of arrival through the use of arrival plazas, special plantings and benches at the intersection of Livingstone Place and the pedestrian paths leading to Granville Avenue and Gilbert Road;
- j) Ensure that pathways are well-lit, visible from the street and clearly marked with entrances embellished by ornamental planters or small (columnar or narrow pyramid-shaped) trees.

8.2.2 MASSING AND HEIGHT

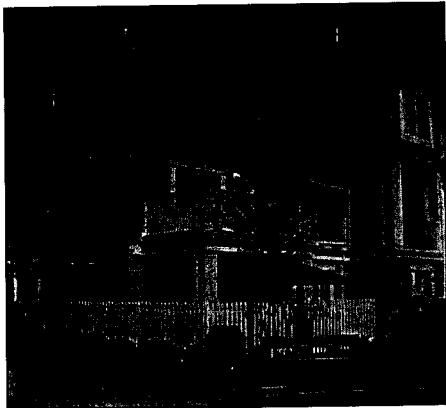
- a) Ensure that the scale, massing and form of new buildings complement adjacent and facing single-family residential areas and contribute to a pedestrian-friendly street environment;
- b) Avoid facing blank building walls onto the street. Orient windows and doors streetward, and create an attractive frontage using windows and doors to help animate the building facade and promote a more human-scale character;



Stepped Building Form



Building Massing



Porches and Balconies



Recessed Balconies

- c) Reduce the apparent height of buildings with architectural treatment that promotes recognition of individual storeys and avoids the appearance of sheer blank walls. Such treatment might include (but is not limited to) the use of: trim; more intense colour accents; secondary roof elements; building recesses; and stepped building forms emphasizing lowered height on ends with fronts;
- d) Where apartments are permitted, limit height to a maximum of four storeys over parking;
- e) Provide a side yard height step-back where apartments are adjacent to townhouse or single-family dwellings. Also provide height step backs at street corners, in the form of lower (eg one or two-storey) building elements (see Building Massing sketch).

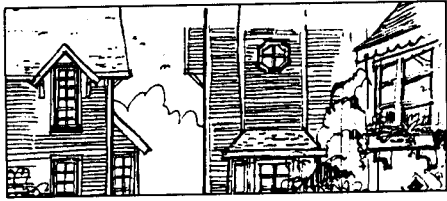
8.2.3 ARCHITECTURAL ELEMENTS

Porches, Balconies, Patios

- a) Use porches and balconies to provide visual interest to buildings while complementing existing neighbourhood character and providing usable private open space to residents;
- b) Large projecting balconies are discouraged along the streetfront, as they emphasize building bulk. However, recessed balconies are encouraged (see Recessed Balconies sketch);
- c) Porches are encouraged along Livingstone Place or Comstock Road, and may project up to 2.5 m (8.2 ft.) into front yard setbacks along these streets. Porches are discouraged along Granville Avenue and Gilbert Road, and may not project into setbacks along these streets;
- d) Porches and covered stairs should be at least 2.5 m (8.2 ft.) deep to allow for usability, and should incorporate prominent main entries;
- e) Porches, porch entrances, and related accents provide desired opportunities for varied and creative design expression. They should enhance individual buildings and be accented with traditional materials such as natural wood or wrought iron. They should also be integrated into the facade rather than appearing "tacked on";

Windows

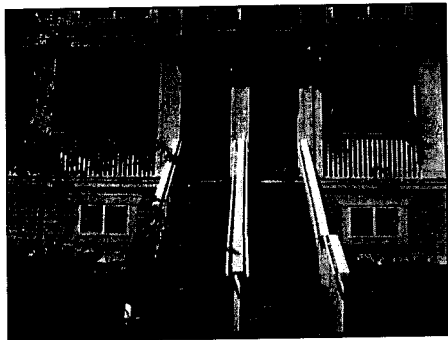
- f) Windows should be operable, of residential scale, with multiple panels, and should add to the sense of neighbourliness and safety and security by providing visual interest and surveillance (see Residential Scale Windows sketch);



Residential Scale Windows



Prominent Main Entries



Grade Access Entry



Single Storey Features

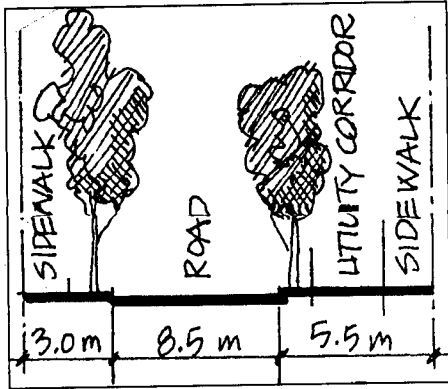
- g) Reinforce the smaller components of buildings which express strong unit identity through appropriate window placement;
- h) Windows, used singly or in combination, should be apparent at eye level and should be clear-glazed (not tinted) to reinforce the sense of surveillance over the street;
- i) Windows should be visually prominent, separated from the building face and defined with colour, trim/shutters, or other decoration. Bay windows, window boxes and similar features are also encouraged;
- j) Traditional window treatment should be used where visible from the street. Skylights, plastic bubble windows, tinted glaze and other non-traditional windows should not be used in prominent locations on the building that are visible from the street;

Entrances

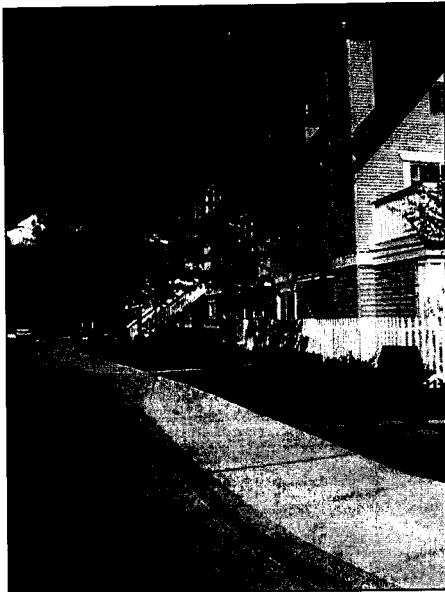
- k) Strongly tie new multiple-family development to quiet residential streets, and use entrances to reinforce and achieve a human-scale and pedestrian-friendly environment along these streets (see Prominent Main Entrance sketch);
- l) Provide front door entries with direct grade access to new townhouse and apartment developments that are oriented to, and clearly visible from either Livingstone Place or Comstock Road. Ensure that entries are accessible to disabled persons;
- m) Architectural treatment of unit entrances should reinforce proximity to grade level and avoid two-storey features (see Single Storey Features sketch);
- n) Entrance features, such as trellises and small entry courts edged with ornamental trees, shrubs and plants, are encouraged;

Materials

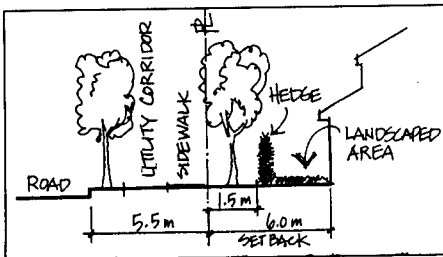
- o) Roofing material should emphasize natural or west-coast produced materials such as cedar. Metal roofs are discouraged and the use of roof tiles should be avoided;
- p) Particularly desirable building materials include: natural stone, cedar siding, and wood shingles. Good quality vinyl-coated aluminum or solid vinyl reproductions of traditional siding materials are acceptable. The use of brick is discouraged and the use of stucco should be minimized, particularly on surfaces visible from the street.



Local Road



Sidewalk along Livingstone Place



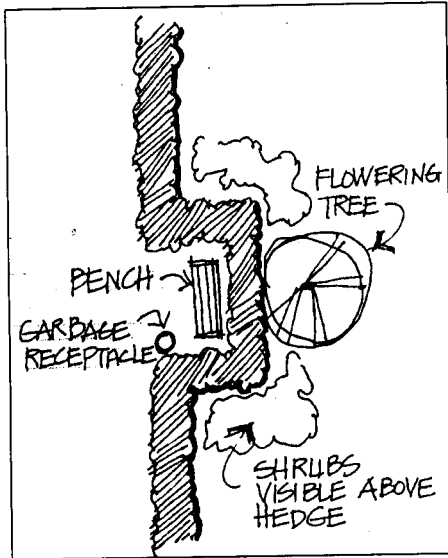
Landscaped Setback

8.2.4 LANDSCAPE ELEMENTS

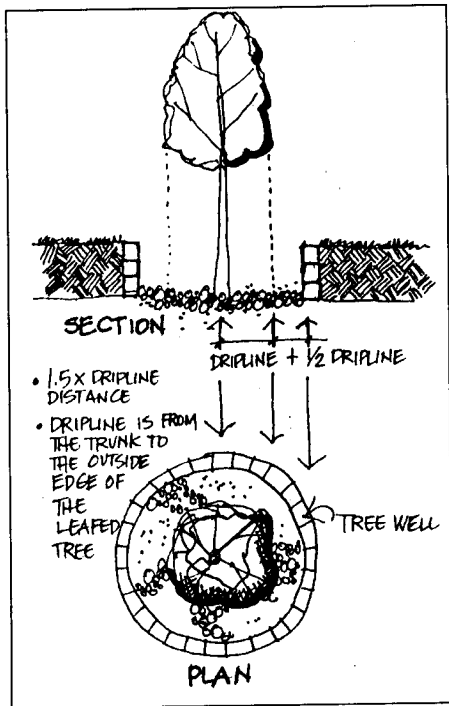
- a) Use landscaping to create a uniform street appearances, and share the visual benefits of attractive landscaping on private property among all neighbours;

Streetscape

- b) Ensure that windows and doors remain visible from the street and are not hidden by vegetation;
- c) Provide sidewalks on both sides of Livingstone Place and on the north side of Comstock Road. Make these sidewalks attractive by providing street trees, and placing all utilities underground (see Local Road sketch);
- d) Along Comstock and Livingstone Place provide a 1.5 m (4.9 ft.) grass strip at the front property line directly beside the sidewalk and include a second row of trees along this grass strip. Trees should be spaced approximately 10 m (32.8 ft.) apart, with oak varieties recommended, such as red oak (*quercus rubra*) or willow oak (*quercus phellos*). Directly behind this grass strip provide contiguous flowering or evergreen hedging (eg. wild rose, boxwood, laurel) of 0.6 m (2 ft.) in height, and at least 0.6 m (2 ft.) wide (see Landscaped Setback sketch);
- e) Although driveways should be minimized, where they create a break in the grass strip and hedging, reduce the visual impact of this break by edging at least the first 3 m (9.8 ft.) of any driveway (leading directly from these streets) with the grass strip and hedge described above;
- f) Along the North and East sides of Livingstone Place, and the North side of Comstock Road, provide privately-owned, publicly-accessible open spaces (P.O.P.A.S.) in the form of small seating areas approximately 2 m by 2 m (6.6 ft. x 6.6 ft.) in size. Provide benches and trash receptacles in these areas. These areas should be spaced approximately every 30 m (98.4 ft.). Divert hedging accordingly, and behind the hedging provide at least one flowering fruit tree, of rounded canopy and moderate to rapid-growing. Create a profusion of vegetation around these open spaces, by clustering taller shrubs and other understorey around the tree and behind the hedge (see P.O.P.A.S. sketch);
- g) Create a tree grove along Granville Avenue and Gilbert Road. Setbacks should be completely landscaped with plant materials;



Privately-owned Publicly-accessible Open Space (P.O.P.A.S.)



Tree Well

Tree Preservation

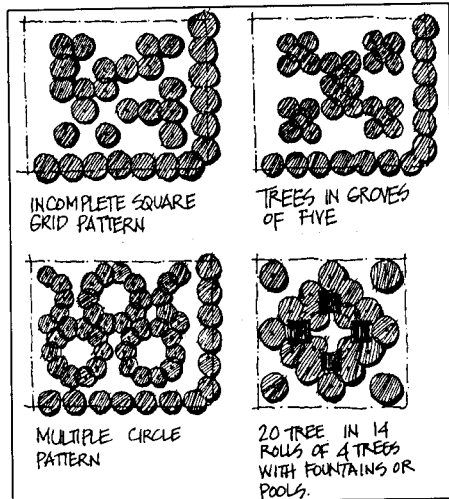
- h) A detailed survey of existing trees and vegetation should be conducted, following the requirements set out in Richmond's Tree Survey Bulletin;
- i) Take special efforts to preserve trees and mature vegetation. Plan open spaces and walkways with landscaping first. Then group buildings around the spaces. Most mature trees exist along Granville Avenue, Gilbert Road, and existing rear property lines. It should be possible to create a grove effect along these streets (large contiguous lines of mature trees/vegetation) through retention and enhancement of existing plant materials. Any new plant materials used should be indigenous or native species which require little maintenance;
- j) Tree wells may be used to facilitate retention of existing trees. Where these are used, they should be a minimum distance of 1.5 times the distance from the trunk of the tree to the drip line (see Tree Well sketch);

Common Open Space

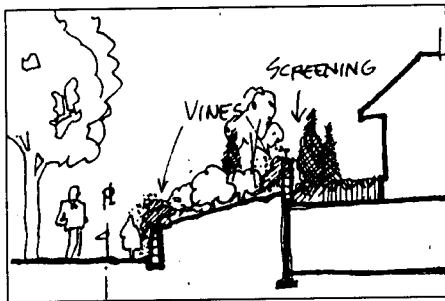
- k) Create open spaces that are usable and practical while also enhancing the streetscape of the neighbourhood;
- l) Special effort should be taken to create and coordinate the retention of contiguous existing mature vegetation on adjacent properties;
- m) Landscape the ground plane intensely with flower beds, flowering trees, shrubs and other plant materials which will foster a garden-like quality. Shrubs or small trees should be clustered between units or planted a minimum of every third unit in the front yards. Include species that will attract birds (which in turn enhance both biodiversity and create pleasant soundscapes). Examples of such plants include: crab apple, cherry, and dogwood trees; holly, serviceberry, elderberry, bayberry, honeysuckle and rose bushes. Where possible, plant tall columnar trees in side yards;
- n) Cluster trees to create "outdoor rooms" or to divide yards into smaller, more intimate areas for people to gather in while avoiding a heavily partitioned character (see Outdoor Rooms sketch);

Fences, Retaining Walls and Hedges

- o) Use hedges and fencing to demarcate private property while avoiding a walled-off effect.



"Outdoor Rooms" adapted by Arnold, Henry E. 1980. Trees in Urban Design



Soften Retaining Walls



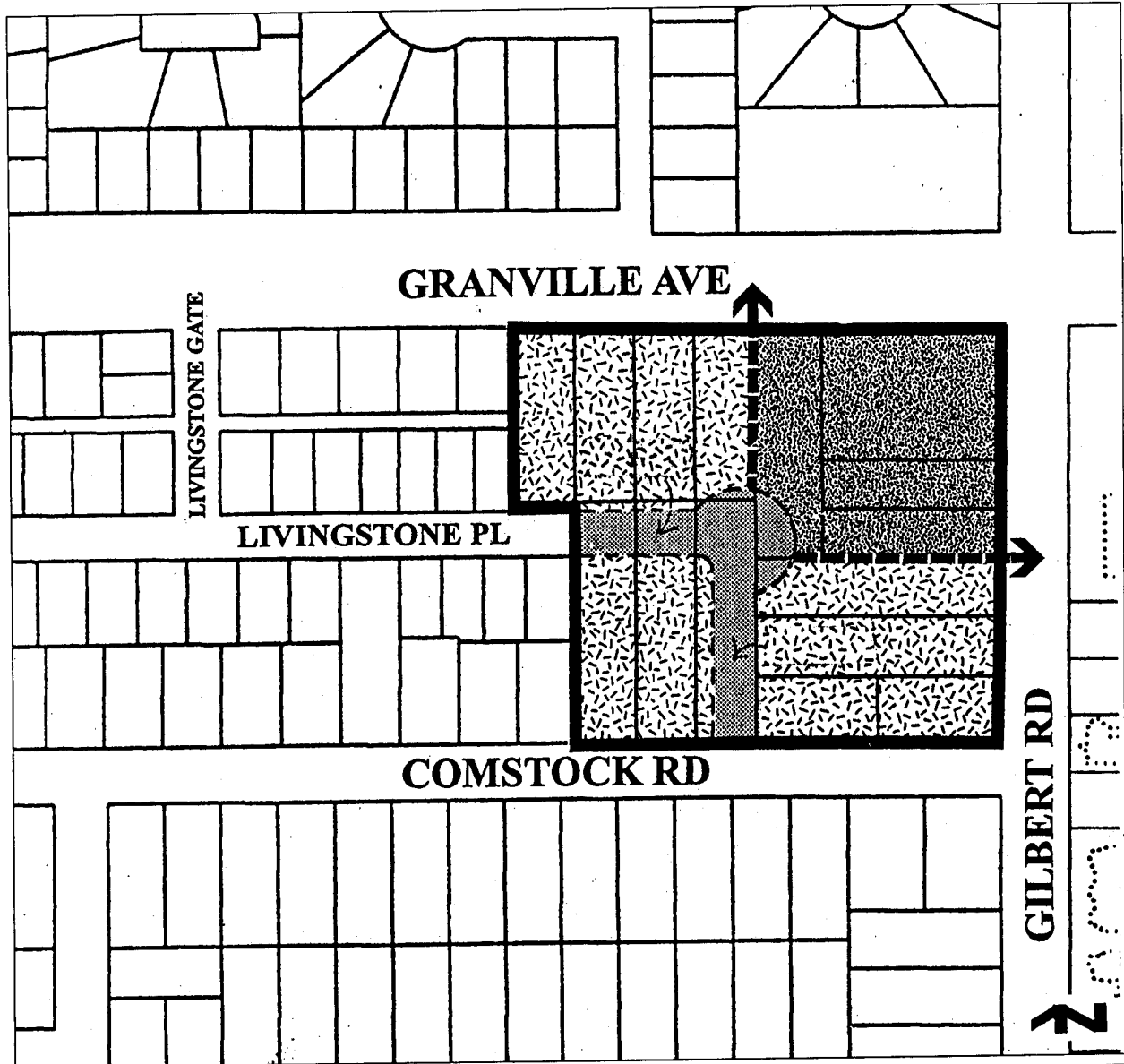
Screen Parking





- p) Fences are discouraged along Comstock Road or Livingstone Place, and may not project into 6 m (19.7 ft.) setbacks. If they are provided along these streets, fences should be no more than 1 m in height, and of a transparent quality, such as wood picket or wrought iron railing;
- q) Fences may be used along Granville Avenue and Gilbert Road, but should not be continuous. They should be screened with landscaping, routed around existing vegetation, and limited to a maximum height of 1.1 m (3.6 t.). Fences may project up to 2 m (6.6 ft.) into the street setbacks, but may be no closer than 4 m (13.1 ft.) to the property line along these streets;
- r) Side yard fences are discouraged;
- s) Hedges and retaining walls may be used along Granville Avenue and Gilbert Road, and should be no more than 1.1 (3.6 ft.) in height along those streets. On apartment sites, retaining walls are encouraged as part of the earth-berming scheme. Where retaining walls are used they should be softened through the planting of with vines or other cover. Hedges are required along Comstock Road and Livingstone Place, but retaining walls along those streets are discouraged (see Soften Retaining Walls sketch).

8.2.5 PARKING AND SERVICES

- a) Minimize the surface area of black top parking and access driveways through alternate paving treatments;
- b) Resident parking should not be visible from the street. On townhouse sites, it should be covered or placed behind buildings and screened. Screening should be a minimum of 2 m (6.6 ft.) in length times 1.5 m (4.9 ft.) landscape in height planting or trellis strips. Trees should also be planted a minimum of one tree for every four parking stalls. In apartment developments, parking should be covered and concealed through earth-berming landscaped intensely with plant materials (see Screen Parking sketch).

Land Use Map



 Small Lot Single-Family, Two-Family or Townhouse Residential	 Townhouse or Low Rise Apartment Residential
 Public Path/Pedestrian Right-of-way (approximate)	 Future road, exact alignment to be determined. Road provision may be phased, with phase one consisting of the connection to Comstock Road.

Richmond Official Community Plan

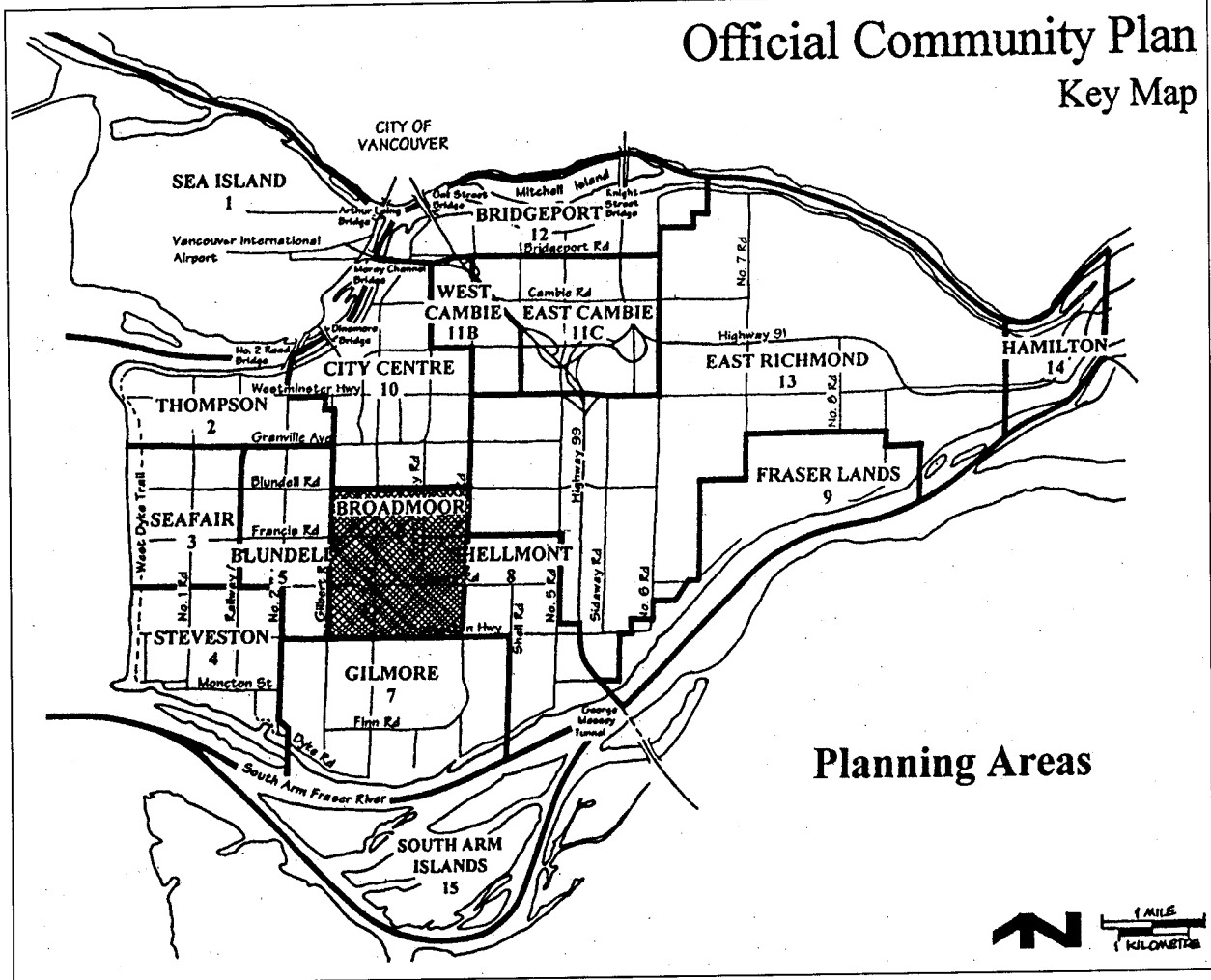


City of Richmond

BROADMOOR AREA ASH STREET SUB-AREA PLAN Bylaw 7100 Schedule 2.6A



KEY MAP



PLAN AREA MAP

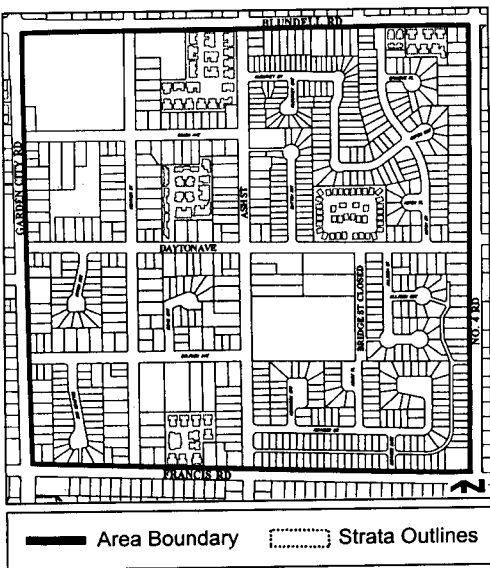


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The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

The Ash Street Sub-Area is located in the Broadmoor Planning Area and is bounded by Blundell, Francis, Garden City and No.4 Roads.

The Ash Street Sub-Area is located in an area where residential growth has led to a need for an expanded sanitary sewer system, closed in ditches and improved roads.

This land use plan will allow for a more precise design of these City services.

The plan also includes projected needs for commercial, parks schools and other community facilities.

This sub-area plan sets out a goal for the area. From this a series of achievable and realistic objectives covering basic planning issues are formulated which together will attain the goal. Specific policy statements then detail the ways in which Council can achieve the objectives.

1.2 GOAL

The goal of the Ash Street Sub-Area Plan is to maintain and improve the existing residential community by upgrading the quality of services and facilities concurrent with new development.

2.0 JOBS & BUSINESS

OBJECTIVE 1:

To provide an opportunity for a community commercial area to serve the weekly shopping needs of residents of the immediate area.

POLICIES:

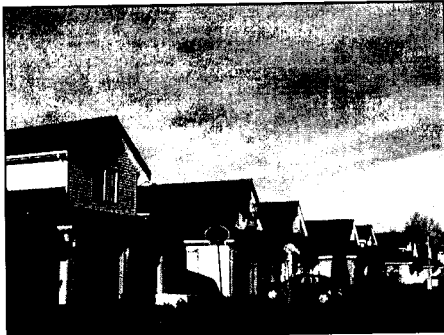
- a) Permit a community commercial area located as per the Land Use Map to serve the shopping needs of residents of the neighbourhood;
- b) Establish buffers along No. 4 Road (the urban/rural boundary) for the purpose of defining the urban/rural edge.

3.0 NEIGHBOURHOODS & HOUSING

3.1 RESIDENTIAL LAND USE

ISSUE:

The trend of residential development in the Ash Street area has been one of gradually increasing densities.



Single Family Development

The Ash Street area was first subdivided into half acre lots then in the late 1970s and early 1980s, two large subdivisions of 12 m (40 ft.) wide lots were built, and the character of the area began to change.

In 1984, for the first time, medium density residential housing was constructed in the area. This housing took the form of detached homes, two storey townhouses and zero lot-line units on small lots about 9 m (30 ft.) wide. This is a trend which is also occurring in other parts of Richmond, and provides more economical family accommodation than traditional large single-family residential lots. The trend is expected to continue in other parts of the Ash Street area, but with variations.

One characteristic of the existing residential neighbourhood is its age which is dramatically emphasized by the many fine old trees located around the homes and lining the streets. In preserving existing homes, care must be taken to also preserve as many of these trees as possible.

New forms of housing can be added to the community in a sensitive manner so that the scale of buildings and life styles of residents are compatible with those existing in the area.



Townhouse Development

This plan recommends forms of new housing which can be built utilizing the rear portions of 1/2 ac. lots, while still allowing the existing residents to retain their homes. This is sometimes referred to as "infill" housing. In most cases two alternative types of housing are recommended for each designated infill area as shown on the attached Land Use Map.

The population of the area at the time of plan adoption (1986) was about 1850 people, living in about 613 dwelling units.

OBJECTIVE 1:

To balance population with community facilities such as the school and park.

POLICIES:

- c) Allow development such that the population increase is commensurate with school and park capacity in the area.

OBJECTIVE 2:

To provide a range of housing types and tenures for a variety of households and age groups.

POLICIES:

- a) Enhance and protect existing single-family areas;
- b) Encourage residential infill development which is compatible with existing single-family areas;
- c) Encourage a variety of dwelling types and tenures by permitting the types and maximum densities indicated for each of the "infill" sites shown on the Land Use Map;
- d) Permit the use and development of lands outside of the "infill" sites shown on the Land Use Map to be governed by the City's normal development application process.



Small Lot Redevelopment

4.0 TRANSPORTATION

ISSUE:

The Ash Street Sub-Area is bounded by four arterial roads, Garden City, Blundell, No. 4, and Francis Roads. It is generally City policy that all traffic within the area should be local traffic only, and travel through the area for purposes other than local access should be discouraged. Satisfactory vehicular access should be provided to arterial roads without impeding the flow of vehicular traffic.

Heather Street, Ash Street and Dayton Avenue have been upgraded, in order to prevent their use for through traffic and to make the area safer for local residents.

In all cases, it is important that residents have equal opportunity to access from one part of the quarter section to any other part, for example to visit friends or to go to DeBeck Park, without using the arterial roads. This is both a transportation and a social consideration and will help maintain the cohesiveness of the neighbourhood.

Various measures have been identified on the Transportation Plan Map.

OBJECTIVE 1:

To provide an efficient transportation system which serves the area without channelling traffic from other sources through the neighbourhood.

POLICIES:

- a) Maintain the integrity of arterial roads by providing access at appropriate locations;
- b) Discourage through traffic on Dayton, Heather, and Ash Streets by the use of barriers, cul-de-sacs or other means, as indicated on the Transportation Plan Map;
- c) Achieve a transportation system as shown on the attached Transportation Plan Map.

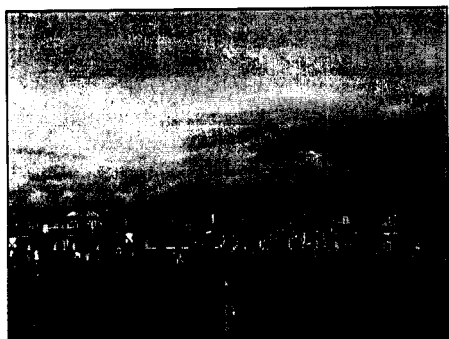


Traffic Barrier

5.0 NATURAL & HUMAN ENVIRONMENT

ISSUE:

As the population of the area grows, there will be an increasing need for parks and open space to meet leisure time needs of residents.



DeBeck School / Park Site

Three kinds of parks serve most Richmond areas: neighbourhood, community, and City facilities.

Because the Ash Street area is small, it has only one kind of facility; neighbourhood parks. Residents travel outside of the area for other recreational opportunities. The closest community park is South Arm Park.

Residents have expressed a need for mini-parks or open spaces in each infill area which is developed for medium density housing, to provide a play space for small children and green space with seating for adults.

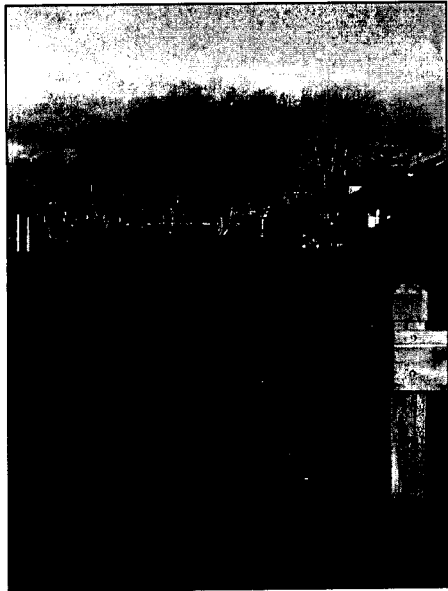
OBJECTIVE 1:

To provide open space and parks for the benefit of area residents.

POLICIES:

- a) Maintain and enhance the DeBeck Park site;
- b) Add adjacent properties to the DeBeck Park site as City finances permit;
- c) Provide for the open space needs of small children and adults in each block where medium density residential infill is developed, by providing a small play area with landscaping and benches.

6.0 COMMUNITY FACILITIES & SERVICES



DeBeck School / Park Site

ISSUE:

Most of the community service and facility needs of Ash Street residents are met outside of this sub-area, however as the population grows more of these needs can be met within the area.

OBJECTIVE 1:

To ensure that a range of community facilities and services are made readily available to residents of the area.

POLICIES:

- a) Encourage the Richmond School Board to retain and utilize the DeBeck site for an elementary school to serve children in the catchment area;
- b) Encourage the existing group home to continue to serve the community;
- c) Encourage the provision of additional space for childcare.

7.0 CITY INFRASTRUCTURE

ISSUE:

When the first half acre lots were subdivided and the first homes were constructed in the Ash Street Sub-Area many years ago, public utilities were minimal. Because servicing in new subdivisions now includes sanitary sewers, storm sewers, curbs, gutters, sidewalks and underground wiring, the level of expectation for services in older areas is rising. Problems with drainage and septic tanks in older areas have also led people to request improvements to their utilities.

As infill development occurs it will be possible to economically install better public utilities for the surrounding areas. Residents would prefer to have sanitary sewers, especially in areas where the sub-soil may contain peat, and where septic tile fields may become flooded during periods of high water table. The installation of sanitary sewers to service half acre lots is not economical. Alternatively, the acquisition of a sewer easement at the rear of existing homes may be difficult until land assembly has occurred prior to infill development.

OBJECTIVE 1:

To improve the quality of public utilities concurrent with development and population growth.

POLICIES:

- a) Install sanitary sewers to serve the Ash Street area, as redevelopment occurs;
- b) Install a sanitary sewer system for the area north-west of Dayton Avenue and Ash Street concurrent with development, and in cases where it is impractical to wait for residential infill to occur, install sanitary sewers prior to development of new homes;
- c) Take necessary measures to maintain public health and safety in older areas.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands which are adjacent (within 30 m (98.4 ft.) to the edge of the Agricultural Land Reserve (ALR) for the purpose of protecting farming (development permits are not required for construction of single family dwellings)).

It is intended that these guidelines be used in conjunction with the City's more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to protect farming on the adjacent ALR lands.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

- a) All development across a road from sites designated as Agricultural Land Reserve (ALR) must provide a minimum 5 m (16.4 ft.) landscaped strip as measured from the back of curb.

8.2.2 LANDSCAPE ELEMENTS

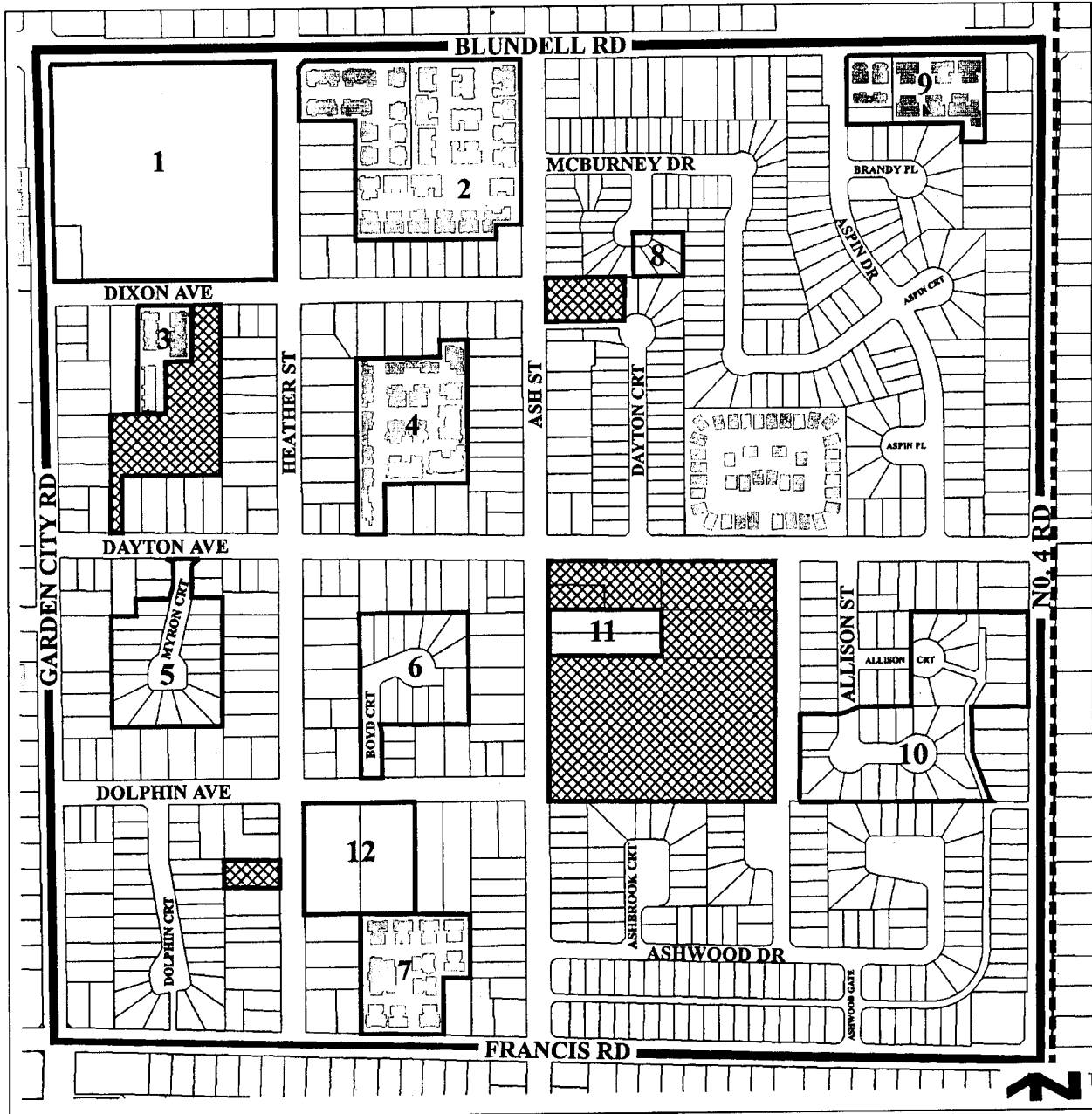
- a) The landscaped strip should reinforce the character of agricultural lands, including large-scale trees, a combination of shrubs and ground cover, and where appropriate, a sidewalk.




Table 1:

Official Community Plan - Specific Infill Land Use Designations Ash Street (Section 22-4-6)					
KEY TO AREA PLAN MAP	APPROXIMATE AREA HA (AC.)	DENSITY UNITS/HA (UNITS/AC.)	MAXIMUM NUMBER OF UNITS	LAND USE	OTHER
1	24 (6)		N.A.	Commercial or townhouses	Maximum two-storey height
2	1.86 (4.61)	29 (12)	77	Townhouses or small lots	Open space and children's play area with townhouses or small lots
3	0.502 (1.25)	*.55 FAR	*.55 FAR	Townhouses or duplex	Open space and children's play area with townhouses
4	1.07 (2.64)	35 (14)	37	Townhouses or small lots	Open space and children's play area with townhouses or small lots
5	.95 (2.34)	18 (7)	18	Small lots	Open space and children's play area
6	.81 (2)	18 (7)	14	Small lots	Open space and children's play area
7	0.830 (2.05)	29 (12)	25	Townhouses or small lots	Open space and children's play area
8	0.12 (0.3)	18 (7)	3	Small lots	
9	0.645 (1.6)	19 (12)	24	Townhouses or small lots with lane	Access to Blundell restricted. Open space and children's play area. Possible lane.
10	1.8 (4.45)	18 (7)	31	Small lots or large lots	
11	#	N.A.	N.A.	Large lots or park	Open up corner for public view of DeBeck school/park
12	#	N.A.	N.A.	Park or small lots	
TOTAL			246		

Note: FAR = Floor Area Ratio

Land Use Map



----- ALR Boundary	——— Area Boundary	 Designated Infill Areas – Refer to Table: 1
 Public, Institutional & Open Space	 Low Density Residential	

Richmond Official Community Plan



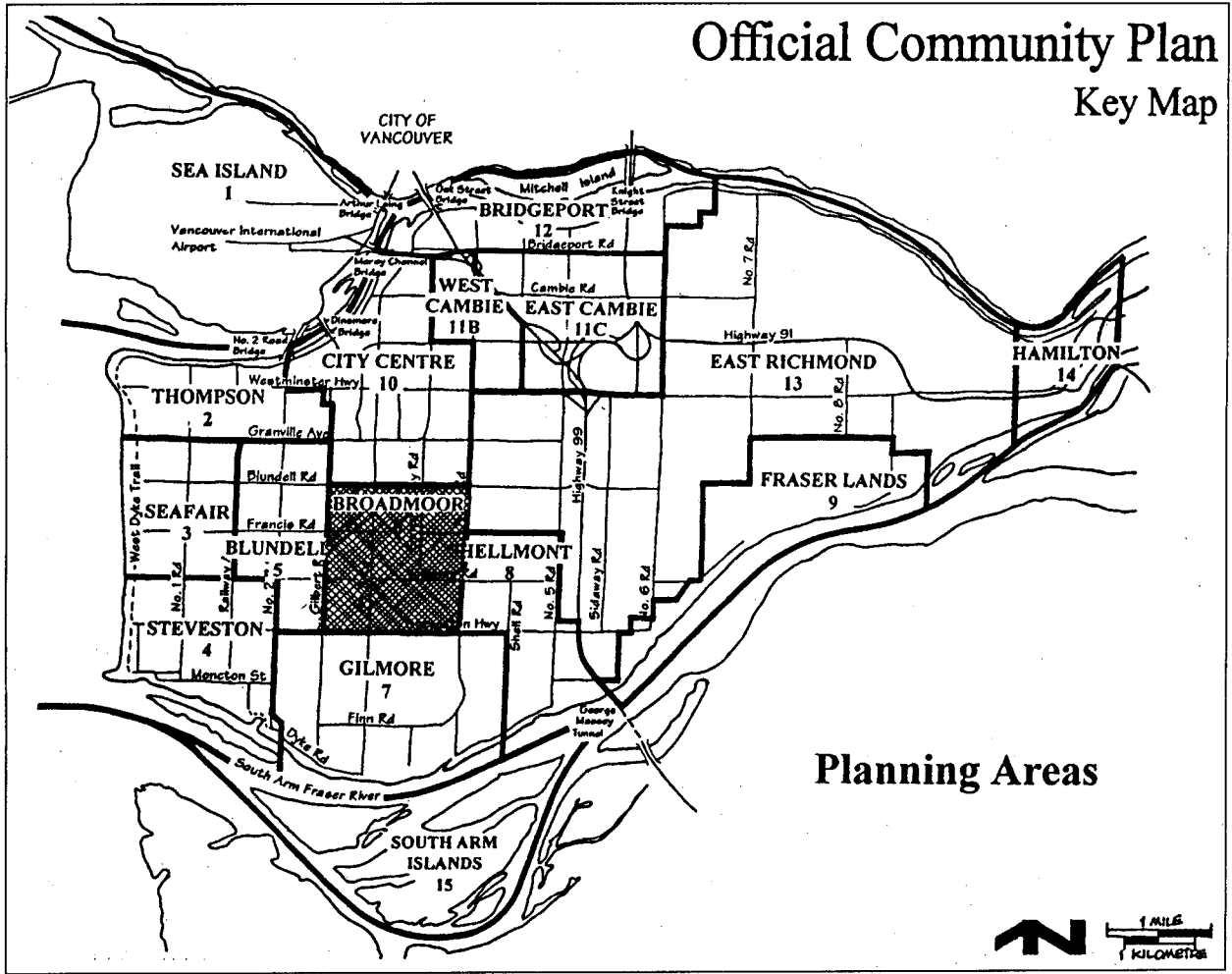
City of Richmond

BROADMOOR AREA CENTRAL WEST SUB-AREA PLAN

Bylaw 7100 Schedule 2.6B



KEY MAP



PLAN AREA MAP

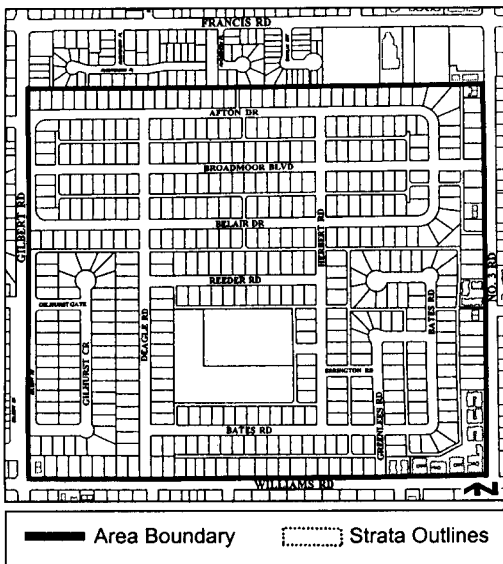


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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City's social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

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Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

1.1 PURPOSE

This plan applies to the area shown on the Plan Area Map.

The Central West Sub-Area is bounded by Williams Road to the south, No. 3 Road to the east, Gilbert Road to the west and to the north side of Afton Drive.

This plan contains the goals, policies, Development Permit Guidelines and land use designations for the Central West Sub-Area.

1.2 GOALS

To create a livable and stable, family oriented residential neighbourhood while consolidating a distinct urban corridor with strong linkages to adjacent neighbourhoods outside Central West Broadmoor, on the neighbourhood's eastern perimeter.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING

3.1 HOUSING AND NEIGHBOURHOOD CHARACTER



Multi and Single-family Homes along No. 3 Road.



Pedestrian Connections

POLICIES:

- a) Maintain the large lot qualities of the neighbourhood while providing a range of housing types on the eastern perimeter that can accommodate a variety of families and households;
- b) Strengthen the identity of the No. 3 Road corridor as part of an emerging neighbourhood village;
- c) Ensure that multiple-family residential is compatible with adjacent single-family neighbourhoods;
- d) Foster a green neighbourhood by encouraging the retention of trees on single-family properties and requiring tree retention or replacement on multiple-family properties;
- e) Ensure that the neighbourhood is well-connected by maintaining existing pedestrian connections from the centre portion of the neighbourhood through to No. 3 Road, and by providing additional pedestrian and cycling connections as opportunities arise. Ensure that these paths are attractive and safe.

4.0 TRANSPORTATION

4.1 CIRCULATION

POLICIES:

- a) Ensure that vehicular access to new developments from No. 3 Road is limited;
- b) Provide vehicular access to single-family and two-family residential developments through lanes. Where possible, also use lanes to provide vehicular access to multiple-family development, to minimize the visual and physical impact of the automobile on neighbourhood streets and the pedestrian realm;
- c) Improve lane access to No. 3 Road at mid-block. Extend the lane west of Bates Road through the City-owned property directly west of 9511 No. 3 Road;
- d) Improve traffic safety on neighbourhood streets. Place stop signs in appropriate locations along Broadmoor Boulevard and consult with residents on additional traffic safety measures in other neighbourhood locations.

5.0 NATURAL & HUMAN ENVIRONMENT

5.1 PARKS AND OPEN SPACE

POLICIES:

- a) Improve access to recreational activities. Maximize the use of park space outside the neighbourhood by making access to these parks safer, through crosswalks and other measures.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the lands designated “Townhouse Residential” on the Land Use Map.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Broadmoor-Central West area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in order to fulfil the sub-area plan goals and objectives. Specifically, the goal of ensuring compatibility between multiple-family and existing single-family neighbourhoods needs to be addressed through design guidelines.

8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

- a) Ensure compatibility with single-family housing across the lane and to the north through complementary scale, massing and building form;
- b) Each dwelling should have a near, middle, and distant view;



Limit Access to No. 3 Road through the Use of Lanes

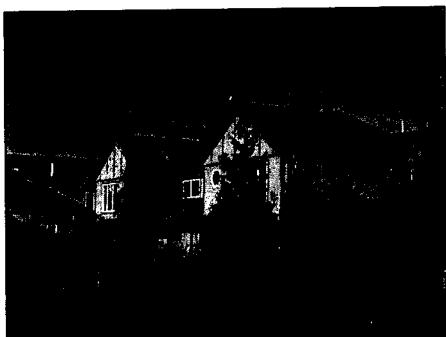
- c) Provide a quiet and livable residential environment by reducing the impact of traffic noise on residential units. All developments on properties adjacent to No. 3 Road must provide a minimum 6 m (19.68 ft.) setback from this road or provide other remedial measures acceptable to staff;
- d) Minimize the visual presence of, and land occupied by, motorized vehicles on sites:
 - Cover or screen resident parking from the street and encourage tandem parking;
 - Limit access driveways to 3 m (10 ft.) width and combine them where possible;
- e) Reduce parking visibility from the street by discouraging placement of garages facing No. 3 Road. Where this is not possible, driveway access to garages must be treated exclusively with special pavers and embellished with ornamental shrubs and trees;
- f) Use measures to ensure that vehicular access from No. 3 Road does not endanger or inconvenience pedestrians or the mobility impaired;
- g) Improve No. 3 Road as a pedestrian thoroughfare, and enhance its visual interest through the built environment;
- h) Enhance safety on public pedestrian paths, by orienting windows and doors towards these paths and limiting fences and hedges along such paths to 1.1 m (3.6 ft.) in height;
- i) Recognize that significant pedestrian activity will also occur on lanes, and improve their appearance and safety.

8.2.2 MASSING AND HEIGHT

- a) Encourage the orientation of front doors and windows towards No. 3 Road. Avoid facing blank building walls onto the street;
- b) Buildings should be set back from streets and open spaces in a manner that promotes continuity of local street definition, landscape, and residential character.

8.2.3 ARCHITECTURAL ELEMENTS

- a) Provide richer detailing, high quality, durable materials and special lighting which will enhance the pedestrian's close-up view of buildings along No. 3 Road;
- b) Encourage the use of sloped roof forms and visually interesting, high quality roofing materials. Heavy tiled roofing is not appropriate;
- c) Discourage the treatment of buildings with decorative patterned stucco, imitation brick, or vinyl siding;
- d) Clearly define and number entrances to each unit.



Sloped Roofs and Detailing



Encourage Retention of Mature Trees

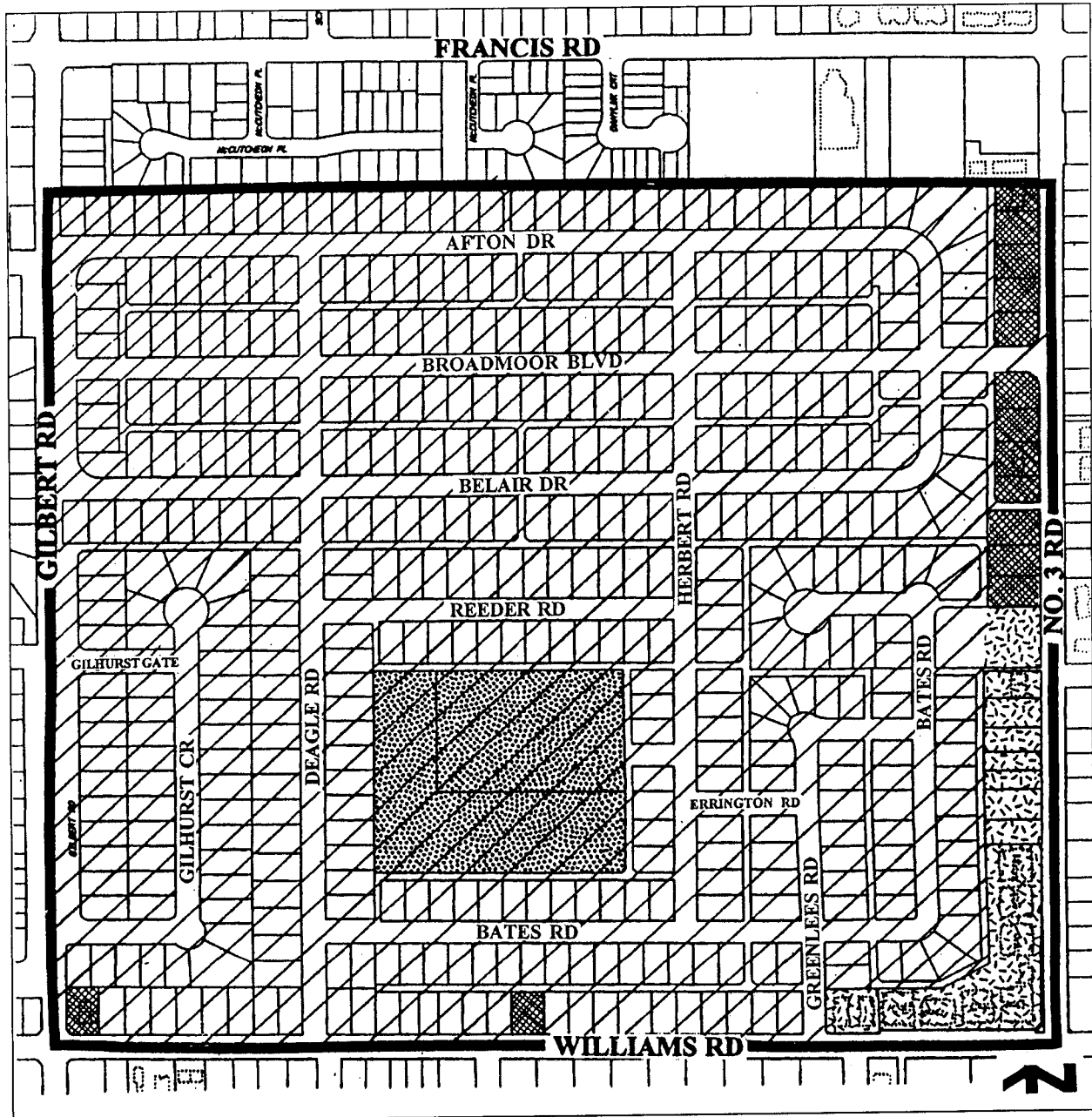
8.2.4 LANDSCAPE ELEMENTS






- a) Reinforce the natural environment on private property:
 - Retain existing trees and mature plants, and augment them with ornamental flowering trees and shrubs;
 - Provide each dwelling unit with its own private outdoor space, and clearly demarcate this space at ground levels;
 - Permit no more than 75% of a site to be covered by buildings and non-porous surfaces. A minimum of 25% of the site must be covered with soft landscaping including plant materials;
- b) Minimize the surface area of blacktop parking and access driveways through alternate paving treatments;
- c) Use special landscaping features, such as trellises, to conceal garages and visitor parking stalls from surrounding private, semi-private and public areas;
- d) Fences and hedges along No. 3 Road should be limited to 1.1 m (3.6 ft.) in height;
- e) Provide pedestrian-oriented lighting along lanes and internal roadways on private developments;
- f) Include landscape materials, complete with necessary space, soil depth and irrigation.

8.2.5 PARKING AND SERVICES

- a) Provide appropriate garbage facilities. Erect a covered, gated structure to contain residents' garbage and recycling materials. The design of this structure should complement the design of units in the project;
- b) Place all utilities underground;
- c) Screen garbage facilities and related uses.

Land Use Map



	Small Lot Single-Family (R1/B)		Large Lot Single-Family (R1/E)		Townhouse (R2) Residential
	Public and Open Space Use		Strata Outlines		

Richmond Official Community Plan

BROADMOOR AREA SUNNYMEDE NORTH SUB-AREA PLAN

Bylaw 7100 Schedule 2.6C

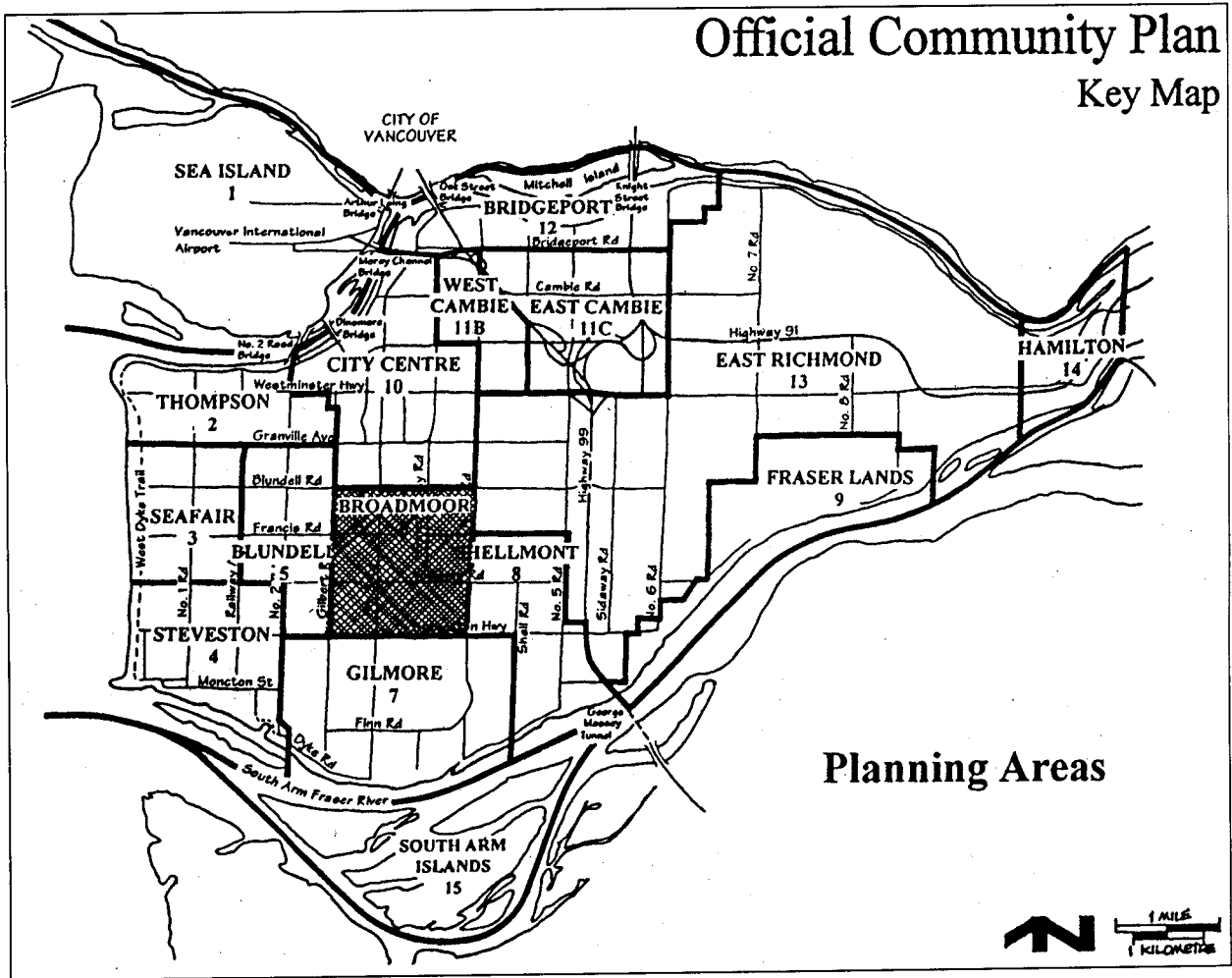


City of Richmond



SUNNYMEDE NORTH SUB-AREA PLAN • SUNNYMEDE NORTH SUB-AREA PLAN • SUNNYMEDE NORTH SUB-AREA PLAN

KEY MAP



DEVELOPMENT PERMIT AREA MAP

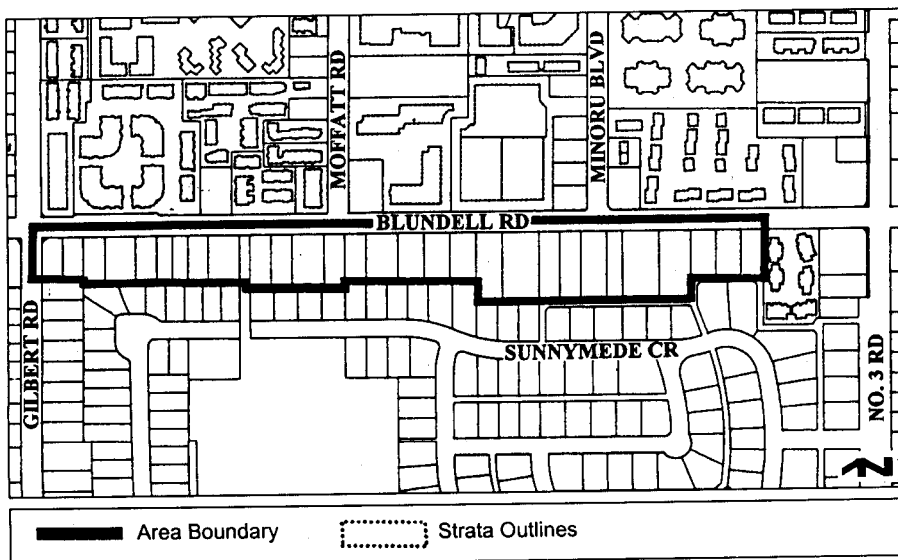


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1.0 PLAN OVERVIEW

See OCP.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING

See OCP.

4.0 TRANSPORTATION

See OCP.

5.0 NATURAL & HUMAN ENVIRONMENT

See OCP.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

This section contains Development Permit Guidelines which apply to the Sunnymede North Sub-Area as shown on the Development Permit Area Map.

The purpose of the guidelines is to supplement the City-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Sunnymede North area.

It is intended that these guidelines be used in conjunction with the City's more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

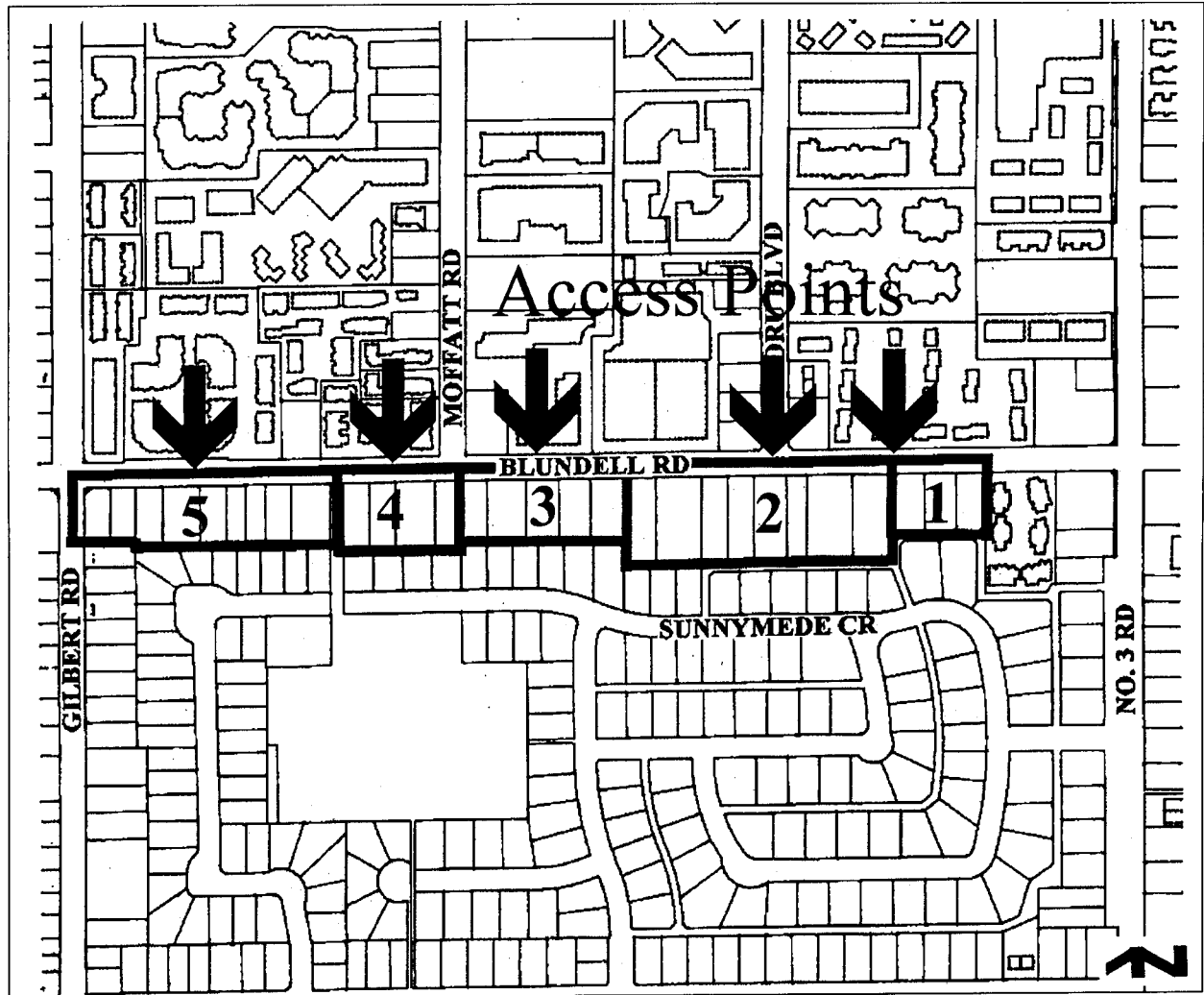
Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to multi-family housing in the Sunnymede North Sub-Area.

There is a need to designate this area for special conditions in order to control access onto Blundell Road as well as to exercise greater control over the form and character of development to ensure a harmonious transition between the medium density residential district to the north in the City Centre and the low density single-family area to the south in Broadmoor.

Access Points Map



8.2 DEVELOPMENT PERMIT GUIDELINES

8.2.1 SETTLEMENT PATTERNS

Building Treatment



Internal Roadway

- a) Design buildings with a main orientation towards Blundell Road and the internal roadways of the projects. Fenestration in facades facing the internal roadways and open spaces should be carefully arranged to provide for internal unit privacy, yet convey a perception of "eyes of the street" enhancing safety and security for residents;
- b) Create highly visible and identifiable principal building entrances through the use of landscape and prominent architectural components such as gateways and covered porches to emphasize the transition from public and semi-private spaces to each unit's private space. Entrances should provide protection from the elements of the weather. Use low level landscaping and/or consider the use of changes in grade to ensure the privacy of an individual unit's private open space without the use of high fences. (The grade change should be no more than .61 m (2 ft.) to .91 m (3 ft.) and should occur between the curb and the main door of each unit);
- c) Stagger units along main east-west internal roads;

Pedestrian Treatment



Prominent Building Entrance

- d) Establish clear and appropriate pedestrian connections between private residences and semi-public areas;
- e) Ensure that the units located directly adjacent to Blundell Road have their front doors facing Blundell Road with a pathway connecting from the sidewalk to each doorway. All of the other units in each development should have their front doors facing the internal roadways;
- f) Locate secondary entrances from private residences onto common walkways located between developments;
- g) Provide internal walkways extending from Blundell Road through each development area to the internal east-west road systems. Define the entrances to the internal north-south walkways by providing arrival plazas. The arrival plazas should consist of the following elements: widened hard surfaces with distinctive paving patterns, arbours, and accent planting;
- h) Use walkways, rather than fences, to separate the development areas. Provide low level landscaping and lighting on both sides of the walkways;

Vehicular Treatment

- i) Minimize the number of vehicular access points from Blundell Road as shown on Attachment 2.
- j) Consider the use of traffic circles or landscaped medians to visually and physically direct people to the separate projects included in each development area;
- k) No gates are permitted at the main vehicular accesses to the internal roadways or between projects within each development area;
- l) Use "gentle" tangent curves along internal road systems to create more interesting roadways;
- m) Traffic noise should be screened from residential units in order to maintain acceptable ambient indoor sound levels. All Development Permit applications shall require evidence in the form of a report and recommendations prepared by a person trained in acoustics. This report shall demonstrate that noise levels in a weighted 24-hour equivalent sound level (defined simply as noise level in decibels) will not exceed:
 - bedrooms – 35;
 - living, dining and recreation rooms – 40;
 - kitchen, bathroom and hallways – 45.

8.2.2 MASSING AND HEIGHT



Secondary Roof Elements

- a) Reduce the scale of building forms adjacent to semi-private open spaces, pedestrian walkways, main vehicular entrances and adjacent to the Sunnymede single-family neighbourhood to the south through the use of secondary roof elements of a human scale such as covered porches or trellis elements;
- b) Units should have their massing designed to provide articulation to the streetscape both vertically and horizontally. Flat street fronts are unacceptable, as are continuous cornice lines;

8.2.3 ARCHITECTURAL ELEMENTS

- a) Incorporate human scale elements (windows, doors, roof elements, trellis, etc.) into the building facade visible from the street;
- b) Ensure that balconies to upper floors facing Blundell Road are small and cantilevered, giving minimal exterior access. Their design should be treated like a decorative exterior element enhancing the building's articulation and massing;

Roofscapes

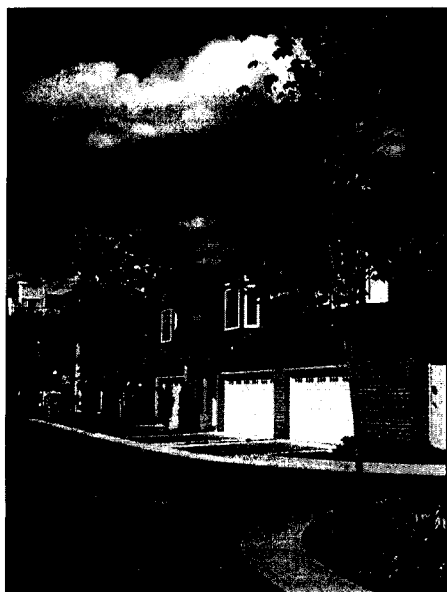
- c) Construct pitched roofs of various heights with a minimum slope of 6 to 12 with gable ends and dormers to create diversity;
- d) Provide soffits with overhangs to add interest and help protect the siding of the buildings;
- e) Select roofing materials which are suitable for the level of articulation desired in the roof forms. Cedar shingles would facilitate this, whereas heavy-tiled roofing would not;

Exterior Finish

- f) Use natural building materials such as wood, stone, brick or stucco on building exteriors. Unacceptable finish materials include imitation brick and highly patterned stucco;
- g) Employ a combination of two exterior materials on facades to help maintain a human scale, create texture and enhance design;
- h) Account for the effects of the Richmond climate in the choice of material and detailing. Stucco should be treated to prevent discolouration and particular care should be taken in detailing the north facades facing Blundell Road;
- i) Use a wide range of natural earth tones commonly produced as "heritage series" by a number of commercial paint manufacturers on the main exterior of the buildings. Compliment and coordinate the range of colours used on the various units with accent colours on the trim. Reserve the use of vivid colours to "highlighting" areas such as the trims and doors;
- j) Use decorative trim, cornices, reveals and projections where appropriate;

Windows

- k) Express a residential character by using a variety of appropriately scaled and proportioned windows;
- l) Articulate building facades through the use of various forms of specialty window projections such as sky lights, bay windows, and dormers to achieve design interest and improved interior light;
- m) Encourage the provision of window treatments such as flower boxes and shutters;
- n) Orient windows of living spaces towards pedestrian walkways and streets to help facilitate supervision of the street and increase pedestrian safety;



Garage Doors

Garages

- o) Incorporate front door entry features that extend 0.61 m (2 ft.) beyond the garage doors;
- p) Minimize the width of garage doors to a maximum of 4.87 (16 ft.) x 2.13 m (7 ft.);
- q) Incorporate decorative architectural treatments, such as windows, on and above garage doors that are complementary to unit finishes;
- r) Ensure that the garage door encompasses no more than 60% of the building width as visible from the internal road systems;
- s) Where units have both the front and back facade fronting a public road or pedestrian access route, ensure that:
 - No more than two garage doors are placed in a row without interrupting with a portion of the building or side yard equal to the width of one garage door;
 - No building block has more than six garage parking spaces;
- t) Where units have only one facade fronting an internal road system road or pedestrian access route, ensure that:
 - For 50% of the units, no more than one garage door is placed in a row where units are attached without interrupting with an amount of building or side yard equal to the width of one garage door;
 - No building block has more than four garage parking spaces.

8.2.4 LANDSCAPE ELEMENTS



Private Outdoor Space

- a) Provide each unit with a private outdoor space that is at least 37 m² (398.28 ft²) in area and 5.25 m (17.22 ft.) in depth;
- b) Design decks and patios as natural extensions of each unit into the landscape. A maximum of 40% of each unit's private space may have a deck, with the remainder in soft landscaping including grass, shrubs and trees. The provision of decks and patios developed at a maximum of 45.72 cm (18 in.) above grade are preferable to second-storey balconies on units located adjacent to the existing Sunnymede single-family neighbourhood;
- c) Separate each unit's rear private open space with a fence no higher than 1.8 m (6 ft.). Consider the importance and the safety of small household pets by ensuring that a gap no larger than 5.08 cm (2 in.) is provided between the ground and the base of the fence;

- d) Create visual focal points by providing landscaped areas at both the curves of the internal roads, and at the "dead-ends" of each of the development sites east-west internal road. Allow for future pedestrian access to adjacent development areas within the landscaped areas at the east-west road ends;

Tree Planting

- e) Use changes in paving materials such as decorative pavers or cobblestones near entrances to walkways and children's play areas;
- f) Soften hard surfaces through the use of landscaping, trellises and tree planting;
- g) Install pedestrian-oriented lighting along the internal roadways of the developments.
- h) Incorporate mature trees and landscaping into the development area. Whenever a major tree is removed, the replacement ratio should be 2:1. The replacement trees should have a minimum calliper of 15 cm (5.9 in.) in diameter;
- i) Enhance the main shared vehicular/pedestrian entry into each of the development areas by planting large specimen trees and accent planting without obstructing sightlines. Trees should frame the main entry driveways as seen from Blundell Road to each of the development area's internal road system;
- j) Plant large feature trees with a minimum calliper of 150 mm (6 in.) measured 1.4 m (4.7 ft.) above grade along with appropriate low level accent planting in the middle of traffic circles or medians;
- k) Plant street trees spaced no more than 9 m (29.52 ft.) apart within landscaped boulevards along the internal roadways;
- l) Separate the individual garages with landscaped areas including trees with a minimum calliper of 100 mm (4 in.) measured 1.4 m (4.7 ft.) above grade and/or with the careful siting of the residential portions of the buildings;



Mature Tree

8.2.5 PARKING AND SERVICES

- a) Locate enclosed garbage container buildings away from pedestrian accesses and children's play areas. The area should be easily accessible for collection purposes and should provide adequate manoeuvring space for 13.7 m (45 ft.) long garbage trucks. A covered recycling area 2.4 m (7.9 ft.) by 3.5 m (11.5 ft.) should also be provided near garbage areas. It should be wheel-chair accessible;

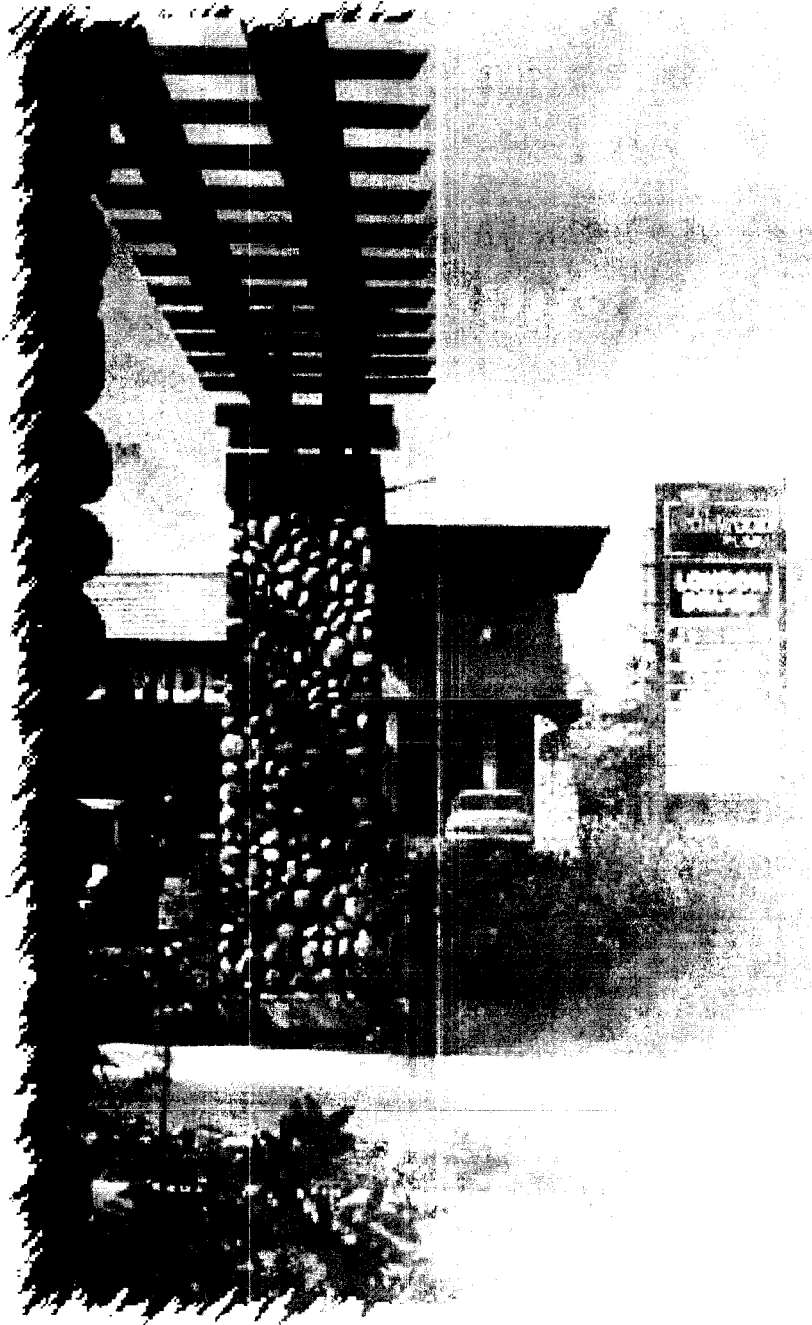
- b) Locate a highly visible amenity space in each development area along with an indoor amenity space in projects which include more than 70 bedrooms.
- c) Locate children's play apparatuses and benches within the open space along with other urban design features such as gardens, fountains, arbours and art.
- d) Locate the outdoor amenity area to take advantage of sun and natural shelter from the elements of the weather.
- e) Ensure that surveillance of the area is provided from adjacent units and the area has barrier free access.
- f) Provide a mixture of hard-surfaced and natural landscaping in the outdoor amenity area.

Richmond Official Community Plan

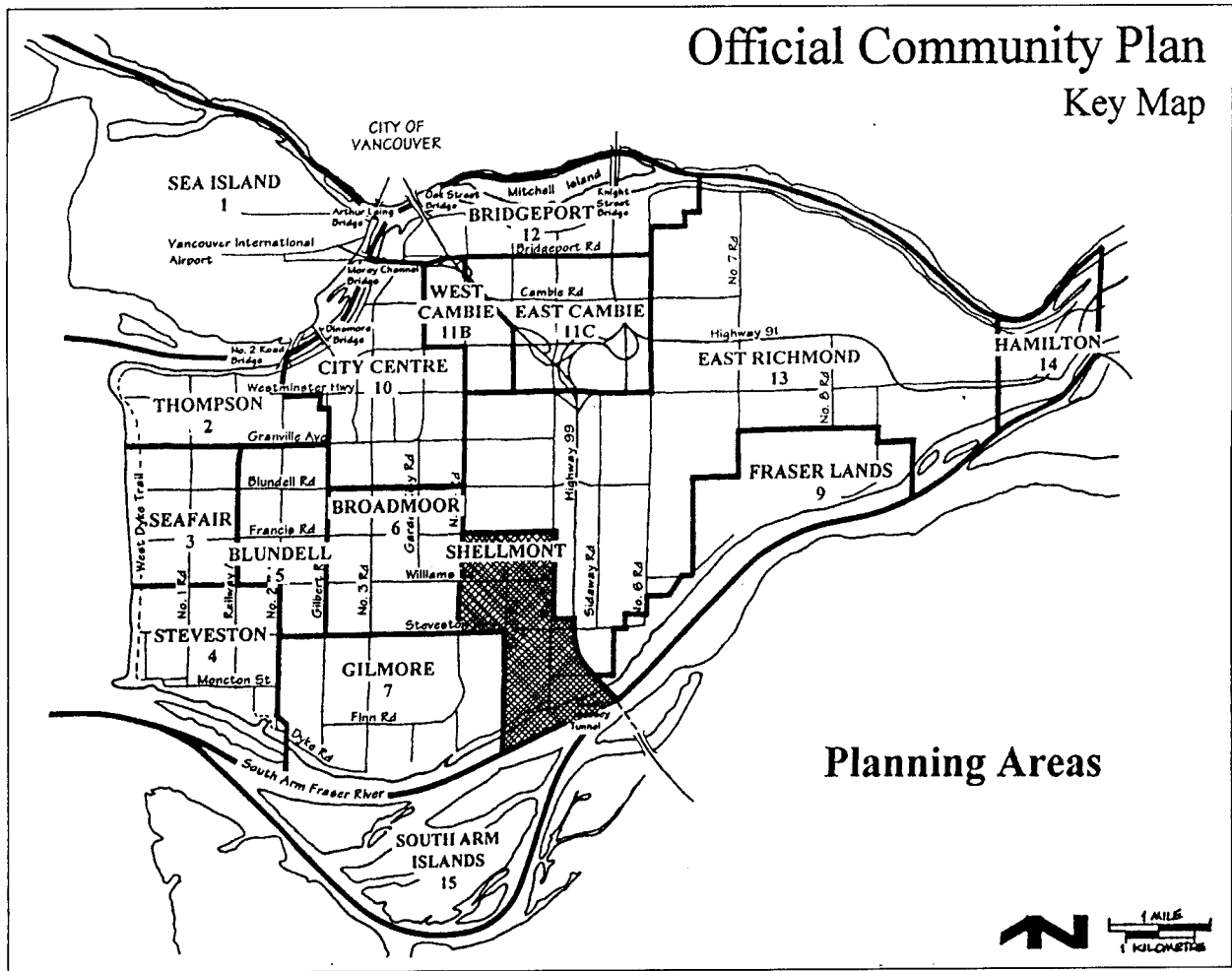


City of Richmond

SHELLMONT AREA IRONWOOD SUB-AREA PLAN Bylaw 7100 Schedule 2.8A



KEY MAP



DEVELOPMENT PERMIT AREA MAP

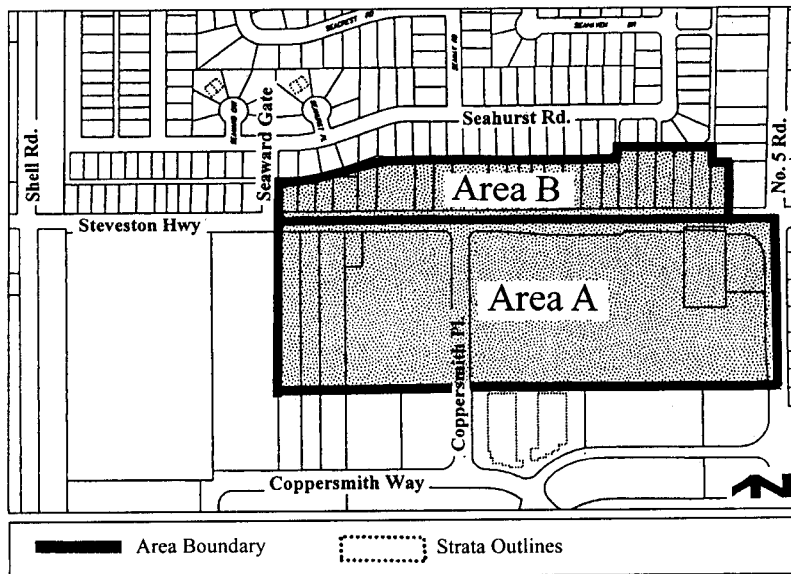


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PLAN INTERPRETATION

What is the Official Community Plan (OCP)?

The OCP is a legal community planning document for managing the City's social, economic, land use, servicing and environmental future. It sets out a vision, goals, objectives, and policies that reflect overall community values that have been determined through a public consultation process.

How is the Plan organized?

The OCP (Bylaw 7100) is comprised of:

- 1) Schedule 1: the overall OCP;
- 2) Schedule 2: Area Plans and Sub-Area Plans.

Area Plans refer to the 15 areas that have been identified within Richmond for planning purposes (see Key Maps).

Sub-Area plans refer to smaller localized areas within specific planning areas.

The OCP addresses broad city wide issues while the Area Plans and Sub-Area Plans address local neighbourhood issues.

Plan Precedence

If there is a conflict with respect to a land use designation between the OCP Generalized Land Use Map and Area Plan Land Use Maps, the Area Plan Maps shall take precedence with the exception of sites designated Conservation Area or Environmentally Sensitive Area (ESA) in which case readers should check Schedule 1 as it takes precedence over this plan.

Changes to this Document

This Plan may be amended from time to time. Please check with the City's Urban Development Division to make sure that this is an up-to-date version containing all of the adopted amendments.

Definitions

See OCP Schedule 1.

1.0 PLAN OVERVIEW

See OCP.

2.0 JOBS & BUSINESS

See OCP.

3.0 NEIGHBOURHOODS & HOUSING

See OCP.

4.0 TRANSPORTATION

See OCP.

5.0 NATURAL & HUMAN ENVIRONMENT

See OCP.

6.0 COMMUNITY FACILITIES & SERVICES

See OCP.

7.0 CITY INFRASTRUCTURE

See OCP.

8.0 DEVELOPMENT PERMIT GUIDELINES

8.1 APPLICATION AND INTENT

8.1.1 DEVELOPMENT PERMIT AREA

The Ironwood Sub-Area contains two “character areas” situated along the north and south side of Steveston Highway to the east of No. 5 Road (Refer to the Development Permit Area Map). This section contains Development Permit Guidelines which apply to these character areas.

The purpose of the guidelines is to supplement the city-wide guidelines contained within the Official Community Plan (OCP) with specific guidelines aimed at supporting a special character within the Ironwood Sub-Area.

It is intended that these guidelines be used in conjunction with the City’s more general Development Permit Guidelines located in the OCP (Schedule 1 of this Bylaw).

Neither set of guidelines requires literal interpretation, in whole or in part. They will, however, be taken into account in consideration of Development Permit applications, and the Development Permit Panel may, at its discretion, refuse or require modification to an application for failure to meet the spirit of these guidelines and/or the standards they prescribe.

Exemptions to the Development Permit process can be found in the OCP (Schedule 1 of the Bylaw).

8.1.2 JUSTIFICATION

It is the objective of these guidelines to promote a co-ordinated approach to commercial development on the south side of Steveston Highway and multiple-family development along the north side of the Steveston Highway due to the following factors:

- a) The intersection of Steveston Highway and No. 5 Road is an important gateway into Richmond. New commercial and residential development should provide an attractive, welcoming entrance appropriate to this role as a key entrance to Richmond;

- b) Due to the traffic volumes there is a need to control access to and ensure safe and convenient pedestrian circulation along Steveston Highway; and
- c) Whenever different land uses adjoin, there is a need to ensure that new development blends in with the character and scale of existing developments and a need to buffer or mitigate potential impacts. Traffic, noise, lighting and other environmental conditions must be taken into consideration in the design of the commercial development. The multi-family development should be designed to provide a boundary between the quiet, low density residential uses on the north side of Steveston Highway and the increasingly active, commercial and business park uses on the south side of Steveston Highway.

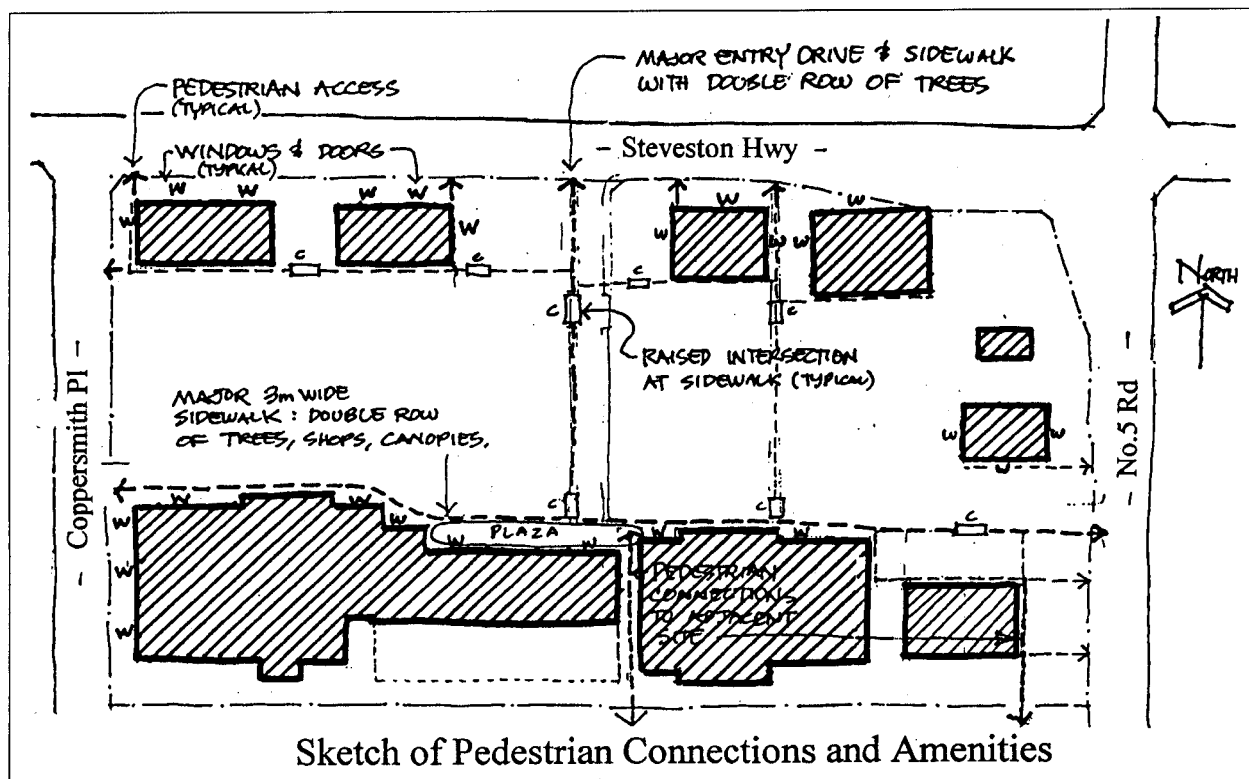
8.2 AREA A – COMMERCIAL DEVELOPMENT ALONG THE SOUTH SIDE OF STEVESTON HIGHWAY

The following guidelines apply to commercial development as shown in Area A on the Development Permit Area Map (inside front cover) in addition to the General and Commercial Guidelines for all Development Permits located in Schedule 1 to this Bylaw.

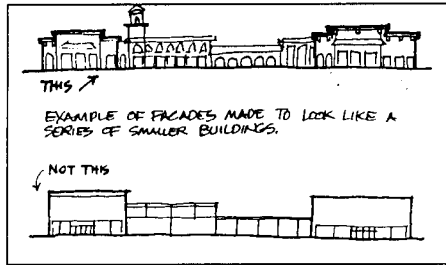
8.2.1 SETTLEMENT PATTERNS

- a) Pedestrian access to the main buildings on each site on both sides of Coppersmith Place should be provided as follows:
 - Minimum 1.5 m (4.9 ft.) wide sidewalks;
 - Located so as to provide a minimum of two accesses from Steveston Highway, one from No. 5 Road, two from Coppersmith Place, and two from Coppersmith Way;
 - Where walkways intersect vehicle paths, the roadways should be raised to the walkway level, and should have a non-asphalt paving material;
 - At least one walkway connecting to the Steveston Highway sidewalk should be fully covered and lighted; and
 - All walkways should be accessible to disabled persons.

- b) Vehicle connections to the streets on each site on both sides of Coppersmith Place should be as follows:
 - To Steveston Highway: one right-in-right-out;
 - To Coppersmith Place: one full movements (at south end) and one loading bay access;
 - To No. 5 Road: one right-in-right-out, one full movements. One to be shared with adjacent property on the south;
 - To Coppersmith Way: one full movement; and
 - Width to be minimized, to promote pedestrian safety.
- c) Pedestrian/vehicle connections to the adjacent properties are to be encouraged. There should be at least one pedestrian connection through the main block of buildings (see Pedestrian Connection sketch).
- d) The design should create defensible spaces, which provide for easy surveillance and safety both day and night.



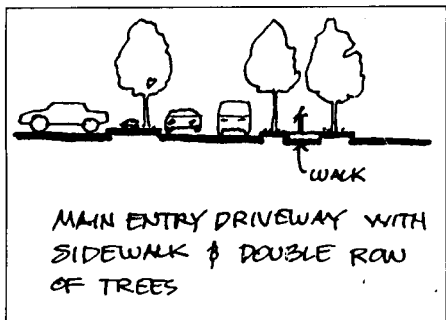
Pedestrian Connection Sketch



Building Façades

8.2.2 ARCHITECTURAL ELEMENTS

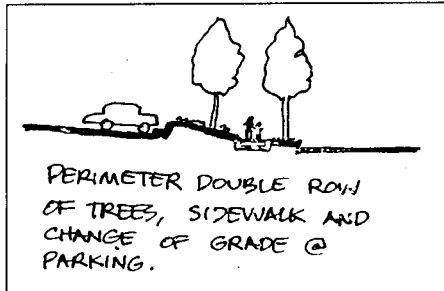
- a) Buildings should be designed so as to break down large façades into smaller elements to create an appearance of a series of smaller buildings (see Building Façades sketch).
- b) Shopfronts should have a minimum of 40% glazing, and all display windows and entrances should be protected from the weather by minimum 2 m (6.6 ft.) deep colonnades or canopies. The north side second floor should have an open corridor or colonnade across its full frontage.
- c) Buildings which front onto public streets should have display windows or glass doors for a minimum of 60% of the building edge. These areas should be paved for a minimum of 2 m (6.6 ft.) in front of the windows or doors.
- d) Pedestrian access areas (sidewalks) between parking lots and store entrances should be a minimum of 3 m (9.8 ft.) wide. Pedestrian amenities should be provided at regular intervals.
- e) On-site employee or public amenities should include change rooms, showers, lockers, a lounge, and a covered outdoor seating area.
- f) Signs should be integrated with the architecture, and limited to the following (in addition to the Sign Bylaw requirements):
 - Façade signs comprised of letters and logos affixed to the building, or in internally illuminated boxes - the latter to be limited to sign bands immediately above main floor windows/doors or copy on awnings; and
 - Freestanding signs limited to directional signs and signs indicating the name and general nature of the services, to a maximum of 2 m (6.6 ft.) in height.



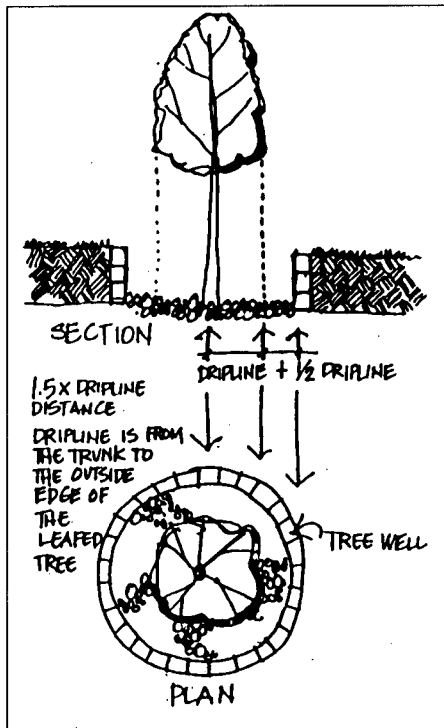
Tree Planting

8.2.3 LANDSCAPE ELEMENTS

- a) Edge conditions should create a high-quality pedestrian environment, by provision of boulevards, street trees, pedestrian amenities, lighting, and landscaping. Pedestrian/vehicle conflicts should be minimized.
- b) Landscaping should be used to create a predominant green aspect of the site and also to soften the presence of large numbers of vehicles, both in the parking lot and on the surrounding streets, by:



Grade Change



Tree Well

- Planting a double row of trees around the perimeter of the site and on the two main entry driveways, to form a canopy over the sidewalks and driveways (see Tree Planting sketch);
 - Planting "groves" of trees and shrubs in the parking lot so that, approximately 10 years after planting, at least 50% of the parking lot will be covered by a canopy of leaves in summer; and
 - Creating a change of grade at the edge of the parking lot and planting low shrubs so that, without compromising visibility and surveillance, parked cars are screened from the road (see Grade Change sketch).
- c) Existing trees should be preserved by:
- Retaining in situ complete with tree wells as necessary (see Tree Well sketch), or relocating on the site; and
 - Erecting construction fencing to City specs around all of the above trees prior to site pre-loading and to remain fenced throughout the construction period.
- d) Efforts should be made to retain, move offsite, or relocate other existing trees and shrubs. Provincial laws regarding cutting of trees containing songbird or raptor nests during the nesting season should be respected.
- e) The south edge of the property should be well landscaped, but should be carefully integrated with the adjoining properties. Fences or hedges on this perimeter should not exceed 1 m (3.3 ft.) in height.
- f) Landscape themes should include the following suggested mix of native and exotic plants, to maintain a consistent level of quality:
- Perimeter and parking lot tree types: Honey Locust, Chanticleer (Bradford) Pear, Sweet Gum, Katsura.
Shrub types: Blueberry, Currant, Red Osier Dogwood, Oregon Grape, Native (Swamp) Rose, Rhododendron; and
 - Feature trees and plants: Windmill Palm, Persian Ironwood, Monkey Puzzle, Sitka Spruce, Pacific Crabapple, Yucca, Bamboo, native and exotic tall grasses.
- g) Preserve natural heritage by retaining, relocating and augmenting existing healthy on-site trees and shrubs. Each existing tree larger than 20.3 cm (8 in.) dbh which is unavoidably cut should be replaced by two suitable trees of minimum 10.2 cm (4 in.) dbh. Wherever possible, plant new landscaping which will be beneficial to native and migratory birds.

- h) At least one lighted shelter should be provided for people waiting for busses.
- i) There should be at least one pedestrian plaza of a minimum size of 550 m² (5,920 ft²), with a minimum of 50 linear metres (164 linear feet) of seating. The plaza should also include a drinking fountain. The plaza should be adjacent to a building containing shop windows and canopies.

8.2.4 PARKING, GARBAGE, RECYCLING AND RELATED ELEMENTS

- a) Parking lot lighting should be directed away from residential areas so that there is a maximum of three footcandles at the north property line. Certain light standards should be designed to accommodate hanging flower baskets complete with irrigation.
- b) Bicycle parking should be provided as per the following minimum standards:
 - Class 1, long-term secured parking: 0.27 spaces per each 100 m² (1,076.43 ft²) of gross leasable area; and
 - Class 2, short term parking: 0.27 spaces per each 100 m² (1,076.43 ft²) of gross leasable area.For details, refer to Appendix 1 – Bicycle Parking and End of Trip Facilities.
- c) Signs and parking lot lighting should be compatible with the adjacent residential area.
- d) Garbage, recycling and pick-up should be situated in areas which do not conflict with pedestrian traffic, and should either be fully enclosed or screened with solid walls higher than the bins.

8.3 AREA B – NORTH SIDE OF STEVESTON HIGHWAY

The following guidelines apply to Area B as shown on the Development Permit Area Map (inside front cover). It is the objective of these guidelines to help define the form and character of new townhouse development along the north side of Steveston Highway to ensure it is both appropriate to the area's "Richmond gateway" location and proximity to Ironwood Shopping Centre, while also addressing livability issues related to vehicular access and traffic impacts.



Centre boulevard across from Ironwood

8.3.1 SETTLEMENT PATTERNS

To establish a pattern of dual fronting townhouse clusters (with sunny yards) linked by a pedestrian-friendly lane system and screened by a “formal front” oriented to Steveston Highway, new development should:

- a) Place emphasis on the establishment of a green, treed and landscaped streetscape along Steveston Highway punctuated by entranceways to individual townhouse clusters;
- b) Typically design townhouse clusters as a combination of rows of units aligned north-south (such that the fronts and rears of individual units are oriented east-west and unit sidewalls front onto Steveston Highway);
- c) Accommodate vehicular access via a public lane system situated along the north edge of Area B. In addition one temporary right-in/right-out access with a maximum dimension of 5 m (16.4 ft.) in width may be permitted on a temporary basis from Steveston Highway until such time as the lane is upgraded to City standards (see Landscape Elements for details regarding temporary vehicular access); and
- d) Setback:
 - Along Steveston Highway - 6 m (19.69 ft.), EXCEPT that where a berm is not provided (as described under section 1.3.3 Landscape Elements) or rows of units are aligned east-west, rather than north-south, the minimum setback shall be 12 m (39.37 ft.); and
 - Along the lane – 6.0 m (19.69 ft.), provided that porches and similar building features may project up to 1 m (3.28 ft.) into the setback.

8.3.2 ARCHITECTURAL ELEMENTS

To address noise-related traffic impacts and establish a pedestrian-friendly streetscape, new development should:

- a) Be designed to maintain an acceptable ambient noise level of 35 dB for indoor spaces and 55 dB for outdoor private spaces;
- b) Create a residential streetscape along the lane that reinforces its use and image as a special local, pedestrian-friendly street (rather than a service access) through features such as porches, front doors to individual townhouse units, bay windows, living/dining room windows, etc.; and

- c) Orient garages so as to be accessed via private, on-site driveways rather than directly from the public lane.

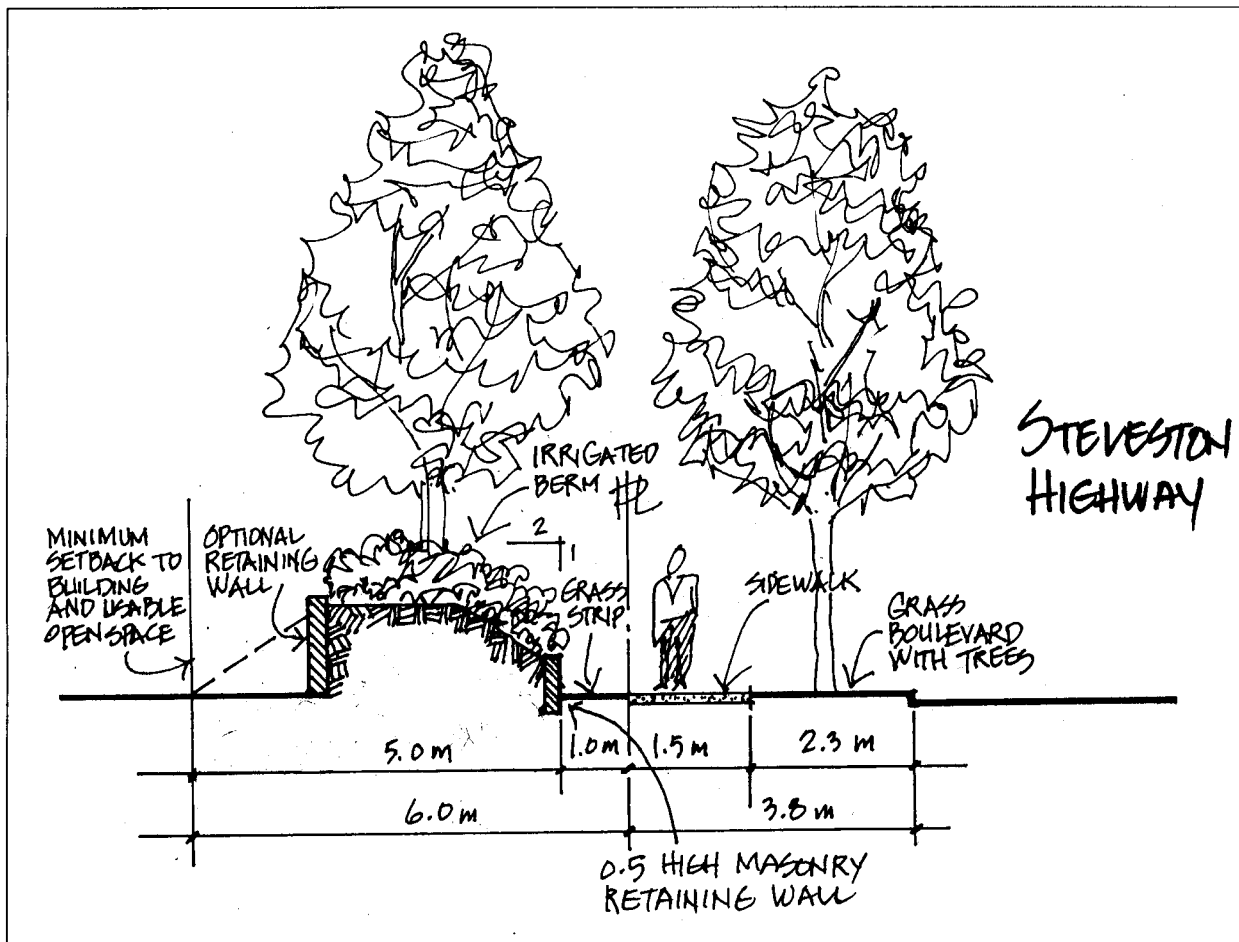
8.3.3 LANDSCAPE ELEMENTS

To enhance the appearance of Steveston Highway as a prominent vehicular artery, screen sensitive residential uses, and create a special, pedestrian-friendly lane environment, new development should:

- a) Provide clear signage on both Steveston Highway and the lane indicating addresses of developments. The Steveston Highway signage should specifically note that “lane access is available from Seaward Gate”;
- b) Along Steveston Highway, contribute a lush, green and pedestrian oriented landscape by accommodating:
- Installation of a 2.3 m (7.55 ft.) wide grass boulevard (complete with a single row of Pin Oaks) at the back of curb and a 1.5 m (4.92 ft.) wide concrete sidewalk;
 - Within the minimum 6 m (19.69 ft.) building setback, a 1 m (3.28 ft.) wide grass strip at the back of sidewalk and a continuous landscaped berm at least 1.2 m (3.94 ft.) high (measured from the adjacent curb), EXCEPT as required to maintain existing mature trees (See diagram: *Steveston Highway Frontage*);
 - Any fencing incorporated as part of the berm should be located at a minimum of 4.4 m (14.43 ft.) from the south property line and not higher than 1.5 m (4.92 ft.) (measured from the curb) EXCEPT where a fence is adjacent to private outdoor space it may be as tall as 1.8 m (5.90 ft.);

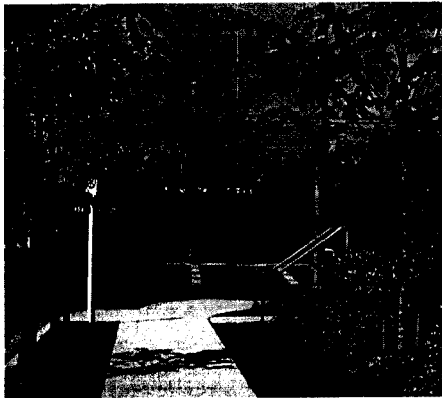


Berm on south side of Steveston Highway

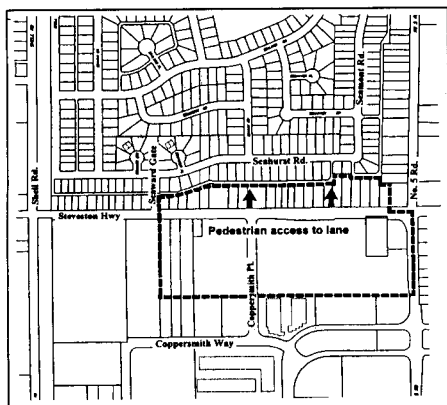


Steveston Highway Frontage

- Significant planting within the berm area, including large growing trees and plant material chosen for its seasonal colour, screening abilities, and visual interest;
- A minimal width and number of breaks in the berm for pedestrian and where necessary, temporary vehicular access. Pedestrian access should be shared by a number of units and typically be confined to a 3 m (13 ft.) break in the berm. One temporary vehicular access is permitted per development and should typically be confined to a 6 m (19.7 ft.) wide break in the berm to accommodate both vehicles and pedestrians. Temporary vehicular access should be designed to be readily convertible to pedestrian only use and character without significant cost (ie, decorative paving materials for the temporary driving surface, use of landscaping and/or bollards); and



Crosswalk at Coppersmith Way

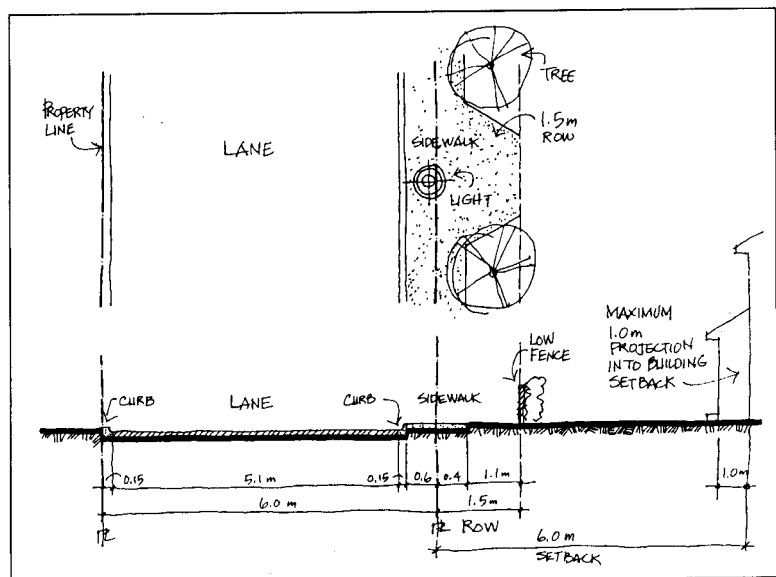


Pedestrian Pathways

- Pedestrian pathways linking the single-family neighbourhood to the north of Area B to the Ironwood shopping centre. Pathways should be located at Coppersmith Way and where the alignment of the lane turns north to Seahurst Road (see Pedestrian Pathways diagram). The pathways require a minimum of 2.4 m (7.87 ft.) of paved surface to accommodate pedestrian and bicycles with a minimum of 0.8 m (2.6 ft.) landscaping on either side for a total width of 4 m (13.12 ft.).

c) For the public lane provide:

- A 6.0 m (19.685 ft.) wide laneway with roll curbs and lighting strip;
- A 1.5 m (4.92 ft.) right-of-way on the southern edge of the laneway to accommodate a concrete aggregate sidewalk and a single row of trees at 9 m (29.53 ft.) on centre (see Lane Frontage sketch);
- High visibility between the lane and the adjacent properties by ensuring that fencing along the lane is no higher than 1.2 m (3.94 ft.). Additionally, any fencing should be located no closer than 1.5 m (4.92 ft.) from the northern property line; and
- Facilities for mail and garbage.



Lane Frontage

**BICYCLE PARKING AND
END OF TRIP FACILITIES**

APPENDIX 1

New development should accommodate the bicycle parking and end-of-trip facility needs of multiple-family residential dwellers, workers, and visitors.

a) CLASS 1 Parking

Secured, long-term bicycle parking shall be provided for the use of residential use and non-residential tenants in the form of waterproof bicycle lockers, or bicycle rooms complete with bicycle racks.

- (i) Parking facilities shall: be at-grade; have uniform 160 lux (min.) lighting which yields true colours; and, be within sight of building entry, elevator, and/or security.
- (ii) Bicycle rooms shall provide: lockable door(s) with window(s); tamper-proof, motion-activated security lighting; and unobstructed view of each room from its entry; and, facilities for no more than 20 bicycles per room (enabling owners to identify one another).
- (iii) Bicycle lockers shall: be constructed of solid, opaque, weather-proof and theft-resistant material, with no exposed fittings or connectors; have lockable doors which open to the full height and width of each locker; be grouped together; not be located at the head of parking spaces; and, have clear minimum dimensions of:

Length	1.80 m (5.91 ft.)
End Width at Door	0.60 m (1.97 ft.)
End Width Opposite Door	0.22 m (0.72 ft.)
Height	1.20 m (3.94 ft.)

b) CLASS 2 Parking

Unsecured, short-term bicycle parking shall be provided for visitors in the form of bicycle racks located within 15 m (49.2 ft.) of a principal building entry.

- (i) Parking shall be situated in well-lit locations, clearly visible from principal building entries and/or public roads.

- (ii) Bicycle racks shall be made of sturdy, theft-resistant material, securely anchored to the floor or ground.
- (iii) Bicycle racks shall be designed to support the bicycle frame, not the wheels, and allow both the frame and the front wheel to be locked to the rack with a U-style lock.